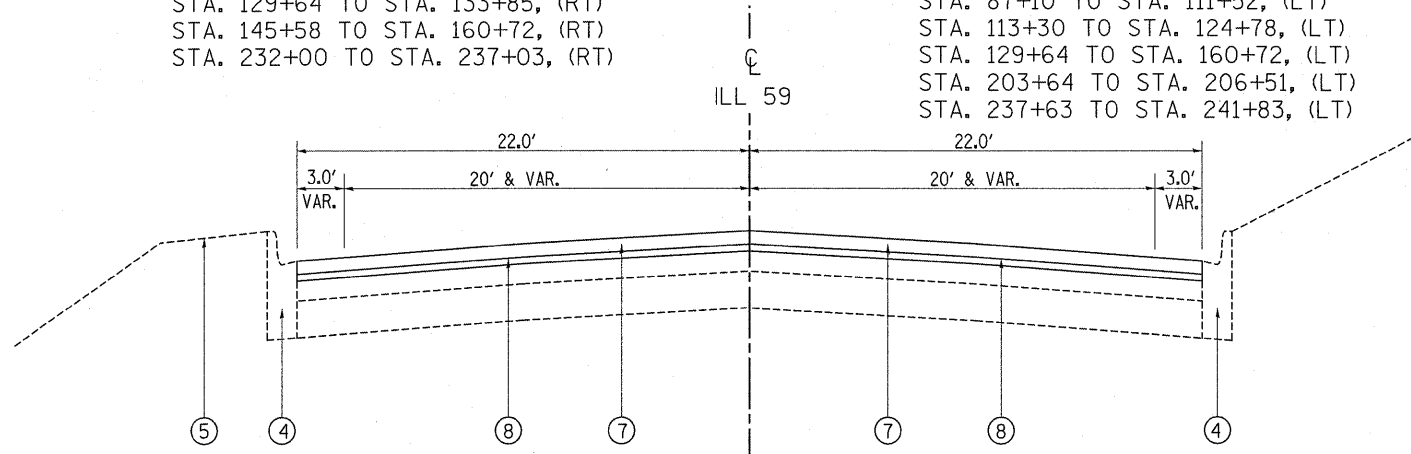


**EXISTING TYPICAL SECTION**  
ILL 59

STA. 10+00 TO STA. 19+63, (RT)  
 STA. 27+69 TO STA. 33+57, (RT)  
 STA. 58+06 TO STA. 75+71, (RT)  
 STA. 79+63 TO STA. 84+28, (RT)  
 STA. 129+64 TO STA. 133+85, (RT)  
 STA. 145+58 TO STA. 160+72, (RT)  
 STA. 232+00 TO STA. 237+03, (RT)

STA. 10+00 TO STA. 23+44, (LT)  
 STA. 27+91 TO STA. 32+96, (LT)  
 STA. 51+95 TO STA. 62+01, (LT)  
 STA. 66+30 TO STA. 68+96, (LT)  
 STA. 87+10 TO STA. 111+52, (LT)  
 STA. 113+30 TO STA. 124+78, (LT)  
 STA. 129+64 TO STA. 160+72, (LT)  
 STA. 203+64 TO STA. 206+51, (LT)  
 STA. 237+63 TO STA. 241+83, (LT)



**PROPOSED TYPICAL SECTION**  
ILL 59

STA. 10+00 TO STA. 19+63, (RT)  
 STA. 27+69 TO STA. 33+57, (RT)  
 STA. 58+06 TO STA. 75+71, (RT)  
 STA. 79+63 TO STA. 84+28, (RT)  
 STA. 129+64 TO STA. 133+85, (RT)  
 STA. 145+58 TO STA. 160+72, (RT)  
 STA. 232+00 TO STA. 237+03, (RT)

STA. 10+00 TO STA. 23+44, (LT)  
 STA. 27+91 TO STA. 32+96, (LT)  
 STA. 51+95 TO STA. 62+01, (LT)  
 STA. 66+30 TO STA. 68+96, (LT)  
 STA. 87+10 TO STA. 111+52, (LT)  
 STA. 113+30 TO STA. 124+78, (LT)  
 STA. 129+64 TO STA. 160+72, (LT)  
 STA. 203+64 TO STA. 206+51, (LT)  
 STA. 237+63 TO STA. 241+83, (LT)

**LEGEND**

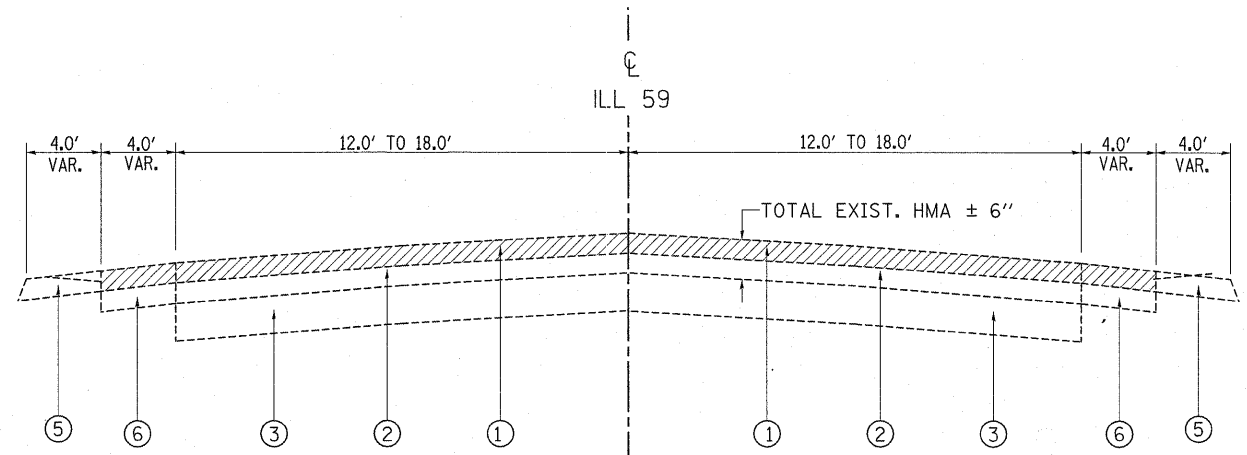
- ① PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4 "
- ② EXISTING REMAINING HMA AFTER MILLING, (±) 4 "
- ③ EXISTING PCC BASE COURSE, (±)9"
- ④ EXISTING COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING HMA SHOULDER
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2 "
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4 "
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

**HMA MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ NDES
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER, (MM), IL-4.75, N50	4% @ 50 GYR
CLASS D PATCHES, (HMA BINDER IL-19 mm)	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

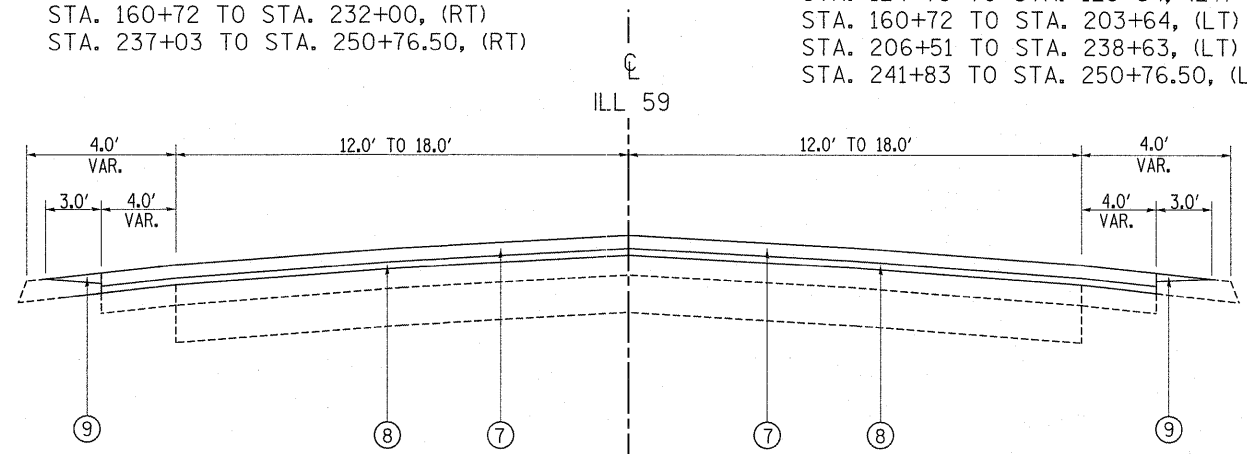
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.



**EXISTING TYPICAL SECTION**  
ILL 59

STA. 19+63 TO STA. 27+69, (RT)  
 STA. 33+57 TO STA. 58+06, (RT)  
 STA. 75+71 TO STA. 79+63, (RT)  
 STA. 84+28 TO STA. 86+66, (RT)  
 STA. 112+28 TO STA. 129+64, (RT)  
 STA. 133+85 TO STA. 145+58, (RT)  
 STA. 160+72 TO STA. 232+00, (RT)  
 STA. 237+03 TO STA. 250+76.50, (RT)

STA. 23+44 TO STA. 27+91, (LT)  
 STA. 32+96 TO STA. 51+95, (LT)  
 STA. 62+01 TO STA. 66+30, (LT)  
 STA. 68+96 TO STA. 87+10, (LT)  
 STA. 111+52 TO STA. 113+30, (LT)  
 STA. 124+78 TO STA. 129+64, (LT)  
 STA. 160+72 TO STA. 203+64, (LT)  
 STA. 206+51 TO STA. 238+63, (LT)  
 STA. 241+83 TO STA. 250+76.50, (LT)



**PROPOSED TYPICAL SECTION**  
ILL 59

STA. 19+63 TO STA. 27+69, (RT)  
 STA. 33+57 TO STA. 58+06, (RT)  
 STA. 75+71 TO STA. 79+63, (RT)  
 STA. 84+28 TO STA. 86+66, (RT)  
 STA. 112+28 TO STA. 129+64, (RT)  
 STA. 133+85 TO STA. 145+58, (RT)  
 STA. 160+72 TO STA. 232+00, (RT)  
 STA. 237+03 TO STA. 250+76.50, (RT)

STA. 23+44 TO STA. 27+91, (LT)  
 STA. 32+96 TO STA. 51+95, (LT)  
 STA. 62+01 TO STA. 66+30, (LT)  
 STA. 68+96 TO STA. 87+10, (LT)  
 STA. 111+52 TO STA. 113+30, (LT)  
 STA. 124+78 TO STA. 129+64, (LT)  
 STA. 160+72 TO STA. 203+64, (LT)  
 STA. 206+51 TO STA. 238+63, (LT)  
 STA. 241+83 TO STA. 250+76.50, (LT)

**NOTE:**  
**THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING**

FILE NAME =	USER NAME = hamdanah	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 59 IL RTE. 22 TO U.S. RTE 12 (RAND RD.)</b>			F.A.P. RTE. 338	SECTION 107-RS-3	COUNTY LAKE	TOTAL SHEETS 30	SHEET NO. 4
c:\pw_work\pwi\dot\hamdanah\d2223594\01111111-sht-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60M58	
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									