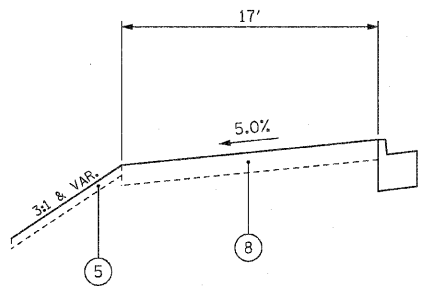


PROPOSED SUPERELEVATION SECTION
U.S. RTE. 30

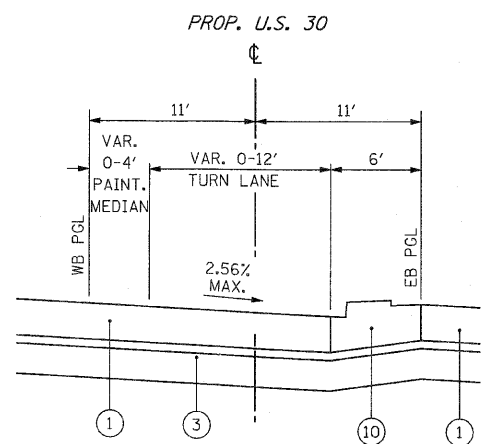
STA. 347+34.88 TO STA. 352+25.00

ROAD	CURVE NAME	S. E. RATE	STATION			
			BEGIN S. E.	FULL S. E.	FULL S. E.	END S. E.
U. S. RTE. 30	PR30CUR2	2.56%	216+34.22	219+01.42	222+88.73	225+55.93
	PR30CUR6	2.30%	324+25.26	327+05.71	351+03.77	
	PR30CUR7	2.56%		354+10.46	371+26.38	373+93.58



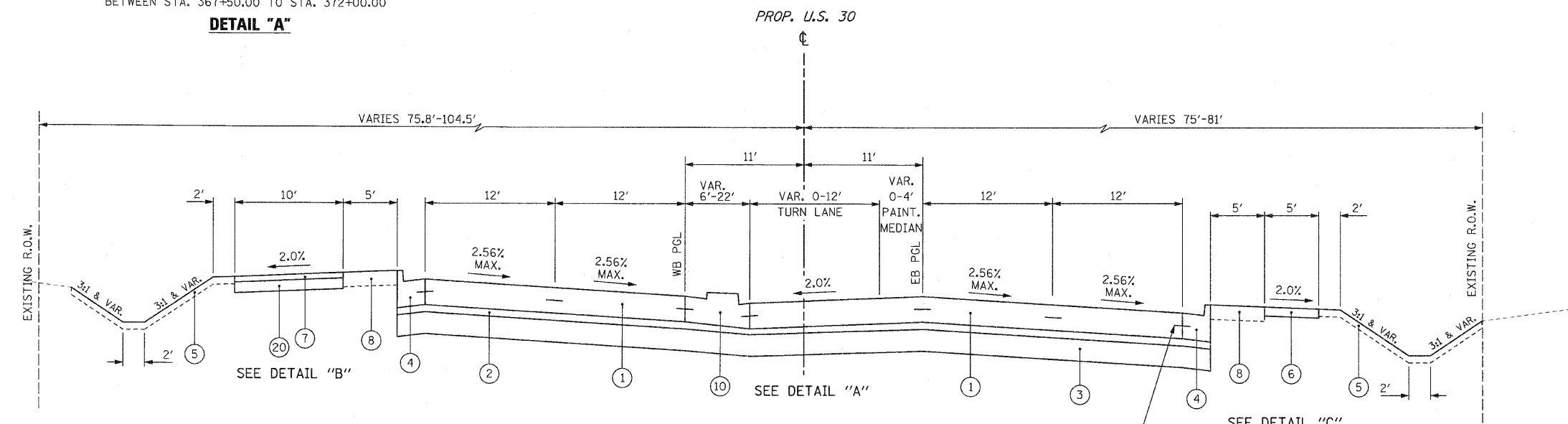
DETAIL "B"

STA. 216+34.22 TO STA. 218+82.00



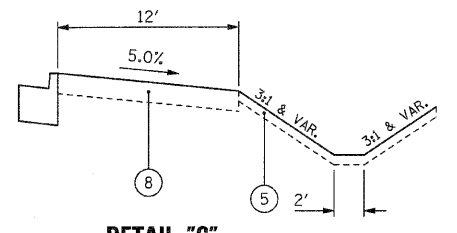
DETAIL "A"

NOTE: MEDIAN SHIFTS FROM EB PGL TO WB PGL BETWEEN STA. 367+50.00 TO STA. 372+00.00



PROPOSED SUPERELEVATION SECTION
U.S. RTE. 30

STA. 367+50.00 TO STA. 372+00.00



DETAIL "C"

STA. 216+34.22 TO STA. 218+82.00

- PROPOSED LEGEND**
- ① PORTLAND CEMENT CONCRETE PAVEMENT, 9 3/4" (JOINTED)
 - ② STABILIZED SUBBASE - HOT MIX ASPHALT, 4 1/2"
 - ③ AGGREGATE SUBGRADE 12", SPECIAL
 - ④ COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
 - ⑤ TOPSOIL FURNISH AND PLACE, 4"
 - ⑥ PORTLAND CEMENT CONCRETE SIDEWALK, 5"
 - ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 3"
 - ⑧ TOPSOIL FURNISH AND PLACE, 8"
 - ⑨ TOPSOIL FURNISH AND PLACE, 24"
 - ⑩ CONCRETE MEDIAN, TYPE SB-6.24 (MODIFIED)
 - ⑪ HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 9"
 - ⑫ AGGREGATE SUBGRADE, 12"
 - ⑬ LEVELING BINDER, (MACHINE METHOD), N50, 3/4"
 - ⑭ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1 1/2"
 - ⑮ COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
 - ⑯ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
 - ⑰ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 6 3/4"
 - ⑱ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
 - ⑲ HOT-MIX ASPHALT BASE COURSE WIDENING, 6 3/4"
 - ⑳ AGGREGATE BASE COURSE, TYPE B 6"
 - ㉑ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
 - ㉒ AGREGATE SHOULDERS, TYPE B 6"

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<p>SUPERELEVATION SECTIONS PROPOSED U.S. RTE. 30 (LINCOLN HWY)</p> <p>SCALE : 1" = NTS DRAWN BY : BEC DATE : 10/12/2010 CHECKED BY : GB</p>