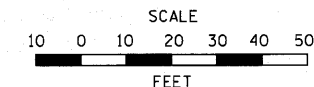


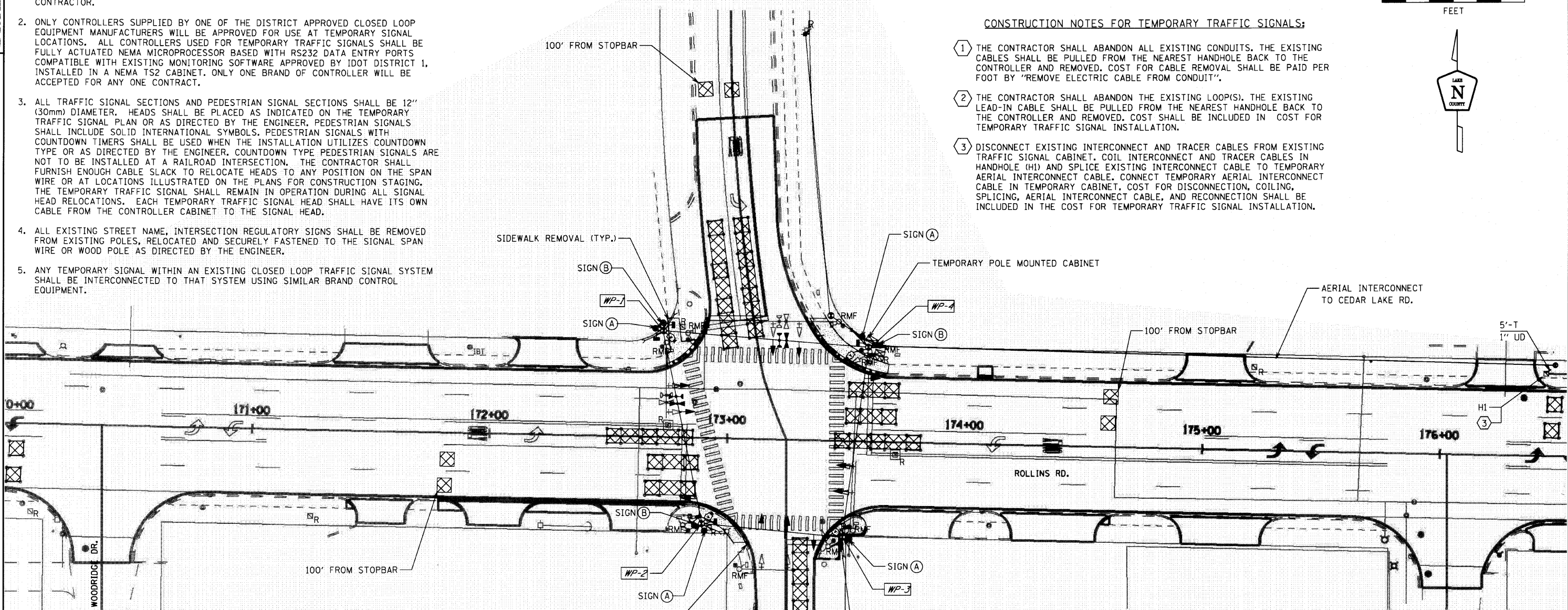
NOTES FOR TEMPORARY TRAFFIC SIGNALS:

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (30mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME, INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.



CONSTRUCTION NOTES FOR TEMPORARY TRAFFIC SIGNALS:

- 1 THE CONTRACTOR SHALL ABANDON ALL EXISTING CONDUITS. THE EXISTING CABLES SHALL BE PULLED FROM THE NEAREST HANDHOLE BACK TO THE CONTROLLER AND REMOVED. COST FOR CABLE REMOVAL SHALL BE PAID PER FOOT BY "REMOVE ELECTRIC CABLE FROM CONDUIT".
- 2 THE CONTRACTOR SHALL ABANDON THE EXISTING LOOP(S). THE EXISTING LEAD-IN CABLE SHALL BE PULLED FROM THE NEAREST HANDHOLE BACK TO THE CONTROLLER AND REMOVED. COST SHALL BE INCLUDED IN COST FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION.
- 3 DISCONNECT EXISTING INTERCONNECT AND TRACER CABLES FROM EXISTING TRAFFIC SIGNAL CABINET. COIL INTERCONNECT AND TRACER CABLES IN HANDHOLE (HI) AND SPLICE EXISTING INTERCONNECT CABLE TO TEMPORARY AERIAL INTERCONNECT CABLE. CONNECT TEMPORARY AERIAL INTERCONNECT CABLE IN TEMPORARY CABINET. COST FOR DISCONNECTION, COILING, SPLICING, AERIAL INTERCONNECT CABLE, AND RECONNECTION SHALL BE INCLUDED IN THE COST FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION.

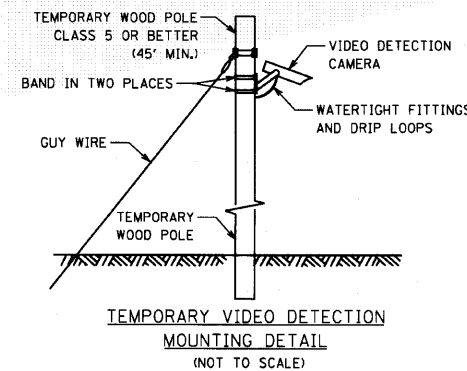


NOTES FOR TEMPORARY TRAFFIC SIGNALS:

6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS, AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL AT THE TIME OF THE TURN-ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT THE TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM (PTZ) CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.



R3-10 4 REQUIRED R3-10 4 REQUIRED
(INCLUDED IN COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION)



REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT EACH 1
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE COUNTY AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE COUNTY YARD AS PER THE TRAFFIC SIGNAL SPECIFICATIONS OR AS DIRECTED BY THE COUNTY TRAFFIC ENGINEER.

- AGENCY: LAKE COUNTY DEPARTMENT OF TRANSPORTATION
- 1 EACH CABINET AND CONTROLLER (COMPLETE)
 - 2 EACH LIGHT DETECTOR
 - 1 EACH LIGHT DETECTOR AMPLIFIER

- THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
- 6 EACH SIGNAL HEAD, 3-SECTION
 - 6 EACH SIGNAL HEAD, 5-SECTION
 - 8 EACH PEDESTRIAN SIGNAL HEAD
 - 8 EACH PEDESTRIAN PUSH-BUTTON
 - 4 EACH STEEL MAST ARM ASSEMBLY AND POLE
 - 4 EACH TRAFFIC SIGNAL POST
 - 1 EACH SERVICE INSTALLATION



USER NAME = JM	DESIGNED - DG	REVISED -
PLOT SCALE = 1 INCH = 20 FEET	DRAWN - JM	REVISED -
PLOT DATE = 10-05-2010	CHECKED - DG	REVISED -
	DATE - 10-05-2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL & TEMPORARY TRAFFIC SIGNAL INSTALLATION
ROLLINS RD. @ LOTUS DR.

ROUTE SECTION	SECTION NUMBER	SHEET	SHEETS
FAU 181	09-00999-07-TL	22	72
SCALE: 1"=20'		CONTRACT NO. 09472	