

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
733	IRS-3, 7RS-5	ILLINOIS	48	1
CONTRACT NO. 72880			ADAMS & HANCOCK	

D-96-515-04

FOR INDEX OF SHEETS, SEE SHEET NO. 2

HIGHWAY CLASSIFICATION

F.A.P. ROUTE 733 (IL 61) & (IL 94)

CLASSIFICATION: MINOR ARTERIAL

ADT: 724 (2009) PV = 83.0%
 ADT: 881 (2029) SU = 6.0%
 DESIGN SPEED: 55 MPH MU = 11.0%
 POSTED SPEED: 55 MPH

**PROPOSED
HIGHWAY PLANS**

**FAP ROUTE 733 (IL 61 & IL 94)
SECTION 1RS-3, 7RS-5
PROJECT : F-0733(007)
RESURFACING
ADAMS & HANCOCK COUNTY
C-96-140-10**

LIMITS OF IMPROVEMENT
ENDS AT STA. 197+32.00

SECTION IRS-3, 7RS-5
ENDS AT STA. 196+97.00

BRIDGE OMISSION #2
STA. 125+35.00 TO 123+19.50
SN 034-0065

(STA. EON. #2)
STA. 7+53.50 (AH)
STA. 1449+49.65 (BK)

BRIDGE OMISSION #1
STA. 1261+39.25 TO 1260+00.75
SN 001-0002

SECTION IRS-3, 7RS-5
BEGINS AT STA. 1233+64.50

LIMITS OF IMPROVEMENT
BEGINS AT STA. 1233+29.50

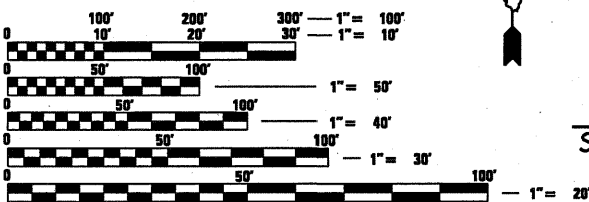
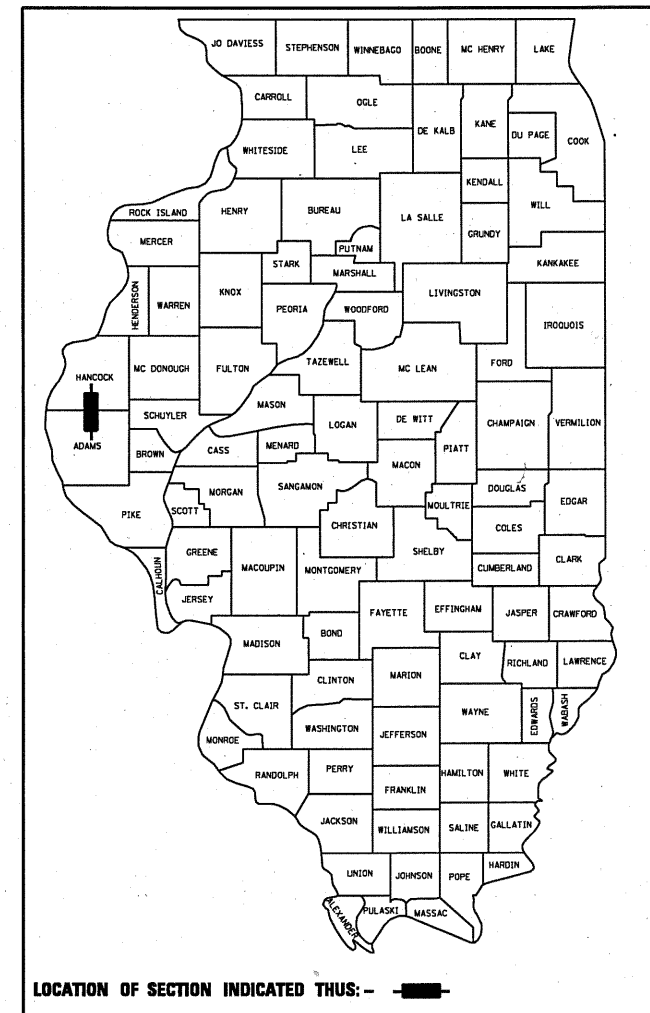
(STA. EON. #6)
STA. 70+93.13 (AH)
STA. 70+50.52 (BK)

(STA. EON. #5)
STA. 59+47.54 (AH)
STA. 60+40.30 (BK)

(STA. EON. #4)
STA. 0+00.94 (AH)
STA. 159+40.16 (BK)

(STA. EON. #3)
STA. 134+50.00 (AH)
STA. 130+62.24 (BK)

(STA. EON. #1)
STA. 1237+84.46 (AH)
STA. 1242+13.77 (BK)

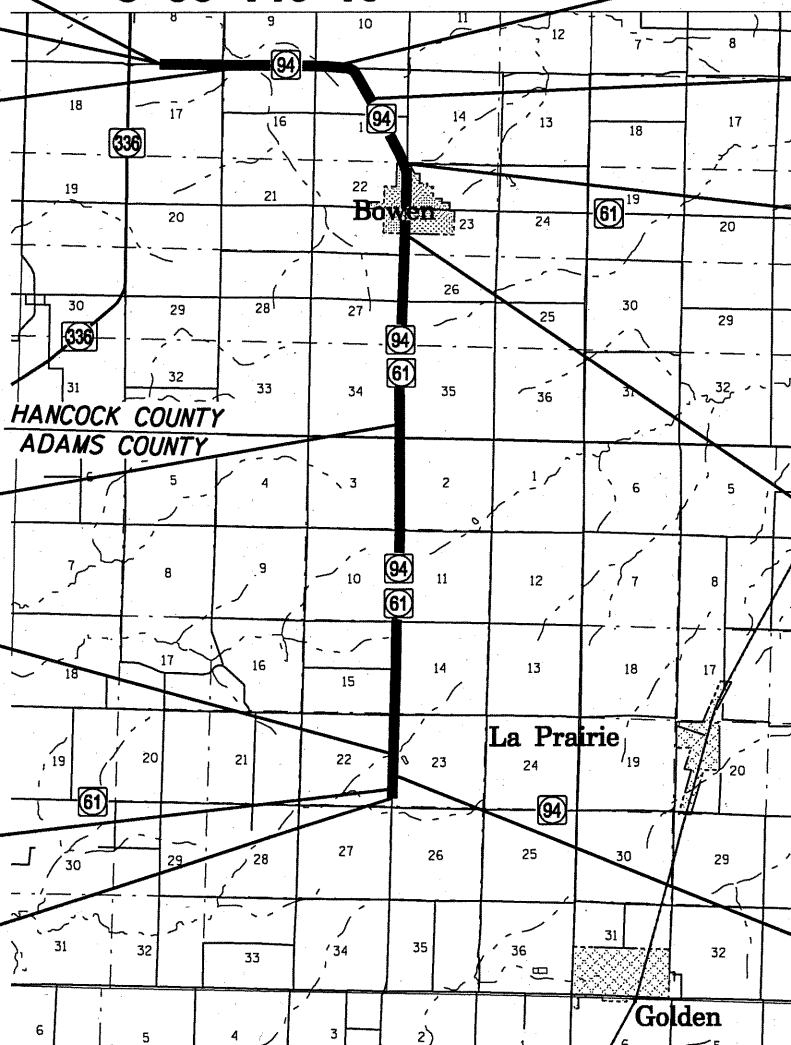


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: SAL MADONIA (217) 782-4761
TEAM ENGINEER: RENE CABRERA (217) 557-9062

CONTRACT NO. 72880



R 6 W LOCATION MAP R 5 W

GROSS LENGTH = 56,416.51 FT. = 10.68 MILE
NET LENGTH = 56,206.51 FT. = 10.65 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED September 14, 2010
Rose Z. Danks
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Dec 10 2010
Scott E. Stitt, P.E. /el
 acting ENGINEER OF DESIGN AND ENVIRONMENT

Dec 10 2010
Christine M. Leadbeater
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

GENERAL NOTES

- ① WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS UNTIL THE OWNER, AND AUTHORIZED SURVEYOR OR AGENT HAS WITNESSES OR OTHERWISE REFERENCED THEIR LOCATION.
- ② THE NOMINAL THICKNESS FOR BASE AND SURFACE COURSES ARE SHOWN ON THE TYPICAL SECTIONS, STANDARDS, SCHEDULES, OR SPECIAL DETAILS. THE CONSTRUCTED THICKNESS OF THE ABOVE ITEM SHALL NOT BE LESS THAN 90 PERCENT OF THE NOMINAL THICKNESS AT ANY LOCATION.
- ③ THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATION OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- ④ ANY REFERENCE TO A STANDARD IN THE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION, AS INDICATED BY THE SUB-NUMBER LISTED IN THE INDEX OF SHEETS, OR THE COPY OF THE STANDARD INCLUDED IN THE PLANS.
- ⑤ THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE JULIE NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCED NOTICE IS REQUIRED. ANY DAMAGE TO THE UNDERGROUND FACILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTORS EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS.
- ⑥ THE LOCATIONS OF THOSE BURIED AND ABOVEGROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVE GROUND UTILITY LOCATIONS, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR, REROUTING, DISCONNECTION, PROTECTION, ...ETC OF ANY UTILITIES MUST BE COORDINATED BETWEEN CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDOUS ASSOCIATED WITH BURIED AND ABOVE GROUND UTILITIES REMAIN THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- ⑦ THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE:

LOCATION(S):	IL 61/94	IL 61/94	IL 61/94
MIXTURE USE(S):	HMA CONC SURF CSE	LEV BIND MM N50	PATCHING
AC/PG:	PG 64-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS	4.0 @ N design = 50	4.0 @ N design = 50	4.0 @ N design = 50
MIXTURE COMPOSITION (Gradation Mixture)	IL 9.5 OR 12.5	IL 9.5	IL 19.0
FRICITION AGGREGATE	MIX "C"	N/A	N/A

LOCATION(S):	IL 61/94	Side Rds. & Entr.
MIXTURE USE(S):	HMA SHOULDERS	INCIDENTAL HMA SURF
AC/PG:	PG 64-22	PG 64-22
DESIGN AIR VOIDS	4.0 @ N design = 50	4.0 @ N design = 50
MIXTURE COMPOSITION (Gradation Mixture)	IL 9.5 OR 12.5	IL 9.5 OR 12.5
FRICITION AGGREGATE	MIX "C"	MIX "C"

- ⑧ IN ADDITION TO FIELD AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- ⑨ THE EXACT LOCATIONS AND SIZES OF PAVEMENT PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- ⑩ THE APPROXIMATE AVERAGE THICKNESS OF 2" IS USED FOR CALCULATING QUANTITIES FOR AGGREGATE SHOULDERS, TYPE B.
- ⑪ SEEDING INVOLVED IN EARTH OR FURNISHED EXCAVATION, SHOULDER WIDENING, AND INSTALLATION OF TYPE I (SPECIAL) GUARDRAIL TERMINALS SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER AND BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR TRAFFIC BARRIER TERMINAL, TYPE I, (SPECIAL) TANGENT.
- ⑫ EXISTING ROADWAY RUMBLE STRIPS BETWEEN STATION 1236+02.50 TO 1239+33.00 LT SHALL BE REMOVED AND INCLUDED IN THE COST FOR HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH.

RATES OF APPLICATION TABLES

THE FOLLOWING RATES OF APPLICATION HAVE BEEN ASSUMED IN CALCULATING PLAN QUANTITIES.

BITUMINOUS MATERIALS (PRIME COAT):	0.00038 TON / 50 YD (ON PAVEMENT)
AGGREGATE MATERIALS (PRIME COAT):	0.001425 TON / 50 YD (ON AGGREGATE)
HMA SURFACE MIX C / BINDER (112 LBS):	0.056 TON / 50 YD • In
AGGREGATE MATERIALS:	2.05 TON / CU YD

COMMITMENTS

(NONE)

INDEX OF SHEETS

- 1 COVER SHEET
- 2 INDEX OF SHEETS
- 3 SUMMARY OF QUANTITIES
- 4 - 8 TYPICAL SECTIONS (5)
- 9 - 15 SCHEDULE OF QUANTITIES (7)
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- 39 MISCELLANEOUS DETAIL (1)
- 40 - 43 SUPERELEVATION DETAILS (4)
- 44 - 48 ENTRANCE AND SIDEROAD DETAILS (5)

STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C & D PATCHES
630001-09	STEEL PLATE BEAM GUARDRAIL
630301-05	SHOULDER WIDENING FOR TYPE I (SPECIAL) GUARDRAIL TERMINALS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701006-03	OFF ROAD OPERATIONS 2L, 2W 15' TO 24' FROM PAVEMENT EDGE (GUARDRAIL INSTALLATION)
701011-02	OFF ROAD MOVING OPERATIONS 2L, 2W DAY ONLY (SHOULDER WORK)
701201-04	LANE CLOSURE 2L, 2W DAY ONLY, FOR SPEEDS ≥ 45MPH (PATCHING)
701301-04	LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS (MARKING PATCHES & CLEAN-UP)
701306-03	LANE CLOSURE 2L, 2W SLOW MOVING OPERATIONS - DAY ONLY FOR SPEEDS ≥ 45MPH (BIT SURF, SHDR OPS, & MILLING)
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY (PAVEMENT MARKING & CLEANUP)
701501-06	URBAN LANE CLOSURE 2L, 2W UNDIVIDED
701901-01	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-02	TYPICAL PAVEMENT MARKING
781001-03	TYPICAL APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKERS

THE ITALICIZED TEXT DESCRIBING STANDARDS ARE GENERAL USES FOR THE APPLICATION SPECIFIED AND NOT LIMITED TO THEM.

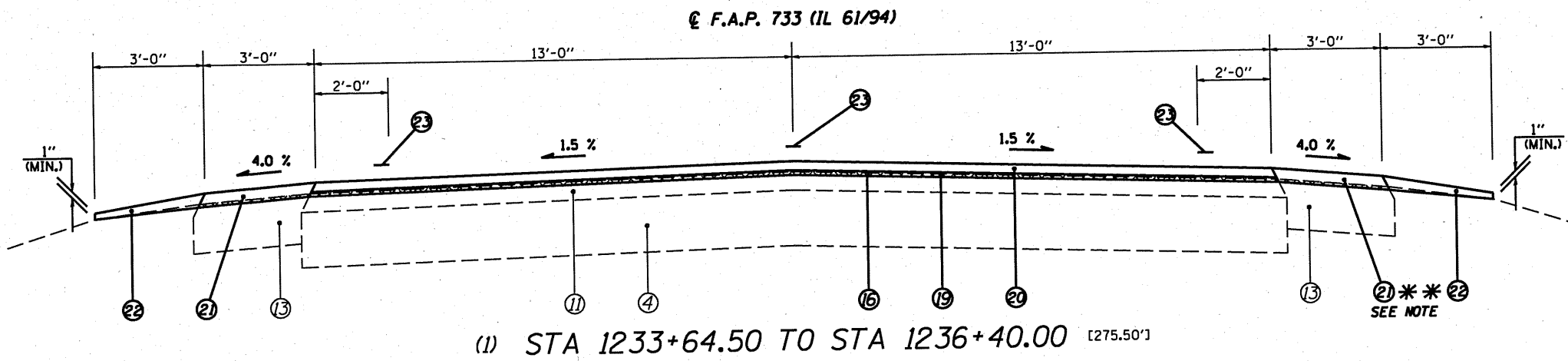
EXAMINED <u>August 23</u> 20 <u>10</u> <i>ARMU</i>
PROGRAM DEVELOPMENT ENGINEER
EXAMINED <u>SEPT 1</u> 20 <u>10</u> <i>Jim Z...</i>
PROGRAM IMPLEMENTATION ENGINEER
DISTRICT SIX
EXAMINED <u>September 1</u> 20 <u>10</u> <i>Paul Waller</i>
OPERATIONS ENGINEER

FILE NAME =	USER NAME = coxju	DESIGNED - RSC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = Sep-13-2010 01:41:38PM	DATE -	REVISED -		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			CONTRACT NO. 72880
										ILLINOIS FED. AID PROJECT ADAMS, HANCOCK

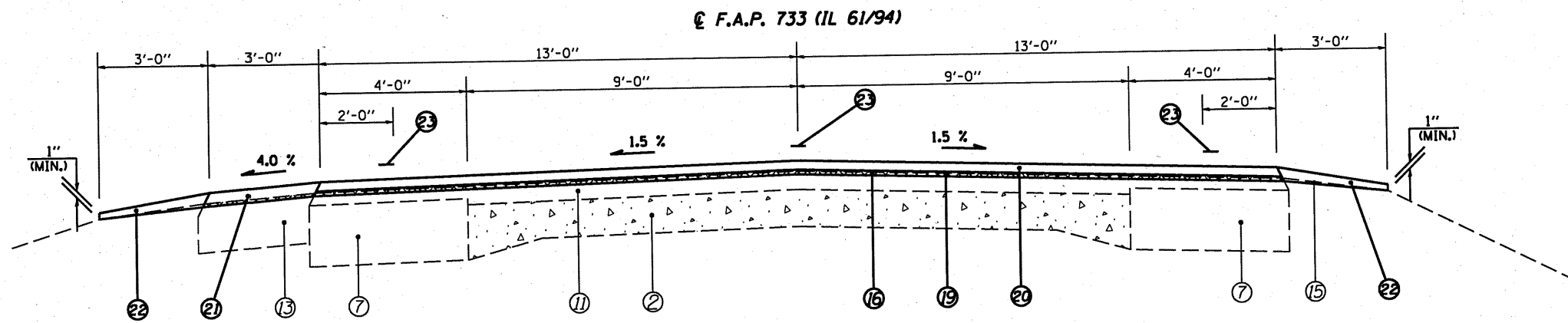
SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES				ADAMS COUNTY 80% FEDERAL 20% STATE IL 61/94	HANCOCK COUNTY 80% FEDERAL 20% STATE IL 61/94	SUMMARY OF QUANTITIES				ADAMS COUNTY 80% FEDERAL 20% STATE IL 61/94	HANCOCK COUNTY 80% FEDERAL 20% STATE IL 61/94
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005	CONSTRUCTION TYPE CODE 0005	SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005	CONSTRUCTION TYPE CODE 0005
CODE NO.	ITEM	UNIT	TOT. QUAN.			CODE NO.	ITEM	UNIT	TOT. QUAN.		
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SO YD	132,115	61,525	70,590	*78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	42		42
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	53	15	38	*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	715	268	447
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	56	24	32	*78200420	GUARDRAIL MARKERS, TYPE B	EACH	4		4
40600300	AGGREGATE (PRIME COAT)	TON	278	124	154	*78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	18	4	14
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	5,531	2,565	2,966	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	518	94	424
40600895	CONSTRUCTING TEST STRIP	EACH	1		1						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	3,260	638	2,622						
40600990	TEMPORARY RAMP	SO YD	87	43	44						
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	13,691	5,150	8,541						
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	444	124	320						
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	2,926		2,926						
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	376		376						
44200168	PAVEMENT PATCHING, TYPE II, 14 INCH	SO YD	675	270	405						
44200172	PAVEMENT PATCHING, TYPE III, 14 INCH	SO YD	425	170	255						
44200174	PAVEMENT PATCHING, TYPE IV, 14 INCH	SO YD	180	72	108						
48101200	AGGREGATE SHOULDERS, TYPE B	TON	3,893	1,587	2,306						
48203100	HOT-MIX ASPHALT SHOULDERS	TON	67	50	17						
*63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6' ^{FOOT} POSTS	FOOT	25		25						
*63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	18	4	14						
63200310	GUARDRAIL REMOVAL	FOOT	890	200	690						
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	1.6	2.4						
67100100	MOBILIZATION	L SUM	1	0.4	0.6						
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	0.4	0.6						
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	0.4	0.6						
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1		1						
70300100	SHORT TERM PAVEMENT MARKING	FOOT	15,387	5,788	9,599						
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	135,233	52,749	82,484						
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	42		42						
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	1,710	643	1,067						
*72000100	SIGN PANEL - TYPE 1	SO FT	9	9							
*72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	14	14							
*78001120	PAINT PAVEMENT MARKING - LINE 5"	FOOT	135,233	52,749	82,484						

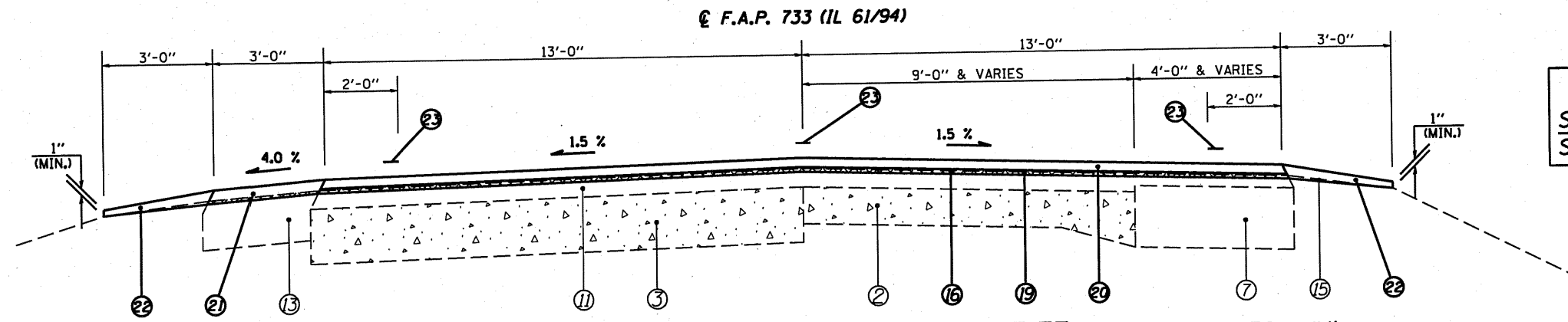
**Specialty Items*



(1) STA 1233+64.50 TO STA 1236+40.00 [275.50']



(2) STA 1236+40.00 TO STA 1239+95.67 [355.67']



(3) STA 1239+95.67 TO STA 1242+13.77 [218.1'] (STA. EON. #1)

LEGEND

- ① EXIST. LIME MODIFIED SOIL 12"
- ② EXIST. P.C.C. PAVEMENT 9"-6"-9"
- ③ EXIST. P.C.C. PAVEMENT 9"
- ④ EXIST. BITUMINOUS BASE COURSE 9"
- ⑤ EXIST. BITUMINOUS BASE COURSE 12"
- ⑥ EXIST. BITUMINOUS BASE COURSE 13 1/2"
- ⑦ EXIST. BITUMINOUS BASE COURSE WIDENING 10"
- ⑧ EXIST. BITUMINOUS BASE COURSE WIDENING 9"
- ⑨ EXIST. P.C.C. WIDENING
- ⑩ EXIST. BITUMINOUS CONCRETE BINDER COURSE, VARIABLE DEPTH
- ⑪ EXIST. BITUMINOUS OVERLAY 3"
- ⑫ EXIST. BITUMINOUS OVERLAY 1 1/2"
- ⑬ EXIST. BITUMINOUS SHOULDERS 8"
- ⑭ EXIST. CONCRETE GUTTER, TYPE B
- ⑮ EXIST. AGGREGATE SHOULDER
- ⑯ PROP. HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) *
- ⑰ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- ⑱ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑲ PROP. LEVELING BINDER (MACHINE METHOD), N50 3/4"
- ⑳ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 1 1/2"
- ㉑ PROP. HOT-MIX ASPHALT SHOULDERS
- ㉒ PROP. AGGREGATE SHOULDERS, TYPE B
- ㉓ PROP. PAINT PAVEMENT MARKING - LINE 5"

* NOMINAL MILLING DEPTH 1/2" @ ①

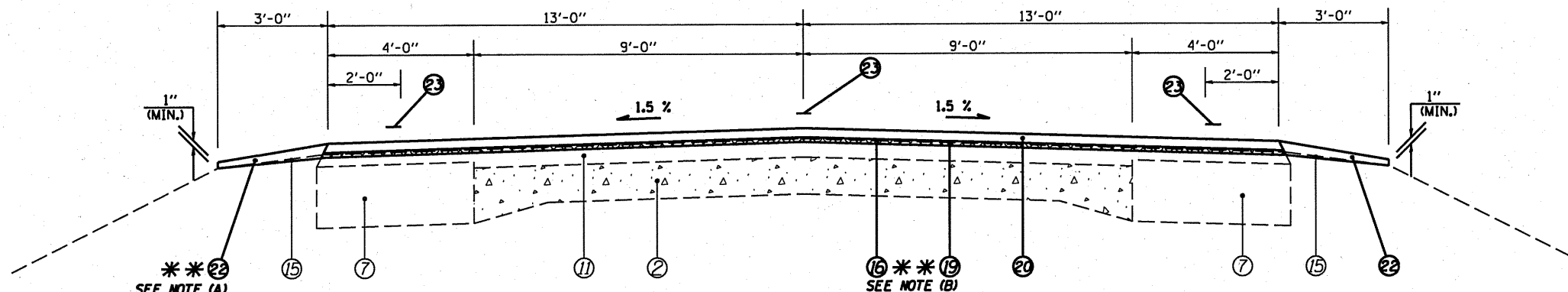
** NOTE:

PROP. HMA SHOULDER ENDS BEGIN AT STA 1236+50 RT.

(STA. EON. #1):
 STA. 1242+13.77 (BK) =
 STA. 1237+84.46 (AH)

FILE NAME =	USER NAME = coxjm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS		F.A.P. RTE. 733	SECTION IRS-3, TRS-5	COUNTY	TOTAL SHEETS 48	SHEET NO. 4	
ct:\pw\work\p\dot\coxjm\d8215198\d672884-Sht-Typical.dgn		DRAWN - RSC	REVISED -		SCALE: NTS	SHEET NO. 1 OF 5 SHEETS	STA. 1233+64.50 TO STA. 1242+13.77	ILLINOIS FED. AID PROJECT				
PLOT SCALE = 100.0000' / IN.		CHECKED - JWC	REVISED -		CONTRACT NO. 72880							
PLOT DATE = Sep-08-2010 10:42:14AM		DATE -	REVISED -		ADAMS, HANCOCK							

☉ F.A.P. 733 (IL 61/94)



1" (MIN.)

SEE NOTE (A) ** (4) STA 1237+84.46 TO STA 1260+00.75 [2,216.29'] (STA. EQN. #1)
 (5) STA 1261+39.25 TO STA 1449+49.65 [18,810.40'] (STA. EQN. #2)
 SEE NOTE (B) ** (6) STA 7+53.50 TO STA 29+33.00 [2,179.50']

BRIDGE OMISSION #1
 S.N.: 001-2010
 STA. 1260+00.75 TO
 STA. 1261+39.25

(STA. EQN. #1):
 STA. 1242+13.77 (BK) =
 STA. 1237+84.46 (AH)

(STA. EQN. #2):
 STA. 1449+49.65 (BK) =
 STA. 7+53.50 (AH)

LEGEND

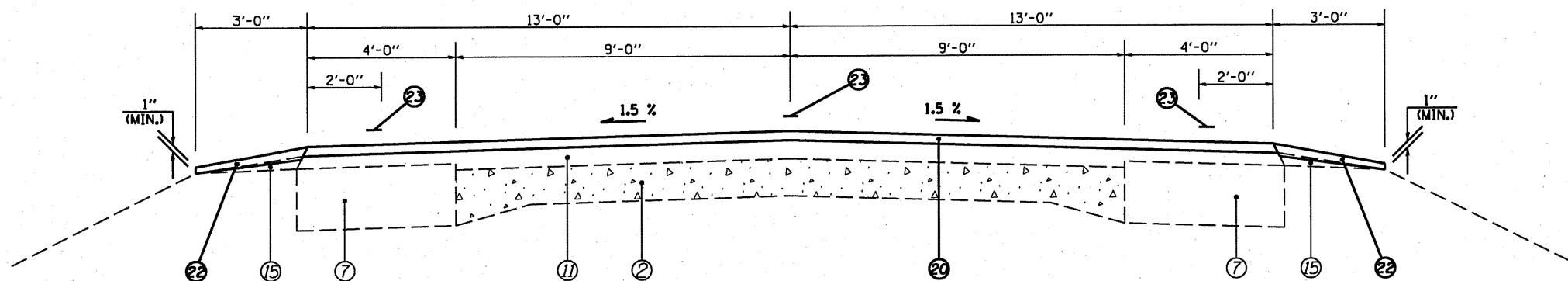
- ① EXIST. LIME MODIFIED SOIL 12"
- ② EXIST. P.C.C. PAVEMENT 9"-6"-9"
- ③ EXIST. P.C.C. PAVEMENT 9"
- ④ EXIST. BITUMINOUS BASE COURSE 9"
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- ⑮ EXIST. AGGREGATE SHOULDER
- ⑯ PROP. HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) *
- ⑰ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- ⑱ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑲ PROP. LEVELING BINDER (MACHINE METHOD), N50 3/4"
- ⑳ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 1 1/2"
- ㉑ PROP. HOT-MIX ASPHALT SHOULDERS
- ㉒ PROP. AGGREGATE SHOULDERS, TYPE B
- ㉓ PROP. PAINT PAVEMENT MARKING - LINE 5"

* NOMINAL MILLING DEPTH 1/2 " ☉ ☉

**** NOTE:**

- (A) PROP. HMA SHOULDER ENDS AT STA 1237+98.23 LT N. OF STA. EQN. #2
- (B) PROP. LEV BIND THICKNESS VAR. FR. 3/4" TO 0 & MILLING THICKNESS VAR. FR. 1/2" TO 0 FR. STA 29+18.00 TO 29+33.00
- (C) PROP. MILLING THICKNESS VAR. FR. 0 TO 1 1/2" FR. STA 125+35.00 TO 125+65.00

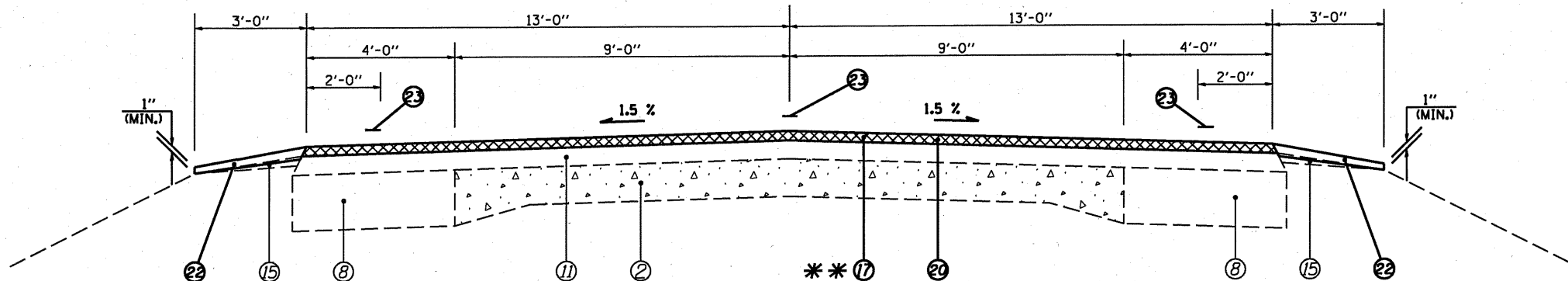
☉ F.A.P. 733 (IL 61/94)



1" (MIN.)

(7) STA 29+33.00 TO STA 125+35.00 [9,602.0']

☉ F.A.P. 733 (IL 61/94)



1" (MIN.)

(8) STA 125+35.00 TO STA 130+62.24 [527.24'] (STA. EQN. #3)

(STA. EQN. #3):
 STA. 130+62.24 (BK) =
 STA. 134+50.00 (AH)

FILE NAME =	USER NAME = cox.jw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS		F.A.P. RTE. 733	SECTION 1RS-3, 7RS-5	COUNTY *	TOTAL SHEETS 48	SHEET NO. 5	
ca:\pwwork\pwwork\cox\jw\d0215198\d672886	-Sht-Typical.dgn	DRAWN - RSC	REVISED -		SCALE: NTS	SHEET NO. 2 OF 5 SHEETS	STA. 1237+84.46 TO STA. 130+62.24	CONTRACT NO. 72880		ILLINOIS FED. AID PROJECT		
	PLOT SCALE = 100.0000 ' / IN.	CHECKED - JWC	REVISED -									
	PLOT DATE = Sep-08-2010 10:42:15AM	DATE -	REVISED -									

LEGEND

- ① EXIST. LIME MODIFIED SOIL 12"
- ② EXIST. P.C.C. PAVEMENT 9"-6"-9"
- ③ EXIST. P.C.C. PAVEMENT 9"
- ④ EXIST. BITUMINOUS BASE COURSE 9"
- ⑤ EXIST. BITUMINOUS BASE COURSE 12"
- ⑥ EXIST. BITUMINOUS BASE COURSE 13 1/2"
- ⑦ EXIST. BITUMINOUS BASE COURSE WIDENING 10"
- ⑧ EXIST. BITUMINOUS BASE COURSE WIDENING 9"
- ⑨ EXIST. P.C.C. WIDENING
- ⑩ EXIST. BITUMINOUS CONCRETE BINDER COURSE, VARIABLE DEPTH
- ⑪ EXIST. BITUMINOUS OVERLAY 3"
- ⑫ EXIST. BITUMINOUS OVERLAY 1 1/2"
- ⑬ EXIST. BITUMINOUS SHOULDERS 8"
- ⑭ EXIST. CONCRETE GUTTER, TYPE B
- ⑮ EXIST. AGGREGATE SHOULDER
- ⑯ PROP. HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) *
- ⑰ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- ⑱ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑲ PROP. LEVELING BINDER (MACHINE METHOD), N50 3/4"
- ⑳ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 1 1/2"
- ㉑ PROP. HOT-MIX ASPHALT SHOULDERS
- ㉒ PROP. AGGREGATE SHOULDERS, TYPE B
- ㉓ PROP. PAINT PAVEMENT MARKING - LINE 5"

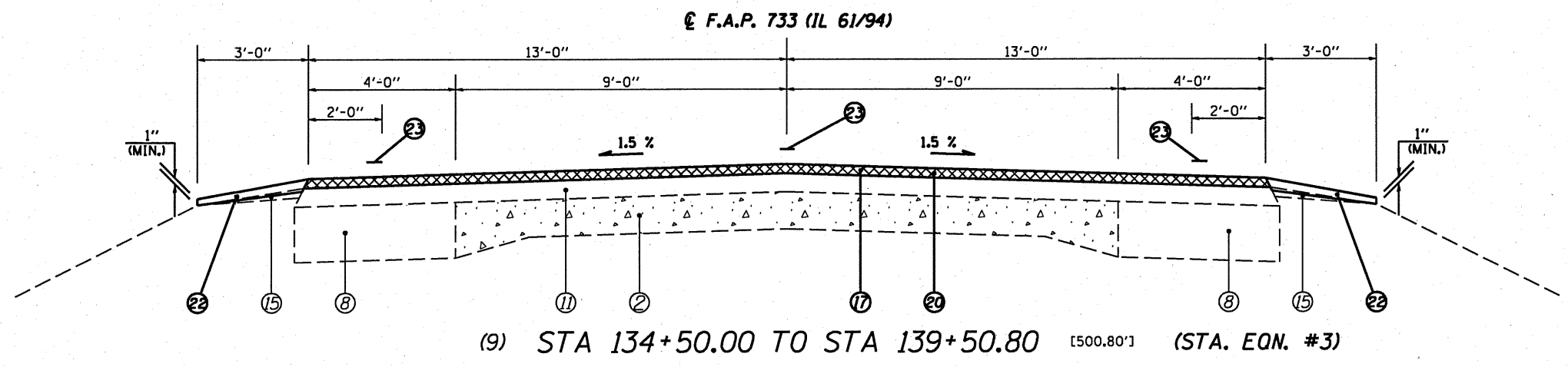
* NOMINAL MILLING DEPTH 1/2" @ ②

**** NOTE:**

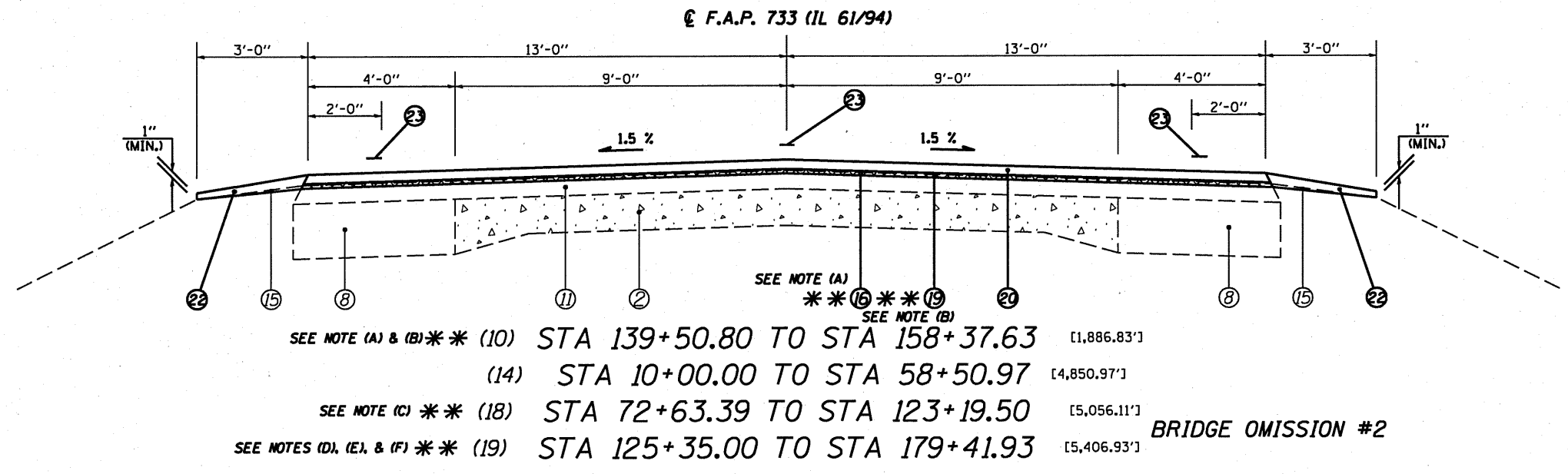
- (A) PROP MILLING THICKNESS VAR. FR. 1 1/2" TO 1/2" FR. STA. 139+50.80 TO 139+70.80
- (B) PROP LEV BIND THICKNESS VAR. FR. 0 TO 3/4" FR. STA. 139+70.80 TO 139+85.80
EXIST. BIT SHLD. PROP HMA SURF REM VD. & PROP HMA SHLD FR. STA.:
- (C) 123+54.50 TO 123+94.50 RT.
- (D) 124+92.00 TO 125+20.00 RT.
- (E) 125+20.00 TO 125+35.00 RT. &
- (F) 125+35.00 TO 127+17.00 RT.

BRIDGE OMISSION #2

S.N.: 034-0065
STA. 123+19.50 TO
STA. 125+35.00



(STA. EQN. #3):
STA. 130+62.24 (BK) =
STA. 134+50.00 (AH)



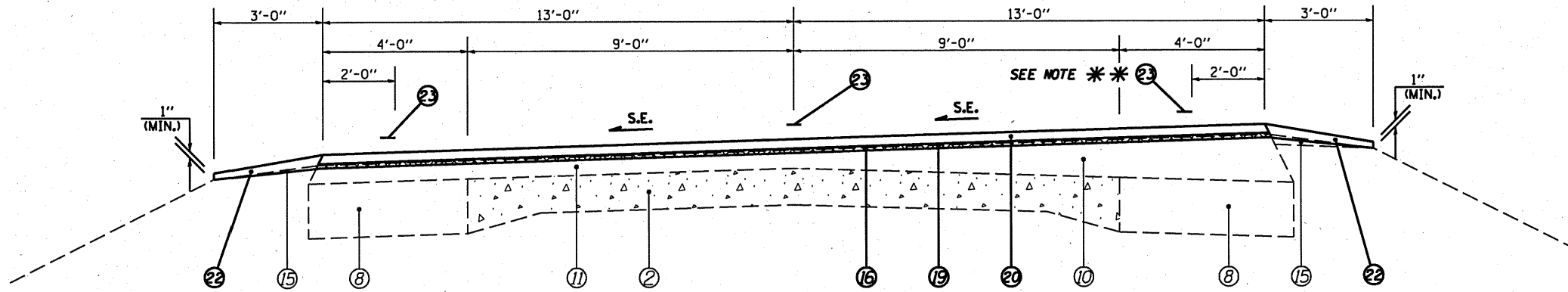
FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\coxjw\d0215198\d672880	Sht-Typical.dgn	DRAWN - RSC	REVISED -		733	1RS-3, 7RS-5		48	6		
PLOT SCALE = 100.0000' / IN.	CHECKED - JWC	REVISIED -	REVISIED -		SCALE: NTS SHEET NO. 3 OF 5 SHEETS STA. 134+50.00 TO STA. 179+41.93			CONTRACT NO. 72880			
PLOT DATE = Sep-08-2010 10:42:15AM	DATE -	REVISIED -	REVISIED -		ILLINOIS FED. AID PROJECT			ADAMS, HANCOCK			

LEGEND

- ① EXIST. LIME MODIFIED SOIL 12"
- ② EXIST. P.C.C. PAVEMENT 9"-6"-9"
- ③ EXIST. P.C.C. PAVEMENT 9"
- ④ EXIST. BITUMINOUS BASE COURSE 9"
- ⑤ EXIST. BITUMINOUS BASE COURSE 12"
- ⑥ EXIST. BITUMINOUS BASE COURSE 13 1/2"
- ⑦ EXIST. BITUMINOUS BASE COURSE WIDENING 10"
- ⑧ EXIST. BITUMINOUS BASE COURSE WIDENING 9"
- ⑨ EXIST. P.C.C. WIDENING
- ⑩ EXIST. BITUMINOUS CONCRETE BINDER COURSE, VARIABLE DEPTH
- ⑪ EXIST. BITUMINOUS OVERLAY 3"
- ⑫ EXIST. BITUMINOUS OVERLAY 1 1/2"
- ⑬ EXIST. BITUMINOUS SHOULDERS 8"
- ⑭ EXIST. CONCRETE GUTTER, TYPE B
- ⑮ EXIST. AGGREGATE SHOULDER
- ⑯ PROP. HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) *
- ⑰ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- ⑱ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑲ PROP. LEVELING BINDER (MACHINE METHOD), N50 3/4"
- ⑳ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 1 1/2"
- ㉑ PROP. HOT-MIX ASPHALT SHOULDERS
- ㉒ PROP. AGGREGATE SHOULDERS, TYPE B
- ㉓ PROP. PAINT PAVEMENT MARKING - LINE 5"

* NOMINAL MILLING DEPTH 1/2" @ C

☉ F.A.P. 733 (IL 61/94)



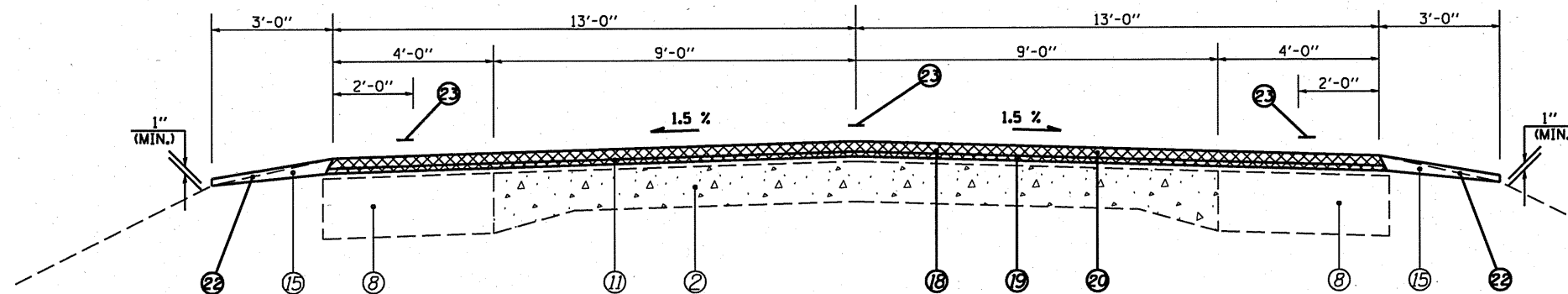
(S.E.#1) (11) STA 158+37.63 TO STA 159+40.16 [102.53'] (STA. EQN. #4)
 (12) STA 0+00.94 TO STA 8+00.00 [799.06']
 (S.E.#2) (15) STA 58+50.97 TO STA 60+40.30 [189.33'] (STA. EQN. #5)
 (16) STA 59+47.54 TO STA 70+50.52 [1,102.98'] (STA. EQN. #6)
 (17) STA 70+93.13 TO STA 72+63.39 [170.26']

(STA. EQN. #4):
 STA. 159+40.16 (BK) =
 STA. 0+00.94 (AH)

(STA. EQN. #5):
 STA. 60+40.30 (BK) =
 STA. 59+47.54 (AH)

** NOTE:
 PROP PAINT PVT MK LINE 6" FR STA 1+46.50
 TO 3+14.00 RT

☉ F.A.P. 733 (IL 61/94)



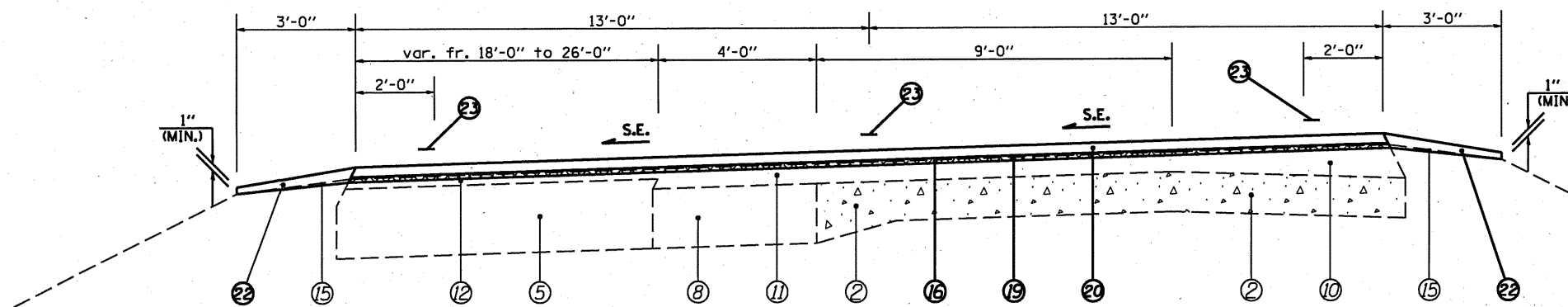
(13) STA 8+00.00 TO STA 10+00.00 [200.0']

(STA. EQN. #6):
 STA. 70+50.52 (BK) =
 STA. 70+93.13 (AH)

(PROP.)
 FULL S.E.: (S.E.#1 = 3.3%)
 STA. 0+31.41 to 6+75.77
 S.E. TRANSITION:
 STA. 158+37.63 TO 159+40.16 (BK) =
 STA. 0+00.94 (AH) TO 0+31.41
 STA. 6+75.77 TO 8+08.77

(EXIST.)
 FULL S.E.: (S.E.#1 = 3.3%)
 STA. 0+52.39 TO 6+43.23
 S.E. TRANSITION:
 STA. 158+12.49 TO 159+40.16 (BK) =
 STA. 0+00.94 (AH) TO 0+52.39
 STA. 6+43.23 TO 8+33.83

☉ F.A.P. 733 (IL 61/94)



(S.E.#3) (20) STA 179+41.93 TO STA 183+15.35 [373.42']

(PROP.)
 FULL S.E.: (S.E.#2 = 8.0%)
 STA. 60+21.41 to 69+76.66
 S.E. TRANSITION:
 STA. 58+50.97 TO 60+40.30 (BK) =
 STA. 59+47.54 (AH) TO 60+21.41
 STA. 69+76.66 TO 70+50.53 (BK) =
 STA. 70+74.06 (AH) TO 72+63.39

(EXIST.)
 FULL S.E.: (S.E.#2 = 8.0%)
 STA. 60+24.20 TO 69+73.86
 S.E. TRANSITION:
 STA. 58+86.97 TO 60+40.30 (BK) =
 STA. 59+47.54 (AH) TO 60+24.20
 STA. 69+73.86 TO 70+50.53 (BK) =
 STA. 70+74.06 (AH) TO 72+27.39

(PROP.)
 FULL S.E.: (S.E.#3 = 5.5%)
 STA. 181+35.83 TO 183+85.76
 S.E. TRANSITION:
 STA. 179+41.93 TO 181+35.83
 STA. 183+85.76 TO 185+79.66

(EXIST.)
 FULL S.E.: (S.E.#3 = 5.5%)
 STA. 181+33.63 TO 183+87.78
 S.E. TRANSITION:
 STA. 179+39.74 TO 181+33.63
 STA. 183+87.78 TO 185+81.68

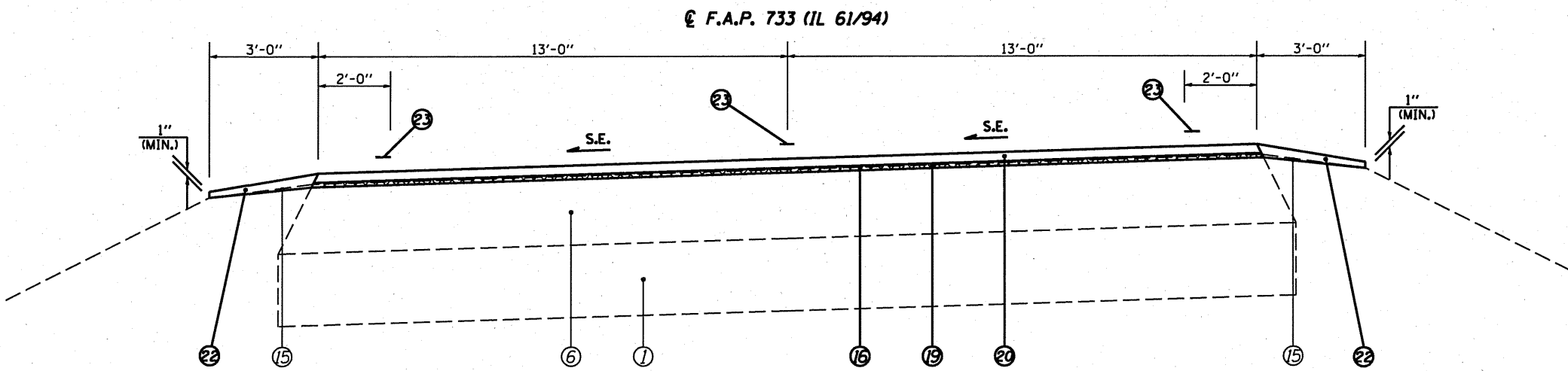
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

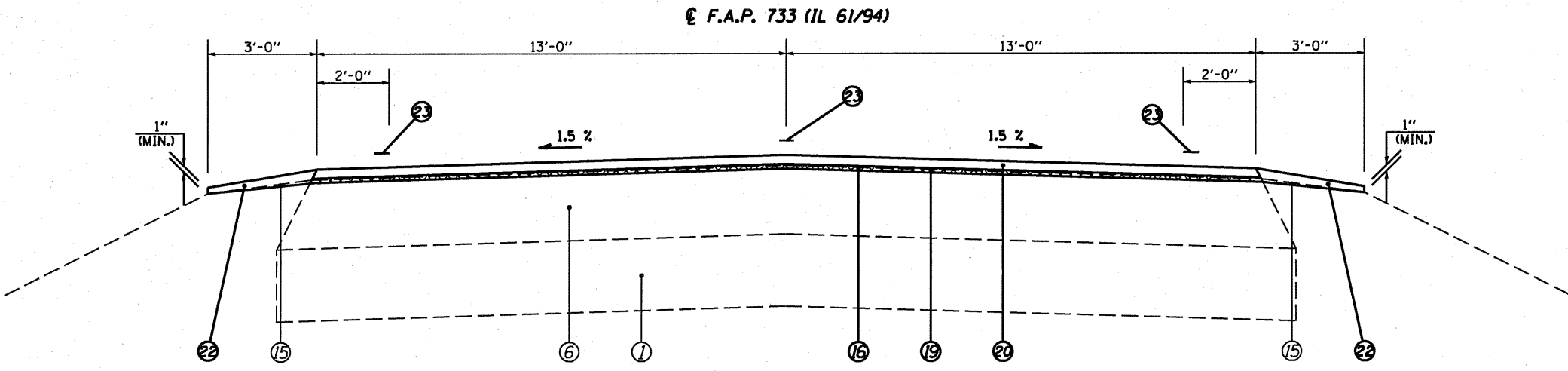
SCALE: NTS SHEET NO. 4 OF 5 SHEETS STA. 158+37.63 TO STA. 183+15.35

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
733	IRS-3, TRS-5		48	7
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72880	

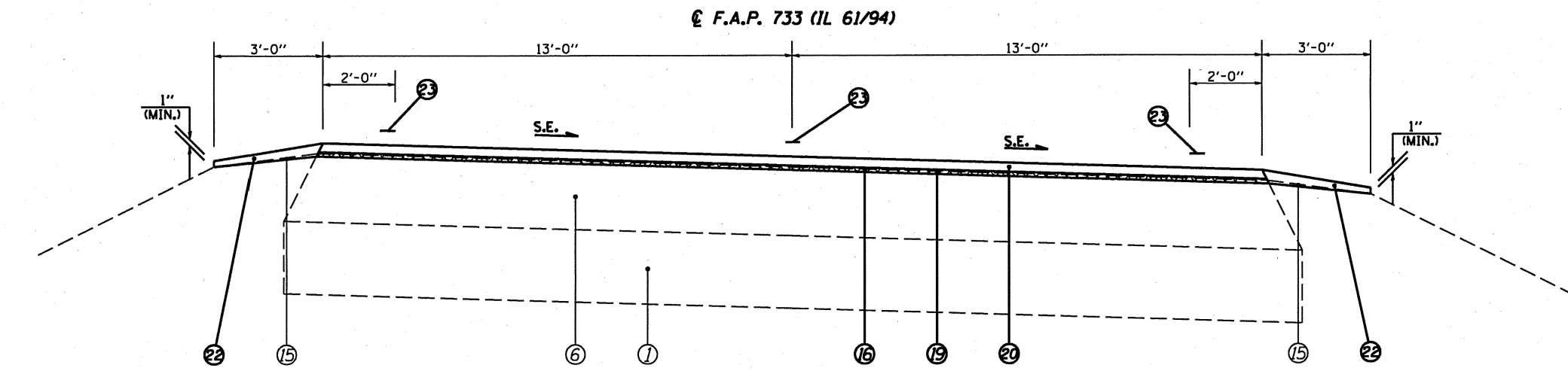
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PLOT SCALE = 1/8" = 1' IN.		CHECKED - JWC	REVISED -
PLOT DATE = Sep-18-2018 09:25:54AM		DATE -	REVISED -



(S.E. #3) (21) STA 183+15.35 TO STA 185+79.66 [264.31']



(22) STA 185+79.66 TO STA 190+73.94 [494.28']



(S.E. #4) (23) STA 190+73.94 TO STA 196+97.00 [623.06']

LEGEND

- ① EXIST. LIME MODIFIED SOIL 12"
- ② EXIST. P.C.C. PAVEMENT 9"-6"-9"
- ③ EXIST. P.C.C. PAVEMENT 9"
- ④ EXIST. BITUMINOUS BASE COURSE 9"
- ⑤ EXIST. BITUMINOUS BASE COURSE 12"
- ⑥ EXIST. BITUMINOUS BASE COURSE 13 1/2"
- ⑦
- ⑧ EXIST. BITUMINOUS BASE COURSE WIDENING 9"
- ⑨ EXIST. P.C.C. WIDENING
- ⑩ EXIST. BITUMINOUS CONCRETE BINDER COURSE, VARIABLE DEPTH
- ⑪ EXIST. BITUMINOUS OVERLAY 3"
- ⑫ EXIST. BITUMINOUS OVERLAY 1 1/2"
- ⑬ EXIST. BITUMINOUS SHOULDERS 8"
- ⑭ EXIST. CONCRETE GUTTER, TYPE B
- ⑮ EXIST. AGGREGATE SHOULDER
- ⑯ PROP. HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) *
- ⑰ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- ⑱ PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"
- ⑲ PROP. LEVELING BINDER (MACHINE METHOD), N50 3/4"
- ⑳ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 1 1/2"
- ㉑ PROP. HOT-MIX ASPHALT SHOULDERS
- ㉒ PROP. AGGREGATE SHOULDERS, TYPE B
- ㉓ PROP. PAINT PAVEMENT MARKING - LINE 5"

* NOMINAL MILLING DEPTH 1/2" @ 1'

FULL S.E.: (S.E.#3 = 5.5%)
 STA. 181+35.83 TO 183+85.76
S.E. TRANSITION:
 STA. 179+41.93 TO 181+35.83
 STA. 183+85.76 TO 185+79.66

FULL S.E.: (S.E.#3 = 5.5%)
 STA. 181+33.63 TO 183+87.78
S.E. TRANSITION:
 STA. 179+39.74 TO 181+33.63
 STA. 183+87.78 TO 185+81.68

FULL S.E.: (S.E.#4 = 5.5%)
 STA. 192+67.84 TO 195+69.24
S.E. TRANSITION:
 STA. 190+73.94 TO 192+67.84
 STA. 195+69.24 TO 197+63.14

FULL S.E.: (S.E.#4 = 5.5%)
 STA. 192+65.69 TO 195+74.47
S.E. TRANSITION:
 STA. 190+71.80 TO 192+65.69
 STA. 195+74.47 TO 197+65.36

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS

SCALE: NTS SHEET NO. 5 OF 5 SHEETS STA. 183+15.35 TO STA. 196+97.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
733	IRS-3, TRS-5		48	8
			CONTRACT NO. 72880	

FILE NAME =	USER NAME = coxjm	DESIGNED -	REVISED -
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PLOT DATE = Sep-08-2010 10:42:16AM		DATE -	REVISED -

HOT-MIX ASPHALT SURFACE REMOVAL

TYP. SEC. #	LOCATION	QUANTITY			
		BUTT JOINT (SQ YD)	VAR. DEPTH (SQ YD)	1.5" (SQ YD)	2.25" (SQ YD)
F. A. P. 733 (IL 61/94)					
(1)	STA. 1233+29.50 TO STA. 1233+49.50	71.11			
(1-2)	STA. 1233+49.50 TO STA. 1236+50.00		1,068.44		
(2-3)	STA. 1236+50.00 TO STA. 1242+13.77		1,816.59		
(STA. EQN. #1)					
STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)					
(4)	STA. 1237+84.46 TO STA. 1237+98.23		44.37		
(4)	STA. 1237+98.23 TO STA. 1260+15.75		6,406.17		
(4)	STA. 1260+15.75 TO STA. 1260+35.75	57.78			
(BRIDGE OMISSION #1) S. N. 001-0002					
STA. 1260+35.75 TO STA. 1261+04.25					
(5)	STA. 1261+04.25 TO STA. 1261+24.25	57.78			
(5)	STA. 1261+24.25 TO STA. 1449+49.65		54,384.49		
(STA. EQN. #2)					
STA. 1449+49.65 (BK) = STA. 7+53.50 (AH)					
(6)	STA. 7+53.50 TO STA. 29+18.00		6,253.00		
(6)	STA. 29+18.00 TO STA. 29+33.00		43.33		
STA. 29+33.00 TO STA. 125+35.00 (NO MILLING)					
(8)	STA. 125+35.00 TO STA. 125+65.00		86.67		
(8)	STA. 125+65.00 TO STA. 130+62.24		1,436.47		
(8)	STA. 128+10.00 TO STA. 128+48.00	(HMA shldrs)	21.33		
(STA. EQN. #3)					
STA. 130+62.24 (BK) = STA. 134+50.00 (AH)					
(9)	STA. 134+50.00 TO STA. 139+50.80		1,446.76		
(10)	STA. 139+50.80 TO STA. 139+70.80		57.78		
(10-11)	STA. 139+70.80 TO STA. 159+40.16		5,689.26		
(STA. EQN. #4)					
STA. 159+40.16 (BK) = STA. 0+00.94 (AH)					
(12)	STA. 0+00.94 TO STA. 8+00.00		2,308.40		
(12)	STA. 8+00.00 TO STA. 8+35.00		101.11		
(12)	STA. 8+35.00 TO STA. 9+65.00			375.56	
(13)	STA. 9+65.00 TO STA. 10+00.00		101.11		
(14-15)	STA. 10+00.00 TO STA. 60+40.30		14,560.87		
(STA. EQN. #5)					
STA. 60+40.30 (BK) = STA. 59+47.54 (AH)					
(16)	STA. 59+47.54 TO STA. 70+50.52		3,186.39		
(STA. EQN. #6)					
STA. 70+50.52 (BK) = STA. 70+93.13 (AH)					
(17-18)	STA. 70+93.13 TO STA. 123+34.50		15,141.74		
(18)	STA. 123+34.50 TO STA. 123+54.50	57.78			
(18)	STA. 123+54.50 TO STA. 123+94.50 RT		17.78		
(BRIDGE OMISSION #2) S. N. 034-0065					
STA. 123+54.50 TO STA. 125+00.00					
(19)	STA. 124+92.00 TO STA. 125+00.00 RT		3.56		
(19)	STA. 125+00.00 TO STA. 125+20.00	66.67			
(19)	STA. 125+20.00 TO STA. 127+17.00		656.67		
(19-23)	STA. 127+17.00 TO STA. 197+12.00		20,207.78		
(23)	STA. 197+12.00 TO STA. 197+32.00	57.78			
INCIDENTAL HMA SURFACING					
ENTRANCES		842.81			
SIDE ROADS		2,047.36			
PROJECT TOTAL		3,260	132,115	2,926	376

TEMPORARY RAMP

TYP. SEC. #	LOCATION	QUANTITY (SQ YD)
F. A. P. 733 (IL 61/94)		
(1)	STA. 1233+29.50 TO STA. 1233+34.50	14.44
(STA. EQN. #1)		
STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)		
(4)	STA. 1260+30.75 TO STA. 1260+35.75	14.44
(BRIDGE OMISSION #1) S. N. 001-0002		
STA. 1260+35.75 = STA. 1261+04.25		
(5)	STA. 1261+04.25 TO STA. 1261+09.25	14.44
(STA. EQN. #2)		
STA. 1449+49.65 (BK) = 7+53.50 (AH)		
(STA. EQN. #3)		
STA. 130+62.24 (BK) = STA. 134+50.00 (AH)		
(STA. EQN. #4)		
STA. 159+40.16 (BK) = STA. 0+00.94 (AH)		
(STA. EQN. #5)		
STA. 60+40.30 (BK) = STA. 59+47.54 (AH)		
(STA. EQN. #6)		
STA. 70+50.52 (BK) = STA. 70+93.13 (AH)		
(18)	STA. 123+49.50 TO STA. 123+54.50	14.44
(BRIDGE OMISSION #2) S. N. 034-0065		
STA. 123+54.50 = STA. 125+00.00		
(19)	STA. 125+00.00 TO STA. 125+05.00	14.44
(23)	STA. 197+27.00 TO STA. 197+32.00	14.44
PROJECT TOTAL		87

INCIDENTAL HOT-MIX ASPHALT SURFACING

LOCATION	QUANTITY (TON)
F. A. P. 733 (IL 61/94)	
EXIST HMA ENTR	87.52
EXIST AGGR ENTR	147.24
SIDE ROADS	209.20
PROJECT TOTAL	444

AGGREGATE SURFACE COURSE, TYPE B

LOCATION	QUANTITY (TON)
F. A. P. 733 (IL 61/94)	
Quantity for HMA PE's and CE's	52.41
PROJECT TOTAL	53

HOT-MIX ASPHALT SHOULDERS

TYP. SEC. *	LOCATION	QUANTITY (TON)
	F. A. P. 733 (IL 61/94)	
(1)	STA. 1233+29.50 TO STA. 1233+49.50	1.12
(1)	STA. 1233+49.50 TO STA. 1233+64.50	1.05
(1-2)	STA. 1233+64.50 TO STA. 1236+50.00	23.98
(2-3)	STA. 1236+50.00 TO STA. 1242+13.77 LT	23.68
	(STA. EQN. #1)	
	STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)	
(4)	STA. 1237+84.46 TO STA. 1237+98.23 LT	0.58
	(BRIDGE OMISSION #1) S. N. 001-0002	
	STA. 1260+35.75 TO STA. 1261+04.25	
	(STA. EQN. #2)	
	STA. 1449+49.65 (BK) = STA. 7+53.50 (AH)	
(8)	STA. 128+10.00 TO STA. 128+48.00 RT	2.69
(8)	STA. 128+80.00 TO STA. 128+85.00 RT	0.42
	(STA. EQN. #3)	
	STA. 130+62.24 (BK) = STA. 134+50.00 (AH)	
	(STA. EQN. #4)	
	STA. 159+40.16 (BK) = STA. 0+00.94 (AH)	
	(STA. EQN. #5)	
	STA. 60+40.30 (BK) = STA. 59+47.54 (AH)	
	(STA. EQN. #6)	
	STA. 70+50.52 (BK) = STA. 70+93.13 (AH)	
(18)	STA. 123+54.50 TO STA. 123+94.50 RT	1.49
	(BRIDGE OMISSION #2) S. N. 034-0065	
	STA. 123+54.50 TO STA. 125+00.00	
(19)	STA. 124+92.00 TO STA. 125+20.00 RT	1.05
(19)	STA. 125+20.00 TO STA. 125+35.00 RT	0.70
(19)	STA. 125+35.00 TO STA. 127+17.00 RT	10.19
	PROJECT TOTAL	67

AGGREGATE SHOULDERS, TYPE B

TYP. SEC. *	LOCATION	QUANTITY (TON)
	F. A. P. 733 (IL 61/94)	
(1-3)	STA. 1233+29.50 TO STA. 1242+13.77	67.14
	(STA. EQN. #1)	
	STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)	
(4)	STA. 1237+84.46 TO STA. 1260+35.75	170.93
	(BRIDGE OMISSION #1) S. N. 001-0002	
	STA. 1260+35.75 TO STA. 1261+04.25	
(5)	STA. 1261+04.25 TO STA. 1449+49.65	1,430.85
	(STA. EQN. #2)	
	STA. 1449+49.65 (BK) = 7+53.50 (AH)	
(6-8)	STA. 7+53.50 TO STA. 130+62.24	934.55
	(STA. EQN. #3)	
	STA. 130+62.24 (BK) = STA. 134+50.00 (AH)	
(9-10)	STA. 134+50.00 TO STA. 159+40.16	189.07
	(STA. EQN. #4)	
	STA. 159+40.16 (BK) = STA. 0+00.94 (AH)	
	[TR 389 (2300E)] (Side Road)	
	STA. 7+46.50 TO STA. 9+55.00 RT	7.92
	STA. 9+55.00 TO STA. 9+90.00	2.66
	F. A. P. 733 (IL 61/94)	
(12)	STA. 0+94 TO STA. 1+13.00	8.51
(12)	STA. 1+13.00 TO STA. 3+14.00 LT	7.63
(12-15)	STA. 3+14.00 TO STA. 60+40.30	434.77
	(STA. EQN. #5)	
	STA. 60+40.30 (BK) = STA. 59+47.54 (AH)	
(16)	STA. 59+47.54 TO STA. 70+50.52	83.74
	(STA. EQN. #6)	
	STA. 70+50.52 (BK) = STA. 70+93.13 (AH)	
(17-18)	STA. 70+93.13 TO STA. 123+54.50	399.47
(18)	STA. 123+54.50 TO STA. 123+62.50 LT	0.30
	(BRIDGE OMISSION #2) S. N. 034-0065	
	STA. 123+54.50 TO STA. 125+00.00	
(19)	STA. 124+60.00 TO STA. 127+17.00 LT	9.76
(19-23)	STA. 127+17.00 TO STA. 198+00.00	537.78
	SUBTOTAL	4,285.09
	[SUBTRACT]	
	SIDE ROADS & ENTRANCES	-392.51
	PROJECT TOTAL	3,893

GUARDRAIL SCHEDULE

TYP. SEC. #	LOCATION	GUARDRAIL REMOVAL (FOOT)	SPBGR TY A 6 FOOT POSTS (FOOT)	TRAF BAR TERM T1 SPL TAN (EACH)	GUARDRAIL MKR - TYPE B (EACH)
	F. A. P. 733 (IL 61/94)				
	(STA. EQN. #1)				
	STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)				
(4)	STA. 1258+19.00 TO STA. 1258+69.00 RT	50		1	
(4)	STA. 1259+19.00 TO STA. 1259+69.00 LT	50		1	
	(BRIDGE OMISSION #1) S. N. 001-0002				
	STA. 1260+35.75 TO STA. 1261+04.25				
(5)	STA. 1261+71.00 TO STA. 1262+21.00 RT	50		1	
(5)	STA. 1262+71.00 TO STA. 1263+21.00 LT	50		1	
	(STA. EQN. #2)				
	STA. 1449+49.65 (BK) = STA. 7+53.50 (AH)				
(18)	STA. 83+05.00 TO STA. 83+55.00 RT	50		1	
(18)	STA. 83+83.00 TO STA. 84+33.00 LT	50		1	
(18)	STA. 86+55.00 TO STA. 87+05.00 RT	50		1	
(18)	STA. 87+33.00 TO STA. 87+83.00 LT	50		1	
(18)	STA. 121+40.00 TO STA. 121+90.00 RT			1	
(18)	STA. 121+75.00 TO STA. 122+65.00 RT	90			
(18)	STA. 121+90.00 TO STA. 122+15.00 RT		25		4
(18)	STA. 122+15.00 TO STA. 122+65.00 RT			1	
	(STA. EQN. #3)				
	STA. 130+62.24 (BK) = STA. 134+50.00 (AH)				
	(STA. EQN. #4)				
	STA. 159+40.16 (BK) = STA. 0+00.94 (AH)				
	(STA. EQN. #5)				
	STA. 60+40.30 (BK) = STA. 59+47.54 (AH)				
	(STA. EQN. #6)				
	STA. 70+50.52 (BK) = STA. 70+93.13 (AH)				
(18)	STA. 122+30.00 TO STA. 122+80.00 LT	50		1	
(18)	STA. 123+12.00 TO STA. 123+62.00 RT	50		1	
	(BRIDGE OMISSION #2) S. N. 034-0065				
	STA. 123+54.50 TO STA. 125+00.00				
(19)	STA. 125+77.50 TO STA. 126+27.50 RT	50		1	
(19)	STA. 126+20.50 TO STA. 126+70.50 LT	50		1	
(19)	STA. 136+05.50 TO STA. 136+55.50 RT	50		1	
(19)	STA. 137+28.50 TO STA. 137+78.50 LT	50		1	
(19)	STA. 139+55.50 TO STA. 140+05.50 RT	50		1	
(19)	STA. 140+78.50 TO STA. 141+28.50 LT	50		1	
PROJECT TOTAL		890	25	18	4

TERMINAL MARKER-DIRECT APPLIED

TYP. SEC. #	LOCATION	QUANTITY (EACH)
	F. A. P. 733 (IL 61/94)	
(4)	STA. 1258+19.00 RT	1
(4)	STA. 1259+19.00 LT	1
	(BRIDGE OMISSION #1) S. N. 001-0002	
	STA. 1260+35.75 TO STA. 1261+04.25	
(5)	STA. 1262+21.00 RT	1
(5)	STA. 1263+21.00 LT	1
(18)	STA. 83+05.00 RT	1
(18)	STA. 83+83.00 LT	1
(18)	STA. 87+05.00 RT	1
(18)	STA. 87+83.00 LT	1
(18)	STA. 121+40.00 RT	1
(18)	STA. 122+65.00 RT	1
(18)	STA. 122+30.00 LT	1
(18)	STA. 123+12.00 RT	1
	(BRIDGE OMISSION #2) S. N. 034-0065	
	STA. 123+54.50 TO STA. 125+00.00	
(19)	STA. 125+77.50 RT	1
(19)	STA. 126+70.50 LT	1
(19)	STA. 136+05.50 RT	1
(19)	STA. 137+28.50 LT	1
(19)	STA. 140+05.50 RT	1
(19)	STA. 141+28.50 LT	1
PROJECT TOTAL		18

SIGN PANEL - TYPE 1

TYP. SEC. #	LOCATION	QUANTITY (SQ FT)
	F. A. P. 733 (IL 61/94)	
	"STOP AHEAD" SIGN (36"X36")	
(1)	STA 1235+36.00, 20' LT	9
PROJECT TOTAL		9

TELESCOPING STEEL SIGN SUPPORT

TYP. SEC. #	LOCATION	QUANTITY (FOOT)
	F. A. P. 733 (IL 61/94)	
(1)	STA 1235+36.00, 20' LT	14
PROJECT TOTAL		14

SHORT-TERM PAVEMENT MARKING

TYP. SEC. #	LOCATION	QUANTITY (FOOT)
	F. A. P. 733 (IL 61/94)	
(1-3)	STA. 1233+29.50 TO STA. 1242+13.77 (STA. EQN. #1)	80.39
	STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)	
(4)	STA. 1237+84.46 TO STA. 1260+35.75 (BRIDGE OMISSION #1) S. N. 001-0002	204.66
	STA. 1260+35.75 TO STA. 1261+04.25	
(5)	STA. 1261+04.25 TO STA. 1449+49.65 (STA. EQN. #2)	1,713.22
	STA. 1449+49.65 (BK) = STA. 7+53.50 (AH)	
(6-8)	STA. 7+53.50 TO STA. 130+62.24 (STA. EQN. #3)	1,118.98
	STA. 130+62.24 (BK) = STA. 134+50.00 (AH)	
(9-11)	STA. 134+50.00 TO STA. 159+40.16 (STA. EQN. #4)	226.38
	STA. 159+40.16 (BK) = STA. 0+00.94 (AH)	
(12-15)	STA. 0+00.94 TO STA. 60+40.30 (STA. EQN. #5)	549.03
	STA. 60+40.30 (BK) = STA. 59+47.54 (AH)	
(16)	STA. 59+47.54 TO STA. 70+50.52 (STA. EQN. #6)	100.27
	STA. 70+50.52 (BK) = STA. 70+93.13 (AH)	
(17-18)	STA. 70+93.13 TO STA. 123+54.50 (BRIDGE OMISSION #2) S. N. 034-0065	478.31
	STA. 123+54.50 TO STA. 125+00.00	
(19-23)	STA. 125+00.00 TO STA. 197+32.00	657.45
	SUBTOTAL (After Milling)	5,128.69
	SUBTOTAL (After Binder)	5,128.69
	SUBTOTAL (After Surface)	5,128.69
	PROJECT TOTAL	15,386

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

LOCATION	QUANTITY (EACH)
F. A. P. 733 (IL 61/94)	
Limits of Improv. to SN 001-0002	24
SN 001-0002 to 2700N Ave.	4
2700N Ave. to 2750N Ave.	10
2750N Ave. to 2800N Ave.	12
2800N Ave. to 2900N Ave.	21
2900N Ave. to 3000N Ave.	23
3000N Ave. to 150N	94
150N Ave. to 200N	34
200N Ave. to Church St.	22
Church St. to DavIs St.	5
DavIs St. to 5th St.	6
5th St. to 4th St.	2
4th St. to 3rd St.	4
3rd St. to 2nd St.	5
2nd St. to 1st St.	4
1st St. to TR 350N	51
TR 350N to 2240E	40
2240E to 2200E	24
2200E to SN 034-0065	43
SN 034-0065 to 2100E	21
2100E to Limits of Improv.	69
PROJECT TOTAL	518

RAISED REFLECTIVE PAVEMENT MARKER

TYP. SEC. #	LOCATION	QUANTITY (EACH)
	F. A. P. 733 (IL 61/94)	
(1-3)	STA. 1233+29.50 TO STA. 1242+13.77 (STA. EQN. #1)	12.05
	STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)	
(4)	STA. 1237+84.46 TO STA. 1260+35.75 (BRIDGE OMISSION #1) S. N. 001-0002	29.14
	STA. 1260+35.75 TO STA. 1261+04.25	
(5)	STA. 1261+04.25 TO STA. 1449+49.65 (STA. EQN. #2)	236.57
	STA. 1449+49.65 (BK) = STA. 7+53.50 (AH)	
(6-8)	STA. 7+53.50 TO STA. 130+62.24 (STA. EQN. #3)	154.86
	STA. 130+62.24 (BK) = STA. 134+50.00 (AH)	
(9-11)	STA. 134+50.00 TO STA. 159+40.16 (STA. EQN. #4)	32.13
	STA. 159+40.16 (BK) = STA. 0+00.94 (AH)	
(12-15)	STA. 0+00.94 TO STA. 60+40.30 (STA. EQN. #5)	76.49
	STA. 60+40.30 (BK) = STA. 59+47.54 (AH)	
(16)	STA. 59+47.54 TO STA. 70+50.52 (STA. EQN. #6)	14.79
	STA. 70+50.52 (BK) = STA. 70+93.13 (AH)	
(17-18)	STA. 70+93.13 TO STA. 123+54.50 (BRIDGE OMISSION #2) S. N. 034-0065	66.77
	STA. 123+54.50 TO STA. 125+00.00	
(19-23)	STA. 125+00.00 TO STA. 197+32.00	91.40
	PROJECT TOTAL	715

WORK ZONE PAVEMENT MARKING REMOVAL

LOCATION	QUANTITY (SQ FT)
(Removal of Short-Term Pavement Marking) (After Surface Course)	
(5,128.69)(4/12)	1,709.56
PROJECT TOTAL	1,710

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE:	SHEET NO. 4 OF 7 SHEETS	STA.	TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				733	1RS-3, 7RS-5	*	48	12
							CONTRACT NO. 72880	
							ILLINOIS FED. AID PROJECT	

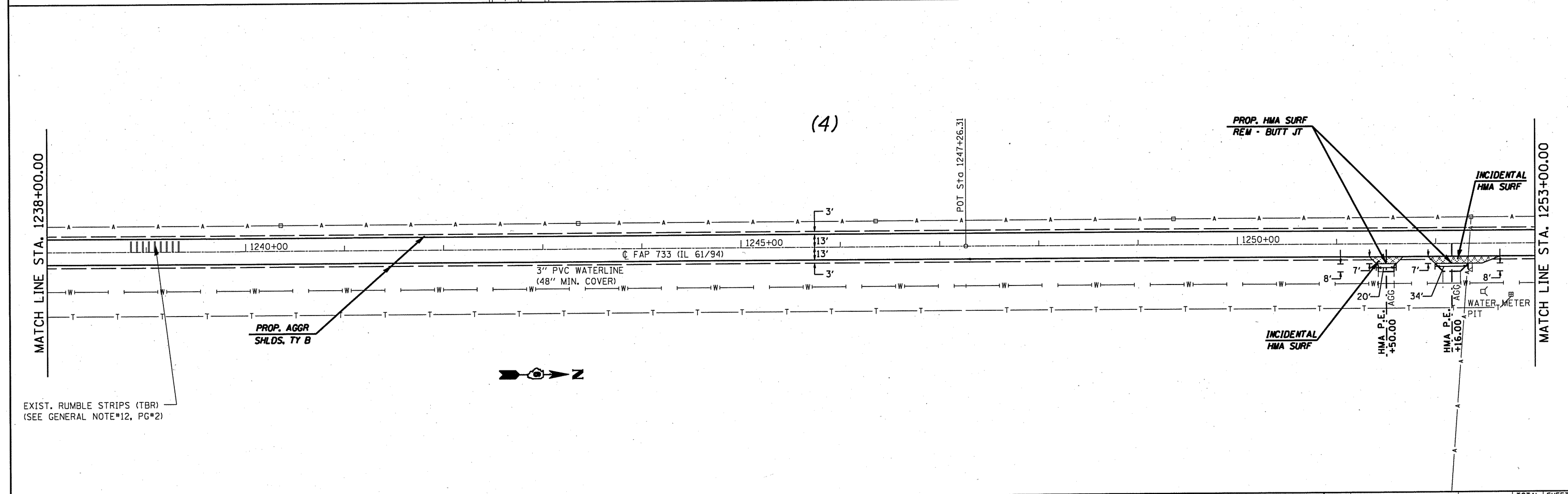
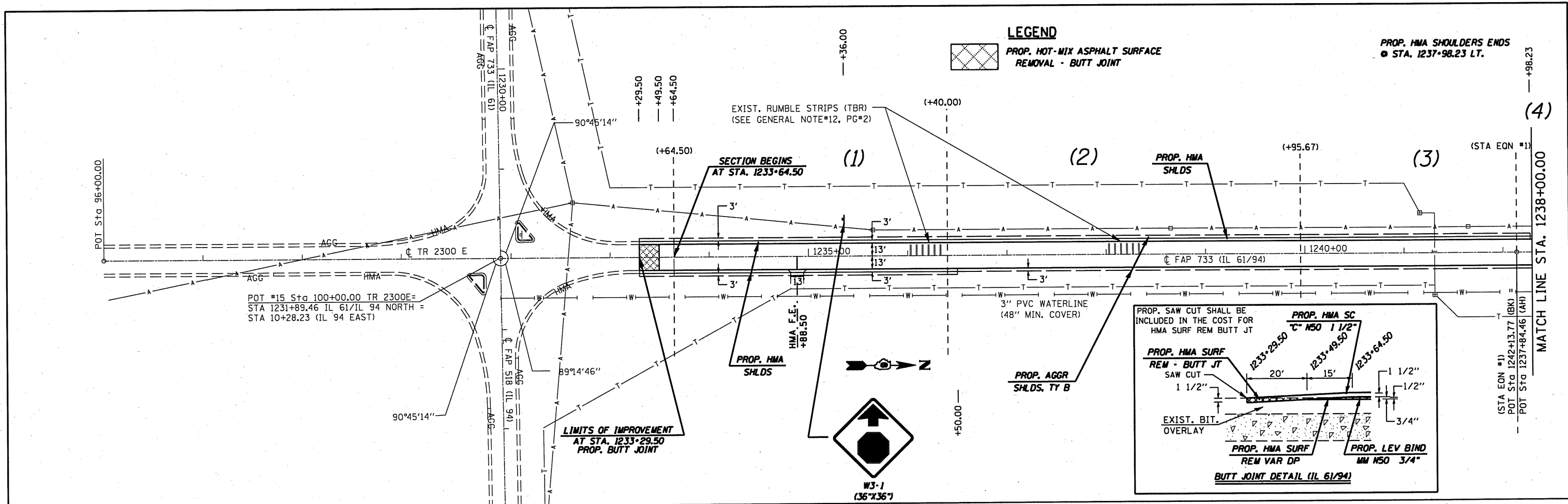
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PLOT DATE = Sep-08-2010 10:42:21AM		DATE -	REVISED -

PAINT PAVEMENT MARKING - LINE

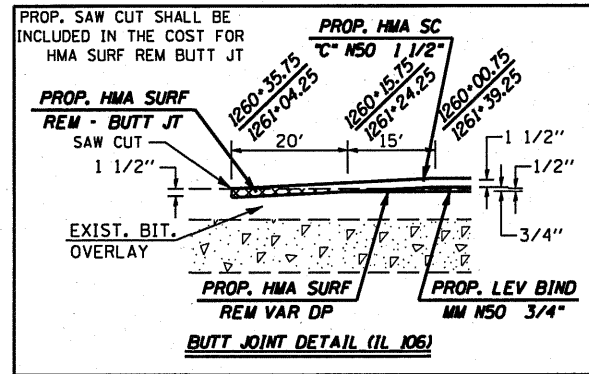
TYP. SEC. #	LOCATION	5"			
		YELLOW			WHITE
		DBL-SOLID (FOOT)	SOLID (FOOT)	SKIP-DASH (FOOT)	SOLID (FOOT)
	F. A. P. 733 (IL 61/94)				
(1-3)	STA. 1233+29.50 TO STA. 1242+13.77 (STA. EQN. #1)	1,768.54			1,768.54
	STA. 1242+13.77 (BK) = STA. 1237+84.46 (AH)				
(4)	STA. 1237+84.46 TO STA. 1244+10.00	1,251.08			
(4)	STA. 1237+84.46 TO STA. 1260+35.75				4,502.58
(4)	STA. 1244+10.00 TO STA. 1256+60.00		1,250.00		
(4)	STA. 1244+10.00 TO STA. 1260+35.75			406.44	
	(BRIDGE OMISSION #1) S. N. 001-0002				
	STA. 1260+35.75 TO STA. 1261+04.25			17.13	137.00
(5)	STA. 1261+04.25 TO STA. 1449+49.65				37,690.80
(5)	STA. 1261+04.25 TO STA. 1449+49.65			4,711.35	
(5)	STA. 1262+45.00 TO STA. 1267+10.00		465.00		
(5)	STA. 1273+25.00 TO STA. 1278+15.00		490.00		
	(STA. EQN. #2)				
	STA. 1449+49.65 (BK) = 7+53.50 (AH)				
(6-8)	STA. 7+53.50 TO STA. 130+62.24			3,077.19	24,617.48
	(STA. EQN. #3)				
	STA. 130+62.24 (BK) = STA. 134+50.00 (AH)				
(9-11)	STA. 134+50.00 TO STA. 159+40.16			622.54	4,980.32
	(STA. EQN. #4)				
	STA. 159+40.16 (BK) = STA. 0+00.94 (AH)				
(12-15)	STA. 0+0.94 TO STA. 60+40.30			1,509.84	12,078.72
	(STA. EQN. #5)				
	STA. 60+40.30 (BK) = STA. 59+47.54 (AH)				
(16)	STA. 59+47.54 TO STA. 70+50.52			275.75	2,205.96
	(STA. EQN. #6)				
	STA. 70+50.52 (BK) = STA. 70+93.13 (AH)				
(17-18)	STA. 70+93.13 TO STA. 123+54.50			1,315.34	10,522.74
	(BRIDGE OMISSION #2) S. N. 034-0065				
	STA. 123+54.50 TO STA. 125+00.00			36.38	291
(19)	STA. 125+00.00 TO STA. 179+40.00			1,360.00	
(19-23)	STA. 125+00.00 TO STA. 197+32.00				14,464
(19-23)	STA. 179+40.00 TO STA. 197+32.00	3,584.00			
	SUBTOTAL	6,603.62	2,205.00	13,331.94	113,259.14
	PROJECT TOTAL			135,400	

PAVEMENT SCHEDULE

TYP SEC #	LOCATION				LENGTH FOOT	PAVEMENT WIDTH FOOT	LEVELING BINDER THICKNESS INCH	HMA SURF THICKNESS INCH	HMA SURF REM BUTT JOINT SQ YD	HMA SURF REMOVAL VAR DP SQ YD	HMA SURF REMOVAL 1 1/2" SQ YD	HMA SURF REMOVAL 2 1/4" SQ YD	HMA CONC SURF CSE "C" N50 TON	LEV BINDER (MM), N50 TON	BIT MAT PRIME COAT TON	AGG PRIME COAT TON
	STATION TO STATION															
	F. A. P. 733 (IL 61/94) (cont.)				-	-	-	-	-	-	-	-	-	-	-	-
(18)	STA. 123+54.50	TO	STA. 123+94.50	RT	40.00	4	-	-	-	-	17.78	-	-	-	0.01	0.04
	(BRIDGE OMISSION #2) STA. 123+54.50 TO STA. 125+00.00															
(19)	STA. 124+92.00	TO	STA. 125+00.00	RT	8.00	4	-	-	-	-	3.56	-	-	-	0.001	0.01
(19)	STA. 125+00.00	TO	STA. 125+20.00		20.00	30	-	-	66.67	-	-	-	-	-	0.03	0.13
(19-23)	STA. 125+00.00	TO	STA. 197+32.00		7,232.00	26	-	1.5	-	-	-	-	1,754.97	-	-	-
(19)	STA. 125+20.00	TO	STA. 125+35.00		15.00	26	0 to 0.75	-	-	-	-	-	-	0.91	-	-
(19)	STA. 125+20.00	TO	STA. 127+17.00		197.00	30	-	-	-	656.67	-	-	-	-	0.25	1.31
(19-23)	STA. 125+35.00	TO	STA. 196+97.00		7,162.00	26	0.75	-	-	-	-	-	-	868.99	-	-
(19-23)	STA. 127+17.00	TO	STA. 197+12.00		6,995.00	26	-	-	-	20,207.78	-	-	-	-	7.68	40.42
(23)	STA. 196+97.00	TO	STA. 197+12.00		15.00	26	0.75 to 0	-	-	-	-	-	-	0.91	-	-
(23)	STA. 197+12.00	TO	STA. 197+32.00		20.00	26	-	-	57.78	-	-	-	-	-	0.02	0.12
	INCIDENTAL HMA SURF. (ENTR & SIDE RDS)				-	-	-	-	2,890.17	-	-	-	-	-	4.12	5.78
	SUBTOTAL								3,014.61	20,864.44	21.33	0.00	1,754.97	870.81	12.10	47.80
PROJECT TOTAL									3,260	132,115	2,926	376	13,691	5,531	56	278



FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pwwork\pwwork\coxjw\08215198\d672880-shs-plan50.dgn		DRAWN - JWC	REVISED -		733	IRS-3, 7RS-5		48	16		
PLOT SCALE = 100.0000 / IN.		CHECKED - RSC	REVISED -		CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT			
PLOT DATE = Sep-13-2010 11:32:48AM		DATE -	REVISED -		SCALE: 50	SHEET NO. 1 OF 23 SHEETS	STA. 1233+29.50 TO STA. 1253+00.00	ADAMS, HANCOCK			

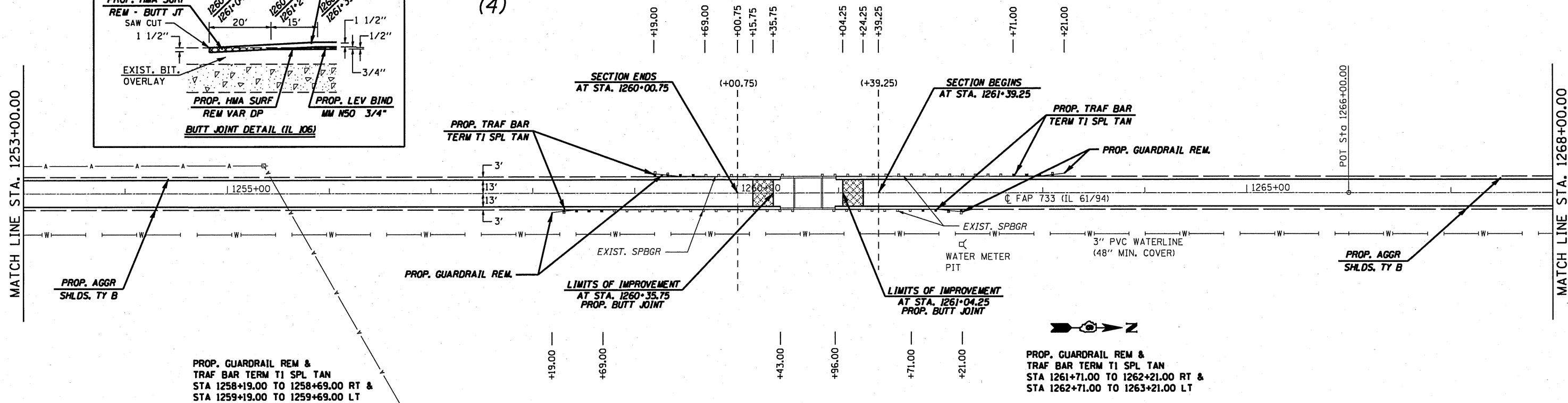


BRIDGE OMISSION #1 S.M. 001-0002
STA 1260+35.75 TO 1261+04.25

(5)

MATCH LINE STA. 1253+00.00

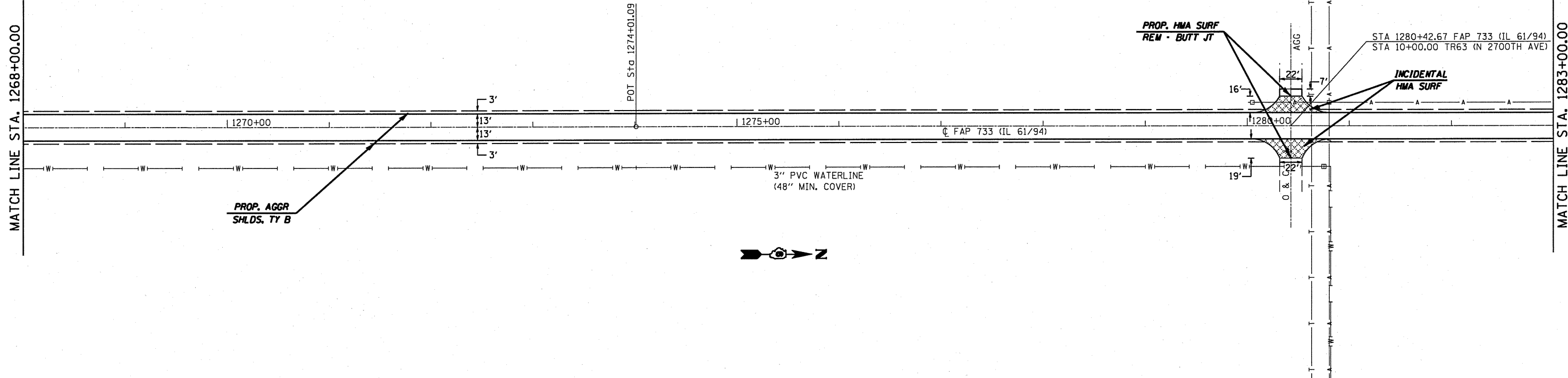
MATCH LINE STA. 1268+00.00



(5)

MATCH LINE STA. 1268+00.00

MATCH LINE STA. 1283+00.00

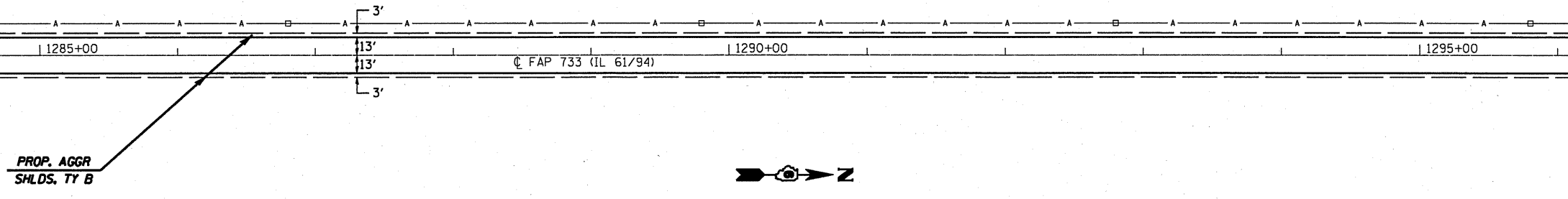


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PLOT DATE = Sep-08-2010 10:42:27AM	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT						
				SCALE: 50		SHEET NO. 2 OF 23 SHEETS		STA. 1253+00.00 TO STA. 1283+00.00		ADAMS, HANCOCK	

(5)

MATCH LINE STA. 1283+00.00

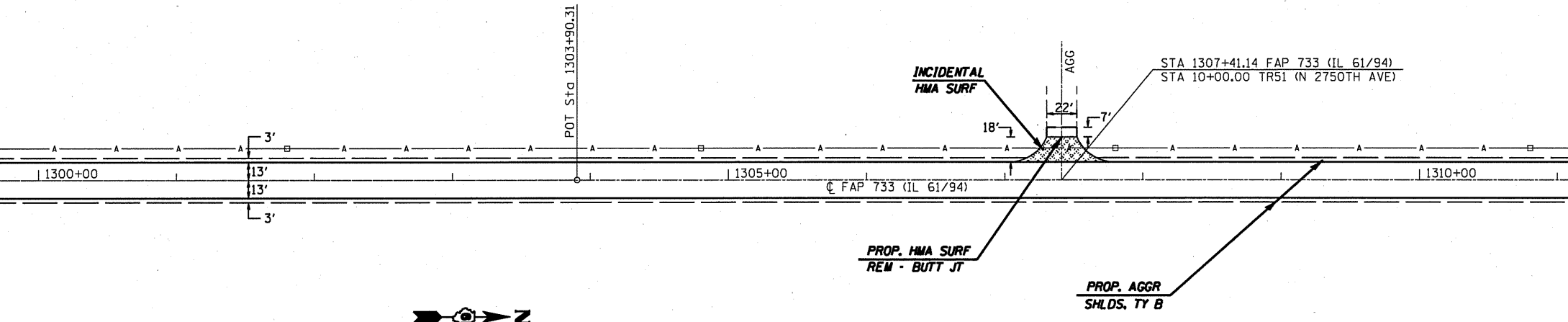
MATCH LINE STA. 1298+00.00



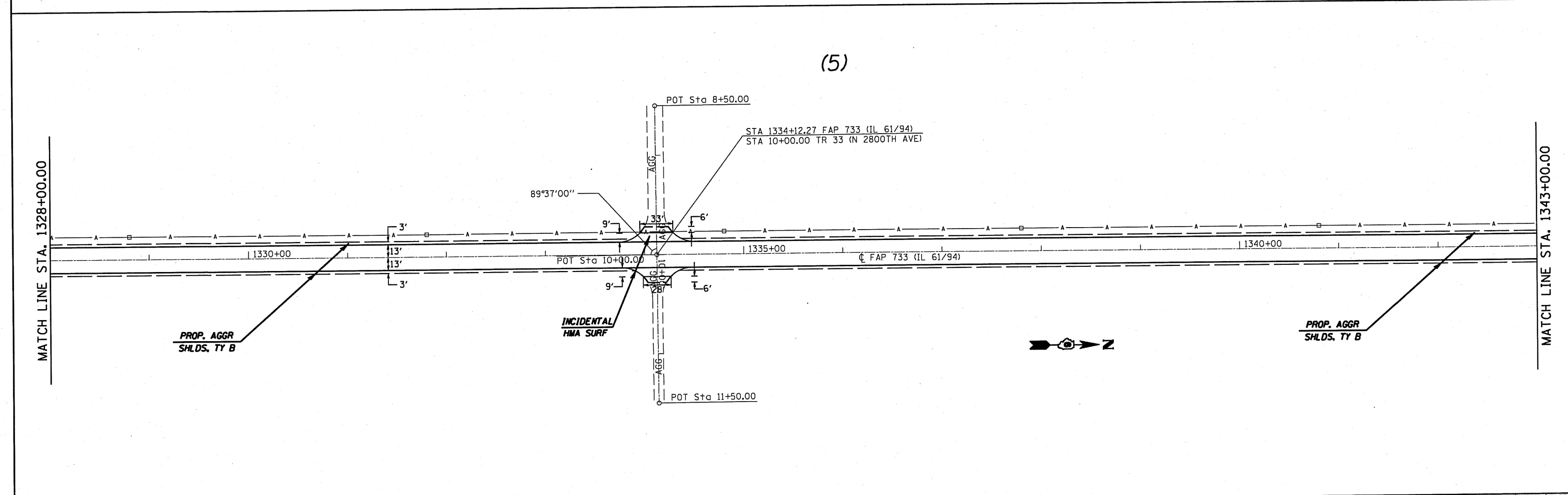
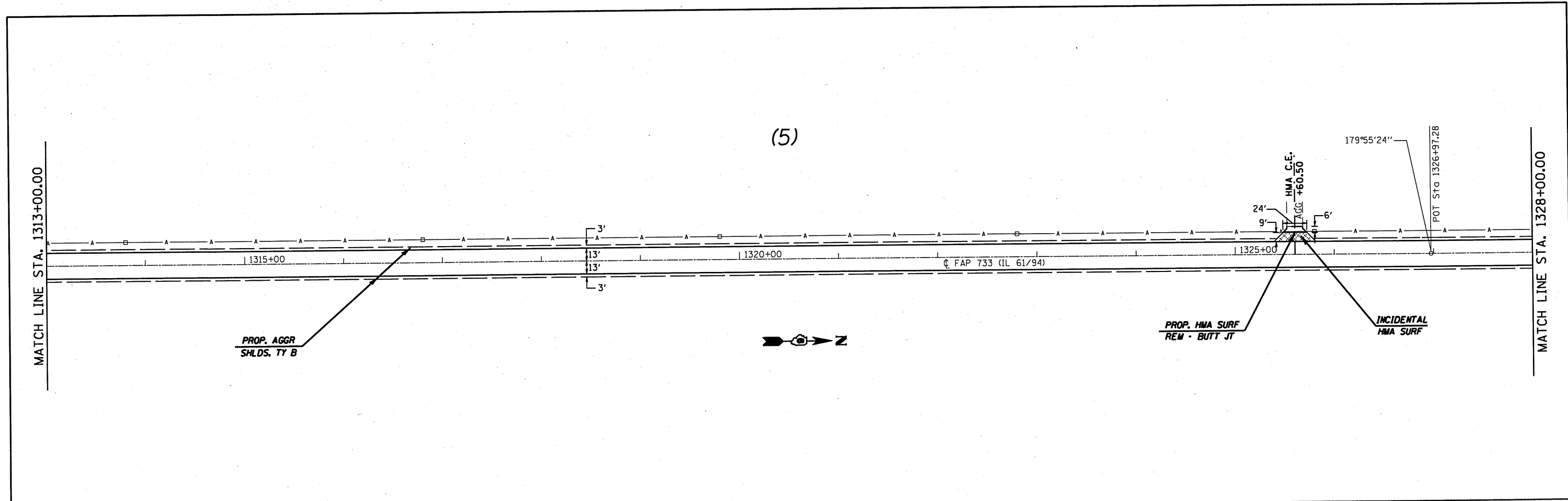
(5)

MATCH LINE STA. 1298+00.00

MATCH LINE STA. 1313+00.00

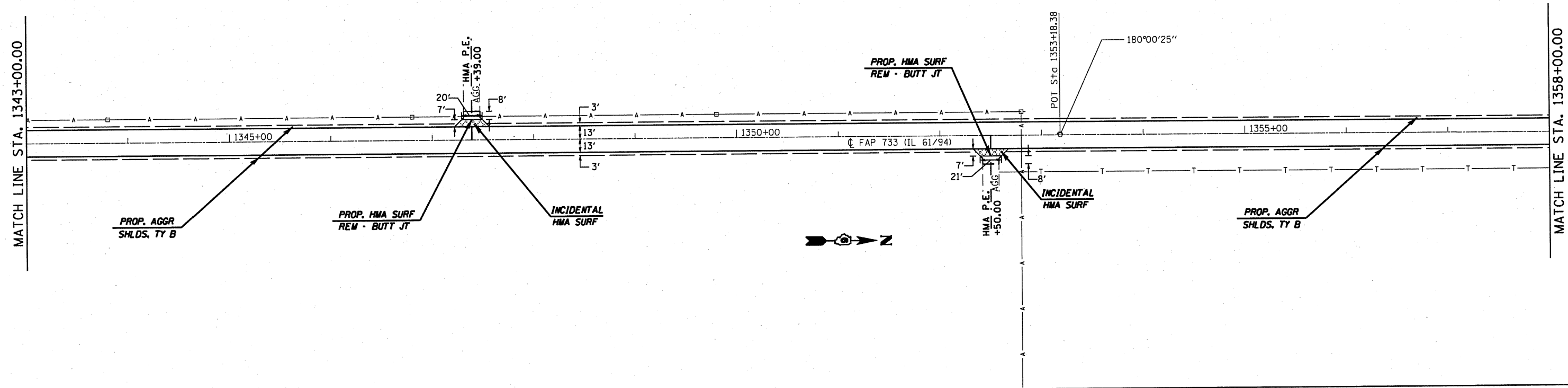


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PLOT SCALE = 100.0000' / IN.	CHECKED - RSC	REVISED -	REVISED -		CONTRACT NO. 72880							
PLOT DATE = Sep-08-2010 10:42:27AM	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE: 50		SHEET NO. 3 OF 23 SHEETS		STA. 1283+00.00 TO STA. 1313+00.00		ADAMS, HANCOCK		

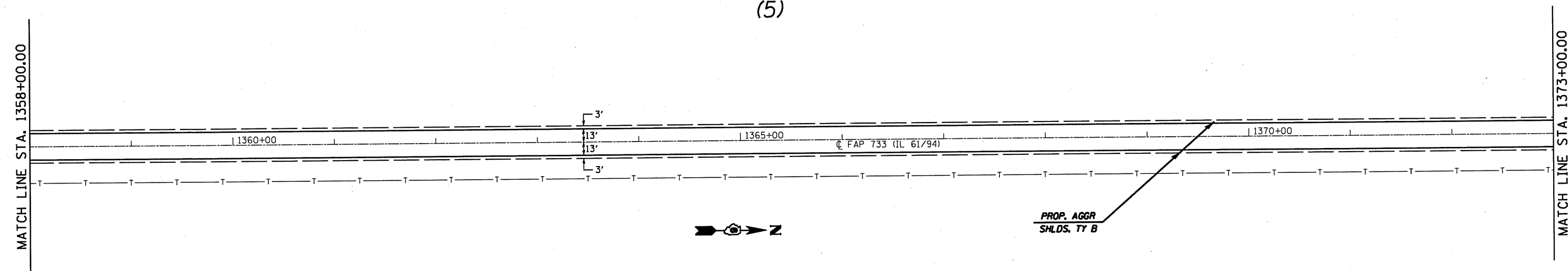


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cr:\pw_work\pwsdot\coxjm\0215198\672880\shp\plan58.dgn		DRAWN - JWC	REVISED -		SCALE: 50	SHEET NO. 4 OF 23 SHEETS	STA. 1313+00.00 TO STA. 1343+00.00	CONTRACT NO. 72880				
		PLOT SCALE = 100.0000' / IN.	REVISED -		ILLINOIS FED. AID PROJECT							
		PLOT DATE = Sep-08-2010 10:42:28AM	REVISED -		* ADAMS, HANCOCK							

(5)



(5)



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PLOT SCALE = 100.0000 / IN.		CHECKED - RSC	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

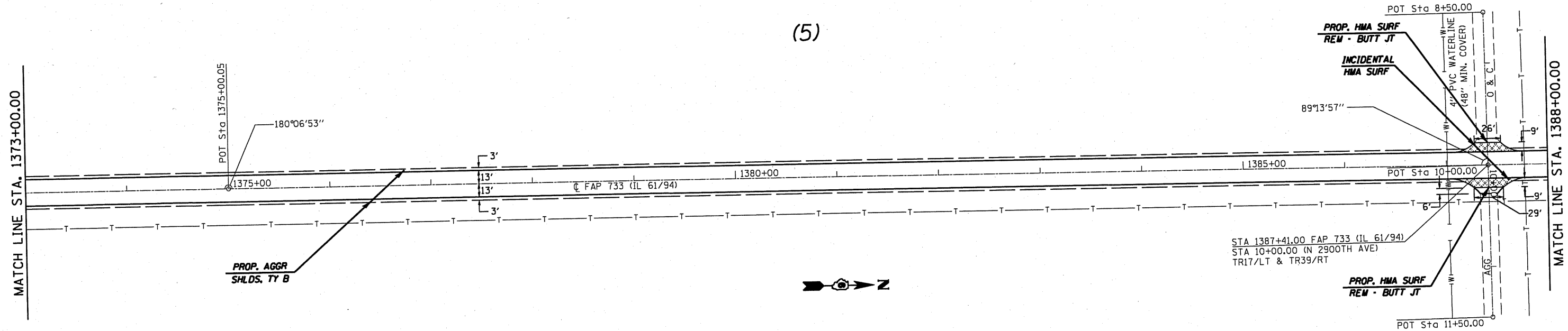
PLAN VIEW

SCALE: 50 SHEET NO. 5 OF 23 SHEETS STA. 1343+00.00 TO STA. 1373+00.00

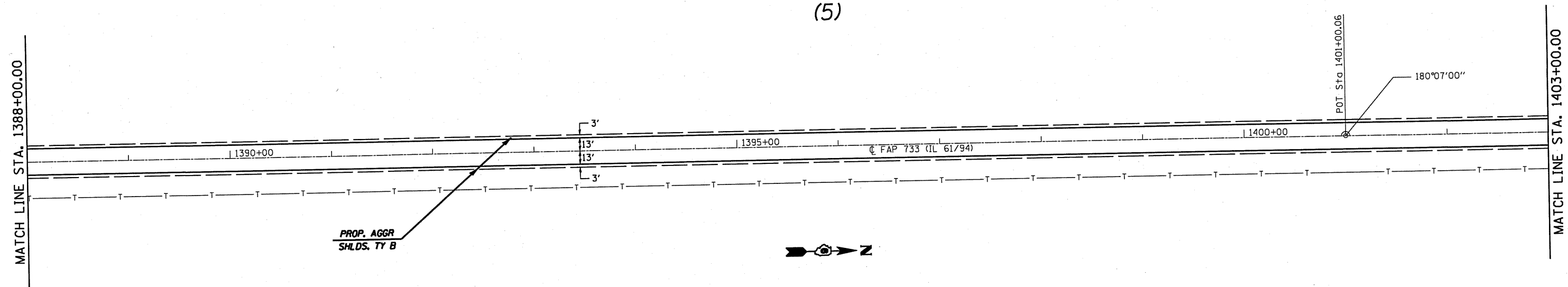
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
733	1RS-3, 7RS-5	*	48	20
CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT	

• ADAMS, HANCOCK

(5)



(5)



FILE NAME =
 c:\pwwork\pwwork\cox\jw\0215198\0672888

USER NAME = coxjw
 shd-plen50.dgn
 PLOT SCALE = 100.0000' / IN.
 PLOT DATE = Sep-08-2010 10:42:28AM

DESIGNED -	REVISED -
DRAWN - JWC	REVISED -
CHECKED - RSC	REVISED -
DATE -	REVISED -

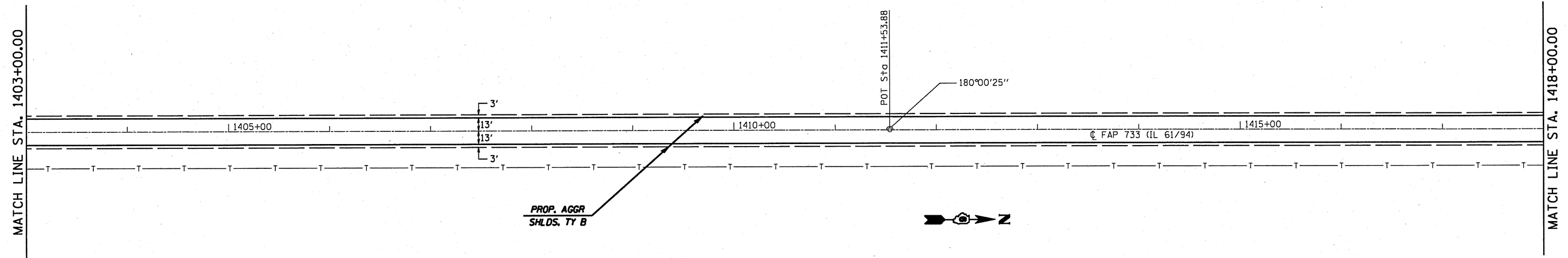
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN VIEW
 SCALE: 50 SHEET NO. 6 OF 23 SHEETS STA. 1373+00.00 TO STA. 1403+00.00

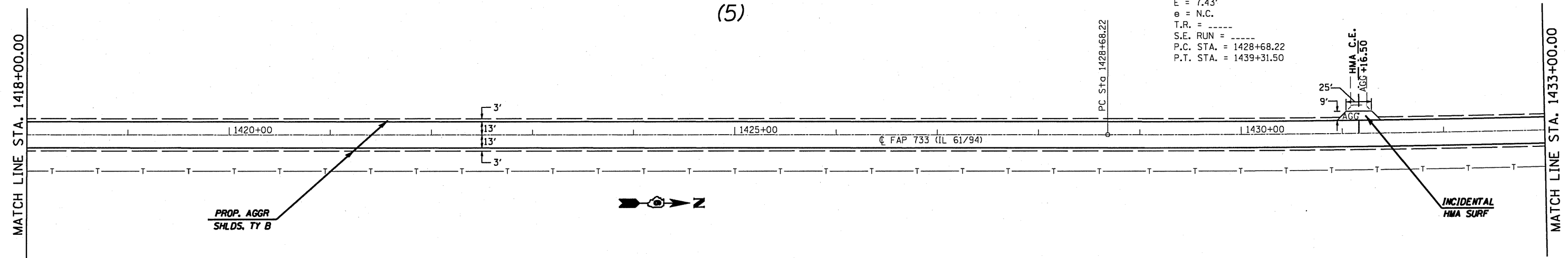
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
733	IRS-3, TRS-5	*	48	21
CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT	

* ADAMS, HANCOCK

(5)

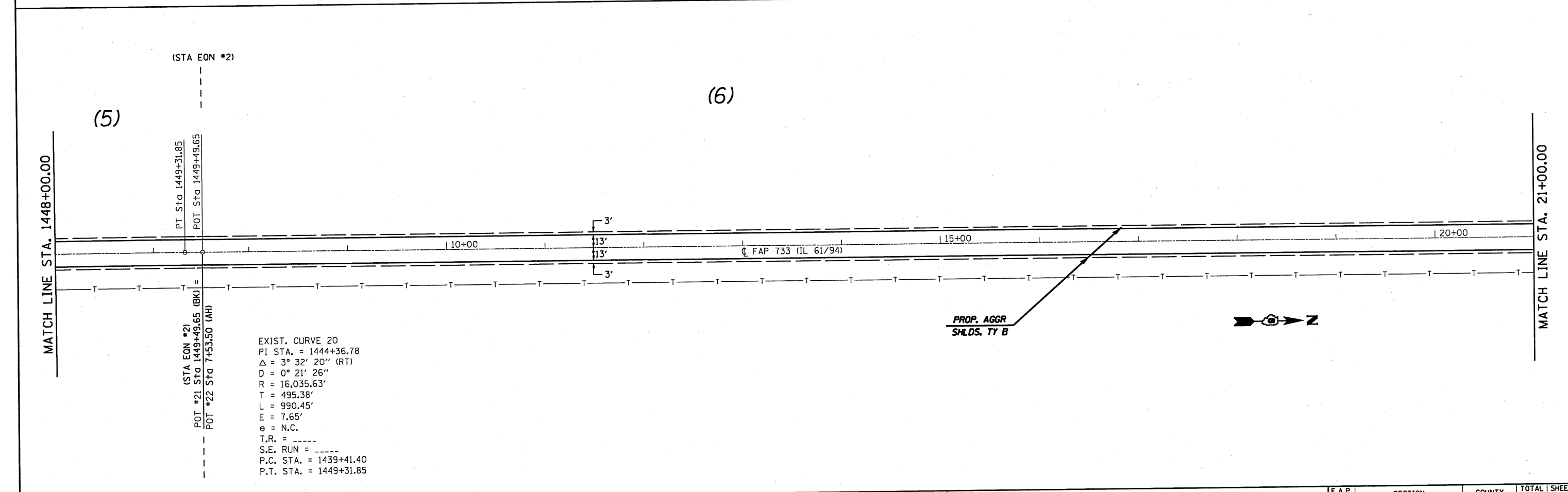
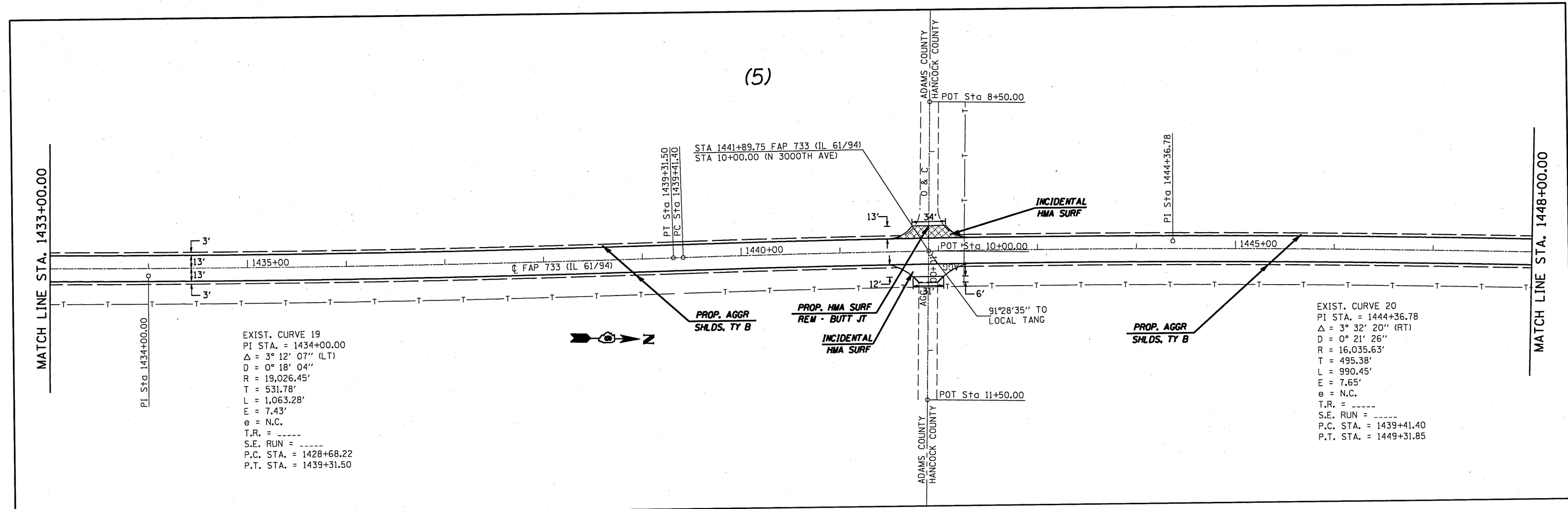


(5)

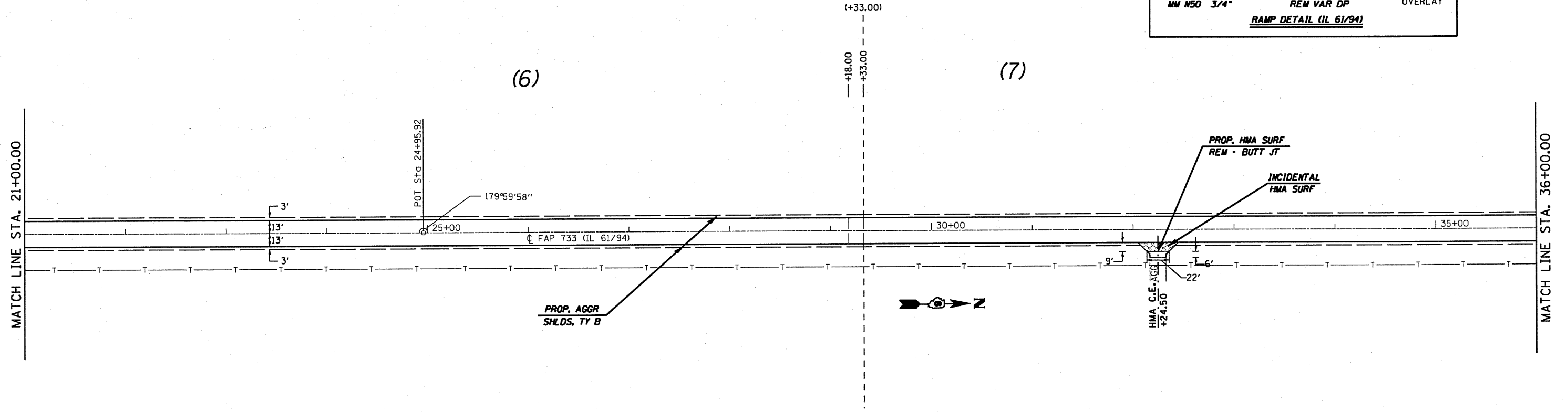
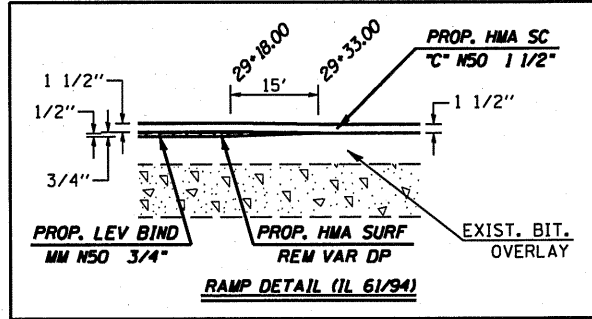


EXIST. CURVE 19
 PI STA. = 1434+00.00
 $\Delta = 3^\circ 12' 07''$ (LT)
 $D = 0^\circ 18' 04''$
 $R = 19,026.45'$
 $T = 531.78'$
 $L = 1,063.28'$
 $E = 7.43'$
 $\theta = \text{N.C.}$
 $T.R. = \text{-----}$
 $S.E. \text{ RUN} = \text{-----}$
 $P.C. \text{ STA.} = 1428+68.22$
 $P.T. \text{ STA.} = 1439+31.50$

FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\coxjw\d0215198\d672886-sh1-plan58.dgn	DRAWN - JWC	REVISED -	REVISED -		733	IRS-3, TRS-5		48	22		
PLOT SCALE = 100.0000' / IN.	CHECKED - RSC	REVISED -	REVISED -		CONTRACT NO. 72880						
PLOT DATE = Sep-08-2010 10:42:29AM	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT						
				SCALE: 50	SHEET NO. 7 OF 23 SHEETS	STA. 1403+00.00 TO STA. 1433+00.00	* ADAMS, HANCOCK				

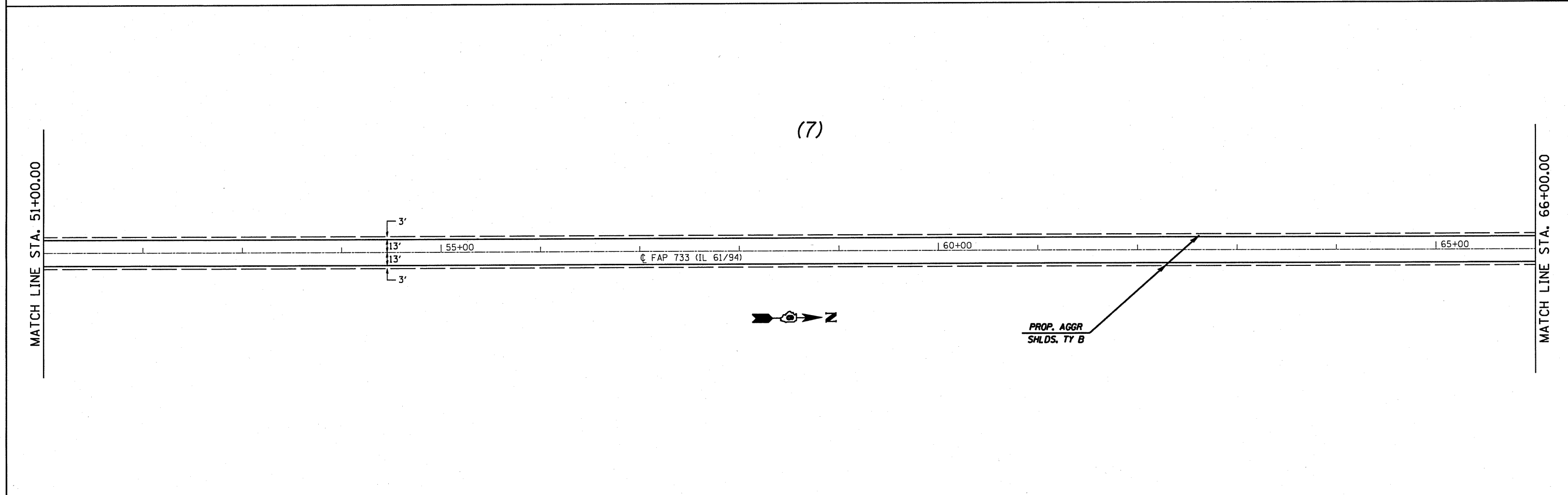
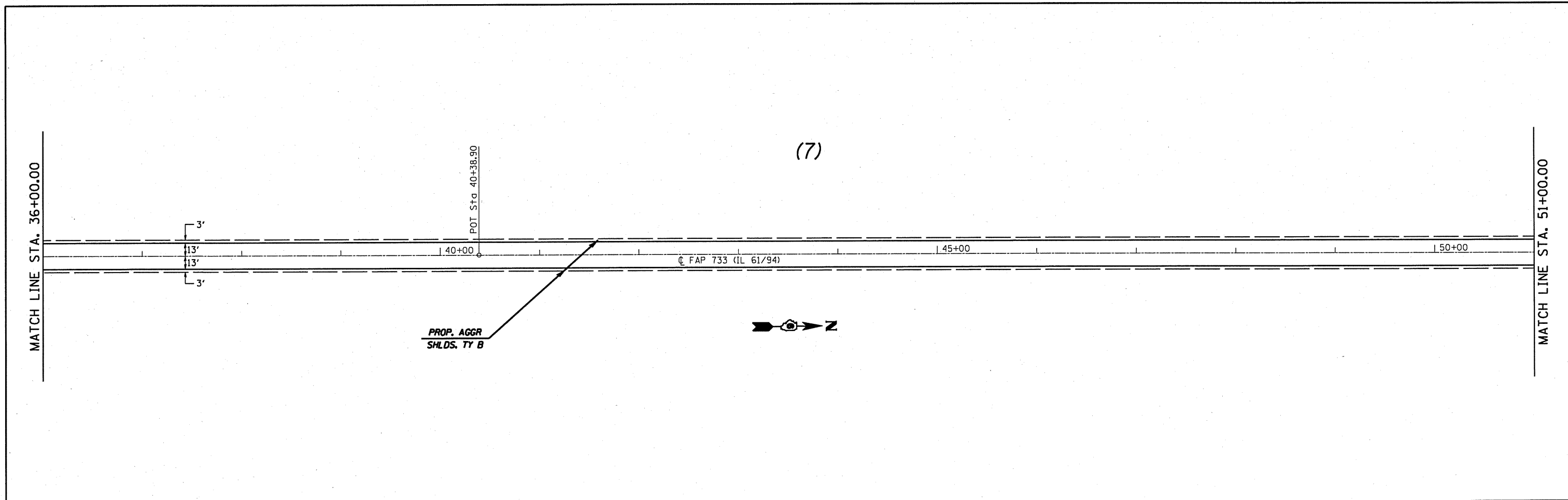


FILE NAME =		USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			PLAN VIEW			F.A.P. RTE. 733	SECTION IRS-3, TRS-5	COUNTY	TOTAL SHEETS 48	SHEET NO. 23
DRAWN - JWC		CHECKED - RSC	DATE -	REVISED -				SCALE: 50	SHEET NO. 8 OF 23 SHEETS	STA. 1433+00.00 TO STA. 21+00.00	CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT	
PLOT SCALE = 100.0000' / IN.		PLOT DATE = Sep-08-2010 10:42:33AM									ADAMS, HANCOCK				

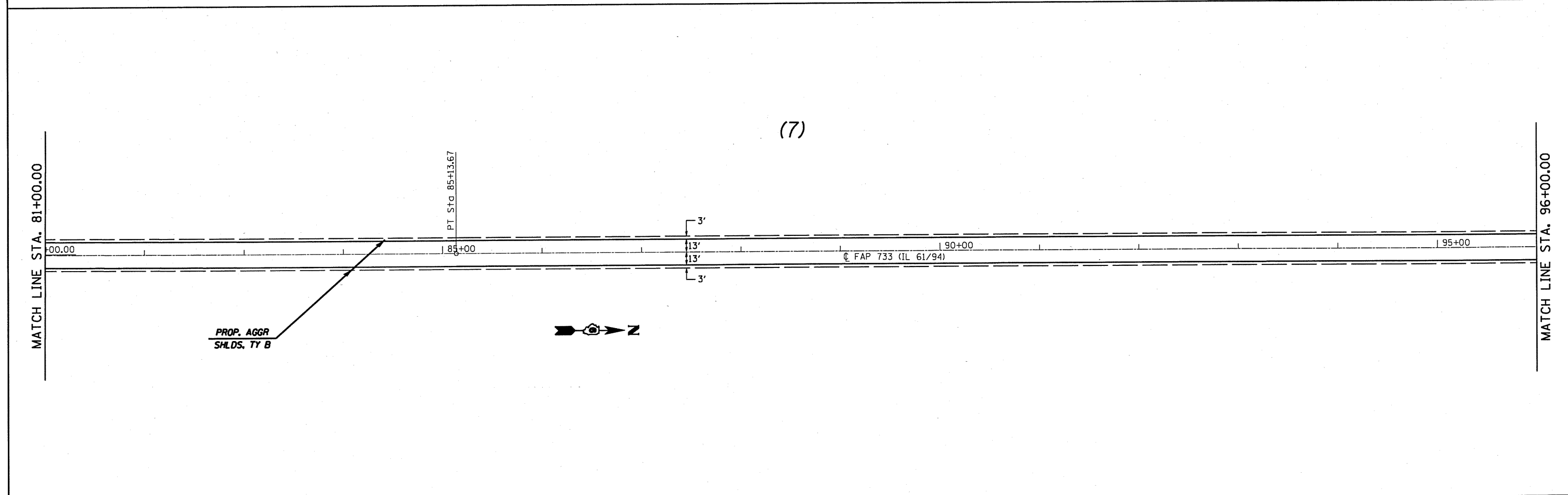
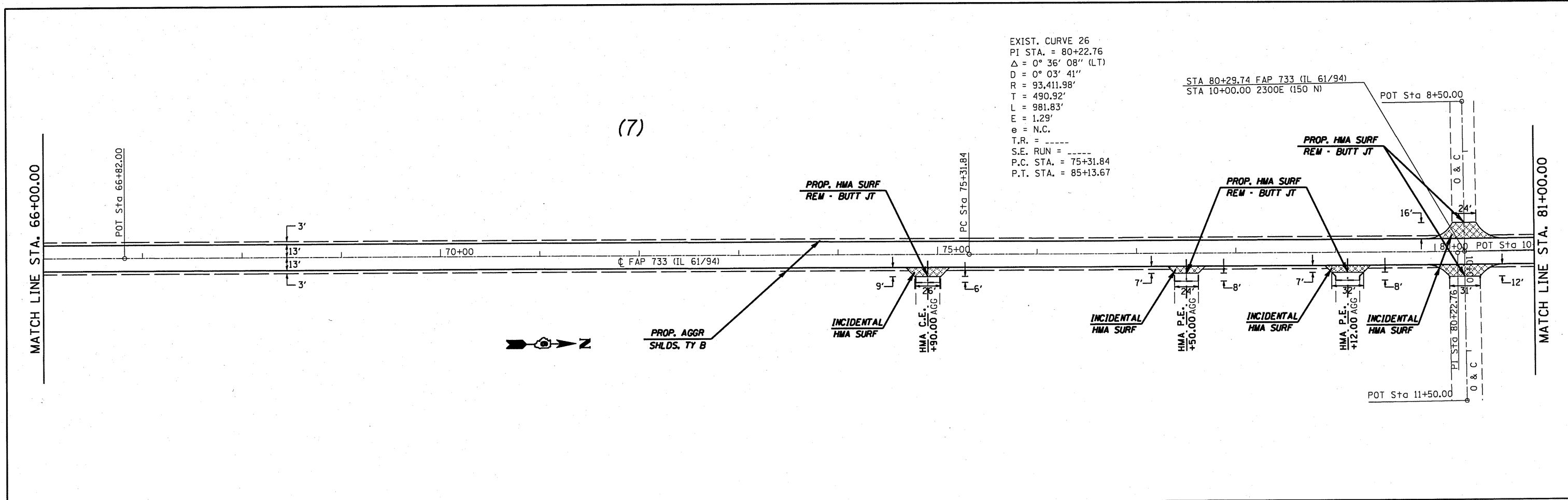


FILE NAME *	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwork\pwidot\coxjw\d0215198\d672880-sh-t-plan50.dgn	DRAWN - JWC	REVISED -	REVISED -		SCALE: 50	SHEET NO. 9 OF 23 SHEETS	STA. 21+00.00 TO STA. 36+00.00	733	IRS-3, 7RS-5	*	48	24
PLOT SCALE = 100.0000 ' / IN.	CHECKED - RSC	REVISED -	REVISED -					CONTRACT NO. 72880				
PLOT DATE = Sep-08-2010 10:42:33AM	DATE -	REVISED -	REVISED -					ILLINOIS FED. AID PROJECT				

* ADAMS, HANCOCK



FILE NAME =	USER NAME = cox,jw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\cox,jw\d0215198\d67288	-sh-1-plan50-1.dgn	DRAWN - JWC	REVISED -		733	IRS-3, 7RS-5	*	48	25		
	PLOT SCALE = 100.0000 ' / IN.	CHECKED - RSC	REVISED -		CONTRACT NO. 72880						
	PLOT DATE = Sep-08-2010 10:42:36AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT						
				SCALE: 50		SHEET NO. 10 OF 23 SHEETS		STA. 36+00.00 TO STA. 66+00.00		ADAMS, HANCOCK	



FILE NAME = c:\pwork\pwidot\coxjw\d0215198\d672880	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW			F.A.P. RTE. 733	SECTION IRS-3, TRS-5	COUNTY •	TOTAL SHEETS 48	SHEET NO. 26
	ahht-plan50-1.dgn	DRAWN - JWC	REVISED -		SCALE: 50			SHEET NO. 11 OF 23 SHEETS			CONTRACT NO. 72880	
	PLOT SCALE = 100.0000 ' / IN.	CHECKED - RSC	REVISED -		STA. 66+00.00 TO STA. 96+00.00			ILLINOIS FED. AID PROJECT				
	PLOT DATE = Sep-08-2010 10:42:37AM	DATE -	REVISED -		• ADAMS, HANCOCK							

(7)

MATCH LINE STA. 96+00.00

PROP. AGGR
SHLDS. TY B

100+00

Q FAP 733 (IL 61/94)

HMA P.E.
+50.50

105+00

POT Sta 106+33.61

POT Sta 8+50.00

STA 107+04.03 FAP 733 (IL 61/94)
STA 10+00.00 2300E (200 N)

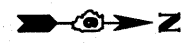
110+00

INCIDENTAL
HMA SURF

PROP. HMA SURF
REM - BUTT JT

INCIDENTAL
HMA SURF

MATCH LINE STA. 111+00.00



(7)

MATCH LINE STA. 111+00.00

PROP. AGGR
SHLDS. TY B

115+00

Q FAP 733 (IL 61/94)

120+00

MATCH LINE STA. 122+00.00



FILE NAME =
c:\pw_work\pwsdot\coxjw\d0215198\d672880\shp\plan50-1.dgn

USER NAME = coxjw
PLOT SCALE = 100.0000' / IN.
PLOT DATE = Sep-08-2010 10:42:37AM

DESIGNED -
DRAWN - JWC
CHECKED - RSC
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

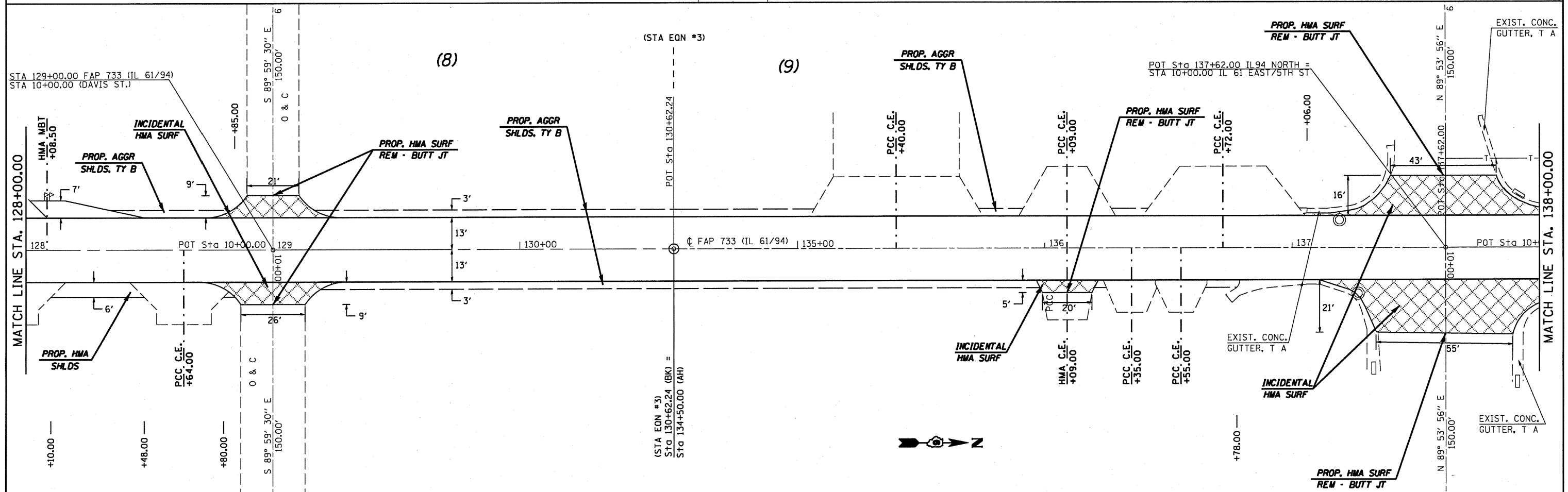
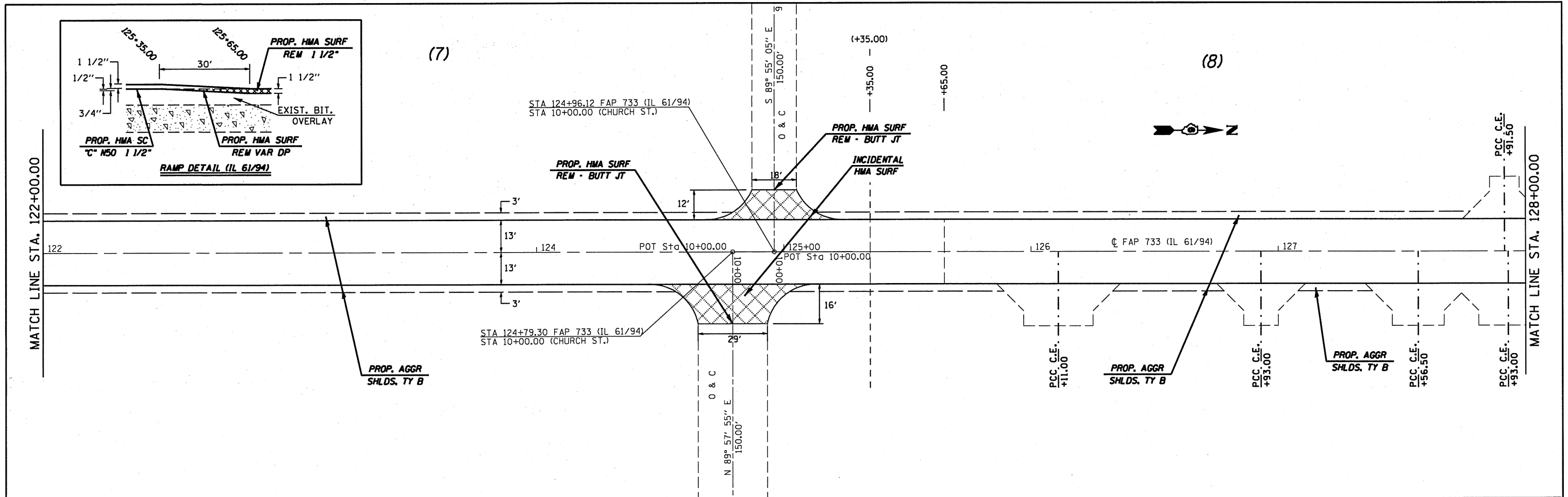
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW

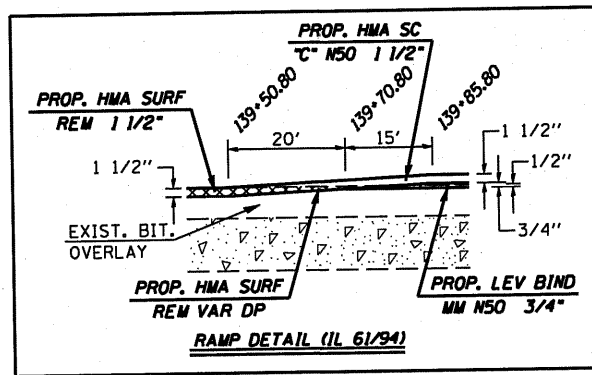
SCALE: 50 SHEET NO. 12 OF 23 SHEETS STA. 96+00.00 TO STA. 122+00.00

F.A.P. RTE. 733	SECTION IRS-3, TRS-5	COUNTY *	TOTAL SHEETS 48	SHEET NO. 27
CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT	

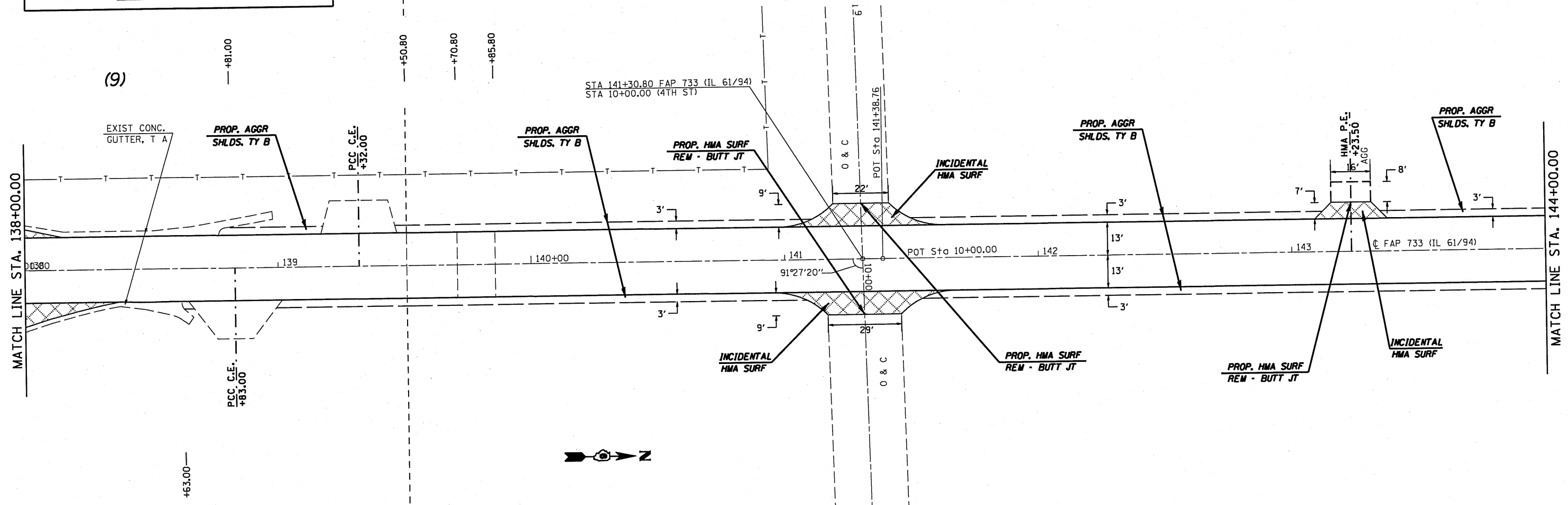
ADAMS, HANCOCK



FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\puidot\coxjw\d0215198\d672880	sht-plan28-ldgn	DRAWN - JWC	REVISED -		733	IRS-3, 7RS-5		48	28		
PLOT SCALE = 40.0000' / IN.	CHECKED - RSC	REVISIONS	REVISIONS		CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT			
PLOT DATE = Sep-08-2010 10:42:40AM	DATE -	REVISIONS	REVISIONS		SCALE: 20	SHEET NO. 13 OF 23 SHEETS	STA. 122+00.00 TO STA. 138+00.00	ADAMS, HANCOCK			



(10)



FILE NAME =	USER NAME = coxjm	DESIGNED -	REVISED -
ca:\pwwork\pwwork\coxjm\0215198\0672880	sh-t-plen28.dgn	DRAWN - JWC	REVISED -
	PLOT SCALE = 40.0000' / IN.	CHECKED - RSC	REVISED -
	PLOT DATE = Sep-08-2010 10:42:43AM	DATE -	REVISED -

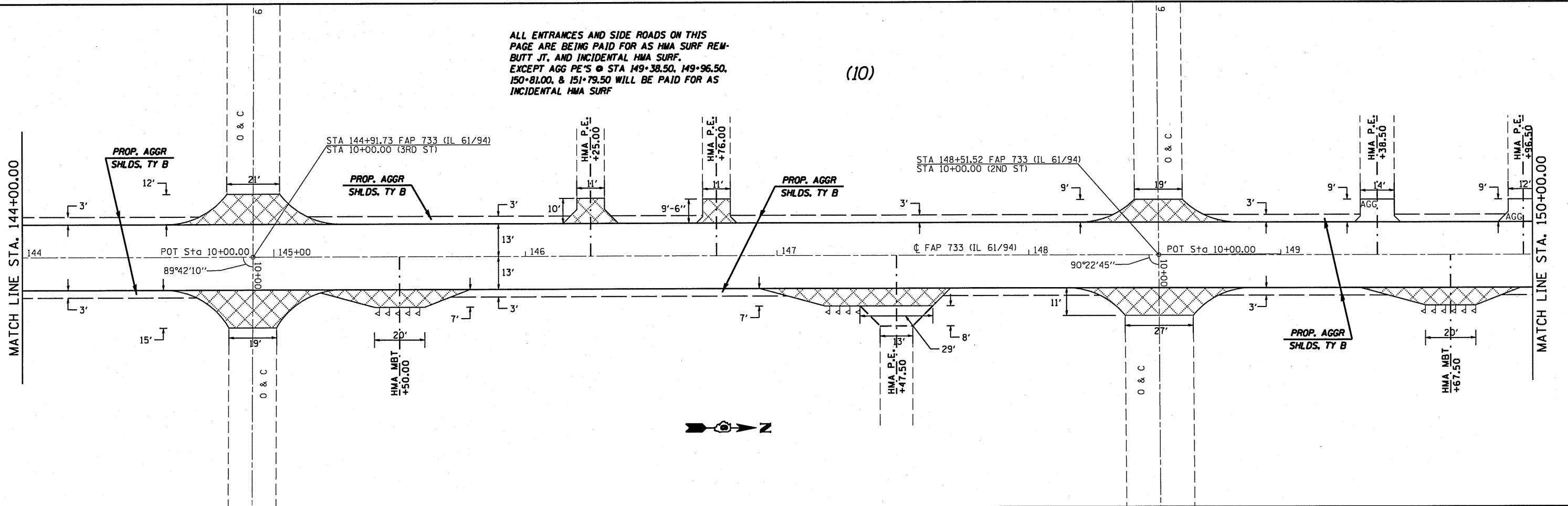
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN VIEW		
SCALE: 20	SHEET NO. 14 OF 23 SHEETS	STA. 138+00.00 TO STA. 144+00.00

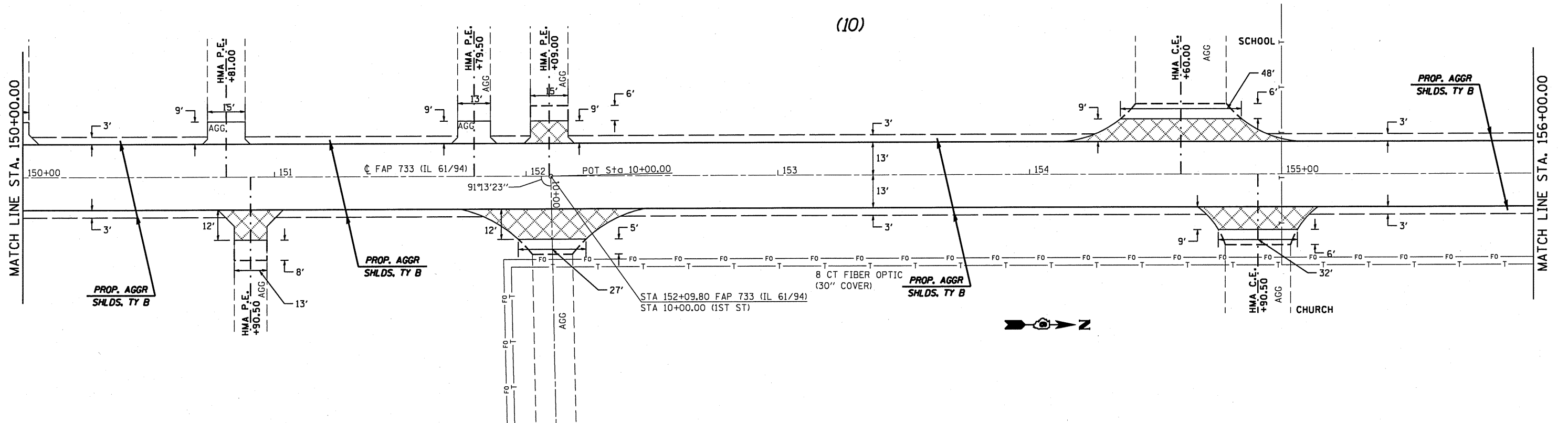
F.A.P. RTE. 733	SECTION IRS-3, 7RS-5	COUNTY	TOTAL SHEETS 48	SHEET NO. 29
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72880	
ADAMS, HANCOCK				

ALL ENTRANCES AND SIDE ROADS ON THIS PAGE ARE BEING PAID FOR AS HMA SURF REM-BUTT JT. AND INCIDENTAL HMA SURF. EXCEPT AGG PE'S @ STA 149+38.50, 149+96.50, 150+81.00, & 151+79.50 WILL BE PAID FOR AS INCIDENTAL HMA SURF

(10)



(10)



FILE NAME =	USER NAME = coxjw
ca:\pwwork\pwwork\coxjw\d0215198\vd672880	sh-t-plan20.dgn
PLOT SCALE = 40,0000' / IN.	
PLOT DATE = Sep-08-2010 10:42:43AM	

DESIGNED -	REVISD -
DRAWN - JWC	REVISD -
CHECKED - RSC	REVISD -
DATE -	REVISD -

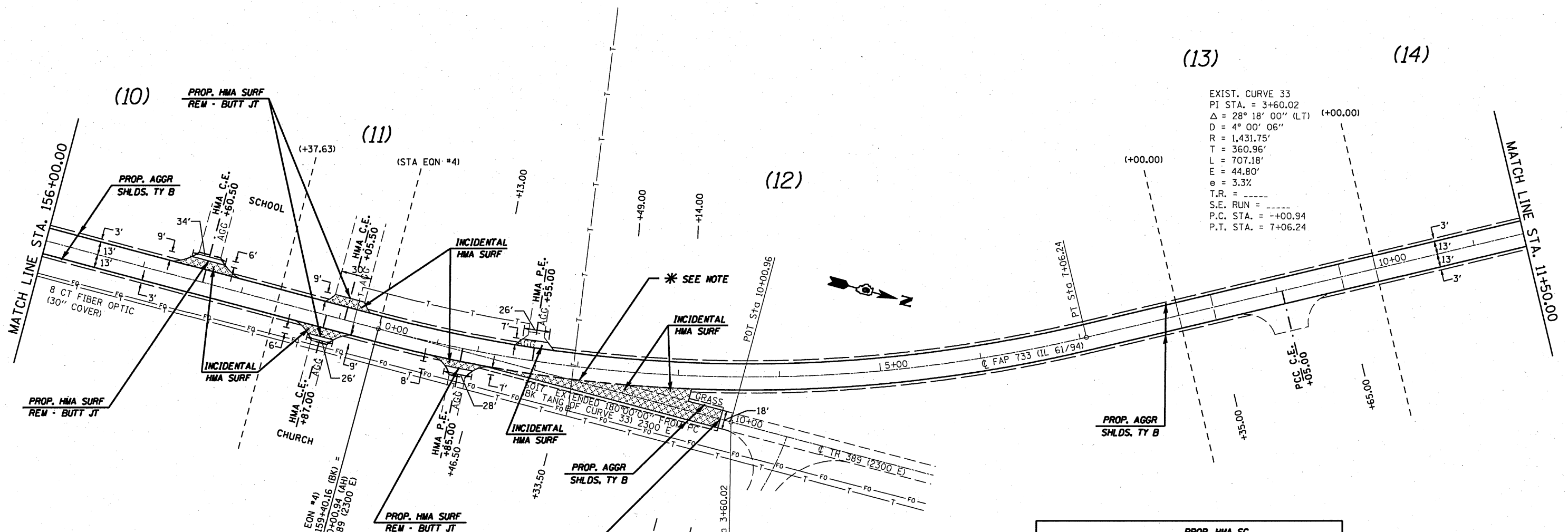
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
SCALE: 20
SHEET NO. 15 OF 23 SHEETS
STA. 144+00.00 TO STA. 156+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
733	IRS-3, TRS-5		48	30
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72880	

ADAMS, HANCOCK

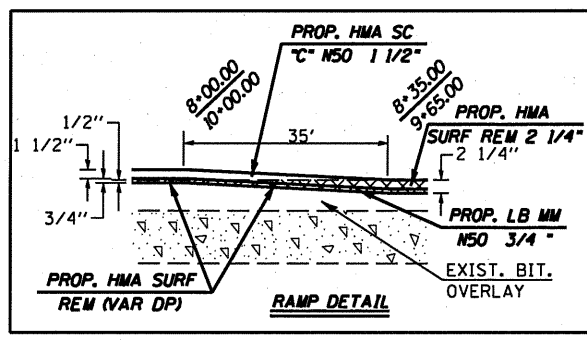
(PROP.)	(EXIST.)
FULL S.E.: (S.E.#1 = 3.3%)	FULL S.E.: (S.E.#1 = 3.3%)
STA. 0+31.41 to 6+75.77	STA. 0+52.39 TO 6+43.23
S.E. TRANSITION:	S.E. TRANSITION:
STA. 158+37.63 TO 159+40.16 (BK) =	STA. 158+12.49 TO 159+40.16 (BK) =
STA. 0+00.94 (AH) TO 0+31.41	STA. 0+00.94 (AH) TO 0+52.39
STA. 6+75.77 TO 8+08.77	STA. 6+43.23 TO 8+33.83



(13) (14)

EXIST. CURVE 33
 PI STA. = 3+60.02
 $\Delta = 28^\circ 18' 00''$ (LT) (+00.00)
 $D = 4^\circ 00' 06''$
 $R = 1,431.75'$
 $T = 360.96'$
 $L = 707.18'$
 $E = 44.80'$
 $e = 3.3\%$
 $T.R. = \text{---}$
 $S.E. RUN = \text{---}$
 $P.C. STA. = -+00.94$
 $P.T. STA. = 7+06.24$

* NOTE:
 PROP. PAINT PVT. MK LINE 6
 FR. STA 1+46.50 TO 3+14.00 RT
 (2"x6"x6" - SHORT SKIP DASH)

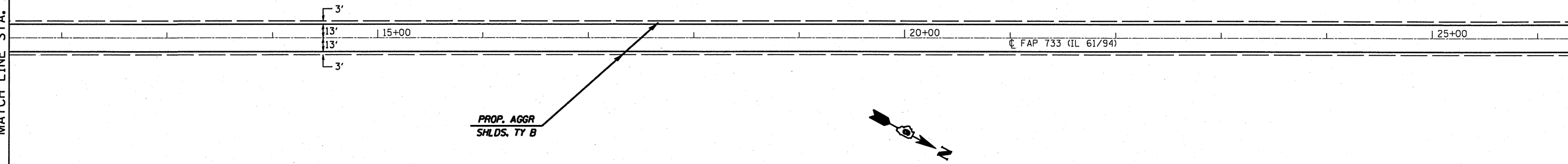


FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\coxjw\d0215198\d672880	sht-plan50.dgn	DRAWN - JWC	REVISED -		SCALE: 50	SHEET NO. 16 OF 23 SHEETS	STA. 156+00.00 TO STA. 11+50.00	733	IRS-3, TRS-5	*	48	31
		CHECKED - RSC	REVISED -					CONTRACT NO. 72880				
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

(14)

MATCH LINE STA. 11+50.00

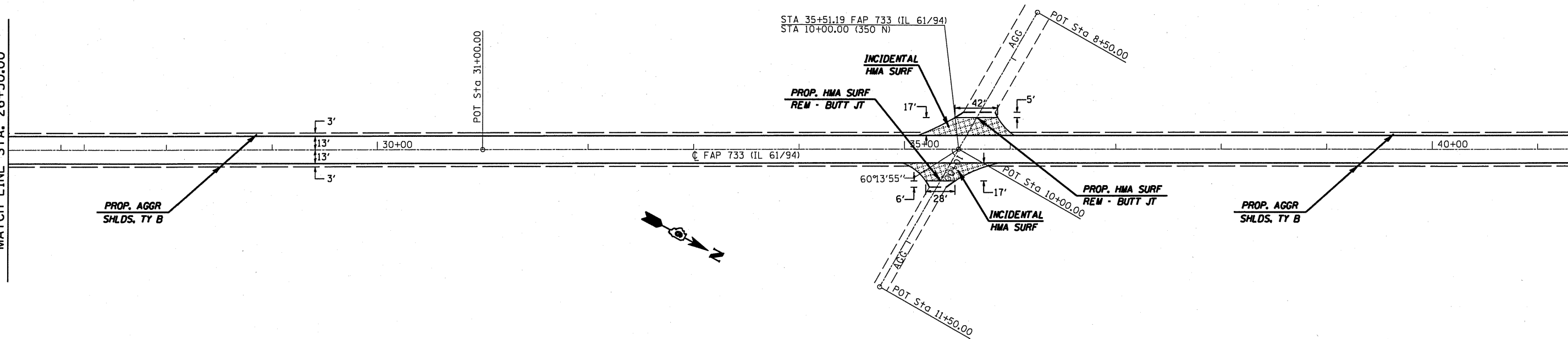
MATCH LINE STA. 26+50.00



(14)

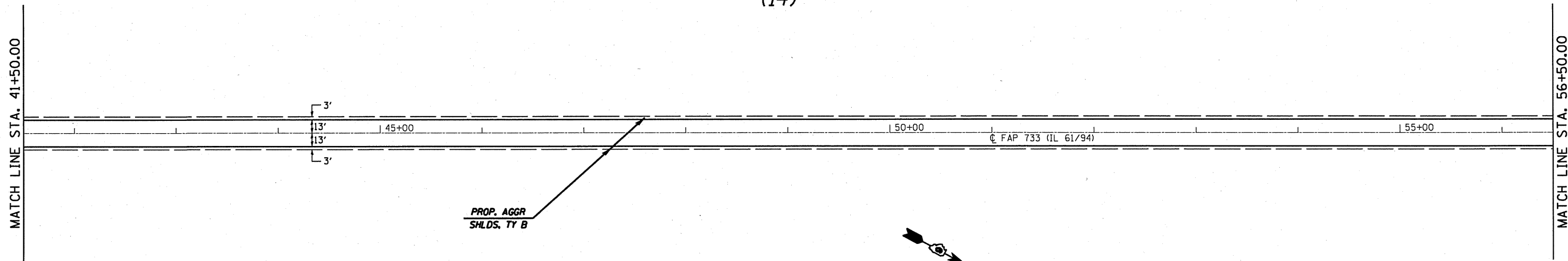
MATCH LINE STA. 26+50.00

MATCH LINE STA. 41+50.00



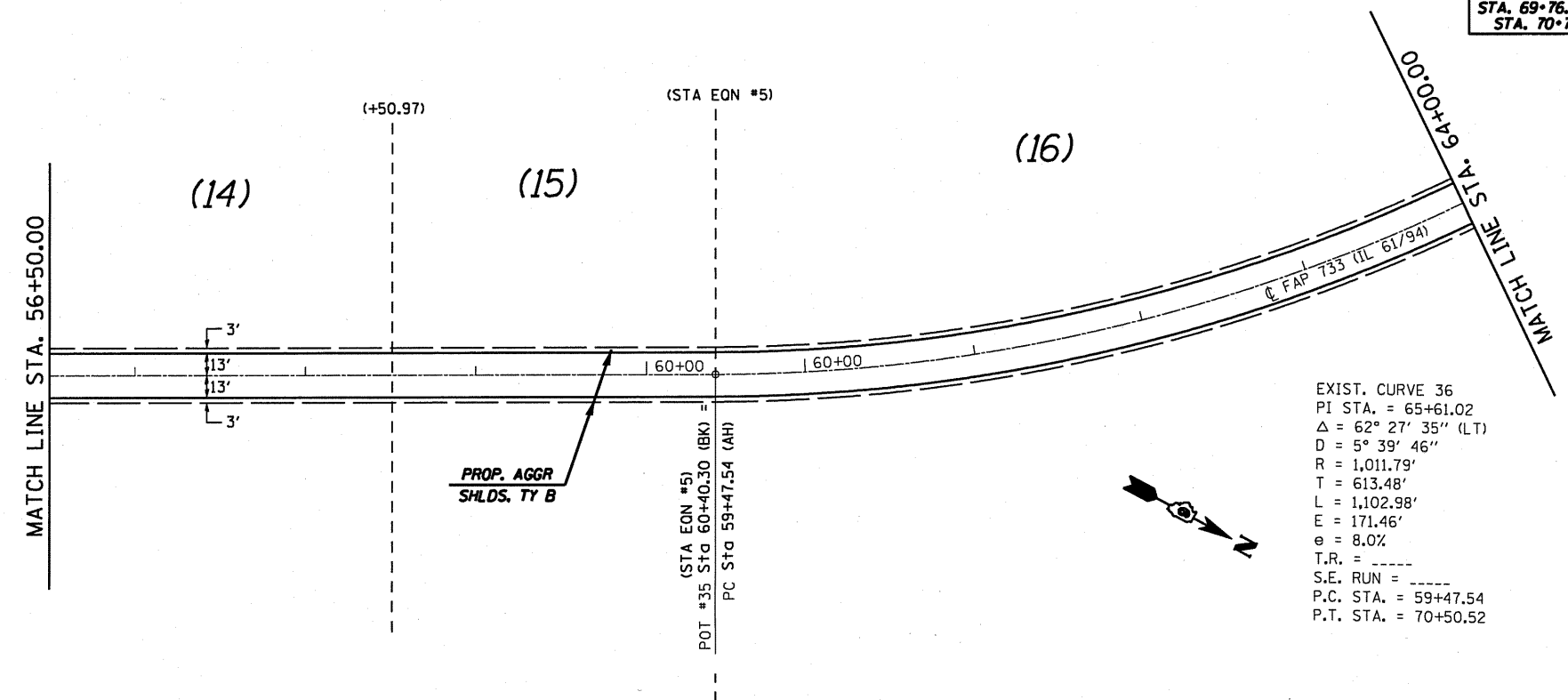
FILE NAME =	USER NAME = cox,jw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\cox,jw\d0215198\d672888	-sht-plan50.dgn	DRAWN - JWC	REVISED -		733	IRS-3, 7RS-5		48	32			
	PLOT SCALE = 100.0000' / IN.	CHECKED - RSC	REVISED -		SCALE: 50 SHEET NO. 17 OF 23 SHEETS STA. 11+50.00 TO STA. 41+50.00			CONTRACT NO. 72880				
	PLOT DATE = Sep-08-2010 10:42:29AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT • ADAMS, HANCOCK							

(14)



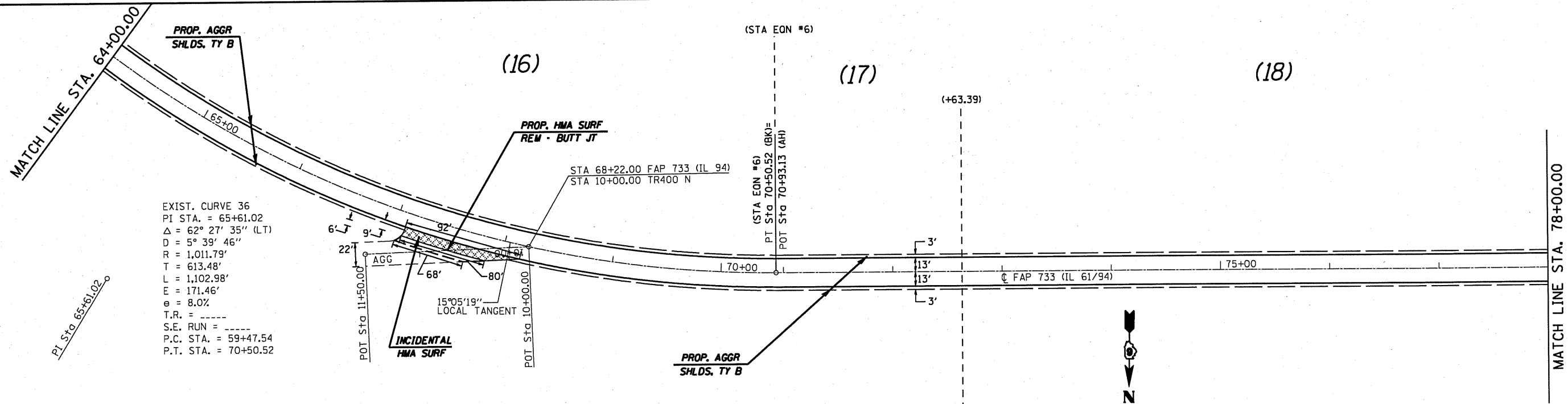
FULL S.E.: STA. 60+21.41 TO 69+76.66 S.E. TRANSITION: STA. 58+50.97 TO 60+40.30 (BK) = STA. 59+47.54 (AH) TO 60+21.41 STA. 69+76.66 TO 70+50.53 (BK) = STA. 70+74.06 (AH) TO 72+63.39	FULL S.E.: STA. 60+24.20 TO 69+73.86 S.E. TRANSITION: STA. 58+86.97 TO 60+40.30 (BK) = STA. 59+47.54 (AH) TO 60+24.20 STA. 69+73.86 TO 70+50.53 (BK) = STA. 70+74.06 (AH) TO 72+27.39
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(16)

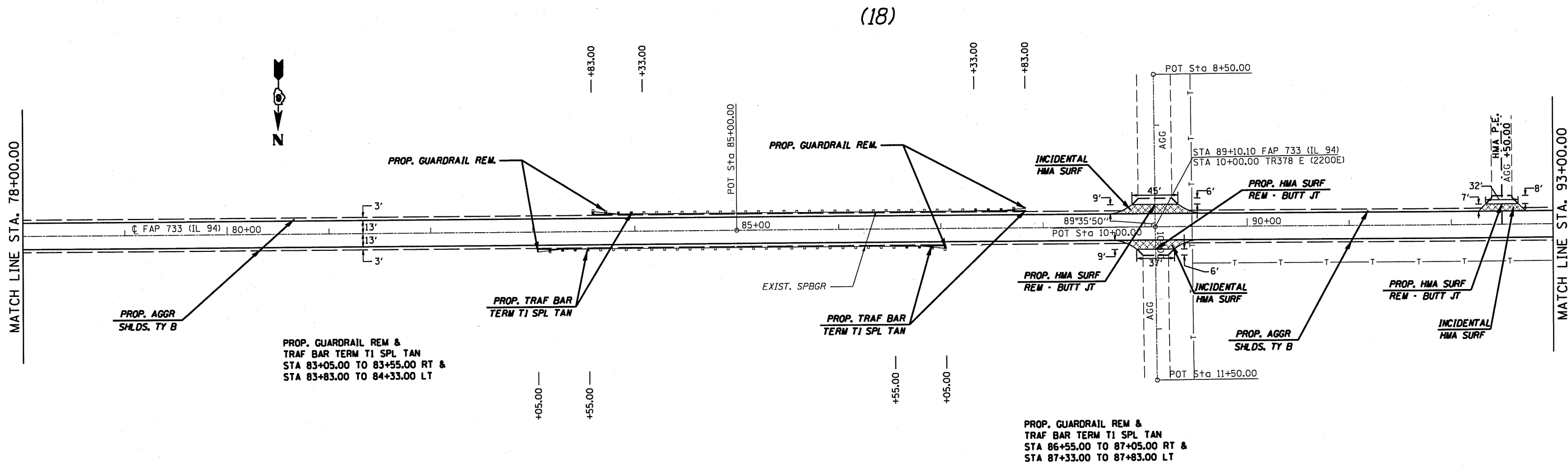


EXIST. CURVE 36
 PI STA. = 65+61.02
 Δ = 62° 27' 35" (LT)
 D = 5° 39' 46"
 R = 1,011.79'
 T = 613.48'
 L = 1,102.98'
 E = 171.46'
 e = 8.0%
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 59+47.54
 P.T. STA. = 70+50.52

FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\dot\coxjw\d0215198\d672880	sh-t-plan50.dgn	DRAWN - JWC	REVISED -		733	IRS-3, 7RS-5		48	33		
	PLOT SCALE = 100.0000' / IN.	CHECKED - RSC	REVISED -		CONTRACT NO. 72880						
	PLOT DATE = Sep-06-2010 10:42:30AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT						
				SCALE:	SHEET NO. 18 OF 23 SHEETS	STA. 41+50.00 TO STA. 64+00.00	* ADAMS, HANCOCK				



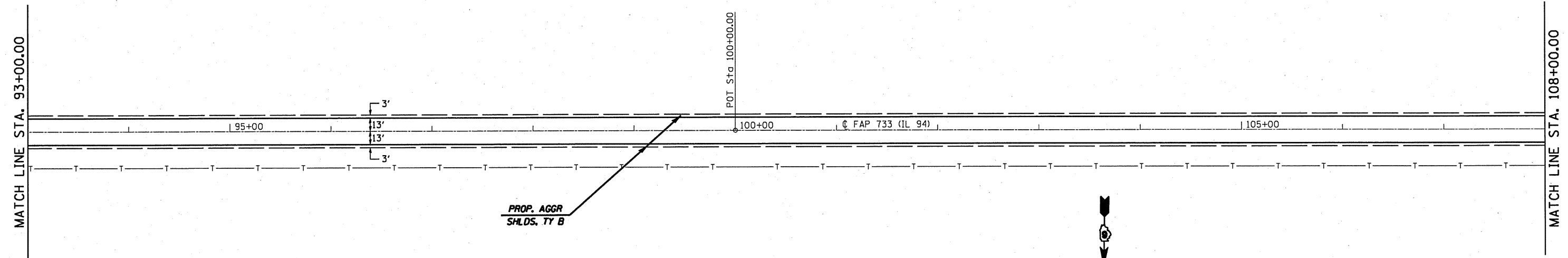
FULL S.E.: (S.E.#2 = 8.0%) STA. 60+21.41 to 69+76.66 S.E. TRANSITION: STA. 58+50.97 to 60+40.30 (BK) = STA. 59+47.54 (AH) to 60+21.41 STA. 69+76.66 to 70+50.53 (BK) = STA. 70+74.06 (AH) to 72+63.39	FULL S.E.: (S.E.#2 = 8.0%) STA. 60+24.20 to 69+73.86 S.E. TRANSITION: STA. 58+86.97 to 60+40.30 (BK) = STA. 59+47.54 (AH) to 60+24.20 STA. 69+73.86 to 70+50.53 (BK) = STA. 70+74.06 (AH) to 72+27.39
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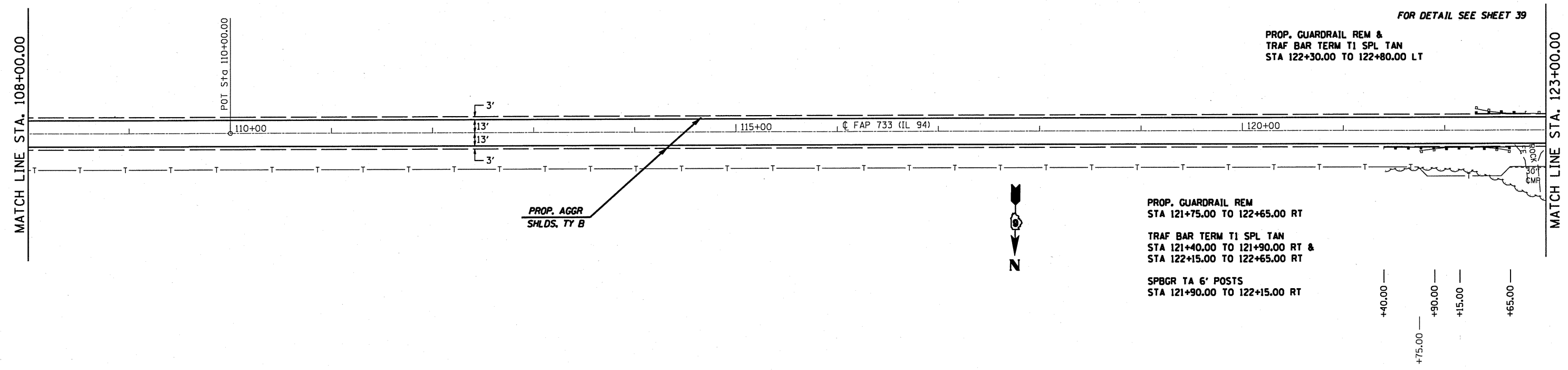
FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE. 733	SECTION IRS-3, TRS-5	COUNTY	TOTAL SHEETS 48	SHEET NO. 34	
al\p\work\p\p\dot\coxjw\08215198\d672886	shtrplan58.dgn	DRAWN - JWC	REVISED -		SCALE: 50	SHEET NO. 19 OF 23 SHEETS	STA. 64+00.00 TO STA. 93+00.00	CONTRACT NO. 72880		(ILLINOIS) FED. AID PROJECT		
		CHECKED - RSC	REVISED -									
		DATE -	REVISED -									

• ADAMS, HANCOCK

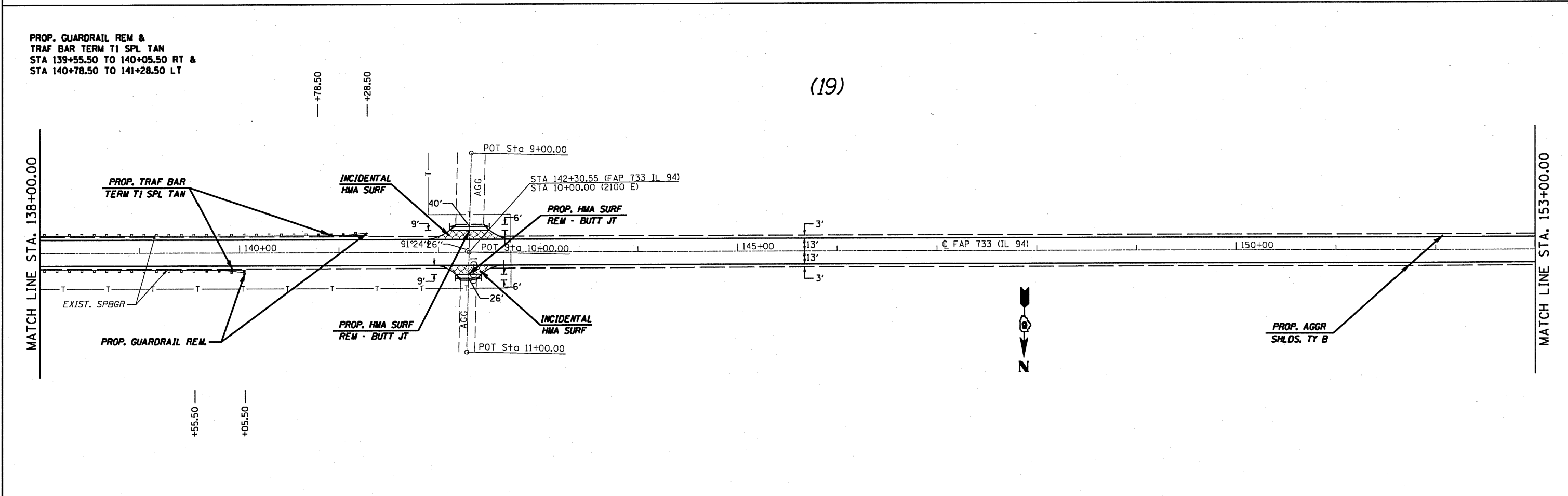
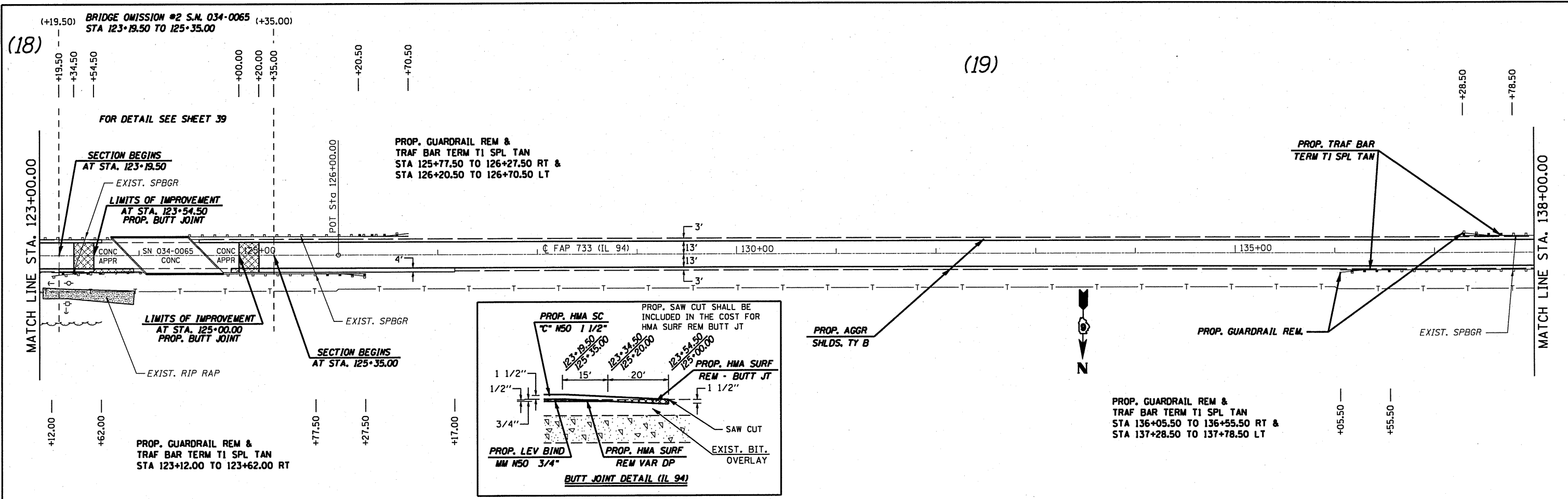
(18)



(18)

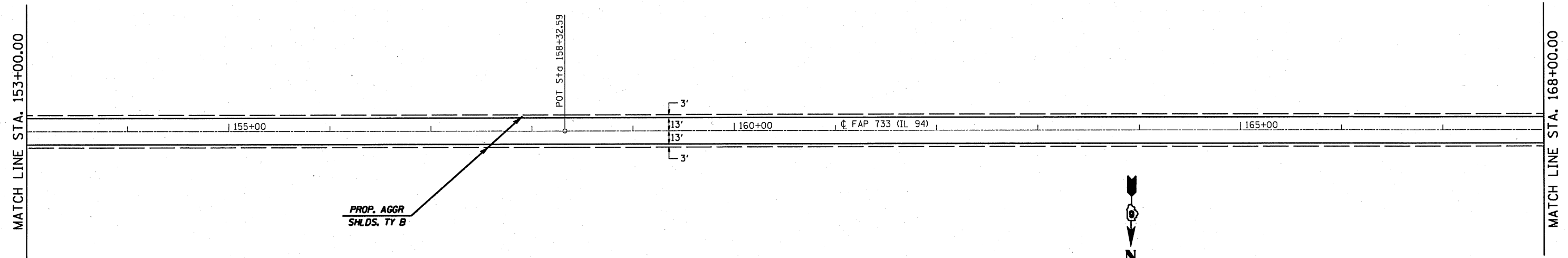


FILE NAME =	USER NAME = cox.jw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwwork\pwwork\cox.jw\d0215198\d672886-sh1-plan58.dgn	DRAWN - JWC	REVISED -	REVISED -		733	IRS-3, 7RS-5	*	48	35		
PLOT SCALE = 100.0000' / IN.	CHECKED - RSC	REVISED -	REVISED -		CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT			
PLOT DATE = Sep-08-2010 10:42:31AM	DATE -	REVISED -	REVISED -		SCALE: 50	SHEET NO. 20 OF 23 SHEETS	STA. 93+00.00 TO STA. 123+00.00	* ADAMS, HANCOCK			



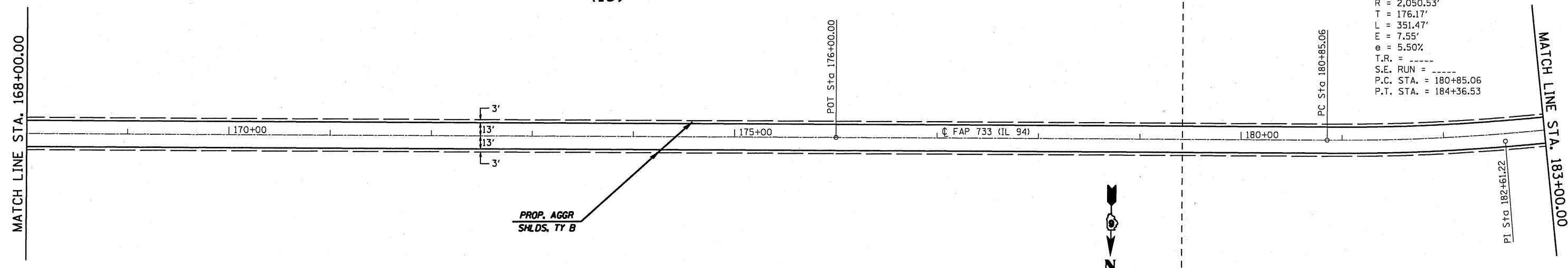
FILE NAME =	USER NAME = cox,jw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\cox\jw\0215198\d672886-shr-plan58.dgn	DRAWN - JWC	REVISED -	REVISED -		733	IRS-3, TRS-5	*	48	36		
PLOT SCALE = 1/8" = 100.0000' / IN.	CHECKED - RSC	REVISED -	REVISED -		CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT			
PLOT DATE = Sep-08-2010 10:42:31AM	DATE -	REVISED -	REVISED -		SCALE: 50	SHEET NO. 21 OF 23 SHEETS	STA. 123+00.00 TO STA. 153+00.00	* ADAMS, HANCOCK			

(19)



(19)

(20)

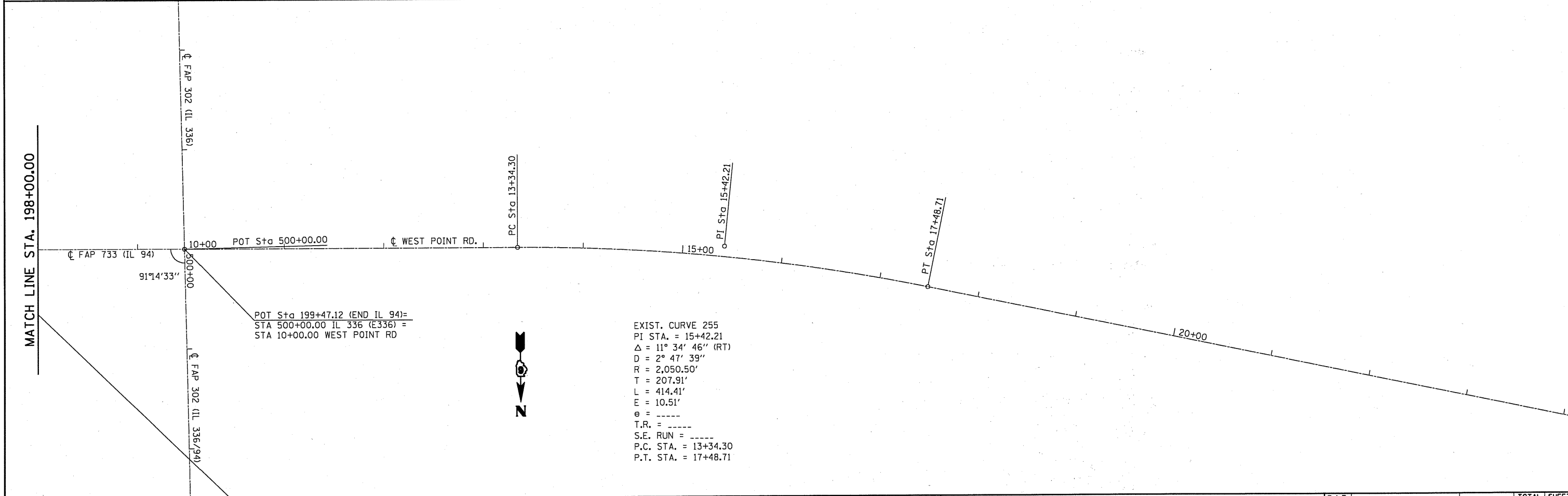
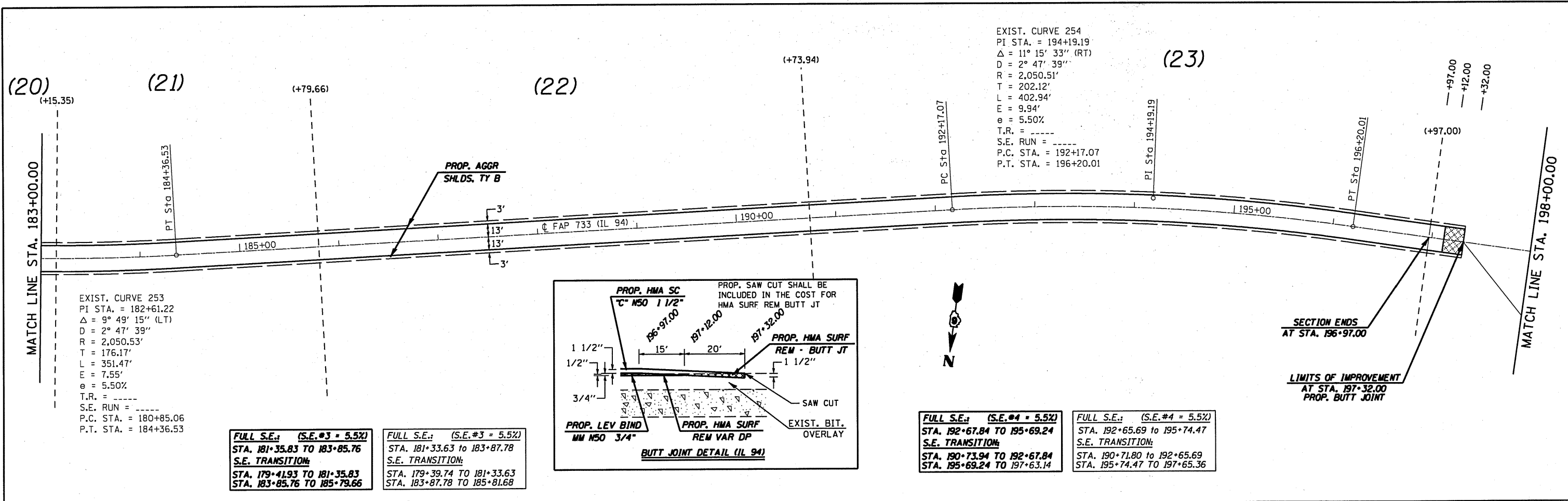


EXIST. CURVE 253
 PI STA. = 182+61.22
 $\Delta = 9^\circ 49' 15''$ (LT)
 $D = 2^\circ 47' 39''$
 $R = 2,050.53'$
 $T = 176.17'$
 $L = 351.47'$
 $E = 7.55'$
 $e = 5.50\%$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 180+85.06$
 $P.T. STA. = 184+36.53$

FULL S.E. (S.E.#3 = 5.5%)
 STA. 181+35.83 TO 183+85.76
 S.E. TRANSITION:
 STA. 179+41.93 TO 181+35.83
 STA. 183+85.76 TO 185+79.66

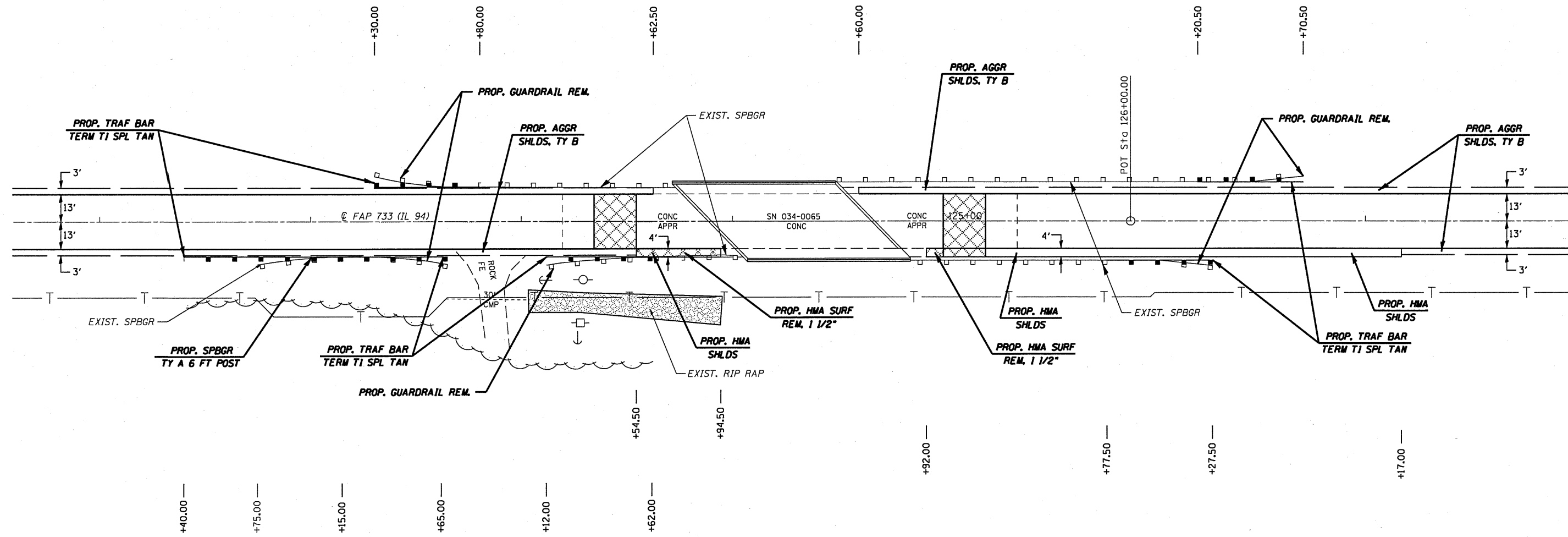
FULL S.E. (S.E.#3 = 5.5%)
 STA. 181+33.63 TO 183+87.78
 S.E. TRANSITION:
 STA. 179+39.74 TO 181+33.63
 STA. 183+87.78 TO 185+81.68

FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pwwork\pwwork\coxjw\d0215198\d672888	shp-plan08.dgn	DRAWN - JWC	REVISED -		733	IRS-3, TRS-5		48	37			
		CHECKED - RSC	REVISED -		SCALE: 50		SHEET NO. 22 OF 23 SHEETS		STA. 153+00.00 TO STA. 183+00.00		CONTRACT NO. 72880	
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

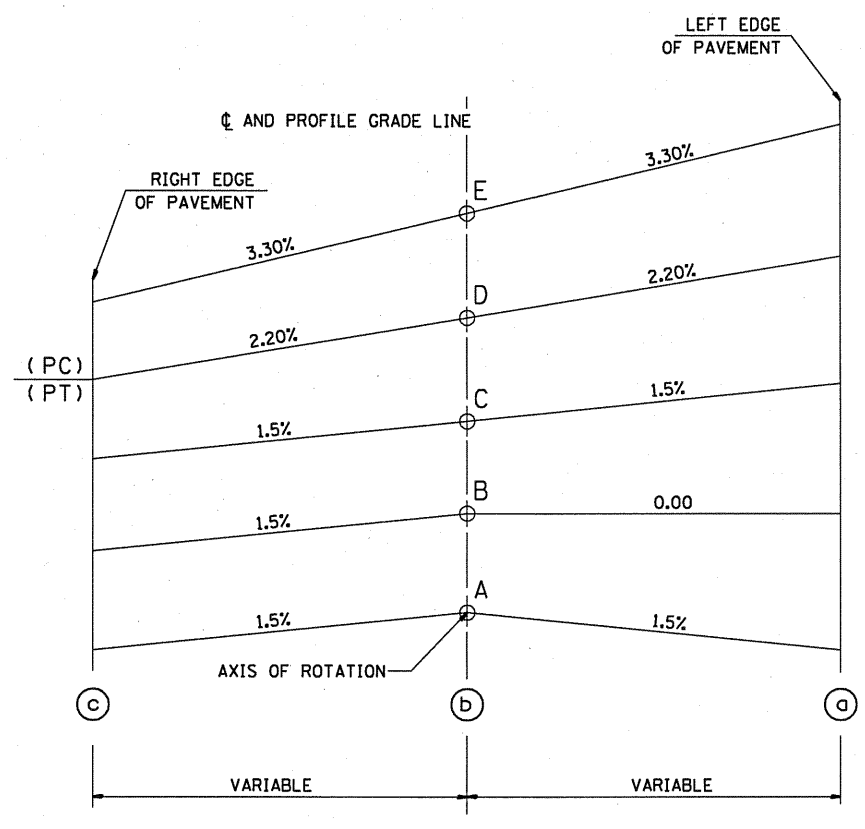
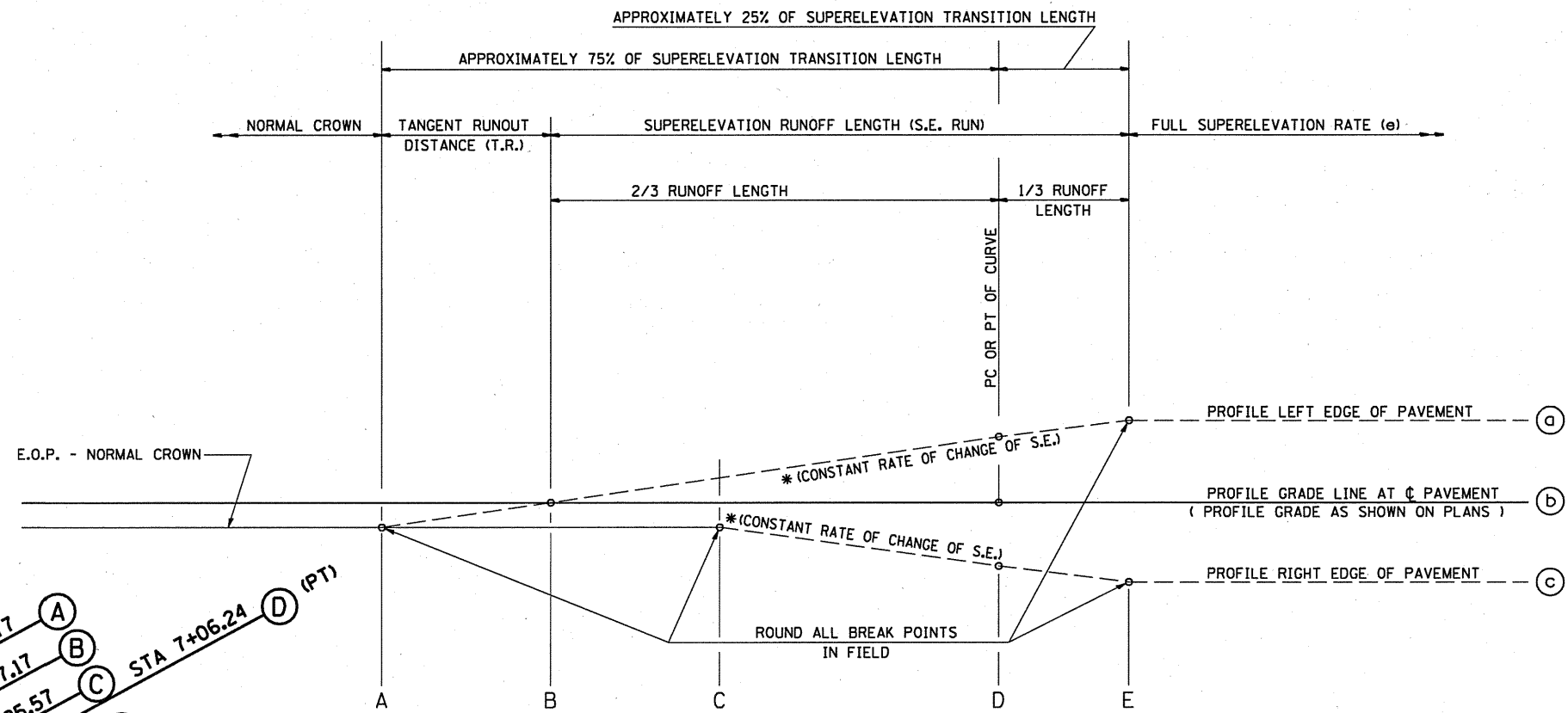


FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN VIEW		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwwork\pwwork\coxjw\d0215198\d672880-shd-plan50.dgn	DRAWN - JWC	REVISED -	REVISED -		733	IRS-3, TRS-5		48	38		
PLOT SCALE = 1/8" = 1' IN.	CHECKED - RSC	REVISED -	REVISED -		CONTRACT NO. 72880			ILLINOIS FED. AID PROJECT			
PLOT DATE = Sep-08-2010 10:42:32AM	DATE -	REVISED -	REVISED -		SCALE: 50	SHEET NO. 23 OF 23 SHEETS	STA. 183+00.00 TO STA. 199+47.12	ADAMS, HANCOCK			

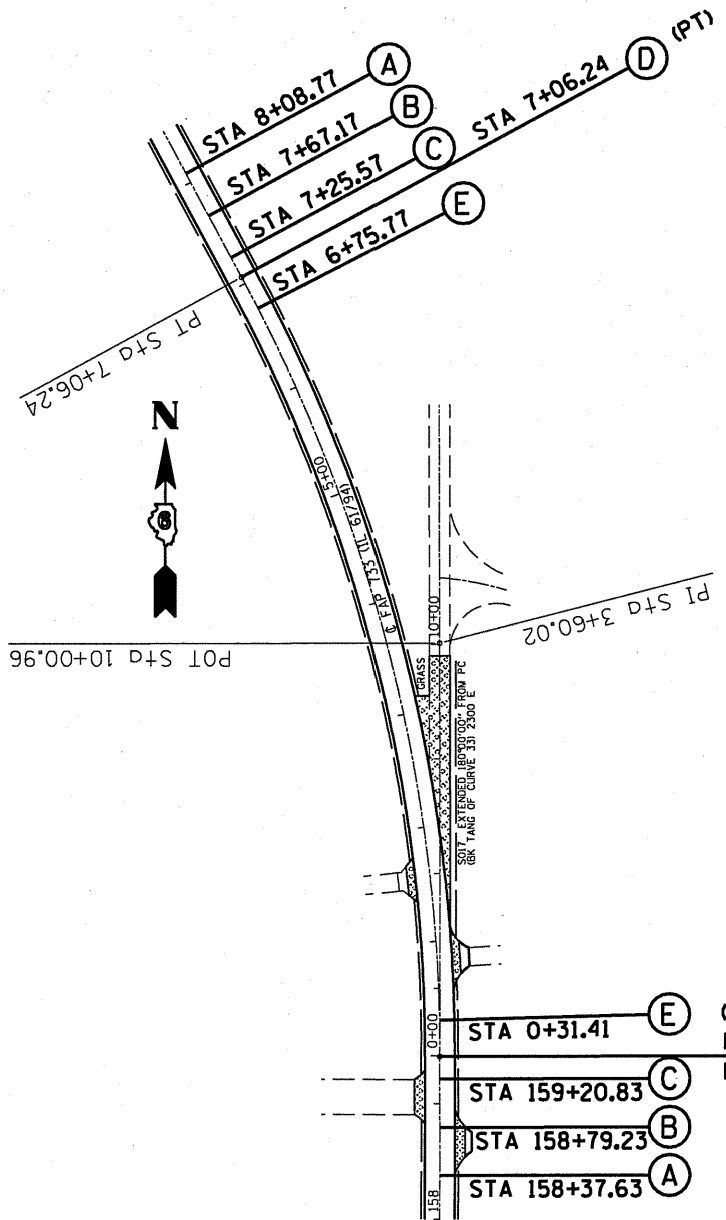
BRIDGE OMISSION #2 S.N. 034-0065
STA 123+19.50 TO 125+35.00



FILE NAME = c:\pwwork\pwwork\cox\jw\d0215198\d672880	USER NAME = coxjw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MISCELLANEOUS DETAIL				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	aht-miscdet.dgn	DRAWN - JWC	REVISED -		733	IRS-3, 7RS-5	•	48	39				
	PLOT SCALE = 100.0000' / IN.	CHECKED - RSC	REVISED -		SCALE: 50 SHEET NO. 1 OF 1 SHEETS STA. 121+50.00 TO STA. 127+00.00				CONTRACT NO. 72880				
	PLOT DATE = Sep-08-2010 10:42:46AM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT				• ADAMS, HANCOCK				



TYPICAL PROFILE - S.E. TRANSITION



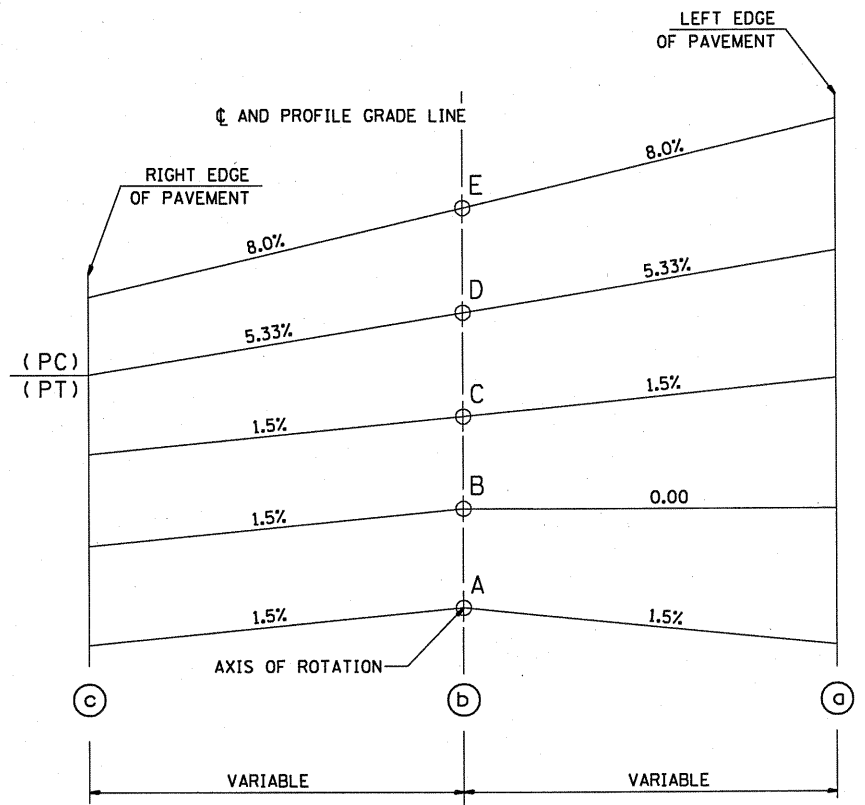
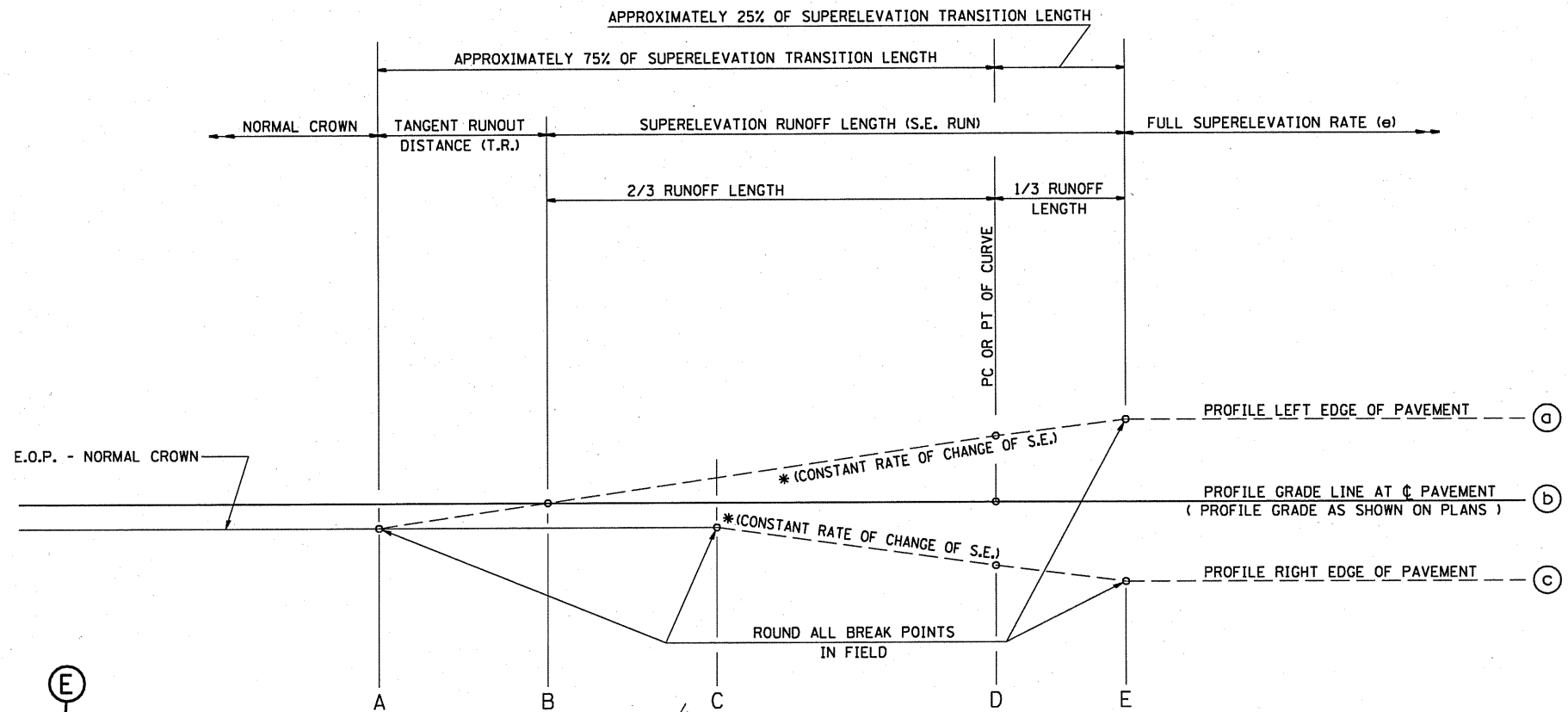
EXIST. CURVE 33
 PI STA. = 3+60.02
 $\Delta = 28^\circ 18' 00''$ (LT)
 $D = 4^\circ 00' 06''$
 $R = 1,431.75'$
 $T = 360.96'$
 $L = 707.18'$
 $E = 44.80'$
 $e = 3.3\%$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = -+00.94$
 $P.T. STA. = 7+06.24$

CURVE NO.	e	A	B	C	D	E	TRANSITION
33	3.30%	158+37.63	158+79.23	159+20.83	159+40.16	0+31.41	Trans. In
33	3.30%	8+08.77	7+67.17	7+25.57	7+06.24	6+75.77	Trans. Out

(PROP.)
FULL S.E.: (S.E.#1 = 3.3%)
 STA. 0+31.41 TO 6+75.77
S.E. TRANSITION:
 STA. 158+37.63 TO 159+40.16 (BK) =
 STA. 0+00.94 (AH) TO 0+31.41
 STA. 6+75.77 TO 8+08.77

(EXIST.)
FULL S.E.: (S.E.#1 = 3.3%)
 STA. 0+52.39 TO 6+43.23
S.E. TRANSITION:
 STA. 158+12.49 TO 159+40.16 (BK) =
 STA. 0+00.94 (AH) TO 0+52.39
 STA. 6+43.23 TO 8+33.83

(STA EON #4)
 STA 0+31.41 (E)
 PC STA 0+00.94 (AH) (D)
 STA 159+20.83 (C)
 STA 158+79.23 (B)
 STA 158+37.63 (A)



TYPICAL PROFILE - S.E. TRANSITION

CURVE NO.	e	A	B	C	D	E	TRANSITION
					(PC)		
36	8.00%	58+50.97	58+92.57	59+34.17	60+40.30	60+21.41	Trans. In
36	8.00%	72+63.39	72+21.79	71+80.19	70+50.52	69+76.66	Trans. Out
					(PT)		

STA 72+63.39 (A)
 STA 72+21.79 (B)
 STA 71+80.19 (C)
 STATION EQUATION (*6)
 PT STA 70+93.13 (AH) (D)
 PT STA 70+50.52 (BK) (E)
 STA 69+76.66 (F)

STA 68+22.00 FAP 733 (IL 94)
 STA 10+00.00 TR400 N

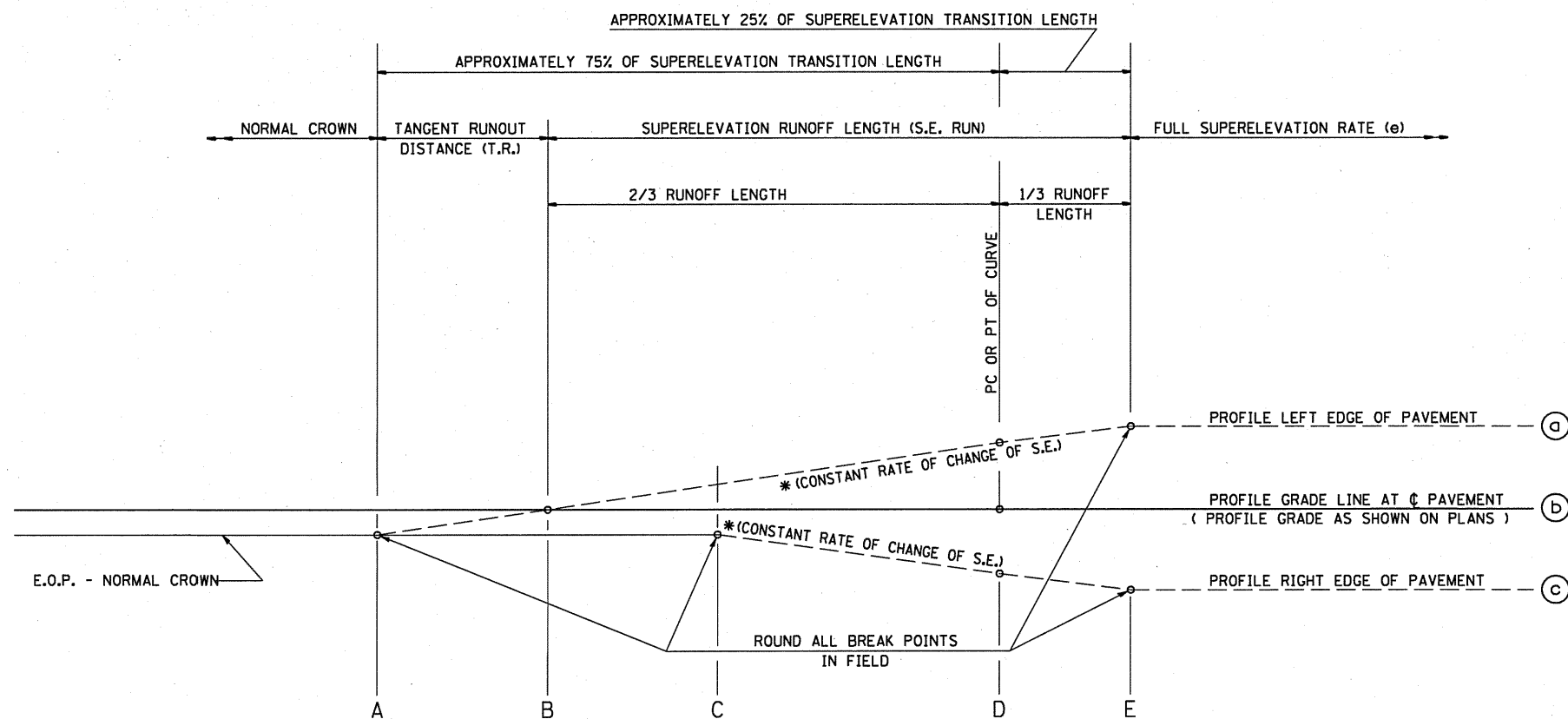
EXIST. CURVE 36
 PI STA. = 65+61.02
 $\Delta = 62^\circ 27' 35''$ (LT)
 $D = 5^\circ 39' 46''$
 $R = 1,011.79'$
 $T = 613.48'$
 $L = 1,102.98'$
 $E = 171.46'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 59+47.54
 P.T. STA. = 70+50.52



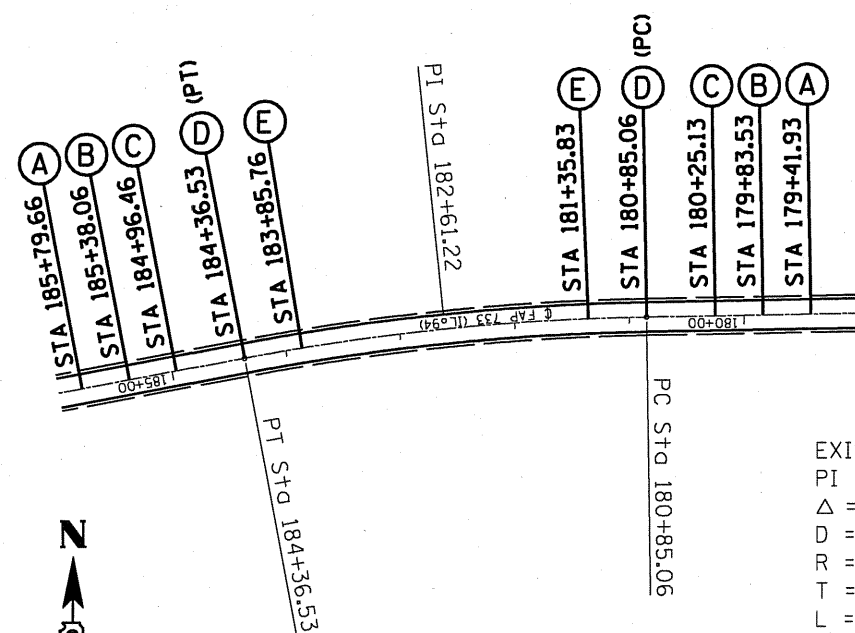
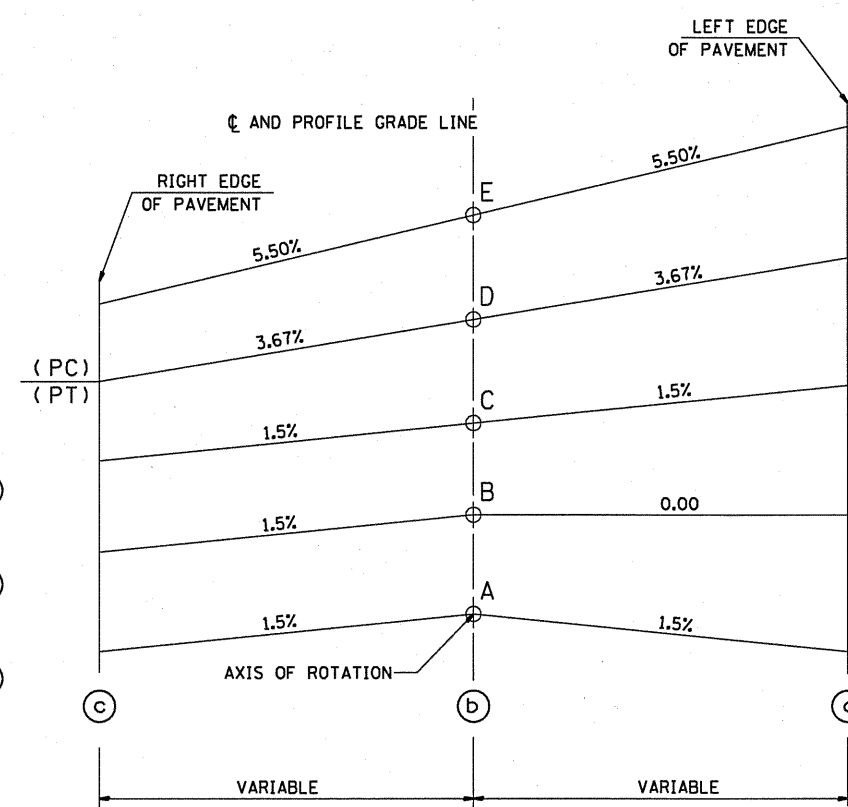
STA 60+21.41 (E)
 STATION EQUATION #5
 PC STA 59+47.54 (AH) (D)
 PC STA 60+40.30 (BK) (E)
 STA 59+34.17 (C)
 STA 58+92.57 (B)
 STA 58+50.97 (A)

(PROP.)
FULL S.E.: (S.E.#2 = 8.0%)
 STA. 60+21.41 TO 69+76.66
S.E. TRANSITION:
 STA. 58+50.97 TO 60+40.30 (BK) =
 STA. 59+47.54 (AH) TO 60+21.21
 STA. 69+76.66 TO 70+50.52 (BK) =
 STA. 70+74.06 (AH) TO 72+63.39

(EXIST.)
FULL S.E.: (S.E.#2 = 8.0%)
 STA. 60+24.20 TO 69+73.86
S.E. TRANSITION:
 STA. 58+86.97 TO 60+40.30 (BK) =
 STA. 59+47.54 (AH) TO 60+24.20
 STA. 69+73.86 TO 70+50.52 (BK) =
 STA. 70+74.06 (AH) TO 72+27.39



TYPICAL PROFILE - S.E. TRANSITION

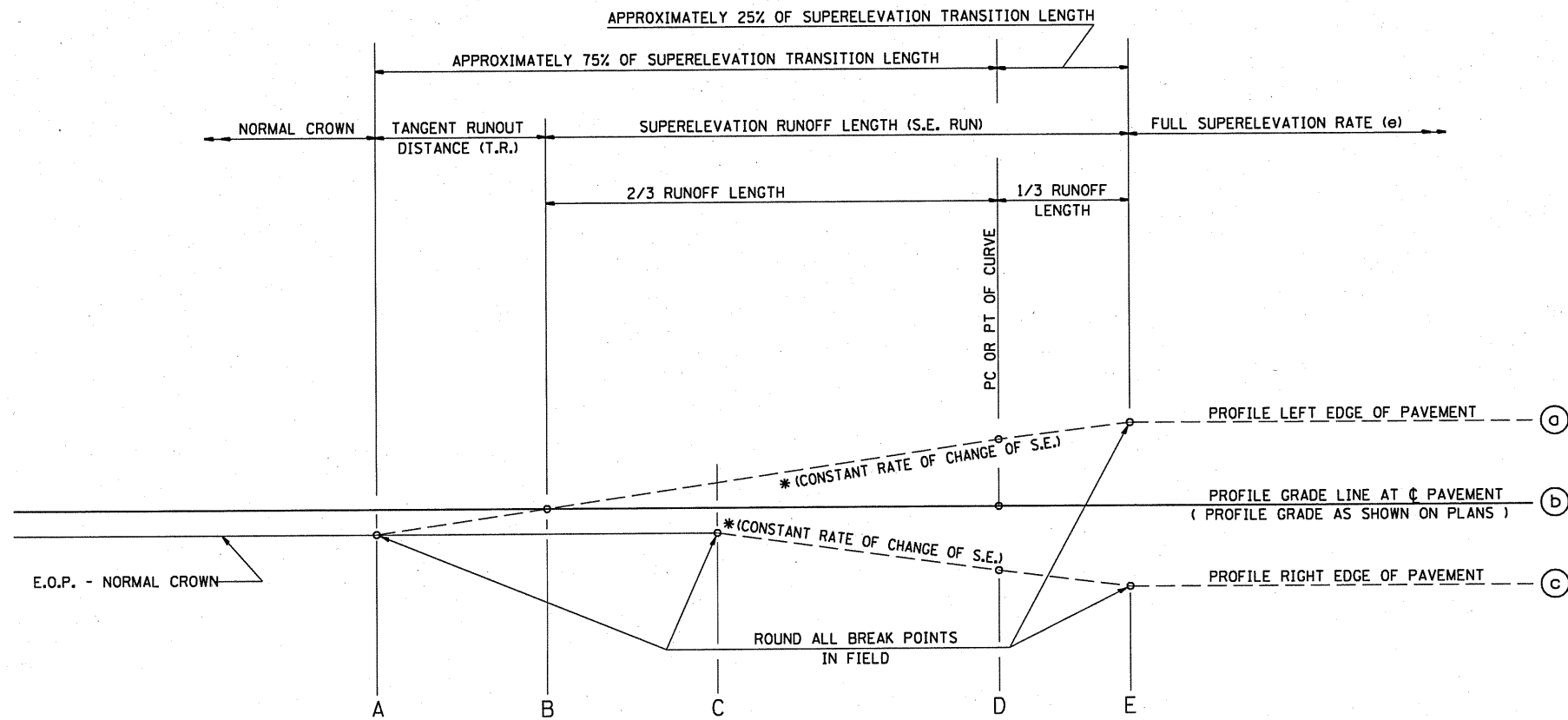


EXIST. CURVE 253
 PI STA. = 182+61.22
 $\Delta = 9^\circ 49' 15''$ (LT)
 $D = 2^\circ 47' 39''$
 $R = 2,050.53'$
 $T = 176.17'$
 $L = 351.47'$
 $E = 7.55'$
 $e = 5.50\%$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 180+85.06$
 $P.T. STA. = 184+36.53$

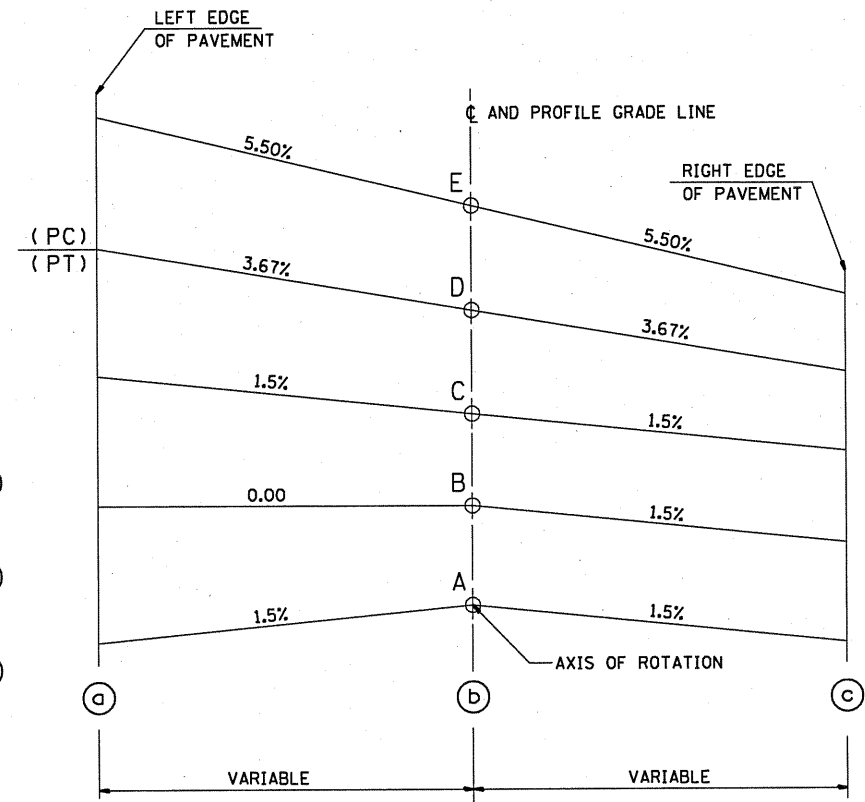
TABLE OF SUPERELEVATION BREAK POINT LOCATIONS S.E.#3							
CURVE NO.	e	A	B	C	D	E	TRANSITION
253	5.50%	179+41.93	179+83.53	180+25.13	180+85.06	181+35.83	Trans. In
253	5.50%	185+79.66	185+38.06	184+96.46	184+36.53	183+85.76	Trans. Out

(PROP.)
FULL S.E.: (S.E.#3 = 5.5%)
STA. 181+35.83 TO 183+85.76
S.E. TRANSITION:
STA. 179+41.93 TO 181+35.83
STA. 183+85.76 TO 185+79.66

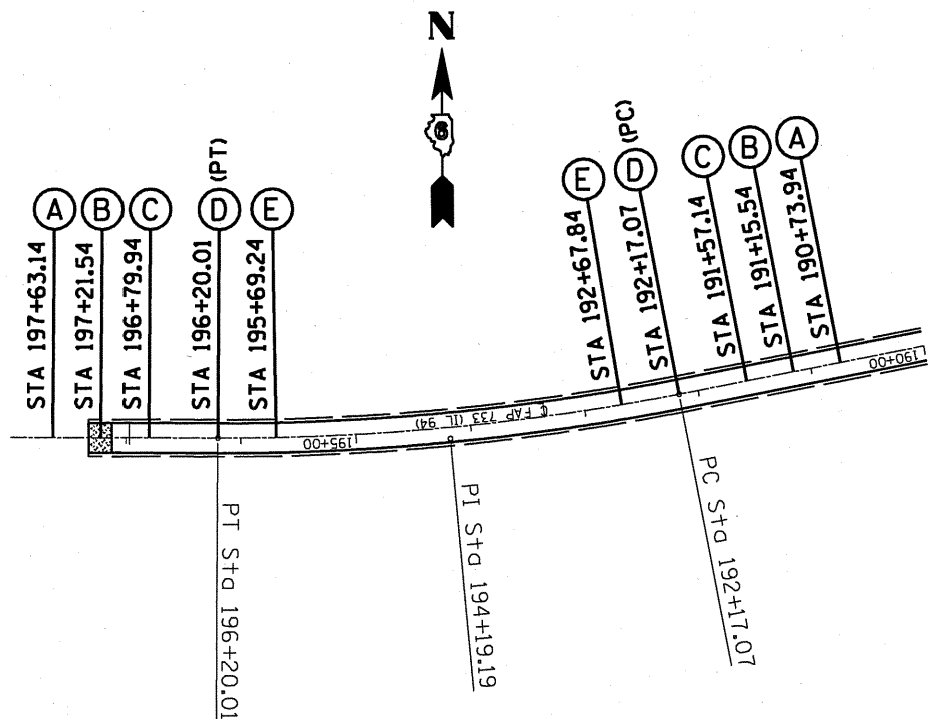
(EXIST.)
FULL S.E.: (S.E.#3 = 5.5%)
STA. 181+33.63 TO 183+87.78
S.E. TRANSITION:
STA. 179+39.74 TO 181+33.63
STA. 183+87.78 TO 185+81.68



TYPICAL PROFILE - S.E. TRANSITION



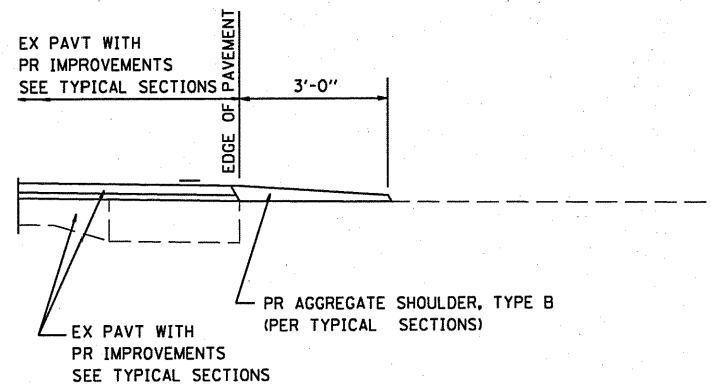
CURVE NO.	e	A	B	C	D	E	TRANSITION
					(PC)		
254	5.50%	190+73.94	191+15.54	191+57.14	192+17.07	192+67.84	Trans. In
254	5.50%	197+63.14	197+21.54	196+79.94	196+20.01	195+69.24	Trans. Out
					(PT)		



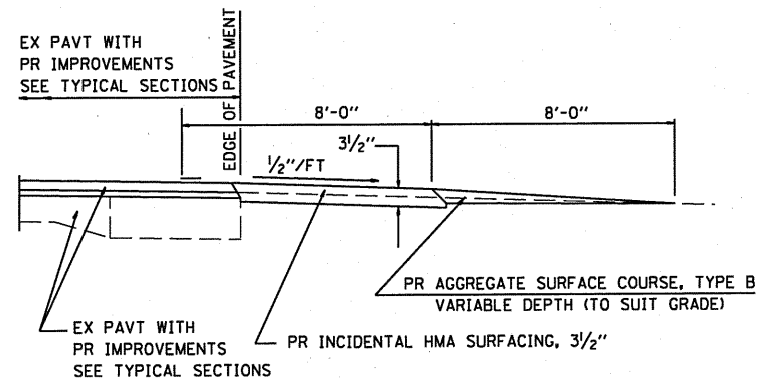
EXIST. CURVE 254
 PI STA. = 194+19.19
 $\Delta = 11^\circ 15' 33''$ (RT)
 $D = 2^\circ 47' 39''$
 $R = 2,050.51'$
 $T = 202.12'$
 $L = 402.94'$
 $E = 9.94'$
 $e = 5.50\%$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 P.C. STA. = 192+17.07
 P.T. STA. = 196+20.01

(PROP.)
FULL S.E.: (S.E.#4 = 5.5%)
STA. 192+67.84 to 195+69.24
S.E. TRANSITION:
STA. 190+73.94 to 192+67.84
STA. 195+69.24 TO 197+63.14

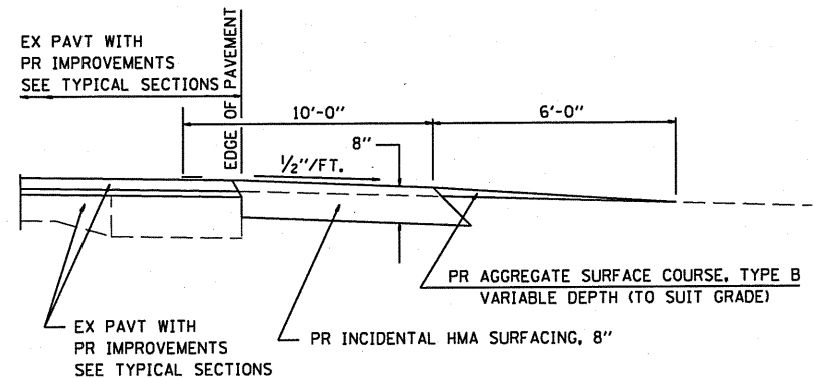
(EXIST.)
FULL S.E.: (S.E.#4 = 5.5%)
STA. 192+65.69 to 195+74.47
S.E. TRANSITION:
STA. 190+71.80 to 192+65.69
STA. 195+74.47 TO 197+65.36



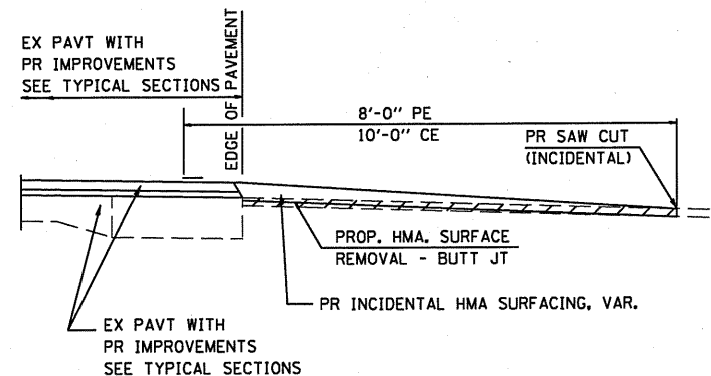
SECTION A-A FOR EX EARTH/ AGGREGATE FE



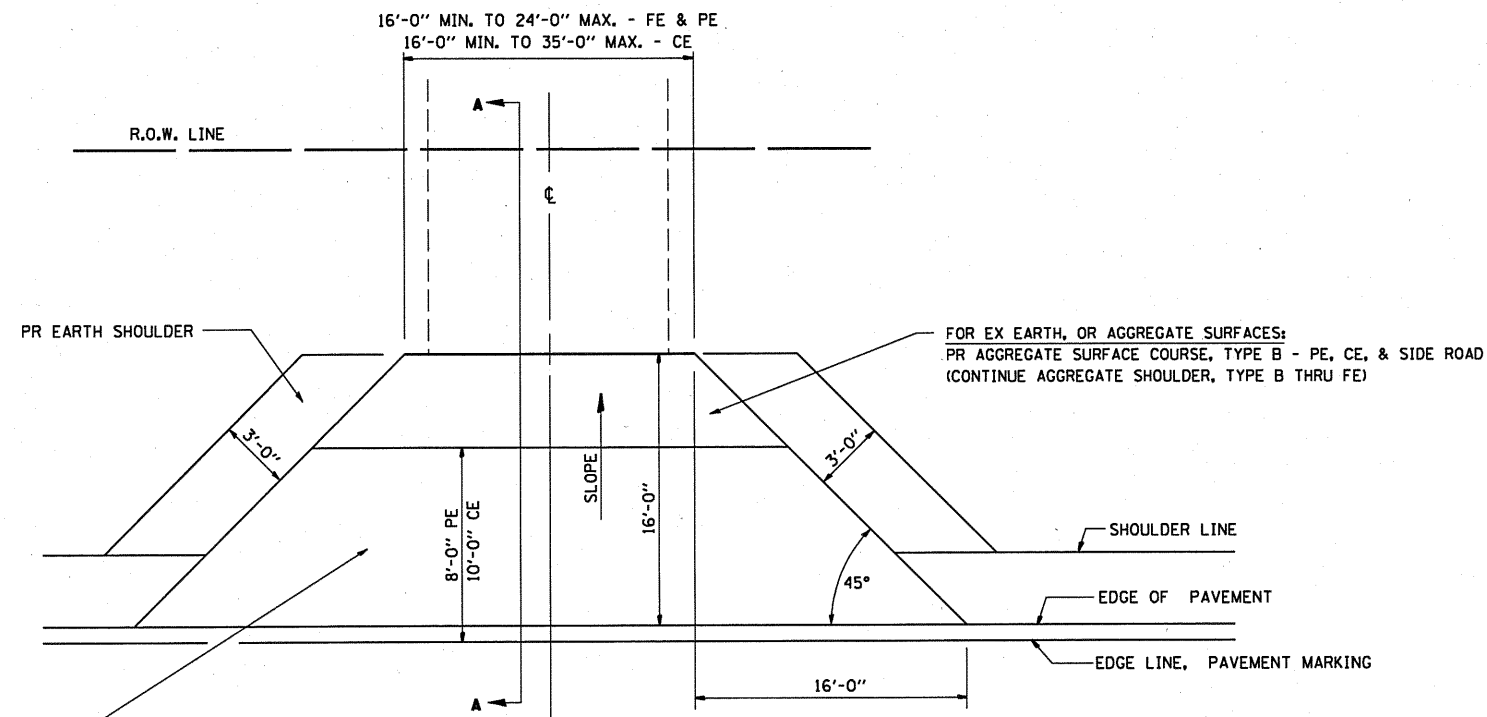
SECTION A-A FOR EX EARTH/AGGREGATE PE



SECTION A-A FOR EX EARTH/AGGREGATE CE & SIDE ROAD



SECTION A-A FOR EX BITUMINOUS/ PC CONCRETE PE, CE & SIDE ROAD



FOR EX EARTH OR AGGREGATE SURFACES:
 PR AGGREGATE SHOULDER, TYPE B THRU - FE
 PR INCIDENTAL HMA SURFACING, 3 1/2 " - PE
 PR INCIDENTAL HMA SURFACING, 8" - CE & SIDE ROAD
 FOR EXISTING HOT-MIX ASPHALT:
 PR HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JT
 PR INCIDENTAL HMA SURFACING, (SEE SHEET 46-48)
 - PE, CE, & SIDE ROAD

GENERAL NOTES:

THE RESIDENT ENGINEER WILL DETERMINE THE EXACT TYPE OF IMPROVEMENT TO BE COMPLETED FOR ALL ENTRANCES, SIDEROADS AND MAILBOX TURNOUTS ON THIS PROJECT.

THE PLAN DETAILS AND SCHEDULES SHOULD BE USED AS A GUIDE FOR THE ENGINEER TO IMPLEMENT THE FINAL DESIGN. THE ENGINEER MAY DECIDE TO SALVAGE PORTIONS OF THE EXISTING ENTRANCE PAVEMENT STRUCTURE; THEREFORE, REDUCING PAY ITEM QUANTITIES. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR THIS REDUCTION IN QUANTITIES.

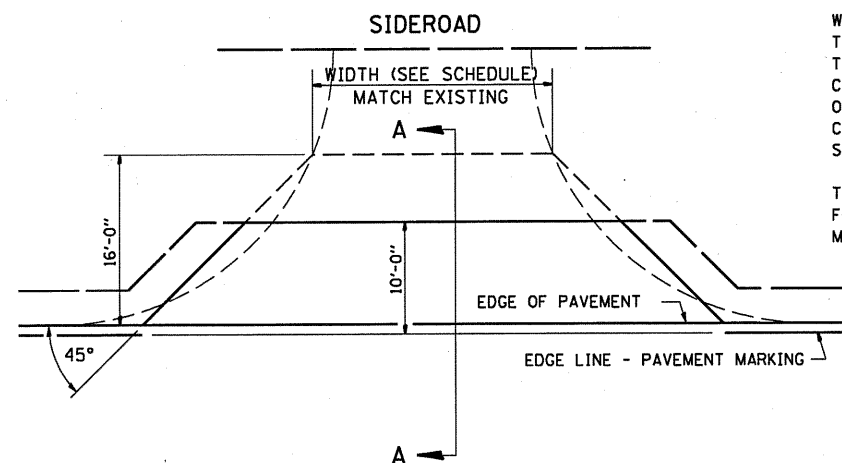
ANY WORK THE ENGINEER REQUIRES WHICH IS NOT COVERED BY A PAY ITEM CONTAINED IN THE PLANS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

HOT-MIX ASPHALT CONCRETE REQUIRED TO CONSTRUCT THE ENTRANCES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 AND 408 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.

WHEN THE HOT-MIX ASPHALT PROPOSED FOR THE IMPROVEMENT IS THICKER THAN 3 INCHES AND REQUIRE PLACEMENT IN MORE THAN ONE LIFT. THE BOTTOM LIFT(S) SHALL MEET THE REQUIREMENTS OF HOT-MIX ASPHALT BASE COURSE IN SECTION 406 OF THE STANDARD SPECIFICATIONS AND THE TOP LIFT OF 2 INCHES SHALL MEET THE REQUIREMENTS OF HOT-MIX ASPHALT SURFACE COURSE, MIXTURE C, CLASS 1, TYPE 2 OF SECTION 406 OF THE STANDARD SPECIFICATIONS.

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR "INCIDENTAL HOT-MIX ASPHALT SURFACING" WHICH SHALL INCLUDE ALL MATERIALS, EQUIPMENT, AND LABOR INVOLVED.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.



**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR RURAL ENTRANCE,
 MAILBOX TURNOUT, AND SIDE ROAD**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
733	1RS-3, TRS-5	*	48	44
			CONTRACT NO. 72880	

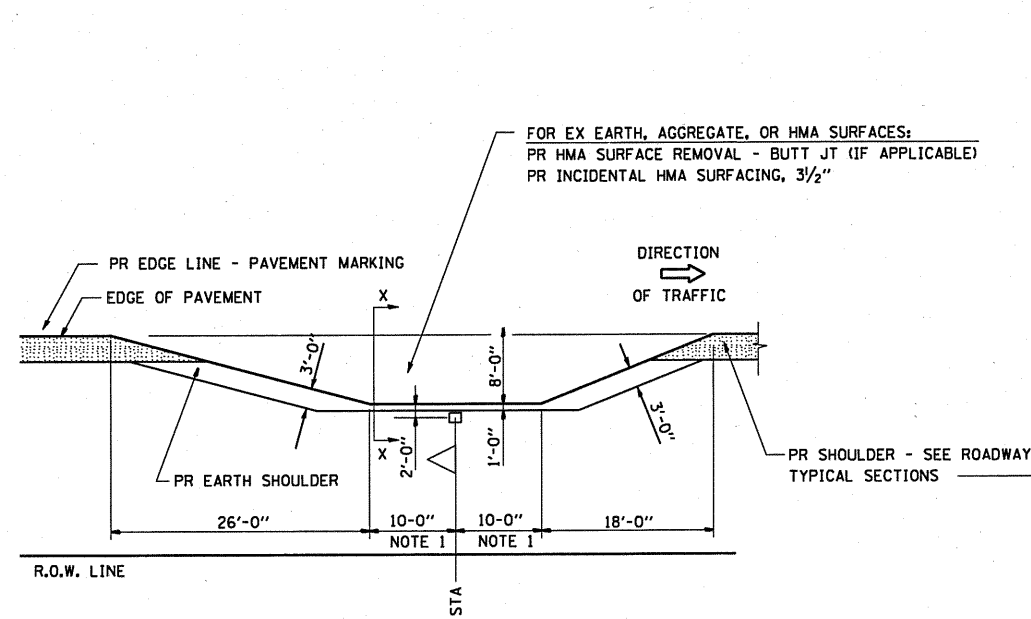
SCALE: SHEET NO. 1 OF 5 SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

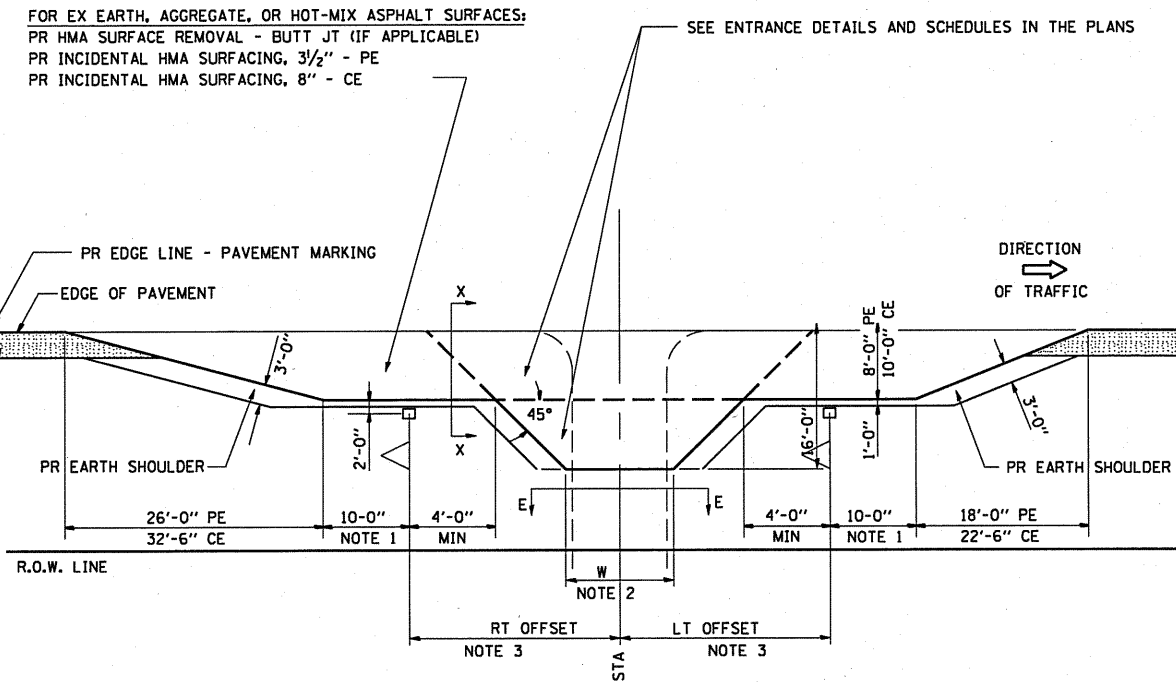
* ADAMS, HANCOCK

FILE NAME =	USER NAME = coxjw	DESIGNED -	REVISED -
ct:\pw_work\pvidot\coxjw\d0215198\d672886-Sht-Details.dgn		DRAWN - JWC	REVISED -
PLOT SCALE = 1/8" = 1' / IN.		CHECKED - RSC	REVISED -
PLOT DATE = Sep-08-2010 10:42:53AM		DATE -	REVISED -

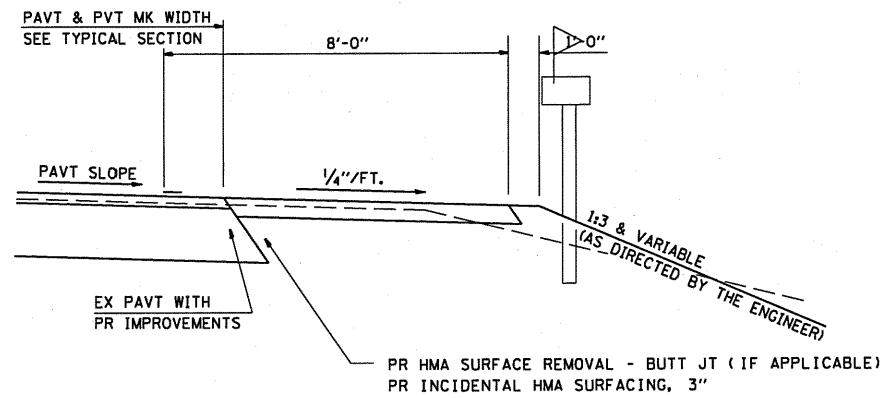
DETAILS OF MAILBOX TURNOUTS



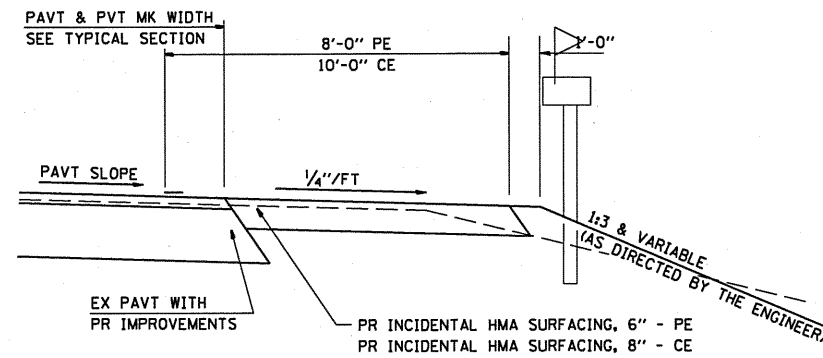
PLAN - MAILBOX TURNOUTS



PLAN - COMBINED MAILBOX TURNOUT WITH TRAILING OR LEADING ENTRANCE



**SECTION X-X THRU MAILBOX TURNOUT
 ALSO APPLIES TO MAILBOX TURNOUTS COMBINED WITH
 EX EARTH, AGGREGATE, OR HOT-MIX ASPHALT PE & FE**



**SECTION X-X THRU MAILBOX TURNOUT
 COMBINED WITH EX HMA & PC CONC PE & CE**

- NOTE 1 IF MORE THAN ONE MAILBOX IS PRESENT, DIMENSION FROM CENTER OF END MAILBOX.
- NOTE 2 FOR ENTRANCE LAYOUT DIMENSIONS AND SECTIONS A-A & E-E REFER TO THE SCHEDULES IN THE PLANS.
- NOTE 3 BOTH LT OR RT OFFSETS FOR MAILBOX SHOWN USE OFFSET DIMENSION PER SCHEDULE AND REFER TO LAYOUT SHOWN ON THE PLAN.

ALL DIMENSIONS ARE IN INCHES
 UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = cox_jw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR RURAL ENTRANCE, MAILBOX TURNOUT, AND SIDE ROAD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pwwork\cox_jw\d0215198\d67288	Sht-Details.dgn	DRAWN - JWC	REVISED -			733	IRS-3, TRS-5	*	48	45	
	PLOT SCALE = 1/8" = 1' IN.	CHECKED - RSC	REVISED -			CONTRACT NO. 72880					
	PLOT DATE = Sep-08-2010 10:42:54AM	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

INCIDENTAL HOT-MIX ASPHALT SURFACING

NO.	DESCRIPTION	STATION	EXIST. ENTR.	FROM E.O.P. (FOOT)	WIDTH (FOOT)	THICKNESS (INCH)	HMA / (EX. AGGR.)		HMA				TOTAL (TON)	Aggr. Shldr AREA (SQ FT)	AGG SURF CSE (TON)		
							ENTR & SIDE RD.		ENTR.		SIDE RD.						
							AREA (SQ FT)	TOTAL (TON)	AREA (SQ FT)	TOTAL (TON)	AREA (SQ FT)	TOTAL (TON)					
[F.A.P. 773 (IL 61/94)]																	
IL 61/94 JCT TO 2.0 MI S. OF BOWEN																	
1	LIMITS OF IMPROVEMENT	1233+29.50															
1	SECTION BEGINS	1233+64.50															
1	HMA FE (RT) [bit]	1234+88.50 RT	BIT	3	13												
	STA EQN #1	1242+13.77 (BK) 1237+84.46 (AH)															
4	HMA PE (RT)	1251+50.00 RT	BIT	7	20	1.875			189.00	2.21			2.21	93.00	1.21		
4	HMA PE (w/ MBT) (RT)	1252+16.00 RT	L BIT	7	34	1.875			423.50	4.94			4.94	202.93	1.97		
	BRIDGE OMISSION #1:	C.L. OF BRIDGE															
	S.N. 001-0002	1260+34.25															
5	TR 63 (N. 2700TH AVE.) RT [O&C]	1280+42.67 RT	BIT	19	22	1.875					779.00	9.09	9.09	171.00			
5	TR 63 (N. 2700TH AVE.) LT	1280+42.67 LT	BIT	16	22	1.875					608.00	7.09	7.09	153.00	1.67		
5	TR 51 (N. 2750TH AVE.) LT	1307+41.14 LT	BIT	18	22	1.875					720.00	8.40	8.40	165.00	1.25		
5	HMA CE (LT)	1325+60.50 LT	BIT	9	24	1.875			297.00	3.47			3.47	117.00	1.03		
5	TR 33 (N. 2800TH AVE.) RT	1334+12.27 RT	AGG	9	28	8	333.00	16.58					16.58	129.00	1.25		
5	TR 33 (N. 2800TH AVE.) LT	1334+12.27 LT	AGG	9	33	8	378.00	18.82					18.82	144.00	1.54		
5	HMA PE (LT)	1347+39.00 LT	BIT	7	20	1.875			189.00	2.21			2.21	93.00	1.21		
5	HMA PE (RT)	1352+50.00 RT	BIT	7	21	1.875			196.00	2.29			2.29	96.00	1.21		
5	(N. 2900TH AVE.) RT	1387+41.00 RT	BIT	9	29	1.875					342.00	3.99	3.99	132.00	1.31		
5	(N. 2900TH AVE.) LT [O&C]	1387+41.00 LT	BIT	9	26	1.875					315.00	3.68	3.68	123.00			
5	HMA CE (LT)	1431+16.50 LT	AGG	9	25	8	306.00	15.23					15.23	120.00			
5	(N. 3000TH AVE.) RT	1441+89.75 RT	AGG	12	31	8	516.00	25.69					25.69	156.00	1.42		
5	(N. 3000TH AVE.) LT [O&C]	1441+89.75 LT	BIT	13	34	1.875					611.00	7.13	7.13	171.00			
	STA EQN #2	1449+49.65 (BK) 7+53.50 (AH)															
7	HMA CE (RT)	32+24.50 RT	BIT	9	22	1.875			279.00	3.26			3.26	111.00	0.91		
7	HMA CE (RT)	74+90.00 RT	BIT	9	26	1.875			367.50	4.29			4.29	178.93	1.37		
7	HMA CE (RT)	77+50.00 RT	BIT	7	24	1.875			353.50	4.12			4.12	172.93	1.21		
7	HMA CE (RT)	79+12.00 RT	BIT	7	32	1.875			409.50	4.78			4.78	196.93	1.82		
7	2300 E (150 N) RT [O&C]	80+29.74 RT	BIT	12	31	1.875					516.00	6.02	6.02	156.00			
7	2300 E (150 N) LT [O&C]	80+29.74 LT	BIT	16	24	1.875					640.00	7.47	7.47	159.00			
7	HMA PE (LT)	105+50.50 LT	BIT	7	24	1.875			217.00	2.53			2.53	105.00	1.21		
7	2300 E (200 N) LT	107+04.03 LT	BIT	9	26	1.875					315.00	3.68	3.68	123.00	1.25		
7	CHURCH ST. RT [O&C]	124+79.30 RT	BIT	16	29	1.875					720.00	8.40	8.40	174.00			
7	CHURCH ST. LT [O&C]	124+96.12 LT	BIT	12	18	1.875					360.00	4.20	4.20	117.00			
8	PCC CE (RT)	126+11.00 RT	PCC	17	28									177.00			
8	PCC CE (RT)	126+93.00 RT	PCC	17	14									135.00			
8	PCC CE (RT)	127+56.50 RT	PCC	17	21									156.00			
8	PCC CE (LT)	127+91.50 LT	PCC	17	12									129.00			
8	PCC CE (RT)	127+93.50 RT	PCC	17	23									162.00			
8	HMA MBT (no entr) (RT)	128+08.50 RT	BIT	7	10	1.5			294.00	2.74			2.74	63.00			
8	PCC CE (RT)	128+64.00 RT	PCC	17	22									159.00			
Subtotal (pg 1) =									1,533.00	76.31	3,215.00	36.82	5,926.00	69.14	182.27	4,540.72	22.87

INCIDENTAL HOT-MIX ASPHALT SURFACING

	DESCRIPTION	STATION	EXIST. ENTR.	FROM E. O. P. (FOOT)	WIDTH (FOOT)	THICKNESS (INCH)	HMA / (EX. AGGR.)		HMA				TOTAL (TON)	Aggr. Shldr AREA (SQ FT)	AGG SURF CSE (TON)					
							ENTR & SIDE RD.		ENTR.		SIDE RD.									
							AREA (SQ FT)	TOTAL (TON)	AREA (SQ FT)	TOTAL (TON)	AREA (SQ FT)	TOTAL (TON)								
[F. A. P. 773 (IL 61/94)]																				
BOWEN TO IL 336/94 JCT																				
8	DAVIS ST. RT [pcc sw]	129+00.00 RT	BIT	9	26	1.5							315.00	2.94	2.94	123.00				
8	DAVIS ST. LT [pcc sw]	129+00.00 LT	BIT	9	21	1.5							270.00	2.52	2.52	108.00				
	STA EQN #3	130+62.24 (BK) 134+50.00 (AH)																		
9	PCC CE (LT)	135+40.00 LT	PCC	16	51											240.00				
9	HMA CE (RT)	136+09.00 RT	BIT	5	20	1.5			125.00	1.17					1.17	81.00				
9	PCC CE (LT)	136+09.00 LT	PCC	20	16											159.00				
9	PCC CE (RT)	136+35.00 RT	PCC	13	12											105.00				
9	PCC CE (RT)	136+55.00 RT	PCC	13	10											99.00				
9	PCC CE (LT)	136+72.00 LT	PCC	20	35											216.00				
9	(IL 61) RT [bit]	137+62.00 RT	BIT	21	55	1.5							1,596.00	14.90	14.90	282.00				
9	(5TH ST) LT [pcc sw]	137+62.00 LT	BIT	16	43	1.5							944.00	8.81	8.81	216.00				
9	PCC CE (RT)	138+83.00 RT	PCC	15	14											123.00				
9	PCC CE (LT)	139+32.00 LT	PCC	13	23											138.00				
10	(4TH ST.) RT [O&C]	141+30.80 RT	BIT	9	29	1.875							342.00	3.99	3.99	132.00				
10	(4TH ST.) LT [O&C]	141+30.80 LT	BIT	9	22	1.875							279.00	3.26	3.26	111.00				
10	HMA PE (LT)	143+23.50 LT	BIT	7	16	1.875			161.00	1.88					1.88	81.00	1.21			
10	(3RD ST.) RT [O&C]	144+91.73 RT	BIT	15	19	1.875							510.00	5.95	5.95	138.00				
10	(3RD ST.) LT [O&C]	144+91.73 LT	BIT	12	21	1.875							396.00	4.62	4.62	126.00				
10	HMA MBT (no entr) (RT)	145+50.00 RT	AGG	7	20	3.5	294.00	6.40							6.40	163.71				
10	HMA PE (LT) [pcc sw]	146+25.00 LT	BIT	10	11	1.875			210.00	2.45					2.45	84.00				
10	HMA PE (LT) [pcc sw]	146+76.00 LT	BIT	9.5	11	1.875			194.75	2.27					2.27	81.00				
10	HMA PE (w/ MBT) (RT)	147+47.50 RT	T BIT	7	29	1.875			416.50	4.86					4.86	206.79	1.59			
10	(2ND ST) RT [pcc sw]	148+51.52 RT	BIT	11	27	1.875							418.00	4.88	4.88	138.00				
10	(2ND ST) LT [O&C]	148+51.52 LT	BIT	9	19	1.875							252.00	2.94	2.94	102.00				
10	HMA PE (LT) [pcc sw]	149+38.50 LT	AGG	9	14	3.5	207.00	4.51							4.51	87.00				
10	HMA MBT (no entr) (RT)	149+67.50 LT	BIT	7	20	1.875			294.00	3.43					3.43	163.71				
10	HMA PE (LT) [pcc sw]	149+96.50 LT	AGG	9	12	3.5	189.00	4.12							4.12	81.00				
10	HMA PE (LT) [pcc sw]	150+81.00 LT	AGG	9	15	3.5	216.00	4.70							4.70	90.00				
10	HMA PE (RT)	150+90.50 RT	BIT	12	13	1.875			300.00	3.50					3.50	102.00	0.97			
10	HMA PE (LT) [pcc sw]	151+79.50 LT	AGG	9	13	3.5	198.00	4.31							4.31	84.00				
10	HMA PE (LT)	152+09.00 LT	BIT	9	15	1.875			216.00	2.52					2.52	90.00	0.85			
10	(1ST ST) RT	152+09.80 RT	BIT	12	27	1.875							468.00	5.46	5.46	144.00	1.20			
10	HMA CE (LT) (School Ent.)	154+60.00 LT	BIT	9	48	1.875			513.00	5.99					5.99	189.00	2.39			
10	HMA CE (RT) Church Ent.	154+90.50 RT	BIT	9	32	1.875			369.00	4.31					4.31	141.00	1.48			
10	HMA CE (LT) (School Ent.)	157+60.50 LT	BIT	9	34	1.875			387.00	4.52					4.52	147.00	1.59			
11	HMA CE (RT) Church Ent.	158+86.90 RT	BIT	9	26	1.875			315.00	3.68					3.68	123.00	1.14			
11	HMA CE (LT)	159+05.50 LT	BIT	9	30	1.875			351.00	4.10					4.10	135.00				
	STA EQN #4	159+40.16 (BK) 0+00.94 (AH)																		
12	TR 389 (2300E) RT	0+00.94 RT	BIT	2,894.97		1.875			SIDE ROAD AREA				2,894.97	33.77	33.77	17,360.82				
Subtotal (pg 2) =									1,104.00	24.04			3,852.25	44.65		8,684.97	94.03	162.73	22,191.03	12.43

INCIDENTAL HOT-MIX ASPHALT SURFACING

NO.	DESCRIPTION	STATION	MATERIAL	FROM E. O. P. (FOOT)	WIDTH (FOOT)	THICKNESS (INCH)	HMA / (EX. AGGR.)		HMA				TOTAL (TON)	Aggr. Shldr AREA (SQ FT)	AGG SURF CSE (TON)
							ENTR & SIDE RD.		ENTR.		SIDE RD.				
							AREA (SQ FT)	TOTAL (TON)	AREA (SQ FT)	TOTAL (TON)	AREA (SQ FT)	TOTAL (TON)			
[F. A. P. 773 (IL 61/94)]															
BOWEN TO IL 336/94 JCT															
12	TR 389 (2300E) RT	0+00.94 RT	BIT		391.27	2.5	HMA SHOULDER AREA		391.27	6.09		6.09	2,338.62		
12	HMA PE (RT)	0+85.00 RT	BIT	7	28	1.875		245.00	2.86			2.86	117.00	1.52	
12	HMA PE (LT)	1+55.00 LT	AGG	7	26	3.5	231.00	5.03				5.03	111.00		
13	PCC (AGG) CE (RT)	9+05.00 RT	PCC	28	34		PCC (NO WORK)						261.00		
14	(350 N) RT	35+51.19 RT	BIT	17	28	1.875				765.00	8.93	8.93	177.00	1.68	
14	(350 N) LT	35+51.19 LT	BIT	17	42	1.875				1,003.00	11.70	11.70	219.00	2.08	
	STA EON #5	60+40.30 (BK)													
		59+47.54 (AH)													
16	(TR 400 N) RT	68+22.00 RT	BIT	(Measured Using CADD)		8	840.98	41.86				41.86	309.86	2.94	
	STA EON #6	70+50.52 (BK)													
		70+93.13 (AH)													
18	TR 378 (2200E) RT	89+10.10 RT	BIT	9	37	1.875				414.00	4.83	4.83	156.00	1.77	
18	TR 378 (2300E) LT	89+10.10 LT	BIT	9	45	1.875				486.00	5.67	5.67	180.00	2.22	
18	HMA PE (LT)	92+50.00 LT	BIT	7	32	1.875		273.00	3.19			3.19	129.00	1.82	
	BRIDGE OMISSION #2:	C.L. OF BRIDGE													
	S. N. 034-0065	124+27.25													
19	(2100E) RT	142+30.55 RT	BIT	9	26	1.875				315.00	3.68	3.68	123.00	1.14	
19	(2100E) LT	142+30.55 LT	BIT	9	40	1.875				441.00	5.15	5.15	165.00	1.94	
23	SECTION ENDS	197+97.00													
23	LIMITS OF IMPROVEMENT	198+32.00													
Subtotal (pg 3) =							1,071.98	46.89	518.00	6.04	3,815.27	46.03	98.97	4,286.48	17.10
Project Total =							3,708.98	147.24	7,585.25	87.52	18,426.24	209.20	443.96	31,018.23	52.41
							412.11		842.81		2,047.36			3,446.47	
											26,011 SF		443.96 Tons		
Total =							412.11 SY (AGG)				2,890.17 SY (HMA)				

TOTAL = 444 TONS