



EXISTING BENCHMARKS			
DESCRIPTION	NORTHING	EASTING	ELEV.
"KEWPORT" NGS MONUMENT	1,652,431.74	2,352,728.14	854.2
"KEWPORT AZ MK" NGS MONUMENT	1,652,491.68	2,349,792.88	850.6

RUNWAY END COORDINATES				
DESCRIPTION	LATITUDE	LONGITUDE	RUNWAY STATION	RUNWAY ELEVATION
RUNWAY 9 END	41°12'14.7602" N	89°58'25.7181" W	218+00	853.7
RUNWAY 27 END	41°12'13.9076" N	89°57'26.8606" W	263+00	856.3
RUNWAY 1 END	41°12'09.3902" N	89°57'45.1290" W	55+00	855.2
RUNWAY 19 END	41°12'40.3903" N	89°57'36.8856" W	87+00	837.5

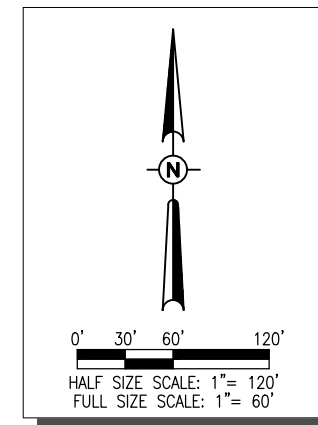
NOTES

1. VERTICAL COORDINATES ARE IN NGVD 29. HORIZONTAL COORDINATES ARE IN STATE PLANE NAD83 ILLINOIS WEST.
2. STATIONS, OFFSETS AND ELEVATIONS SHOWN ARE IN FEET.
3. THE APPROACH END OF RUNWAY 1 IS STATION 55+00.
4. THE AIRPORT REFERENCE CODE FOR RUNWAY 9-27 IS B-II. RUNWAY 9 HAS A NON-PRECISION APPROACH WITH VISIBILITY MINIMUM OF 1 MILE WHILE RUNWAY 27 HAS A VISUAL APPROACH.
5. THE AIRPORT REFERENCE CODE FOR RUNWAY 1-19 IS B-I. RUNWAY 1 HAS A NON-PRECISION APPROACH WITH VISIBILITY MINIMUM OF 1 MILE WHILE RUNWAY 19 HAS A VISUAL APPROACH.

CRITICAL POINTS						
POINT NO.	DESCRIPTION	LATITUDE	LONGITUDE	GND ELEV. (MSL)	MAX EQUIP. HEIGHT (FT)	TOP ELEV. GND + EQUIP HEIGHT (MSL)
1	CONSTRUCTION LIMITS	N041° 12' 07.32"	W089° 57' 39.02"	851	25	876
2	CONSTRUCTION LIMITS	N041° 12' 10.95"	W089° 57' 39.01"	852	25	877
3	CONSTRUCTION LIMITS	N041° 12' 10.94"	W089° 57' 35.43"	855	25	880
4	CONSTRUCTION LIMITS	N041° 12' 09.96"	W089° 57' 29.37"	851	25	876
6	BARRICADES	N041° 12' 10.29"	W089° 57' 42.84"	854	3	857
7	BARRICADES	N041° 12' 12.08"	W089° 57' 37.38"	854	3	857
8	CONSTRUCTION STAGING AREA	N041° 12' 03.04"	W089° 57' 32.14"	848	25	873

NOTE:

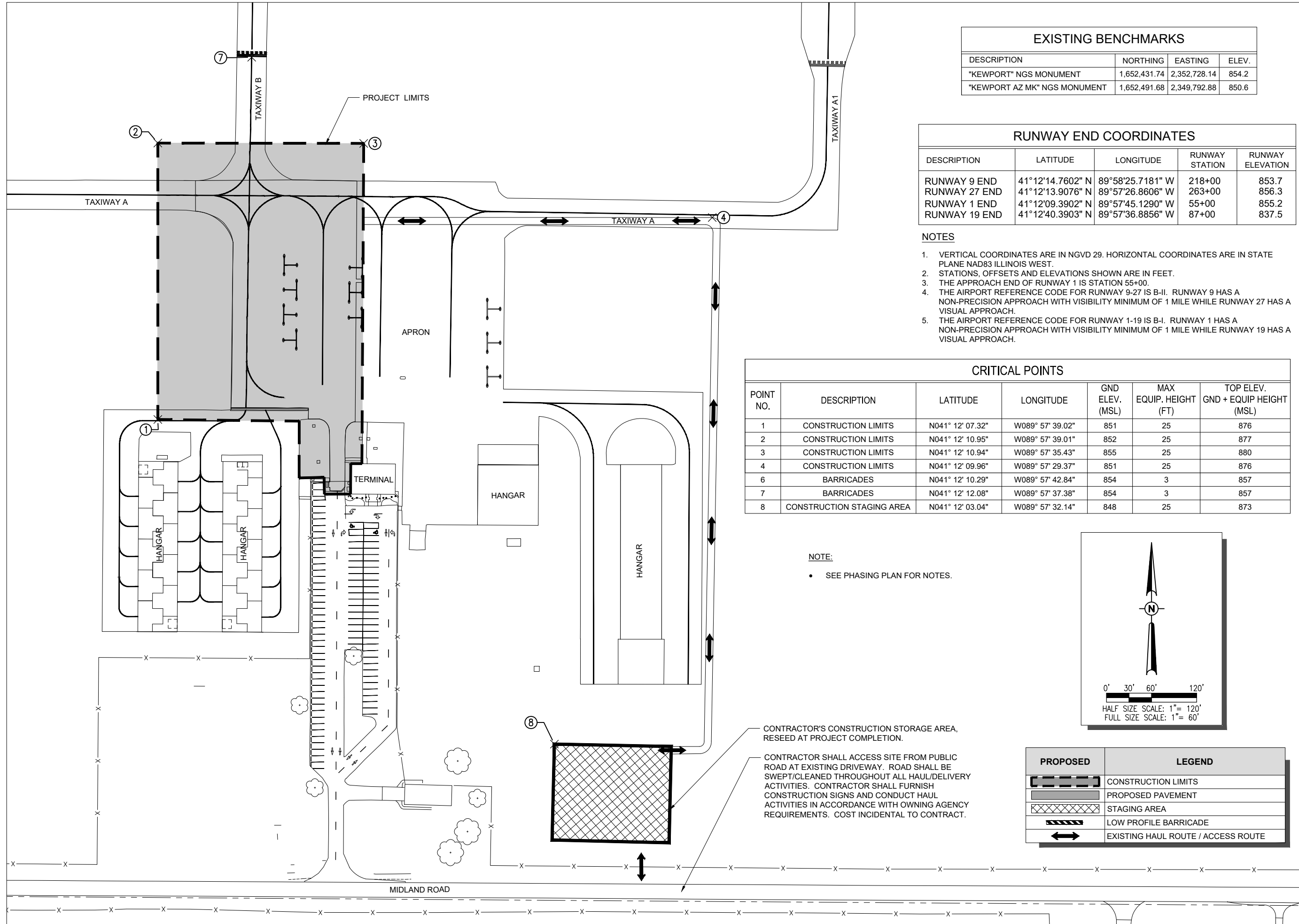
- SEE PHASING PLAN FOR NOTES.



PROPOSED	LEGEND
	CONSTRUCTION LIMITS
	PROPOSED PAVEMENT
	STAGING AREA
	LOW PROFILE BARRICADE
	EXISTING HAUL ROUTE / ACCESS ROUTE

CONTRACTOR'S CONSTRUCTION STORAGE AREA, RESEED AT PROJECT COMPLETION.

CONTRACTOR SHALL ACCESS SITE FROM PUBLIC ROAD AT EXISTING DRIVEWAY. ROAD SHALL BE SWEEP/CLEANED THROUGHOUT ALL HAUL/DELIVERY ACTIVITIES. CONTRACTOR SHALL FURNISH CONSTRUCTION SIGNS AND CONDUCT HAUL ACTIVITIES IN ACCORDANCE WITH OWNING AGENCY REQUIREMENTS. COST INCIDENTAL TO CONTRACT.



RECONSTRUCT WEST PORTION OF RAMP

IDA No: EZI-4883

SBGP No:
3-17-SBGP-162/171

Contract No.: KE018

NO.	DATE	DESCRIPTION		
		LAY	DWN	REV

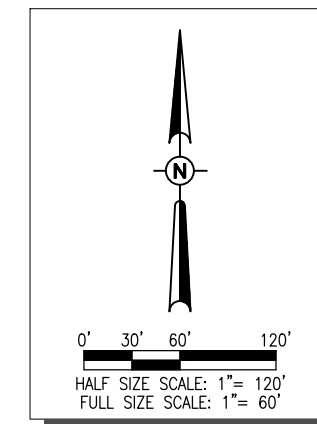
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PROJECT NO: 21A0008
CAD FILE: C-101-SOW.DWG
LAYOUT BY: LDH 08/24/2021
DRAWN BY: HLE 08/24/2021
REVIEWED BY: LDH 11/17/21
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SITE AND SAFETY PLAN

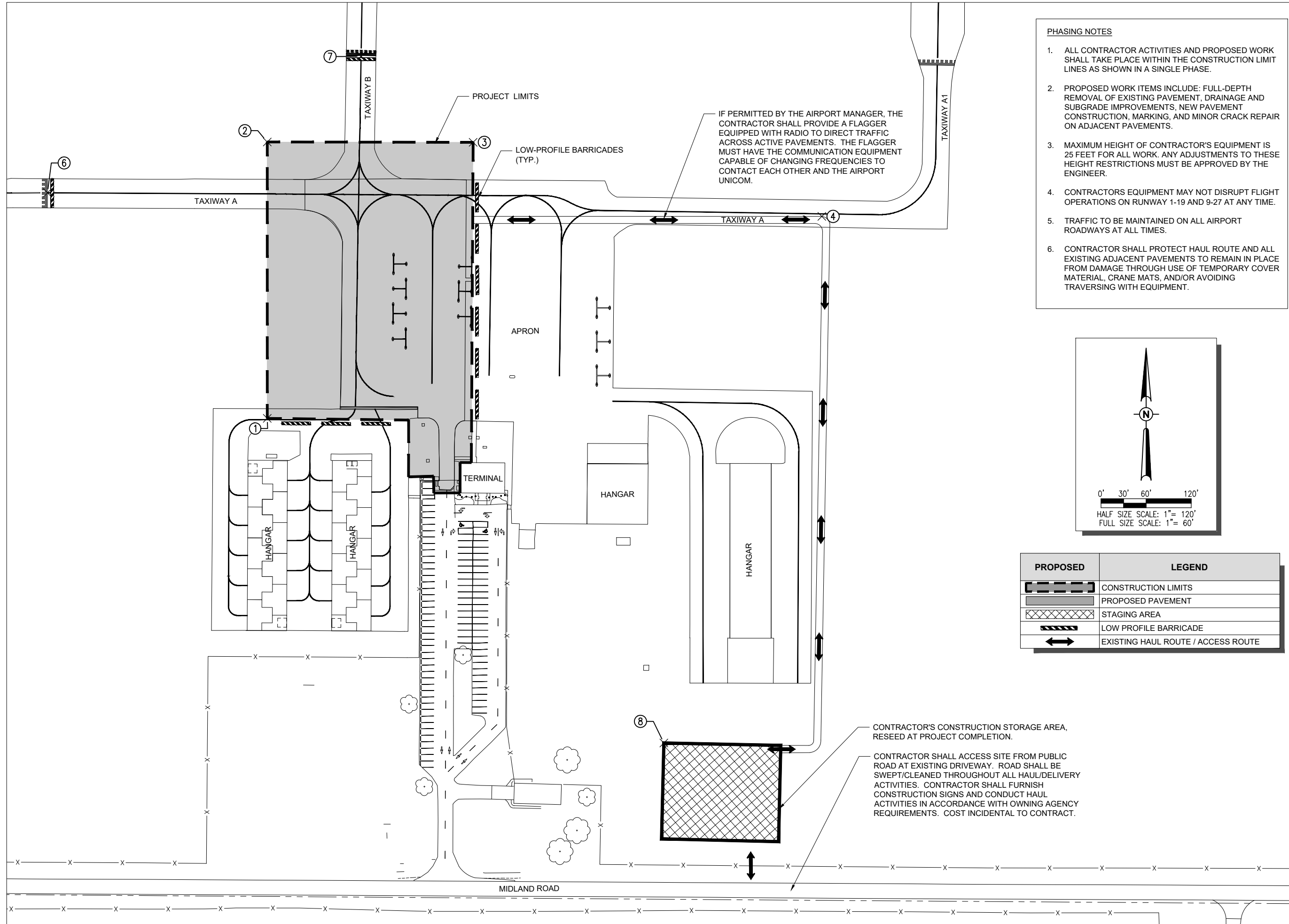


PHASING NOTES

1. ALL CONTRACTOR ACTIVITIES AND PROPOSED WORK SHALL TAKE PLACE WITHIN THE CONSTRUCTION LIMIT LINES AS SHOWN IN A SINGLE PHASE.
2. PROPOSED WORK ITEMS INCLUDE: FULL-DEPTH REMOVAL OF EXISTING PAVEMENT, DRAINAGE AND SUBGRADE IMPROVEMENTS, NEW PAVEMENT CONSTRUCTION, MARKING, AND MINOR CRACK REPAIR ON ADJACENT PAVEMENTS.
3. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT IS 25 FEET FOR ALL WORK. ANY ADJUSTMENTS TO THESE HEIGHT RESTRICTIONS MUST BE APPROVED BY THE ENGINEER.
4. CONTRACTORS EQUIPMENT MAY NOT DISRUPT FLIGHT OPERATIONS ON RUNWAY 1-19 AND 9-27 AT ANY TIME.
5. TRAFFIC TO BE MAINTAINED ON ALL AIRPORT ROADWAYS AT ALL TIMES.
6. CONTRACTOR SHALL PROTECT HAUL ROUTE AND ALL EXISTING ADJACENT PAVEMENTS TO REMAIN IN PLACE FROM DAMAGE THROUGH USE OF TEMPORARY COVER MATERIAL, CRANE MATS, AND/OR AVOIDING TRAVERSING WITH EQUIPMENT.



PROPOSED	LEGEND
	CONSTRUCTION LIMITS
	PROPOSED PAVEMENT
	STAGING AREA
	LOW PROFILE BARRICADE
	EXISTING HAUL ROUTE / ACCESS ROUTE



RECONSTRUCT WEST PORTION OF RAMP

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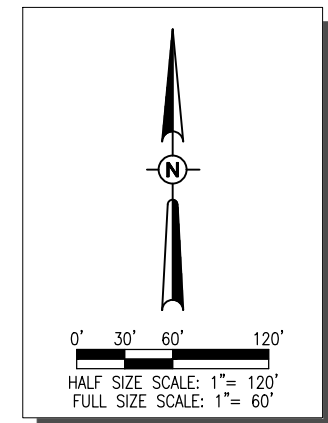
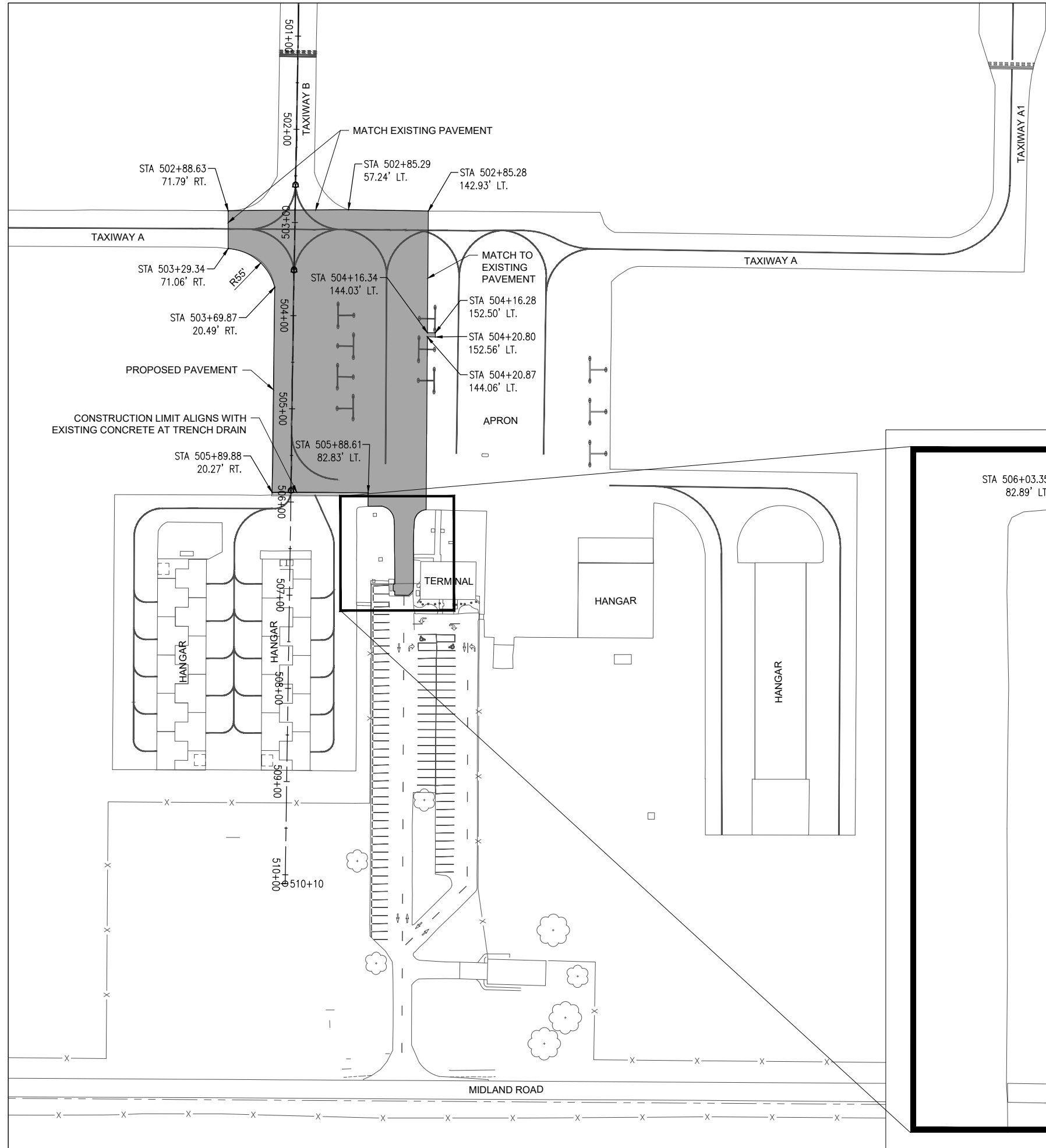
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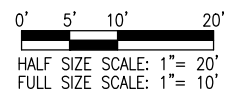
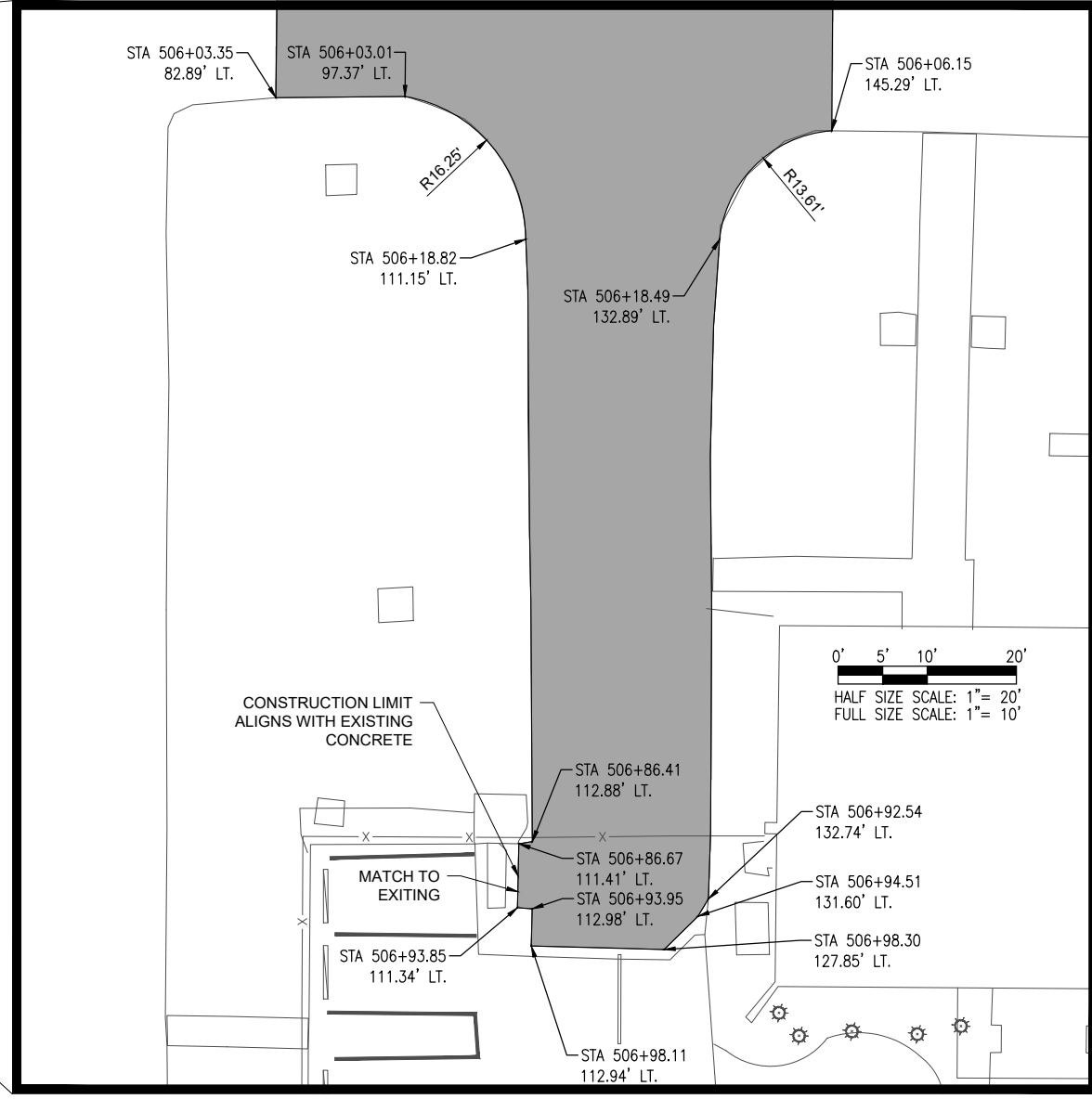
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PHASING PLAN



Alignment	Description	Station	Project Coordinates	
			Northing	Easting
Existing Apron	Beginning of Alignment	501+00	1652344.2035	2353346.3244
	Prop. Pavement Start	502+86.77	1652157.4608	2353343.1209
	Prop. Pavement End	505+89.43	1651854.8350	2353338.7099
	End of Alignment	510+10	1651434.7160	2353332.4774

NOTE: ADDITIONAL PAVEMENT LAYOUT DATA AND GRADING IS INCLUDED ON SHEET 12.



RECONSTRUCT WEST PORTION OF RAMP

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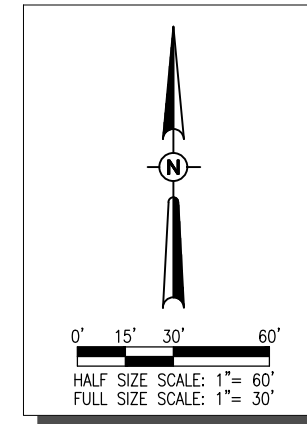
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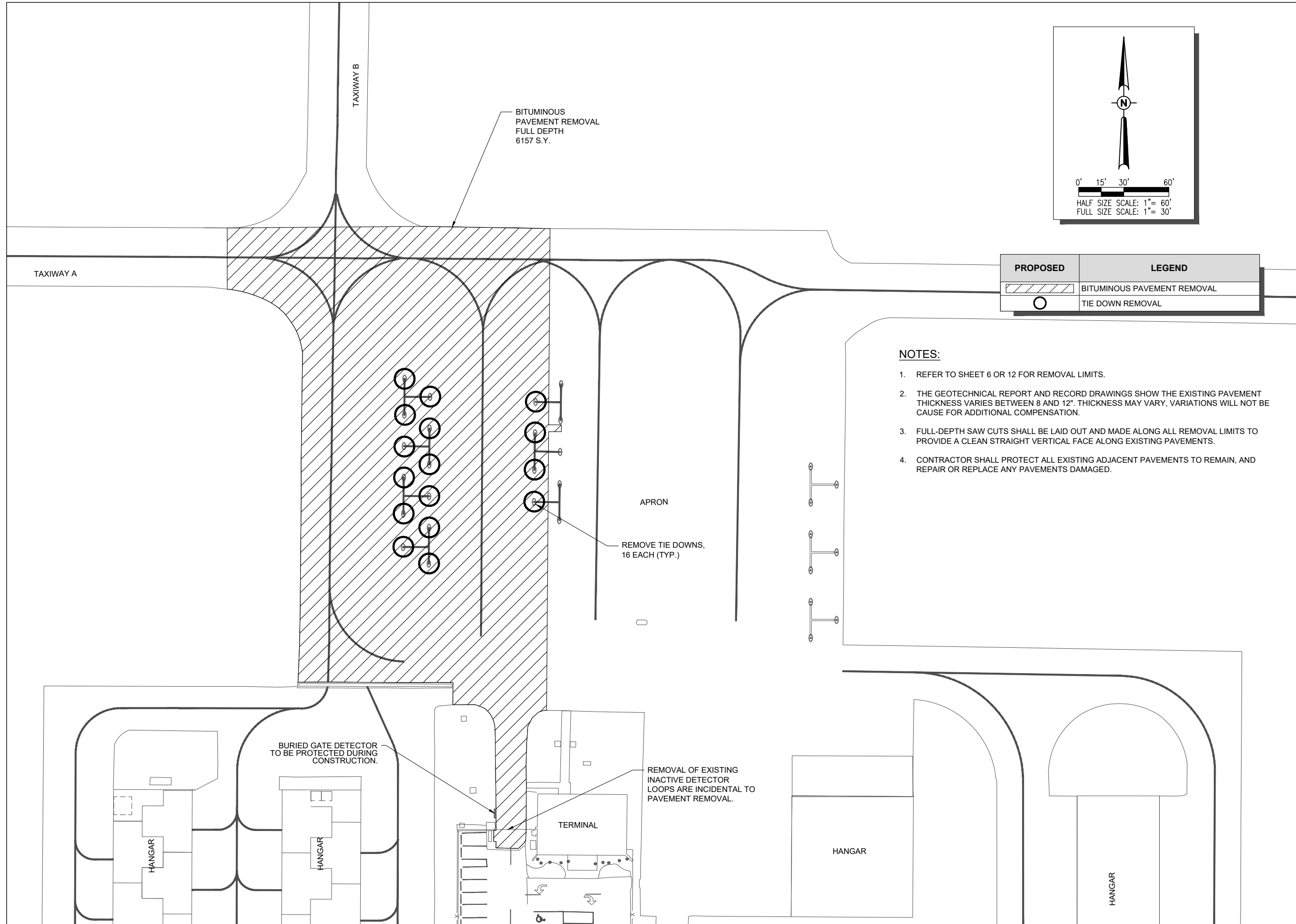
ALIGNMENT DATA TABLE AND PAVEMENT LAYOUT



PROPOSED	LEGEND
	BITUMINOUS PAVEMENT REMOVAL
	TIE DOWN REMOVAL

NOTES:

1. REFER TO SHEET 6 OR 12 FOR REMOVAL LIMITS.
2. THE GEOTECHNICAL REPORT AND RECORD DRAWINGS SHOW THE EXISTING PAVEMENT THICKNESS VARIES BETWEEN 8 AND 12". THICKNESS MAY VARY, VARIATIONS WILL NOT BE CAUSE FOR ADDITIONAL COMPENSATION.
3. FULL-DEPTH SAW CUTS SHALL BE LAID OUT AND MADE ALONG ALL REMOVAL LIMITS TO PROVIDE A CLEAN STRAIGHT VERTICAL FACE ALONG EXISTING PAVEMENTS.
4. CONTRACTOR SHALL PROTECT ALL EXISTING ADJACENT PAVEMENTS TO REMAIN, AND REPAIR OR REPLACE ANY PAVEMENTS DAMAGED.



RECONSTRUCT WEST PORTION OF RAMP

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REMOVAL PLAN



UNDERDRAIN SCHEDULE									
STRUCTURE	NORTHING	EASTING	DESCRIPTION	RIM EL.	INVERT EL.	PIPE LENGTH	SIZE	TYPE	INV. SLOPE
S-1	1652161.14	2353484.02	CLEANOUT	851.79	847.80				
						3.0	6.0	PERF. HDPE	0.50%
S-1A	1652158.14	2353483.99	PASS BELOW EXISTING UD	---	847.78				
						332.0	6.0	PERF. HDPE	0.50%
S-2	1651826.14	2353481.66	HDPE PIPE BEND W/ INSP. HOLE	849.89	846.12				
						64.4	6.0	PERF. HDPE	0.34%
S-3	1651813.31	2353418.58	CONNECT TO EXIST. STORM INLET	848.87	845.90				

Tie Down	Station	Offset (LT)
T1	503+87.05	47.83
T2	503+98.80	64.90
T3	504+11.03	47.98
T4	504+19.96	65.04
T5	504+32.19	48.12
T6	504+43.96	65.19
T7	504+52.90	48.25
T8	504+64.77	65.33
T9	504+76.88	48.41
T10	504+85.90	65.47
T11	504+98.13	48.55
T12	505+09.90	65.62
T13	504+01.44	134.97
T14	504+22.60	134.78
T15	504+46.58	135.11
T16	504+67.92	134.93

Marking	Station	Offset
M1	503+08.36	96.45 RT
M2	503+07.86	45.85 RT
M3	502+63.72	0.00
M4	502+12.32	0.00
M5	503+07.40	0.00
M6	503+07.15	42.68 LT
M7	503+06.96	56.95 LT
M8	503+06.18	134.46 LT
M9	503+06.09	142.65 LT
M10	503+05.56	188.15 LT
M11	503+51.76	0.00
M12	503+52.55	99.57 LT
M13	503+55.64	177.32 LT
M14	505+25.12	0.00
M15	505+89.43	0.00
M16	505+75.23	50.48 LT
M17	505+57.53	100.95 LT

PAVEMENT NOTES:

- REFER TO SHEET 6 FOR ALIGNMENT DATA TABLE AND SHEET 6 OR 12 FOR PROPOSED PAVEMENT LIMITS.
- SEE TYPICAL PAVEMENT SECTION A-A ON SHEET 11.

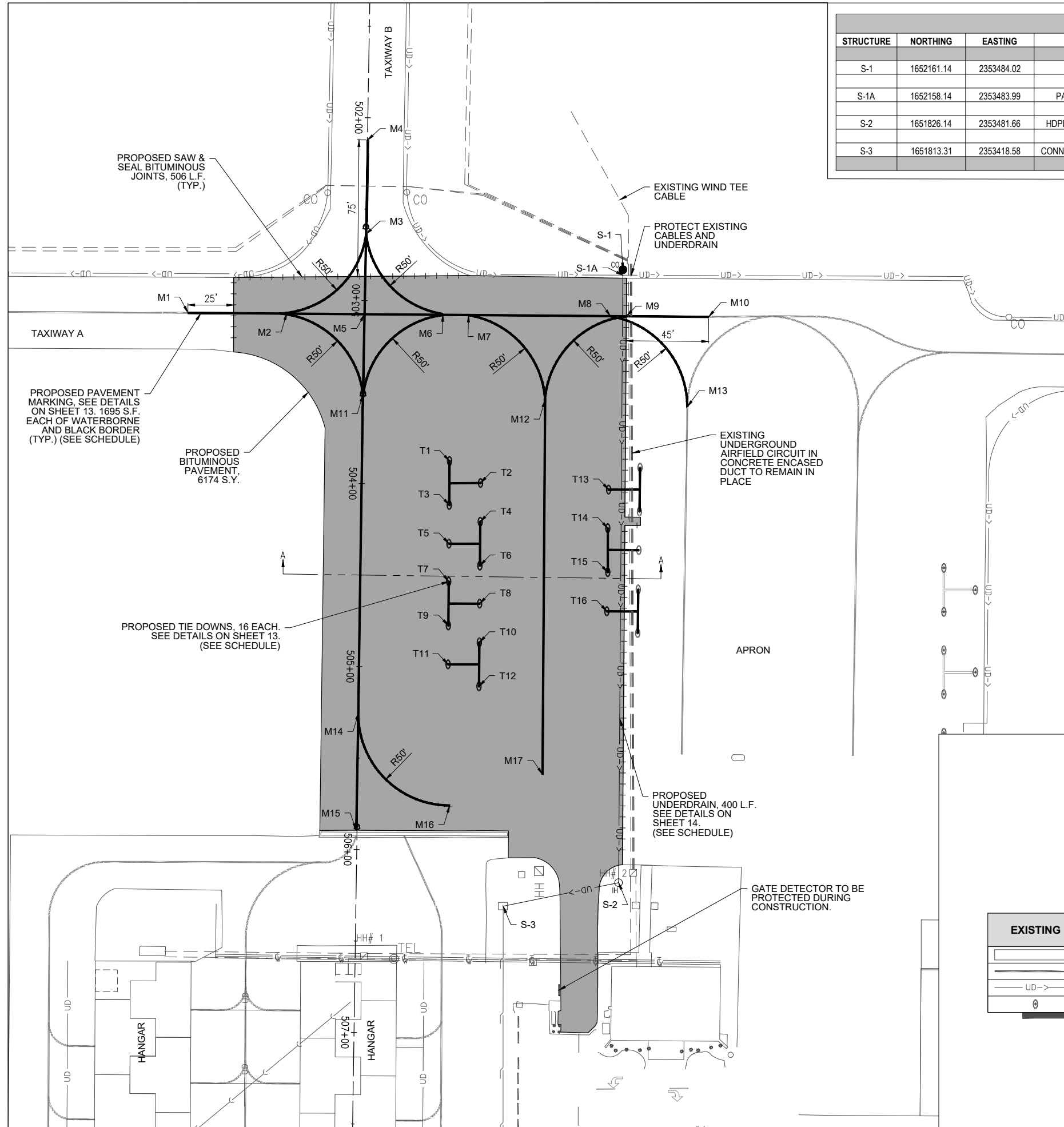
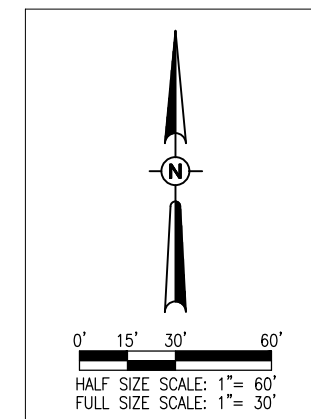
CRACK REPAIR NOTES:

- EXISTING CRACKS ON THE ADJACENT APRON PAVEMENT TO REMAIN WERE SURVEYED, BUT OMITTED FROM THE PLANS FOR CLARITY. THE ACTUAL LOCATION OF CLEAN AND SEAL CRACK QUANTITY SHALL BE DETERMINED BY THE RESIDENT ENGINEER.
- CONTRACTOR SHALL PERFORM THE WORK AS DIRECTED BY THE RESIDENT ENGINEER AND SHALL NOT BE ENTITLED TO, OR MAKE ANY CLAIM FOR, ADDITIONAL COMPENSATION IF THE ACTUAL QUANTITY IS MORE OR LESS THAN THE BID QUANTITY ESTIMATED FOR EACH REPAIR ITEM.

MARKING NOTES:

- TIE DOWN MARKING STATION AND OFFSETS ARE THE SAME AS THE TIE DOWN SCHEDULE.
- GLASS SPHERES ARE REQUIRED FOR ALL YELLOW MARKINGS. GLASS SPHERES ARE NOT REQUIRED FOR BLACK MARKINGS.
- ALL MARKING SHALL HAVE A 6" BLACK BORDER.

EXISTING	PROPOSED	LEGEND
		BITUMINOUS PAVEMENT
		MARKING
		UNDERDRAIN
		TIE DOWN



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RECONSTRUCT WEST PORTION OF RAMP

IDA No: EZI-4883

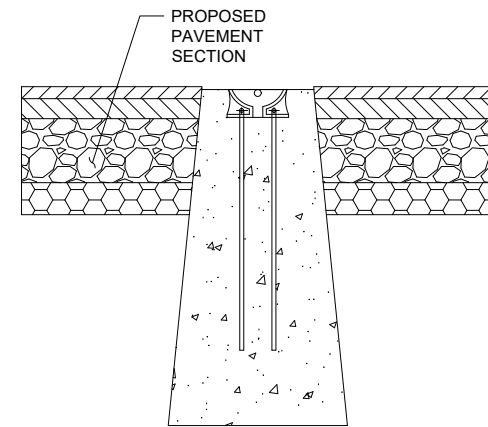
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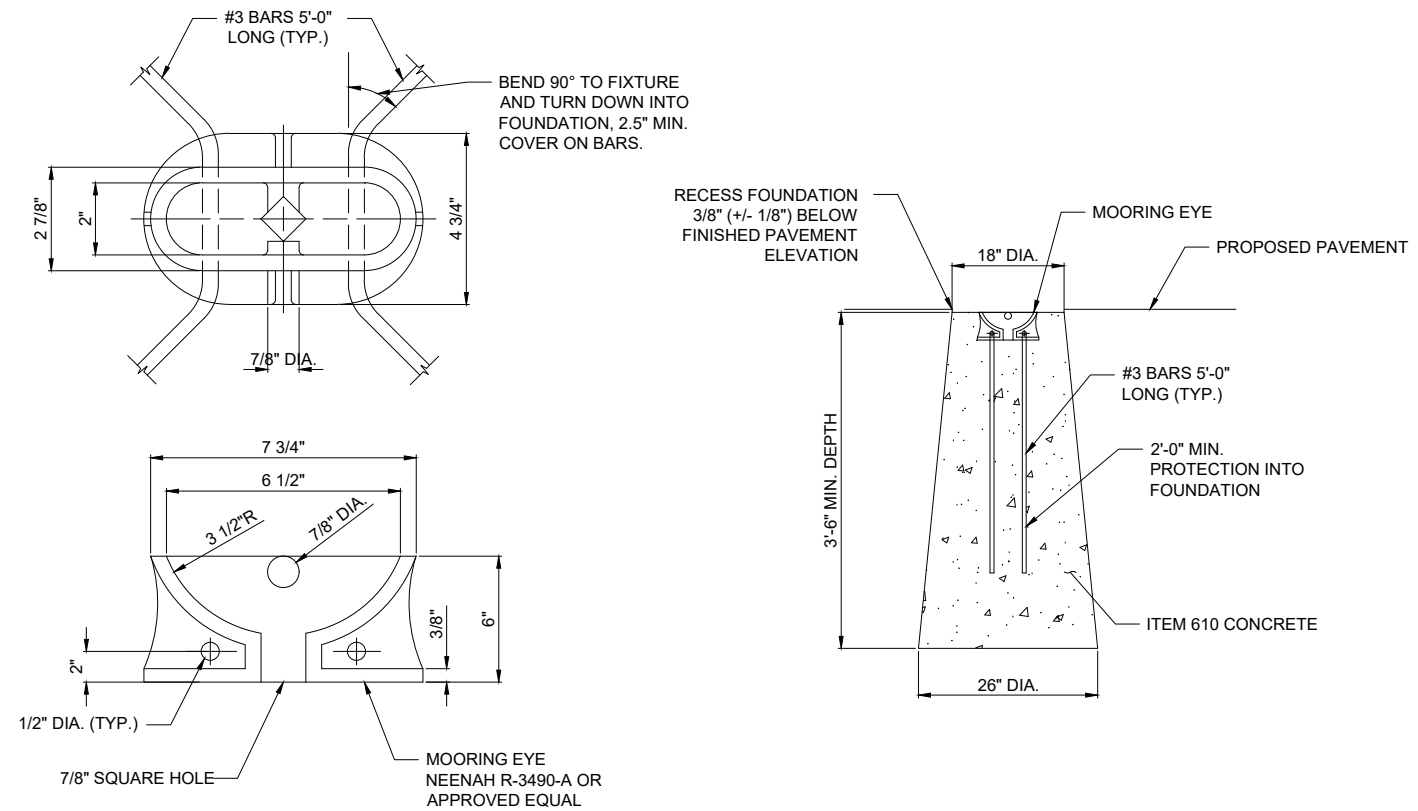
PROPOSED PLAN



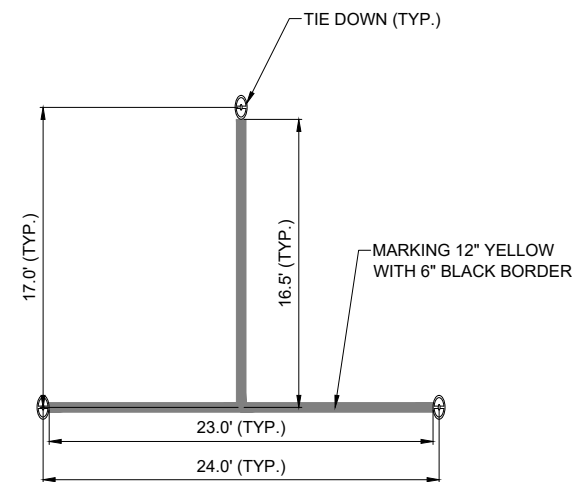
NOTES:

- EXCAVATION, BACKFILL, CONCRETE FOUNDATION AND MOORING EYE SHALL BE INCIDENTAL TO TIE DOWN.

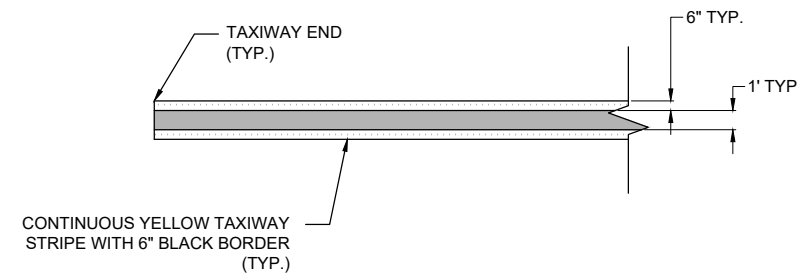
NEW TIE DOWN IN NEW PAVEMENT AREA



TIE DOWN (MOORING EYE)



TIE DOWN MARKING DETAIL



TAXIWAY CENTERLINE DETAIL
NOT TO SCALE

RECONSTRUCT WEST PORTION OF RAMP

IDA No: EZI-4883

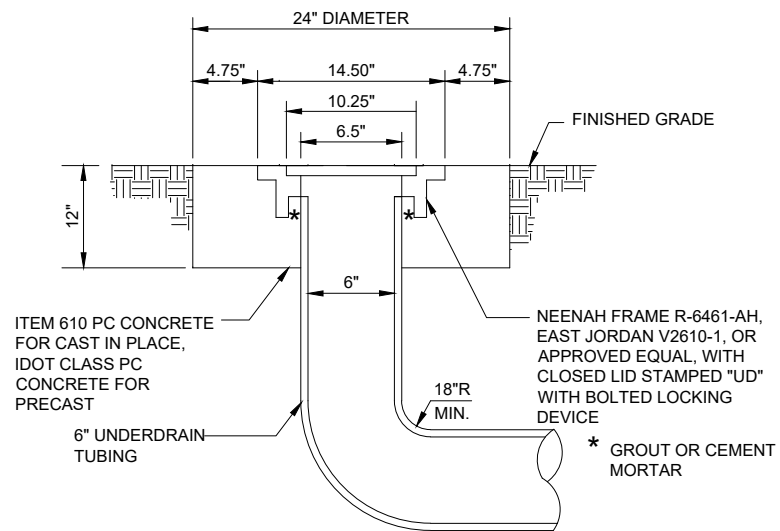
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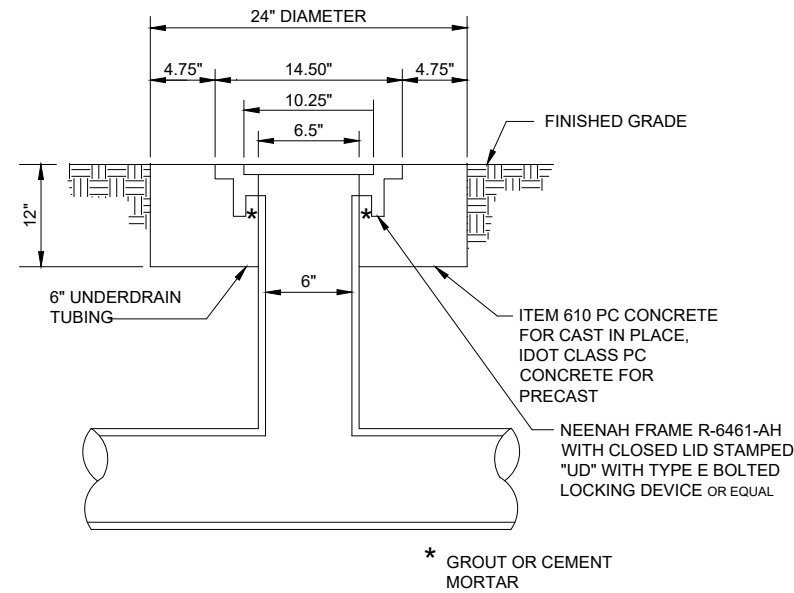
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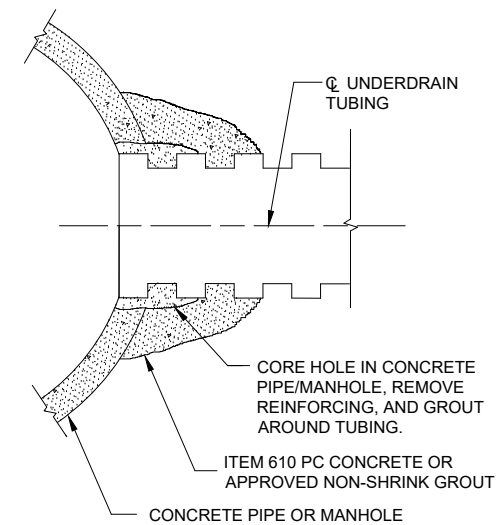
TIE DOWN AND MARKING DETAILS



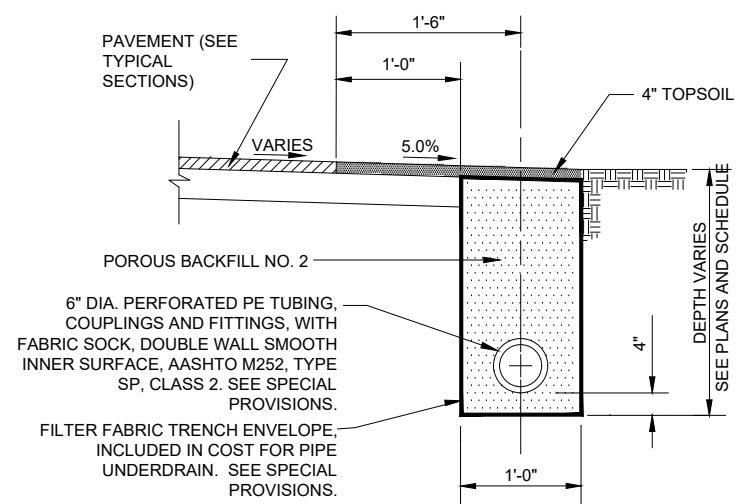
UNDERDRAIN CLEANOUT



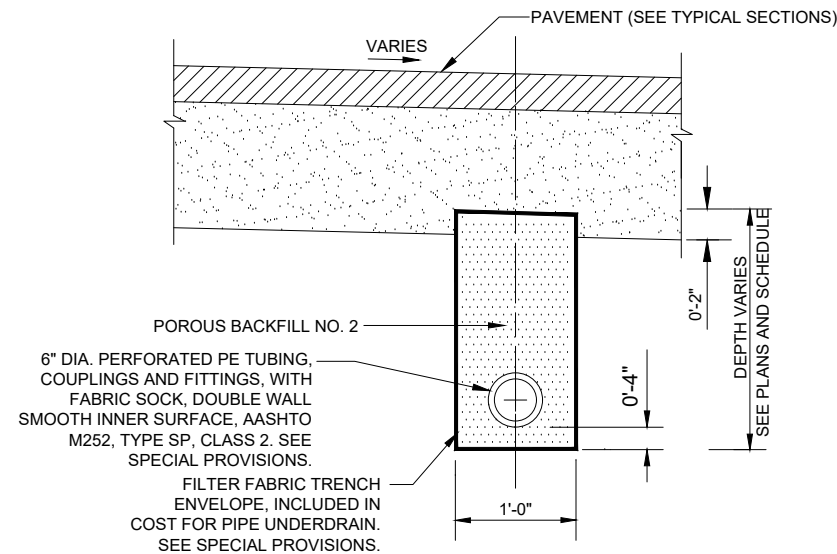
UNDERDRAIN INSPECTION HOLE



STORM SEWER CONCRETE COLLAR AND GROUT CONNECTION



UNDERDRAIN ALONG PAVEMENT EDGE



UNDERDRAIN UNDER PAVEMENT

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DRAINAGE DETAILS