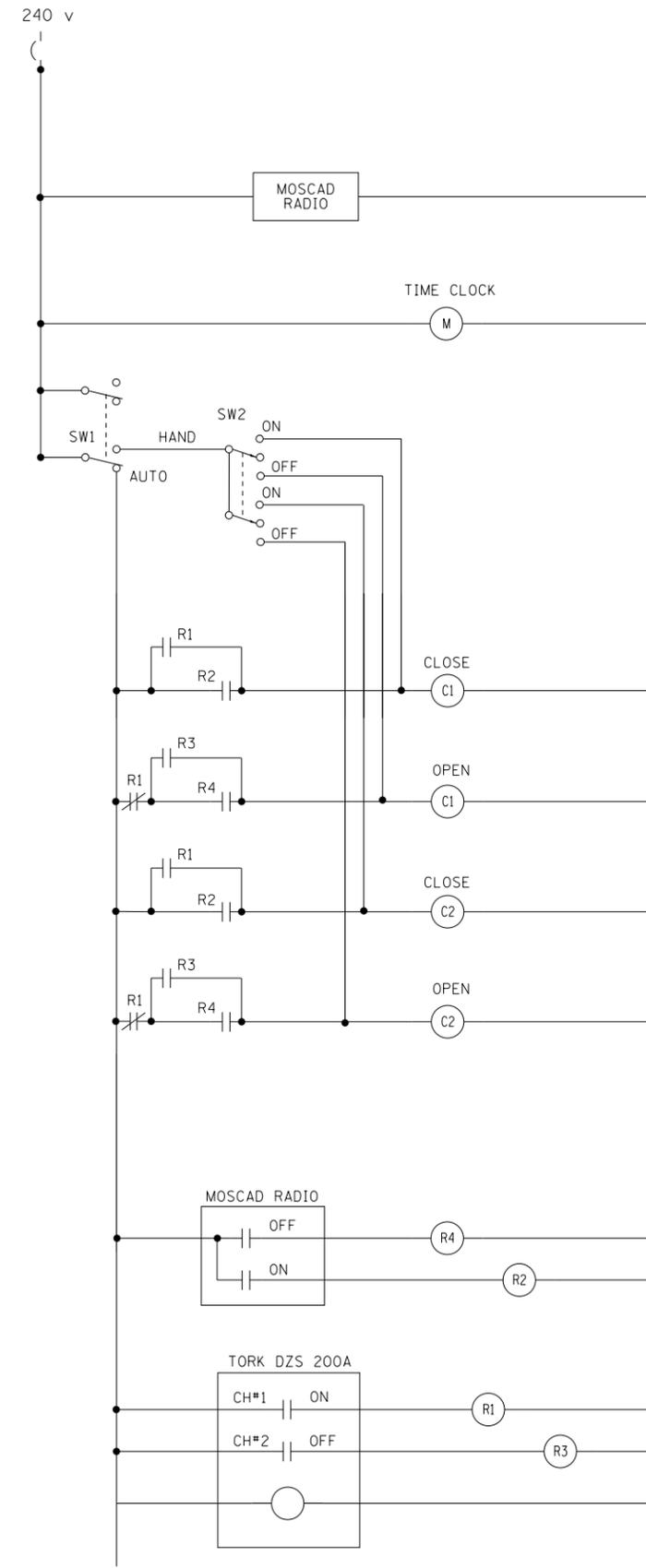


NOTES

- CABINET SHALL BE FABRICATED FROM 0.125-INCH (3.175) SHEET ALUMINUM #3003H14, FORMED AND ARC WELDED.
- ALL SCREWS AND HARDWARE SHALL BE PLATED, GALVANIZED, OR MADE OF BRASS, ALUMINUM OR STAINLESS STEEL, UNLESS OTHERWISE NOTED.
- NAME PLATE SHALL HAVE ENGRAVED 0.75-INCH (19.05) HIGH LETTERS FILLED IN BLACK: "STATE OF ILLINOIS LIGHTING CONTROLS" UNLESS OTHERWISE SPECIFIED.
- ONE INCH THICK POLYISOCYANURATE INSULATION SHALL BE INSTALL AND PERMANENTLY CEMENTED ON ALL SIDES OF THE CABINET AND DOORS.
- CABINET SHALL BE PRIMED AND PAINTED AS SPECIFIED.
- ELECTRIC UTILITY METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET AS SHOWN ON THE PANEL LAYOUT DIAGRAM.
- THE COMPLETED CONTROLLER SHALL BE U.L. LISTED AS AN INDUSTRIAL CONTROL PANEL UNDER UL508.
- METAL MOUNTING PANEL SHALL BE FABRICATED FROM THE SAME MATERIAL AS THE CABINET AND SHALL BE FLANGED BACK 0.75-INCHES I.D. ON 4 SIDES.
- CIRCUIT BREAKERS AND CONTACTORS AND OTHER COMPONENTS SHALL BE MOUNTED ON 0.125-INCH (3.175) THICK GLASTIC INSULATION BACK PANEL.
- ALL DEVICES SHALL BE FRONT REMOVABLE.
- TIME CLOCK CHANNEL 1 N.O. CONTACT IS CLOSED NIGHT AND OPEN DAY (LIGHTS ON).
- SET LATITUDE TO 42 DEGREES, SET CH.1 TO 23 MINUTES AFTER ASTRONOMICAL SUNSET, 50 MINUTES BEFORE ASTRONOMICAL SUNRISE. SET CH.2 TO 60 MINUTES AFTER ASTRONOMICAL SUNSET (WITH A SIGNAL LENGTH OF 1 SECOND), +28 MINUTES AFTER ASTRONOMICAL SUNRISE (WITH A SIGNAL LENGTH OF 7 SECONDS.)
- BUS BAR SHALL HAVE 22 LUG TERMINALS SIZED TO ACCOMMODATE REQUIRED WIRE SIZES. 240V NEUTRAL BUS SHALL BE PAINTED WHITE, GROUND BUS SHALL BE PAINTED GREEN, AND THE 120V NEUTRAL BUS SHALL BE PAINTED GREY.
- ALL LUGS SHALL BE OF COPPER SCREWS AND CONNECTORS, SPRING HELD.
- ALL WIRING TERMINATIONS SHALL BE RATED NOT LESS THAN 75 DEGREE CENTIGRADE.
- ALL CONTROL WIRING SHALL BE 600V #12 TYPE MTW, SCADA WIRING SHALL BE #18.
- ALL POWER WIRING SHALL BE 600V TYPE RHH/RHW.
- ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED:

R - RED	Y - YELLOW
B - BLACK	W - WHITE
BL - BLUE	G - GREEN
	GR - GREY
- MOSCAD I/O WIRING SHALL BE:
 - DIGITAL INPUT (DI) WIRING SHALL BE #18 MTW PURPLE.
 - ANALOG INPUT (AI) WIRING SHALL BE #18, 2/C SHIELDED.
 - AI AND DI WIRING MAY BE BUNDLED TOGETHER, BUT SHALL NOT BE BUNDLED WITH OTHER WIRING.
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
- SCHEMATIC SHOWN WITH BREAKER OPEN, CONTACTOR OPEN, CABINET DOOR CLOSED, CLOCK NOT ACTIVE (DE-ENERGIZED STATE).
- A LAMINATED COPY OF THE CIRCUIT SCHEMATIC AND SCADA I/O DIAGRAM (NO SMALLER THAN 11"x17" EACH) SHALL BE ATTACHED TO THE INSIDE OF THE CONTROLLER WITH STAINLESS STEEL SCREWS.



CONTROL CIRCUIT LADDER LOGIC DIAGRAM

MOSCAD I/O ASSIGNMENTS		
TERM	MOSCAD DESTINATION	DESCRIPTION OF INPUT
1	DIGITAL INPUT 1	ALARM KNOWLEDGE
2	DIGITAL INPUT 2	DOOR OPEN
3	DIGITAL INPUT 3	MAINS) BREAKER OPEN
4	DIGITAL INPUT 4	CONTACTOR 1 OPEN
5	DIGITAL INPUT 5	CONTACTOR 2 OPEN
6	DIGITAL INPUT 6	CABINET IN NON-AUTO
7	DIGITAL INPUT 7	BACK-UP CLOCK OFF CALL
8	DIGITAL INPUT 8	BACK-UP CLOCK ON CALL
17	24 V+	24+VDC
18	DI COMMON	COMMON
21	K1 C	K1 COMMON
22	K1 NO	LIGHTS ON CALL
24	K2 C	K2 COMMON
25	K2 NO	LIGHTS OFF CALL
32	ANALOG INPUT 1 (+)	CABINET NEUTRAL CURRENT
33	ANALOG INPUT 1 (-)	CABINET NEUTRAL CURRENT
34	ANALOG INPUT 2 (+)	CABINET SERVICE VOLTAGE
35	ANALOG INPUT 2 (-)	CABINET SERVICE VOLTAGE
40	P. GROUND	GROUND

ALL ANALOG INPUTS WILL BE 4-20 MA ONLY. DIGITAL OUTPUT RELAYS WILL BE ELECTRICALLY ENERGIZED AND MOMENTARILY HELD
MIXED I/O MODULE MODEL NUMBER V436

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - R. TOMSONS 08-19-04
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

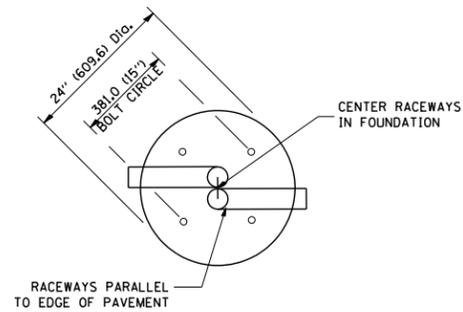
LIGHTING CONTROLLER, BASE MOUNTED, 480VOLT, 200AMP (DUAL) RADIO SCADA

SCALE: NONE SHEET NO. 4 OF 4 SHEETS STA. TO STA.

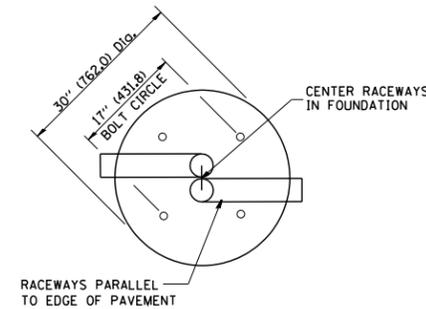
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BE-205		886	801
		CONTRACT NO.	60L70	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY O _u = 0.375 TON/SO. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY O _u = 0.75 TON/SO.FT	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY O _u = 1.50 TON/SO. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)



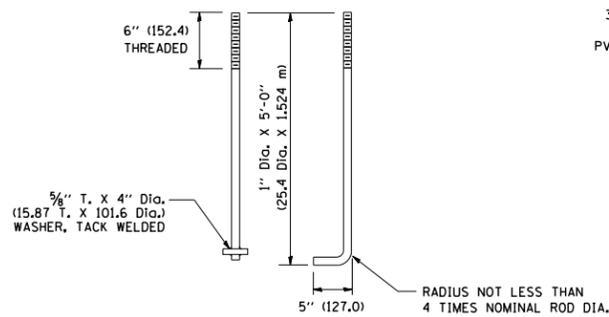
TOP VIEW



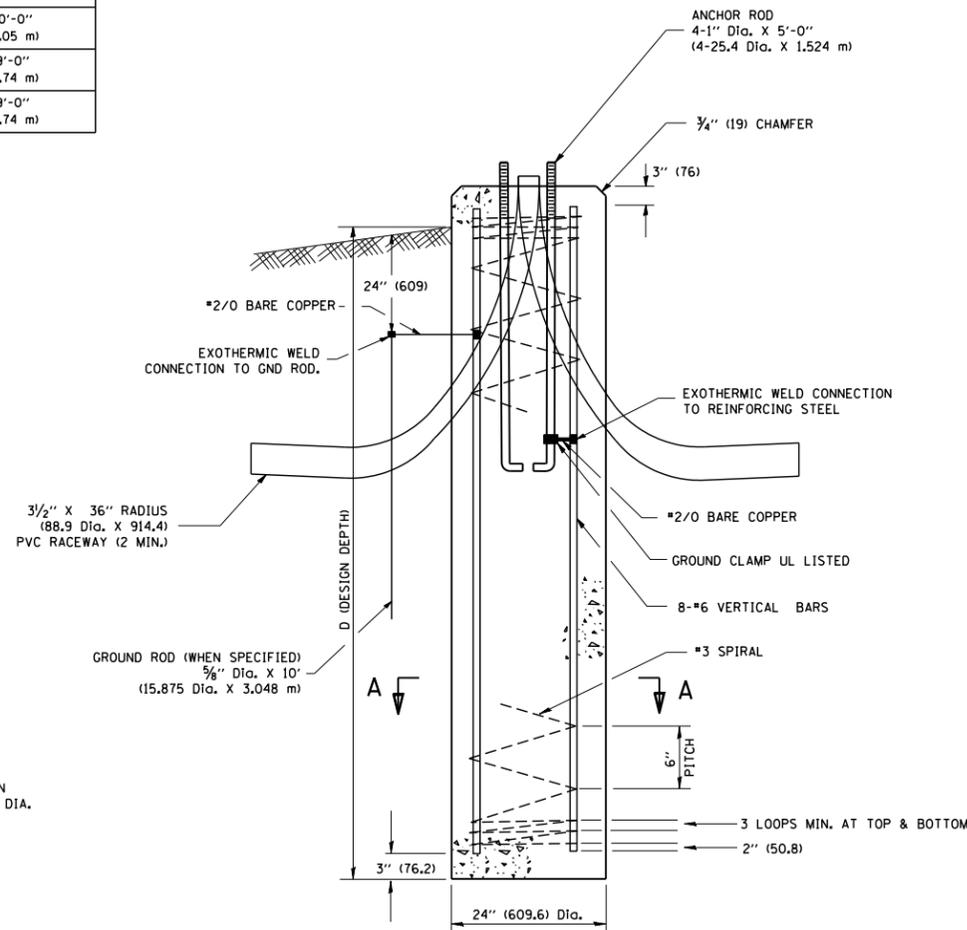
TOP VIEW

NOTES

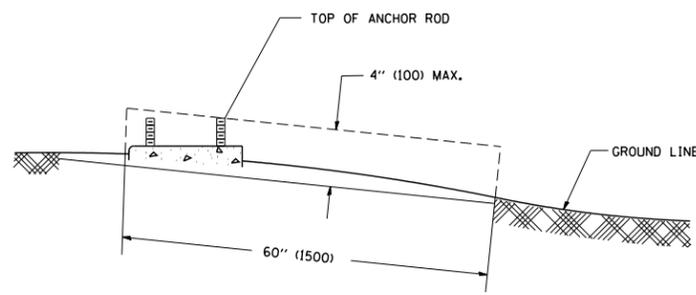
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



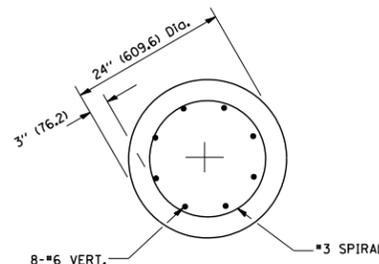
ANCHOR ROD DETAIL



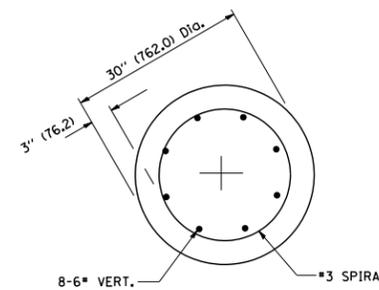
FOUNDATION DETAIL



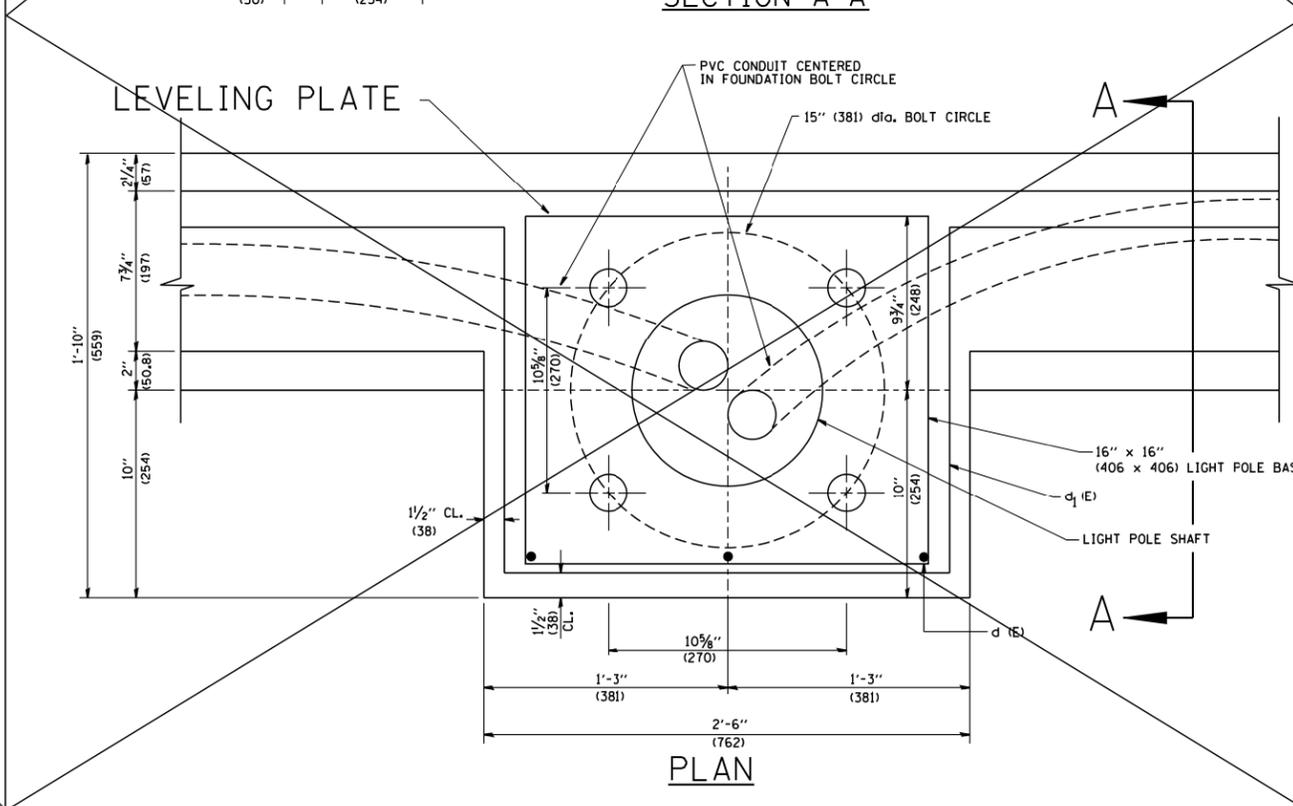
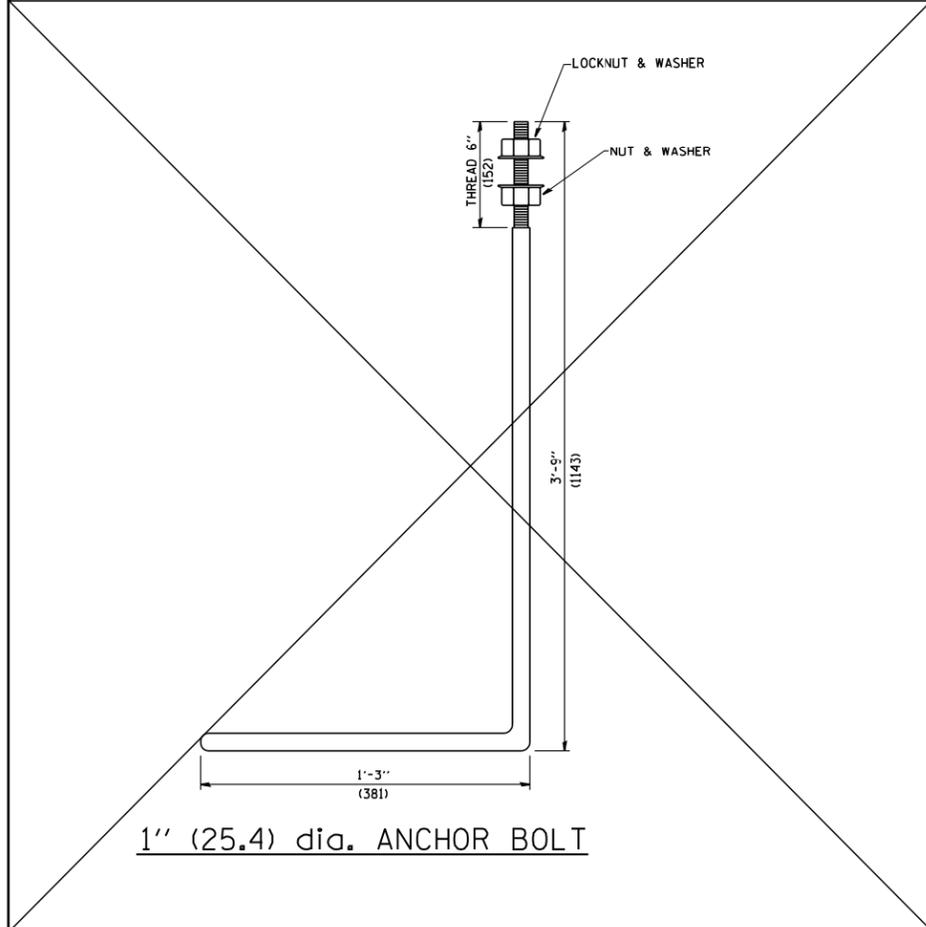
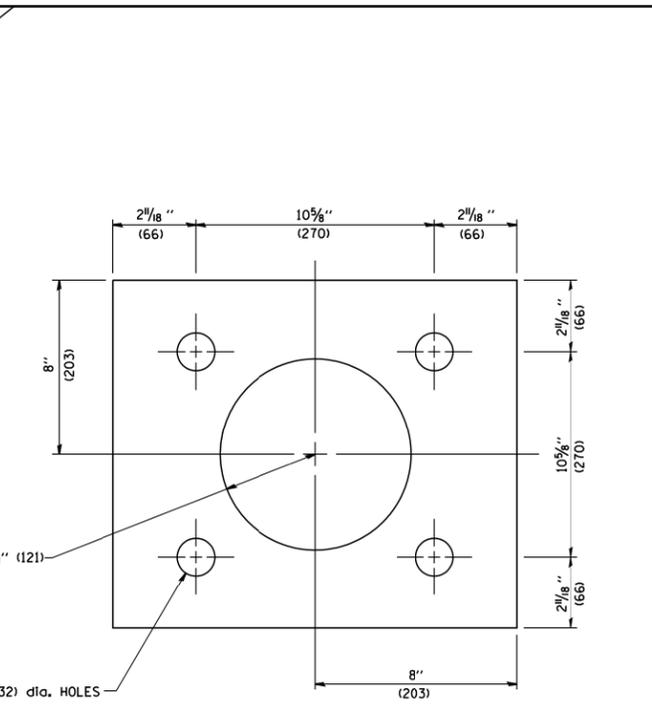
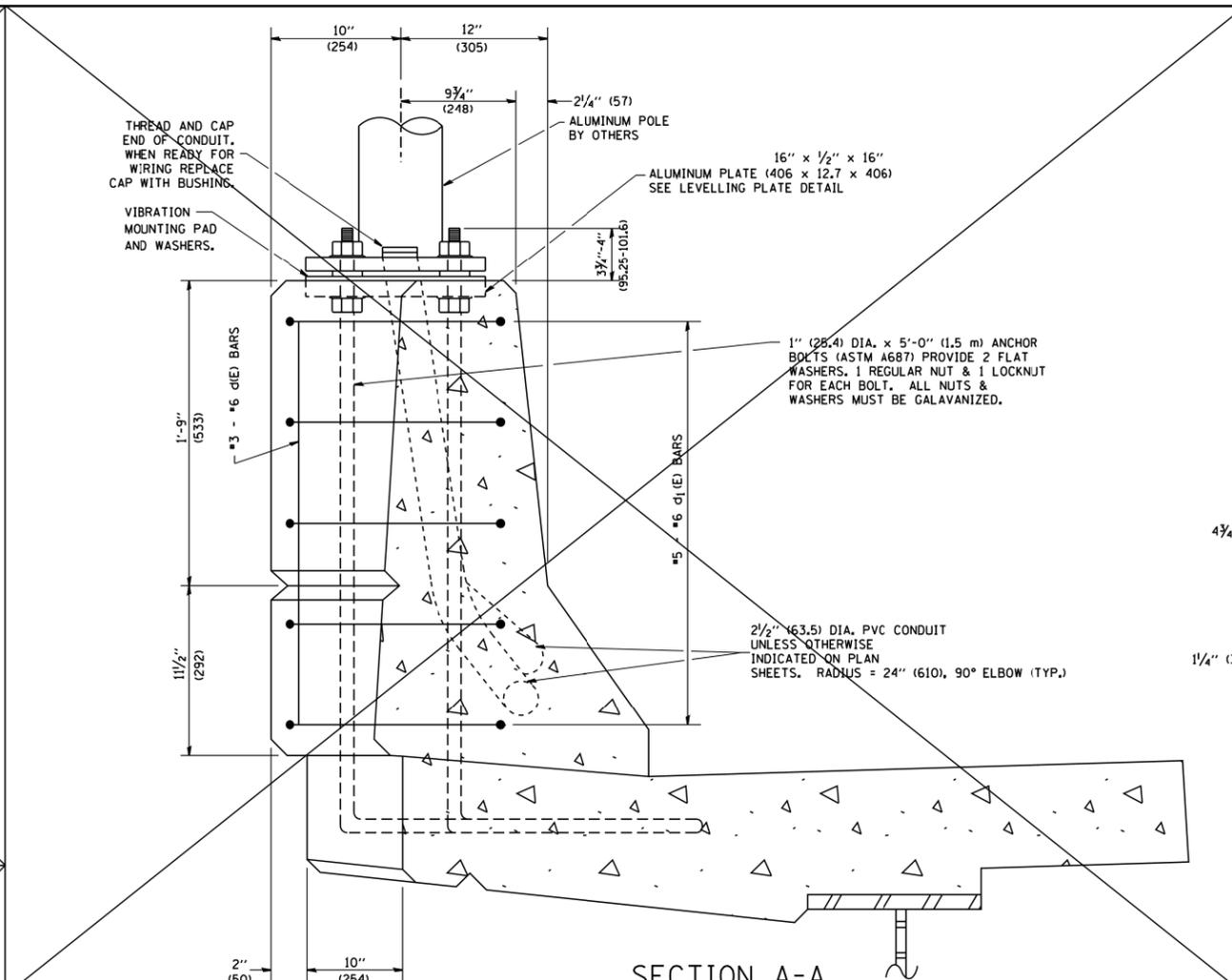
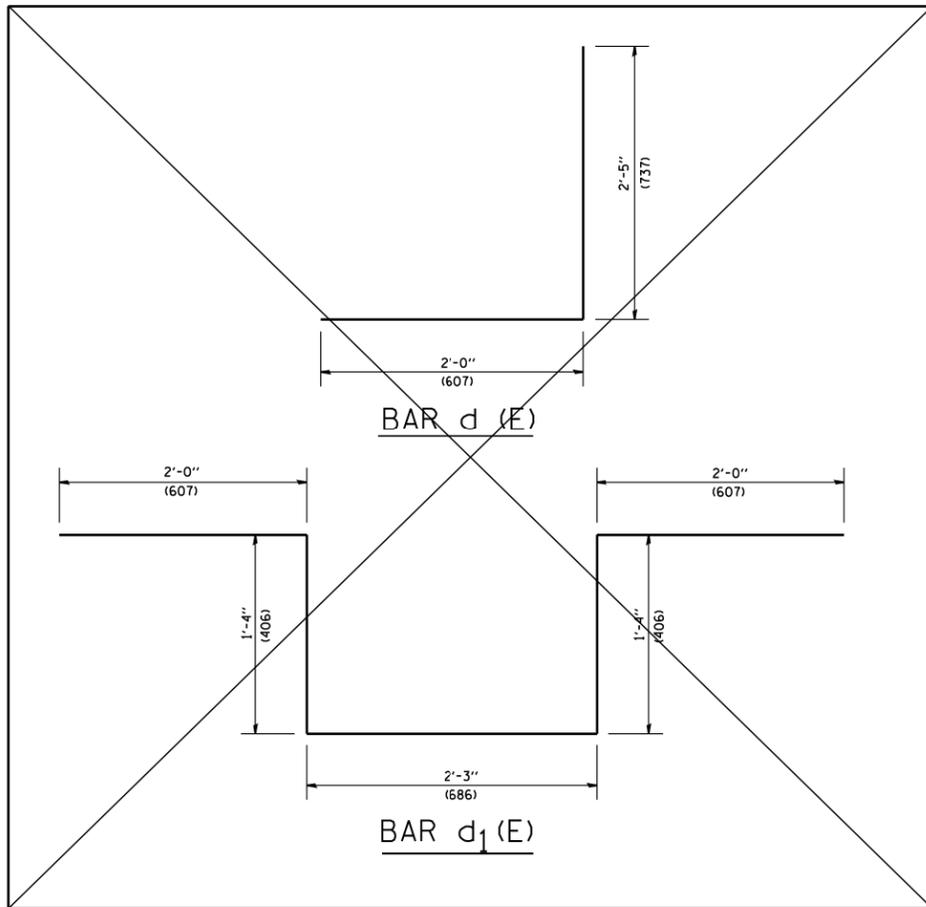
FOUNDATION EXTENSION DETAIL



SECTION A-A

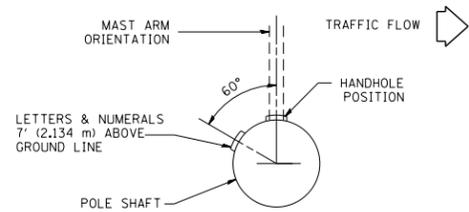


SECTION A-A

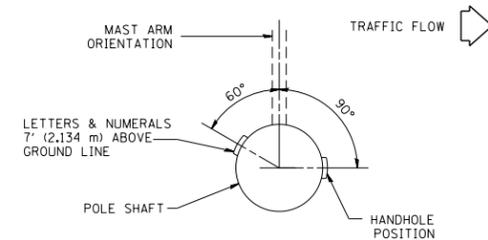


- NOTES**
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 2. LEVEL LIGHT POLE PLATES, USING THE FLANGE NUTS, PRIOR TO POURING THE PARAPET WALL. THE TOP OF THE PLATE SHALL BE AT THE SAME ELEVATION AS THE FINISHED CONCRETE PARAPET.
 3. THE COST OF ANCHOR BOLTS, CONDUIT, LEVELLING PLATE AND FOUNDATION IS INCLUDED IN THE COST OF THE BRIDGE STRUCTURE.
 4. THIS DRAWING IS FOR THE LEVELING PLATE DETAIL ONLY. FOR ADDITIONAL LIGHT POLE MOUNTING DETAILS SEE STRUCTURAL PLANS.

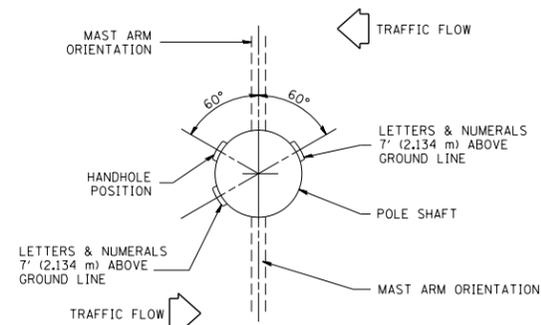
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			15" (381 mm) BOLT CIRCLE						886	803	
	PLOT DATE = 1/4/2008	DATE -	REVISED -			SCALE: NONE			SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 60L70
						SECTION A-A			PLAN			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



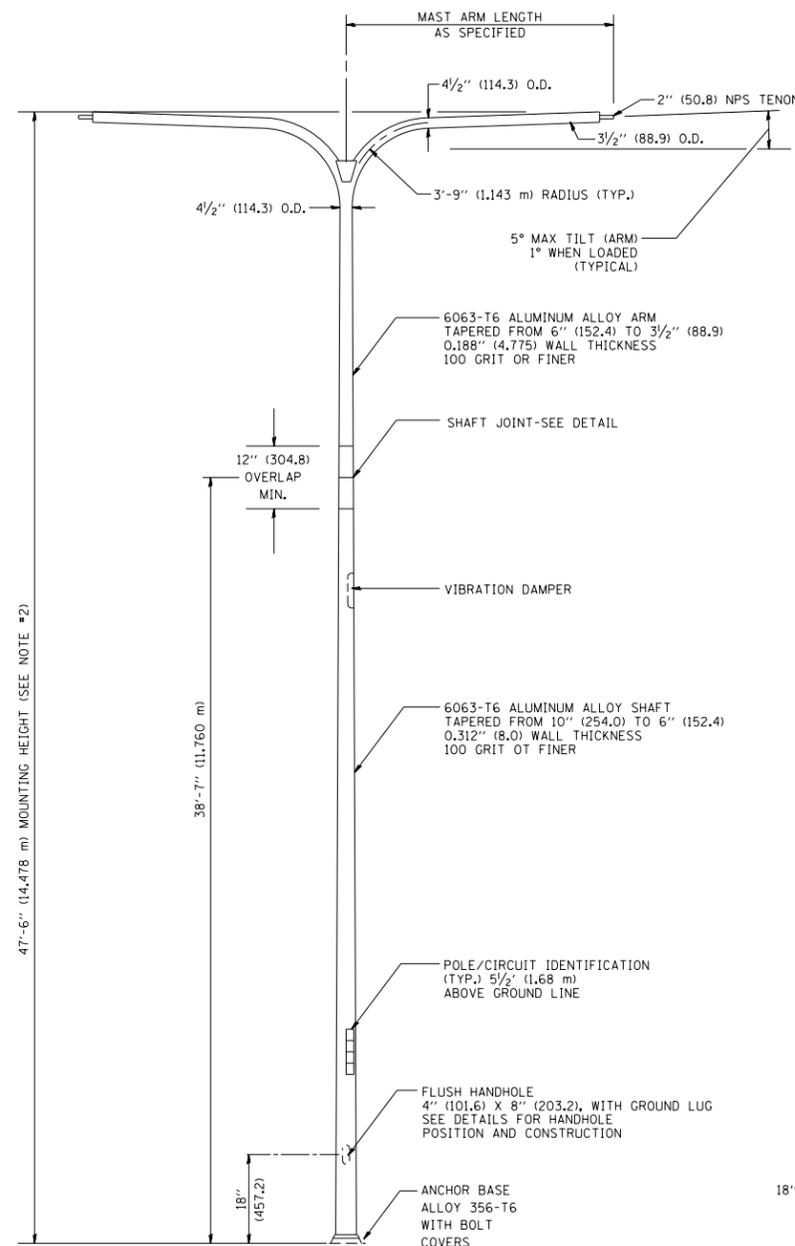
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



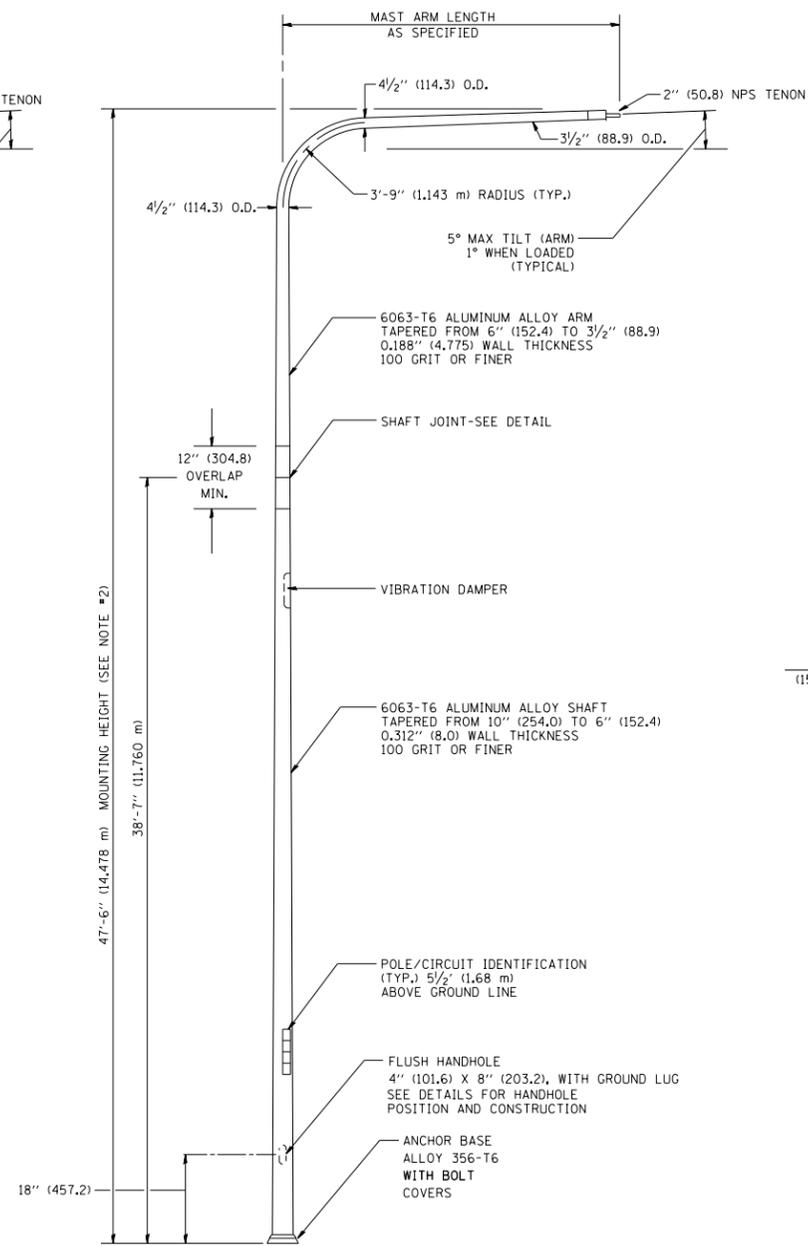
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES



POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES



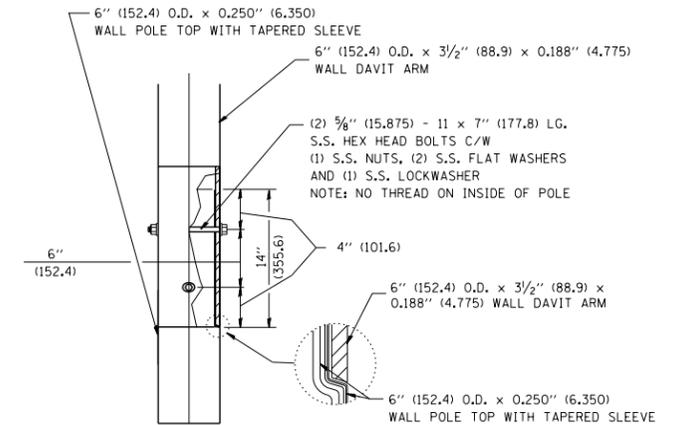
TWIN ARM POLE



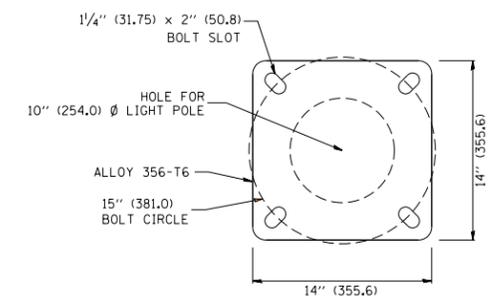
SINGLE ARM POLE

NOTES:

1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
3. TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
4. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
5. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
6. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
7. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
8. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.

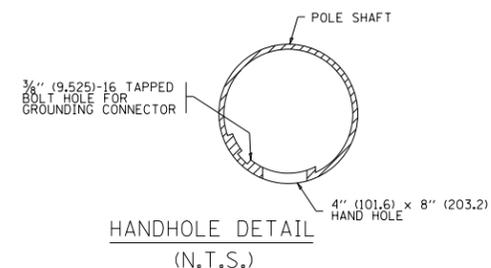


DAVIT ARM CONNECTION
[14" (355.6) OVERLAP SHOWN]



LIGHT POLE BASE PLATE DETAIL

(FOR POLE MOUNTED ON 15 INCH (381.0) BOLT CIRCLE FOUNDATION)



HANDHOLE DETAIL
(N.T.S.)

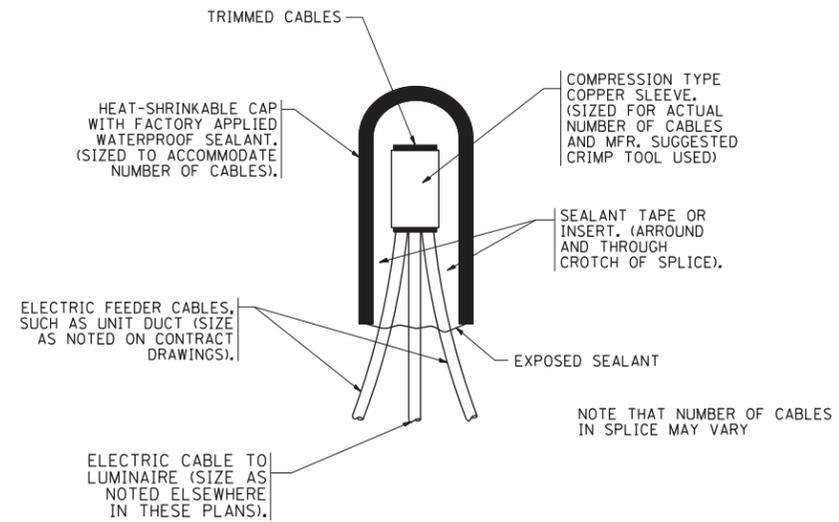
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	PLOT DATE = 4/4/2013	DATE -	REVISED - R. TOMSONS 01-18-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DAVIT LIGHT POLE			
47'-6" (14.478 m) MOUNTING HEIGHT			
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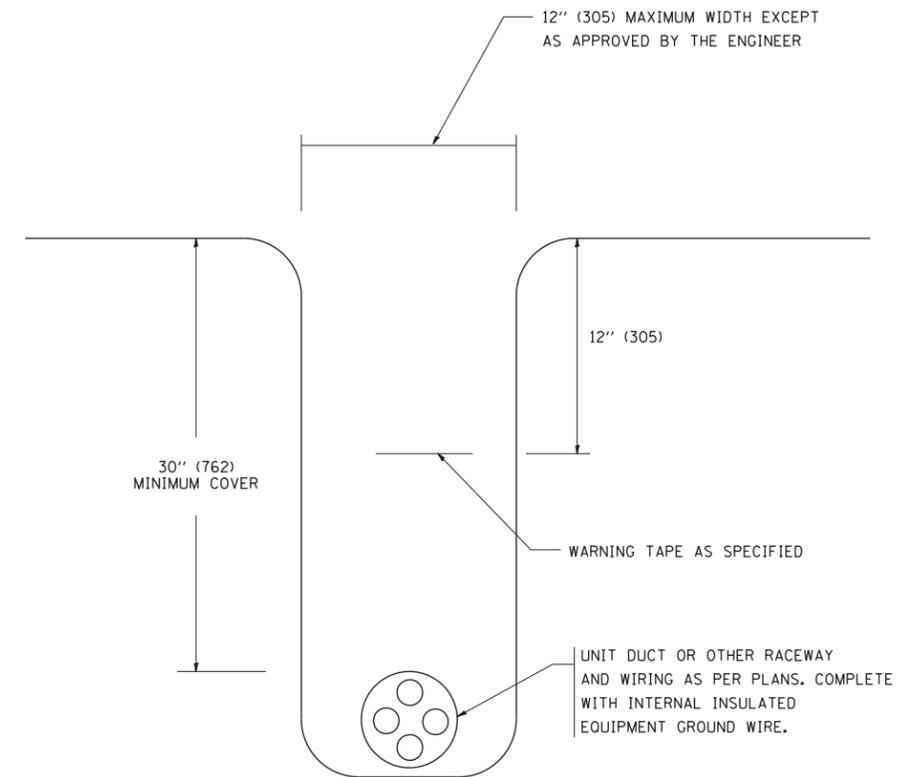
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			886	804
BE-410		CONTRACT NO. 60L70		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



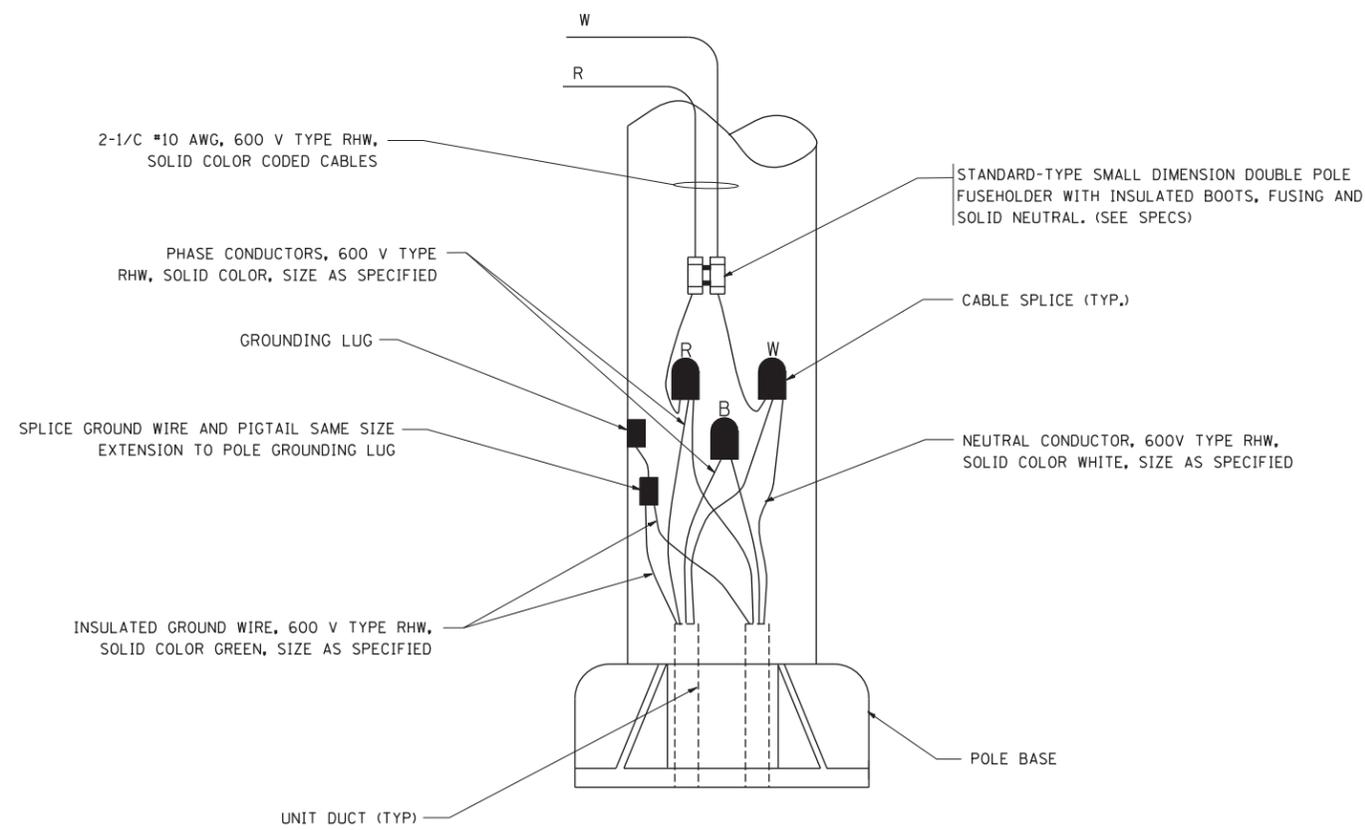
TYPICAL SPLICE DETAIL

N.T.S.



TYPICAL WIRING IN TRENCH DETAIL

N.T.S.



POLE WIRING DETAIL

N.T.S.

REVISIONS	
NAME	DATE

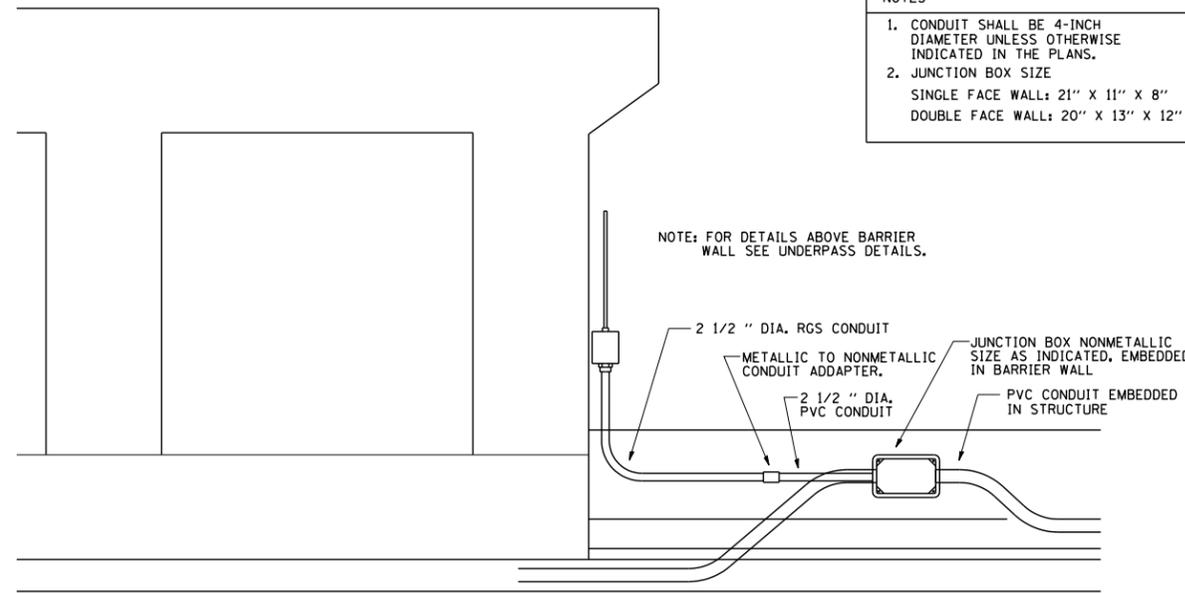
ILLINOIS DEPARTMENT OF TRANSPORTATION

MISC. ELECTRICAL DETAILS
SHEET A

VERT. SCALE: HORIZ. DATE: 2/27/2007

DRAWN BY
CHECKED BY
BE-702

REVISION DATE: 01/01/07

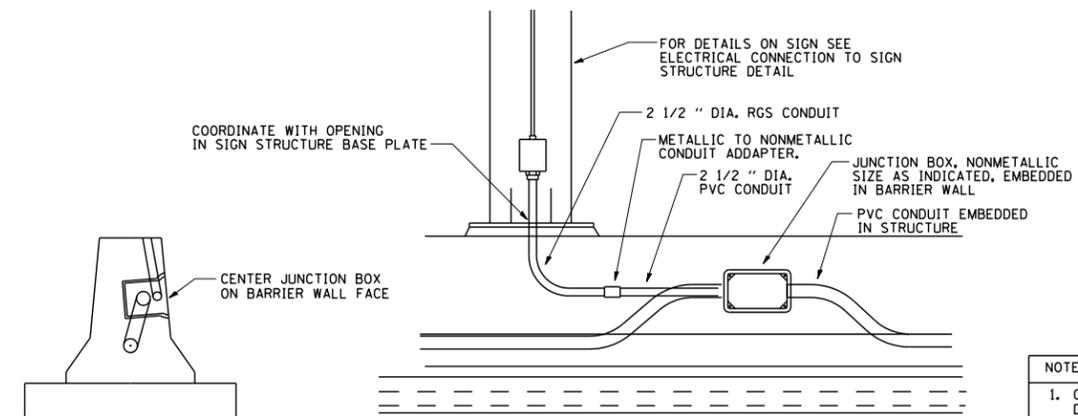


NOTES

1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

NOTE: FOR DETAILS ABOVE BARRIER WALL SEE UNDERPASS DETAILS.

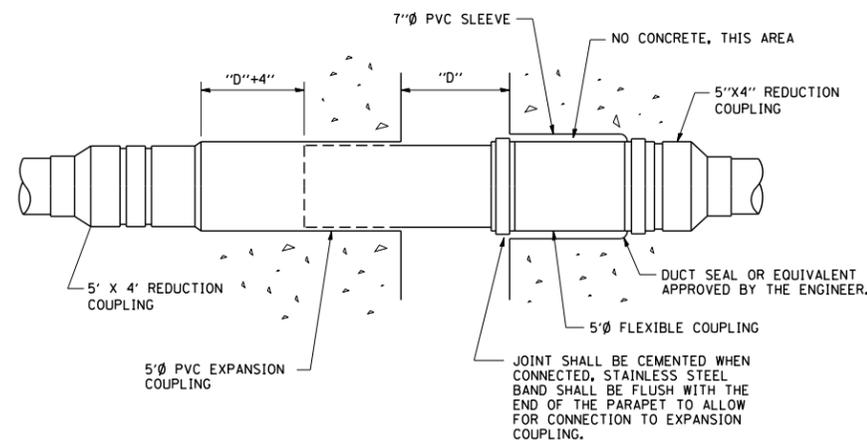
ED - BWD
ELECTRIC CONNECTION TO UNDERPASS LIGHTING



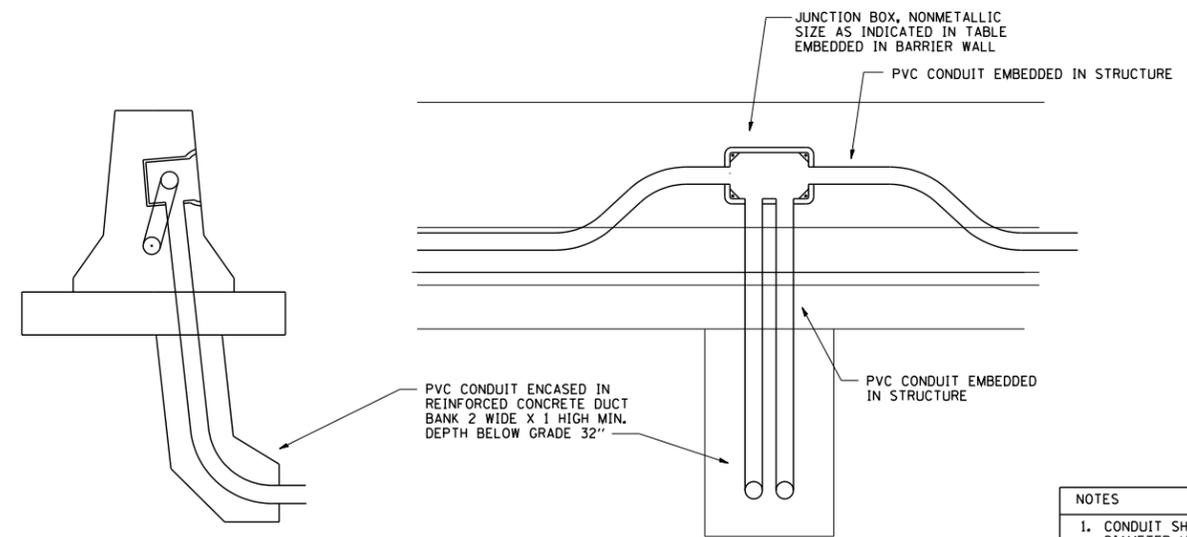
NOTES

1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

ED - SGN
JUNCTION BOX EMBEDDED IN BARRIER WALL FOR SIGN LIGHTING



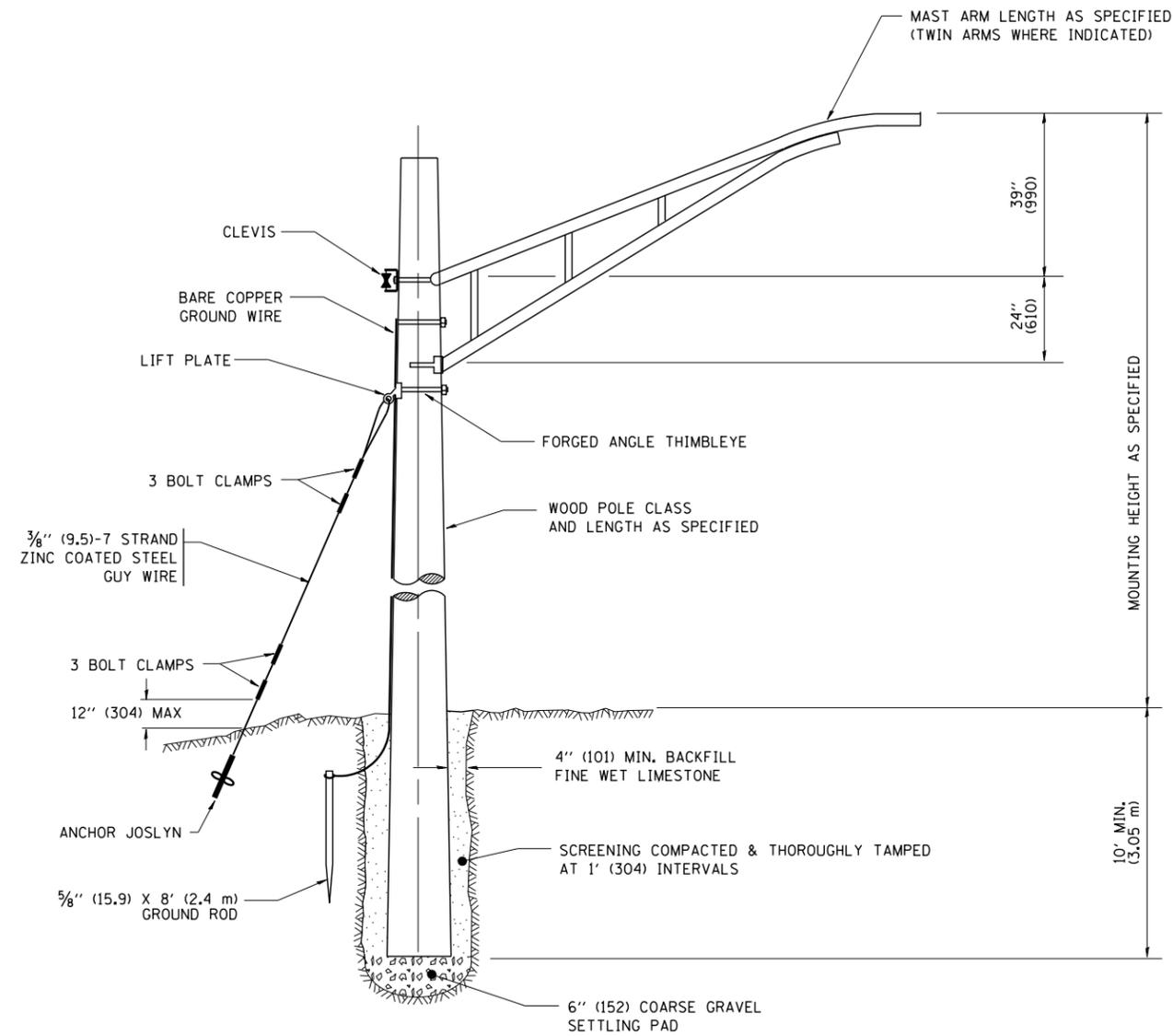
INSTALLATION OF CONDUIT
IN BRIDGE PARAPET EXPANSION JOINT
(N.T.S.)



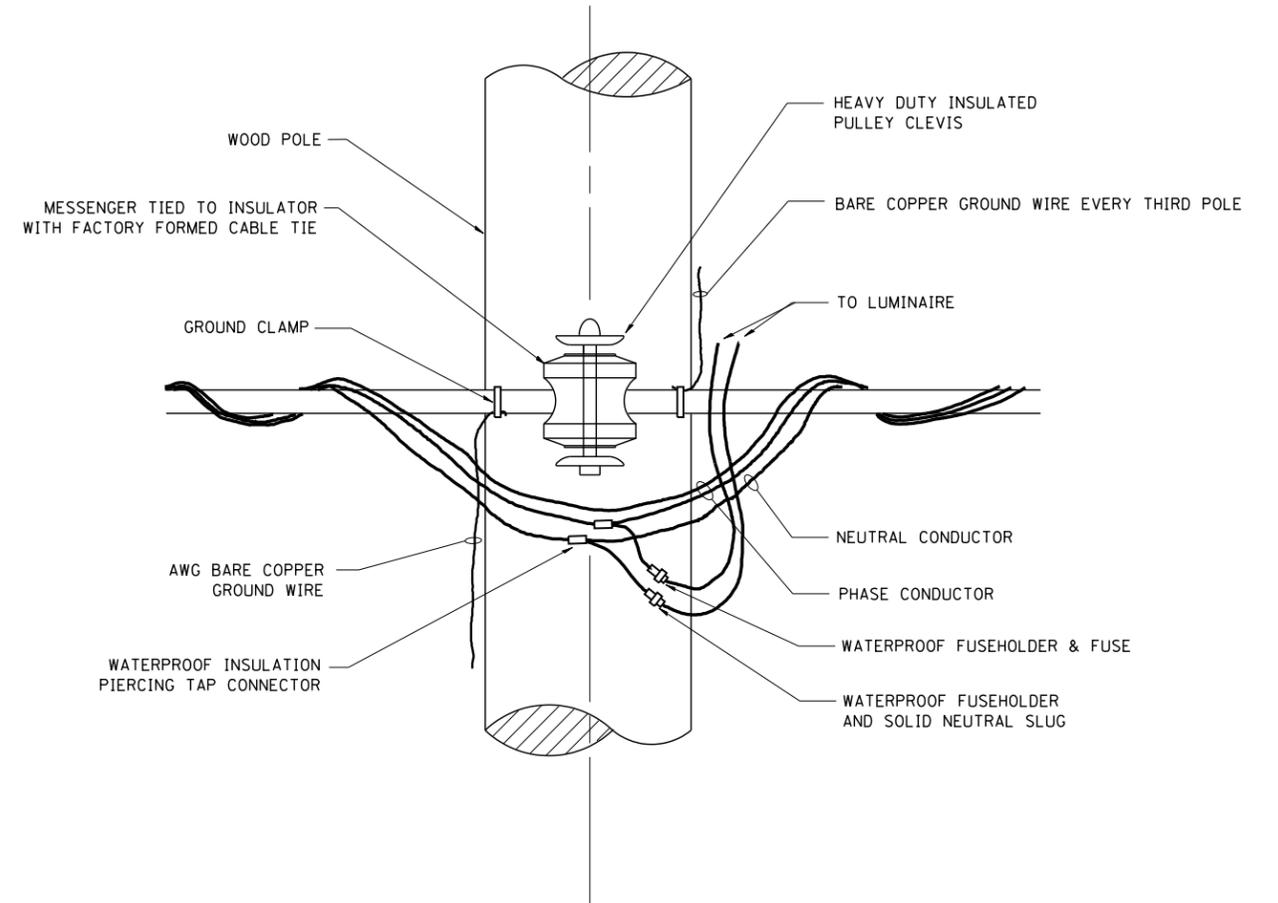
NOTES

1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

ED - BW
JUNCTION BOX EMBEDDED IN BARRIER WALL



TEMPORARY LIGHT POLE DETAIL

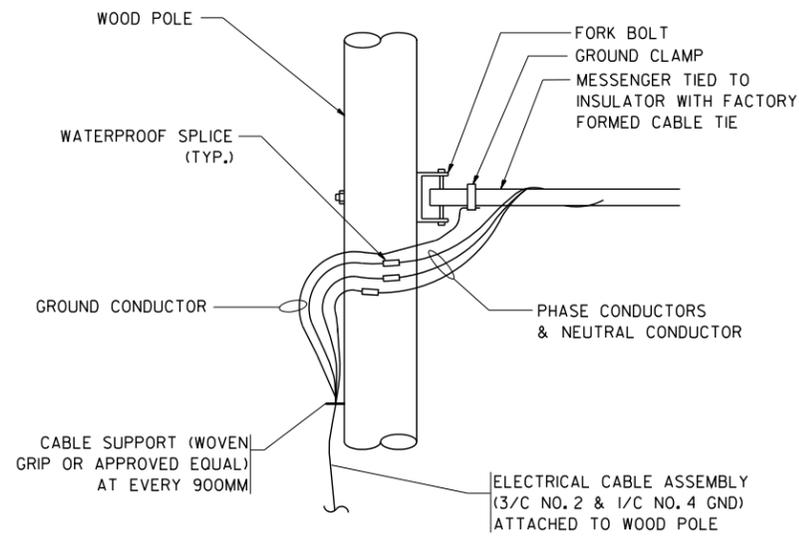


TEMPORARY LIGHT POLE ATTACHMENT DETAIL

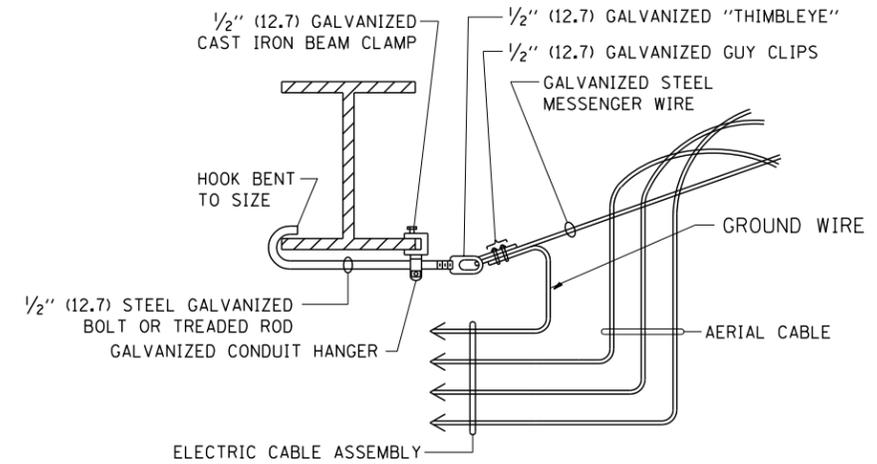
NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED

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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	886	808	CONTRACT NO. 60L70	
PLOT DATE = 1/4/2008	DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



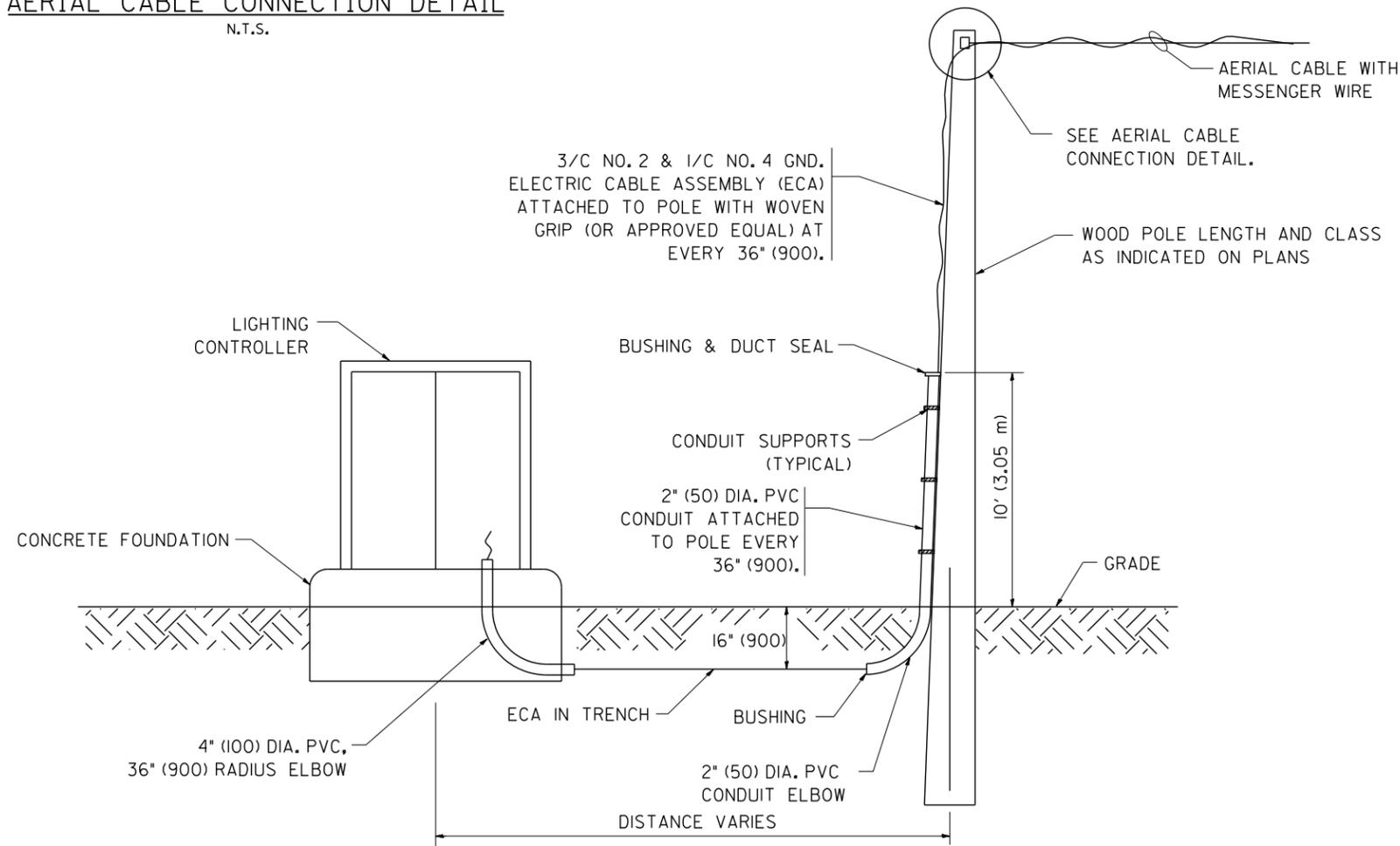
AERIAL CABLE CONNECTION DETAIL
N.T.S.



AERIAL CABLE ATTACHED TO STRUCTURE
NOT TO SCALE

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. SEE PROPOSED LIGHTING PLAN FOR CONDUIT, CABLE AND ROUTING.
3. THE CONTRACTOR SHALL PROVIDE INTERMEDIATE SUPPORTS TO MAINTAIN MINIMUM CLEARANCES. REFER TO AERIAL AERIAL CABLE ATTACHED TO STRUCTURE DETAIL.
4. COST OF SPLICES AND MOUNTING HARDWARE SHALL BE INCLUDED IN THE UNIT PRICE FOR AERIAL CABLE.



WOOD POLE TO LIGHTING CONTROLLER WIRING CONNECTION DETAIL
N.T.S.

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PLOT SCALE = 50.000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED -
DATE -

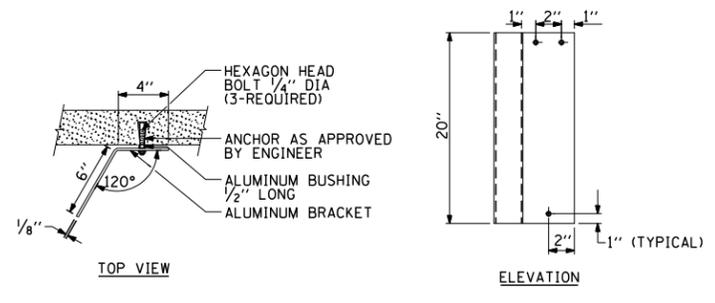
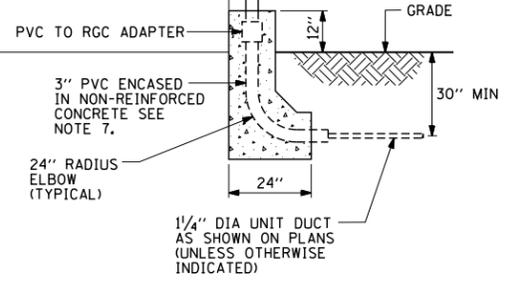
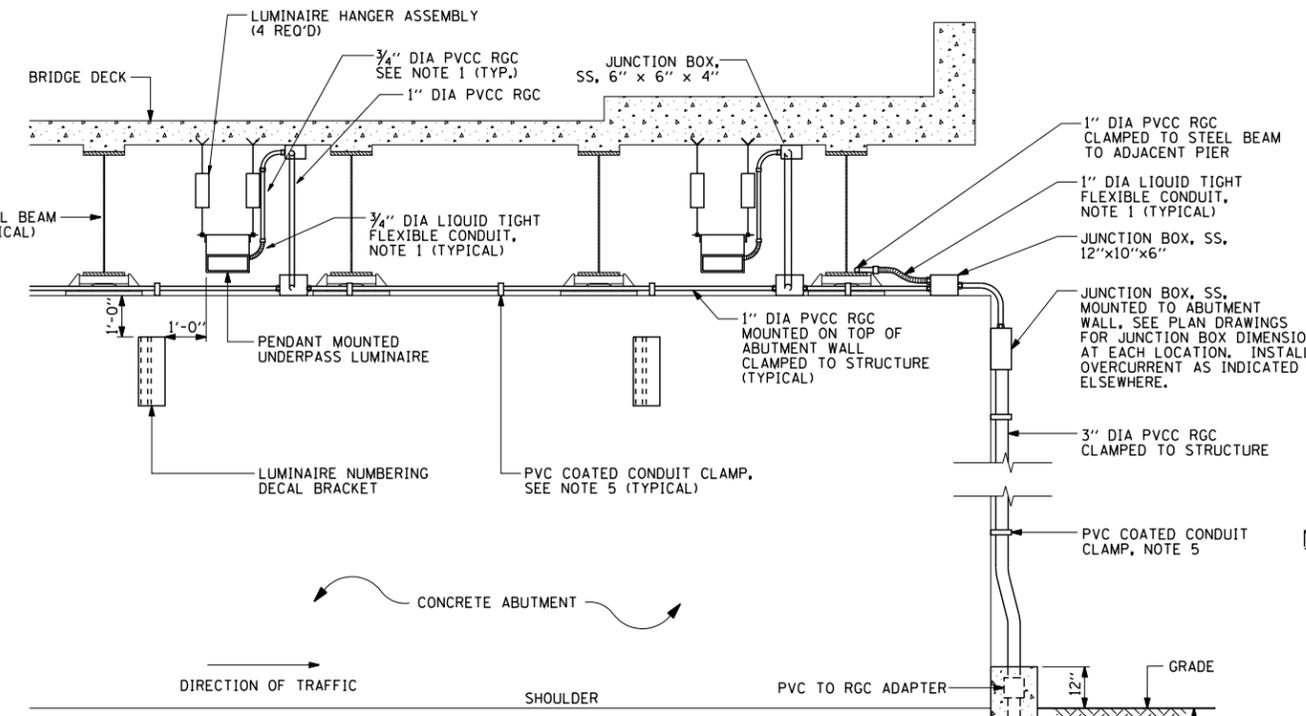
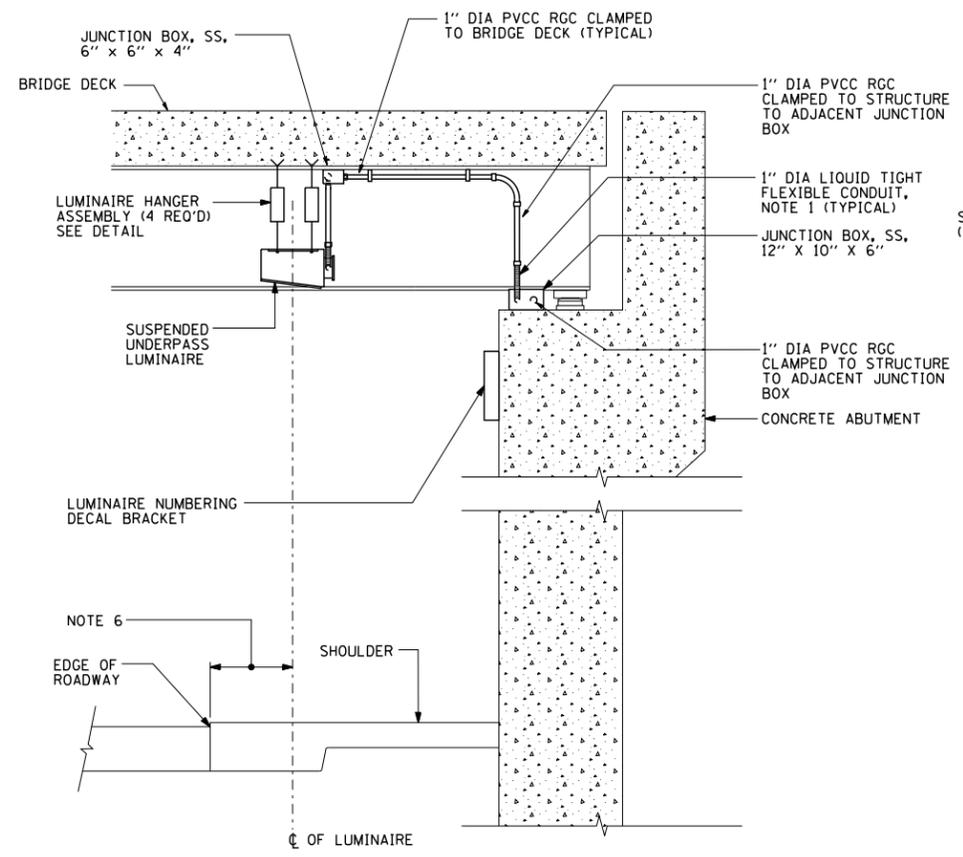
REVISED - 08-08-03
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

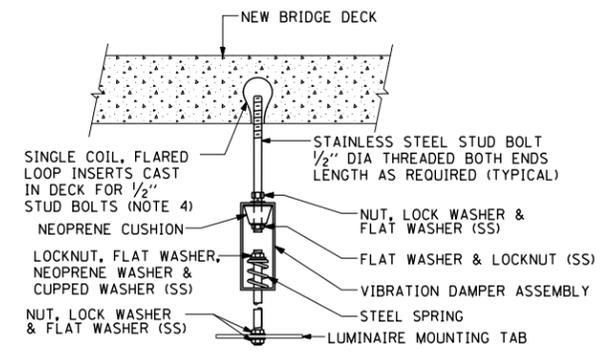
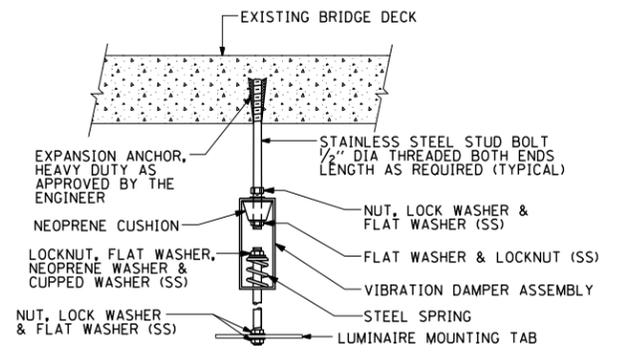
TEMPORARY AERIAL CABLE INSTALLATION

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	809
BE-001		CONTRACT NO. 60L70		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



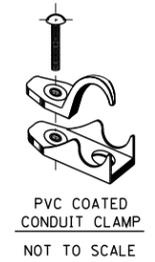
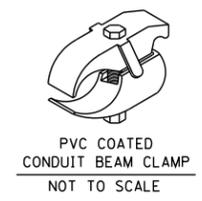
- NOTES:**
- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT 3/4" DIA. CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE COST OF UNDERPASS LUMINAIRE INSTALLATION.
 - SEE UNDERPASS LIGHTING PLANS FOR INSTALLATION LOCATION OF UNDERPASS LIGHTING LUMINAIRES.
 - THE CONTRACTOR SHALL USE APPROVED SINGLE COIL FLARED LOOP INSERTS WHEN SUSPENDED MOUNTING AN UNDERPASS LUMINAIRE TO A NEW BRIDGE DECK. THE FLARED LOOP INSERTS MUST BE CAST INTO THE CONCRETE DECK. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND COORDINATING THE INSERT LOCATIONS FOR MOUNTING THE UNDERPASS LIGHTING SYSTEM AS SHOWN ON THE PLANS WITH THE BRIDGE DECK CONTRACTOR. SEE DETAIL.
 - THE UNDERPASS LUMINAIRE HANGER ASSEMBLY COMPLETE WITH HEAVY DUTY ANCHORS/INSERTS AND ALL APPLICABLE HARDWARE SHALL BE INCLUDED IN THE COST OF THE UNDERPASS LUMINAIRE PAY ITEM.
 - SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
 - ALL UNDERPASS LUMINAIRES MUST BE CENTERED IN THE BEAM SPACE AS INDICATED ON THE PLANS UNLESS OTHERWISE DIRECTED BY THE ENGR. LUMINAIRE SETBACK SHALL BE AS INDICATED IN PLANS FOR EACH SPECIFIC UNDERPASS.
 - THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
 - ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.



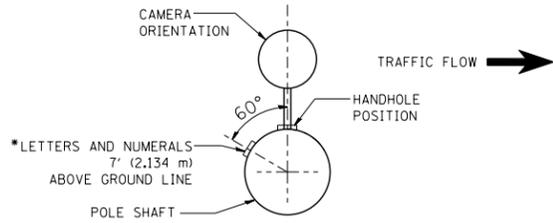
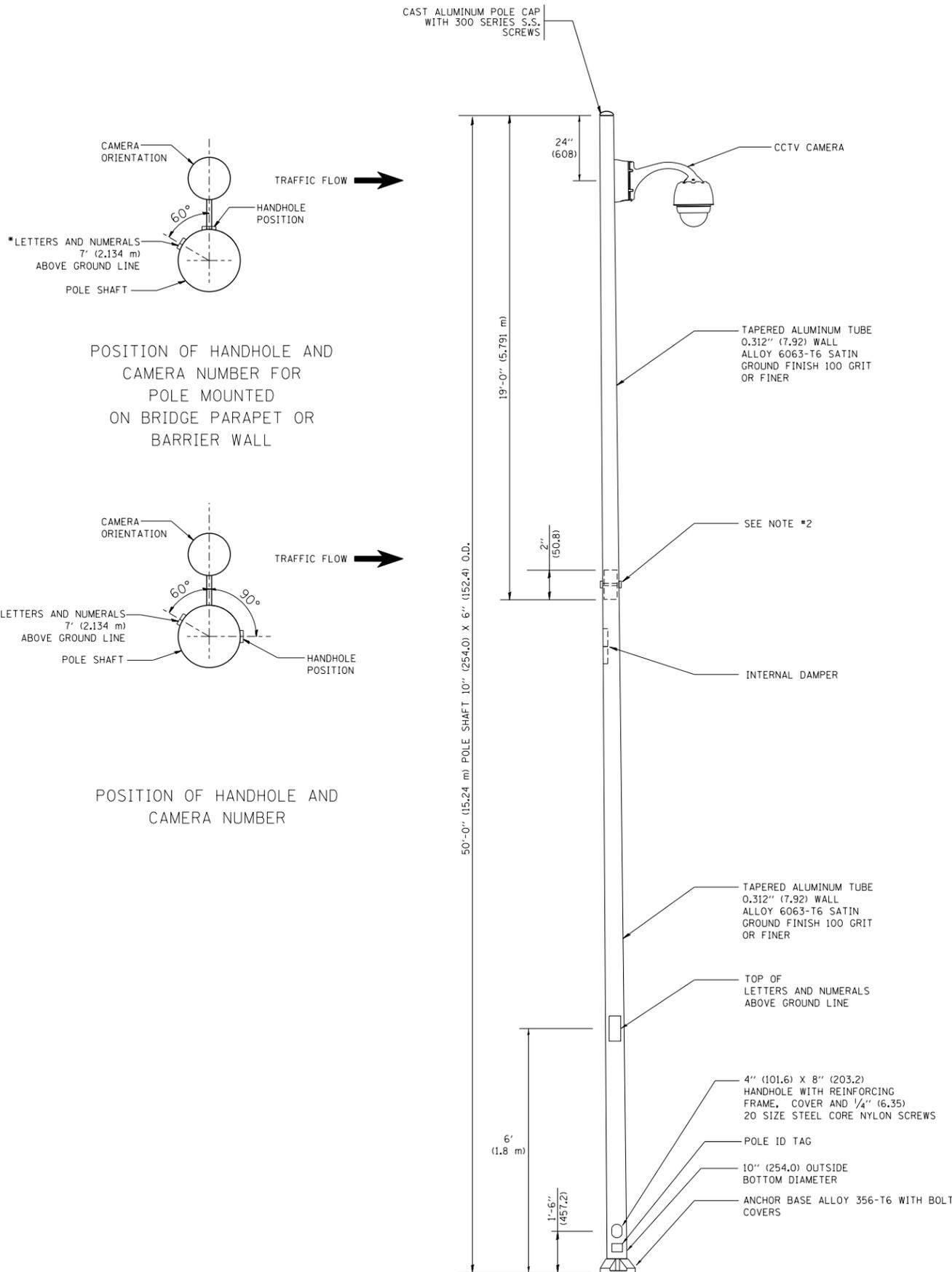
EXISTING BRIDGE DECK INSTALLATION

NEW BRIDGE DECK INSTALLATION

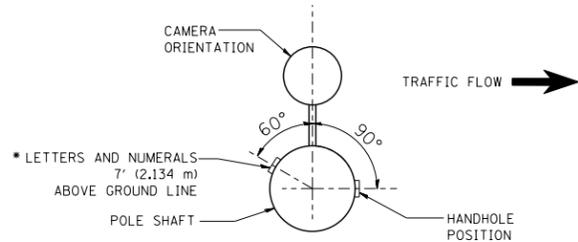
TYPICAL LUMINAIRE HANGER ASSEMBLY DETAILS



FILE NAME = W:\diststd\22x34\be900.dgn	USER NAME = gaglionobt	DESIGNED -	REVISED - 12-12-05	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUSPENDED MOUNT UNDERPASS LUMINAIRE INSTALLATION DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BE-900	CONTRACT NO. 60L70	886	810
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -									
		DATE -	REVISED -						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



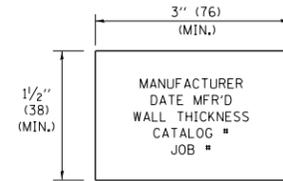
POSITION OF HANDHOLE AND CAMERA NUMBER FOR POLE MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



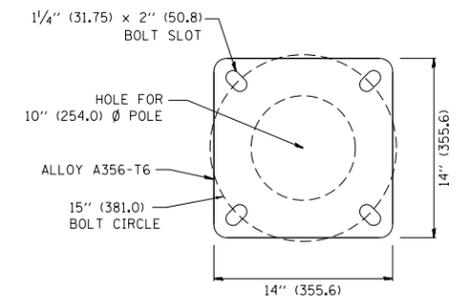
POSITION OF HANDHOLE AND CAMERA NUMBER

NOTES:

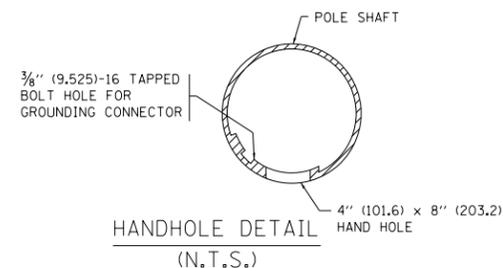
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
3. THE POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
4. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR. BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
5. POLES WILL BE INSTALLED IN ACCORDANCE TO MANUFACTURER'S INSTRUCTIONS.
6. POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.



POLE ID TAG
NTS



POLE BASE PLATE DETAIL
15 INCH (381.0) BOLT CIRCLE



FILE NAME =	USER NAME = bauerdl	DESIGNED -	REVISED - R. TOMSONS 09-06-00
ct:\pw\work\p\dot\bauerdl\d0108315\be1000.dgn		DRAWN -	REVISED - R. TOMSONS 09-03-03
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - R. TOMSONS 02-27-13
	PLOT DATE = 2/27/2013	DATE -	REVISED -

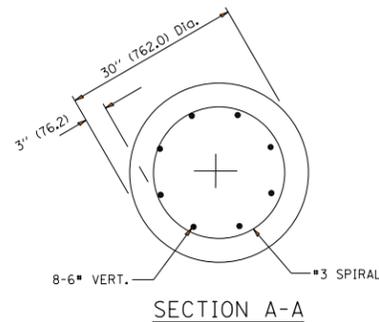
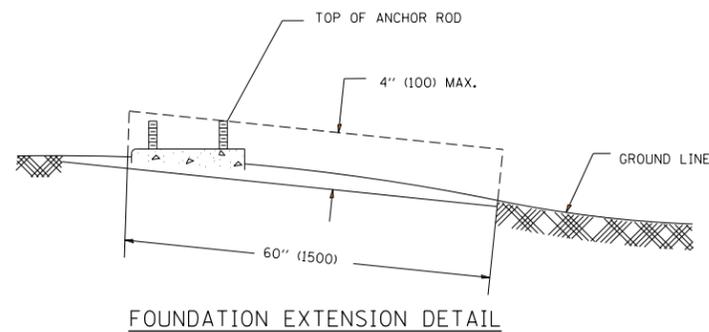
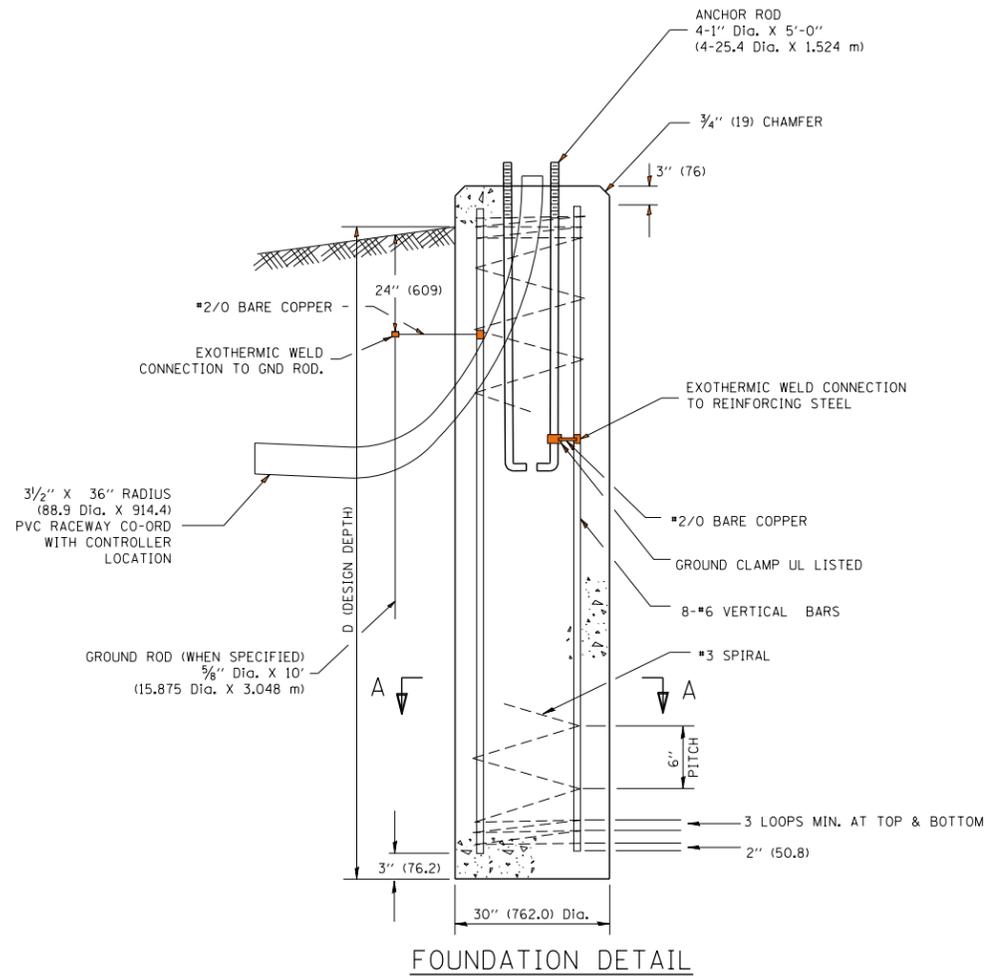
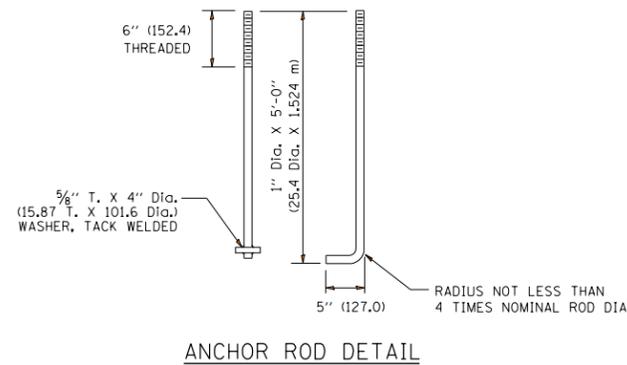
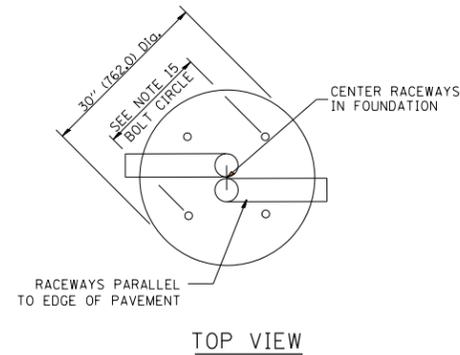
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CCTV CAMERA STRUCTURE			
50' (15.24 m) MOUNTING HEIGHT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	811
BE-1000			CONTRACT NO. 60L70	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

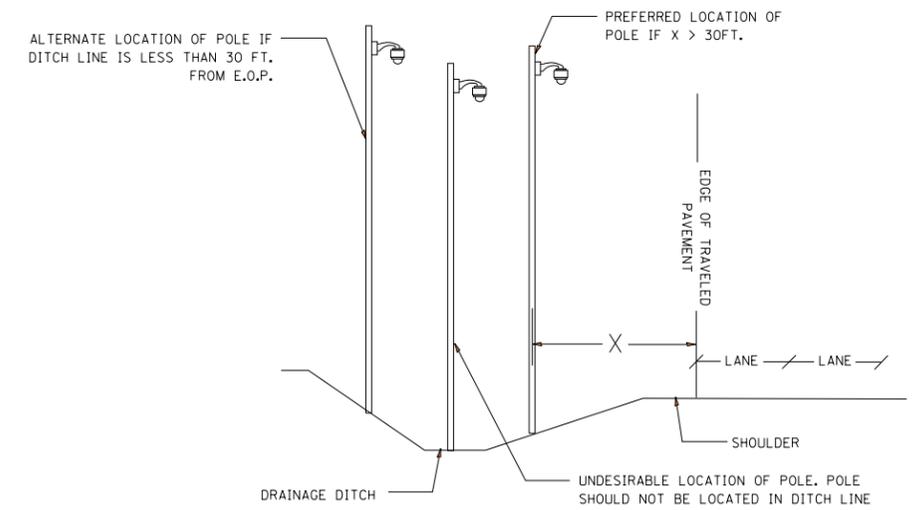
CCTV CAMERA POLE FOUNDATION DEPTH TABLE

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION
SOFT CLAY Qu = 0.375 TON/SQ. FT.	13'-0" (3.96 m)
MEDIUM CLAY Qu = 0.75 TON/SQ.FT	9'-6" (2.09 m)
STIFF CLAY Qu = 1.50 TON/SQ. FT.	7'-0" (2.13 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)

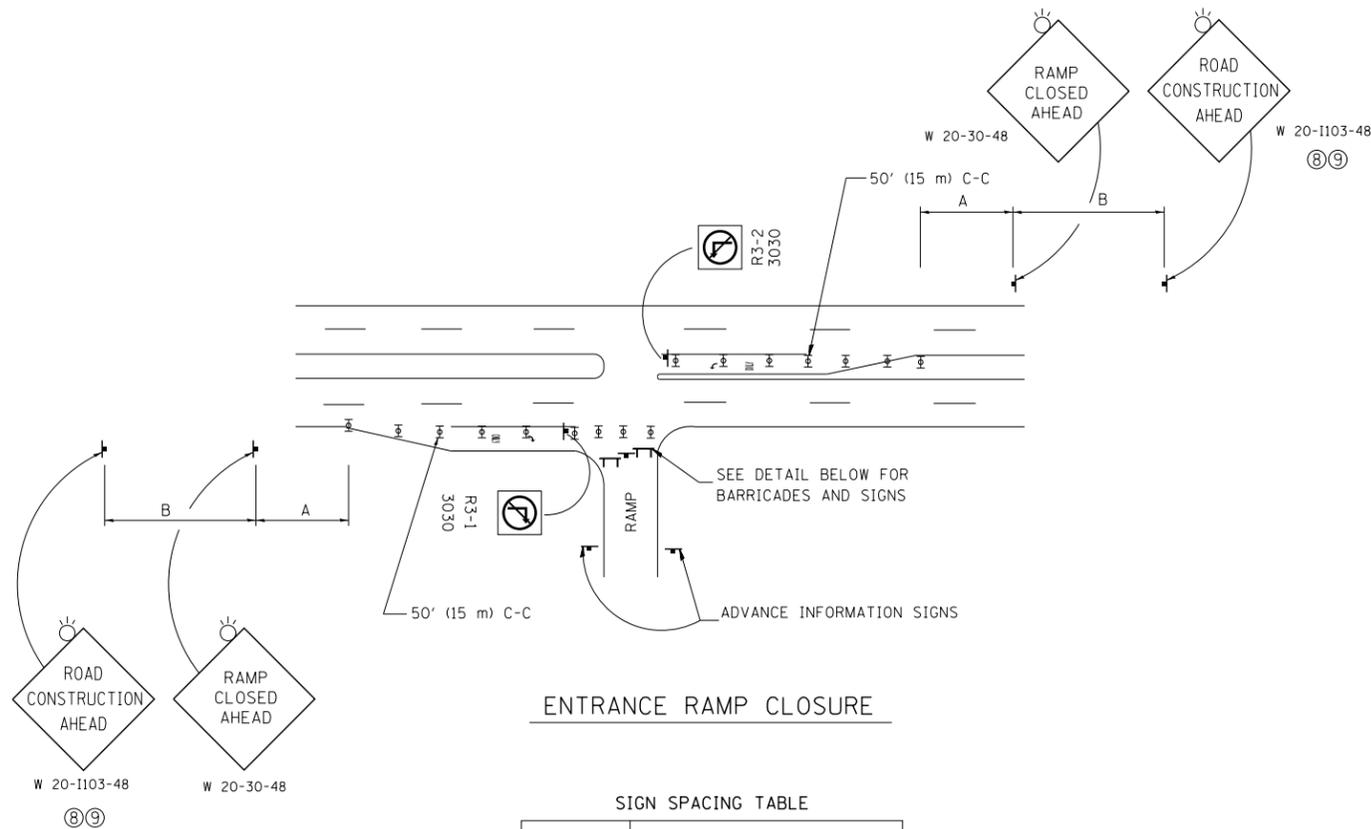


NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.
- ANCHOR ROD BOLT CIRCLE TO BE COORDINATED WITH CAMERA STRUCTURE



FILE NAME =	USER NAME = footemj	DESIGNED - TOMSONS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CCTV CAMERA STRUCTURE FOUNDATION 50' (15 - 24m) MOUNTING HEIGHT			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p\dot\footemj\d0108315\bel100.dgn		DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BE-1001	CONTRACT NO. 60L70	886	812
		CHECKED -	REVISED -									
		DATE - 03-11-13	REVISED -						FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

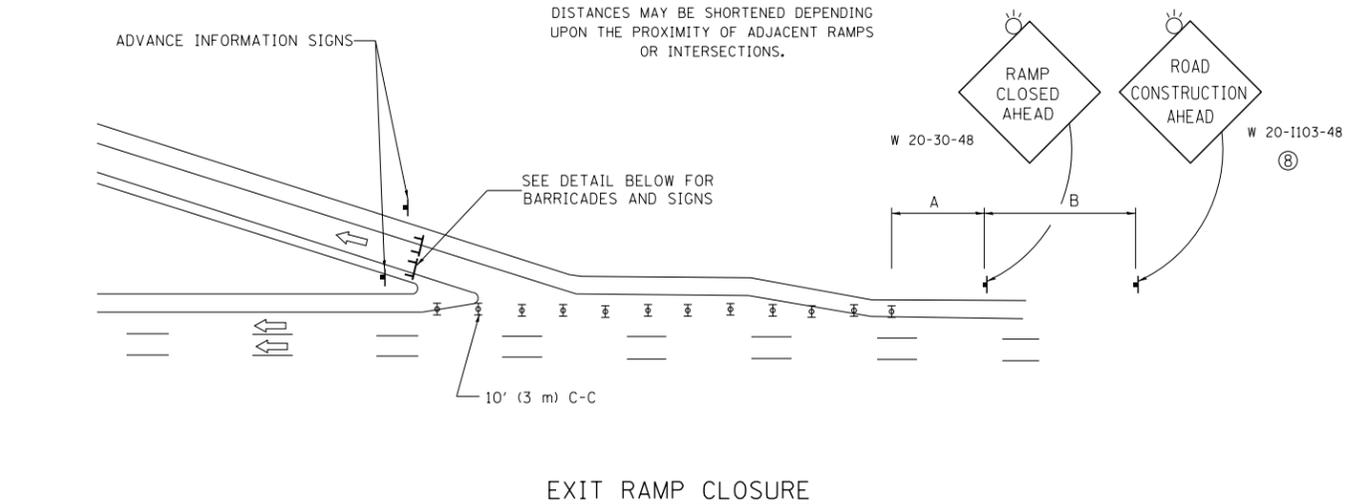


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

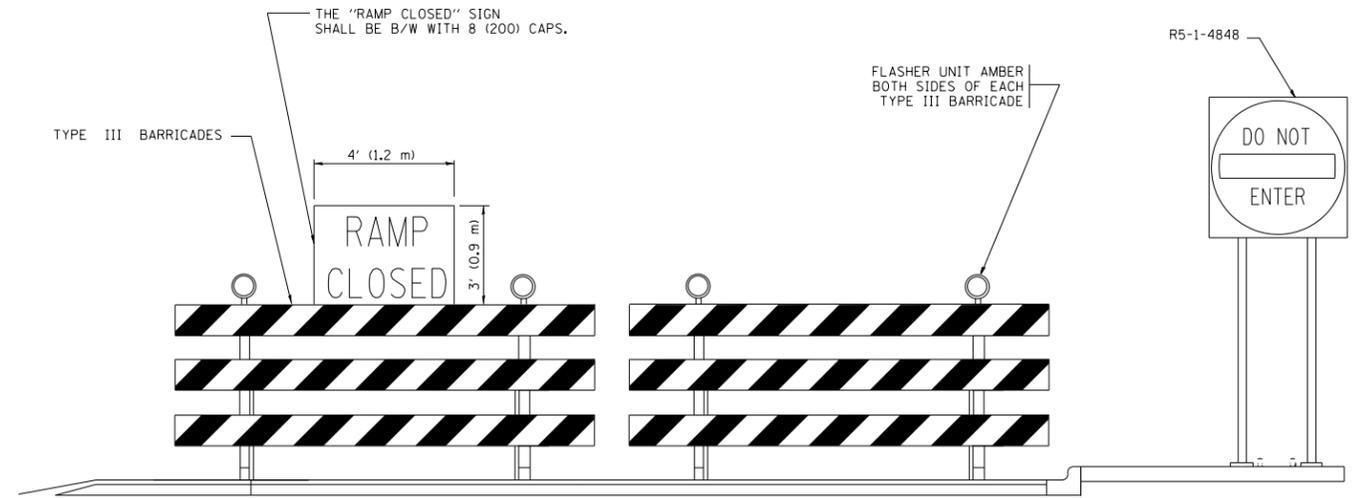
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

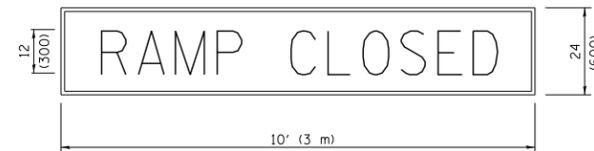
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊥ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



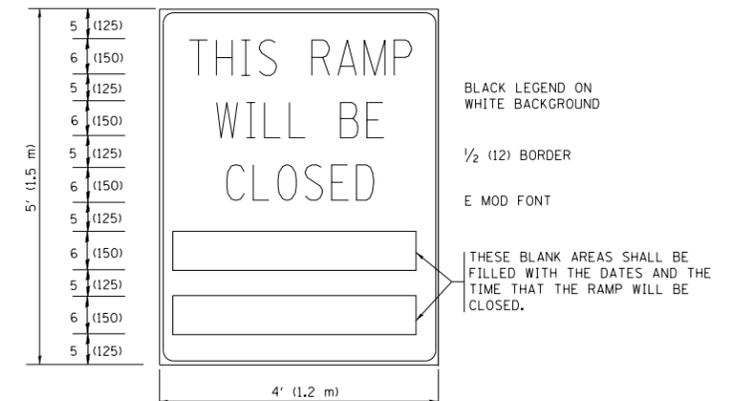
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY E MOD FONT 1 (25) BORDER
 THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = footemj	DESIGNED - DWS	REVISED - JAF 02-06
ct:\pwork\pwork\dot\footemj\d0108315\tc08.dgn		DRAWN -	REVISED - SPB 01-07
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - SPB 12-09
	PLOT DATE = 7/8/2013	DATE - 02-83	REVISED - MD 06-13

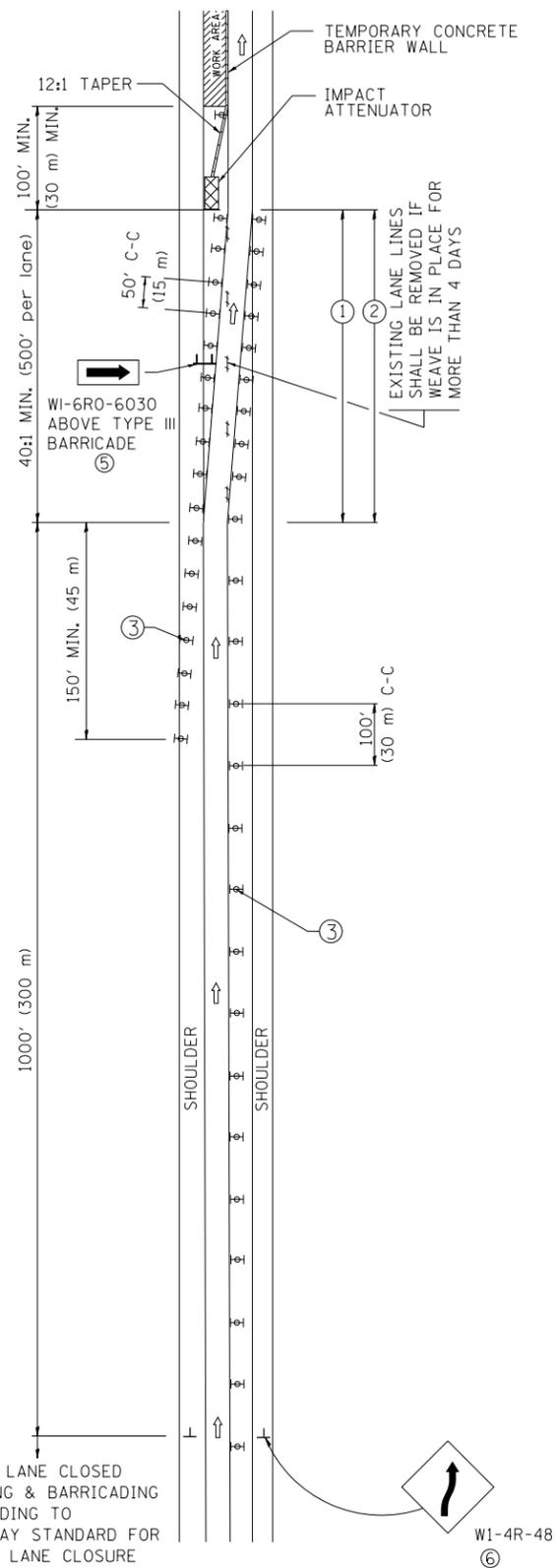
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

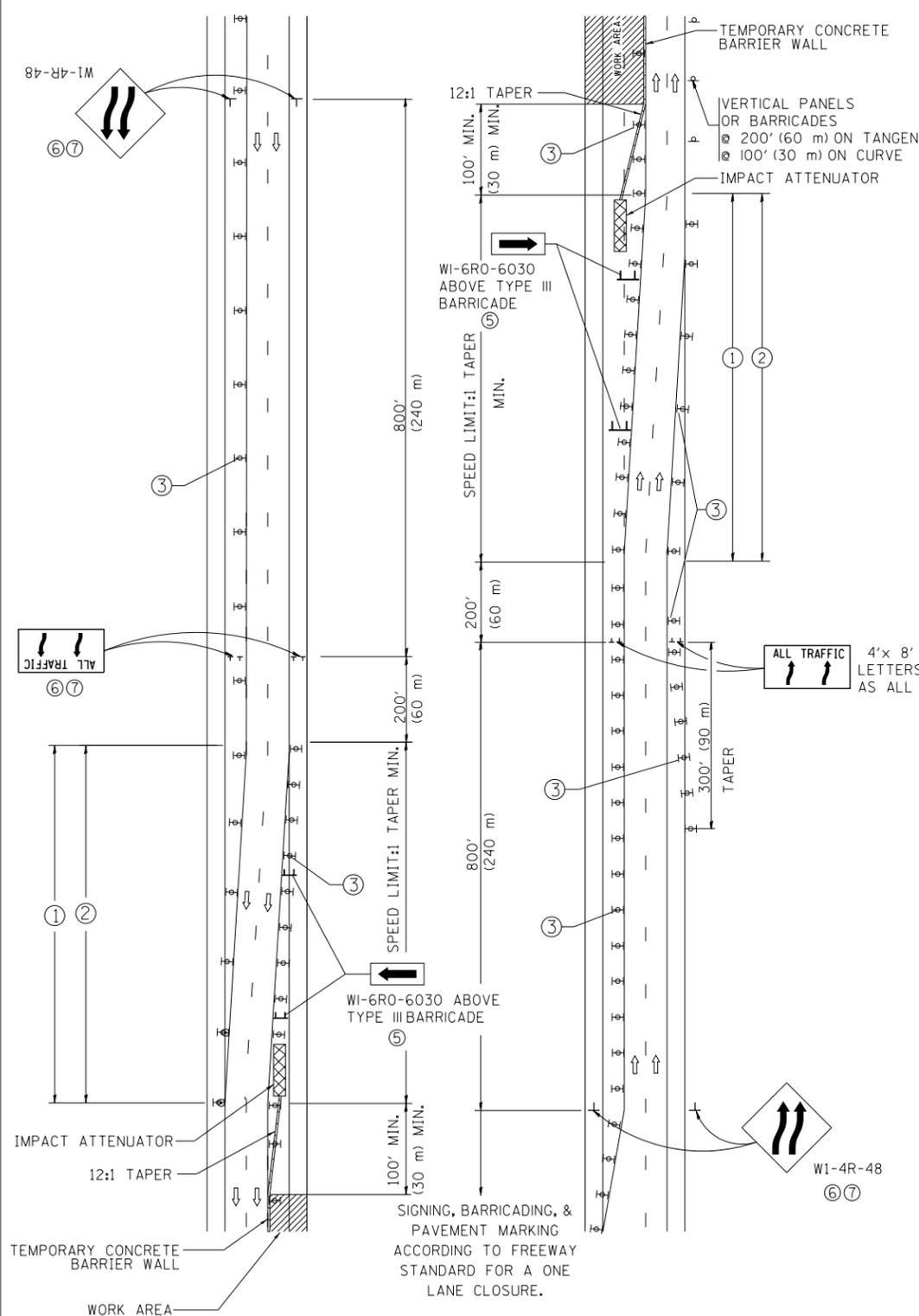
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-08		886	813
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L70	

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

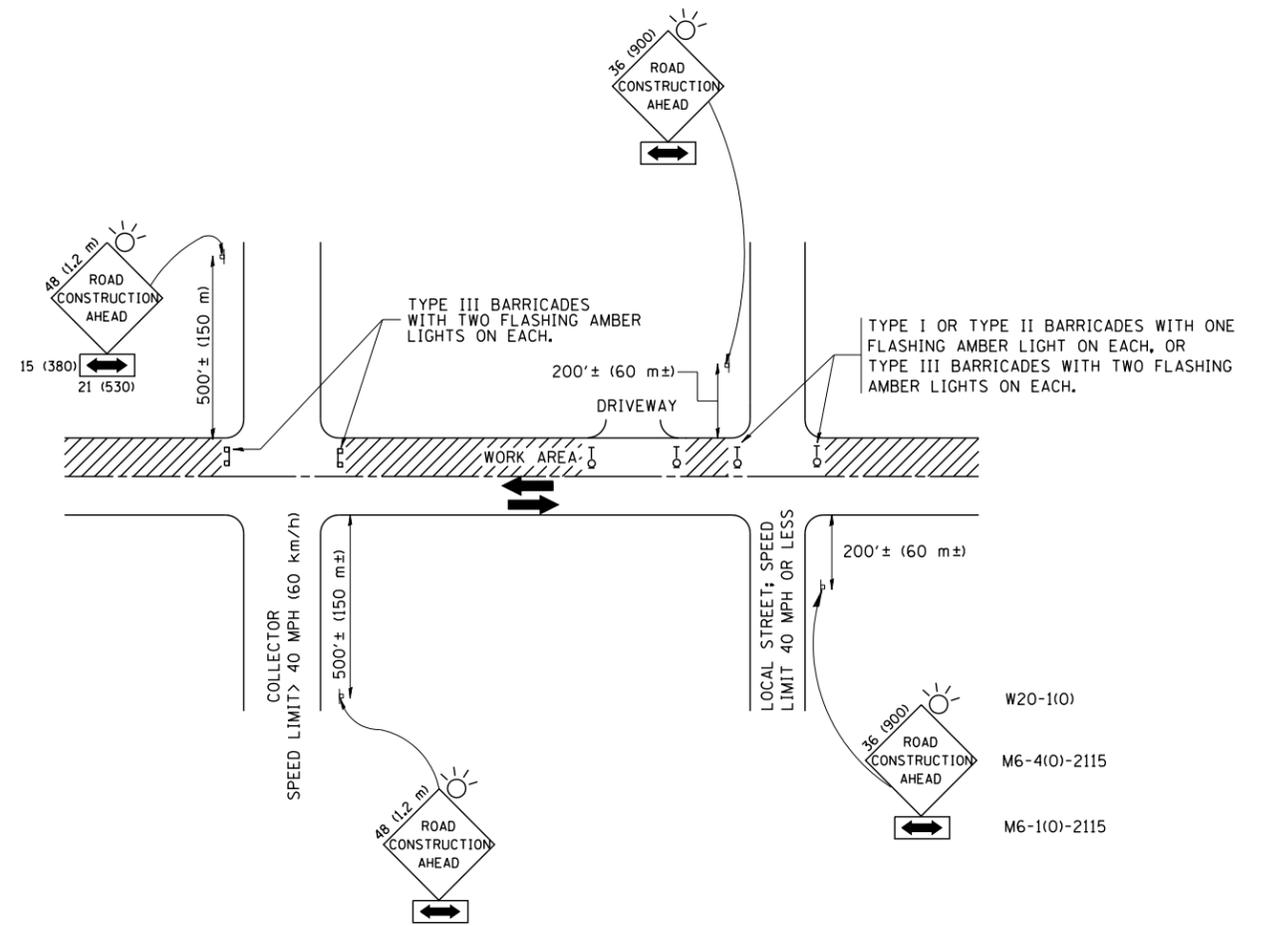
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. WI-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48 (6, 7)
- W24-1-48 (7)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = footemj	DESIGNED - DWS	REVISED - JAF 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p\dot\footemj\d0108315\tc09.dgn	DRAWN -	REVISED - SPB 01-07	REVISED - SPB 12-09								886	814
PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - MD 06-13			TC-09			CONTRACT NO. 60L70				
PLOT DATE = 7/1/2013	DATE - 02-87				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

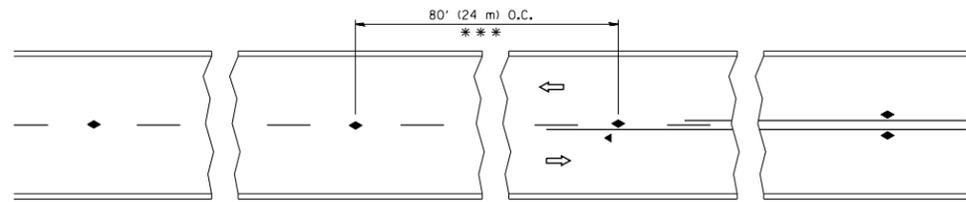
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

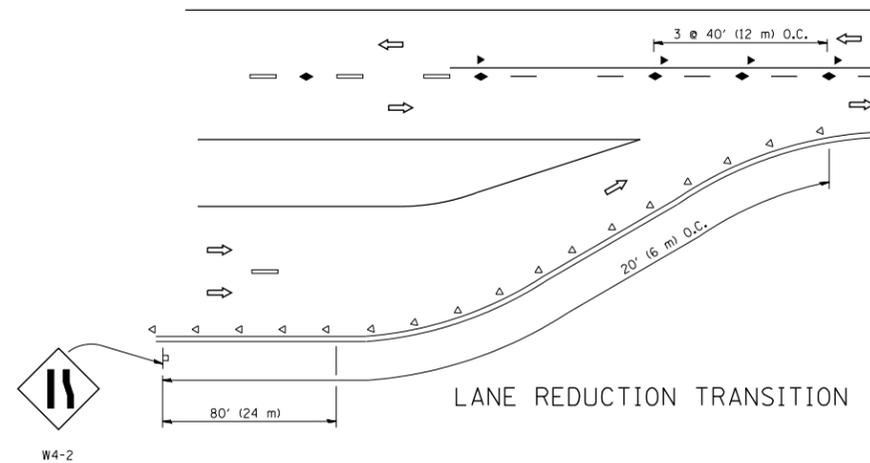
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	815
TC-10			CONTRACT NO. 60L70	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

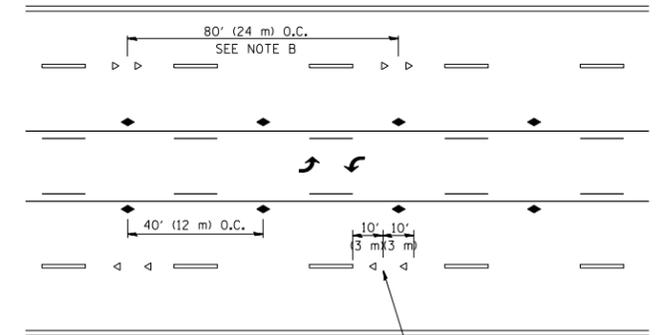


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

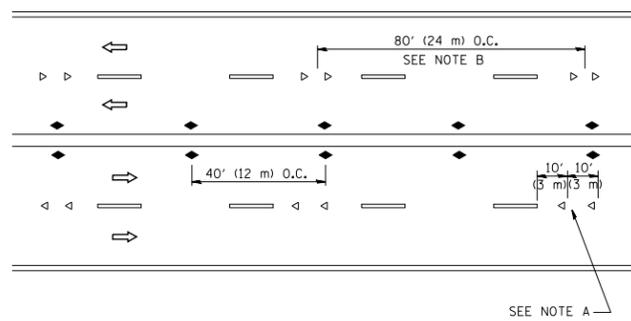
TWO-LANE/TWO-WAY



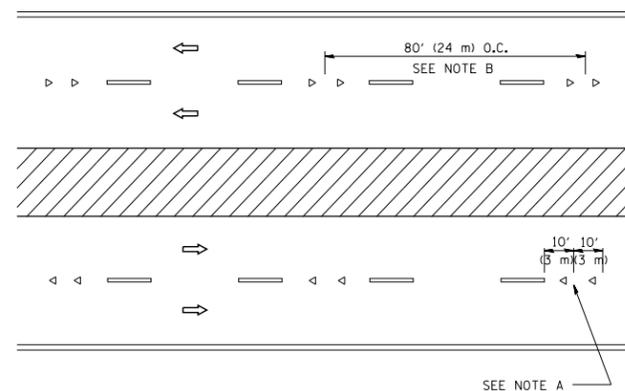
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

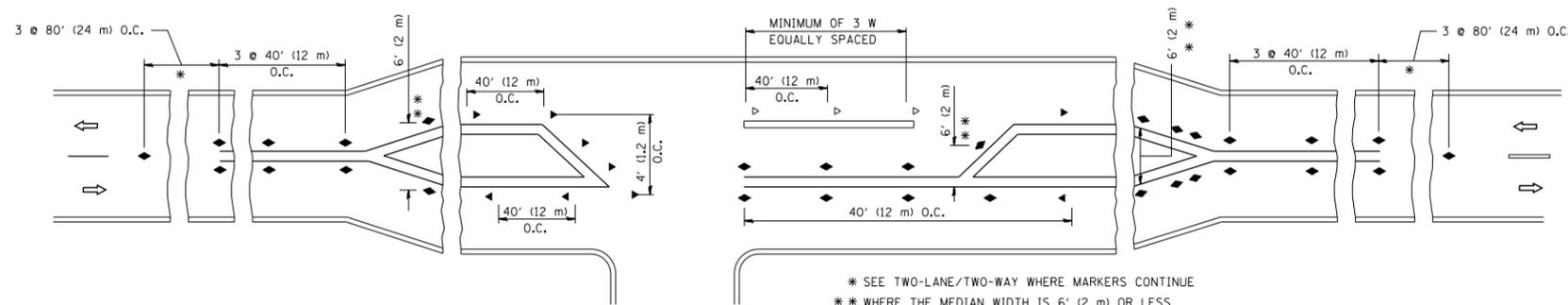
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

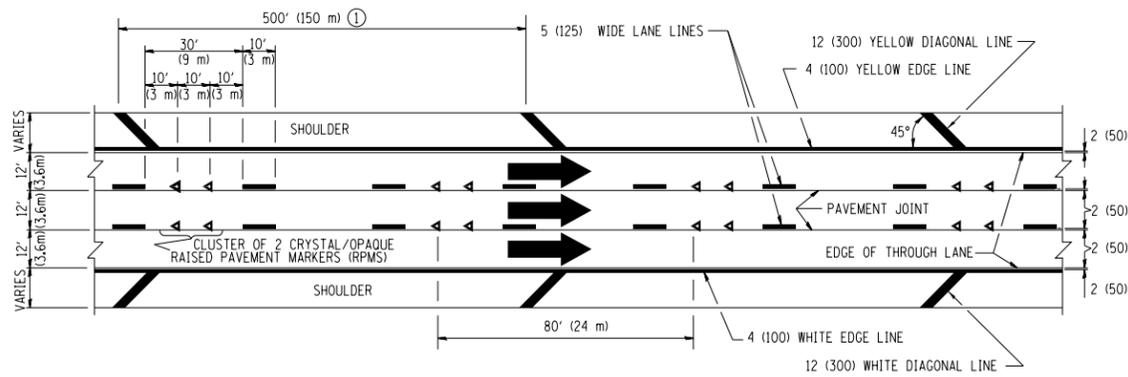
All dimensions are in inches (millimeters) unless otherwise shown.

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		CHECKED -	REVISED - T. RAMMACHER 01-06-00
		DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

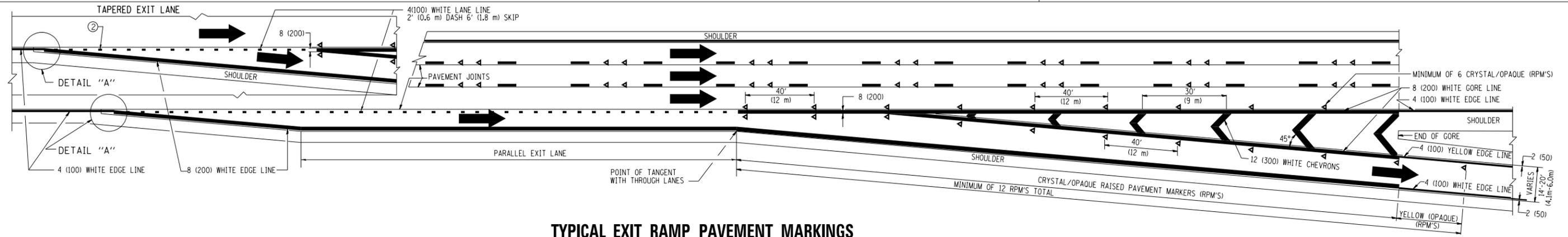
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	816
TC-11			CONTRACT NO. 60L70	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

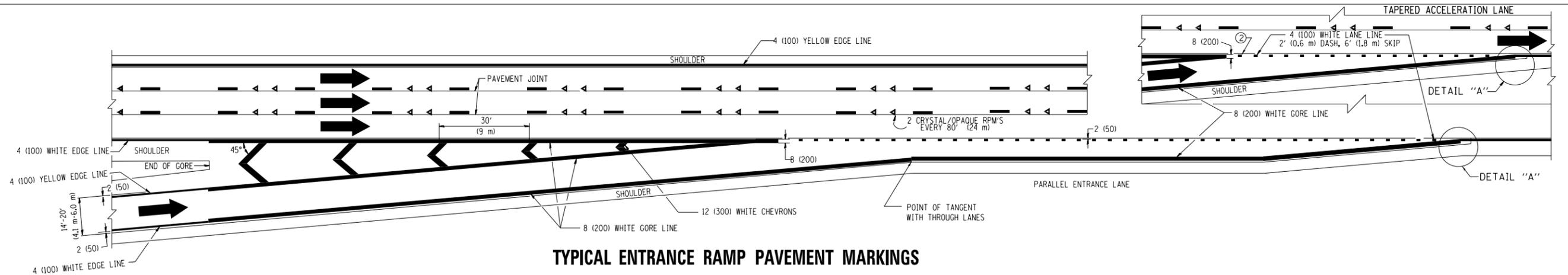


TYPICAL EDGE LINES & LANE LINES

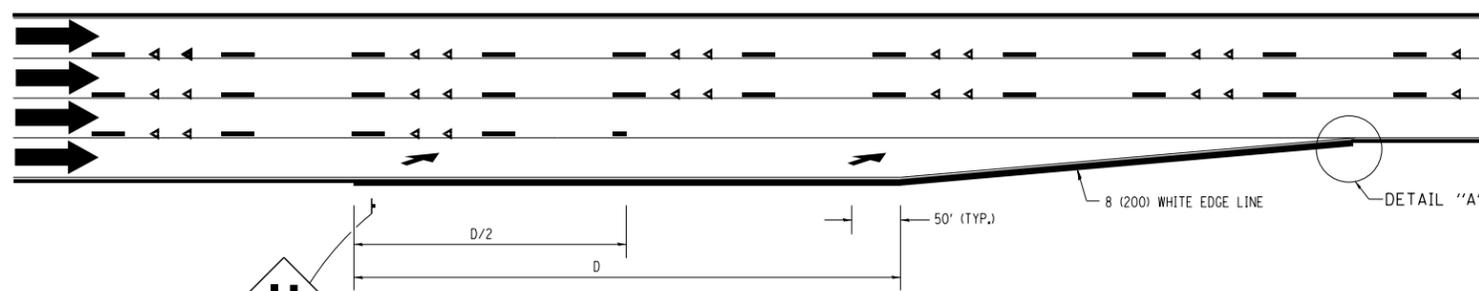
- PAVEMENT MARKING MATERIALS**
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
 2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE; INLAID OR GROOVED IN SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENT PROJECTS.
 3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC PROJECTS.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

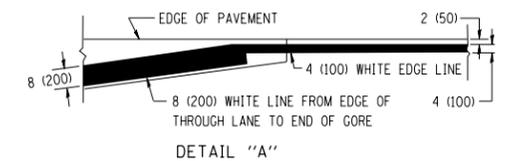


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



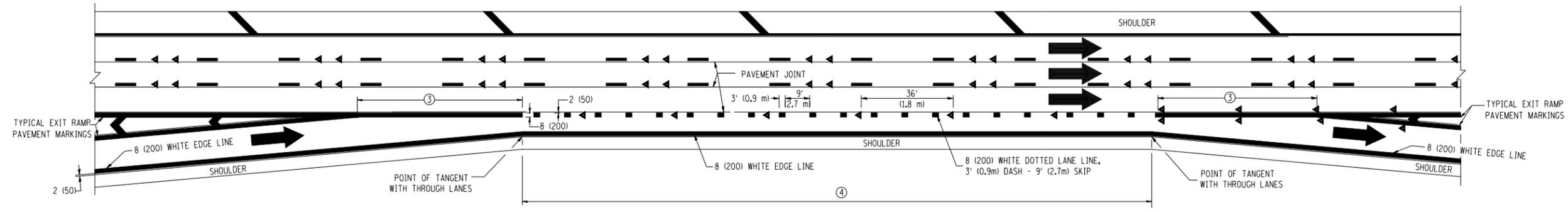
- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

FILE NAME =	USER NAME = footemj	DESIGNED - D.W.S.	REVISED - J.A.F. 02-06
ct:\pwork\pwork\footemj\d0108315\tcl2.dgn		DRAWN -	REVISED - S.P.B. 01-07
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - S.P.B. 01-10
	PLOT DATE = 7/8/2013	DATE - 01-90	REVISED - M.D. 05-13

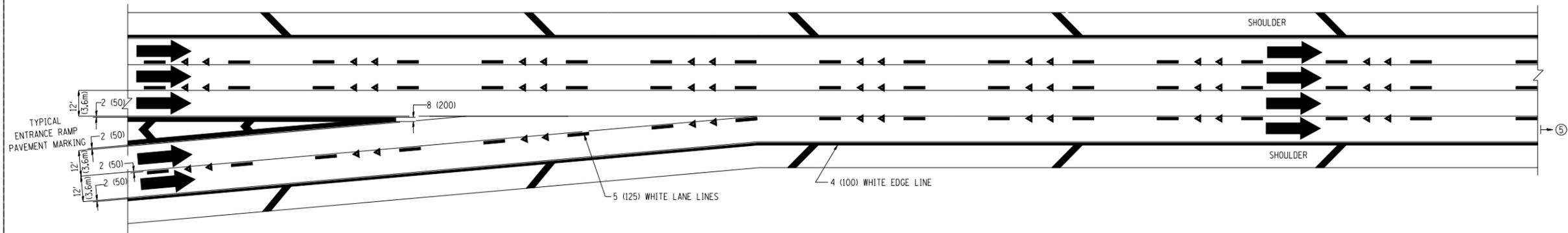
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS	
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STA.	TO STA.

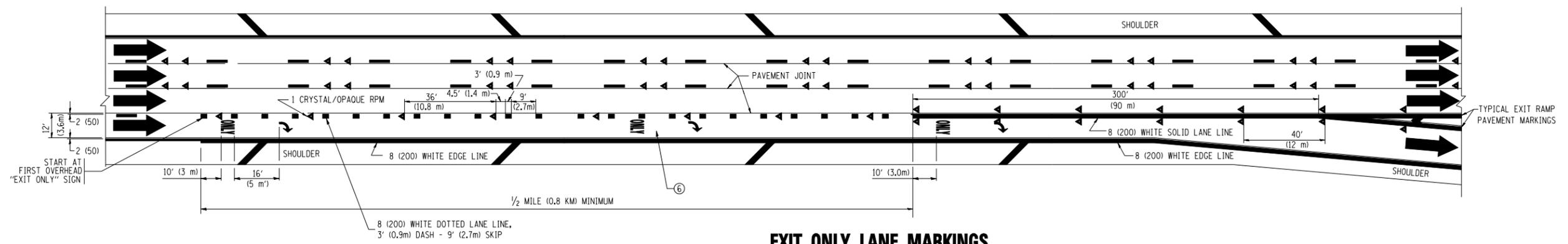
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	TC-12		886	817
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L70	



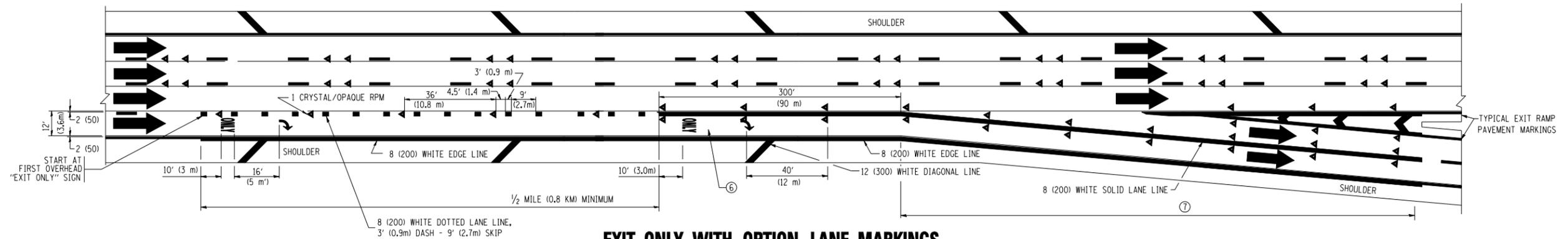
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

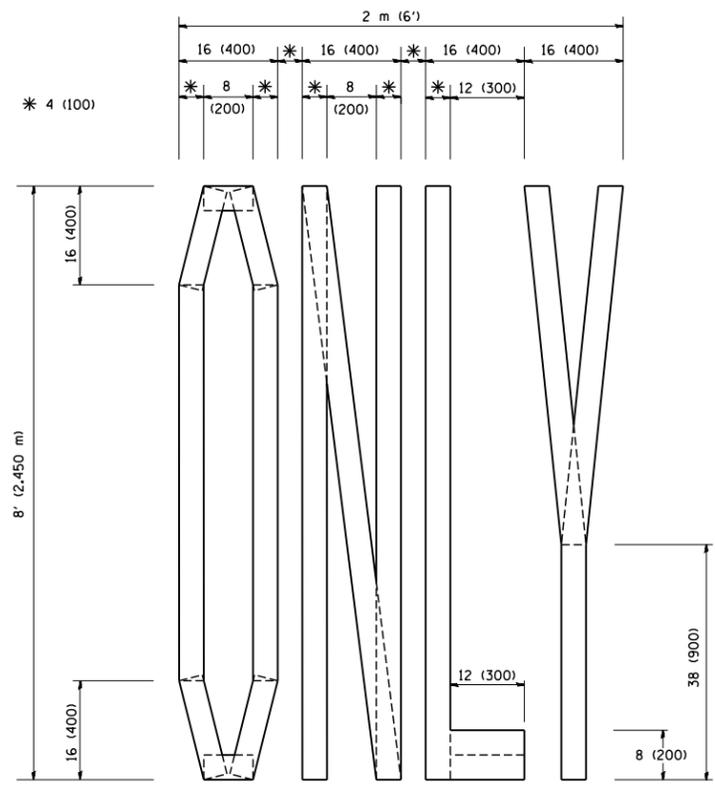
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = leysa	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96
ce:\pwwork\pwidot\LEYSAd0108315\c12.dgn		DRAWN -	REVISED - J.A.F. 02-06
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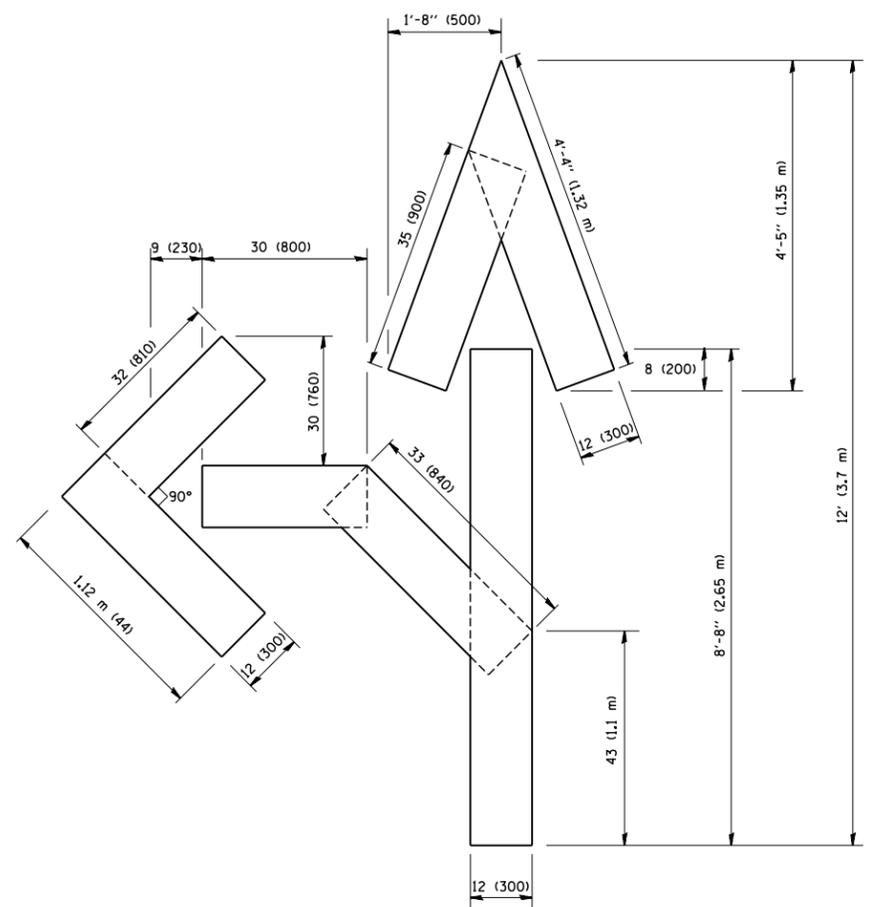
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			
SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

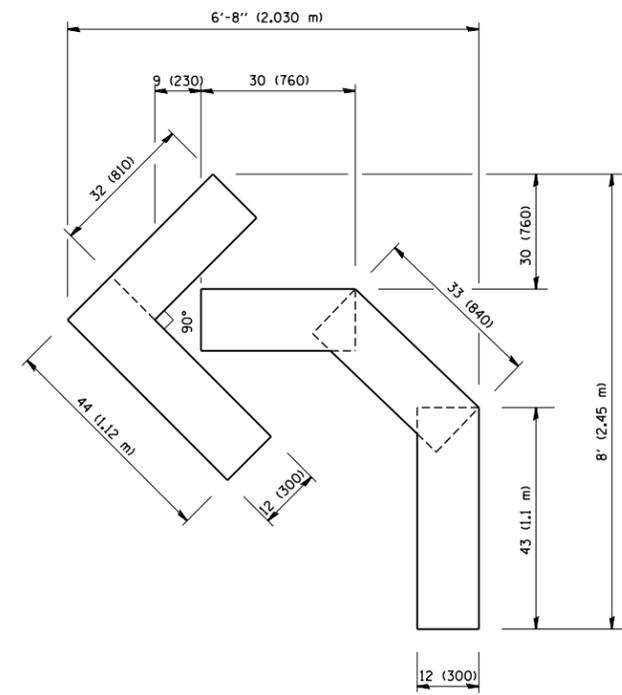
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	818
TC-12		CONTRACT NO. 60L70		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\16.dgn	USER NAME = gaglionobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

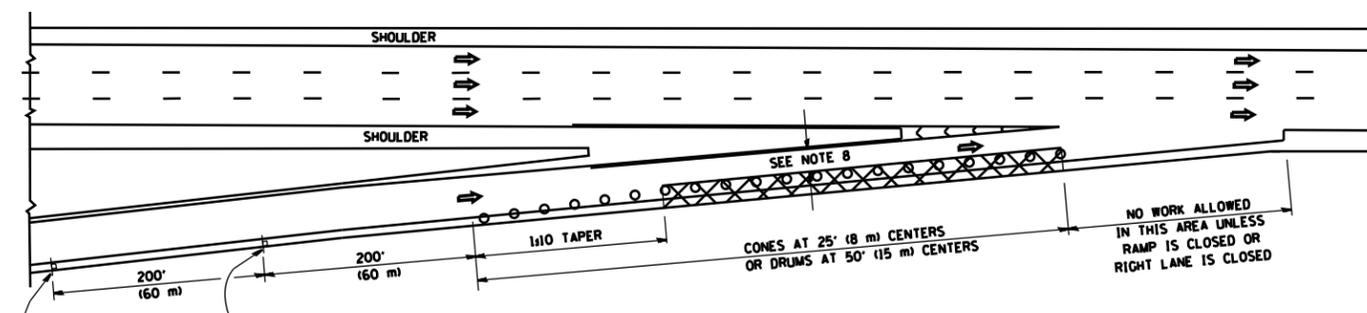
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

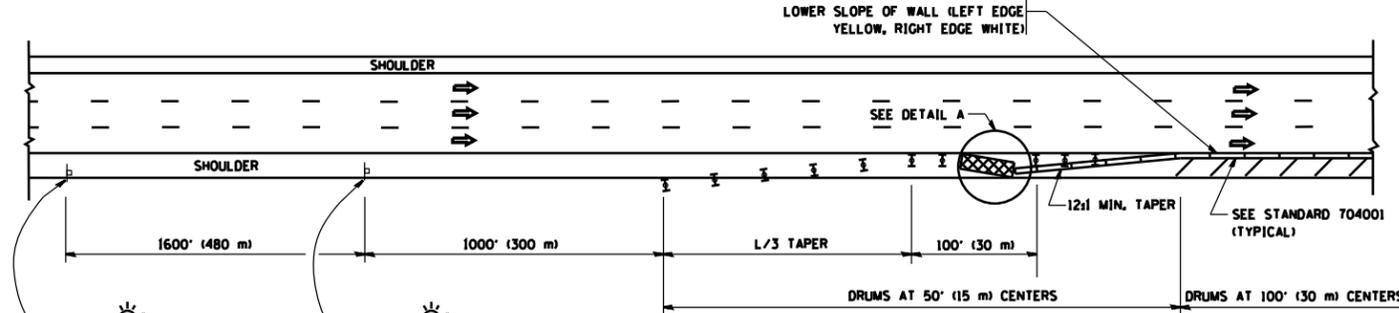
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TC-16			CONTRACT NO. 60L70	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PARTIAL RAMP CLOSURE DETAILS

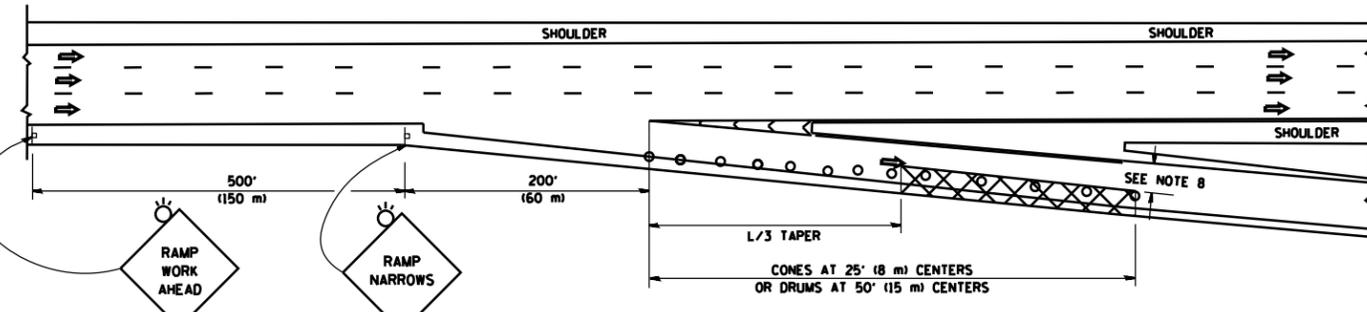
SHOULDER CLOSURE DETAILS



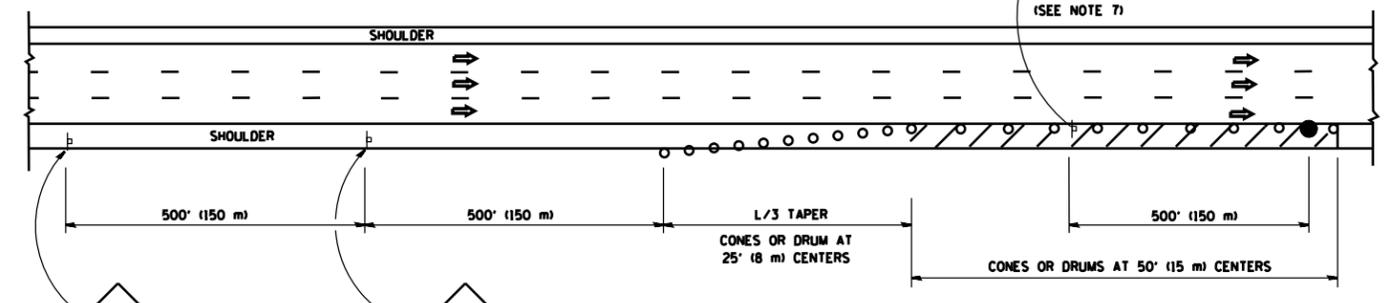
TYPICAL ENTRANCE RAMP



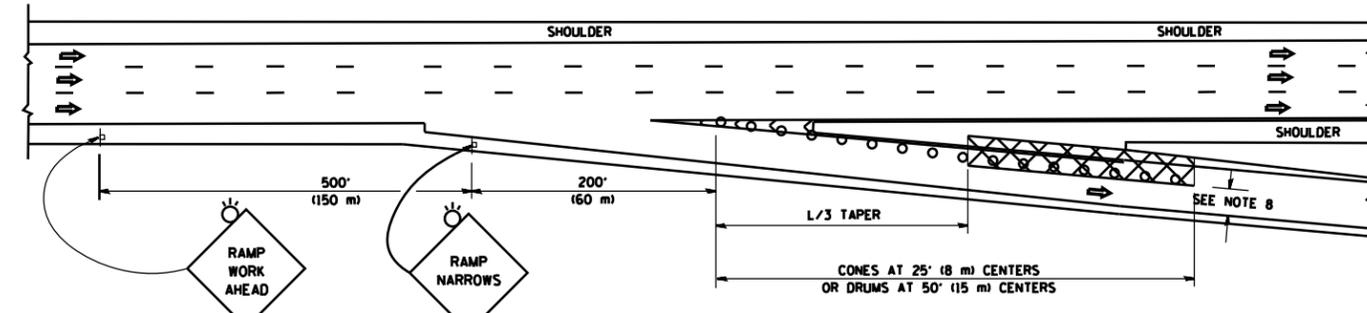
PERMANENT SHOULDER CLOSURE



TYPICAL EXIT RAMP



DAYTIME SHOULDER CLOSURE



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L = 0.65(WNS)$ $L = (WNS)$

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350/MASH COMPLIANT.

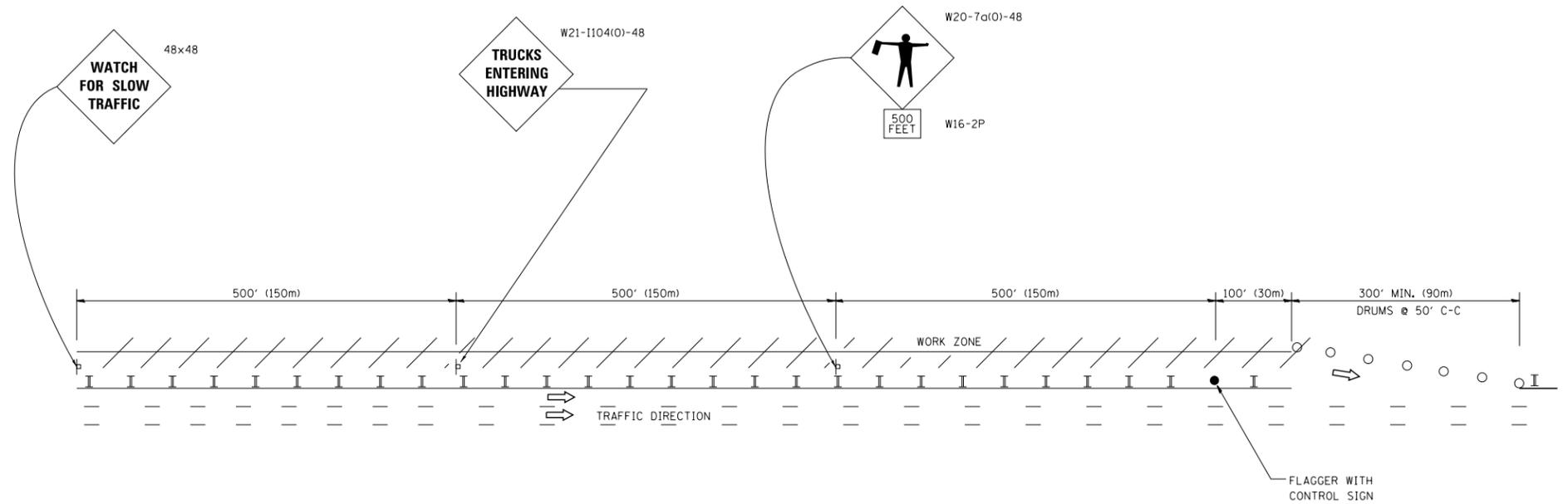
DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

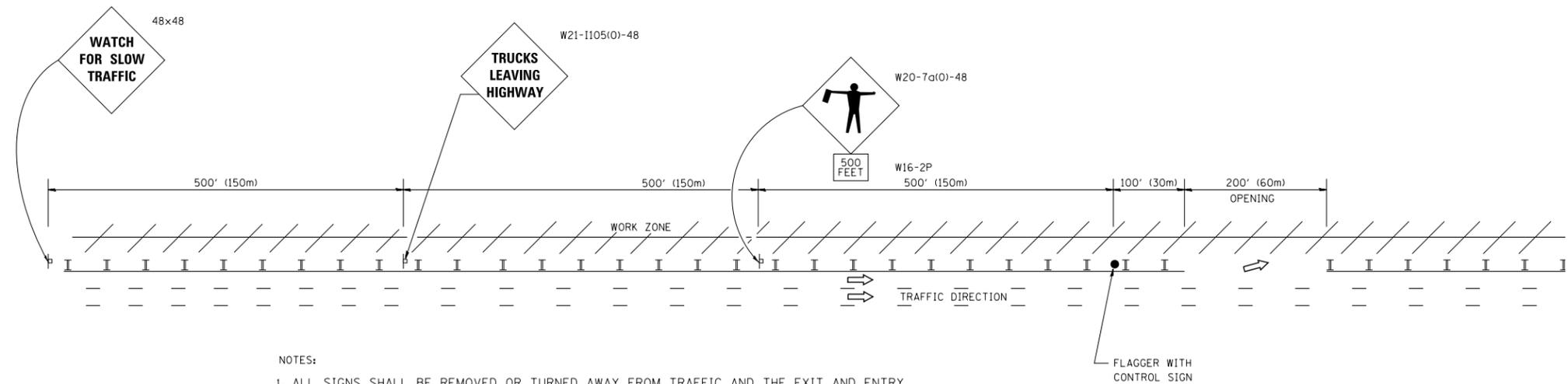
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		CHECKED -	REVISED - S.P.B. 12-09									
		DATE - 11-96	REVISED - M.D. 06-13									
											CONTRACT NO. 60L70	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - J.A.F. 02-06
ct:\pw\work\p\dot\footemj\d0108315\tc18.dgn		DRAWN -	REVISED - S.P.B. 01-07
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - S.P.B. 12-09
	PLOT DATE = 7/8/2013	DATE -	REVISED - M.D. 06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-18		886	821
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L70	

ROUTE MARKERS

-  FOR U.S. ROUTES
M1-40-2424
-  FOR ILLINOIS ROUTES
M1-50-2424
-  R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

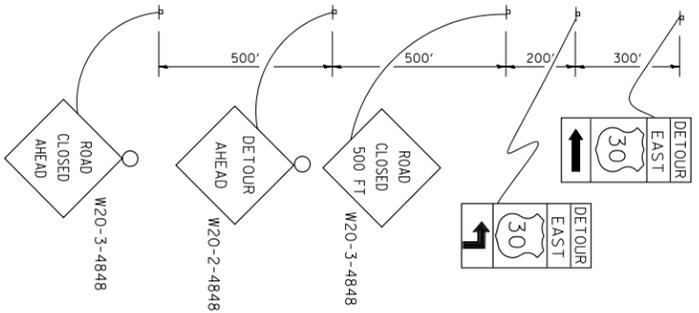
ARROWS SIGNS

-  M5-1L-2115
-  M5-1R-2115
-  M6-1-2115
-  M6-1-2115
-  M6-3-2115

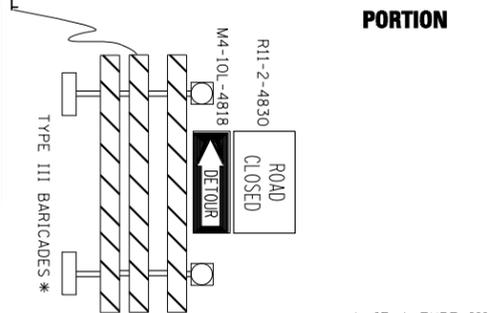
CARDINAL DIRECTION & DETOUR SIGNS

-  NORTH M3-1-2412
-  EAST M3-2-2412
-  SOUTH M3-3-2412
-  WEST M3-4-2412
-  DETOUR M4-8-2412

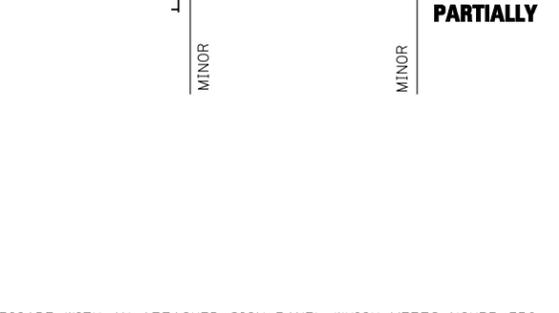
STATE ROUTE



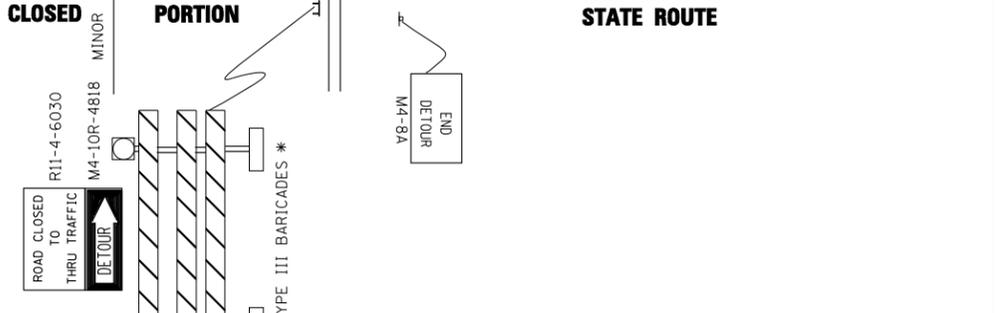
COMPLETELY CLOSED PORTION



PARTIALLY CLOSED PORTION



STATE ROUTE



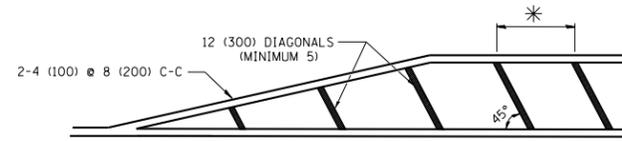
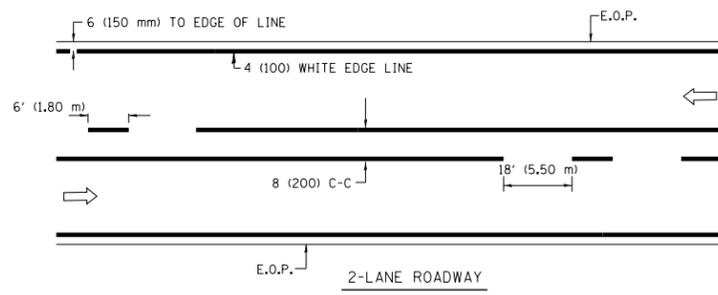
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - 10-18-02
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		PLOT SCALE = 49.9999' / IN.	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

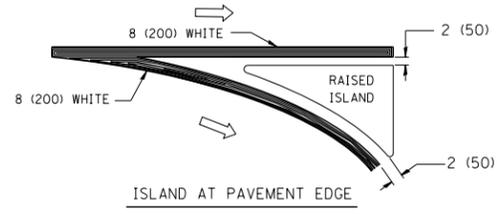
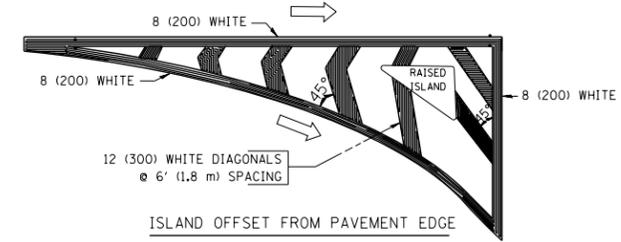
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

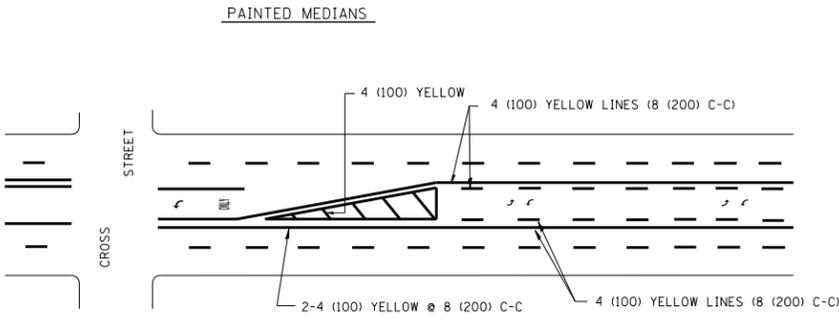
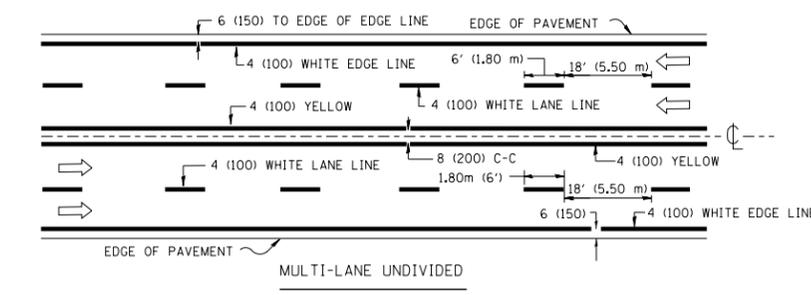
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-21		886	822
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT	CONTRACT NO. 60L70	



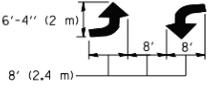
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 * DIAGONAL LINE SPACING: 20' (6.1 m) C-C



TYPICAL ISLAND MARKING

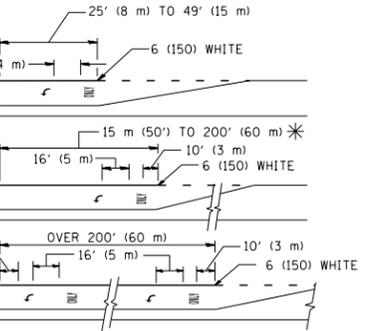
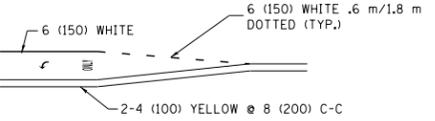
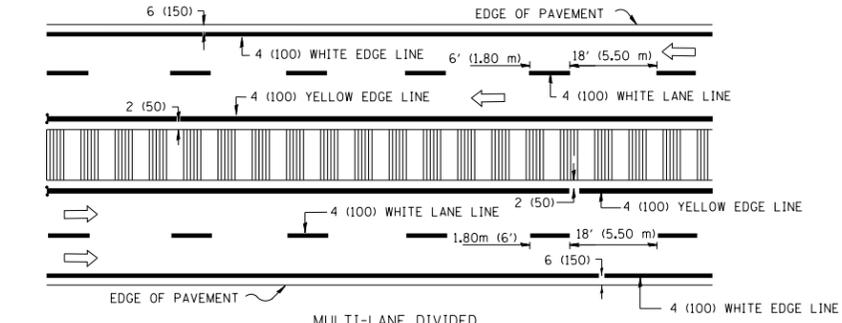


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

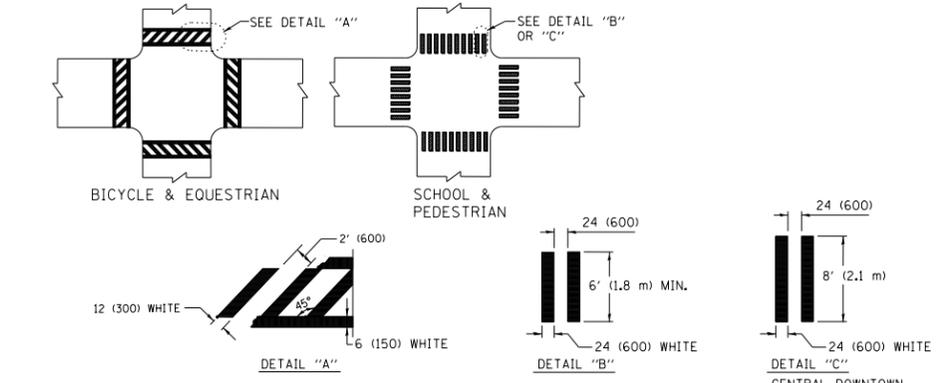


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

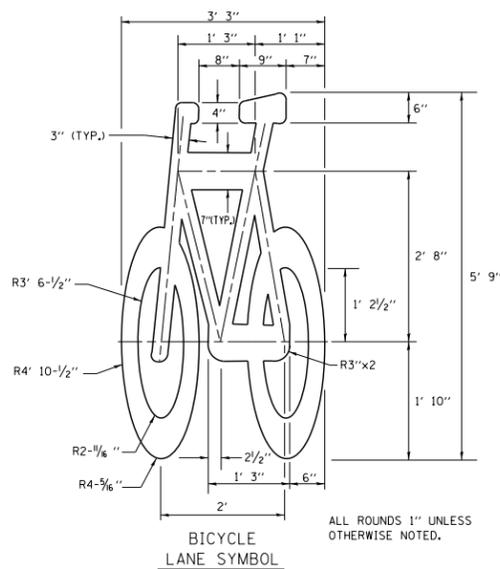
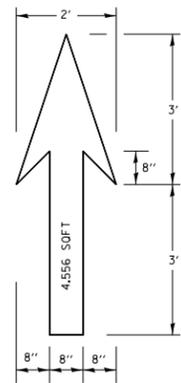
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
ca:\pwork\pwork\dot\drivakosgn\d0108315\te24.dgn		DRAWN -	REVISED - K. ENG 02-28-12
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/1/2012	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

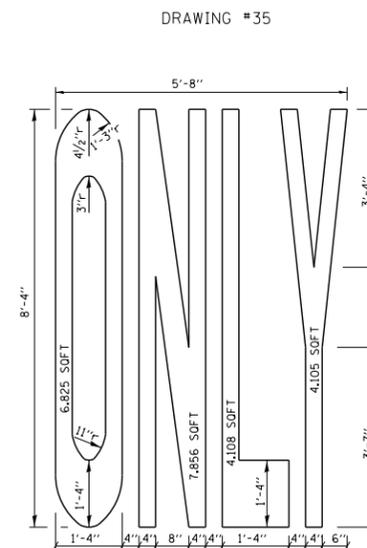
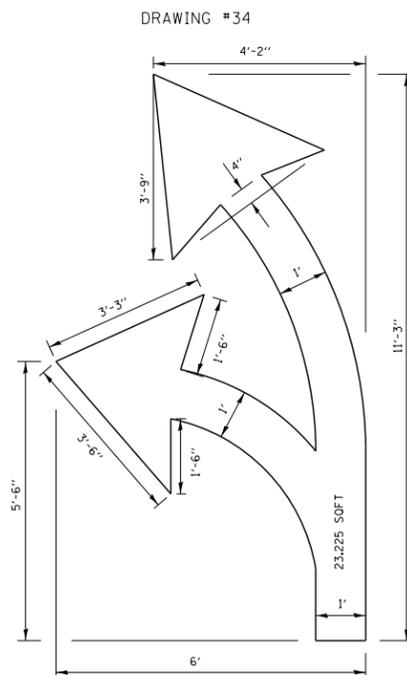
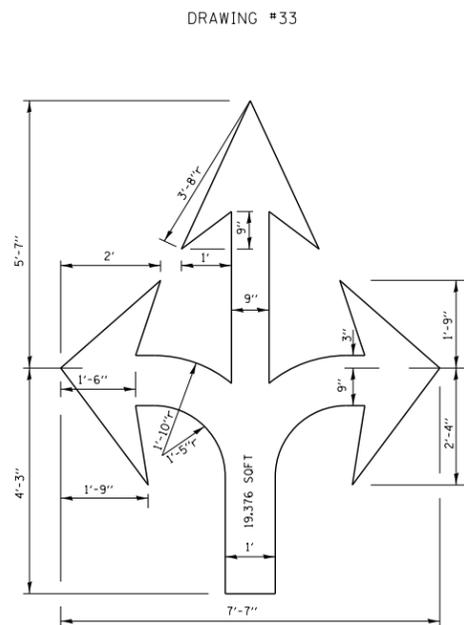
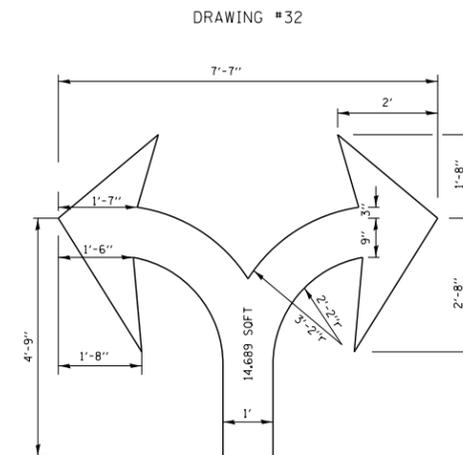
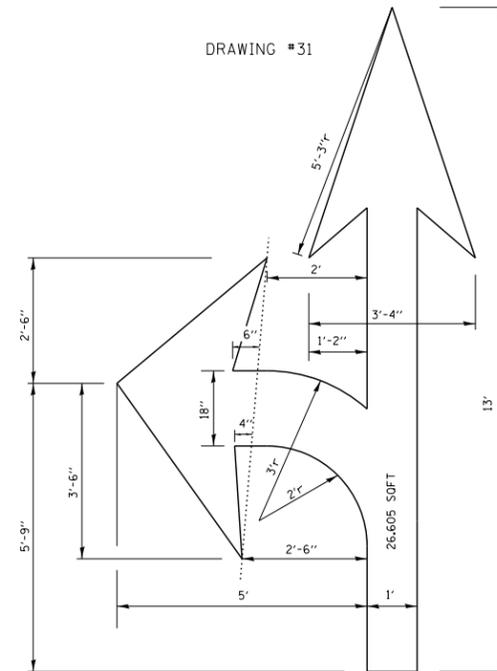
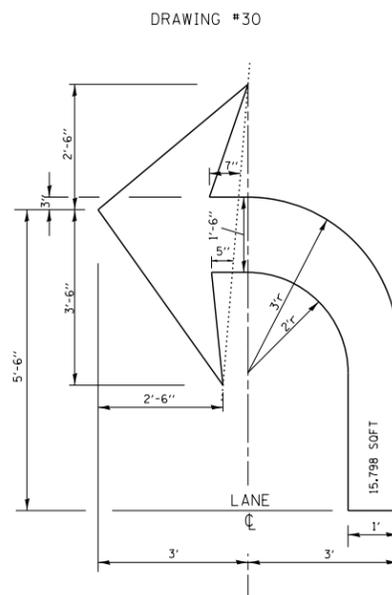
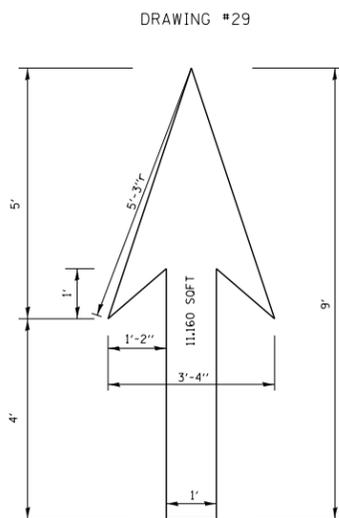
CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-24		886	823
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT	CONTRACT NO. 60L70	



- NOTE:**
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

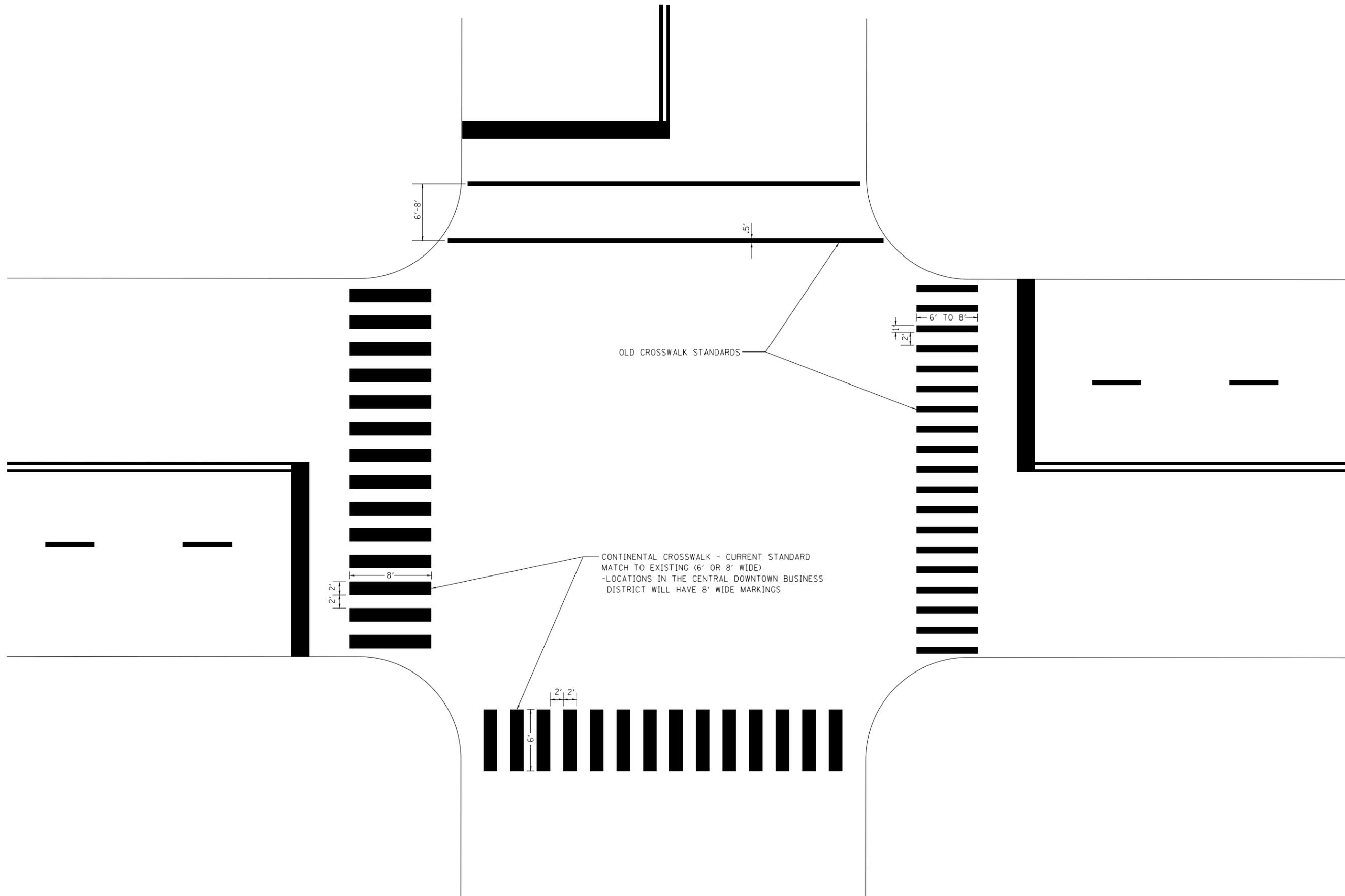
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	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/29/2012	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	824
TC-24		CONTRACT NO. 60L70		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



OLD CROSSWALK STANDARDS

CONTINENTAL CROSSWALK - CURRENT STANDARD
 MATCH TO EXISTING (6' OR 8' WIDE)
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS
 DISTRICT WILL HAVE 8' WIDE MARKINGS

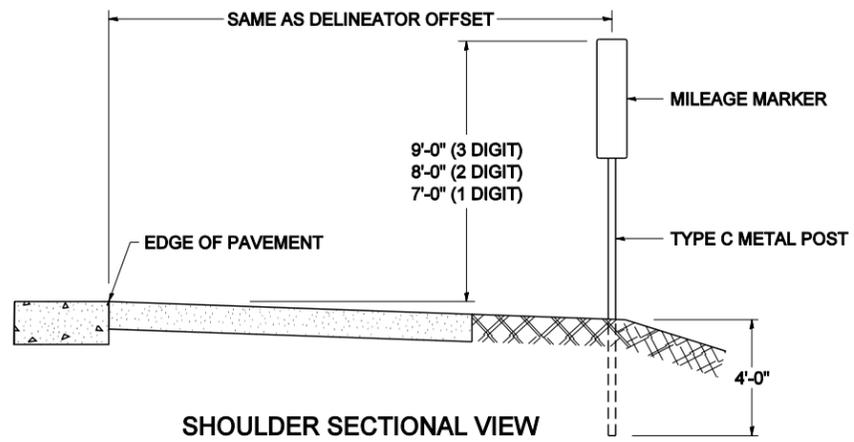
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	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/29/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

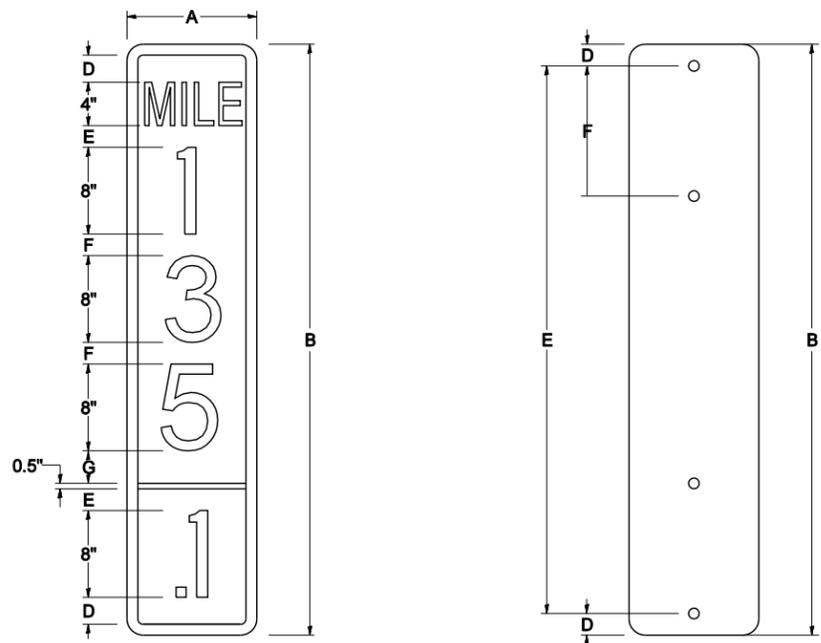
CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-24		886	825
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L70	

STANDARD DESIGN FOR MILE POST



SHOULDER SECTIONAL VIEW

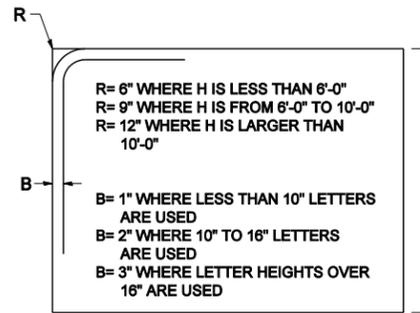


SIGN SIZE	DIMENSIONS							
	A	B	C	D	E	F	G	DIGIT
12 x 24	12.0	24.0	1.5	1.5	1.5	N/A	1.5	1
12 x 36	12.0	36.0	1.5	2.0	2.0	2.0	1.5	2
12 x 48	12.0	48.0	1.5	2.5	2.0	2.0	2.5	3

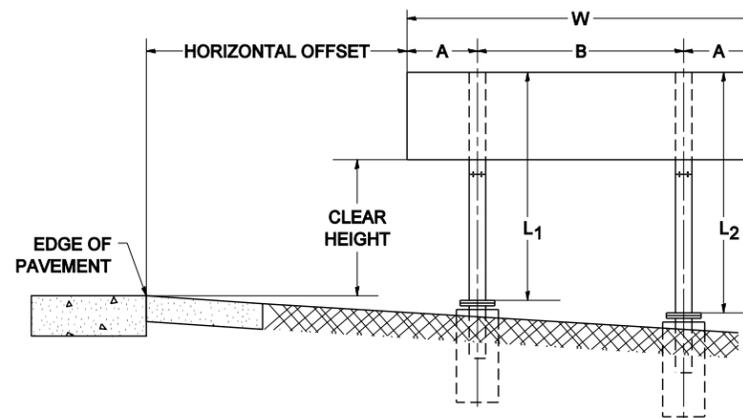
BLANK	A	B	C	D	E	F
B9-1224	12.0	24.0	1.5	2.0	20.0	N/A
B9-1236	12.0	36.0	1.5	2.0	32.0	12.0
B9-1248	12.0	48.0	1.5	2.0	44.0	12.0

SIGN SIZE	SERIES					BLANK STD.	
	LINES						
	1	2	3	4	5	BORDER	
12 x 24	4C	8D	4C	N/A	N/A	0.5	B9-1224
12 x 36	4C	8D	8D	4C	N/A	0.5	B9-1236
12 x 48	4C	8D	8D	8D	4C	0.5	B9-1248

BORDER AND RADIUS LAYOUT



MAJOR GUIDE SIGN LAYOUT

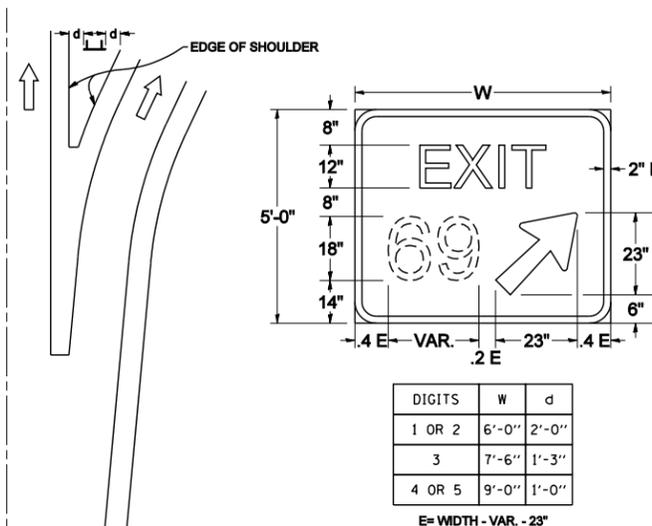


NUMBER OF STEEL SUPPORTS	A	B
2	.2 W	.6 W
3	.15 W	.35 W
4	.125 W	.25 W
5	.1 W	.2 W

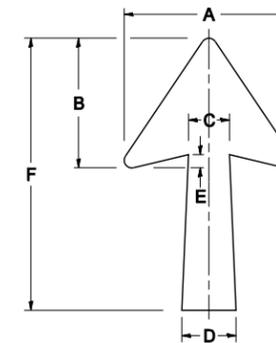
L1 IS THE LENGTH OF SUPPORT, NOT INCLUDING THE STUB PROJECTION, CLOSEST TO THE EDGE OF THE PAVEMENT.

A IS THE DISTANCE FROM THE SIGN EDGE TO THE CENTERLINE OF THE NEAREST SUPPORT. B IS THE DISTANCE BETWEEN CENTERLINES OF SUPPORTS.

GORE SIGNS

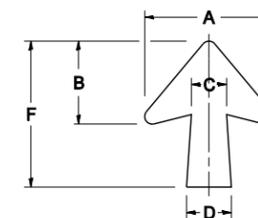


STANDARD ARROWS FOR INTERSTATE GUIDE SIGNS



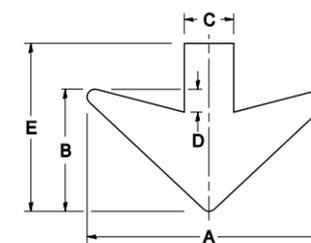
ARROW SYMBOL	A	B	C	D	E	F	R
24 1/4 x 15 1/8	15 1/8	11 1/8	3 3/4	5	1 1/2	2 1/4	1/8
29 1/4 x 18 1/4	18 1/4	14	4 1/2	6	1 1/2	2 9/4	3/4
35 5/8 x 22 1/4	22 1/4	17	5 3/8	7 1/8	1 3/4	35 5/8	1
18 1/4 x 11 1/4	11 1/4	8 3/4	3 1/8	3 3/8		18 1/4	

NOTE: D & F ARE RECOMMENDED DIMENSIONS. TAPER SHOULD BE HELD CONSTANT FOR LONGER OR SHORTER SHAFT LENGTHS



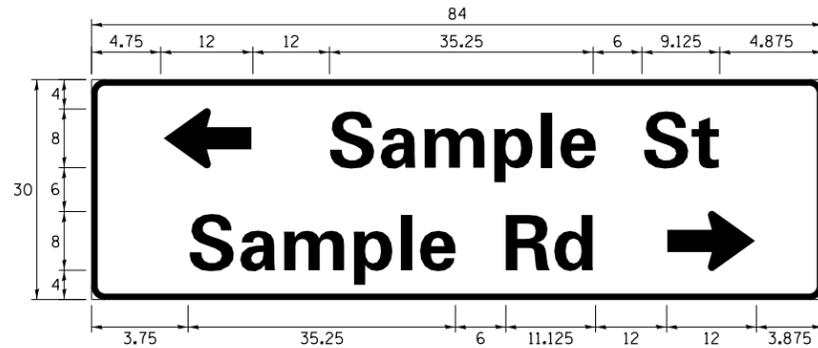
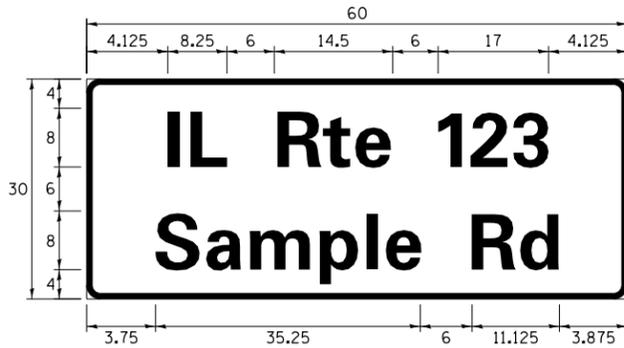
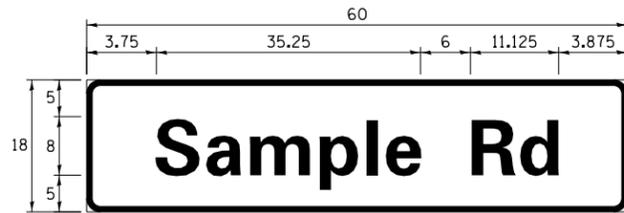
ARROW SYMBOL	A	B	C	D	E	F	R
17 1/4 x 14 1/4	14 1/4	9 3/8	3 3/8	4 1/2	1 5/8	17 1/4	3/4
20 1/4 x 17 1/4	17 1/4	11 3/4	4 3/8	5 5/8	1 1/2	20 1/4	
25 x 21 1/8	21 1/8	14 1/4	5	6 3/4	1 3/4	25	1
9 5/8 x 8 1/8	8 1/8	5 5/8	2 3/8	2 3/8		9 5/8	1/2

DOWN ARROWS



ARROW SYMBOL	A	B	C	D	E	R
16 1/2 x 24	24	12	5	1 1/2	16 1/2	3/4
22 x 32	32	16	6 1/2	3	22	1

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS SHALL BE 3/4" WIDE. CORNER RADIUS SHALL BE 1-7/8". THE SPACING BETWEEN THE WORDS SHOULD BE 6", IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THERE IS SPACE AVAILABLE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

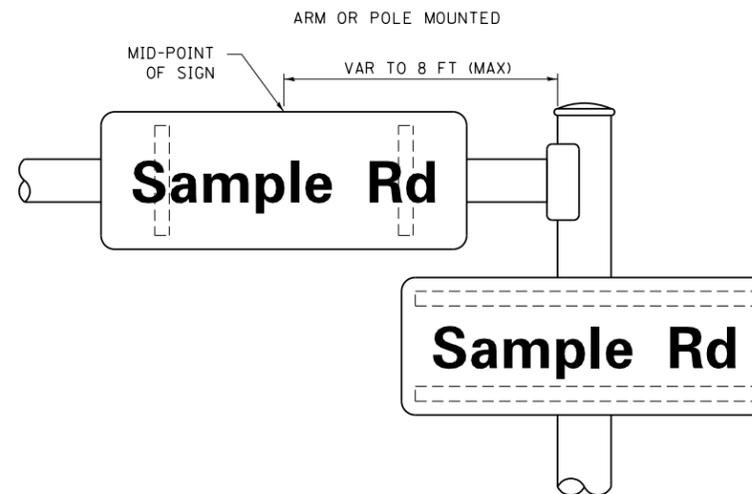
- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA
- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

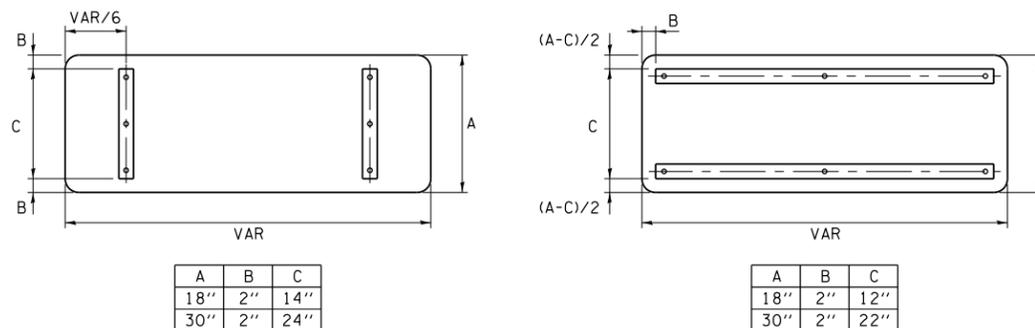
- SIGN CHANNEL PART *HPN053 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
PART *HPN034 (UNIVERSAL)
- BRACKETS CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



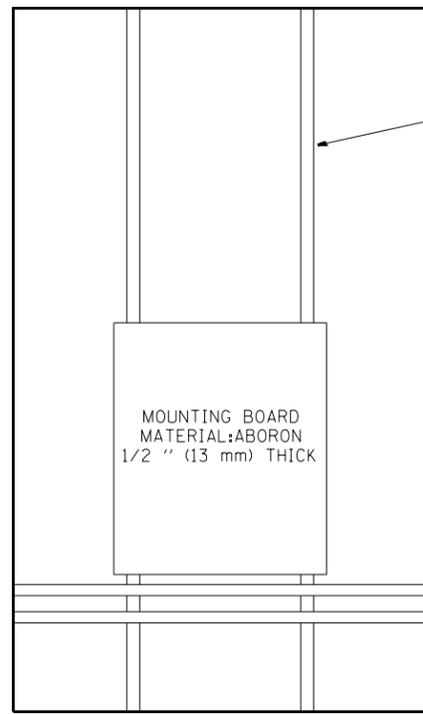
SUPPORTING CHANNELS



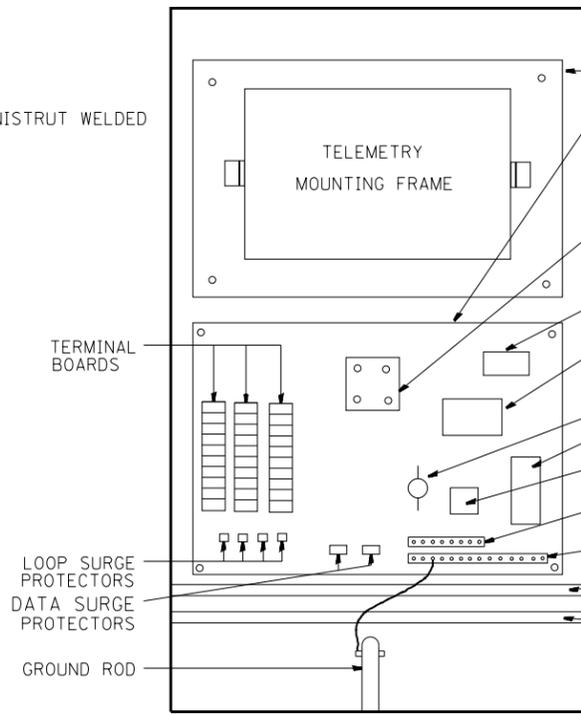
STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

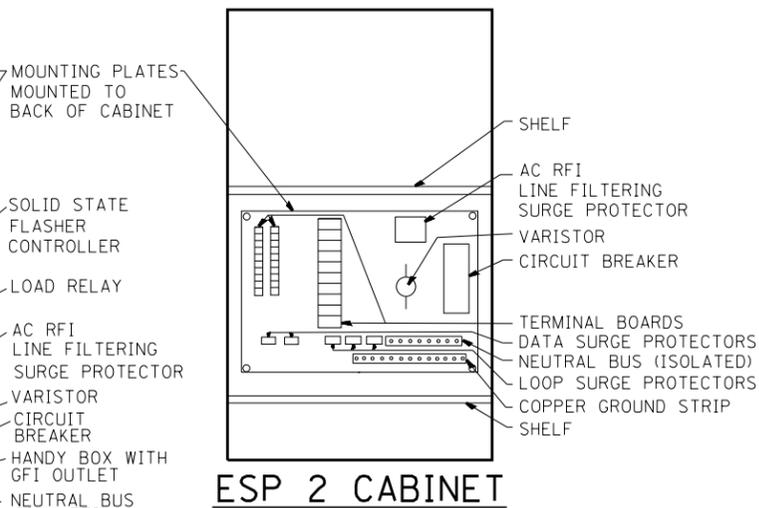
CHARACTER	FHWA SERIES "C"			CHARACTER	FHWA SERIES "D"		
	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)		LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240



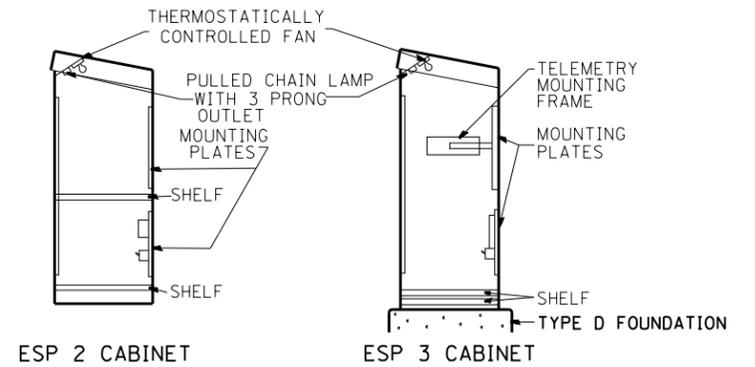
SIDE VIEW ESP 3 & 4 CABINET



ESP 3 CABINET

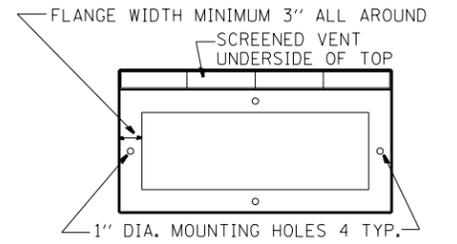


ESP 2 CABINET



PROFILE VIEWS

NOTE: MOUNTING PLATES TO BE MOUNTED TO BACK PANEL OF CABINET



BOTTOM VIEW MOUNTING PATTERN

TYPICAL CABINET INTERIORS
STANDARD TRAFFIC SYSTEMS CENTER CABINETS

TYPE	HEIGHT (IN-mm)	WIDTH (IN-mm)	DEPTH (IN-mm)	THICKNESS (IN-mm)	MATERIAL
ESP1	22.5" (571.5 mm)	14.25" (361.95mm)	9.75" (247.65mm)	3/16" (4.76mm)	FABRICATED ALUMINUM
ESP2	36" (914.4mm)	20" (508.0mm)	15" (381.0mm)	73/16" (4.76mm)	FABRICATED ALUMINUM
ESP3	49.5" (1.26 m)	30" (762.0mm)	17" (431.8mm)	3/16" (4.76mm)	FABRICATED ALUMINUM
ESP4	55" (1.4 m)	44" (1.12 m)	26" (660.4mm)	3/16" (4.76mm)	FABRICATED ALUMINUM

NOTES:

- CABINETS, CABINET POSTS AND CABINET PEDESTALS SHALL BE PRIMED AND PAINTED IN ACCORDANCE WITH SECTION T637 OF THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS". THE FINAL COAT SHALL BE (X) IN COLOR. THE INTERIOR SHALL BE PAINTED WHITE. SIGNAL POSTS AND HEADS TO BE FEDERAL YELLOW 89-19(MAUTZ).
- CABINETS SHALL BE INSTALLED IN ACCORDANCE WITH APPLICABLE PORTIONS OF SECTION T400 OF THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS".
- ALL CABINETS WHICH ARE SERVICED BY 117 VOLTS A.C. POWER SHALL BE EQUIPPED WITH A 10 AMP CIRCUIT BREAKER, A.C. R.F.I. LINE FILTERING SURGE PROTECTOR, VARISTOR, DATA SURGE AND LOOP SURGE PROTECTORS AS INCIDENTAL TO THE COST OF THE CABINET. CMS CABINETS TYPE IV SHALL HAVE A 60 AMP. CIRCUIT BREAKER MINIMUM.
- ESP 2/3/4 CABINETS SHALL BE FITTED WITH A THERMOSTATICALLY CONTROLLED FAN. IT SHALL BE MOUNTED AT THE TOP OF THE CABINET. THE FAN SHALL BE CAPABLE OF OPERATING AT 130 CPM AT 160' (48.8 m) OF STATIC WATER PRESSURE. A PORCLAIN BASED PULL CHAIN FIXTURE WITH 3 PRONG OUTLET SHALL ALSO BE PROVIDED.
- RAMP METERING ESP 3 TYPE CABINETS SHALL ALSO BE EQUIPPED WITH A LOAD RELAY AND 2 CIRCUIT FLASHER. LAMPS, FAN, LOAD RELAY, AND 2 CIRCUIT FLASHER SHALL BE INCIDENTAL TO THE COST OF THE CABINET
- INCIDENTAL TO THE COST OF EACH CABINET THE CONTRACTOR SHALL CONSTRUCT A 5 INCH (130mm) PCC SIDEWALK OF A RECTANGULAR AREA 3 FEET (915 mm) BY 4 FEET (1.25 m) IMMEDIATELY ADJACENT TO THE CABINET FOUNDATION ON THE SAME SIDE OF THE FOUNDATION AS THE CABINET DOOR TO PROVIDE FOOTING DURING INSTALLATION AND MAINTENANCE.
- ANCHOR BOLTS FOR PEDESTAL AND BASE MOUNTED CABINETS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE CABINET.
- ALL CABINETS SHALL HAVE TERMINAL BLOCKS AND SHELVES AS SHOWN. THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE CABINET.
- THE CABINET DOOR SHALL BE HINGED ON THE RIGHT SIDE WHEN FACING THE CABINET. THE DOOR SHALL BE FURNISHED WITH A GASKET THAT SHALL FORM A WEATHER TIGHT SEAL BETWEEN THE CABINET AND DOOR. THE HINGES SHALL BE CONTINUOUS AND BOLTED TO THE CABINET AND DOOR UTILIZING 1/4-20 STAINLESS STEEL CARRIAGE BOLTS AND NY-LOCK NUTS. THE HINGES WILL BE MADE OF STAINLESS STEEL WITH A 0.25 INCH (6.35 mm) DIAMETER STAINLESS STEEL HINGE PIN. THE HINGE PIN SHALL BE CAPPED TOP AND BOTTOM BY WELD TO RENDER IT TAMPER PROOF.
- THE LATCHING MECHANISM SHALL BE A 3 POINT DRAW ROLLER TYPE. THE CENTER CATCH AND PUSHRODS SHALL BE EITHER CADMIUM OR ZINC PLATED, TYPE II CLASS I. PUSHRODS WILL BE TURNED EDGEWISE AT THE OUTWARD SUPPORTS AND SHALL BE 0.25 INCH (6.35 mm) BY 0.75 INCH (19.05 mm). MINIMUM. ROLLERS SHALL HAVE A MINIMUM DIAMETER OF 0.875 INCH (22.22 mm) AND WILL BE MADE OF NYLON. THE CENTER CATCH SHALL BE FABRICATED FROM 0.14 INCH (3.55 mm) STEEL, MINIMUM. WHEN THE DOOR IS CLOSED AND LATCHED, IT WILL BE LOCKED. THE LATCHING HANDLE SHALL HAVE A PROVISION FOR PADLOCKING IN THE CLOSED POSITION. AN OPERATING HANDLE SHALL BE FURNISHED WITH EACH LOCK. THE HANDLE WILL BE STAINLESS STEEL WITH A 0.75 INCH (19.05 mm) DIAMETER SHANK.
- THE ENCLOSURE SHALL BE EQUIPPED WITH TWO ADJUSTABLE "C" MOUNTING CHANNELS WELDED ON BOTH SIDE WALLS AND BACK WALL OF THE ENCLOSURE, ALLOWING VERSATILE POSITIONING OF SHELVES OR PANELS. MOUNTING CHANNELS SHALL BE FACTORY PAINTED SAME COLOR AS INTERIOR OF CABINET.
- CABINET DOOR SHALL NOT HAVE COMPARTMENT DOORS OR LOUVERS.
- ALL FIELD CABINETS SHALL BE FITTED WITH BRASS LOCKS.
- ESP TYPE 2 & 3 CABINETS FITTED WITH TWO SHELVES AS SHOWN.
- POST TOP MOUNTED CABINETS, SHALL HAVE A 0.25 INCH (6.3 mm) BOTTOM OF CABINET WELDED.
- THE CONTROL CABINET SHALL BE SET PLUMB ON THE FOUNDATION AND FASTENED TO THE ANCHOR BOLTS WITH NUTS AND WASHERS. FLAT WASHERS SHALL BE INSTALLED BELOW AND ABOVE THE BASE PLATE OF THE CONTROL CABINET. LOCKWASHERS SHALL BE INSTALLED ON TOP OF THE TOP FLAT WASHER.

(X)

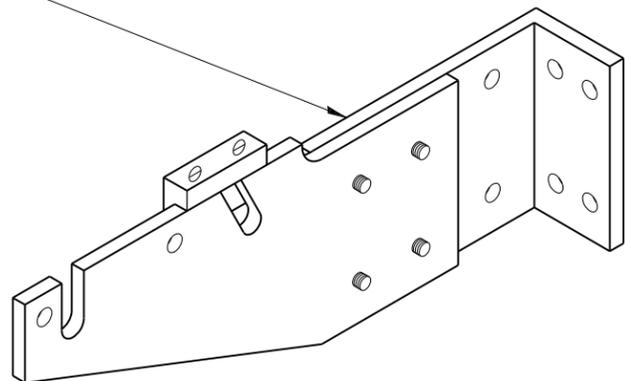
- | | |
|------------------|---------------------|
| EDENS | WALNUT * |
| KENNEDY | BLUE STREAK ** |
| EISENHOWER | CARIBBEAN BLUE * |
| I-290/IL53/I-355 | POST OFFICE BLUE ** |
| RYAN | YELLOW STONE II ** |
| I-55 | MEDIUM BRONZE * |
| I-57 | RED BARON ** |
| CAL-KING | BLUE STREAK ** |
| LAKE SHORE DR. | GREEN * |
| I-80 | STATUARY BRONZE ** |

ALL RAMP METERING CABINETS LIME GREEN ***. ALL POSTS, T.S. HEADS AND SERVICES WILL BE PAINTED FEDERAL YELLOW.
* MORTON POWDER PAINT COLOR OR EQUIVALENT.
** O'BRIEN POWDER PAINT COLOR OR EQUIVALENT.
*** BENJAMIN MOORE ENAMEL COLOR OR EQUIVALENT.

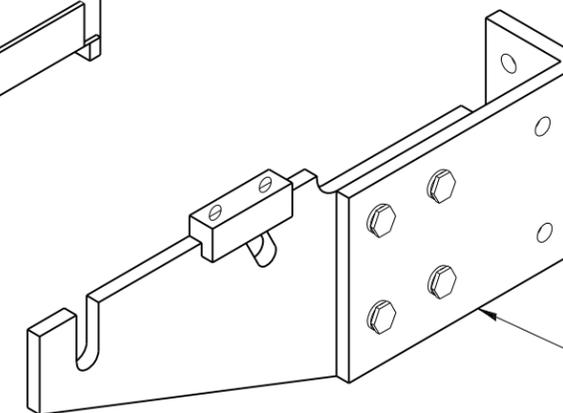
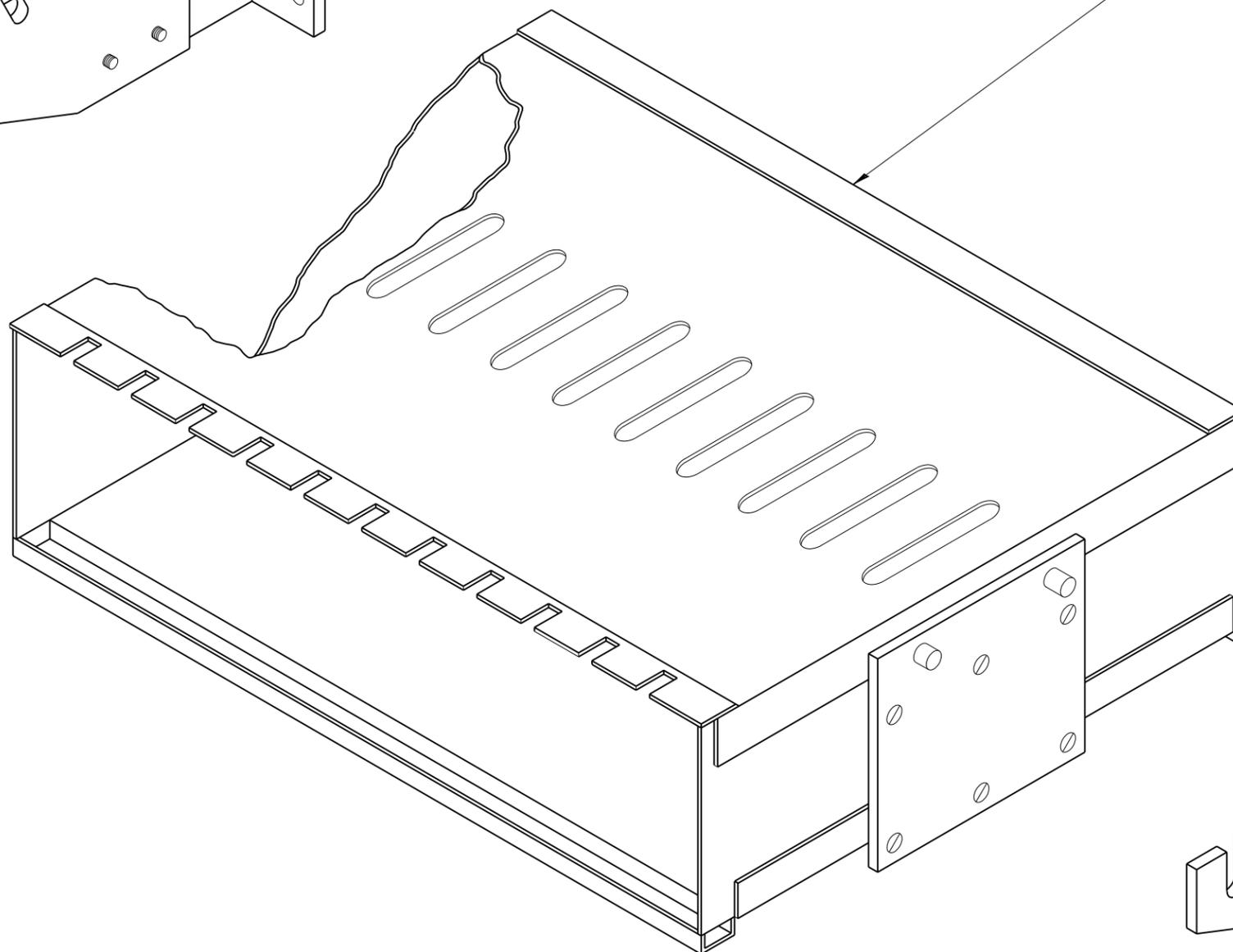
NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR CONFORMING TO COLOR REQUIREMENTS

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 12/94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	CABINET DETAIL SHEET			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / 1"	CHECKED - R.L.	REVISED - 02/98	CONTRACT NO. 60L70									
PLOT DATE = 2/7/2013	DATE - 06/21/94	REVISED - 03/99										

CRADLE



II MODULE MOUNTING FRAME
(FOR II TYPE "A" PLUG-IN TYPE TONE MODULES)

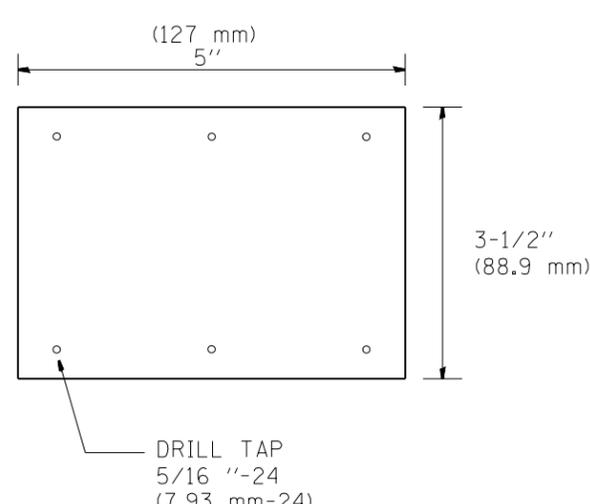
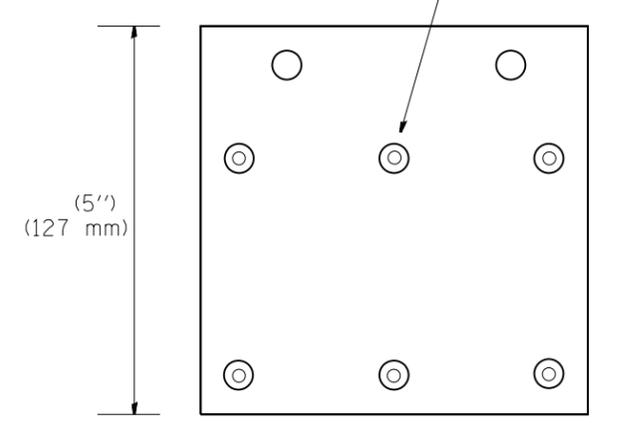
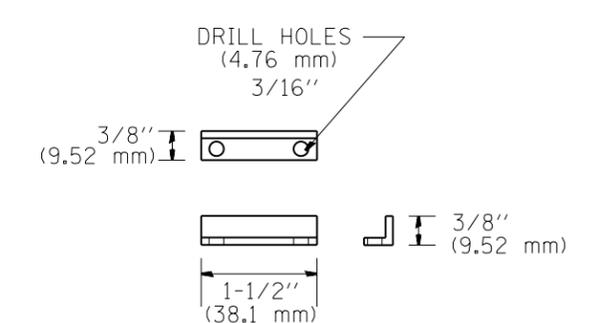
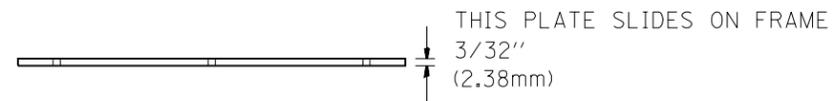
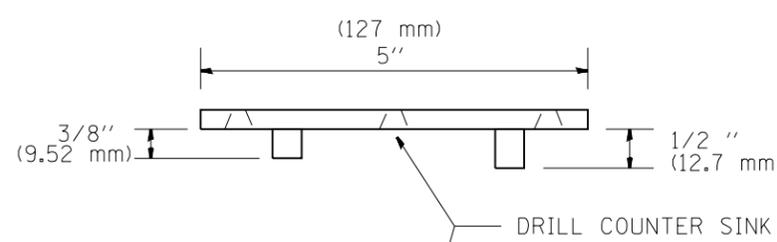
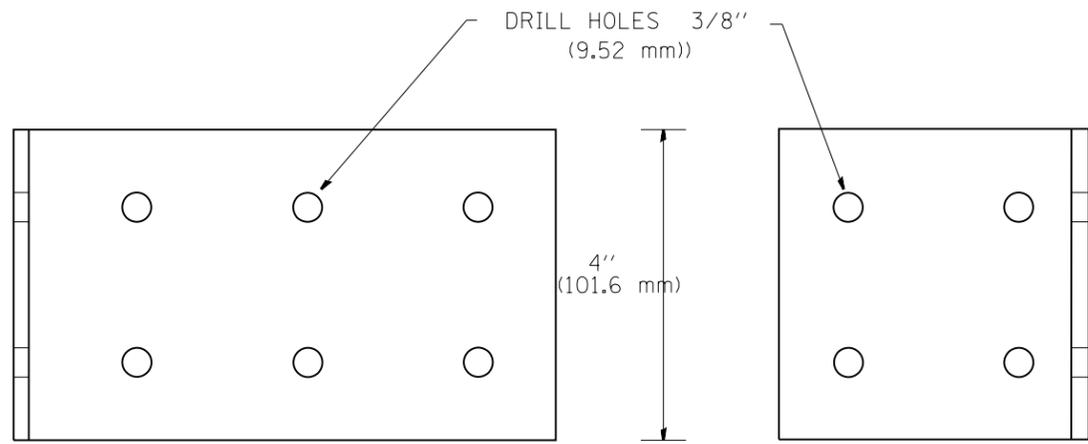
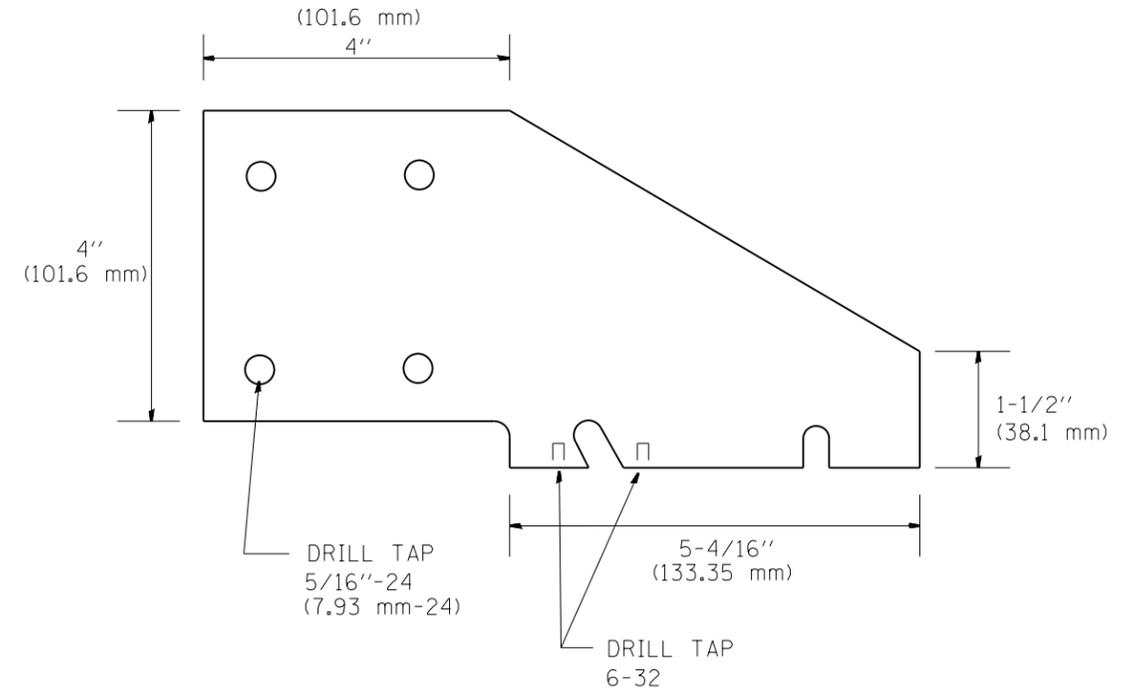
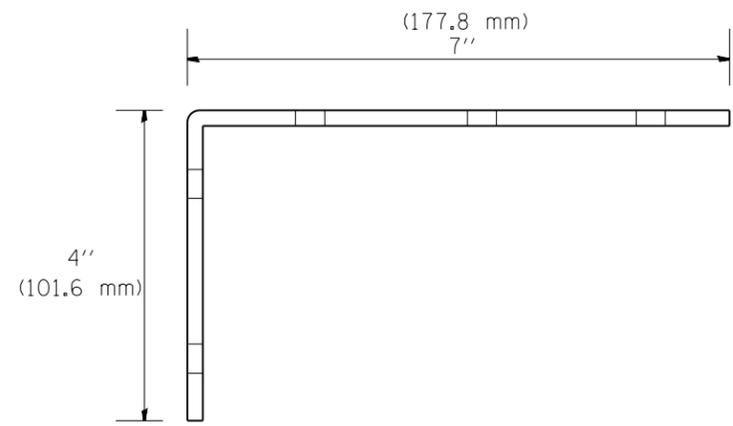


CRADLE

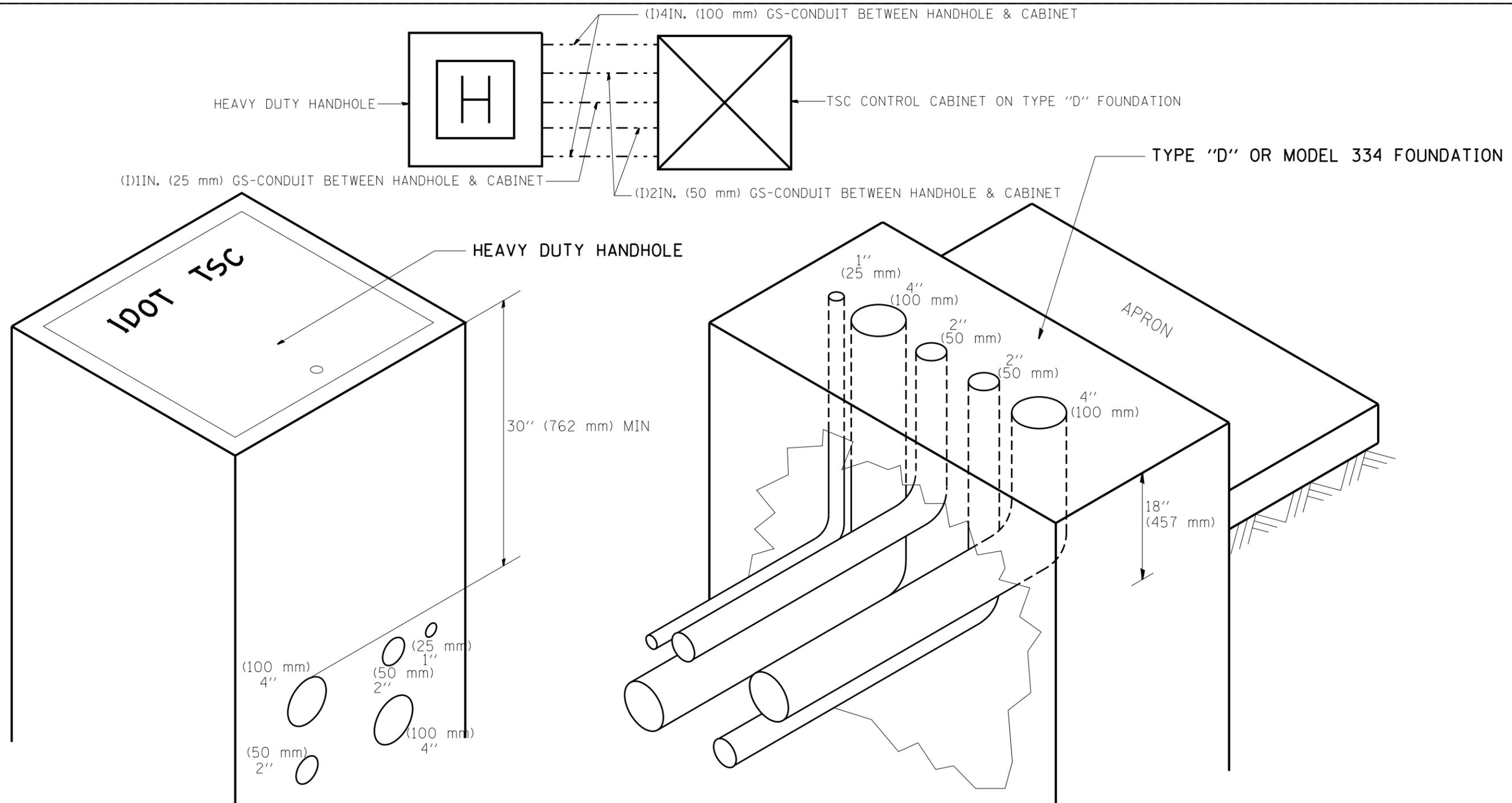
NOTE:

TYPE "A" TONE MODULES ARE PLUG
IN UNIT MEASURING 5-7/32" (132.55 mm) X 1.5" (38.1 mm) X 13-3/4" (349.25 mm)

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 06/94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	FIELD MOUNTING FRAME WITH CRADLE ASSEMBLY			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / 1in.	CHECKED - R.L.	REVISED -	CONTRACT NO. 60L70									
PLOT DATE = 2/7/2013	DATE - 06/21/94	REVISED -										



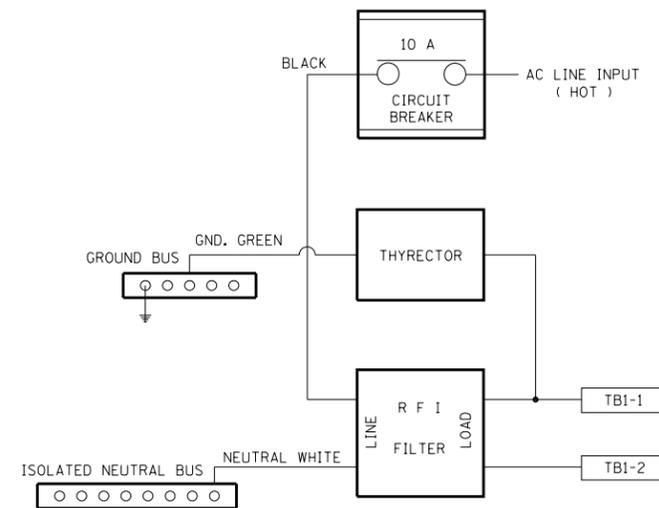
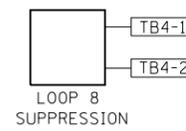
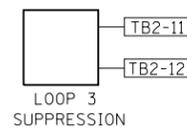
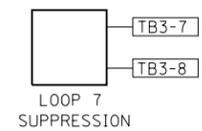
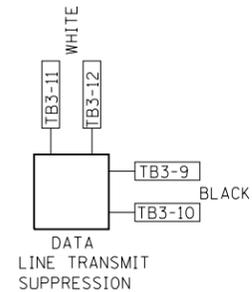
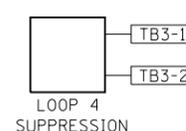
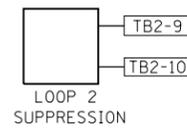
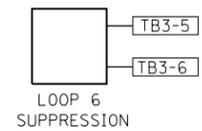
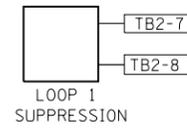
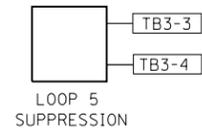
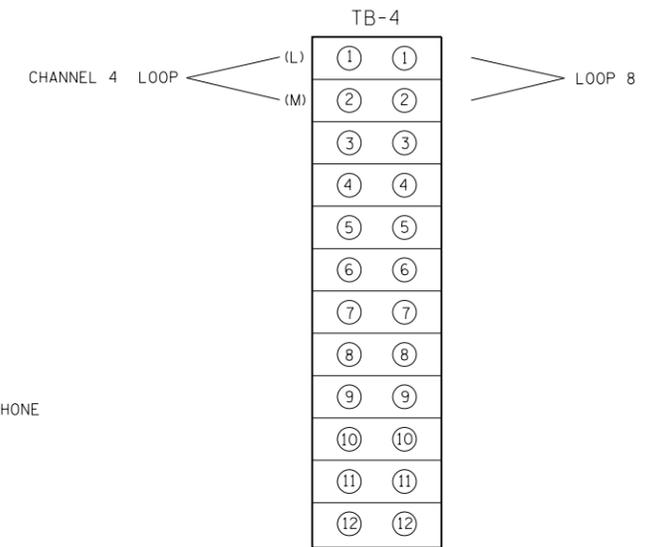
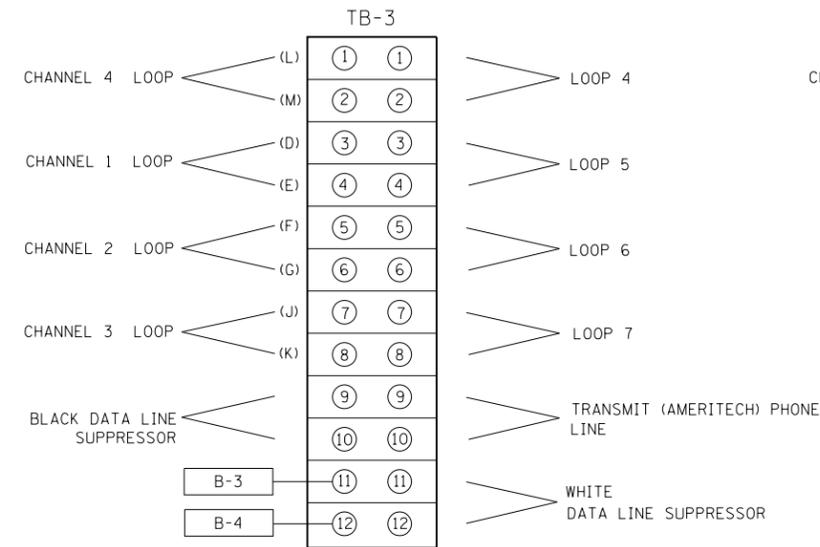
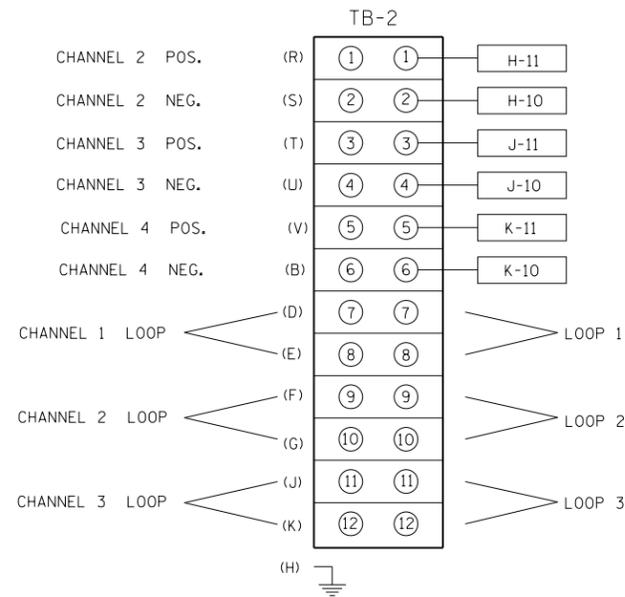
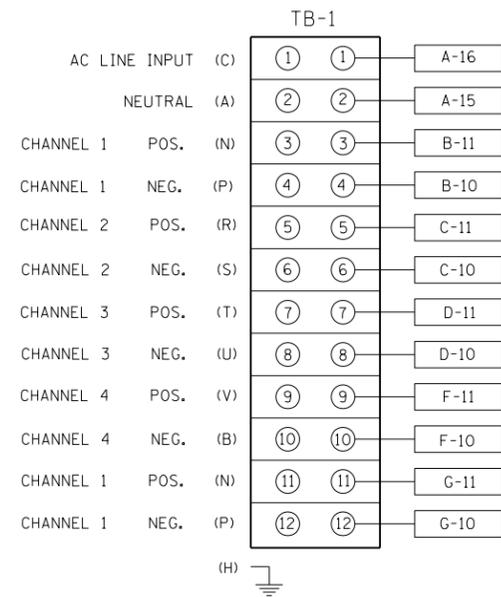
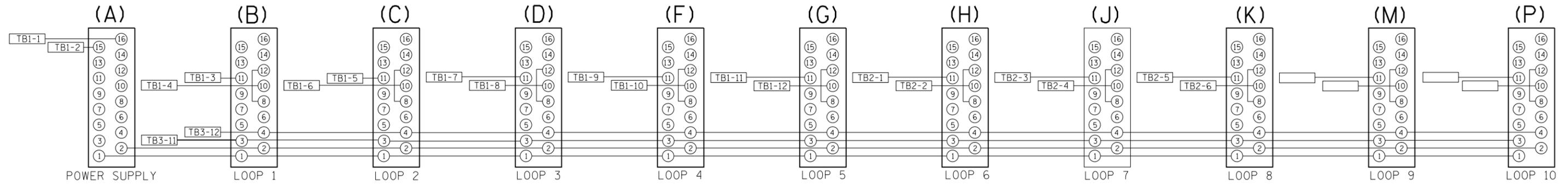
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et:\pw\work\p\dot\mezag\d0287541\TSCYP.dgn		DRAWN - G.M.	REVISED - 09/96		SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.			886	830
		CHECKED - R.L.	REVISED -		CONTRACT NO. 60L70								
		DATE - 06/21/94	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT								



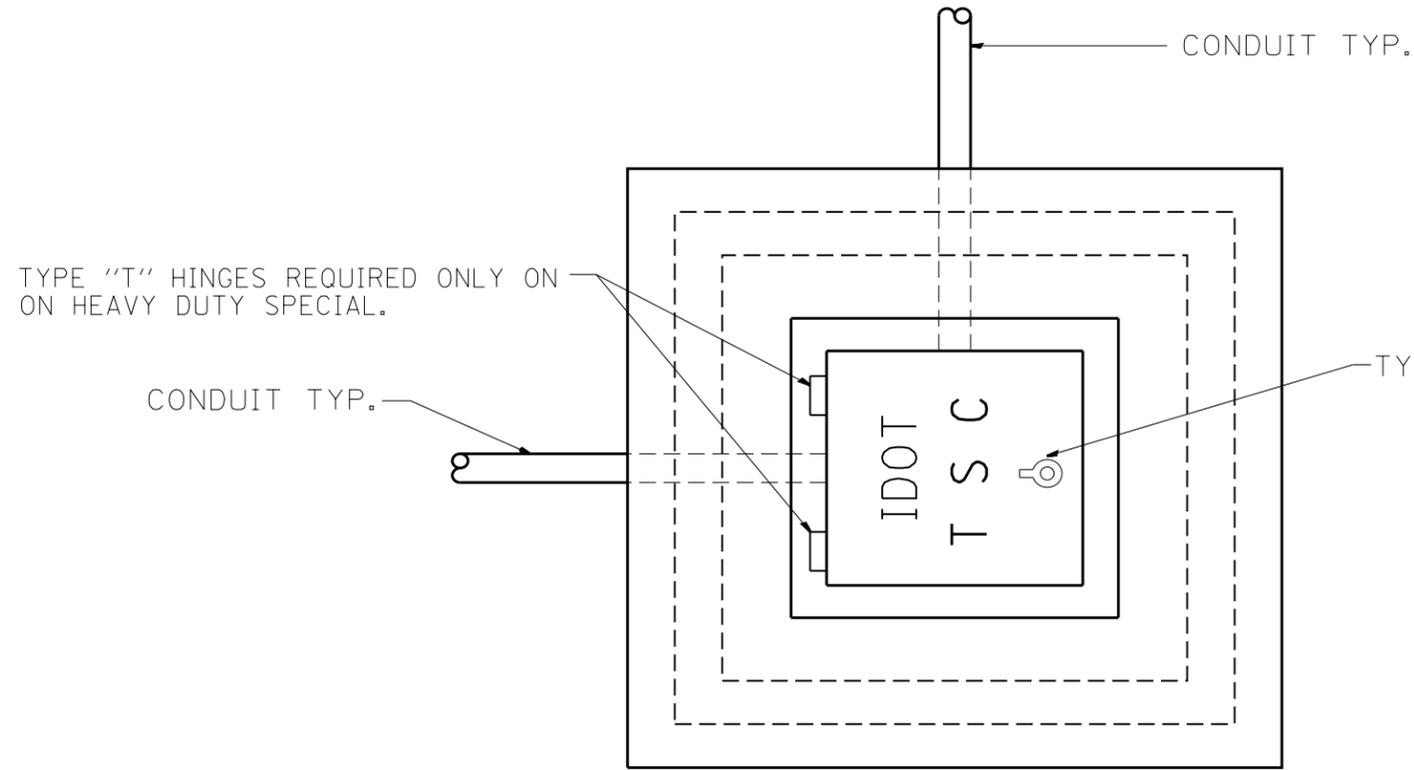
- NOTES:
- 1) ALL DUCTS SHALL BE CONED IN HANDHOLES.
 - 2) ALL DUCTS SHALL BE GS-CONDUIT & GS 90 DEG. ELBOWS USED WHERE NEEDED.
 - 3) ALL DUCTS ENTER HANDHOLE AT MINIMUM DEPTH OF 30 INCH (762 mm)
 - 4) ALL HANDHOLE COVERS SHALL READ "IDOT TSC".
 - 5) ALL CABINET HANDHOLES SHALL BE HEAVY DUTY.
 - 6) DUCTS SHALL BE CENTERED IN CABINET FOUNDATION/HANDHOLE AS SHOWN.
 - 7) CONDUITS SHALL BE SPACED 305 mm (1 FOOT) CENTER TO CENTER IN HEAVY DUTY HANDHOLE.
 - 8) INSTALL 3/4" X 10' (20 mm X 3 m) COPPER CLAD STEEL GROUND ROD IN HDHH PROVIDED AS CABINET PAD. EXOTHERMIC WELD CONNECTION FROM GROUND ROD TO #6 GROUND WIRE INSULATED (GREEN).
 - 9) BOND ALL GSC CONDUITS IN CABINET FOUNDATION.
 - 10) INSTALL #6 GROUND WIRE IN 1IN. (25 mm) GSC FROM HANDHOLE TO CABINET.
 - 11) TYPE "D" FOUNDATION SHALL BE 18" FROM TOP OF FOUNDATION TO FINISHED GRADE.

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 09/96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	CABINET - HANDHOLE CONDUIT DETAIL			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 100.0000' / 1in.	CHECKED - R.L.	REVISED - 04/99											
PLOT DATE = 2/7/2013	DATE - 06/05/95	REVISED - 07/2010											

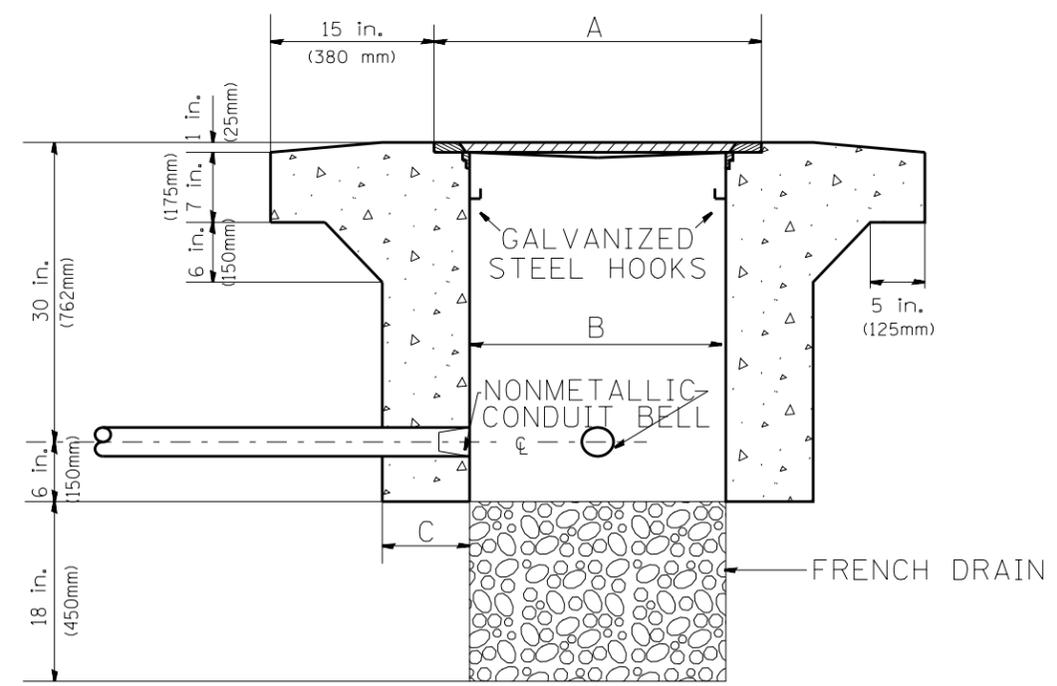
BACK VIEW OF TONE RACK



FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 02-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	TYPE 3 CABINET WIRING DIAGRAM			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p1dot\mezag\d0287541\TSCTYP.dgn	PLOT SCALE = 100.0000' / in.	DRAWN - G.M.	REVISED - 05-99								886	832
	PLOT DATE = 2/7/2013	CHECKED - R.L.	REVISED -		SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60L70				
		DATE - 10-17-95	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							



PLAN



ELEVATION

PC CONCRETE - HEAVY DUTY HAND HOLE

HEAVY DUTY HANDHOLE MINIMUM DIMENSIONS (UNHINGED)

A	28" (711 mm)
B	22" (559 mm)
C	8" (200 mm)

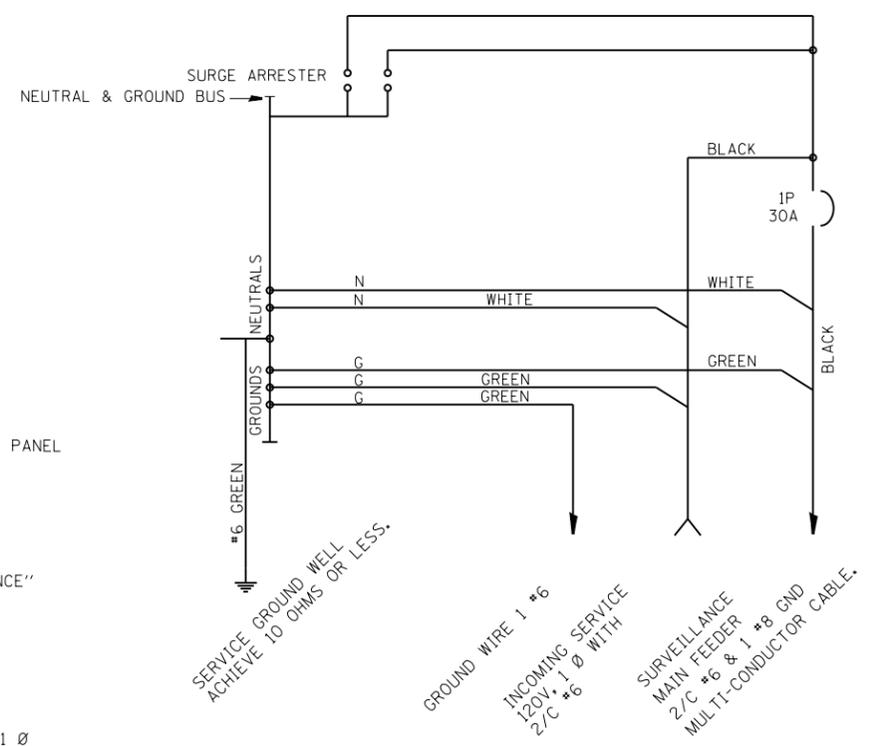
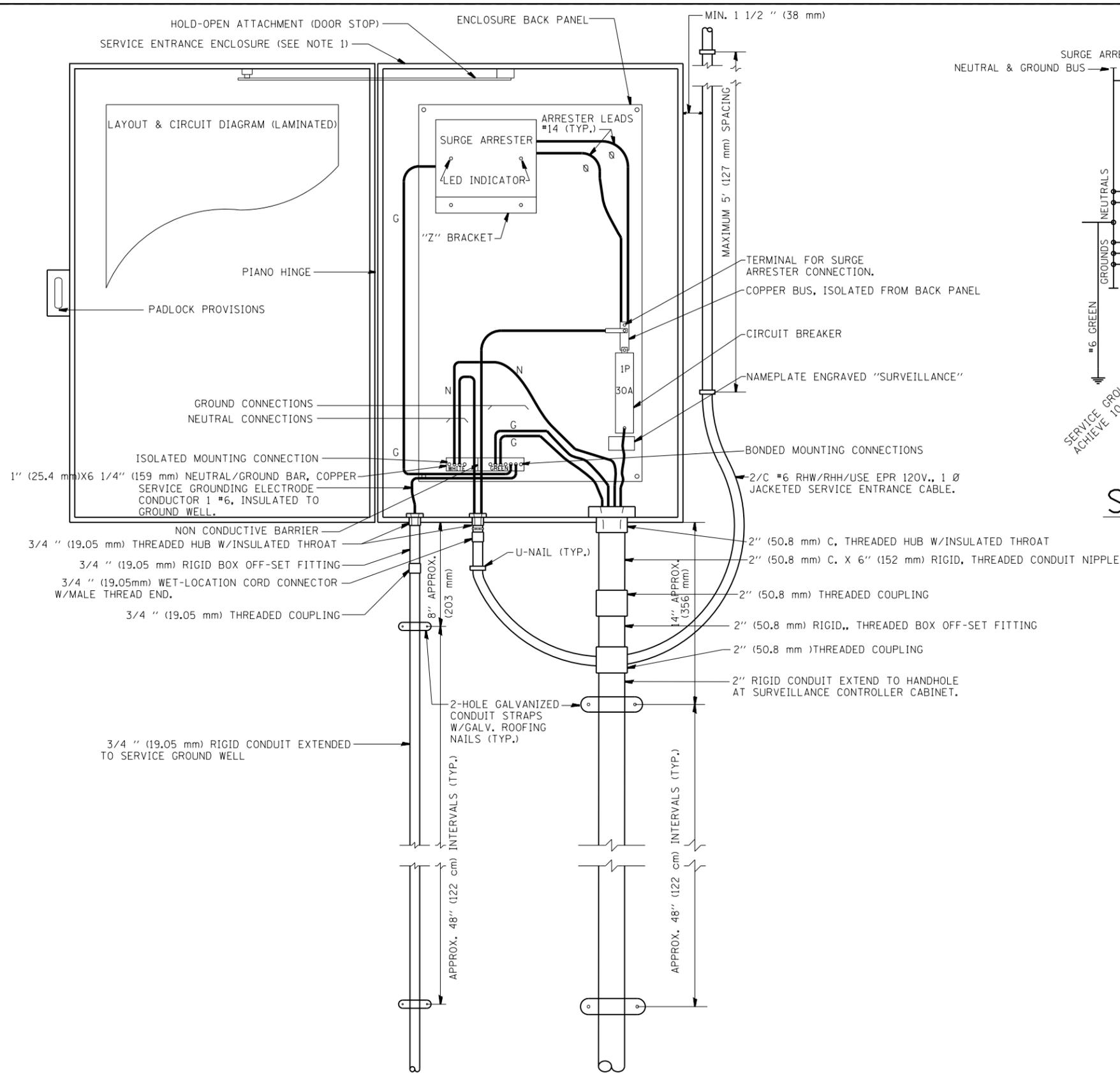
(FRAME AND COVER 260 LBS. (118 Kg.) MIN.)

HEAVY DUTY HANDHOLE SPECIAL MINIMUM DIMENSIONS

A	31.5" (800 mm)
B	30.0" (762 mm)
C	10.0" (250 mm)

(FRAME AND COVER 405 LBS. (184 Kg. (405))

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 04/97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	PC CONCRETE - HEAVY DUTY HAND HOLE			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRAWN - G.M.	CHECKED - R.L.	REVISED -	SCALE: NONE					SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
PLOT SCALE = 100.0000' / 1in.	DATE - 09/11/96	REVISED -											
PLOT DATE = 2/7/2013													

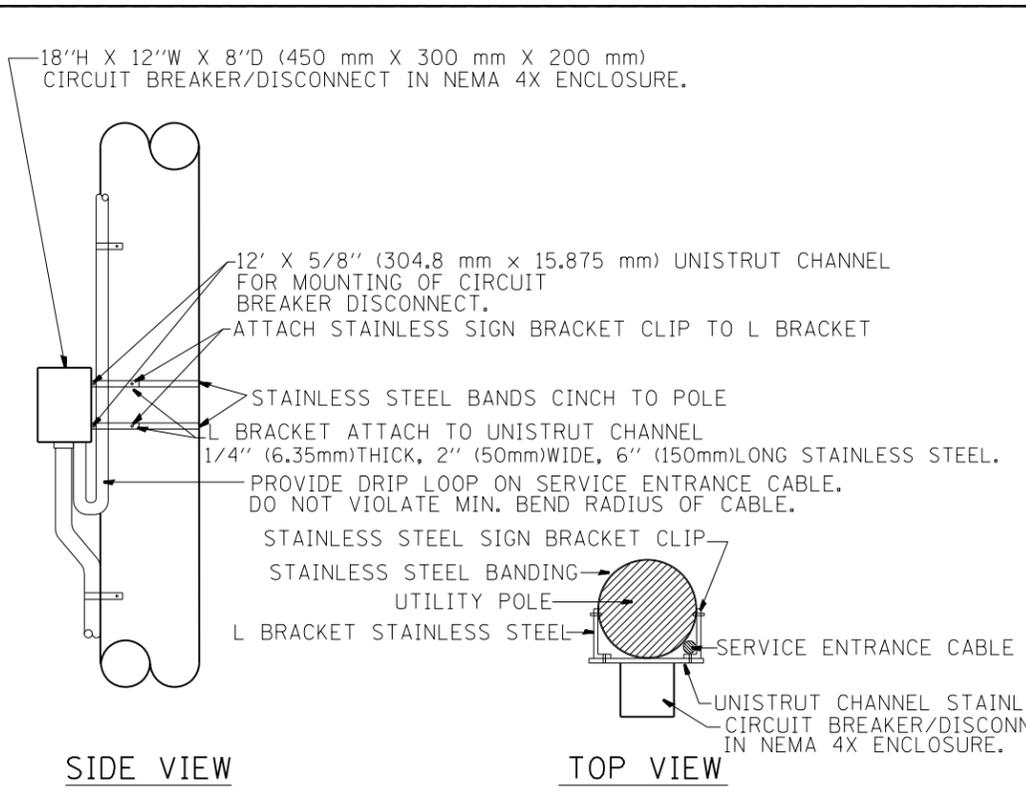
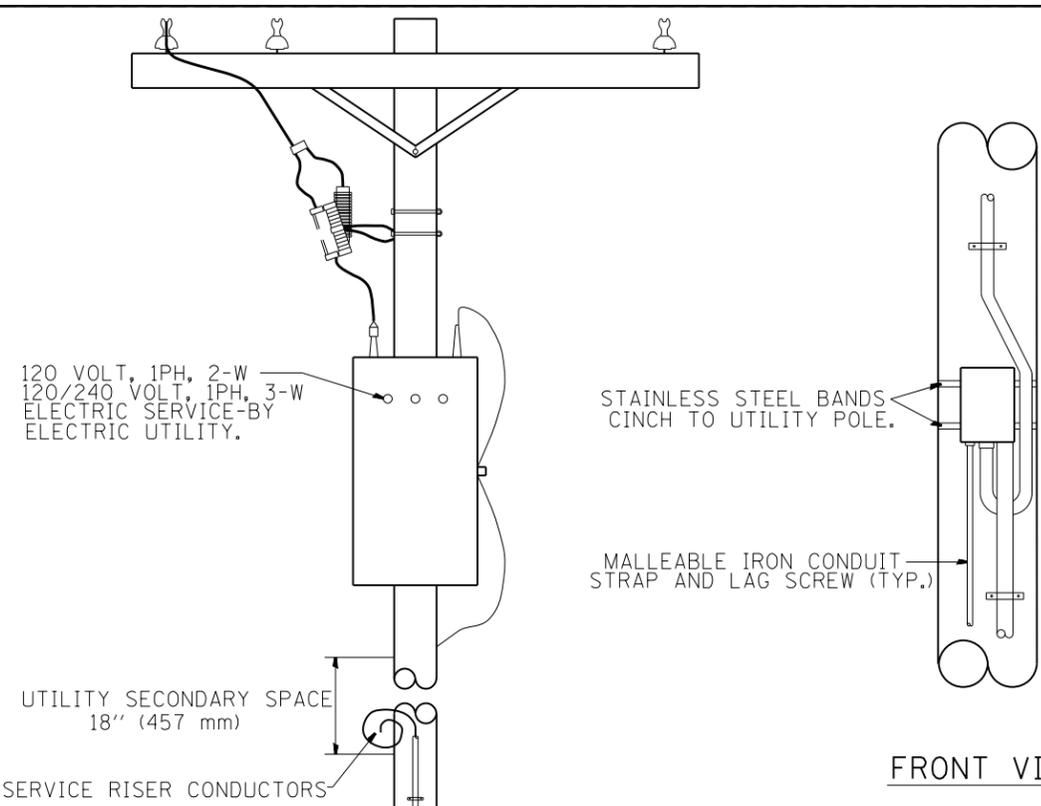


SCHEMATIC DIAGRAM

- NOTES:**
- 1.- ELECTRIC SERVICE SHALL BE OF THE VOLTAGE INDICATED. WHERE 120-VOLT SERVICE IS INDICATED, SERVICE DROP CABLE SHALL BE INSTALLED ACCORDINGLY AND LIGHTING MAIN FEEDER CABLE SHALL BE OMITTED.
 - 2.- THE ELECTRIC SERVICE BOX SHALL BE NEMA 4X STAINLESS STEEL, NOMINALLY 12" (305 mm) X 18" (457 mm) X 8" (203 mm), WITH PIANO HINGED DOOR, STEEL BACK PANEL, FAST-ACTING STAINLESS STEEL ENCLOSURE CLAMPS, PADLOCK PROVISIONS, DOOR STOP KIT AND STEEL BACK PANEL, HOFFMAN CATALOG A-16H120BSS6LP/A-16P2/A-DSTOPK/C-PMK12, OR APPROVED EQUAL.
 - 3.- THE ELECTRIC SERVICE EQUIPMENT ASSEMBLY SHALL BE UL LABELED, SUITABLE FOR USE AS SERVICE EQUIPMENT.
 - 4.- CIRCUIT BREAKERS SHALL BE THERMAL MAGNETIC BOLT-ON TYPE WITH A MINIMUM INTERRUPTING CAPACITY OF 25,000 SYMMETRICAL AMPERES AT 240 VOLTS. THEY SHALL BE LOCKABLE IN THE "OFF" POSITION FOR COMPLIANCE WITH OSHA LOCK-OUT/TAG-OUT REQUIREMENTS. HANDLES SHALL BE TRIP FREE.
 - 5.- THE SURGE PROTECTOR SHALL BE SUITABLE FOR 240/120 VOLT SINGLE PHASE 60HZ AC ELECTRICAL SERVICE, WITH A SURGE ENERGY CAPABILITY OF >3600 JOULES OR BETTER AT 8/20 MICROSECONDS, RATED -40 TO 65 DEGREES C., WITH LED OPERATING INDICATORS, AND SHALL BE UL LISTED PER UL 1449, CUTLER-HAMMER CMOV 230L065XST OR APPROVED EQUAL. SURGE PROTECTOR SHALL BE WIRED FOR 120 V SERVICE. FOLLOW MANUFACTURER RECOMMENDED WIRING SPECIFICATIONS.
 - 6.- BUS BARS, CONNECTORS AND LUGS SHALL BE COPPER, INSULATED AND ISOLATED AND CONFIGURED TO PREVENT SHORTED CONDITIONS FROM TIGHTENING TERMINATIONS, ETC. THE OVERALL BUS SECTION SHALL BE CONFIGURED BEHIND AN INSULATING BARRIER SHIELD WHICH IS REMOVABLE FOR ACCESS TO CONNECTIONS.
 - 7.- THE COMBINATION GROUND AND NEUTRAL BAR SHALL BE CONFIGURED WITH SEPARATE GROUND AND NEUTRAL SECTIONS AND SPARE TERMINALS AS INDICATED. THE HEADS OF GROUND SCREWS SHALL BE PAINTED GREEN. THE HEADS OF NEUTRAL SCREWS SHALL BE PAINTED WHITE.
 - 8.- A PLASTIC LAMINATED LAYOUT AND CIRCUIT DIAGRAM SHALL BE AFFIXED TO THE INTERIOR SIDE OF THE ENCLOSURE DOOR.
 - 9.- A 2-COLOR ENGRAVED PLASTIC NAMEPLATE, ATTACHED WITH SCREWS, AND ENGRAVED AS INDICATED, SHALL BE PROVIDED FOR EACH MAIN BREAKER.
 - 10.- PROVIDE ON LAYOUT AND CIRCUIT DIAGRAM A BILL OF MATERIALS USED WITH CATALOG NUMBERS.
 - 11.- REFER TO T.S.C. TYPICAL DRAWING TY-1TSC-400*20 FOR POLE MOUNTED DISCONNECT MOUNTING DETAILS.

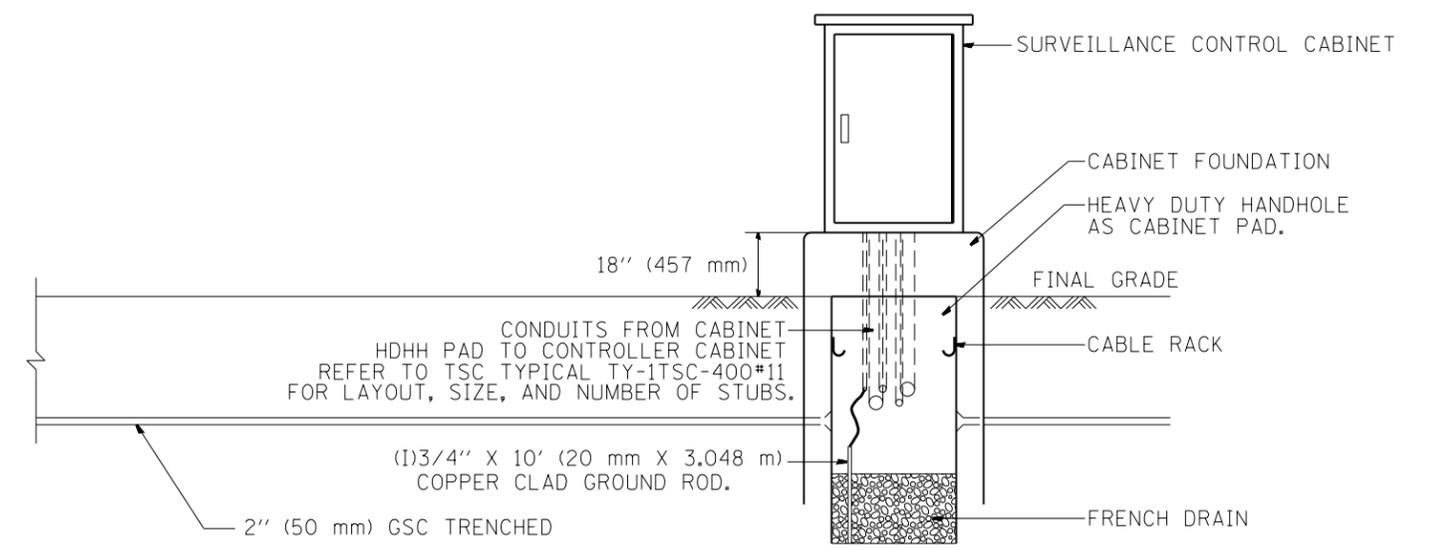
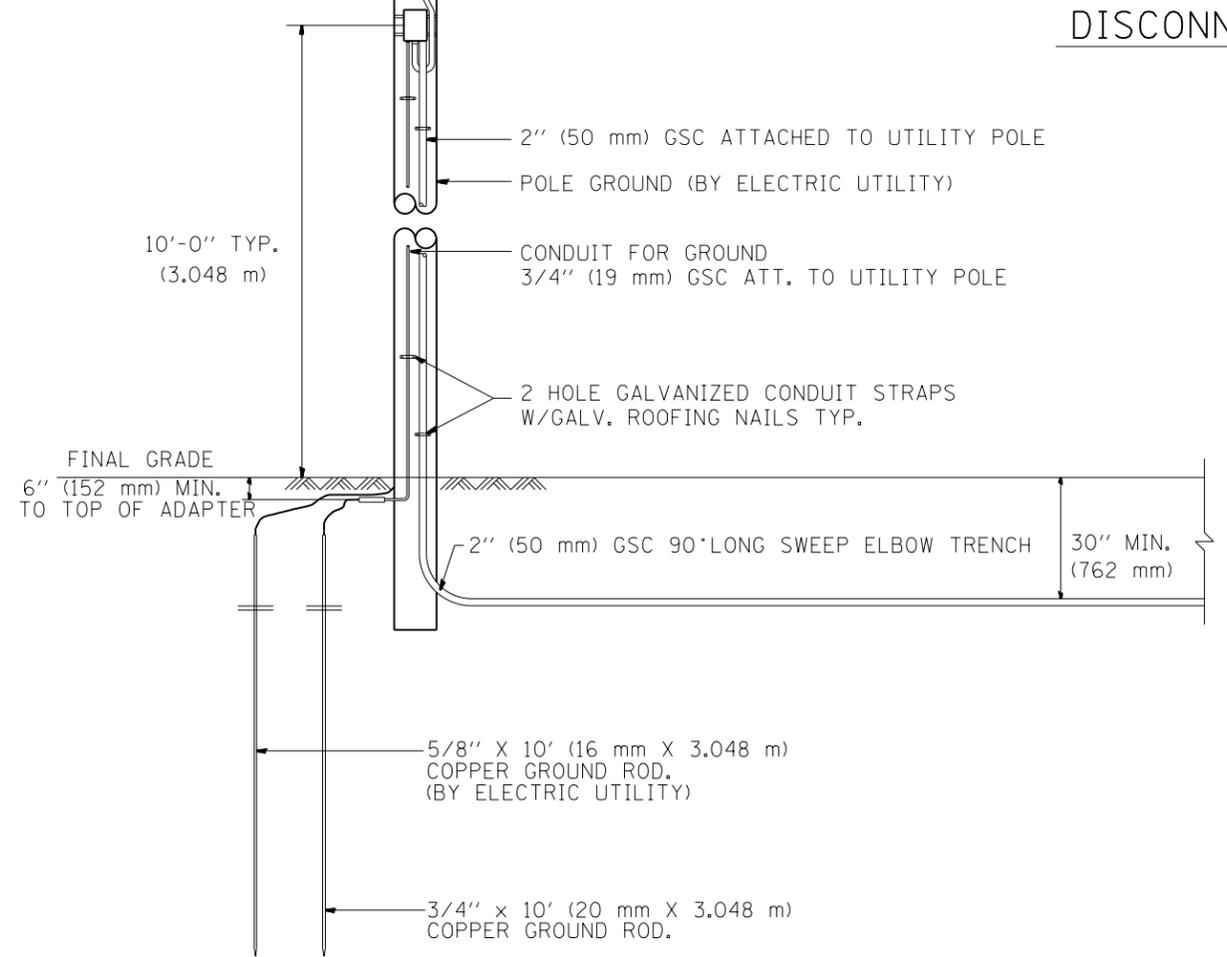
ELECTRIC SERVICE
GENERAL LAYOUT DIAGRAM

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 03/01/99	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	DISTRICT 1 SURVEILLANCE POLE-MOUNTED ELECTRIC SERVICE BOX DETAIL			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\mezag\d0287541\TSC TY.dgn		DRAWN - G.M.	REVISED - 03/30/99		SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.			886	834
		CHECKED - R.L.	REVISED - 04/99					CONTRACT NO. 60L70					
		DATE - 02/24/99	REVISED - 04/12/99					ILLINOIS FED. AID PROJECT					

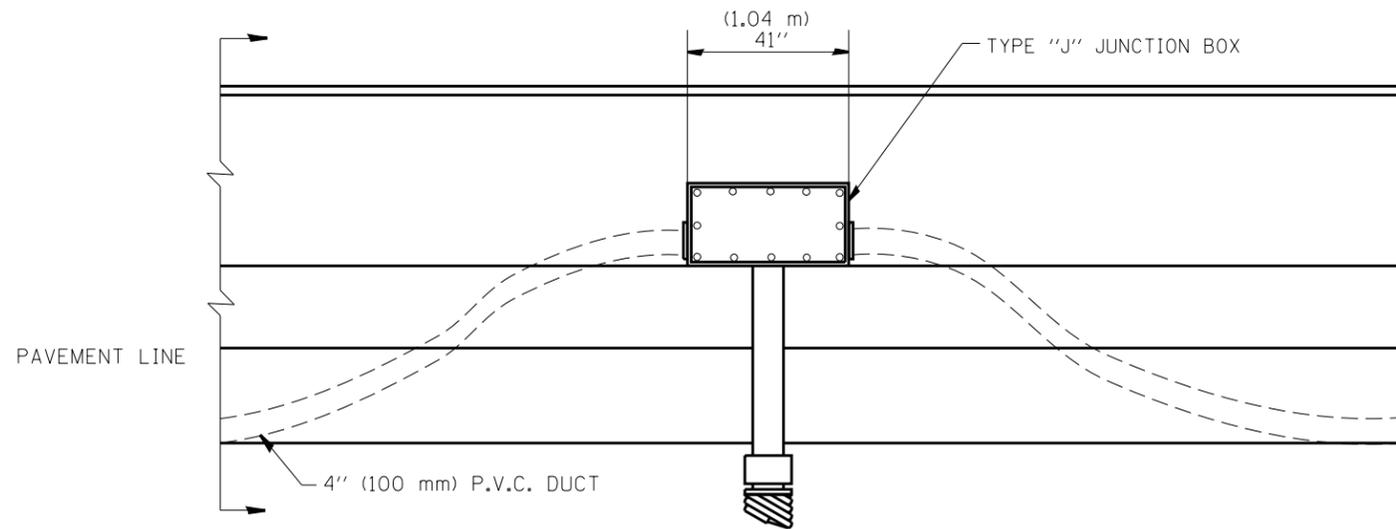


- NOTES:**
- 1.- ALL CONDUIT BUSHINGS SHALL HAVE AN ISOLATED THROAT.
 - 2.- PROVIDE HEAT SHRINK BOOT AT THE TOP OF THE SERVICE ENTRANCE CABLE FOR MOISTURE PROOFING.
 - 3.- ALL CONNECTIONS TO GROUND RODS SHALL BE EXOTHERMIC UNLESS OTHERWISE NOTED.
 - 4.- ATTACH INCOMING ELECTRIC SERVICE CABLE TO UTILITY POLE EVERY 5 FEET USING INSULATED U-NAIL.
 - 5.- PROVIDE CABLE RACK IN HANDHOLES.
 - 6.- ALL CONDUCTORS SHALL BE COPPER.
 - 7.- PROVIDE STAINLESS STEEL HARDWARE TO ATTACH L BRACKETS TO UNISTRUT AND TO SIGN HANGER.

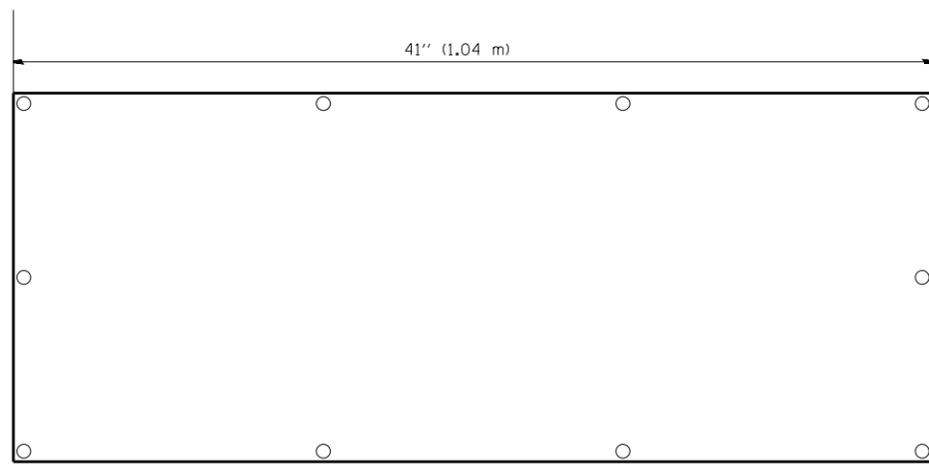
NTS
DISCONNECT MOUNTING DETAIL



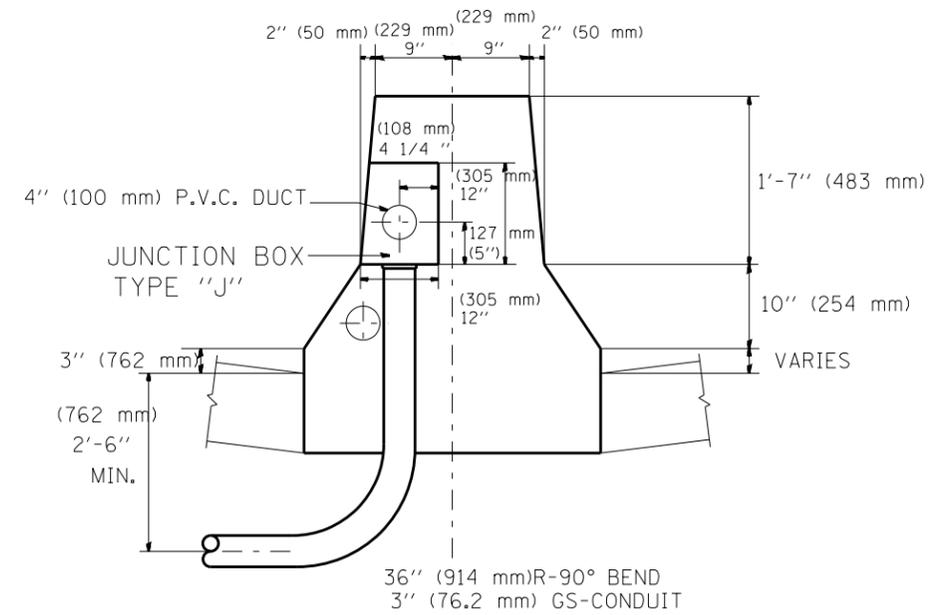
FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	POLE MOUNTED DISCONNECT MOUNTING DETAILS			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw\work\p\dot\mezag\d0287541\TSC1P.dgn		DRAWN - G.M.	REVISED -		SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	886	835
PLOT SCALE = 100.0000' / 1in.		CHECKED - R.L.	REVISED -					CONTRACT NO. 60L70					
PLOT DATE = 2/7/2013		DATE - 03/30/99	REVISED -										



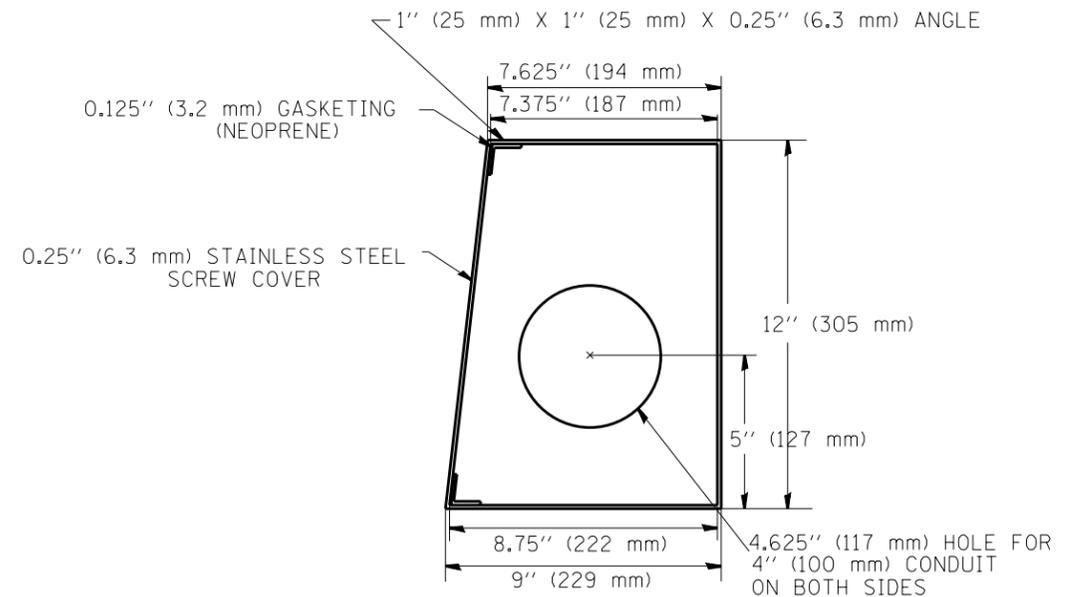
**ELEVATION
BARRIER WALL DUCT DETAIL**



FRONT VIEW



DOUBLE FACED BARRIER WALL

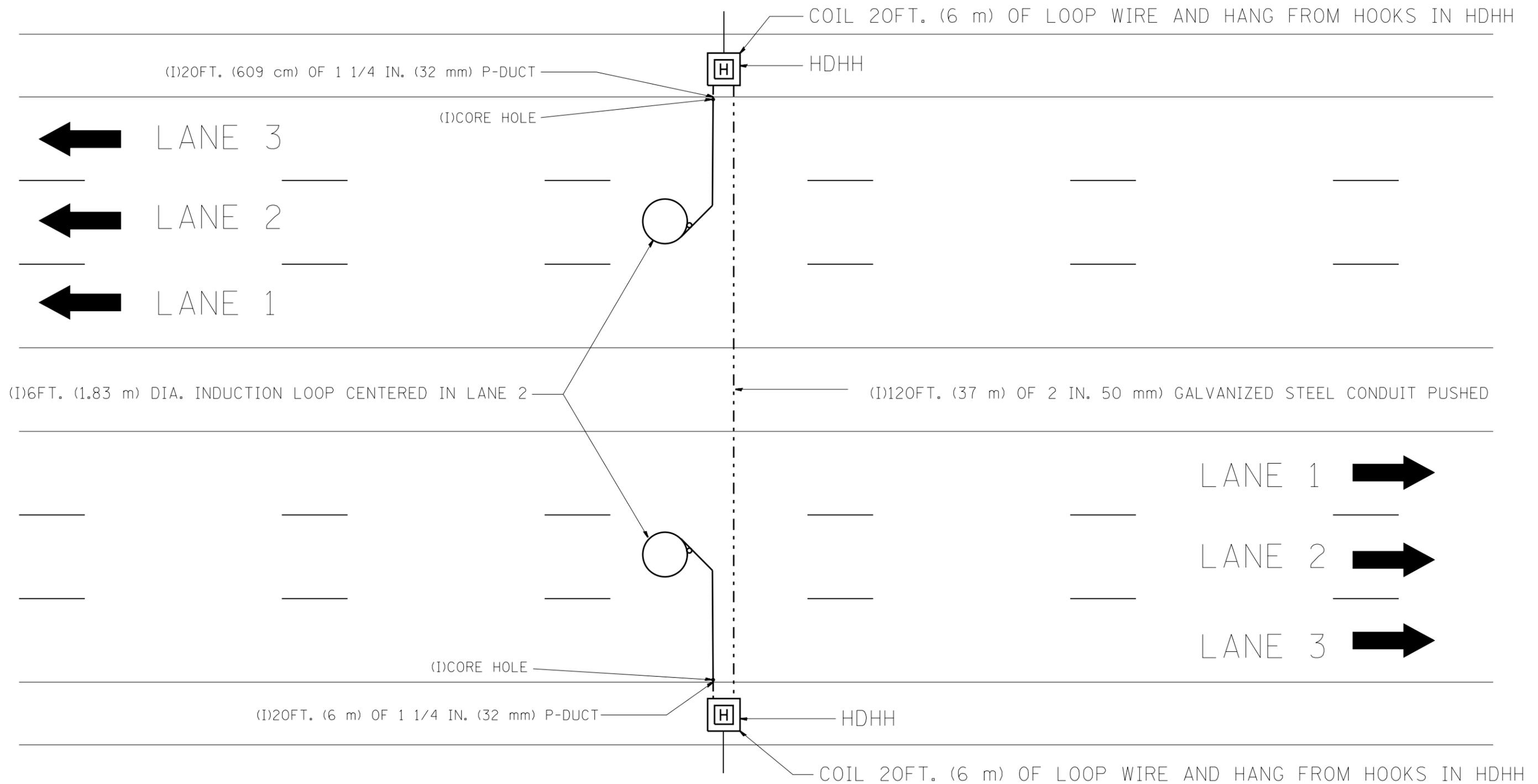


**SIDE VIEW
JUNCTION BOX TYPE "J"**

ALL WELDS SHALL BE CONTINUOUS AND LEAK PROOF
BOX AND COVER SHALL BE 0.25" (6.4 mm) TYPE 316 STAINLESS STEEL

BARRIER WALL SHALL BE GAPPED A MINIMUM OF 15' (4.57 m) FOR PROPER PLACEMENT OF JUNCTION BOX TYPE "J"
AND FOR A SMOOTH TRANSITION OF 4" (100 mm) PVC SURVEILLANCE DUCT(S) FROM BARRIER WALL FOOTER INTO JUNCTION BOX.

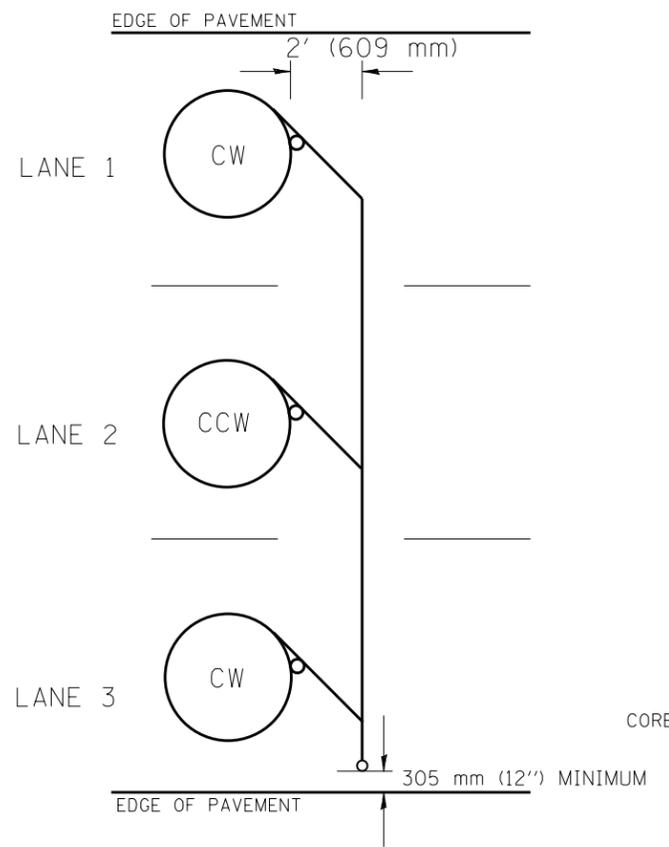
FILE NAME =	USER NAME = mezag	DESIGNED - J.G.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	JUNCTION BOX "TYPE J" IN DOUBLE FACED BARRIER WALL			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 2/7/2013	CHECKED - J.G.	REVISED -		SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60L70		ILLINOIS FED. AID PROJECT		
		DATE - 04-19-05	REVISED -		TRAFFIC SYSTEMS CENTER (TY-1TSC-400#30)							



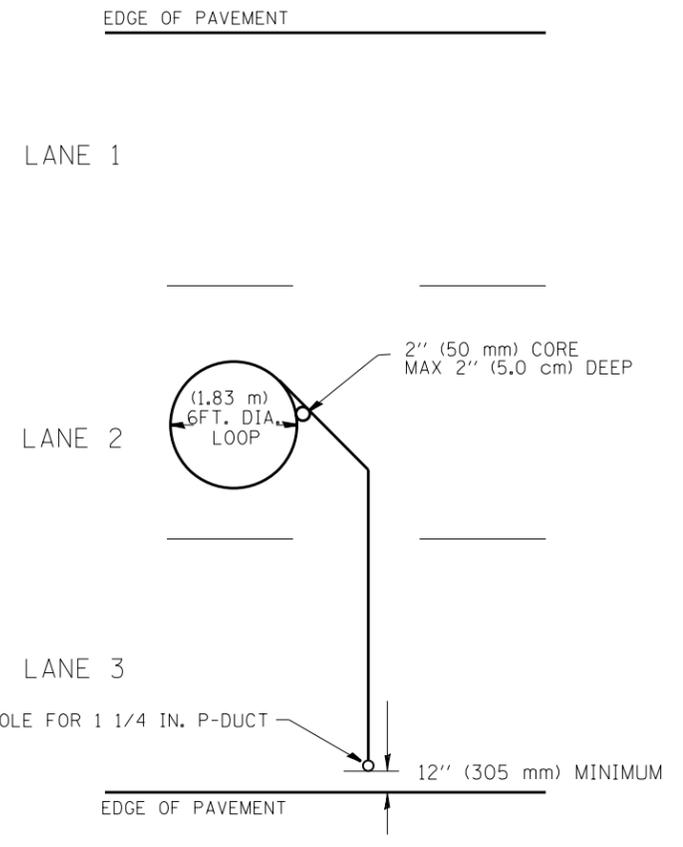
NOTE:

THE COST OF LOOP WIRE IN HDHH IS INCIDENTAL TO THE INDUCTION LOOP.
IT SHALL NOT BE MEASURE FOR PAYMENT.

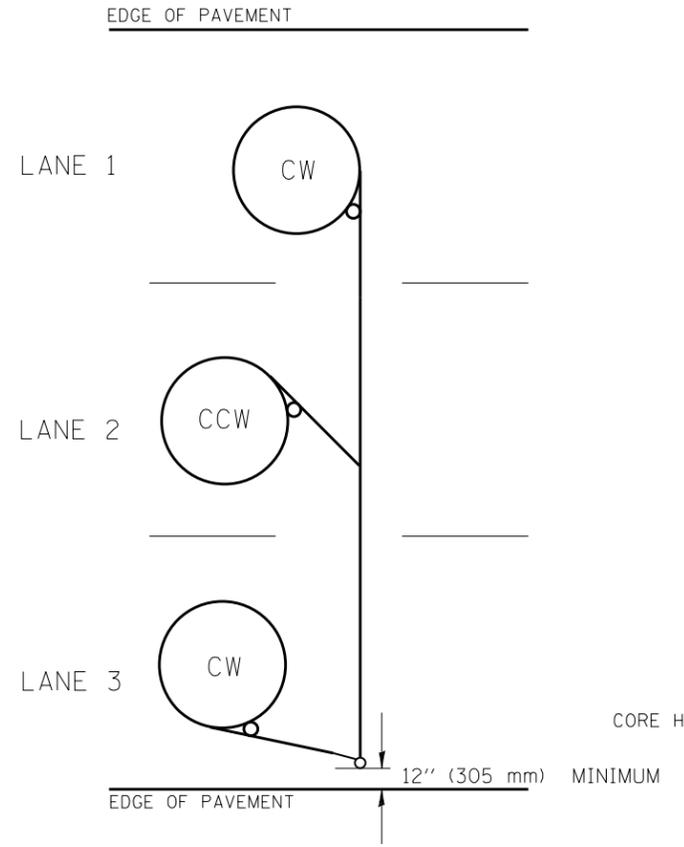
FILE NAME =	USER NAME = mezag	DESIGNED - J.G.	REVISED - 12-02-2010	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	ONE LANE COUNT STATION		F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw\work\p\dot\mezag\d0287541\TSCTYP.dgn	PLOT SCALE = 100.0000' / in.	DRAWN - G.M.	REVISED -							886	837	
	PLOT DATE = 2/7/2013	CHECKED - J.G.	REVISED -		SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60L70		
		DATE - 01-31-07	REVISED -					FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



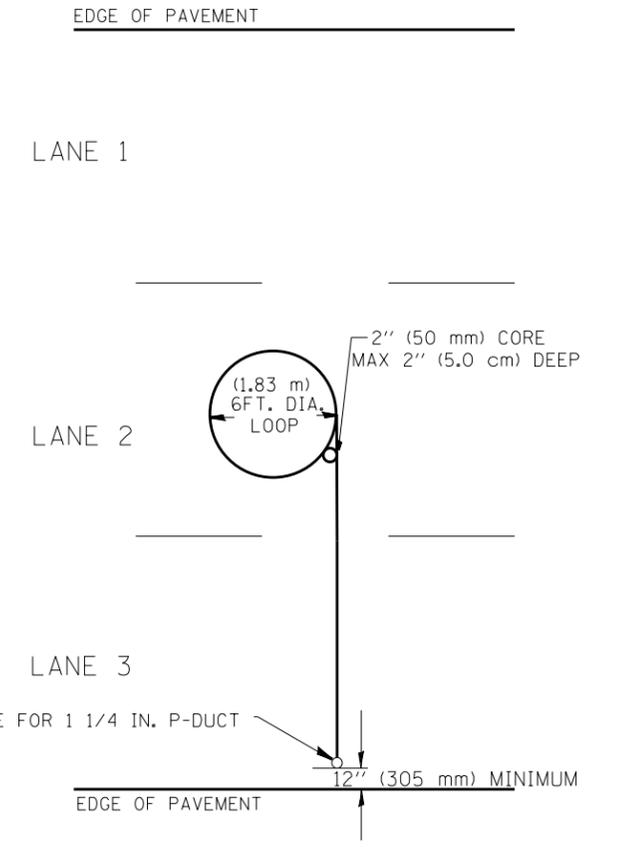
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



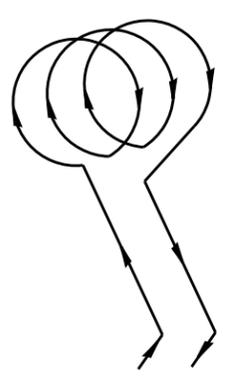
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



WIRING DETAILS

NOTES

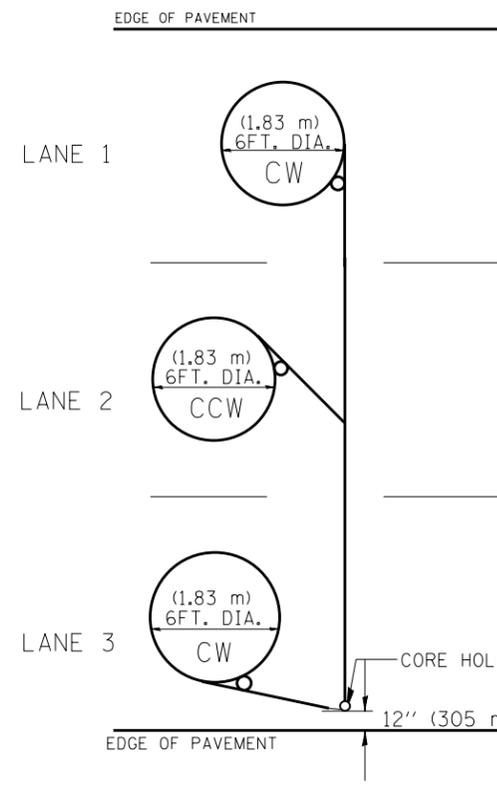
1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 6/94
et:\pw\work\p1dot\mezag\d0287541\TSC1P.dgn		DRAWN - G.M.	REVISED - 9/96
	PLOT SCALE = 100.0000' / 1in.	CHECKED - R.L.	REVISED - R.L. 03/2011
	PLOT DATE = 2/3/2012	DATE - 6-22-94	REVISED -

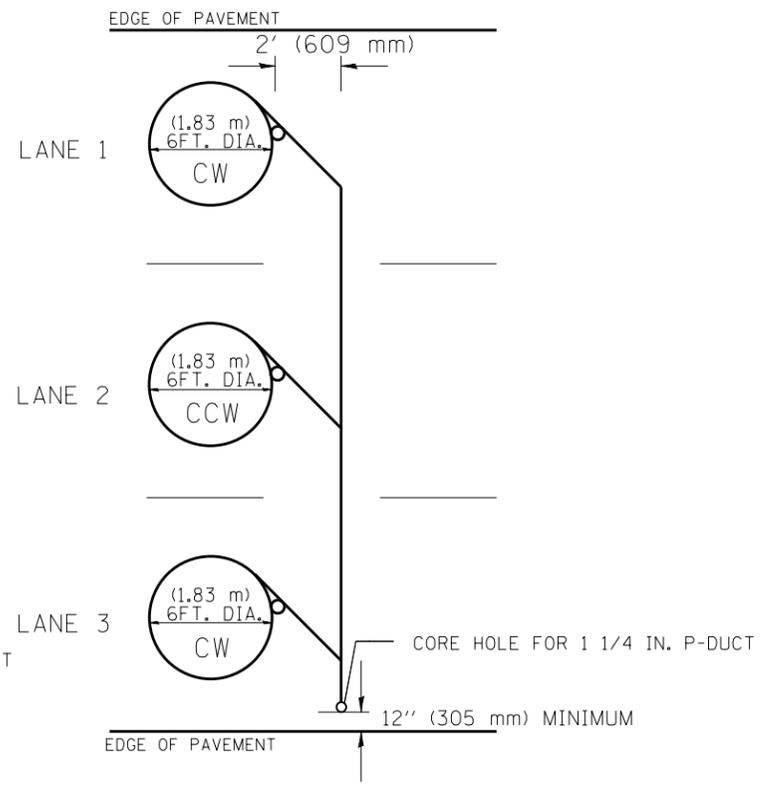
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

EXISTING ROUND LOOP INSTALLATION			
SCALE: NONE	SHEET NO.	OF SHEETS	STA. TO STA.

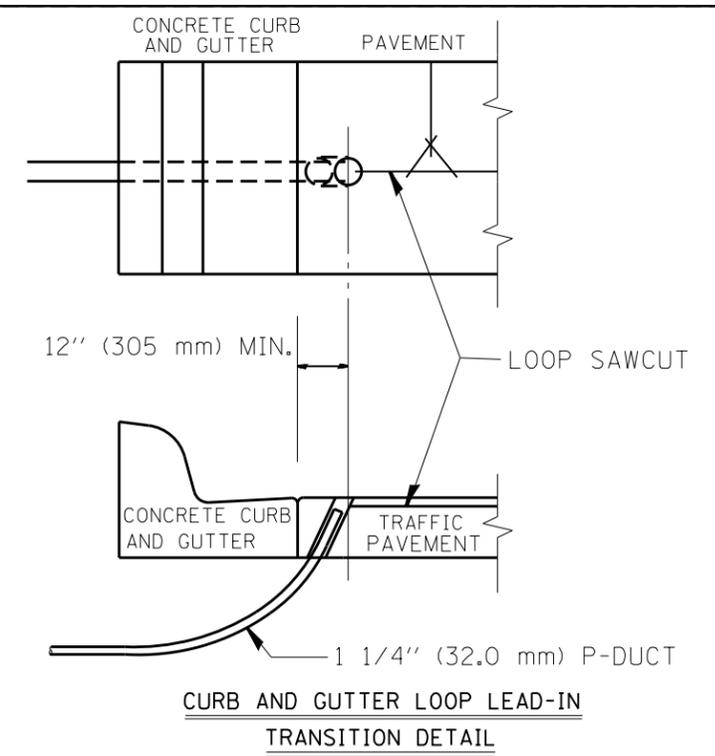
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	838
CONTRACT NO. 60L70				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



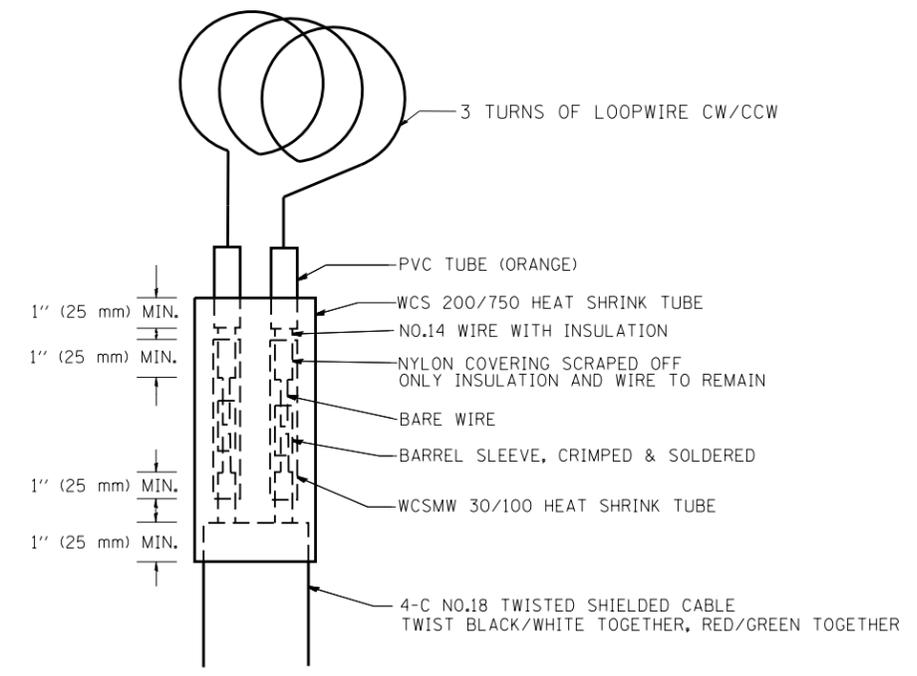
TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL LAYOUT FOR MULTIPLE LANE ROADWAY



CURB AND GUTTER LOOP LEAD-IN TRANSITION DETAIL



LOOP SPLICING REQUIREMENTS

NOTES

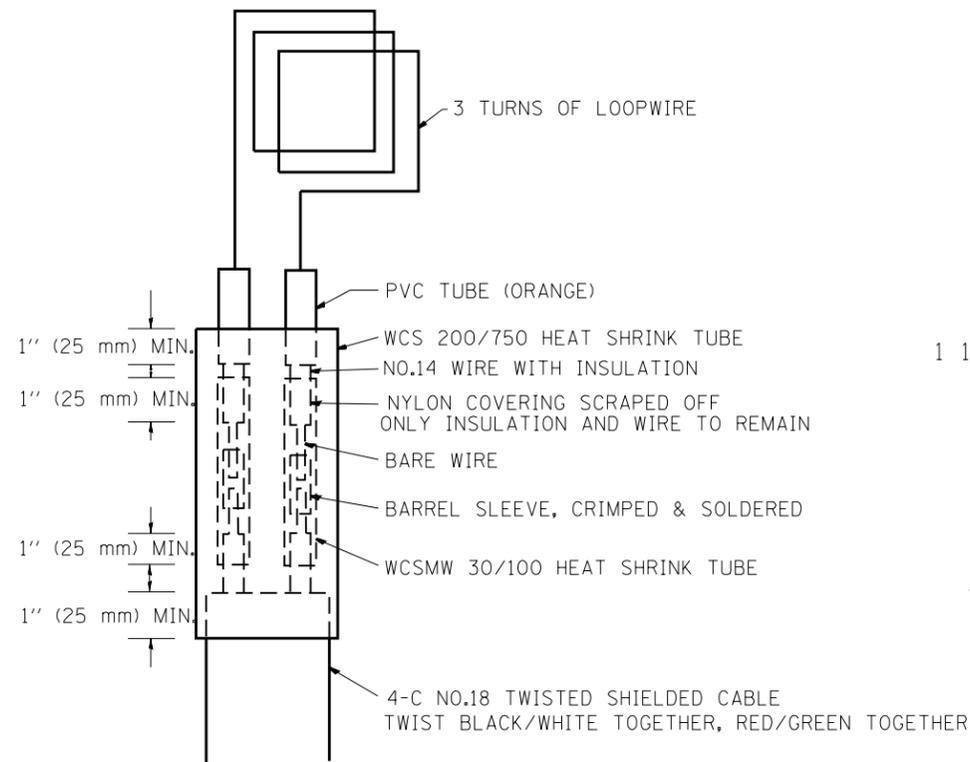
1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150FT. (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 6/94
et:\pw\work\p1dot\mezag\d0287541\TSCYP.dgn		DRAWN - G.M.	REVISED - 10/96
	PLOT SCALE = 100.0000' / in.	CHECKED - R.L.	REVISED - R.L. 03/2011
	PLOT DATE = 2/3/2012	DATE - 6-22-94	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER**

EXISTING ROUND INDUCTION LOOP TYPICALS			
SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	

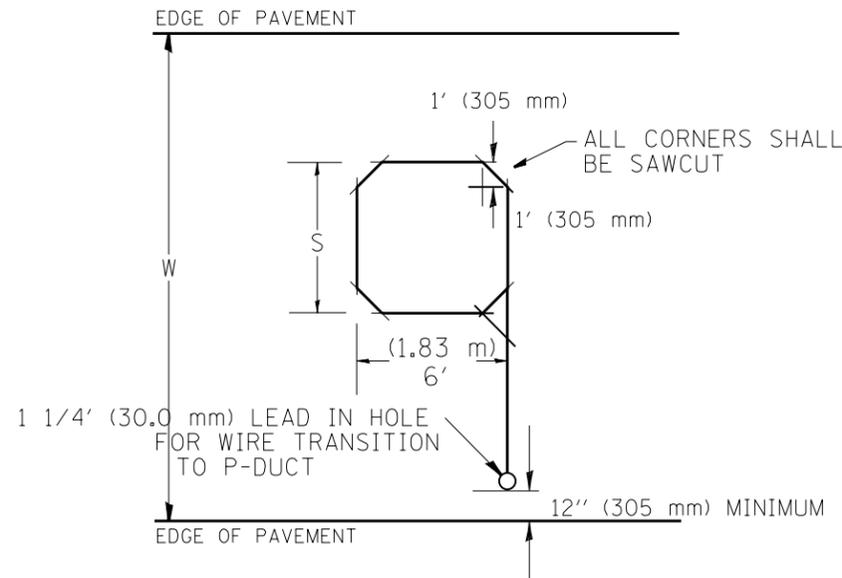
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	839
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L70	



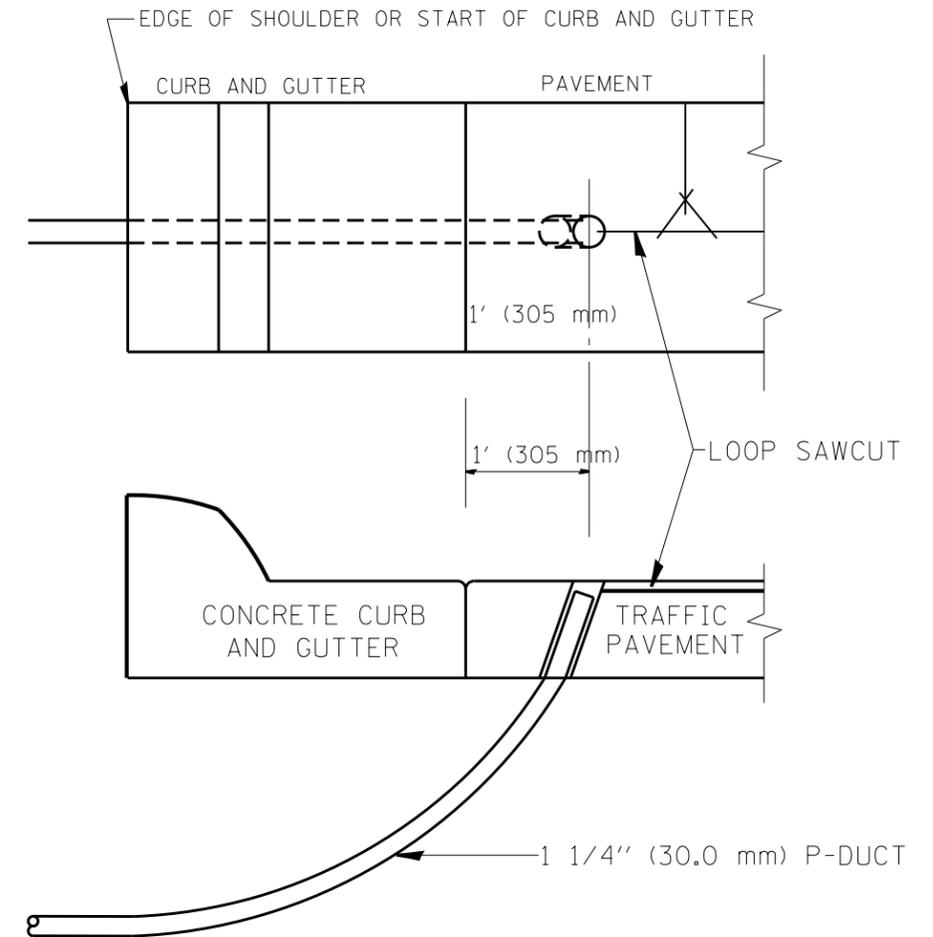
LOOP SPLICING REQUIREMENTS

MINIMUM 1" (25 mm) HEAT SHRINK TUBING OVERLAP ON WIRE, PVC & SHIELDED CABLE TO FORM WATER TIGHT SEAL

WIDTH (W)	WIDTH (S)
12' (3.7 m)	8' (2.5 m)
13' (4.0 m)	9' (2.8 m)
14' (4.3 m)	10' (3.1 m)
15' (4.6 m)	11' (3.4 m)
16' (4.9 m)	12' (3.7 m)
17' (5.2 m)	13' (4.0 m)
18' (5.5 m)	14' (4.3 m)
19' (5.8 m)	15' (4.6 m)
20' (6.1 m)	18' (4.9 m)
21' (6.4 m)	17' (5.2 m)
22' (6.7 m)	18' (5.5 m)
23' (7.0 m)	19' (5.8 m)
24' (7.3 m)	20' (6.1 m)
25' (7.6 m)	21' (6.4 m)



TYPICAL "S" FT. BY 6' (1.83 m) INDUCTION LOOP SAWCUT LAYOUT FOR RAMPS

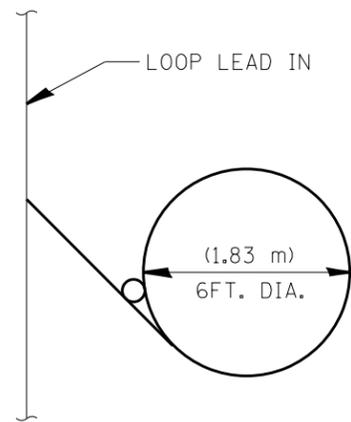


CURB AND GUTTER LOOP LEAD-IN TRANSITION DETAIL

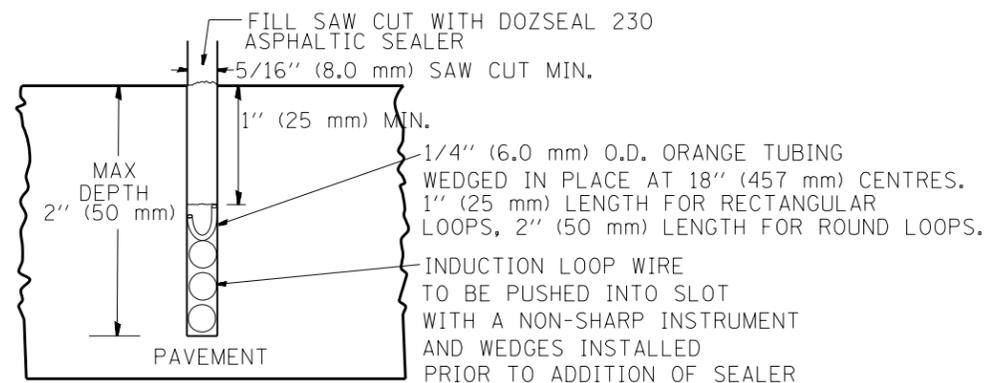
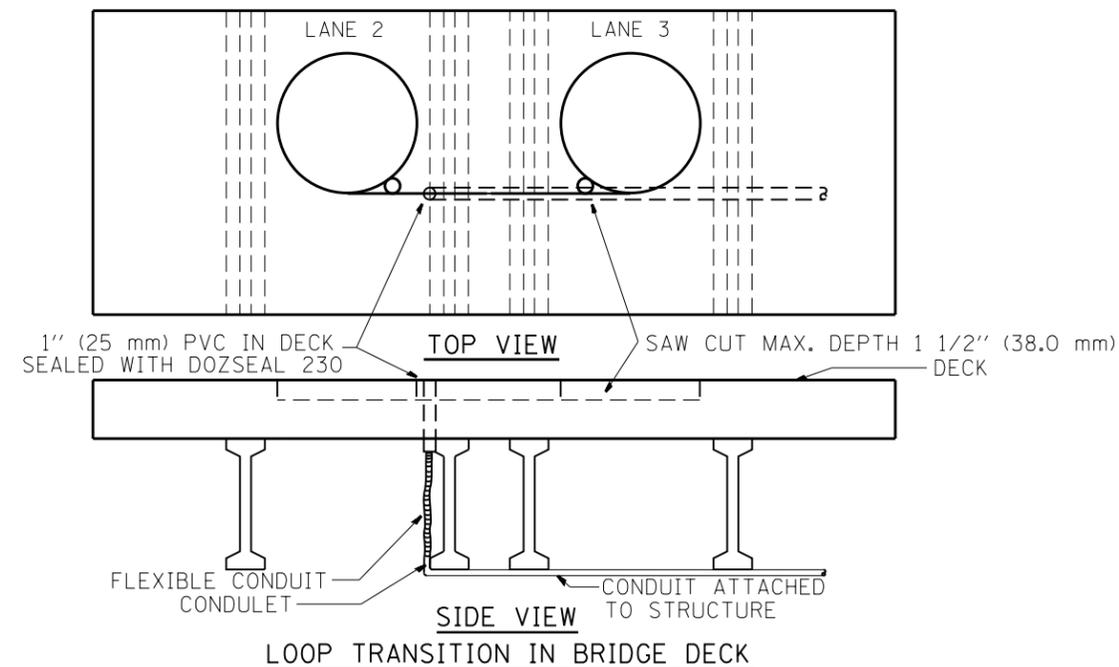
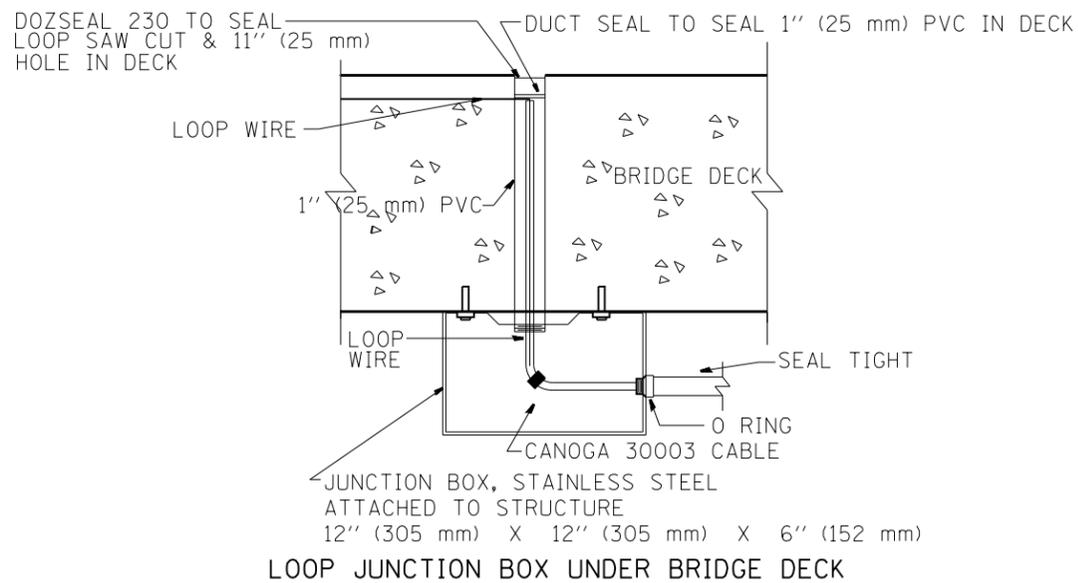
NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

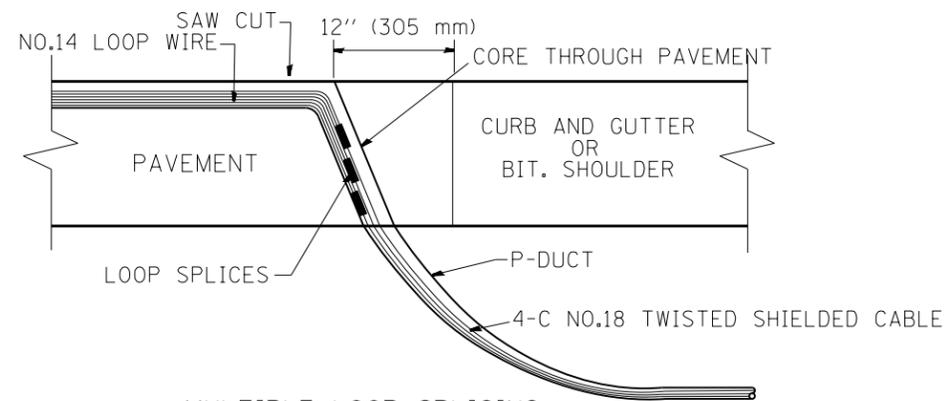
FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 6/94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	RECTANGULAR INDUCTION LOOP TYPICAL			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\p\dot\mezag\d0287541\TSC1P.dgn		DRAWN - G.M.	REVISED - 11/95								886	840
PLOT SCALE = 100.0000' / 1"		CHECKED - R.L.	REVISED - 05/96		SCALE: NONE			CONTRACT NO. 60L70				
PLOT DATE = 2/7/2013		DATE - 6-22-94	REVISED - 10/96		SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			



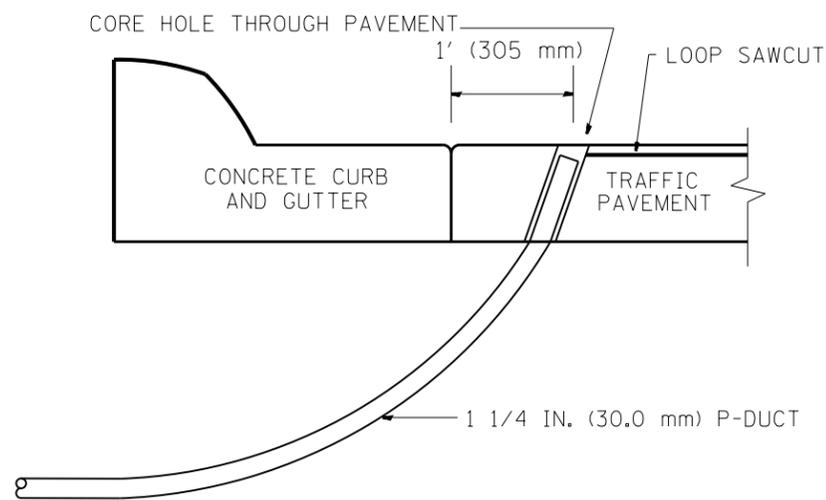
TYPICAL LOOP SAWCUT LAYOUT



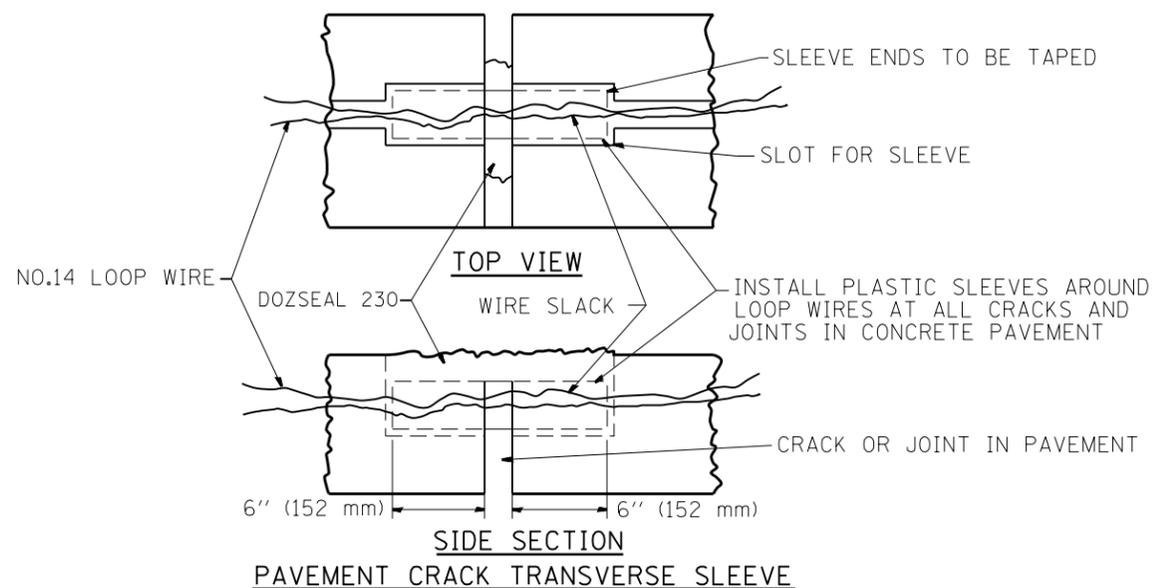
LOOP CROSS SECTION IN PAVEMENT



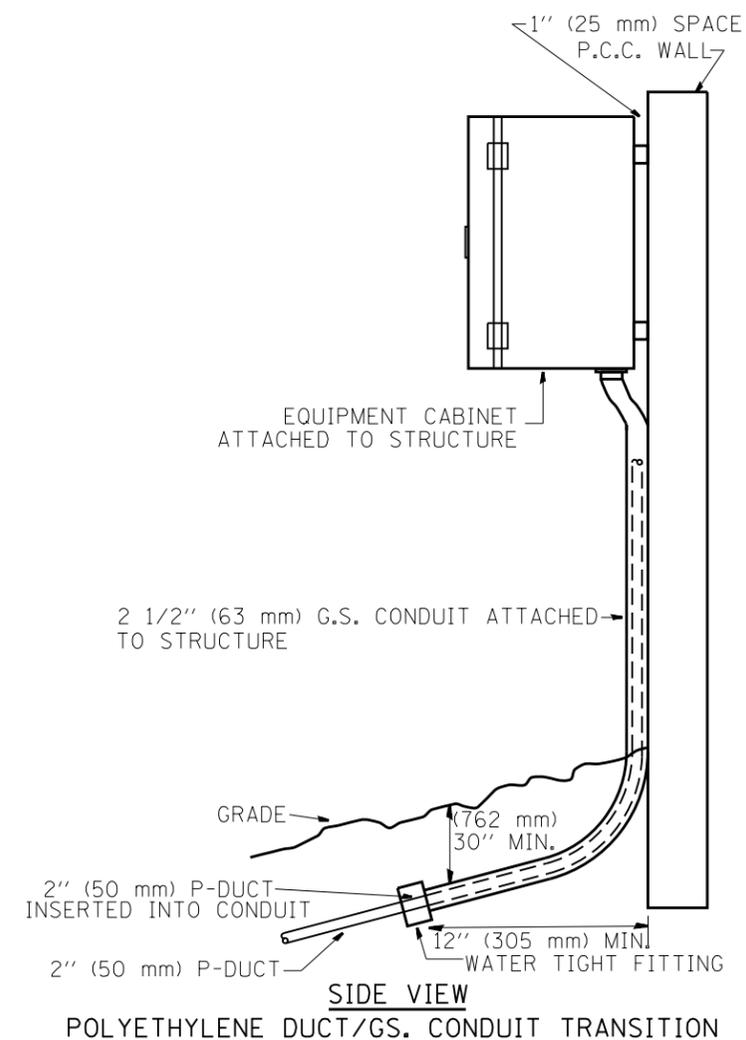
MULTIPLE LOOP SPLICING



SIDE SECTION LOOP LEAD-IN TRANSITION DETAIL



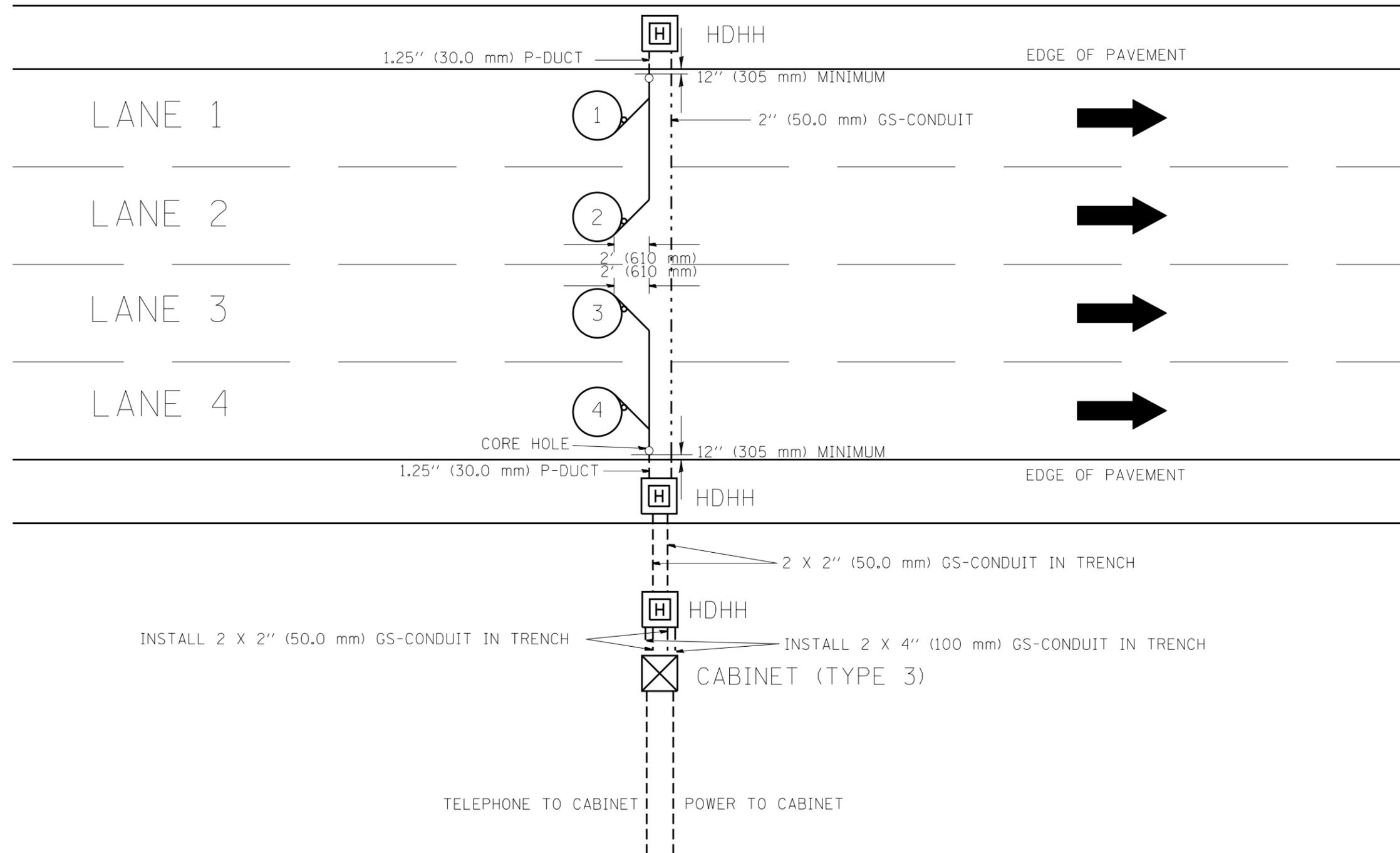
PAVEMENT CRACK TRANSVERSE SLEEVE



POLYETHYLENE DUCT/GS. CONDUIT TRANSITION

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 06/94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	LOOP, CONDUIT & DUCT INSTALLATION DETAILS			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw\work\p\idot\mezag\d0287541\TSCTYP.dgn		DRAWN - G.M.	REVISED - 03/95		SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	886	841
		CHECKED - R.L.	REVISED - 11/95					CONTRACT NO. 60L70					
		DATE - 06/22/94	REVISED - 10/96										

NOTE: ALL MAIN LINE LOOPS ARE ROUND 6' DIA. (1.83 m)



NOTES

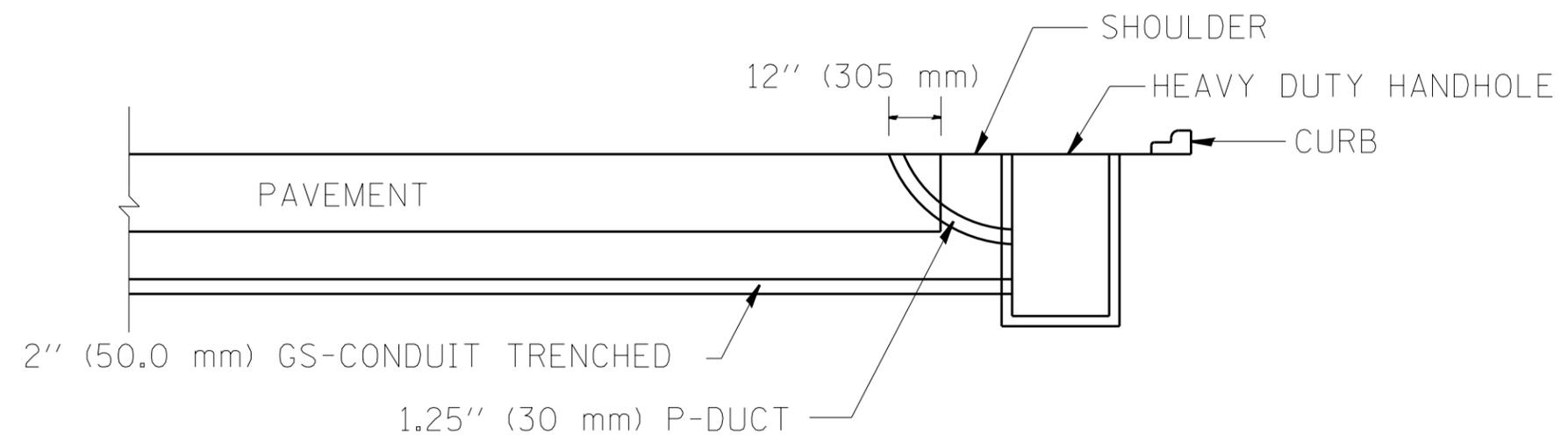
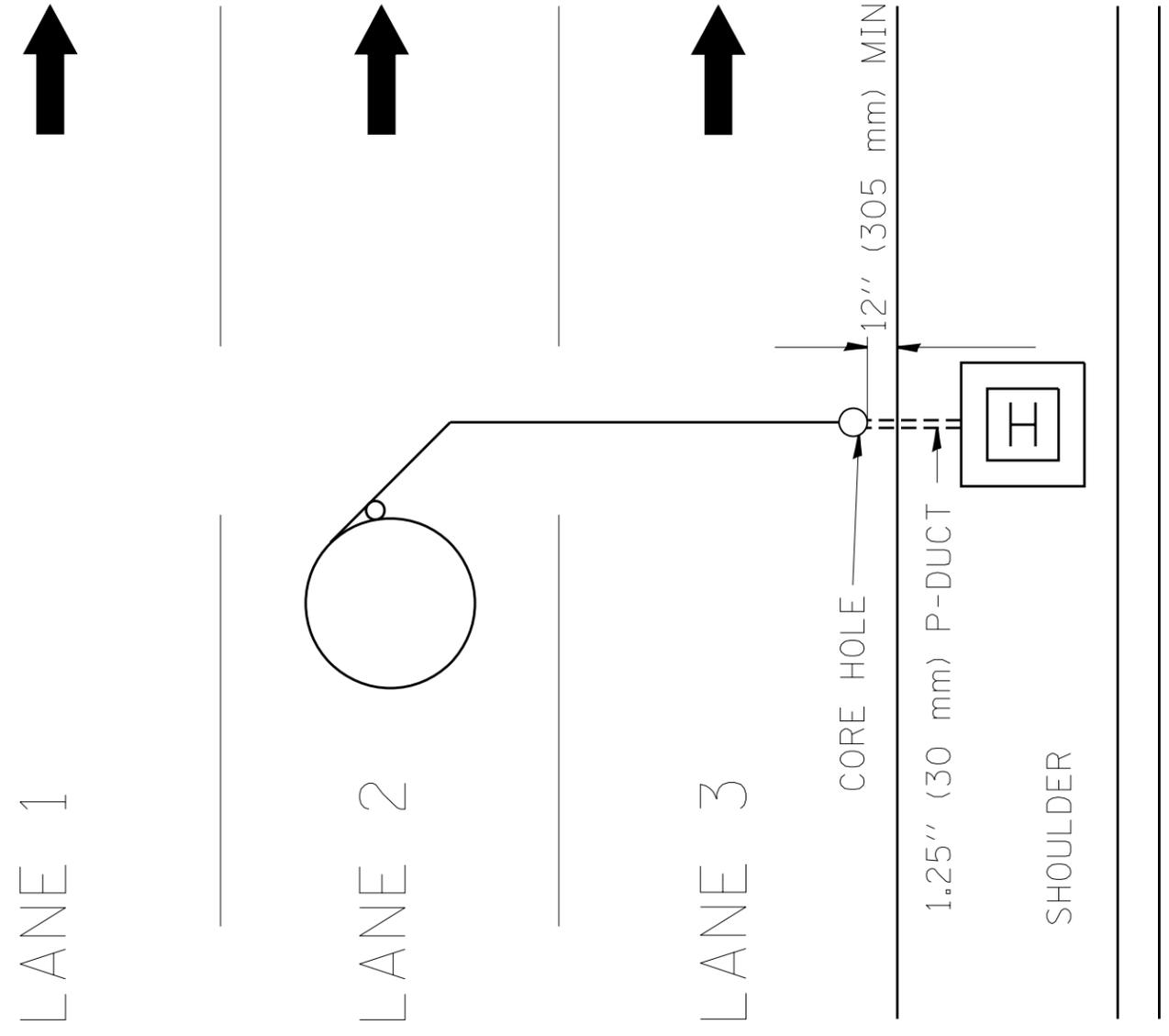
1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - R.L. 03/2011
et:\pw\work\p1dot\mezag\d0287541\TSCTYP.dgn		DRAWN - G.M.	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED - R.L.	REVISED -
	PLOT DATE = 2/7/2013	DATE - 6-27-94	REVISED -

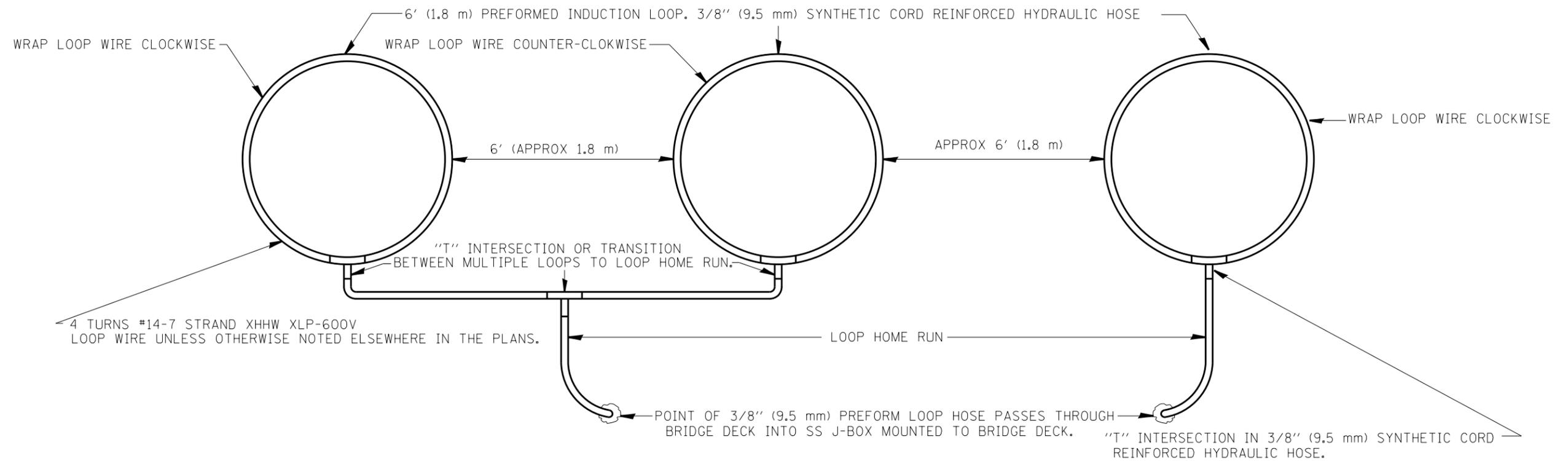
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

NEW CONSTRUCTION ROUND INDUCTION LOOP TYPICAL INSTALLATION			
SCALE: NONE	SHEET NO.	OF SHEETS	STA. TO STA.

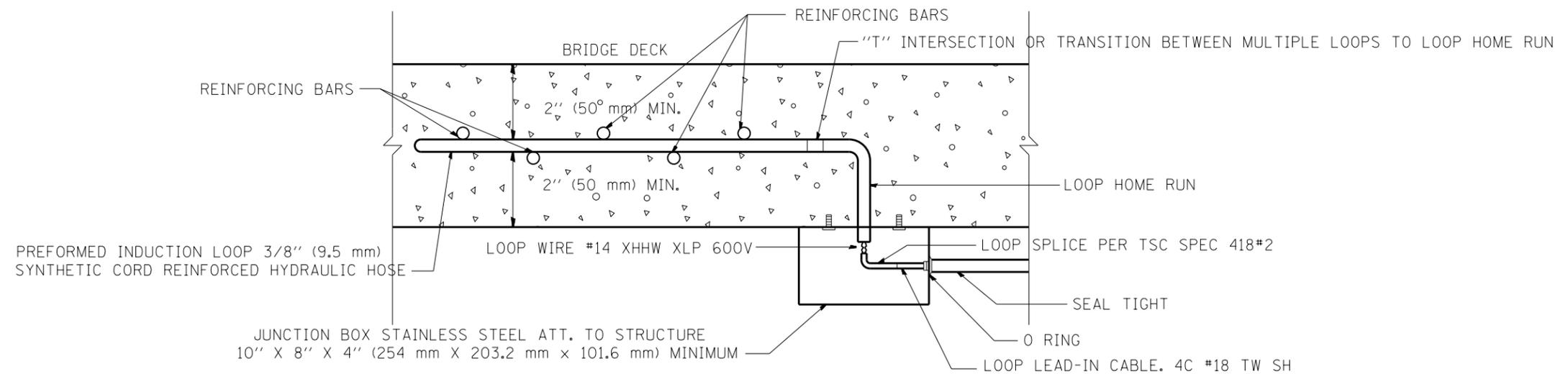
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	842
CONTRACT NO. 60L70				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - R.L.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	DIVE HOLE DUCT SYSTEM			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\mezag\d0287541\TSCTYP.dgn	DRAWN - G.M.	REVISED - T.C.								886	844	
PLOT SCALE = 100.0000' / 1in.	CHECKED - R.L.	REVISED -	CONTRACT NO. 60L70									
PLOT DATE = 2/7/2013	DATE - 11/7/95	REVISED -	SCALE: NONE		SHEET NO.	OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



TYPICAL 3 LANE COUNT STATION EMBEDDED IN BRIDGE DECK

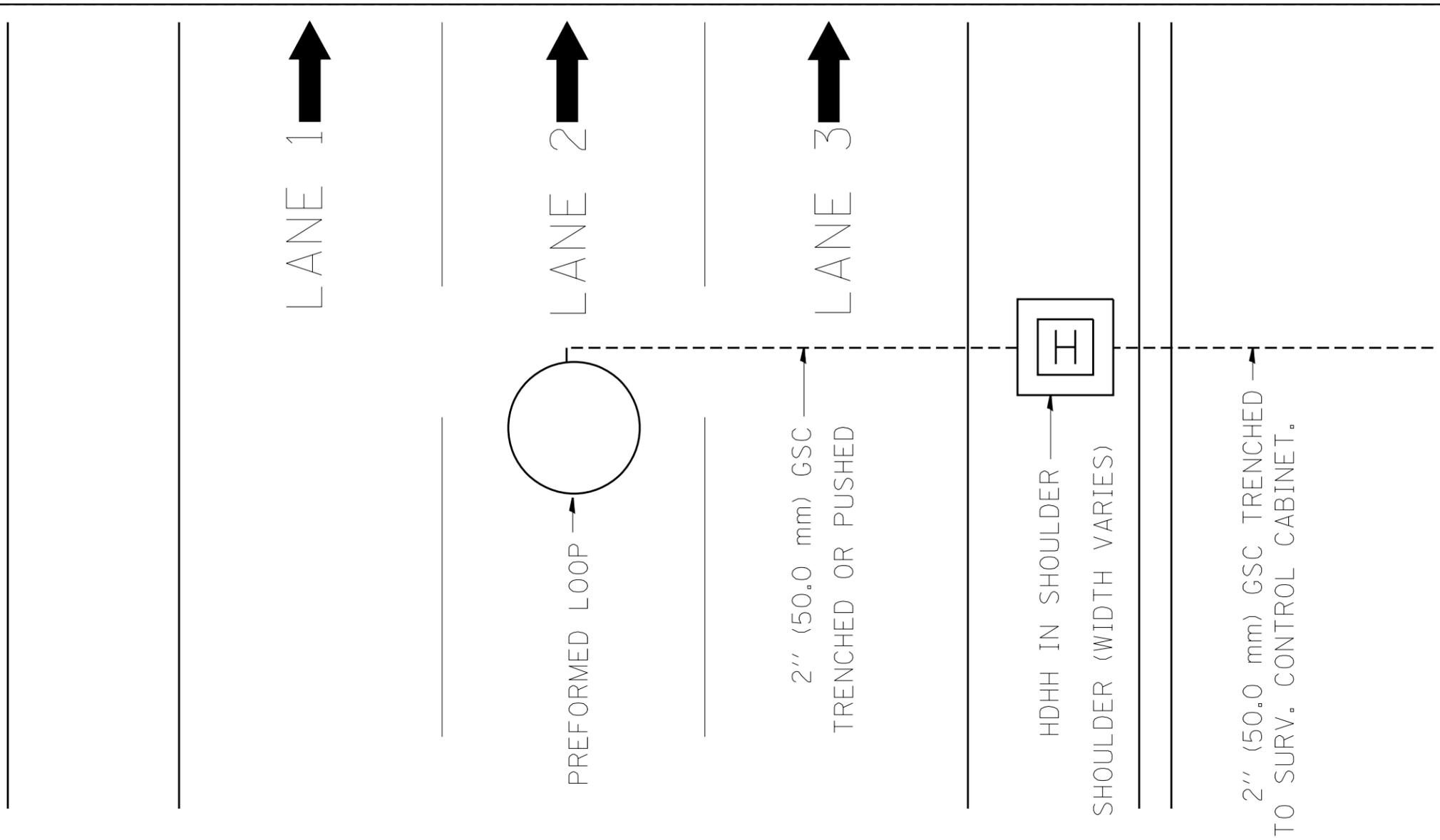


LOOP JUNCTION BOX UNDER BRIDGE DECK

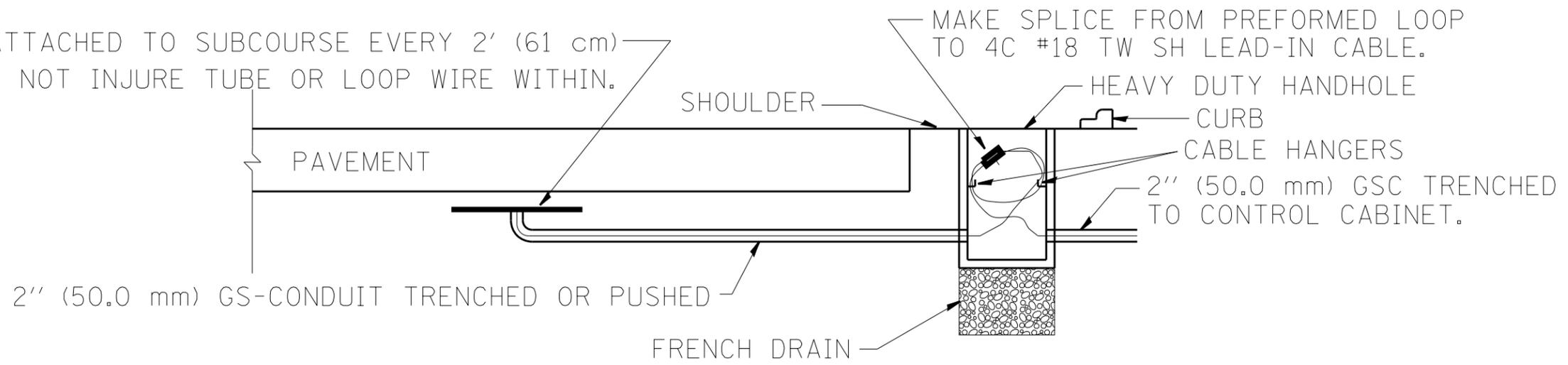
NOTES:

- 1.- EACH INDUCTION LOOP SHALL HAVE ITS OWN LEAD-IN CABLE TO CABINET.
- 2.- INDUCTION LOOPS SHALL NOT BE CONNECTED IN SERIES WITH OTHER LOOPS.
- 3.- LOOPS LOCATED OVER 1000' (300 m) FROM CABINET SHALL REQUIRE 5 TURNS OF #14 WIRE.

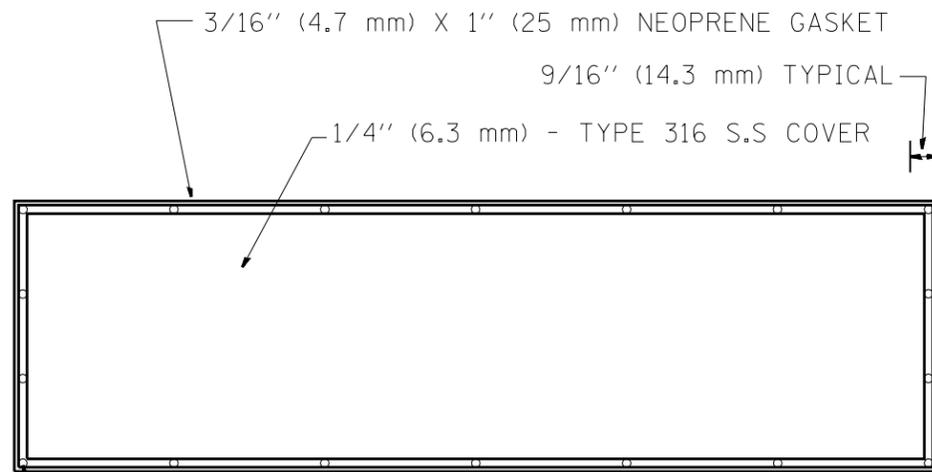
FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	PREFORMED LOOP INSTALLATION IN BRIDGE DECK			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\mezag\d0287541\TSC1P.dgn	PLOT SCALE = 100.0000' / 1in.	DRAWN - G.M.	REVISED -								886	845
	PLOT DATE = 2/7/2013	CHECKED - R.L.	REVISED -		SCALE: NONE			SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60L70		
		DATE - 2-97	REVISED -		TRAFFIC SYSTEMS CENTER (TY-1TSC-418#9)			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



PREFORMED LOOP ATTACHED TO SUBCOURSE EVERY 2' (61 cm)
ATTACHMENT SHALL NOT INJURE TUBE OR LOOP WIRE WITHIN.



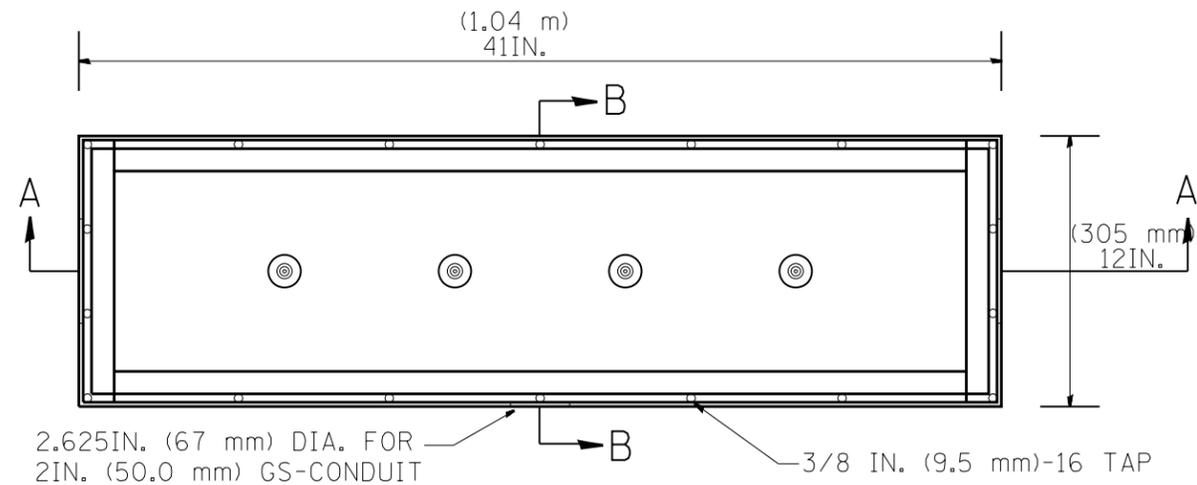
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et:\pw\work\p1dot\mezag\d0287541\TSC1P.dgn	PLOT SCALE = 100.0000' / 1in.	DRAWN - G.M.	REVISED -					SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.
	PLOT DATE = 2/7/2013	CHECKED - R.L.	REVISED -		CONTRACT NO. 60L70								
		DATE - 03/18/02	REVISED -		TRAFFIC SYSTEMS CENTER (TY-1TSC-418#10)								



DRILL AND COUNTERSINK TOP OF COVER FOR
3/8" (9.5 mm) 16 X 3/4" (19 mm)
FLAT HEAD STAINLESS STEEL SLOTTED SCREWS

BACK VIEW

STAINLESS STEEL COVER WITH NEOPRENE GASKET

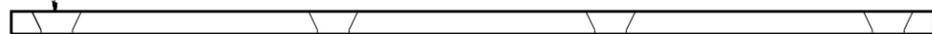


2.625 IN. (67 mm) DIA. FOR
2 IN. (50.0 mm) GS-CONDUIT

3/8 IN. (9.5 mm)-16 TAP

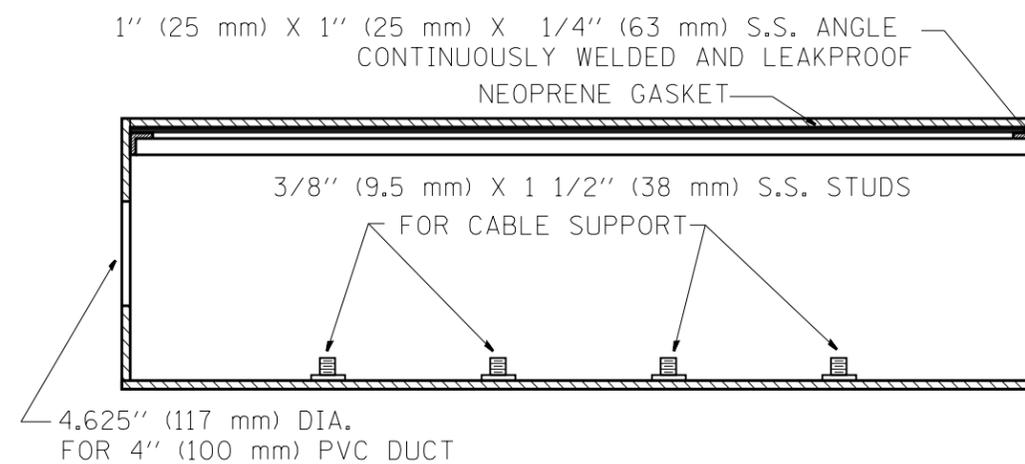
TOP VIEW

STAINLESS STEEL JUNCTION BOX TYPE "J"



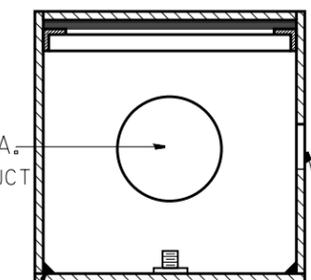
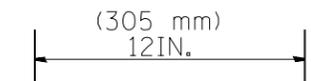
SIDE VIEW

STAINLESS STEEL COVER



4.625" (117 mm) DIA.
FOR 4" (100 mm) PVC DUCT

SIDE VIEW SECTION A-A



WELDED SEAMS (TYP.)
2.625 IN. (67 mm) DIA. HOLE

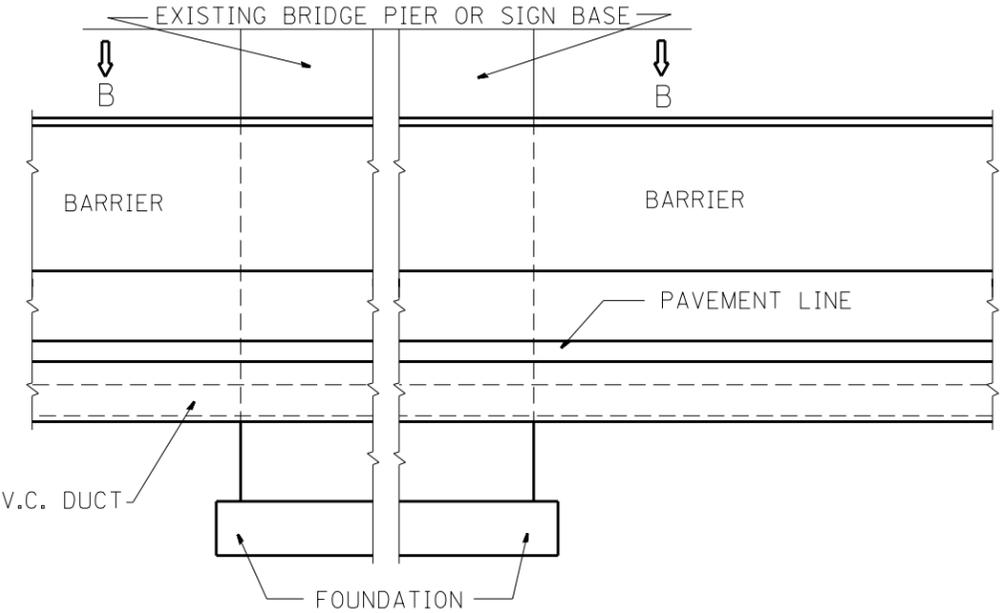
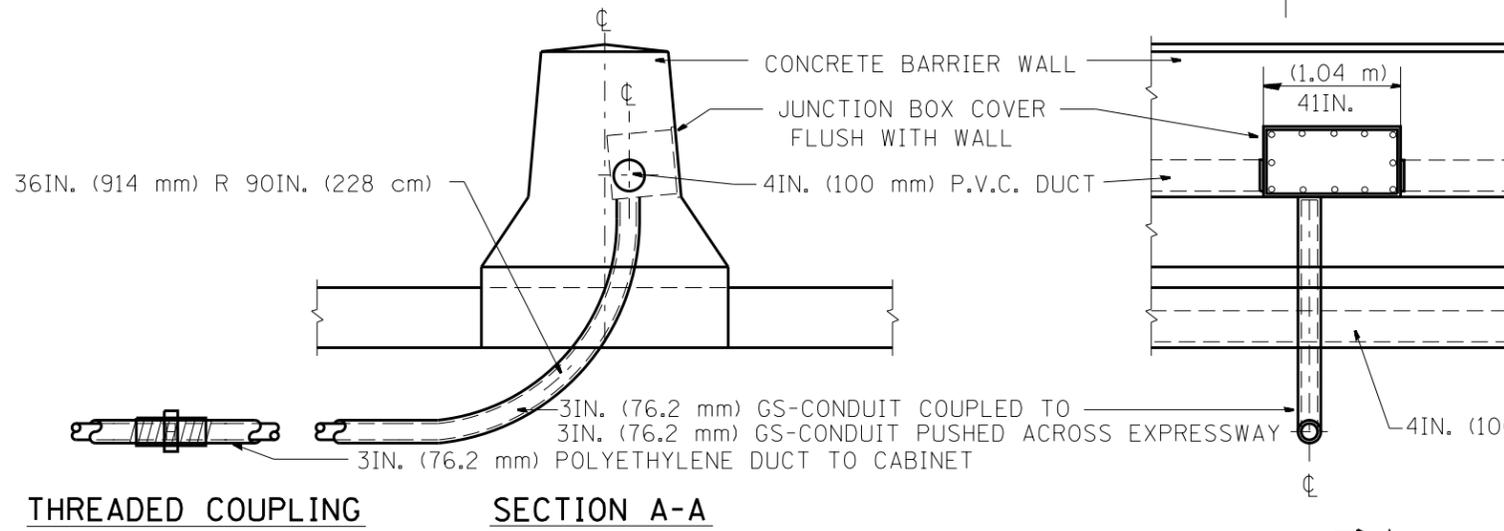
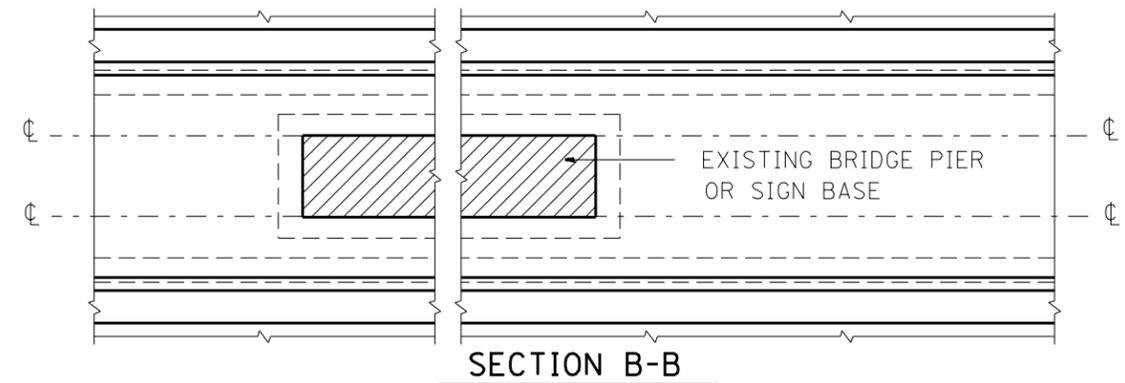
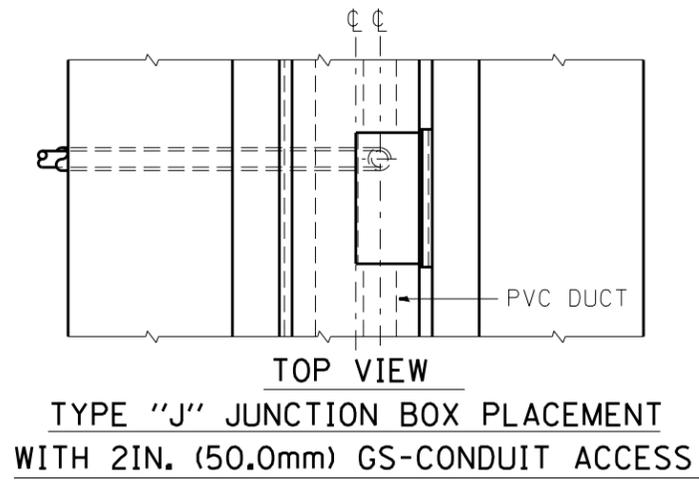
SIDE VIEW SECTION B-B

NOTE:

JUNCTION BOX TO BE CONSTRUCTED OF
1/4 IN. (6.3 mm) TYPE 316 STAINLESS STEEL
ALL WELDS SHALL BE CONTINUOUS AND LEAKPROOF
ALL COVERS SHALL BE UNIVERSAL IN FIT.

BARRIER WALL SHALL BE GAPPED A MINIMUM OF 15FT. (4.57 m) FOR PROPER PLACEMENT OF JUNCTION BOX TYPE "J" AND FOR A SMOOTH TRANSITION OF 4IN. (100 mm) PVC SURVEILLANCE DUCT(S) FROM BARRIER WALL FOOTER INTO JUNCTION BOX.

FILE NAME =	USER NAME = mezag	DESIGNED - R.L.	REVISED - 06/94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	CONCRETE BARRIER WALL JUNCTION BOX TYPE "J"			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
et:\pw\work\p\idot\mezag\d0287541\TSCTYP.dgn	DRAWN - G.M.	REVISED - 10/96	REVISED - 10/96								886	847		
PLOT SCALE = 100.0000' / in.	CHECKED - R.L.	REVISED -	REVISED -					CONTRACT NO. 60L70						
PLOT DATE = 2/7/2013	DATE - 06/22/94	REVISED -	REVISED -					SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

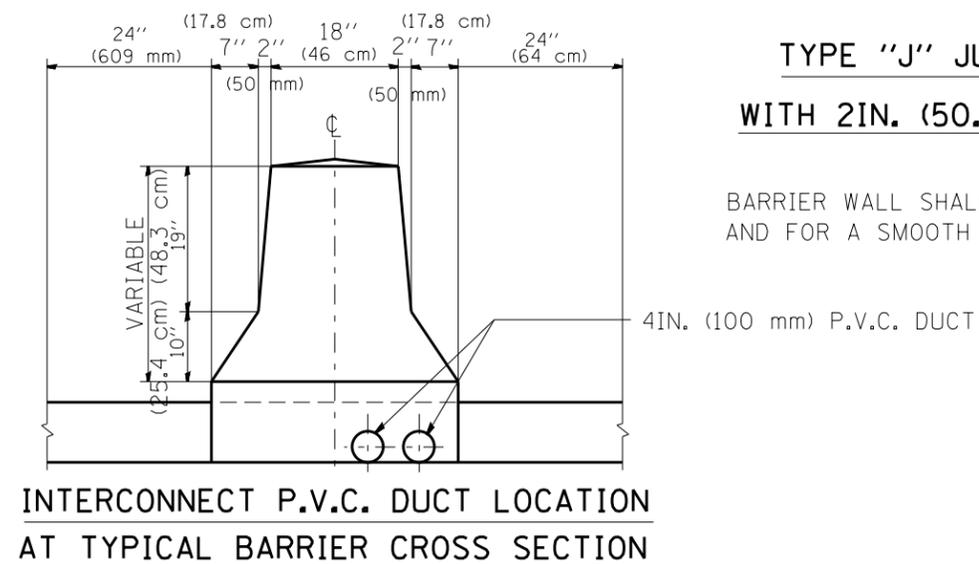


TYPE "J" JUNCTION BOX PLACEMENT
WITH 2IN. (50.0 mm) GS-CONDUIT ACCESS

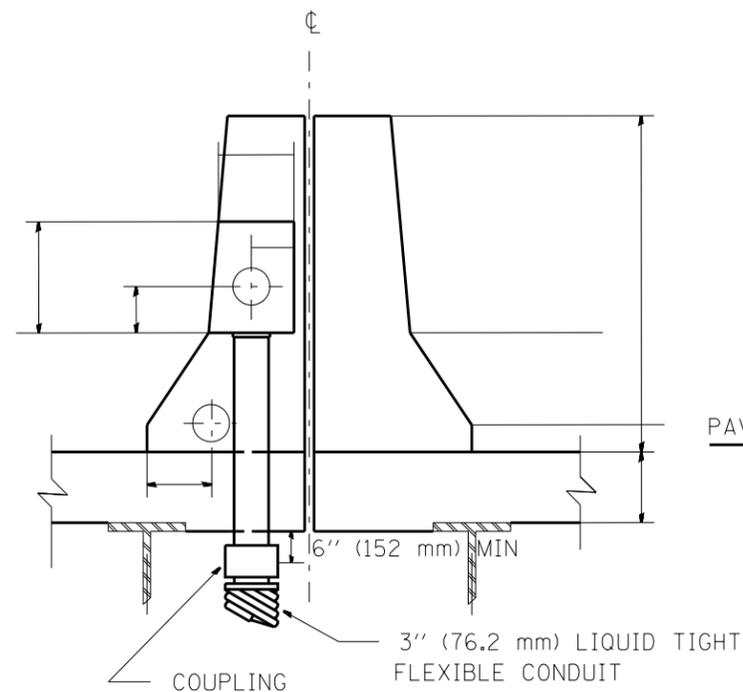
(SEE NOTE NO. 1)

BARRIER WALL AT BRIDGE PIER
P.V.C. DUCT PLACEMENT

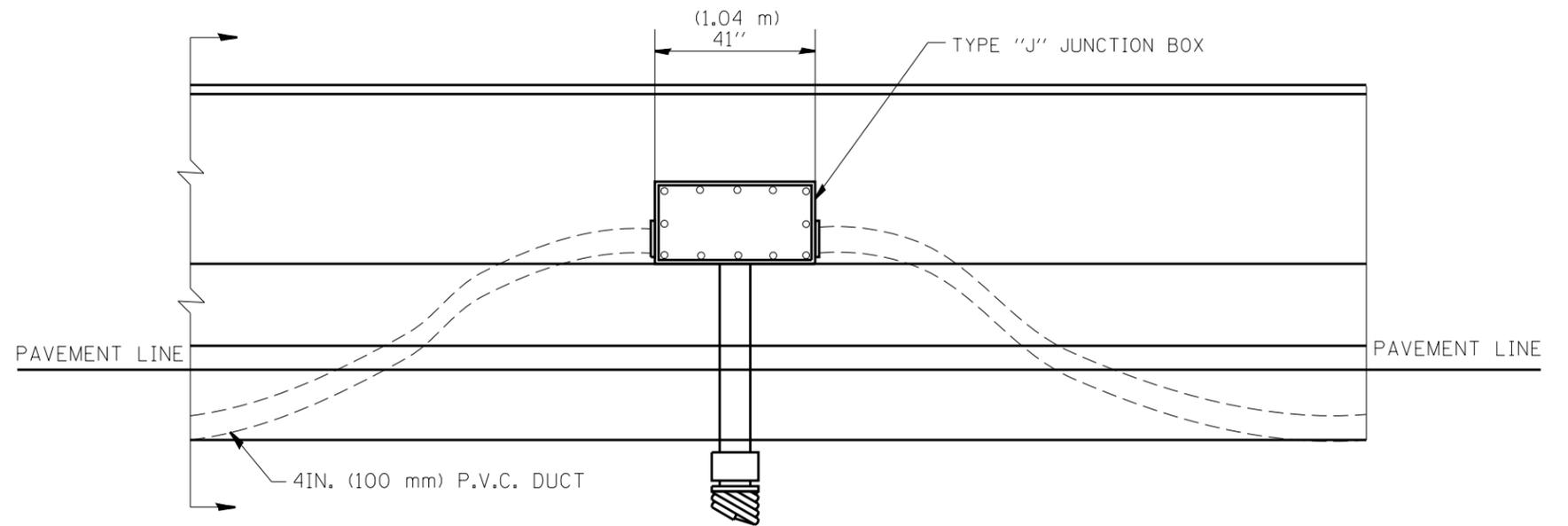
BARRIER WALL SHALL BE GAPPED A MINIMUM OF 15FT. (4.57 m) FOR PROPER PLACEMENT OF JUNCTION BOX TYPE "J" AND FOR A SMOOTH TRANSITION OF 4IN. (100 mm) PVC SURVEILLANCE DUCT(S) FROM BARRIER WALL FOOTER INTO JUNCTION BOX.



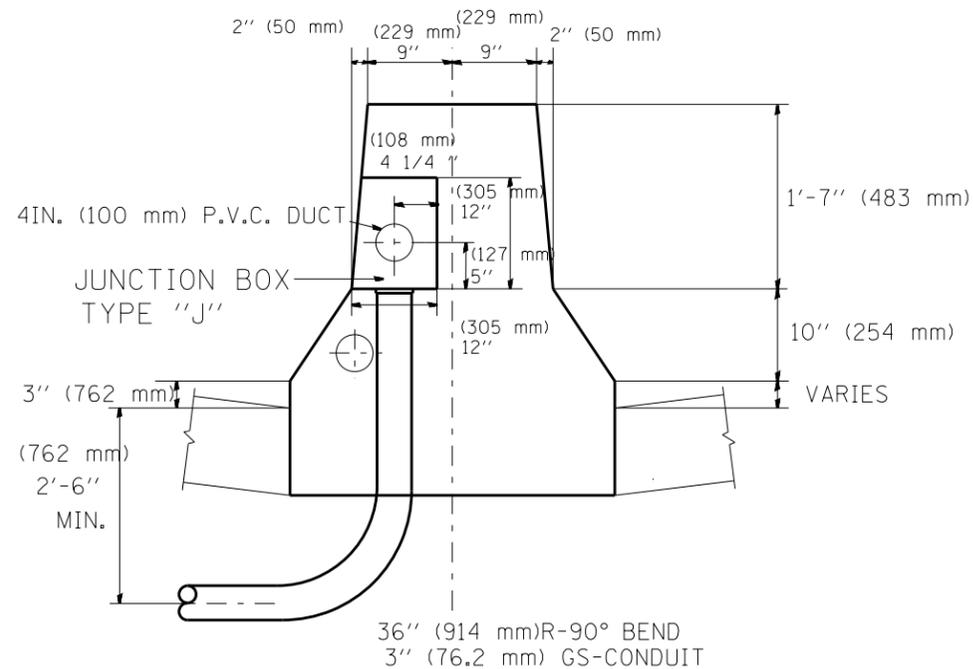
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et:\pw\work\p\idot\mezag\d0287541\TSCTYP.dgn	DRAWN - G.M.	REVISED - 10/96	SCALE: NONE			SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
PLOT SCALE = 100.0000' / 1"	CHECKED - R.L.	REVISED -	CONTRACT NO. 60L70								
PLOT DATE = 2/7/2013	DATE - 06/22/94	REVISED -	TRAFFIC SYSTEMS CENTER (TY-1TSC-663#2)								



**DOUBLE SINGLE FACED BARRIER WALL
ON BRIDGE DECK**



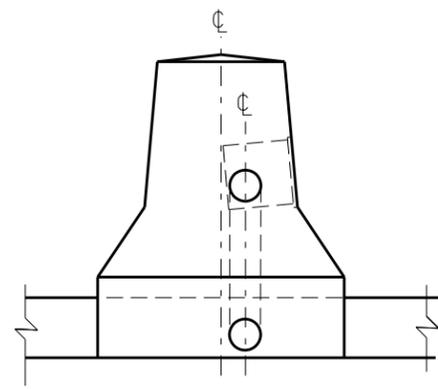
**ELEVATION
BARRIER WALL DUCT DETAIL
ON BRIDGE DECK**



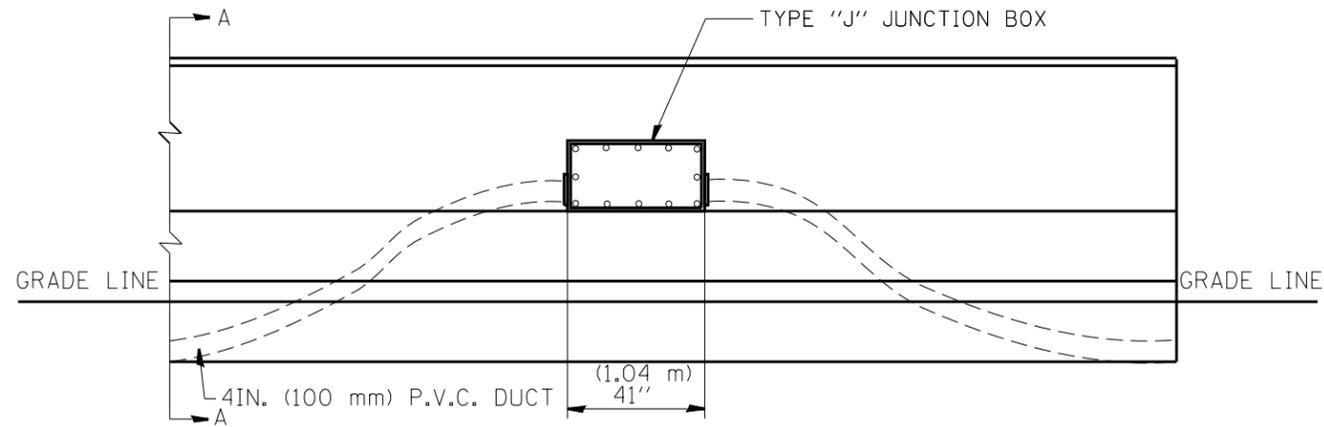
DOUBLE FACED BARRIER WALL

BARRIER WALL SHALL BE GAPPED A MINIMUM OF 15FT. (4.57 m) FOR PROPER PLACEMENT OF JUNCTION BOX TYPE "J"
AND FOR A SMOOTH TRANSITION OF 4IN. (100 mm) PVC SURVEILLANCE DUCT(S) FROM BARRIER WALL FOOTER INTO JUNCTION BOX.

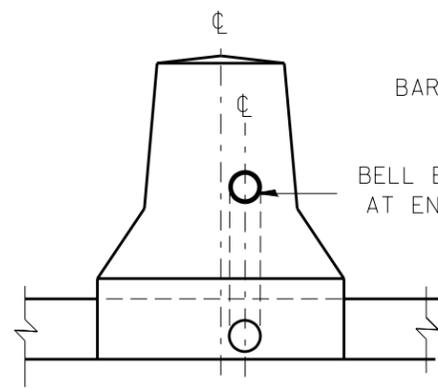
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		DATE - 06/22/94	REVISED -										



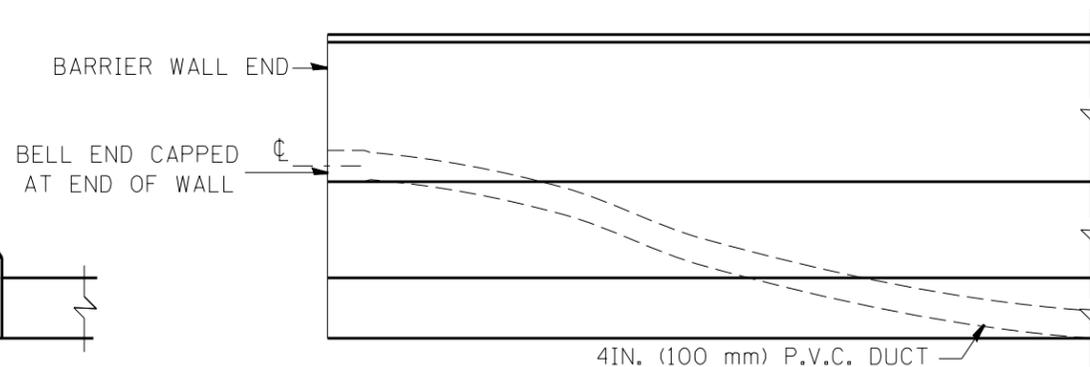
SECTION A-A



ELEVATION
BARRIER WALL DUCT DETAIL
1500FT. (457.2 m) INTERVAL

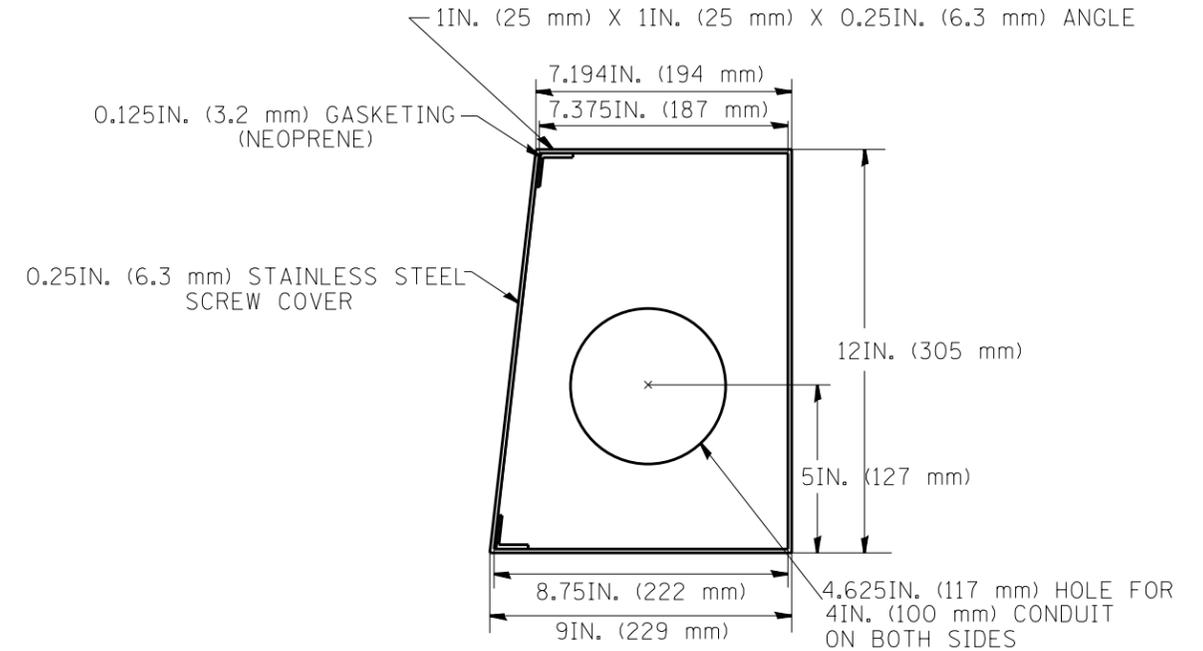


END VIEW



ELEVATION

END OF BARRIER WALL DUCT DETAIL



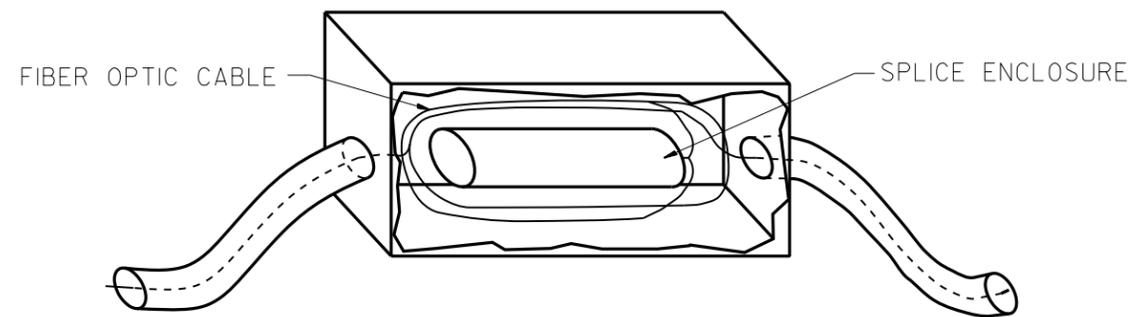
SIDE VIEW

SPECIAL JUNCTION BOX TYPE "J" TO BE
INSTALLED IN BARRIER WALL TYPE "2" ONLY

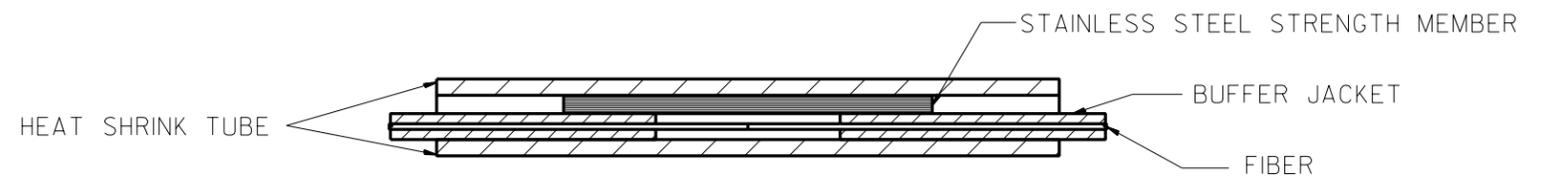
ALL WELDS SHALL BE CONTINUOUS AND LEAK PROOF
BOX AND COVER SHALL BE 0.25IN. (6.4 mm) TYPE 316 STAINLESS STEEL

BARRIER WALL SHALL BE GAPPED A MINIMUM OF 15FT. (4.57 m) FOR PROPER PLACEMENT OF JUNCTION BOX TYPE "J"
AND FOR A SMOOTH TRANSITION OF 4IN. (100 mm) PVC SURVEILLANCE DUCT(S) FROM BARRIER WALL FOOTER INTO JUNCTION BOX.

FILE NAME = c:\pwork\work\pwork\mezag\d0287541\TSCTYP.dgn	USER NAME = mezag	DESIGNED - R.L.	REVISED - 06/94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER	TRAFFIC SURVEILLANCE BARRIER WALL DUCT DETAILS			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100.0000' / 1IN.	CHECKED - R.L.	REVISED - 10/96	REVISED - 02/94								886	850
PLOT DATE = 2/7/2013	DATE - 06/22/94	REVISED -			SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60L70		
										FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		



JUNCTION BOX DETAIL



FIBER SPLICE DETAIL

FILE NAME =	USER NAME = #USER#	DESIGNED - D.S.	REVISED -
#FILE#		DRAWN - G.M.	REVISED -
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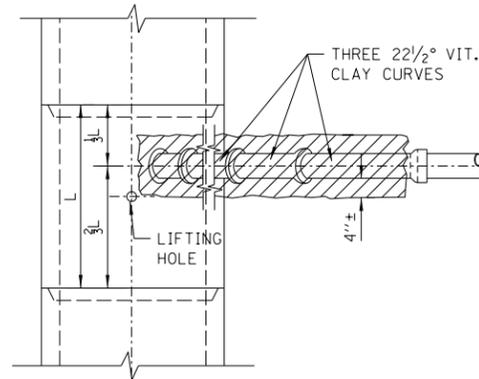
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

FIBER OPTIC
WIRING DETAIL

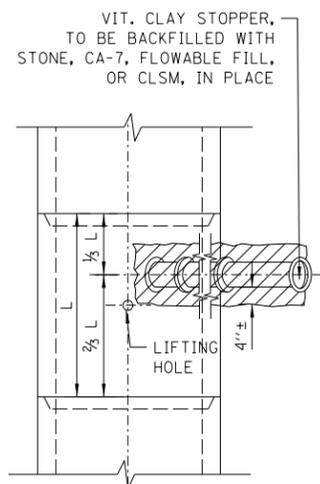
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			886	851
FED. ROAD DIST. NO.			CONTRACT NO. 60L70	
ILLINOIS			FED. AID PROJECT	

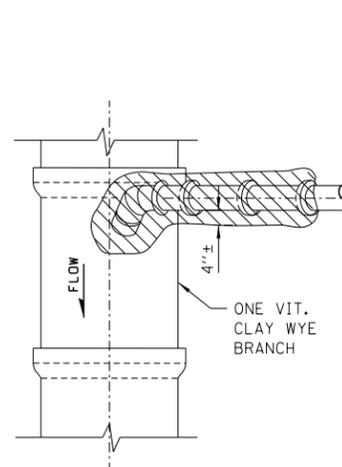
TRAFFIC SYSTEMS CENTER (TY-ITSC-663#13)



PLAN

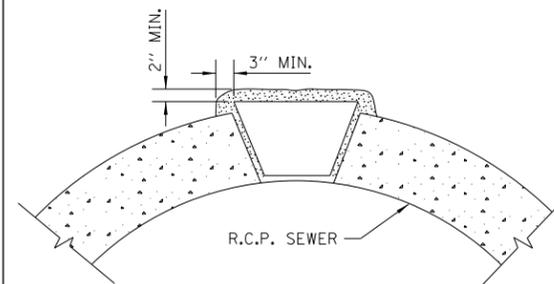


PLAN

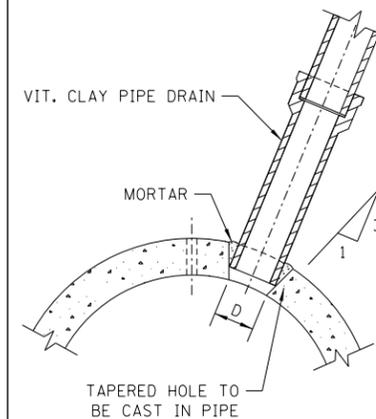


PLAN

NOTE:
PLUG TO BE COATED WITH MORTAR AND
DRIVEN INTO PLACE WITH A WOODEN
MALLET AND THEN SEALED WITH MORTAR.



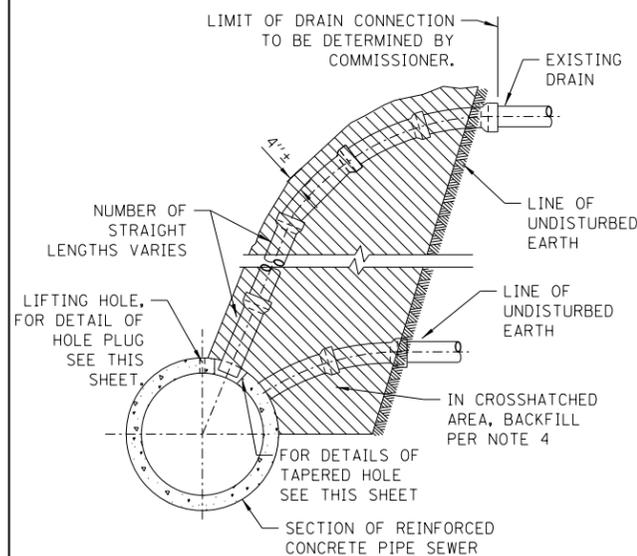
LIFTING HOLE PLUG DETAIL
FOR CONCRETE PIPE



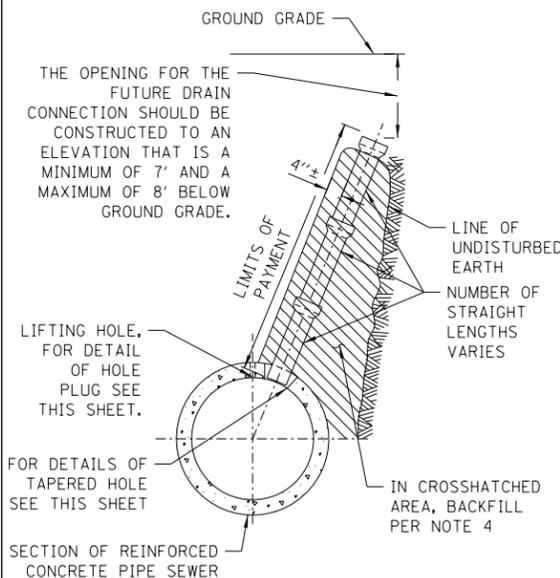
DETAIL OF TAPERED HOLE

SIZE OF DRAIN	"D"
6"	6 7/8"
8"	9 1/8"
10"	11 1/8"
12"	13 1/2"
15"	17"
18"	20 1/4"
21"	23 7/8"
24"	27 1/4"

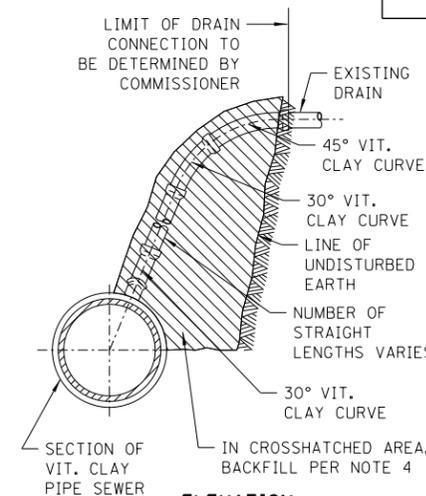
NOTE:
GRANULAR MATERIAL WILL BE USED AS BEDDING FOR
ALL PIPE SEWERS: SEE SPECIFICATIONS, BOOK 3.



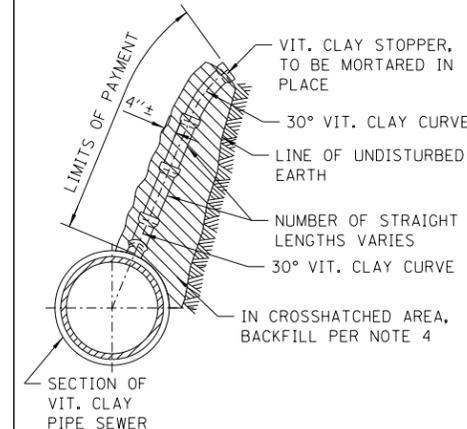
TYPICAL DRAIN CONNECTIONS
FOR EXISTING DRAINS



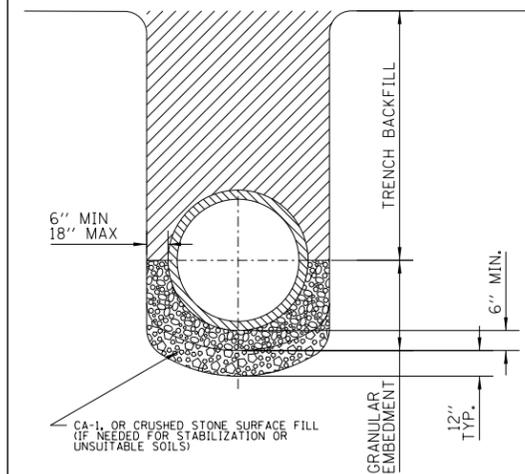
TYPICAL DRAIN STACKS
FOR FUTURE USE



TYPICAL DRAIN CONNECTIONS
FOR EXISTING DRAINS



TYPICAL DRAIN STACKS
FOR FUTURE USE



DETAIL OF GRANULAR
EMBEDMENT AND STABILIZATION
FOR PIPE SEWERS

NOTES:
FOR STABILIZATION OF TRENCH BOTTOM AND
PROVISIONS RELATING TO TRENCH BOX SUPPORT
SEE SPECIFICATIONS, BOOK 3

- NOTES:
1. ALL DRAIN CONNECTION JOINTS MUST BE MADE AS SPECIFIED IN SPECIFICATIONS..
 2. FOR DUCTILE IRON PIPE DRAIN CONNECTIONS SEE SHEET NO. A.2.
 3. FOR ALL GRANULAR EMBANKMENT, USE CA-7 OR CA-11
 4. FOR BACKFILL OF HATCHED SUPPORT AREAS, USE CONCRETE, CA-11, FLOWABLE FILL, OR CLSM.

STANDARD REVISIONS	
DATE	DESCRIPTION
1/22/14	APPROVED PLAN

PERCENT COMPLETE	DATE
30	
60	
75	
90	
100	
BULLETIN	

CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES	
VITRIFIED CLAY PIPE DRAIN CONNECTIONS	

DRAWN: SDB	DESIGNED: SDB	CHECKED: DD, CC, SO	REVIEWED: SDB
			A.1
			OF
			PN

FILE NAME = D:\60L70-Sht-CDW\DW Det\1-01.dgn



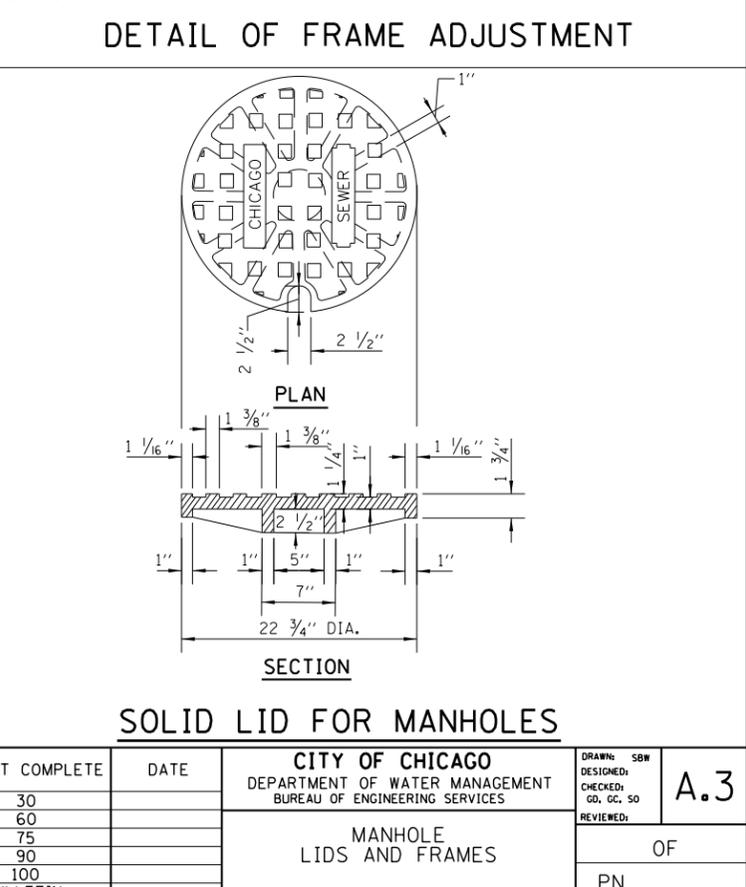
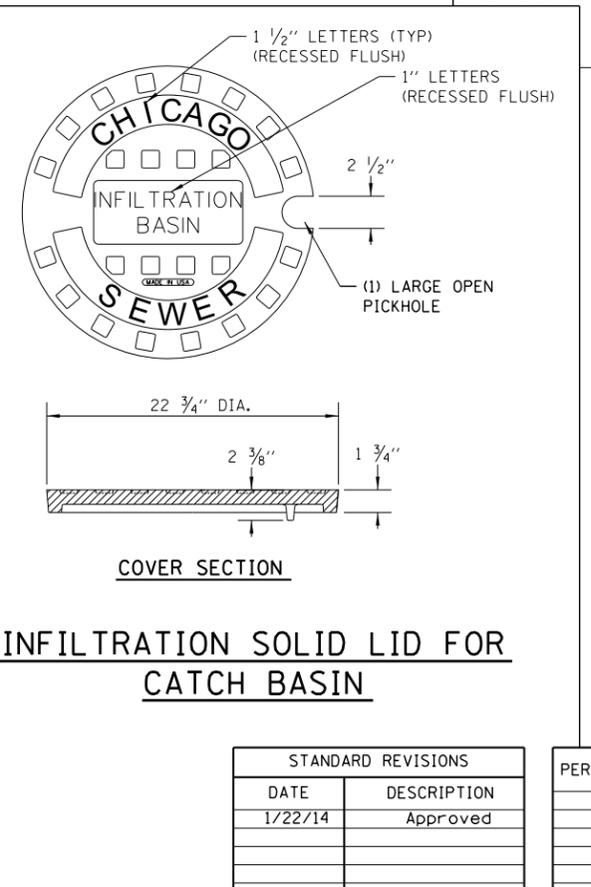
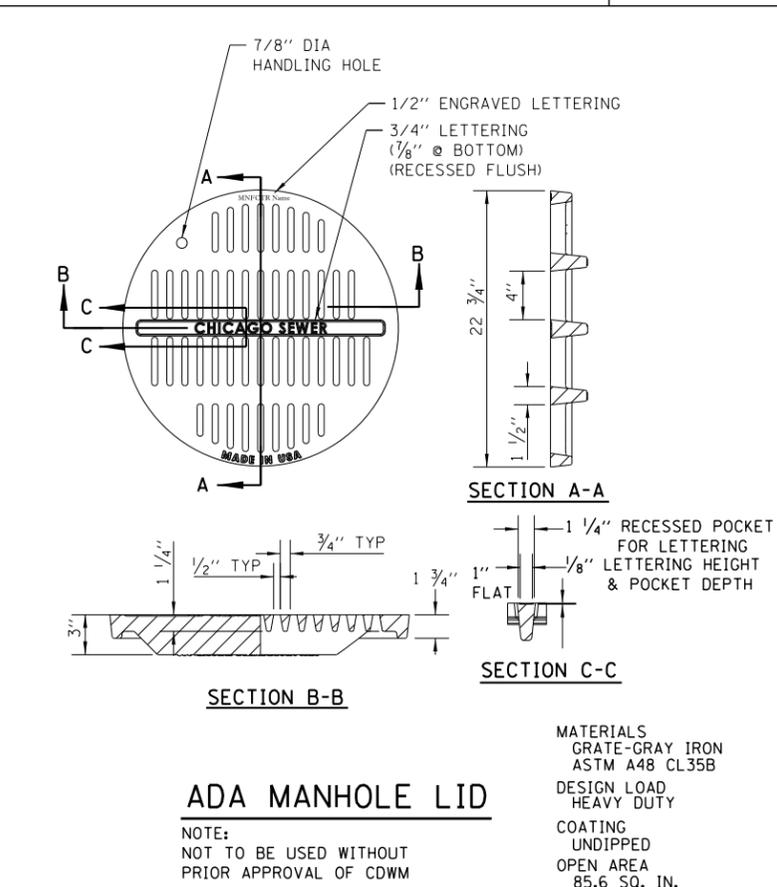
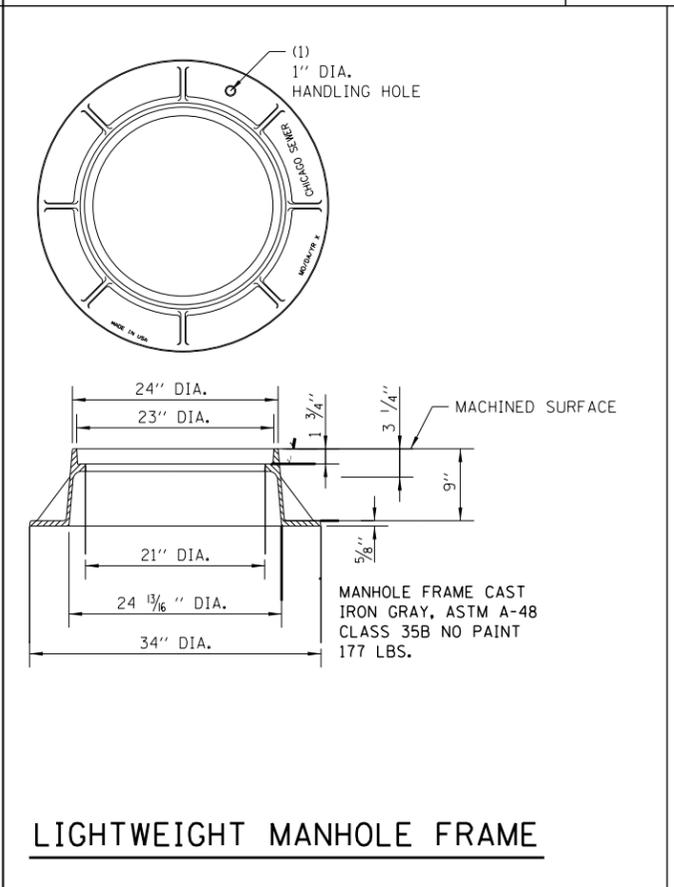
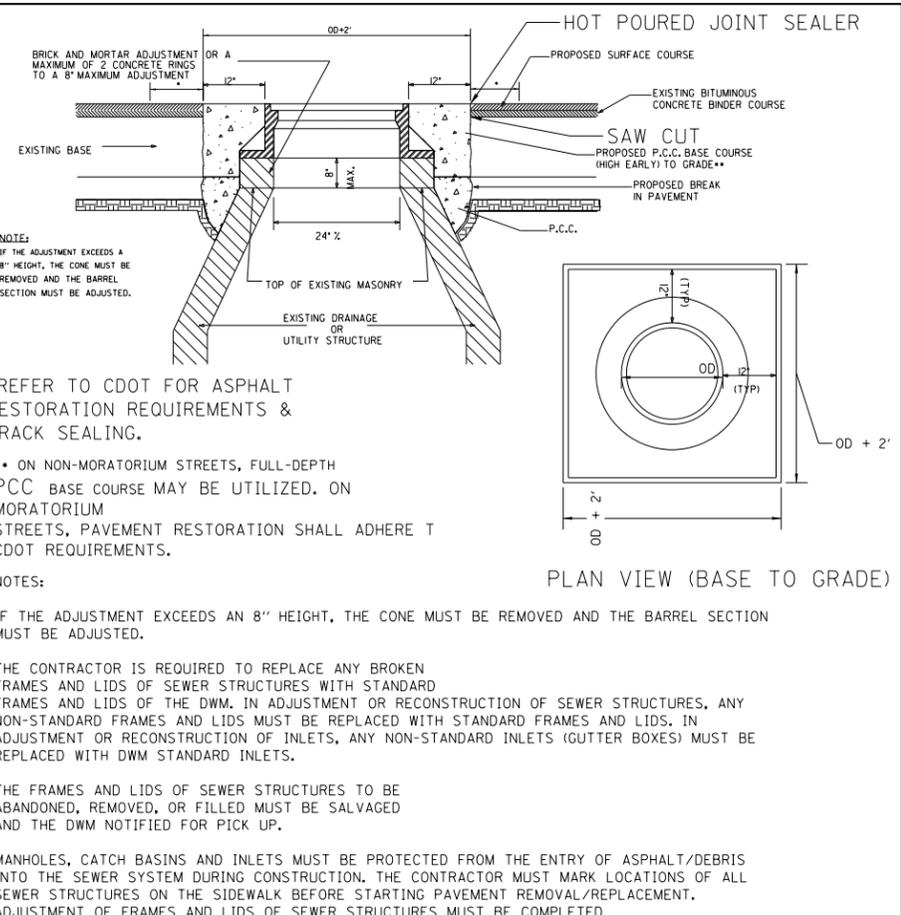
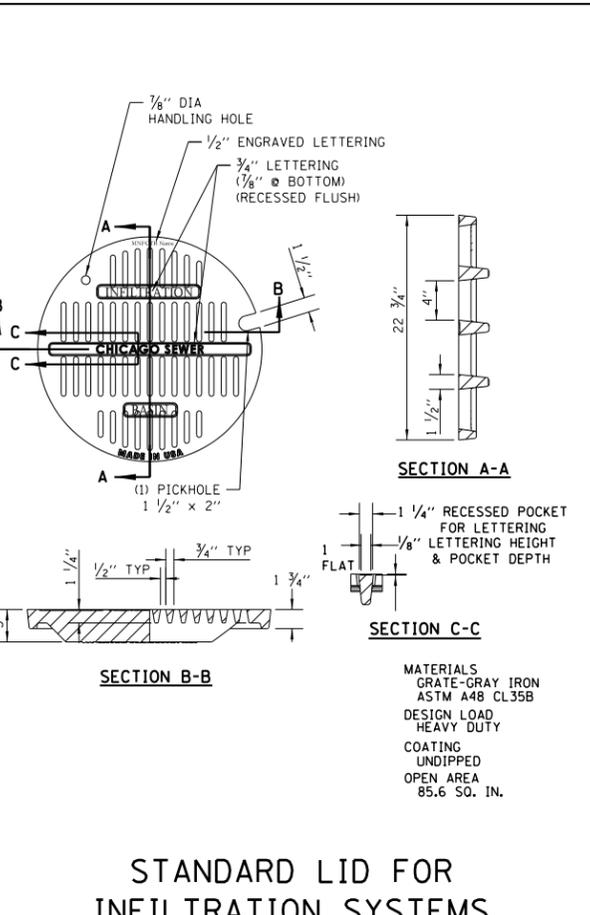
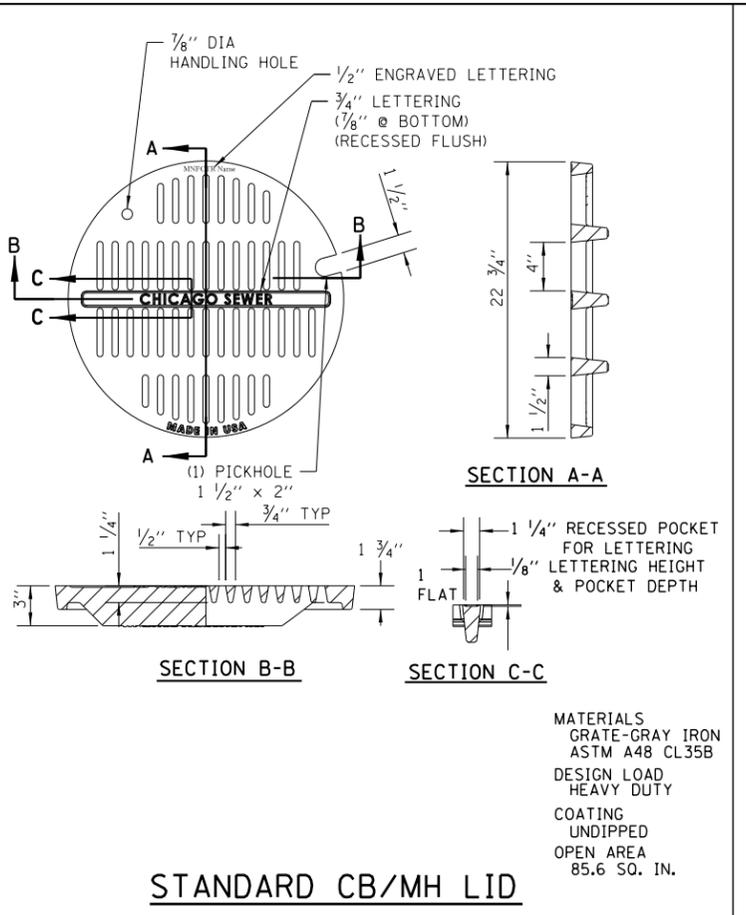
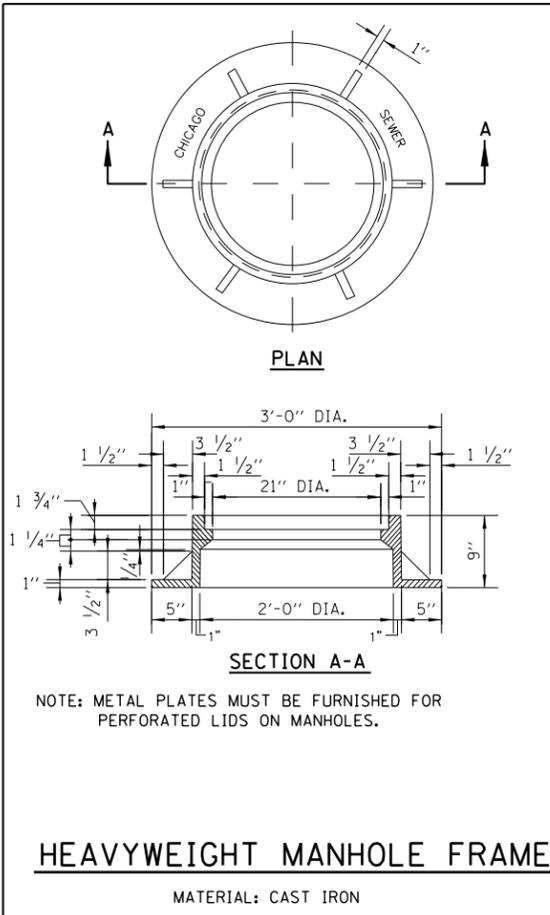
USER NAME	DESIGNED	REVISIONS
ouyeungh	-	-
	-	-
	-	-
	-	-
	-	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF WATER MANAGEMENT
STANDARD DETAILS

SCALE: NTS SHEET NO. 1 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	852
				CONTRACT NO. 60L70
ILLINOIS FED. AID PROJECT				



STANDARD REVISIONS	
DATE	DESCRIPTION
1/22/14	Approved

PERCENT COMPLETE	DATE
30	
60	
75	
90	
100	
BULLETIN	

CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES		DRAWN: SBR	A.3
		CHECKED: GD, GC, SO	
MANHOLE LIDS AND FRAMES		REVIEWED:	OF
			PN

AECOM 303 EAST WACKER DRIVE, SUITE 1400 CHICAGO, IL 60601-3276 PHONE: (312) 373-7700 FAX: (312) 373-8800	USER NAME = augeungh	DESIGNED -	REVISED -
	PLOT SCALE = 1:8000 / 1"	DRAWN -	REVISED -
	PLOT DATE = 11/19/2014	CHECKED -	REVISED -
		DATE - 11/14/2014	REVISED -

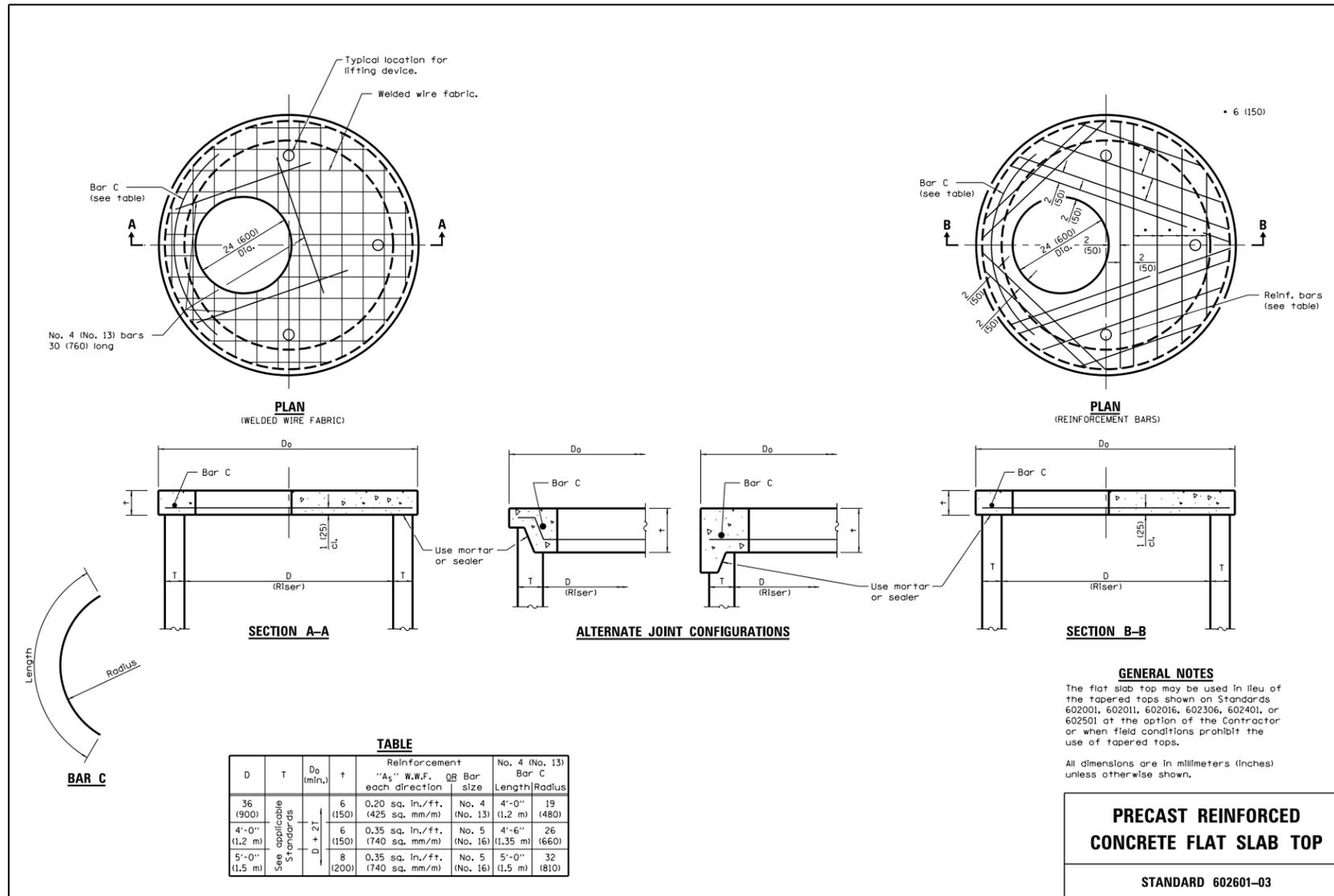
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CHICAGO DEPARTMENT OF WATER MANAGEMENT STANDARD DETAILS
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SCALE: NTS	SHEET NO. 2 OF 5 SHEETS	STA. TO STA.
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F.A.I. RTE. 55	SECTION 2010-080-B	COUNTY COOK	TOTAL SHEETS 886	SHEET NO. 853
CONTRACT NO. 60L70				
ILLINOIS FED. AID PROJECT				

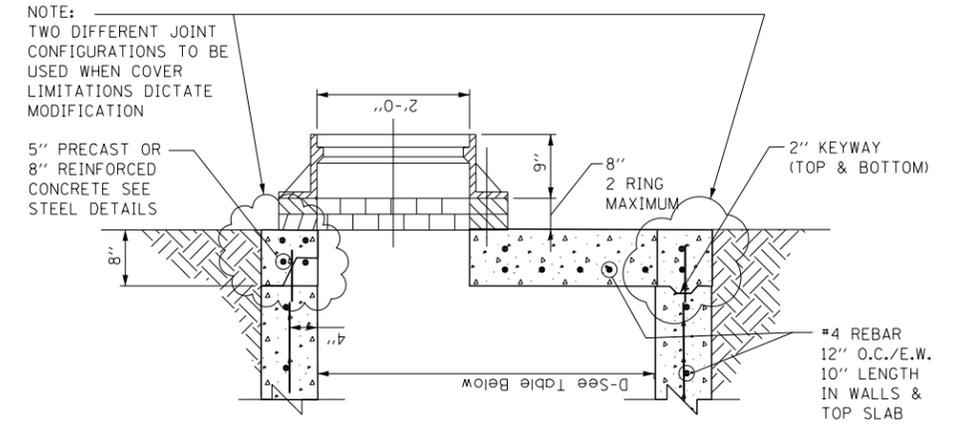
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SPECIAL DRAINAGE STRUCTURES FOR PUBLIC STREETS AND ALLEYS



GENERAL NOTES
 The flat slab top may be used in lieu of the tapered tops shown on Standards 602001, 602011, 602016, 602306, 602401, or 602501 at the option of the Contractor or when field conditions prohibit the use of tapered tops.
 All dimensions are in millimeters (inches) unless otherwise shown.

PRECAST REINFORCED CONCRETE FLAT SLAB TOP
 STANDARD 602601-03



STANDARD FLAT TOP SLAB FOR CATCH BASINS

NOTES:
 FLAT TOP SLAB CAN ONLY BE USED WITH WRITTEN PERMISSION FROM CDWM.
 USE LATEST IDOT DETAIL, #602601

FILE NAME = D:\602601-03.dgn



USER NAME = augeungh	DESIGNED -	REVISED -
PLOT SCALE = 1:8000 / 1"	DRAWN -	REVISED -
PLOT DATE = 11/19/2014	CHECKED -	REVISED -
	DATE = 11/14/2014	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

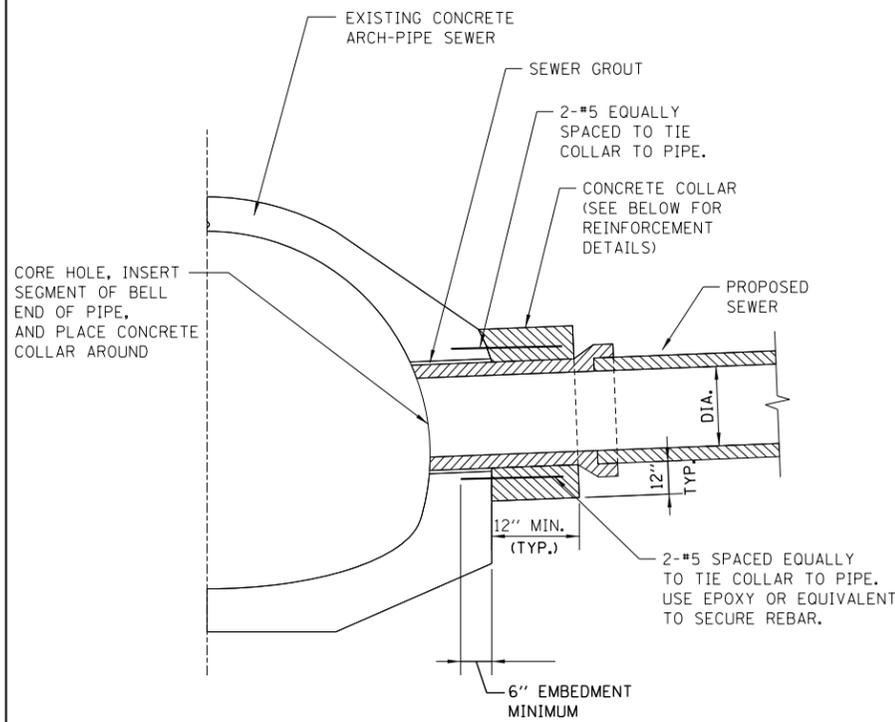
STANDARD REVISIONS	
DATE	DESCRIPTION
1/22/14	APPROVED PLAN

PERCENT COMPLETE	DATE
30	
60	
75	
90	
100	
BULLETIN	

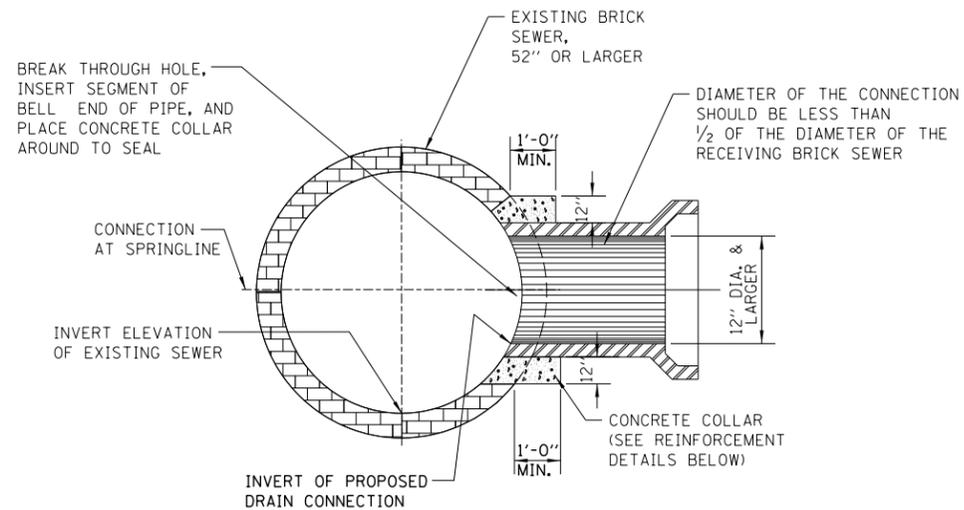
CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES		DRAWN: SDB DESIGNED: CHECKED: REVIEWED:
FLAT TOP SLAB DETAILS		A.8
		OF
		PN

CHICAGO DEPARTMENT OF WATER MANAGEMENT
 STANDARD DETAILS
 SCALE: NTS SHEET NO. 3 OF 5 SHEETS STA. TO STA.

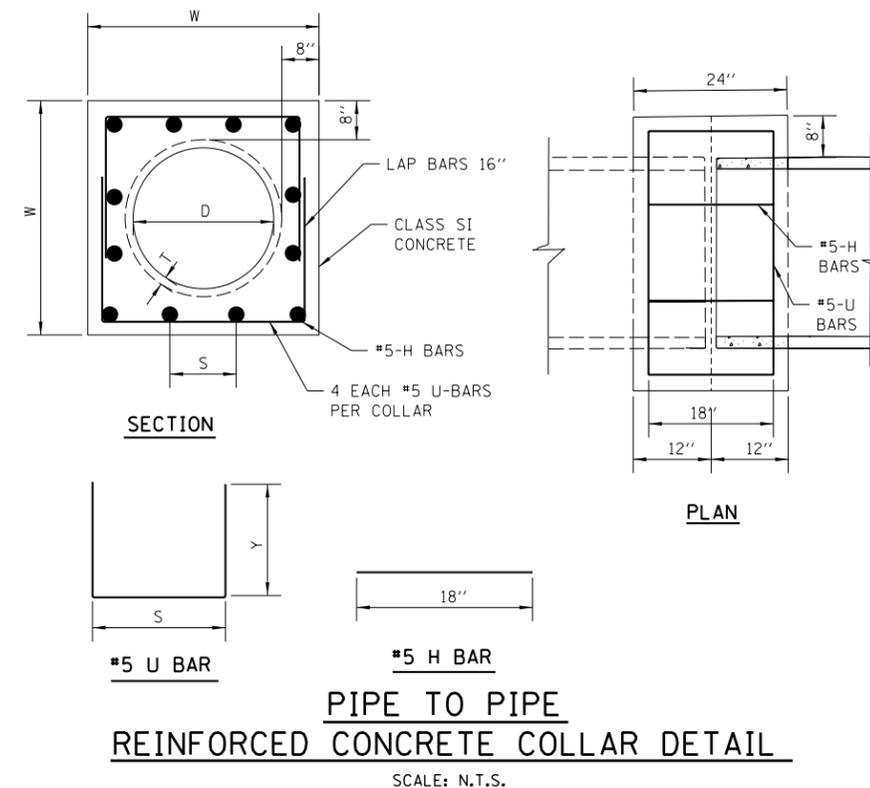
F.A.I. RTE. 55	SECTION 2010-080-B	COUNTY COOK	TOTAL SHEETS 886	SHEET NO. 854
CONTRACT NO. 60L70				
ILLINOIS FED. AID PROJECT				



EXAMPLE TYPICAL CONNECTION DETAIL
(NOT TO SCALE)



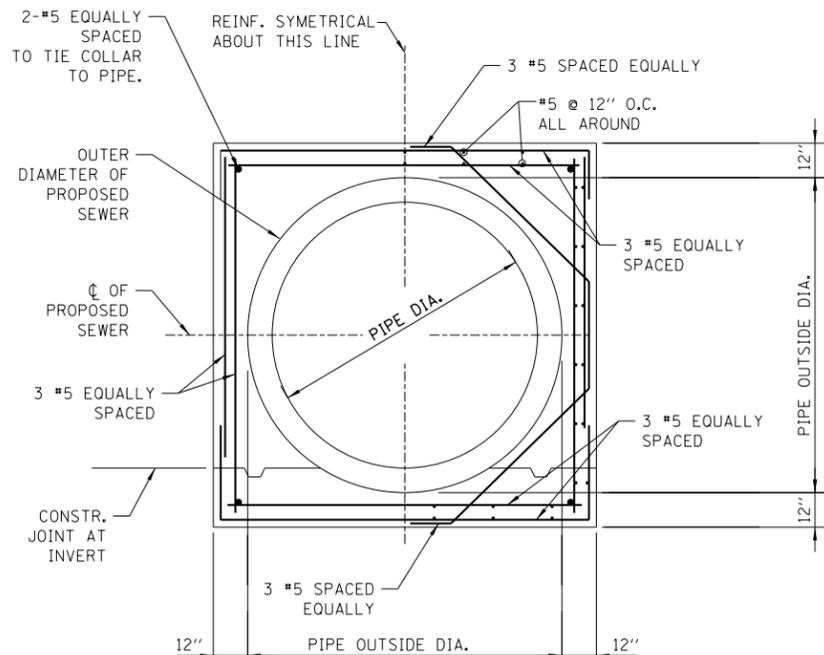
EXAMPLE TYPICAL BRICK SEWER CONNECTION FOR CONNECTING PIPE SIZES GREATER THAN 12" DIA.
(NOT TO SCALE)



PIPE TO PIPE REINFORCED CONCRETE COLLAR DETAIL
SCALE: N.T.S.

CONCRETE COLLAR NOTES:

1. ALL ITEMS AND MATERIALS SHALL CONFORM TO THE LATEST IDOT SSRBC SPECIFICATIONS, UNLESS OTHERWISE NOTED IN SUPPLEMENTAL SPECIFICATIONS FOR THE SPECIFIC PROJECT BEING CONSTRUCTED.
2. ALL CONCRETE SHALL CONFORM TO IDOT SSRBC ARTICLE 1020.04, CLASS SI, WITH A COMPRESSIVE STRENGTH OF 3500 PSI.
3. ALL EPOXY COATED REINFORCEMENT BARS SHALL CONFORM TO IDOT SSRBC SECTION 508, AND ARTICLE 1006.10.
4. UNLESS OTHERWISE SHOWN, THE COVER FOR REINFORCING STEEL SHALL BE AS FOLLOWS: A.) CONCRETE CAST AGAINST PERMANENTLY EXPOSE EARTH: 3" B.) ALL OTHER REINFORCING BARS: 2"
5. CONCRETE COLLARS SHALL BE USED AT ALL EXISTING/PROPOSED PIPE CONNECTIONS. TRIM EXISTING PIPE END TO PROVIDE FLUSH BUTT JOINT, INSTALL REBAR, AND PLACE CONCRETE COLLAR.
6. THE CONTRACTOR SHALL PROVIDE ALL MEASURES AND PRECAUTIONS NECESSARY TO PREVENT DAMAGE TO THE EXISTING SEWER DURING CONSTRUCTION. CONTRACTOR SHALL ADEQUATELY BRACE OR SHORE EXISTING SEWER IF REQUIRED TO MAINTAIN INTEGRITY OF SEWER DURING CONSTRUCTION. SUBMIT DESIGN AND DETAILS, SEALED AND SIGNED BY AN ILLINOIS LICENSED STRUCTURAL ENGINEER, SHOWING TEMPORARY BRACING FOR THE EXISTING SEWER DURING CONSTRUCTION FOR REVIEW PRIOR TO CONSTRUCTION. ANY DAMAGE TO EXISTING STRUCTURE IS RESPONSIBILITY OF THE CONTRACTOR.
7. CONTRACTOR SHALL DIVERT ALL FLOW FROM THE EXISTING SEWER PRIOR TO CONSTRUCTION SO THAT THE WORK CAN BE PERFORMED IN THE DRY CONDITION. SEWER MUST BE MAINTAINED IN SERVICE AT ALL TIMES. SUBMIT MEANS OF FLOW DIVERSION FOR REVIEW PRIOR BREAKING INTO EXISTING BRICK SEWER. ALL EXCAVATION SHALL BE KEPT DEWATERED DURING CONSTRUCTION OPERATIONS UNTIL BACKFILL IN PLACE. PROVISIONS SHALL BE MADE TO PREVENT THE BOTTOM OF ALL EXCAVATIONS FROM FREEZING OR FLOODING AT ALL TIMES. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING STRUCTURE SHALL BE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE OF THE WORK.



EXAMPLE CONCRETE COLLAR REINFORCEMENT - SECTION
(NOT TO SCALE)

D RCP DIA. (IN)	T PIPE THICKNESS (IN)	W WIDTH (IN)	U BAR				H BAR				Class SI Concrete (CU YD)	#5 Epoxy Rebar (LBS)
			x (IN)	y (IN)	QTY (EACH)	LENGTH (FT)	S (IN)	QTY (EACH)	LENGTH (FT)	TOTAL		
12*	2.00	32.0	26.00	21.00	4	22.7	8 1/8	12.0	18.0	0.42	42.5	
15*	2.25	35.5	29.50	22.75	4	25.0	9 3/8	12.0	18.0	0.49	44.8	
18*	2.50	39.0	33.00	24.50	4	27.3	10 1/2	12.0	18.0	0.57	47.2	
21*	2.75	42.5	36.50	26.25	4	29.7	8 3/4	16.0	24.0	0.65	56.0	
24	3.00	46.0	40.00	28.00	4	32.0	9 5/8	16.0	24.0	0.72	58.4	
27	3.25	49.5	43.50	29.75	4	34.3	10 1/2	16.0	24.0	0.81	60.8	
30	3.50	53.0	47.00	31.50	4	36.7	11 3/8	16.0	24.0	0.89	63.3	
33	3.75	56.5	50.50	33.25	4	39.0	9 4/5	20.0	30.0	0.98	72.0	
36	4.00	60.0	54.00	35.00	4	41.3	10 1/2	20.0	30.0	1.07	74.4	
42	4.50	67.0	61.00	38.50	4	46.0	9 7/8	24.0	36.0	1.26	85.5	
48	5.00	74.0	68.00	42.00	4	50.7	11 1/8	24.0	36.0	1.46	90.4	

*Note: Option to use without Rebar

STANDARD REVISIONS	
DATE	DESCRIPTION
1/22/14	APPROVED PLAN

PERCENT COMPLETE	DATE
30	
60	
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BULLETIN	

CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES				DRAWN: SBR DESIGNED: SBR CHECKED: SBR REVIEWED: SBR
CONCRETE COLLAR / CONNECTION DETAILS				A.14
OF				PN



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

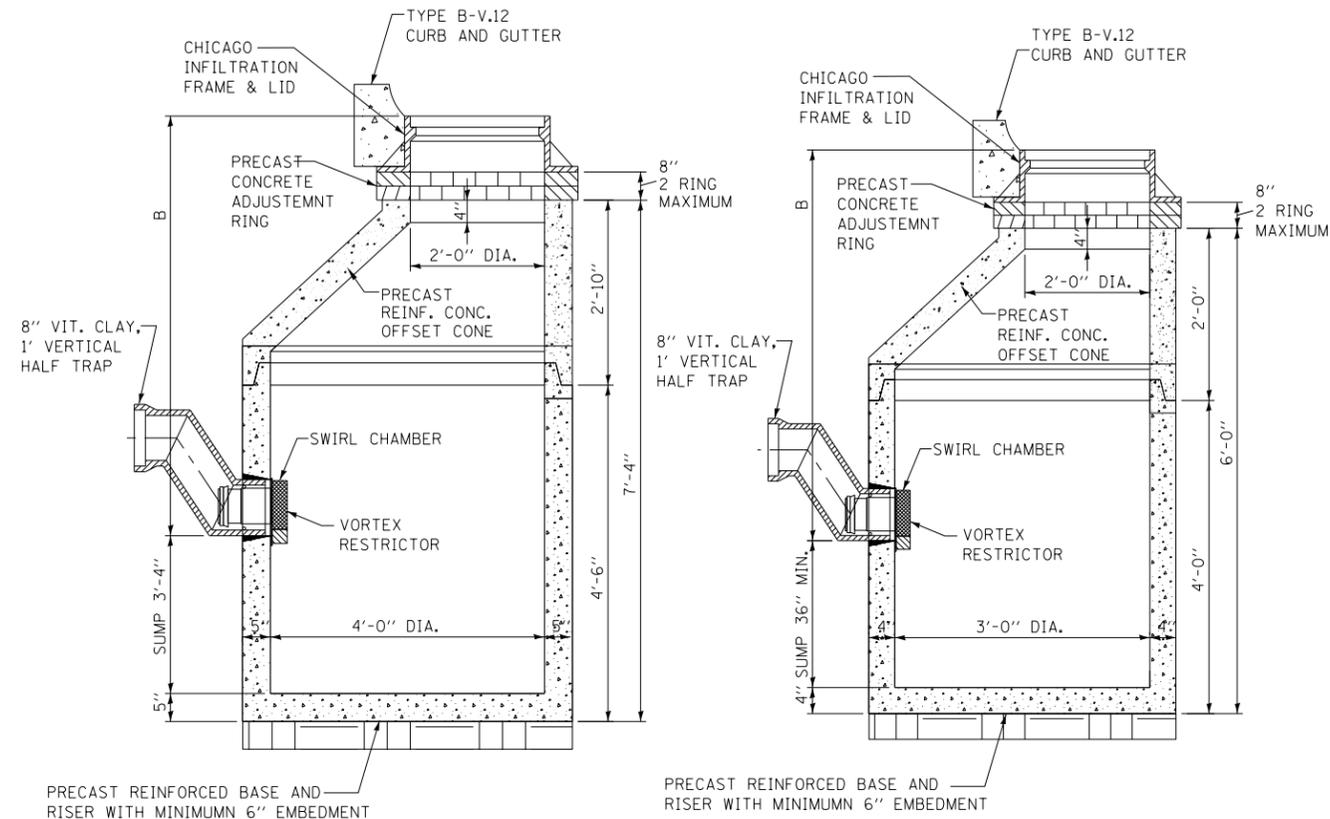
CHICAGO DEPARTMENT OF WATER MANAGEMENT
STANDARD DETAILS

SCALE: NTS SHEET NO. 4 OF 5 SHEETS STA. TO STA.

F.A.I. R.T.E. SECTION COUNTY TOTAL SHEETS SHEET NO.
55 2010-080-B COOK 886 855
CONTRACT NO. 60L70
ILLINOIS FED. AID PROJECT

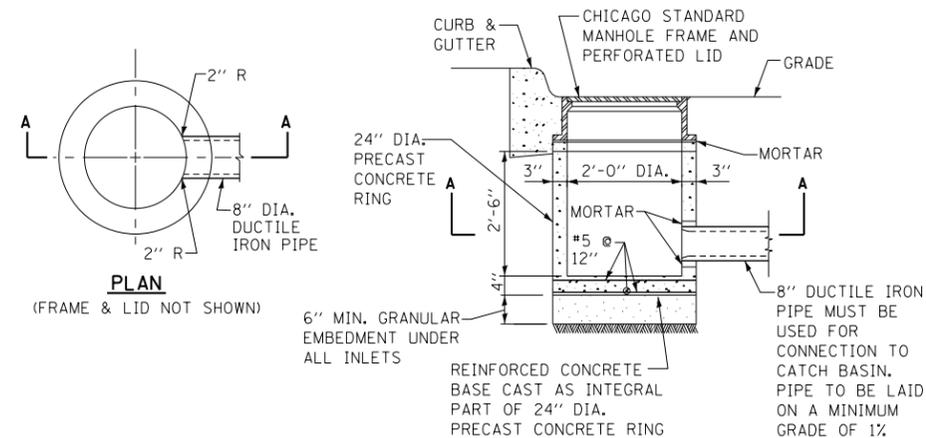
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STANDARD DRAINAGE STRUCTURES FOR PUBLIC STREETS



STANDARD CATCH BASIN-4' DIA.

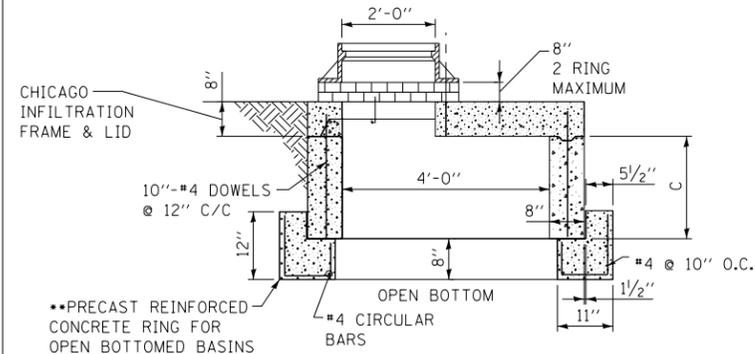
STANDARD CATCH BASIN-3' DIA.



STANDARD INLET-2' DIA.

NOTE:
INLETS AND 3' DIAMETER CATCH BASINS ARE TO BE USED ONLY WITH PRIOR APPROVAL OF DWM SEWER ENGINEER OR FIELD INSPECTOR.

STANDARD DRAINAGE STRUCTURES FOR PUBLIC ALLEYS



GREEN ALLEY OPEN BOTTOM CATCH BASIN

TO BE USED IF STANDARD CONE WITH REQUIRED SUMP DEPTH INSTALLATION IS IMPOSSIBLE.

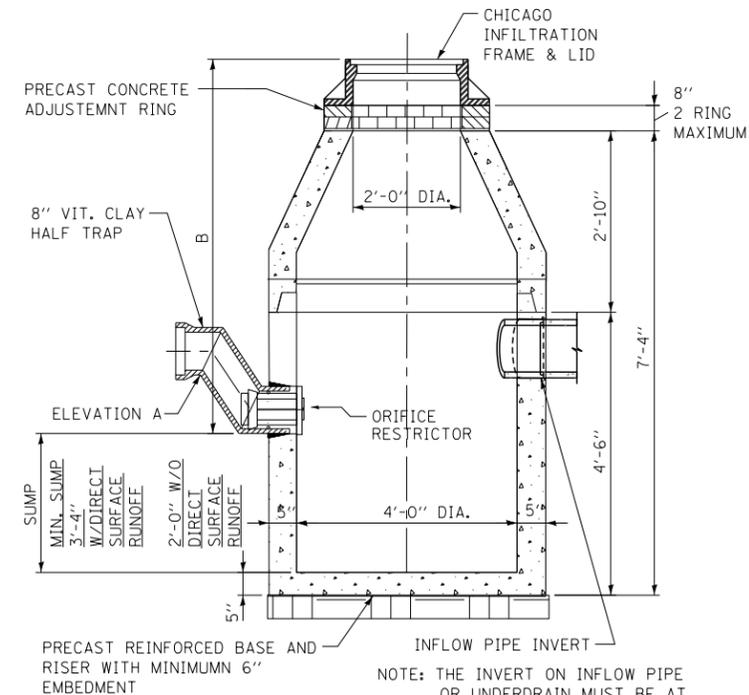
••IF FLAT TOP SLAB CATCH BASIN IS USED IN AN OPEN BOTTOM APPLICATION, THE FOLLOWING CRITERIA MUST BE FOLLOWED:

- "C" MUST BE A MINIMUM OF 3 FEET.

- PROVIDE A MINIMUM AGGREGATE BASE OF 1.0 FEET BELOW PRECAST REINFORCED CONCRETE RING.

- GEOTECH FABRIC MUST BE PLACED ON SIDES AND BOTTOM OF AGGREGATE SURROUNDING BASIN.

- AGGREGATE BASE MUST PROVIDE ADEQUATE STORMWATER STORAGE CAPACITY PER THE APPROVED/PERMITTED PLANS.



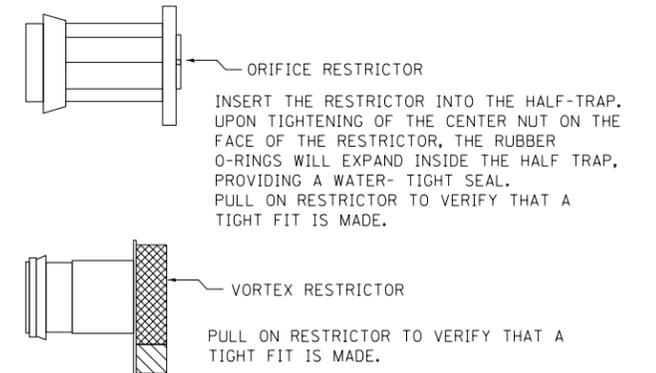
CATCH BASIN-ORIFICE RESTRICTOR

N.T.S.

NOTES:

- FOR ANY DRAIN TILE CONNECTION, THE WATER TABLE MUST BE AT LEAST 3.5 FEET BELOW ELEVATION A.
- PVC UNDERDRAINS MUST BE 4" MINIMUM DIAMETER, (6" RECOMMENDED) AND MUST BE WRAPPED IN GEOTECH FABRIC.

DRAINAGE STRUCTURES RESTRICTORS



GENERAL NOTES:

- CATCH BASIN TO CATCH BASIN CONNECTIONS ARE ALLOWED IN PRIVATE SITES & ALLEYS. ONLY THE DOWNSTREAM CATCH BASIN IS REQUIRED TO HAVE A HALF-TRAP.
- IF B < 4 FEET, THEN USE A DUCTILE IRON PIPE HALF TRAP AND FLAT TOP SLAB CATCH BASIN AS NECESSARY.
- INLETS AND 3' DIAMETER CATCH BASINS ARE TO BE USED ONLY WITH PRIOR APPROVAL OF DWM FIELD INSPECTOR.

RESTRICTOR NOTES:

THE DWM'S RAIN BLOCKER RESTRICTOR PROGRAM MUST BE MAINTAINED WITH ANY ROADWAY IMPROVEMENT.

THE DESIGN OF ANY ROADWAY IMPROVEMENT MUST CONSIDER LIMITING THE NUMBER OF CATCH BASINS TO THE EXTENT PRACTICAL. THE NUMBER OF EXISTING STRUCTURES SHOULD NOT BE INCREASED.

THE RESTRICTORS CAN BE OBTAINED FROM DWM CENTRAL DISTRICT AT 3901 S. ASHLAND AVE. THE CONTRACTOR SHOULD ARRANGE FOR PICK UP BY CONTACTING 312-747-1177 (7AM TO 3PM, M-F)

FLOW RESTRICTORS MUST BE INSTALLED IN ALL CATCH BASINS OUTSIDE OF THE CENTRAL BUSINESS DISTRICT. RESTRICTORS MUST NOT BE INSTALLED IN CATCH BASINS IN CLOSE PROXIMITY TO VIADUCT AREAS, BUS STOPS, OR EMERGENCY ENTRANCES. THE DWM MUST APPROVE THE NON-INSTALLATION OR REMOVAL OF ANY RESTRICTOR. REQUIREMENTS FOR RESTRICTOR INSTALLATION ARE AS FOLLOWS:

- ARTERIAL STREETS: 3-INCH ORIFICE RESTRICTOR
- BUS ROUTES: 3-INCH ORIFICE RESTRICTOR
- RESIDENTIAL STREETS: 3-INCH VORTEX RESTRICTOR
- ALLEYS: 3-INCH ORIFICE RESTRICTOR IN THE LAST CB.
- CLOSED LIDS ARE REQUIRED ON ALL MANHOLES EXCEPT AT INTERSECTIONS WHERE A PERFORATED LID SHALL BE USED.

STANDARD REVISIONS	
DATE	DESCRIPTION
1/22/14	APPROVED

PERCENT COMPLETE	DATE
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BULLETIN	

CITY OF CHICAGO		DRAWN: SBR	A.15
DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES		CHECKED:	
DRAINAGE STRUCTURE DETAILS		REVIEWED:	OF
			PN

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CHICAGO DEPARTMENT OF WATER MANAGEMENT
STANDARD DETAILS**

SCALE: NTS SHEET NO. 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	856
CONTRACT NO. 60L70				
ILLINOIS FED. AID PROJECT				

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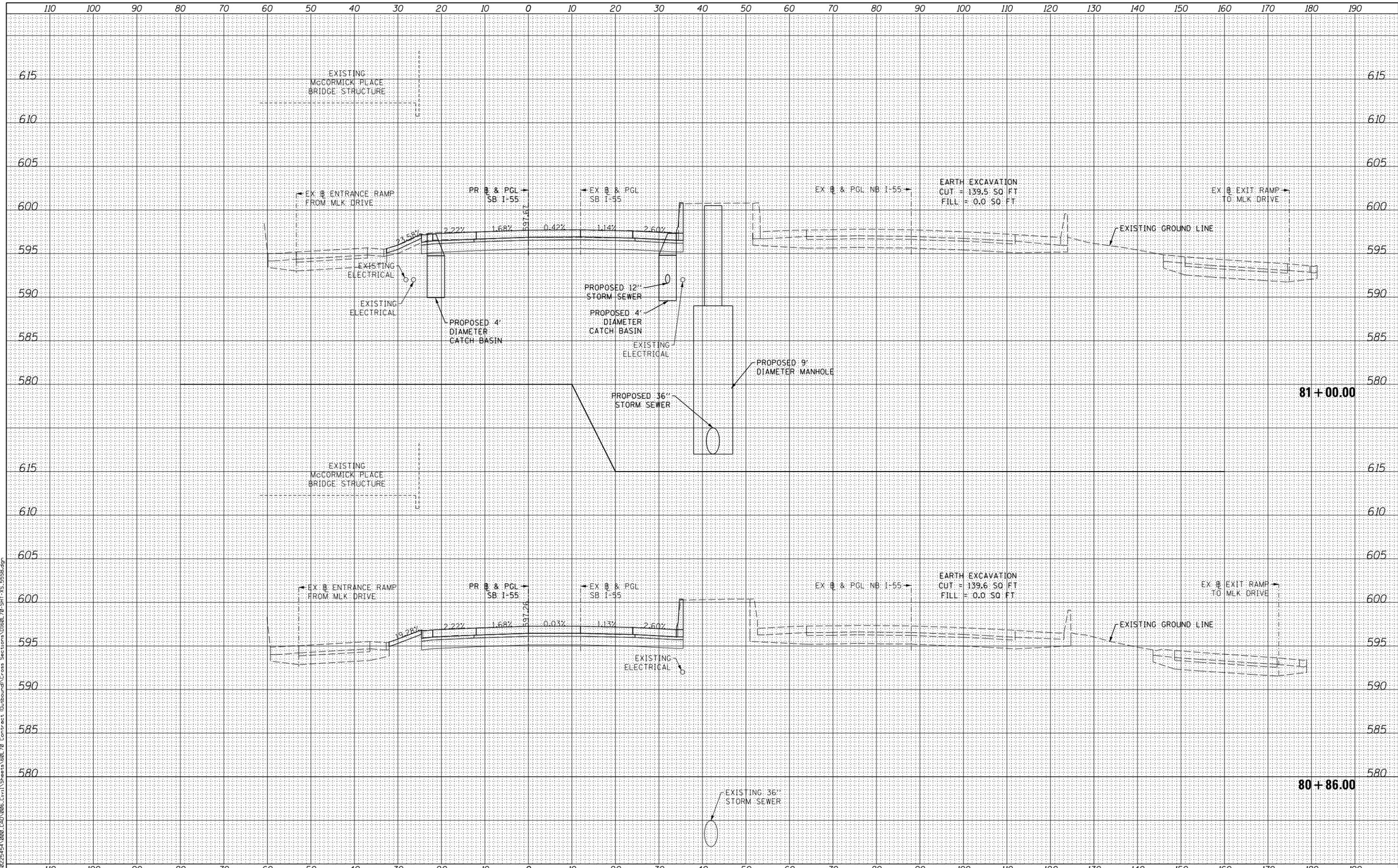
AECOM
303 EAST WACKER DRIVE, SUITE 1400
CHICAGO, IL 60601-3276
PHONE: (312) 373-7700 FAX: (312) 373-8800

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 SB I-55

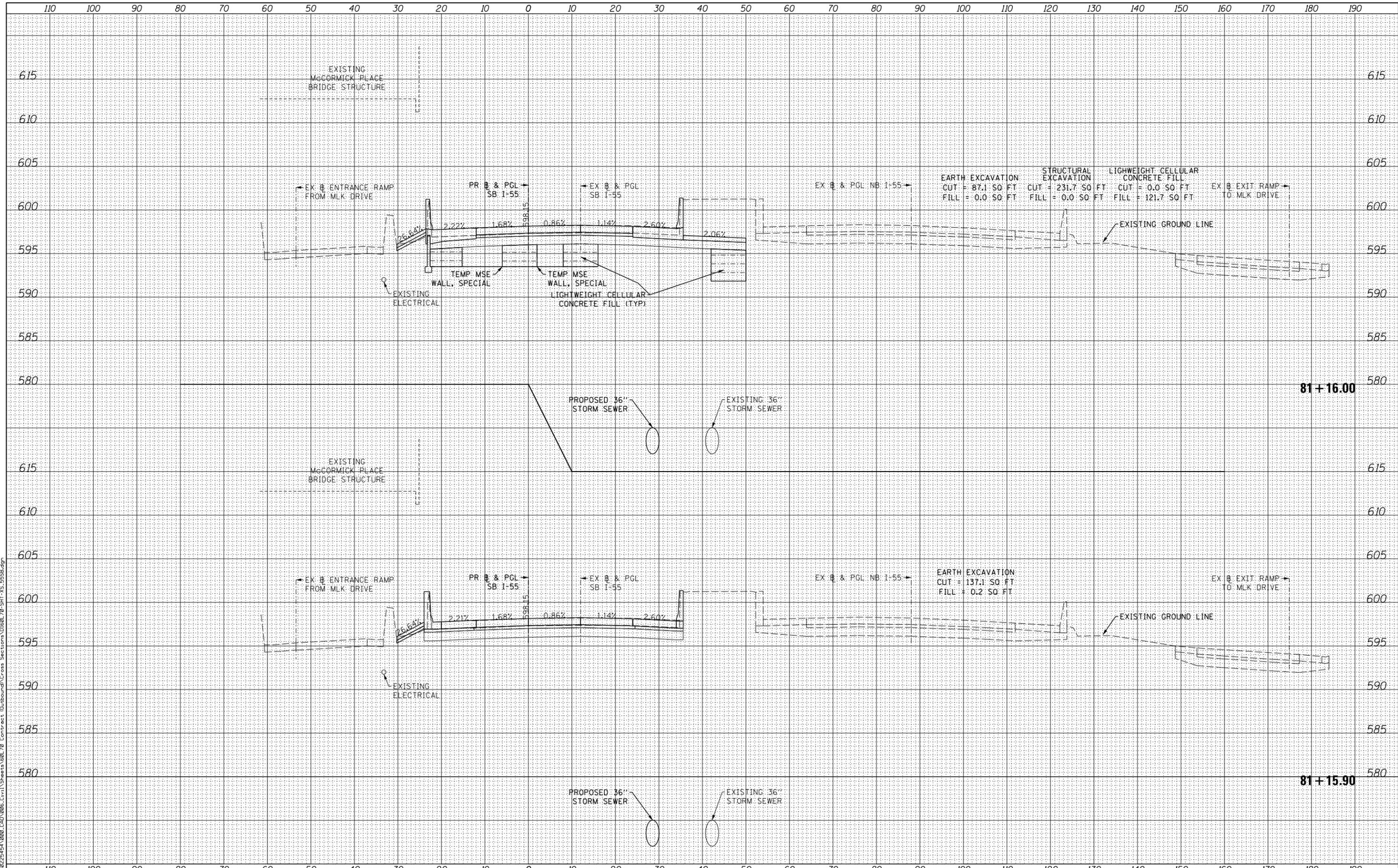
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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STATE OF ILLINOIS
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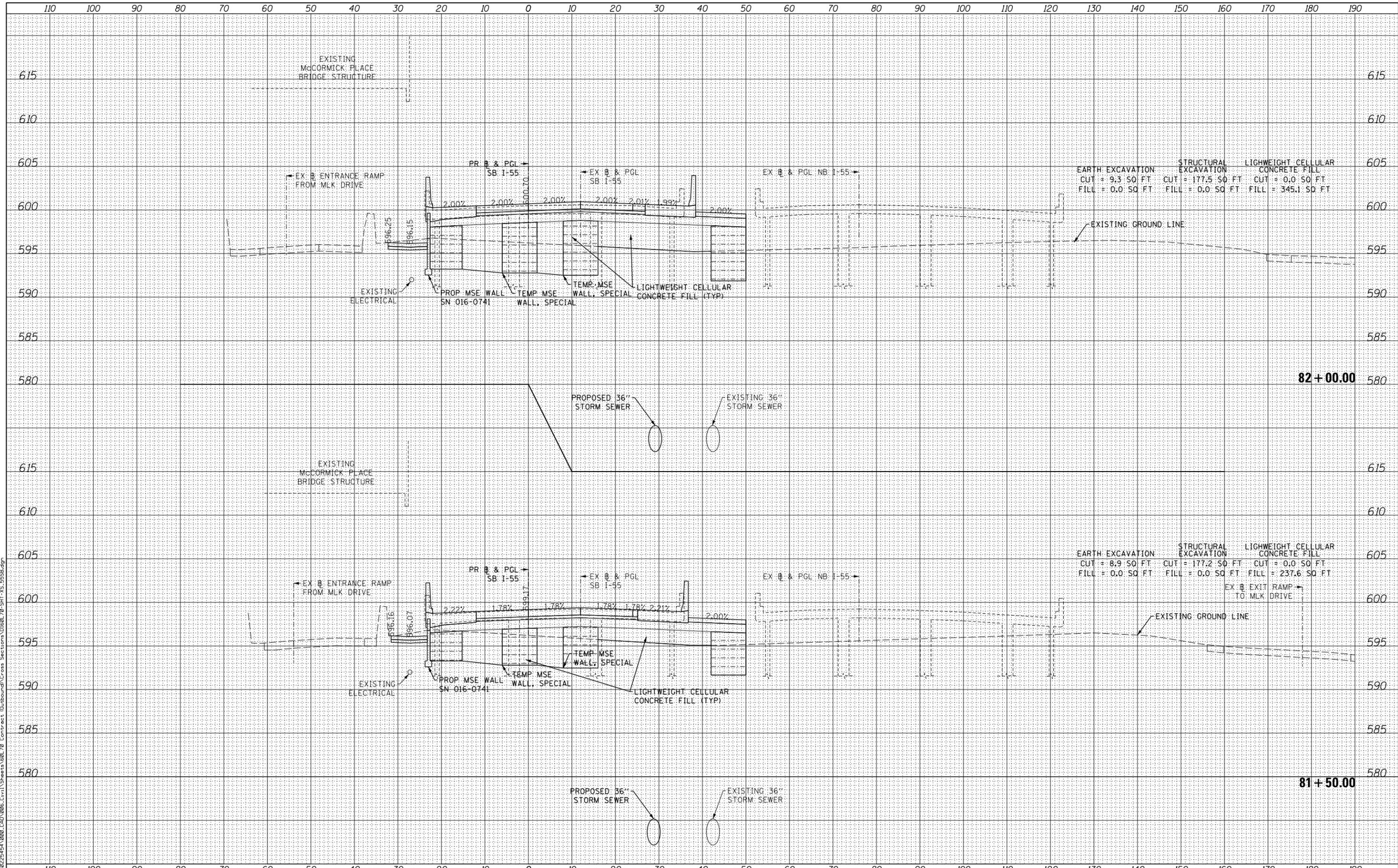
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	858
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

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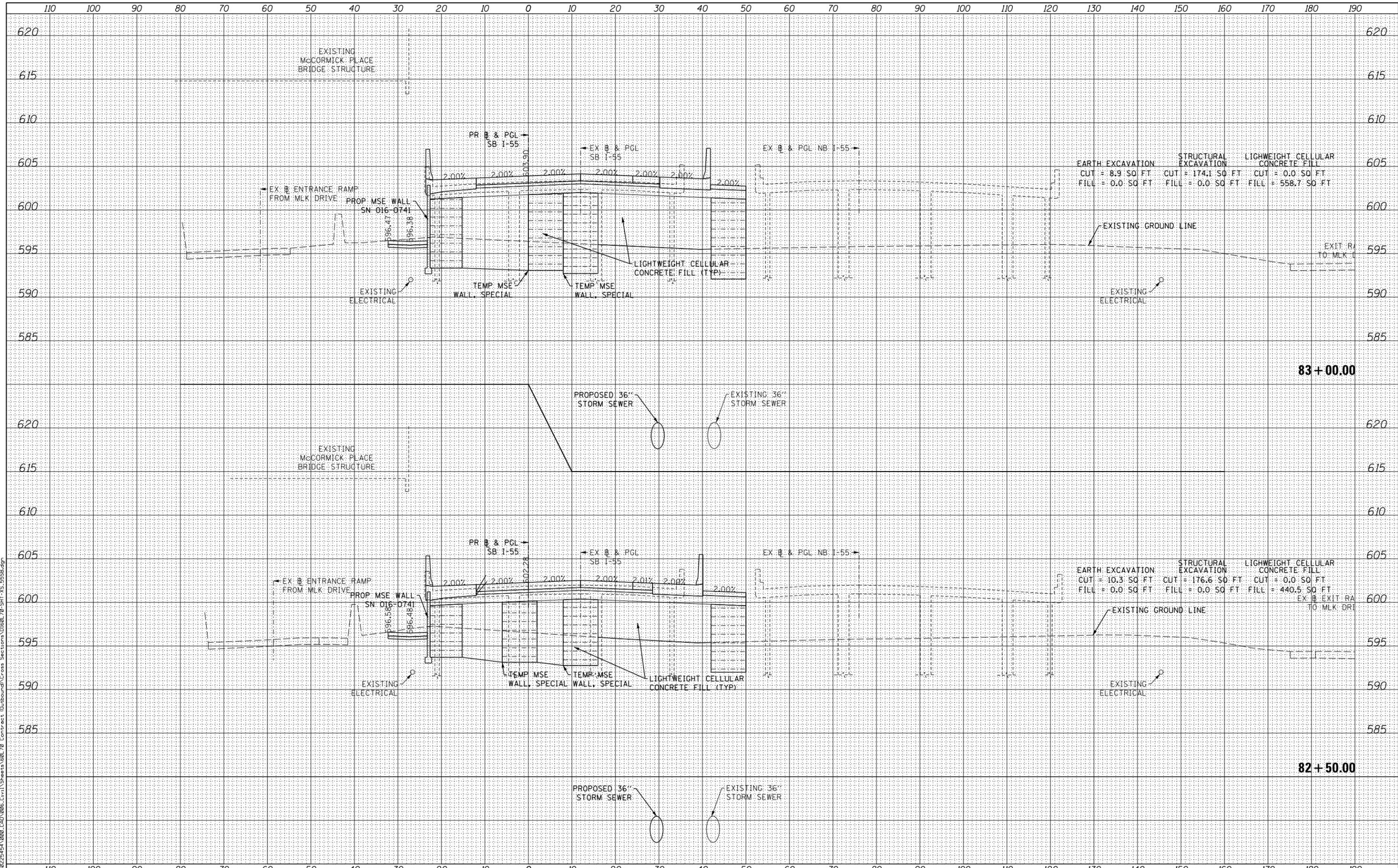
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

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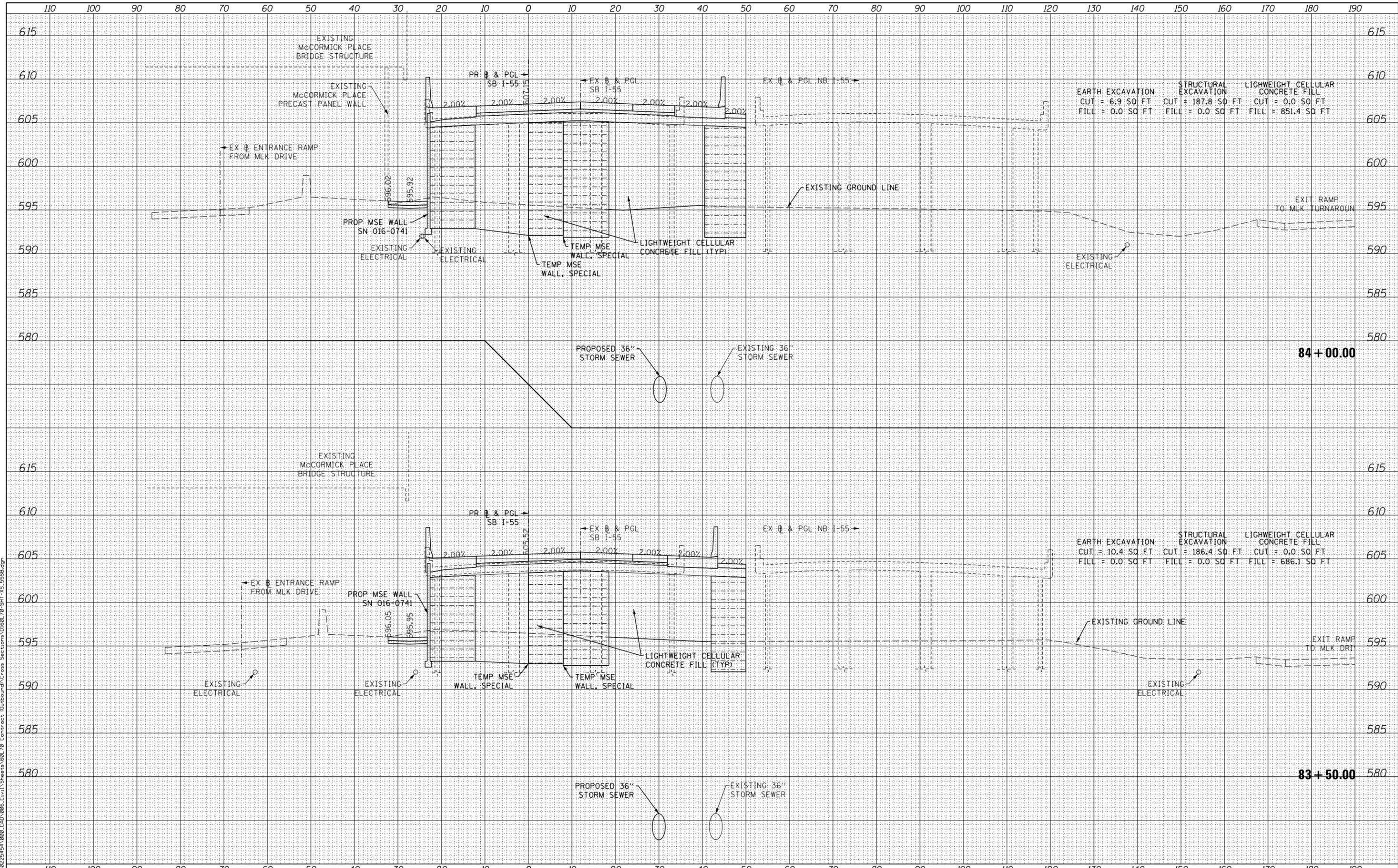
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	860
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

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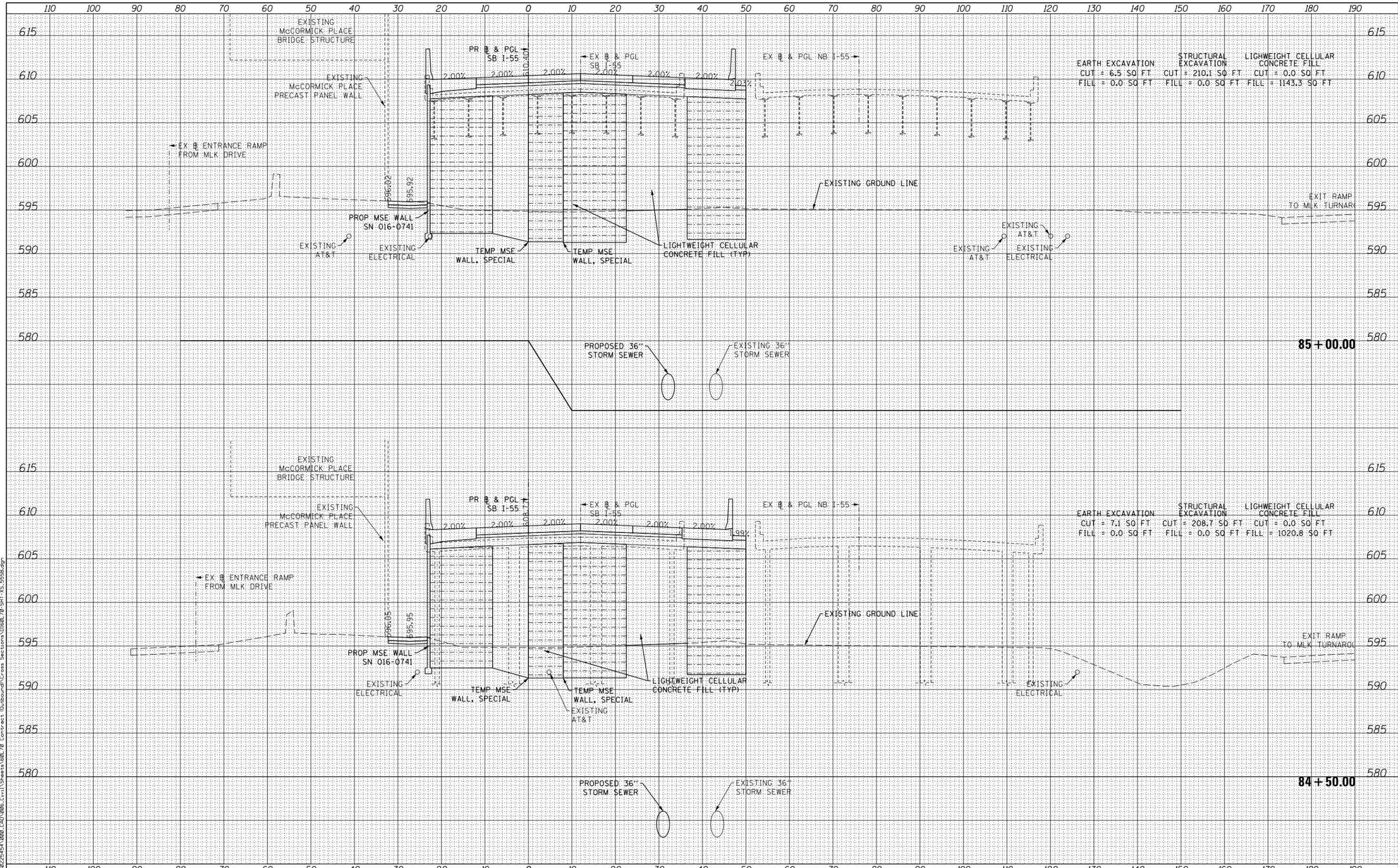
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F.A.I. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	861
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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 FILL = 0.0 SQ FT
 STRUCTURAL EXCAVATION CUT = 210.1 SQ FT
 FILL = 0.0 SQ FT
 LIGHTWEIGHT CELLULAR CONCRETE FILL CUT = 0.0 SQ FT
 FILL = 1143.3 SQ FT

EARTH EXCAVATION CUT = 7.1 SQ FT
 FILL = 0.0 SQ FT
 STRUCTURAL EXCAVATION CUT = 208.7 SQ FT
 FILL = 0.0 SQ FT
 LIGHTWEIGHT CELLULAR CONCRETE FILL CUT = 0.0 SQ FT
 FILL = 1020.8 SQ FT



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 DEPARTMENT OF TRANSPORTATION

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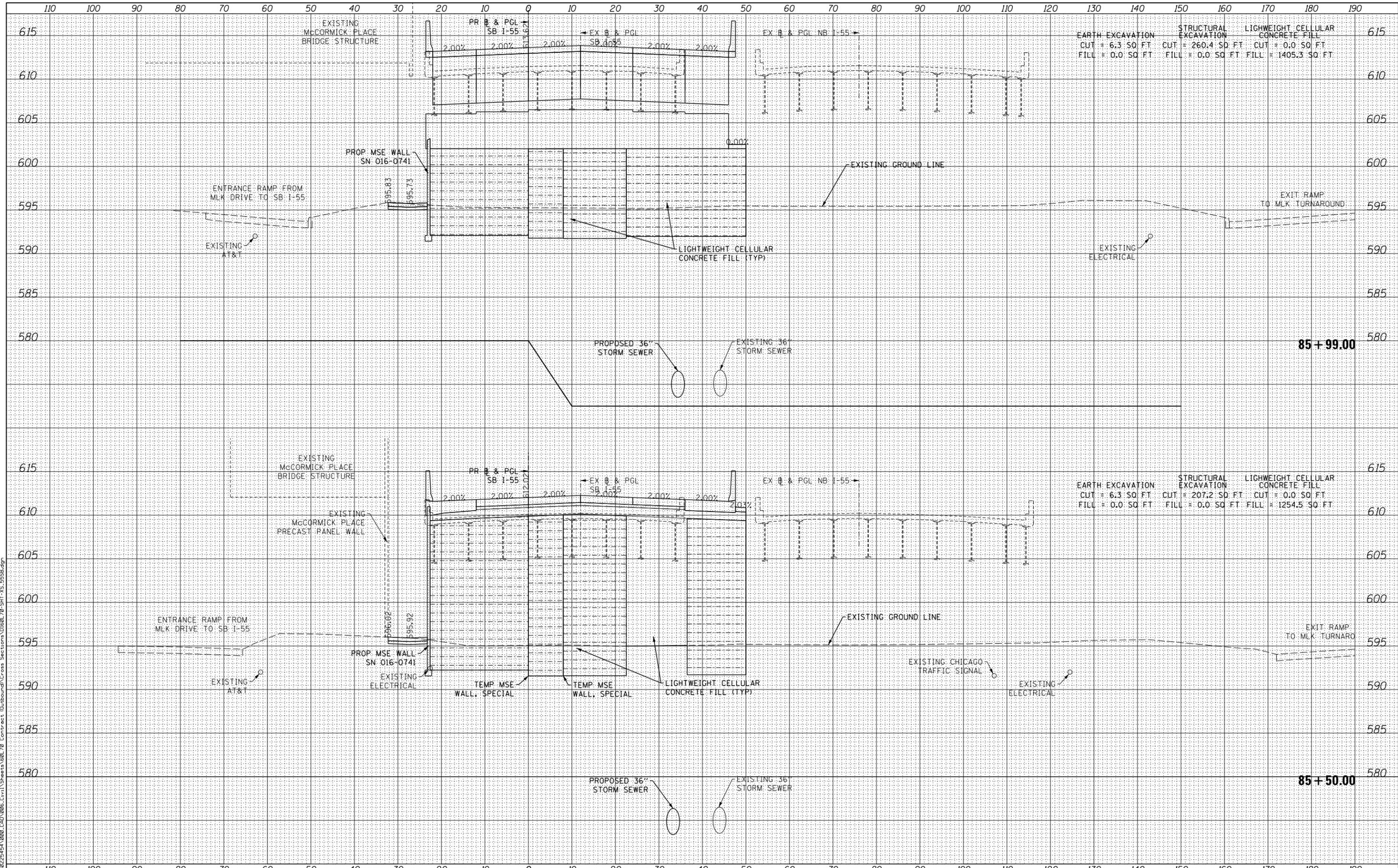
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	862
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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 DEPARTMENT OF TRANSPORTATION

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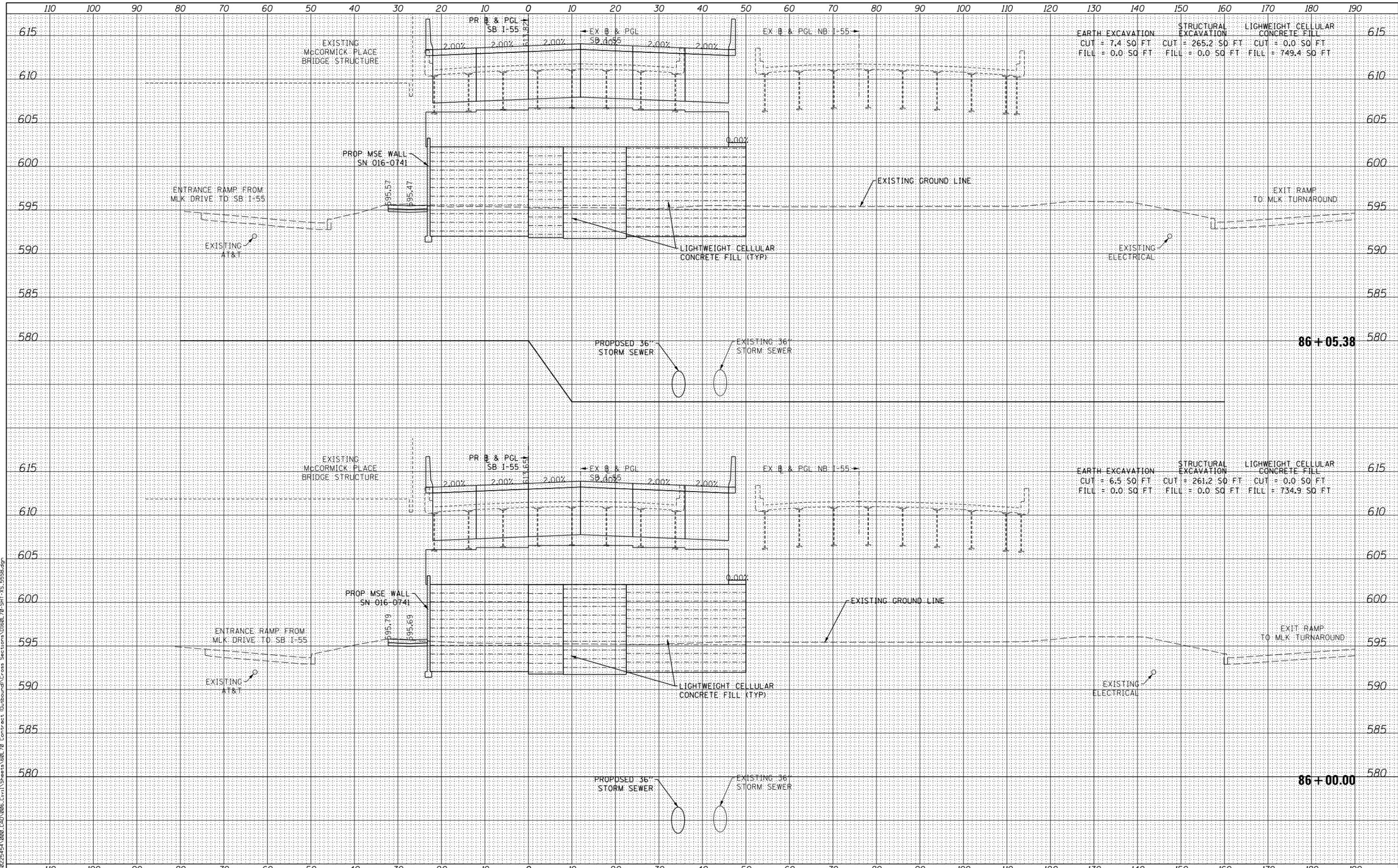
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	863
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 SB I-55

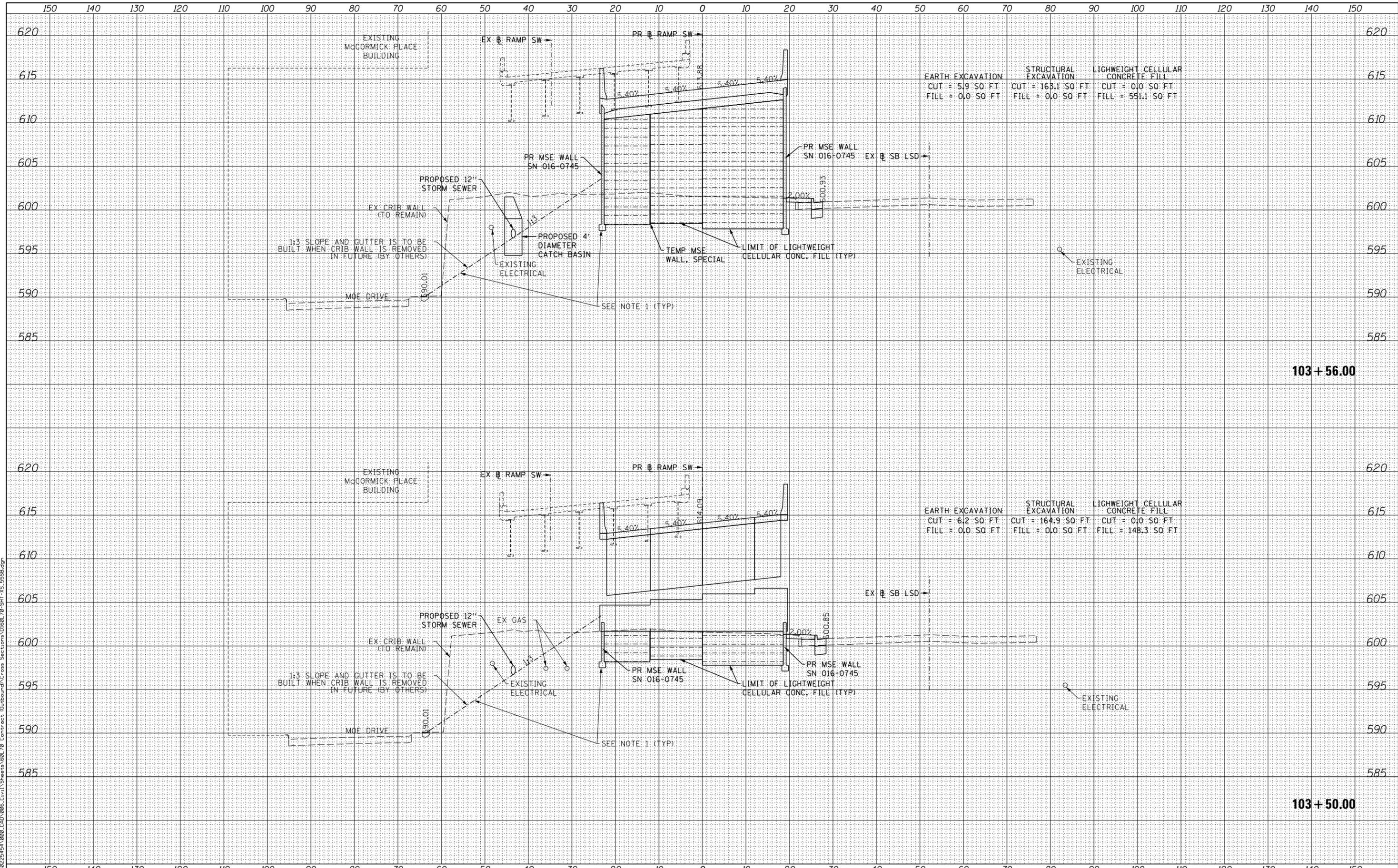
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	864
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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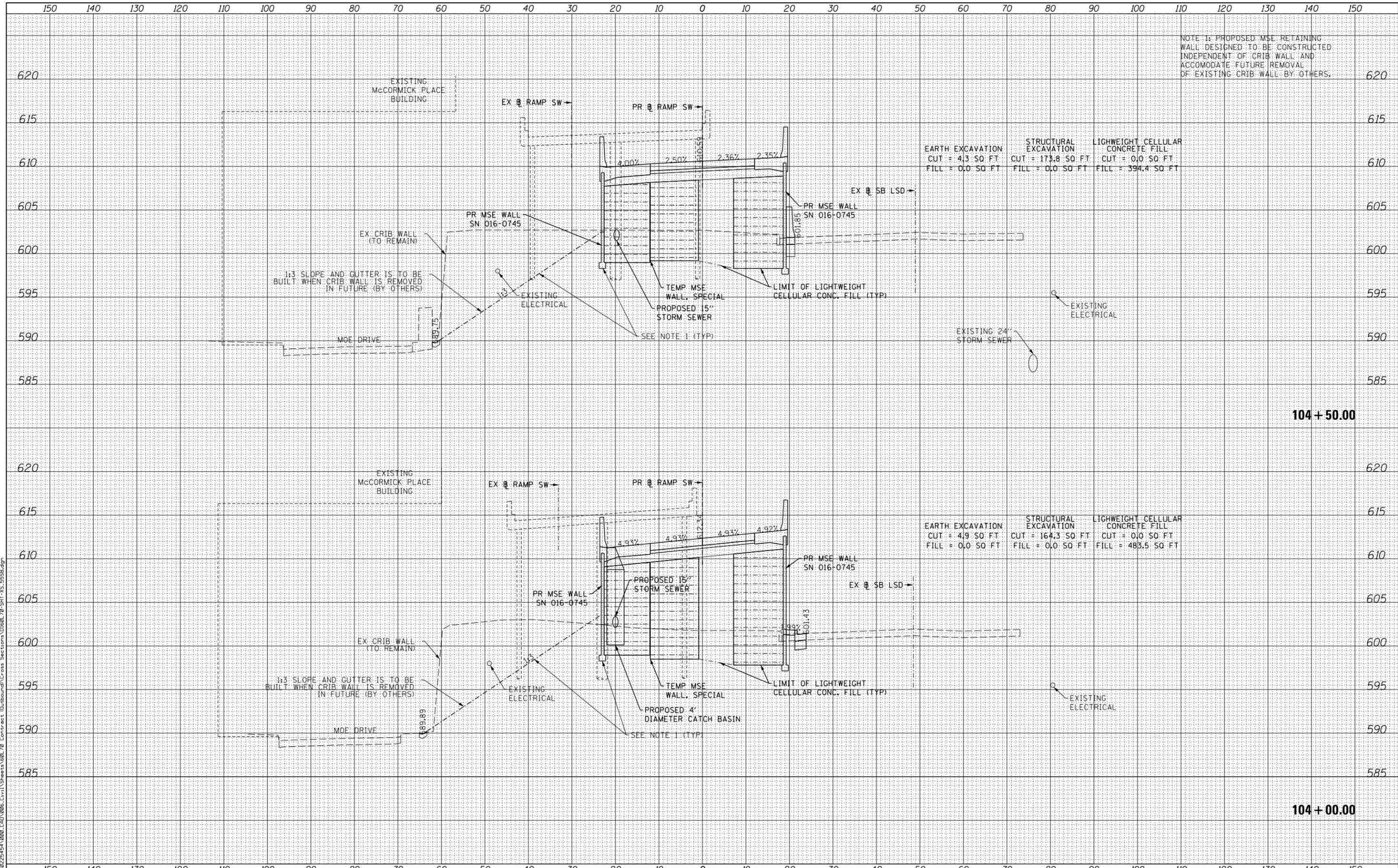
SCALE: SHEET NO. 1 OF 11 SHEETS STA. 103+50.00 TO STA. 103+56.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	865
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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FILE PATH = P:\68225451-080_CAD_006_Civil\Sheet_60L70_Cross Sections\0160L70-SHT_XS_055B.dgn



NOTE 1: PROPOSED MSE RETAINING WALL DESIGNED TO BE CONSTRUCTED INDEPENDENT OF CRIB WALL AND ACCOMMODATE FUTURE REMOVAL OF EXISTING CRIB WALL BY OTHERS.

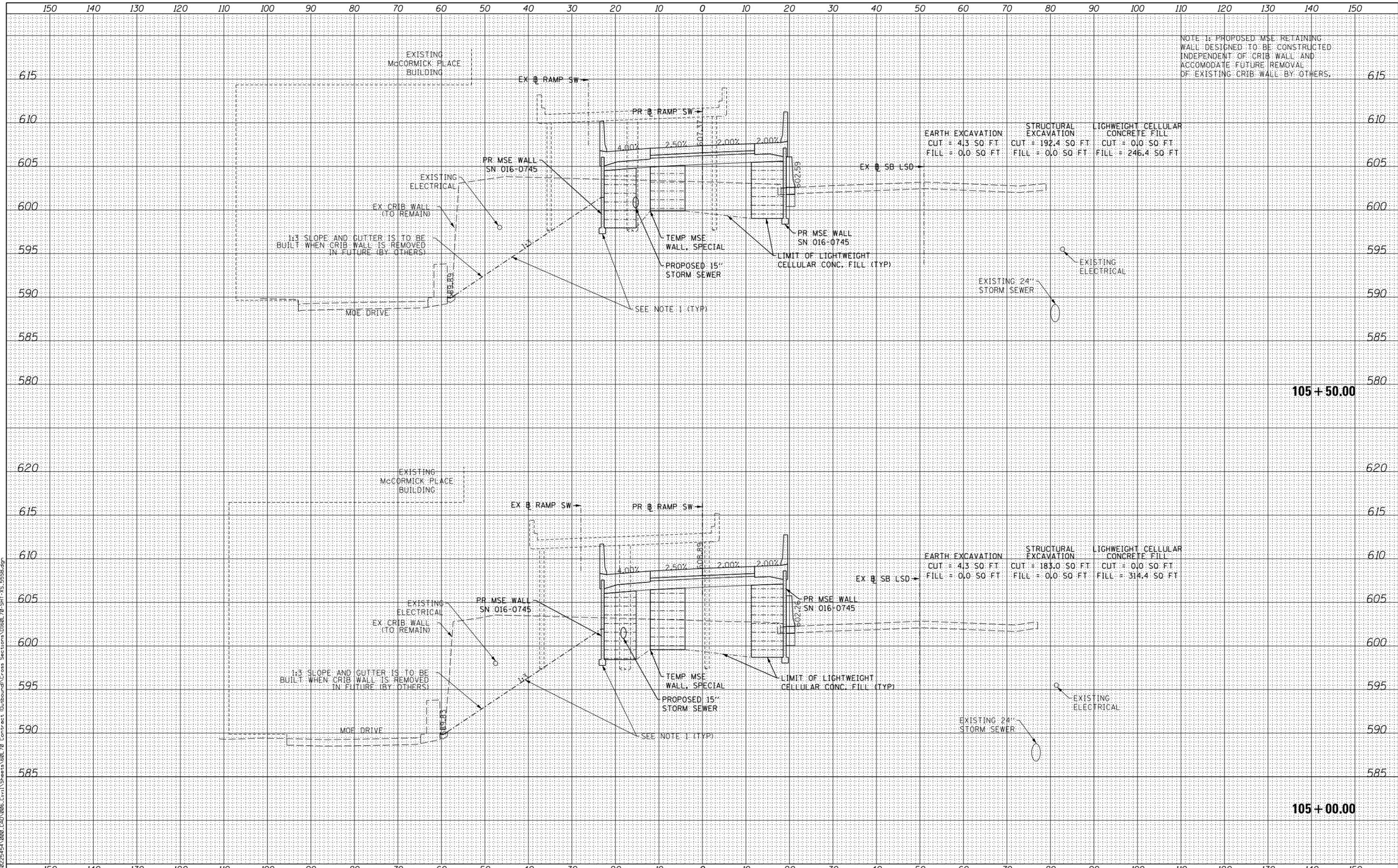
EARTH EXCAVATION CUT = 4.3 SQ FT FILL = 0.0 SQ FT
 STRUCTURAL EXCAVATION CUT = 173.8 SQ FT FILL = 0.0 SQ FT
 LIGHTWEIGHT CELLULAR CONCRETE FILL CUT = 0.0 SQ FT FILL = 394.4 SQ FT

EARTH EXCAVATION CUT = 4.9 SQ FT FILL = 0.0 SQ FT
 STRUCTURAL EXCAVATION CUT = 164.3 SQ FT FILL = 0.0 SQ FT
 LIGHTWEIGHT CELLULAR CONCRETE FILL CUT = 0.0 SQ FT FILL = 483.5 SQ FT

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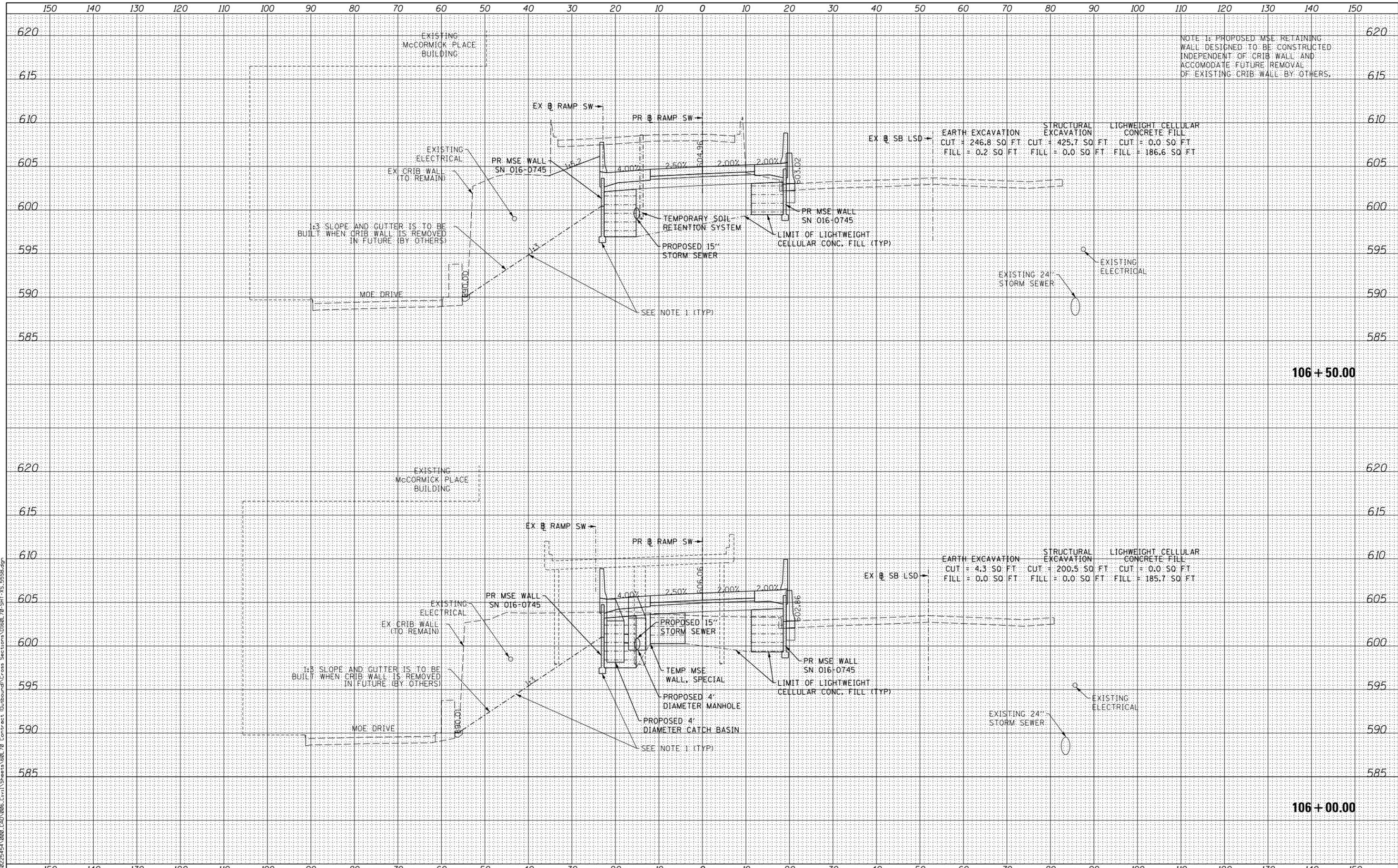
NOTE 1: PROPOSED MSE RETAINING WALL DESIGNED TO BE CONSTRUCTED INDEPENDENT OF CRIB WALL AND ACCOMMODATE FUTURE REMOVAL OF EXISTING CRIB WALL BY OTHERS.

<p>303 EAST WACKER DRIVE, SUITE 1400 CHICAGO, IL 60601-3276 PHONE: (312) 373-7700 FAX: (312) 373-8800</p>	USER NAME = augeburgh DESIGNED - WBS DRAWN - WBS CHECKED - MJE DATE - 11/14/2014	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS RAMP SW	F.A.I. RTE. = 55 SECTION = 2010-080-B COUNTY = COOK CONTRACT NO. = 60L70	TOTAL SHEETS = 886 SHEET NO. = 867
	PLOT SCALE = 10.0000' / in. PLOT DATE = 11/19/2014	SCALE: STA. 105+00.00 TO STA. 105+50.00			SHEET NO. 03 OF 11 SHEETS	

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FILE PATH = F:\68225454-080_CAD_006_Civil\Sheet\04\Cross Sections\0160L70-SHT XS.B55B.dgn



USER NAME = auyeahugh	DESIGNED - WBS	REVISED -
	DRAWN - WBS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - MJE	REVISED -
PLOT DATE = 11/19/2014	DATE - 11/14/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

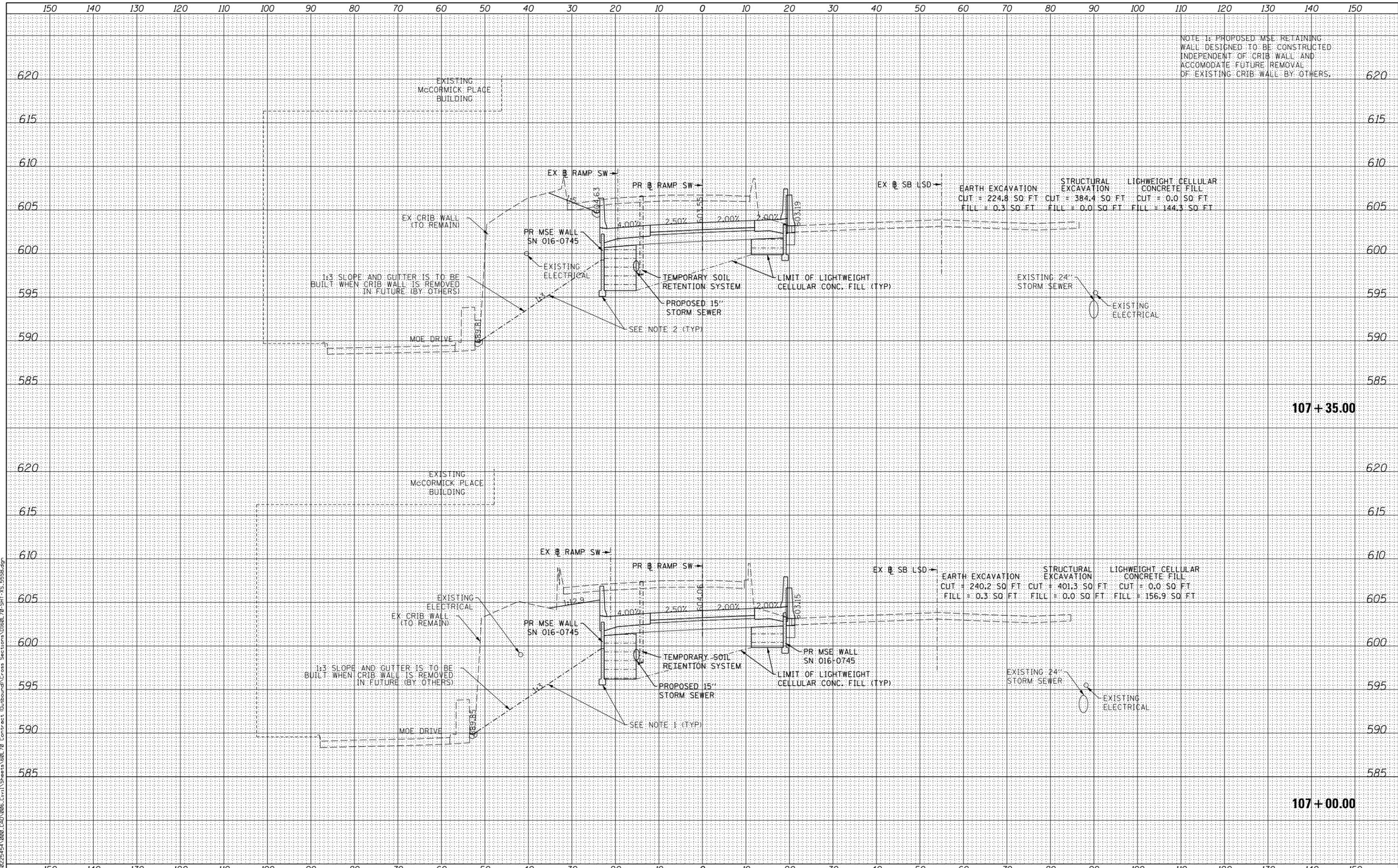
CROSS SECTIONS	
RAMP SW	
SCALE:	SHEET NO. 04 OF 11 SHEETS STA. 106+00.00 TO STA. 106+50.00

F.A.I. RTE. 55	SECTION 2010-080-B	COUNTY COOK	TOTAL SHEETS 886	SHEET NO. 868
			CONTRACT NO. 60L70	
ILLINOIS FED. AID PROJECT				

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ORIGINAL SURVEY	
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NOTE 1: PROPOSED MSE RETAINING WALL DESIGNED TO BE CONSTRUCTED INDEPENDENT OF CRIB WALL AND ACCOMMODATE FUTURE REMOVAL OF EXISTING CRIB WALL BY OTHERS.

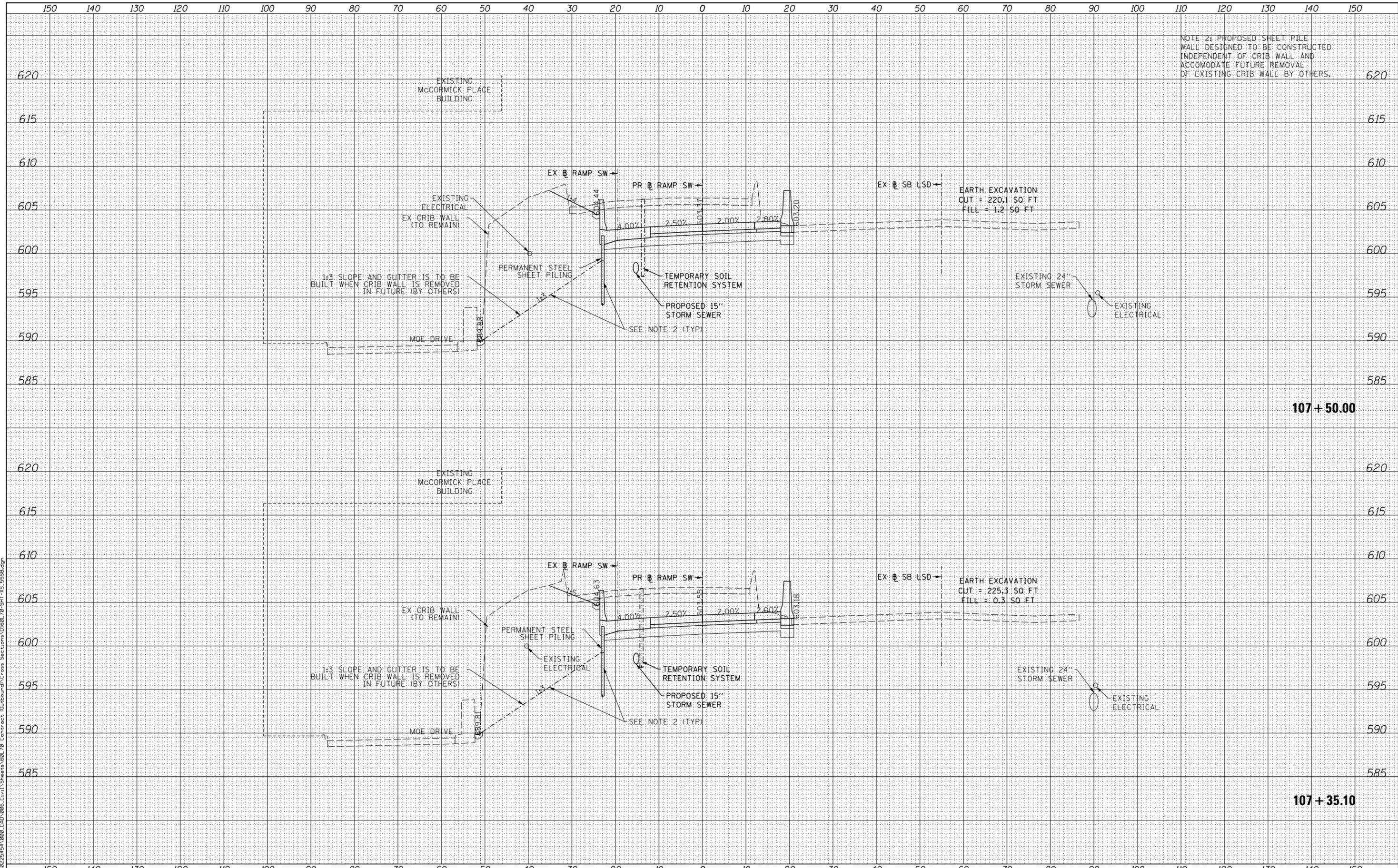
EARTH EXCAVATION	STRUCTURAL EXCAVATION	LIGHTWEIGHT CELLULAR CONCRETE FILL
CUT = 224.8 SQ FT	CUT = 384.4 SQ FT	CUT = 0.0 SQ FT
FILL = 0.3 SQ FT	FILL = 0.0 SQ FT	FILL = 144.3 SQ FT

EARTH EXCAVATION	STRUCTURAL EXCAVATION	LIGHTWEIGHT CELLULAR CONCRETE FILL
CUT = 240.2 SQ FT	CUT = 401.3 SQ FT	CUT = 0.0 SQ FT
FILL = 0.3 SQ FT	FILL = 0.0 SQ FT	FILL = 156.9 SQ FT

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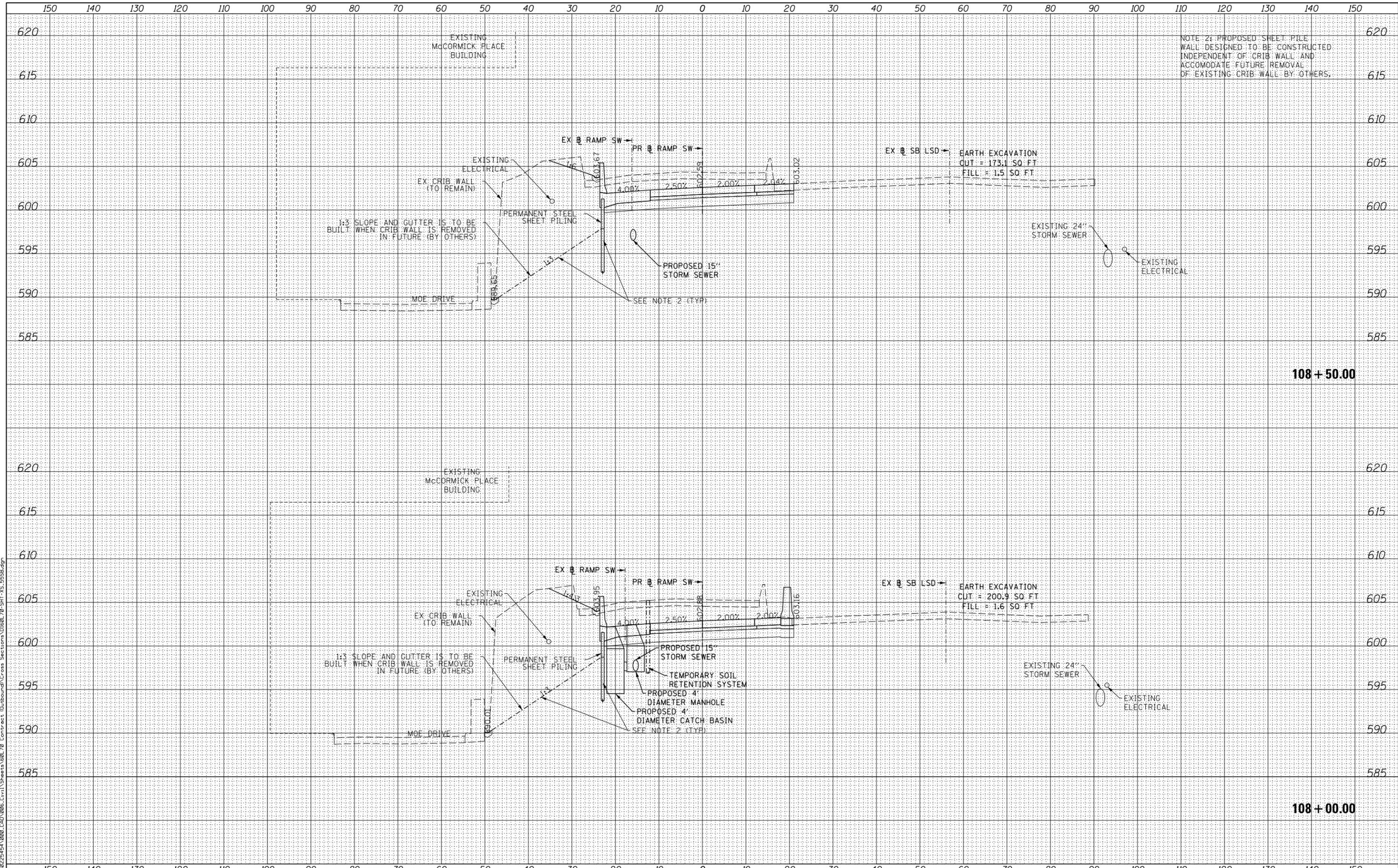
NOTE 2: PROPOSED SHEET PILE WALL DESIGNED TO BE CONSTRUCTED INDEPENDENT OF CRIB WALL AND ACCOMMODATE FUTURE REMOVAL OF EXISTING CRIB WALL BY OTHERS.

	USER NAME = augeungh	DESIGNED - WBS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS RAMP SW		F.A.I. RTE. = 55	SECTION = 2010-080-B	COUNTY = COOK	TOTAL SHEETS = 886	SHEET NO. = 870
	PLOT SCALE = 10.0000' / in.	CHECKED - MJE	REVISED -		SCALE:	SHEET NO. 06 OF 11 SHEETS	STA. 107+35.10 TO STA. 107+50.00	CONTRACT NO. 60L70		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 11/19/2014	DATE = 11/14/2014	REVISED -								

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NOTE 2: PROPOSED SHEET PILE WALL DESIGNED TO BE CONSTRUCTED INDEPENDENT OF CRIB WALL AND ACCOMMODATE FUTURE REMOVAL OF EXISTING CRIB WALL BY OTHERS.

EARTH EXCAVATION
OUT = 173.1 SQ. FT.
FILL = 1.5 SQ. FT.

EARTH EXCAVATION
CUT = 200.9 SQ. FT.
FILL = 1.6 SQ. FT.



USER NAME = augeungh
DESIGNED - WBS
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CHECKED - MJE
DATE - 11/14/2014

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RAMP SW

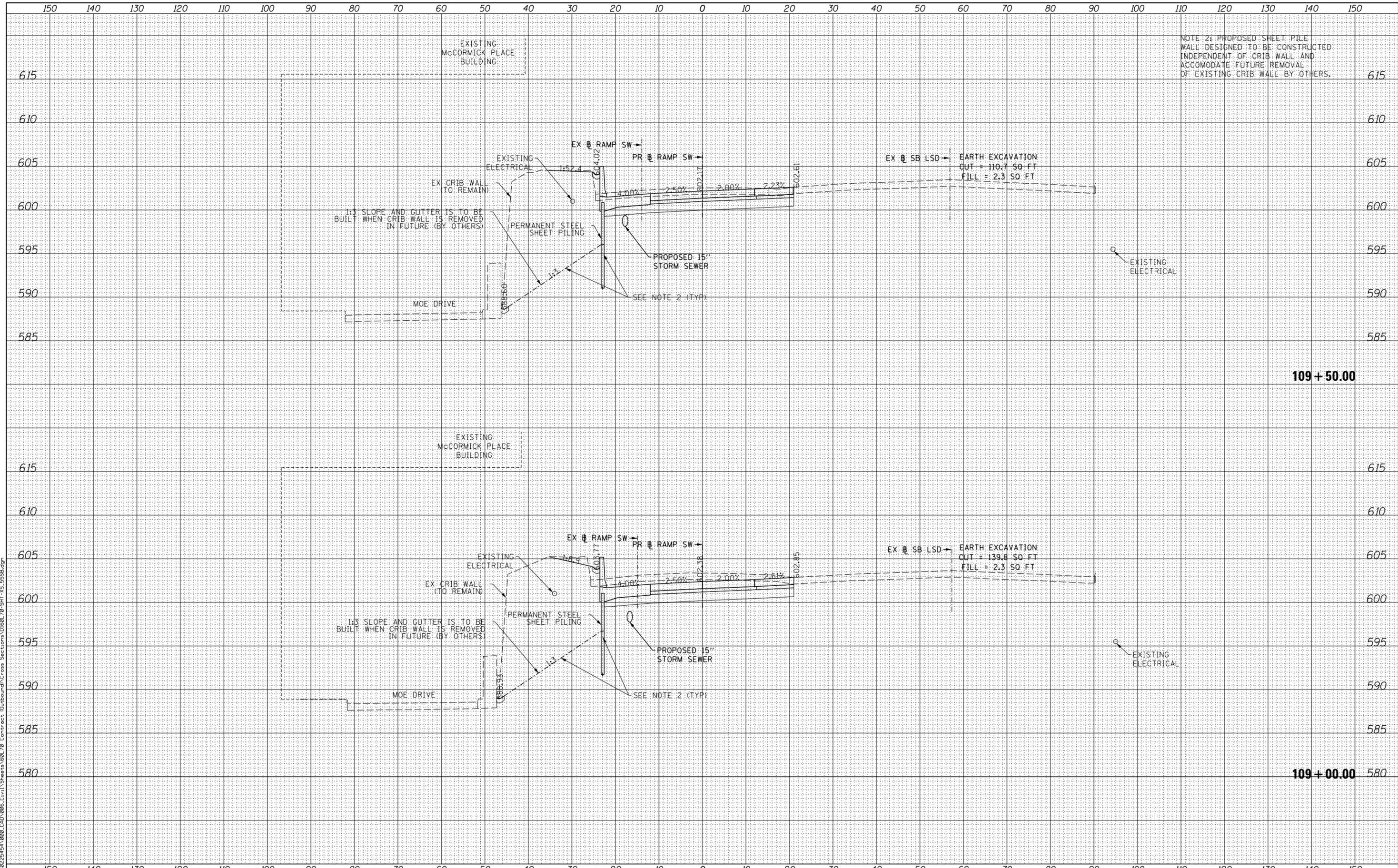
SCALE: SHEET NO. 07 OF 11 SHEETS STA. 108+00.00 TO STA. 108+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	871
CONTRACT NO.			60L70	
ILLINOIS FED. AID PROJECT				

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NOTE 2: PROPOSED SHEET PILE WALL DESIGNED TO BE CONSTRUCTED INDEPENDENT OF CRIB WALL AND ACCOMMODATE FUTURE REMOVAL OF EXISTING CRIB WALL BY OTHERS.



USER NAME = augeungh	DESIGNED - WBS	REVISED -
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PLOT SCALE = 10.0000' / in.	CHECKED - MJE	REVISED -
PLOT DATE = 11/19/2014	DATE - 11/14/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RAMP SW

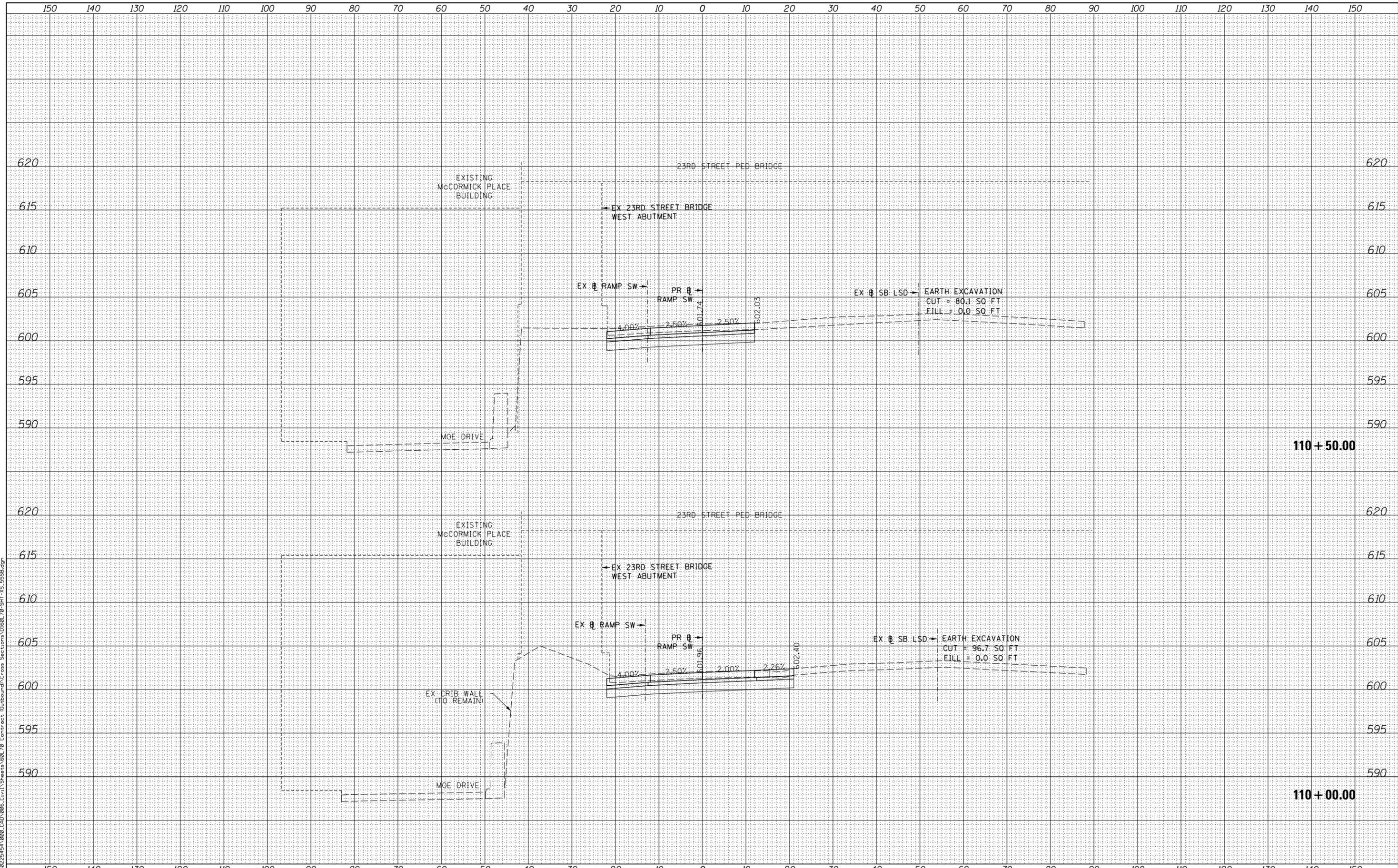
SCALE: SHEET NO. 08 OF 11 SHEETS STA. 109+00.00 TO STA. 109+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	872
			CONTRACT NO.	60L70
ILLINOIS FED. AID PROJECT				

DATE	
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FILE PATH = P:\68225454-080_CAD_006_Civil\Sheet\080_Cross Sections\0168L 70-SH XS-B55B.dgn



USER NAME = augeungh
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 DATE - 11/14/2014

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 RAMP SW

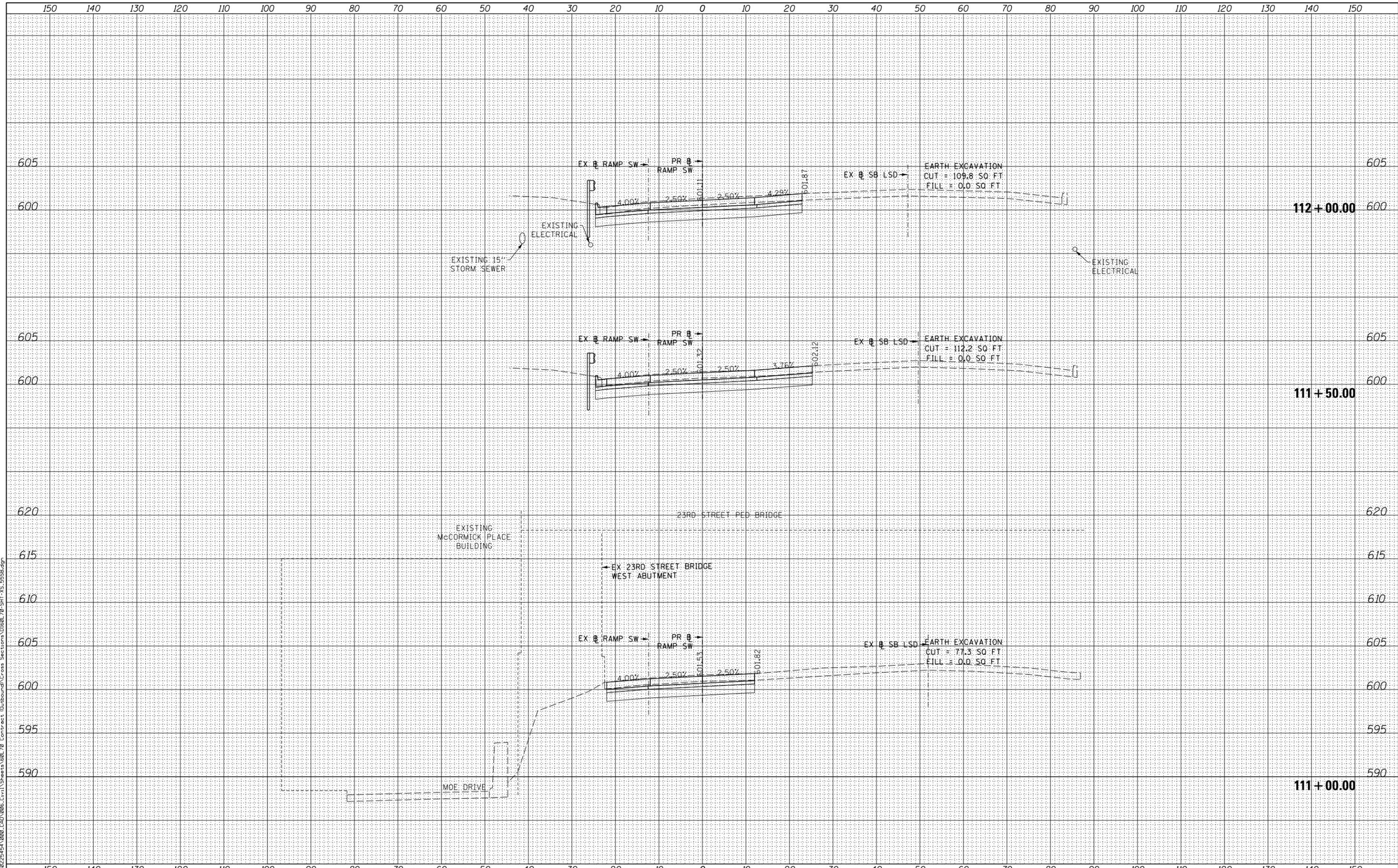
SCALE: SHEET NO. 09 OF 11 SHEETS STA. 110+00.00 TO STA. 110+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	873
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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USER NAME = augeaugh	DESIGNED - WBS	REVISED -
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PLOT SCALE = 10.0000' / in.	CHECKED - MJE	REVISED -
PLOT DATE = 11/19/2014	DATE - 11/14/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RAMP SW

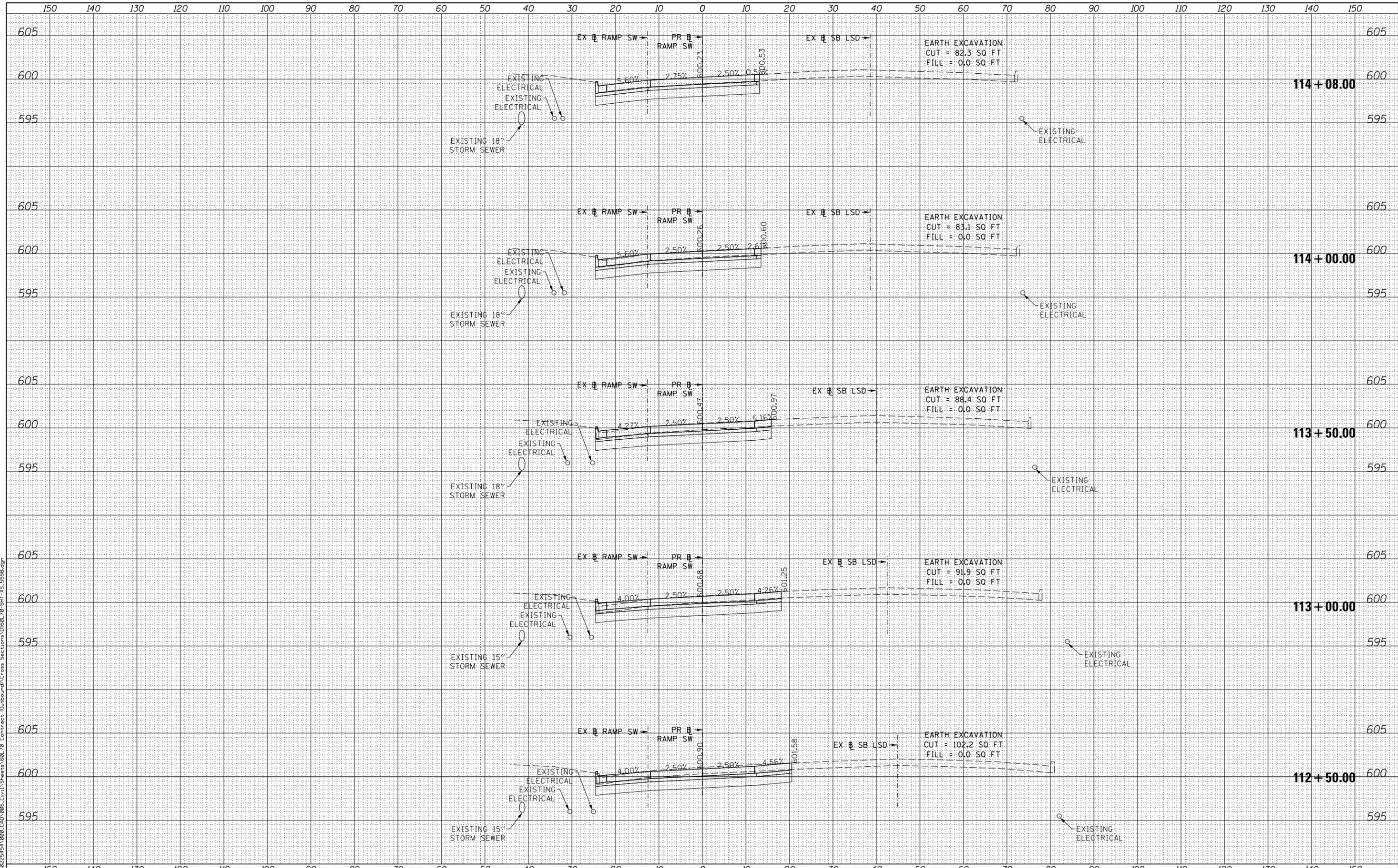
SCALE: SHEET NO. 10 OF 11 SHEETS STA. 111+00.00 TO STA. 112+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	874
			CONTRACT NO.	60L70
ILLINOIS FED. AID PROJECT				

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USER NAME = auyeaugh
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
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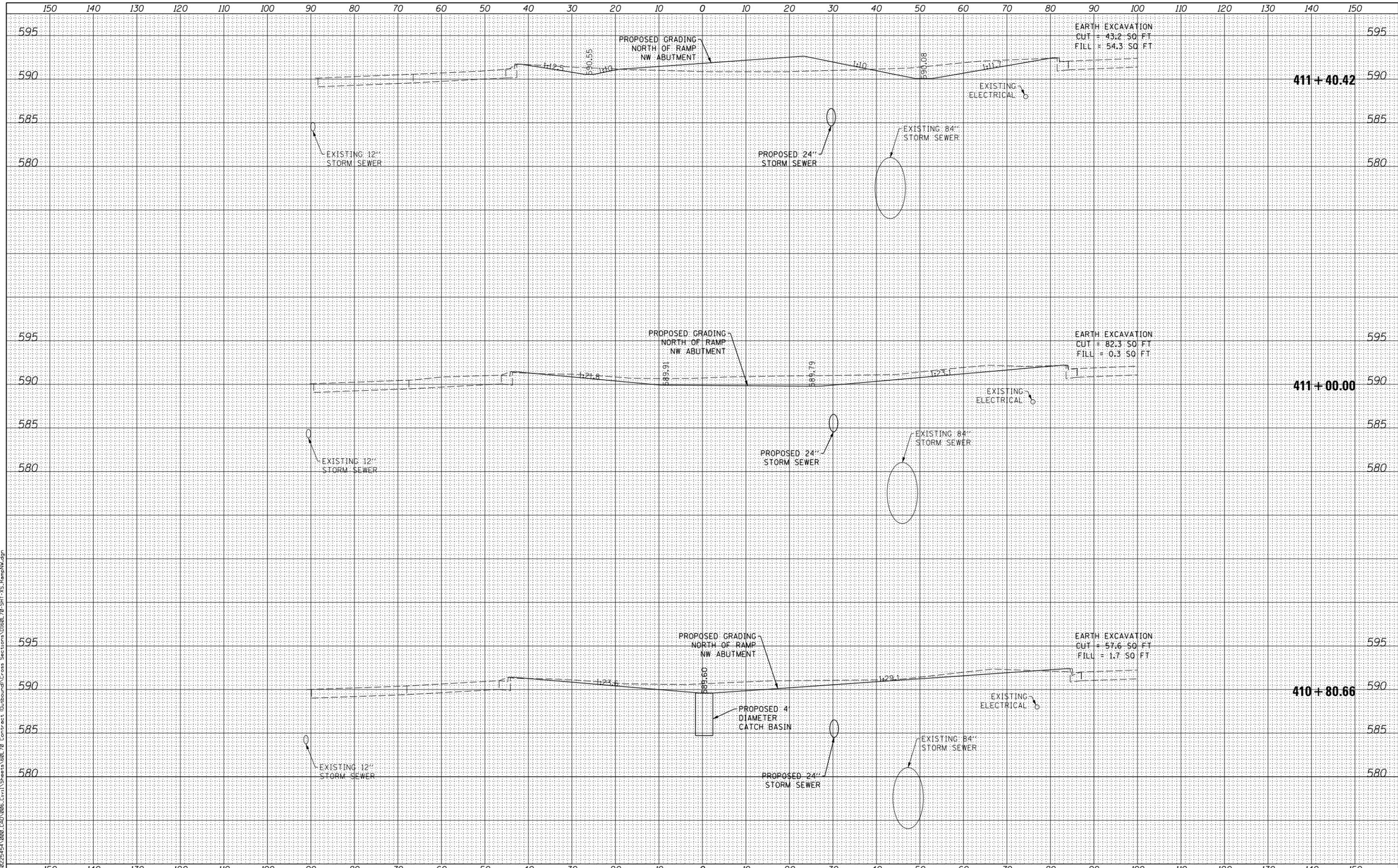
SCALE: SHEET NO. 11 OF 11 SHEETS STA. 112+50.00 TO STA. 113+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	875
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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USER NAME = augeaugh
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 RAMP NW

SHEET NO. 1 OF 11 SHEETS STA. 410+80.66 TO STA. 411+40.42

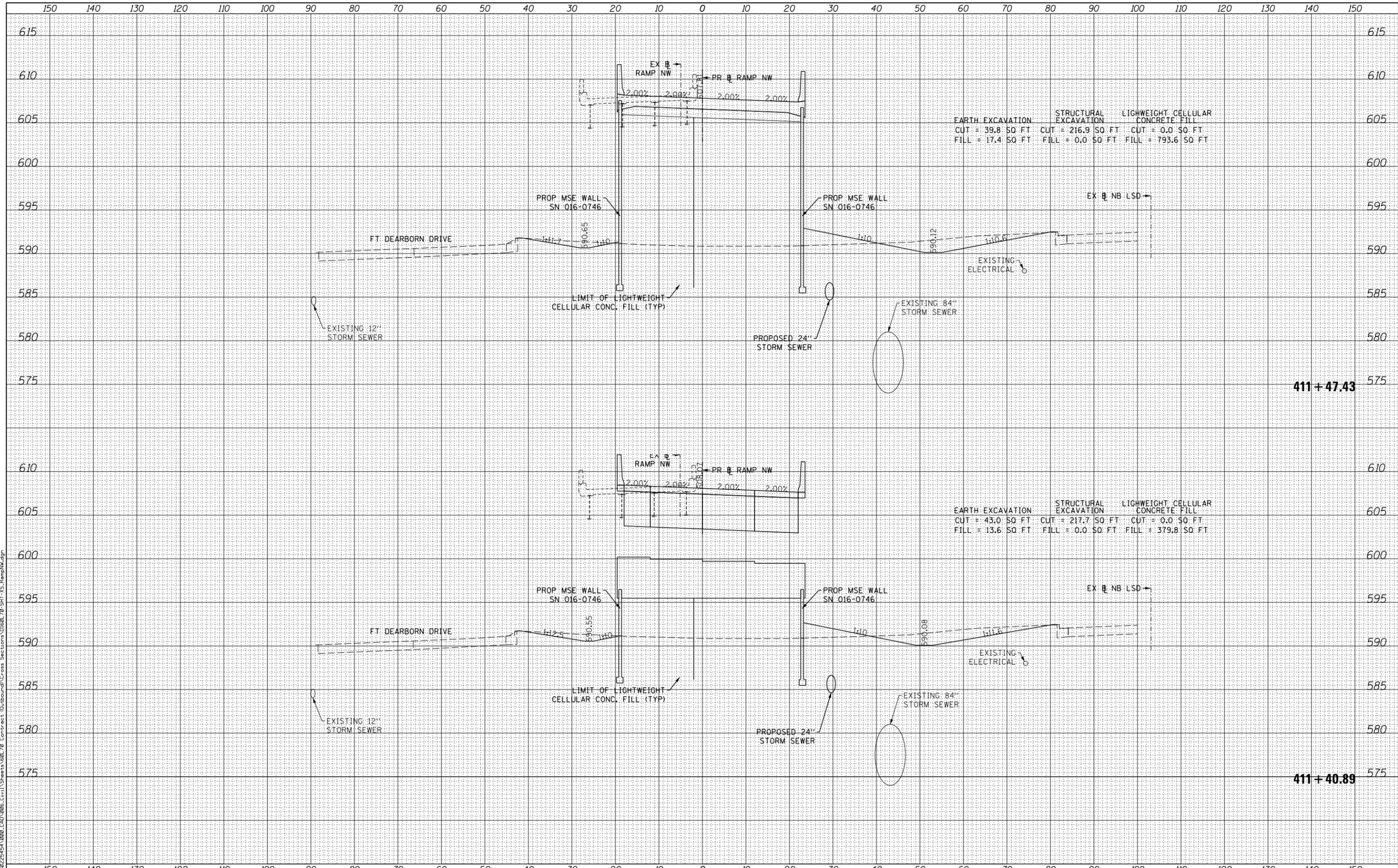
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	876
CONTRACT NO. 60L70				

ILLINOIS FED. AID PROJECT

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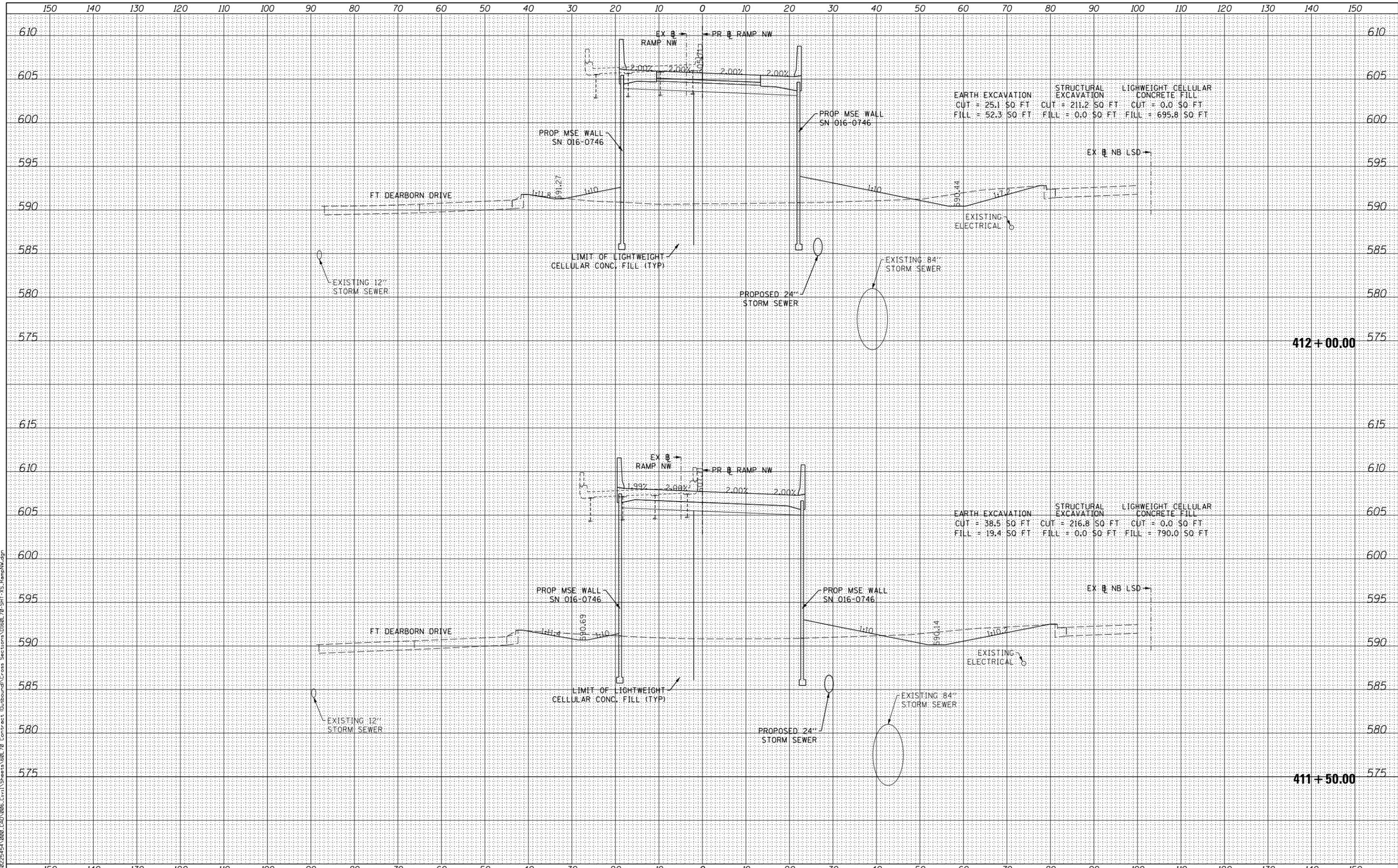
EARTH EXCAVATION CUT = 39.8 SQ FT
 STRUCTURAL EXCAVATION CUT = 216.9 SQ FT
 LIGHTWEIGHT CELLULAR CONCRETE FILL FILL = 0.0 SQ FT
 FILL = 17.4 SQ FT FILL = 0.0 SQ FT FILL = 793.6 SQ FT

EARTH EXCAVATION CUT = 43.0 SQ FT
 STRUCTURAL EXCAVATION CUT = 217.7 SQ FT
 LIGHTWEIGHT CELLULAR CONCRETE FILL FILL = 0.0 SQ FT
 FILL = 13.6 SQ FT FILL = 0.0 SQ FT FILL = 379.8 SQ FT

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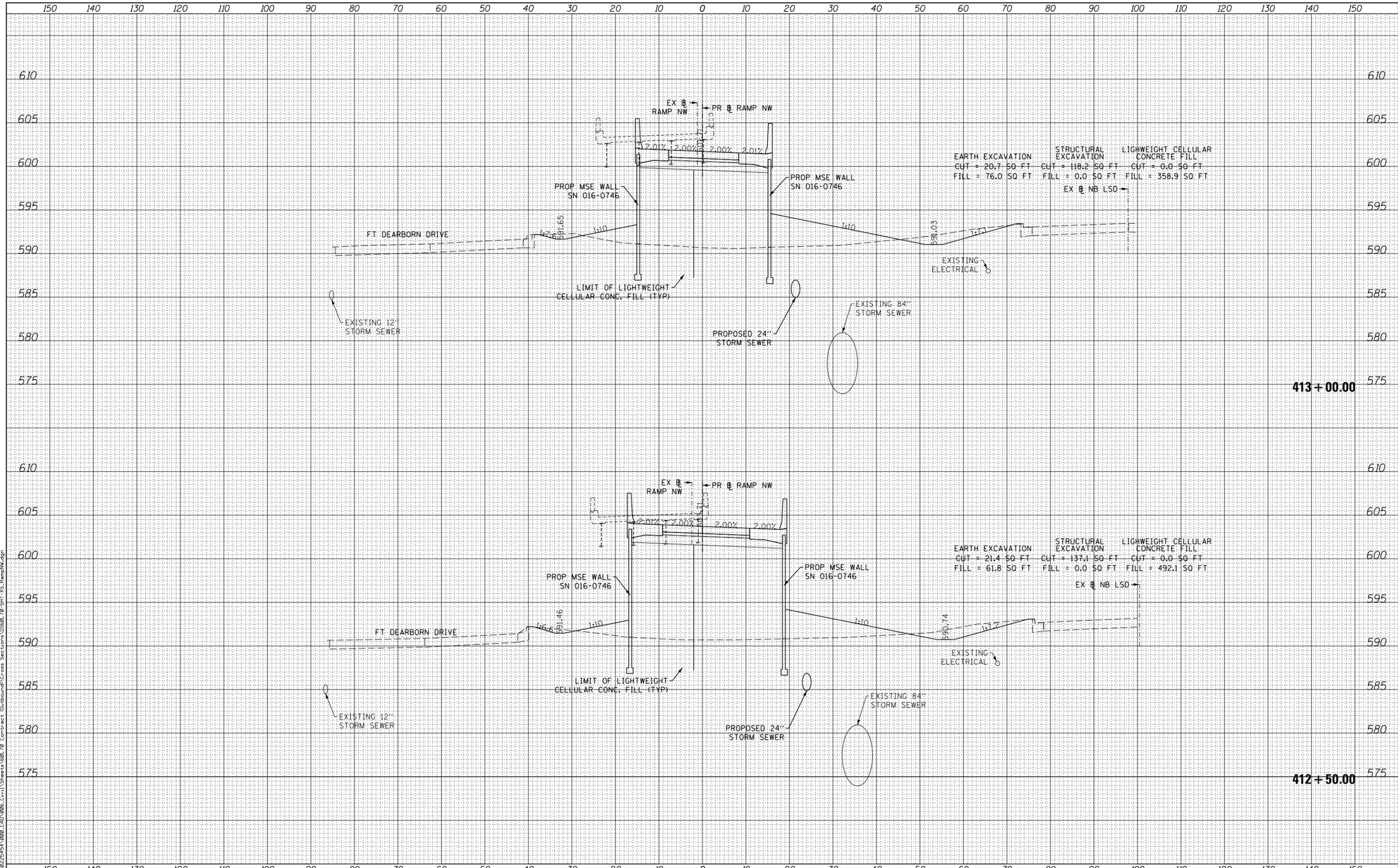
EARTH EXCAVATION	STRUCTURAL EXCAVATION	LIGHTWEIGHT CELLULAR CONCRETE FILL
CUT = 25.1 SQ FT	CUT = 211.2 SQ FT	CUT = 0.0 SQ FT
FILL = 52.3 SQ FT	FILL = 0.0 SQ FT	FILL = 695.8 SQ FT

EARTH EXCAVATION	STRUCTURAL EXCAVATION	LIGHTWEIGHT CELLULAR CONCRETE FILL
CUT = 38.5 SQ FT	CUT = 216.8 SQ FT	CUT = 0.0 SQ FT
FILL = 19.4 SQ FT	FILL = 0.0 SQ FT	FILL = 790.0 SQ FT

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USER NAME = auyeaugh
 DESIGNED - WBS
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 DATE - 11/14/2014

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 RAMP NW

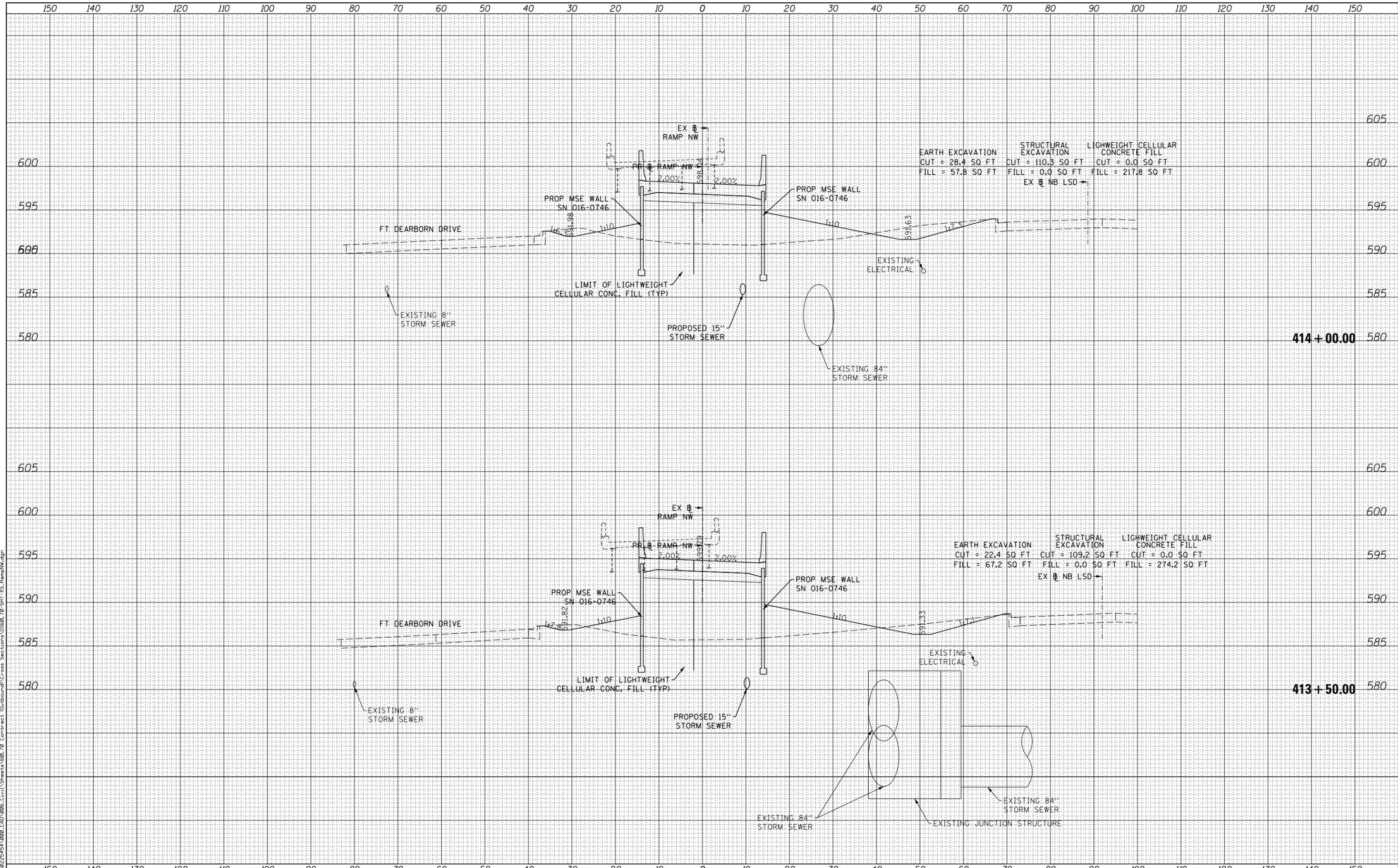
SHEET NO. 4 OF 11 SHEETS STA. 412+50.00 TO STA. 413+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	879
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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FILE PATH = P:\60225451-080_CAD_006_Civil\Cross Sections\0168L 70-SHT XS RampNW.dgn



USER NAME = augeaugh
 DESIGNED - WBS
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 DATE - 11/14/2014

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 RAMP NW

SHEET NO. 5 OF 11 SHEETS STA. 413+50.00 TO STA. 414+00.00

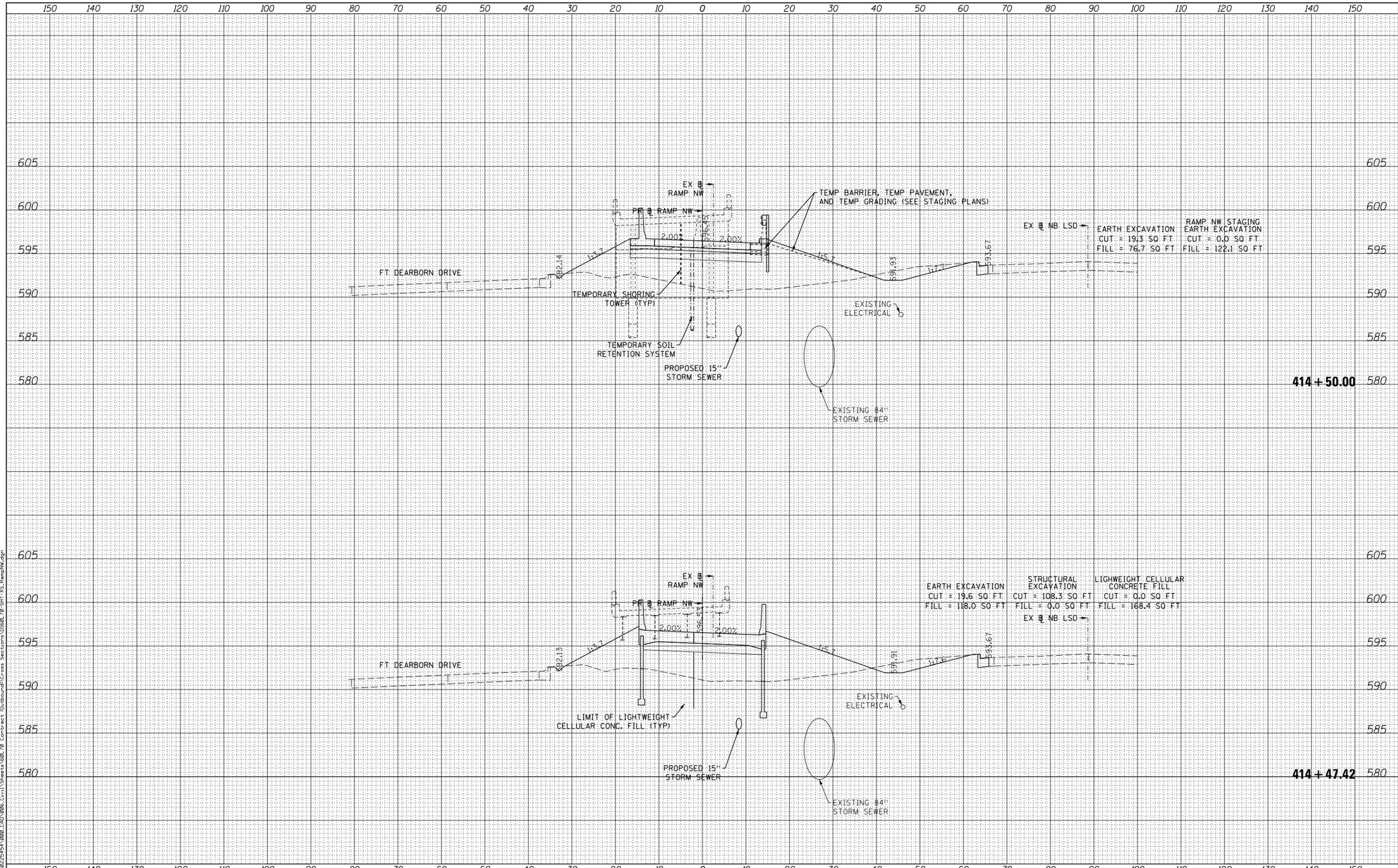
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	880
CONTRACT NO. 60L70				

ILLINOIS FED. AID PROJECT

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FILE PATH = P:\60225451-080-CAD-006-Civil\Sheet\60L70-Cross Sections\0160L70-SHT XS-RampNW.dgn



USER NAME = auyeaugh
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
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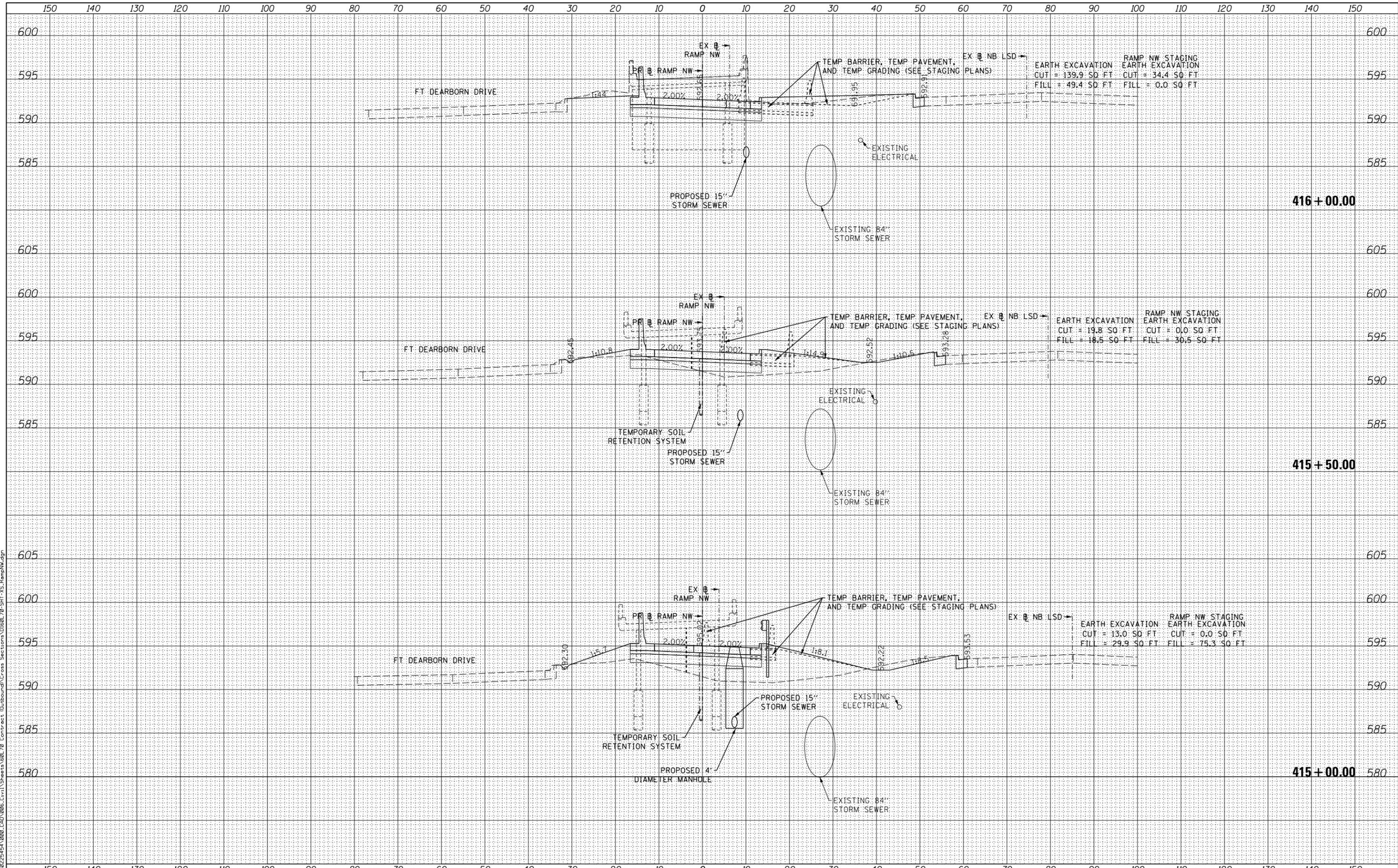
SHEET NO. 6 OF 11 SHEETS STA. 414+47.42 TO STA. 414+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	881
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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FILE PATH = P:\68225451-080_CAD_006_Civil\Sheet\Outbound\Cross Sections\0168L70-SHT-XS-RampNW.dgn



USER NAME = augeungh
 DESIGNED - WBS
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
 RAMP NW**

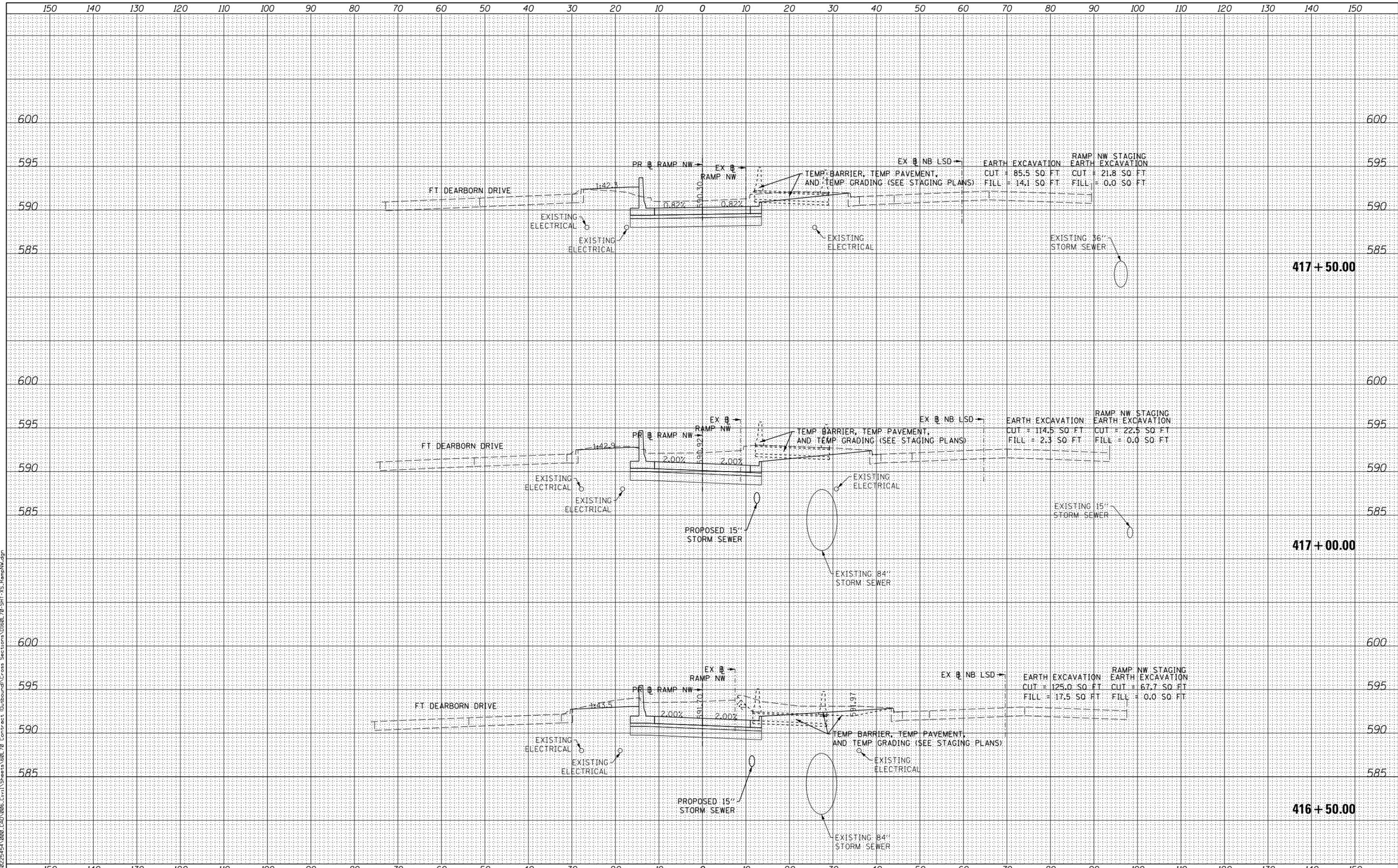
SHEET NO. 7 OF 11 SHEETS STA. 415+00.00 TO STA. 416+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	882
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

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FILE PATH = P:\68225451-080_CAD_086_Civil\Sheet\Cross Sections\0168L 70-SHT XS RampNW.dgn



USER NAME = augeungh	DESIGNED - WBS	REVISED -
	DRAWN - WBS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - MJE	REVISED -
PLOT DATE = 11/19/2014	DATE - 11/14/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RAMP NW

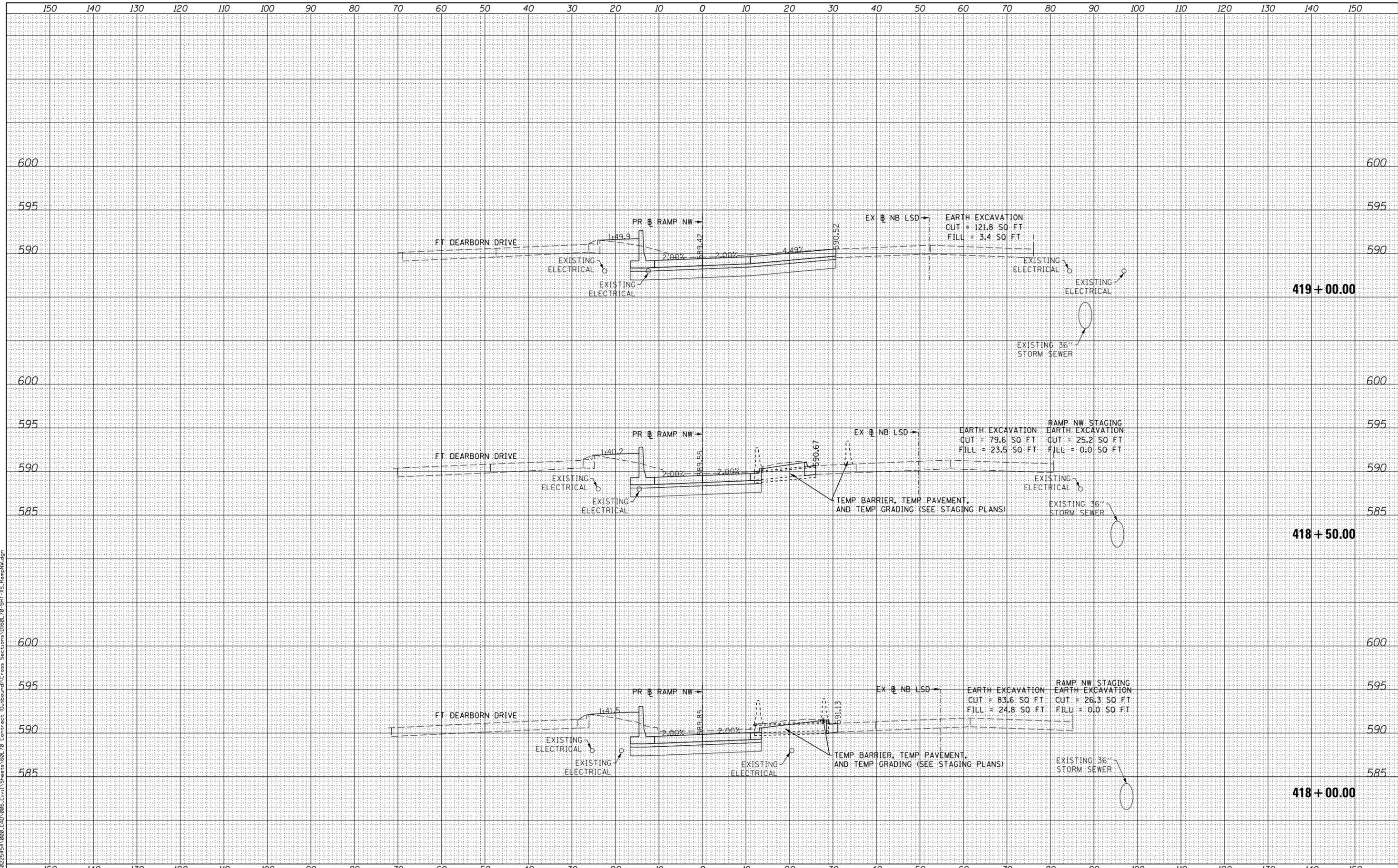
SHEET NO. 8 OF 11 SHEETS STA. 416+50.00 TO STA. 417+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	883
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

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USER NAME = augeungh
 DESIGNED - WBS
 DRAWN - WBS
 CHECKED - MJE
 DATE - 11/14/2014

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 RAMP NW

SHEET NO. 9 OF 11 SHEETS STA. 418+00.00 TO STA. 419+00.00

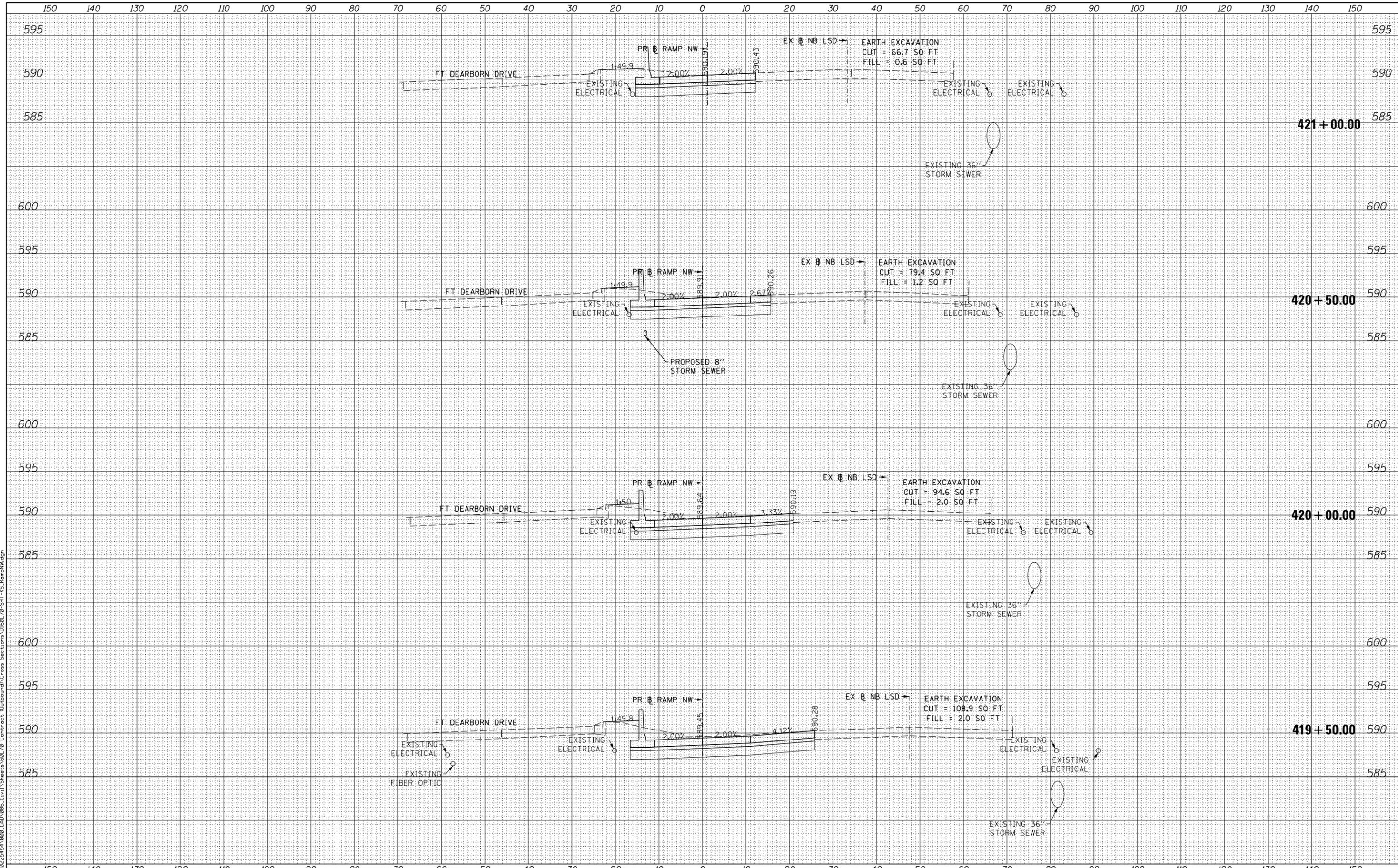
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	884
			CONTRACT NO. 60L70	

ILLINOIS FED. AID PROJECT

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
AREAS CHECKED	
NO.	
FINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	
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DATE	
BY	
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USER NAME = auyeaugh
 DESIGNED - WBS
 DRAWN - WBS
 CHECKED - MJE
 DATE - 11/14/2014

REVISED -
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 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 RAMP NW

SHEET NO. 10 OF 11 SHEETS STA. 419+50.00 TO STA. 421+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2010-080-B	COOK	886	885
CONTRACT NO. 60L70			ILLINOIS FED. AID PROJECT	

