

01-30-15 LETTING ITEM 015

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF LA GRANGE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**  
FAU ROUTE 1365 (COSSITT AVENUE)  
GILBERT AVENUE TO BRAINARD AVENUE  
RESURFACING  
SECTION 12-00086-00-RS  
PROJECT NO.: M-4003(081)  
VILLAGE OF LA GRANGE  
COOK COUNTY  
JOB NO.: C-91-010-13

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	1
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED AID PROJECT M-4003(081)	
CONTRACT 61A84				

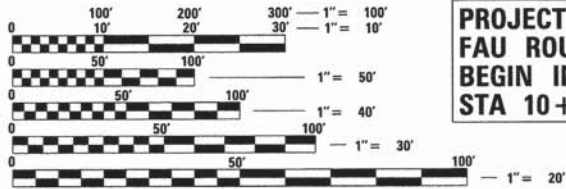


**TRAFFIC DATA**

COSSITT AVENUE  
POSTED SPEED LIMIT = 20-25 MPH  
2014 ADT = 2,100 VPD

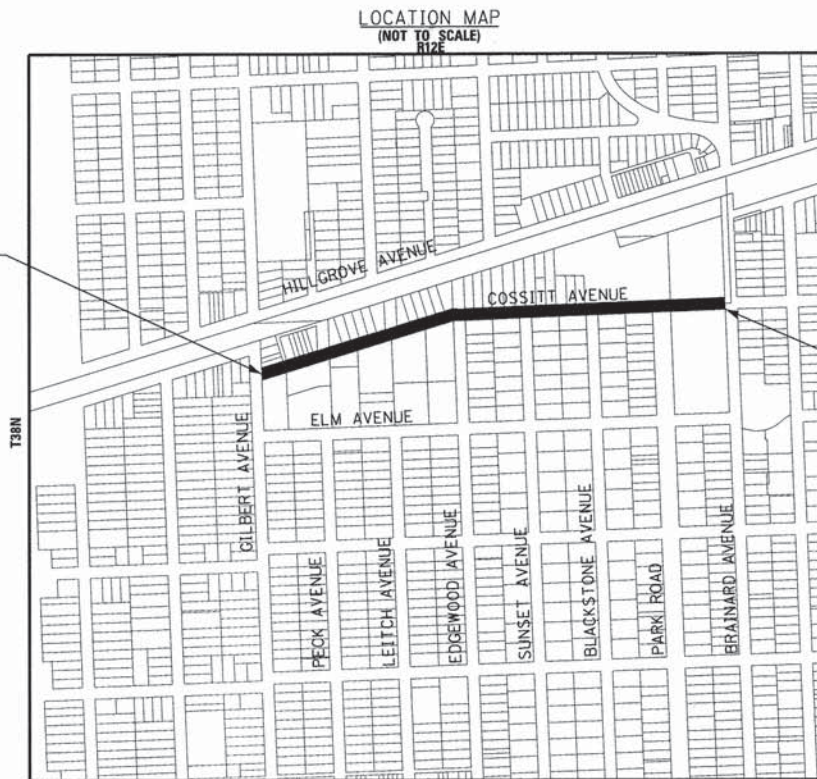
**DESIGN DESIGNATION**

MAJOR COLLECTOR



PROJECT NUMBER: M-4003(081)  
FAU ROUTE 1365 (COSSITT AVENUE)  
BEGIN IMPROVEMENTS  
STA 10+12

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



PROJECT NUMBER: M-4003(081)  
FAU ROUTE 1365 (COSSITT AVENUE)  
END IMPROVEMENTS  
STA 36+56

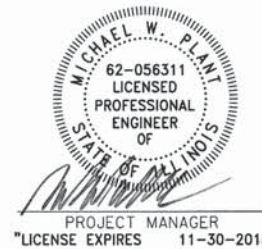
R12E  
THE THIRD PRINCIPAL MERIDIAN  
LYONS TOWNSHIP  
GROSS LENGTH OF IMPROVEMENT = 2,644 LF OR 0.501 MILES  
NET LENGTH OF IMPROVEMENT = 2,644 LF OR 0.501 MILES

J.U.L.I.E. DESIGN STAGE REQUEST  
DIG. No. X3250403



CONTACT JULIE AT 811 OR 800-892-0123  
WITH THE FOLLOWING:  
COUNTY = COOK  
CITY-TOWNSHIP = LA GRANGE/LYONS  
SEC. & 1/4 SEC. NO. = 5NESE, T38N R12E  
48 HOURS (2 working days) BEFORE YOU DIG

**BAXTER & WOODMAN**  
Consulting Engineers



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED 10/16/2014  
*Rudolph*  
VILLAGE OF LA GRANGE DIRECTOR OF PUBLIC WORKS

PASSED November 7, 2014  
*Christopher Holt*  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW  
November 10, 2014  
*John Fortmann*  
DEPUTY DIRECTOR OF  
HIGHWAYS/REGION 1 ENGINEER

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OF THE STATE OF ILLINOIS

CONTRACT NO. 61A84

B&W PROJECT NO.: 100261 DATE: 08-12-14

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 STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM  
 LICENSE NO. 08-00000000-00000000-00000000-00000000  
 11/27/2004 10/16/2014 8/25/14 AM  
 FEDERAL AID PROGRAM ENGINEER FAWAD AQUEEL, PE PTOE (847-705-4021), SCHAUMBURG, IL

## GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR SHALL NOTIFY THE VILLAGE DIRECTOR OF PUBLIC WORKS AND THE ENGINEER AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE VILLAGE DIRECTOR OF PUBLIC WORKS AND THE ENGINEER.
5. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, SIDEWALKS AND EXCAVATION FOR NEW SIDEWALKS ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
6. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
7. ACCESS TO DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING TEMPORARY AGGREGATE.
8. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO PAVEMENT RECONSTRUCTION, SIDEWALK REPLACEMENT, CURB AND GUTTER REPLACEMENT OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS AT LEAST 24 HOURS PRIOR TO PLANNED CLOSURE. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED. THE COST OF THIS WORK IS INCLUDED IN THE PAY ITEM CAUSING THE CLOSURE.
9. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
10. ALL SIGNS AND MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH VILLAGE STANDARDS AND IN ACCORDANCE WITH ARTICLE 107.20. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
11. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE COST OF THE APPROPRIATE REMOVAL PAY ITEM.
12. IN AREAS WHERE THE EXISTING DRIVEWAY (HOT-MIX ASPHALT OR PCC) OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS DRIVEWAY PAVEMENT REMOVAL OR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
13. RESERVED
14. ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
15. THE DAYS PAVING OPERATION SHALL RESULT IN A SINGLE TRANSVERSE JOINT. COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
16. THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTORS OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
17. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
18. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
19. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES PRIVATE DRIVEWAYS AND 8-INCHES WHERE THE SIDEWALK CROSSES COMMERCIAL DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4-INCH SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET. THIS WORK IS INCLUDED IN THE COST OF THE APPLICABLE PCC SIDEWALK PAY ITEM.
20. THE CONTRACTOR WILL ONLY BE ALLOWED TO REMOVE AND REPLACE CURB AND GUTTER ON ONE SIDE OF THE ROAD AT A TIME TO MINIMIZE CONGESTION. REPLACEMENT MUST BE COMPLETE ON ONE SIDE OF THE ROAD BEFORE THE CONTRACTOR IS ALLOWED TO BEGIN REMOVING CURB AND GUTTER ON THE OTHER SIDE OF THE ROAD.
21. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCHES ABOVE THE GUTTER FLAG.
22. A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT.
23. THE CONTRACTOR SHALL CONTACT THE LOCAL AGENCY MATERIAL INSPECTOR (SOIL AND MATERIAL CONSULTANTS - THOMAS JOHNSON - 847-870-0544) AT LEAST 48 HOURS PRIOR TO ANY CONCRETE OR HOT-MIX ASPHALT MATERIAL DELIVERIES.
24. A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION IN ACCORDANCE WITH ARTICLE 107.08.
25. THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN ON THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS SI CONCRETE TO AN ELEVATION 2-3/4" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS PLACED HIGHER THAN 2-3/4" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-3/4" DEPTH. THIS WORK IS INCLUDED IN THE COST OF THE APPLICABLE CURB AND GUTTER PAY ITEMS.
26. ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE EXISTING STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE IN ADDITION TO THE REQUIREMENTS FOR ANY STRUCTURE TO BE ADJUSTED AND SHALL BE PAID FOR ONCE AT THE CONTRACT UNIT PRICE FOR THE APPLICABLE PAY ITEM.
27. ALL FRAME AND LID CASTINGS LOCATED WITHIN THE PAVEMENT WHICH REQUIRE RESETTING TO FINISH GRADE SHALL BE BACKFILLED WITH CLASS PP-1 CONCRETE. HMA MATERIALS WILL NOT BE ALLOWED AS BACKFILL AROUND AN ADJUSTED CASTING. THIS WORK SHALL APPLY AT ALL CASTINGS ADJUSTED OR RECONSTRUCTED AS PART OF THIS CONTRACT, WHETHER PAID FOR SEPARATELY OR INCLUDED IN OTHER CONTRACT WORK. SEE SPECIAL PROVISION.
28. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND SHALL NOT EXCEED 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. A MAXIMUM GRADE DIFFERENCE OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H), AS DETERMINED BY THE ENGINEER.
29. THE DAYS MILLING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY MILLING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE MILLING OF THE ADJACENT LANE IN THE SAME DAY. A TEMPORARY RAMP SHALL BE INSTALLED AT THE SINGLE TRANSVERSE JOINT AT THE END OF EACH DAY. THE COST OF THIS WORK IS INCLUDED IN THE APPLICABLE HOT-MIX ASPHALT SURFACE REMOVAL PAY ITEM.
30. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS."
31. DURING CONSTRUCTION, THE CONTRACTOR WILL BE PERMITTED TO LIMIT ON-STREET PARKING IN ORDER TO COMPLETE CONSTRUCTION OPERATIONS. THE CONTRACTOR WILL BE REQUIRED TO COORDINATE WITH THE MUNICIPALITY A MINIMUM OF 48 HOURS IN ADVANCE. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PLACE ADVANCE SIGNS TO ALERT RESIDENTS AND COMMUTERS OF THE CONSTRUCTION WORK. THE PLACEMENT OF THESE SIGNS SHALL TAKE PLACE 48 HOURS IN ADVANCE IN ORDER TO ALLOW SUFFICIENT TIME FOR RESIDENTS AND GENERAL PUBLIC TO REVISE THEIR PARKING PATTERNS.
32. STREET NAME PLAQUES ARE LOCATED IN THE EXISTING SIDEWALK AT INTERSECTIONS. IT WILL BE THE CONTRACTORS RESPONSIBILITY TO PROTECT THE STREET NAME PLAQUES FROM DAMAGE AT ALL TIMES; NOTE THE EXISTING LOCATIONS AND ORIENTATIONS; REMOVE FROM THE EXISTING SIDEWALK; STORE APPROPRIATELY; THEN RE-INSTALL IN THE NEW SIDEWALK MATCHING THE PRE-CONSTRUCTION CONDITION. SHOULD DAMAGE OCCUR, THE CONTRACTOR SHALL REPLACE THE SIGNS AT THEIR OWN EXPENSE. THIS WORK IS INCLUDED IN THE SIDEWALK REMOVAL PAY ITEM.
33. INLET FILTERS SHALL BE CLEANED OF ALL SEDIMENT AND DEBRIS OR REPLACED AFTER EVERY 1/2" OR GREATER RAINFALL OR AS REQUIRED BY THE ENGINEER. COST INCLUDED IN PAY ITEM INLET FILTERS.
34. THE LOCATIONS OF CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER DURING CONSTRUCTION.
35. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE 24" X 60" NOMINAL PANEL WIDTH. THE PANEL SHALL BE A POLYMER COMPOSITE AND COMPLY WITH ADA REQUIREMENTS. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. THE PANEL COLOR SHALL BE SELECTED BY THE ENGINEER AS COORDINATED WITH THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
36. WORK AND MATERIALS REQUIRED TO INSTALL 1-INCH UNIT DUCT SHALL BE INCLUDED IN THE PAY ITEM DETECTOR LOOP REPLACEMENT.

## INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES
3 - 5	SUMMARY OF QUANTITIES
6 - 8	TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
9 - 11	PLAN AND PROFILE - COSSITT AVENUE
12	MISCELLANEOUS DETAILS (NOT USED)
13	DISTRICT 1 DETAIL - BD-08 FRAMES AND LIDS ADJUSTMENT WITH MILLING AND FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING
14	DISTRICT 1 DETAIL - BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
15	DISTRICT 1 DETAIL - BD-24 CURB AND CURB AND GUTTER REMOVAL AND REPLACEMENT
16	DISTRICT 1 DETAIL - BD-32 BUTT JOINTS AND HMA TAPER
17	DISTRICT 1 DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
18	DISTRICT 1 DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
19	DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN
20	DISTRICT 1 DETAIL - TS-05 DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS
21	DISTRICT 1 DETAIL - TS-07 DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING

## HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
363001-04	PCC BASE COURSE WITH HMA BINDER AND SURFACE COURSES
420001-08	PAVEMENT JOINTS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442101-07	CLASS B PATCHES
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS TYPE 1
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720021-02	SIGN PANELS EXTRUDED ALUMINUM TYPE
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
DRAWN - KAR	REVISED -
CHECKED - MWP	REVISED -
DATE - 08-12-14	FILE - 100261SHT-GenNotes.dgn

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

## INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	2
				CONTRACT NO. 61A84
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT M-40030801				

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 LICENSE NO. - 184-00121 - EXPIRES 4/30/2015  
 10/27/2014 10:05:05 AM  
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**BAXTER & WOODMAN**  
Consulting Engineers

**SUMMARY OF QUANTITIES**

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				RESURFACING	
				0005	STU
20200100	EARTH EXCAVATION	CU YD	21		21
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	81		81
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	637		637
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	15		15
20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	15		15
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	15		15
25200110	SODDING, SALT TOLERANT	SQ YD	637		637
25200200	SUPPLEMENTAL WATERING	UNIT	42		42
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	17		17
28000510	INLET FILTERS	EACH	31		31
# 30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	22		22
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	874		874
# 35300200	PORTLAND CEMENT CONCRETE BASE COURSE 7"	SQ YD	1,261		1,261
# 35800100	PREPARATION OF BASE	SQ YD	1,261		1,261
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	6,026		6,026

\* SPECIALTY ITEM  
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				RESURFACING	
				0005	STU
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	4		4
# 40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	464		464
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	182		182
# 40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	46		46
# 40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	168		168
# 40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	793		793
42001300	PROTECTIVE COAT	SQ YD	1,159		1,159
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	16		16
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	40		40
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	7,016		7,016
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	100		100
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	225		225
42400800	DETECTABLE WARNINGS	SQ FT	460		460
44000100	PAVEMENT REMOVAL	SQ YD	1,261		1,261
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	3,948		3,948

\* SPECIALTY ITEM  
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM  
 LICENSE NO. 184-00121 - EXPIRES 4/30/2015  
 ILLINOIS PROFESSIONAL DESIGNER REGISTRATION NO. 0224204  
 PROJECT: SURVEY DRAWINGS 0026 SH - 50



DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
DRYING - KAR	REVISED -
CHECKED - MWP	REVISED -
DATE - 08-12-14	FILE - 100261SHT-500.dgn

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES**

SCALE: NONE

STA.

STA.

F.A.U. RTE. 1365

CD-RS

DOCK

INSTR

CT

1A84

SHEET NO. 3

**SUMMARY OF QUANTITIES**

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE
				RESURFACING
				0005
				STU
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	1,294	1,294
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	56	56
44000600	SIDEWALK REMOVAL	SQ FT	7,429	7,429
# 44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	210	210
# 44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	35	35
44200911	CLASS B PATCHES, TYPE IV, 6 INCH	SQ YD	220	220
44200944	CLASS B PATCHES, TYPE IV, 8 INCH	SQ YD	30	30
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	30	30
44201682	CLASS D PATCHES, TYPE II, 3 INCH	SQ YD	56	56
44201683	CLASS D PATCHES, TYPE III, 3 INCH	SQ YD	68	68
44201684	CLASS D PATCHES, TYPE IV, 3 INCH	SQ YD	60	60
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	60	60
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	30	30
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	30	30
* # 56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	2	2

\* SPECIALTY ITEM  
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE
				RESURFACING
				0005
				STU
# 60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1
# 60266600	VALVE BOXES TO BE ADJUSTED	EACH	4	4
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	3	3
67100100	MOBILIZATION	L SUM	1	1
# 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
# 70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	8,148	8,148
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	907	907
72000100	SIGN PANEL - TYPE 1	SQ FT	252	252
72300100	INSTALL EXISTING SIGN PANEL	SQ FT	12	12
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	27	27
72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	12	12
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	615	615
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	80	80

\* SPECIALTY ITEM  
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

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DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
DRAWN - KAR	REVISED -
CHECKED - MWP	REVISED -
DATE - 08-12-14	FILE - 100261SHT-500.dgn

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: NONE STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	4
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-0031081			CONTRACT NO. 61A84	

**SUMMARY OF QUANTITIES**

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE
				RESURFACING
				0005
				STU
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,499	1,499
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,123	1,123
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	94	94
* 78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	120	120
* 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	108	108
* # 88600600	DETECTOR LOOP REPLACEMENT	FOOT	72	72
# Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,275	1,275
# Z0019600	DUST CONTROL WATERING	UNIT	16	16
# Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	26	26
# Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	234	234
# X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	3	3
# X4023000	TEMPORARY ACCESS (ROAD)	EACH	2	2
# X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	2,520	2,520
# X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	25	25

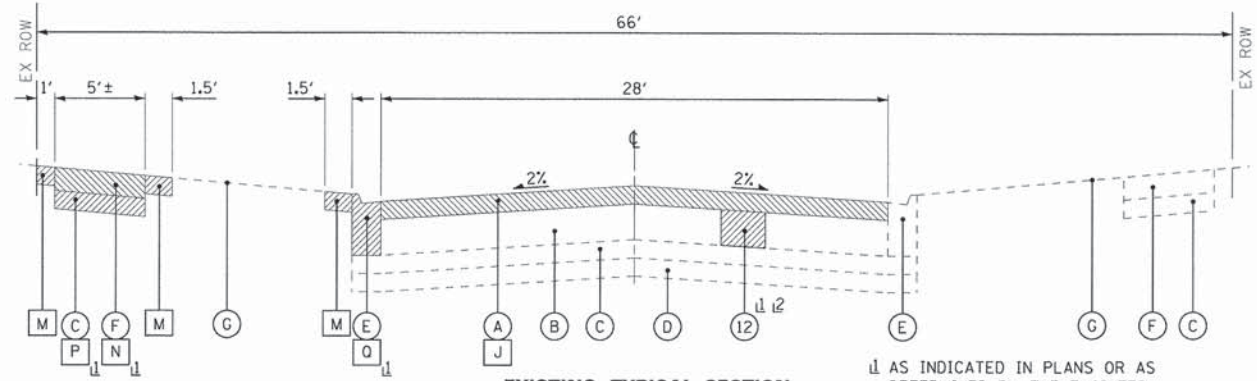
\* SPECIALTY ITEM  
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

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<b>BAXTER &amp; WOODMAN</b> Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
	DRAWN - KAR	REVISED -
	CHECKED - MWP	REVISED -
	DATE - 08-12-14	FILE - 100261SHT-500.dgn

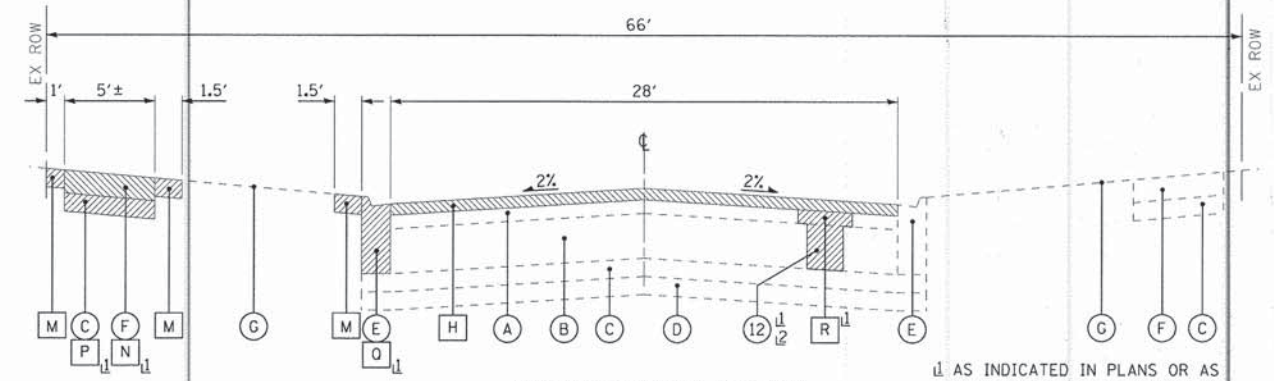
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>SUMMARY OF QUANTITIES</b>		F.A.U. RTE. 1365	SECTION 12-00086-00-RS	COUNTY COOK	TOTAL SHEET NO. 21	SHEET NO. 5
SCALE: NONE	STA. TO STA.	FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT M-40310811 CONTRACT NO. 61A84		



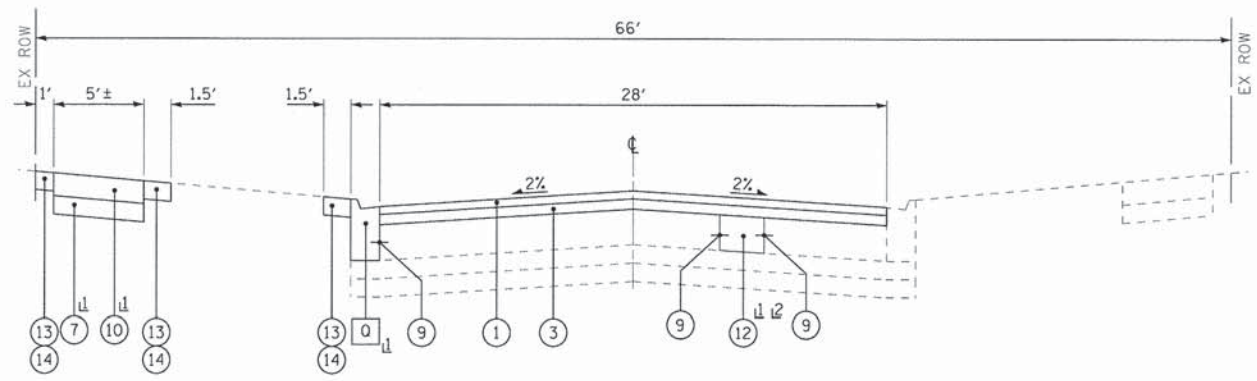
**EXISTING TYPICAL SECTION**  
STA 10+12 TO STA 13+90, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 8 INCH



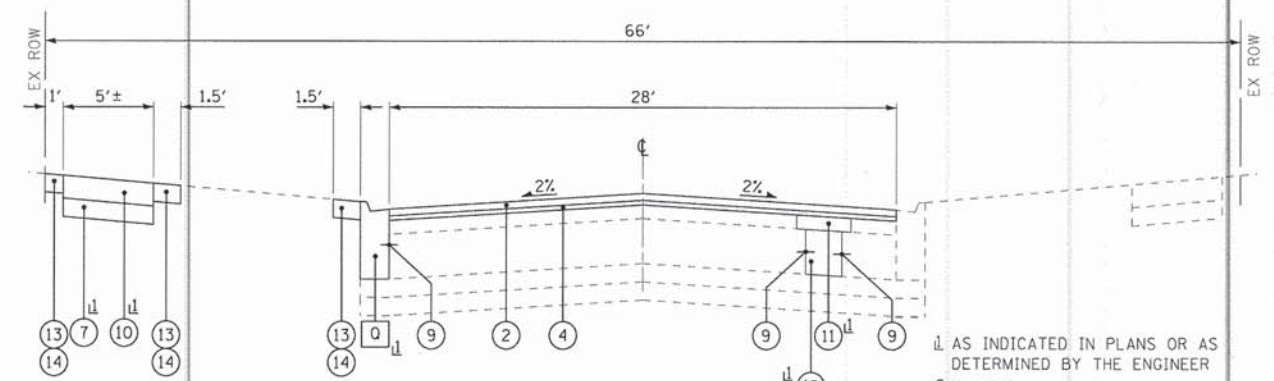
**EXISTING TYPICAL SECTION**  
STA 13+90 TO STA 17+79, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 6 INCH



**PROPOSED TYPICAL SECTION**  
STA 10+12 TO STA 13+90, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 8 INCH



**PROPOSED TYPICAL SECTION**  
STA 13+90 TO STA 17+79, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 6 INCH

**EXISTING LEGEND**

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT
- (B) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE
- (C) EXISTING AGGREGATE BASE COURSE
- (D) EXISTING SUB-GRADE
- (E) EXISTING COMBINATION CONCRETE CURB AND GUTTER
- (F) EXISTING SIDEWALK
- (G) EXISTING GROUND SURFACE
- (H) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (J) HOT-MIX ASPHALT PAVEMENT REMOVAL, 4"
- (K) PAVEMENT REMOVAL
- (L) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (UNDERCUT)\*\*
- (M) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)
- (N) SIDEWALK REMOVAL
- (P) AGGREGATE BASE COURSE REMOVAL (INCLUDED IN (N) PAY ITEM) (NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)
- (O) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (R) HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"
- (S) HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"
- (T) HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- (Hatched) ITEM TO BE REMOVED

**PROPOSED LEGEND**

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 3/4"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 2 1/4"
- (4) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- (5) PORTLAND CEMENT CONCRETE BASE COURSE, 7"
- (6) PREPARATION OF BASE
- (7) AGGREGATE BASE COURSE, TYPE B 4"
- (8) AGGREGATE SUB-GRADE IMPROVEMENT\*\*
- (9) STEEL REINFORCEMENT (INCLUDING DOWEL BARS) (INCLUDED IN (O) OR (S) OR (T) PAY ITEM) (NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)
- (10) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (11) HOT-MIX ASPHALT REPLACEMENT OVER PATCHES
- (12) CLASS B PATCHES
- (13) TOPSOIL FURNISH AND PLACE, 4"
- (14) SODDING
- (15) CLASS D PATCHES

**HMA MIXTURE REQUIREMENTS**

CONTRACTOR WILL MILL FIRST

MIXTURE TYPE	AIR VOIDS @ Ndes
<b>RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm); 1-1/2", 1-3/4"	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2-1/4"	4% @ 50 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 Gyr.
<b>PATCHING</b>	
CLASS D PATCHES (HMA BINDER, IL-19 mm, N50); 3" (1-LIFT), 6", 8" (2-LIFTS), 10" (3-LIFTS)	4% @ 70 Gyr.
HMA REPLACEMENT OVER PATCHES (HMA BINDER, IL-19 mm, N50); 3", 4"	4% @ 70 Gyr.

\*\* AGGREGATE SUBGRADE IMPROVEMENT (ASI) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS.

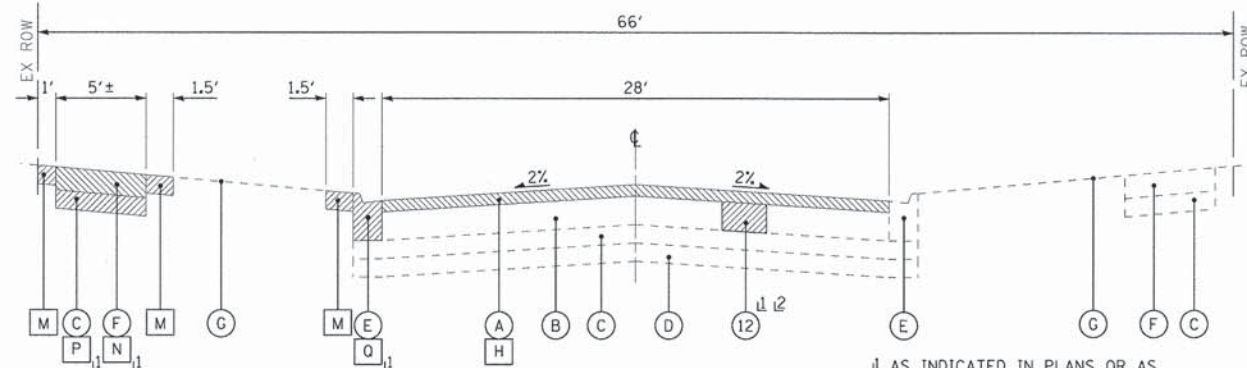
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 PROJECT NO. - 12-00086-00-RS  
 DRAWING NO. - 100261SHT-TypSec.dgn  
 DATE - 08-12-14  
 FILE - 100261SHT-TypSec.dgn

<b>BAXTER &amp; WOODMAN</b> Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
	DRAWN - KAR	REVISED -
	CHECKED - MWP	REVISED -
	DATE - 08-12-14	FILE - 100261SHT-TypSec.dgn

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

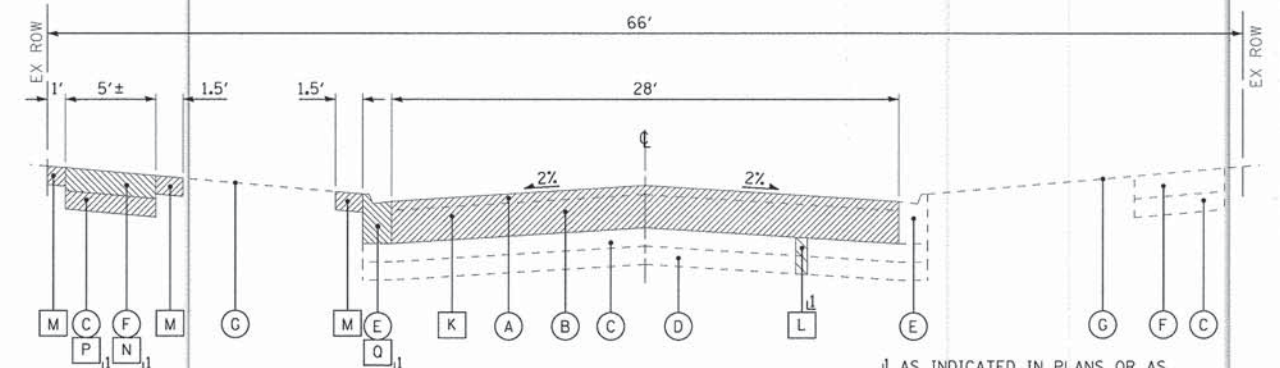
**TYPICAL SECTIONS AND HMA MIXTURE REQUIREMENTS**

SCALE: NONE	STA. TO STA.	F.A.U. RTE. 1365	SECTION 12-00086-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 6
				CONTRACT NO. 61A84		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT M-4003081						



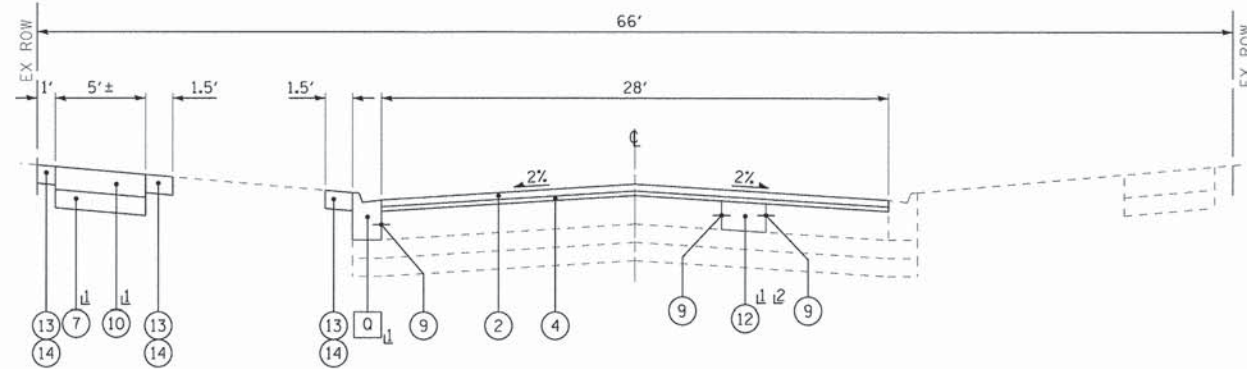
**EXISTING TYPICAL SECTION**  
STA 17+79 TO STA 21+80, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 6 INCH



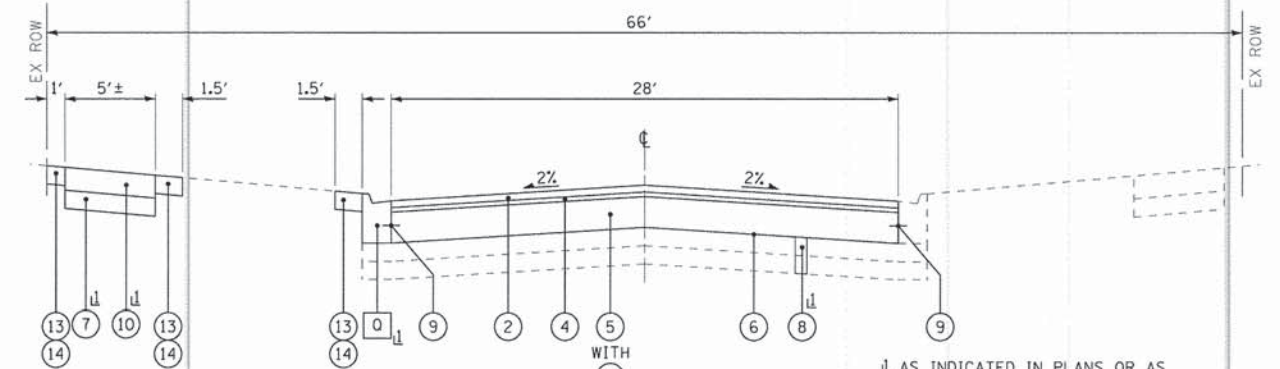
**EXISTING TYPICAL SECTION**  
STA 21+80 TO STA 25+50, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER



**PROPOSED TYPICAL SECTION**  
STA 17+79 TO STA 21+80, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 6 INCH



**PROPOSED TYPICAL SECTION**  
STA 21+80 TO STA 25+50, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER

**EXISTING LEGEND**

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT
- (B) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE
- (C) EXISTING AGGREGATE BASE COURSE
- (D) EXISTING SUB-GRADE
- (E) EXISTING COMBINATION CONCRETE CURB AND GUTTER
- (F) EXISTING SIDEWALK
- (G) EXISTING GROUND SURFACE
- (H) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (J) HOT-MIX ASPHALT PAVEMENT REMOVAL, 4"
- (K) PAVEMENT REMOVAL
- (L) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (UNDERCUT)\*\*
- (M) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)
- (N) SIDEWALK REMOVAL
- (P) AGGREGATE BASE COURSE REMOVAL (INCLUDED IN (N) PAY ITEM) (NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)
- (Q) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (R) HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"
- (S) HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"
- (T) HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH ITEM TO BE REMOVED

**PROPOSED LEGEND**

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 3/4"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 2 1/4"
- (4) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- (5) PORTLAND CEMENT CONCRETE BASE COURSE, 7"
- (6) PREPARATION OF BASE
- (7) AGGREGATE BASE COURSE, TYPE B 4"
- (8) AGGREGATE SUB-GRADE IMPROVEMENT\*\*
- (9) STEEL REINFORCEMENT (INCLUDING DWEL BARS) (INCLUDED IN (Q) OR (S) OR (T) PAY ITEM) (NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)
- (10) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (11) HOT-MIX ASPHALT REPLACEMENT OVER PATCHES
- (12) CLASS B PATCHES
- (13) TOPSOIL FURNISH AND PLACE, 4"
- (14) SODDING
- (15) CLASS D PATCHES

\*\* AGGREGATE SUBGRADE IMPROVEMENT (ASI) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

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**BAXTER & WOODMAN**  
Consulting Engineers

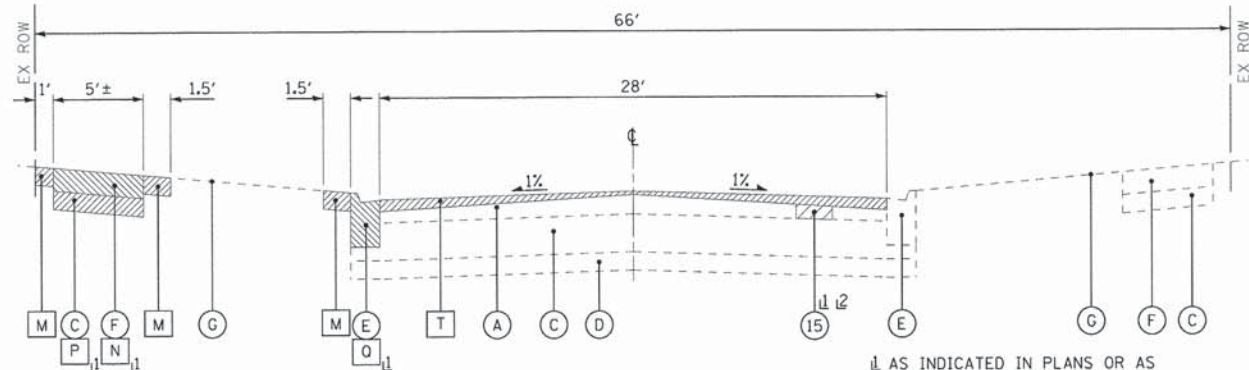
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DATE - 08-12-14	FILE - 100261SHT-TypSec.dgn

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS AND HMA MIXTURE REQUIREMENTS**

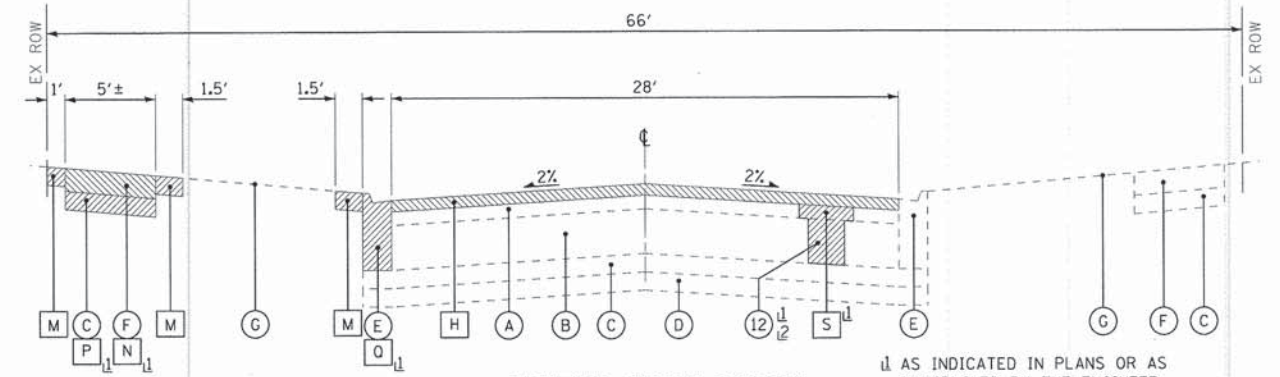
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	7
CONTRACT NO. 61A84				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT M-40031081				



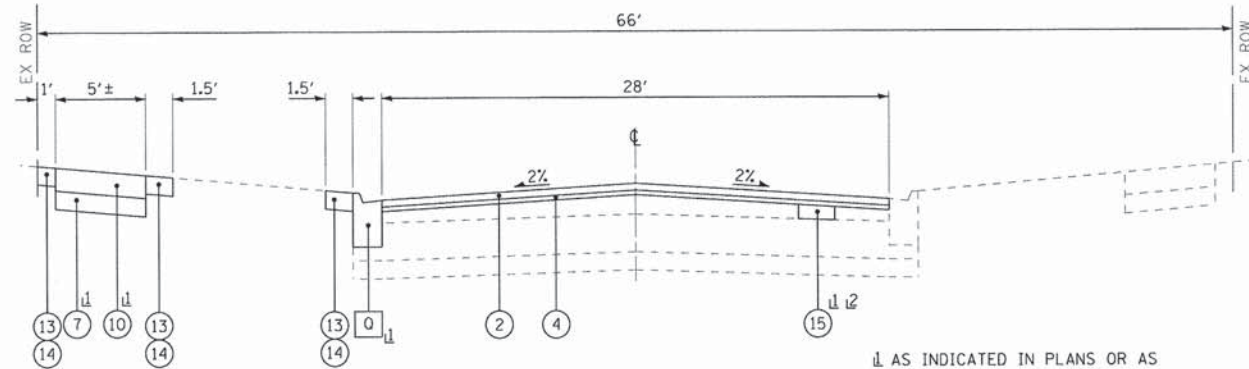
**EXISTING TYPICAL SECTION**  
STA 25+50 TO STA 32+83, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 3 INCH



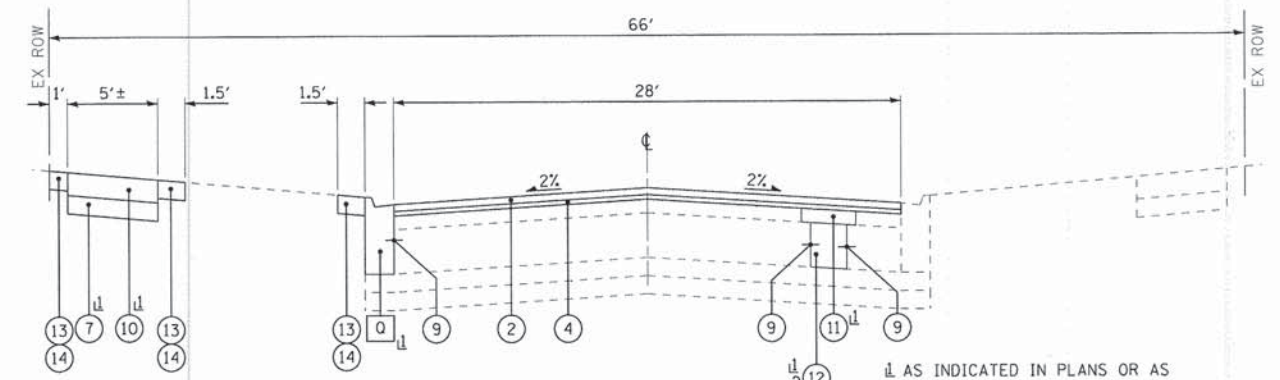
**EXISTING TYPICAL SECTION**  
STA 32+83 TO STA 36+56, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 10 INCH



**PROPOSED TYPICAL SECTION**  
STA 25+50 TO STA 32+83, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 3 INCH



**PROPOSED TYPICAL SECTION**  
STA 32+83 TO STA 36+56, COSSITT AVENUE

↓ AS INDICATED IN PLANS OR AS DETERMINED BY THE ENGINEER  
⑫ 10 INCH

**EXISTING LEGEND**

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT
- (B) EXISTING PORTLAND CEMENT CONCRETE BASE COURSE
- (C) EXISTING AGGREGATE BASE COURSE
- (D) EXISTING SUB-GRADE
- (E) EXISTING COMBINATION CONCRETE CURB AND GUTTER
- (F) EXISTING SIDEWALK
- (G) EXISTING GROUND SURFACE
- (H) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (J) HOT-MIX ASPHALT PAVEMENT REMOVAL, 4"
- (K) PAVEMENT REMOVAL
- (L) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (UNDERCUT)\*\*
- (M) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)
- (N) SIDEWALK REMOVAL
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- (S) HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"
- (T) HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- (Hatched) ITEM TO BE REMOVED

**PROPOSED LEGEND**

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 3/4"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
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- (6) PREPARATION OF BASE
- (7) AGGREGATE BASE COURSE, TYPE B 4"
- (8) AGGREGATE SUB-GRADE IMPROVEMENT\*\*
- (9) STEEL REINFORCEMENT (INCLUDING DOWEL BARS) (INCLUDED IN (O) OR (S) OR (T) PAY ITEM) (NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)
- (10) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (11) HOT-MIX ASPHALT REPLACEMENT OVER PATCHES
- (12) CLASS B PATCHES
- (13) TOPSOIL FURNISH AND PLACE, 4"
- (14) SODDING
- (15) CLASS D PATCHES

\*\* AGGREGATE SUBGRADE IMPROVEMENT (ASI) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

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**BAXTER & WOODMAN**  
Consulting Engineers

DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
DRAWN - KAR	REVISED -
CHECKED - MWP	REVISED -
DATE - 08-12-14	FILE - 100261SHT-TypSec.dgn

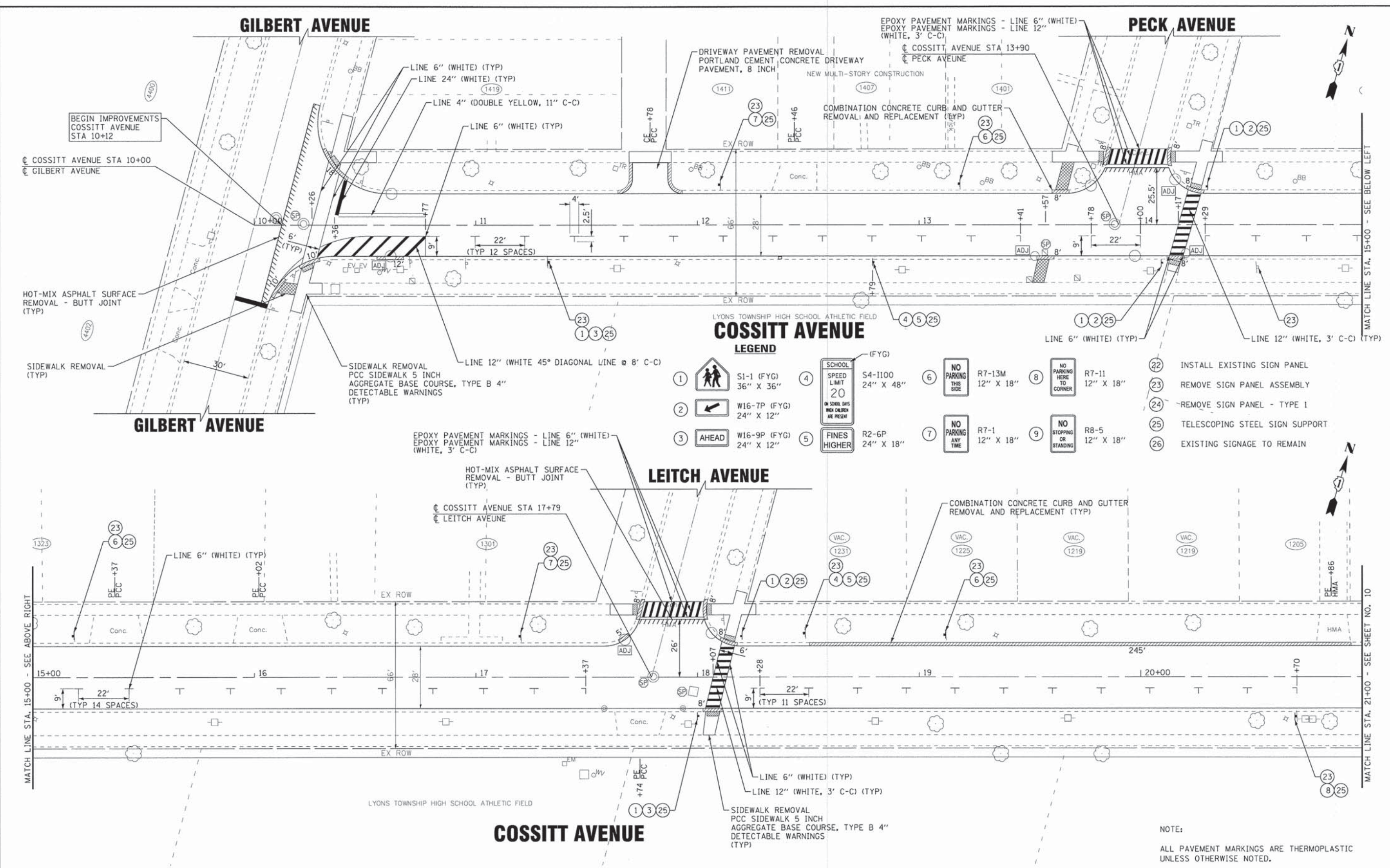
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS AND HMA MIXTURE REQUIREMENTS**

SCALE: NONE STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	8
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-40030811			CONTRACT NO. 61A84	





BEGIN IMPROVEMENTS  
COSSITT AVENUE  
STA 10+12

☐ COSSITT AVENUE STA 10+00  
☐ GILBERT AVENUE

HOT-MIX ASPHALT SURFACE  
REMOVAL - BUTT JOINT  
(TYP)

SIDEWALK REMOVAL  
(TYP)

GILBERT AVENUE

SIDEWALK REMOVAL  
PCC SIDEWALK 5 INCH  
AGGREGATE BASE COURSE, TYPE B 4"  
DETECTABLE WARNINGS  
(TYP)

EPOXY PAVEMENT MARKINGS - LINE 6" (WHITE)  
EPOXY PAVEMENT MARKINGS - LINE 12" (WHITE, 3' C-C)

LEITCH AVENUE

HOT-MIX ASPHALT SURFACE  
REMOVAL - BUTT JOINT  
(TYP)

☐ COSSITT AVENUE STA 17+79  
☐ LEITCH AVENUE

COMBINATION CONCRETE CURB AND GUTTER  
REMOVAL AND REPLACEMENT (TYP)

MATCH LINE STA. 15+00 - SEE ABOVE RIGHT

15+00

22'  
(TYP 14 SPACES)

16

66'

28'

37'

17

18

22'

(TYP 11 SPACES)

19

20+00

70'

245'

21+00

MATCH LINE STA. 21+00 - SEE SHEET NO. 10

COSSITT AVENUE

SIDEWALK REMOVAL  
PCC SIDEWALK 5 INCH  
AGGREGATE BASE COURSE, TYPE B 4"  
DETECTABLE WARNINGS  
(TYP)

NOTE:  
ALL PAVEMENT MARKINGS ARE THERMOPLASTIC  
UNLESS OTHERWISE NOTED.

COSSITT AVENUE

LEGEND

- ① S1-1 (FYG) 36" X 36"
- ② W16-7P (FYG) 24" X 12"
- ③ AHEAD W16-9P (FYG) 24" X 12"
- ④ S4-1100 (FYG) 24" X 48"
- ⑤ R2-6P (FYG) 24" X 18"
- ⑥ NO PARKING THIS SIDE R7-13M 12" X 18"
- ⑦ NO PARKING ANY TIME R7-1 12" X 18"
- ⑧ NO PARKING HERE TO CORNER R7-11 12" X 18"
- ⑨ NO STOPPING OR STANDING R8-5 12" X 18"
- ⑩ INSTALL EXISTING SIGN PANEL
- ⑪ REMOVE SIGN PANEL ASSEMBLY
- ⑫ REMOVE SIGN PANEL - TYPE 1
- ⑬ TELESCOPING STEEL SIGN SUPPORT
- ⑭ EXISTING SIGNAGE TO REMAIN

**BAXTER & WOODMAN**  
Consulting Engineers

DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
DRAWN - KAR	REVISED -
CHECKED - MWP	REVISED -
DATE - 08-12-14	FILE - 100261SHT-Plan1.dgn

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

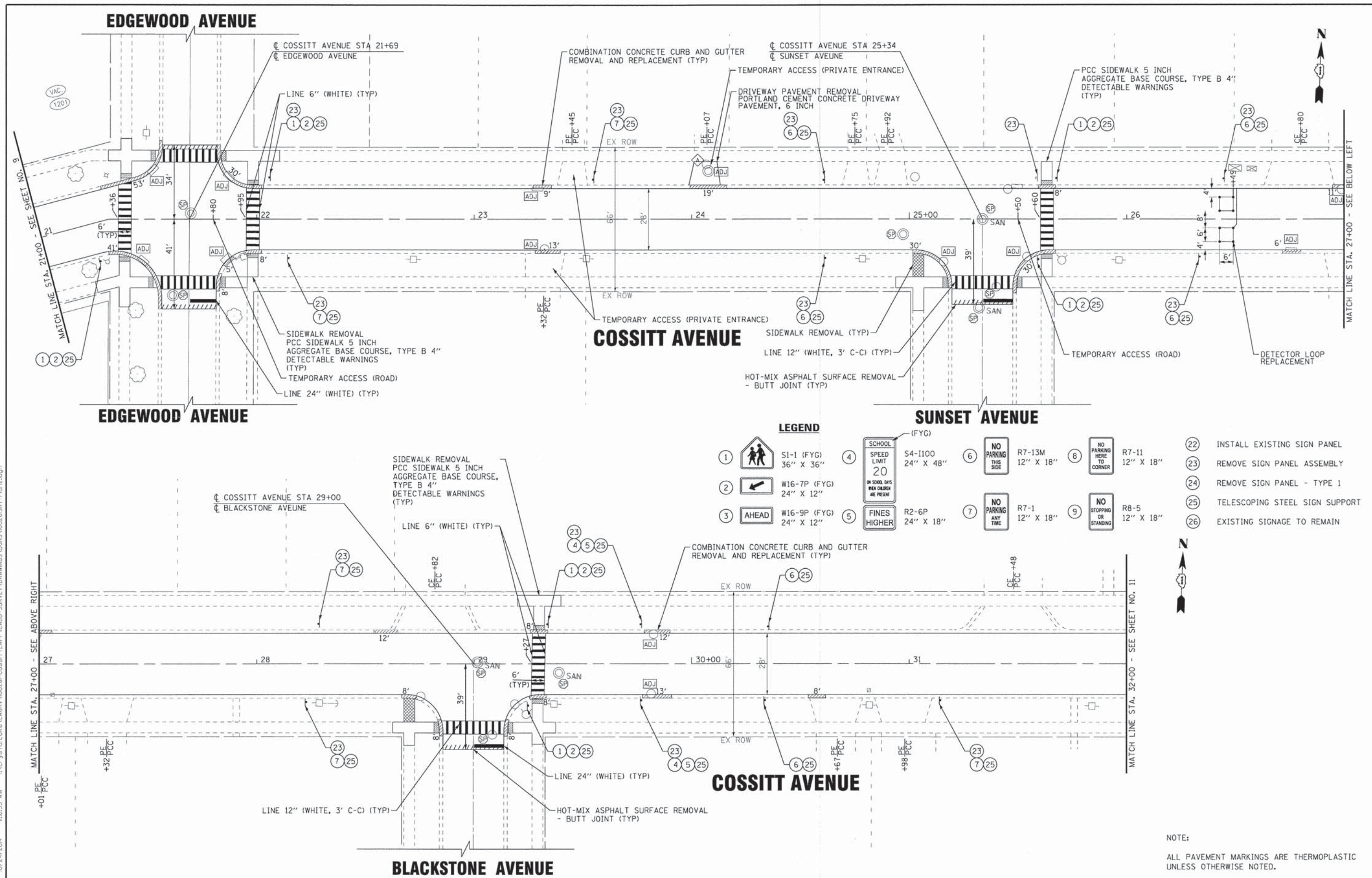
ROADWAY PLAN -  
COSSITT AVENUE

SCALE: 1" = 20'

STA. 10+00 TO STA. 21+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	9
CONTRACT NO. 61A84				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-4003081				

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**COSSITT AVENUE**

**SUNSET AVENUE**

**BLACKSTONE AVENUE**

**LEGEND**

- 1 S1-1 (FYG) 36" X 36"
- 2 W16-7P (FYG) 24" X 12"
- 3 W16-9P (FYG) 24" X 12"
- 4 S4-1100 (FYG) 24" X 48"
- 5 R2-6P 24" X 18"
- 6 R7-13M 12" X 18"
- 7 R7-1 12" X 18"
- 8 R7-11 12" X 18"
- 9 R8-5 12" X 18"
- 22 INSTALL EXISTING SIGN PANEL
- 23 REMOVE SIGN PANEL ASSEMBLY
- 24 REMOVE SIGN PANEL - TYPE 1
- 25 TELESCOPING STEEL SIGN SUPPORT
- 26 EXISTING SIGNAGE TO REMAIN

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<b>BAXTER &amp; WOODMAN</b> Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
	DRAWN - KAR	REVISED -
	CHECKED - MWP	REVISED -
	DATE - 08-12-14	FILE - 100261SHT-Plan2.dgn

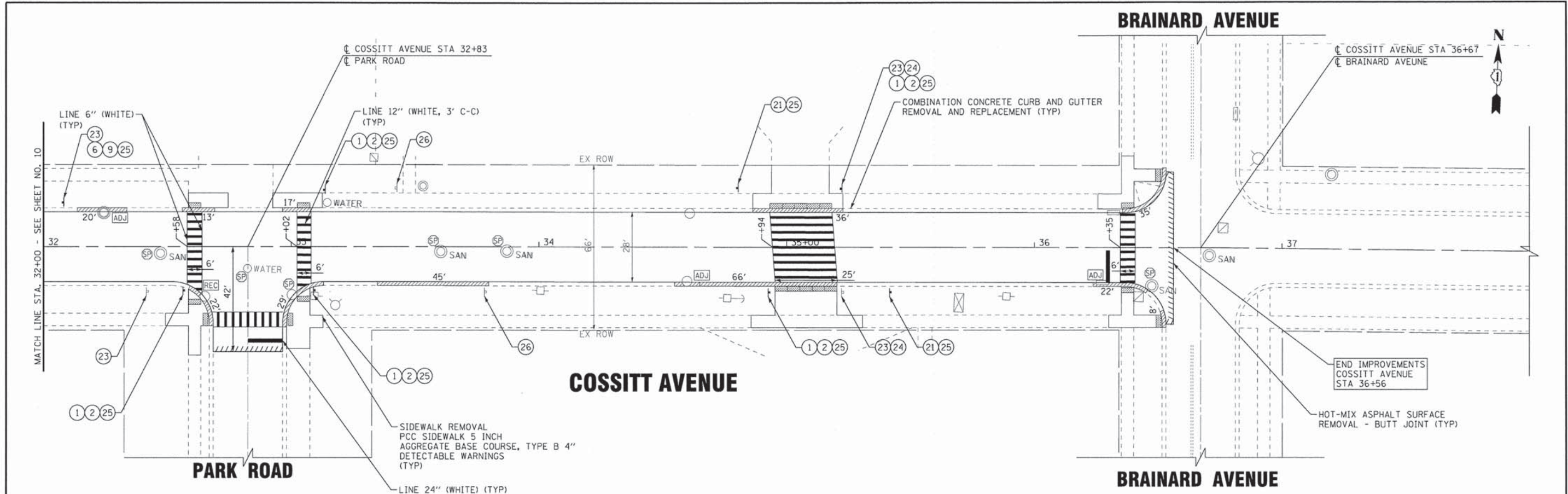
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN -  
COSSITT AVENUE**

SCALE: 1" = 20'      STA. 21+00 TO STA. 32+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	10
CONTRACT NO. 61A84				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT M-4003081				

**NOTE:**  
ALL PAVEMENT MARKINGS ARE THERMOPLASTIC UNLESS OTHERWISE NOTED.



- LEGEND**
- ① SI-1 (FYG) 36" X 36"
  - ② W16-7P (FYG) 24" X 12"
  - ③ W16-9P (FYG) 24" X 12"
  - ④ S4-1100 (FYG) 24" X 48"
  - ⑤ R2-6P 24" X 18"
  - ⑥ R7-13M 12" X 18"
  - ⑦ R7-1 12" X 18"
  - ⑧ R7-11 12" X 18"
  - ⑨ R8-5 12" X 18"
  - ⑩ 22 INSTALL EXISTING SIGN PANEL
  - ⑪ 23 REMOVE SIGN PANEL ASSEMBLY
  - ⑫ 24 REMOVE SIGN PANEL - TYPE 1
  - ⑬ 25 TELESCOPING STEEL SIGN SUPPORT
  - ⑭ 26 EXISTING SIGNAGE TO REMAIN

NOTE:  
ALL PAVEMENT MARKINGS ARE THERMOPLASTIC UNLESS OTHERWISE NOTED.

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<b>BAXTER &amp; WOODMAN</b> Consulting Engineers	DESIGNED - MWP	REVISED - IDOT REVIEW 10-27-14
	DRAWN - KAR	REVISED -
	CHECKED - MWP	REVISED -
	DATE - 08-12-14	FILE - 100261SHT-Plan3.dgn

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY PLAN - COSSITT AVENUE</b>	
SCALE: 1" = 20'	STA. 32+00 TO STA. 38+00

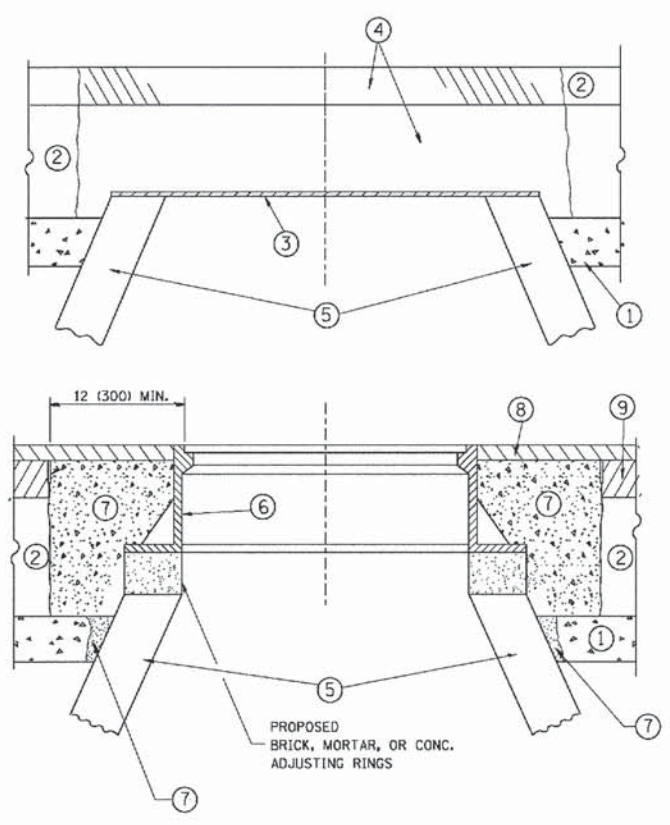
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	11
CONTRACT NO. 61A84				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT M-40030811				

**THIS SHEET INTENTIONALLY BLANK**

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	CHECKED - MWP	REVISED -			CONTRACT NO. 61A84				
	DATE - 08-12-14	FILE - 100261SHT-Details.dgn			SCALE: NONE	STA.	TO STA.	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT M-4003081	

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**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

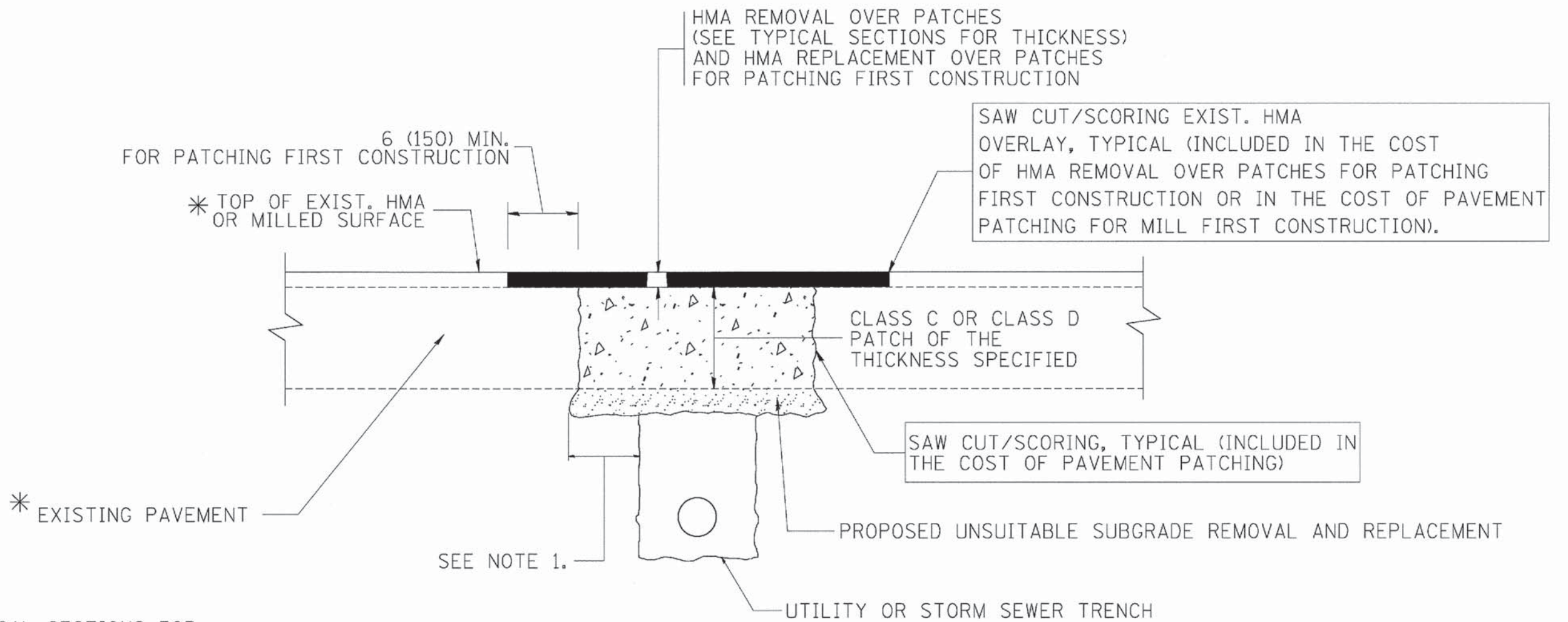
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE. 1365	SECTION 12-00086-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 13
<b>BD600-03 (BD-8)</b>				CONTRACT NO. 61AB4
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT M-4003(OB1)				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

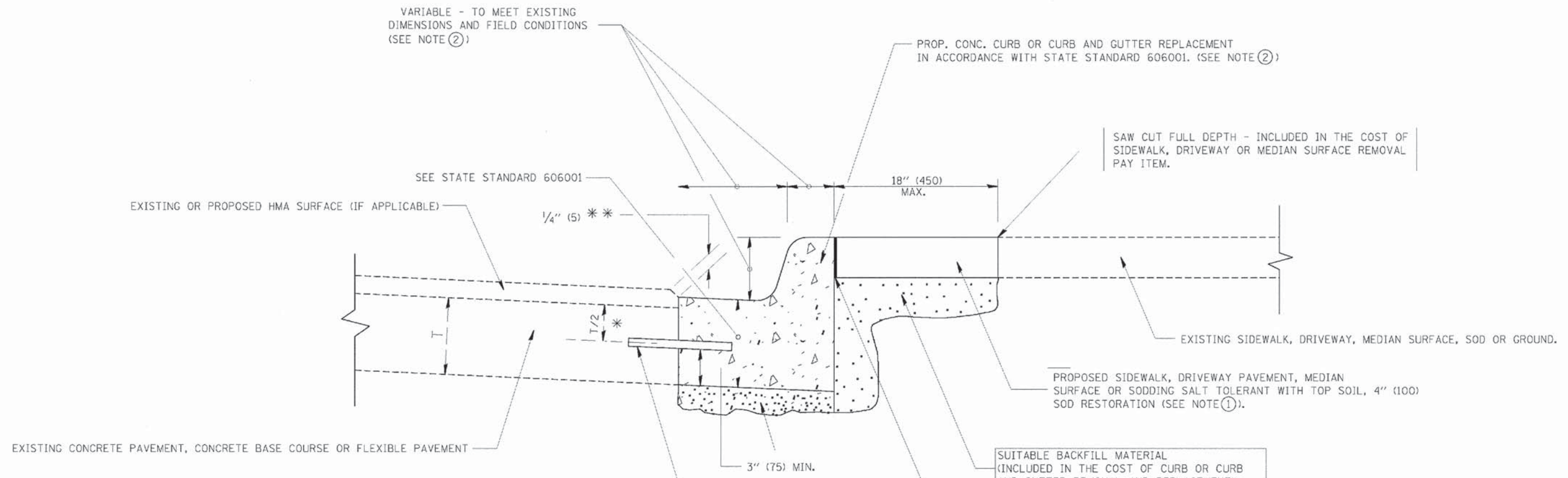
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 CHECKED -  
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 REVISED - A. ABBAS 04-27-98  
 REVISED - R. BORO 01-01-07  
 REVISED - R. BORO 09-04-07  
 REVISED - K. ENG 10-27-08

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PLOT DATE = 10/27/2008		DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	14
BD400-04 (BD-22)			CONTRACT NO. 61A84	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT M-4003(081)				



\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.  
 \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

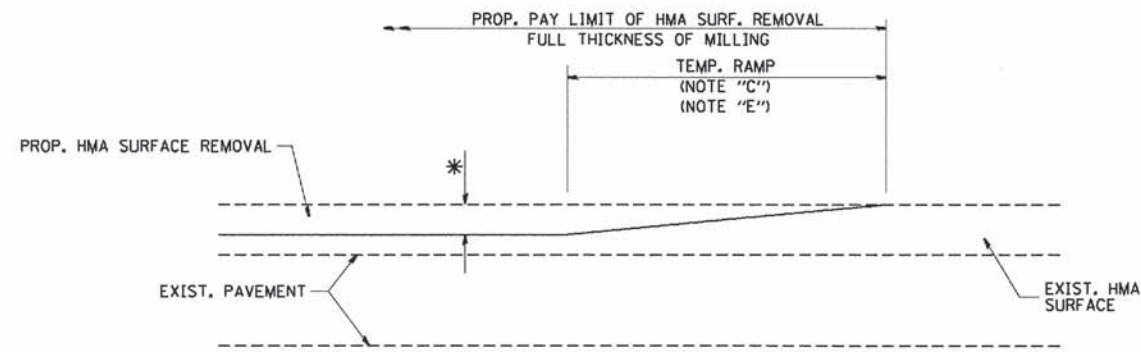
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
 SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

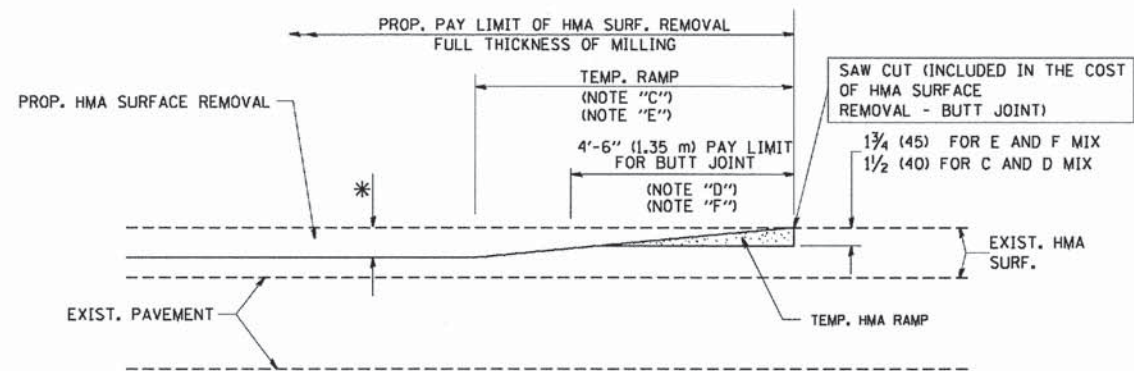
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 DATE: 03-11-94  
 REVISIONS: R. BORO 12-15-09  
 R. SHAH 10-03-96  
 A. ABRAS 03-21-97  
 LICENSE NO. 284-00021 - EXPIRES 7/30/2005  
 11/24/2004

DESIGNED	A. HOUSEH	REVISED	R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRAWN	-	REVISED	- A. ABRAS 03-21-97			1365	12-00086-00-RS	COOK	21	15	
CHECKED	-	REVISED	- M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>					
DATE	- 03-11-94	REVISED	- R. BORO 12-15-09			CONTRACT NO. 61A84					
SCALE: NONE						SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003(081)	



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

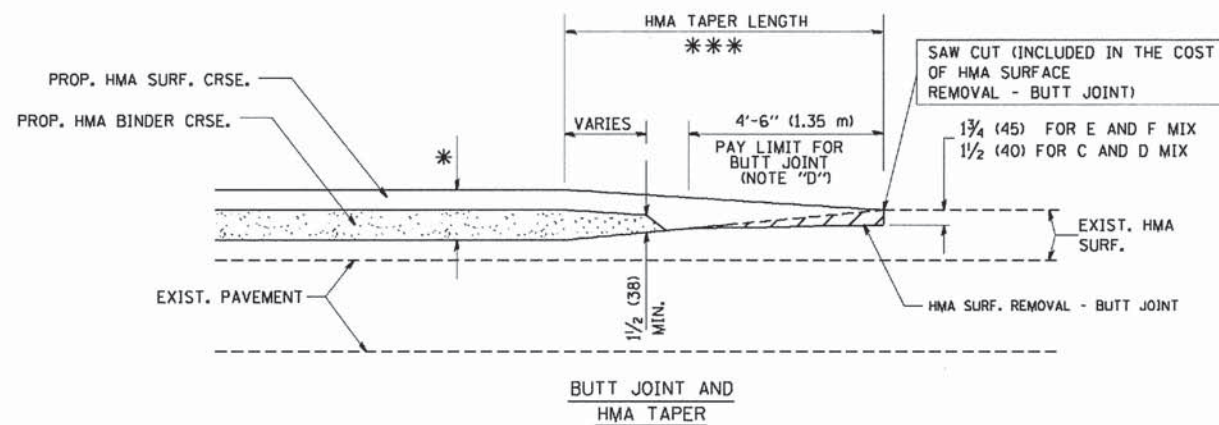
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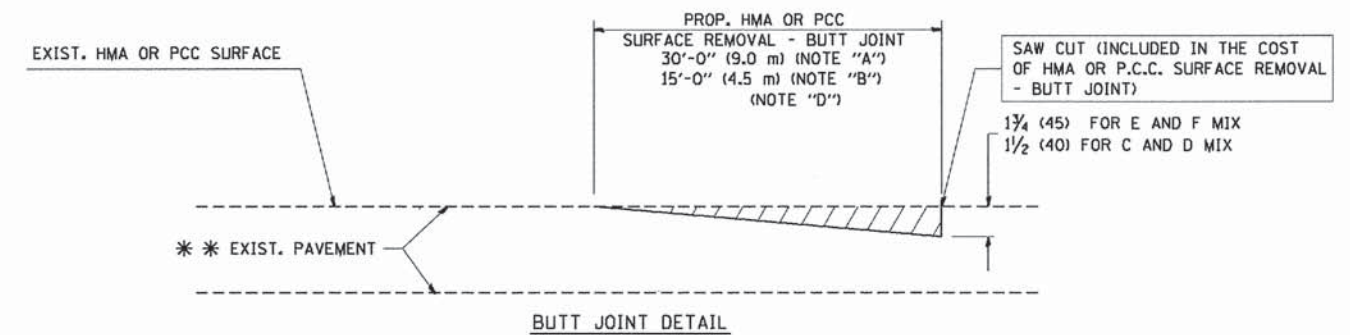
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

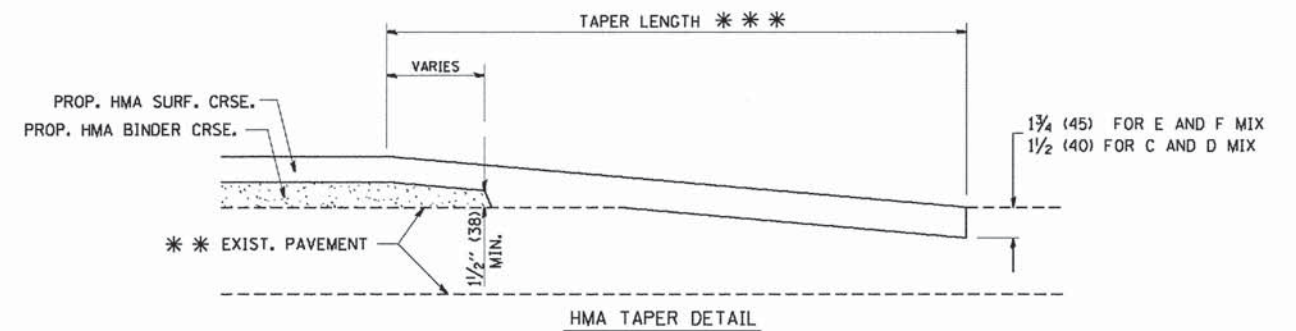
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

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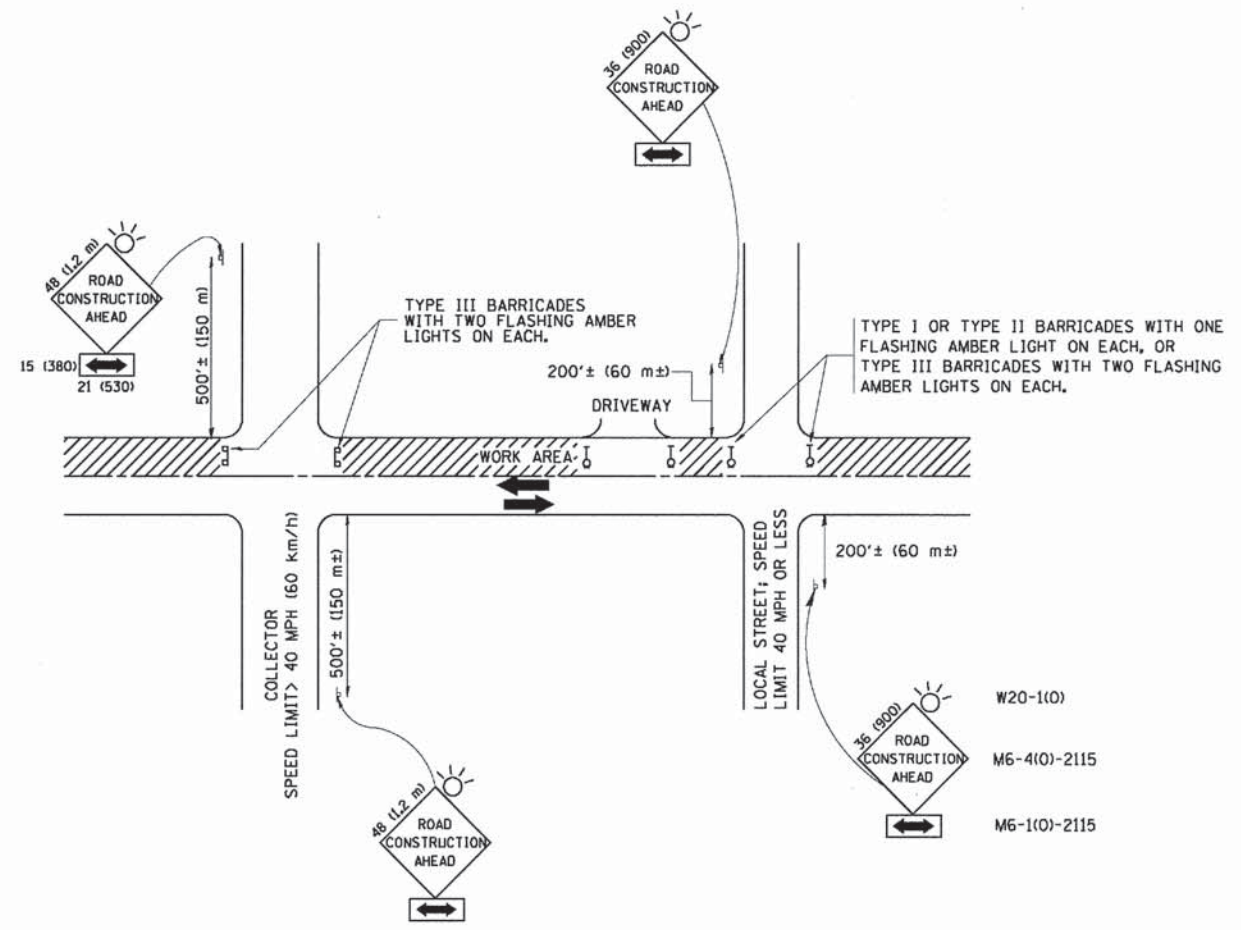
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	16
BD400-05 BD32			CONTRACT NO. 61A84	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(081)				



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 01/28/2004 10:16:16 AM



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

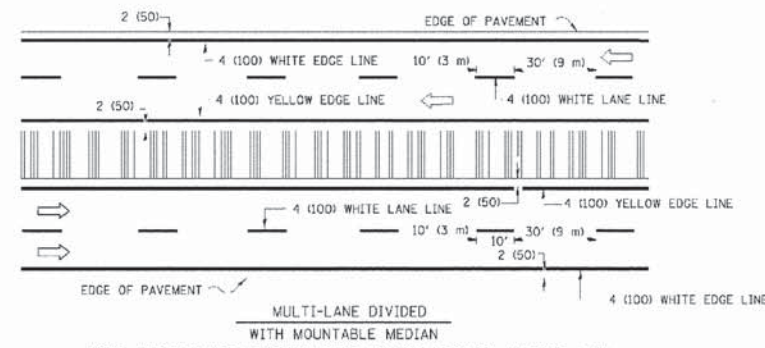
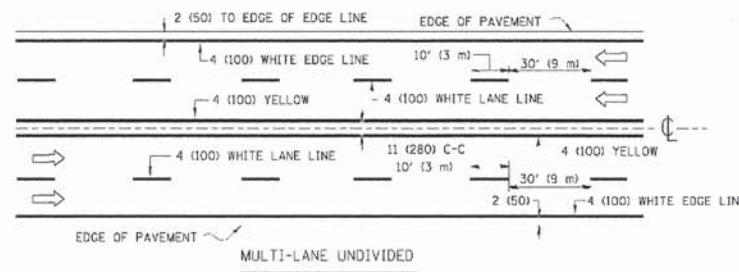
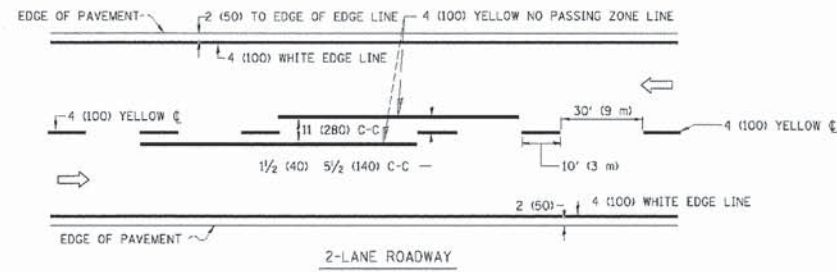
All dimensions are in millimeters (inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 58.0000 1/ IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-09	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

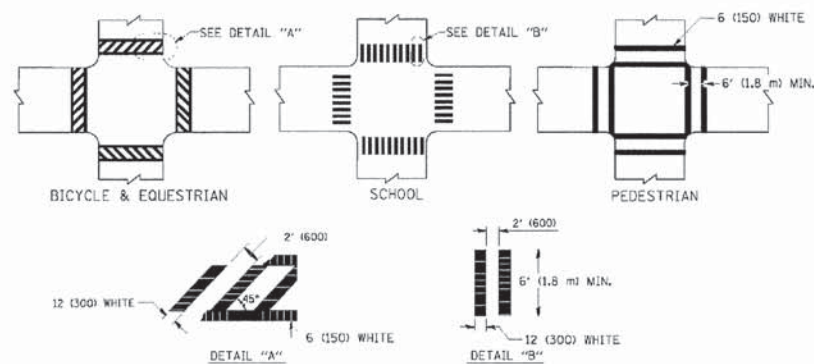
<b>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	17
<b>TC-10</b>			<b>CONTRACT NO. 61A84</b>	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003(081)				

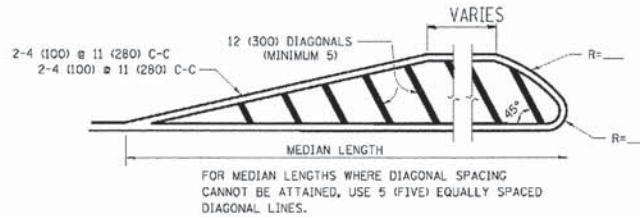
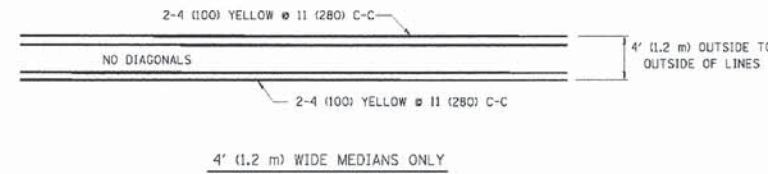


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

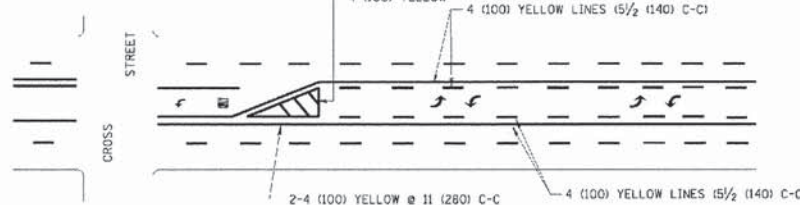


TYPICAL CROSSWALK MARKING

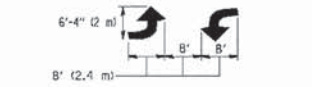


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

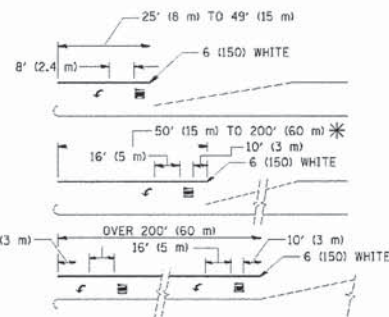
TYPICAL PAINTED MEDIAN MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

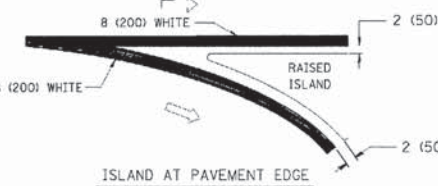
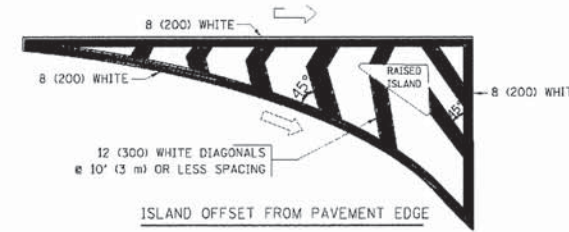


TYPICAL TURN LANE MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY"

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2' (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 6 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2' (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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 5606040  
 07/24/2014 BRW:ZS AM

DESIGNED	EVERS	REVISED	T. RAMMACHER 10-27-94
DRAWN	-	REVISED	-C. JUCIUS 09-09-09
CHECKED	-	REVISED	-
DATE	03-19-90	REVISED	-

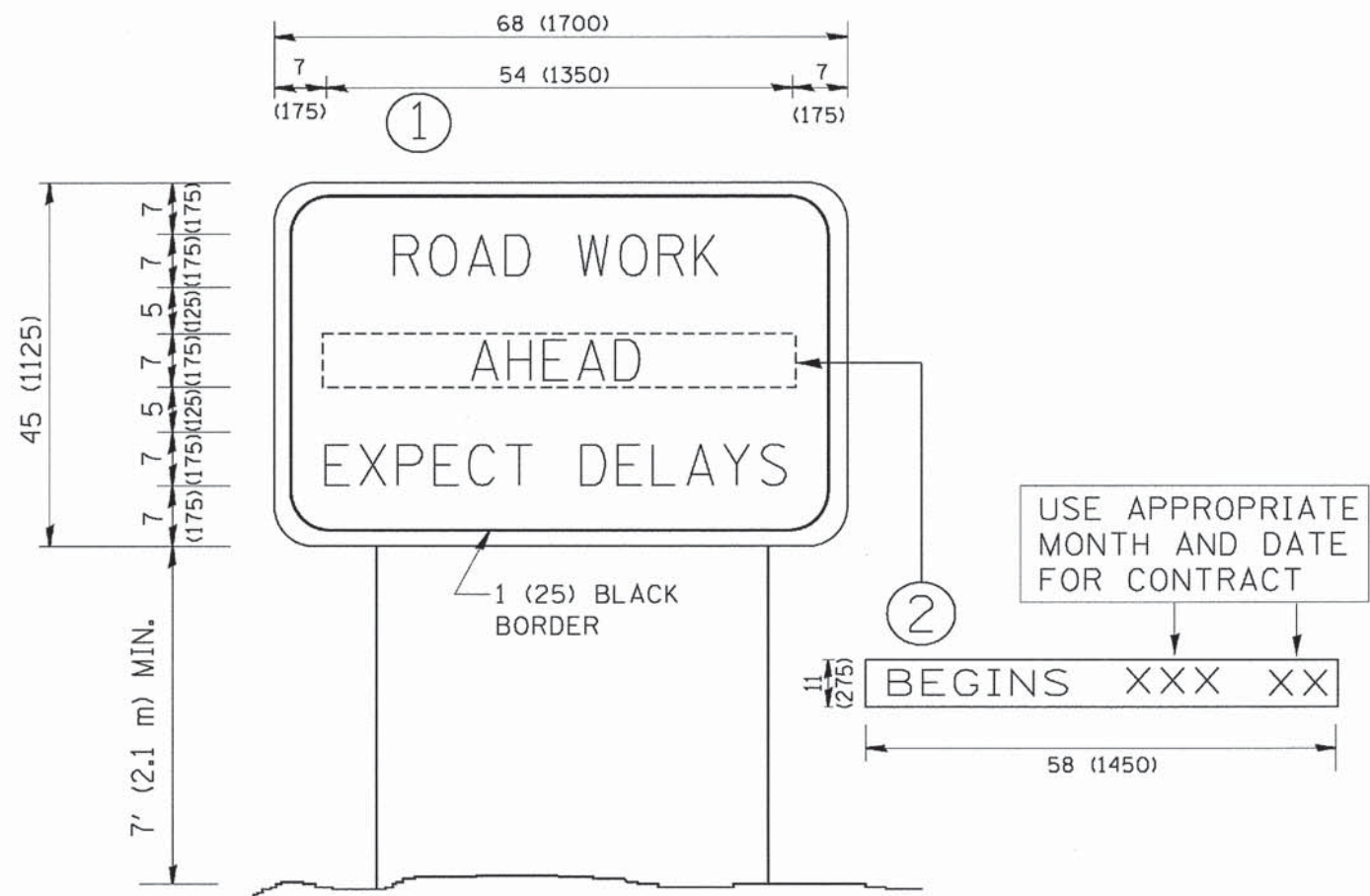
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1365	12-00086-00-RS	COOK	21	18
TC-13			CONTRACT NO. 61A84	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003(081)				

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 LICENSE NO. 84-00021 - EXPIRES 7/30/2005  
 56060-0  
 FILE NAME = W:\disto\22x34\to22.dgn  
 USER NAME = geglenabt  
 DESIGNED - R. MIRS 09-15-97  
 DRAWN - R. MIRS 12-11-97  
 CHECKED - T. RAMMACHER 02-02-99  
 DATE - C. JUCIUS 01-31-07  
 REVISIONS:  
 1. REVISED - R. MIRS 09-15-97  
 2. REVISED - R. MIRS 12-11-97  
 3. REVISED - T. RAMMACHER 02-02-99  
 4. REVISED - C. JUCIUS 01-31-07



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

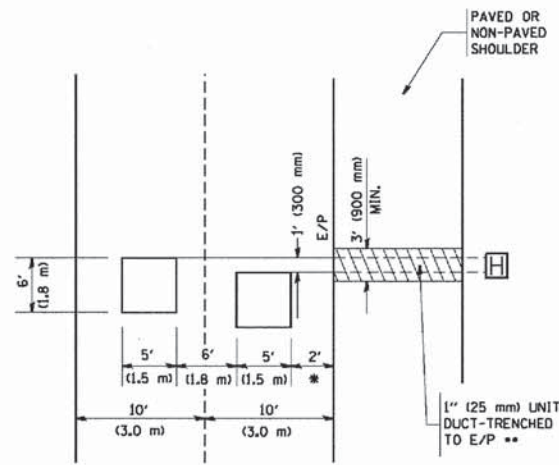
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\disto\22x34\to22.dgn	USER NAME = geglenabt	DESIGNED - R. MIRS 09-15-97	REVISIONS: 1. REVISED - R. MIRS 09-15-97 2. REVISED - R. MIRS 12-11-97 3. REVISED - T. RAMMACHER 02-02-99 4. REVISED - C. JUCIUS 01-31-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.U. RTE. 1365	SECTION 12-00086-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 19
PLOT SCALE = 5/8" = 1' IN.		PLOT DATE = 1/4/2008		TC-22 CONTRACT NO. 61A84		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003(081)							



**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

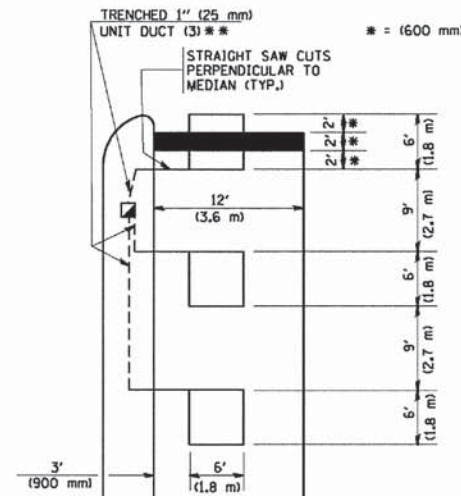


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



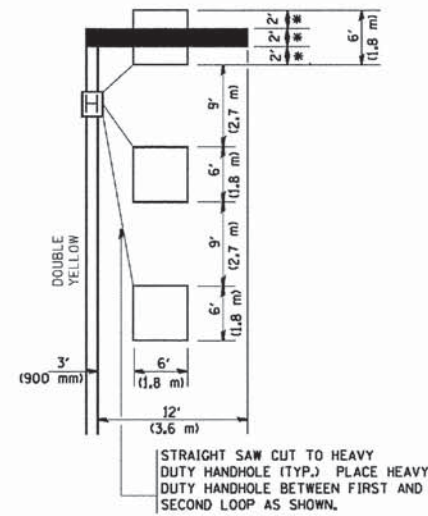
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

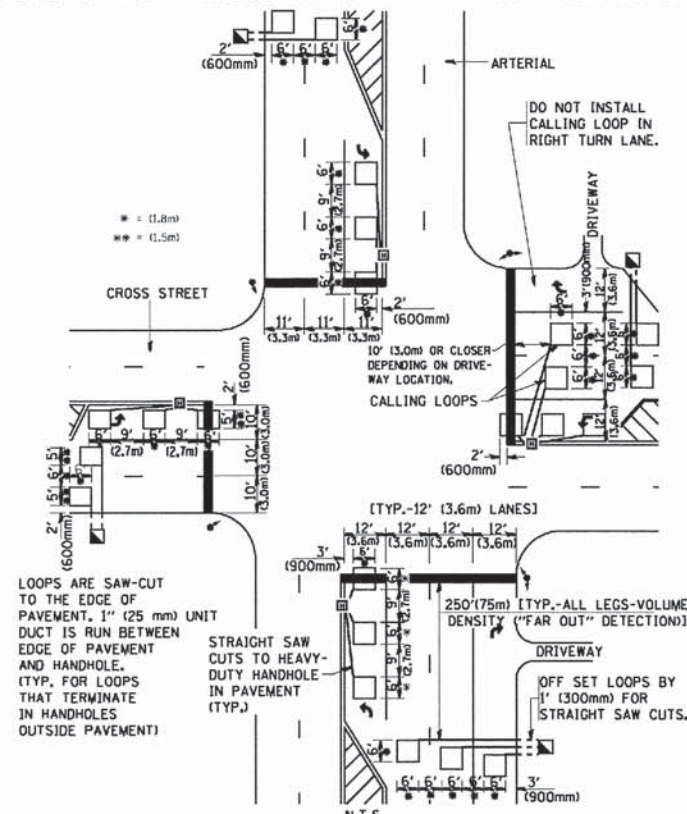
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

\* = (600 mm)



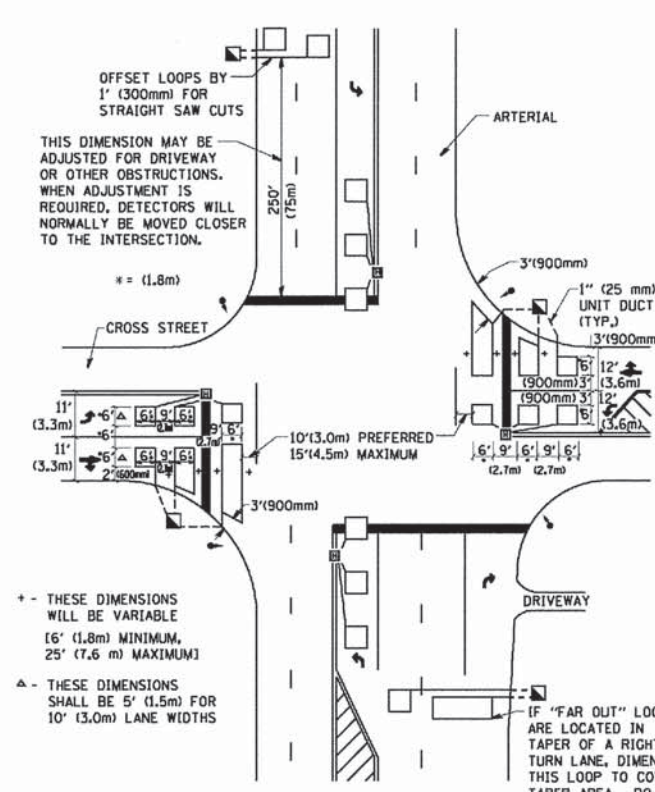
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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FILE NAME = N:\disto\22x34\ts07.dgn	USER NAME = gnglienab	DESIGNED - DRAWN -	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>		F.A.U. RTE. 1365	SECTION 12-00086-00-RS	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 21
PLOT SCALE = 5/8" = 1' IN.	CHECKED - DATE -	R.K.F.	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>TS-07</b>		CONTRACT NO. 61A84	
PLOT DATE = 1/4/2008	DATE -		REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-40031081				