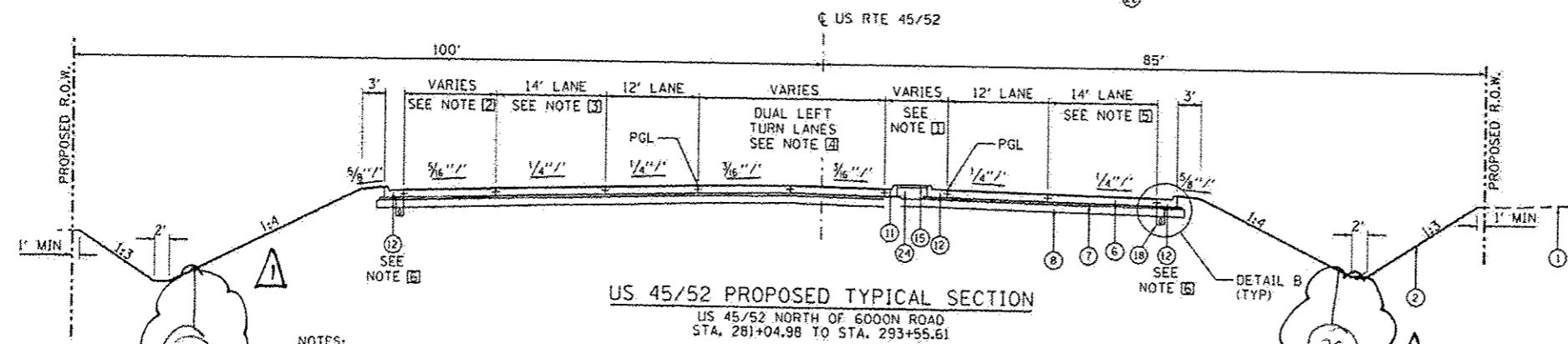
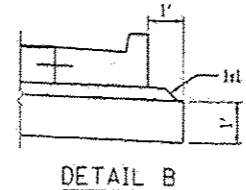


- NOTES:**
- 1 MEDIAN WIDTH
STA. 265+40.72 TO STA. 268+26.53 = NO MEDIAN
STA. 268+26.53 TO STA. 269+75.19 = CORRUGATED MEDIAN 0.0' TO 14.8'
STA. 269+75.19 TO STA. 274+40.72 = TRANSITION FROM 14.8' TO 32.0'
STA. 274+40.72 TO STA. 274+64.88 = 32.0'
STA. 274+64.88 TO STA. 276+56.49 = TRANSITION FROM 32.0' TO 8.0'
STA. 276+56.49 TO STA. 277+91.35 = 8.0'
 - 2 LANE DROP
STA. 265+40.72 TO STA. 274+40.72 = TRANSITION FROM 0.0' TO 14.0'
STA. 274+40.72 TO STA. 277+10.03 = 14.0'
STA. 277+10.03 TO STA. 277+91.35 = TRANSITION FROM 14.0' TO 22.1'
 - 3 DUAL TURN LANES
STA. 265+40.72 TO STA. 274+64.88 = NO TURN LANES
STA. 274+64.88 TO STA. 276+56.49 = TRANSITION FROM 0.0' TO 24.0'
STA. 276+56.49 TO STA. 277+91.35 = 24.0'

- 4 LANE TAPER
STA. 265+40.72 TO STA. 268+04.58 = TRANSITION FROM 0.0' TO 14.0'
STA. 268+04.58 TO STA. 277+91.35 = 14.0'
- 5 TURN LANE
STA. 265+40.72 TO STA. 272+90.02 = NO TURN LANE
STA. 272+90.02 TO STA. 274+90.03 = TRANSITION FROM 0.0' TO 12.0'
STA. 274+90.03 TO STA. 277+91.35 = 12.0'
- 6 PCC SHOULDERS
STA. 265+40.72, LT TO STA. 276+99.98, LT = 10' PCC SHOULDER
STA. 276+99.98, LT TO STA. 277+79.88, LT = TRANSITIONING TO CC&G TYPE B-6.24
STA. 277+79.88, LT TO STA. 277+91.35, LT = CC&G TYPE B-6.24
STA. 265+40.72, RT TO STA. 272+90.02, RT = 10' PCC SHOULDER
STA. 272+90.02, RT TO STA. 273+89.91, RT = TRANSITION FROM 10' TO 4' PCC SHOULDER
STA. 273+89.91, RT TO STA. 274+90.03, RT = 4' PCC SHOULDER
STA. 274+90.03, RT TO STA. 277+91.35, RT = CC&G TYPE B-6.24



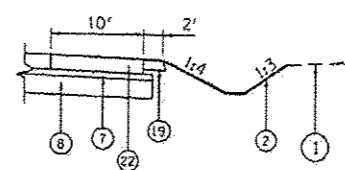
- NOTES:**
- 1 MEDIAN WIDTH
STA. 281+04.98 TO STA. 282+24.81 = 8.0'
STA. 282+24.81 TO STA. 284+16.41 = TRANSITION FROM 8.0' TO 32.0'
STA. 284+16.41 TO STA. 284+55.61 = 32.0'
STA. 284+55.61 TO STA. 289+22.79 = TRANSITION FROM 32.0' TO 14.8'
STA. 289+22.79 TO STA. 290+69.79 = CORRUGATED MEDIAN 14.8' TO 0.0'
STA. 290+69.79 TO STA. 293+55.61 = NO MEDIAN
 - 2 TURN LANE
STA. 281+04.98 TO STA. 283+05.50 = 12.0'
STA. 283+05.50 TO STA. 285+05.50 = TRANSITION FROM 12.0' TO 0.0'
STA. 285+05.50 TO STA. 293+55.61 = NO TURN LANE
 - 3 LANE ADDITION
STA. 281+04.98 TO STA. 290+91.49 = 14.0'
STA. 290+91.49 TO STA. 293+55.61 = TRANSITION FROM 14.0' TO 0.0'

- 4 DUAL TURN LANES
STA. 281+04.98 TO STA. 282+24.81 = 24.0'
STA. 282+24.81 TO STA. 284+16.41 = TRANSITION FROM 24.0' TO 0.0'
STA. 284+16.41 TO STA. 293+55.61 = NO TURN LANES
- 5 LANE DROP
STA. 281+04.98 TO STA. 281+86.30 = TRANSITION FROM 23.1' TO 14.0'
STA. 281+86.30 TO STA. 284+55.62 = 14.0'
STA. 284+55.62 TO STA. 293+55.61 = TRANSITION FROM 14.0' TO 0.0'
- 6 PCC SHOULDERS
STA. 281+04.98, LT TO STA. 283+05.51, LT = CC&G TYPE B-6.24
STA. 283+05.51, LT TO STA. 284+05.62, LT = 4' PCC SHOULDER
STA. 284+05.62, LT TO STA. 285+05.50, LT = TRANSITION FROM 4' TO 10' PCC SHOULDER
STA. 285+05.50, LT TO STA. 293+55.61, LT = 10' PCC SHOULDER
STA. 281+04.98, RT TO STA. 281+96.35, RT = TRANSITIONING CC&G TYPE B-6.24
STA. 281+96.35, RT TO STA. 293+55.61, RT = 10' PCC SHOULDER

- LEGEND**
- 1 EXISTING GROUND LINE
 - 2 GROUND LINE
 - 3 CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - 4 PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
 - 5 PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - 6 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - 7 STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
 - 8 AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - 9 UNUSED
 - 10 AGGREGATE BASE COURSE, TYPE A 6"
 - 11 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - 12 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - 13 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
 - 14 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
 - 15 CONCRETE MEDIAN SURFACE, 4 INCH
 - 16 CONCRETE MEDIAN SURFACE, CORRUGATED
 - 17 AGGREGATE SUBGRADE IMPROVEMENT, 24"
 - 18 PIPE UNDERDRAINS 4"
 - 19 AGGREGATE SHOULDERS, TYPE B 6"
 - 20 PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 21 PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
 - 22 PORTLAND CEMENT CONCRETE SHOULDERS 10"
 - 23 PORTLAND CEMENT CONCRETE SIDEWALK, 4"
 - 24 AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
 - 25 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
 - 26 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
 - 27 HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
 - 28 HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

US 45/52 : 2971(34) OTHER PRINCIPAL ARTERIAL 10.32 (PCC-20)

29 TOPSOIL EXCAVATION AND PLACEMENT (4")



Rev. 1-20-15

FILE NAME D:\2014\2014-01-20-15\1001	USER NAME MMH	DESIGNED -	REVISED -
MODEL NAME TYP	PLOT SCALE AS SHOWN	CHECKED -	REVISED -
	PLOT DATE 3/6/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS
I-57 AND 6000N RD (BOURBONNAIS PARKWAY)
BOURBONNAIS, IL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	146-118K-1	KANKAKEE	819	29
CONTRACT NO. 66982				

SCALE: AS SHOWN SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT