

GENERAL NOTES

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

THE BASE COURSE WIDENING SHALL BE CARRIED THROUGH ALL ENTRANCES, SIDE ROADS, AND MAILBOX TURNOUTS. EXCEPTIONS WILL BE SHOWN ON THE PLANS.

EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.

BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.

FOR STABILIZATION, ALL TYPE III BARRICADES WILL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED WILL BE DETERMINED BY THE ENGINEER.

ONLY THOSE TREES DESIGNATED BY THE ENGINEER OR LISTED IN THE TREE REMOVAL SCHEDULE SHALL BE REMOVED. THE CONTRACTOR SHALL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO HIS OPERATIONS.

THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP FOUR INCHES (100 MILLIMETERS) IN AREAS TO BE SEEDED OR SODDED. THE VEGETATION SUSTAINING SOIL REQUIRED WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.

SHORT TERM PAVEMENT MARKING SHALL BE USED TO OUTLINE EXIT AND ENTRANCE RAMPS FOR THE PRIME COAT APPLICATION AND EACH

DETECTABLE WARNING SURFACES SHALL EXTEND 2.0 FT MINIMUM IN THE DIRECTION OF PEDESTRIAN TRAVEL. AT CURB RAMPS AND BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL WIDTH OF THE RAMP RUN (EXCLUDING AND FLARES) OR BLENDED TRANSITIONS. SOME DETECTABLE WARNING PRODUCTS REQUIRE A CONCRETE BORDER FOR PROPER INSTALLATION. THE CONCRETE BORDER SHALL NOT EXCEED 2 INCHES.

ON EXISTING PAVEMENT WHICH MAY BE SUPERELEVATED, THE NEW HMA PAVEMENT SHALL BE BUILT WITH THE SAME SUPERELEVATION UNLESS NEW SUPERELEVATION RATES ARE GIVEN ON THE PLANS.

ALL ELEVATIONS REFERRING TO U.S.G.S. MEAN SEA LEVEL DATUM.

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

ANY EXCAVATED SOILS THAT ARE FROST SUSCEPTIBLE MAY BE USED IN THE EMBANKMENTS IF THEY ARE CAPPED WITH A MINIMUM OF THREE FEET OF NON FROST SUSCEPTIBLE SOILS.

REMOVAL OF EXISTING CORRUGATED METAL PIPE CULVERTS AT ENTRANCES WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

REMOVAL OF EXISTING AGGREGATE ENTRANCES WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

THE BORING LOG INDICATED THAT GROUNDWATER LEVELS MAY ENCRDACH ON THE CONSTRUCTION LIMITS OF THE BOX CULVERTS AND SOME STORM SEWERS. IT SHALL BE THE RESPONSIBILITY OF HE CONTRACTOR TO CONTROL THE GROUND WATER AND DIVERT THE STREAM FLOW DURING CONSTRUCTION IN ORDER TO KEEP THE CONSTRUCTION AREA FREE OF WATER. THE METHOD OF CONTROLLING THE WATER SHALL BE SUBJECT TO APPROVAL OF THE ENGINEER AND THE COST WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PRECAST CONCRETE BOX CULVERTS AND STORM SEWERS.

CULVERT FLOWS MUST BE MAINTAINED THROUGHOUT THE PROJECT. NORMAL FLOW SHALL BE ALLOWED TO PASS AT THE RATE IT ENTERS THE JOBSITE. HIGH FLOWS SHALL BE ALLOWED TO PASS WITHOUT CAUSING DAMAGE TO UPSTREAM PROPERTIES.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

| | | |
|------------------------------|--------|----------------------------|
| GRANULAR MATERIALS | 2.05 | TONS / CU YD |
| HMA RESURFACING | 112 | LBS / SQ YD / IN |
| SHORT TERM PAVEMENT MARKING | 10 | FT / 100 FT OF APPLICATION |
| MIX FOR CRACKS, JTS & FLGWYS | 0.0003 | TONS / SQ YD |
| LEVEL BINDER (HAND METHOD) | 0.0005 | TONS / SQ YD |
| SUPPLEMENTAL WATERING | 3 | GAL / SQ YD / APPLICATION |
| CALCIUM CHLORIDE | 2 | LB / SQ YD / APPLICATION |
| AGGREGATE DITCH CHECKS | 5 | TONS AGGREGATE |

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE PRESENCE OF DEPARTMENT-OWNED UNDERGROUND ELECTRICAL CABLE WITHIN THE LIMITS OF THE PROPOSED IMPROVEMENT. THE CONTRACTOR SHALL REQUEST THE ILLINOIS DEPARTMENT OF TRANSPORTATION IN OTTAWA (815-434-8417) TO LOCATE THE UNDERGROUND FACILITIES, PROVIDING A MINIMUM OF 72 HOURS NOTICE. THE DEPARTMENT IS NOT A MEMBER OF THE JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS (JULIE) SYSTEM.

ALL DAMAGE TO DEPARTMENT OWNED UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE. THIS SHALL INCLUDE ALL TEMPORARY REPAIRS REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS. SPLICING OF ELECTRIC CABLE WILL NOT BE ALLOWED. ELECTRIC CABLE SHALL BE REPLACED FROM POLE TO POLE OR CONTROLLER.

THE WORK REQUIRED TO CONNECT ANY SEWER TO AN EXISTING DRAINAGE STRUCTURE OR PIPE WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE BID FOR THE SEWER ITEMS.

MEMBERS OF JULIE KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

- COMED, AN EXELON COMPANY
- EXPLORER PIPELINE
- BUCKEYE PIPELINE
- CMS / ILLINOIS CENTURY NETWORK
- AT&T
- COMCAST
- NICOR
- AQUA ILLINOIS
- WINDSTREAM
- 36ONETWORKS

NON-MEMBERS OF JULIE KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

NONE

COMMITMENTS:

THE WETLAND AREA LOCATED IN THE SOUTHEAST QUADRANT OF THE INTERCHANGE SHALL BE DELINEATED TO PREVENT ENCRDACHMENT DURING CONSTRUCTION ACTIVITIES, PER THE WETLAND AREAS SPECIAL PROVISION.

TWO PRAIRIE REMNANTS WERE LOCATED ALONG THE WEST SIDE OF ILLINOIS ROUTE 50. BOTH ARE LOCATED APPROXIMATELY 8' FROM THE ROADSIDE EDGE AND IN ORDER TO PROTECT THE PRAIRIE THE FOLLOWING SHALL BE DONE: MINIMIZE THE CONSTRUCTION LIMITS ALONG THE REMNANTS, AS MUCH AS POSSIBLE; NO PARKING OF VEHICLES OR STORAGE OF EQUIPMENT OR MATERIALS SHALL OCCUR ON THE WEST SIDE OF IL 50 IN THE DESIGNATED PRAIRIE AREAS; AND THE AREAS DISTURBED BY CONSTRUCTION SHALL BE RESEDED WITH A NATIVE MIX (CLASS 4 AND 5A).

ALL EXCAVATED MATERIAL AT THE PROPOSED CROSS-OVERS AT STATION 399+00 AND 558+00 ON I-57 SHALL REMAIN ONSITE AND BE INCORPORATED INTO THE EXISTING MEDIAN/SHOULDER SLOPES AS DIRECTED BY THE ENGINEER.

MINOR RESIDENTIAL TILE / SUMP PUMP OUTLETS THAT CURRENTLY OUTLET INTO THE BACKSLOPES OF EXISTING DITCHES SHALL BE MARKED AS FOUND IN THE FIELD. THE CONTRACTOR SHALL MAINTAIN FLOW THROUGHOUT THE DURATION OF CONSTRUCTION. ALL TILES OUTLETTING TO AN OPEN DITCH SHALL HAVE A CLASS SI CONCRETE HEADWALL IN ACCORDANCE TO THE PIPE DRAINS AND CONCRETE HEADWALLS DETAIL.

A COMMITMENT WAS MADE THAT THE PROPOSED VULCAN MATERIALS ENTRANCE LOCATED AT STA 564+35 RT CAN BE UTILIZED TEMPORARILY AS A TWO WAY DRIVE DURING CONSTRUCTION UNTIL VULCAN COMPLETES THEIR INTERNAL ACCESS CIRCULATION ROADS FOR ONE-WAY ENTRANCE AND ONE WAY EXIT.



| | |
|---------|------------|
| DPA | 01/08/2014 |
| LMF | 01/08/2014 |
| REVISED | 01/11/2014 |
| DPA | 01/11/2014 |

FILE NAME * 038942839-ahs-gennote002.dgn

USER NAME * carpenterd.j
DESIGNED -
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PLOT SCALE * 100.0034' / 1in.
PLOT DATE * 7/28/2014

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, LEGEND, SCHEDULES AND INDEX OF SHEETS
I-57 AND 6000N RD (BOURBONNAIS PARKWAY)
BOURBONNAIS, IL

| | | | | |
|-----------------------------|------------|----------|-----------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57 | 146-UMBK-1 | KANKAKEE | 819 | 3 |
| SCALE: | | | SHEET _____ OF _____ SHEETS | |
| STA. #STA1# | | | TO STA. #STA2# | |
| [ILLINOIS] FED. AID PROJECT | | | | |

Rev. 1-20-15

CONTRACT NO. 66982



M001

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05

CONSTRUCTION CODE

| CODE NO. | ITEM | UNIT | URBAN TOTAL QUANTITY | CONSTRUCTION CODE | | | | | | | | | |
|------------|---------------------------------------|-------|----------------------|-----------------------|----------------------|--|--|-------------------------------------|---------------------------------|--|--------------|-------------------------------------|----------------------------|
| | | | | 90% FEDERAL 10% STATE | | 80% FEDERAL 20% BOURBONNAIS | | 90% FEDERAL 5% STATE 5% BOURBONNAIS | | 80% FEDERAL 20% BRADLEY | | 90% FEDERAL 5% STATE 5% BOURBONNAIS | |
| | | | | ROADWAY 0003 | BRIDGE 046-0148 0010 | HWY LIGHTING TRAFFIC SIGNALS I-57 & RAMPS 0021 | HWY LIGHTING SIGNAL PRE-EMPTION COMB LIGHTING 0021 | ROADWAY 0028 | TRAFFIC SIGNALS US 45 / 52 0021 | HWY LIGHTING SIGNAL PRE-EMPTION COMB LIGHTING 0021 | ROADWAY 0028 | TRAFFIC SIGNALS IL 50 0021 | TRAFFIC SIGNALS IL 50 0021 |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 1609 | 1609 | | | | | | | | | |
| 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 1315 | 1315 | | | | | | | | | |
| 20100500 | TREE REMOVAL, ACRES | ACRE | 1 | 1 | | | | | | | | | |
| 20101000 | TEMPORARY FENCE | FOOT | 1279 | 1279 | | | | | | | | | |
| 20200100 | EARTH EXCAVATION | CU YD | 82399 | 82399 | | | | | | | | | |
| * 20200200 | ROCK EXCAVATION | CU YD | 4760 | 4760 | | | | | | | | | |
| 20400800 | FURNISHED EXCAVATION | CU YD | 307394 | 307394 | | | | | | | | | |
| 20700220 | POROUS GRANULAR EMBANKMENT | CU YD | 1435 | 1435 | | | | | | | | | |
| 20800150 | TRENCH BACKFILL | CU YD | 17337 | 17285 | | | | 52 | | | | | |
| 20900110 | POROUS GRANULAR BACKFILL | CU YD | 48 | 48 | | | | | | | | | |
| 21101505 | TOPSOIL EXCAVATION AND PLACEMENT | CU YD | 41,400 | 41,400 | | | | | | | | | |
| 25000210 | SEEDING, CLASS 2A | ACRE | 74 | 74 | | | | | | | | | |
| 25000300 | SEEDING, CLASS 3 | ACRE | 3 | 3 | | | | | | | | | |
| 25000310 | SEEDING, CLASS 4 | ACRE | 0.1 | 0.1 | | | | | | | | | |
| 25000322 | SEEDING, CLASS 5A | ACRE | 0.1 | 0.1 | | | | | | | | | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 6908 | 6908 | | | | | | | | | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 6908 | 6908 | | | | | | | | | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 6908 | 6908 | | | | | | | | | |
| 25100115 | MULCH, METHOD 2 | ACRE | 22 | 22 | | | | | | | | | |
| 25100630 | EROSION CONTROL BLANKET | SO YD | 270931 | 270931 | | | | | | | | | |

19 * SPECIALITY ITEM

Rev. 1-20-15

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 12.03.13

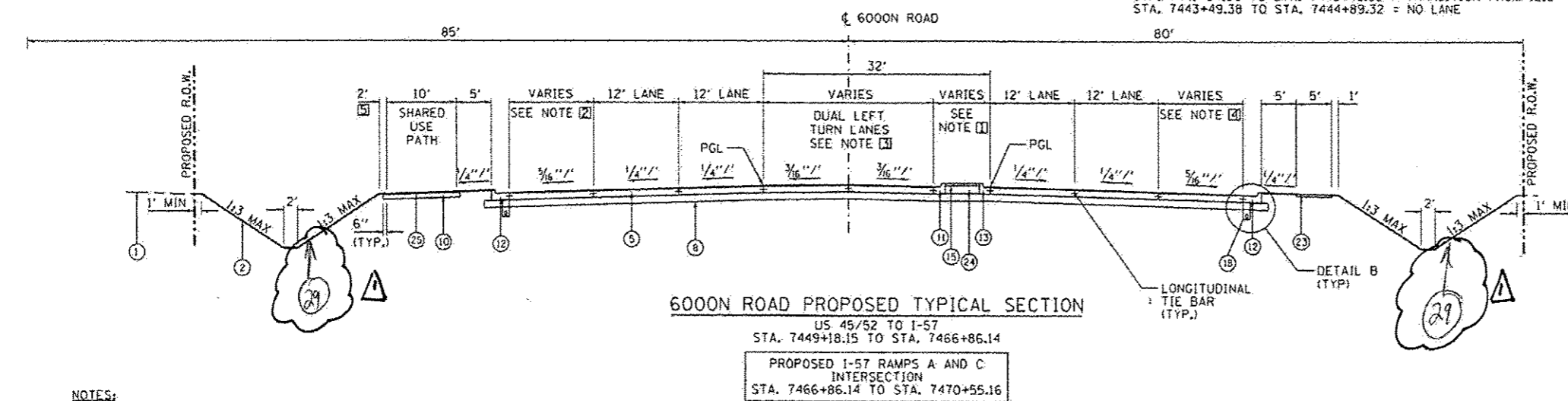
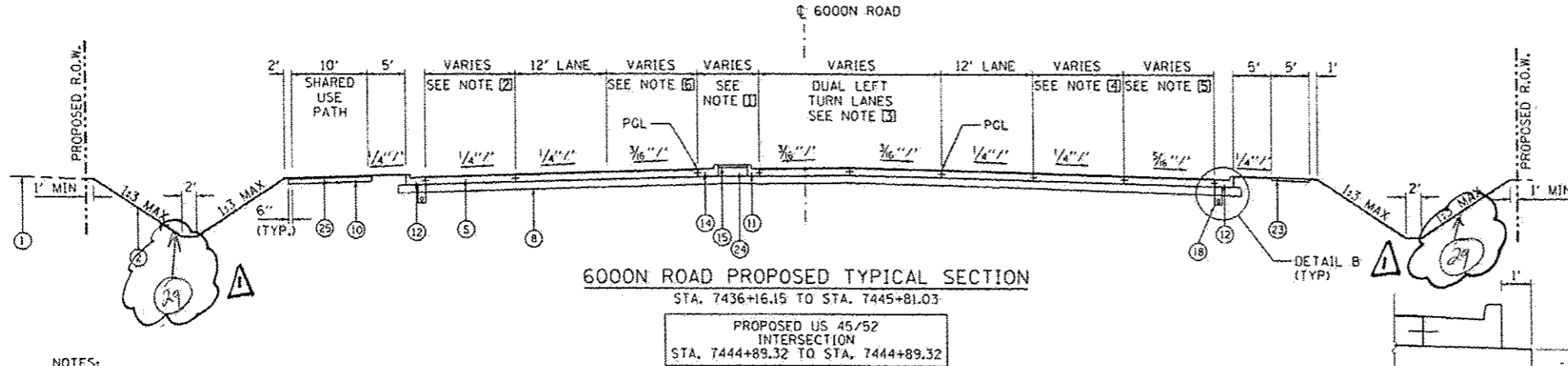
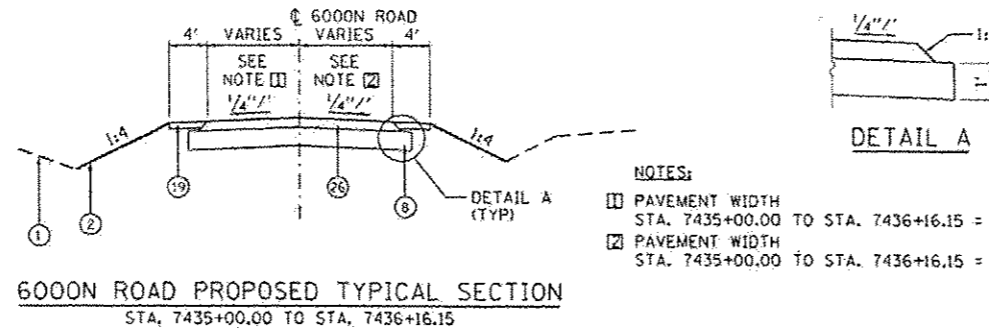
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
I-57 AND 6000N RD (BOURBONNAIS PARKWAY)
BOURBONNAIS, IL

SCALE: AS SHOWN SHEET NO. OF SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------|----------|---|-----------|
| 57 | 146-1H8X-1 | KANKAKEE | 819 | 4 |
| CONTRACT NO. 66982 | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | |



- LEGEND**
- 1 EXISTING GROUND LINE
 - 2 GROUND LINE
 - 3 CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - 4 PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
 - 5 PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - 6 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - 7 STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
 - 8 AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - 9 UNUSED
 - 10 AGGREGATE BASE COURSE, TYPE A 6"
 - 11 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - 12 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - 13 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
 - 14 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
 - 15 CONCRETE MEDIAN SURFACE, 4 INCH
 - 16 CONCRETE MEDIAN SURFACE, CORRUGATED
 - 17 AGGREGATE SUBGRADE IMPROVEMENT, 24"
 - 18 PIPE UNDERDRAINS 4"
 - 19 AGGREGATE SHOULDERS, TYPE B 6"
 - 20 PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 21 PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
 - 22 PORTLAND CEMENT CONCRETE SHOULDERS 10"
 - 23 PORTLAND CEMENT CONCRETE SIDEWALK, 4"
 - 24 AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
 - 25 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
 - 26 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
 - 27 HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
 - 28 HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

29 TOPSOIL EXCAVATION AND PLACEMENT (4")

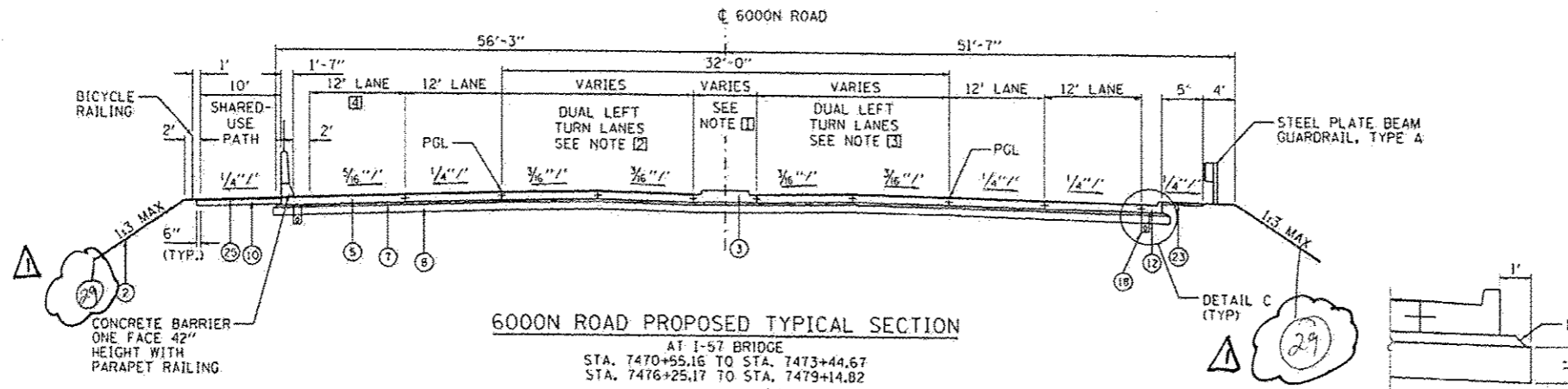
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| DATE | DESIGNED | DRAWN | CHECKED | DATE |
| 12/25/2014 | DPA | MWH | DPA | 12/12/13 |

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| MODEL NAME | PLDT SCALE | CHECKED | REVISION |
| MODEL | AS SHOWN | DPA | - |
| | | DATE | REVISION |
| | | 12/12/13 | - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | | |
|--|---------------------|--------------------|------------|---|--------------|-----------|
| PROPOSED TYPICAL SECTIONS | | F.S.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-57 AND 6000 RD (BOURBONNAIS PARKWAY) | | 57 | 146-110K-1 | KANKAKEE | 819 | 26 |
| BOURBONNAIS, IL | | CONTRACT NO. 66982 | | | | |
| SCALE: AS SHOWN | SHEET NO. OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |



6000N ROAD PROPOSED TYPICAL SECTION

AT I-57 BRIDGE
 STA. 7470+55.16 TO STA. 7473+44.67
 STA. 7476+25.17 TO STA. 7479+14.82

PROPOSED I-57 BRIDGE OMISSION
 STA. 7473+44.67 TO STA. 7476+25.17

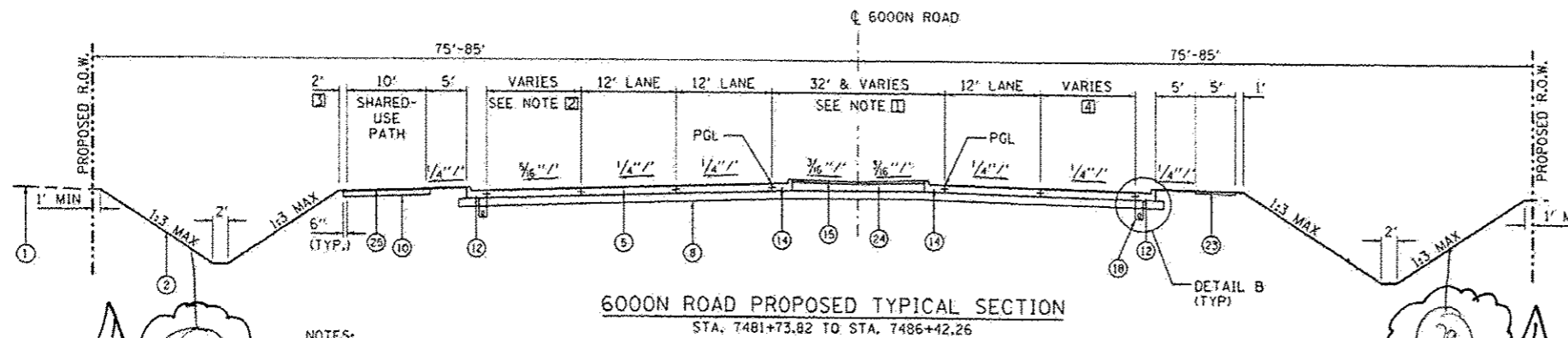
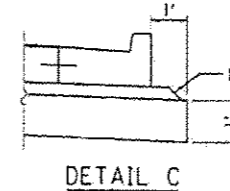
PROPOSED I-57 RAMP B AND D INTERSECTION
 STA. 7479+14.82 TO STA. 7481+73.82

☐ DUAL TURN LANES
 STA. 7470+55.16 TO STA. 7473+44.67 = NO TURN LANES
 STA. 7476+25.17 TO STA. 7476+39.77 = TRANSITION FROM 22.9' TO 24.0'
 STA. 7476+39.77 TO STA. 7479+14.82 = 24.0'

☐ TURN LANE
 STA. 7478+14.82 TO STA. 7479+14.82 = 12.0' TO 19.0'

NOTES:

- ☐ MEDIAN WIDTH
 STA. 7470+55.16 TO STA. 7471.65.15 = TRANSITION FROM 4.0' TO 8.0'
 STA. 7471.65.15 TO STA. 7473+44.67 = 8.0'
 STA. 7476+25.17 TO STA. 7478+04.82 = 8.0'
 STA. 7478+04.82 TO STA. 7479+14.82 = TRANSITION FROM 8.0' TO 4.0'
- ☐ DUAL TURN LANES
 STA. 7470+55.16 TO STA. 7473+30.21 = 24.0'
 STA. 7473+30.21 TO STA. 7473+44.67 = TRANSITION FROM 24.0' TO 22.9'
 STA. 7476+25.17 TO STA. 7479+14.82 = NO TURN LANES



6000N ROAD PROPOSED TYPICAL SECTION

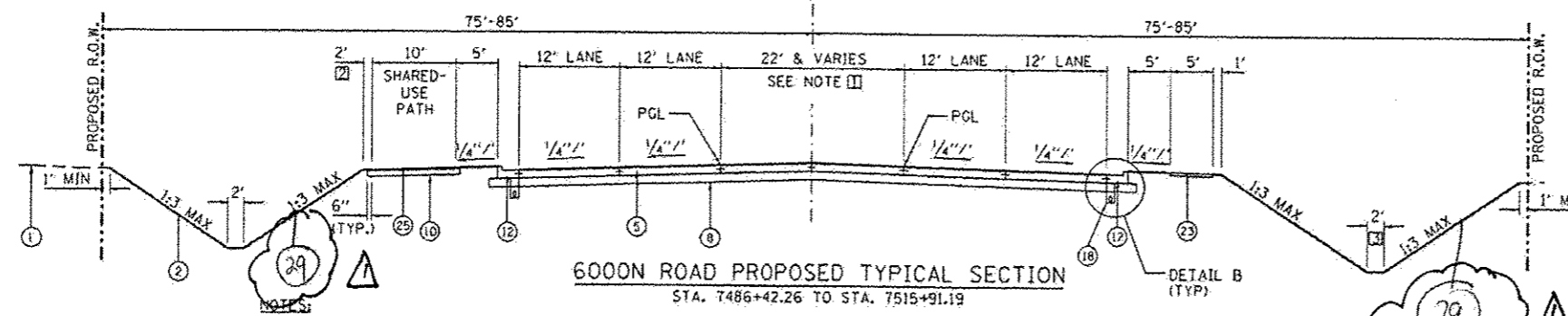
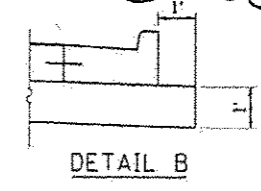
STA. 7481+73.82 TO STA. 7486+42.26

NOTES:

- ☐ MEDIAN WIDTH
 STA. 7481+73.82 TO STA. 7485+51.06 = 32.0'
 STA. 7485+51.06 TO STA. 7486+42.26 = TRANSITION FROM 32.0' TO 9.5'
- ☐ TURN LANE
 STA. 7481+73.82 TO STA. 7482+95.48 = 12.0'
 STA. 7482+95.48 TO STA. 7484+95.48 = TRANSITION FROM 12.0' TO 0.0'
 STA. 7484+95.48 TO STA. 7486+42.26 = NO TURN LANE
- ☐ STA. 7481+73.82 TO STA. 7486+42.26 = 3.0'
- ☐ TURN LANE
 STA. 7481+73.82 TO STA. 7482+93.89 = 24.4' TO 12.0'

- LEGEND**
- ① EXISTING GROUND LINE
 - ② GROUND LINE
 - ③ CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - ④ PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
 - ⑤ PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - ⑥ PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - ⑦ STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
 - ⑧ AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - ⑨ UNUSED
 - ⑩ AGGREGATE BASE COURSE, TYPE A 6"
 - ⑪ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
 - ⑭ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
 - ⑮ CONCRETE MEDIAN SURFACE, 4 INCH
 - ⑯ CONCRETE MEDIAN SURFACE, CORRUGATED
 - ⑰ AGGREGATE SUBGRADE IMPROVEMENT, 24"
 - ⑱ PIPE UNDERDRAINS 4"
 - ⑲ AGGREGATE SHOULDERS, TYPE B 6"
 - ⑳ PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - ㉑ PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
 - ㉒ PORTLAND CEMENT CONCRETE SHOULDERS 10"
 - ㉓ PORTLAND CEMENT CONCRETE SIDEWALK, 4"
 - ㉔ AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
 - ㉕ HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
 - ㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
 - ㉗ HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
 - ㉘ HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

6000N : 3030(34) ARTERIAL 7.75 (PCC-20)
 ㉔ TOPSOIL EXCAVATION AND PLACEMENT (4")



6000N ROAD PROPOSED TYPICAL SECTION

STA. 7486+42.26 TO STA. 7515+91.19

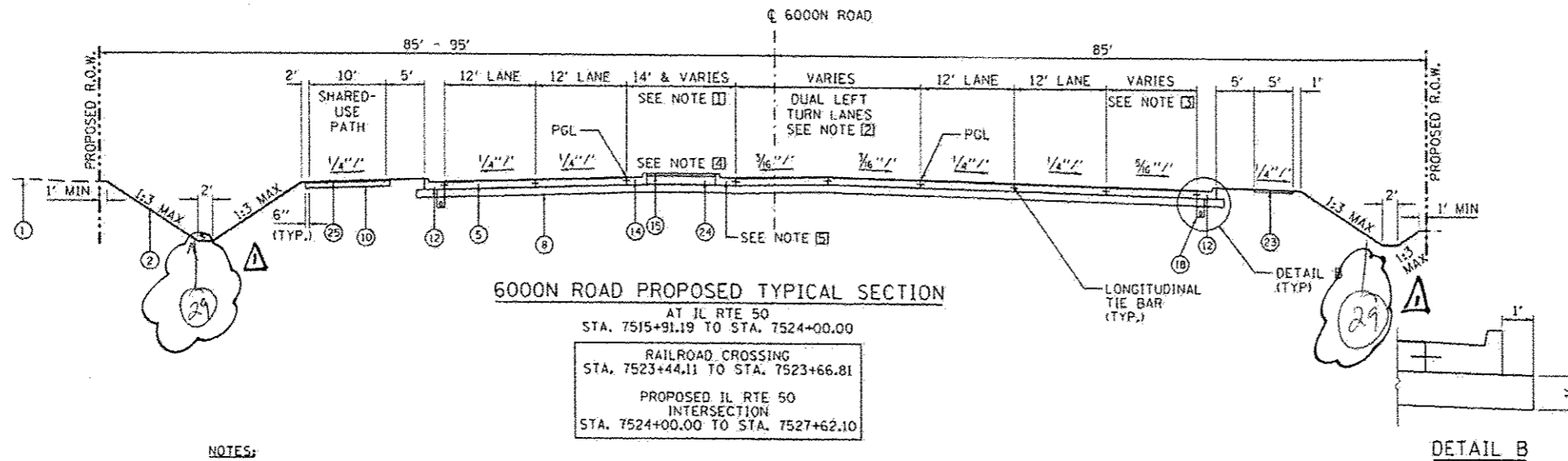
NOTES:

- ☐ MEDIAN WIDTH
 STA. 7486+42.26 TO STA. 7488+83.82 = TRANSITION FROM 32.0' TO 22.0'
 STA. 7488+83.82 TO STA. 7511+54.88 = 22.0'
 STA. 7511+54.88 TO STA. 7515+14.88 = TRANSITION FROM 22.0' TO 38.0'
- ☐ STA. 7486+42.26 TO STA. 7489+00.00 = 3.0'
- ☐ STA. 7490+52.00 TO STA. 7496+00.00 PROPOSED 6' WIDE DITCH BOTTOM

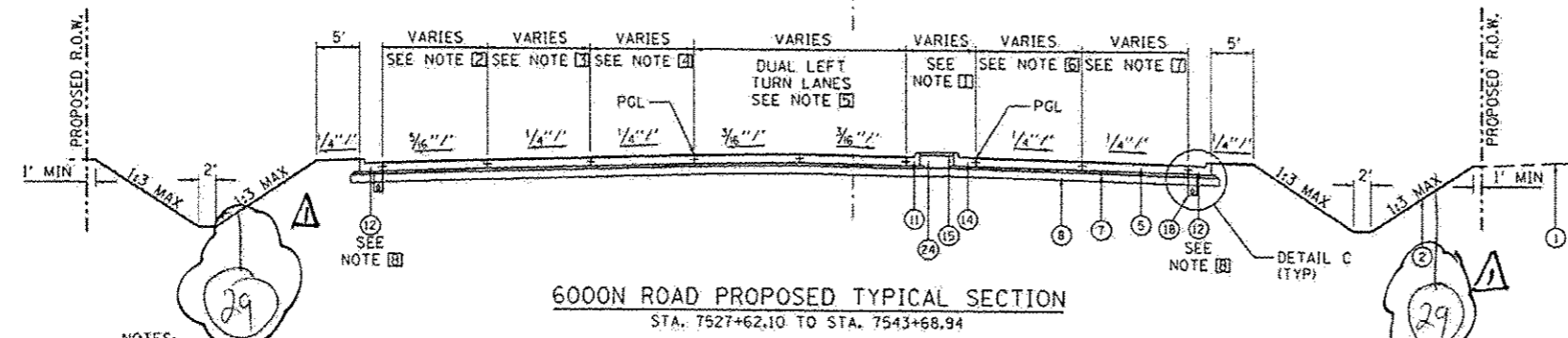
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|---|--|--|--|---|--|--|--|--------------------|---------------------|-----------------|------------------|--------------|
| STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | | | PROPOSED TYPICAL SECTIONS I-57 AND 6000N RD (BOURBONNAIS PARKWAY) BOURBONNAIS, IL | | | | F.A.I. RTE. 57 | SECTION (46-UHBR-1) | COUNTY KANKAKEE | TOTAL SHEETS 819 | SHEET NO. 27 |
| SCALE: AS SHOWN | | | | SHEET NO. OF SHEETS STA. TO STA. | | | | CONTRACT NO. 66982 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | | | | | |



- NOTES:**
- [1] MEDIAN WIDTH
STA. 7515+91.19 TO STA. 7517+27.00 = 9.9' TO 38.0'
STA. 7517+27.00 TO STA. 7519+89.88 = TRANSITION FROM 38.0' TO 14.0'
STA. 7519+89.88 TO STA. 7523+27.00 = 14.0'
 - [2] DUAL TURN LANES
STA. 7515+91.19 TO STA. 7517+27.00 = NO TURN LANES
STA. 7517+27.00 TO STA. 7520+27.00 = TRANSITION FROM 0.0' TO 24.0'
STA. 7520+27.00 TO STA. 7523+30.82 = 24.0'
 - [3] TURN LANE
STA. 7516+14.88 TO STA. 7518+14.88 = TRANSITION FROM 0.0' TO 12.0'
STA. 7518+14.88 TO STA. 7523+30.82 = 12.0'
 - [4] MEDIAN SURFACE
1/6" 1/6" STA. 7515+91.19 TO 7519+00.00
1/6" 1/6" STA. 7519+00.00 TO 7523+30.82
 - [5] CURB AND GUTTER
11 STA. 7515+91.19 TO STA. 7517+27.00
12 STA. 7517+27.00 TO STA. 7518+77.00
13 STA. 7518+77.00 TO STA. 7523+30.82

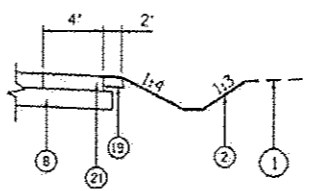
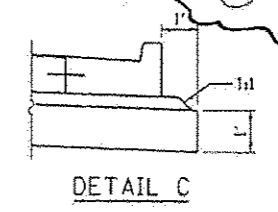


- NOTES:**
- [1] MEDIAN WIDTH
STA. 7527+62.10 TO STA. 7529+83.94 = 8.0'
STA. 7529+83.94 TO STA. 7533+43.94 = TRANSITION FROM 8.0' TO 32.0'
STA. 7533+43.94 TO STA. 7533+93.94 = 32.0'
STA. 7533+93.94 TO STA. 7536+67.91 = TRANSITION FROM 32.0' TO 26.3'
STA. 7536+67.91 TO STA. 7539+58.10 = TRANSITION FROM 26.3' TO 2.0' PARTIAL CORRUGATED
STA. 7539+58.10 TO STA. 7543+68.94 = NO MEDIAN
 - [2] TURN LANE
STA. 7527+62.10 TO STA. 7531+10.99 = 12.0'
STA. 7531+10.99 TO STA. 7533+10.99 = TRANSITION FROM 12.0' TO 0.0'
STA. 7533+10.99 TO STA. 7543+68.94 = NO TURN LANE
 - [3] LANE DROP
STA. 7527+62.10 TO STA. 7539+57.94 = 12.0'
STA. 7539+57.94 TO STA. 7541+77.75 = TRANSITION FROM 12.0' TO 0.0'
 - [4] LANE TAPER
STA. 7527+62.10 TO STA. 7541+77.75 = 12.0'
STA. 7541+77.75 TO STA. 7543+68.94 = TRANSITION FROM 12.0' TO 9.0'
 - [5] DUAL TURN LANES
STA. 7527+62.10 TO STA. 7529+83.94 = 24.0'
STA. 7529+83.94 TO STA. 7533+43.94 = TRANSITION FROM 24.0' TO 0.0'
STA. 7533+43.94 TO STA. 7543+68.94 = NO TURN LANES
 - [6] LANE TAPER
STA. 7527+62.10 TO STA. 7528+65.80 = 50.8' TO 12.0'
STA. 7528+65.80 TO STA. 7540+56.52 = 12.0'
STA. 7540+56.52 TO STA. 7543+68.94 = TRANSITION FROM 12.0' TO 9.0'
 - [7] LANE DROP
STA. 7527+62.10 TO STA. 7534+42.57 = 12.0'
STA. 7534+42.57 TO STA. 7540+56.52 = TRANSITION FROM 12.0' TO 0.0'
 - [8] PCC SHOULDERS
STA. 7541+68.27, RT TO STA. 7543+68.94, RT = 4' PCC SHOULDER

- LEGEND**
- 1 EXISTING GROUND LINE
 - 2 GROUND LINE
 - 3 CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - 4 PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
 - 5 PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - 6 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - 7 STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
 - 8 AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - 9 UNUSED
 - 10 AGGREGATE BASE COURSE, TYPE A 6"
 - 11 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - 12 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - 13 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
 - 14 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
 - 15 CONCRETE MEDIAN SURFACE, 4 INCH
 - 16 CONCRETE MEDIAN SURFACE, CORRUGATED
 - 17 AGGREGATE SUBGRADE IMPROVEMENT, 24"
 - 18 PIPE UNDERDRAINS, 4"
 - 19 AGGREGATE SHOULDERS, TYPE B 6"
 - 20 PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 21 PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
 - 22 PORTLAND CEMENT CONCRETE SHOULDERS 10"
 - 23 PORTLAND CEMENT CONCRETE SIDEWALK, 4"
 - 24 AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
 - 25 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
 - 26 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
 - 27 HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
 - 28 HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

6000N : 3030(34) ARTERIAL 7.75 (PCC-20)

29 TOPSOIL EXCAVATION AND PLACEMENT (4")



Rev. 1-20-15

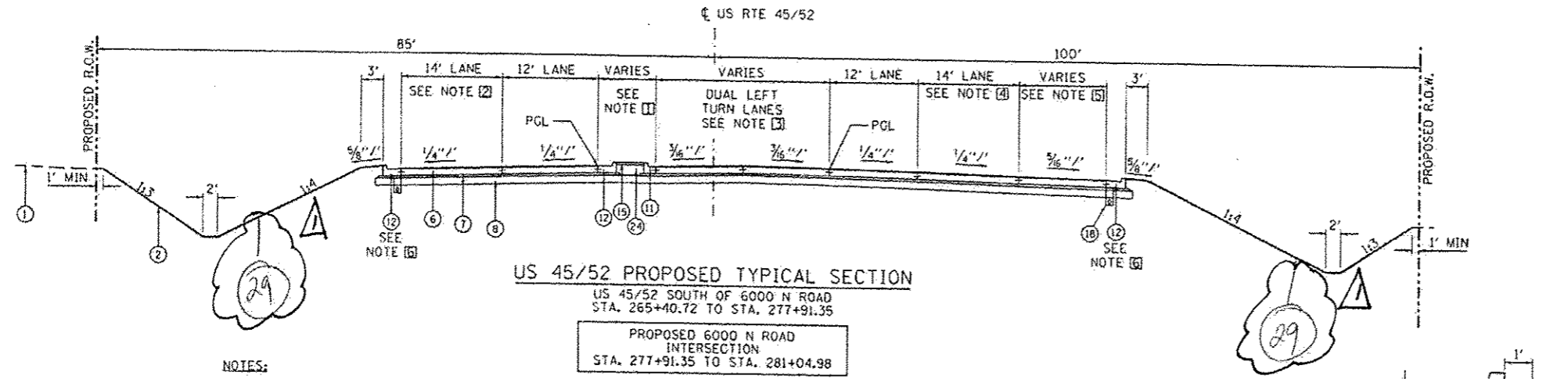
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|----------|----------|
| DESIGNED | REVISION |
| CHECKED | DATE |

| | | | |
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| FILE NAME : 03030(34) ARTERIAL 7.75 (PCC-20) | USER NAME : MWH | DESIGNED - | REVISED - |
| MODEL NAME : 12PA | PLOT SCALE : AS SHOWN | DRAWN - | REVISED - |
| | PLOT DATE : 5/20/2014 | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

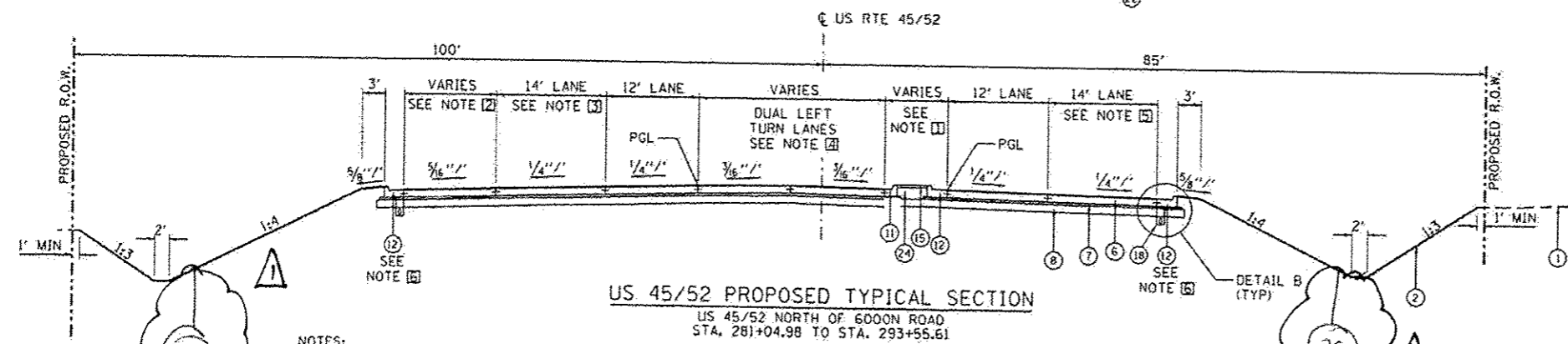
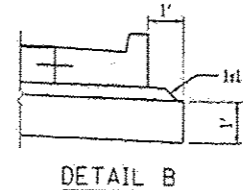
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|---|---------------------|--------------|--|
| PROPOSED TYPICAL SECTIONS I-57 AND 6000N RD (BOURBONNAIS PARKWAY) BOURBONNAIS, IL | | | |
| SCALE: AS SHOWN | SHEET NO. OF SHEETS | STA. TO STA. | |

| | | | | |
|---|--------------------|-----------------|--------------------|--------------|
| F.A.I. RTE. 57 | SECTION 146-110K-1 | COUNTY KANKAKEE | TOTAL SHEETS 819 | SHEET NO. 28 |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 66982 | |



- NOTES:**
- 1 MEDIAN WIDTH
STA. 265+40.72 TO STA. 268+26.53 = NO MEDIAN
STA. 268+26.53 TO STA. 269+75.19 = CORRUGATED MEDIAN 0.0' TO 14.8'
STA. 269+75.19 TO STA. 274+40.72 = TRANSITION FROM 14.8' TO 32.0'
STA. 274+40.72 TO STA. 274+64.88 = 32.0'
STA. 274+64.88 TO STA. 276+56.49 = TRANSITION FROM 32.0' TO 8.0'
STA. 276+56.49 TO STA. 277+91.35 = 8.0'
 - 2 LANE DROP
STA. 265+40.72 TO STA. 274+40.72 = TRANSITION FROM 0.0' TO 14.0'
STA. 274+40.72 TO STA. 277+10.03 = 14.0'
STA. 277+10.03 TO STA. 277+91.35 = TRANSITION FROM 14.0' TO 22.1'
 - 3 DUAL TURN LANES
STA. 265+40.72 TO STA. 274+64.88 = NO TURN LANES
STA. 274+64.88 TO STA. 276+56.49 = TRANSITION FROM 0.0' TO 24.0'
STA. 276+56.49 TO STA. 277+91.35 = 24.0'

- 4 LANE TAPER
STA. 265+40.72 TO STA. 268+04.58 = TRANSITION FROM 0.0' TO 14.0'
STA. 268+04.58 TO STA. 277+91.35 = 14.0'
- 5 TURN LANE
STA. 265+40.72 TO STA. 272+90.02 = NO TURN LANE
STA. 272+90.02 TO STA. 274+90.03 = TRANSITION FROM 0.0' TO 12.0'
STA. 274+90.03 TO STA. 277+91.35 = 12.0'
- 6 PCC SHOULDERS
STA. 265+40.72, LT TO STA. 276+99.98, LT = 10' PCC SHOULDER
STA. 276+99.98, LT TO STA. 277+79.88, LT = TRANSITIONING TO CC&G TYPE B-6.24
STA. 277+79.88, LT TO STA. 277+91.35, LT = CC&G TYPE B-6.24
STA. 265+40.72, RT TO STA. 272+90.02, RT = 10' PCC SHOULDER
STA. 272+90.02, RT TO STA. 273+89.91, RT = TRANSITION FROM 10' TO 4' PCC SHOULDER
STA. 273+89.91, RT TO STA. 274+90.03, RT = 4' PCC SHOULDER
STA. 274+90.03, RT TO STA. 277+91.35, RT = CC&G TYPE B-6.24



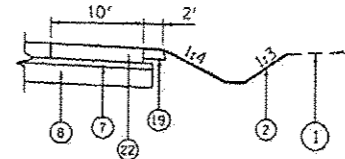
- NOTES:**
- 1 MEDIAN WIDTH
STA. 281+04.98 TO STA. 282+24.81 = 8.0'
STA. 282+24.81 TO STA. 284+16.41 = TRANSITION FROM 8.0' TO 32.0'
STA. 284+16.41 TO STA. 284+55.61 = 32.0'
STA. 284+55.61 TO STA. 289+22.79 = TRANSITION FROM 32.0' TO 14.8'
STA. 289+22.79 TO STA. 290+69.79 = CORRUGATED MEDIAN 14.8' TO 0.0'
STA. 290+69.79 TO STA. 293+55.61 = NO MEDIAN
 - 2 TURN LANE
STA. 281+04.98 TO STA. 283+05.50 = 12.0'
STA. 283+05.50 TO STA. 285+05.50 = TRANSITION FROM 12.0' TO 0.0'
STA. 285+05.50 TO STA. 293+55.61 = NO TURN LANE
 - 3 LANE ADDITION
STA. 281+04.98 TO STA. 290+91.49 = 14.0'
STA. 290+91.49 TO STA. 293+55.61 = TRANSITION FROM 14.0' TO 0.0'

- 4 DUAL TURN LANES
STA. 281+04.98 TO STA. 282+24.81 = 24.0'
STA. 282+24.81 TO STA. 284+16.41 = TRANSITION FROM 24.0' TO 0.0'
STA. 284+16.41 TO STA. 293+55.61 = NO TURN LANES
- 5 LANE DROP
STA. 281+04.98 TO STA. 281+86.30 = TRANSITION FROM 23.1' TO 14.0'
STA. 281+86.30 TO STA. 284+55.62 = 14.0'
STA. 284+55.62 TO STA. 293+55.61 = TRANSITION FROM 14.0' TO 0.0'
- 6 PCC SHOULDERS
STA. 281+04.98, LT TO STA. 283+05.51, LT = CC&G TYPE B-6.24
STA. 283+05.51, LT TO STA. 284+05.62, LT = 4' PCC SHOULDER
STA. 284+05.62, LT TO STA. 285+05.50, LT = TRANSITION FROM 4' TO 10' PCC SHOULDER
STA. 285+05.50, LT TO STA. 293+55.61, LT = 10' PCC SHOULDER
STA. 281+04.98, RT TO STA. 281+96.35, RT = TRANSITIONING CC&G TYPE B-6.24
STA. 281+96.35, RT TO STA. 293+55.61, RT = 10' PCC SHOULDER

- LEGEND**
- 1 EXISTING GROUND LINE
 - 2 GROUND LINE
 - 3 CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - 4 PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
 - 5 PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - 6 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - 7 STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
 - 8 AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - 9 UNUSED
 - 10 AGGREGATE BASE COURSE, TYPE A 6"
 - 11 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - 12 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - 13 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
 - 14 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
 - 15 CONCRETE MEDIAN SURFACE, 4 INCH
 - 16 CONCRETE MEDIAN SURFACE, CORRUGATED
 - 17 AGGREGATE SUBGRADE IMPROVEMENT, 24"
 - 18 PIPE UNDERDRAINS 4"
 - 19 AGGREGATE SHOULDERS, TYPE B 6"
 - 20 PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 21 PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
 - 22 PORTLAND CEMENT CONCRETE SHOULDERS 10"
 - 23 PORTLAND CEMENT CONCRETE SIDEWALK, 4"
 - 24 AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
 - 25 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
 - 26 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
 - 27 HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
 - 28 HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

US 45/52 : 2971(34) OTHER PRINCIPAL ARTERIAL 10.32 (PCC-20)

29 TOPSOIL EXCAVATION AND PLACEMENT (4")



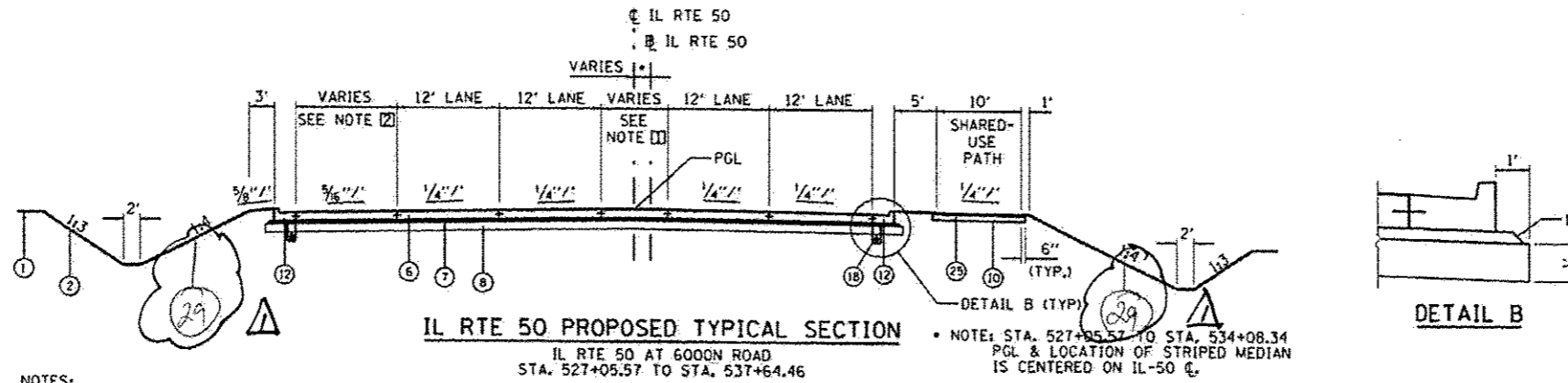
Rev. 1-20-15

| | | | |
|---|------------------------|---------------|--------------|
| FILE NAME D:\2014\02\2014-02-10\1001 | USER NAME MMH | DESIGNED - | REVISED - |
| MODEL NAME TYP | PLOT SCALE AS SHOWN | CHECKED - | REVISED - |
| | PLOT DATE 3/6/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

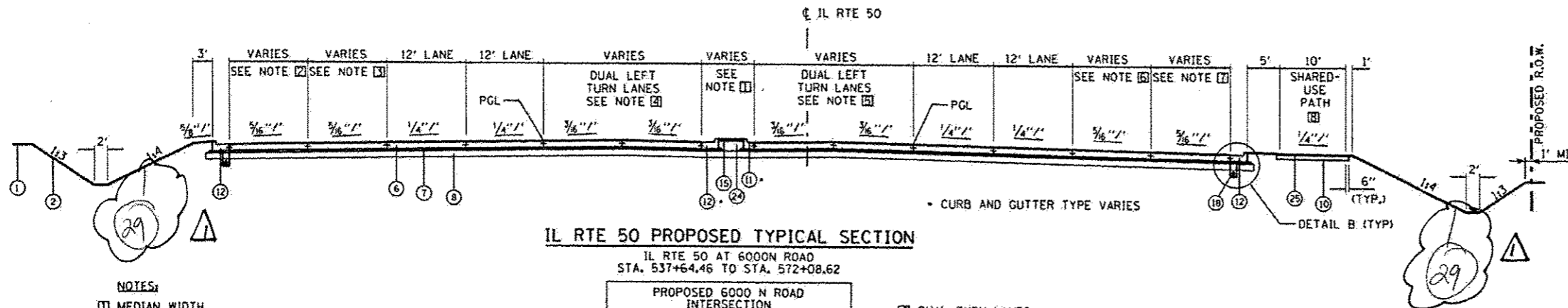
**PROPOSED TYPICAL SECTIONS
I-57 AND 6000N RD (BOURBONNAIS PARKWAY)
BOURBONNAIS, IL**

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|------------|----------|---------------------|--------------|
| 57 | 146-118K-1 | KANKAKEE | 819 | 29 |
| SCALE: AS SHOWN | | | SHEET NO. OF SHEETS | STA. TO STA. |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT | | | | |



NOTES:
 1 MEDIAN WIDTH
 STA. 527+05.57 TO STA. 534+08.34 = TRANSITION FROM 0.0' TO 12.0' (STRIPED MEDIAN)
 STA. 534+08.34 TO STA. 537+64.46 = 12.0' (STRIPED MEDIAN)

2 LANE DROP
 STA. 527+05.57 TO STA. 529+53.38 = NO LANE
 STA. 529+53.38 TO STA. 535+53.38 = TRANSITION FROM 0.0' TO 12.0'
 STA. 535+53.38 TO STA. 537+64.46 = 12.0'



NOTES:
 1 MEDIAN WIDTH
 STA. 537+64.46 TO STA. 542+67.88 = TRANSITION FROM 12.0' TO 32.0'
 STA. 542+67.88 TO STA. 547+70.22 = 32.0'
 STA. 547+70.22 TO STA. 551+30.22 = TRANSITION FROM 32.0' TO 8.0'
 STA. 551+30.22 TO STA. 552+54.54 = 8.0'
 STA. 556+92.31 TO STA. 558+87.13 = 8.0'
 STA. 558+87.13 TO STA. 561+87.26 = TRANSITION FROM 8.0' TO 32.0'
 STA. 561+87.26 TO STA. 562+75.60 = 32.0'
 STA. 562+75.60 TO STA. 563+80.32 = TRANSITION FROM 32.0' TO NO MEDIAN
 STA. 563+80.32 TO STA. 564+43.00 = NO MEDIAN
 STA. 564+43.00 TO STA. 564+58.63 = TRANSITION FROM NO MEDIAN TO 32.0'
 STA. 564+58.63 TO STA. 568+78.58 = 32.0'
 STA. 568+78.58 TO STA. 572+08.63 = TRANSITION FROM 32.0' TO NO MEDIAN

2 AUX. LANE S. OF 6000 / TURN LANE N. OF 6000
 STA. 537+64.46 TO STA. 544+31.92 = NO AUX. LANE
 STA. 544+31.92 TO STA. 546+51.92 = TRANSITION FROM 0.0' TO 12.0'
 STA. 546+51.92 TO STA. 552+54.54 = 12.0'
 STA. 556+92.31 TO STA. 559+81.31 = 14.0'
 STA. 559+81.31 TO STA. 564+83.32 = TRANSITION FROM 14.0' TO 0.0'
 STA. 564+83.32 TO STA. 572+08.62 = NO TURN LANE

3 LANE DROP
 STA. 527+05.57 TO STA. 529+53.38 = NO LANE
 STA. 529+53.38 TO STA. 535+53.02 = TRANSITION FROM 0.0' TO 12.0'
 STA. 535+53.02 TO STA. 552+54.54 = 12.0'
 STA. 556+92.31 TO STA. 559+81.31 = 10.0'
 STA. 559+81.31 TO STA. 564+83.32 = TRANSITION FROM 10.0' TO 0.0'
 STA. 564+83.32 TO STA. 572+08.63 = NO LANE

4 DUAL TURN LANES
 STA. 537+64.16 TO STA. 562+43.32 = NO TURN LANES
 STA. 566+92.31 TO STA. 558+27.26 = 24.0'
 STA. 558+27.26 TO STA. 561+87.26 = TRANSITION FROM 24.0' TO 0.0'
 STA. 561+87.26 TO STA. 572+08.62 = NO TURN LANES

5 DUAL TURN LANES
 STA. 537+64.16 TO STA. 547+70.22 = NO TURN LANES
 STA. 547+70.22 TO STA. 551+30.22 = TRANSITION FROM 0.0' TO 24.0'
 STA. 551+30.22 TO STA. 552+54.54 = 24.0'
 STA. 556+92.31 TO STA. 572+08.62 = NO TURN LANES

6 LANE DROP
 STA. 537+64.16 TO STA. 543+92.44 = NO LANE
 STA. 543+92.44 TO STA. 546+32.44 = TRANSITION FROM 0.0' TO 12.0'
 STA. 546+32.44 TO STA. 552+54.54 = 12.0'
 STA. 556+92.31 TO STA. 572+08.62 = NO LANE

7 TURN LANE
 STA. 537+64.16 TO STA. 547+32.44 = NO TURN LANE
 STA. 547+32.44 TO STA. 549+52.44 = TRANSITION FROM 0.0' TO 12.0'
 STA. 549+52.44 TO STA. 552+54.54 = 12.0'
 STA. 556+92.31 TO STA. 572+08.62 = NO TURN LANE

8 SHARED-USE PATH (SOUTH OF 6000 ONLY)
 STA. 552+54.54 TO STA. 553+50.00 = 10' WIDE SHARED USE PATH
 STA. 553+50.00 TO STA. 572+08.62 = NO SHARED-USE PATH

PROPOSED 6000 N ROAD
 INTERSECTION
 STA. 552+54.54 TO STA. 556+92.31

LEGEND

- EXISTING GROUND LINE
- GROUND LINE
- CONCRETE MEDIAN, TYPE SB (SPECIAL)
- PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
- PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
- PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- AGGREGATE SUBGRADE IMPROVEMENT, 12"
- UNUSED
- AGGREGATE BASE COURSE, TYPE A 6"
- COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
- COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
- CONCRETE MEDIAN SURFACE, 4 INCH
- CONCRETE MEDIAN SURFACE, CORRUGATED
- AGGREGATE SUBGRADE IMPROVEMENT, 24"
- PIPE UNDERDRAINS 4"
- AGGREGATE SHOULDERS, TYPE B 6"
- PORTLAND CEMENT CONCRETE SHOULDERS 9"
- PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
- PORTLAND CEMENT CONCRETE SHOULDERS 10"
- PORTLAND CEMENT CONCRETE SIDEWALK, 4"
- AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
- HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
- HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
- HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
- HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

US 50 : 4770(34) OTHER PRINCIPAL ARTERIAL 10.24 (PCC-20)

29 TOPSOIL EXCAVATION AND PLACEMENT (4")

Rev. 1-20-15

| | |
|----------|----------|
| DESIGNED | DPA |
| DRAWN | MWH |
| CHECKED | DPA |
| DATE | 12.12.13 |

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 MODEL NAME : TYPE

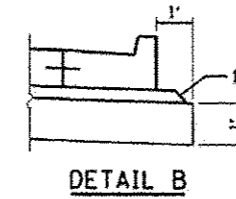
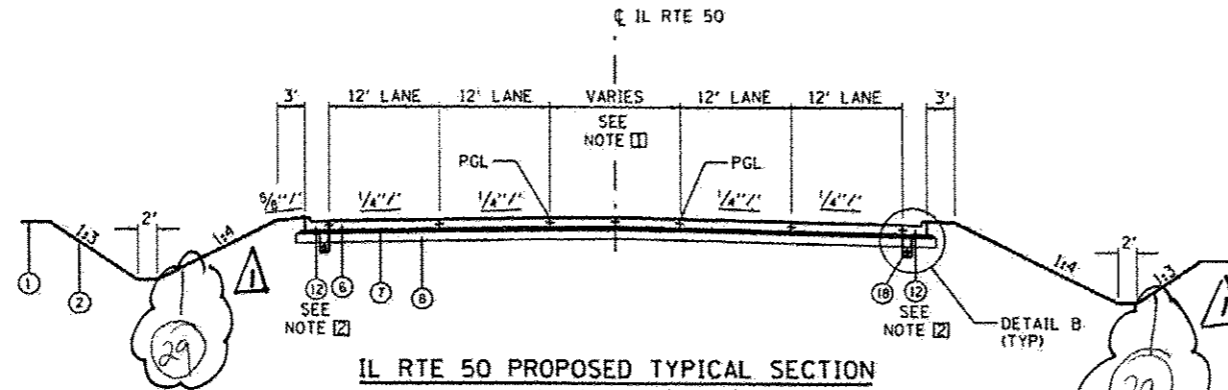
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|-----------------------|-----------------|-----------|
| USER NAME : MWH | DESIGNED : DPA | REVISED : |
| PLOT SCALE : AS SHOWN | DRAWN : MWH | REVISED : |
| PLOT DATE : 8/7/2014 | CHECKED : DPA | REVISED : |
| | DATE : 12.12.13 | REVISED : |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS
 I-57 AND 6000 RD (BOURBONNAIS PARKWAY)
 BOURBONNAIS, IL

SCALE: AS SHOWN SHEET NO. OF SHEETS STA. TO STA.

| P.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|------------|----------|--------------------|-----------|
| 57 | (46-1)HK-1 | KANKAKEE | 819 | 30 |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT | | | CONTRACT NO. 66982 | |



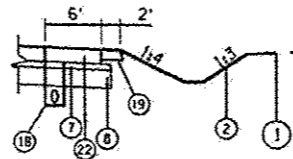
NOTES:

☐ MEDIAN WIDTH

STA. 572+08.62 TO STA. 572+80.45 = NO MEDIAN
 STA. 572+80.45 TO STA. 573+07.76 = TRANSITION FROM NO MEDIAN TO 14.9' (STRIPED MEDIAN)
 STA. 573+07.76 TO STA. 576+60.44 = TRANSITION FROM 14.9' TO 0.0' (STRIPED MEDIAN)
 STA. 576+60.44 TO STA. 578+16.73 = NO MEDIAN

☑ PCC SHOULDERS

STA. 574+08.66, LT TO STA. 578+16.73, LT = 6' PCC SHOULDER.
 STA. 574+09.12, RT TO STA. 578+23.62, RT = 6' PCC SHOULDER.



LEGEND

- ① EXISTING GROUND LINE
- ② GROUND LINE
- ③ CONCRETE MEDIAN, TYPE SB (SPECIAL)
- ④ PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
- ⑤ PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
- ⑥ PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- ⑦ STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- ⑧ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑨ UNUSED
- ⑩ AGGREGATE BASE COURSE, TYPE A 6"
- ⑪ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
- ⑭ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
- ⑮ CONCRETE MEDIAN SURFACE, 4 INCH
- ⑯ CONCRETE MEDIAN SURFACE, CORRUGATED
- ⑰ AGGREGATE SUBGRADE IMPROVEMENT, 24"
- ⑱ PIPE UNDERDRAINS, 4"
- ⑲ AGGREGATE SHOULDERS, TYPE B 6"
- ⑳ PORTLAND CEMENT CONCRETE SHOULDERS, 9"
- ㉑ PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
- ㉒ PORTLAND CEMENT CONCRETE SHOULDERS 10"
- ㉓ PORTLAND CEMENT CONCRETE SIDEWALK, 4"
- ㉔ AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
- ㉕ HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
- ㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
- ㉗ HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
- ㉘ HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

US 50 : 4770(34) OTHER PRINCIPAL ARTERIAL 10.24 (PCC-20)

⚠ (29) TOP SOIL EXCAVATION AND PLACEMENT (4")

MIXTURES TABLE

| MIXTURE USES | HMA BINDER (6000N) | HMA SURFACE (6000N) | HMA SURFACE (1-57) | HMA SURFACE (SHARED-USE PATH) | HMA SHOULDERS (1-57) | HMA STABILIZED-SUBBASE |
|------------------------|--------------------|---------------------|--------------------|-------------------------------|----------------------|------------------------|
| PG GRADE | PG64-22 | PG64-22 | SBS PG70-22 | PG64-22 | PG64-22 | PG64-22 |
| DESIGN AIR VOIDS | 4.0% @ N50 | 4.0% @ N50 | 4.0% @ N90 | 4.0% @ N30 | 4.0% @ N50 | 2.0% @ N30 |
| MIXTURE COMPOSITION | IL 19.0 | IL 9.5 | IL 9.5 | IL 9.5 | IL 9.5 | IL 19.0L |
| FRICITION AGGREGATE | | MIXTURE C | MIXTURE E | MIXTURE C | MIXTURE C | |
| DENSITY CONTROL METHOD | CORES | CORES | CORES | CORES / CORRELATION | CORES / CORRELATION | CORES / CORRELATION |

⚠ Rev. 1-20-15

| | | | |
|-----------|-----------|----------|---------|
| FILE NAME | USER NAME | DESIGNED | REVISED |
| DRAWN | CHECKED | DATE | |
| REVISIONS | | | |

| | | | |
|-----------|-----------|----------|---------|
| FILE NAME | USER NAME | DESIGNED | REVISED |
| DRAWN | CHECKED | DATE | |
| REVISIONS | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

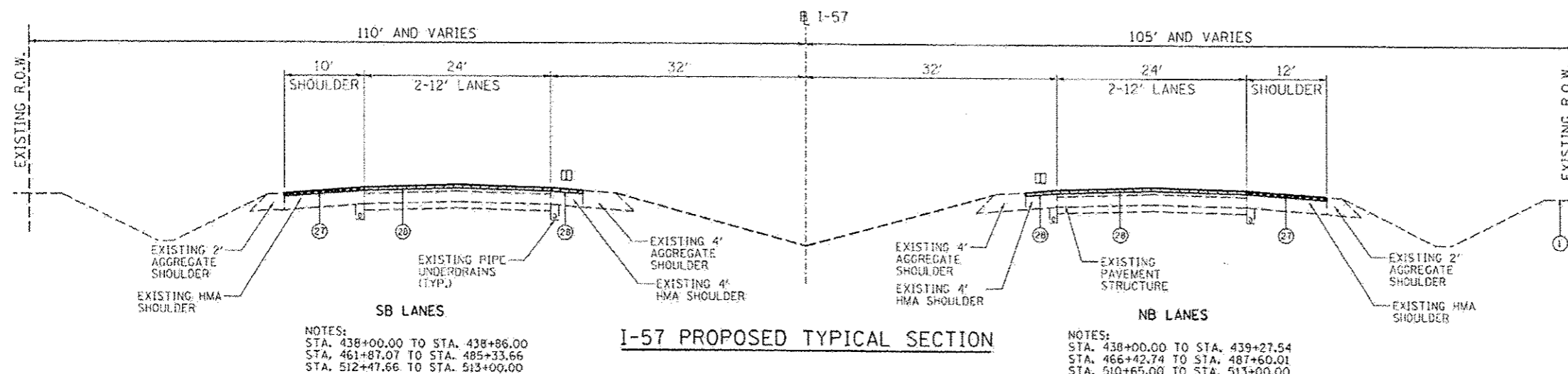
| | | | | |
|----------|-----------|-----------|------|---------|
| SCALE | SHEET NO. | OF SHEETS | STA. | TO STA. |
| AS SHOWN | | | | |

| | | | | |
|--------------------|-------------|----------|---------------------------|-----------|
| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57 | (46-1)HBK-1 | KANKAKEE | 819 | 31 |
| CONTRACT NO. 66982 | | | ILLINOIS FED. AID PROJECT | |

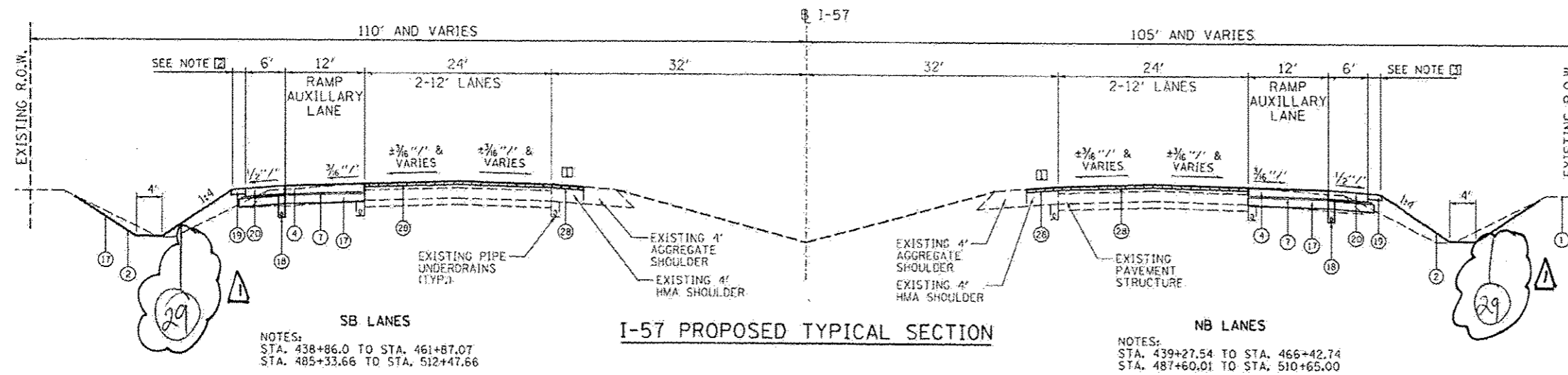


- LEGEND**
- ① EXISTING GROUND LINE
 - ② GROUND LINE
 - ③ CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - ④ PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
 - ⑤ PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - ⑥ PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - ⑦ STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
 - ⑧ AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - ⑨ UNUSED
 - ⑩ AGGREGATE BASE COURSE, TYPE A 6"
 - ⑪ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑫ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - ⑬ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
 - ⑭ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
 - ⑮ CONCRETE MEDIAN SURFACE, 4 INCH
 - ⑯ CONCRETE MEDIAN SURFACE, CORRUGATED
 - ⑰ AGGREGATE SUBGRADE IMPROVEMENT, 24"
 - ⑱ PIPE UNDERDRAINS 4"
 - ⑲ AGGREGATE SHOULDERS, TYPE B 6"
 - ⑳ PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - ㉑ PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
 - ㉒ PORTLAND CEMENT CONCRETE SHOULDERS 10"
 - ㉓ PORTLAND CEMENT CONCRETE SIDEWALK, 4"
 - ㉔ AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
 - ㉕ HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
 - ㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
 - ㉗ HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
 - ㉘ HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

㉙ TOPSOIL EXCAVATION AND PLACEMENT (4")



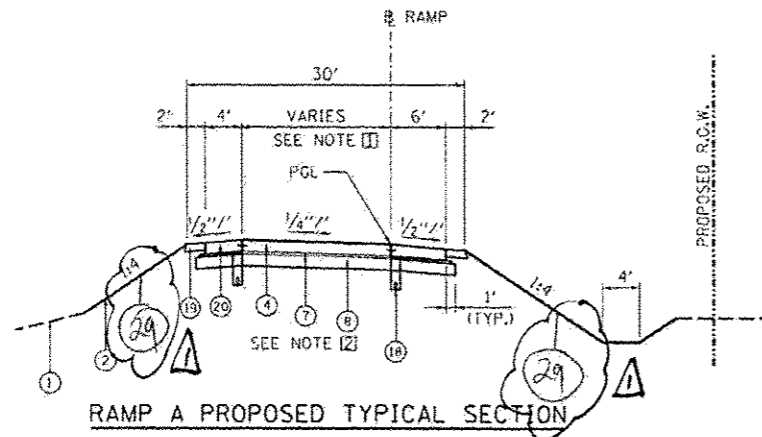
NOTES:
 ① THE I-57 INSIDE SHOULDER SHALL BE PAVED CONCURRENTLY WITH THE MAINLINE PAVEMENT.



I-57 RAMPS : 1140(34) INTERSTATE 5.41 (PCC-20)

Rev. 1-20-15

| | | | | | | | | | | |
|---|---|---|---|---|--|--|-----------------------|-------------------|--------------------|----------------|
| FILE NAME = USER NAME = MCH DESIGNED - DPA DRAWN - MWH CHECKED - DPA DATE - 12.03.13 | MODEL NAME = PLOT SCALE = AS SHOWN PLOT DATE = 12/03/2014 | REVISIONS REVISION - REVISION - REVISION - REVISION - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PROPOSED TYPICAL SECTIONS I-57 AND 6000N RD (BOURBONNAIS PARKWAY) BOURBONNAIS, IL | | F.A.I. R.T.E. = 57 | SECTION = (46-118K-1) | COUNTY = KANKAKEE | TOTAL SHEETS = 819 | SHEET NO. = 32 |
| | | | | SCALE: AS SHOWN SHEET NO. OF SHEETS STA. TO STA. | | FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT CONTRACT NO. 66982 | | | | |

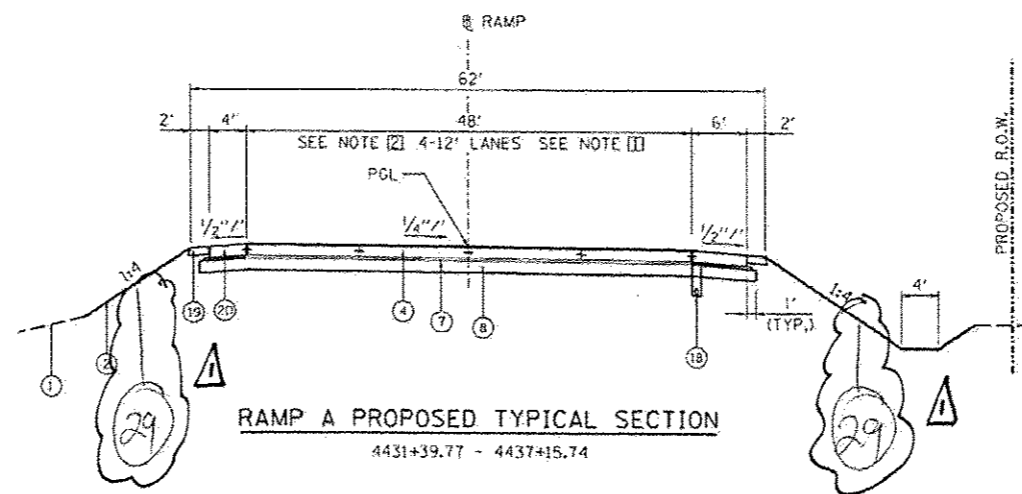


RAMP A PROPOSED TYPICAL SECTION

STA. 44+00.00 TO STA. 4431+39.77

PROPOSED 6000N INTERSECTION
STA. 4437+15.74 TO STA. 4438+18.20

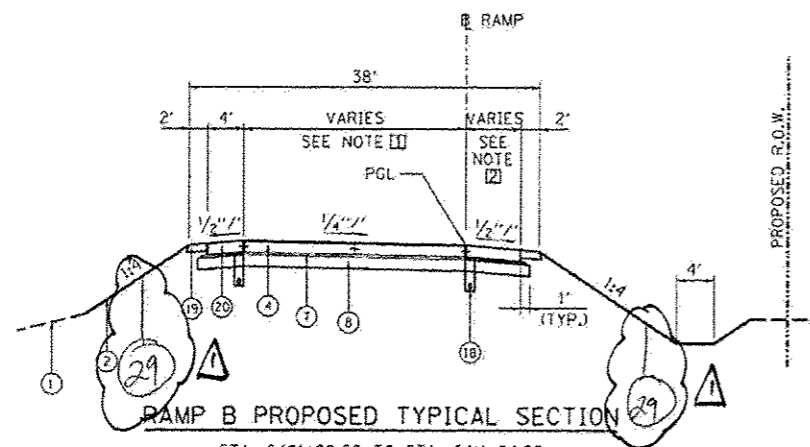
- NOTES:
- 1) PAVEMENT WIDTH
STA. 4400+00.00 TO STA. 4418+00.67 = (SEE I-57 TYPICAL SECTIONS)
STA. 4418+00.67 TO STA. 4421+15.08 = TRANSITION FROM 12' TO 16'
STA. 4421+15.08 TO STA. 4431+39.77 = 16'
 - 2) AGGREGATE SUBGRADE IMPROVEMENT
STA. 4418+00.67 TO STA. 4422+00.00 = 24"
STA. 4422+00.00 TO STA. 4431+39.77 = 12"



RAMP A PROPOSED TYPICAL SECTION

4431+39.77 - 4437+15.74

- NOTES:
- 1) TURNLANE
STA. 4431+39.77 TO STA. 4434+99.77 = 0'-24"
STA. 4434+99.77 TO STA. 4437+15.74 = 24'
 - 2) PAVEMENT WIDTH
STA. 4431+39.77 TO STA. 4433+59.01 = 16'
STA. 4433+59.01 TO STA. 4435+59.01 = 16'-24"
STA. 4435+59.01 TO STA. 4437+15.74 = 24'

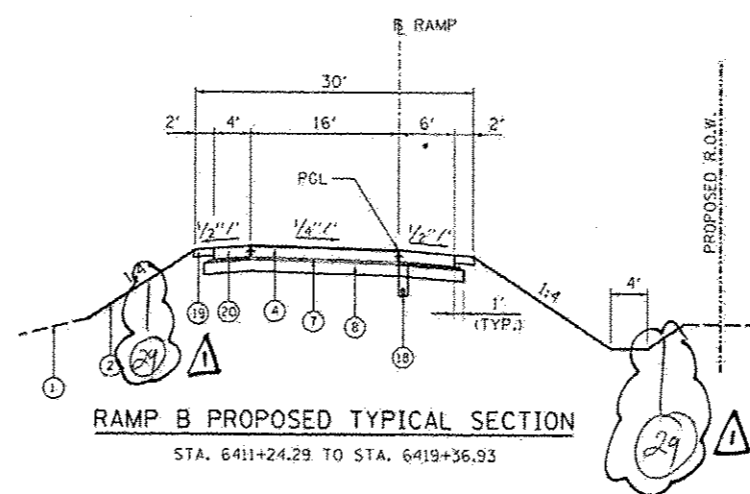


RAMP B PROPOSED TYPICAL SECTION

STA. 6401+92.00 TO STA. 6411+24.29

PROPOSED 6000N INTERSECTION
STA. 6400+47.00 TO STA. 6401+92.00

- NOTES:
- 1) PAVEMENT WIDTH
STA. 6401+92.00 TO STA. 6407+24.30 = 24'
STA. 6407+24.30 TO STA. 6411+24.29 = 24'-16'
 - 2) SHOULDER WIDTH
STA. 6401+92.00 TO STA. 6409+62.38 = 10'
STA. 6409+62.38 TO STA. 6410+44.44 = 10'-6"
STA. 6410+44.44 TO STA. 6411+24.29 = 6'



RAMP B PROPOSED TYPICAL SECTION

STA. 6411+24.29 TO STA. 6419+36.93

- NOTES:
- 1) AGGREGATE SUBGRADE IMPROVEMENT
STA. 6419+36.93 TO STA. 6436+86.87 = (SEE I-57 TYPICAL SECTION FOR CONTINUATION)
STA. 6411+24.29 TO STA. 6414+00.00 = 12"
STA. 6414+00.00 TO STA. 6419+36.93 = 24'

LEGEND

- 1) EXISTING GROUND LINE
- 2) GROUND LINE
- 3) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- 4) PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
- 5) PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
- 6) PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- 7) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- 8) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- 9) UNUSED
- 10) AGGREGATE BASE COURSE, TYPE A 6"
- 11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6-12
- 12) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6-24
- 13) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6-12 (TIP OUT)
- 14) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6-24 (TIP OUT)
- 15) CONCRETE MEDIAN SURFACE, 4 INCH
- 16) CONCRETE MEDIAN SURFACE, CORRUGATED
- 17) AGGREGATE SUBGRADE IMPROVEMENT, 24"
- 18) PIPE UNDERDRAINS, 4"
- 19) AGGREGATE SHOULDERS, TYPE B 6"
- 20) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- 21) PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
- 22) PORTLAND CEMENT CONCRETE SHOULDERS 10"
- 23) PORTLAND CEMENT CONCRETE SIDEWALK, 4"
- 24) AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
- 25) HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
- 26) HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
- 27) HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
- 28) HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

29) TOP SOIL EXCAVATION AND PLACEMENT (4")

I-57 RAMPS : 1140(34) INTERSTATE 5.41 (PCC-20)

Rev-1-20-15

| | |
|---------|----------|
| DATE | 12/03/13 |
| BY | MWH |
| CHECKED | DPA |
| DATE | 12/03/13 |

| | |
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| FILE NAME | D:\38710000\1140(34) I-57 TYPICAL |
| MODEL NAME | TYP9 |

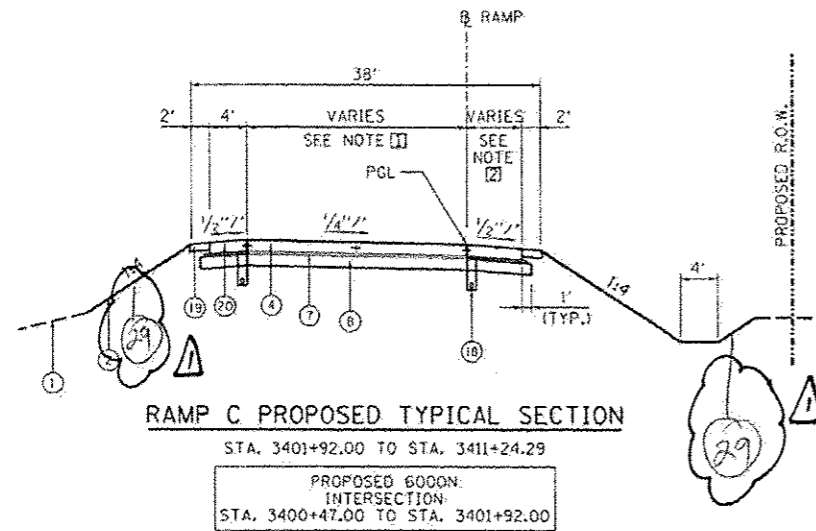
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|-----------|----------|
| USER NAME | MWH |
| DESIGNED | DPA |
| DRAWN | MWH |
| CHECKED | DPA |
| DATE | 12.03.13 |

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| REVISED | |
| REVISED | |
| REVISED | |
| REVISED | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

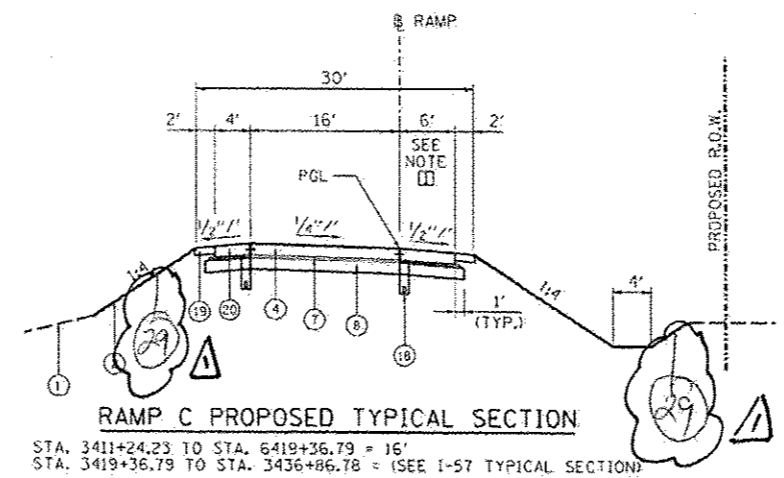
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| PROPOSED TYPICAL SECTIONS | | | |
| I-57 AND 6000N RD (BOURBONNAIS PARKWAY), | | | |
| BOURBONNAIS, IL | | | |
| SCALE: AS SHOWN | SHEET NO. | OF | SHEETS |
| | 57 | | 33 |

| | | | | |
|---|------------|----------|--------------|-----------|
| F.A.I. REL. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57 | 146-118K-1 | KANKAKEE | 819 | 33 |
| CONTRACT NO. 66982 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

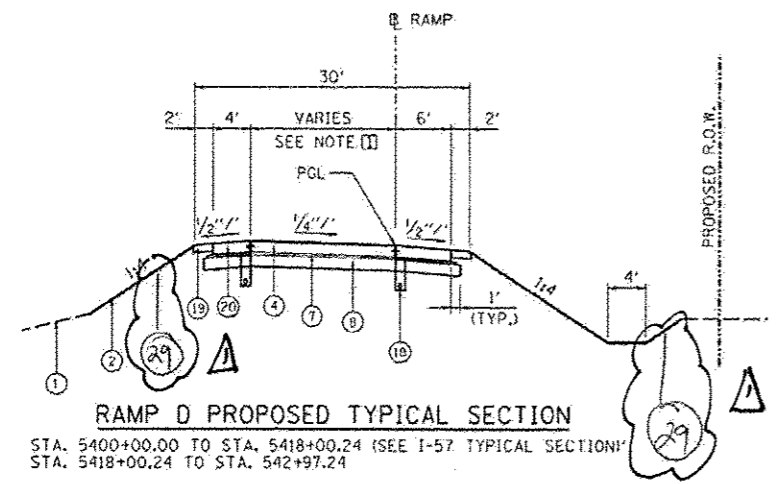


NOTES:
 PAVEMENT WIDTH
 STA. 3401+92.00 TO STA. 3407+24.23 = 24'
 STA. 3407+24.23 TO STA. 3411+24.23 = 24'-16'

SHOULDER WIDTH
 STA. 3401+92.00 TO STA. 3409+62.31 = 10'
 STA. 3409+62.31 TO STA. 3410+44.37 = TRANSITION FROM 10'-6"
 STA. 3410+44.37 TO STA. 3411+24.23 = 6'

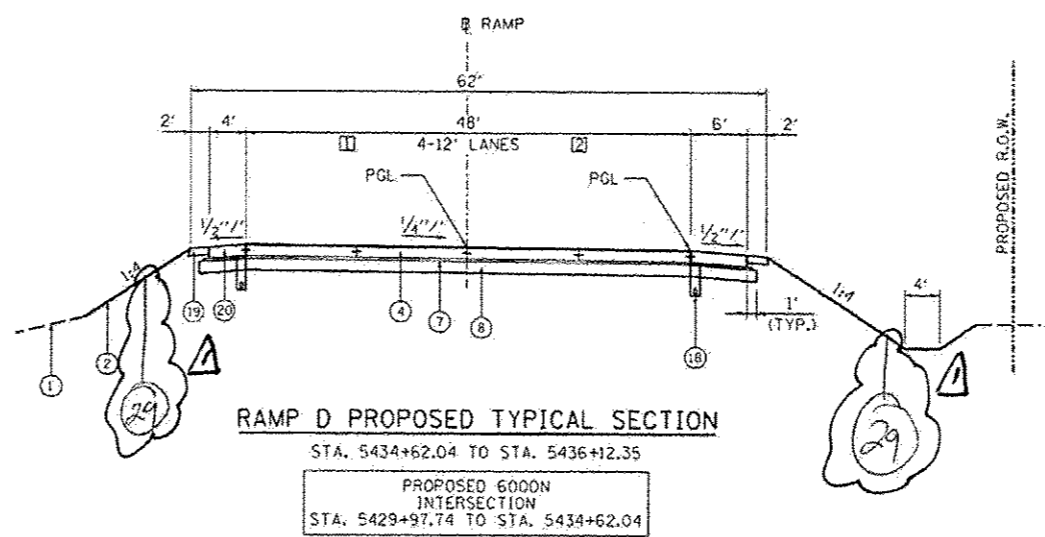


NOTES:
 AGGREGATE SUBGRADE IMPROVEMENT
 STA. 3411+24.23 TO STA. 3414+00.00 = 12"
 STA. 3414+00.00 TO STA. 3419+36.79 = 24"



NOTES:
 PAVEMENT WIDTH
 STA. 5418+00.24 TO STA. 5421+15.06 = 12' TO 16'
 STA. 5421+15.06 TO STA. 5429+97.74 = 16'

AGGREGATE SUBGRADE IMPROVEMENT
 STA. 5418+00.24 TO STA. 5423+00.00 = 24"
 STA. 5423+00.00 TO STA. 5429+97.74 = 12"



NOTES:
 PAVEMENT WIDTH
 STA. 5429+97.74 TO STA. 5433+57.74 = 0'-24"
 STA. 5433+57.74 TO STA. 5434+62.04 = 24'

TURN LANE
 STA. 5429+97.74 TO STA. 5432+43.78 = 16' TO 24"
 STA. 5432+43.78 TO STA. 5434+62.04 = 24'

- LEGEND**
- 1 EXISTING GROUND LINE
 - 2 GROUND LINE
 - 3 CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - 4 PORTLAND CEMENT CONCRETE PAVEMENT, 9" (JOINTED)
 - 5 PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - 6 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
 - 7 STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
 - 8 AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - 9 UNUSED
 - 10 AGGREGATE BASE COURSE, TYPE A 6"
 - 11 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - 12 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - 13 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (TIP OUT)
 - 14 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (TIP OUT)
 - 15 CONCRETE MEDIAN SURFACE, 4 INCH
 - 16 CONCRETE MEDIAN SURFACE, CORRUGATED
 - 17 AGGREGATE SUBGRADE IMPROVEMENT, 24"
 - 18 PIPE UNDERDRAINS 4"
 - 19 AGGREGATE SHOULDERS, TYPE B 6"
 - 20 PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 21 PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"
 - 22 PORTLAND CEMENT CONCRETE SHOULDERS 10"
 - 23 PORTLAND CEMENT CONCRETE SIDEWALK, 4"
 - 24 AGGREGATE FILL TO BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4"
 - 25 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N30, 2"
 - 26 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50, 2"
HOT-MIX ASPHALT BINDER, N50, 8"
 - 27 HMA SURFACE REMOVAL, 2"
HMA SURFACE COURSE, MIX C, N50, 2"
 - 28 HMA SURFACE REMOVAL, 2"
POLYMERIZED HMA SURFACE COURSE, MIX E, N90, 2"

24 TOPSOIL EXCAVATION AND PLACEMENT (4")

I-57 RAMPS : 1140(34) INTERSTATE 5.41 (PCC-20)

Rev. 1-20-15

| | |
|-----------|---------------------------|
| FILE NAME | 03279-0026-ahc-typica1081 |
| USER NAME | MWH |
| DESIGNED | DPA |
| DRAWN | MWH |
| CHECKED | DPA |
| DATE | 12.03.13 |

| | |
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| FILE NAME | 03279-0026-ahc-typica1081 |
| USER NAME | MWH |
| DESIGNED | DPA |
| DRAWN | MWH |
| CHECKED | DPA |
| DATE | 12.03.13 |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED TYPICAL SECTIONS
 I-57 AND 6000 RD (BOURBONNAIS PARKWAY)
 BOURBONNAIS, IL**

| | | | | |
|---------------------|------------|----------|---------------------------|-----------|
| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57 | 146-118K-1 | KANKAKEE | 819 | 34 |
| SCALE: AS SHOWN | | | SHEET NO. | OF SHEETS |
| SCALE: AS SHOWN | | | STA. | TO STA. |
| FED. ROAD DIST. NO. | | | ILLINOIS FED. AID PROJECT | |

CONTRACT NO. 66982