

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	HEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	1

ILLINOIS CONTRACT NO. 62Y26

D-91-147-25

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED IN
THE VILLAGE OF MONEE

DESIGN DESIGNATION
IL ROUTE 50 - MINOR ARTERIAL

TRAFFIC DATA
ADT: 5,800 VPD (2023)
POSTED SPEED: 45 MPH
DESIGN SPEED: 50 MPH



V3 Companies
7325 Janes Avenue
Woodridge, IL 60517
630.724.9200 phone
630.724.9202 fax
www.v3co.com



Accurate
GROUP, INC.



SWE SOLUTIONS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: MR. D. WILGREEN, P.E.
(847) 705-4240
PROJECT MANAGER: MR. J. ALAIN MIDY, P.E.
(847) 221-3056
CONTRACT NO. 62Y26

PROPOSED HIGHWAY PLANS

FAP ROUTE 840: IL RTE 50 (S. GOVERNORS HIGHWAY)

LIMIT: AT CN RAILROAD

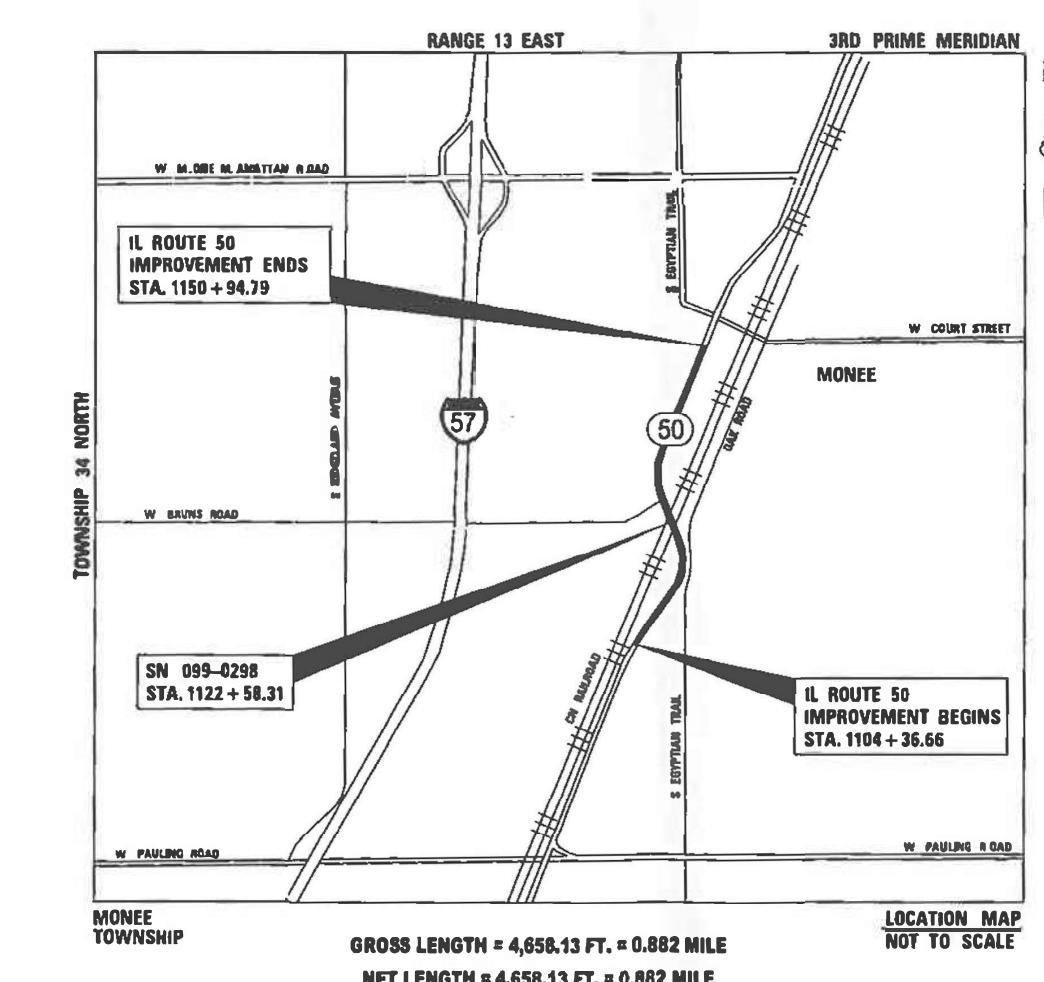
SECTION: (143VB-R-1(90)) BR 25

PROJECT: BR-I81L(103)

BRIDGE DECK OVERLAY AND JOINT REPLACEMENT

WILL COUNTY

C-91-222-25

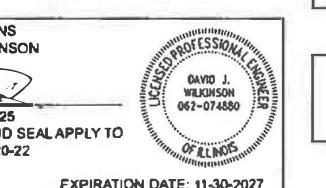
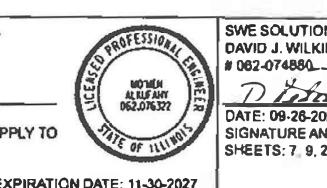
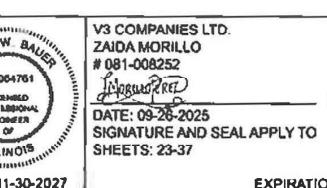
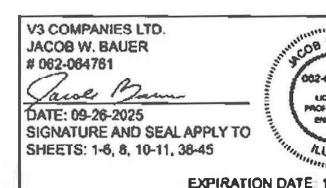


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *Dec 2nd 2025* *John Doe* IR
January 23 2026
REGIONAL ENGINEER

John Doe *EE-ET*
ENGINEER OF DESIGN AND ENVIRONMENT

January 23 2026
John Doe 5
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION



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OF THE STATE OF ILLINOIS

REV-SEP

INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES, INDEX OF SHEETS AND HIGHWAY STANDARDS
- 3 - 6 SUMMARY OF QUANTITIES
- 7 TYPICAL SECTIONS
- 8 SCHEDULE OF QUANTITIES
- 9 REMOVAL PLAN
- 10 ROADWAY PLAN
- 11 BRIDGE APPROACH DETAILS
- 12 MAINTENANCE OF TRAFFIC GENERAL NOTES AND SEQUENCE
- 13 MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
- 14 - 19 MAINTENANCE OF TRAFFIC PLAN
- 20 - 22 PAVEMENT MARKING PLAN
- 23 - 37 STRUCTURAL SHEETS (S.N. 099-0298)
 - 38 BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
 - 39 BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
 - 40 TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
 - 41 TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
 - 42 TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
 - 43 TC-14 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
 - 44 TC-16 SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
 - 45 TC-22 ARTERIAL ROAD INFORMATION SIGN

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED UTILITY FACILITIES. 48 HOUR NOTICE IS REQUIRED.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD CHECK ALL DIMESNONS AND ELEVATIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIAL OR BEGINNING CONSTRUCTION. ANY DISCREPANCIES FROM THE PLANS SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, WILL COUNTY, AND THE VILLAGE OF MONEE.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE APPROPRIATE DEPARTMENT.
5. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC FIELD AREA ENGINEER ERIC CAMPOS, AT ERIC.CAMPOS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
6. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
7. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH SHALL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE PROMPTLY REPAIRED AT THE CONTRACTOR'S EXPENSE.
8. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES AND SIDE ROADS AT ALL TIMES DURING CONSTRUCTION OPERATIONS, UNLESS OTHERWISE NOTED ON THE PLANS.
9. DURING CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN ALL SURFACE DRAINAGE WITHIN THE PROJECT LIMITS. ALL STORM FLOW MUST BE MAINTAINED AT ALL TIMES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
10. WHENEVER LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINES OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC., SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
11. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS NOT AUTHORIZED IN THE REMOVAL PLANS ARE TO BE REPLACED PROMPTLY AT THE CONTRACTOR'S EXPENSE.
12. BEFORE BEGINNING ANY WORK, THE CONTRCTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
13. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

COMMITMENTS

NONE

HIGHWAY STANDARDS

000001-09	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
442101-09	CLASS B PATCHES
606401-02	PAVED DITCH
630001-13	STEEL PLATE BEAM GUARDRAIL
701101-05	OFF-RD OPERATIONS, MULTILANE, 15" (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701423-10	LANE CLOSURE, MULTILANE, WITH BARRIER, FOR SPEEDS > 45 MPH TO 55 MPH
701431-13	LANE CLOSURE, MULTILANE, UNDIVIDED WITH CROSSOVER, FOR SPEEDS > 45 MPH TO 55 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRaversable MEDIAN
701611-01	URBAN ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701901-11	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

DISTRICT 1 STANDARD DETAILS

- BD-08 FRAMES AND LIDS ADJUSTMENT WITH MILLING
- BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
- TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
- TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC-14 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
- TC-16 SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
- TC-22 ARTERIAL ROAD INFORMATION SIGN

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	URBAN	CONSTRUCTION CODE	
					0059	
					80% FEDERAL 20% STATE	
	28000510	INLET FILTERS	EACH	2	2	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	382	382	
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	591	591	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	432	432	
	40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	84	84	
	42000060	WELDED WIRE REINFORCEMENT	SQ YD	81	81	
	44004000	PAVED DITCH REMOVAL	FOOT	80	80	
	44201053	CLASS B PATCHES, TYPE IV, 17 INCH	SQ YD	81	81	
	44201299	DOWEL BARS 1 1/2"	EACH	154	154	
	44213200	SAW CUTS	FOOT	367	367	
	50102400	CONCRETE REMOVAL	CU YD	23.8	23.8	
	50157300	PROTECTIVE SHIELD	SQ YD	609	609	
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	22.5	22.5	
	50300300	PROTECTIVE COAT	SQ YD	2,202	2,202	
	50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	3.4	3.4	
	50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	69	69	
	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3,010	3,010	
	50800515	BAR SPLICERS	EACH	12	12	
	52000110	PREFORMED JOINT STRIP SEAL	FOOT	186	186	



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USER NAME = Iyanisch
DESIGNED - LRY
DRAWN - DRP
CHECKED - JWB
PLOT DATE = 6/27/2025

REVISED -
REVISED -
REVISED -
REVISED -

DATE - 09/26/2025

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SCHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	3
				CONTRACT NO. 62Y26

ILLINOIS FED. AID PROJECT

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	URBAN	CONSTRUCTION CODE	
					0059	
					80%	FEDERAL 20% STATE
	52100520	ANCHOR BOLTS, 1"	EACH	3	3	
	58700300	CONCRETE SEALER	SQ FT	369	369	
	60615400	PAVED DITCH, TYPE A-15	FOOT	80	80	
*	63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	37.5	37.5	
	63200310	GUARDRAIL REMOVAL	FOOT	37.5	37.5	
	67100100	MOBILIZATION	L SUM	1	1	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	56	56	
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	15,360	15,360	
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	824	824	
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	825	825	
	70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	41,148	41,148	
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	825	825	
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	700	700	
	70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
	70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,702	9,702	
*	78004635	PREFORMED PLASTIC PAVEMENT MARKING - TYPE D - STANDARD - LINE 7"	FOOT	190	190	
*	78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,520	1,520	
*	78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	190	190	



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SCHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	4
				CONTRACT NO. 62Y26

ILLINOIS FED. AID PROJECT

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	URBAN	CONSTRUCTION CODE	
					0059	
					80%	20% STATE
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	12	12	
	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	8	8	
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	5,016	5,016	
	X0325748	ACRYLIC COATING	SQ YD	13	13	
	X0325749	FIBER WRAP	SQ FT	119	119	
	X2010516	SELECTIVE CLEARING	UNIT	1	1	
	X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	1,659	1,659	
	X5060700	CLEANING AND PAINTING BEARINGS	EACH	16	16	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	2	
	X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12	
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
	X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	169	169	
	X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	449	449	
*	X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	449	449	
	Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	2	2	
	53101410	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	2,158	2,158	
	53101002	BRIDGE DECK SCARIFICATION, 3/4 INCH	SQ YD	2,178	2,178	
	53212754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	53	53	
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	



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USER NAME = lyanisch

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REVISED -

PLOT DATE = 6/27/2025

DATE - 09/26/2025

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.P. RTE. 840	SECTION (143VB-R-1(90)) BR 25	COUNTY WILL	TOTAL SHEETS 45	SHEET NO. 5
CONTRACT NO. 62Y26				ILLINOIS FED. AID PROJECT

MODEL: Sheet View



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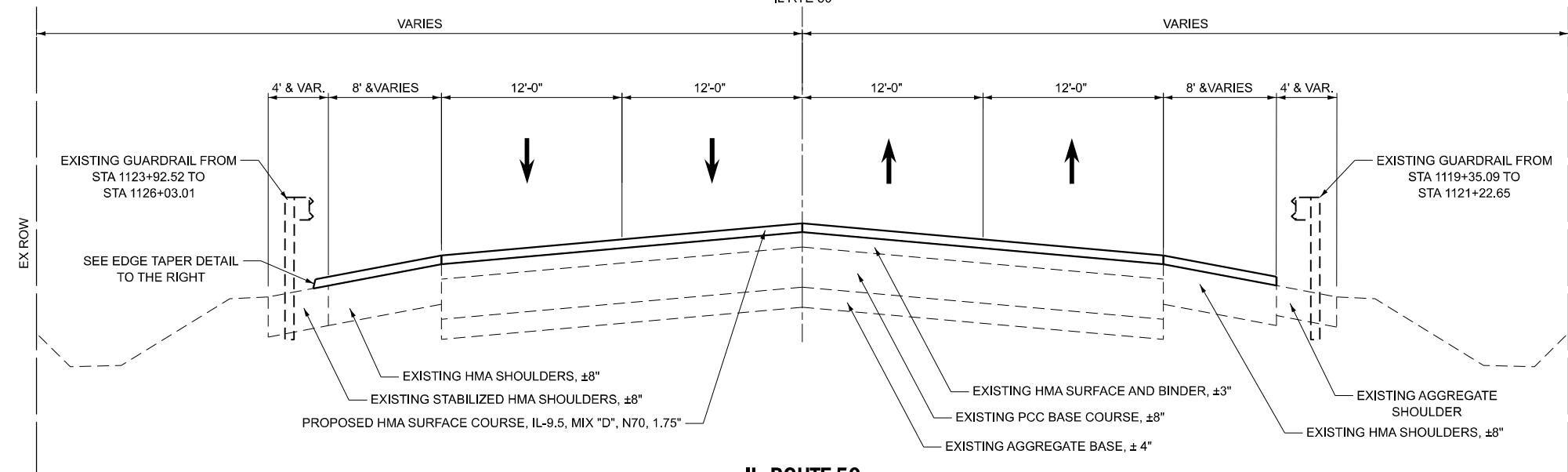
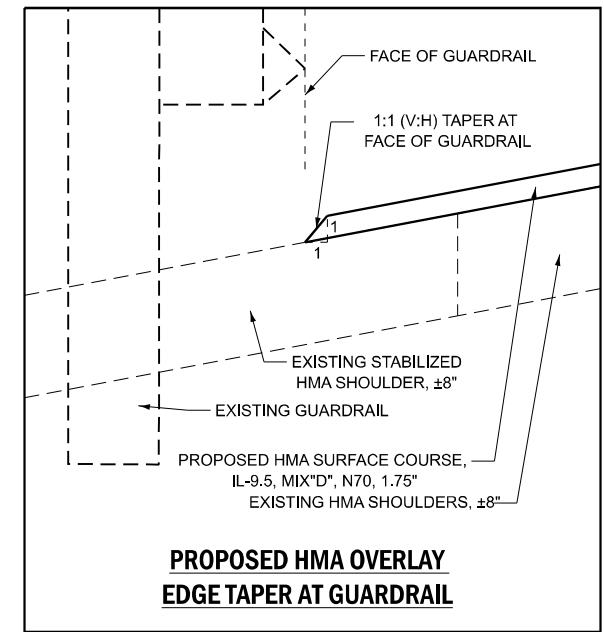
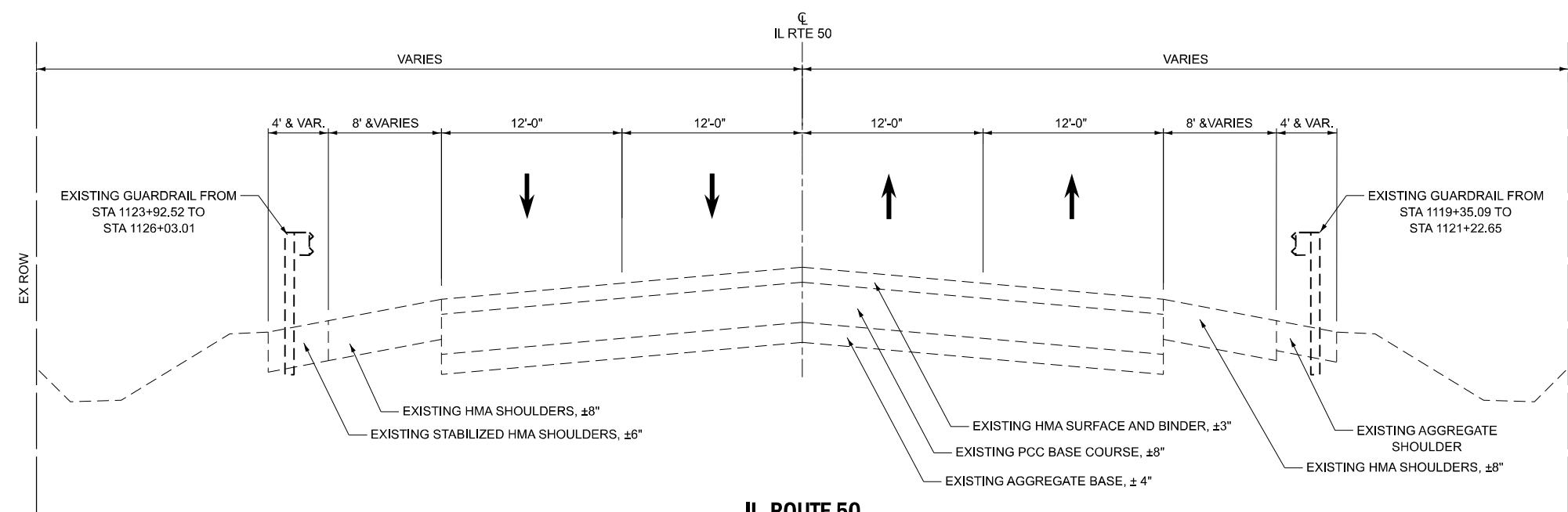
USER NAME = lyanisch	DESIGNED -	LRY	REVISED -
	DRAWN -	DRP	REVISED -
	CHECKED -	JWB	REVISED -
PLOT DATE = 6/27/2025	DATE -	09/26/2025	REVISED -



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES					F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	HEET NO.
					840	(143VB-R-1(90)) BR 25	WILL	45	6
					CONTRACT NO. 62Y26				
SCALE: NONE	SHEET 4	OF 4	SHEETS	STA.			ILLINOIS	FED. AID PROJECT	



HOT-MIX ASPHALT REQUIREMENTS

MIXTURE TYPE	AIR Voids @ Ndes	QMP DESIGNATION
IL ROUTE 50 SHOULDER / BRIDGE APPROACH OVERLAY / BUTT JOINT		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.75"	4% @ 70 GYR	QC/QA
QMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

NOTES

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIAL SPECIFICATIONS.

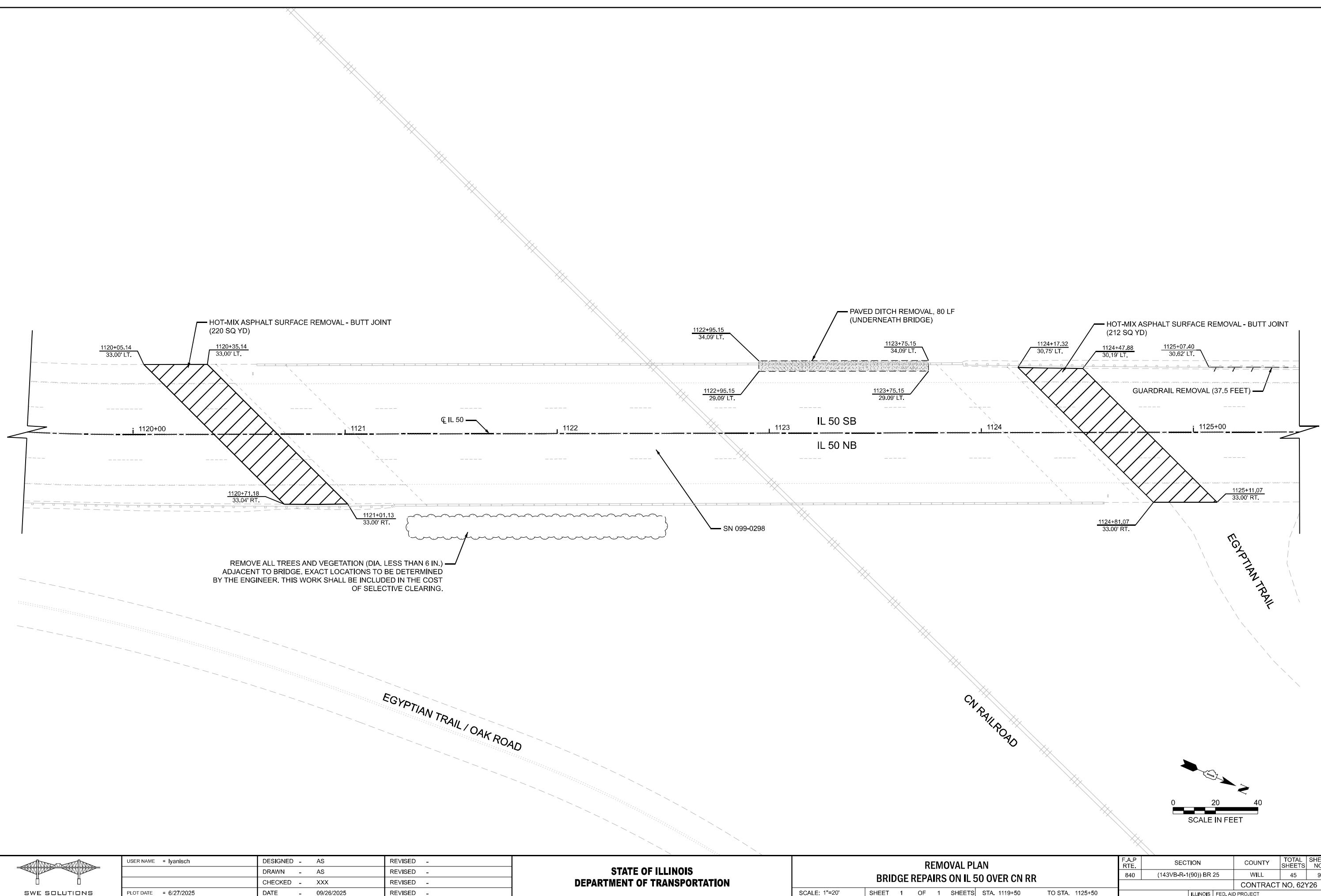
IL ROUTE 50 PAVEMENT MARKINGS SCHEDULE

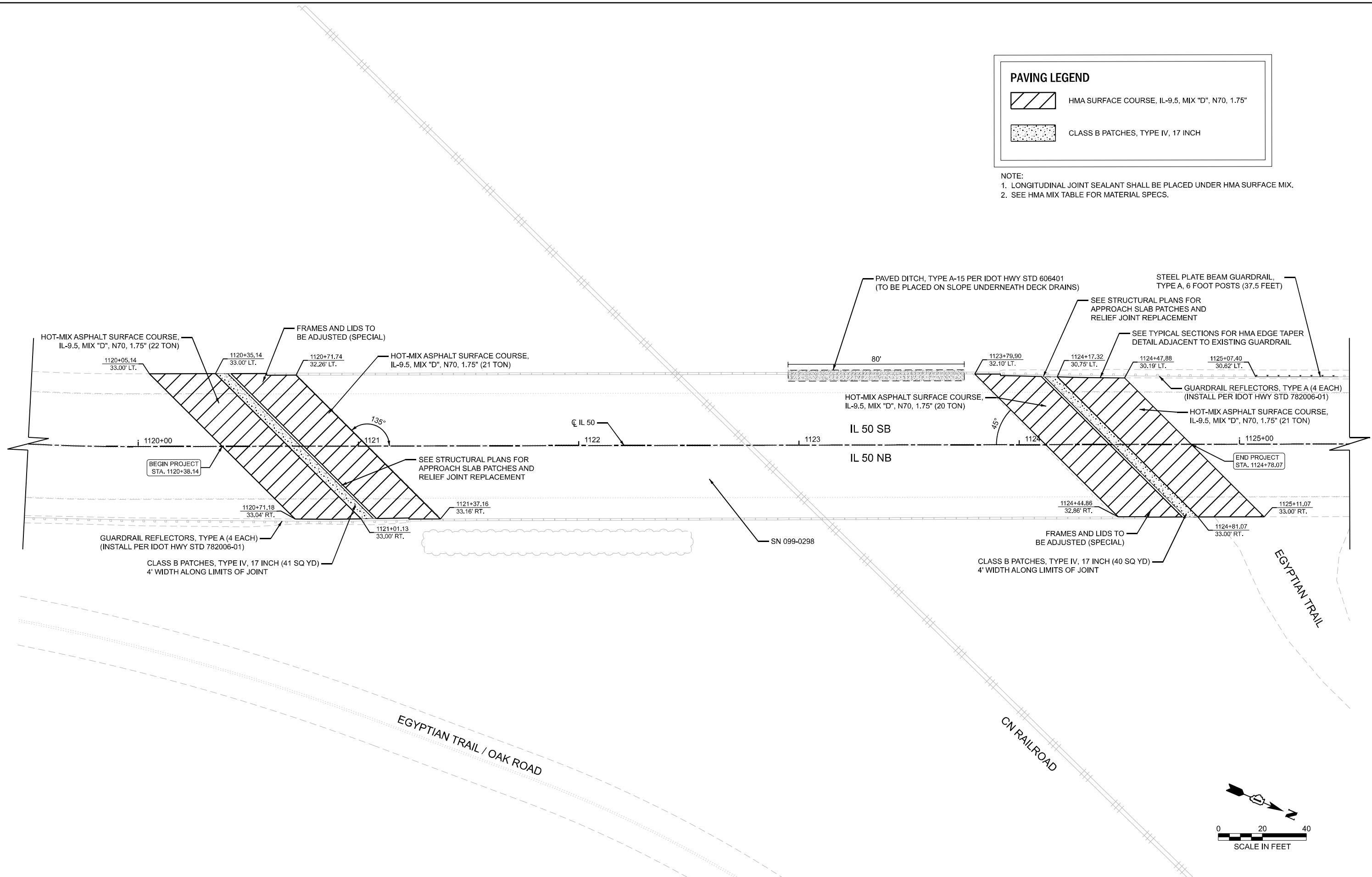
LOCATION		THERMOPLASTIC PAVEMENT MARKING - LINE 4"		MODIFIED URETHANE PAVEMENT MARKING - LINE 4"		PREFORMED PLASTIC PAVEMENT MARKING, TYPE D- STANDARD- LINE 7"	GROOVING FOR RECESSED PAVEMENT MARKING 8"
STATION	STATION	WHITE	YELLOW	WHITE	YELLOW	WHITE	FOOT
		FOOT		FOOT		FOOT	
SOUTH OF BRIDGE							
1099+39.90	1106+20.35	170					
1106+77.03	1120+80.36	351					
1113+95.90	1120+44.22	648					
1106+76.60	1120+70.96		2,783				
1110+70.75	1120+56.23	246					
1117+75.23	1120+92.19	317					
ON BRIDGE							
1120+92.15	1124+72.07			380			
1120+44.24	1124+24.74			381			
1120+80.30	1124+59.99					95	95
1120+56.24	1124+36.08					95	95
1120+68.12	1124+48.01				760		
1120+38.13	1124+78.04						
NORTH OF BRIDGE							
1124+34.34	1124+87.31	53					
1124+72.07	1129+85.21	513					
1124+17.32	1125+90.06	173					
1126+81.04	1130+49.98	369					
1124+71.96	1136+24.95	288					
1124+36.07	1141+41.85	426					
1124+48.04	1141+30.00		3,364				
TOTAL		9,702		1,520		190	190

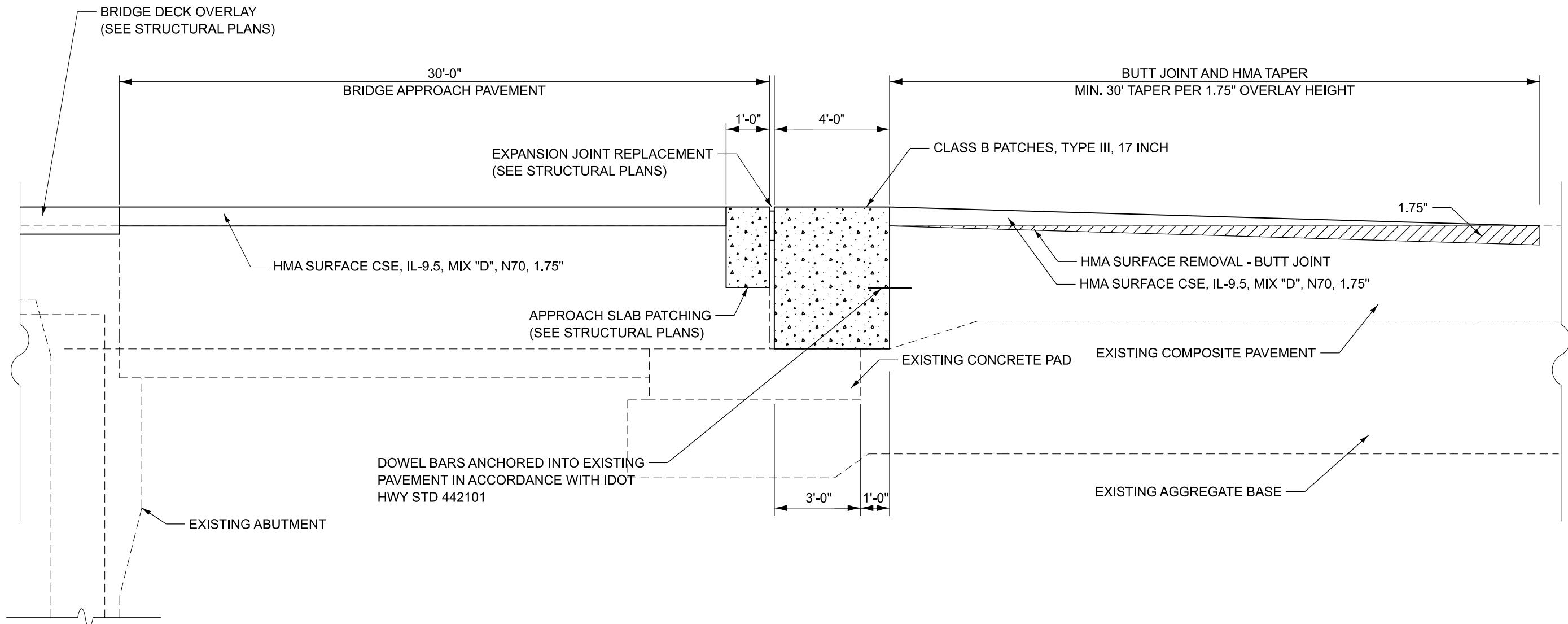
IL ROUTE 50 PAVEMENT MARKING REFLECTOR

SCHEDULE

LOCATION		RAISED REFELCTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT
STATION	STATION	EACH	EACH	EACH
SOUTH OF BRIDGE				
1105+10.00	1115+00.00		25	25
1115+00.00	1121+17.00		16	16
1105+10.00	1115+00.00		50	50
1115+00.00	1118+32.00		17	17
1099+40.00	1112+00.00		32	32
1109+60.00	1114+00.00		22	22
1114+00.00	1120+00.00		30	30
1110+60.00	1114+00.00		9	9
1114+00.00	1120+92.00		18	18
1120+38.14	1120+68.14	6		
NORTH OF BRIDGE				
1124+55.00	1136+25.00		30	30
1125+65.00	1130+00.00		22	22
1130+00.00	1137+25.00		37	37
1129+85.00	1130+00.00		1	1
1130+00.00	1136+25.00		16	16
1125+50.00	1129+00.00		18	18
1129+00.00	1141+30.00		62	62
1123+99.00	1141+30.00		44	44
1124+48.07	1124+78.07	6		
TOTAL		12	449	449







APPROACH SLAB HMA OVERLAY, JOINT REPAIR, AND HMA BUTT JOINT DETAILS



USER NAME = Iyanisch
DESIGNED - LRY
DRAWN - DRP
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BRIDGE APPROACH DETAILS
BRIDGE REPAIRS ON IL 50 OVER CN RR

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	11
				CONTRACT NO. 62Y26
				ILLINOIS FED. AID PROJECT

GENERAL NOTES

- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR KALPANA KANNAN-HOSADURGA, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING OF WORK.
- THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGE OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
- CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING AND/ OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEERING FOR APPROVAL PRIOR TO IMPLEMENTING ANY CHANGES.
- PAVEMENT MARKING TAPE, TYPE IV SHOWN ON THE PLANS FOR ANY CONSTRUCTION STAGE THAT THE CONTRACTOR PROPOSES TO EXTEND OVER THE WINTER PERIOD SHALL MEAN MODIFIED URETHANE PAVEMENT MARKING AND WILL BE PAID FOR UNDER TEMPORARY PAVEMENT MARKING - PAINT AT THE CONTRACT UNIT PRICE.
- PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED ON ALL FINAL PAVEMENT SURFACES AND EXISTING PAVEMENT SURFACES TO REMAIN.
- ALL EXISTING LANE LINE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED VIA WATER BLASTING WITH VACUUM RECOVERY IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 4 DAYS. THE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN KIND AFTER THE COMPLETION OF THE STAGING.
- TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS THAT ARE LOCATED WITHIN A WORK AREA. THIS WORK SHALL BE PAID FOR PER DISTRICT 1 DETAIL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS SIGN RELOCATION WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.
- THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL PHASES OF CONSTRUCTION.
- TEMPORARY CONCRETE BARRIER SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE IDOT STANDARD SPECIFICATIONS. ALL TEMPORARY CONCRETE BARRIER APPROACH AND DEPARTING END UNITS SHALL BE ANCHORED TO THE PAVEMENT USING SIX ANCHOR PINS AS SHOWN IN IDOT STANDARD 704001. PINNING OF ADDITIONAL BARRIER UNITS WITH THREE ANCHOR PINS ON THE TRAFFIC SIDE HOLES WITHIN THE INSTALLATION SHALL BE REQUIRED WHEN EQUIPMENT, VEHICLES, MATERIALS, FIXED OBJECTS, OR A DROP-OFF IS LOCATED WITHIN 24" BEHIND THE BARRIER. THE 24" OF CLEAR PAVEMENT MEASUREMENT SHALL BE FROM THE BASE OF THE NON-TRAFFIC SIDE OF THE BARRIER. TRAFFIC SIDE PINNED BARRIER SHALL HAVE A MINIMUM OF 6" OF CLEAR PAVEMENT BEHIND THE BARRIER. WHERE BOTH PINNED AND UNPINNED BARRIER UNITS ARE USED IN A CONTINUOUS INSTALLATION, A TRANSITION SHALL BE PROVIDED BETWEEN THEM. THE TRANSITION FROM PINNED TO UNPINNED BARRIER SHALL CONSIST OF TWO ANCHOR PINS INSTALLED IN THE END HOLES ON THE TRAFFIC SIDE OF THE FIRST BARRIER BEYOND THE PINNED SECTION AND ONE ANCHOR PIN INSTALLED IN THE MIDDLE HOLE OF THE TRAFFIC SIDE OF THE SECOND BARRIER BEYOND THE PINNED SECTION. THE THIRD BARRIER BEYOND THE PINNED SECTION SHALL THEN BE UNPINNED.
- WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICY MEMORANDUM 4-21. DROP-OFFS GREATER THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC, THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY AND ENSURE THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES/DRUMS AT THE END OF EACH WORKDAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. IF THE ABOVE REQUIREMENTS CAN'T BE MET, AND IT IS DETERMINED THAT OVERNIGHT LANE CLOSURES AND/OR TEMPORARY CONCRETE BARRIER WALL INSTALLATION WILL BE NECESSARY, THEN IDOT WRITTEN APPROVAL WILL BE REQUIRED PRIOR TO THE INSTALLATION OF THESE ITEMS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT. WHERE POSITIVE PROTECTION (TEMPORARY CONCRETE BARRIER PER STD. 704001) IS PROVIDED, THIS REQUIREMENT IS NULLIFIED.

SUGGESTED SEQUENCE OF CONSTRUCTION AND MAINTENANCE OF TRAFFIC

THE FOLLOWING SEQUENCE OF CONSTRUCTION AND MAINTENANCE OF TRAFFIC IS SUGGESTED. VARIATIONS MAY BE MADE WITH THE APPROVAL OF THE ENGINEER. FOR EACH STAGE OF CONSTRUCTION, PROVIDE TRAFFIC CONTROL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS. COORDINATE INSTALLATION OF TRAFFIC CONTROL DEVICES WITH THE EXISTING TRAFFIC PATTERNS AT THE ENDS OF THE PROJECT ALONG IL ROUTE 50 AND IMPACTED CROSS STREETS. THE IMPROVEMENTS WILL BE CONSTRUCTED USING LANE CLOSURES AND CROSSOVERS PER THE PROJECT MAINTENANCE OF TRAFFIC PLANS DISTRICT ONE STANDARDS TC-10, TC-11, TC-13, TC-14, TC-16, TC-22 AND TC-26, AND HIGHWAY STANDARDS 701101, 701423, 701431, 701601, 701611, 701901, 704001 AND 781001.

STAGE 1

- CONTRACTOR WILL REMOVE CONFLICTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS, THEN INSTALL TEMPORARY PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES AS SHOWN ON MOT PLANS. DURING THIS STAGE TRAFFIC WILL BE SHIFTED EAST TO THE NORTHBOUND ROADWAY.
- DURING THIS STAGE, THE CONTRACTOR WILL COMPLETE WEST SIDE STRUCTURAL REPAIRS INCLUDING BRIDGE DECK OVERLAY, JOINT REMOVAL AND REPLACEMENT, SUBSTRUCTURE REPAIRS, AND MILL AND OVERLAY OF THE APPROACH PAVEMENT AND HOT-MIX ASPHALT ROADWAY.

STAGE 2

- CONTRACTOR WILL REMOVE CONFLICTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS, THEN INSTALL TEMPORARY PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES AS SHOWN ON MOT PLANS. DURING THIS STAGE TRAFFIC WILL BE SHIFTED WEST TO THE SOUTHBOUND ROADWAY.
- DURING THIS STAGE, THE CONTRACTOR WILL COMPLETE EAST SIDE STRUCTURAL REPAIRS INCLUDING BRIDGE DECK OVERLAY, JOINT REMOVAL AND REPLACEMENT, SUBSTRUCTURE REPAIRS, AND MILL AND OVERLAY OF THE APPROACH PAVEMENT AND HOT-MIX ASPHALT ROADWAY.



USER NAME = Iyanisch
DESIGNED - MA
DRAWN - MA
CHECKED - JMT
PLOT DATE = 6/27/2025

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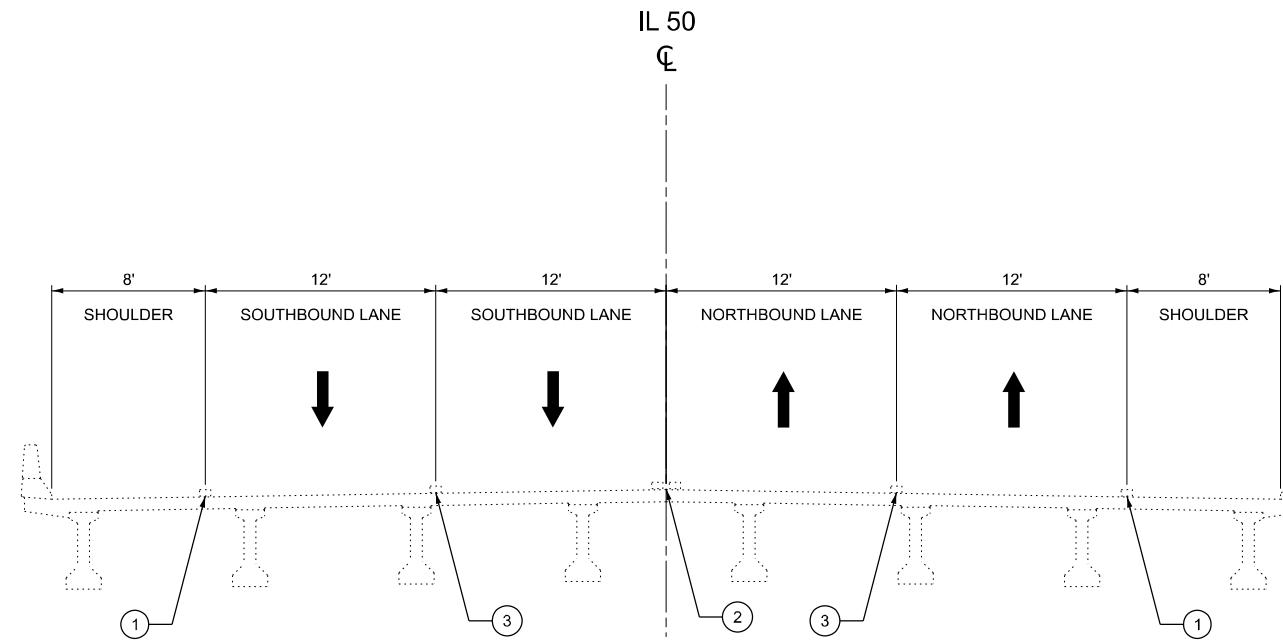
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MOT - GENERAL NOTES AND SEQUENCE
BRIDGE REPAIRS ON IL 50 OVER CN RR

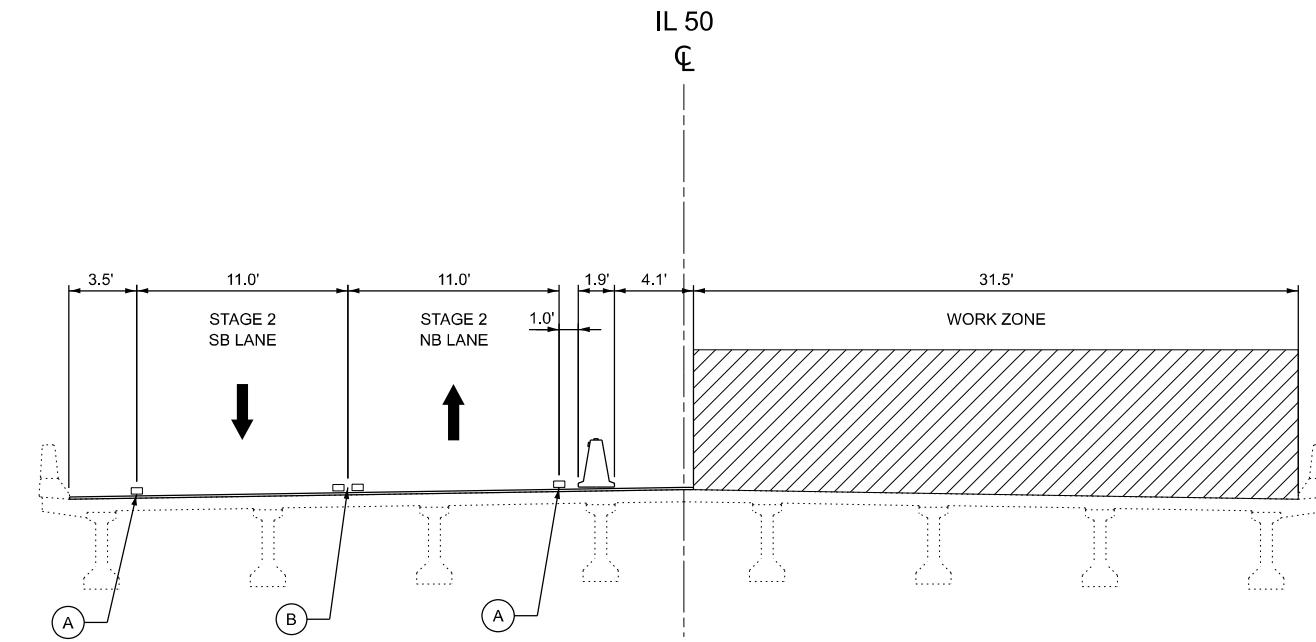
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE. 840	SECTION (143VB-R-1(90)) BR 25	COUNTY WILL	TOTAL SHEETS 45	SHEET NO. 12
				CONTRACT NO. 62Y26
				ILLINOIS FED. AID PROJECT



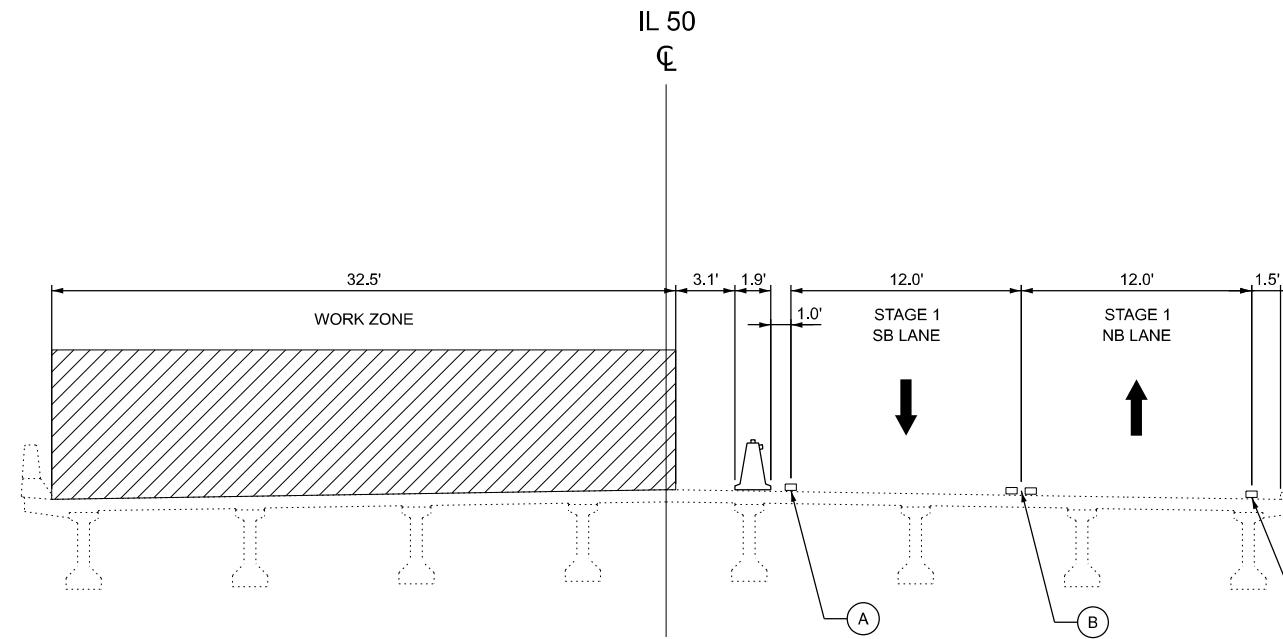
EXISTING TYPICAL SECTION

STA. 1100+00 TO STA. 1150+85
(LOOKING NORTH)



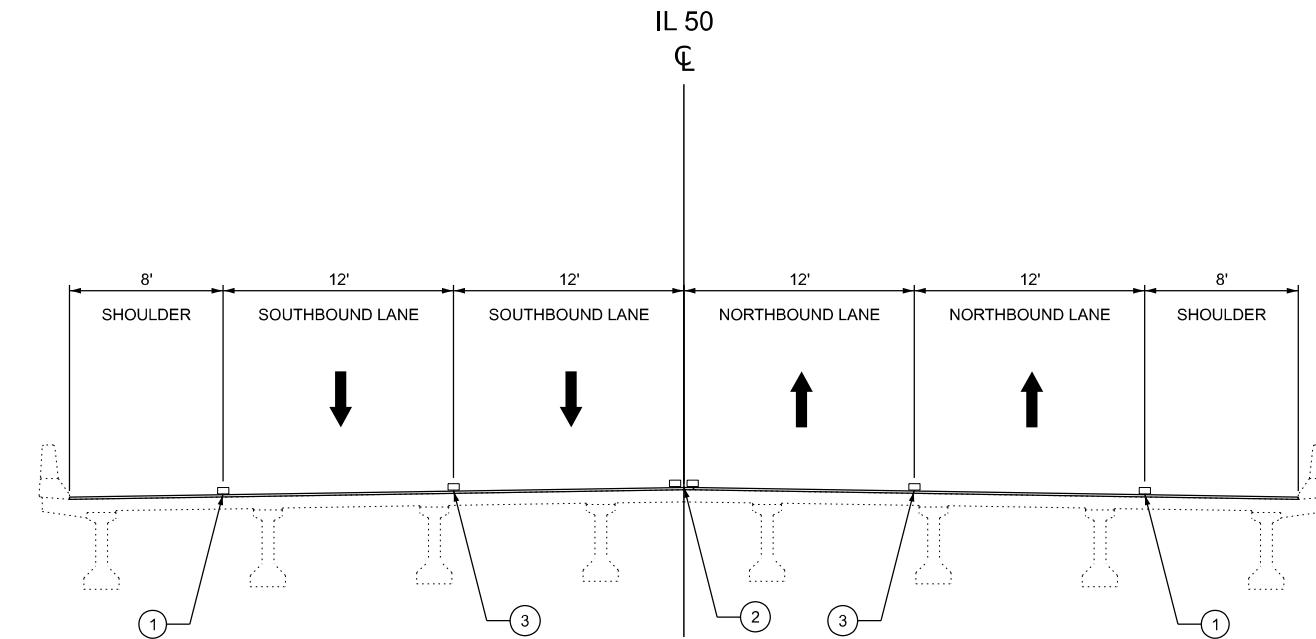
STAGE 2 TYPICAL SECTION

STA. 1100+00 TO 1141+50
(LOOKING NORTH)



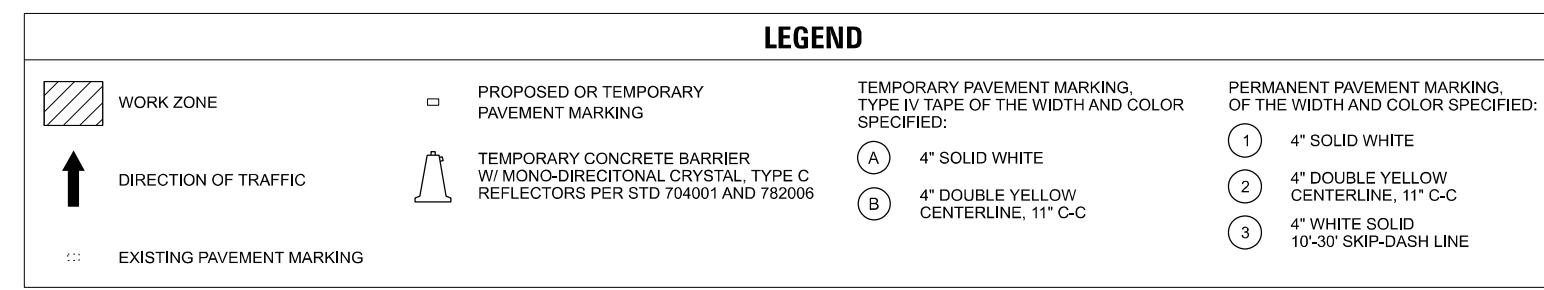
STAGE 1 TYPICAL SECTION

STA. 1105+10 TO STA. 1150+85
(LOOKING NORTH)



PROPOSED TYPICAL SECTION

STA. 1100+00 TO STA. 1150+85
(LOOKING NORTH)



USER NAME = Iyanisch
DESIGNED - MA
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CHECKED - JMT
PLOT DATE = 6/27/2025

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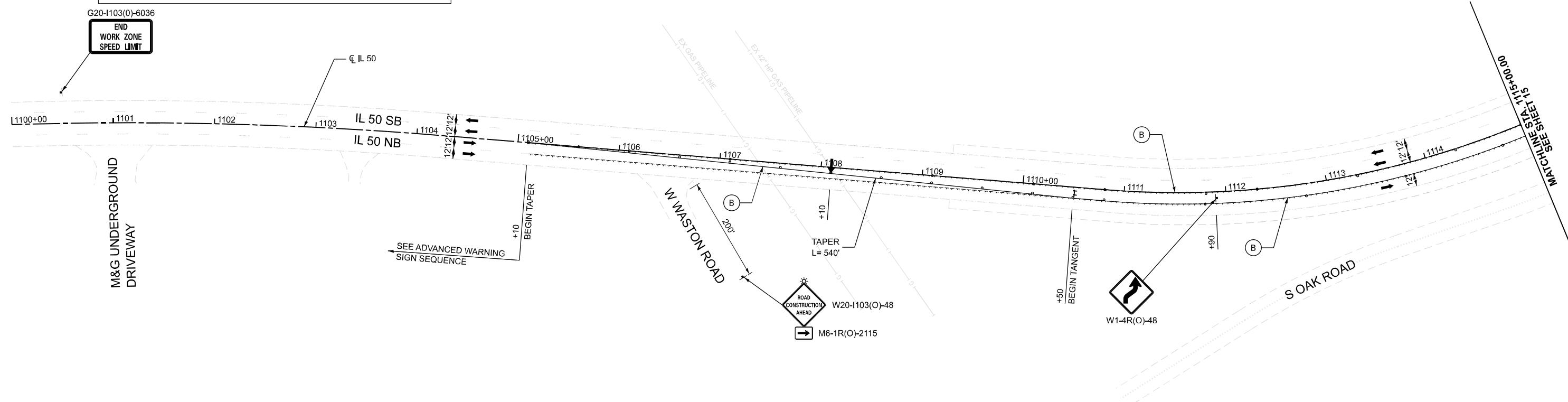
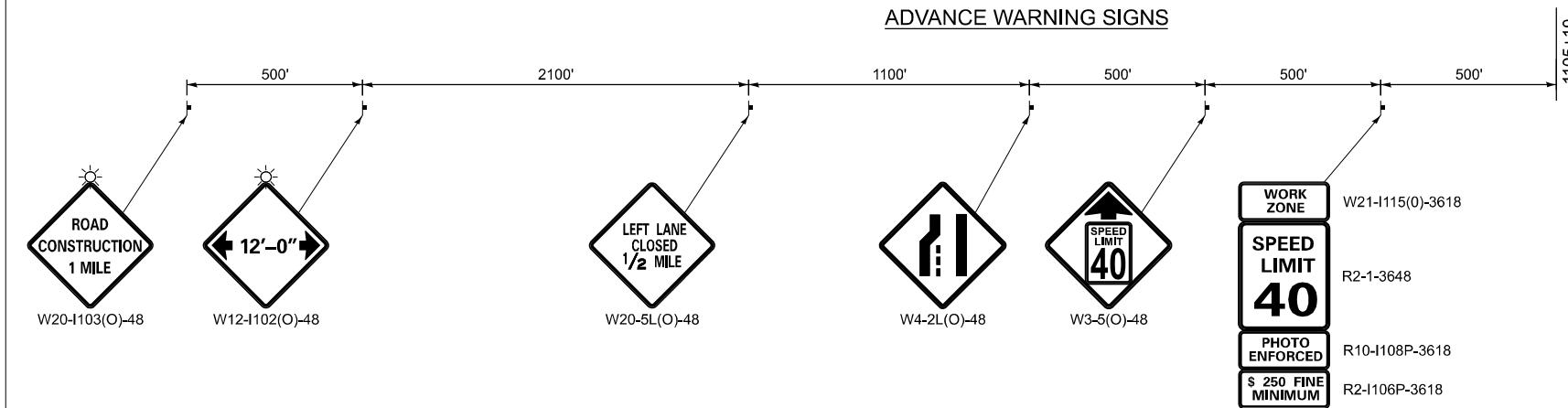
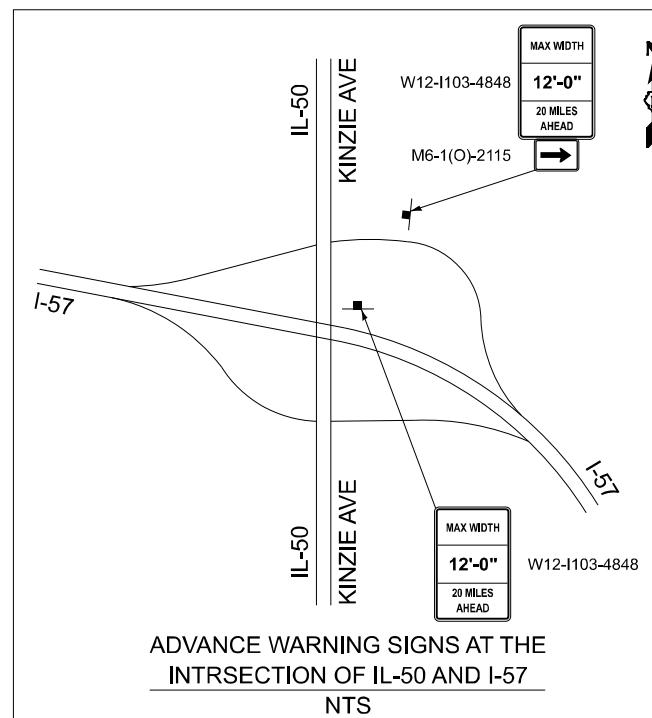
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MOT PLAN - TYPICAL SECTIONS
BRIDGE REPAIR ON IL 50 OVER CN RR

SCALE: 1"=5 SHEET 1 OF 1 SHEETS STA. TO STA.

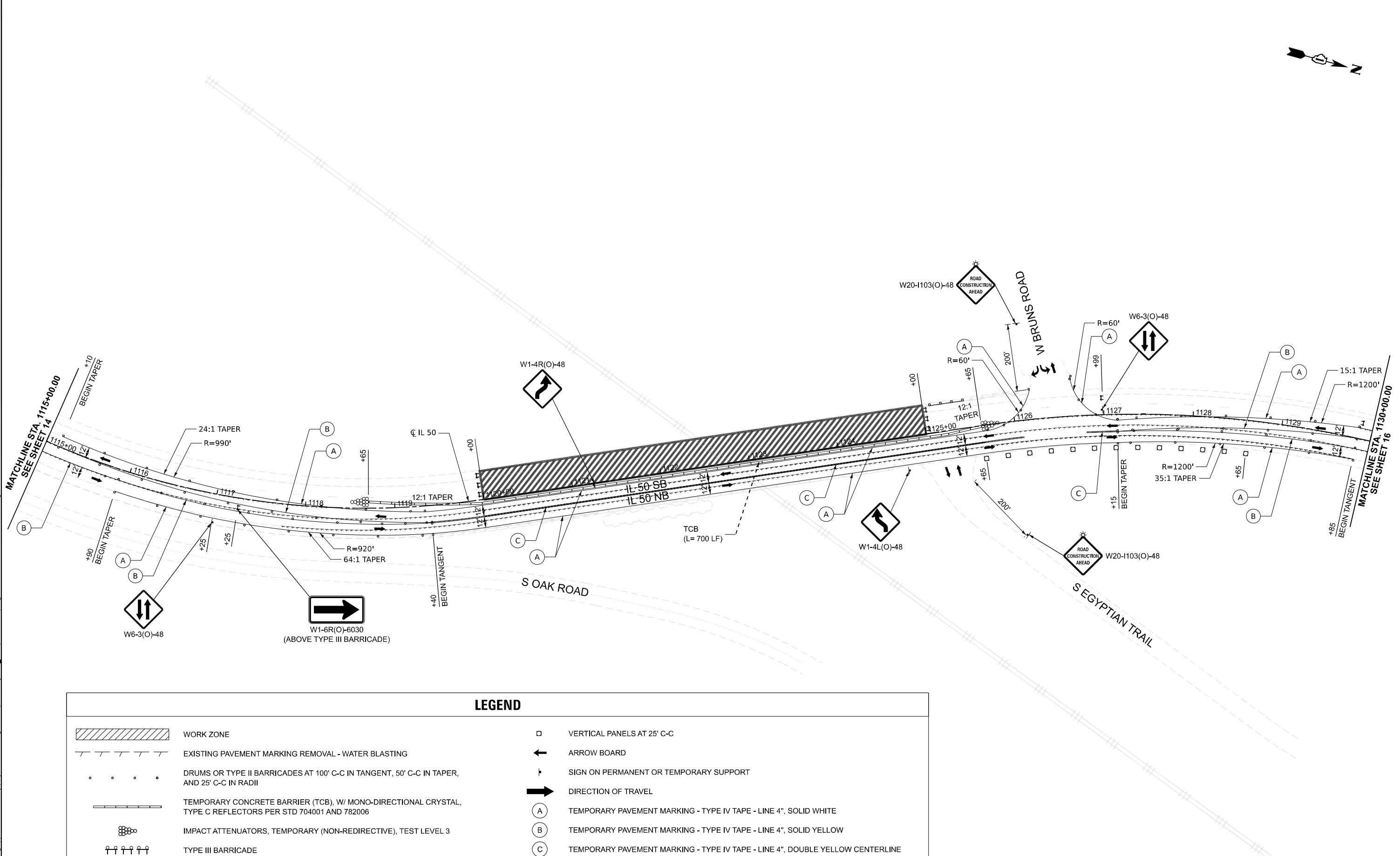
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	13
				CONTRACT NO. 62Y26

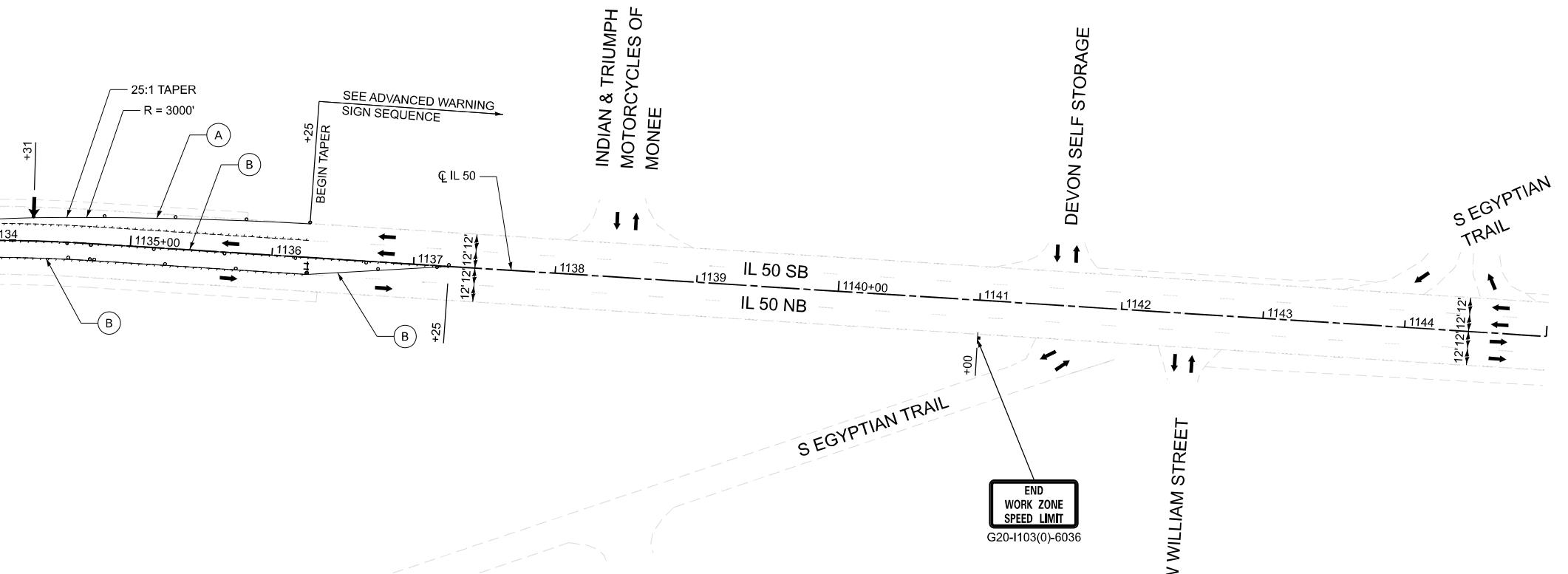
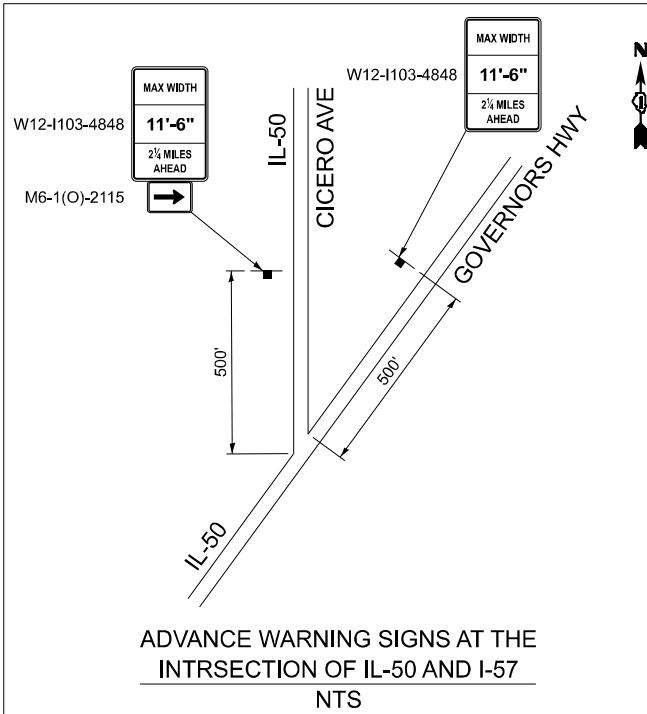
ILLINOIS FED. AID PROJECT



LEGEND

	WORK ZONE
	EXISTING PAVEMENT MARKING REMOVAL - WATER BLASTING
	DRUMS OR TYPE II BARRICADES AT 100' C-C IN TANGENT, 50' C-C IN TAPER, AND 25' C-C IN RADII
	TEMPORARY CONCRETE BARRIER (TCB), W/ MONO-DIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 AND 782006
	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
	TYPE III BARRICADE
	ARROW BOARD
	SIGN ON PERMANENT OR TEMPORARY SUPPORT
	DIRECTION OF TRAVEL
	A TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID WHITE
	B TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID YELLOW
	C TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", DOUBLE YELLOW CENTERLINE

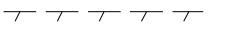




LEGEND



WORK ZONE

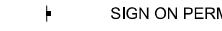


EXISTING PAVEMENT MARKING REMOVAL - WATER BLASTING
DRUMS OR TYPE II BARRICADES AT 100' C-C IN TANGENT. 50' C-C

TEMPORARY CONCRETE BARRIER (TCB), W/ MONO-DIRECTIONAL CRYSTAL
TYPE C REFLECTORS PER STD 701001 AND 782006



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■ SIGN ON PERMANENT OR TEMPORARY SUPPORT



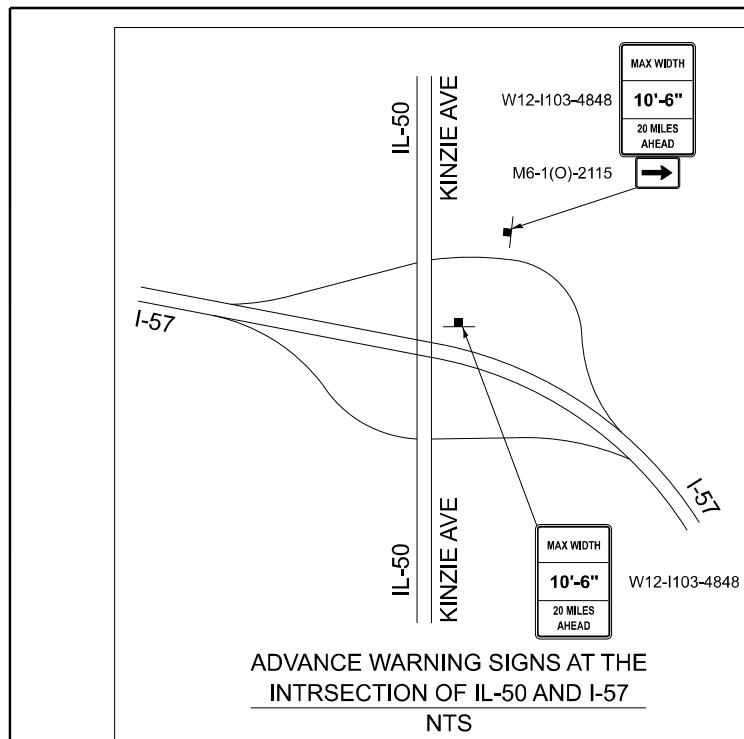
 DIRECTION OF TRAVEL



A TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID WHITE

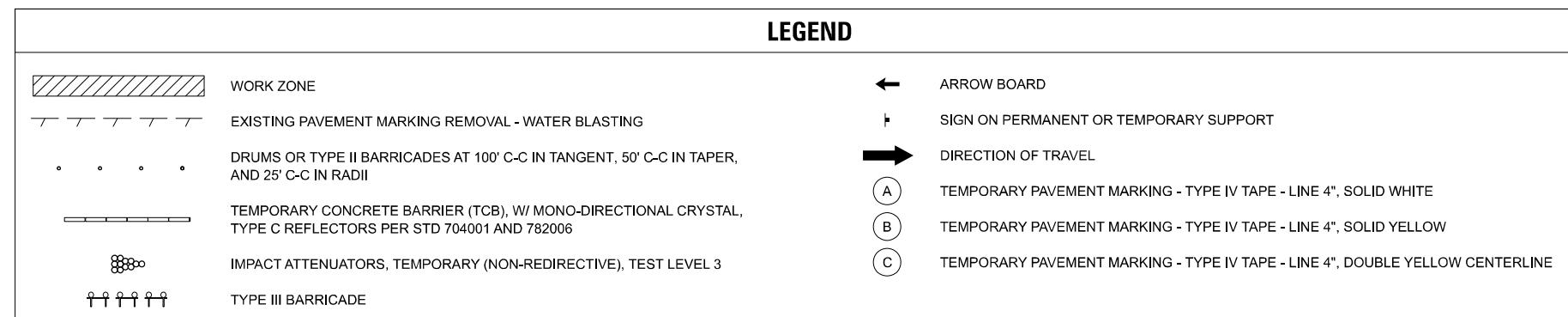
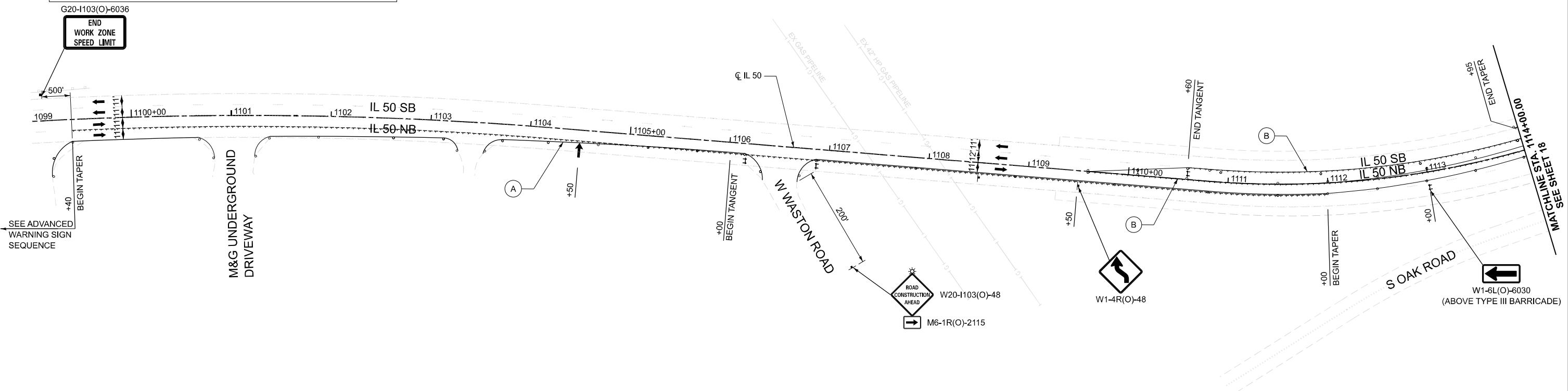


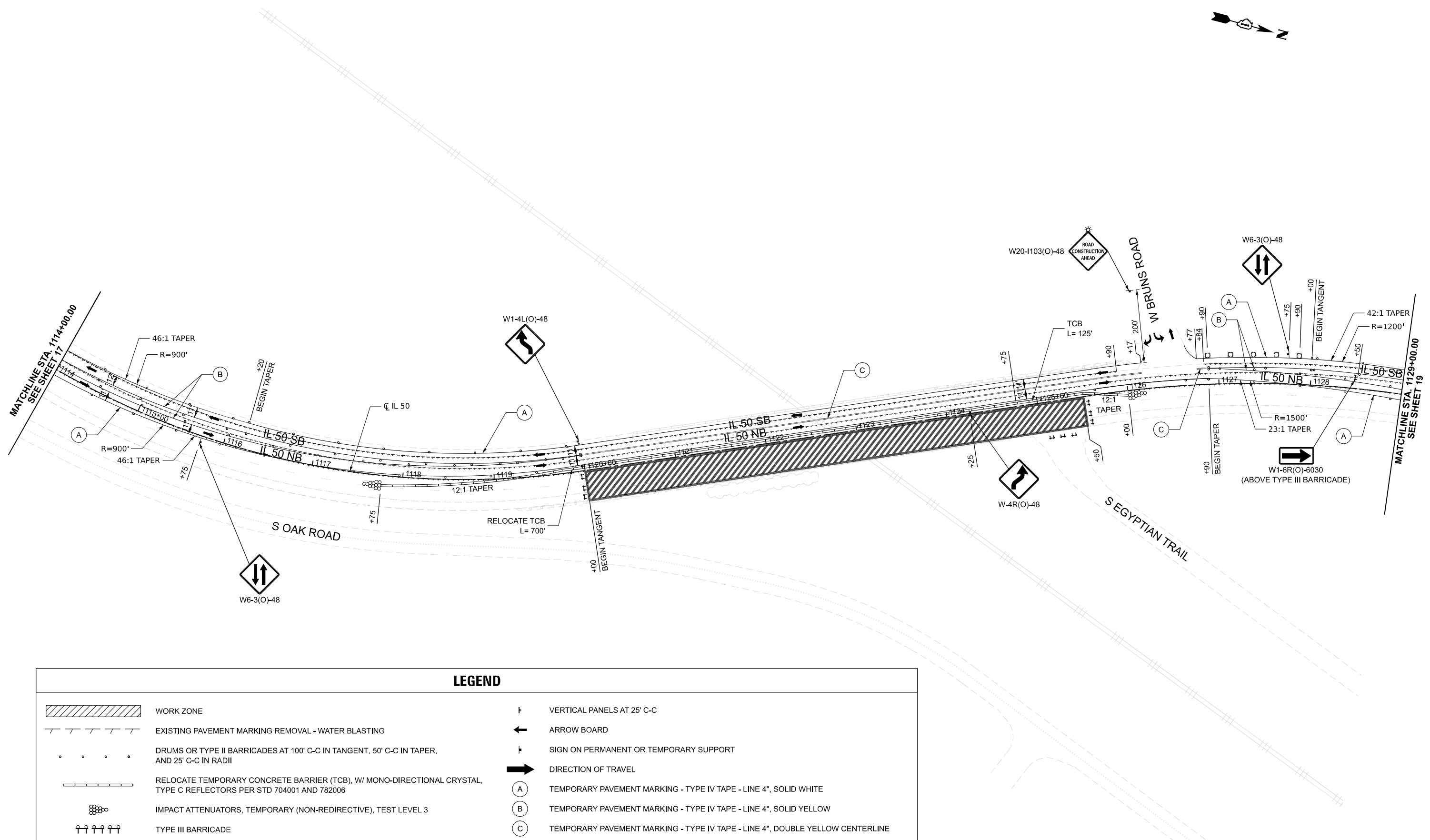
B TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID YELLOW

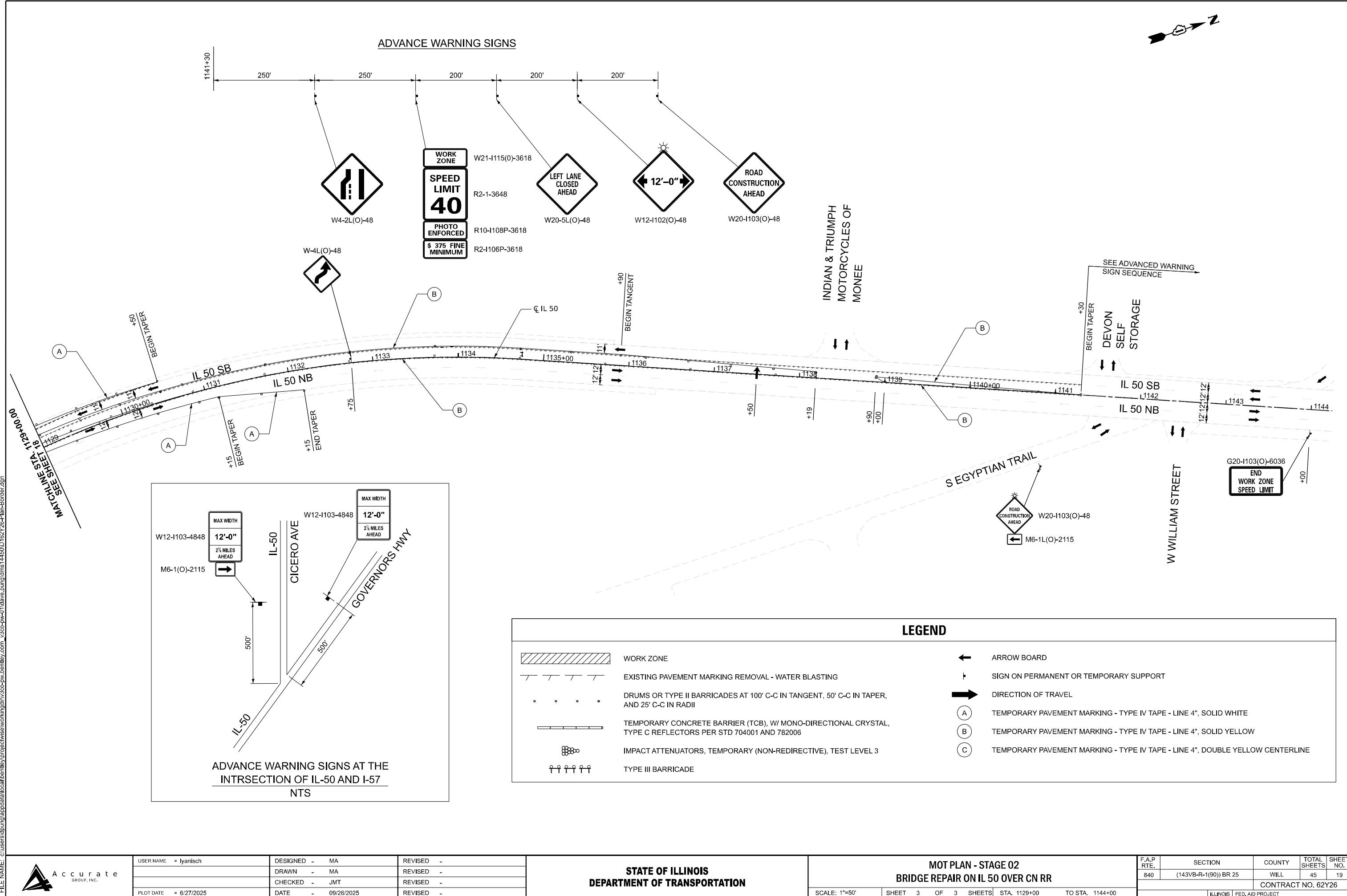


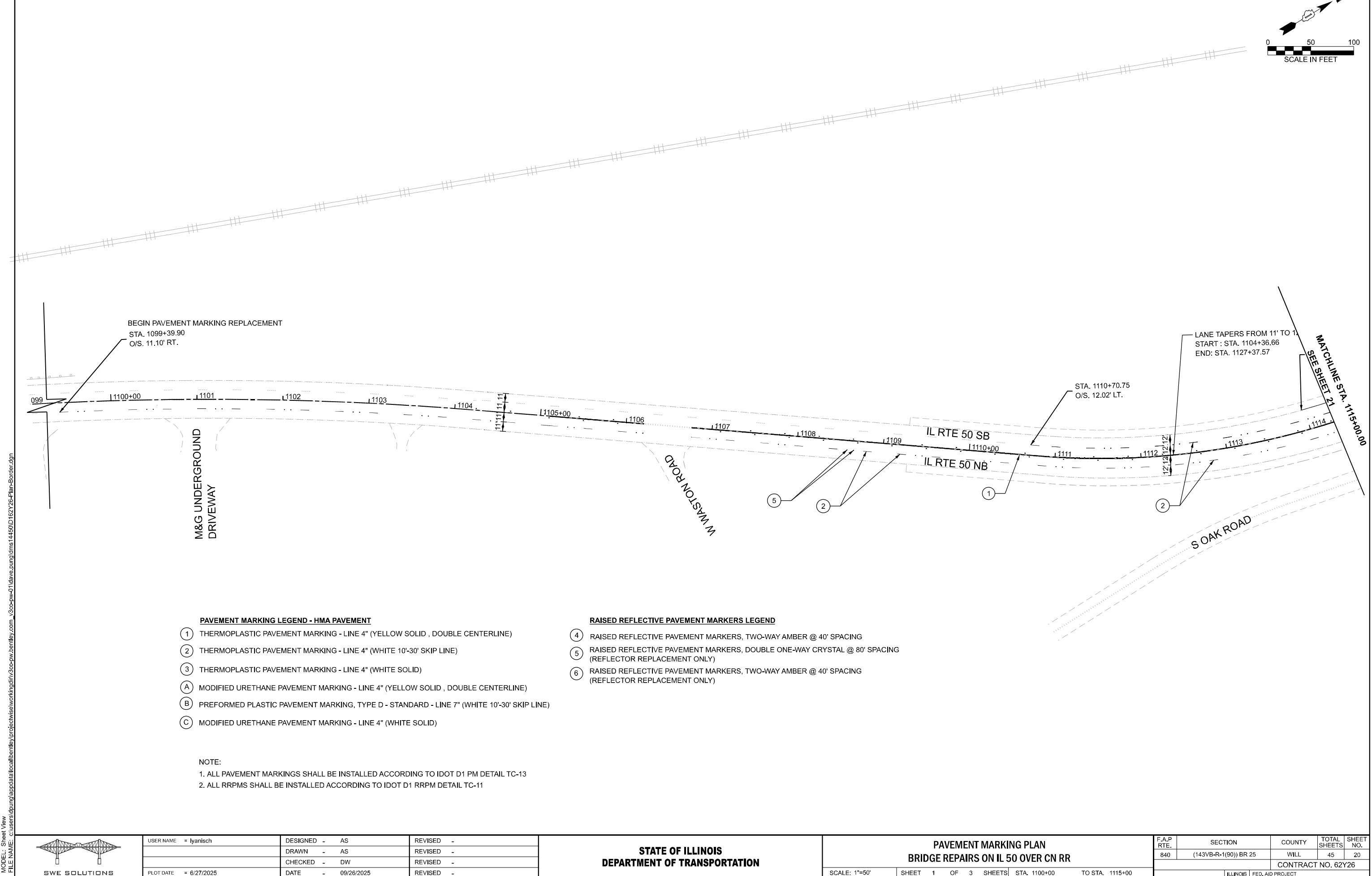
ADVANCE WARNING SIGNS AT THE
INTERSECTION OF IL-50 AND I-57
NTS

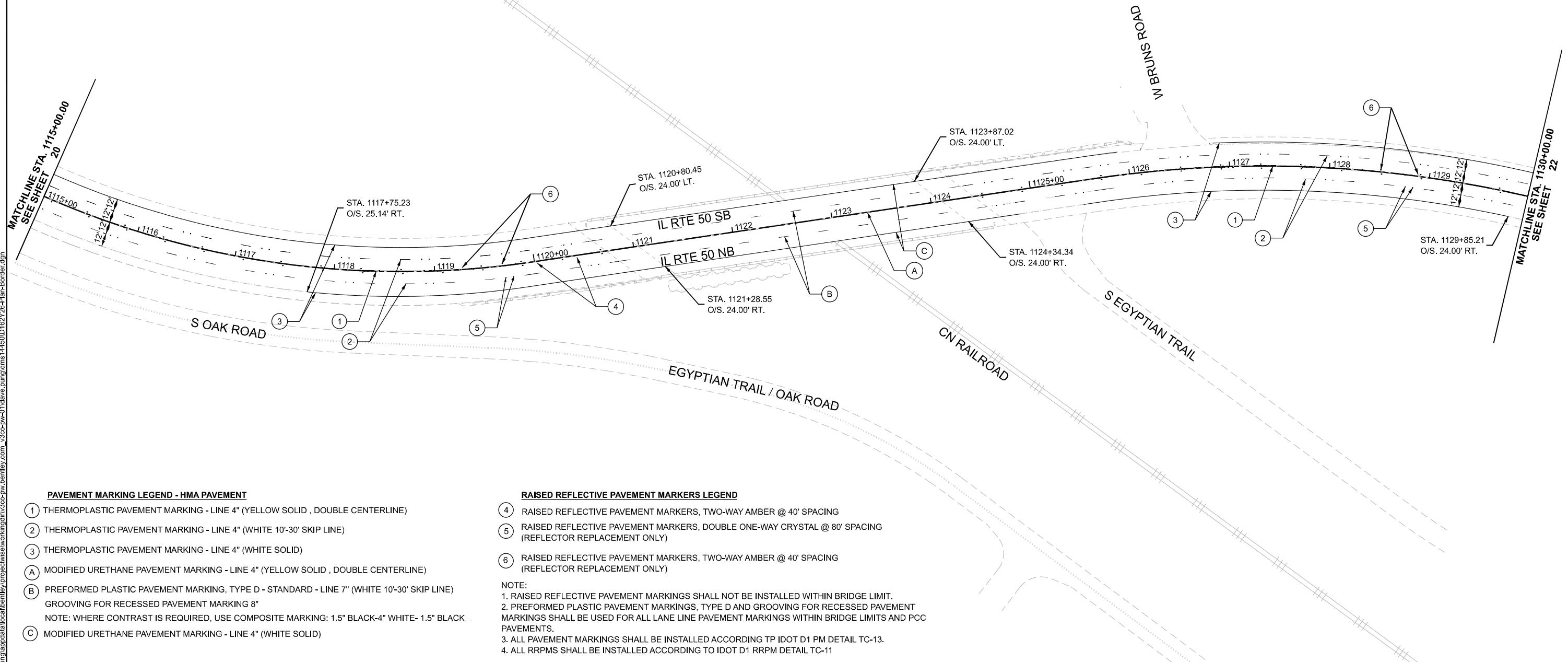
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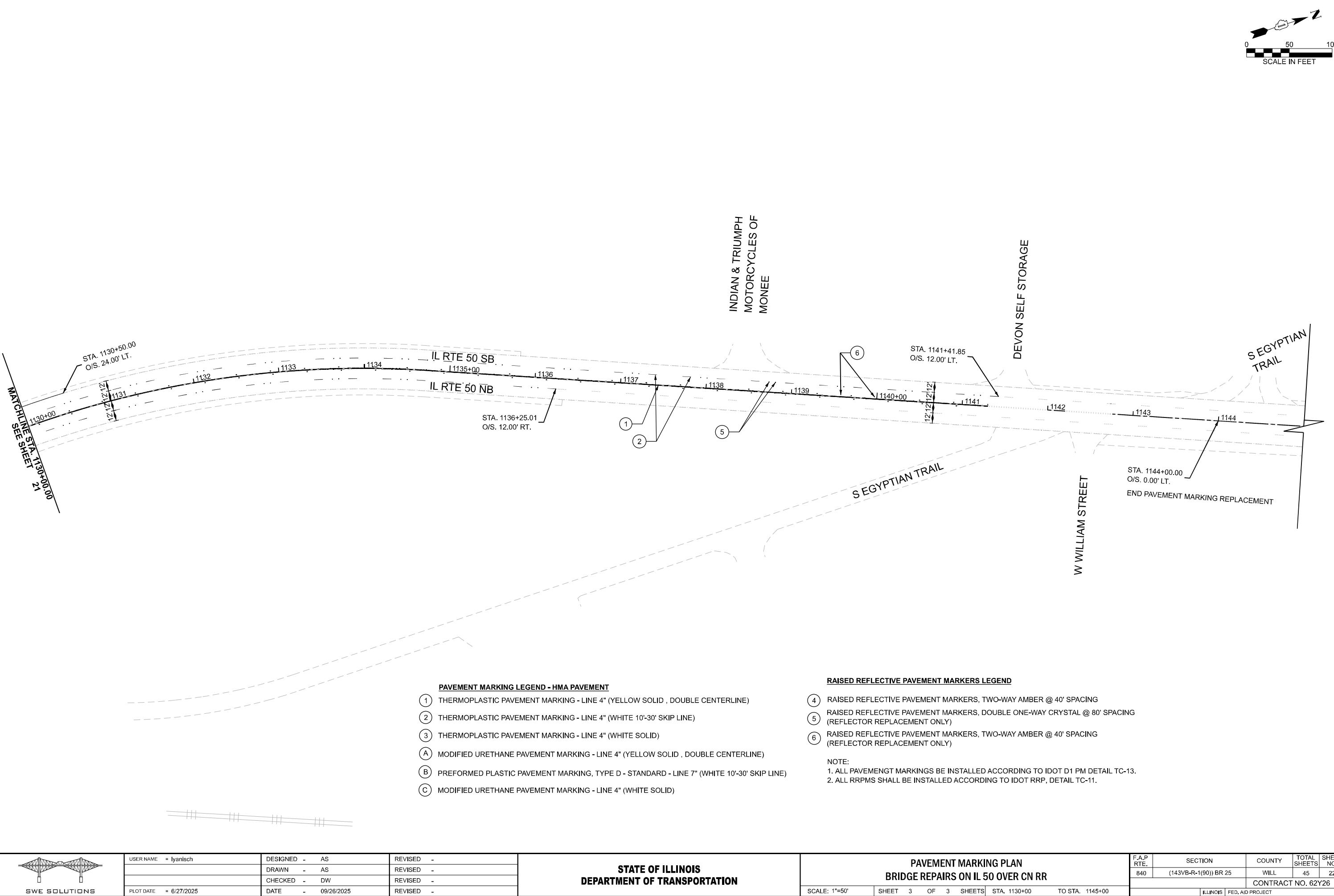












GENERAL NOTES

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into the concrete removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Cost of removal and disposal of existing expansion joints shall be included in the cost of Concrete Removal.

For HMA Overlay on Approach Slab, see Roadway Plans.

Protective Coat shall be applied to the reconstructed transverse Expansion Joints and to the surface of the new Latex Concrete Overlay.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specification when the deck is poured at an ambient temperature other than 50°F.

Expansion joints shall be fabricated to conform to the existing cross slope of the bridge.

The Contractor shall exercise extreme caution during concrete removal to avoid damaging P.P.C. beams and diaphragms to remain. Any damage to the existing P.P.C. beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the engineer at no cost to the Department.

Concrete Sealer shall be applied to the top and inside face of parapets and to the designated areas of the abutments.

The Contractor shall take appropriate measures to assure that the protective coat is not applied to the floor drains.

Prior to the application of the Concrete Sealer, the Contractor shall clean all existing debris from the abutment seats. The method of debris removal shall not damage the existing concrete and shall be approved by the Engineer. The debris shall be disposed of according to Article 202.03 of the Standard Specifications. The cost of cleaning is included in the cost of Concrete Sealer.

Cleaning and painting of the existing elastomeric bearings shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All bearings shall be cleaned per Near White Blast Cleaning (SSPC-SP10). The designated areas cleaned per Near White Blast Cleaning (SSPC-SP10) shall be painted according to the requirements of the Organic Zinc-Rich/Epoxy/Urethane paint system. The color of the final finish coat for all elastomeric bearings shall be Gray, Munsell No. 5B 7/1.

INDEX OF SHEETS

1. General Plan and Elevation
2. General Data
3. Stage Construction
4. Temporary Concrete Barrier
5. Bridge Deck and Approach Slab Repair Plan
6. Parapet Repair Plan
7. Approach Joint Removal and Replacement
8. Beam End Repair Details
9. Diaphragm and Bearing Repair Details
10. Expansion Joint Replacement Details (1 of 3)
11. Expansion Joint Replacement Details (2 of 3)
12. Expansion Joint Replacement Details (3 of 3)
13. Preformed Joint Strip Seal
14. Deck Floor Drain Details
15. Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	23.0	-	23.0
Protective Shield	Sq. Yd.	609	-	609
Concrete Superstructure	Cu. Yd.	22.5	-	22.5
Protective Coat	Sq. Yd.	2,202	-	2,202
Concrete Superstructure (Approach Slab)	Cu. Yd.	3.4	-	3.4
Furnishing and Erecting Structural Steel	Pound	-	69	69
Reinforcement Bars, Epoxy Coated	Pound	3,010	-	3,010
* Bar Splicers	Each	12	-	12
Preformed Joint Strip Seal	Foot	186	-	186
Anchor Bolts, 1"	Each	-	3	3
Concrete Sealer	Sq. Ft.	271	98	369
* Acrylic Coating	Sq. Yd.	13	-	13
* Fiber Wrap	Sq. Ft.	119	-	119
* Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1,659	-	1,659
* Cleaning and Painting Bearings	Each	16	-	16
* Approach Slab Repair (Partial Depth)	Sq. Yd.	2	-	2
* Bridge Deck Latex Concrete Overlay, 2 ³ / ₄ Inches	Sq. Yd.	2,158	-	2,158
* Bridge Deck Scarification 3/4"	Sq. Yd.	2,178	-	2,178
* Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq. Ft.	47	6	53
* Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	19	-	19
* Diamond Grinding (Bridge Section)	Sq. Yd.	2,042	-	2,042
* Precast Prestressed Concrete I-Beam Repair	Sq. Ft.	18	-	18

* Special Provision

LIST OF ABBREVIATIONS

Abut.	Abutment
Appr.	Approach
Bk.	Back
Brg.	Bearing
bot.	Bottom
cts.	Centers
¢	Centerline
Const.	Construction
Ø	Diameter
Ex.	Existing
Exp.	Expansion
F.F.	Front Face
I.F.	Inside Face
Max.	Maximum
Min.	Minimum
N.B.	Northbound
O.F.	Outside Face
¶	Plate
Pvmt.	Pavement
Reinf.	Reinforcement
RR	Railroad
Rte.	Route
S.B.	Southbound
SF	Square Foot
Shldr.	Shoulder
Sta.	Station
Std.	Standard
typ.	Typical



USER NAME = Iyanisch
DESIGNED - LJM
DRAWN - LJM
CHECKED - WJV
PLOT DATE = 6/27/2025

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DATE - 09/26/2025
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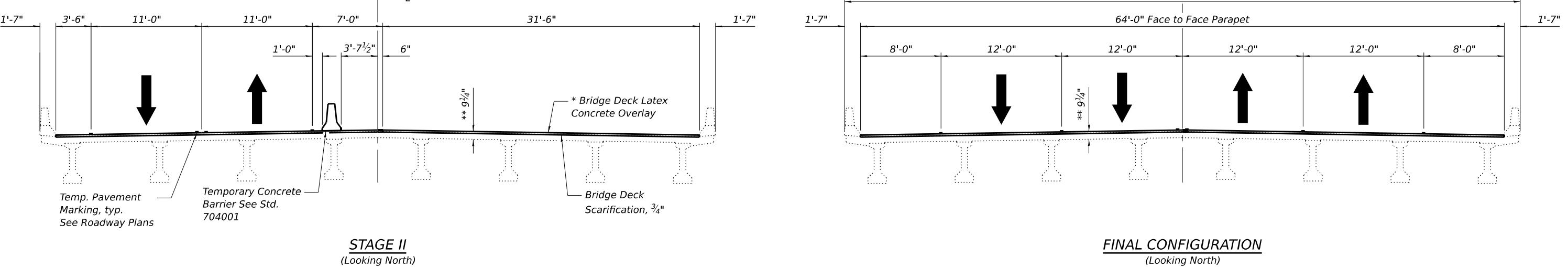
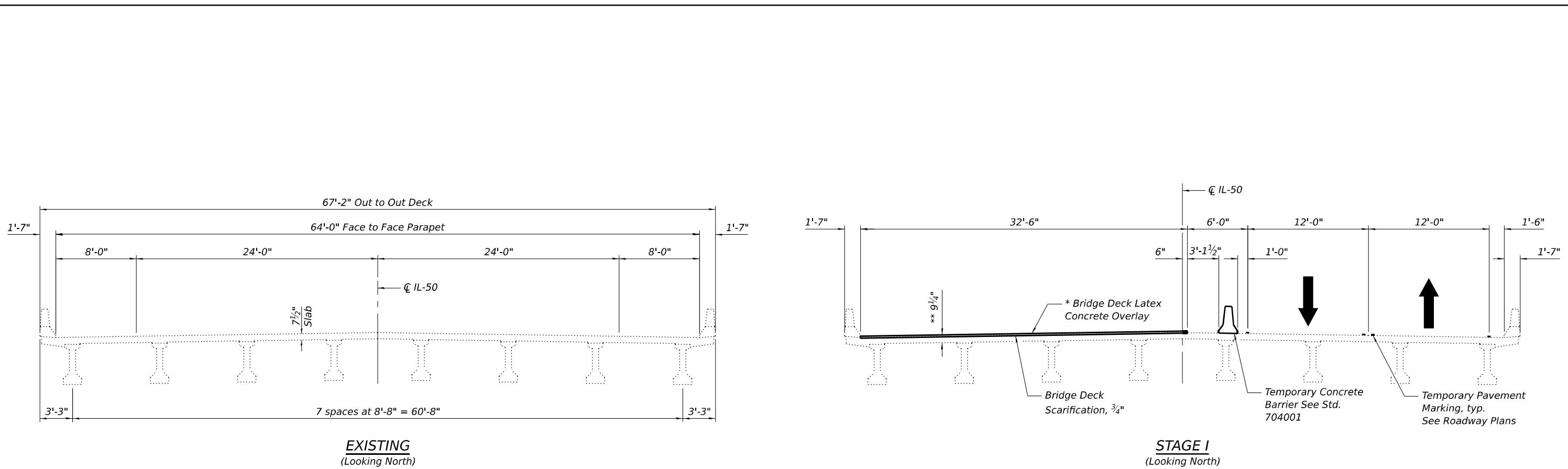
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL DATA
STRUCTURE NO. 099-0298

SCALE: SHEET 2 OF 15 SHEETS STA. TO STA.

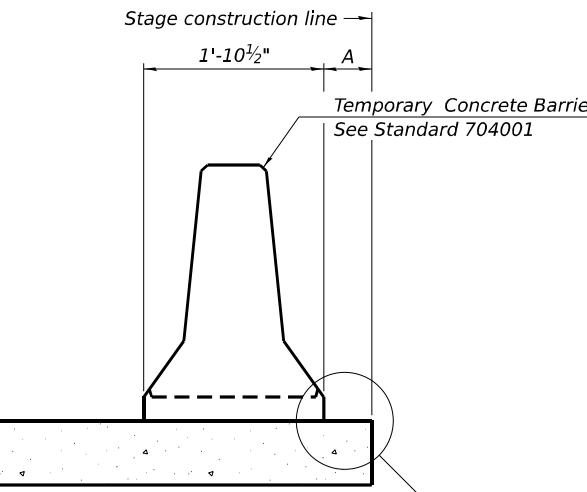
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				CONTRACT NO. 62Y26

ILLINOIS FED. AID PROJECT



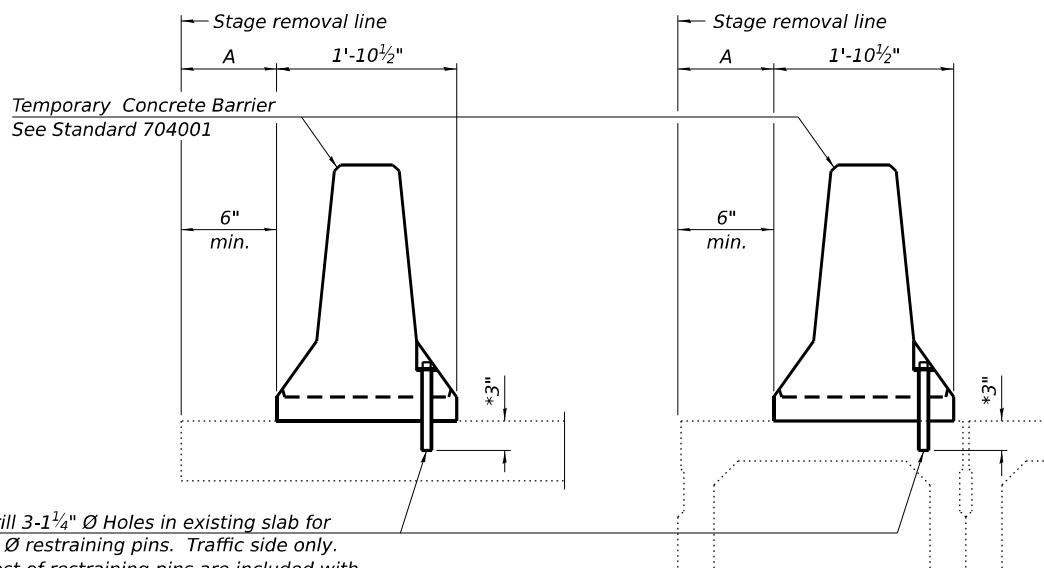
- * Place $2\frac{3}{4}$ " latex concrete overlay on bridge deck. Final overlay thickness shall be $2\frac{1}{2}$ " after $\frac{1}{4}$ " Diamond Grinding (See Special Provisions.)

** *Prior to grinding*

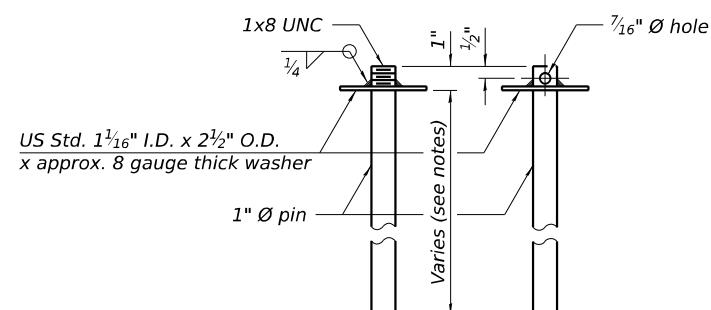


When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

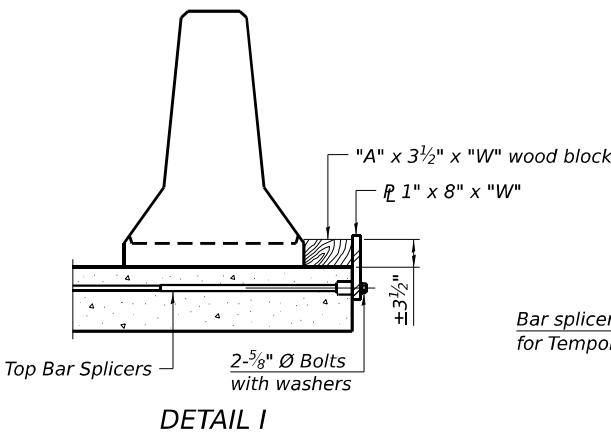


* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.



RESTRAINING PIN

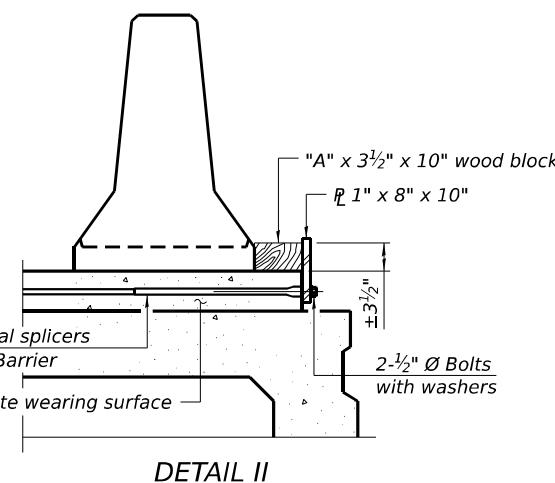
SECTIONS THRU SLAB OR DECK BEAM



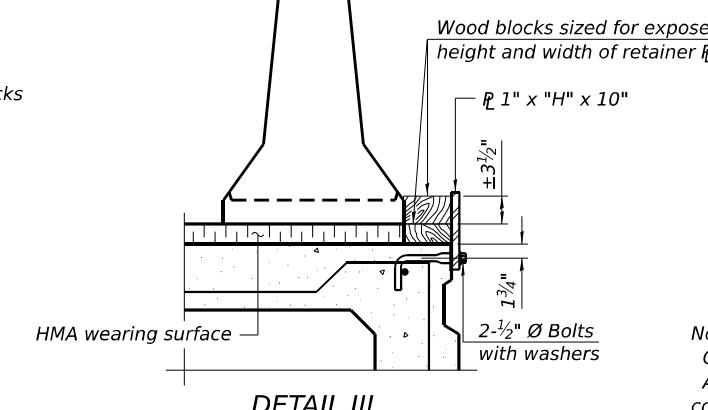
Bar splices and additional splices for Temporary Concrete Barrier

Concrete wearing surface

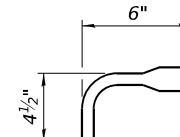
DETAIL I



DETAIL II



DETAIL III



BAR SPlicer FOR #4 BAR - DETAIL III

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier. A retainer assembly shall be located at the approximate $\frac{C}{4}$ of each temporary concrete barrier.

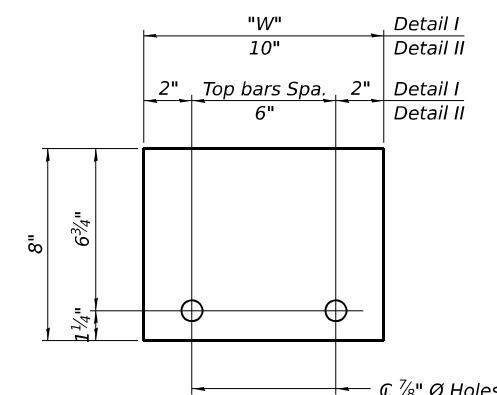
The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

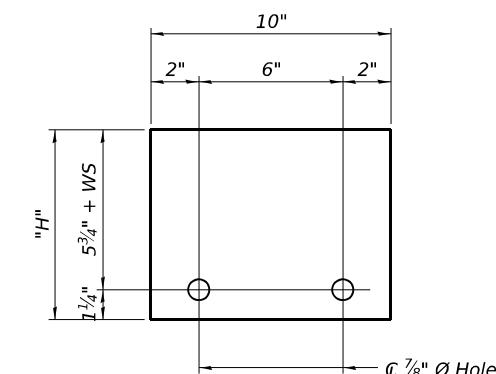
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splices shall be provided at 6'-0" centers and paired with the bar splices of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splices is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splices, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splices is included with the deck beam.



STEEL RETAINER P 1" x 8" x "W"

(Detail I and II)



STEEL RETAINER P 1" x "H" x 10"

(Detail III)

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27

5-15-2023



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DATE - 09/26/2025

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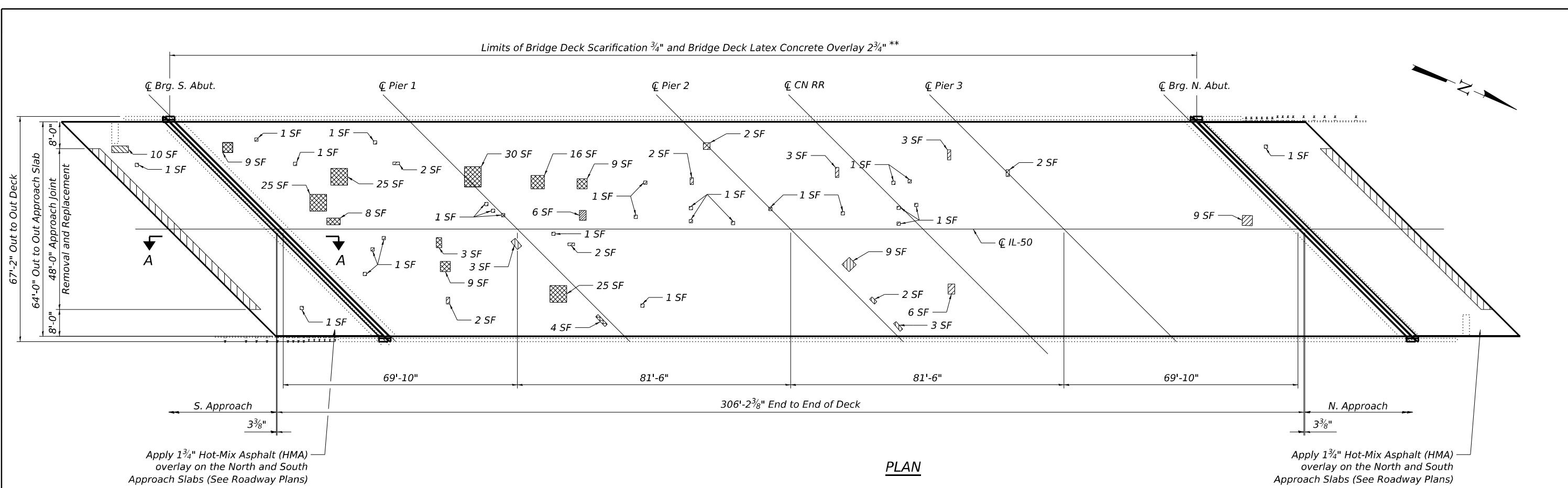
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER
STRUCTURE NO. 099-0298

SCALE: SHEET 4 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840 (143VB-R-1(90)) BR 25		WILL	45	26
				CONTRACT NO. 62Y26

ILLINOIS FED. AID PROJECT



LEGEND

-  *Deck Slab Repair (Partial) (For Information Only)*
-  *Deck Slab Repair (Full Depth, Type II)*
-  *Approach Slab Repair (Partial Depth)*
-  ** Approach Joint Removal and Replacement*

* See sheet 7 of 15 for details and Bill of Materials.

** Prior to $\frac{1}{4}$ " Diamond Grinding

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	19
Bridge Deck Scarification $\frac{3}{4}$ "	Sq. Yd.	2,178
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1,659
Bridge Deck Latex Concrete Overlay, $2\frac{3}{4}$ Inches	Sq. Yd.	2,158
Diamond Grinding (Bridge Section)	Sq. Yd.	2,042
Protective Coat	Sq. Yd.	2,202
Concrete Sealer	Sq. Ft.	271
Approach Slab Repair (Partial Depth)	Sq. Yd.	2

Notes:

For Section A-A see Sheet 7 of 15.

Areas of deck repair shown are estimated. The Engineer shall show actual locations of deck repairs at the time of construction.

For bridge deck final cross section, see Sheet 3 of 15.

For bridge deck final cross section, see Sheet 3 of 15.
For North and South transverse joint removal and reconstruction,
see Sheets 10 through 12.

see Sheets 10 through 12.
Perform Bridge Deck Grooving (Longitudinal) on traffic lanes.
Perform $\frac{1}{4}$ " Diamond Grinding to top of bridge deck and abutment

Protective Coat shall be applied to the reconstructed expansion joint

and to the top of Latex Concrete Overlay.

Concrete sealer shall be applied to top and inside face of parapets.

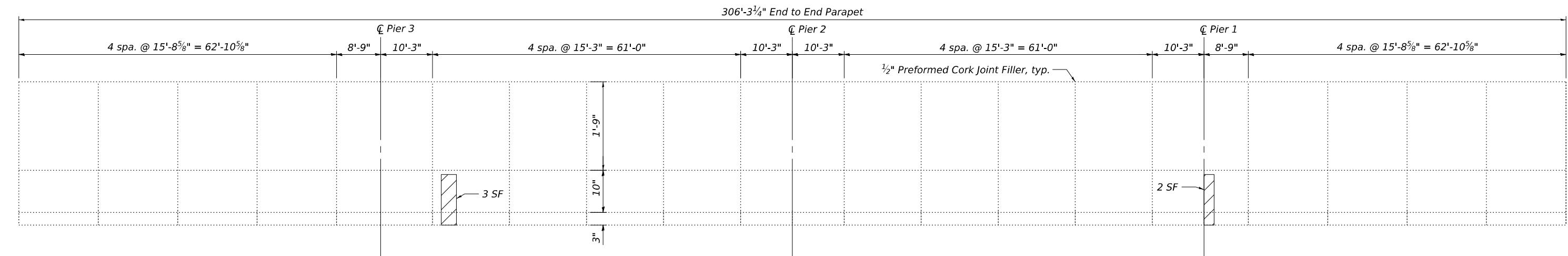
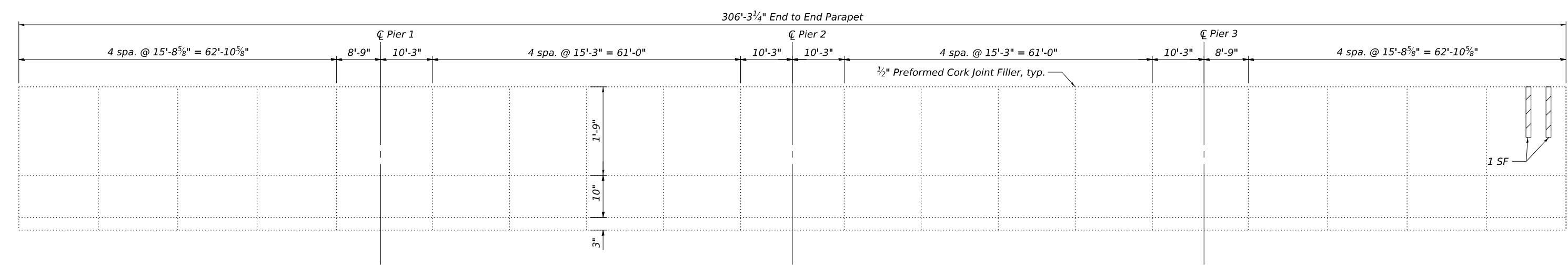
Any reinforcement bars that are damaged during concrete removal

Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using approved bar splicer or anchorage system. Cost incidental to Concrete Removal.

Deck Slab Repair (Full Depth) quantity has been estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used will be determined by the Engineer in the field

Bridge Deck Scarification shall be performed per IDOT BDE Special at the time of construction.

Bridge Deck Rehabilitation shall be performed per ACI 318 Special Provision 80475.
Deck Slab Repair (Partial) quantity is for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 $\frac{3}{4}$ Inches.



BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	7

LEGEND

Structural Repair of Concrete (Depth Equal to or Less than 5 inches)



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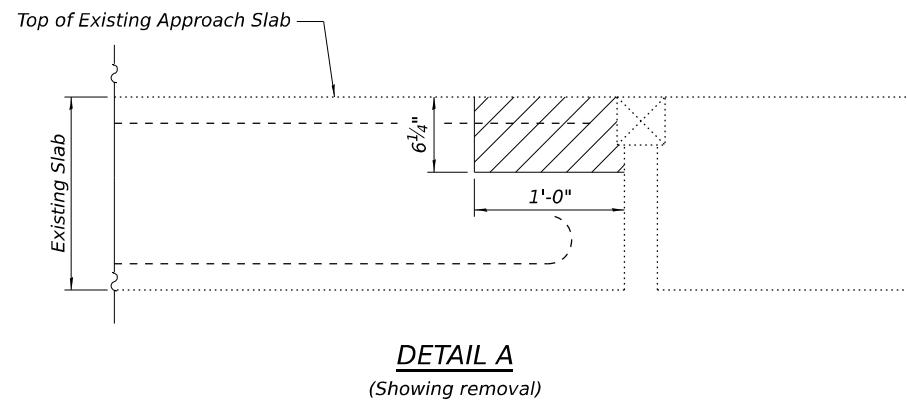
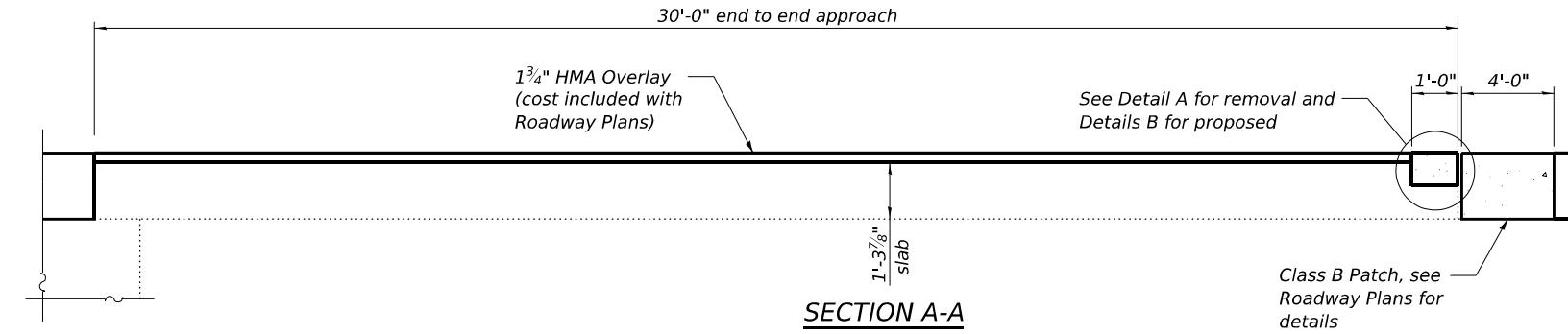
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PARAPET REPAIR PLAN
STRUCTURE NO. 099-0298

SCALE: SHEET 6 OF 15 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	28
				CONTRACT NO. 62Y26

ILLINOIS FED. AID PROJECT

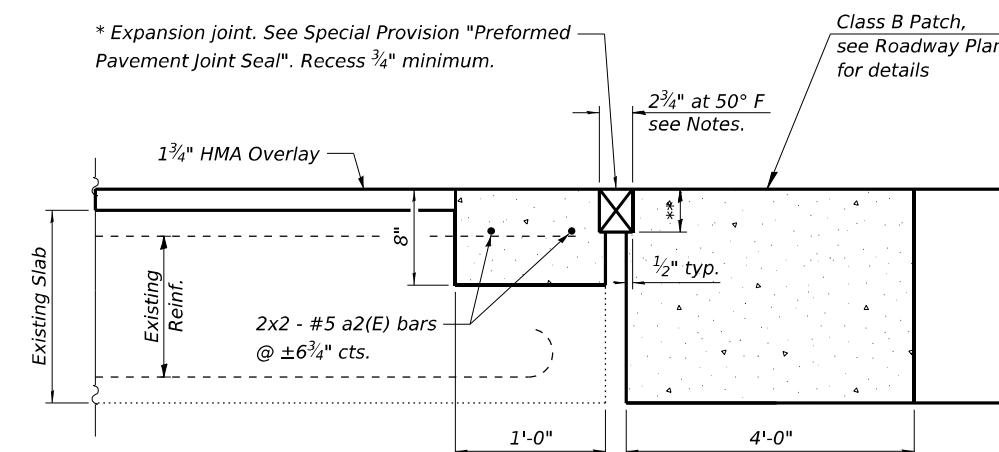


BILL OF MATERIAL				
Bar	No.	Size	Length	Shape
a2(E)	8	#5	35'-9"	—
Reinforcement Bars, Epoxy Coated		Pound	300	
Concrete Removal		Cu. Yd.	2.6	
Concrete Superstructure (Approach Slab)		Cu. Yd.	3.4	

LEGEND



Notes:
Reinforcement bars designated (E) shall be epoxy coated.
For approach pavement details see Roadway Plans.
The joint opening shall be adjusted for temperature per Article 520.04 of the Standard Specifications.



* Cost included with Concrete Superstructure (Approach Slab)

** Per manufacturer recommendations



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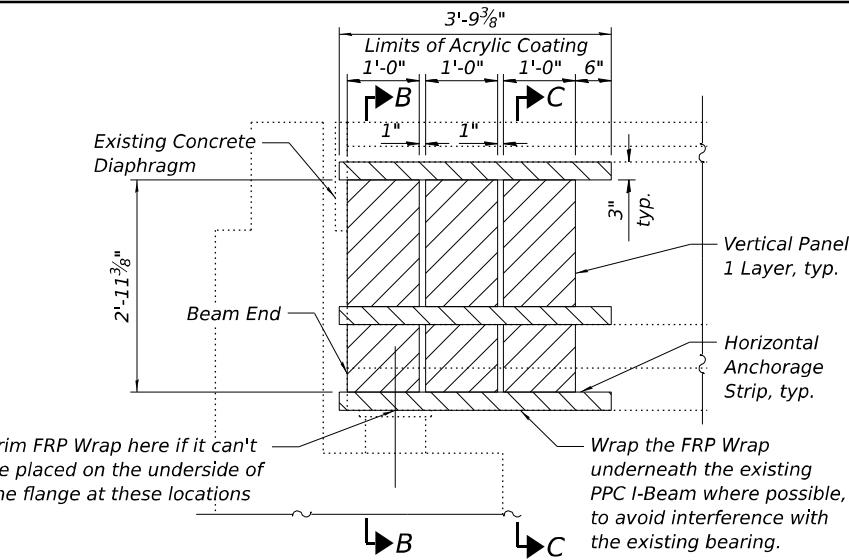
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROACH JOINT REMOVAL AND REPLACEMENT
STRUCTURE NO. 099-0298

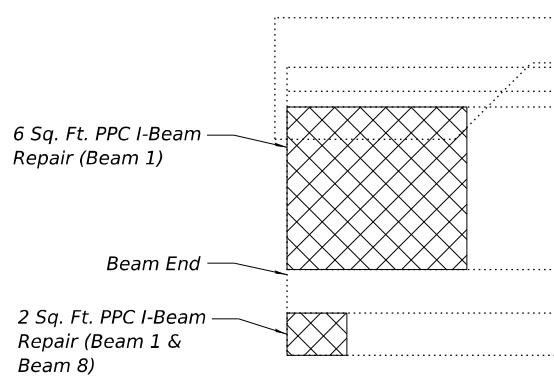
SCALE: SHEET 7 OF 15 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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				CONTRACT NO. 62Y26

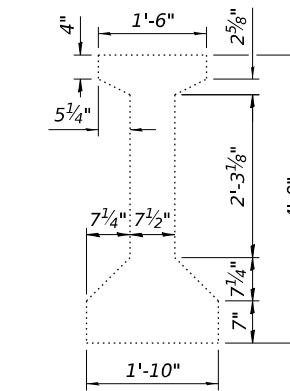
ILLINOIS FED. AID PROJECT



ELEVATION OF BEAM 1 AT N. ABUT. & BEAM 8 AT S. ABUT.



REPAIR DETAIL BEAM 1 AT N. ABUT.
& BEAM 8 AT S. ABUT.



CROSS SECTION

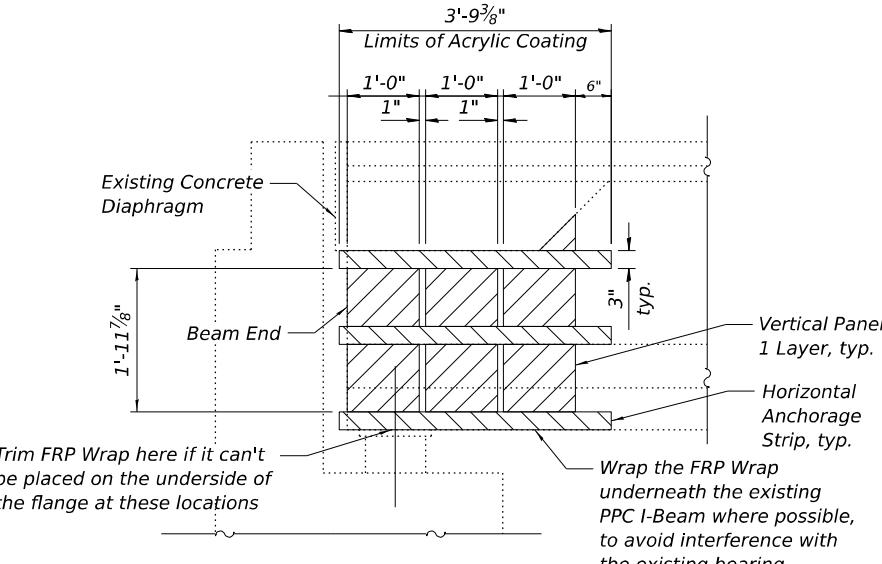
PPC I-BEAM REPAIR	
LOCATION	SQ. FT.
Beam 1 West Face - N. Abut.	8
Beam 4 East Face - S. Abut.	6
Beam 7 East Face - N. Abut.	2
Beam 8 East Face - S. Abut.	2

LEGEND

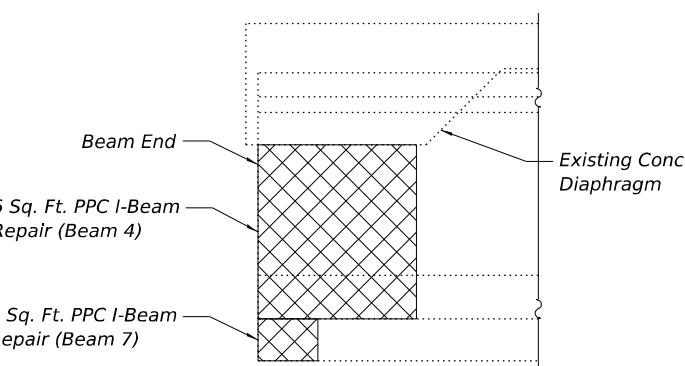
PPC I-Beam Repair

Vertical Panel

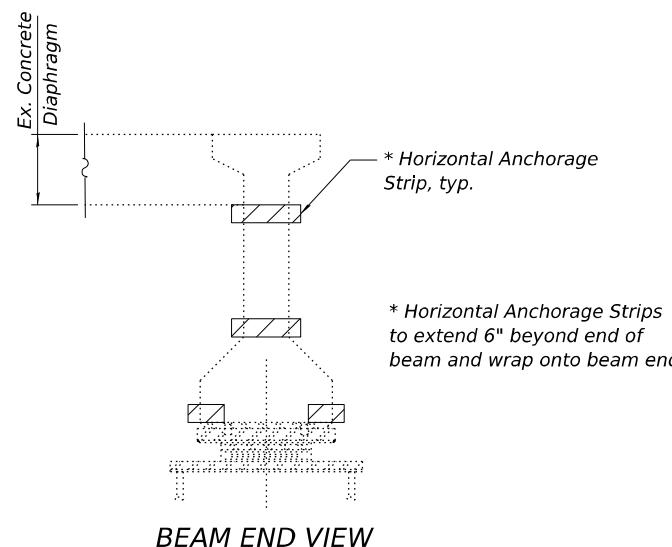
Horizontal Anchorage Strip



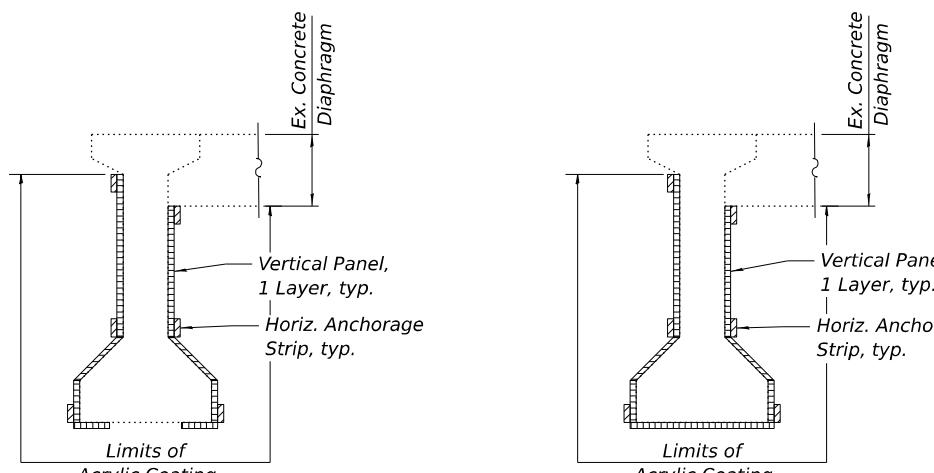
ELEVATION OF BEAM 4 AT S. ABUT. & BEAM 7 AT N. ABUT.



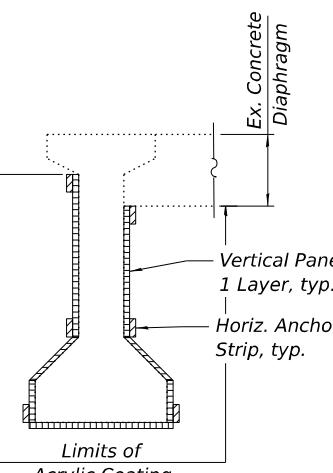
REPAIR DETAIL BEAM 4 AT S. ABUT.
& BEAM 7 AT N. ABUT.



BEAM END VIEW



SECTION B-B



SECTION C-C

NOTES:

Vertical panels must be 12". The space between each vertical panel shall be 1". Vertical panels shall extend beyond the repair zone by a minimum of 3". Vertical panels located above the bearing location shall be placed in two pieces as shown in Section A-A. At locations in front of the bearing, the vertical panels shall be one continuous strip wrapping beneath the bottom flange as shown in Section B-B.

Horizontal anchorage strips shall be 3" wide and extend a minimum of 6" beyond the vertical panels. The horizontal anchorage strips shall be placed on top of the vertical panels. In areas where deterioration occurs in the ends of beams, the anchorage strip shall wrap around the beam end in one continuous strip.

Spalled concrete and exposed reinforcement shall be repaired according to the special provision "PRECAST PRESTRESSED CONCRETE I-BEAM REPAIRS." Acrylic coating shall be placed over fiber wrap repairs. Two coats shall be applied. See special provisions for "FRP STRENGTHENING FOR PPC I-BEAM REPAIRS".

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Acrylic Coating	Sq Yd	13
Fiber Wrap	Sq Ft	119
Precast Prestressed Concrete I-Beam Repair	Sq Ft	18



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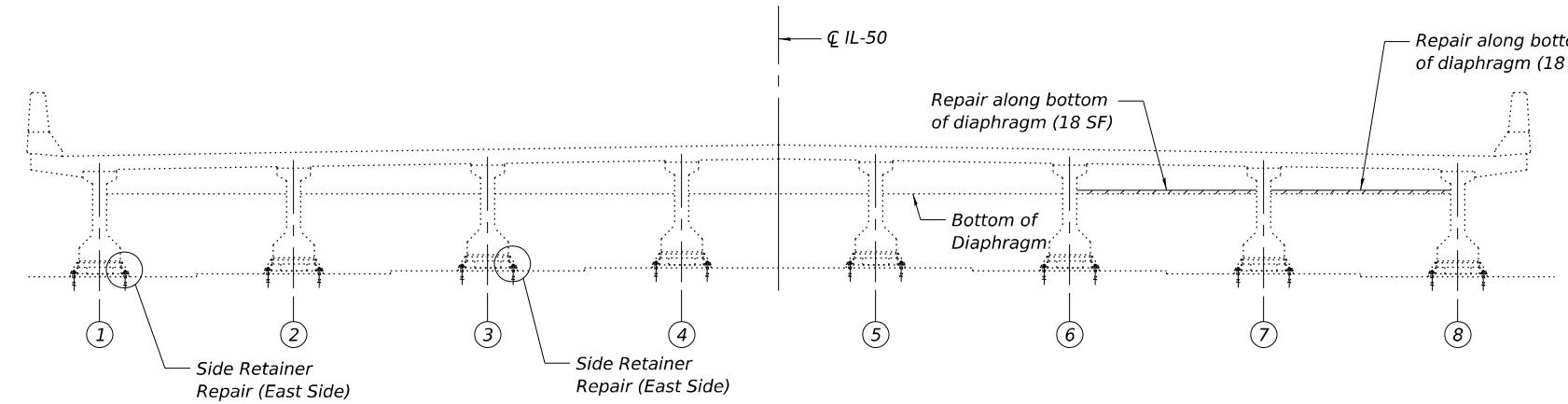
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM END REPAIR DETAILS
STRUCTURE NO. 099-0298

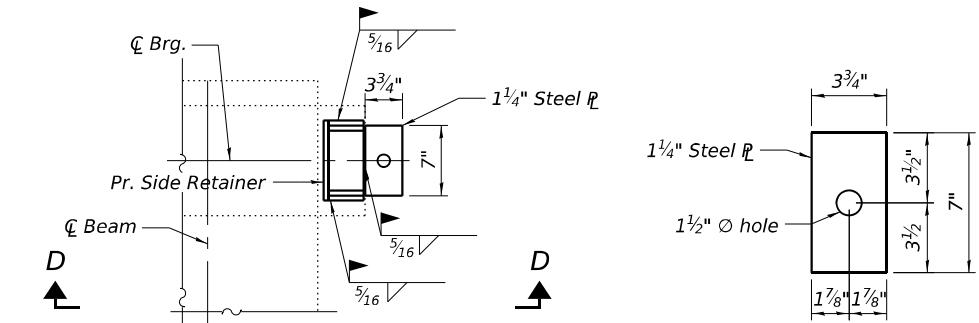
SCALE: SHEET 8 OF 15 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	30
				CONTRACT NO. 62Y26

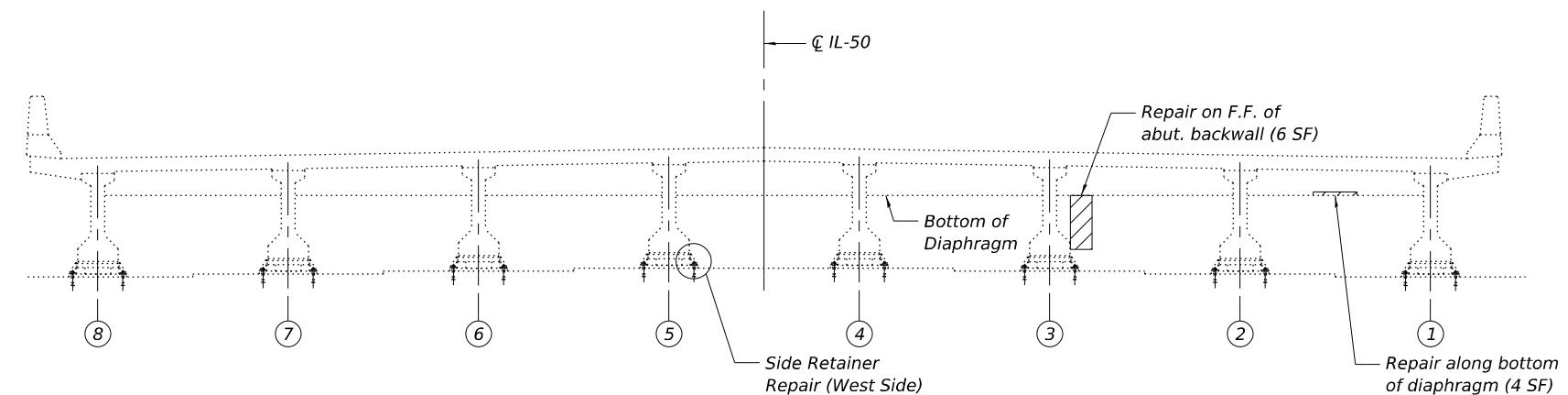
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N. ABUTMENT ELEVATION
(Looking North)



SIDE RETAINER
REPAIR PLAN



S. ABUTMENT ELEVATION
(Looking South)

Notes:

Repair of existing abutment backwalls & diaphragms shall include but may not be limited to the areas shown. The actual area to be repaired will be determined by the Engineer at the time of construction.

Side Retainers shall be hot-dip galvanized according to Article 506.09 of the Standard Specifications.

Concrete Sealer is to be applied to the abutment seats and the bottom of the flange and the top of the wall.

2 feet of the abutment backwall.

*Clean and paint all bearings at North and South abutments.
Anchor bolts shall be ASTM F1554 all-threaded of the grade(s) and diameter(s) specified.*

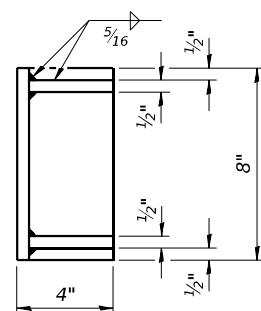
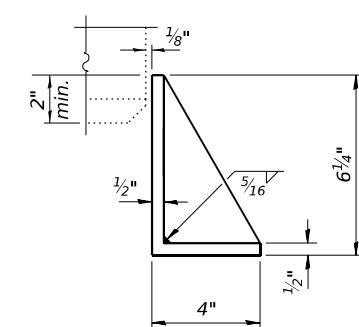
Drilled and set anchor bolts shall be installed according to Article 521.0 of the Standard Specification.

LEGEND



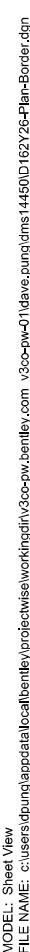
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)

Item	Unit	Total
<i>Furnishing and Erecting Structural Steel</i>	Pound	69
<i>Anchor Bolts, 1"</i>	Each	3
<i>Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)</i>	Sq. Ft.	46
<i>Concrete Sealer</i>	Sq. Ft.	98
<i>Cleaning and Painting Bearings</i>	Each	16



SIDE RETAINER

SIDE RETAINER



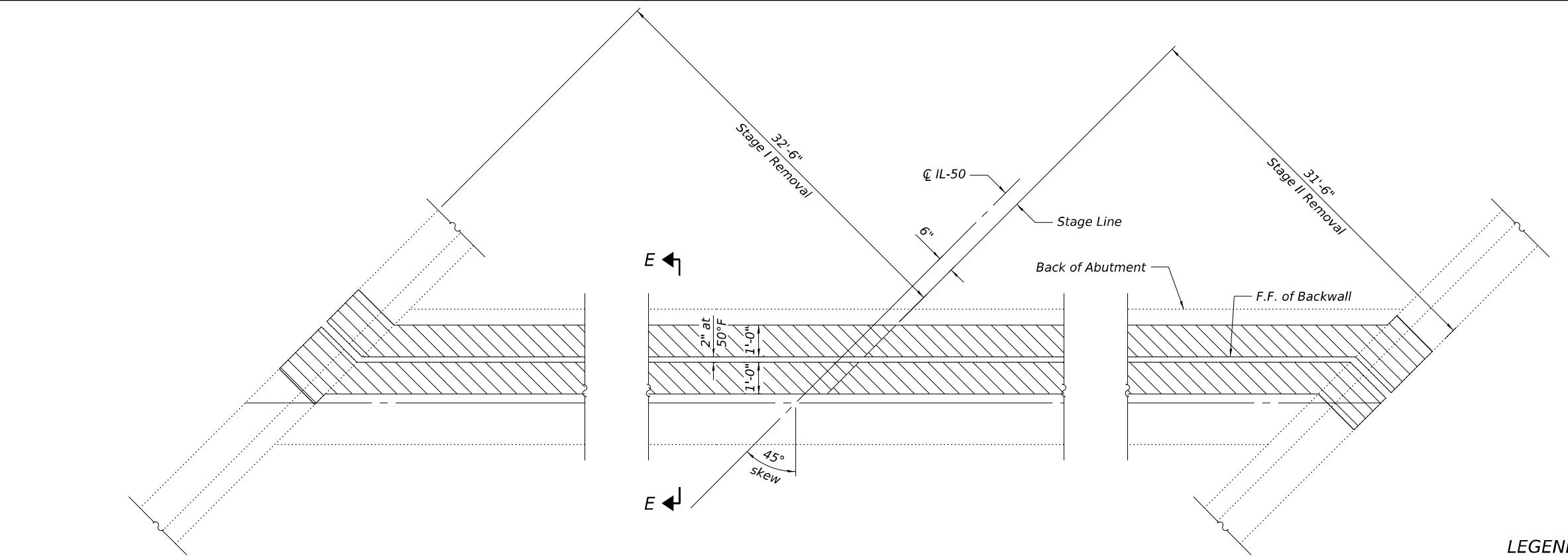
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DIAPHRAGM AND BEARING REPAIR DETAILS
STRUCTURE NO. 099-0298

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	HEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	31
			CONTRACT NO. 62Y26	



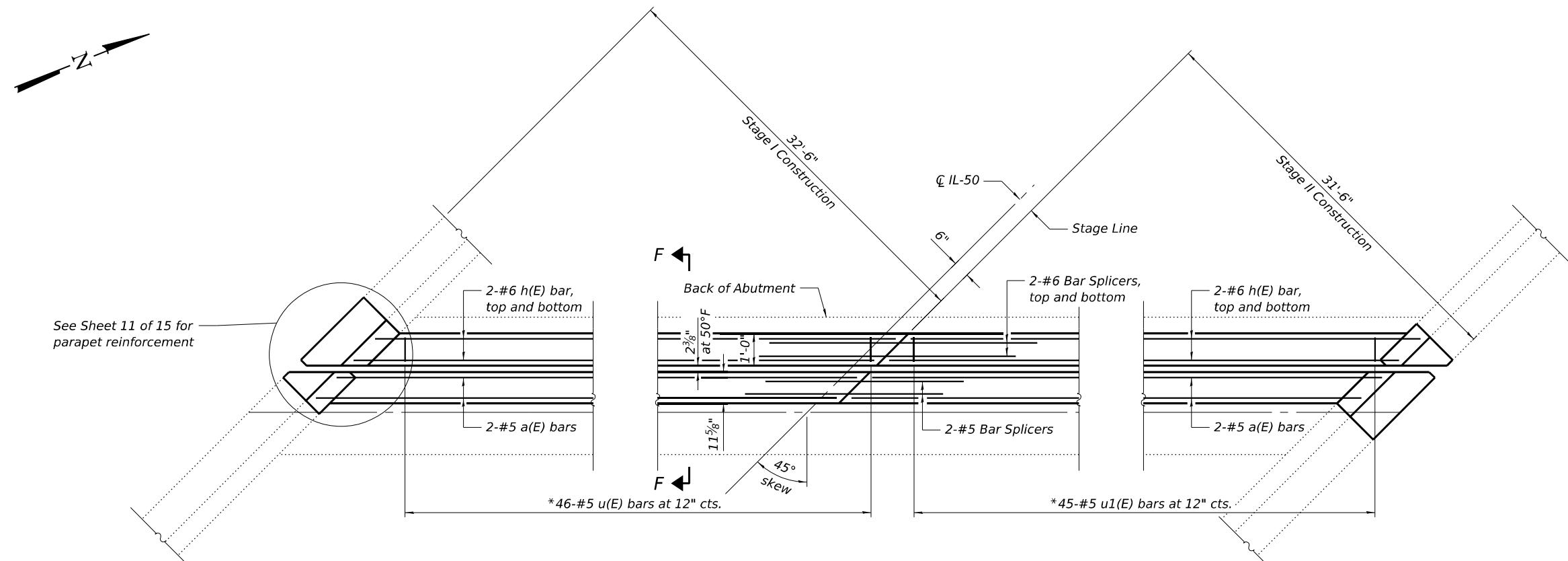
JOINT REMOVAL PLAN

*North abutment shown.
South abutment similar by rotation*

LEGEND



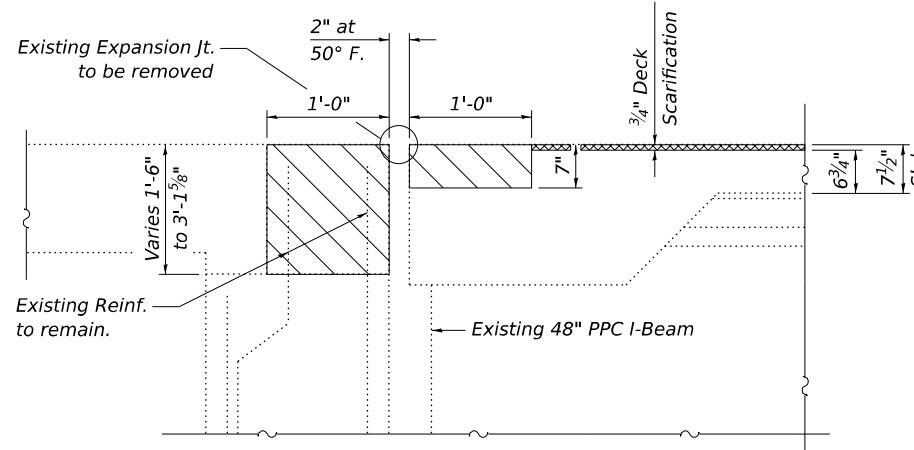
Concrete Removal



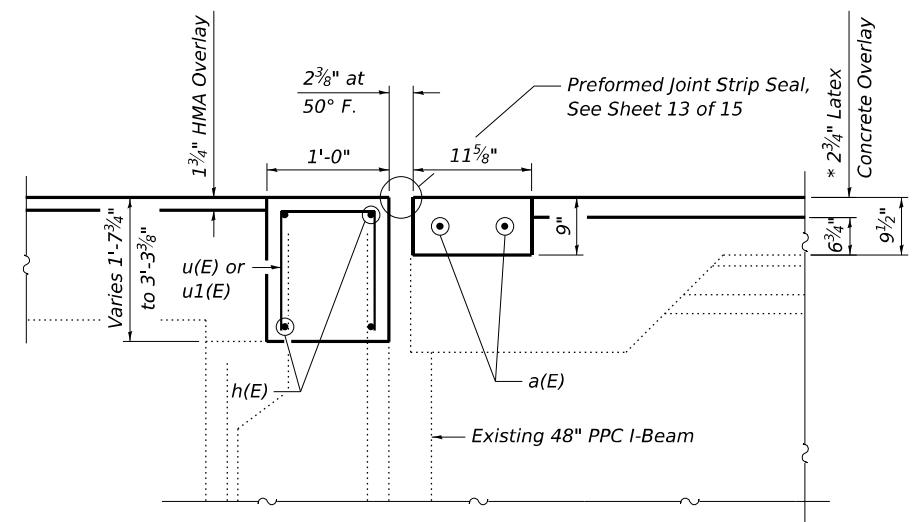
JOINT REPLACEMENT PLANS

*North abutment shown.
South abutment similar by rotation*

Notes:
See Sheet 11 of 15 for Section E-E and Section F-F.

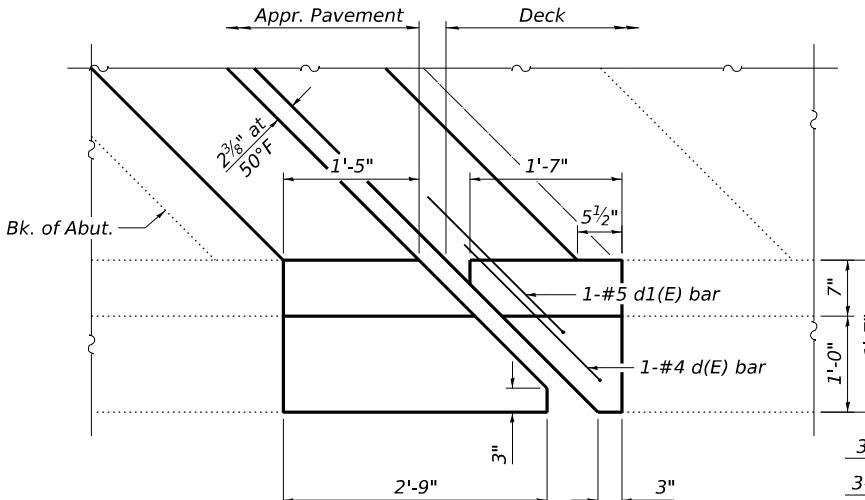


SECTION E-E



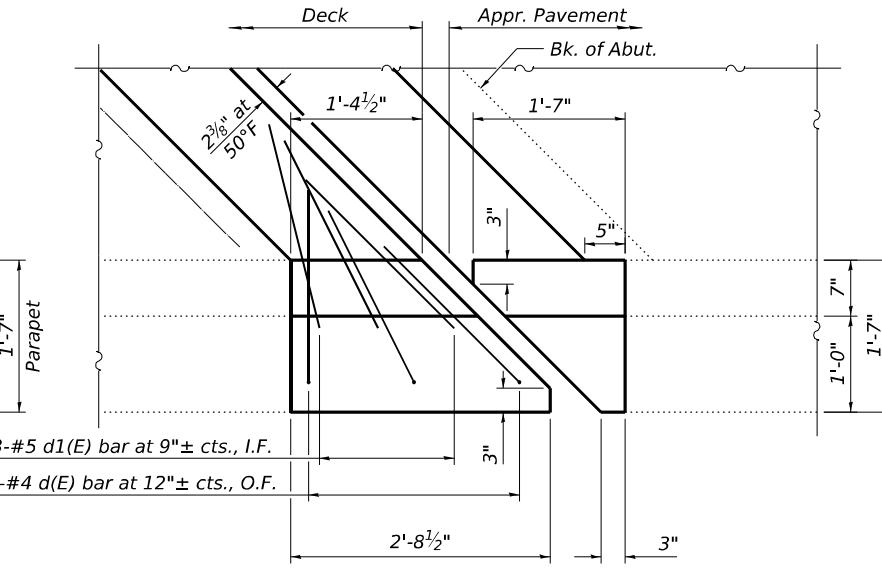
SECTION F-F

* Prior to grinding



PARAPET REPLACEMENT PLAN

(West end of North Abut. and East end of South Abut.)



PARAPET REPLACEMENT PLAN

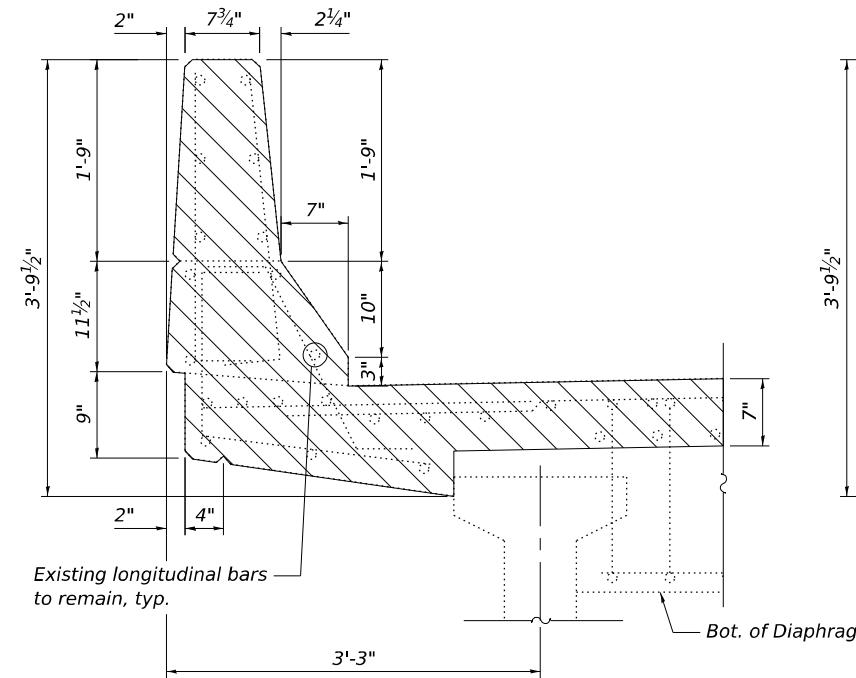
(East end of North Abut. and West end of South Abut.)

LEGEND

- 3/4" Deck Scarification
- Concrete Removal

**SECTION THRU PARAPET AT JOINT
ON BRIDGE DECK SIDE**

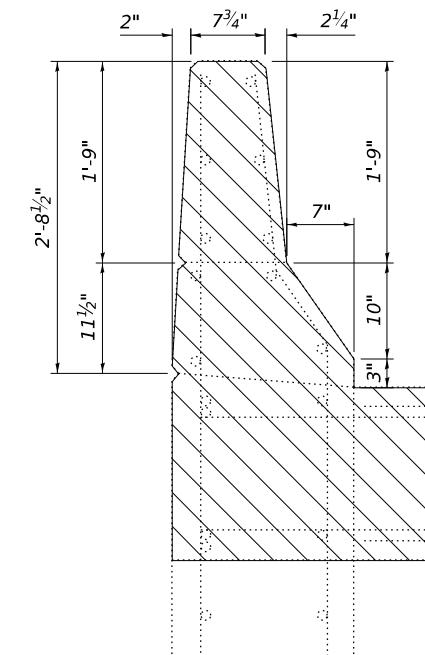
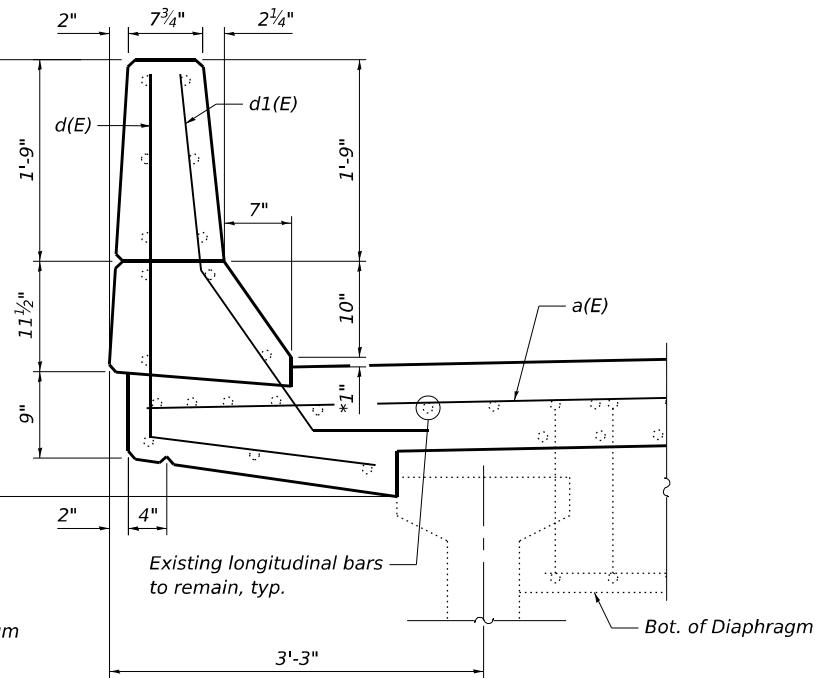
(Showing Removal)



**SECTION THRU PARAPET AT JOINT
ON BRIDGE DECK SIDE**

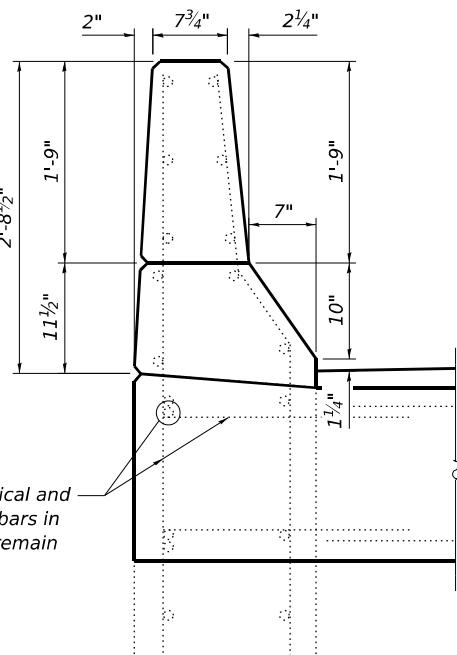
(Showing Proposed)

* Prior to grinding



**SECTION THRU PARAPET AT JOINT
ON APPROACH SIDE**

(Showing Removal)



**SECTION THRU PARAPET AT JOINT
ON APPROACH SIDE**

(Showing Proposed)



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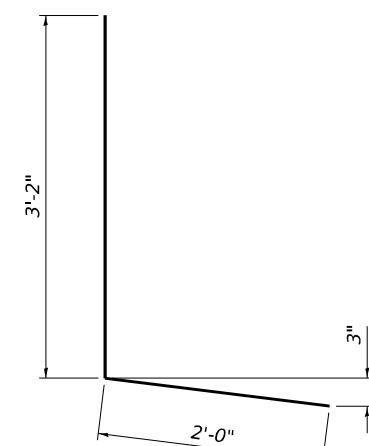
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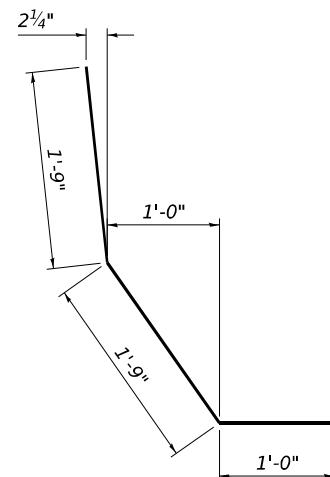
EXPANSION JOINT REPLACEMENT DETAILS (2 OF 3)
STRUCTURE NO. 099-0298

SCALE: SHEET 11 OF 15 SHEETS STA. TO STA.

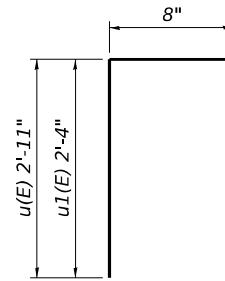
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	33
CONTRACT NO. 62Y26				
ILLINOIS		FED. AID PROJECT		



BAR d(E)



BAR d1(E)



BAR u(E) & u1(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	4	5	47'-6"	—
a1(E)	4	5	46'-0"	—
d(E)	8	4	5'-2"	L
d1(E)	8	5	4'-6"	L
h(E)	8	6	47'-6"	—
h1(E)	8	6	46'-0"	—
u(E)	92	5	6'-6"	□
u1(E)	90	5	5'-4"	□
Reinforcement Bars, Epoxy Coated				Lbs. 2,710
Concrete Superstructure				Cu. Yd. 22.5
Concrete Removal				Cu. Yd. 20.4
Bar Splicers				Each 12



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PLOT DATE = 6/27/2025

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REVISED -

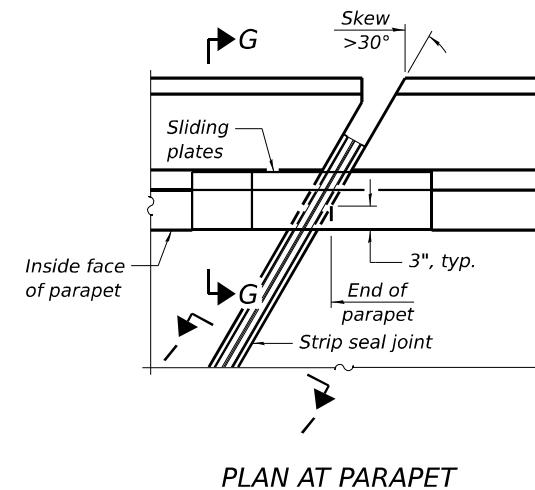
DATE - 09/26/2025

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

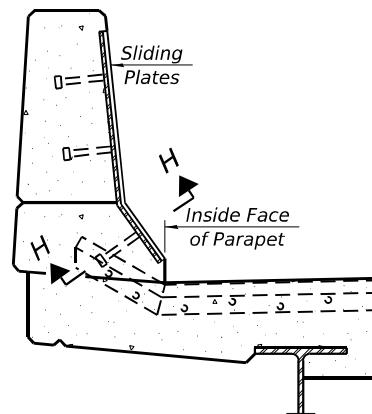
EXPANSION JOINT REPLACEMENT DETAILS (3 OF 3)
STRUCTURE NO. 099-0298

SCALE: SHEET 12 OF 15 SHEETS STA. TO STA.

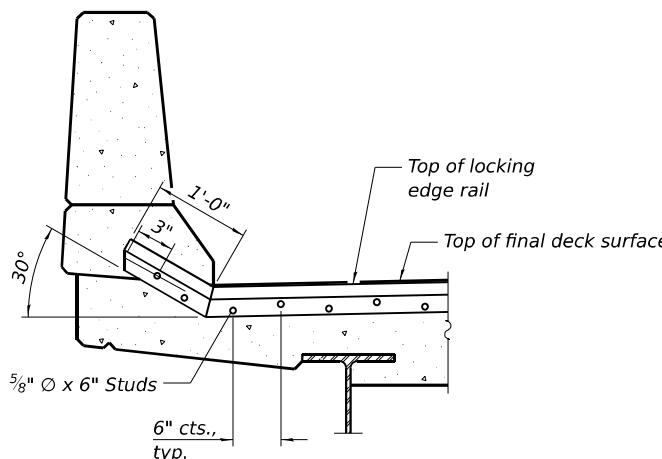
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				CONTRACT NO. 62Y26
				ILLINOIS FED. AID PROJECT



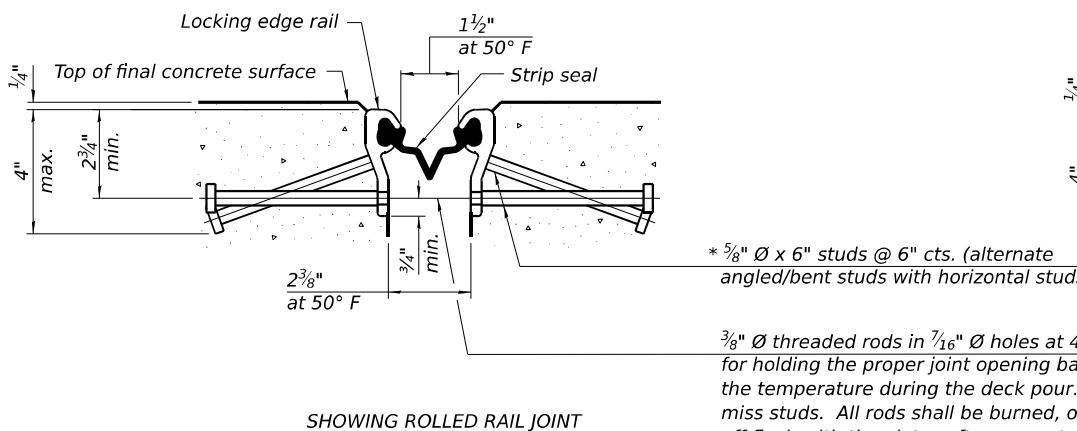
PLAN AT PARAPET



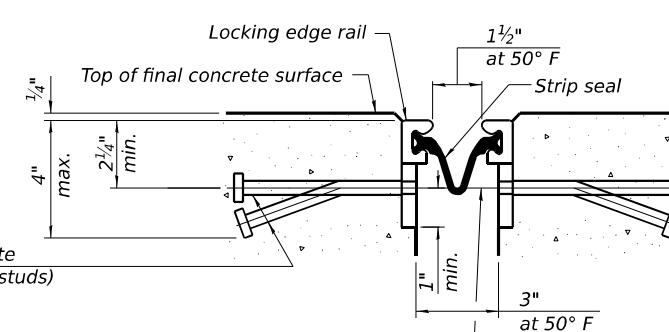
SECTION G-G



TYPICAL END TREATMENT
AT PARAPET



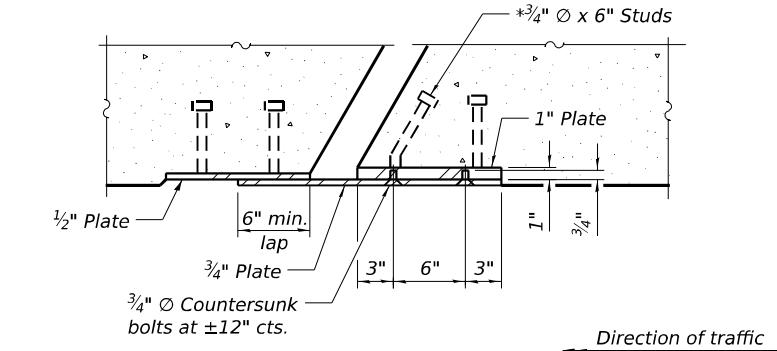
SHOWING ROLLED RAIL JOINT



SHOWING WELDED RAIL JOINT

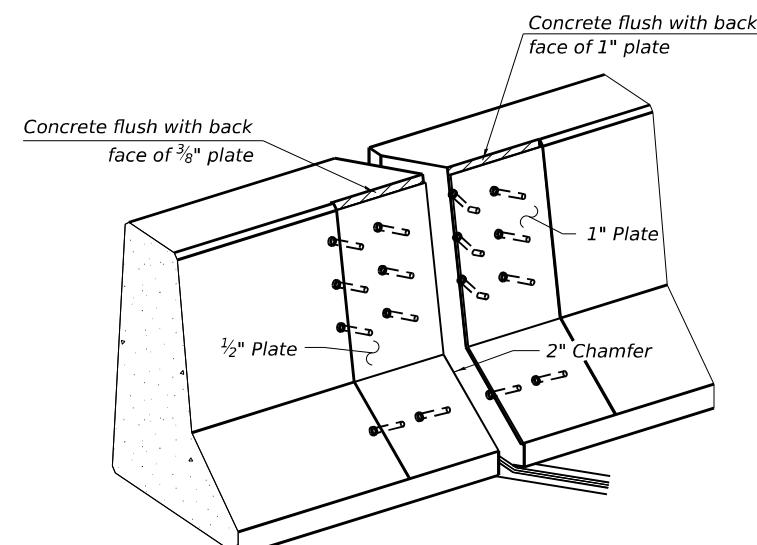
SECTION I-I

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

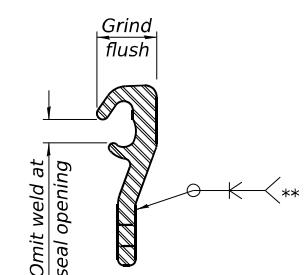
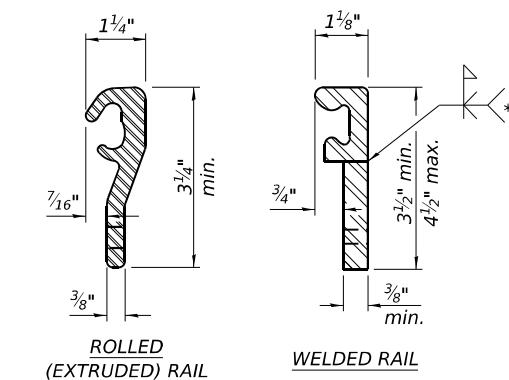


SECTION H-H

($3/4" \text{ Ø}$ countersunk bolts extending into concrete shall have hex nuts tack welded to the back of the embedded plates with end caps provided.)



TRIMETRIC VIEW
(Showing embedded plates only)



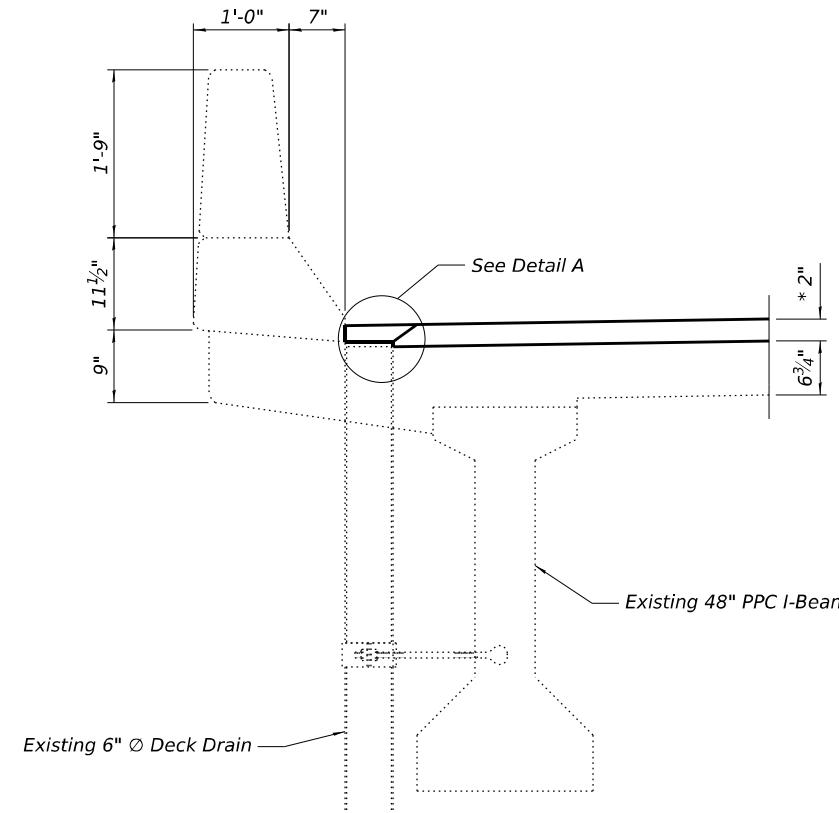
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

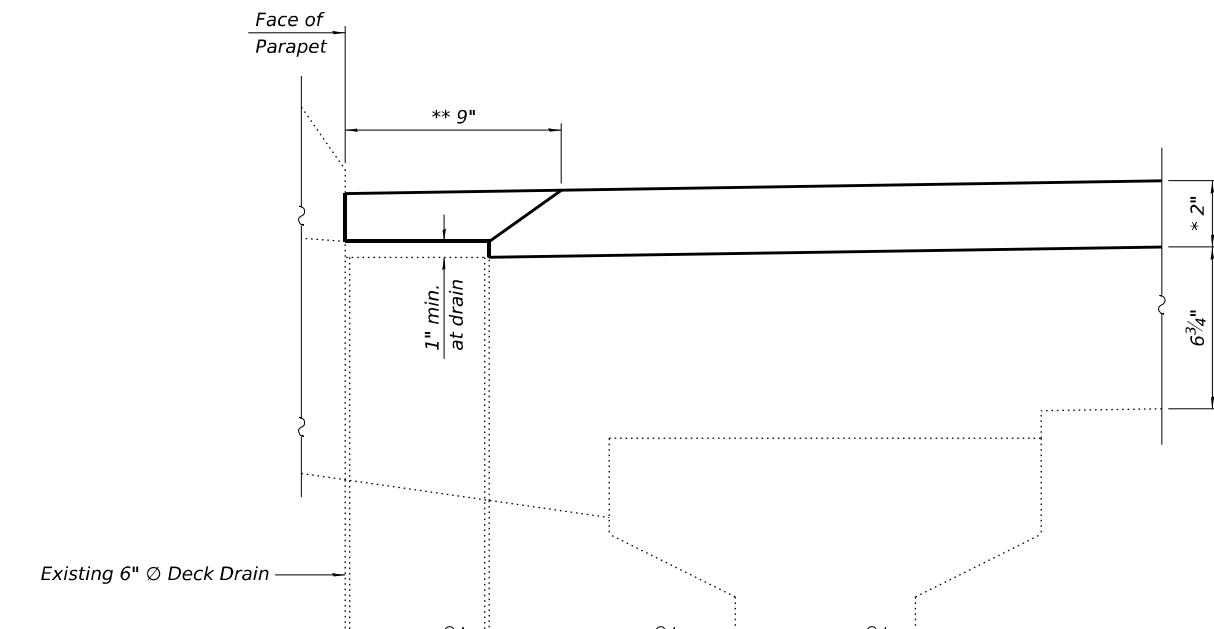
BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	186

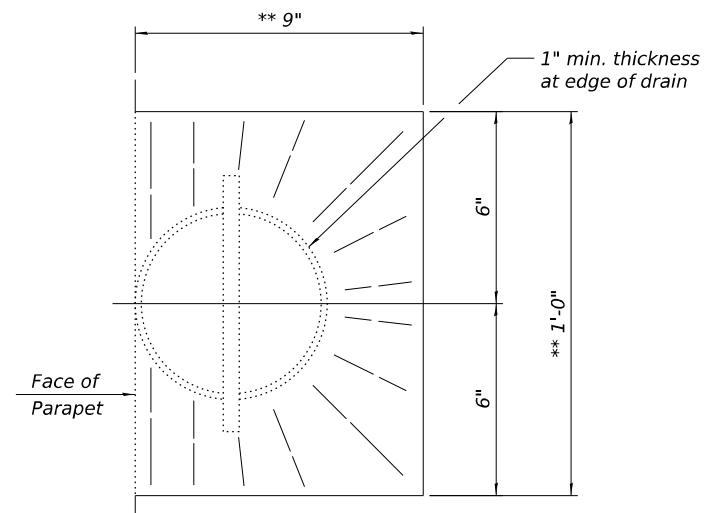
45



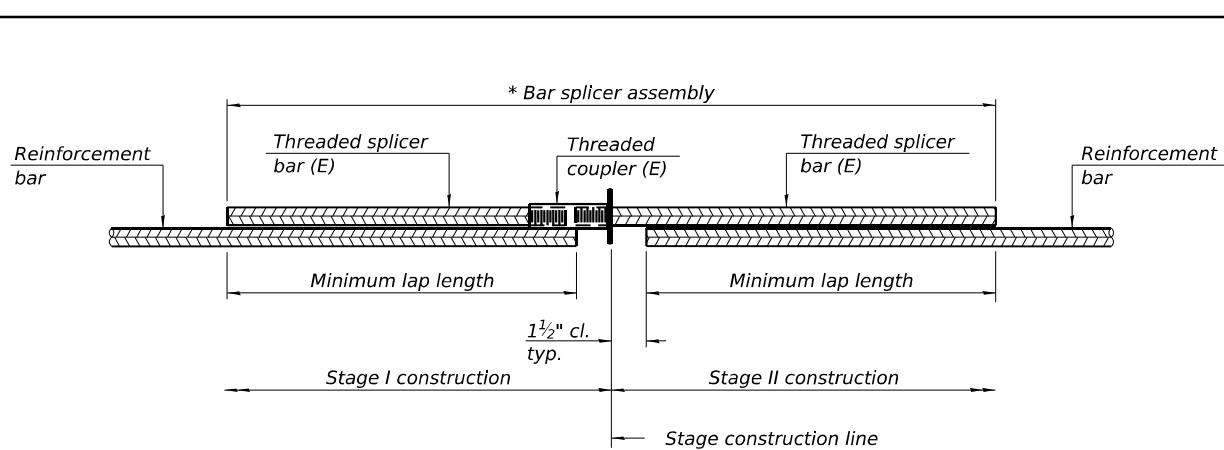
SECTION AT DECK DRAIL



DETAIL A



DECK DRAIN DETAIL



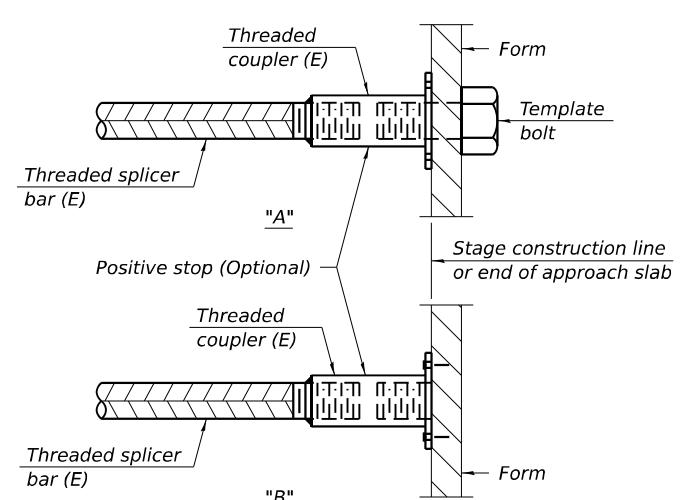
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1½" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
N. Abut.	#5	2	2'-10"
N. Abut.	#6	4	3'-11"
S. Abut.	#5	2	2'-10"
S. Abut.	#6	4	3'-11"



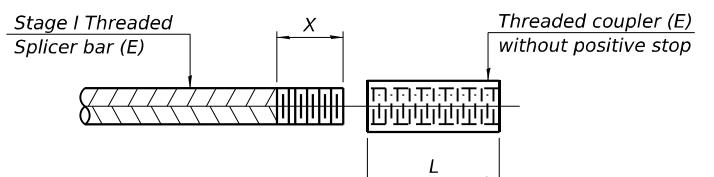
STANDARD MECHANICAL SPLICER

INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



THREADING OF ASSEMBLIES

The threaded length "X" shall be no more than $L/2$. The bar should be tightened until 0-1 thread(s) is/are exposed.

Notes:

Notes:
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

4-4-2025

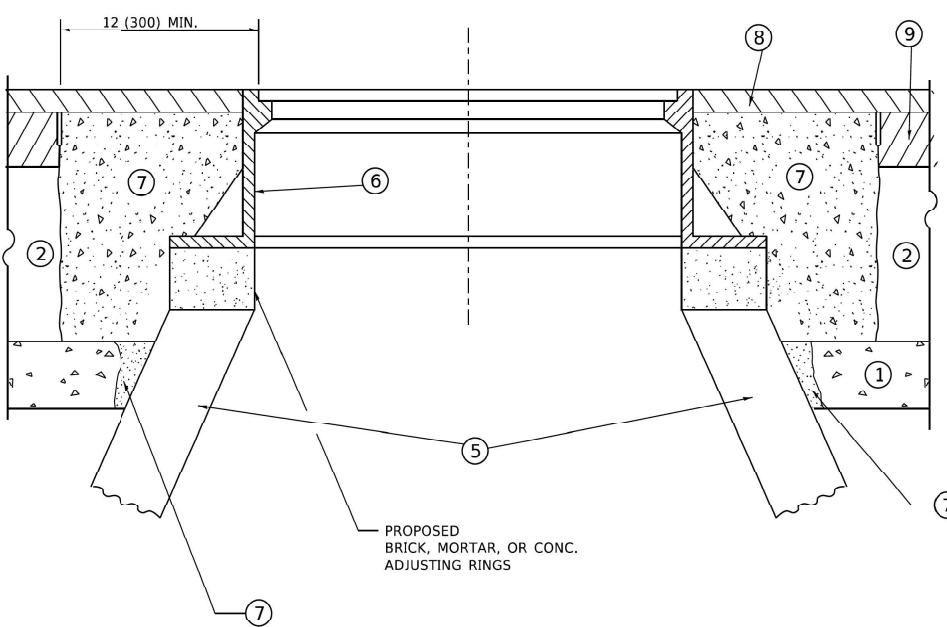
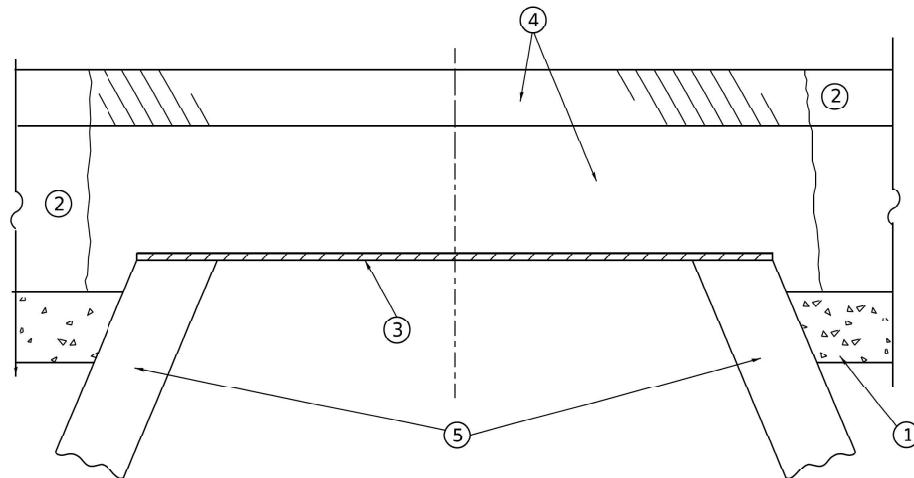


USER NAME	= lyanisch	DESIGNED	-	JRR	REVISED
		DRAWN	-	JRR	REVISED
		CHECKED	-	WJV	REVISED
PLOT DATE	= 6/27/2025	DATE	-	09/26/2025	REVISED



BAR SPlicer ASSEMBLY AND MECHANICAL SPlicer DETAILS
STRUCTURE NO. 099-0298

STRUCTURE NO. 6295 CONTRACT NO. 62Y26



DETAILS FOR FRAMES AND LIDS ADJUSTMENT

WITH MILLING

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.01 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-2* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

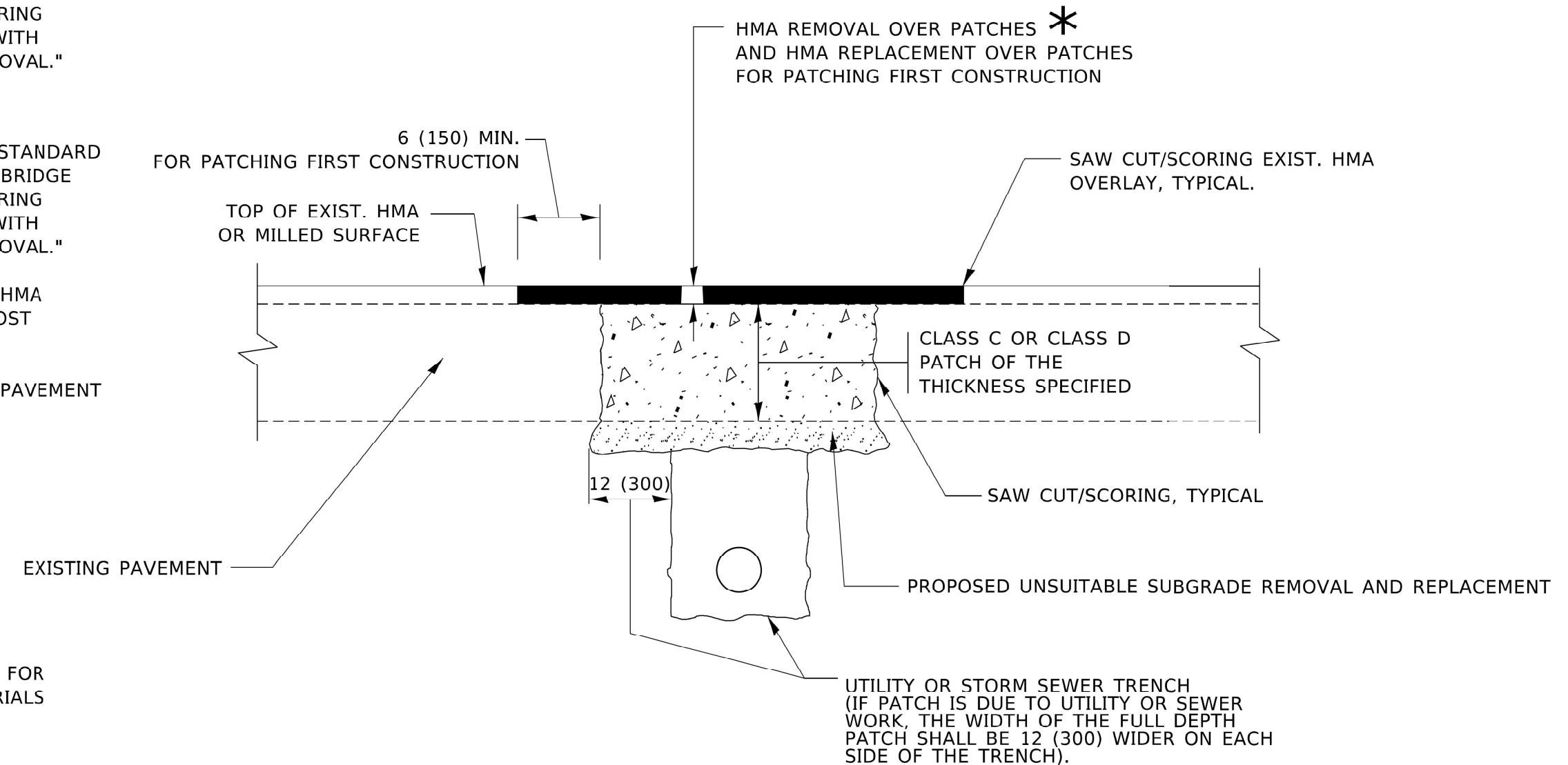
FILE NAME: wsd1	USER NAME	- Lawrence.DeManche	DESIGNED	R. SHAH	REVISED	R. BORO 03-09-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR					F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHE NO
	DRAWN	-	REVISED	-	R. BORO	12-06-11		840	(143B-R-1(90)) BR 25					WILL	45	36	
	PLOT SCALE	= 100.0000 ' / in.	CHECKED	-	REVISED	-		FRAMES AND LIDS ADJUSTMENT WITH MILLING					BD600-03 (BD-08)			CONTRACT NO. 62Y26	
	PLOT DATE	= 9/15/2023	DATE	-	10-25-94	REVISED	-	K. SMITH	09-15-23	SCALE: NONE	1	OF 1	Sheets	STA.	TO STA.	ILLINOIS	FED. AID PROJECT

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

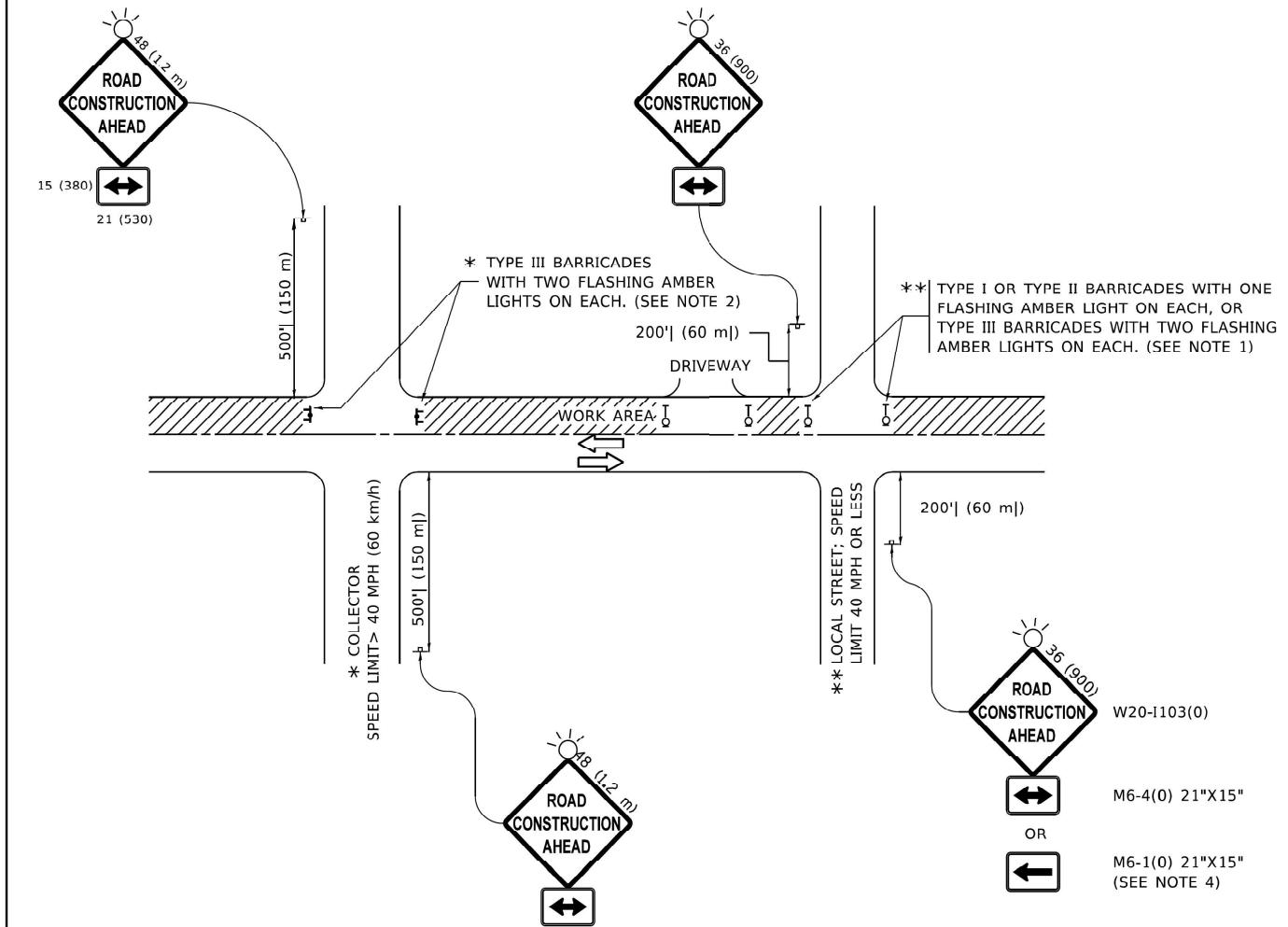
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME	DESIGNED	DRAWN	REVISED	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Lawrence.DeManche	R. SHAH	-	R. BORO 01-01-07				
			REVISED - R. BORO 09-04-07				
PLOT SCALE = 100.0000' / in.	CHECKED	-	REVISED - K. ENG 10-27-08				
PLOT DATE = 11/18/2022	DATE	- 10-25-94	REVISED - K. SMITH 11-18-22				
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			
SCALE: NONE	SHEET 1	OF 1	SHEETS STA. TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
				840	(143VB-R-1(90) BR 25	WILL	45 39
					BD400-04 (BD-22)		CONTRACT NO. 62Y26
						ILLINOIS	FED. AID PROJECT



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

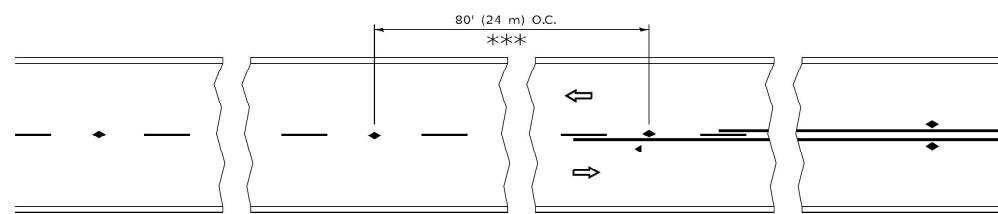
All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME	- Lawrence.DeManche	DESIGNED	L.H.A.	REVISED	T. RAMMACHER 01-06
		DRAWN	-	REVISED	- A. SCHUETZE 07-01
PLOT SCALE	= 100.0000 ' / in.	CHECKED	-	REVISED	- A. SCHUETZE 09-15
PLOT DATE	= 5/3/2024	DATE	- 06-89	REVISED	- D. SENDERAK 05-03

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

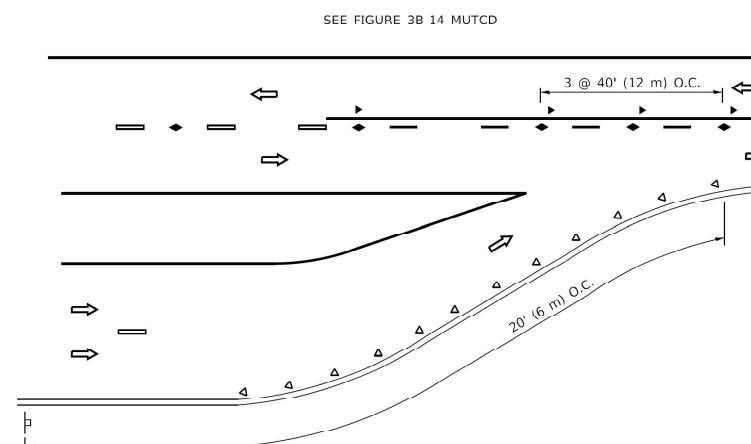
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SEE NO.
		840	(143VB-R-1(90)) BR 25	WILL	45	40
		TC-10		CONTRACT NO. 62Y26		
SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.			ILLINOIS	FED. AID PROJECT	

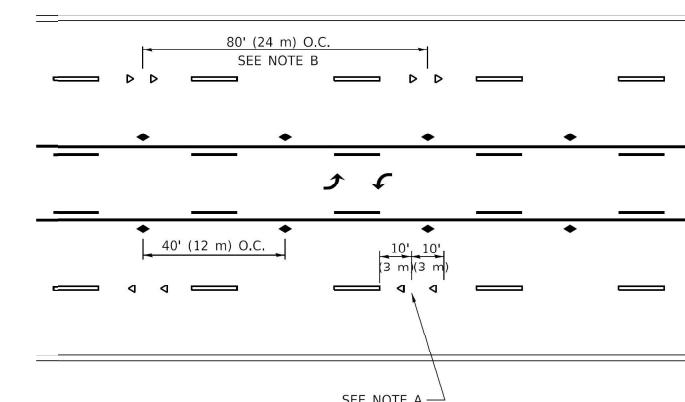


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

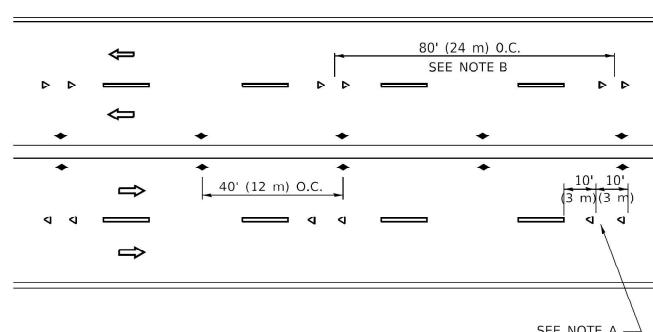
TWO-LANE/TWO-WAY



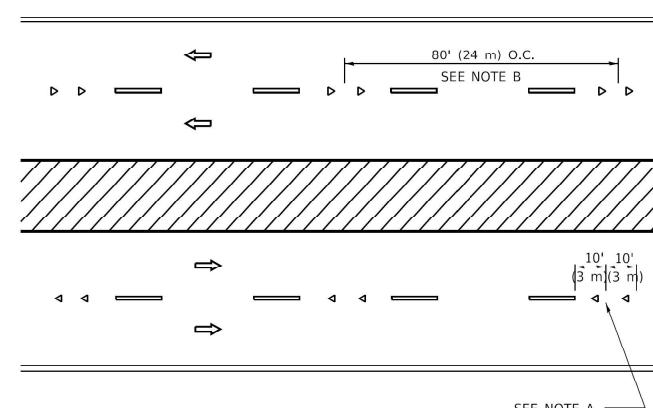
LANE REDUCTION TRANSITION



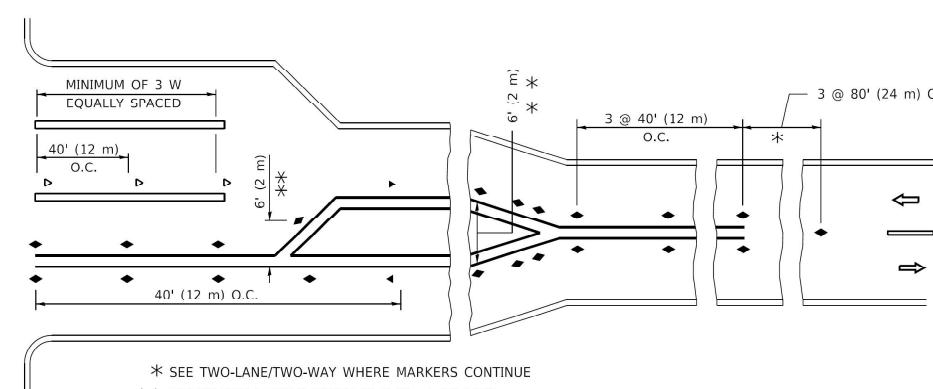
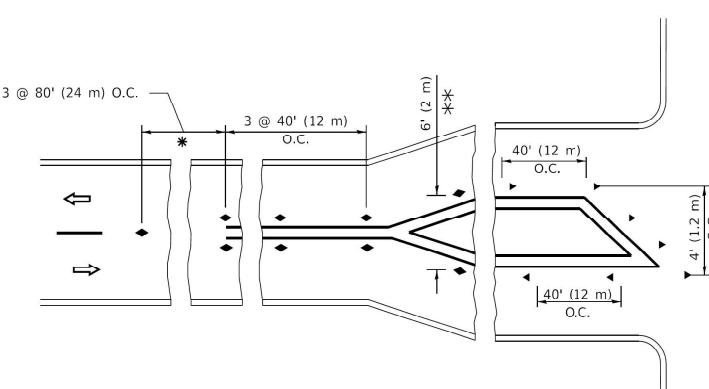
TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED



TURN LANES

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLO STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME - footermj	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50,000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

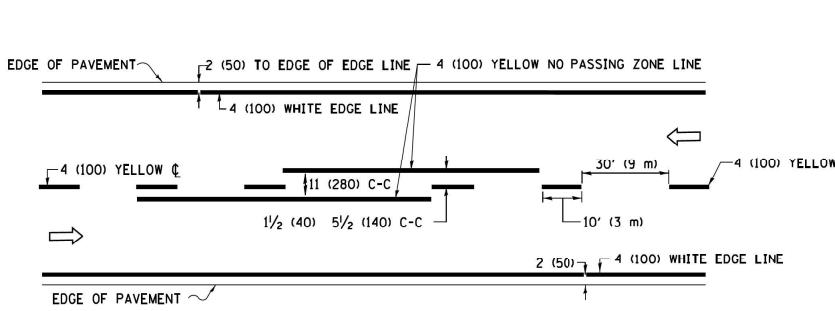
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

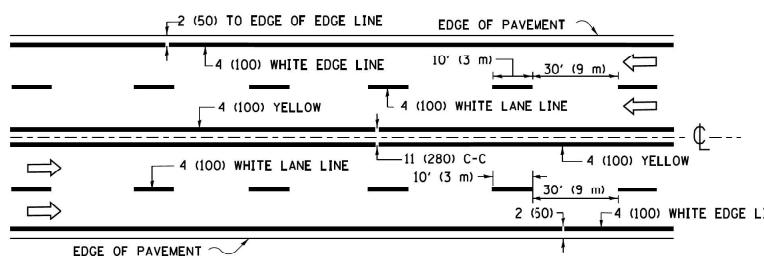
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	41
TC-11		CONTRACT NO. 62Y26		

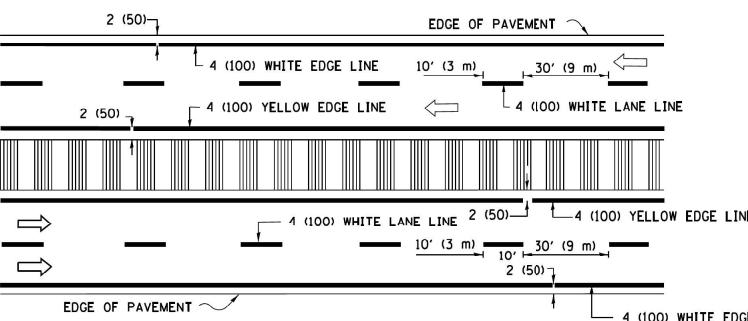
ILLINOIS FED. AID PROJECT



2-LANE ROADWAY

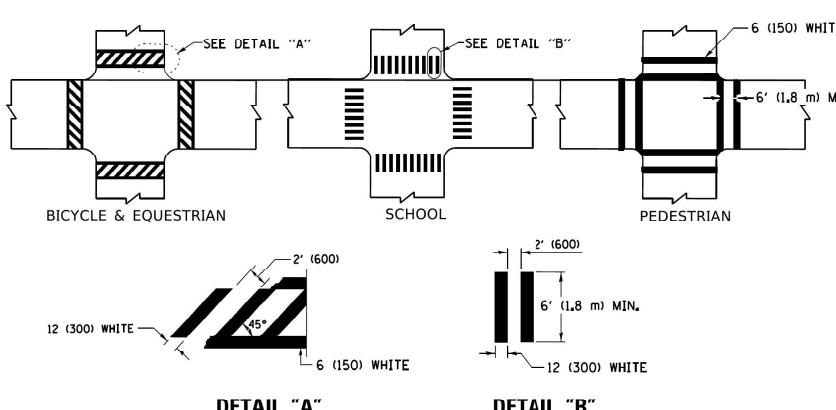


MULTI-LANE UNDIVIDED



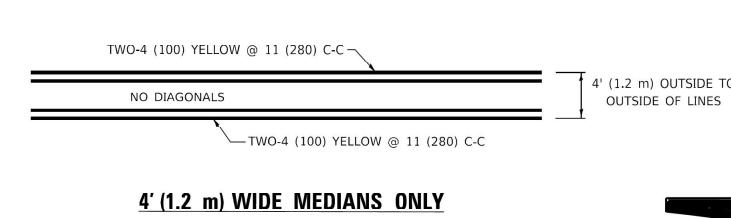
MULTI-LANE DIVIDED

TYPICAL LANE AND EDGE LINE MARKING

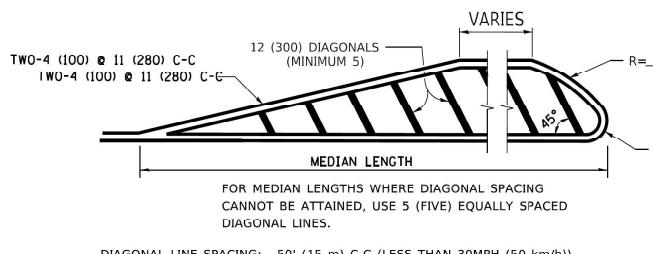


TYPICAL CROSSWALK MARKING

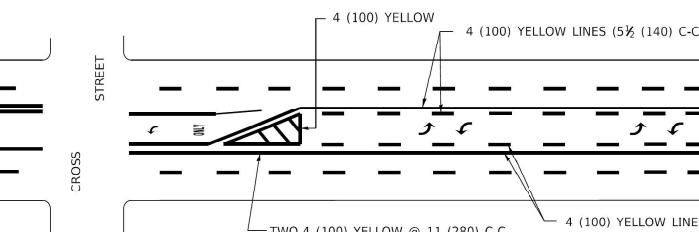
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



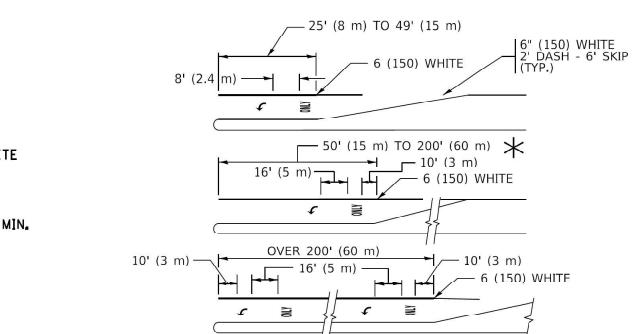
4' (1.2 m) WIDE MEDIAN ONLY



MEDIAN OVER 4' (1.2 m) WIDE

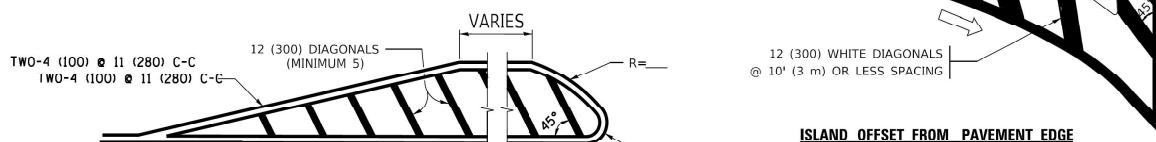


TYPICAL PAINTED MEDIAN MARKING

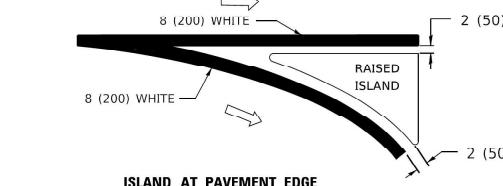


TYPICAL LEFT (OR RIGHT) TURN LANE

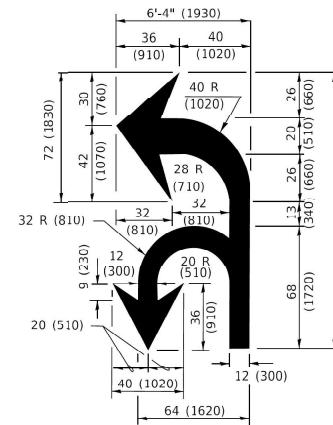
TYPICAL TURN LANE MARKING



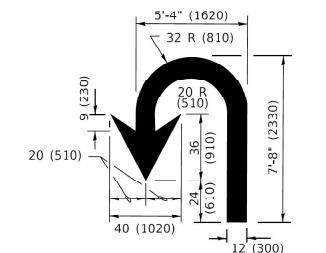
ISLAND OFFSET FROM PAVEMENT EDGE



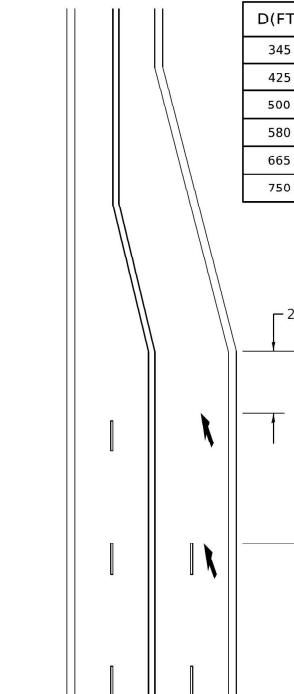
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN



LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIAN IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID IN PAIRS	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK IF PRESENT; OTHERWISE PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIAN	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIAN	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDER > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT WHITE - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 LANE COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME - footermj	DESIGNED - EVER	REVISED - C. JUCIUS 09-09-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -		REVISED - C. JUCIUS 07-01-13		840	(143VB-R-1(90)) BR 25	WILL	45	42
PLOT SCALE = 50,000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15		TC-13	CONTRACT NO. 62Y26			
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16		SCALE: NONE	1 OF 2 SHEETS STA. TO STA.	ILLINOIS	FED. AID PROJECT	

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

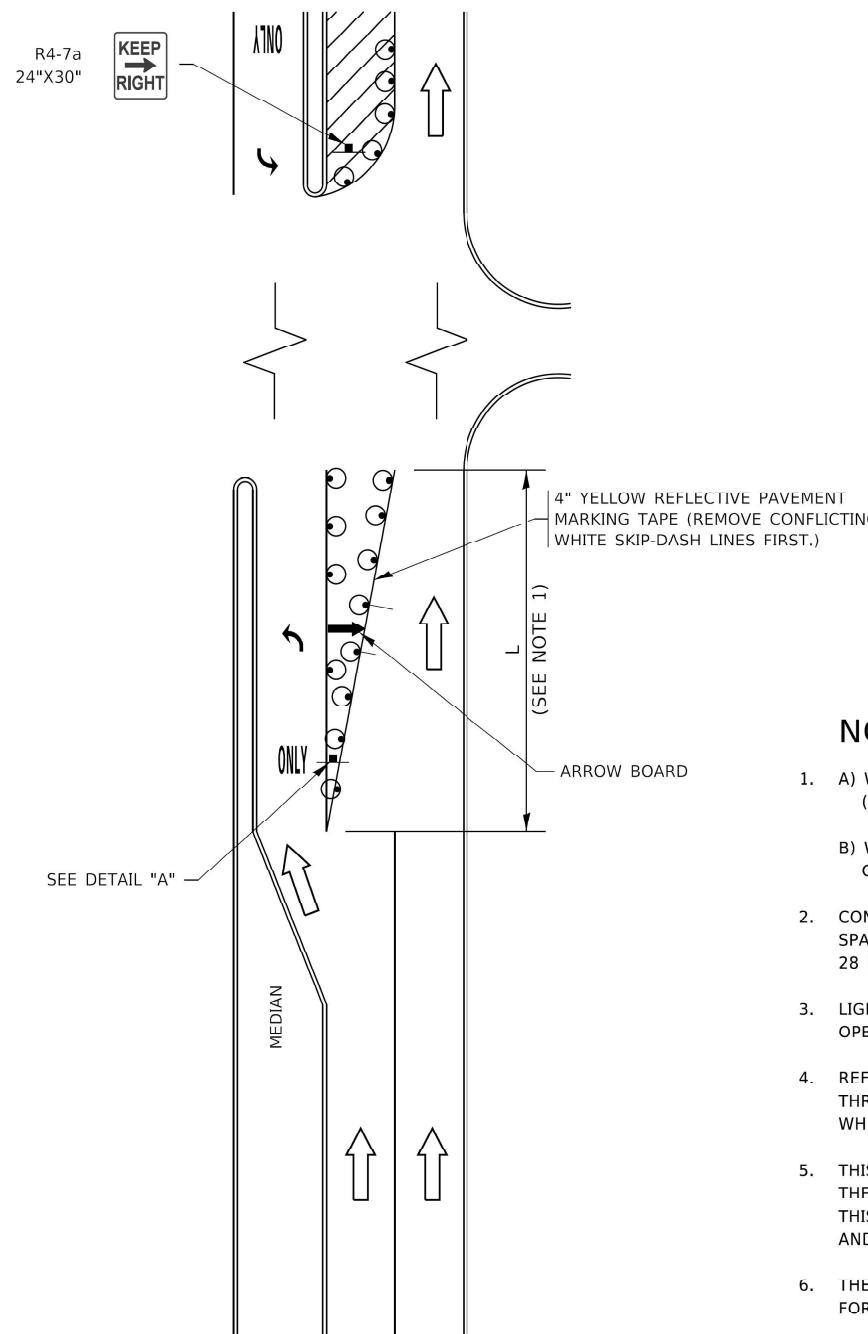
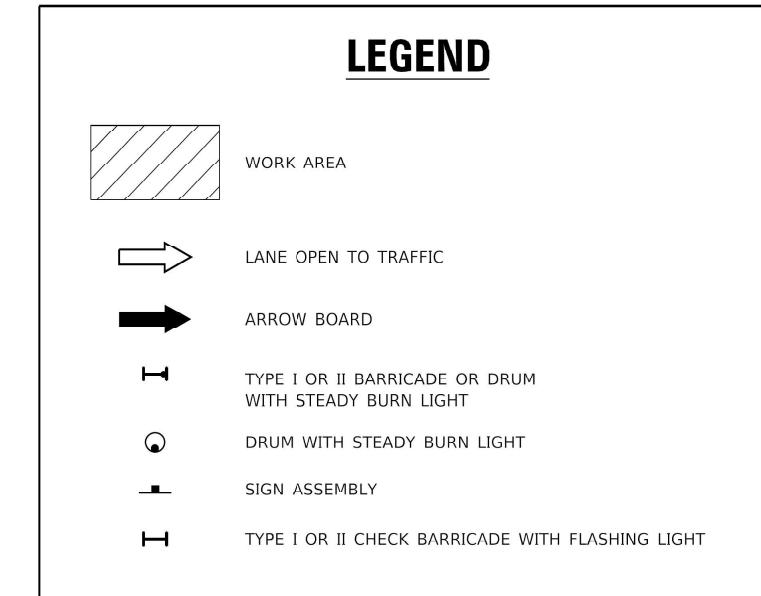


FIGURE 1



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

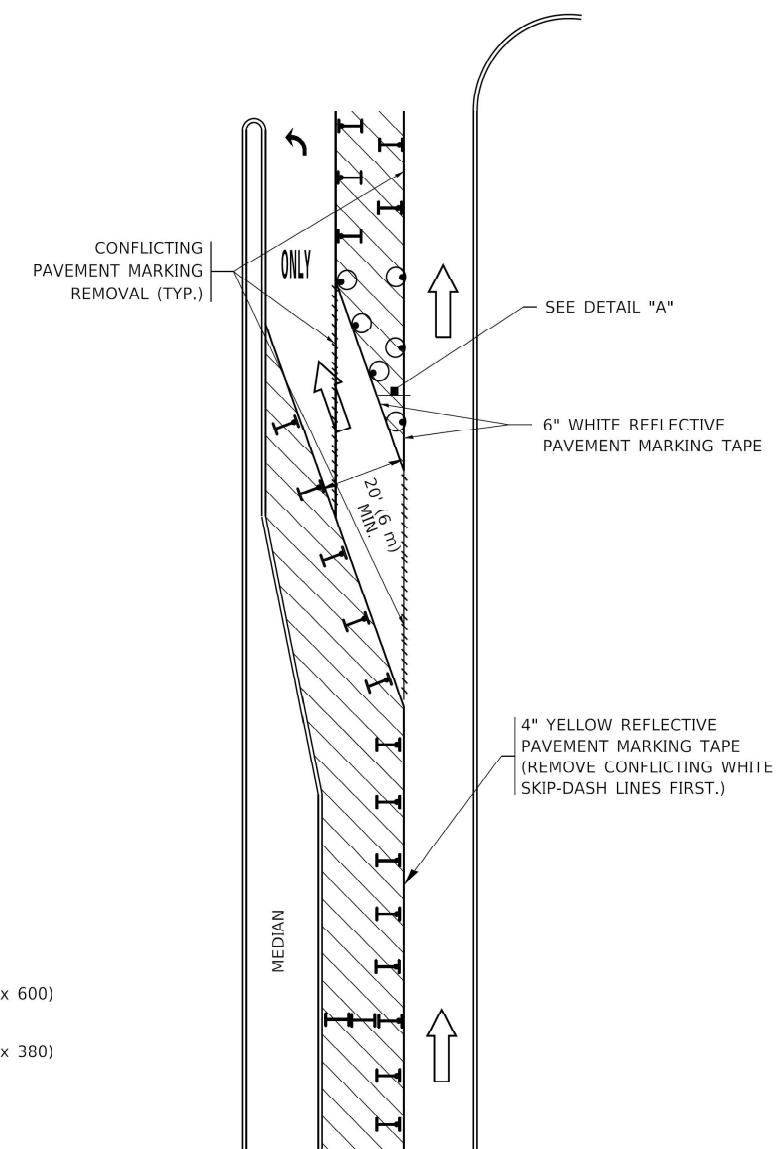
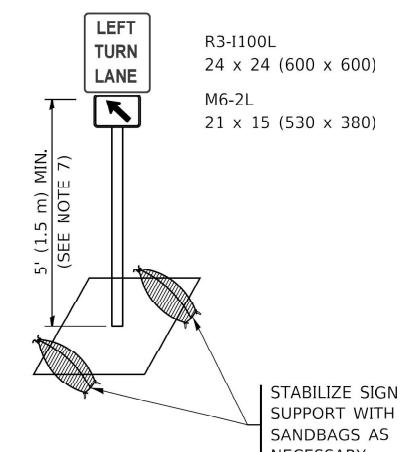
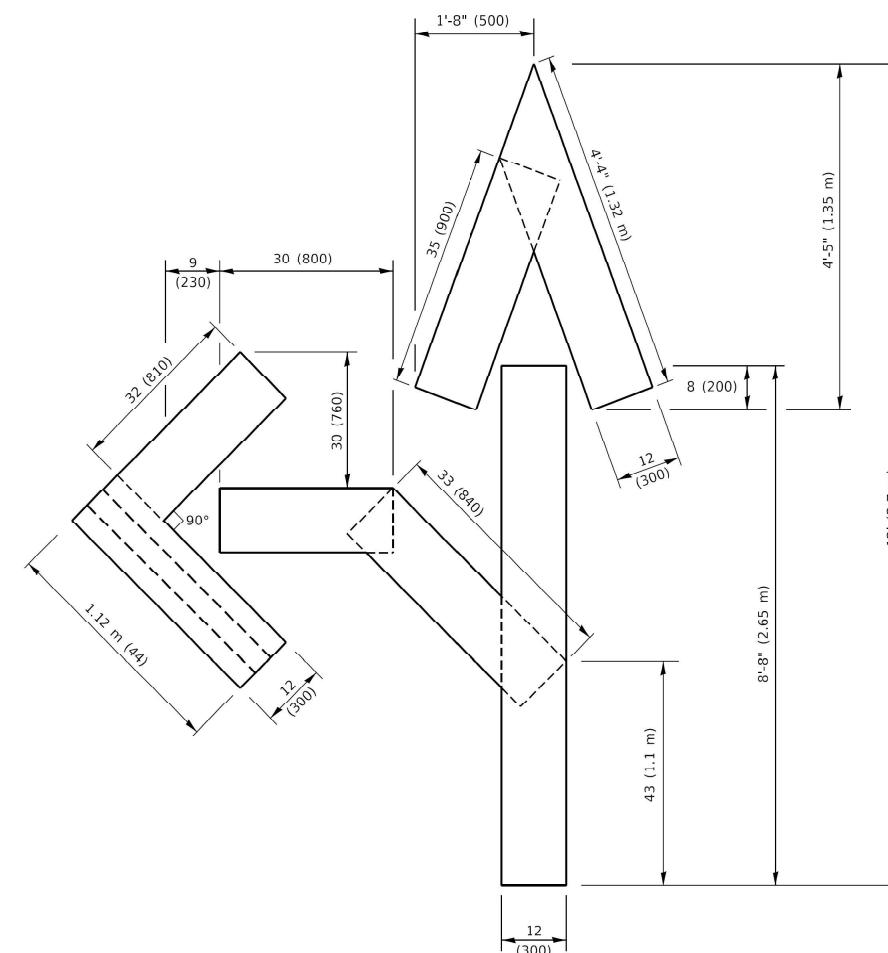
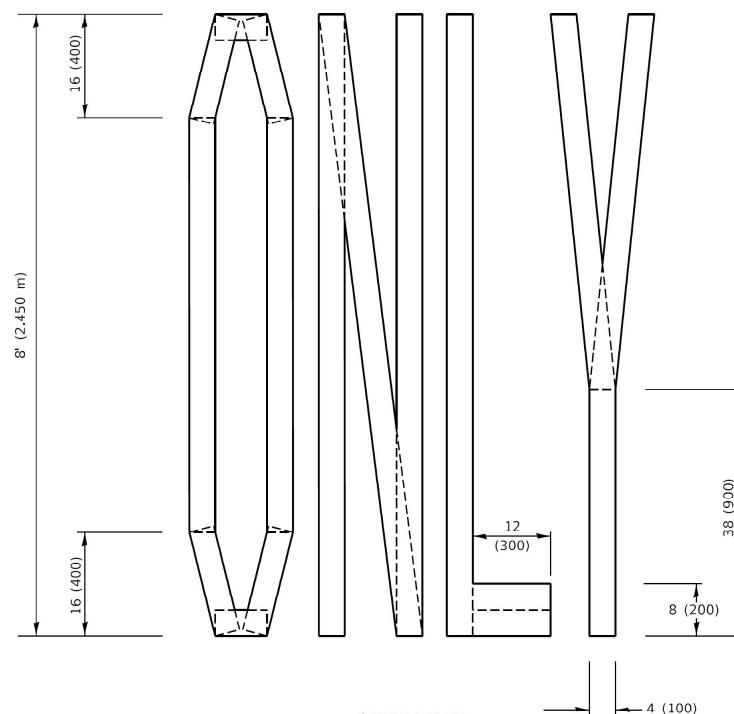
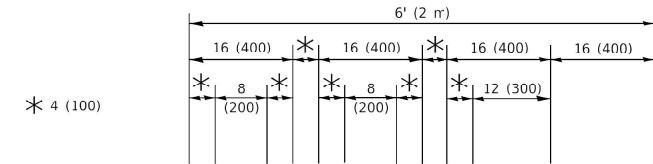
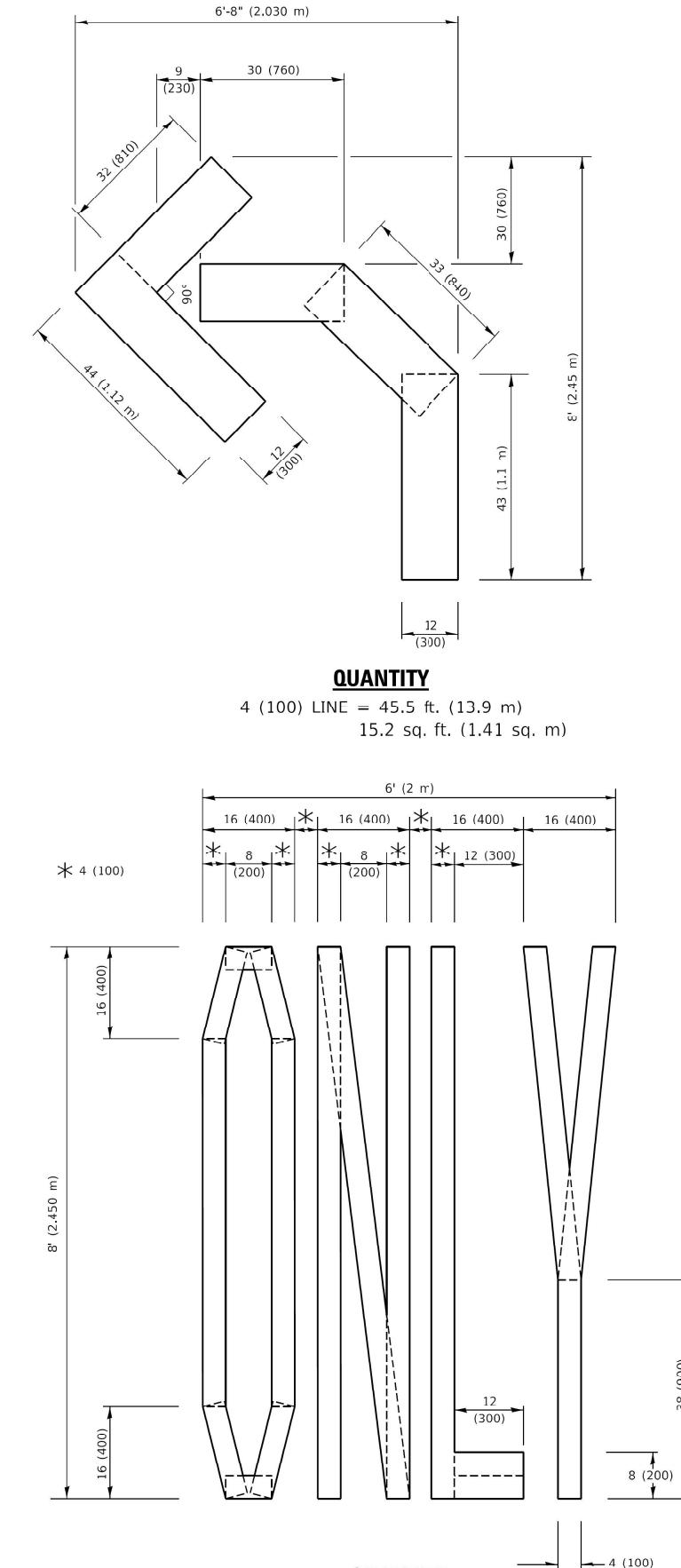


FIGURE 2



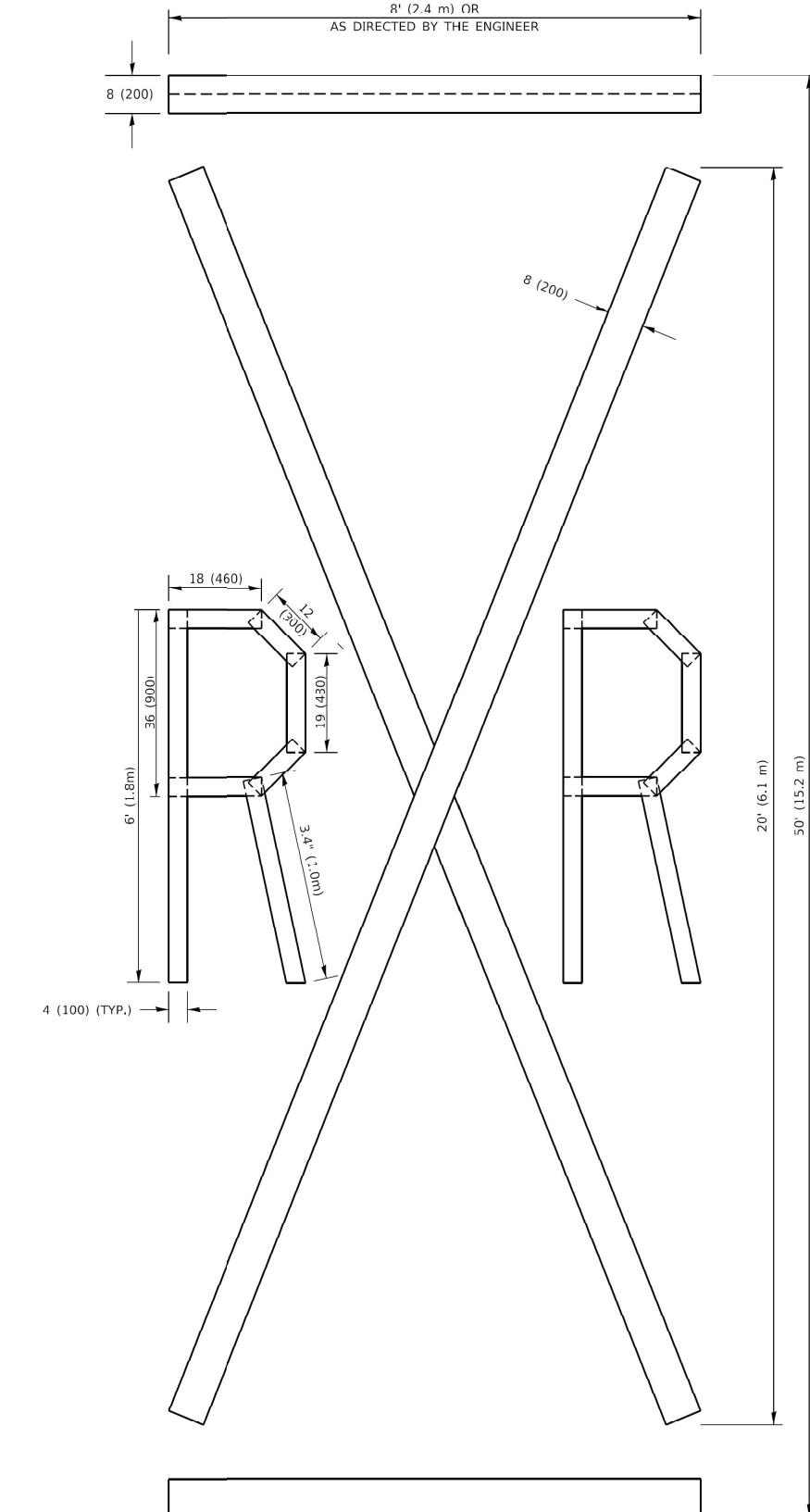
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.



NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



All dimensions are in inches (millimeters)
unless otherwise shown.

USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
PLOT SCALE = 50.0068 ' / in.	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

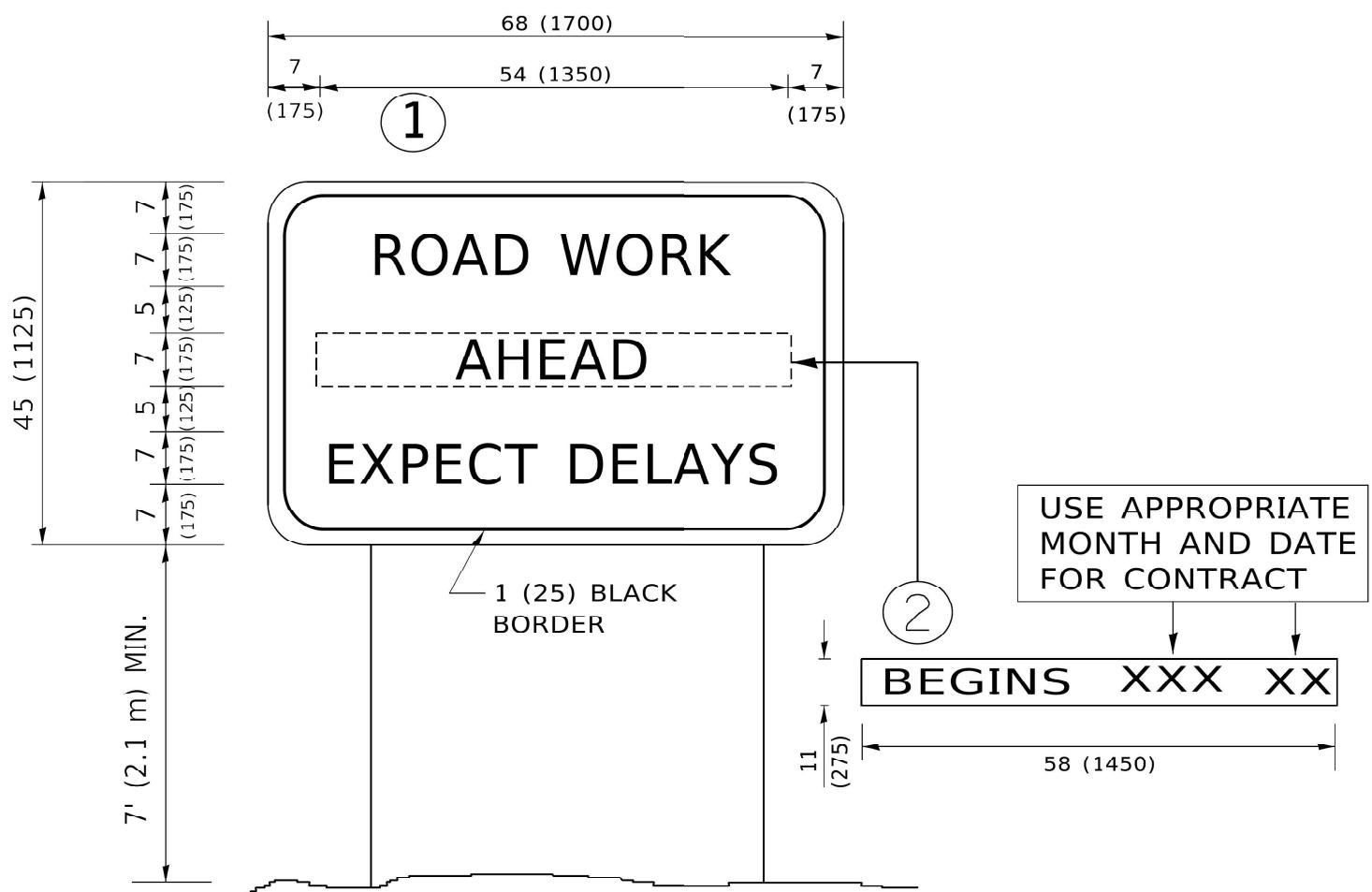
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	44
TC-16		CONTRACT NO. 62Y26		

ILLINOIS FED. AID PROJECT



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

USER NAME - footermj	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISED - R. MIRS 12-11-97	840		(143VB-R-1(90)) BR 25	WILL	45	45				
CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22								CONTRACT NO. 62Y26	
PLOT SCALE = 50,0000 ' / in.	REVISED - C. JUCIUS 01-31-07	SCALE: NONE		1	OF 1	SHEETS STA.	TO STA.	ILLINOIS	FED. AID PROJECT		
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07									