

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL.	45	1
		ILLINOIS	CONTRACT NO. 62Y26	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED IN
THE VILLAGE OF MONEE

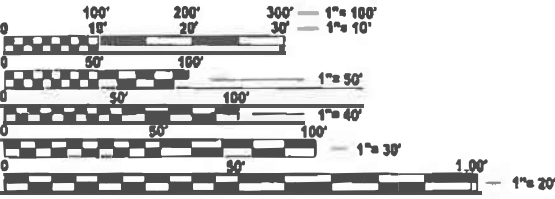
DESIGN DESIGNATION
IL ROUTE 50 - MINOR ARTERIAL

TRAFFIC DATA
ADT: 5,800 VPD (2023)
POSTED SPEED: 45 MPH
DESIGN SPEED: 50 MPH

V3 Companies
7325 Janes Avenue
Woodridge, IL 60517
630.724.9200 phone
630.724.9202 fax
www.v3co.com

Accurate
GROUP, INC.

SWE SOLUTIONS

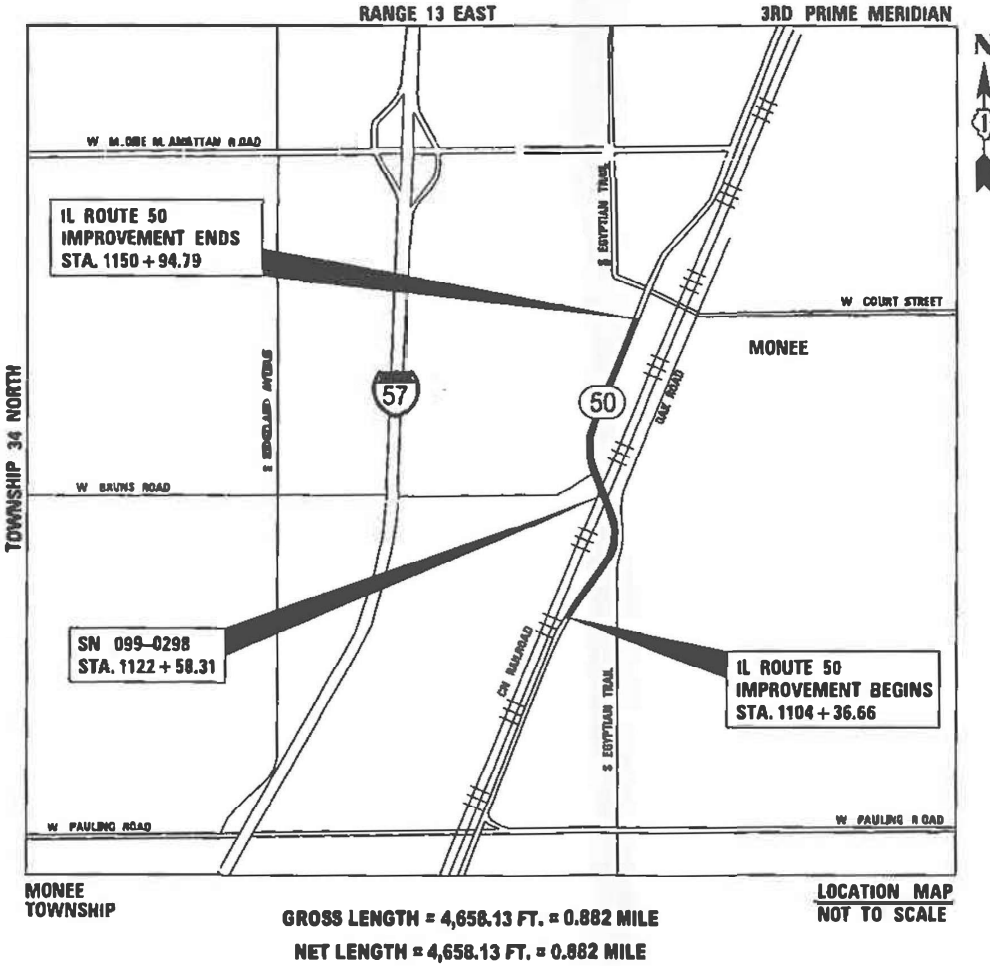


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: MR. D. WILGREEN, P.E.
(847) 705-4240
PROJECT MANAGER: MR. J. ALAIN MIDY, P.E.
(847) 221-3056
CONTRACT NO. 62Y26

PROPOSED
HIGHWAY PLANS
FAP ROUTE 840: IL RTE 50 (S. GOVERNORS HIGHWAY)
LIMIT: AT CN RAILROAD
SECTION: (143VB-R-1(90)) BR 25
PROJECT: BR-181L(103)
BRIDGE DECK OVERLAY AND JOINT REPLACEMENT
WILL COUNTY
C-91-222-25



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Dec 2nd 2025 IR

David J. Wilkinson REGIONAL ENGINEER

January 23, 2026

David J. Wilkinson ENGINEER OF DESIGN AND ENVIRONMENT

January 23, 2026

David J. Wilkinson DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

<p>V3 COMPANIES LTD. JACOB W. BAUER # 062-064761</p> <p><u>Jacob W. Bauer</u></p> <p>DATE: 09-26-2025 SIGNATURE AND SEAL APPLY TO SHEETS: 1-6, 8, 10-11, 36-45</p> <p>ILLINOIS</p> <p>EXPIRATION DATE: 11-30-2027</p>	<p>V3 COMPANIES LTD. ZAIDA MORILLO # 081-008252</p> <p><u>Zaida Morillo</u></p> <p>DATE: 09-26-2025 SIGNATURE AND SEAL APPLY TO SHEETS: 23-37</p> <p>ILLINOIS</p> <p>EXPIRATION DATE: 11-30-2026</p>	<p>ACCURATE GROUP, INC. MO'AMEN ALKUFAYHY # 062-074880</p> <p><u>Mo'amen Alkufayhy</u></p> <p>DATE: 09-26-2025 SIGNATURE AND SEAL APPLY TO SHEETS: 12-19</p> <p>ILLINOIS</p> <p>EXPIRATION DATE: 11-30-2027</p>	<p>SWE SOLUTIONS DAVID J. WILKINSON # 062-074880</p> <p><u>David J. Wilkinson</u></p> <p>DATE: 09-26-2025 SIGNATURE AND SEAL APPLY TO SHEETS: 7, 9, 20-22</p> <p>ILLINOIS</p> <p>EXPIRATION DATE: 11-30-2027</p>
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INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES, INDEX OF SHEETS AND HIGHWAY STANDARDS
- 3 - 6 SUMMARY OF QUANTITIES
- 7 TYPICAL SECTIONS
- 8 SCHEDULE OF QUANTITIES
- 9 REMOVAL PLAN
- 10 ROADWAY PLAN
- 11 BRIDGE APPROACH DETAILS
- 12 MAINTENANCE OF TRAFFIC GENERAL NOTES AND SEQUENCE
- 13 MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
- 14 - 19 MAINTENANCE OF TRAFFIC PLAN
- 20 - 22 PAVEMENT MARKING PLAN
- 23 - 37 STRUCTURAL SHEETS (S.N. 099-0298)
- 38 BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
- 39 BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
- 40 TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
- 41 TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
- 42 TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- 43 TC-14 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
- 44 TC-16 SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
- 45 TC-22 ARTERIAL ROAD INFORMATION SIGN

HIGHWAY STANDARDS

- 000001-09 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 442101-09 CLASS B PATCHES
- 606401-02 PAVED DITCH
- 630001-13 STEEL PLATE BEAM GUARDRAIL
- 701101-05 OFF-RD OPERATIONS, MULTILANE, 15" (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
- 701423-10 LANE CLOSURE, MULTILANE, WITH BARRIER, FOR SPEEDS > 45 MPH TO 55 MPH
- 701431-13 LANE CLOSURE, MULTILANE, UNDIVIDED WITH Crossover, FOR SPEEDS > 45 MPH TO 55 MPH
- 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
- 701611-01 URBAN ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701901-11 TRAFFIC CONTROL DEVICES
- 704001-08 TEMPORARY CONCRETE BARRIER
- 781001-04 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
- 782006-01 GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

DISTRICT 1 STANDARD DETAILS

- BD-08 FRAMES AND LIDS ADJUSTMENT WITH MILLING
- BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
- TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
- TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC-14 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
- TC-16 SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
- TC-22 ARTERIAL ROAD INFORMATION SIGN

GENERAL NOTES

- 1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED UTILITY FACILITIES. 48 HOUR NOTICE IS REQUIRED.
- 2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD CHECK ALL DIMESNONS AND ELEVATIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIAL OR BEGINNING CONSTRUCTION. ANY DISCREPANCIES FROM THE PLANS SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.
- 3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, WILL COUNTY, AND THE VILLAGE OF MONEE.
- 4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE APPROPRIATE DEPARTMENT.
- 5. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC FIELD AREA ENGINEER ERIC CAMPOS, AT ERIC.CAMPOS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- 6. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- 7. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH SHALL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE PROMPTLY REPAIRED AT THE CONTRACTOR'S EXPENSE.
- 8. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES AND SIDE ROADS AT ALL TIMES DURING CONSTRUCTION OPERATIONS, UNLESS OTHERWISE NOTED ON THE PLANS.
- 9. DURING CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN ALL SURFACE DRAINAGE WITHIN THE PROJECT LIMITS. ALL STORM FLOW MUST BE MAINTAINED AT ALL TIMES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- 10. WHENEVER LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINES OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC., SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- 11. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS NOT AUTHORIZED IN THE REMOVAL PLANS ARE TO BE REPLACED PROMPTLY AT THE CONTRACTOR'S EXPENSE.
- 12. BEFORE BEGINNING ANY WORK, THE CONTRCTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- 13. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

COMMITMENTS

NONE



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7325 Janes Avenue
Woodridge, IL 60517
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	DRAWN - DRP	REVISED -
	CHECKED - JWB	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, INDEX OF SHEETS
AND HIGHWAY STANDARDS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	2
		CONTRACT NO. 62Y26		
		ILLINOIS	FED. AID PROJECT	

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SPECIALTY ITEM	CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE	
					0059 80% FEDERAL 20% STATE	
	28000510	INLET FILTERS	EACH	2	2	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	382	382	
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	591	591	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	432	432	
	40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	84	84	
	42000060	WELDED WIRE REINFORCEMENT	SQ YD	81	81	
	44004000	PAVED DITCH REMOVAL	FOOT	80	80	
	44201053	CLASS B PATCHES, TYPE IV, 17 INCH	SQ YD	81	81	
	44201299	DOWEL BARS 1 1/2"	EACH	154	154	
	44213200	SAW CUTS	FOOT	367	367	
	50102400	CONCRETE REMOVAL	CU YD	23.8	23.8	
	50157300	PROTECTIVE SHIELD	SQ YD	609	609	
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	22.5	22.5	
	50300300	PROTECTIVE COAT	SQ YD	2,202	2,202	
	50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	3.4	3.4	
	50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	69	69	
	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3,010	3,010	
	50800515	BAR SPLICERS	EACH	12	12	
	52000110	PREFORMED JOINT STRIP SEAL	FOOT	186	186	



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REVISED -
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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	3
		CONTRACT NO. 62Y26		
		ILLINOIS	FED. AID PROJECT	

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SPECIALTY ITEM	CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE	
					0059 80% FEDERAL 20% STATE	
	52100520	ANCHOR BOLTS, 1"	EACH	3	3	
	58700300	CONCRETE SEALER	SQ FT	369	369	
	60615400	PAVED DITCH, TYPE A-15	FOOT	80	80	
*	63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	37.5	37.5	
	63200310	GUARDRAIL REMOVAL	FOOT	37.5	37.5	
	67100100	MOBILIZATION	L SUM	1	1	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	56	56	
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	15,360	15,360	
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	824	824	
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	825	825	
	70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	41,148	41,148	
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	825	825	
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	700	700	
	70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
	70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,702	9,702	
*	78004635	PREFORMED PLASTIC PAVEMENT MARKING - TYPE D - STANDARD - LINE 7"	FOOT	190	190	
*	78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,520	1,520	
*	78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	190	190	



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DATE -	09/26/2025

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE	SHEET 2 OF 4 SHEETS	STA.	TO STA.
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	4
		CONTRACT NO. 62Y26		
		ILLINOIS	FED. AID PROJECT	

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SPECIALTY ITEM	CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE	
					0059 80% FEDERAL 20% STATE	
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	12	12	
	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	8	8	
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	5,016	5,016	
	X0325748	ACRYLIC COATING	SQ YD	13	13	
	X0325749	FIBER WRAP	SQ FT	119	119	
	X2010516	SELECTIVE CLEARING	UNIT	1	1	
	X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	1,659	1,659	
	X5060700	CLEANING AND PAINTING BEARINGS	EACH	16	16	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	2	
	X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12	
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
	X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	169	169	
	X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	449	449	
*	X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	449	449	
	Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	2	2	
	53101410	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	2,158	2,158	
	53101002	BRIDGE DECK SCARIFICATION, 3/4 INCH	SQ YD	2,178	2,178	
	53212754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	53	53	
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	



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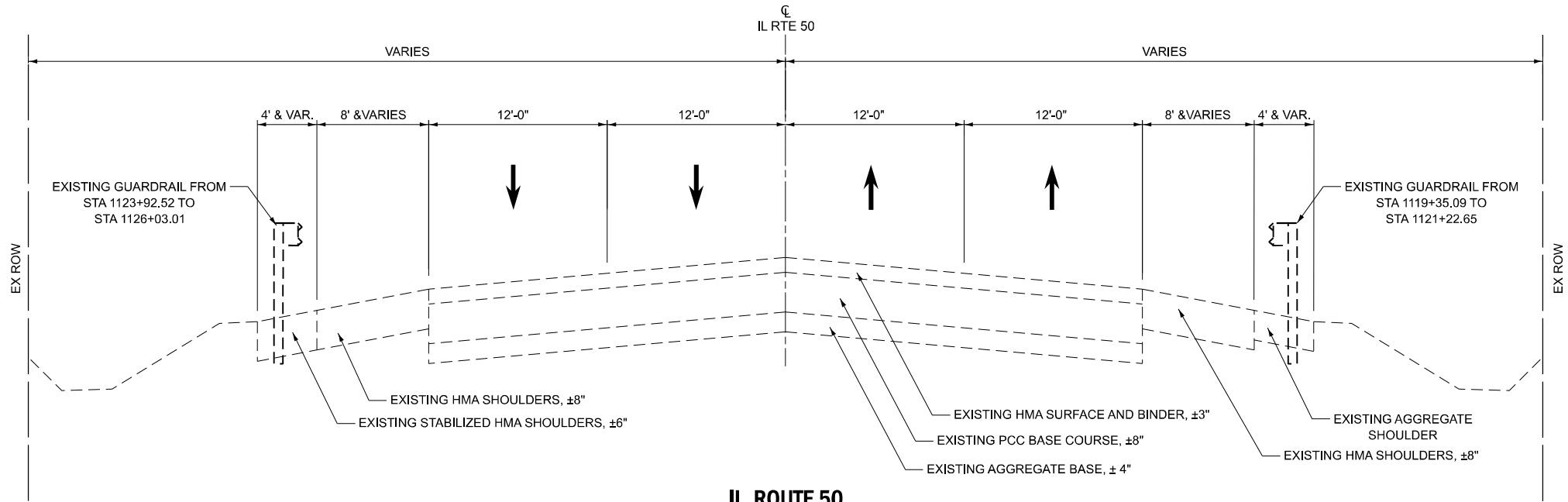
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 3 OF 4 SHEETS STA. TO STA.

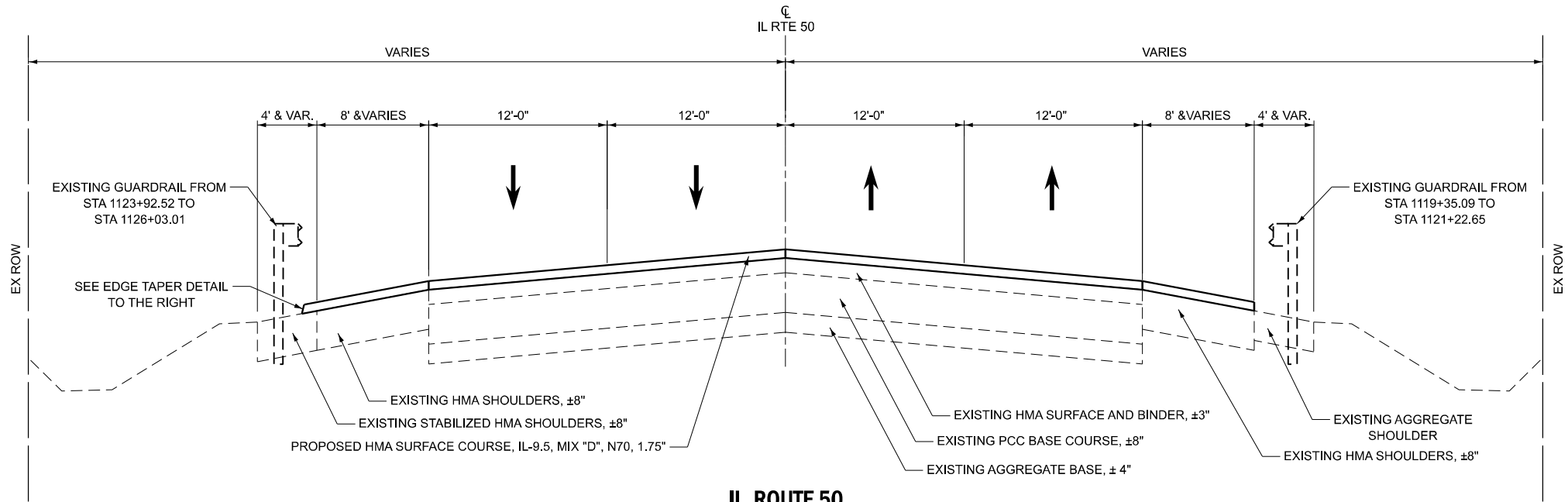
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	5
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

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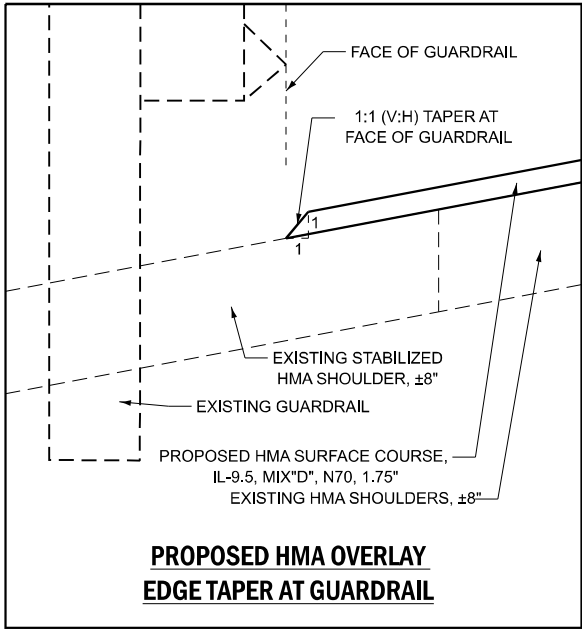
IL ROUTE 50
EXISTING TYPICAL SECTION LOOKING NORTH

STA 1109+28.48 TO STA 1120+35.14
STA 1124+17.32 TO STA 1135+81.33
BRIDGE OMISSION STA 1120+39.19 TO STA 1124+12.34



IL ROUTE 50
PROPOSED TYPICAL SECTION LOOKING NORTH

STA 1109+28.48 TO STA 1120+35.14
STA 1124+17.32 TO STA 1135+81.33
BRIDGE OMISSION STA 1120+39.19 TO STA 1124+12.34



PROPOSED HMA OVERLAY
EDGE TAPER AT GUARDRAIL

HOT-MIX ASPHALT REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QMP DESIGNATION
IL ROUTE 50 SHOULDER / BRIDGE APPROACH OVERLAY / BUTT JOINT		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 1.75"	4% @ 70 GYR	QC/QA
QMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

NOTES

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIAL SPECIFICATIONS.



USER NAME = Iyanisch	DESIGNED - AS	REVISED -
	DRAWN - AS	REVISED -
	CHECKED - LRY	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
BRIDGE REPAIRS ON IL 50 OVER CN RR

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	7
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

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IL ROUTE 50 PAVEMENT MARKINGS SCHEDULE							
LOCATION		THERMOPLASTIC PAVEMENT MARKING - LINE 4"		MODIFIED URETHANE PAVEMENT MARKING - LINE 4"		PREFORMED PLASTIC PAVEMENT MARKING, TYPE D- STANDARD- LINE 7"	GROOVING FOR RECESSED PAVEMENT MARKING 8"
STATION	STATION	WHITE	YELLOW	WHITE	YELLOW	WHITE	FOOT
		FOOT		FOOT		FOOT	
SOUTH OF BRIDGE							
1099+39.90	1106+20.35	170					
1106+77.03	1120+80.36	351					
1113+95.90	1120+44.22	648					
1106+76.60	1120+70.96		2,783				
1110+70.75	1120+56.23	246					
1117+75.23	1120+92.19	317					
ON BRIDGE							
1120+92.15	1124+72.07			380			
1120+44.24	1124+24.74			381			
1120+80.30	1124+59.99					95	95
1120+56.24	1124+36.08					95	95
1120+68.12	1124+48.01				760		
1120+38.13	1124+78.04						
NORTH OF BRIDGE							
1124+34.34	1124+87.31	53					
1124+72.07	1129+85.21	513					
1124+17.32	1125+90.06	173					
1126+81.04	1130+49.98	369					
1124+71.96	1136+24.95	288					
1124+36.07	1141+41.85	426					
1124+48.04	1141+30.00		3,364				
TOTAL		9,702		1,520		190	190

IL ROUTE 50 PAVEMENT MARKING REFLECTOR SCHEDULE				
LOCATION		RAISED REFELCTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT
STATION	STATION	EACH	EACH	EACH
SOUTH OF BRIDGE				
1105+10.00	1115+00.00		25	25
1115+00.00	1121+17.00		16	16
1105+10.00	1115+00.00		50	50
1115+00.00	1118+32.00		17	17
1099+40.00	1112+00.00		32	32
1109+60.00	1114+00.00		22	22
1114+00.00	1120+00.00		30	30
1110+60.00	1114+00.00		9	9
1114+00.00	1120+92.00		18	18
1120+38.14	1120+68.14	6		
NORTH OF BRIDGE				
1124+55.00	1136+25.00		30	30
1125+65.00	1130+00.00		22	22
1130+00.00	1137+25.00		37	37
1129+85.00	1130+00.00		1	1
1130+00.00	1136+25.00		16	16
1125+50.00	1129+00.00		18	18
1129+00.00	1141+30.00		62	62
1123+99.00	1141+30.00		44	44
1124+48.07	1124+78.07	6		
TOTAL		12	449	449



V3 Companies
7325 Janes Avenue
Woodridge, IL 60517
630.724.9200 phone
630.724.9202 fax
www.v3co.com

USER NAME = Iyanisch	DESIGNED - LRY	REVISED -
	DRAWN - DRP	REVISED -
	CHECKED - JWB	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

DESIGNED - LRY	REVISED -
DRAWN - DRP	REVISED -
CHECKED - JWB	REVISED -
DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.
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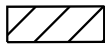
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	8
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	9
		CONTRACT NO. 62Y26		
ILLINOIS		FED. AID PROJECT		

PAVING LEGEND

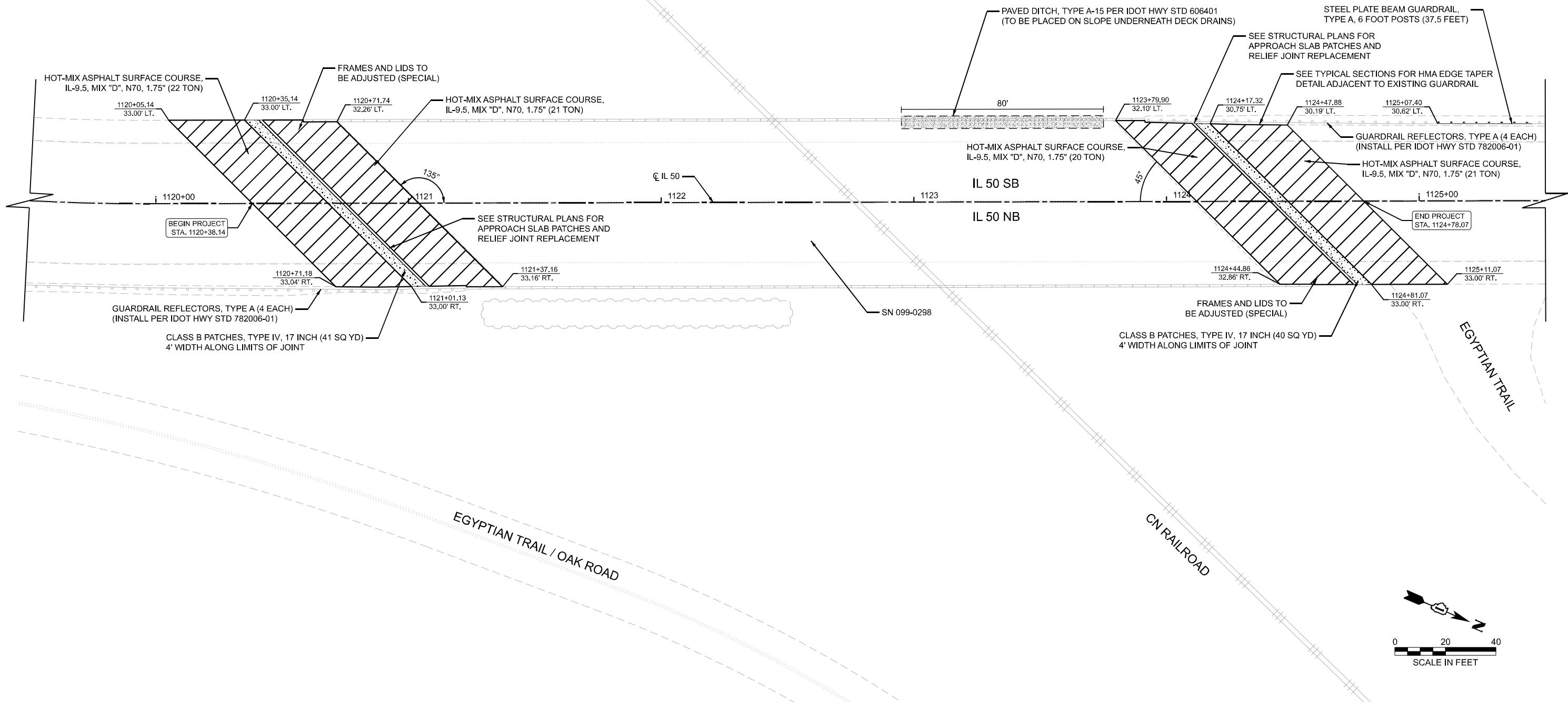


HMA SURFACE COURSE, IL-9.5, MIX "D", N70, 1.75"



CLASS B PATCHES, TYPE IV, 17 INCH

- NOTE:
1. LONGITUDINAL JOINT SEALANT SHALL BE PLACED UNDER HMA SURFACE MIX.
 2. SEE HMA MIX TABLE FOR MATERIAL SPECS.



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DATE - 09/26/2025

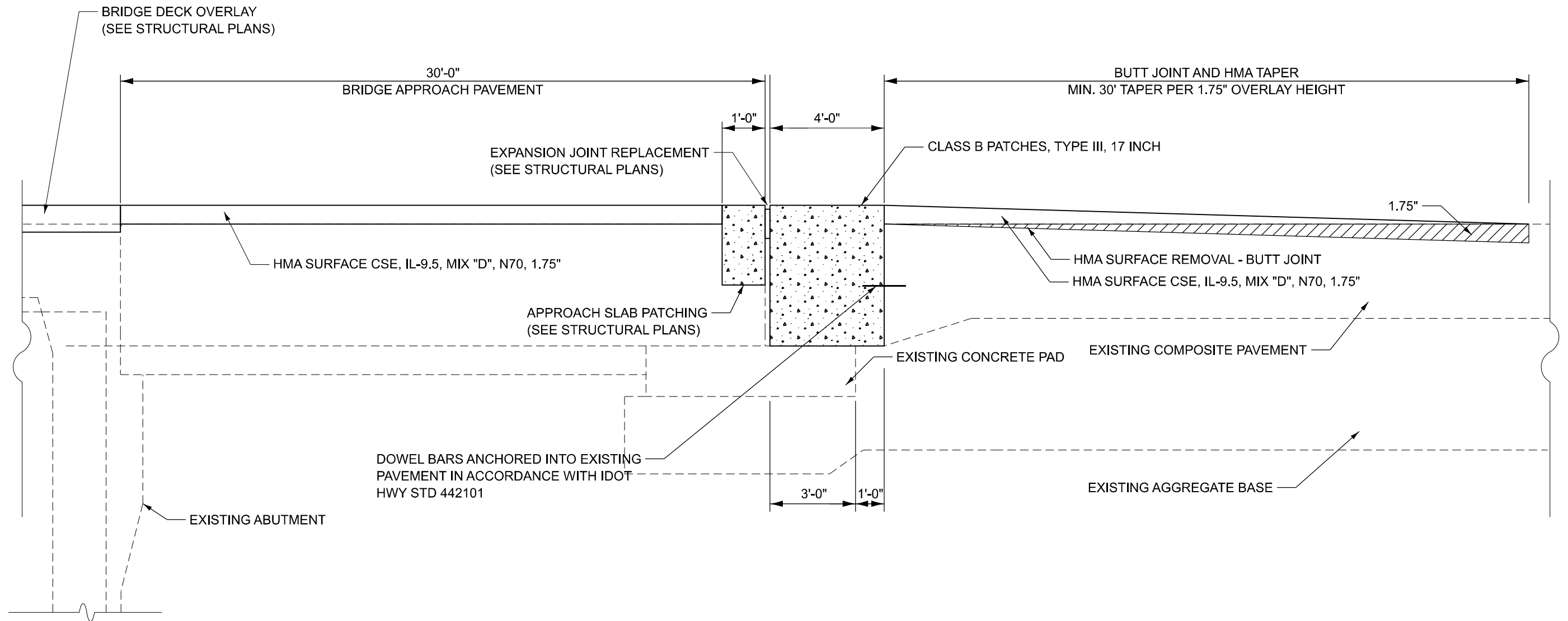
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REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
BRIDGE REPAIRS ON IL 50 OVER CN RR

SCALE: 1"=20' SHEET 1 OF 1 SHEETS STA. 1119+50.00 TO STA. 1125+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	10
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



APPROACH SLAB HMA OVERLAY, JOINT REPAIR, AND HMA BUTT JOINT DETAILS

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V3 Companies
7325 Janes Avenue
Woodridge, IL 60517
630.724.9200 phone
630.724.9202 fax
www.v3co.com

USER NAME = lyanisch
PLOT DATE = 6/27/2025

DESIGNED - LRY
DRAWN - DRP
CHECKED - JWB
DATE - 09/26/2025

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BRIDGE APPROACH DETAILS
BRIDGE REPAIRS ON IL 50 OVER CN RR

SCALE: NONE

SHEET 1 OF 1 SHEETS

STA.

TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	11
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR KALPANA KANNAN-HOSADURGA, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING OF WORK.
2.

THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGE OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
3.

CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUEING AND/ OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
4.

THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEERING FOR APPROVAL PRIOR TO IMPLEMENTING ANY CHANGES.
6.

PAVEMENT MARKING TAPE, TYPE IV SHOWN ON THE PLANS FOR ANY CONSTRUCTION STAGE THAT THE CONTRACTOR PROPOSES TO EXTEND OVER THE WINTER PERIOD SHALL MEAN MODIFIED URETHANE PAVEMENT MARKING AND WILL BE PAID FOR UNDER TEMPORARY PAVEMENT MARKING - PAINT AT THE CONTRACT UNIT PRICE.
7.

PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED ON ALL FINAL PAVEMENT SURFACES AND EXISTING PAVEMENT SURFACES TO REMAIN.
8.

ALL EXISTING LANE LINE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED VIA WATER BLASTING WITH VACUUM RECOVERY IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 4 DAYS. THE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN KIND AFTER THE COMPLETION OF THE STAGING.
9.

TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS THAT ARE LOCATED WITHIN A WORK AREA. THIS WORK SHALL BE PAID FOR PER DISTRICT 1 DETAIL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS SIGN RELOCATION WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.
10.

THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
11.

THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL PHASES OF CONSTRUCTION.
12.

TEMPORARY CONCRETE BARRIER SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE IDOT STANDARD SPECIFICATIONS. ALL TEMPORARY CONCRETE BARRIER APPROACH AND DEPARTING END UNITS SHALL BE ANCHORED TO THE PAVEMENT USING SIX ANCHOR PINS AS SHOWN IN IDOT STANDARD 704001. PINNING OF ADDITIONAL BARRIER UNITS WITH THREE ANCHOR PINS ON THE TRAFFIC SIDE HOLES WITHIN THE INSTALLATION SHALL BE REQUIRED WHEN EQUIPMENT, VEHICLES, MATERIALS, FIXED OBJECTS, OR A DROP-OFF IS LOCATED WITHIN 24" BEHIND THE BARRIER. THE 24"OF CLEAR PAVEMENT MEASUREMENT SHALL BE FROM THE BASE OF THE NON-TRAFFIC SIDE OF THE BARRIER. TRAFFIC SIDE PINNED BARRIER SHALL HAVE A MINIMUM OF 6" OF CLEAR PAVEMENT BEHIND THE BARRIER. WHERE BOTH PINNED AND UNPINNED BARRIER UNITS ARE USED IN A CONTINUOUS INSTALLATION, A TRANSITION SHALL BE PROVIDED BETWEEN THEM. THE TRANSITION FROM PINNED TO UNPINNED BARRIER SHALL CONSIST OF TWO ANCHOR PINS INSTALLED IN THE END HOLES ON THE TRAFFIC SIDE OF THE FIRST BARRIER BEYOND THE PINNED SECTION AND ONE ANCHOR PIN INSTALLED IN THE MIDDLE HOLE OF THE TRAFFIC SIDE OF THE SECOND BARRIER BEYOND THE PINNED SECTION. THE THIRD BARRIER BEYOND THE PINNED SECTION SHALL THEN BE UNPINNED.
13.

WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICY MEMORANDUM 4-21. DROP-OFFS GREATER THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC, THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY AND ENSURE THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES/DRUMS AT THE END OF EACH WORKDAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. IF THE ABOVE REQUIREMENTS CAN'T BE MET, AND IT IS DETERMINED THAT OVERNIGHT LANE CLOSURES AND/OR TEMPORARY CONCRETE BARRIER WALL INSTALLATION WILL BE NECESSARY, THEN IDOT WRITTEN APPROVAL WILL BE REQUIRED PRIOR TO THE INSTALLATION OF THESE ITEMS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT. WHERE POSITIVE PROTECTION (TEMPORARY CONCRETE BARRIER PER STD. 704001) IS PROVIDED, THIS REQUIREMENT IS NULLIFIED.

SUGGESTED SEQUENCE OF CONSTRUCTION AND MAINTENANCE OF TRAFFIC

THE FOLLOWING SEQUENCE OF CONSTRUCTION AND MAINTENANCE OF TRAFFIC IS SUGGESTED. VARIATIONS MAY BE MADE WITH THE APPROVAL OF THE ENGINEER. FOR EACH STAGE OF CONSTRUCTION, PROVIDE TRAFFIC CONTROL AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS. COORDINATE INSTALLATION OF TRAFFIC CONTROL DEVICES WITH THE EXISTING TRAFFIC PATTERNS AT THE ENDS OF THE PROJECT ALONG IL ROUTE 50 AND IMPACTED CROSS STREETS. THE IMPROVEMENTS WILL BE CONSTRUCTED USING LANE CLOSURES AND CROSSEOVERS PER THE PROJECT MAINTENANCE OF TRAFFIC PLANS DISTRICT ONE STANDARDS TC-10, TC-11, TC-13, TC-14, TC-16, TC-22 AND TC-26, AND HIGHWAY STANDARDS 701101, 701423, 701431, 701601, 701611, 701901, 704001 AND 781001.

STAGE 1

1.

CONTRACTOR WILL REMOVE CONFLICTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS, THEN INSTALL TEMPORARY PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES AS SHOWN ON MOT PLANS. DURING THIS STAGE TRAFFIC WILL BE SHIFTED EAST TO THE NORTHBOUND ROADWAY.
2.

DURING THIS STAGE, THE CONTRACTOR WILL COMPLETE WEST SIDE STRUCTURAL REPAIRS INCLUDING BRIDGE DECK OVERLAY, JOINT REMOVAL AND REPLACEMENT, SUBSTRUCTURE REPAIRS, AND MILL AND OVERLAY OF THE APPROACH PAVEMENT AND HOT-MIX ASPHALT ROADWAY.

STAGE 2

1.

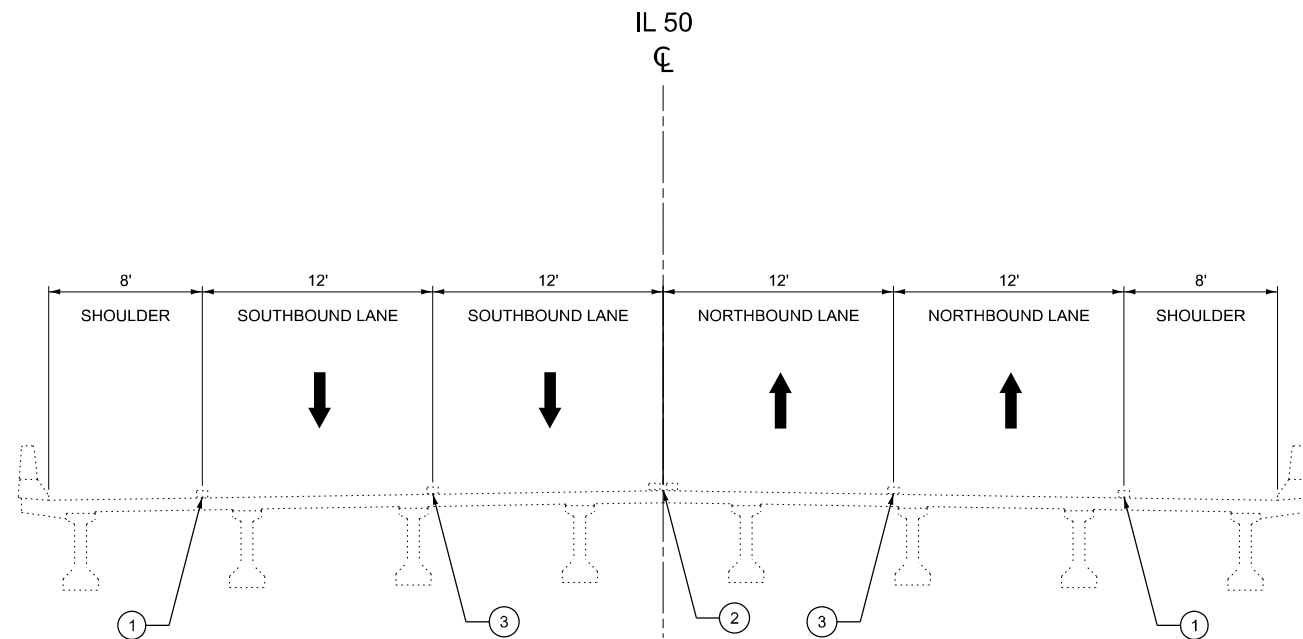
CONTRACTOR WILL REMOVE CONFLICTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS, THEN INSTALL TEMPORARY PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES AS SHOWN ON MOT PLANS. DURING THIS STAGE TRAFFIC WILL BE SHIFTED WEST TO THE SOUTHBOUND ROADWAY.
2.

DURING THIS STAGE, THE CONTRACTOR WILL COMPLETE EAST SIDE STRUCTURAL REPAIRS INCLUDING BRIDGE DECK OVERLAY, JOINT REMOVAL AND REPLACEMENT, SUBSTRUCTURE REPAIRS, AND MILL AND OVERLAY OF THE APPROACH PAVEMENT AND HOT-MIX ASPHALT ROADWAY.

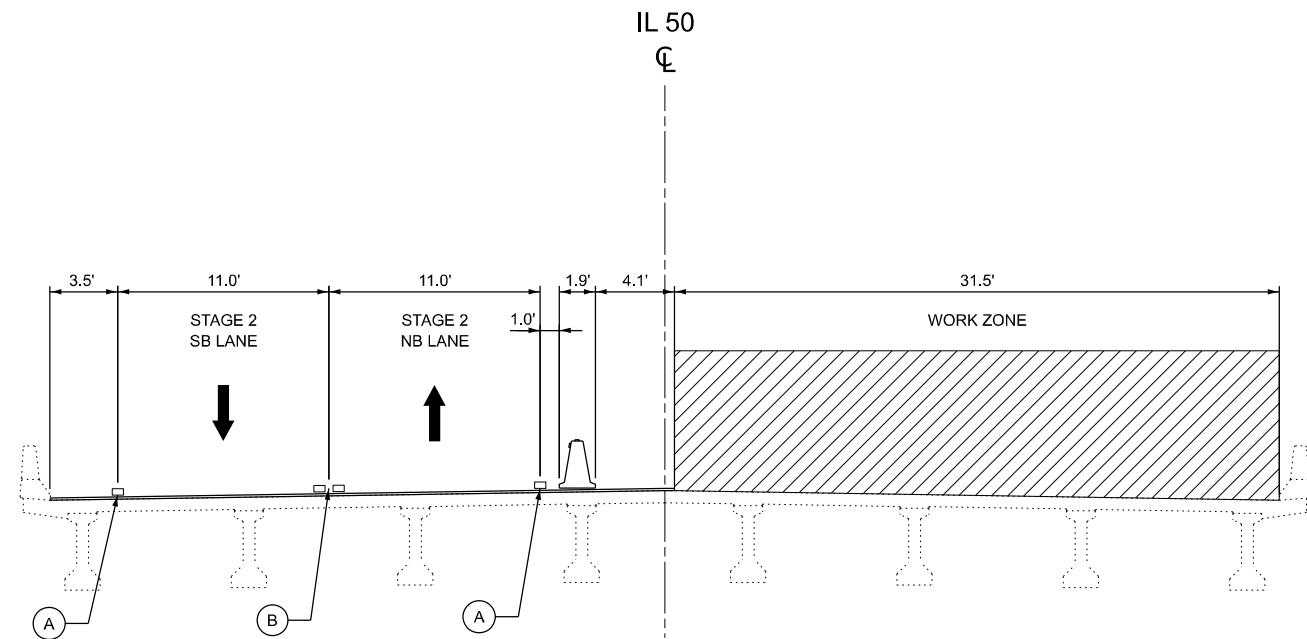
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<div><div><div></div></div><div>Accurate</div><div>GROUP, INC.</div></div>	USER NAME = Iyanisch	DESIGNED - MA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MOT - GENERAL NOTES AND SEQUENCE BRIDGE REPAIRS ON IL 50 OVER CN RR			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MA	REVISED -					840	(143VB-R-1(90)) BR 25	WILL	45	12
		CHECKED - JMT	REVISED -					CONTRACT NO. 62Y26				
	PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

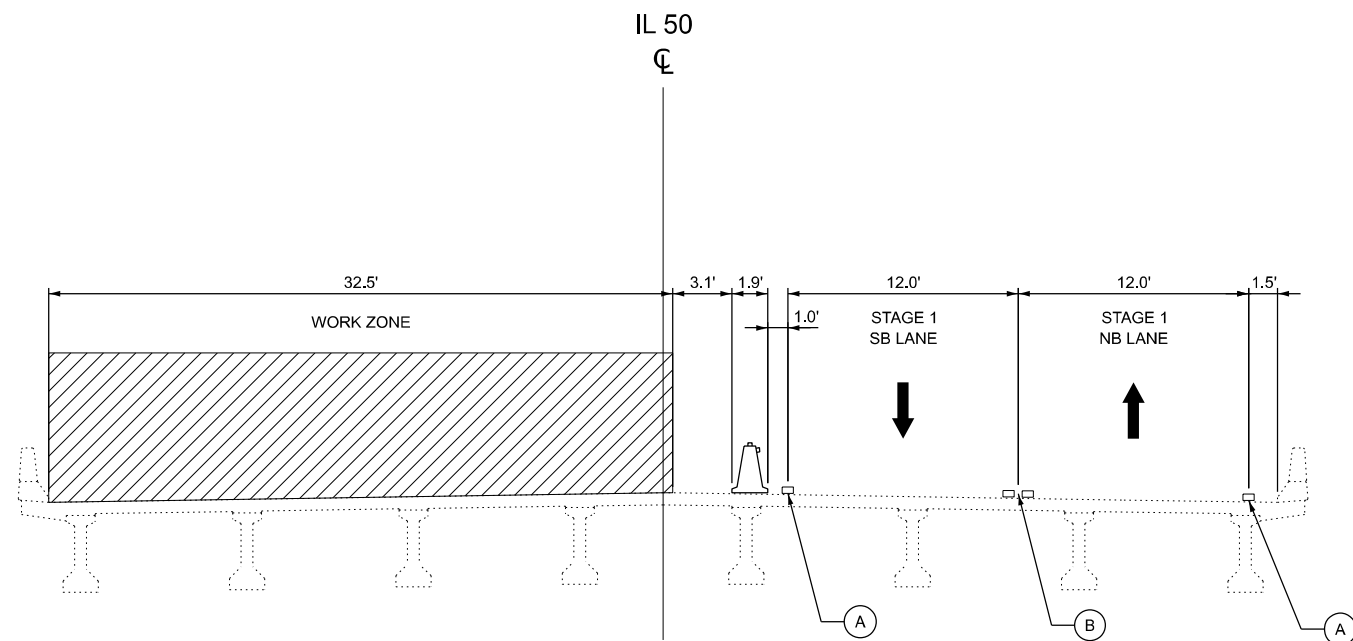
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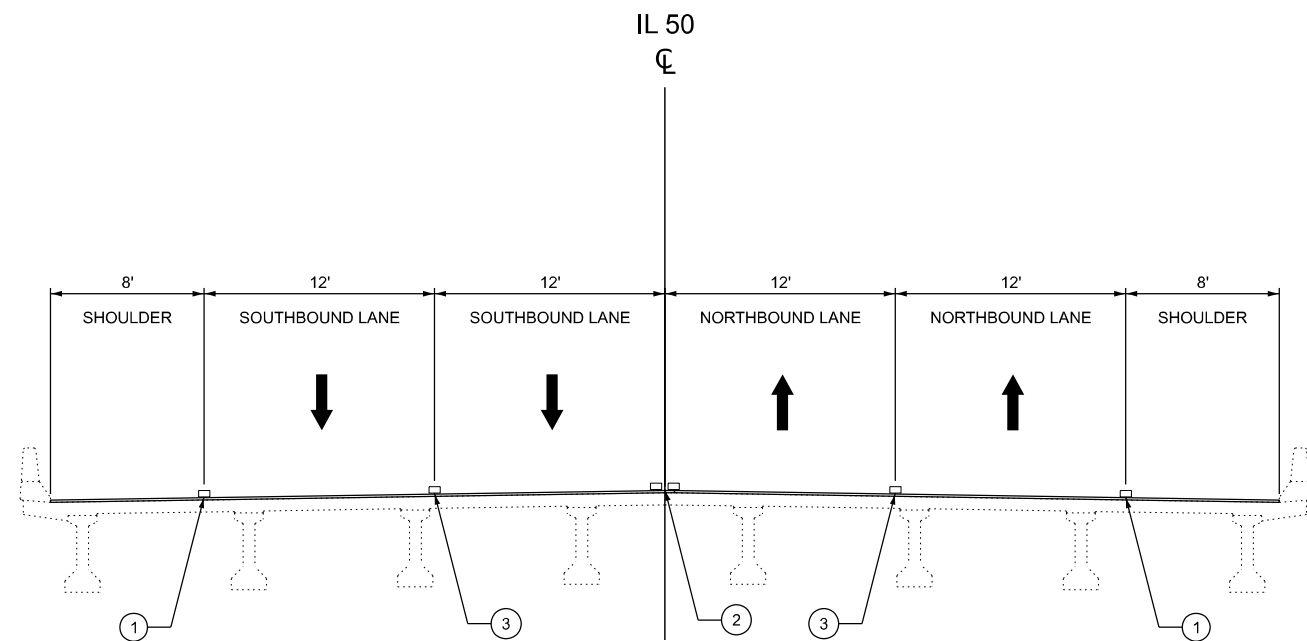
EXISTING TYPICAL SECTION
STA. 1100+00 TO STA. 1150+85
(LOOKING NORTH)



STAGE 2 TYPICAL SECTION
STA. 1100+00 TO 1141+50
(LOOKING NORTH)



STAGE 1 TYPICAL SECTION
STA. 1105+10 TO STA. 1150+85
(LOOKING NORTH)



PROPOSED TYPICAL SECTION
STA. 1100+00 TO STA. 1150+85
(LOOKING NORTH)

LEGEND



WORK ZONE



DIRECTION OF TRAFFIC



EXISTING PAVEMENT MARKING



PROPOSED OR TEMPORARY
PAVEMENT MARKING



TEMPORARY CONCRETE BARRIER
W/ MONO-DIRECITONAL CRYSTAL, TYPE C
REFLECTORS PER STD 704001 AND 782006

TEMPORARY PAVEMENT MARKING,
TYPE IV TAPE OF THE WIDTH AND COLOR
SPECIFIED:

(A)

4" SOLID WHITE

(B)

4" DOUBLE YELLOW
CENTERLINE, 11" C-C

PERMANENT PAVEMENT MARKING,
OF THE WIDTH AND COLOR SPECIFIED:

(1)

4" SOLID WHITE

(2)

4" DOUBLE YELLOW
CENTERLINE, 11" C-C

(3)

4" WHITE SOLID
10'-30' SKIP-DASH LINE



USER NAME = Iyanisch
PLOT DATE = 6/27/2025

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DRAWN - MA
CHECKED - JMT
DATE - 09/26/2025

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MOT PLAN - TYPICAL SECTIONS
BRIDGE REPAIR ON IL 50 OVER CN RR**

SCALE: 1"=5'

SHEET 1 OF 1 SHEETS

STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	13
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

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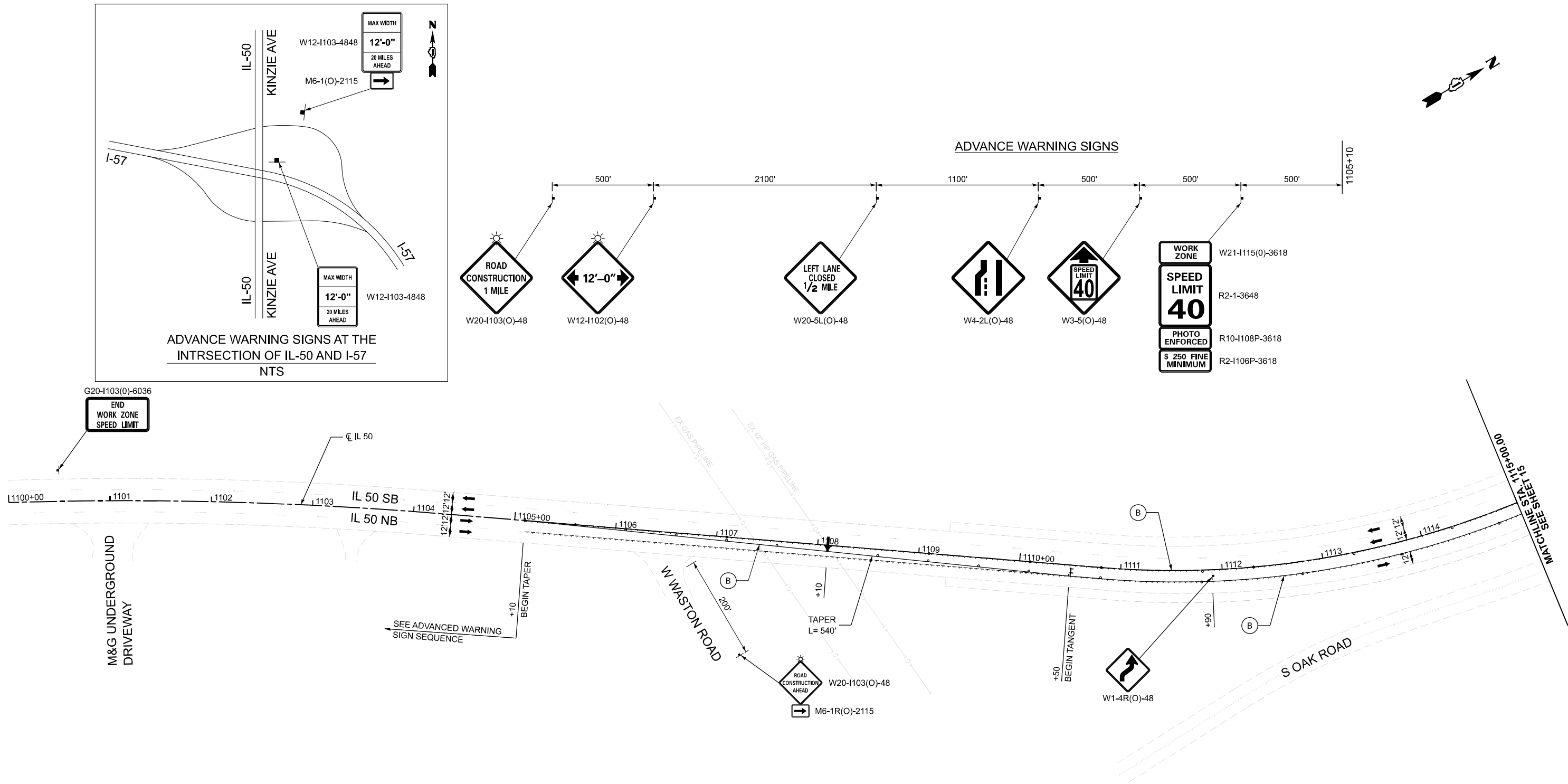
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	CHECKED - JMT	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MOT PLAN - STAGE 01
BRIDGE REPAIR ON IL 50 OVER CN RR

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. 1100+00 TO STA. 1115+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

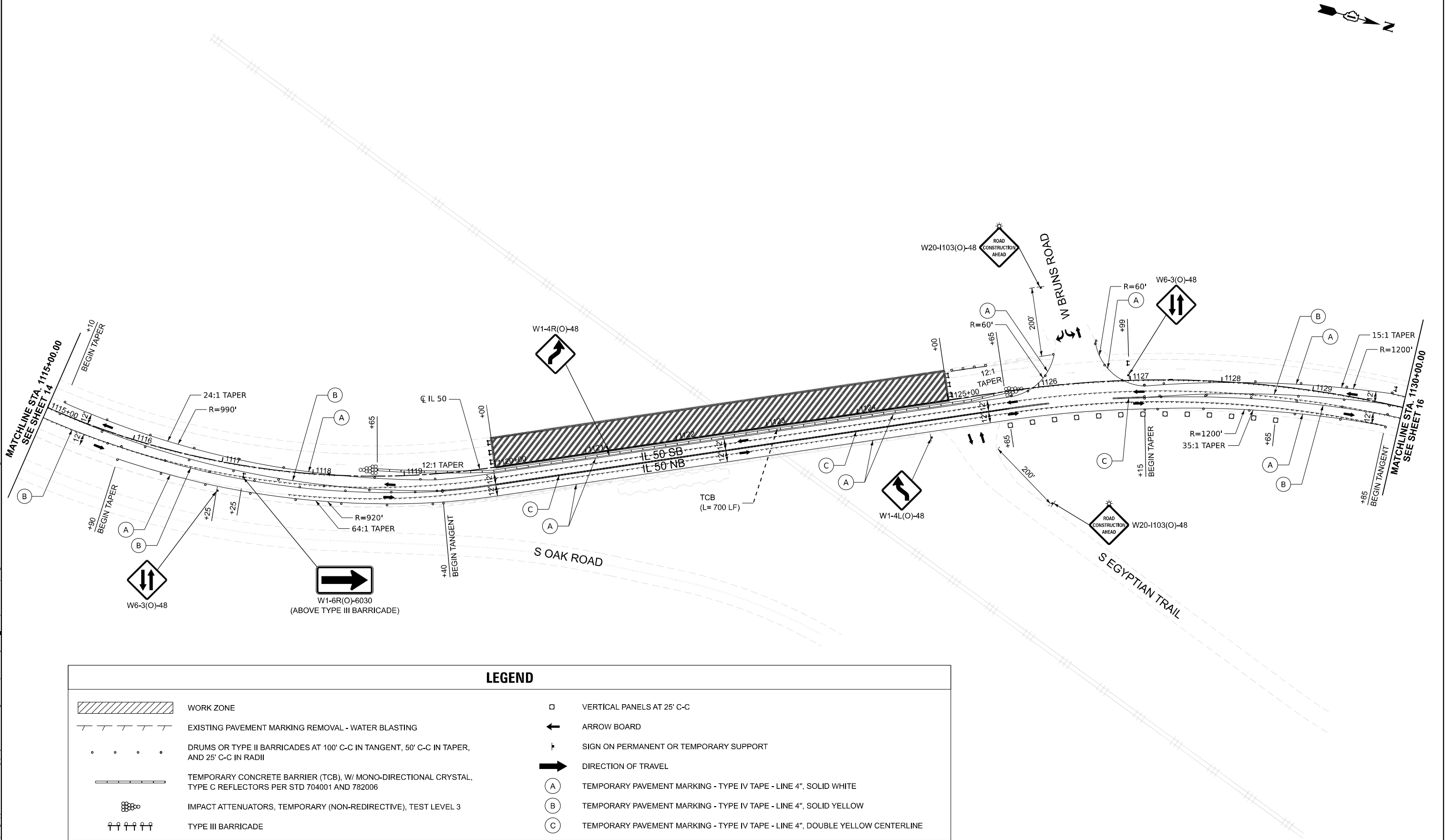


ADVANCE WARNING SIGNS AT THE
INTRSECTION OF IL-50 AND I-57
NTS

LEGEND

	WORK ZONE		ARROW BOARD
	EXISTING PAVEMENT MARKING REMOVAL - WATER BLASTING		SIGN ON PERMANENT OR TEMPORARY SUPPORT
	DRUMS OR TYPE II BARRICADES AT 100' C-C IN TANGENT, 50' C-C IN TAPER, AND 25' C-C IN RADII		DIRECTION OF TRAVEL
	TEMPORARY CONCRETE BARRIER (TCB), W/ MONO-DIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 AND 782006		TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID WHITE
	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3		TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID YELLOW
	TYPE III BARRICADE		TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", DOUBLE YELLOW CENTERLINE

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LEGEND

- | | | | |
|--|--|--|---|
| | WORK ZONE | | VERTICAL PANELS AT 25' C-C |
| | EXISTING PAVEMENT MARKING REMOVAL - WATER BLASTING | | ARROW BOARD |
| | DRUMS OR TYPE II BARRICADES AT 100' C-C IN TANGENT, 50' C-C IN TAPER, AND 25' C-C IN RADII | | SIGN ON PERMANENT OR TEMPORARY SUPPORT |
| | TEMPORARY CONCRETE BARRIER (TCB), W/ MONO-DIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 AND 782006 | | DIRECTION OF TRAVEL |
| | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 | | TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID WHITE |
| | TYPE III BARRICADE | | TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID YELLOW |
| | | | TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", DOUBLE YELLOW CENTERLINE |



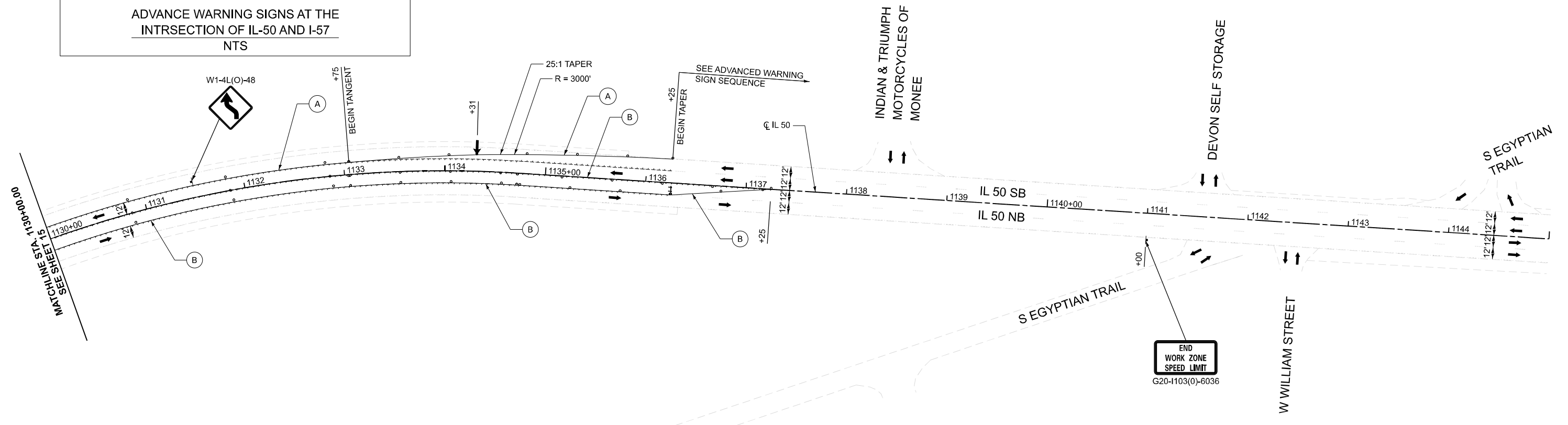
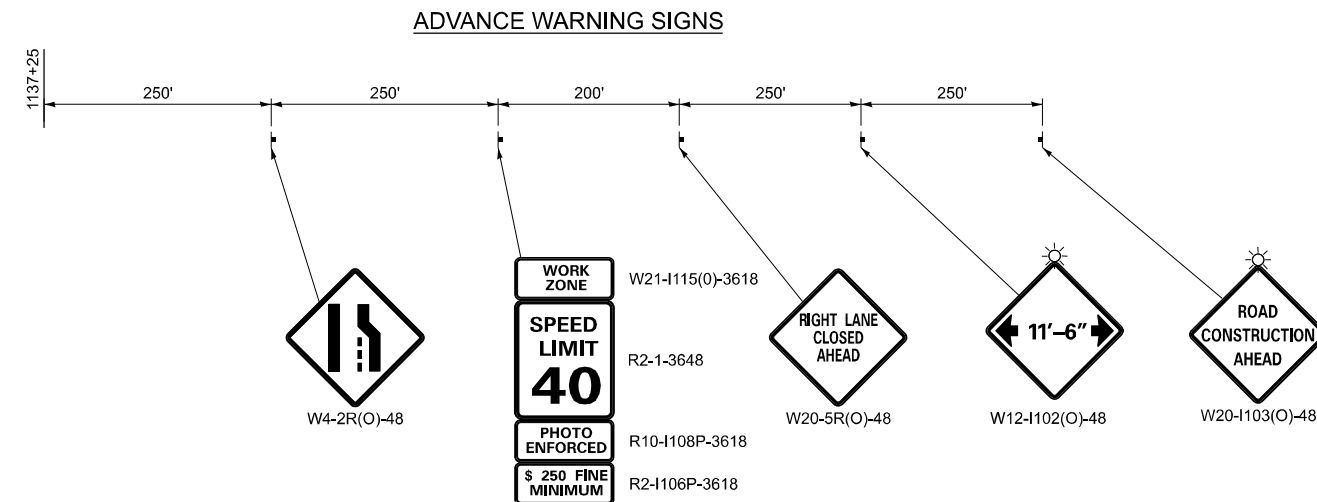
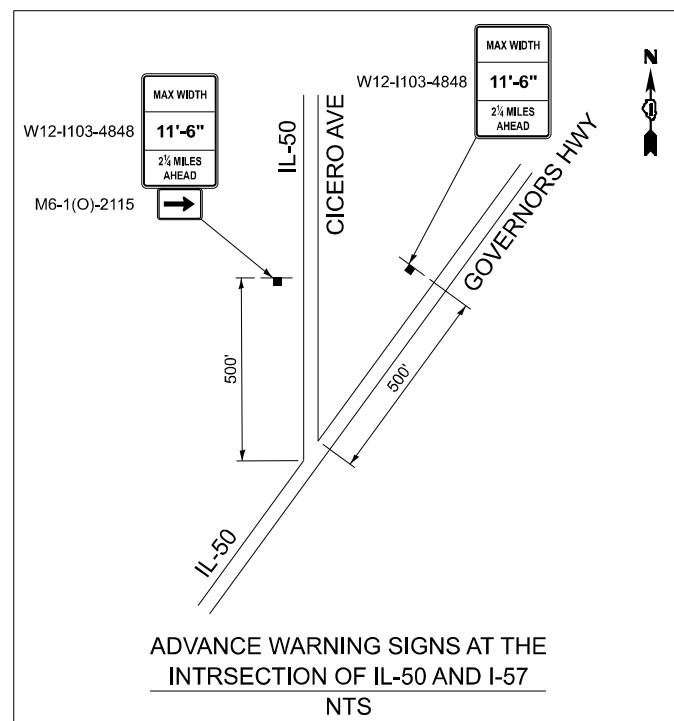
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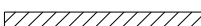











STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MOT PLAN - STAGE 01
BRIDGE REPAIR ON IL 50 OVER CN RR

SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. 1115+00 TO STA. 1130+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	15
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



LEGEND			
	WORK ZONE		ARROW BOARD
	EXISTING PAVEMENT MARKING REMOVAL - WATER BLASTING		SIGN ON PERMANENT OR TEMPORARY SUPPORT
	DRUMS OR TYPE II BARRICADES AT 100' C-C IN TANGENT, 50' C-C IN TAPER, AND 25' C-C IN RADII		DIRECTION OF TRAVEL
	TEMPORARY CONCRETE BARRIER (TCB), W/ MONO-DIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 AND 782006		TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID WHITE
	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3		TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID YELLOW
	TYPE III BARRICADE		TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", DOUBLE YELLOW CENTERLINE

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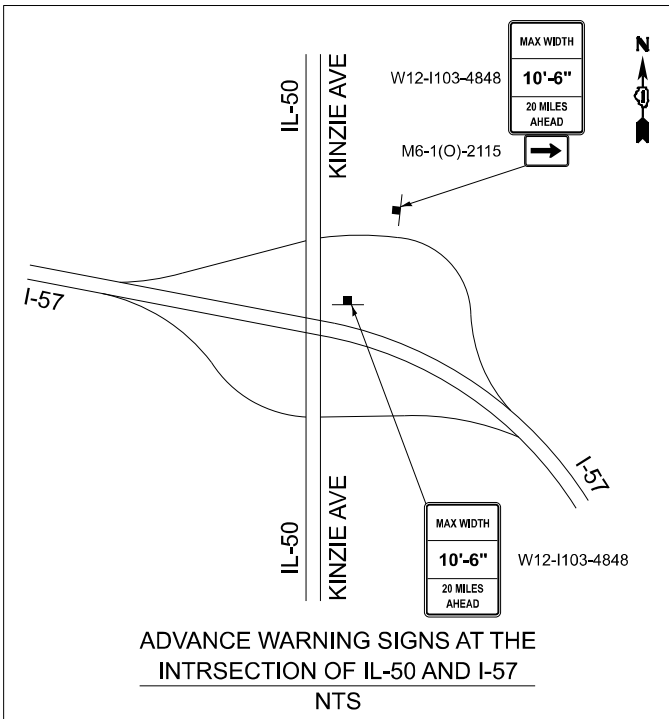
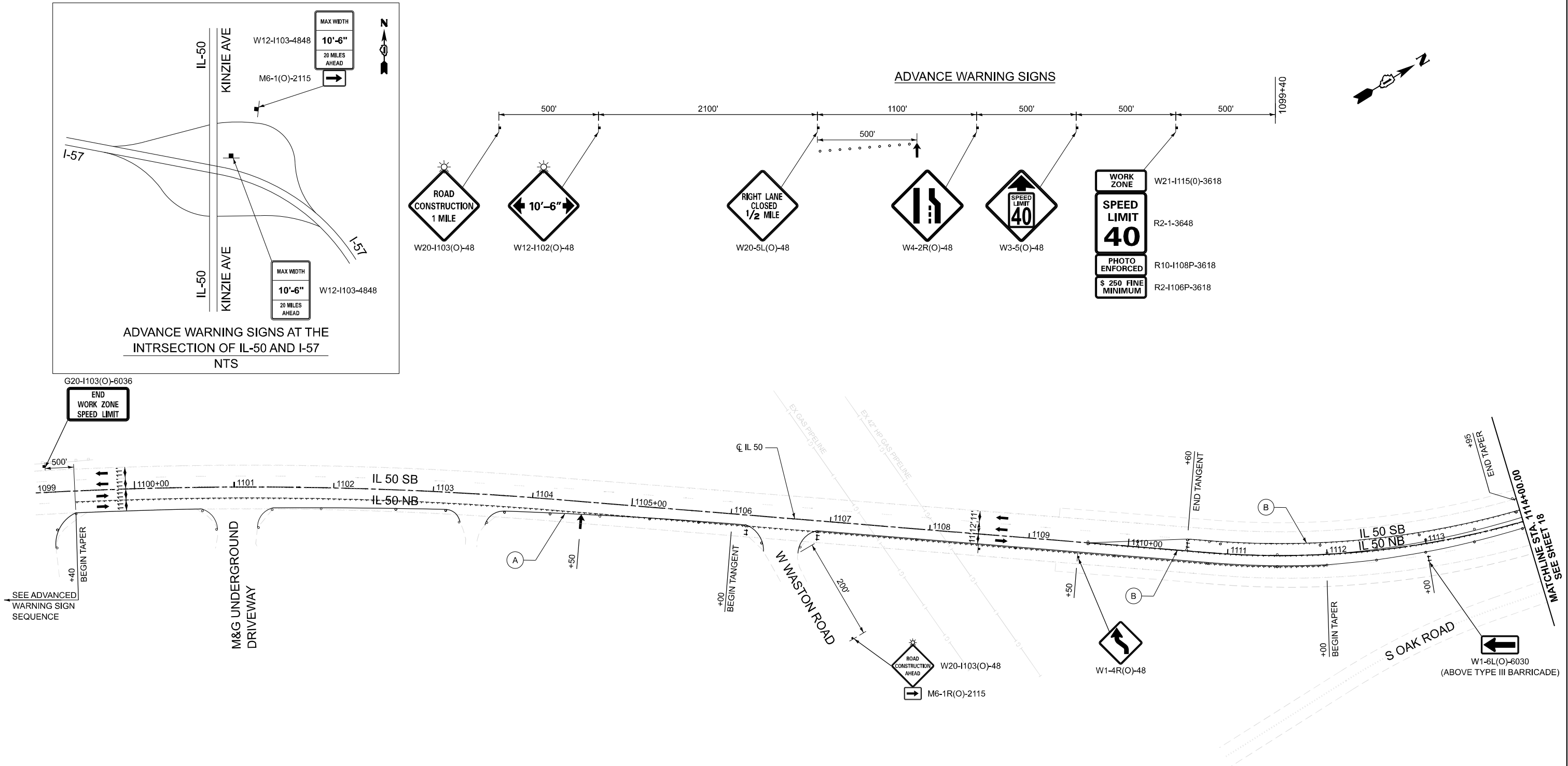
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	CHECKED -	JMT	REVISED -
	DATE -	09/26/2025	REVISED -
PLOT DATE = 6/27/2025			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MOT PLAN - STAGE 02
BRIDGE REPAIR ON IL 50 OVER CN RR

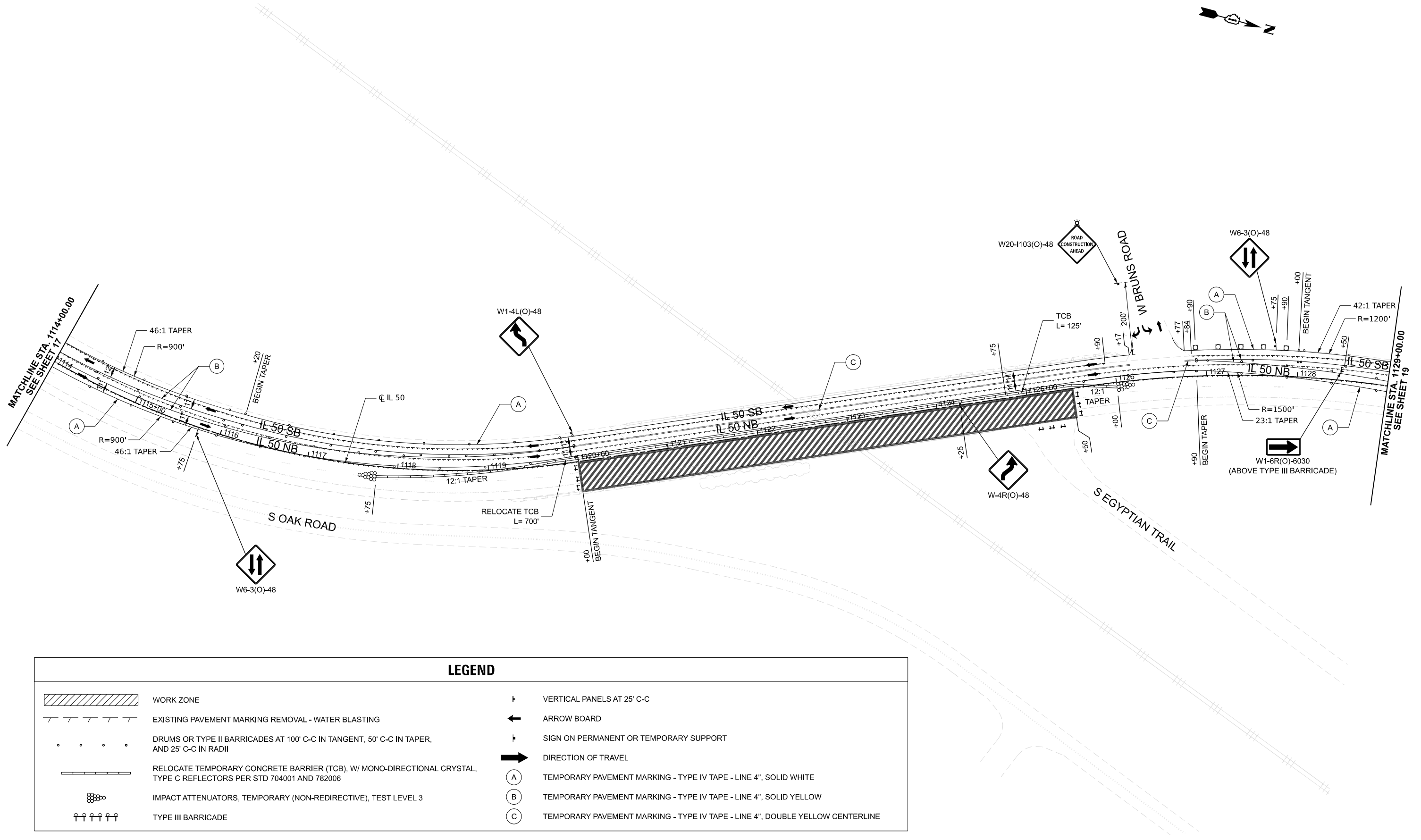
SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. 1099+40 TO STA. 1114+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	17
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



ADVANCE WARNING SIGNS AT THE
INTRSECTION OF IL-50 AND I-57
NTS

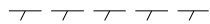
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LEGEND



WORK ZONE



EXISTING PAVEMENT MARKING REMOVAL - WATER BLASTING



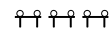
DRUMS OR TYPE II BARRICADES AT 100' C-C IN TANGENT, 50' C-C IN TAPER, AND 25' C-C IN RADII



RELOCATE TEMPORARY CONCRETE BARRIER (TCB), W/ MONO-DIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 AND 782006



IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3



TYPE III BARRICADE



VERTICAL PANELS AT 25' C-C



ARROW BOARD



SIGN ON PERMANENT OR TEMPORARY SUPPORT



DIRECTION OF TRAVEL



TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID WHITE



TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", SOLID YELLOW



TEMPORARY PAVEMENT MARKING - TYPE IV TAPE - LINE 4", DOUBLE YELLOW CENTERLINE



USER NAME	= Iyanisch	DESIGNED -	MA	REVISED -	
		DRAWN -	MA	REVISED -	
		CHECKED -	JMT	REVISED -	
PLOT DATE	= 6/27/2025	DATE	= 09/26/2025	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MOT PLAN - STAGE 02
BRIDGE REPAIR ON IL 50 OVER CN RR

SCALE: 1"=50'

SHEET 2 OF 3 SHEETS STA. 1115+00 TO STA. 1130+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	18
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

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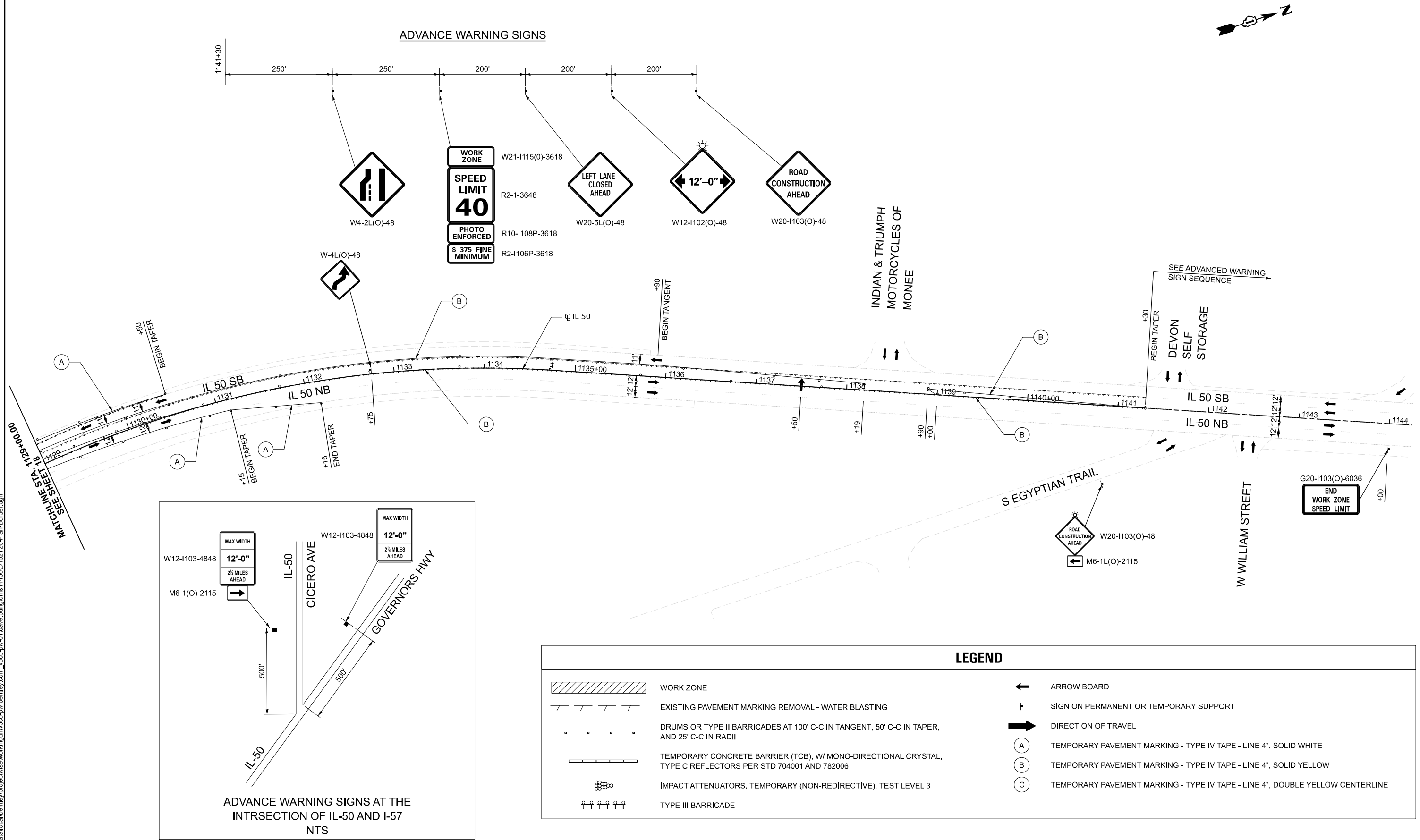
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PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

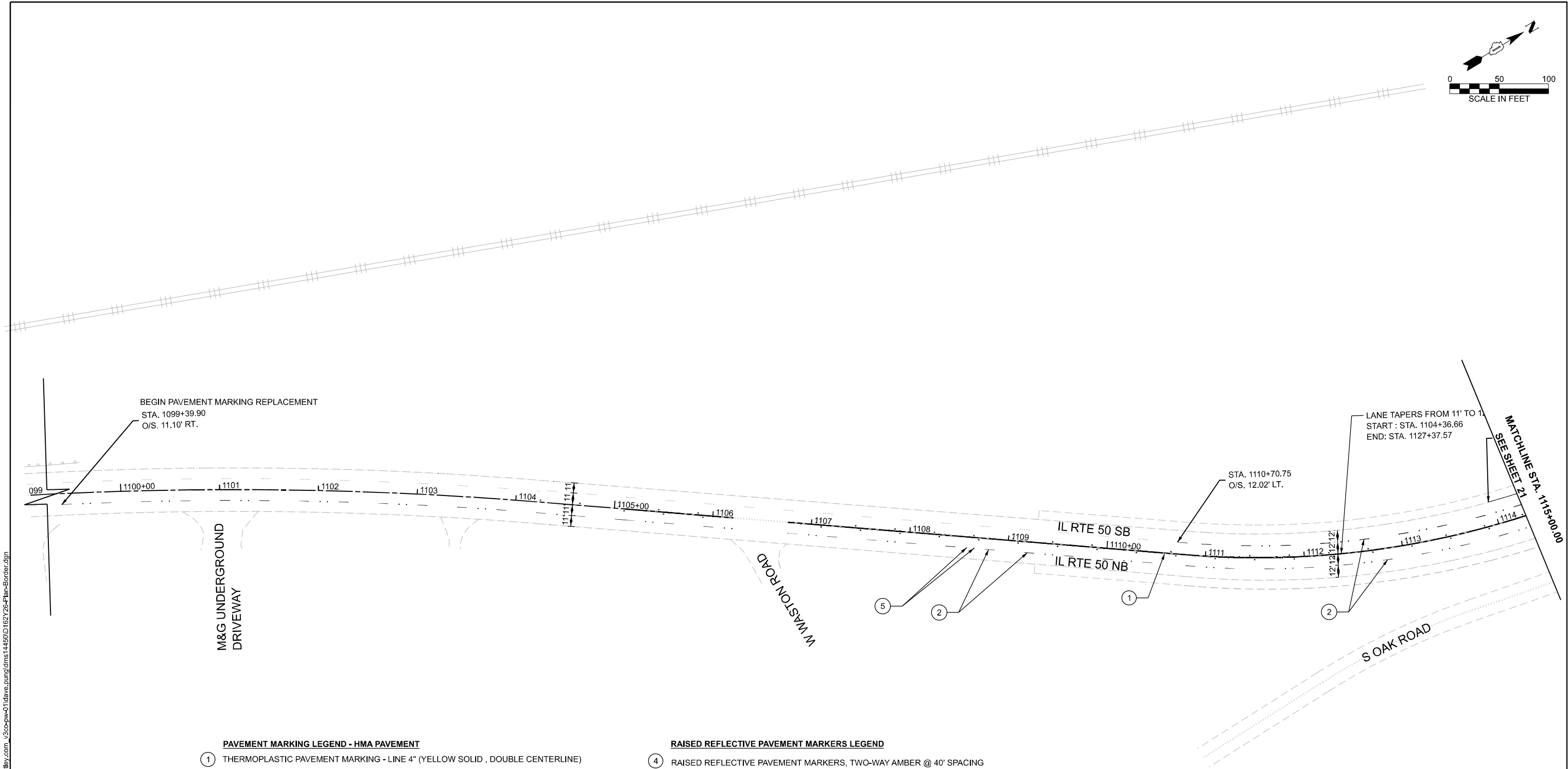
MOT PLAN - STAGE 02
BRIDGE REPAIR ON IL 50 OVER CN RR

SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 1129+00 TO STA. 1144+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	19
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



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PAVEMENT MARKING LEGEND - HMA PAVEMENT

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID , DOUBLE CENTERLINE)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE 10'-30' SKIP LINE)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID)
- Ⓐ MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (YELLOW SOLID , DOUBLE CENTERLINE)
- Ⓑ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7" (WHITE 10'-30' SKIP LINE)
- Ⓒ MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (WHITE SOLID)

RAISED REFLECTIVE PAVEMENT MARKERS LEGEND

- ④ RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER @ 40' SPACING
- ⑤ RAISED REFLECTIVE PAVEMENT MARKERS, DOUBLE ONE-WAY CRYSTAL @ 80' SPACING (REFLECTOR REPLACEMENT ONLY)
- ⑥ RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER @ 40' SPACING (REFLECTOR REPLACEMENT ONLY)

NOTE:
1. ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT D1 PM DETAIL TC-13
2. ALL RRPMS SHALL BE INSTALLED ACCORDING TO IDOT D1 RRPDM DETAIL TC-11



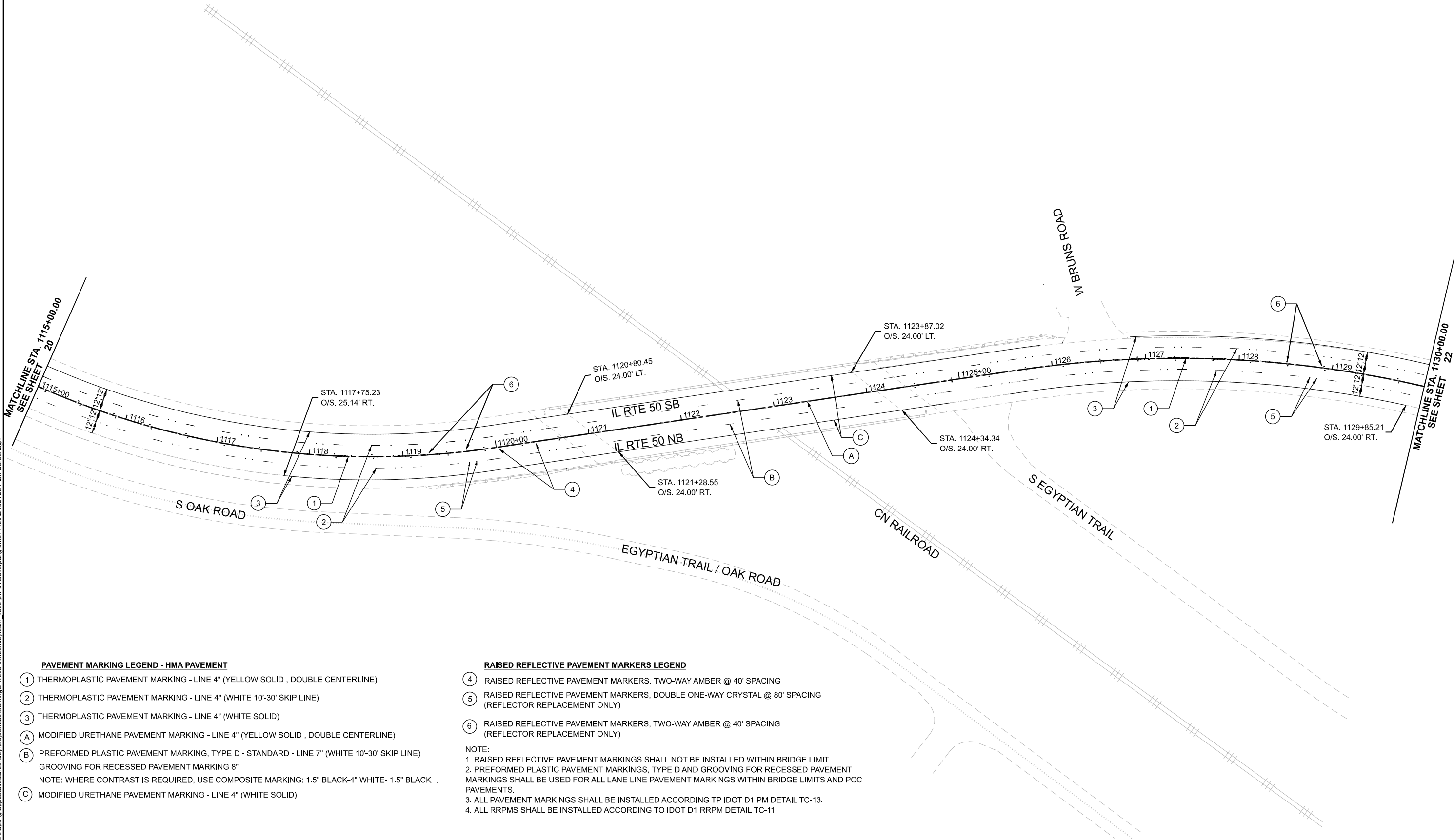
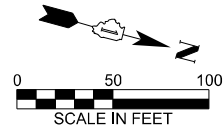
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	DRAWN - AS	REVISED -
PLOT DATE = 6/27/2025	CHECKED - DW	REVISED -
	DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
BRIDGE REPAIRS ON IL 50 OVER CN RR

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. 1100+00 TO STA. 1115+00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	20
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND - HMA PAVEMENT

- 1 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID , DOUBLE CENTERLINE)
- 2 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE 10'-30' SKIP LINE)
- 3 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID)
- A MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (YELLOW SOLID , DOUBLE CENTERLINE)
- B PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7" (WHITE 10'-30' SKIP LINE)
GROOVING FOR RECESSED PAVEMENT MARKING 8"
NOTE: WHERE CONTRAST IS REQUIRED, USE COMPOSITE MARKING: 1.5" BLACK-4" WHITE- 1.5" BLACK .
- C MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (WHITE SOLID)

RAISED REFLECTIVE PAVEMENT MARKERS LEGEND

- 4 RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER @ 40' SPACING
- 5 RAISED REFLECTIVE PAVEMENT MARKERS, DOUBLE ONE-WAY CRYSTAL @ 80' SPACING
(REFLECTOR REPLACEMENT ONLY)
- 6 RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER @ 40' SPACING
(REFLECTOR REPLACEMENT ONLY)

NOTE:

1. RAISED REFLECTIVE PAVEMENT MARKINGS SHALL NOT BE INSTALLED WITHIN BRIDGE LIMIT.
2. PREFORMED PLASTIC PAVEMENT MARKINGS, TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.
3. ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TP IDOT D1 PM DETAIL TC-13.
4. ALL RRPMS SHALL BE INSTALLED ACCORDING TO IDOT D1 RRPDM DETAIL TC-11



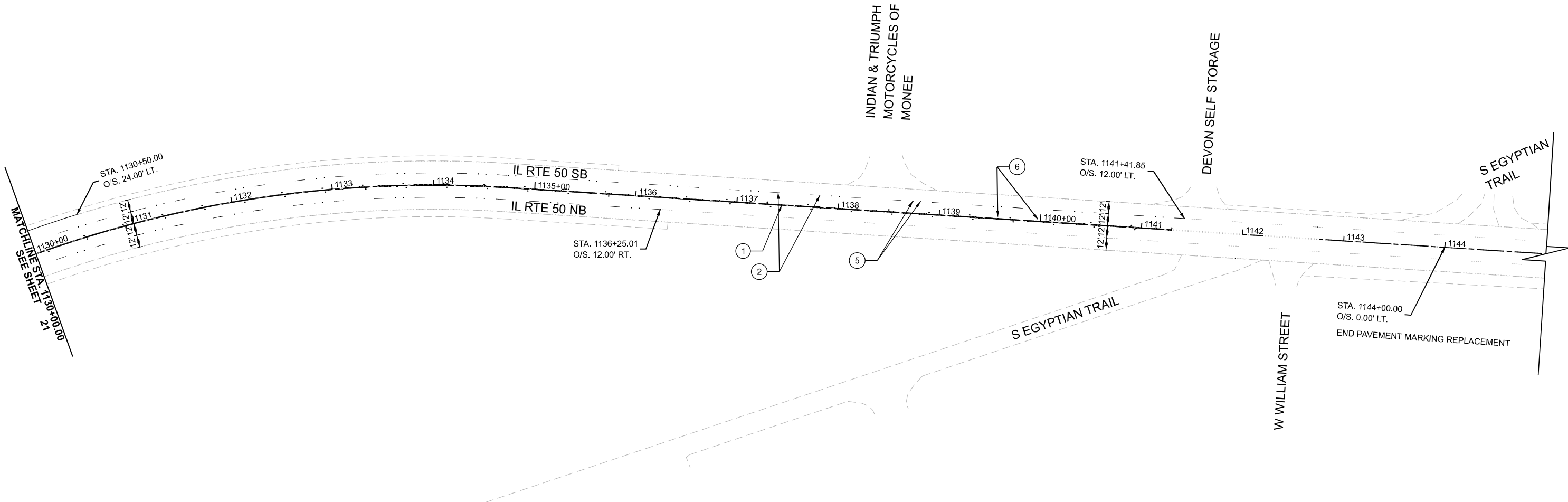
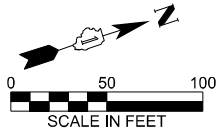
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PLOT DATE	= 6/27/2025	DATE -	09/26/2025	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
BRIDGE REPAIRS ON IL 50 OVER CN RR

SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. 1115+00 TO STA. 1130+00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	21
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND - HMA PAVEMENT

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID , DOUBLE CENTERLINE)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE 10'-30' SKIP LINE)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID)
- Ⓐ MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (YELLOW SOLID , DOUBLE CENTERLINE)
- Ⓑ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7" (WHITE 10'-30' SKIP LINE)
- Ⓒ MODIFIED URETHANE PAVEMENT MARKING - LINE 4" (WHITE SOLID)

RAISED REFLECTIVE PAVEMENT MARKERS LEGEND

- ④ RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER @ 40' SPACING
- ⑤ RAISED REFLECTIVE PAVEMENT MARKERS, DOUBLE ONE-WAY CRYSTAL @ 80' SPACING (REFLECTOR REPLACEMENT ONLY)
- ⑥ RAISED REFLECTIVE PAVEMENT MARKERS, TWO-WAY AMBER @ 40' SPACING (REFLECTOR REPLACEMENT ONLY)

NOTE:
1. ALL PAVEMENT MARKINGS BE INSTALLED ACCORDING TO IDOT D1 PM DETAIL TC-13.
2. ALL RRPMS SHALL BE INSTALLED ACCORDING TO IDOT RRP, DETAIL TC-11.

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USER NAME = Iyanisch	DESIGNED - AS	REVISED -
	DRAWN - AS	REVISED -
	CHECKED - DW	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
BRIDGE REPAIRS ON IL 50 OVER CN RR

SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 1130+00 TO STA. 1145+00

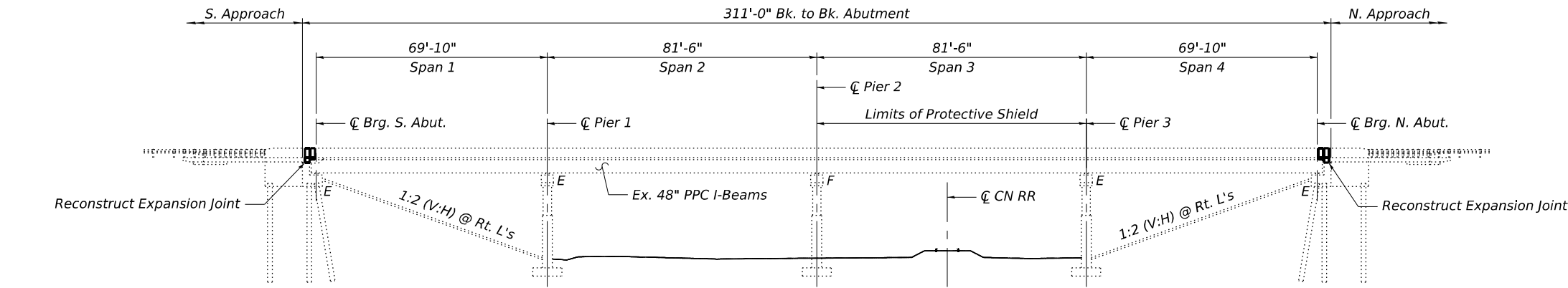
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	22
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

Benchmark: "□" Cut in top of curb w. side of Ill. Rte. 50, Sta. 1111+00, Elev. 782.82.

Existing Structure: SN 099-0298 was built in 1994 as F.A.P. Rte. 840 (IL-50) over CN RR.
The existing structure is a 4-span continuous concrete multi-beam bridge measuring 311'-0" back to back abutments and 67'-2" out to out deck width. The superstructure consists of 48" PPC I-beams supporting a 7½" reinforced concrete slab. The substructure consists of three multi-column piers on spread footings and stub abutments on metal shell piles. The skew is 45°0'0".

Traffic Control: Traffic will be maintained using stage construction.

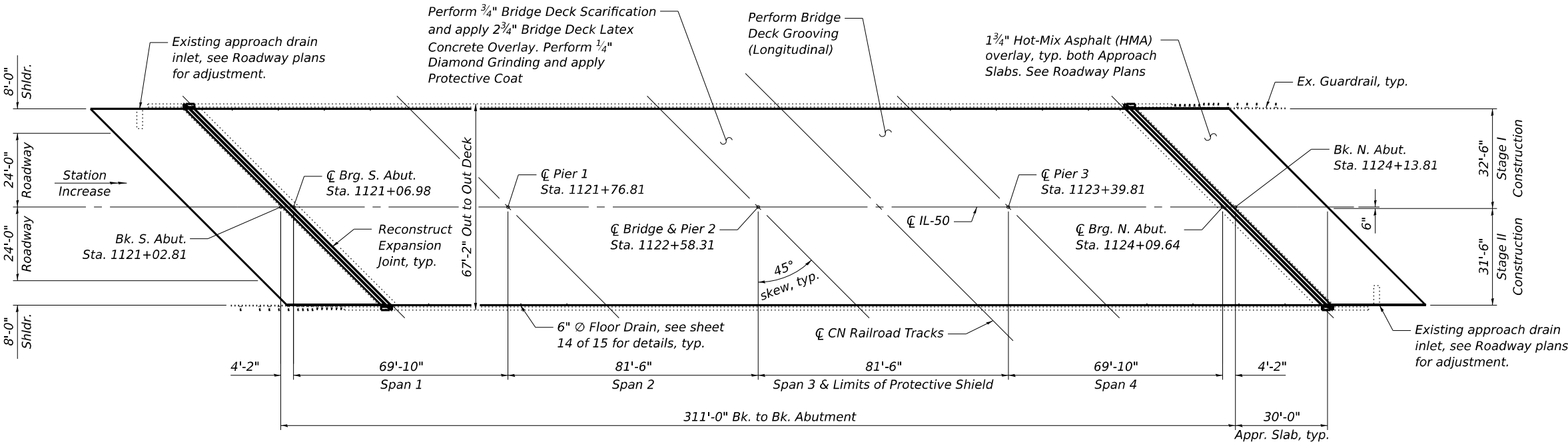
No Salvage.



ELEVATION



Zaida Morillo
Licensed Structural Engineer
State of Illinois 081-008252
Expires 11-30-2026



PLAN

SCOPE OF WORK

1. Install Protective Shield over CN RR.
2. Scarify ¾" from the bridge deck slab.
3. Perform Deck Slab and Parapet Repairs.
4. Remove and Reconstruct Expansion Joints at North and South abutments and install new Preformed Joint Strip Seals.
5. Apply 2¾" Bridge Deck Latex Concrete Overlay on Bridge Deck. Apply 1¾" Hot-Mix Asphalt (HMA) Overlay on the Approach Pavement (See roadway plans).
6. Perform ¼" Diamond Grinding to top of bridge deck and abutment hatched block.
7. Perform Bridge Deck Grooving (Longitudinal) on traffic lanes.
8. Apply Protective Coat on reconstructed transverse Expansion Joints and on the surface of the Latex Concrete Overlay.
9. Perform structural concrete repairs on abutments and diaphragms as noted on plans.
10. Apply Concrete Sealer on the top and inside faces of parapets and designated areas of abutments.
11. Repair Beams 9 and 10 at the North Abutment using Fiber Wrap and Acrylic Coating.
12. Clean and paint bearings at North and South Abutments.
13. Repair bearings at Beams 2 and 4 on South Abutment and Beam 4 on North Abutment by replacing side retainer angle.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications
for Highway Bridge, 17th Edition

DESIGN STRESSES

FIELD UNITS - NEW CONSTRUCTION

f'c = 4,000 psi (Superstructure)
fy = 60,000 psi (Reinforcement)

FIELD UNITS - EXISTING CONSTRUCTION

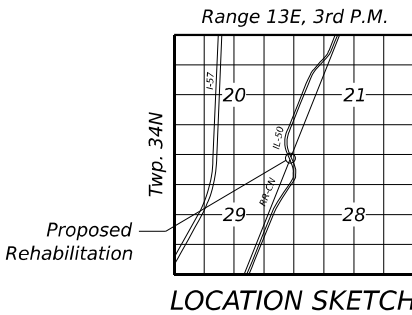
Cast in Place Reinforced Concrete
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

Precast Pre-stressed Units

f'c = 6,000 psi
f'ci = 5,000 psi
Low Relaxation Strands
f's = 270,000 psi
f'si = 201,960 psi

LOADING HS 20-44

No allowance for future wearing surface



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
F.A.P. RTE. 840 (IL-50) OVER CN RR
WILL COUNTY
STATION 1122+58.31
STRUCTURE NO. 099-0298

MODEL: Sheet View
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V3 Companies
7325 Janes Avenue
Woodridge, IL 60517
630.724.9200 phone
630.724.9202 fax
www.v3co.com

USER NAME = Iyanisch	DESIGNED - JRR	REVISED -
	DRAWN - LJM	REVISED -
	CHECKED - WJV	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 099-0298

SCALE: SHEET 1 OF 15 SHEETS STA. 1121+02.81 TO STA. 1124+13.81

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	23
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into the concrete removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Cost of removal and disposal of existing expansion joints shall be included in the cost of Concrete Removal.

For HMA Overlay on Approach Slab, see Roadway Plans.

Protective Coat shall be applied to the reconstructed transverse Expansion Joints and to the surface of the new Latex Concrete Overlay.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specification when the deck is poured at an ambient temperature other than 50°F.

Expansion joints shall be fabricated to conform to the existing cross slope of the bridge.

The Contractor shall exercise extreme caution during concrete removal to avoid damaging P.P.C. beams and diaphragms to remain. Any damage to the existing P.P.C. beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the engineer at no cost to the Department.

Concrete Sealer shall be applied to the top and inside face of parapets and to the designated areas of the abutments.

The Contractor shall take appropriate measures to assure that the protective coat is not applied to the floor drains.

Prior to the application of the Concrete Sealer, the Contractor shall clean all existing debris from the abutment seats. The method of debris removal shall not damage the existing concrete and shall be approved by the Engineer. The debris shall be disposed of according to Article 202.03 of the Standard Specifications. The cost of cleaning is included in the cost of Concrete Sealer.

Cleaning and painting of the existing elastomeric bearings shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All bearings shall be cleaned per Near White Blast Cleaning (SSPC-SP10). The designated areas cleaned per Near White Blast Cleaning (SSPC-SP10) shall be painted according to the requirements of the Organic Zinc-Rich/Epoxy/Urethane paint system. The color of the final finish coat for all elastomeric bearings shall be Gray, Munsell No. 5B 7/1.

INDEX OF SHEETS

- 1. General Plan and Elevation
- 2. General Data
- 3. Stage Construction
- 4. Temporary Concrete Barrier
- 5. Bridge Deck and Approach Slab Repair Plan
- 6. Parapet Repair Plan
- 7. Approach Joint Removal and Replacement
- 8. Beam End Repair Details
- 9. Diaphragm and Bearing Repair Details
- 10. Expansion Joint Replacement Details (1 of 3)
- 11. Expansion Joint Replacement Details (2 of 3)
- 12. Expansion Joint Replacement Details (3 of 3)
- 13. Preformed Joint Strip Seal
- 14. Deck Floor Drain Details
- 15. Bar Splicer Assembly and Mechanical Splicer Details

LIST OF ABBREVIATIONS

Abut.	Abutment
Appr.	Approach
Bk.	Back
Brg.	Bearing
bot.	Bottom
cts.	Centers
CL	Centerline
Const.	Construction
Ø	Diameter
Ex.	Existing
Exp.	Expansion
F.F.	Front Face
I.F.	Inside Face
Max.	Maximum
Min.	Minimum
N.B.	Northbound
O.F.	Outside Face
PL	Plate
Pvmt.	Pavement
Reinf.	Reinforcement
RR	Railroad
Rte.	Route
S.B.	Southbound
SF	Square Foot
Shldr.	Shoulder
Sta.	Station
Std.	Standard
typ.	Typical

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	23.0	-	23.0
Protective Shield	Sq. Yd.	609	-	609
Concrete Superstructure	Cu. Yd.	22.5	-	22.5
Protective Coat	Sq. Yd.	2,202	-	2,202
Concrete Superstructure (Approach Slab)	Cu. Yd.	3.4	-	3.4
Furnishing and Erecting Structural Steel	Pound	-	69	69
Reinforcement Bars, Epoxy Coated	Pound	3,010	-	3,010
* Bar Splicers	Each	12	-	12
Preformed Joint Strip Seal	Foot	186	-	186
Anchor Bolts, 1"	Each	-	3	3
Concrete Sealer	Sq. Ft.	271	98	369
* Acrylic Coating	Sq. Yd.	13	-	13
* Fiber Wrap	Sq. Ft.	119	-	119
* Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1,659	-	1,659
* Cleaning and Painting Bearings	Each	16	-	16
* Approach Slab Repair (Partial Depth)	Sq. Yd.	2	-	2
* Bridge Deck Latex Concrete Overlay, 2¾ Inches	Sq. Yd.	2,158	-	2,158
* Bridge Deck Scarification ¾"	Sq. Yd.	2,178	-	2,178
* Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq. Ft.	47	6	53
* Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	19	-	19
* Diamond Grinding (Bridge Section)	Sq. Yd.	2,042	-	2,042
* Precast Prestressed Concrete I-Beam Repair	Sq. Ft.	18	-	18

* Special Provision

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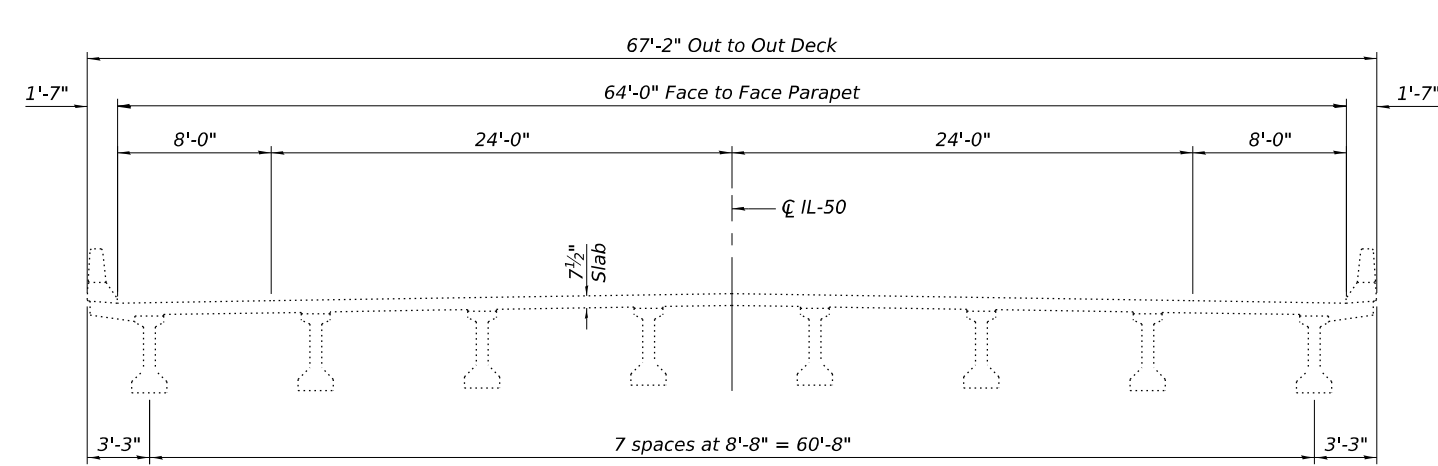
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

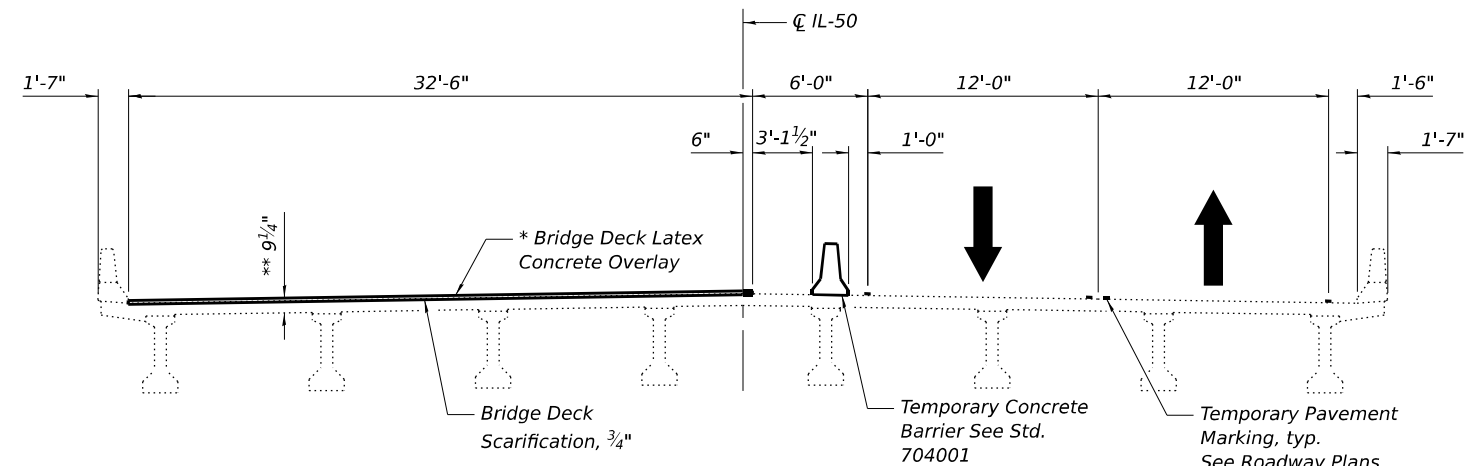
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STRUCTURE NO. 099-0298

SCALE:	SHEET 2 OF 15 SHEETS	STA.	TO STA.
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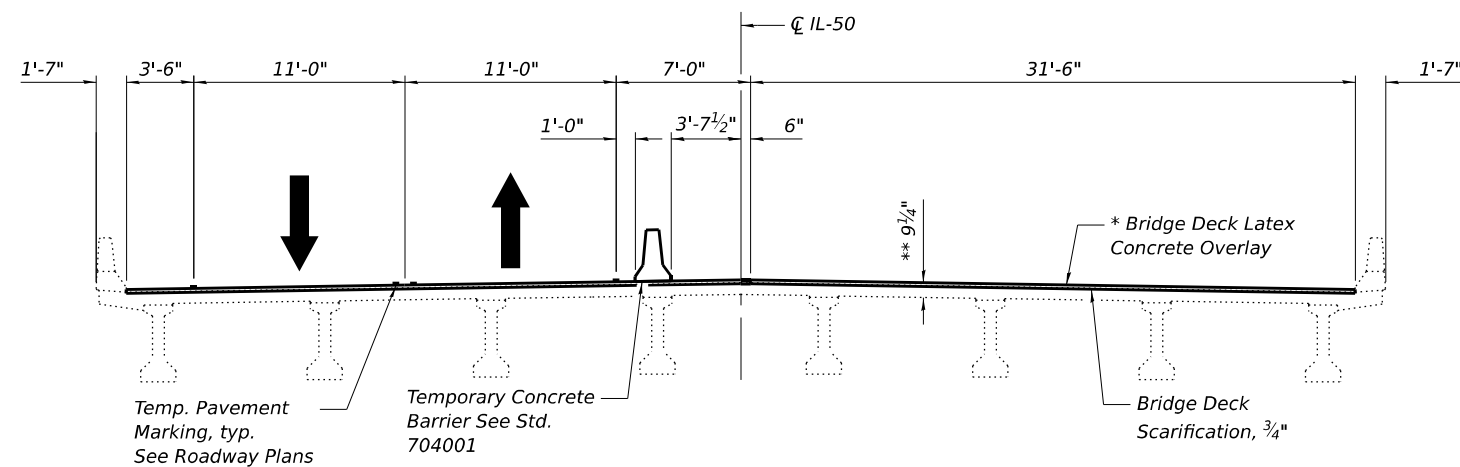
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		CONTRACT NO. 62Y26		
		ILLINOIS	FED. AID PROJECT	



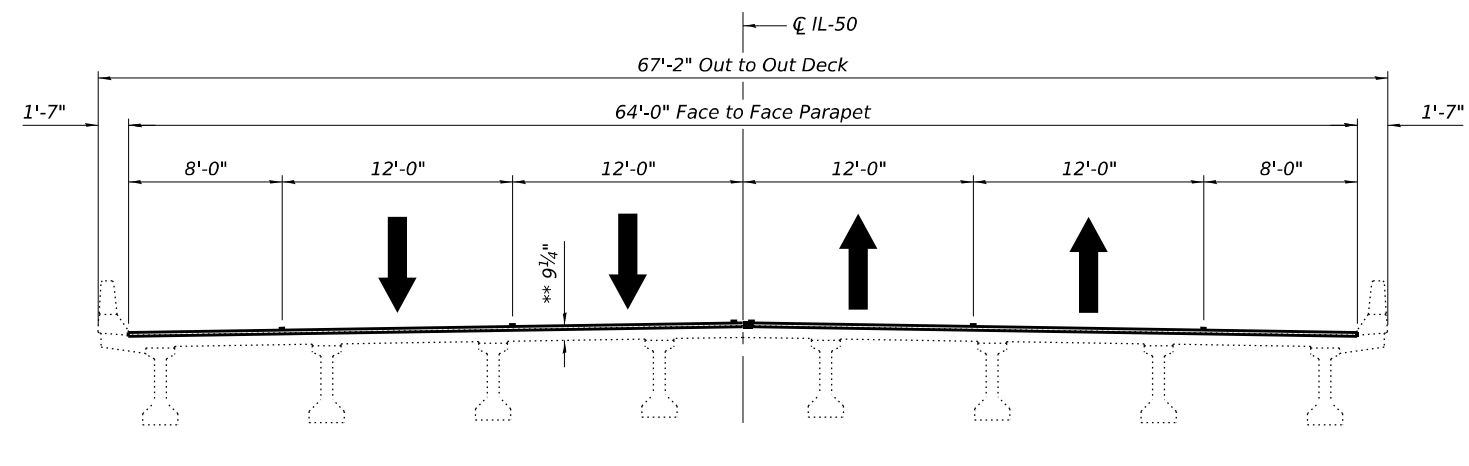
EXISTING
(Looking North)



STAGE I
(Looking North)



STAGE II
(Looking North)



FINAL CONFIGURATION
(Looking North)

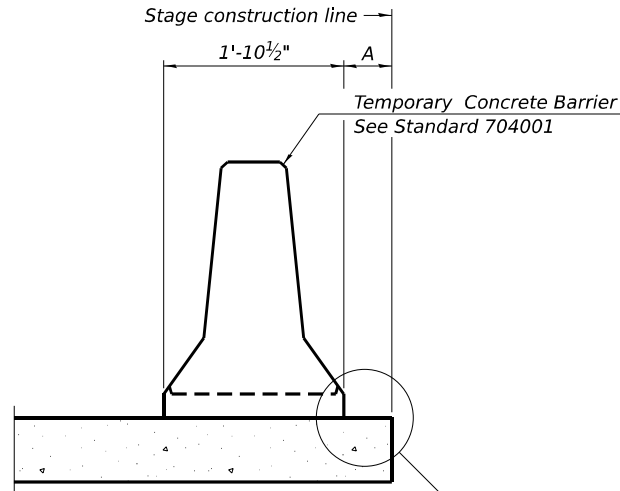
* Place 2 3/4" latex concrete overlay on bridge deck. Final overlay thickness shall be 2 1/2" after 1/4" Diamond Grinding. (See Special Provisions.)

** Prior to grinding

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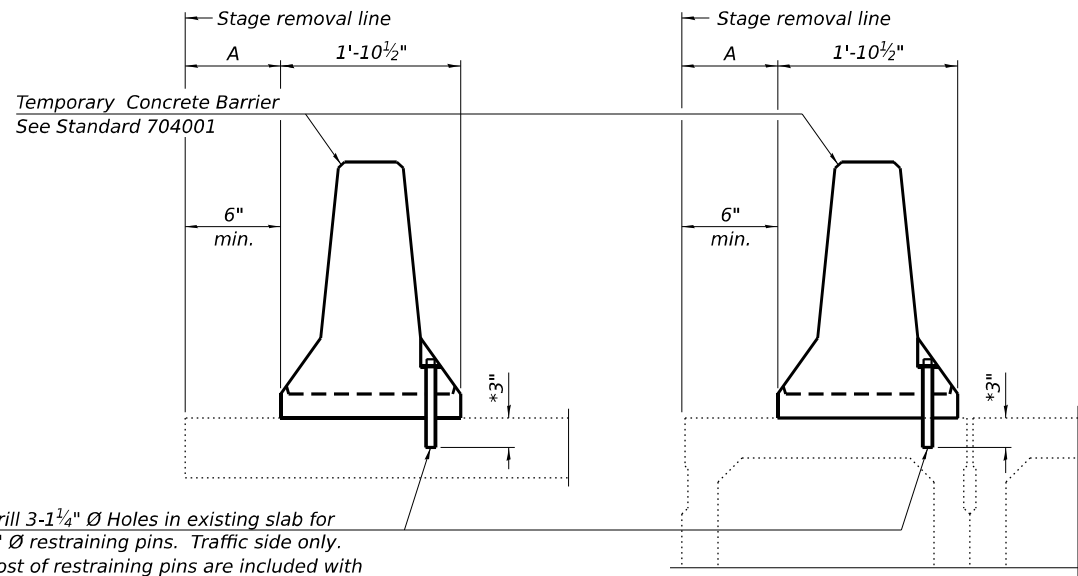
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			DRAWN - JRR	REVISED -			840	(143VB-R-1(90)) BR 25	WILL	45	25				
			CHECKED - WJV	REVISED -		CONTRACT NO. 62Y26									
	PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -	SCALE:		SHEET 3 OF 15 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT						

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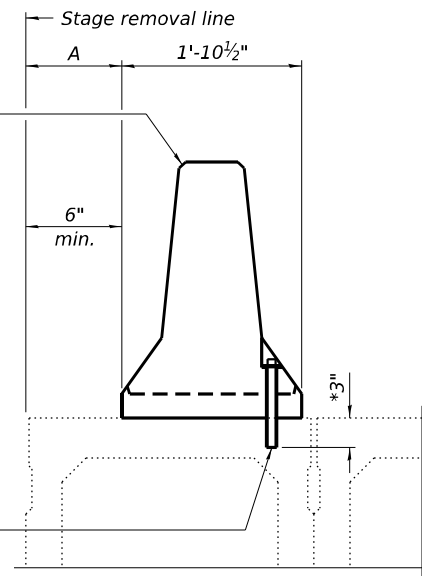
When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



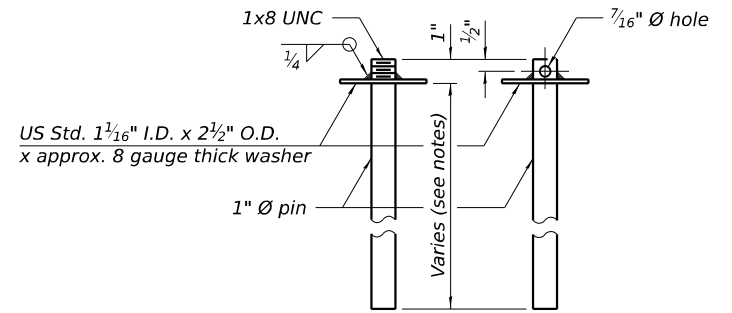
Drill 3-1 1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB



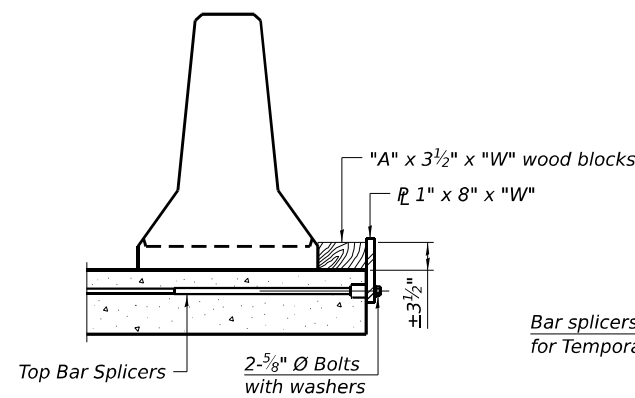
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

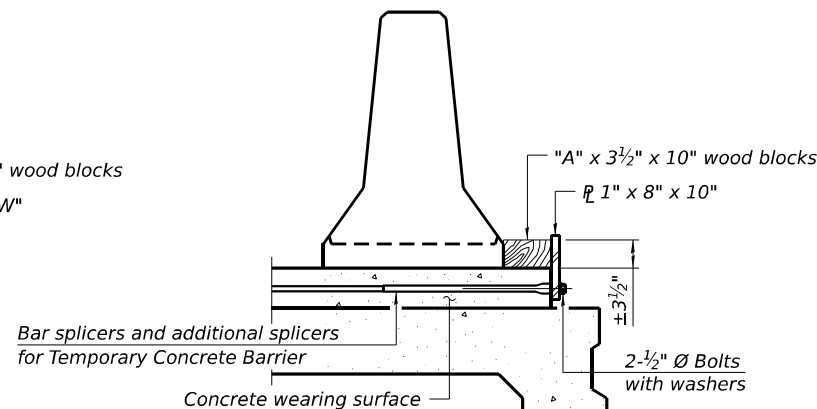


RESTRAINING PIN

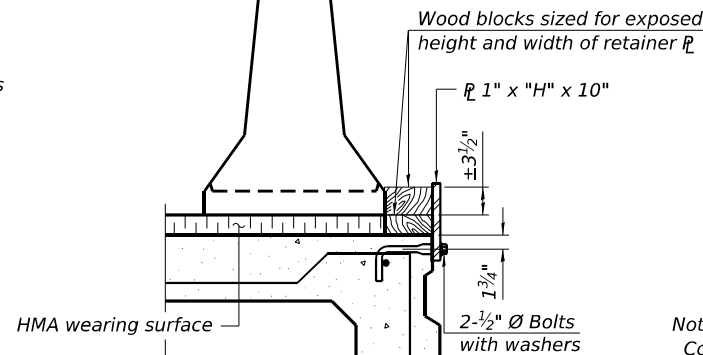
SECTIONS THRU SLAB OR DECK BEAM



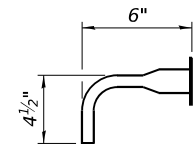
DETAIL I



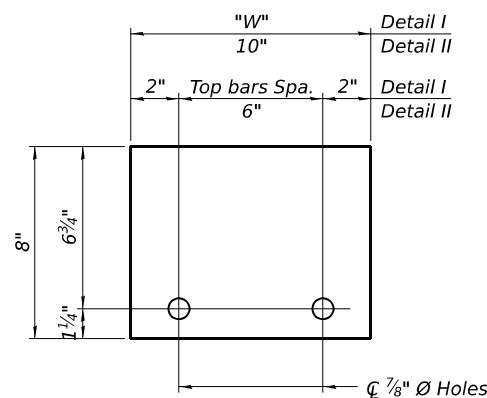
DETAIL II



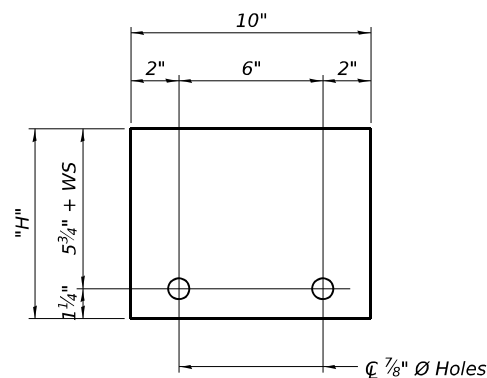
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
Cost of retainer assembly is included with Temporary Concrete Barrier.
A retainer assembly shall be located at the approximate C of each temporary concrete barrier.
The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27

5-15-2023



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REVISED	-
REVISED	-
REVISED	-

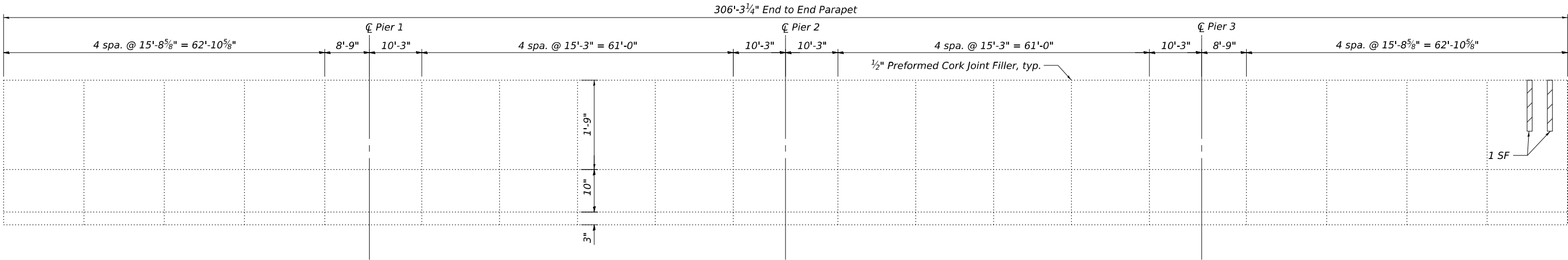
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER
STRUCTURE NO. 099-0298

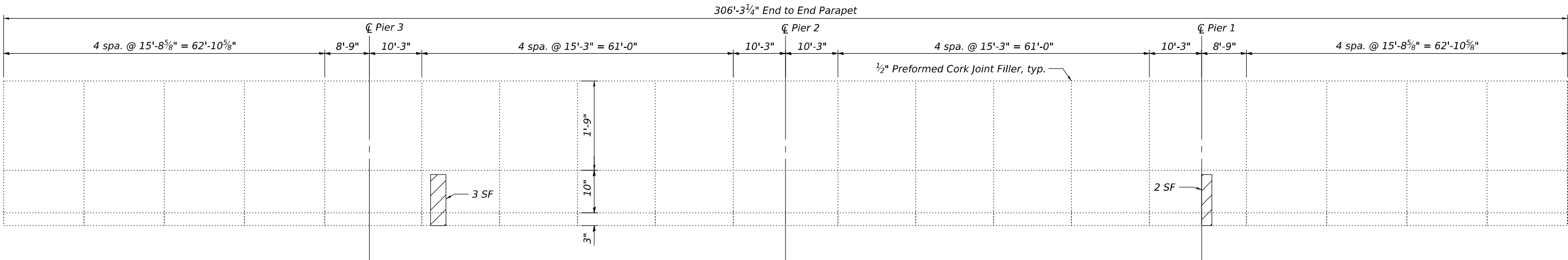
SCALE: SHEET 4 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	26
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

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INSIDE ELEVATION OF WEST PARAPET




INSIDE ELEVATION OF EAST PARAPET

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	7

LEGEND

 Structural Repair of Concrete (Depth Equal to or Less than 5 inches)



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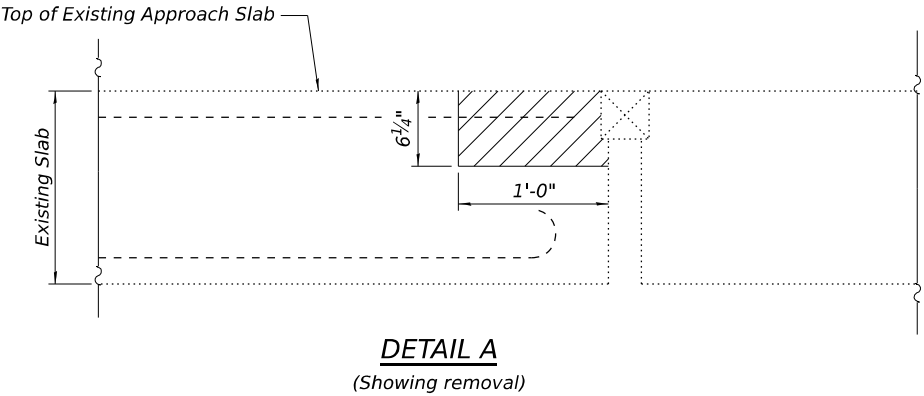
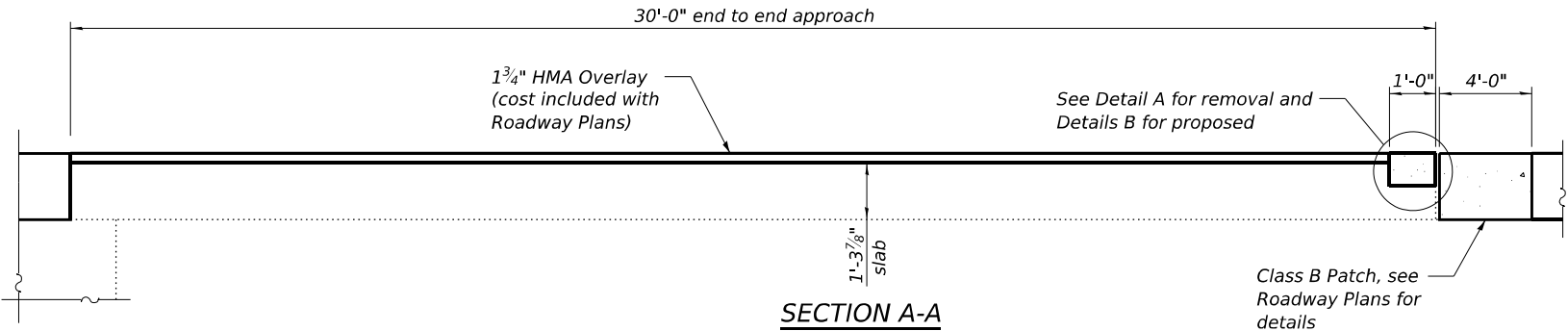
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	CHECKED - WJV	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PARAPET REPAIR PLAN
STRUCTURE NO. 099-0298

SCALE: SHEET 6 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



BILL OF MATERIAL

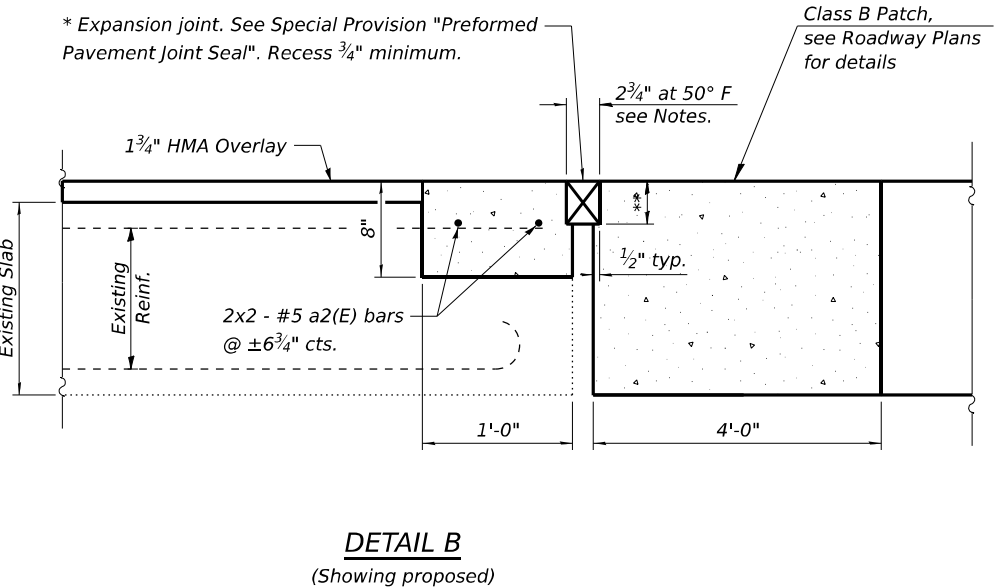
Bar	No.	Size	Length	Shape
a2(E)	8	#5	35'-9"	—
Reinforcement Bars, Epoxy Coated			Pound	300
Concrete Removal			Cu. Yd.	2.6
Concrete Superstructure (Approach Slab)			Cu. Yd.	3.4

LEGEND



Concrete Removal

Notes:
Reinforcement bars designated (E) shall be epoxy coated.
For approach pavement details see Roadway Plans.
The joint opening shall be adjusted for temperature per Article 520.04 of the Standard Specifications.



* Cost included with Concrete Superstructure (Approach Slab)
** Per manufacturer recommendations

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROACH JOINT REMOVAL AND REPLACEMENT
STRUCTURE NO. 099-0298

SCALE: SHEET 7 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	29
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

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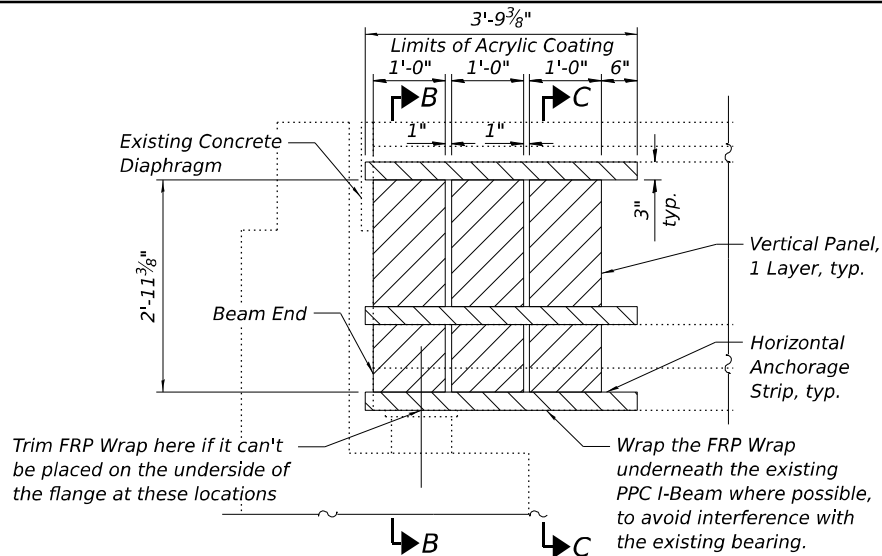


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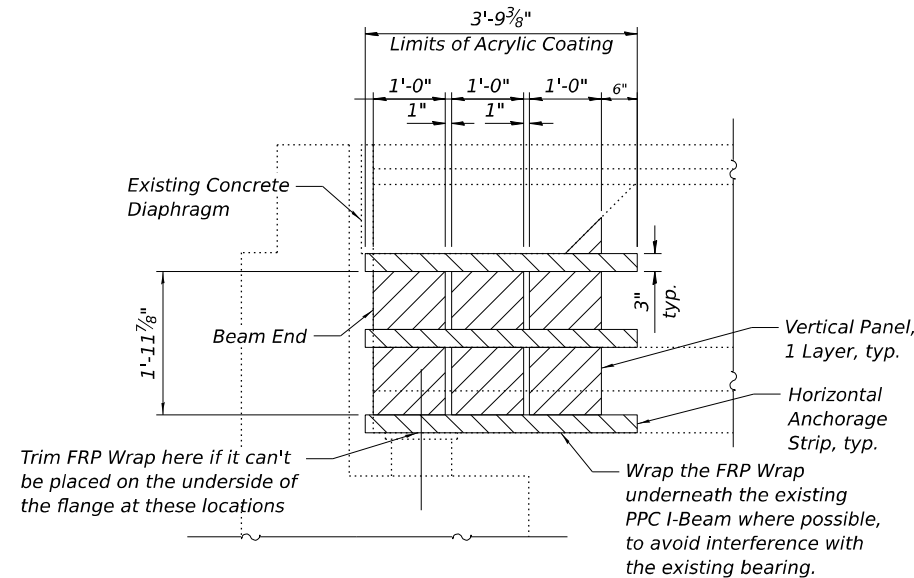
USER NAME = Iyanisch
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DATE - 09/26/2025

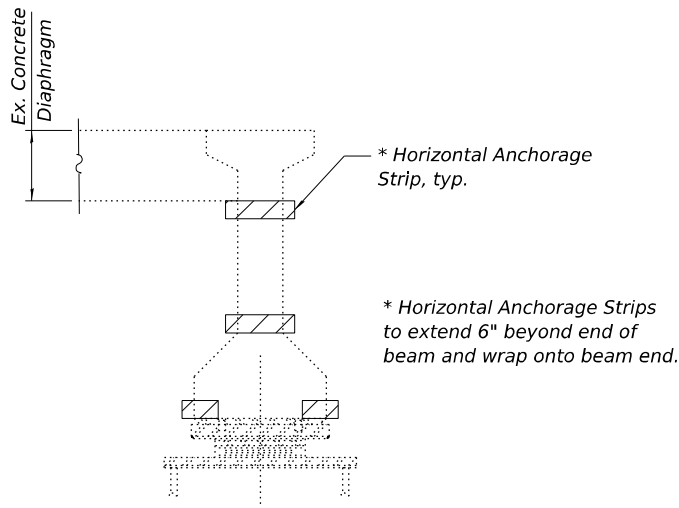
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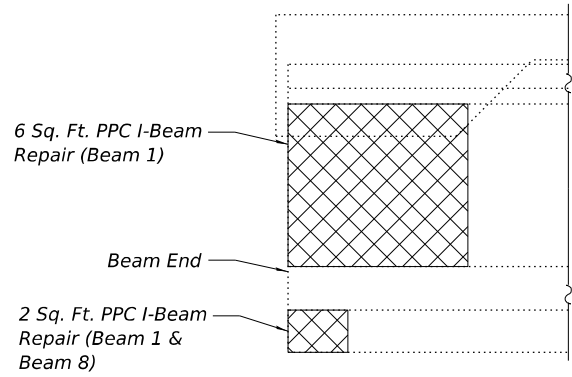
ELEVATION OF BEAM 1 AT N. ABUT. & BEAM 8 AT S. ABUT.



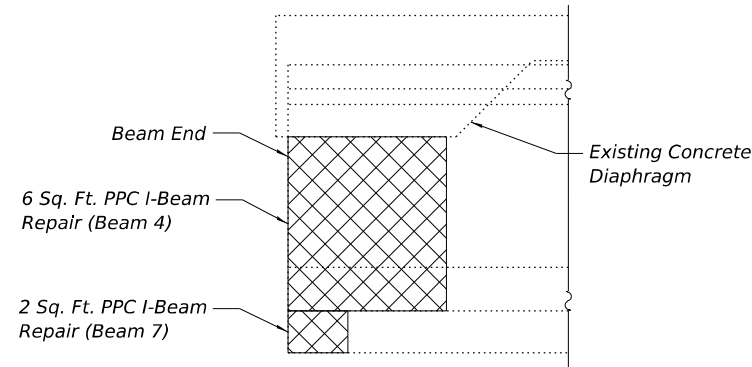
ELEVATION OF BEAM 4 AT S. ABUT. & BEAM 7 AT N. ABUT.



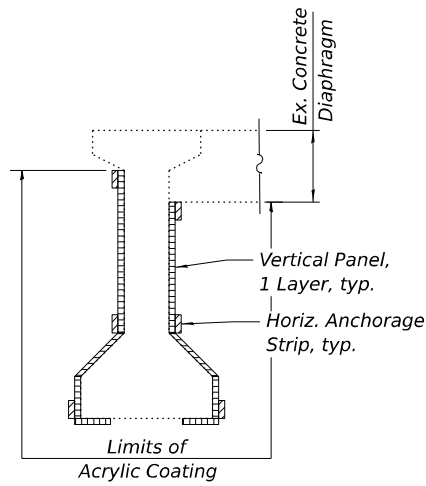
BEAM END VIEW



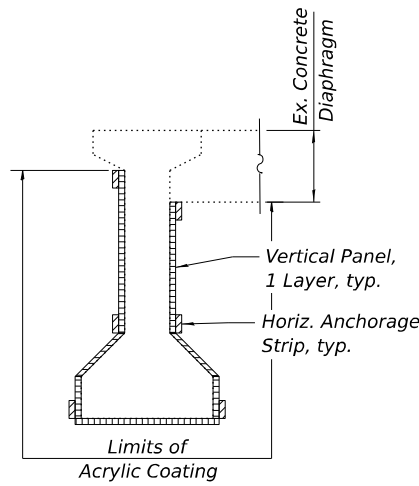
REPAIR DETAIL BEAM 1 AT N. ABUT. & BEAM 8 AT S. ABUT.



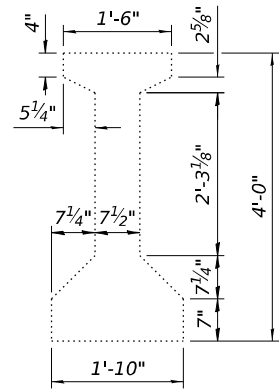
REPAIR DETAIL BEAM 4 AT S. ABUT. & BEAM 7 AT N. ABUT.



SECTION B-B



SECTION C-C



CROSS SECTION

PPC I-BEAM REPAIR

LOCATION	SQ. FT.
Beam 1 West Face - N. Abut.	8
Beam 4 East Face - S. Abut.	6
Beam 7 East Face - N. Abut.	2
Beam 8 East Face - S. Abut.	2

LEGEND

- PPC I-Beam Repair
- Vertical Panel
- Horizontal Anchorage Strip

NOTES:

Vertical panels must be 12". The space between each vertical panel shall be 1". Vertical panels shall extend beyond the repair zone by a minimum of 3". Vertical panels located above the bearing location shall be placed in two pieces as shown in Section A-A. At locations in front of the bearing, the vertical panels shall be one continuous strip wrapping beneath the bottom flange as shown in Section B-B. Horizontal anchorage strips shall be 3" wide and extend a minimum of 6" beyond the vertical panels. The horizontal anchorage strips shall be placed on top of the vertical panels. In areas where deterioration occurs in the ends of beams, the anchorage strip shall wrap around the beam end in one continuous strip. Spalled concrete and exposed reinforcement shall be repaired according to the special provision "PRECAST PRESTRESSED CONCRETE I-BEAM REPAIRS." Acrylic coating shall be placed over fiber wrap repairs. Two coats shall be applied. See special provisions for "FRP STRENGTHENING FOR PPC I-BEAM REPAIRS".

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Acrylic Coating	Sq Yd	13
Fiber Wrap	Sq Ft	119
Precast Prestressed Concrete I-Beam Repair	Sq Ft	18

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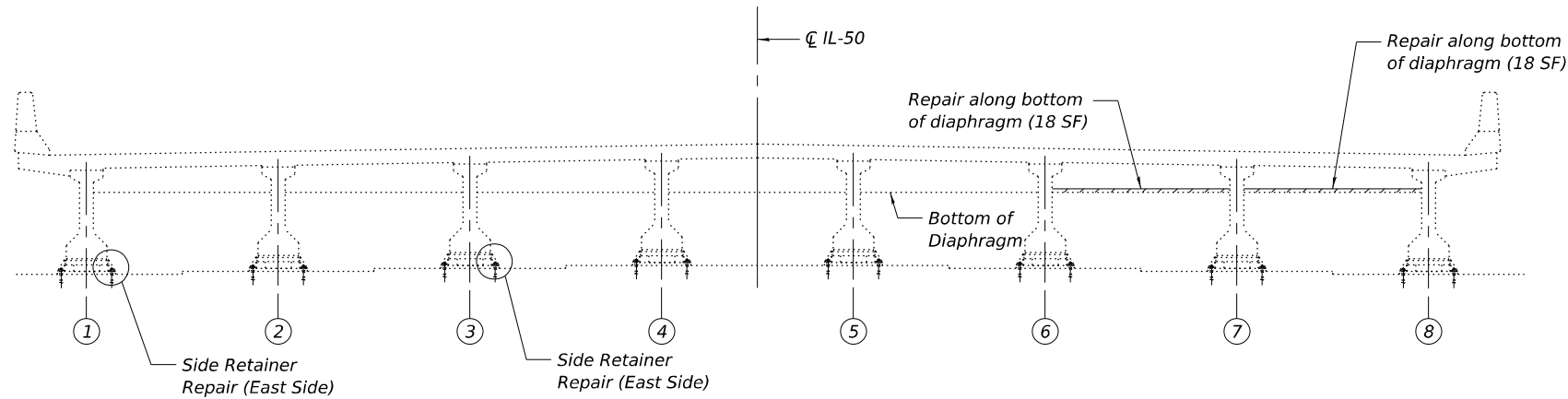
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	CHECKED - WJV	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

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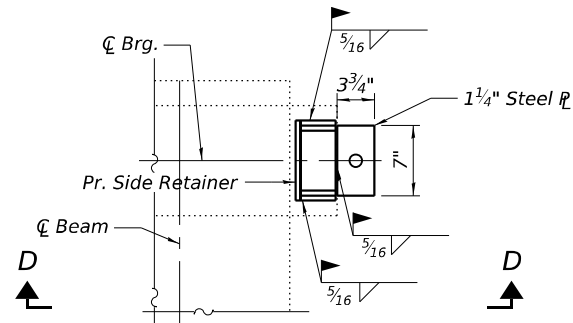
BEAM END REPAIR DETAILS
STRUCTURE NO. 099-0298

SCALE: SHEET 8 OF 15 SHEETS STA. TO STA.

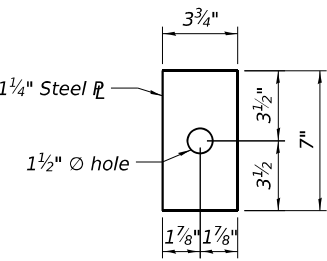
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CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



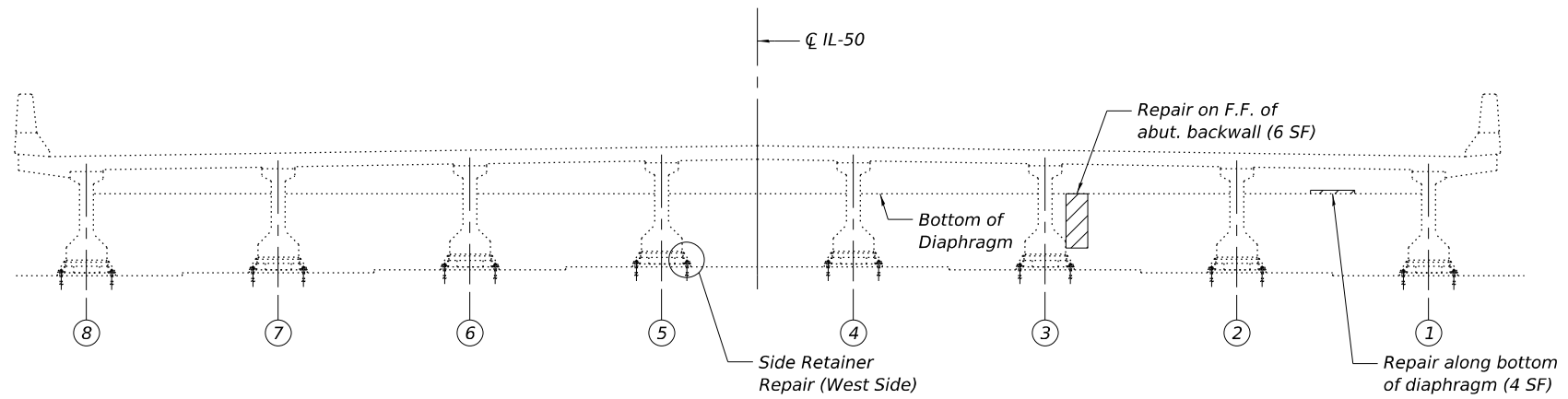
N. ABUTMENT ELEVATION
(Looking North)



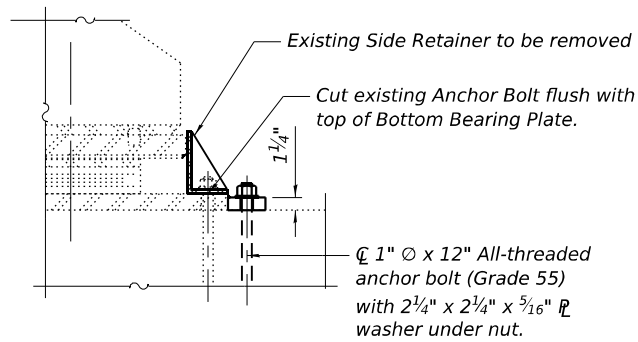
SIDE RETAINER REPAIR PLAN



STEEL PLATE




S. ABUTMENT ELEVATION
(Looking South)



SECTION D-D

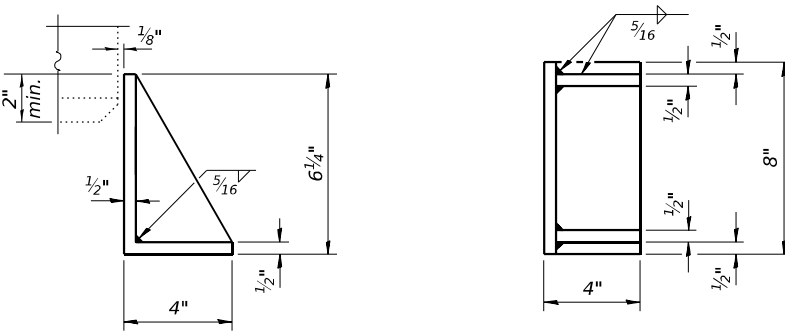
Notes:
 Repair of existing abutment backwalls & diaphragms shall include but may not be limited to the areas shown. The actual area to be repaired will be determined by the Engineer at the time of construction.
 Side Retainers shall be hot-dip galvanizes according to Article 506.09 of the Standard Specifications.
 Concrete Sealer is to be applied to the abutment seats and the bottom 2 feet of the abutment backwall.
 Clean and paint all bearings at North and South abutments.
 Anchor bolts shall be ASTM F1554 all-threaded of the grade(s) and diameter(s) specified.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specification.

LEGEND

 Structural Repair of Concrete
(Depth Equal to or Less than 5 Inches)

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Structural Steel	Pound	69
Anchor Bolts, 1"	Each	3
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq. Ft.	46
Concrete Sealer	Sq. Ft.	98
Cleaning and Painting Bearings	Each	16



SIDE RETAINER
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

MODEL: Sheet View
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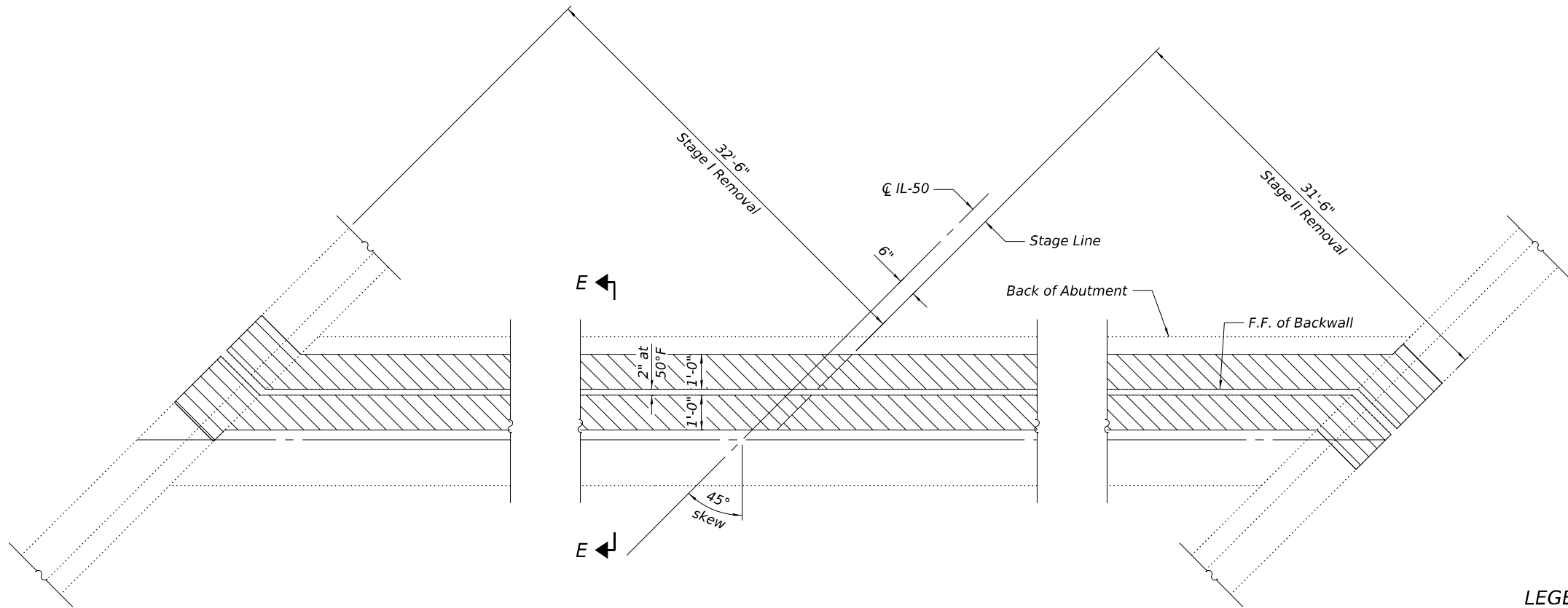
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**STATE OF ILLINOIS
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**DIAPHRAGM AND BEARING REPAIR DETAILS
 STRUCTURE NO. 099-0298**

SCALE: SHEET 9 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	31
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



JOINT REMOVAL PLAN

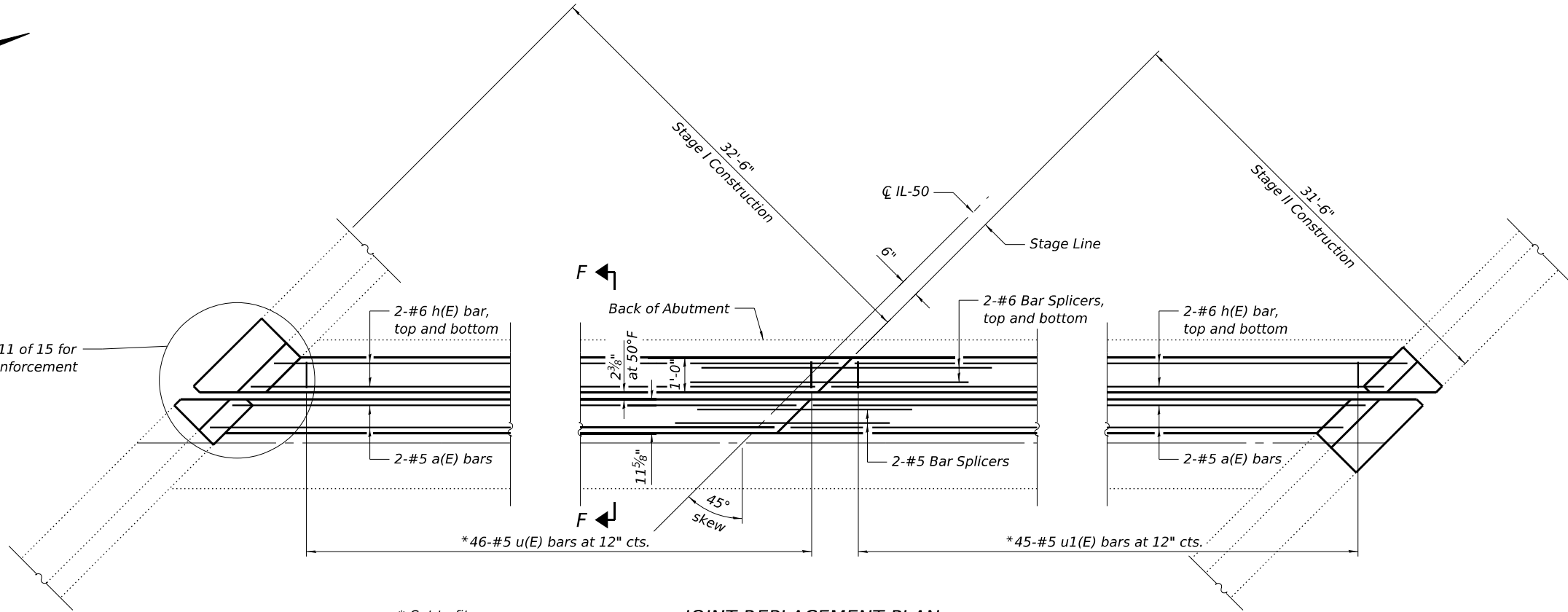
North abutment shown.
South abutment similar by rotation.

LEGEND



Concrete Removal

See Sheet 11 of 15 for
parapet reinforcement



JOINT REPLACEMENT PLAN

North abutment shown.
South abutment similar by rotation.

Notes:
See Sheet 11 of 15 for Section E-E and Section F-F.

* Cut to fit

MODEL: Sheet View
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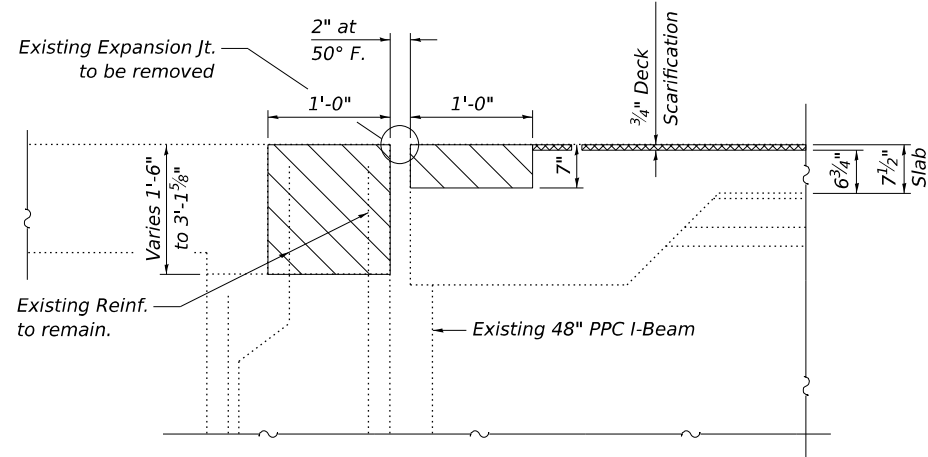
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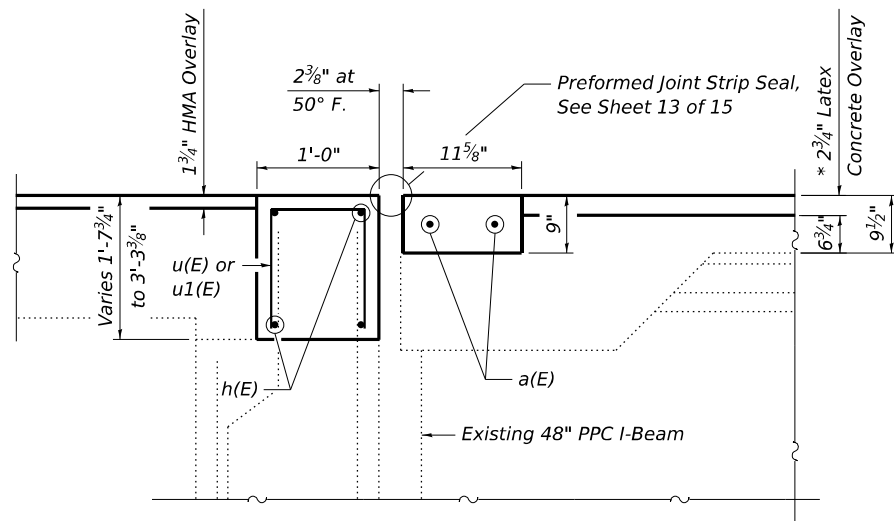
EXPANSION JOINT REPLACEMENT DETAILS (1 OF 3)
STRUCTURE NO. 099-0298

SCALE: SHEET 10 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	32
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

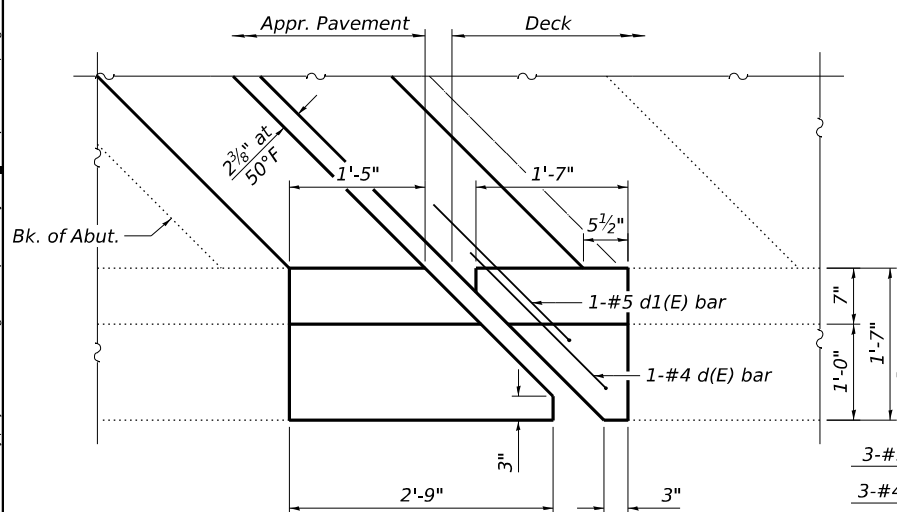


SECTION E-E



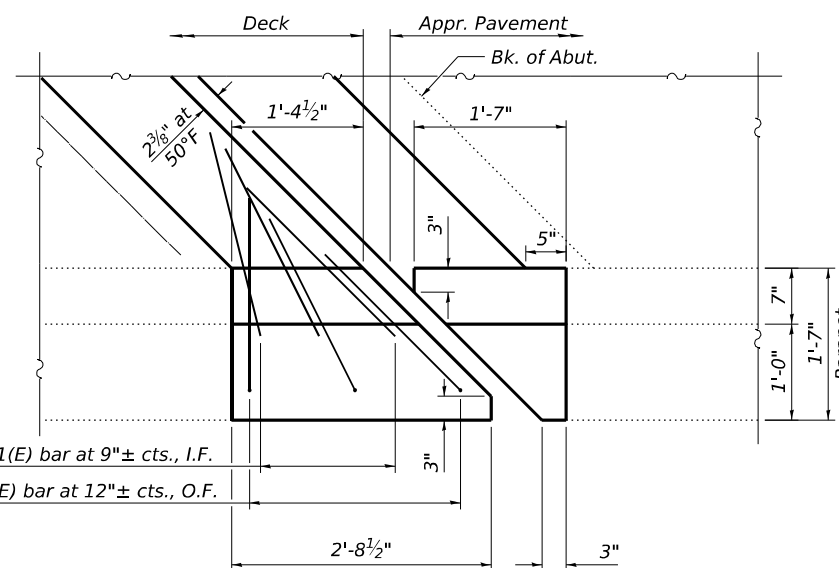
SECTION F-F

* Prior to grinding



PARAPET REPLACEMENT PLAN

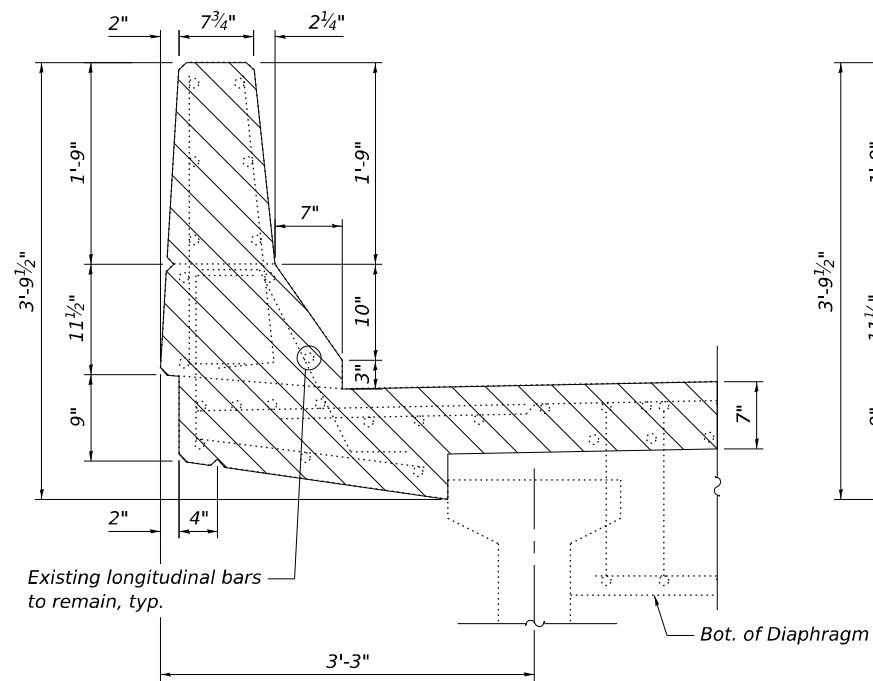
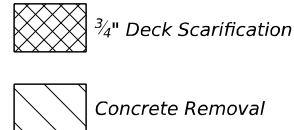
(West end of North Abut. and East end of South Abut.)



PARAPET REPLACEMENT PLAN

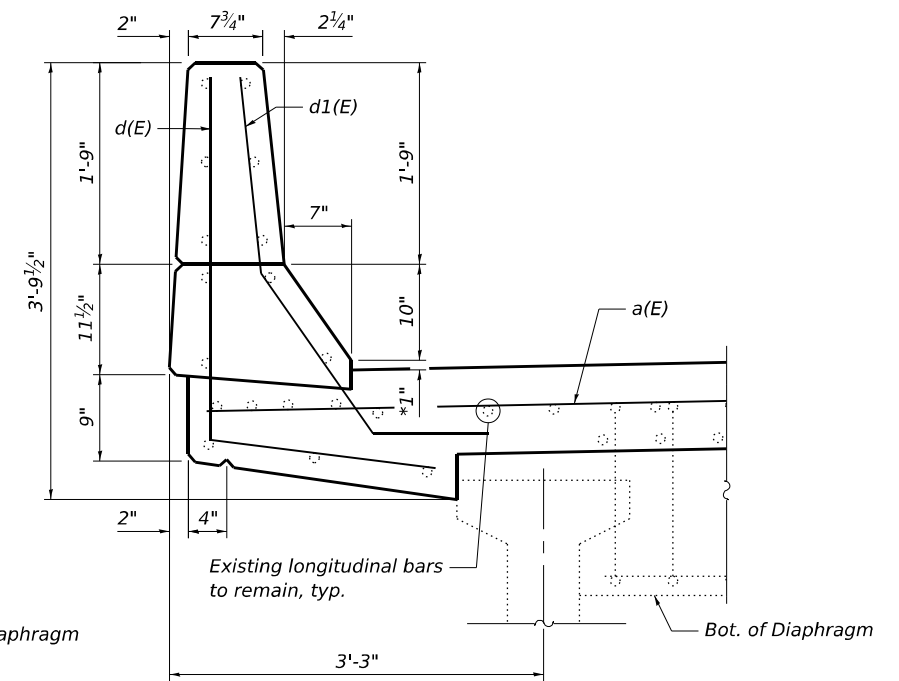
(East end of North Abut. and West end of South Abut.)

LEGEND



**SECTION THRU PARAPET AT JOINT
ON BRIDGE DECK SIDE**

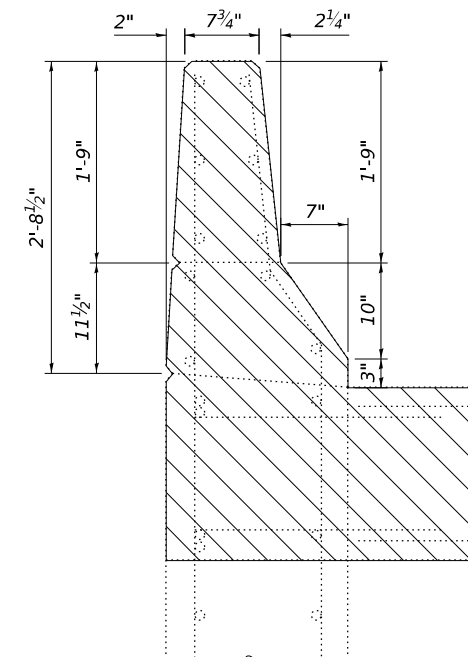
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**SECTION THRU PARAPET AT JOINT
ON BRIDGE DECK SIDE**

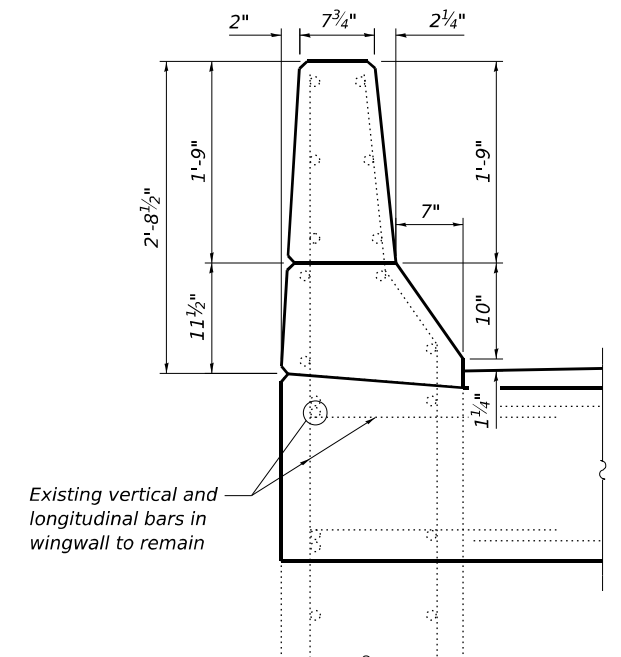
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* Prior to grinding



**SECTION THRU PARAPET AT JOINT
ON APPROACH SIDE**

(Showing Removal)



**SECTION THRU PARAPET AT JOINT
ON APPROACH SIDE**

(Showing Proposed)

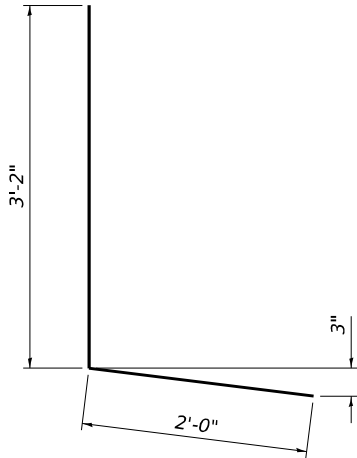
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXPANSION JOINT REPLACEMENT DETAILS (2 OF 3)
STRUCTURE NO. 099-0298**

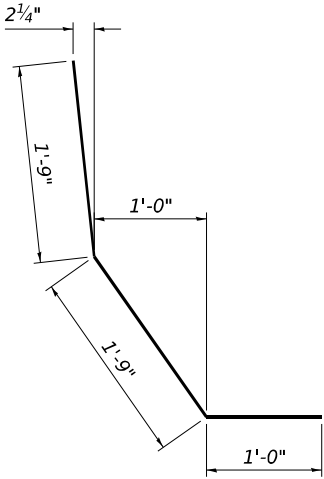
SCALE: SHEET 11 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

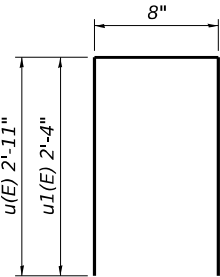
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	DRAWN - LJM	REVISED -
	CHECKED - WJV	REVISED -
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BAR d(E)



BAR d1(E)



BAR u(E) & u1(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	4	5	47'-6"	—
a1(E)	4	5	46'-0"	—
d(E)	8	4	5'-2"	L
d1(E)	8	5	4'-6"	L
h(E)	8	6	47'-6"	—
h1(E)	8	6	46'-0"	—
u(E)	92	5	6'-6"	Π
u1(E)	90	5	5'-4"	Π
Reinforcement Bars, Epoxy Coated			Lbs.	2,710
Concrete Superstructure			Cu. Yd.	22.5
Concrete Removal			Cu. Yd.	20.4
Bar Splicers			Each	12

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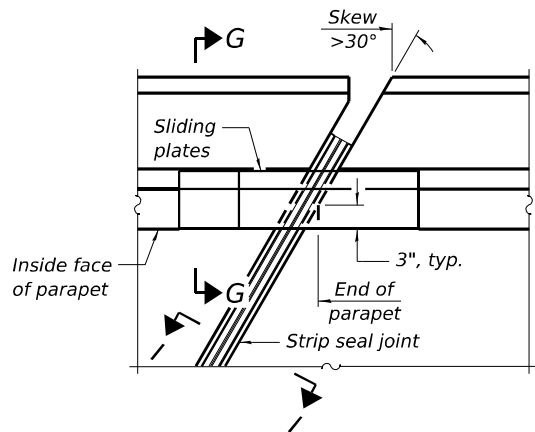
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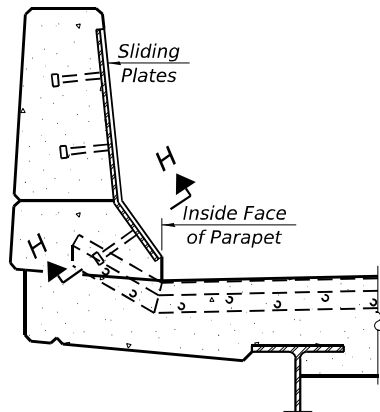
EXPANSION JOINT REPLACEMENT DETAILS (3 OF 3)
STRUCTURE NO. 099-0298

SCALE: SHEET 12 OF 15 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	34
CONTRACT NO. 62Y26				
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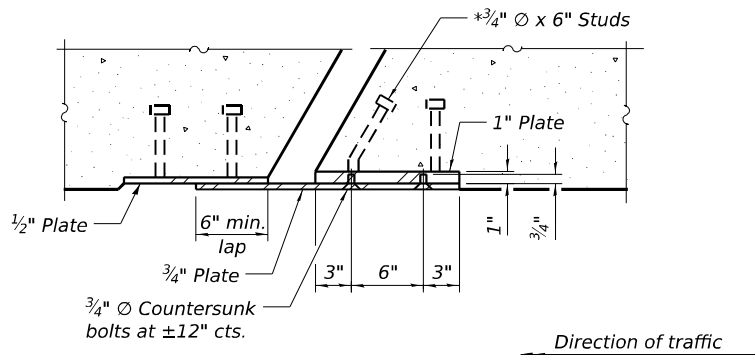
PLAN AT PARAPET



SECTION G-G

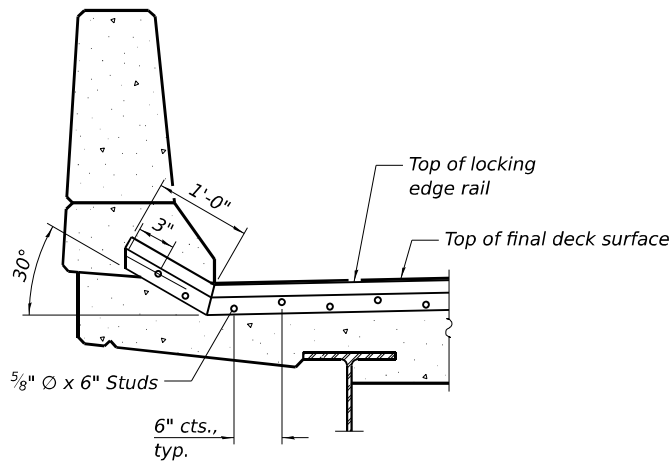
POINT BLOCK DETAILS

(for skews > 30°)



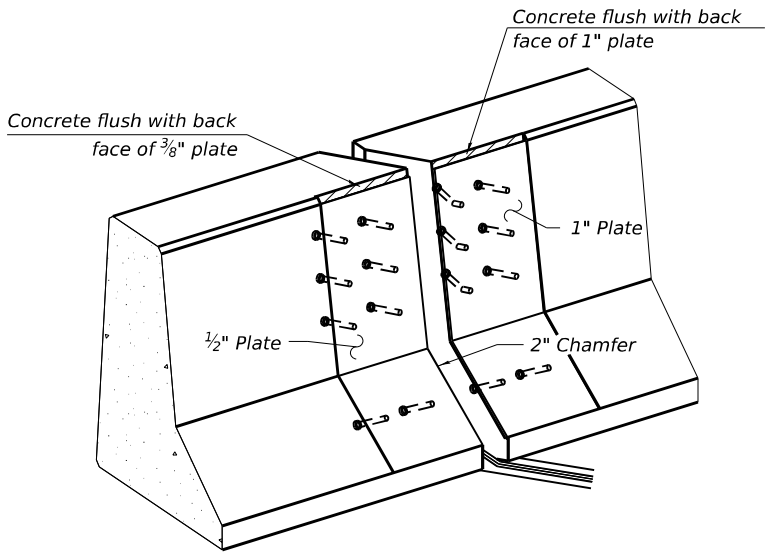
SECTION H-H

(3/4" Ø countersunk bolts extending into concrete shall have hex nuts tack welded to the back of the embedded plates with end caps provided.)



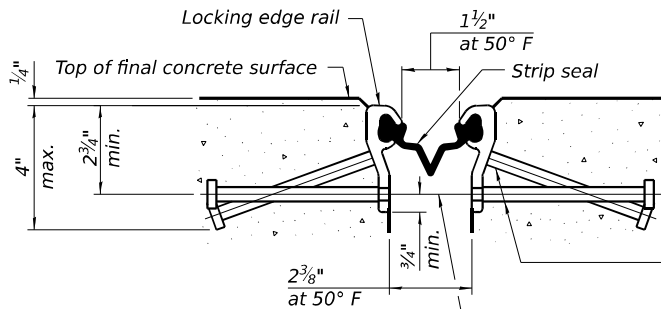
TYPICAL END TREATMENT

AT PARAPET



TRIMETRIC VIEW

(Showing embedded plates only)



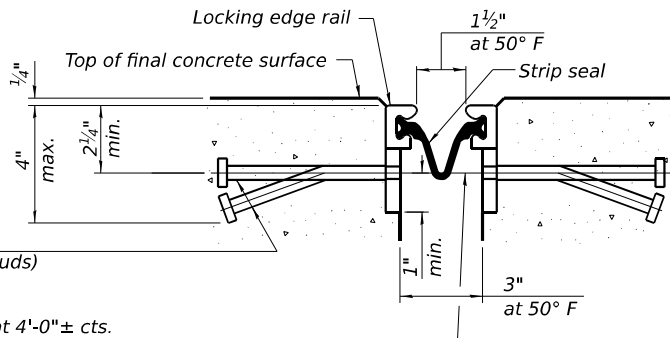
SHOWING ROLLED RAIL JOINT

* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

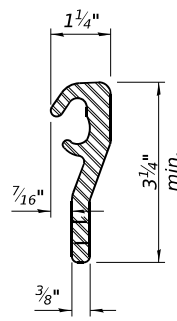
3/8" Ø threaded rods in 7/16" Ø holes at 4'-0" ± cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION I-I

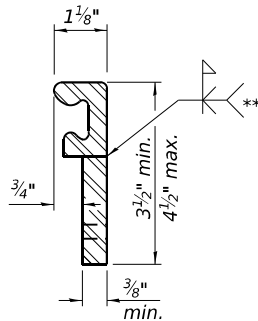
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



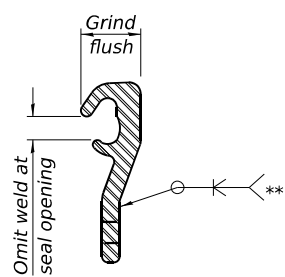
SHOWING WELDED RAIL JOINT



ROLLED (EXTRUDED) RAIL



WELDED RAIL



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	186

MODEL: Sheet View
FILE NAME: c:\users\jgung\appdata\local\h Bentley\project\working\dir\300-pw-bentley.com_v300-pw-01\ave.pung\jgung\14450\162Y26-Plan-Board.dgn



V3 Companies
7325 Janes Avenue
Woodridge, IL 60517
630.724.9200 phone
630.724.9202 fax
www.v3co.com

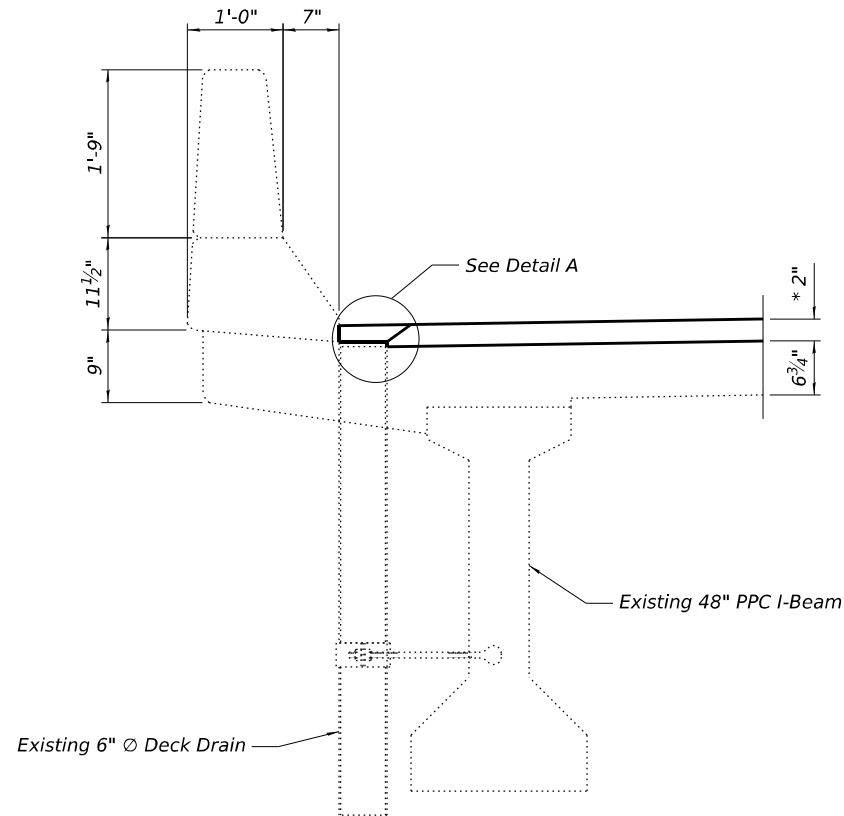
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	DRAWN - JRR	REVISED -
	CHECKED - WJV	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

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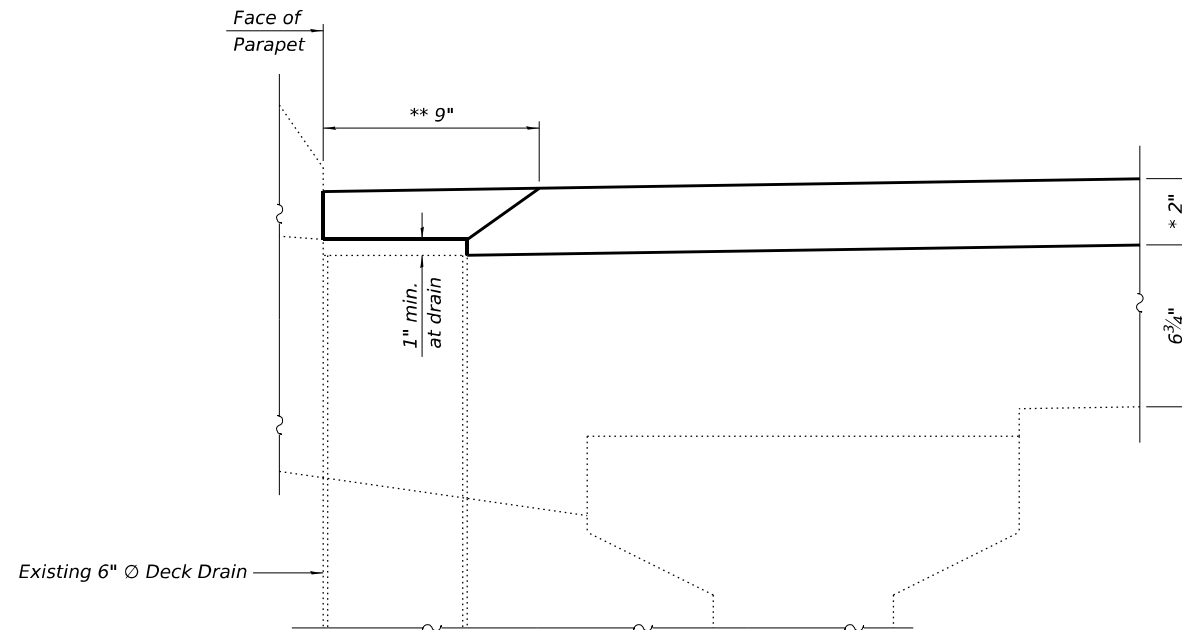
PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 099-0298

SCALE: SHEET 13 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				

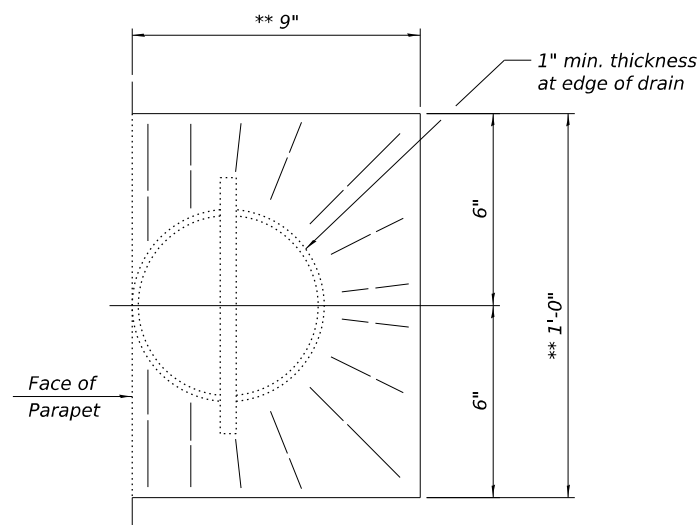


SECTION AT DECK DRAIN



DETAIL A

* Before diamond grinding
** Slope to drain



DECK DRAIN DETAIL

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DECK FLOOR DRAIN DETAILS
STRUCTURE NO. 099-0298**

SCALE: SHEET 14 OF 15 SHEETS STA. TO STA.

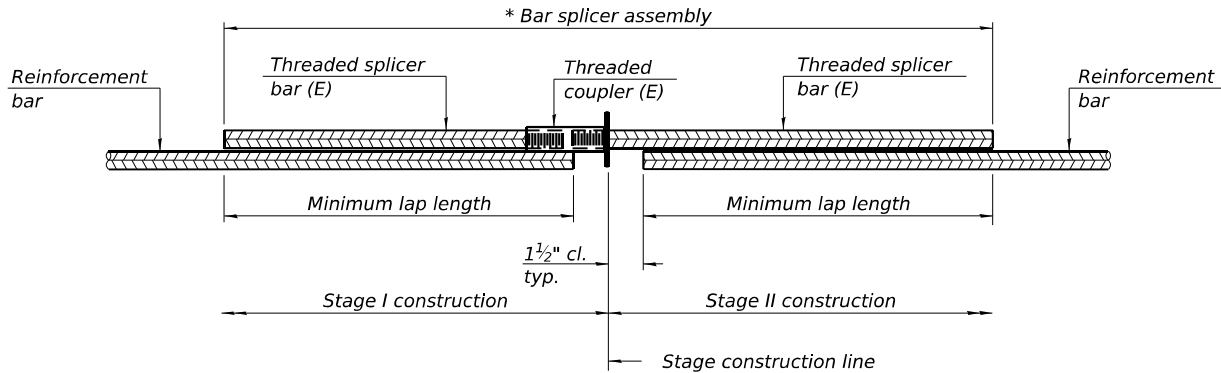
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	36
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



USER NAME	= Iyanisch
PLOT DATE	= 6/27/2025

DESIGNED	-	JRR
DRAWN	-	JRR
CHECKED	-	WJV
DATE	-	09/26/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-



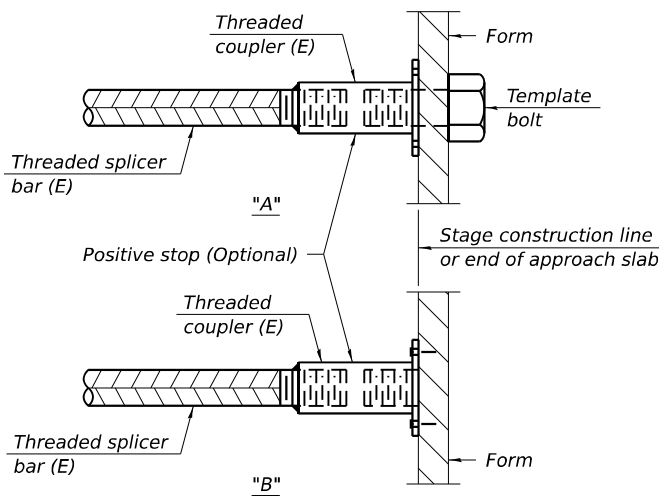
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
N. Abut.	#5	2	2'-10"
N. Abut.	#6	4	3'-11"
S. Abut.	#5	2	2'-10"
S. Abut.	#6	4	3'-11"

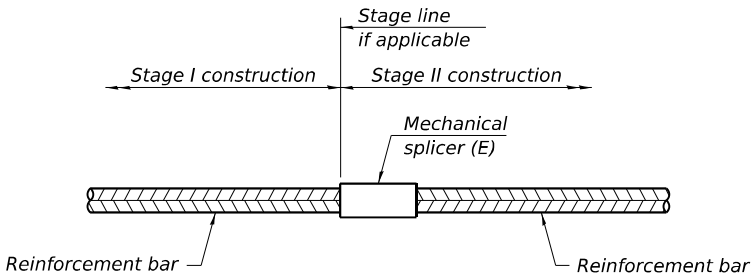


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

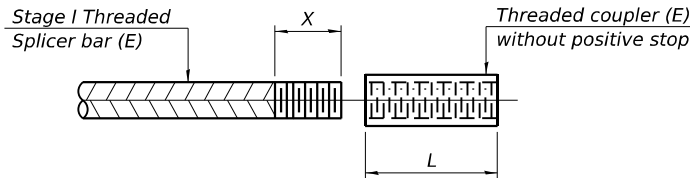
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



THREADING OF ASSEMBLIES

The threaded length "X" shall be no more than L/2. The bar should be tightened until 0-1 thread(s) is/are exposed.

Notes:
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Sheet View
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BSD-1

4-4-2025



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630.724.9200 phone
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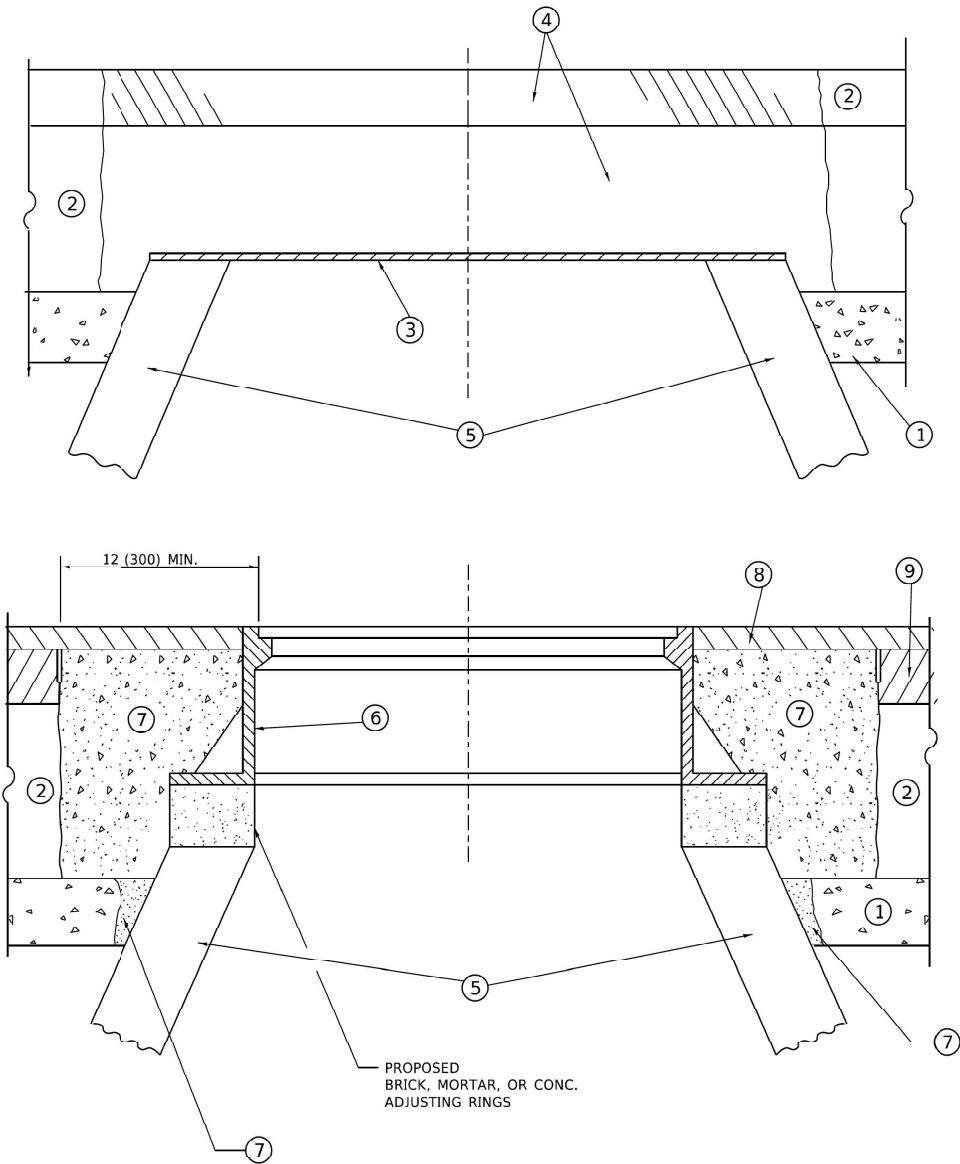
USER NAME = Iyanisch	DESIGNED - JRR	REVISED -
	DRAWN - JRR	REVISED -
	CHECKED - WJV	REVISED -
PLOT DATE = 6/27/2025	DATE - 09/26/2025	REVISED -

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DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 099-0298

SCALE: SHEET 15 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	37
CONTRACT NO. 62Y26				
ILLINOIS FED. AID PROJECT				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL Default
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USER NAME - Lawrence,DeMancho	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11
	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

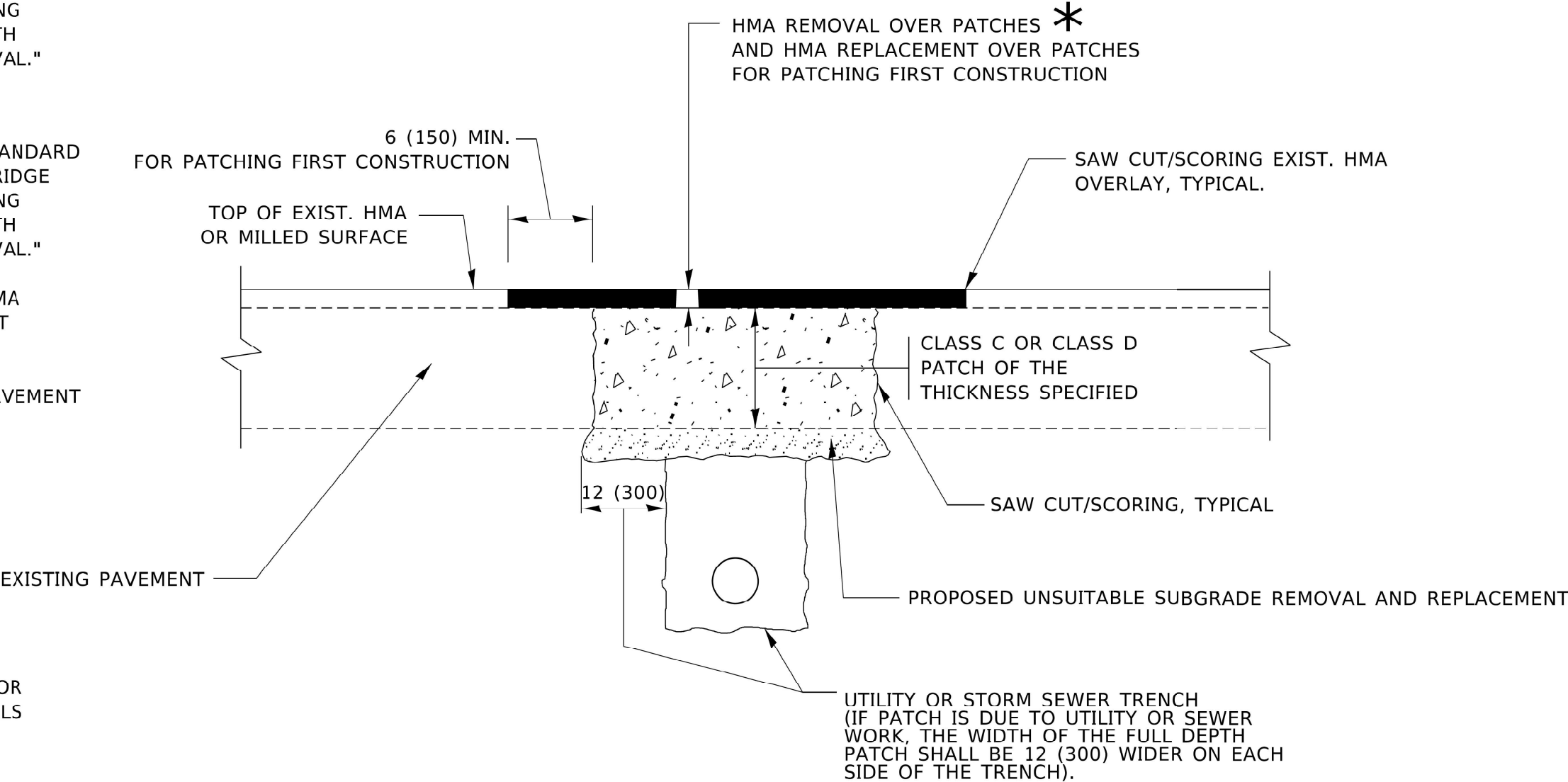
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840	(143VB-R-1(90)) BR 25	WILL	45	38
BD600-03 (BD-08)		CONTRACT NO. 62Y26		
ILLINOIS		FED. AID PROJECT		

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT SCALE: 100.0000' / in.
DESIGNED: R. SHAH
DRAWN: -
CHECKED: -
DATE: 10-25-94
REVISED: R. BORO 01-01-07
REVISED: R. BORO 09-04-07
REVISED: K. ENG 10-27-08
REVISED: K. SMITH 11-18-22

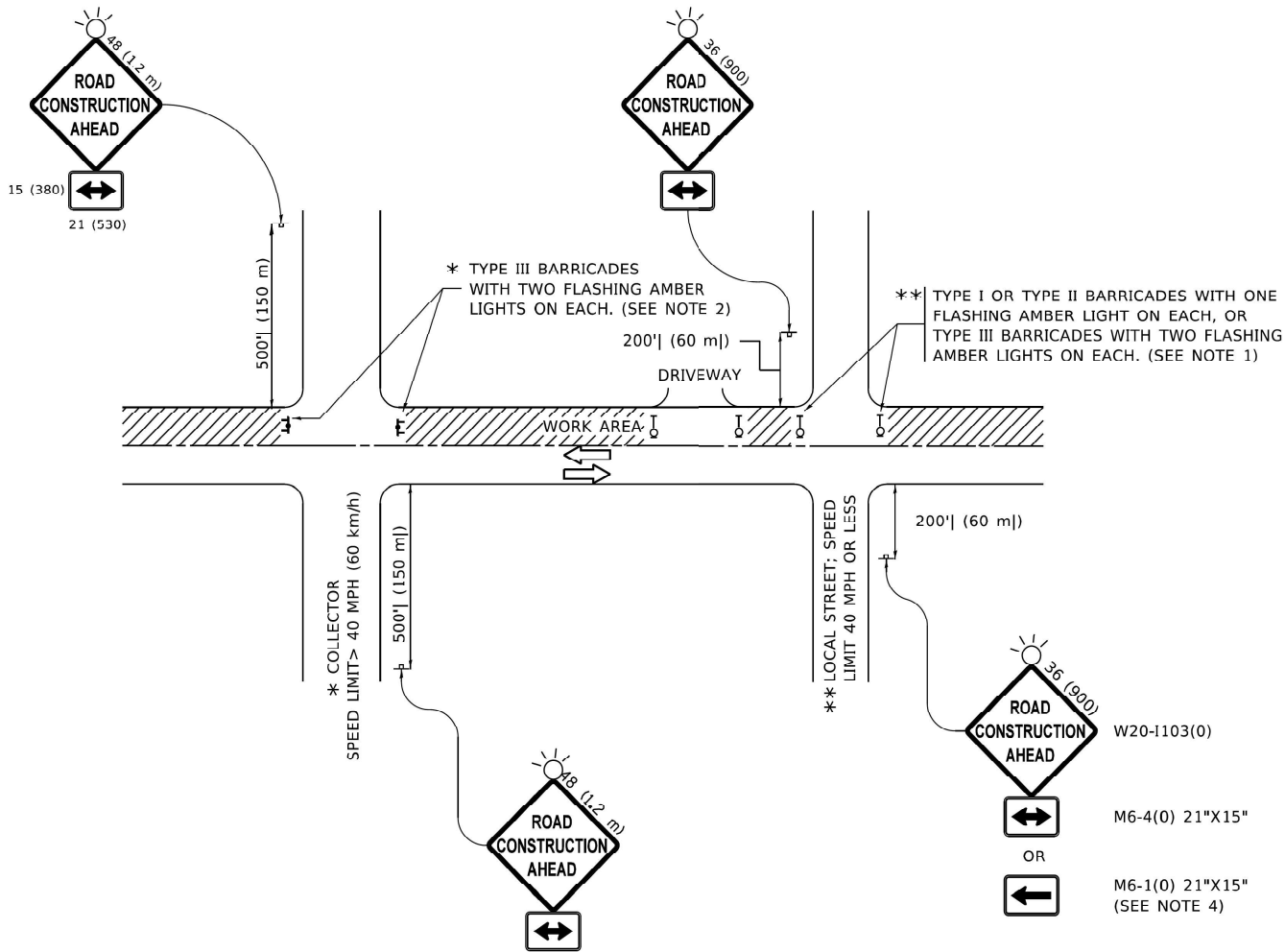
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	39
BD400-04 (BD-22)		CONTRACT NO. 62Y26		
		ILLINOIS FED. AID PROJECT		

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NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters)
unless otherwise shown.

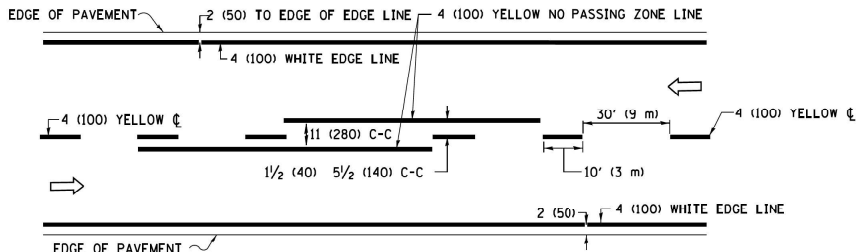
USER NAME	- Lawrence,DeMancho	DESIGNED	L.H.A.	REVISED	T. RAMMACHER 01-06-00
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STATE OF ILLINOIS
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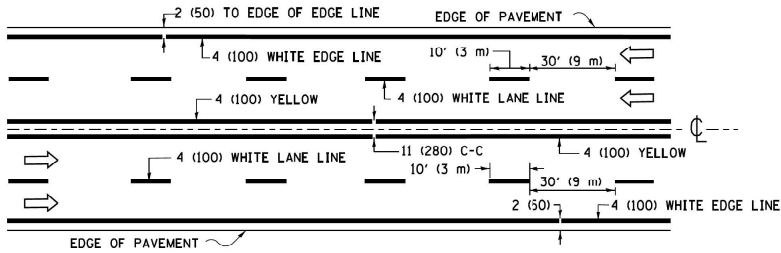
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

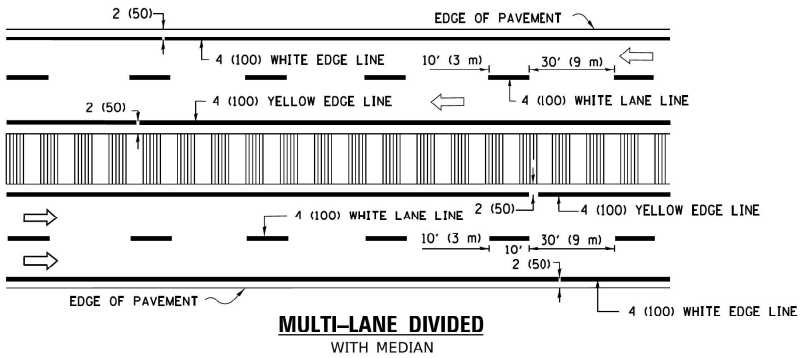
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		ILLINOIS	FED. AID PROJECT	



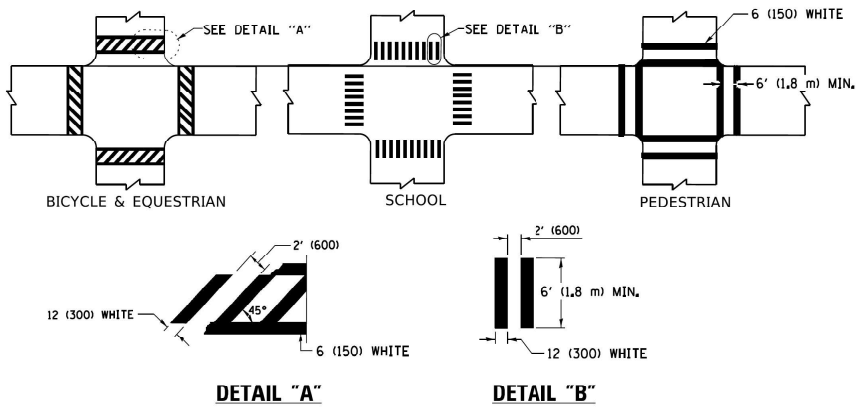
2-LANE ROADWAY



MULTI-LANE UNDIVIDED

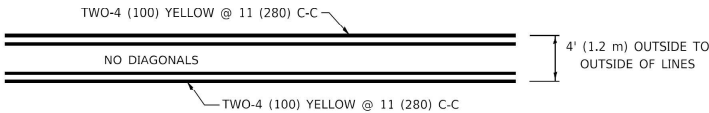


TYPICAL LANE AND EDGE LINE MARKING

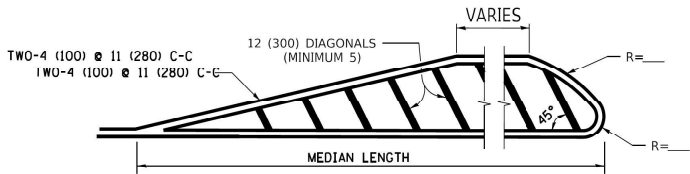


TYPICAL CROSSWALK MARKING

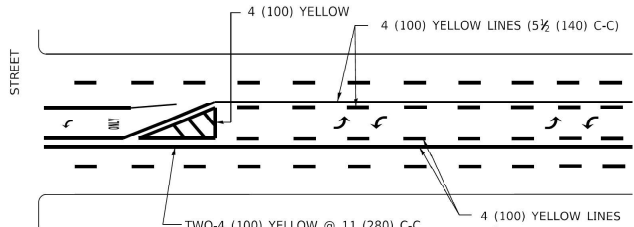
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY

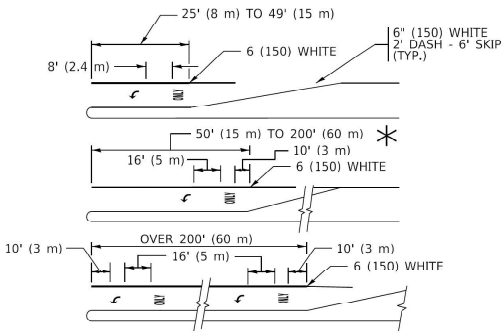


MEDIANS OVER 4' (1.2 m) WIDE



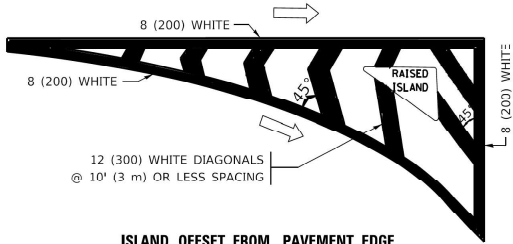
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

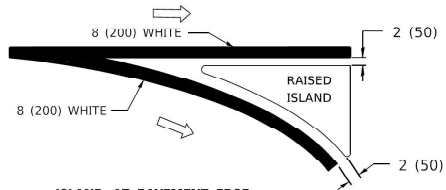


TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

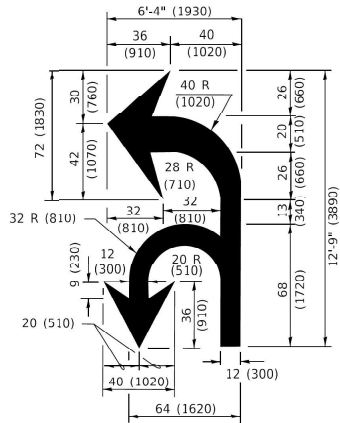


ISLAND OFFSET FROM PAVEMENT EDGE

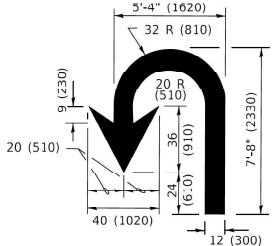


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m²) EACH *X*=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME - footenj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000 " / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	42
TC-13		CONTRACT NO. 62Y26		
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

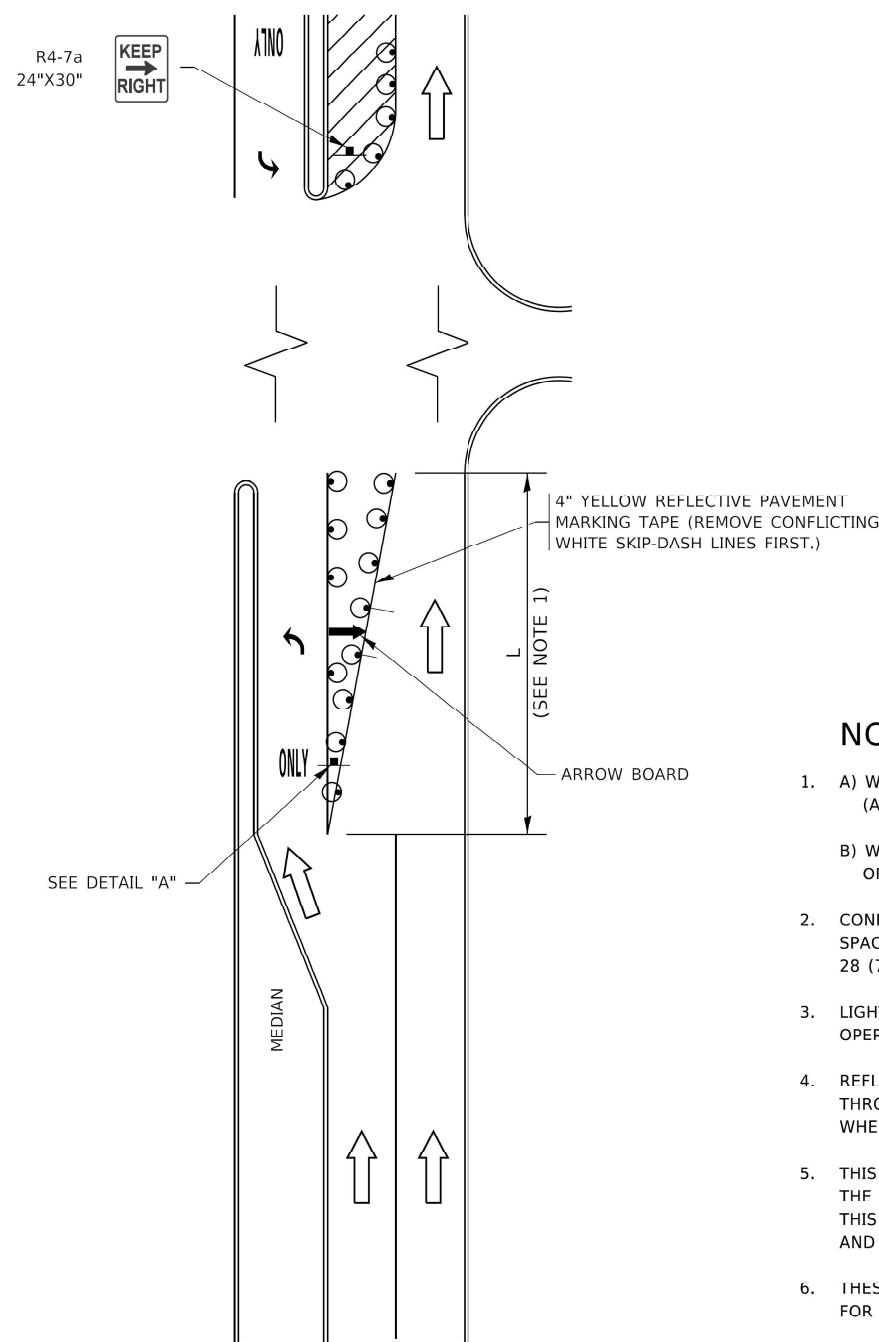
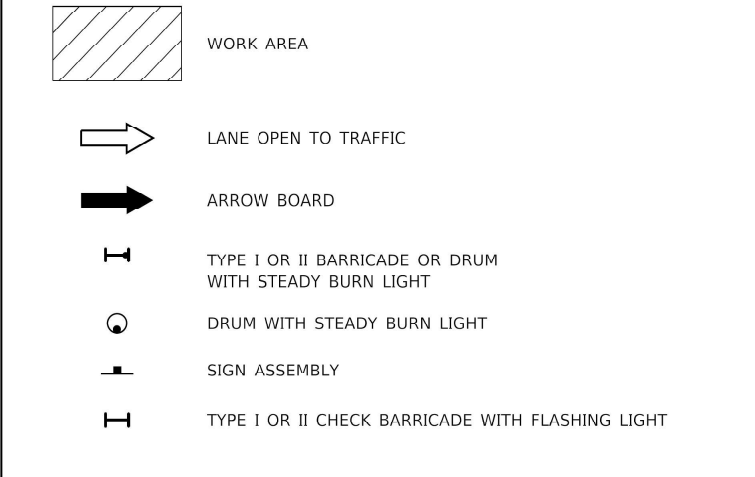


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

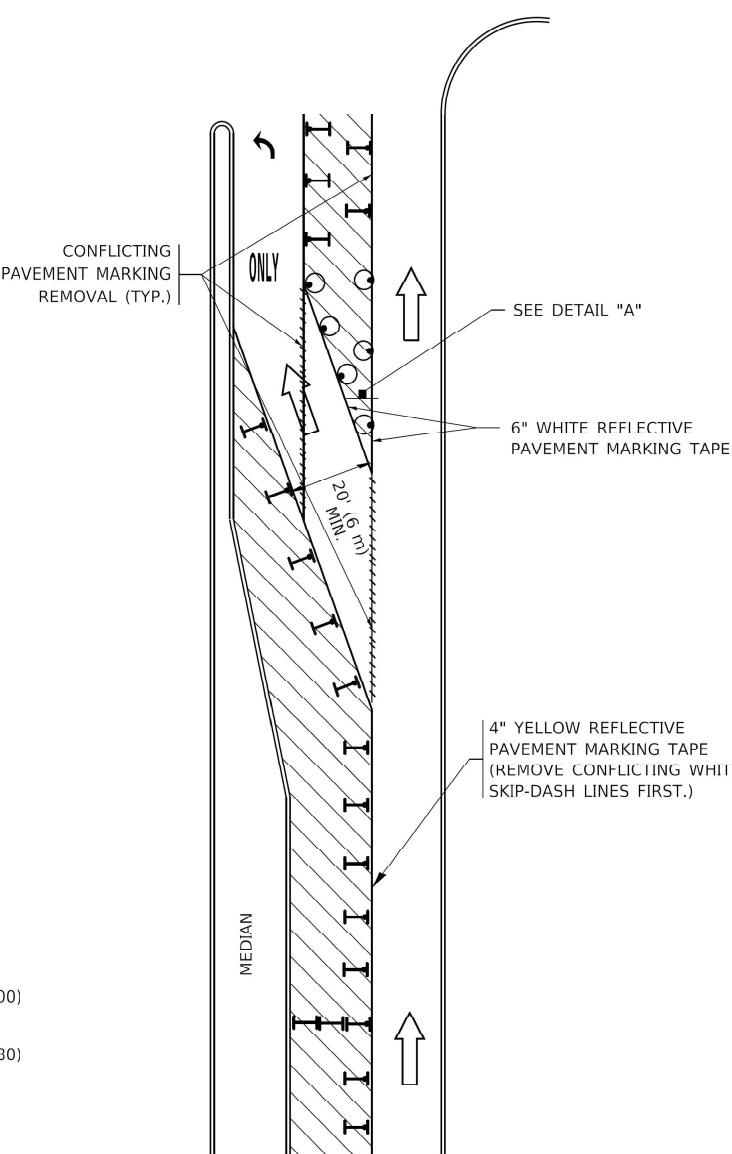
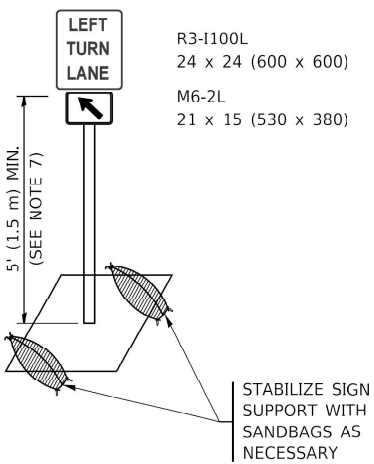


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME	- footemj
DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
PLOT SCALE	= 50.0000 " / in.
PLOT DATE	= 3/4/2019
CHECKED	- A. HOUSEH 10-12-96
DATE	- T. RAMMACHER 01-06-00

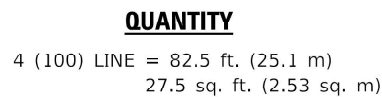
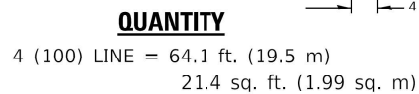
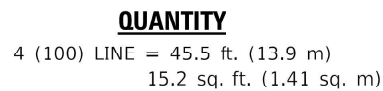
REVISED	- R. BORO 09-14-09
REVISED	- A. SCHUETZE 07-01-13
REVISED	- A. SCHUETZE 09-15-16
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

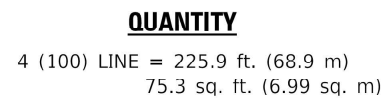
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	43
TC-14		CONTRACT NO. 62Y26		
ILLINOIS		FED. AID PROJECT		



ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS						F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						840	(143VB-R-1(90)) BR 25	WILL	45	44
						TC-16		CONTRACT NO. 62Y26		
SCALE: NONE		SHEET 1	OF 1	SHEETS	STA.	TO STA.				
						ILLINOIS FED. AID PROJECT				

MODEL Default
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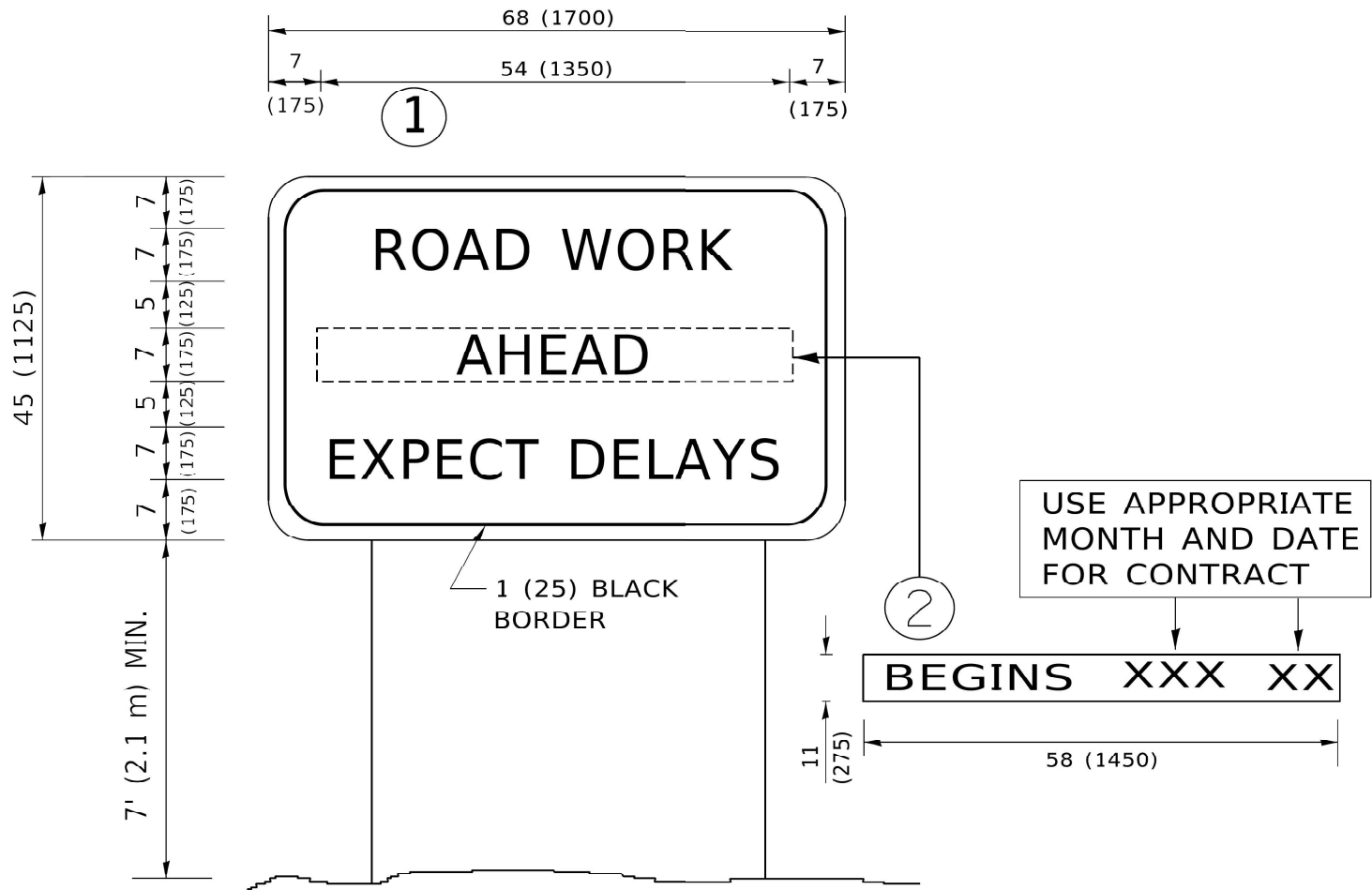
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		DRAWN	-	REVISED	-	R. MIRS 12-11-97
PLOT SCALE	= 50.0000 " / in.	CHECKED	-	REVISED	-	T. RAMMACHER 02-02-99
PLOT DATE	= 3/4/2019	DATE	-	REVISED	-	C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
840	(143VB-R-1(90)) BR 25	WILL	45	45
TC-22		CONTRACT NO. 62Y26		
ILLINOIS		FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN①WITH INSTALLED PANEL②ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL②SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.