

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	HEET NO.
334	2025-1096-RS	DUPAGE	28	1

* 28 + 1 = 29 TOTAL SHEETS

D-91-161-25

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS IMPROVEMENT IS IN THE
VILLAGE OF BENSENVILLE

TRAFFIC DATA

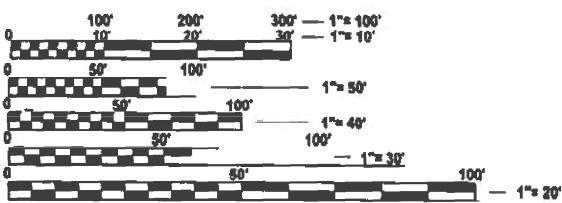
ADT: 34700 (2023)
SPEED LIMIT: 45 MPH

PROPOSED HIGHWAY PLANS

FAP ROUTE 334: IL-83 (BUSSE RD)
LIMIT: S OF BRYN MAWR AVE TO IL-19 (IRVING PARK RD)
SECTION: 2025-1096-RS
PROJECT: NHPP - WQ08(202)
SMART OVERLAY & ADA IMPROVEMENT
DUPAGE COUNTY

C-91-241-25

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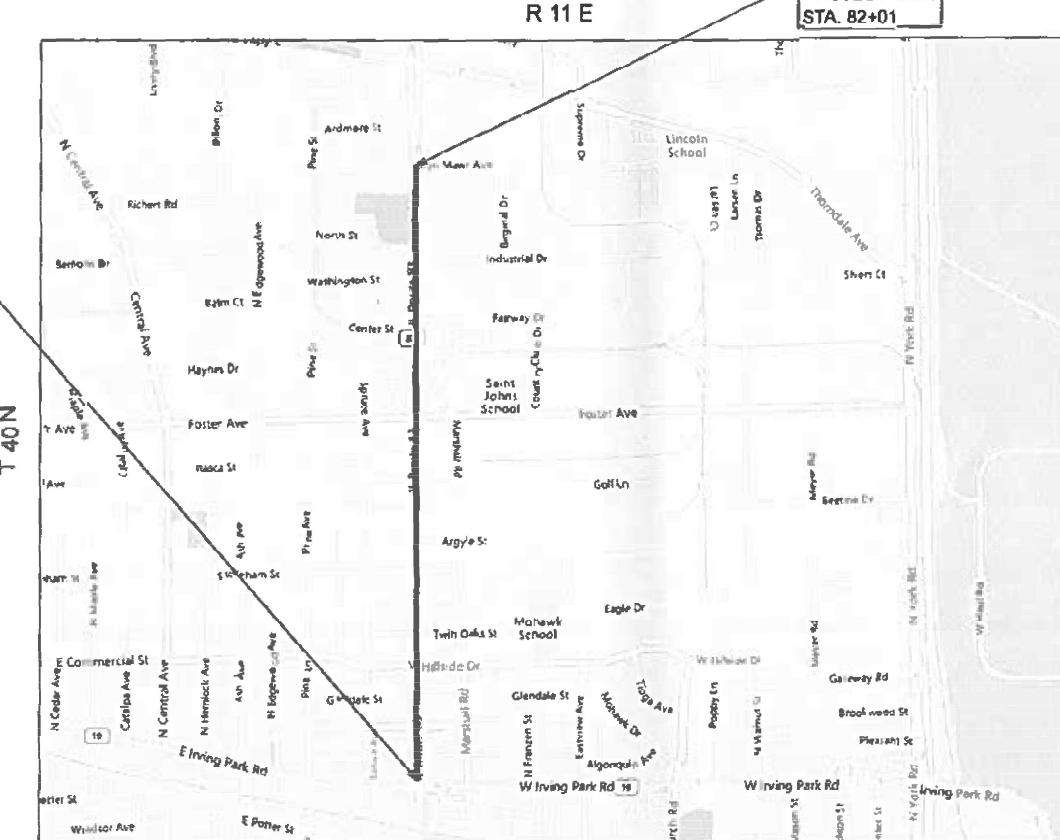
PROJECT END:
STA. 82+01

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240
PROJECT MANAGER: J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 80B08



ADDISON TOWNSHIP

GROSS & NET LENGTH = 6,625 FT. = 1.25 MILE



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED	Dec 2 nd 2025
REGIONAL ENGINEER	James [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT	John [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION	John [Signature]

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OF THE STATE OF ILLINOIS

REV-SEP

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25	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)	
26	ARTERIAL ROAD INFORMATION SIGN (TC-22)	
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28	PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS (PD-01)	

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-09	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-04	CLASS C and D PATCHES
701101-05	OFF-RD OPERATIONS, MULTILANE 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE MORE THAN 15' (4.5 m) AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS \geq 45 MPH TO 55 MPH
701422-10	LANE CLOSURE MULTILANE, FOR SPEEDS \geq 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS \geq 45 MPH
701601-09	URBAN LANE CLOSURE MULTILANE, 1W OR 2W WITH NONTRaversable MEDIAN
701701-10	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-11	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES, ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIAN IN THE FIELD, UNLESS OTHERWISE SHOWN.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT MR. EMAD ALHUSSEINI AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- ALL PAVEMENT MARKINGS ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING DETAIL TC-13.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ALL RAISED REFLECTIVE PAVEMENT MARKERS (RRPM) SHALL BE PLACED THROUGHOUT THE IMPROVEMENT AND INSTALLED ACCORDING TO DISTRICT STANDARDS, AS SHOWN IN THE IDOT D1 RRPM DETAIL TC-11.
- CONTRACTOR MUST NOT ENCROACH, PERFORM ANY CONSTRUCTION ACTIVITIES, OR PARK ANY CONSTRUCTION EQUIPMENT OR VEHICLES IN THE AREA WITHIN 30' FROM THE CENTERLINE OF THE NEAREST RAILROAD TRACK.
- PAVEMENT MARKING, TYPE IV TAPE SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- STORM SEWERS AND CULVERT BOX CLEANING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, VILLAGE OF BENESINVILLE, AND DUPAGE COUNTY.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- TEMPORARY PAVEMENT MARKINGS ON SHORT-TERM PAVEMENT MARKINGS ON INTERMEDIATE SURFACES SHALL NOT BE REMOVED UNLESS DIRECTED BY THE ENGINEER.

SUMMARY OF QUANTITIES				TYPE CODE					
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
				ROADWAY	ROADWAY	SIGNALS			
				80% FED 20% STATE	100% STATE	80% FED 20% STATE			
Code No.	Item	Unit	Total Quantity	0005	0044	0021			
20200100	EARTH EXCAVATION	CU YD	10	10					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	22	22					
25200110	SODDING, SALT TOLERANT	SQ YD	22	22					
25200200	SUPPLEMENTAL WATERING	UNIT	1	1					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	33063	33063					
40600370	LONGITUDINAL JOINT SEALANT	FOOT	29120	29120					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	111	111					
40600405	MATERIAL TRANSFER DEVICE	TON	6950	6950					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	330	330					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	321	321					
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	1279	1279					
40605036	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, MIX "F", N80	TON	6950	6950					
42001300	PROTECTIVE COAT	SQ YD	165	165					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	280	280					
42400800	DETECTABLE WARNINGS	SQ FT	80	80					
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	73472	73472					
44000600	SIDEWALK REMOVAL	SQ FT	294	294					
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	3146	3146					
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	184	184					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	654	654					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	1103	1103					
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	919	919					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	981	981					

MODEL: SOO01 [Sheet] FILE NAME: ctbw.worldwideillinois.gov_baraa.alsmadi@illinois.gov

SUMMARY OF QUANTITIES						TYPE CODE					
						URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
						ROADWAY	ROADWAY	SIGNALS			
						80% FED 20% STATE	100% STATE	80% FED 20% STATE			
Code No.	Item	Unit	Total Quantity	0005	0044	0021					
60257900	MANHOLES TO BE RECONSTRUCTED						EACH	1	1		
60300305	FRAMES AND LIDS TO BE ADJUSTED						EACH	3	3		
Δ 60905305	BOX CULVERTS TO BE CLEANED						FOOT	125		125	
* 66900200	NON-SPECIAL WASTE DISPOSAL						CU YD	10	10		
* 66900530	SOIL DISPOSAL ANALYSIS						EACH	2	2		
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN						L SUM	1	1		
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT						L SUM	1	1		
* 66901006	REGULATED SUBSTANCES MONITORING						CAL DA	2	2		
67100100	MOBILIZATION						L SUM	1	1		
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421						L SUM	1	1		
70100320	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422						L SUM	1	1		
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601						L SUM	1	1		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701						L SUM	1	1		
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801						L SUM	1	1		
70200100	NIGHTTIME WORK ZONE LIGHTING						L SUM	1	1		
70300100	SHORT TERM PAVEMENT MARKING						FOOT	13581	13581		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL						SQ FT	2264	2264		
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT						SQ FT	535	535		
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT						FOOT	33671	33671		
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT						FOOT	2942	2942		
70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT						FOOT	793	793		

* SPECIALTY ITEM

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION						F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						334	2025-1096-RS	DUPAGE	28	3
SCALE:	SHEET	OF 2	SHEETS	STA.	TO STA.			ILLINOIS	FED. AID PROJECT	

USER NAME = Baraa.alsmadi	DESIGNED -	REVISED -	
DRAWN -		REVISED -	
PLOT SCALE = 0.1666633' / in.	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

SUMMARY OF QUANTITIES						TYPE CODE					
						URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
						ROADWAY	ROADWAY	SIGNALS			
						80% FED 20% STATE	100% STATE	80% FED 20% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0044	0021				
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"-PAINT	FOOT	2046	2046						
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"-PAINT	FOOT	459	459						
	70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	6791	6791						
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	535	535						
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	27047	27047						
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2942	2942						
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	793	793						
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2046	2046						
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	459	459						
*	78004620	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 4"	FOOT	6624	6624						
*	78011025	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	6624	6624						
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	669	669						
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	699	699						
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	193		193					
	81400200	HEAVY-DUTY HANHOLE	EACH	2		2					
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2		2					
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	843		843					
*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	351		351					
*	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	4040		4040					
*	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	111		111					
*	87900100	DRILL EXISTING FOUNDATION	EACH	3		3					
	87900205	DRILL EXISTING HEAVY DUTY HANHOLE	EACH	3		3					
*	88500100	INDUCTIVE LOOP DETECTOR	EACH	8		8					
*	88600100	DETECTOR LOOP, TYPE I	FOOT	2195		2195					

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						URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
						ROADWAY	ROADWAY	SIGNALS			
						80% FED 20% STATE	100% STATE	80% FED 20% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0044	0021				
*	89500200	RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	1							
	89502300	REMOVE EXISTING ELECTRIC CABLE FROM CONDUIT	FOOT	1267			1267				
*	89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	21			21				
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2			2				
	89502380	REMOVE EXISTING HANHOLE	EACH	2			2				
	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	1			1				
*	X1400367	PEDESTRIAN SIGNAL POST, 10 FT.	EACH	1			1				
*	X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	3			3				
	X420430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)	SQ FT	13	13						
	X2020110	GRADING AND SHAPING SHOULDERS	UNIT	10	10						
	X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	100	100						
	X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	350	350						
Δ	X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	341		341					
Δ	X5538600	STORM SEWERS TO BE CLEANED 36"	FOOT	585		585					
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13	13						
	X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12						
	X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	77	77						
*	X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	6		6					
	X8780012	CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	16		16					
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1						
Δ	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	15		15					
*	Z003044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	2		2					

* SPECIALTY ITEM

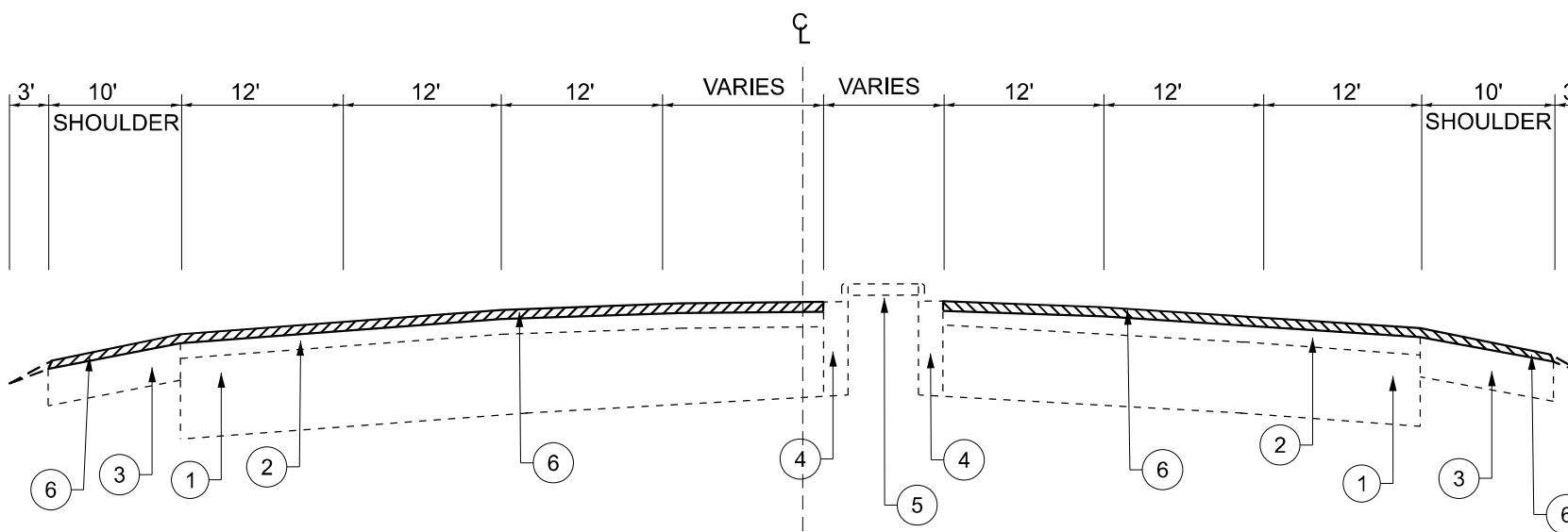
SUMMARY OF QUANTITIES						FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S OF BRYN MAWR AVE TO IL-19 (IRVING PARK RD)						334	2025-1096-RS	DUPAGE	28	4

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DRAWN -	REVISED -		
PLOT SCALE = 0.1666633' / in.	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

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LEGEND



IL ROUTE 83
STA. 15+76 TO STA. 59+48

1. EXISTING P.C.C. BASE, $\pm 9"$
2. EXISTING HOT-MIX ASPHALT, $\pm 3"$
3. EXISTING HOT-MIX ASPHALT SHOULDER, $\pm 10"$
4. EXISTING CURB AND GUTTER TYPE B-6.12 OR B-6.24
5. EXISTING LANDSCAPE OR CONCRETE MEDIAN SURFACES 5"
6. PROPOSED HMA SURFACE REMOVAL 2"
7. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 12.5, MIX "F", N80, 2"
8. PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"
9. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
10. PROPOSED GRADING AND SHAPING SHOULDERS

NOTE:

1. CONTRACTOR SHALL PATCH BEFORE MILLING
2. LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE

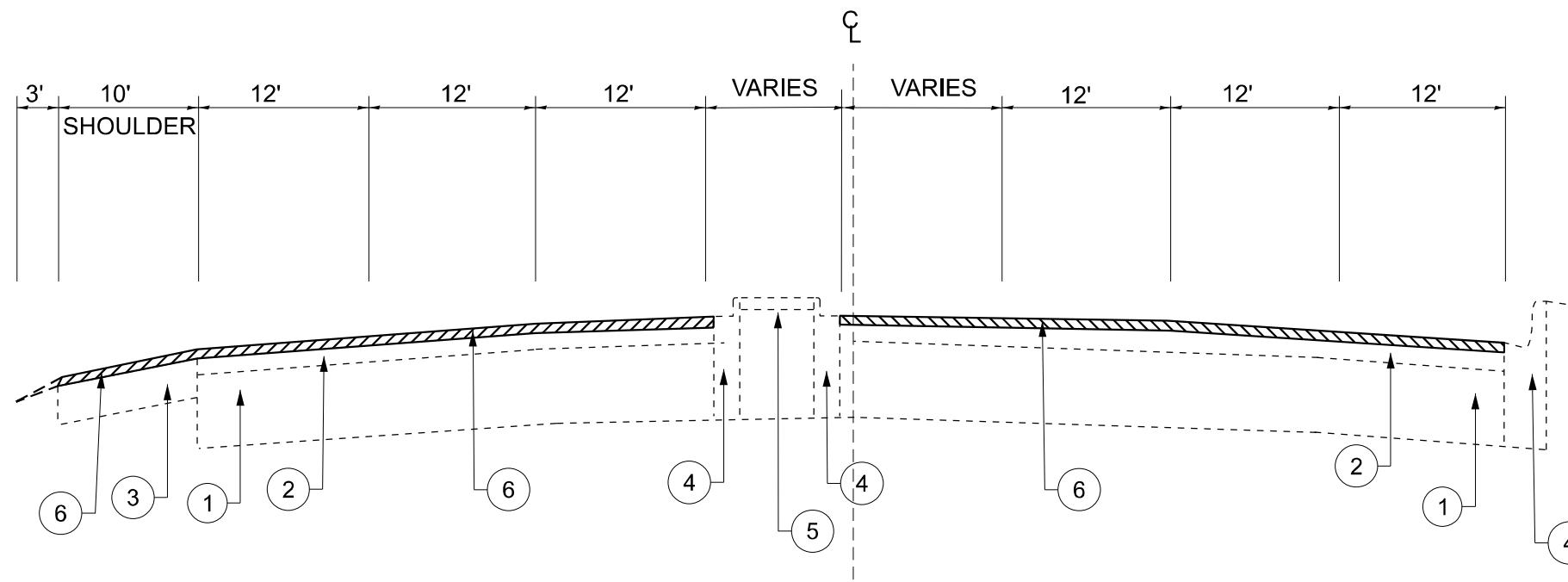
HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QMP	MTD (YES/NO)
MIXTURE TYPE	AIR VOIDS @ Ndesign		
PAVEMENT RESURFACING			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, MIX "F", N80, 2"	3.5% @ 80 Gyr.	QCP	YES
HMA SHOULDER RESURFACING			
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	4% @ 70 Gyr.	QC/QA	NO
PATCHING			
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 90 Gyr.	QC/QA	NO
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 90 Gyr.	QC/QA	NO
QMP Designations: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)			

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS

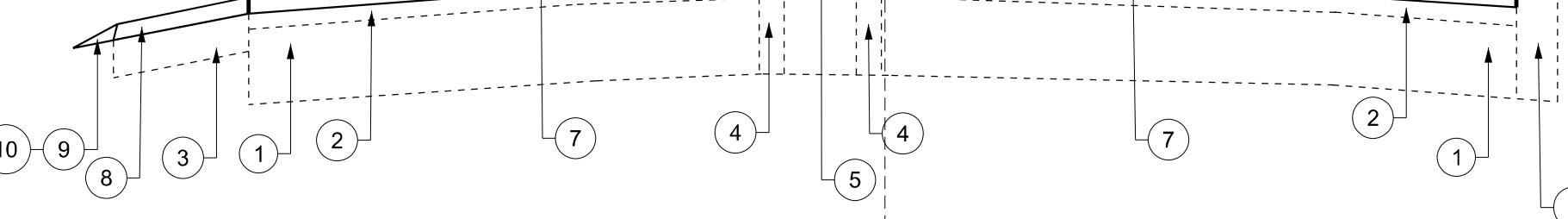
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	DRAWN	-	REVISED	-				334	2025-1096-RS	DUPAGE	28	5					
	CHECKED	-	REVISED	-										CONTRACT NO. 80B08			
	PLOT DATE	= 12/5/2025	DATE	-	REVISED	-		SCALE:	SHEET 1	OF 2 SHEETS	STA.	TO STA.			ILLINOIS	FED. AID PROJECT	



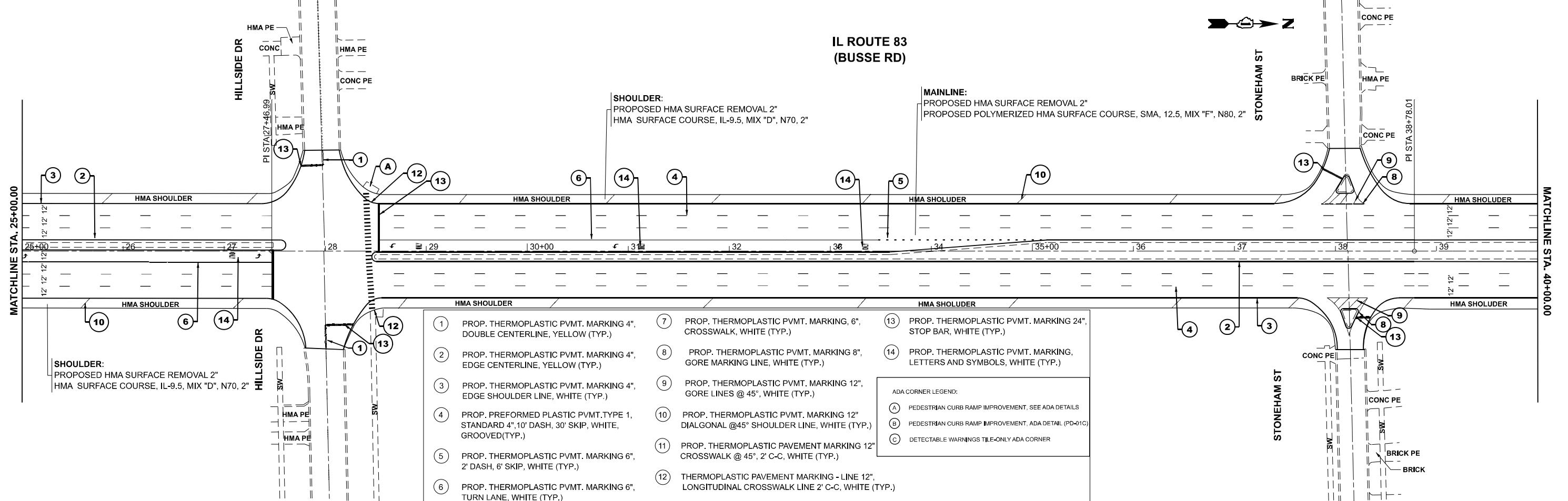
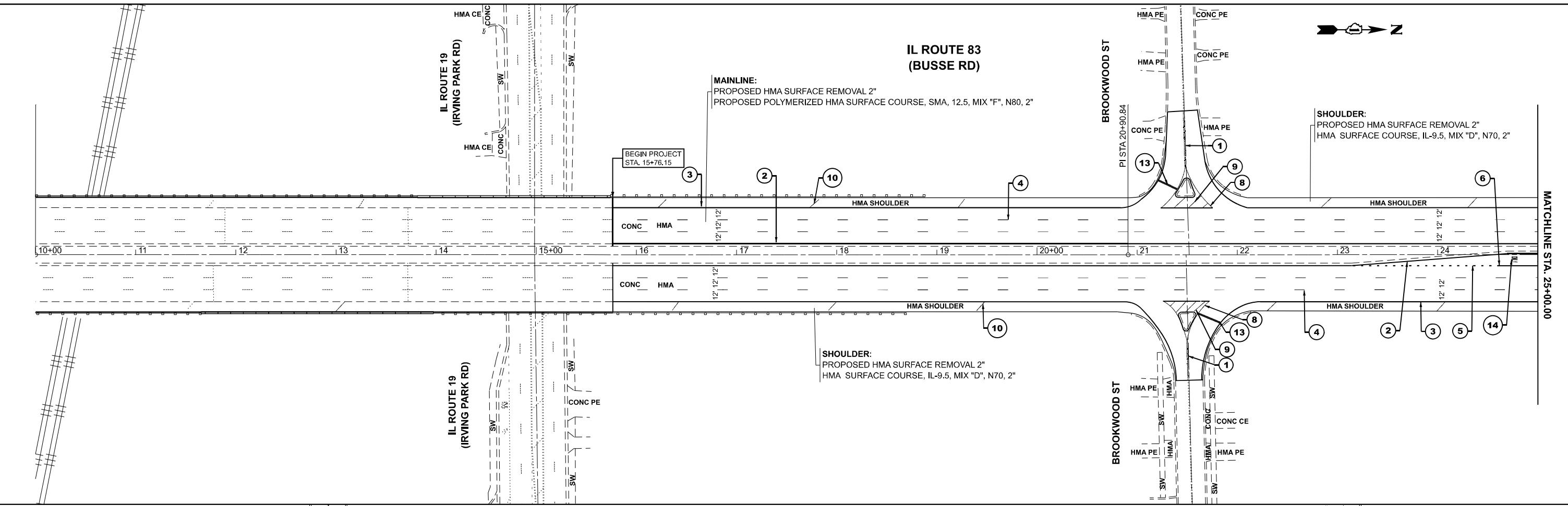
IL ROUTE 83
STA. 59+48 TO STA. 82+01

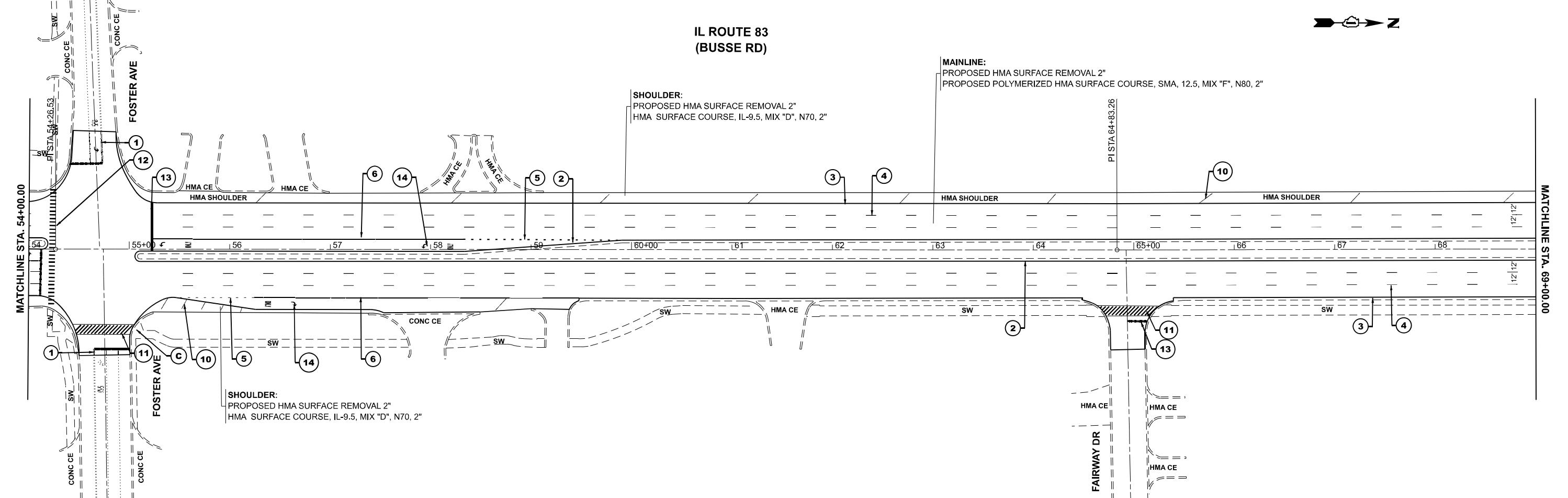
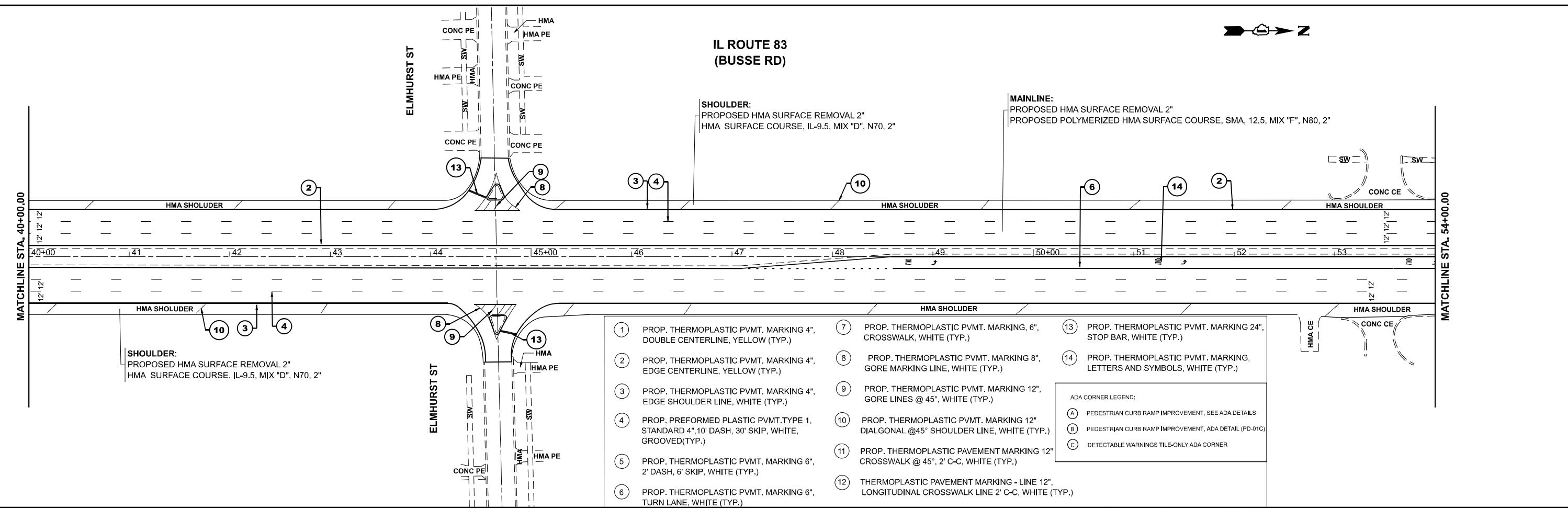
EXISTING TYPICAL SECTION

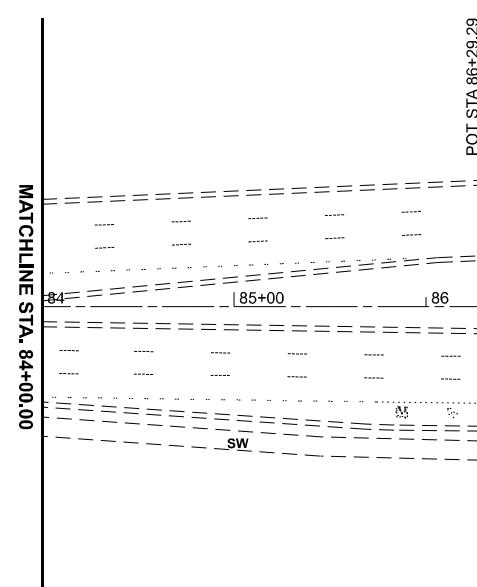
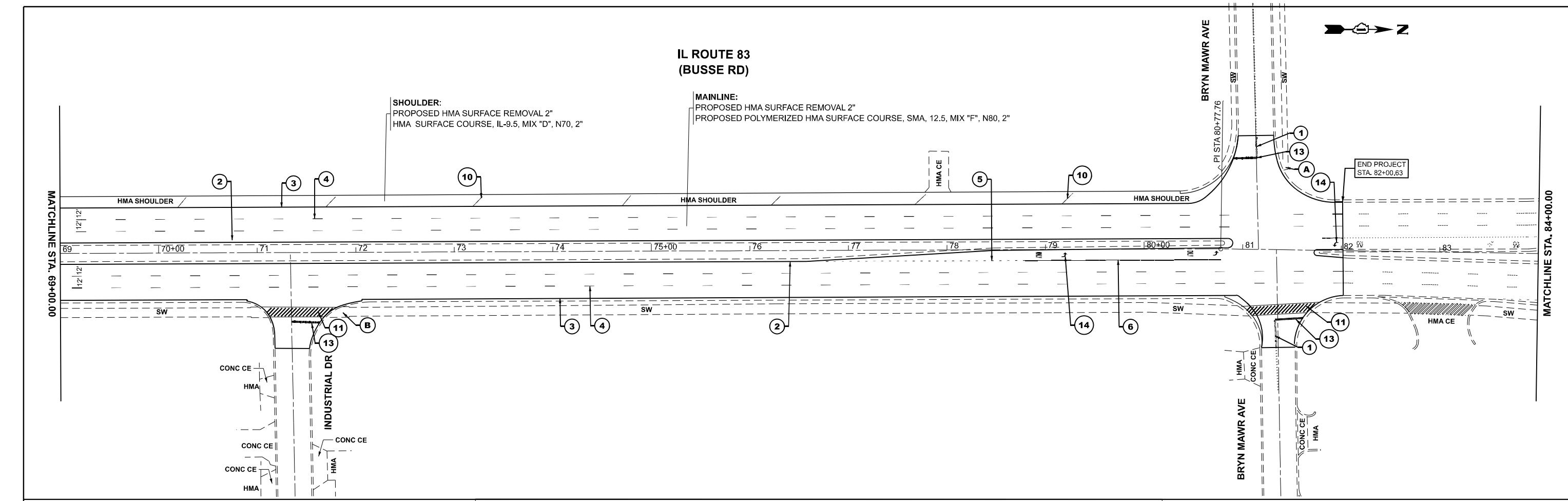


IL ROUTE 83
STA. 59+48 TO STA. 82+01

PROPOSED TYPICAL SECTION







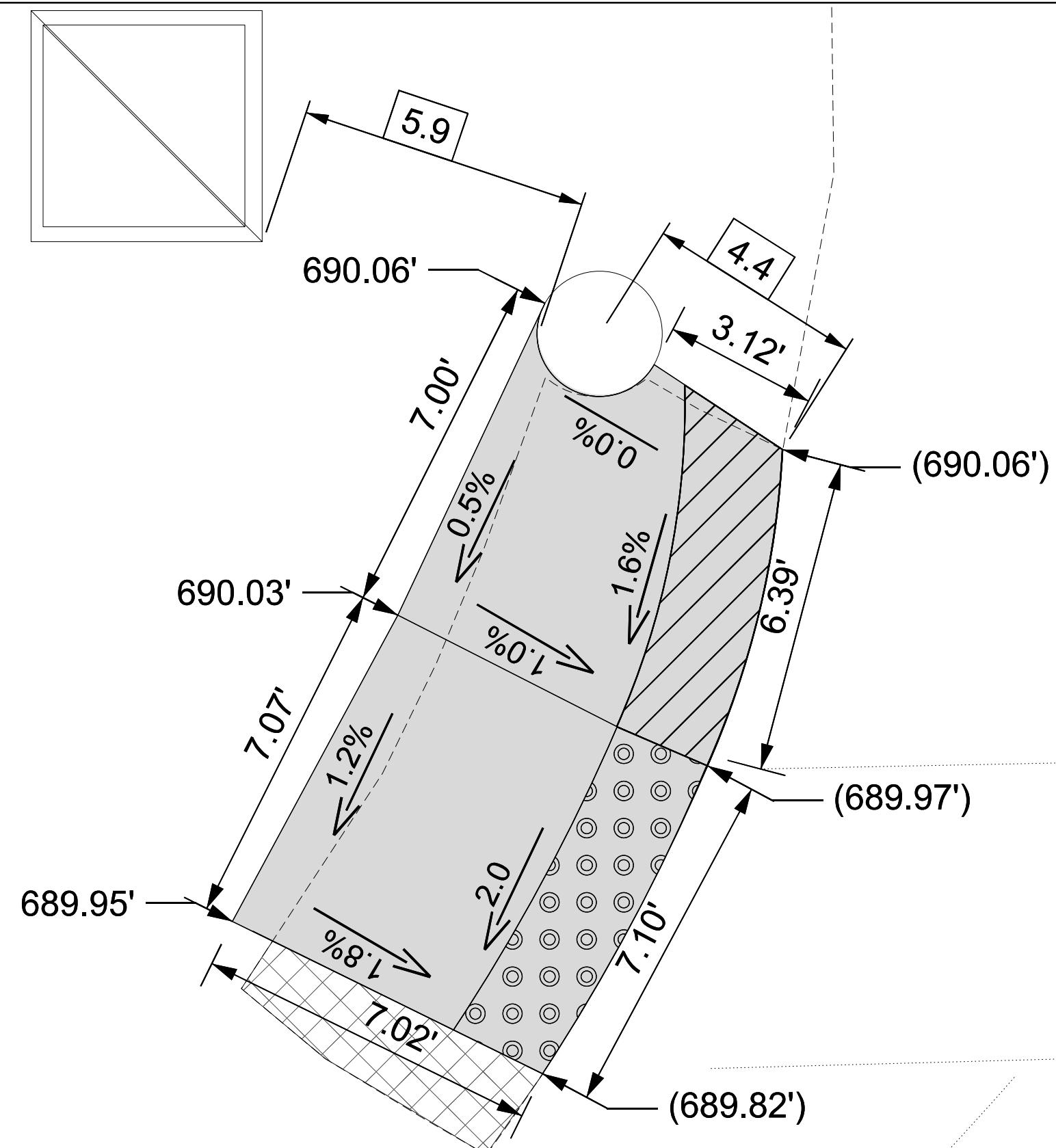
①	PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE CENTERLINE, YELLOW (TYP.)	⑦	PROP. THERMOPLASTIC PVMT. MARKING, 6", CROSSWALK, WHITE (TYP.)	⑬	PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
②	PROP. THERMOPLASTIC PVMT. MARKING 4", EDGE CENTERLINE, YELLOW (TYP.)	⑧	PROP. THERMOPLASTIC PVMT. MARKING 8", GORE MARKING LINE, WHITE (TYP.)	⑭	PROP. THERMOPLASTIC PVMT. MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)
③	PROP. THERMOPLASTIC PVMT. MARKING 4", EDGE SHOULDER LINE, WHITE (TYP.)	⑨	PROP. THERMOPLASTIC PVMT. MARKING 12", GORE LINES @ 45°, WHITE (TYP.)	ADA CORNER LEGEND: (A) PEDESTRIAN CURB RAMP IMPROVEMENT, SEE ADA DETAILS (B) PEDESTRIAN CURB RAMP IMPROVEMENT, ADA DETAIL (PD-01C) (C) DETECTABLE WARNINGS TILE-ONLY ADA CORNER	
④	PROP. PREFORMED PLASTIC PVMT. TYPE 1, STANDARD 4", 10' DASH, 30' SKIP, WHITE, GROOVED (TYP.)	⑩	PROP. THERMOPLASTIC PVMT. MARKING 12" DIALOGNAL @45° SHOULDER LINE, WHITE (TYP.)		
⑤	PROP. THERMOPLASTIC PVMT. MARKING 6", 2' DASH, 6' SKIP, WHITE (TYP.)	⑪	PROP. THERMOPLASTIC PAVEMENT MARKING 12" CROSSWALK @ 45°, 2' C-C, WHITE (TYP.)		
⑥	PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)	⑫	THERMOPLASTIC PAVEMENT MARKING - LINE 12", LONGITUDINAL CROSSWALK LINE 2' C-C, WHITE (TYP.)		

ADA CORNER LEGEND:

- (A) PEDESTRIAN CURB RAMP IMPROVEMENT, SEE ADA DETAILS
- (B) PEDESTRIAN CURB RAMP IMPROVEMENT, ADA DETAIL (PD-01C)
- (C) DETECTABLE WARNING TILE ONLY ADA CORNER

IL ROUTE 83 (BUSSE RD.)

HILLSIDE DR.



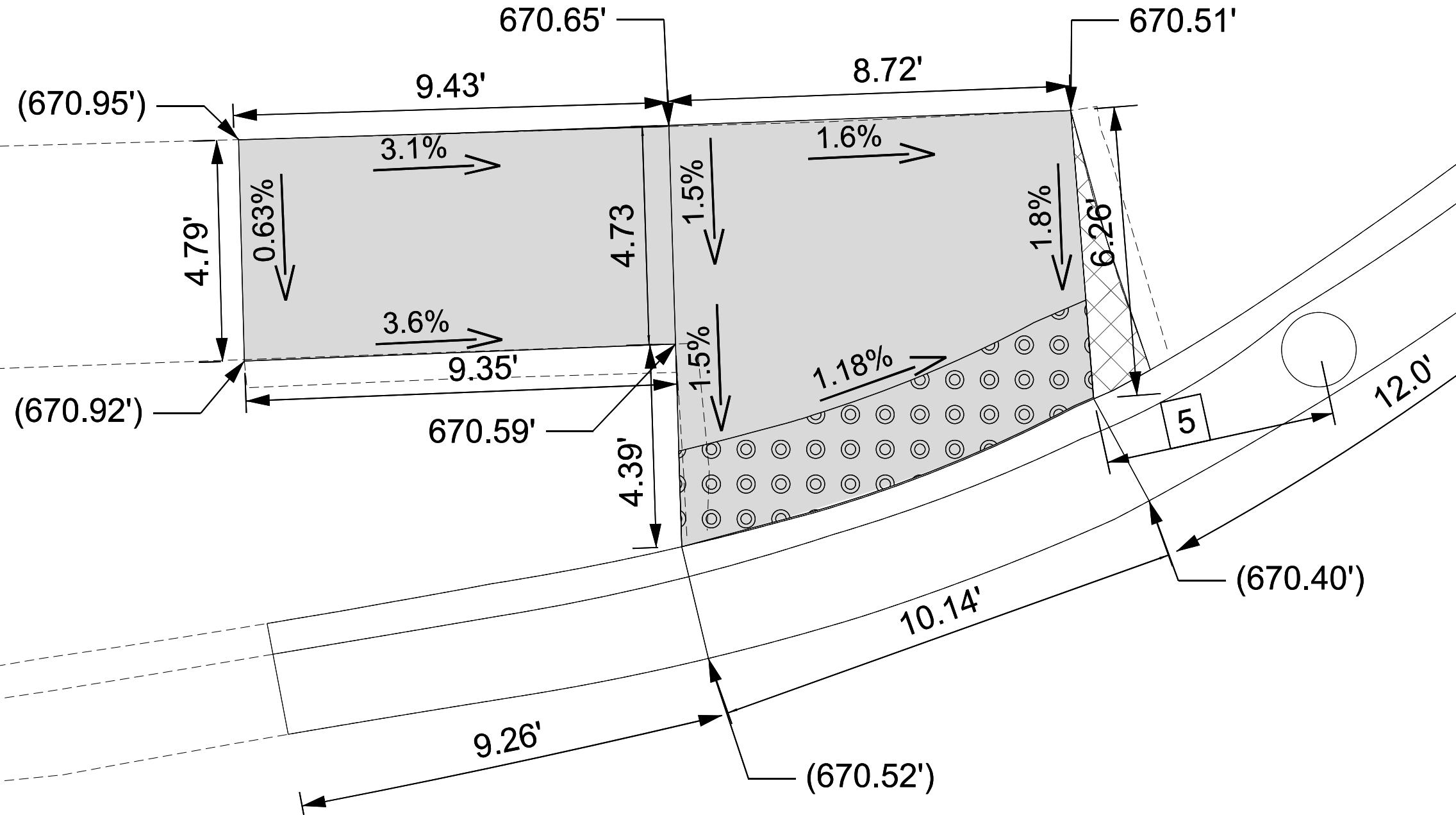
LEGEND

XX.XX ¹	EXISTING LENGTH		DETECTABLE WARNINGS
=====	PROPOSED SIDE CURB		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
()	EXISTING ELEVATION/SLOPE		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENT DETAILS
NW CORNER AT (HILLSIDE DR. & IL-83 (BUSSE RD.))**

ADA IMPROVEMENT DETAILS NW CORNER AT (HILLSIDE DR. & IL-83 (BUSSE RD.))					F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
					334	2025-1096-RS	DUPAGE	28	10
					CONTRACT NO. 80B08				
SCALE:	SHEET 1	OF 5	SHEETS	STA.	TO STA.		ILLINOIS	FED, AID PROJECT	



BRYN MAWR AVE.

MODEL: Bryn Mawr Ave [Sheet]

LEGEND

xx.xx¹

PROPOSED SIDE CUR

() EXISTING ELEVATION/SLC

PROPOSED SIDEWALK



DETECTABLE WARNINGS

SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA IMPROVEMENT DETAILS

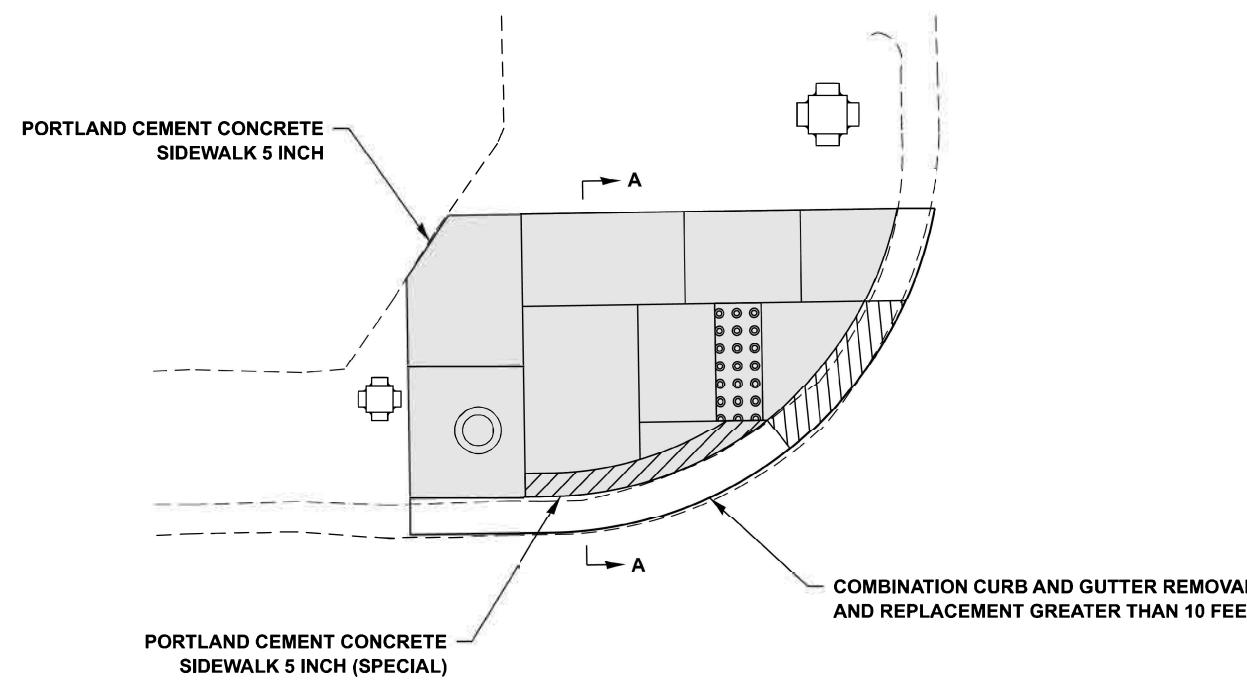
NW CORNER AT (BRYN MAWR AVE & IL-83 (BUSSE RD.))

	USER NAME = Baraa.alsm

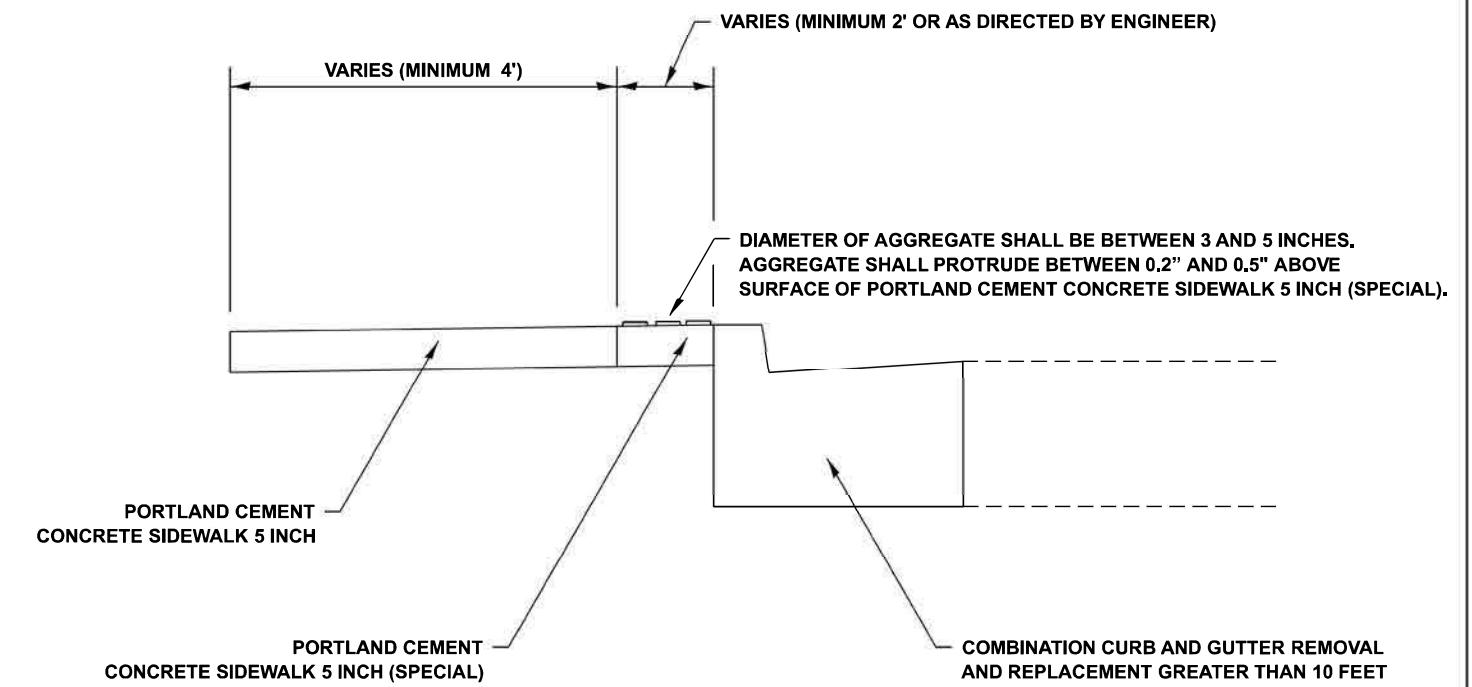
DESIGNED -		REVISED
DRAWN -		REVISED
CHECKED -		REVISED
DATE -		REVISED

ADA IMPROVEMENT DETAILS
NW CORNER AT (BRYN MAWR AVE & IL-83 (BUSSE RD.))

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	11
CONTRACT NO. 80B08				
ILLINOIS FED. AID PROJECT				



TYPICAL PLAN FOR PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)



SECTION A-A

USER NAME = Baraa.Alsmadi	DESIGNED -	REVISED -
DRAWN -	REVISED -	
CHECKED -	REVISED -	
PLOT DATE = 1/6/2026	DATE -	REVISED -

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	HEET NO.
334	2025-1096-RS	DUPAGE	28	11A
				CONTRACT NO. 80B08

REMOVAL NOTE:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT UNIT BID PRICE.

1 EACH TRAFFIC SIGNAL POST

0 20 40
SCALE IN FEET

EX INTERCONNECT TO HILLSIDE DR

EX. 55' MA - 36" DIA. FDN. (15' DEPTH)

DETAIL A (SHEET 2 OF 2)

EX. 16' TS POST

4'-E-2½"

25'-E-3"

FOSTER AVE

EX. 36' MA - 30" DIA. FDN. (13.5' DEPTH)

23'-E-3"

5'-E-(4)4"

260'-E-2"

IL RTE 83 (BUSSE RD)

90'-E-2"

EX. 28' MA - 30" DIA. FDN. (10' DEPTH)

100'-E-4"

16'-E-3'

100'-E-4"

77'-E-2"

IL RTE 83 (BUSSE RD)

MATCHLINE A-A

NOTES:

1. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.
2. ALL PROPOSED TRAFFIC SIGNAL CABLE SHALL BE ROUTED THROUGH THE PROPOSED CONDUIT.
3. A SEPARATE LEAD-IN CABLE SHALL BE INSTALLED FOR THE DETECTOR LOOPS ON THE NB AND SB IL RTE 83 (BUSSE RD) INSIDE THROUGH LANES.
4. ALL PROPOSED PUSH-BUTTONS SHALL BE APS.
5. THIS LOCATION HAS BEEN DESIGNED TO BE PROWAG/MUTCD COMPLIANT. ANY DEVIATION FROM THIS PLAN FOR THE TRAFFIC SIGNAL POSTS THAT HAVE PEDESTRIAN EQUIPMENT WILL HAVE TO BE APPROVED BY THE ENGINEER TO ENSURE COMPLIANCE.
6. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER.
7. DIMENSIONS SHOWN FOR THE APS PLACEMENT ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE, PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK.
8. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
9. THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.
10. EACH DETECTOR SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
11. PULL BACK EXISTING PUSHBUTTON CABLE TO THE EXISTING DOUBLE HANDHOLE ON THE NORTHEAST QUADRANT AND THEN ROUTE THROUGH PROPOSED CONDUIT TO UTILIZE FOR PROPOSED APS PUSHBUTTON. THIS SHALL NOT BE DONE UNTIL PROPOSED CONDUIT AND PEDESTRIAN POST ARE INSTALLED TO MINIMIZE DOWN TIME.

EX. 26' MA - 30" DIA. FDN. (10' DEPTH)
29'-E-3"

DETAIL B (SHEET 2 OF 2)

A

EX. 30' MA - 30" DIA. FDN. (13.5' DEPTH)
23'-E-3"

FOSTER AVE

EX. 68' MA - 42" DIA. FDN. (25' DEPTH)
8'-E-3"

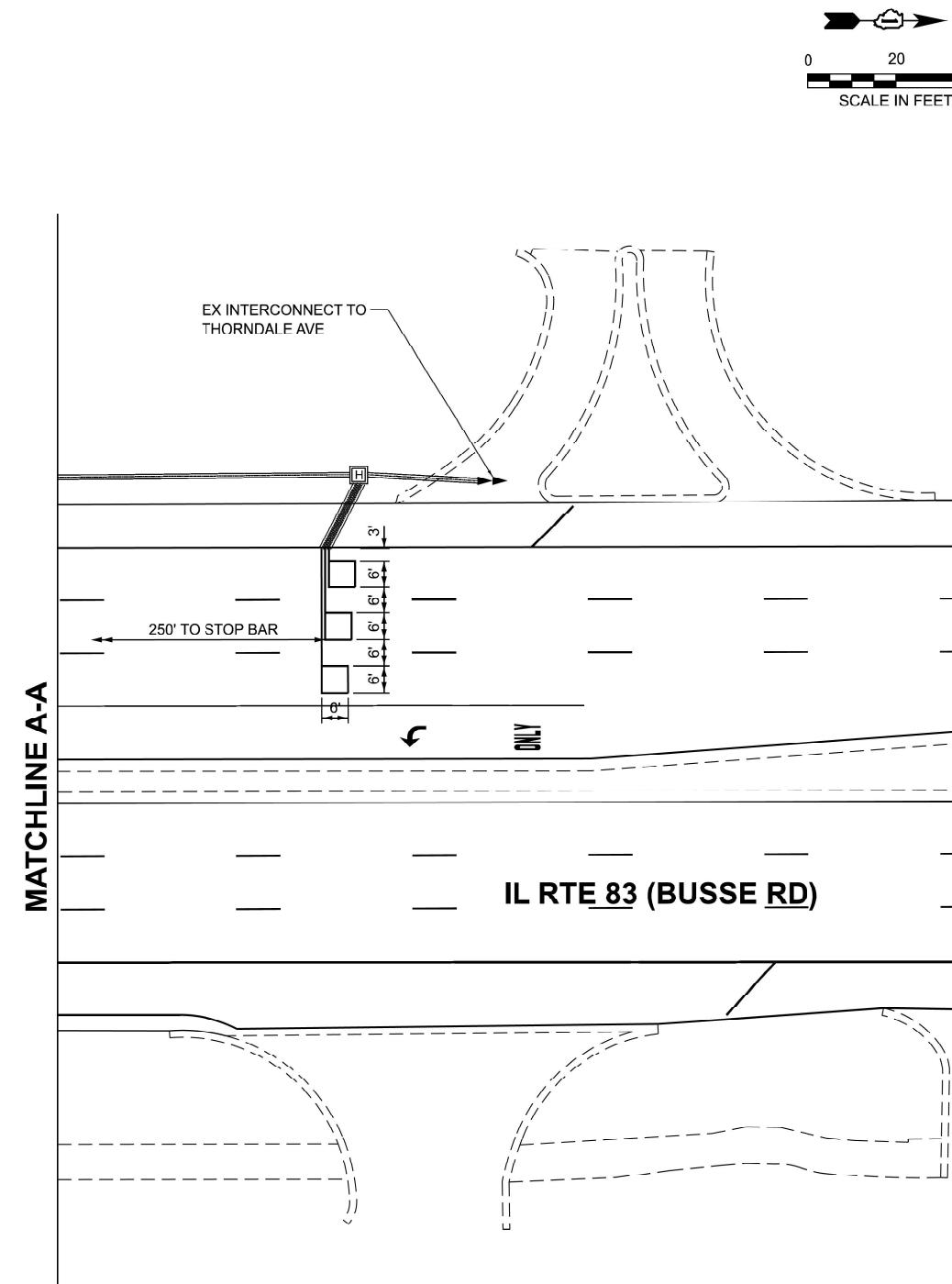
SEE NOTE 11

DETAIL C (SHEET 2 OF 2)

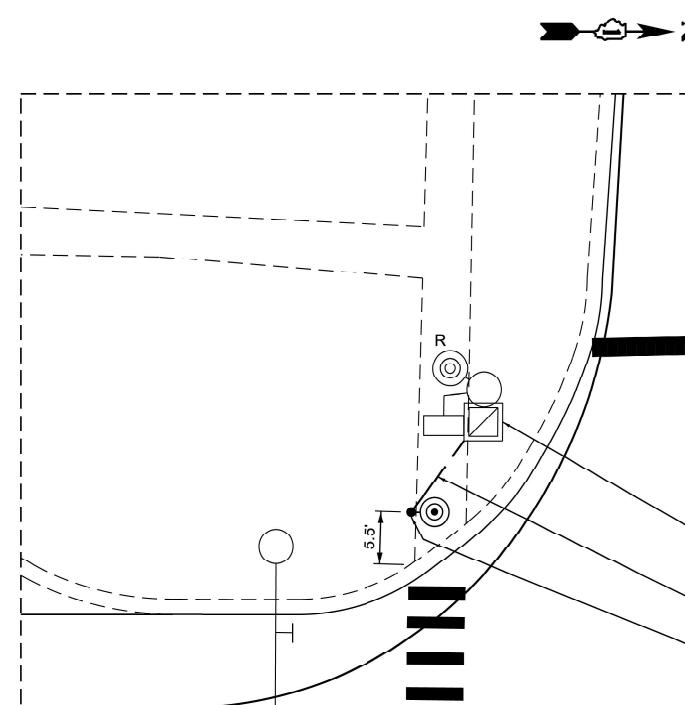
21'-E-2½"

EX. 16' TS POST
71'-E-2"

TS 590
ECON 111

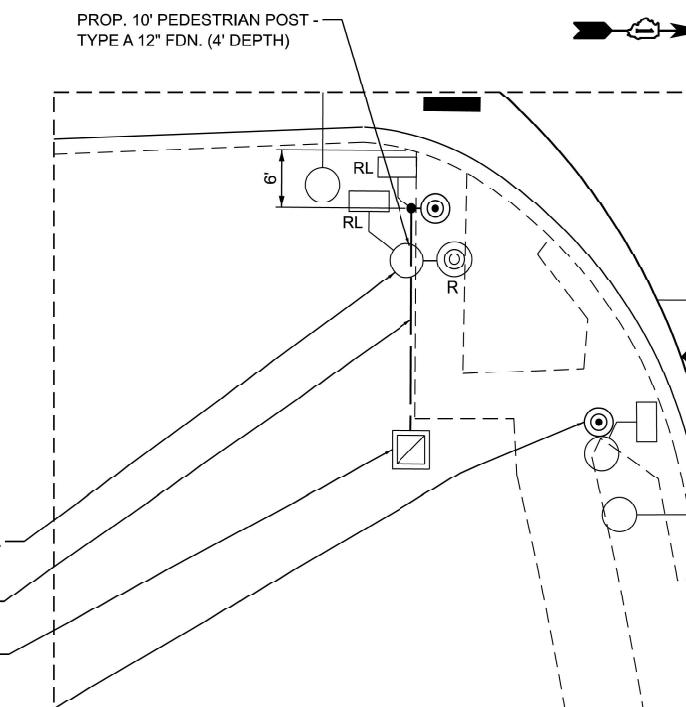


SCALE IN FEET



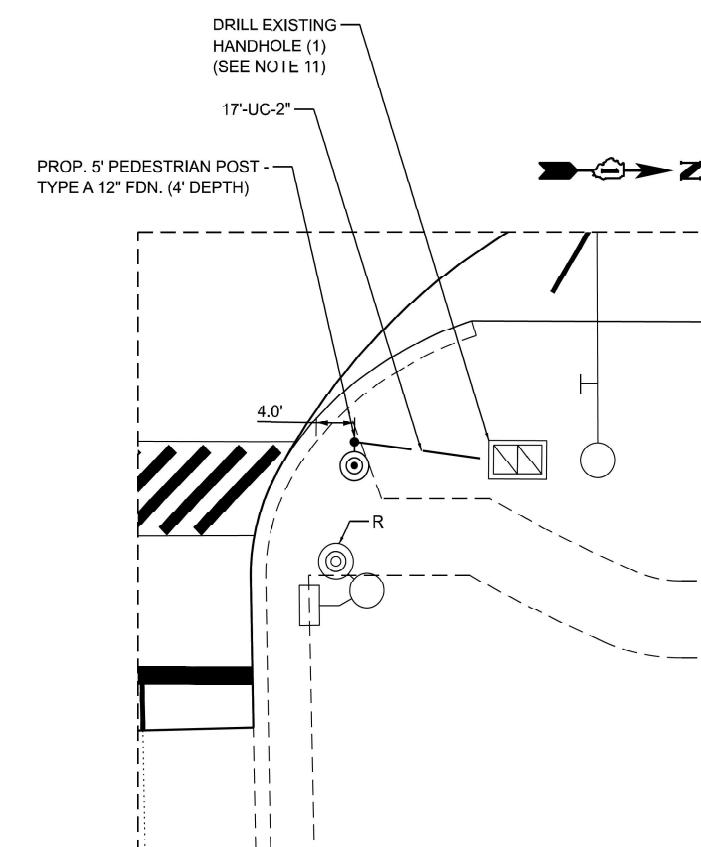
DETAIL A - SOUTHWEST QUADRANT

N.T.S



DETAIL B - SOUTHEAST QUADRANT

N.T.S

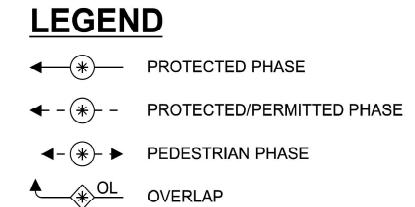
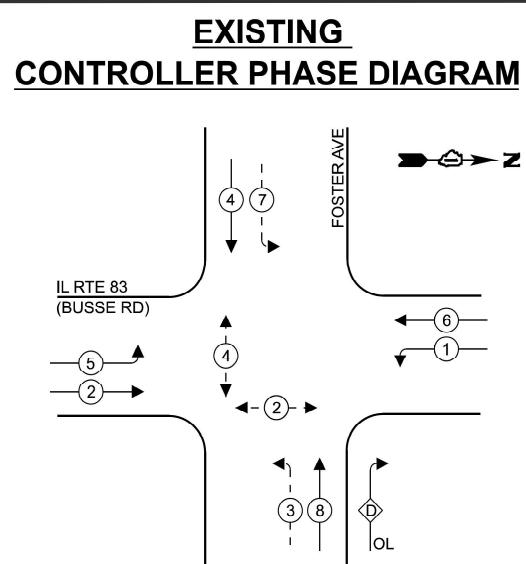


DETAIL C - NORTHEAST QUADRANT

N.T.S

TS 590
ECON 111

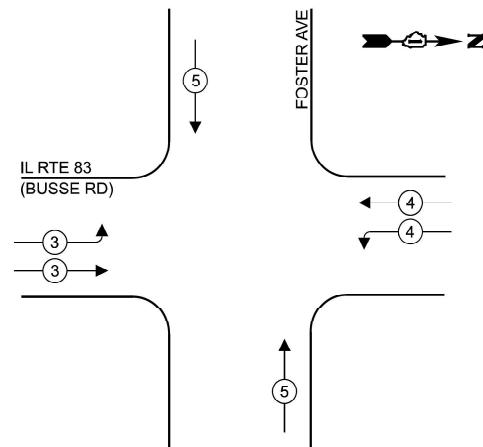
USER NAME = Baraa.Alsmadi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODERNIZATION PLAN				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISED -			IL RTE 83 (BUSSE RD) AND FOSTER AVE - SHEET 2 OF 2				334	2025-1096-RS	DUPAGE	28	13
CHECKED -	REVISED -											CONTRACT NO. 80B08
PLOT DATE = 1/20/2026	DATE -	REVISED -		SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.								
										ILLINOIS	FED. AID PROJECT	



RIGHT TURN OVERLAP PHASE DESIGNATION

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTIVE PHASE		
D	=	8	+	1

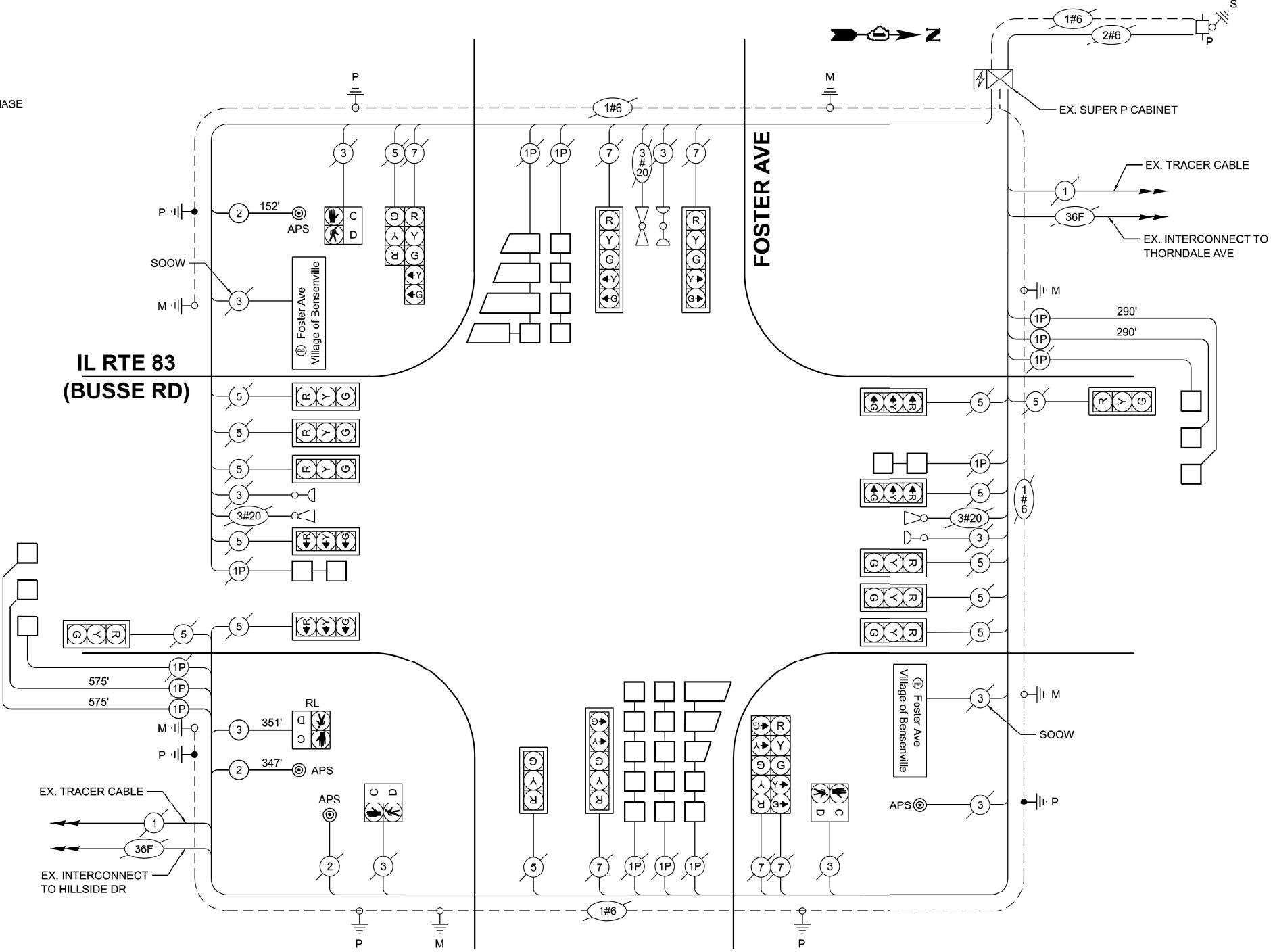
EXISTING EMERGENCY VEHICLE PREEMPTION PHASE DIAGRAM



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE		QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
NAL HEAD	1 OR 3-SECTION	14	11	154
	4-SECTION	-	14	-
	5-SECTION	6	13	78
PROGRAMMABLE	3-SECTION	-	22	-
	4-SECTION	-	32	-
	5-SECTION	-	28	-
DESTRIAN SIGNAL		4	15	60
NTROLLER		1	150	150
STER CONTROLLER		-	100	-
S		1	25	25
TECTION	RADAR OR VIDEO	-	20	-
ANK-OUT SIGN		-	25	-
WORK SWITCH	II OR III	-	35	-
LLULAR MODEM		-	15	-
TOTAL UPS SIZING				467
S CHARGING		1	225	225
TERY HEATER MAT		1	180	180
BINET HEATER		1	200	200
ASHER		-	15	-
STREET NAME SIGN		2	120	240
MINAIRE		-	240	-
TOTAL SERVICE WIRE SIZING				1312

ENERGY COSTS TO:
ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER CT,
SCHAUMBURG, IL 60196
ENERGY SUPPLY:
CONTACT: ANTONIO RIOS
PHONE: (630)-696-6855
COMPANY: COMED GLENBARD REGION
ACCOUNT NUMBER: XXXXXXXXXX
METER NUMBER: _____



CABLE PLAN

PAY ITEM DESCRIPTION	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	54
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	499
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	351
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1PAIR	FOOT	1730
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	87
DRILL EXISTING HANDHOLE	EACH	3
INDUCTIVE LOOP DETECTOR	EACH	4
DETECTOR LOOP, TYPE I	FOOT	1397
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	1
REMOVE EXISTING ELECTRIC CABLE FROM CONDUIT	FOOT	695
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	21
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	2
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	12
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

REMOVAL NOTE:

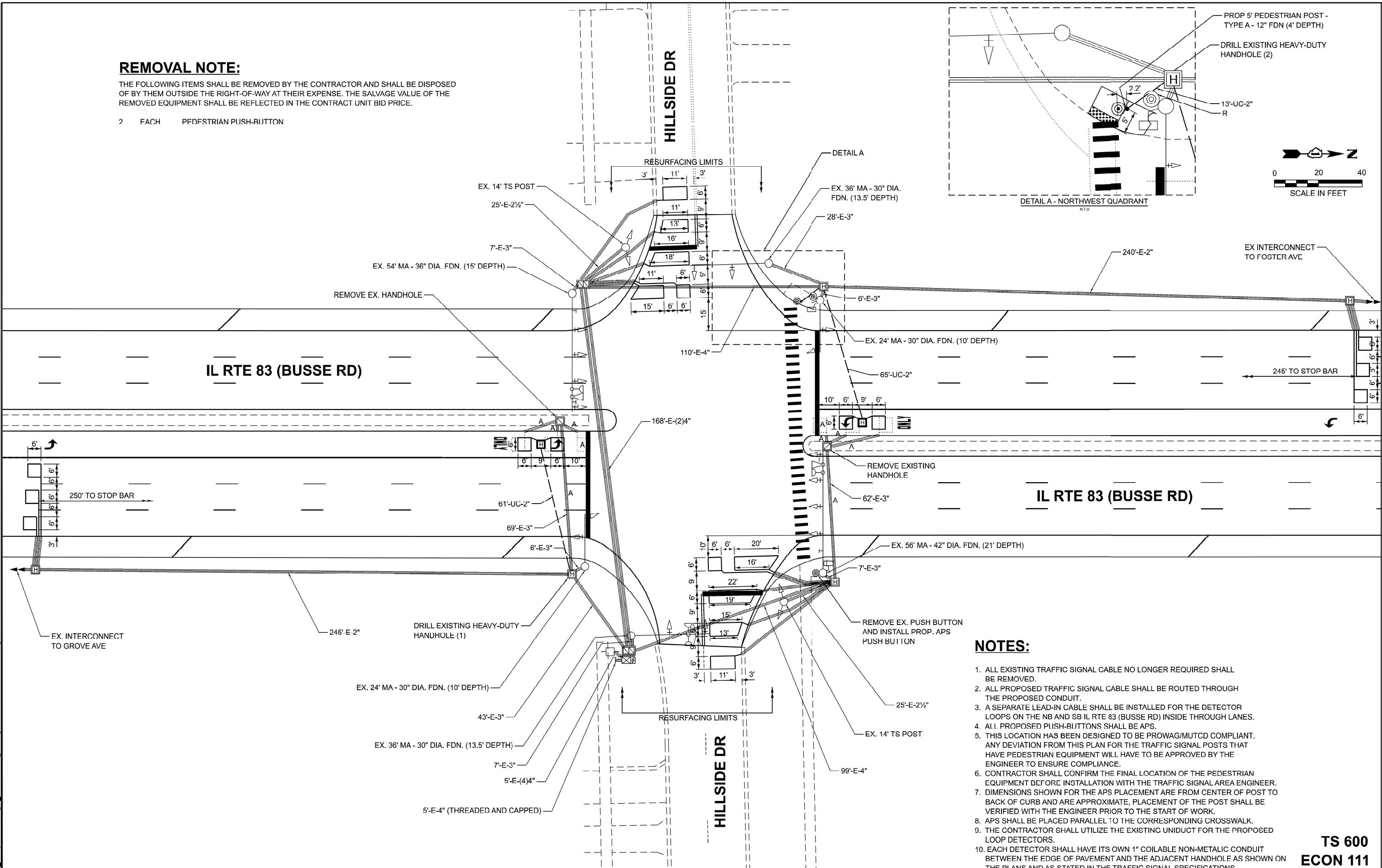
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT UNIT BID PRICE.

2 EACH PEDESTRIAN PUSH-BUTTON

IL RTE 83 (BUSSE RD)

IL RTE 83 (BUSSE RD)

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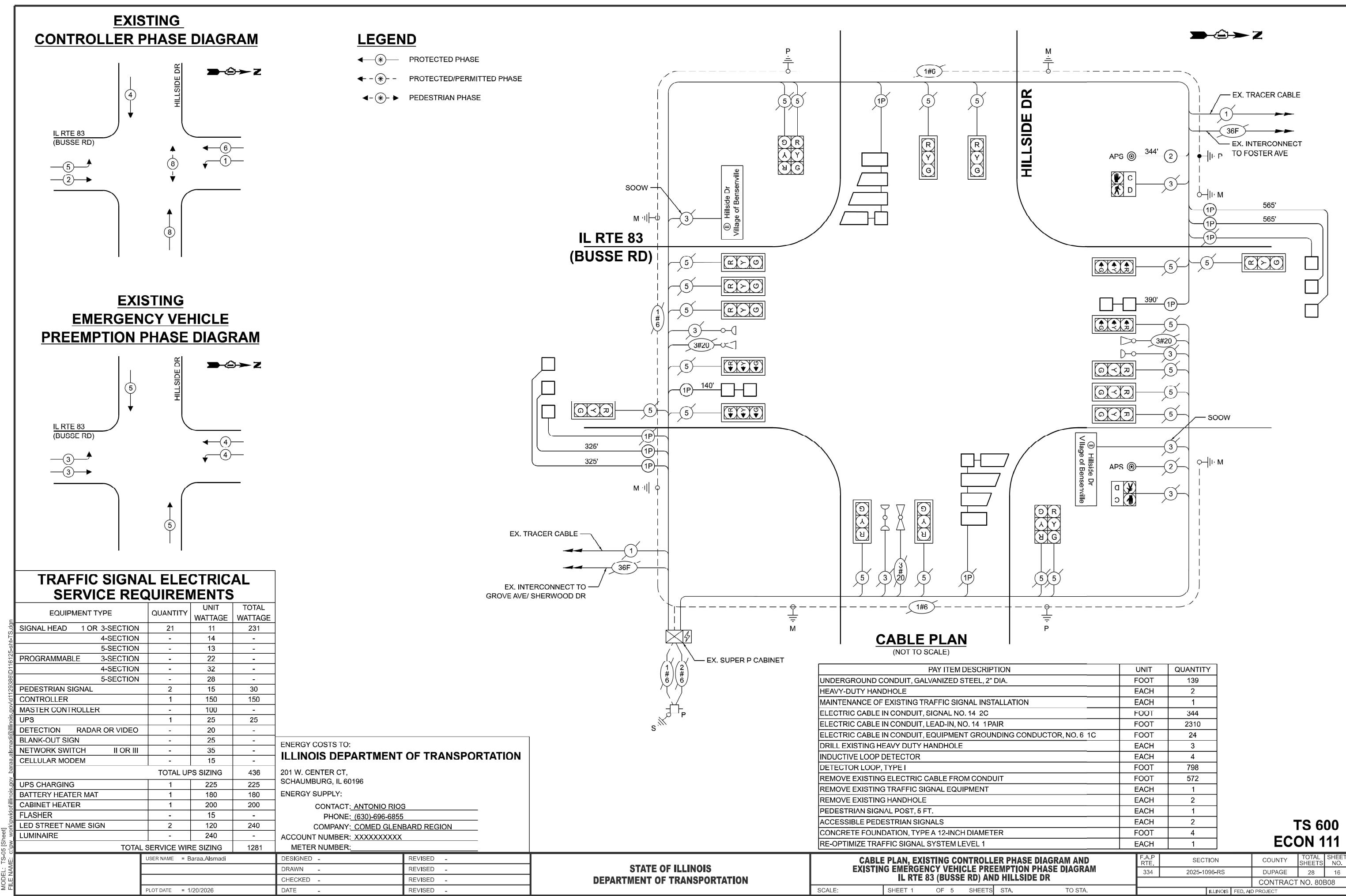


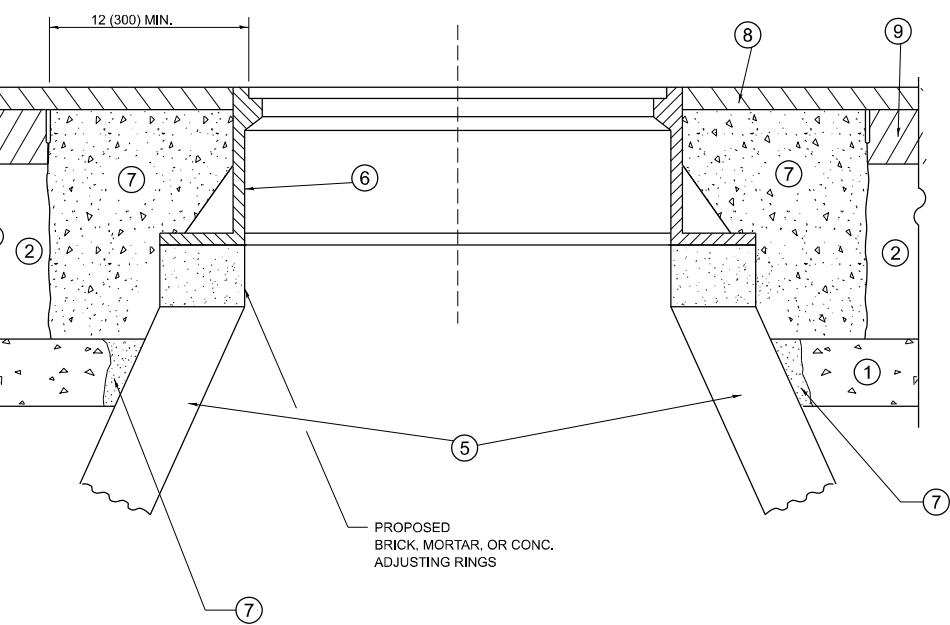
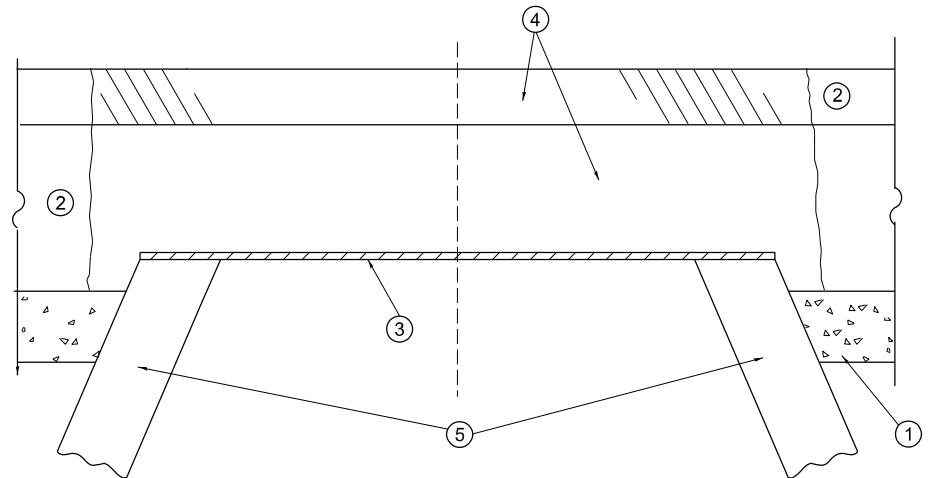
NOTES:

1. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.
2. ALL PROPOSED TRAFFIC SIGNAL CABLE SHALL BE ROUTED THROUGH THE PROPOSED CONDUIT.
3. A SEPARATE LEAD-IN CABLE SHALL BE INSTALLED FOR THE DETECTOR LOOPS ON THE NB AND SB IL RTE 83 (BUSSE RD) INSIDE THROUGH LANES.
4. ALL PROPOSED PUSH-BUTTONS SHALL BE APS.
5. THIS LOCATION HAS BEEN DESIGNED TO BE PROWAG/MUTCD COMPLIANT. ANY DEVIATION FROM THIS PLAN FOR THE TRAFFIC SIGNAL POSTS THAT HAVE PEDESTRIAN EQUIPMENT WILL HAVE TO BE APPROVED BY THE ENGINEER TO ENSURE COMPLIANCE.
6. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER.
7. DIMENSIONS SHOWN FOR THE APS PLACEMENT ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE, PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK.
8. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
9. THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.
10. EACH DETECTOR SHALL HAVE ITS OWN 1" COILABLE NON-METALIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

TS 600
ECON 111

USER NAME = Baraa.Alsmadi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODERNIZATION PLAN TE 83 (BUSSE RD) AND HILLSIDE DR					F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED -		334	2025-1096-RS	DUPAGE	28	15					
	CHECKED -	REVISED -											CONTRACT NO. 80B08
	PLOT DATE = 1/20/2026	DATE -		SCALE:	SHEET 1	OF 5	SHEETS	STA.	TO STA.		ILLINOIS	FED. AID PROJECT	





DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

① SUB-BASE GRANULAR MATERIAL	⑥ FRAME AND LID (SEE NOTES)
② EXISTING PAVEMENT	⑦ CLASS PP-2* CONCRETE
③ 36 (900) DIAMETER METAL PLATE	⑧ PROPOSED HMA SURFACE COURSE
④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX	⑨ PROPOSED HMA BINDER COURSE
⑤ EXISTING STRUCTURE	

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

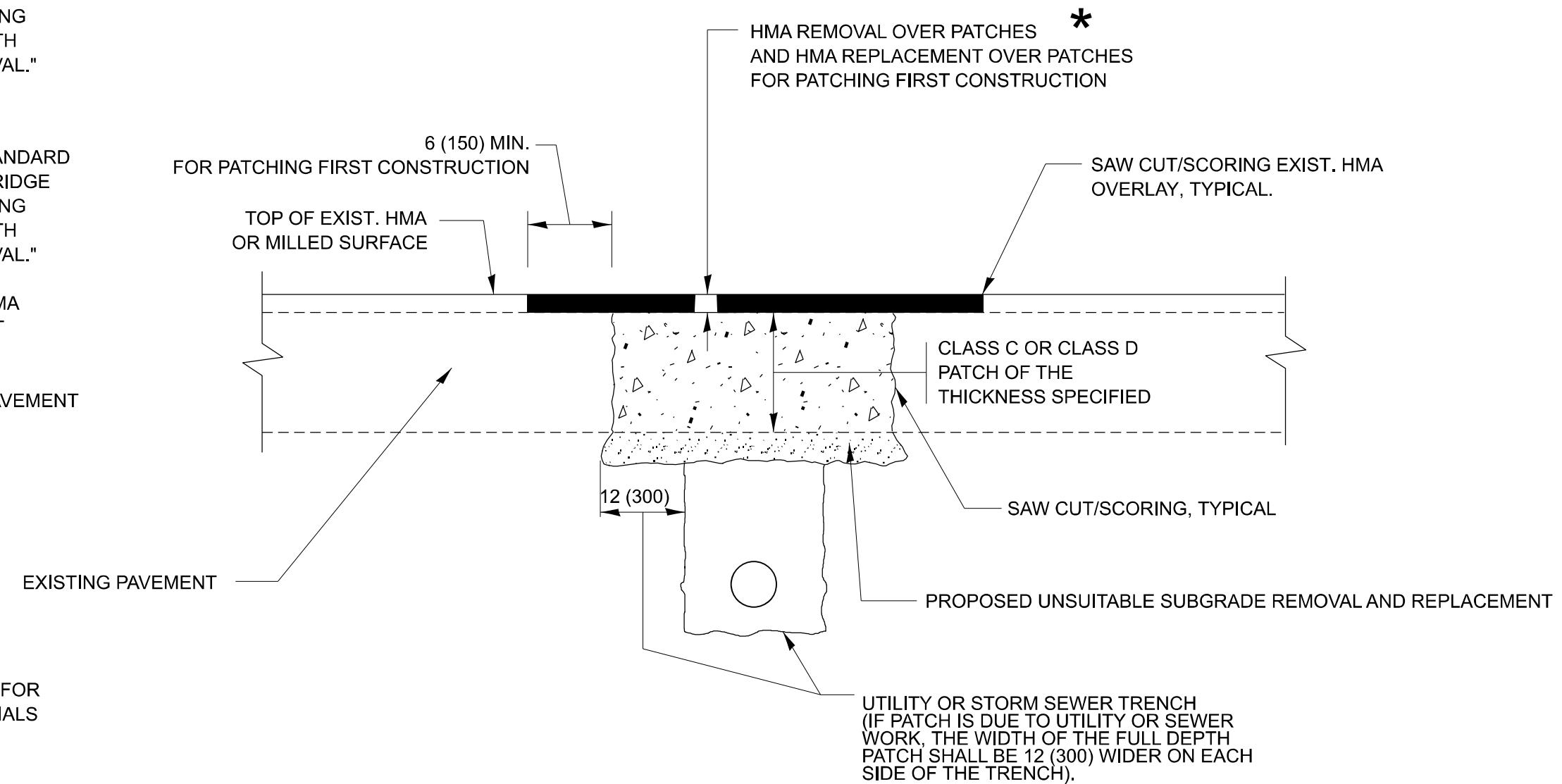
USER NAME	DESIGNED	R. SHAH	REVISED	R. BORO 03-09-11	STATE OF ILLINOIS	DETAILS FOR		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
						CHECKED	REVISED	K. SMITH 11-18-22	REVISED	K. SMITH 09-15-23	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	SCALE: NONE	DATE	10-25-94	STA.	TO STA.	BD600-03 (BD-08)
					DEPARTMENT OF TRANSPORTATION											ILLINOIS	FED. AID PROJECT

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

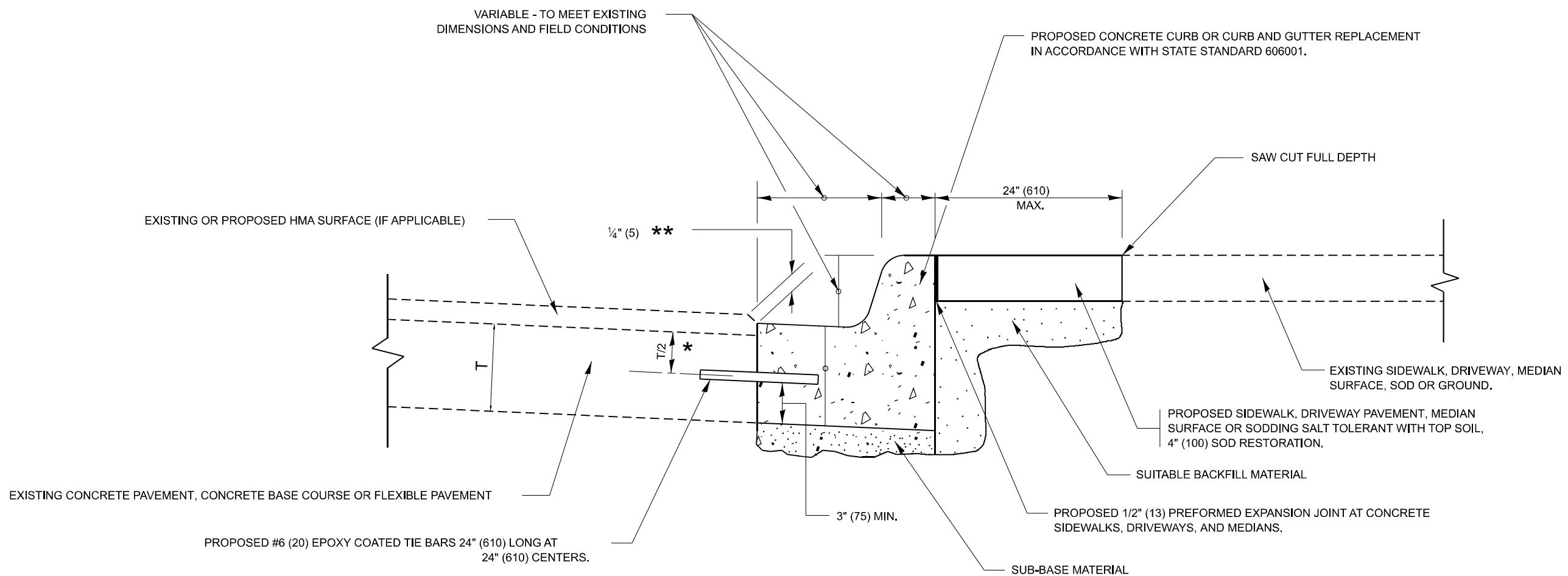
SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



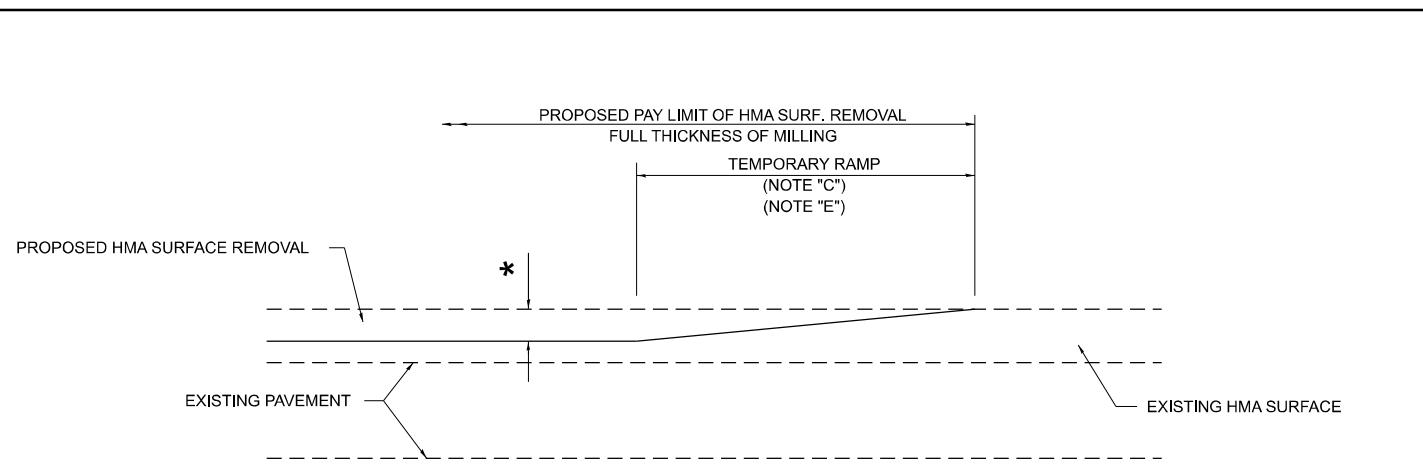
* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

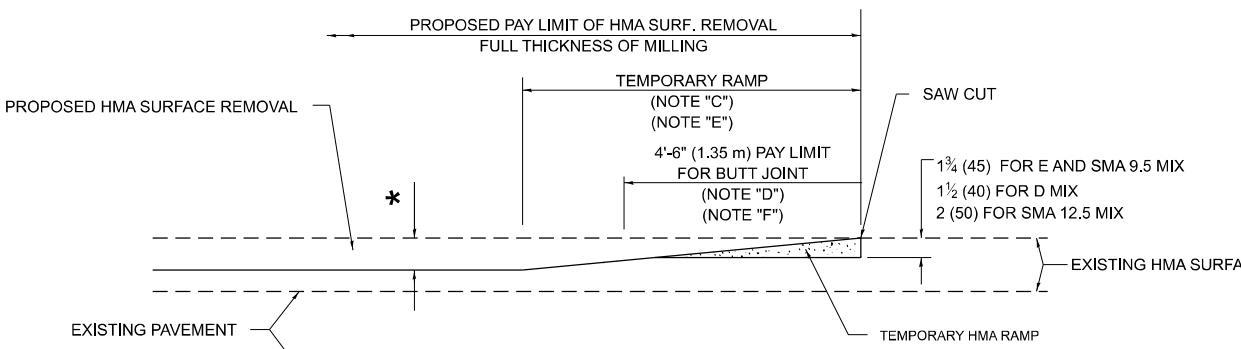
USER NAME = Baraa.alsmadi	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT				F.A.P RTE. 334	SECTION 2025-1096-RS	COUNTY DUPAGE	TOTAL SHEETS 28	SHEET NO. 19
	DRAWN -	REVISED - M. GOMEZ 01-22-01										
	CHECKED -	REVISED - R. BORO 12-15-09										
PLOT DATE = 12/5/2025	DATE - 03-11-94	REVISED - K. SMITH 07-11-19		SCALE: NONE	1	OF 1	SHETS	STA.	TO STA.		BD600-06 (BD-24)	CONTRACT NO. 80B08



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

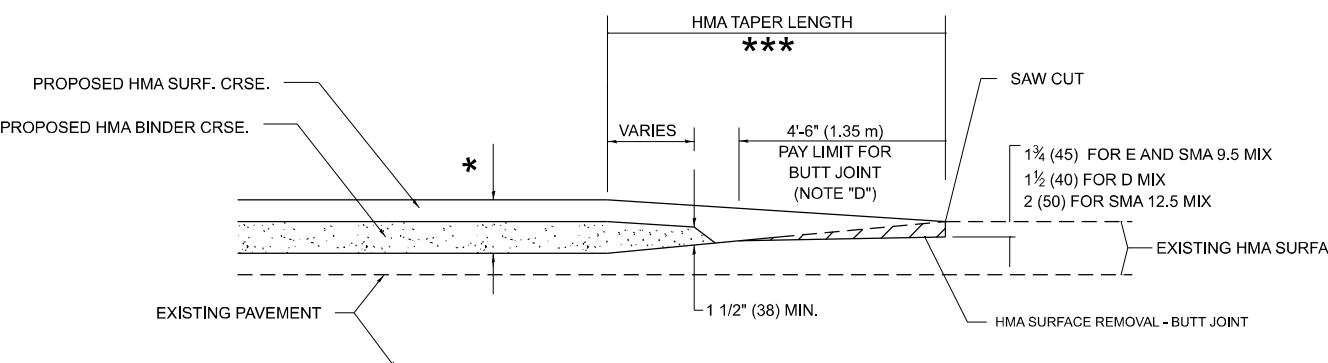


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

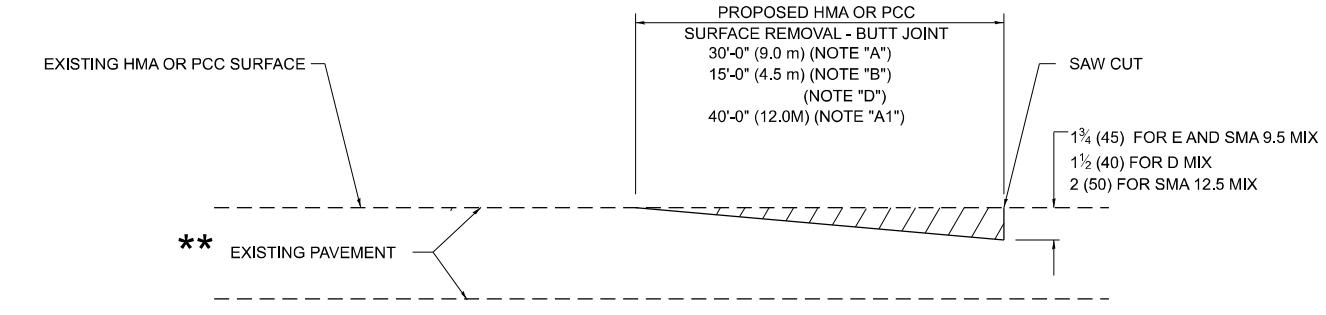
OPTION 2

TYPICAL TEMPORARY RAMP

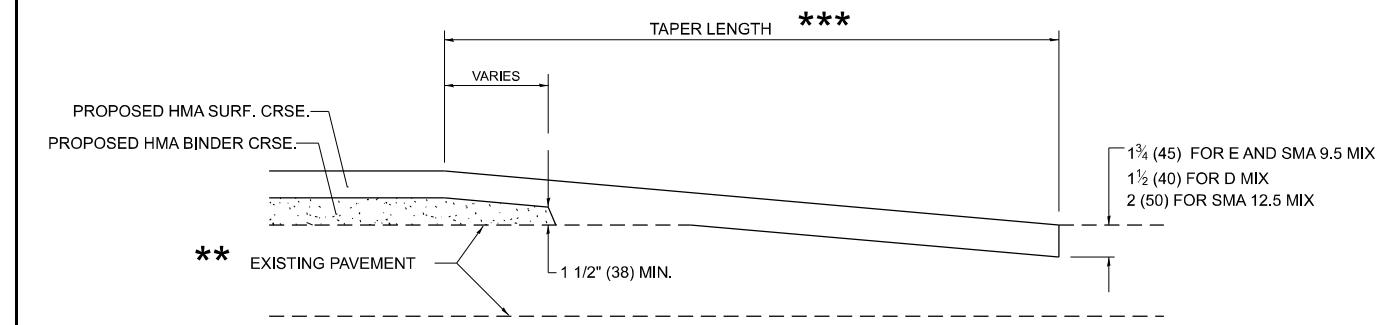


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

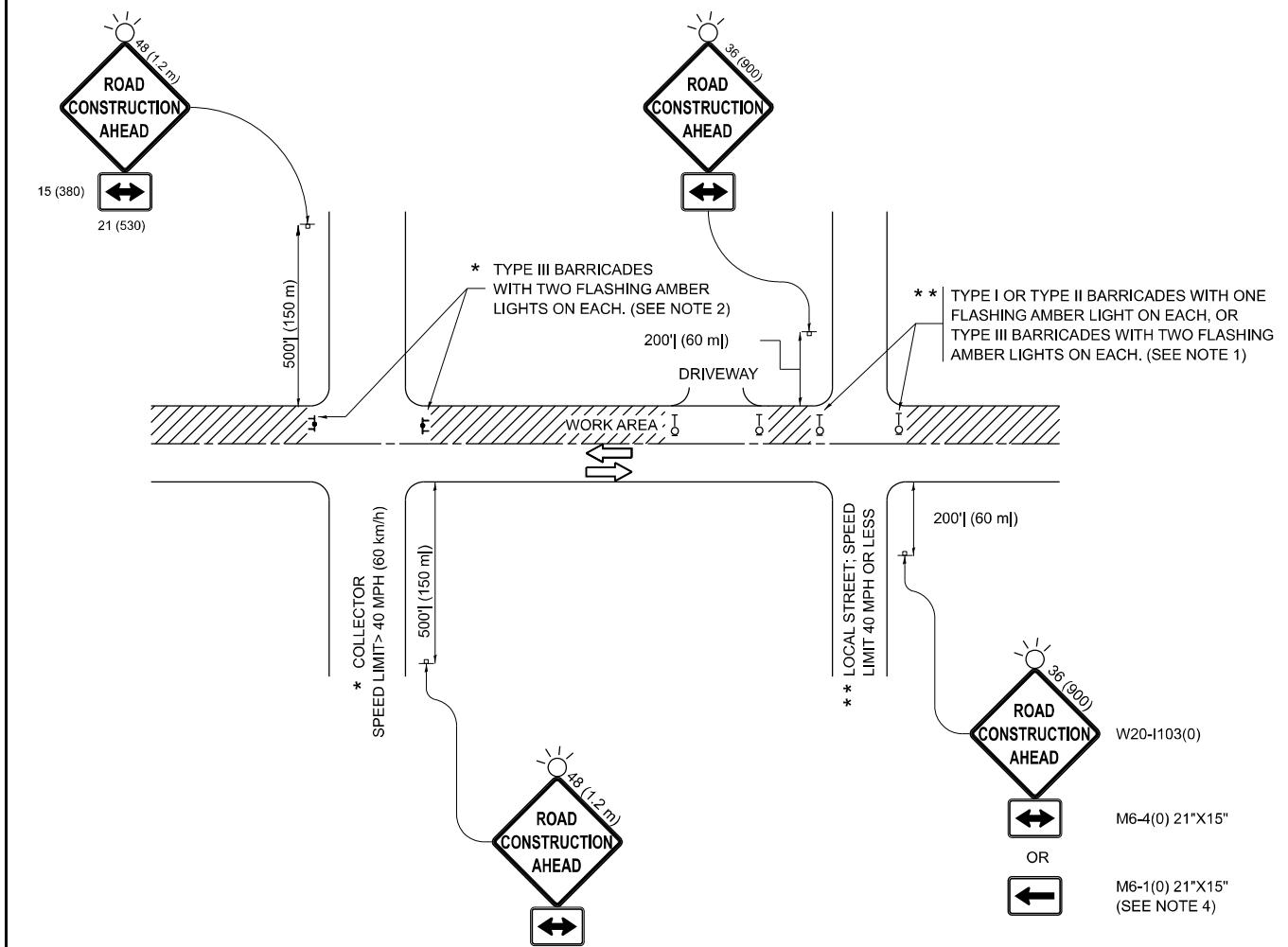
20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = Baraa.alsmadi	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
DRAWN -		REVISED - M. GOMEZ 04-06-01
CHECKED -		REVISED - R. BORO 01-01-07
PLOT DATE = 12/5/2025	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

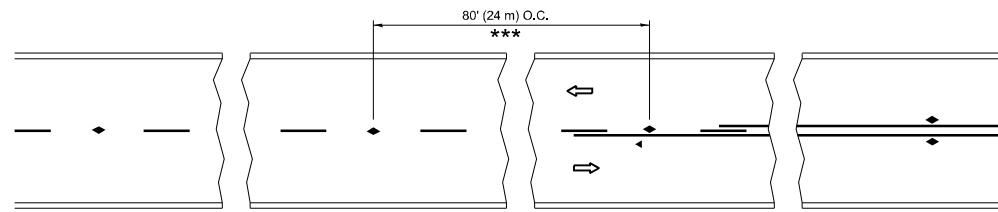


NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

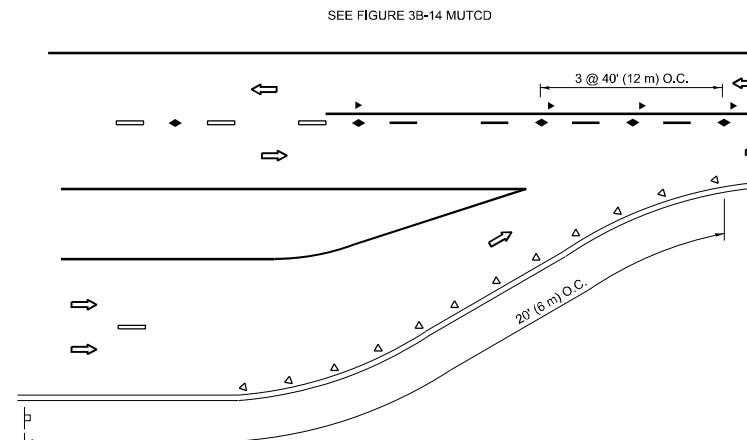
All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = Baraa.alsmadi			DESIGNED - L.H.A.		REVISED - T. RAMMACHER 01-06-00		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
			DRAWN -		REVISED - A. SCHUETZE 07-01-13									334	2025-1096-RS	DUPAGE	28	21			
			CHECKED -		REVISED - A. SCHUETZE 09-15-06									TC-10				CONTRACT NO. 80B08			
			PLOT DATE = 12/5/2025	DATE = 06-89	REVISED = D. SENDERAK 05-03-24					SCALE:	SHEET	OF	SHEETS	STA.	TO STA.						
																	ILLINOIS	FED. AID PROJECT			

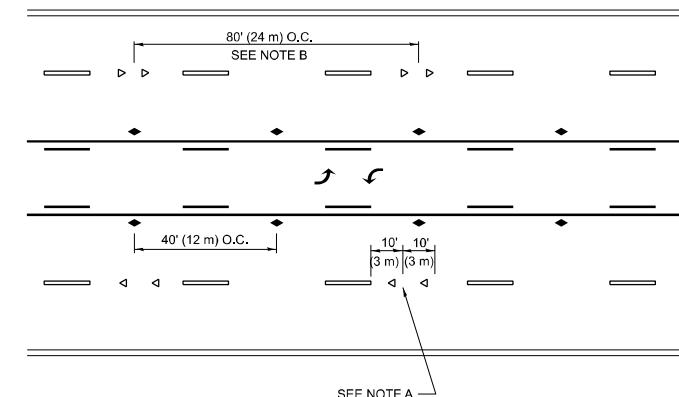


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

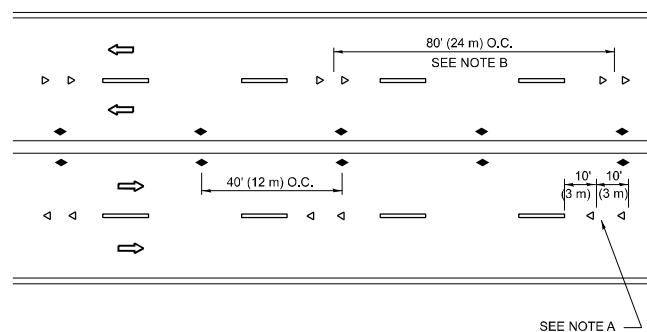
TWO-LANE/TWO-WAY



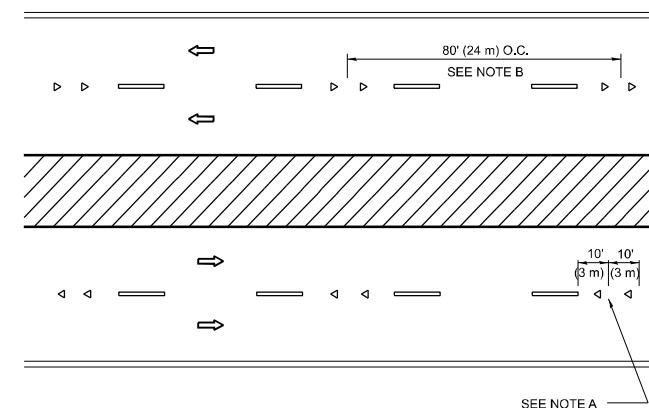
LANE REDUCTION TRANSITION



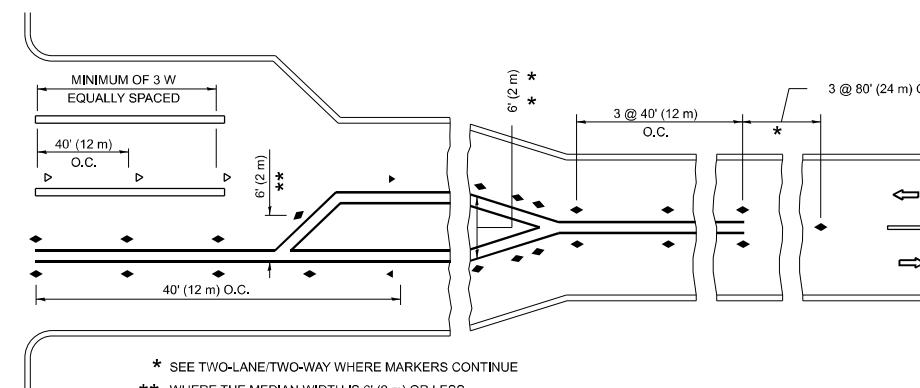
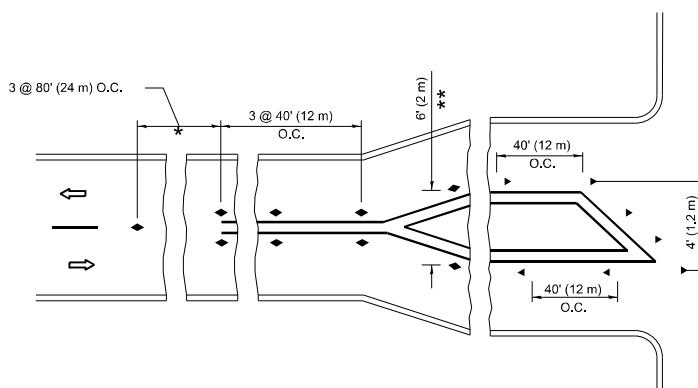
TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED



TURN LANES

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLO STRIPE
- WHITE STRIPE
- ▲ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME	DESIGNED	REVISED	TYPICAL APPLICATIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN	CHECKED	DATE					
USER NAME = Baraa.alsmadi	DESIGNED -	REVISED - T. RAMMACHER 03-12-99								
	DRAWN -	REVISED - T. RAMMACHER 01-06-00								
	CHECKED -	REVISED - C. JUCIUS 09-09-09								
PLOT DATE = 12/5/2025	DATE -	REVISED - C. JUCIUS 07-01-13								
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.			
						F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						334	2025-1096-RS	DUPAGE	28	22
							TC-11			CONTRACT NO. 80B08
								ILLINOIS		FED. AID PROJECT

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

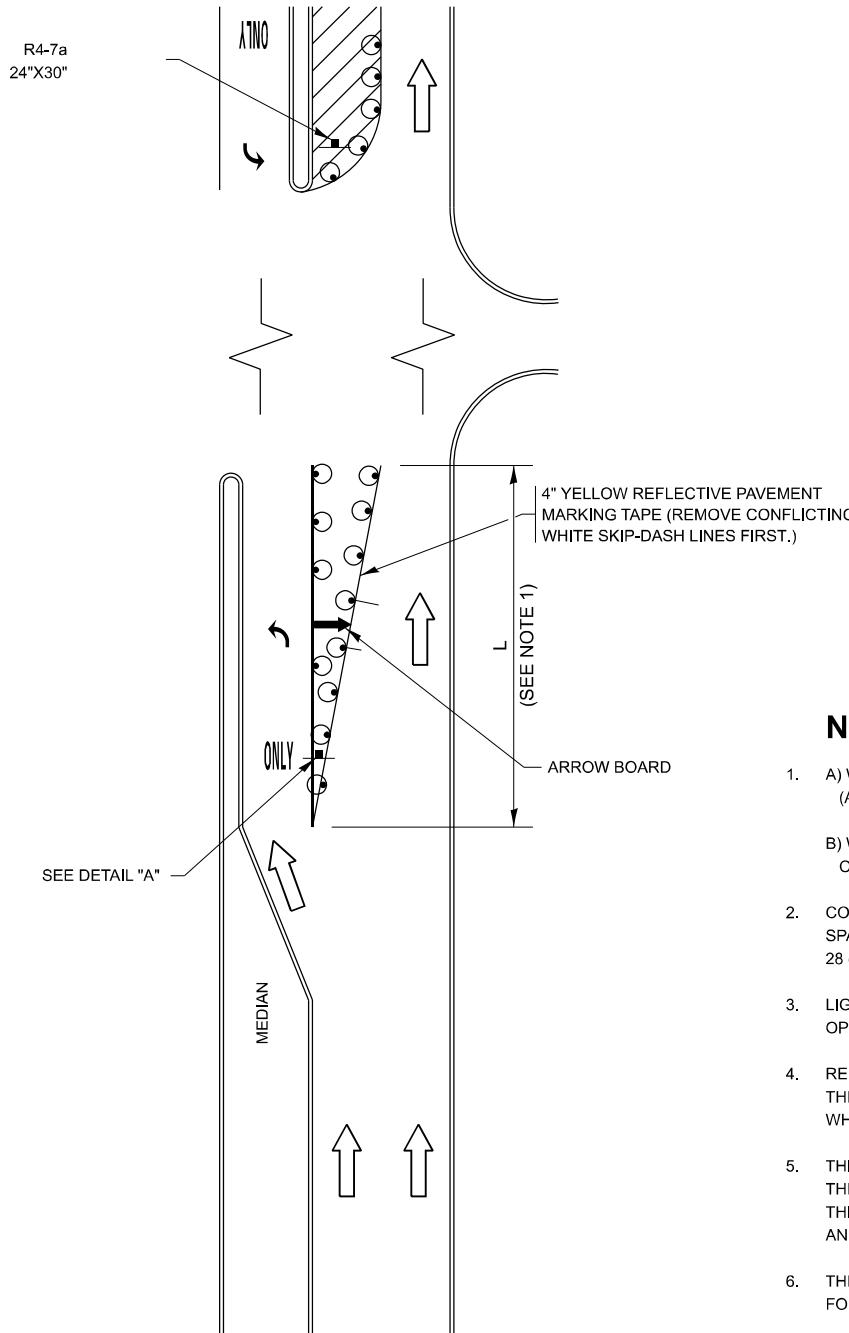
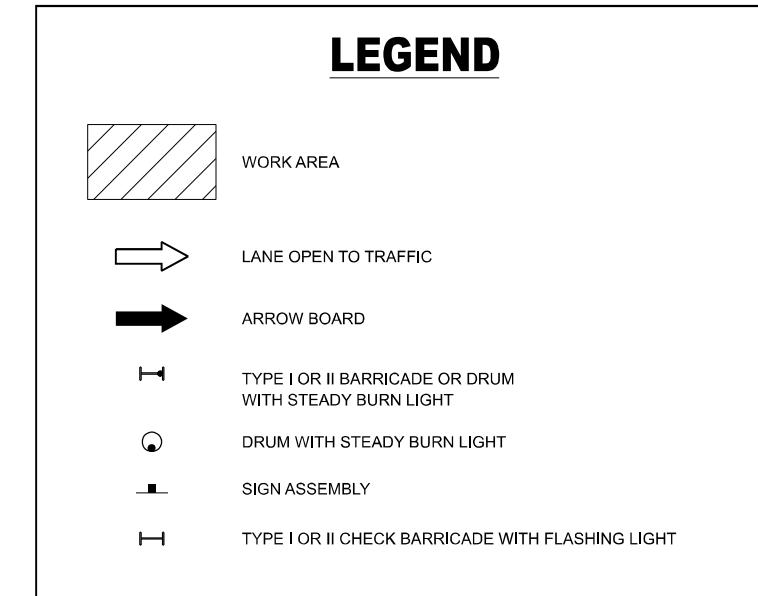


FIGURE 1



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
2. B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
5. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
6. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
7. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
8. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

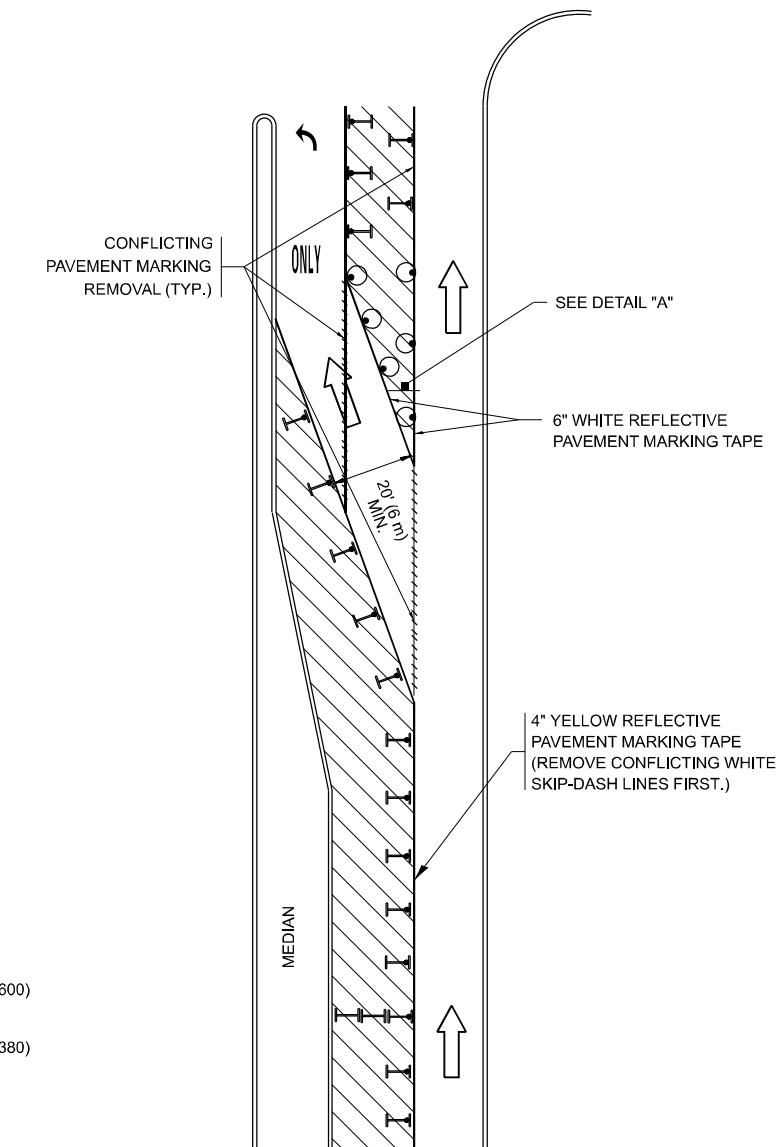
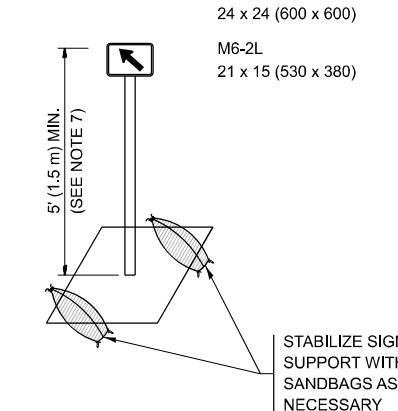
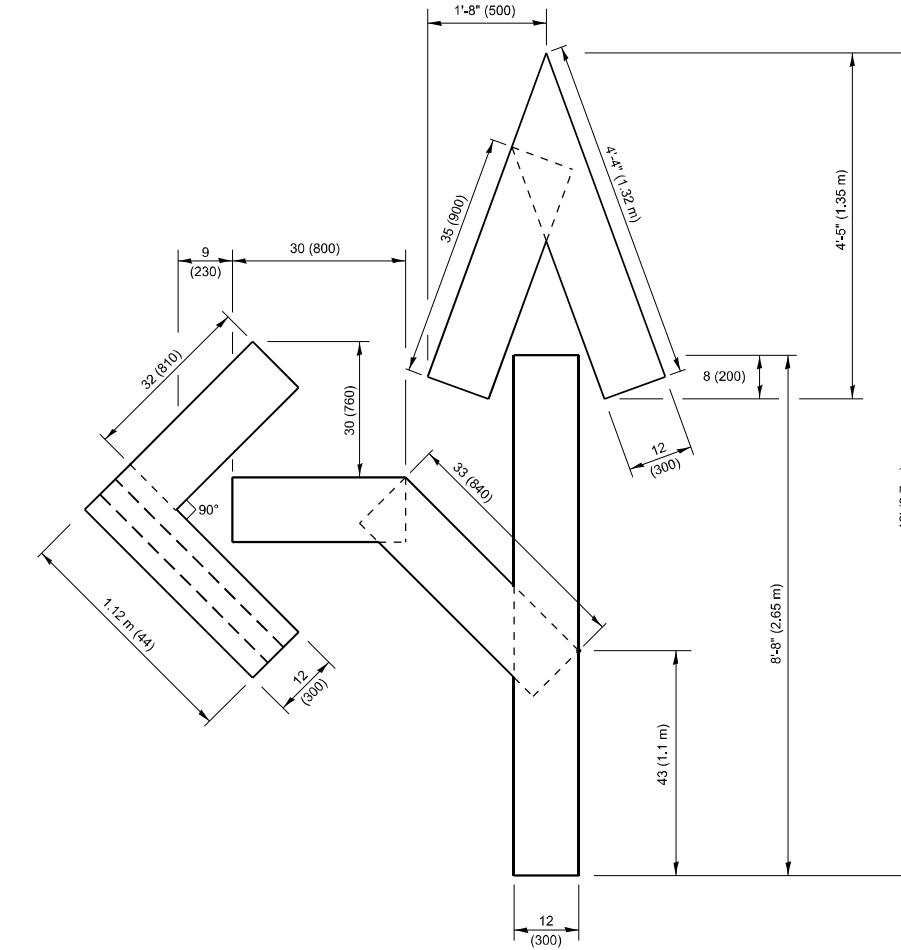
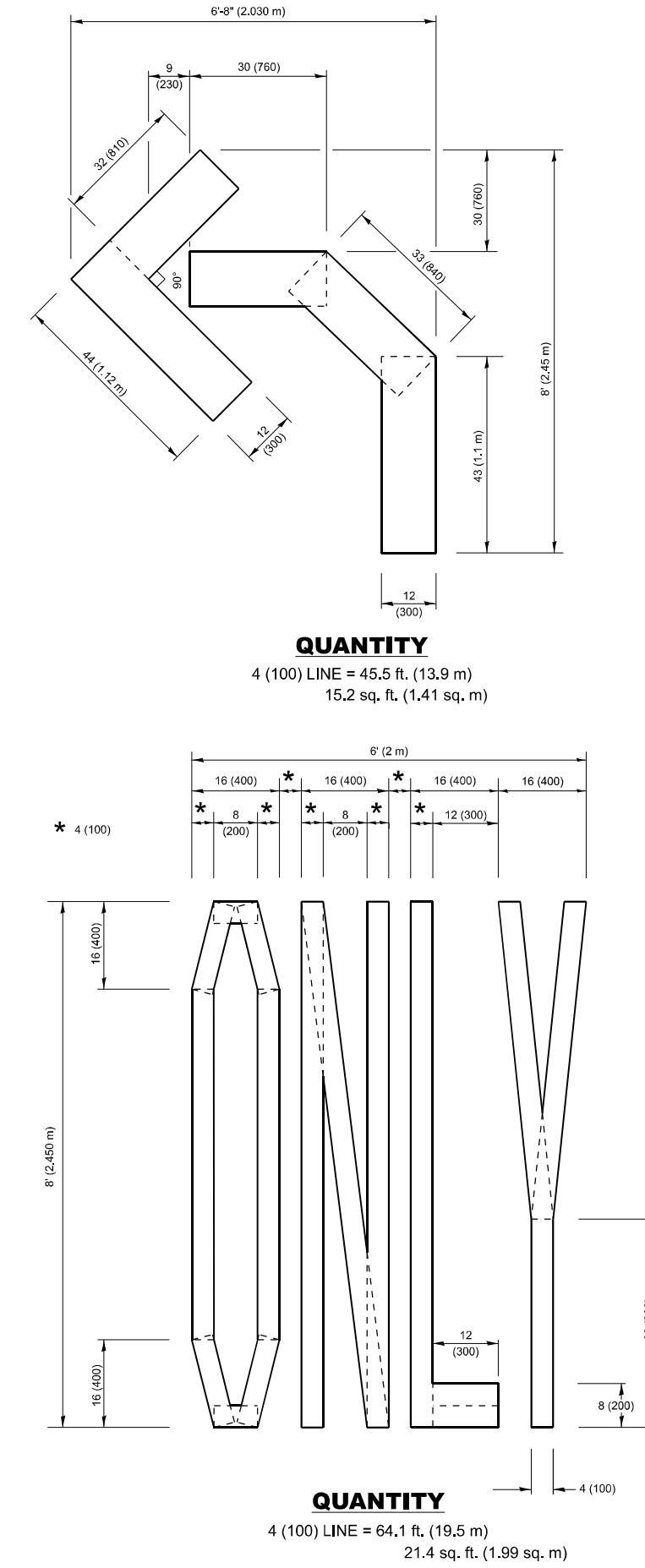


FIGURE 2

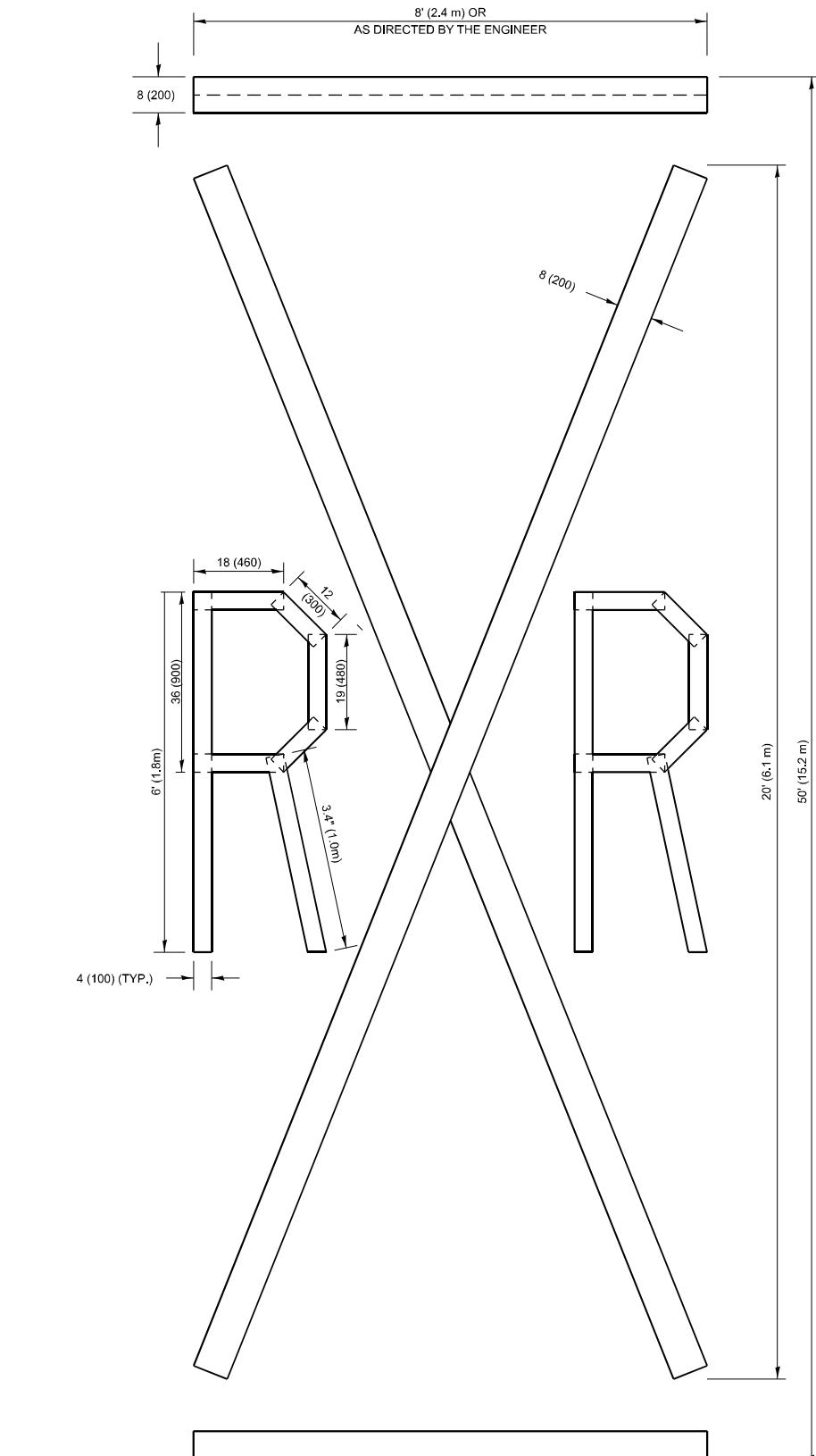


DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.



NOTE:
ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



All dimensions are in inches (millimeters)
unless otherwise shown.

USER NAME = Baraa.alsmadi	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
DRAWN -		REVISED - E. GOMEZ 08-28-00
CHECKED -		REVISED - E. GOMEZ 08-28-00
PLOT DATE = 12/5/2025	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

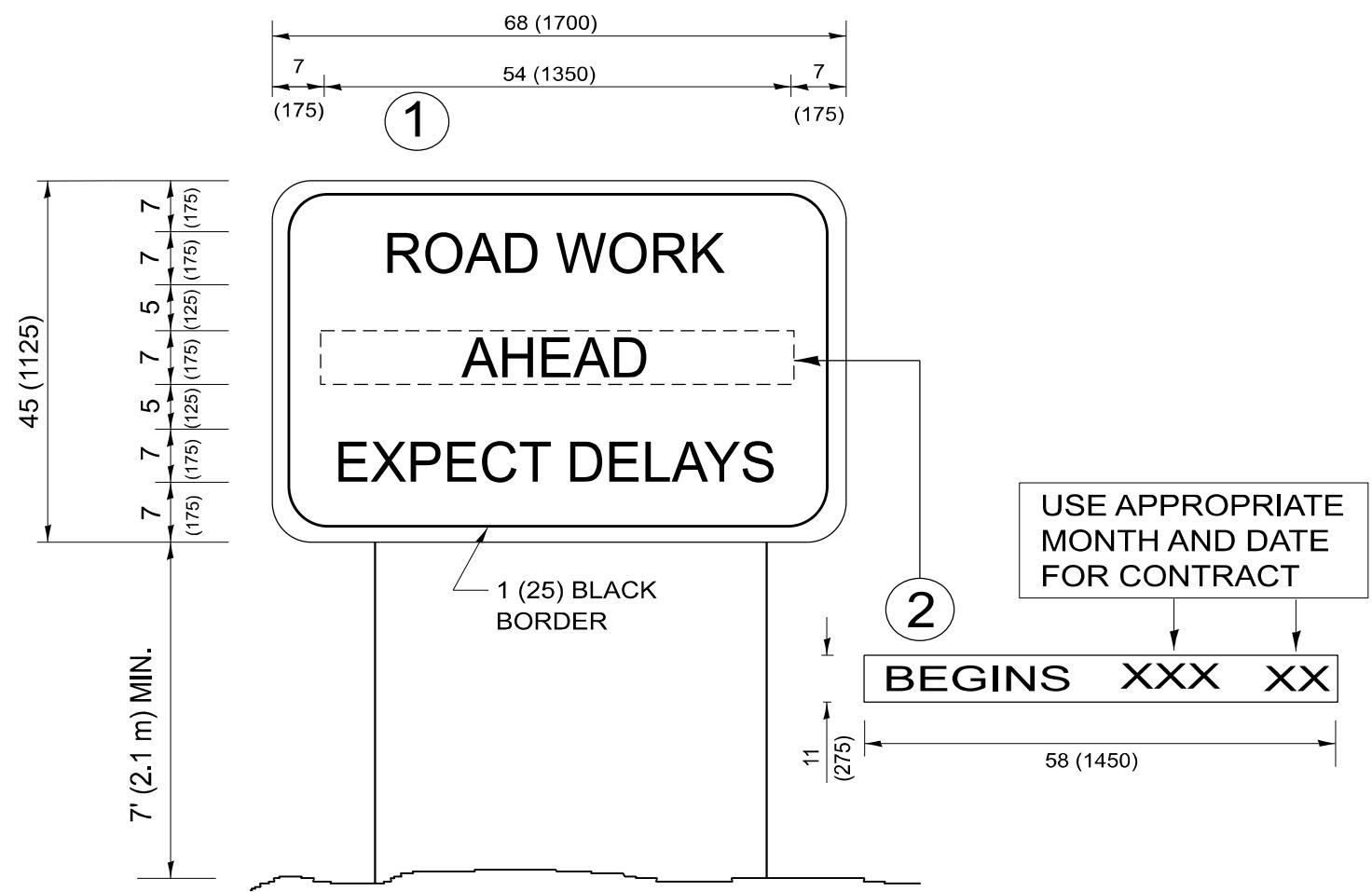
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	25
TC-16				CONTRACT NO. 80B08

ILLINOIS FED. AID PROJECT

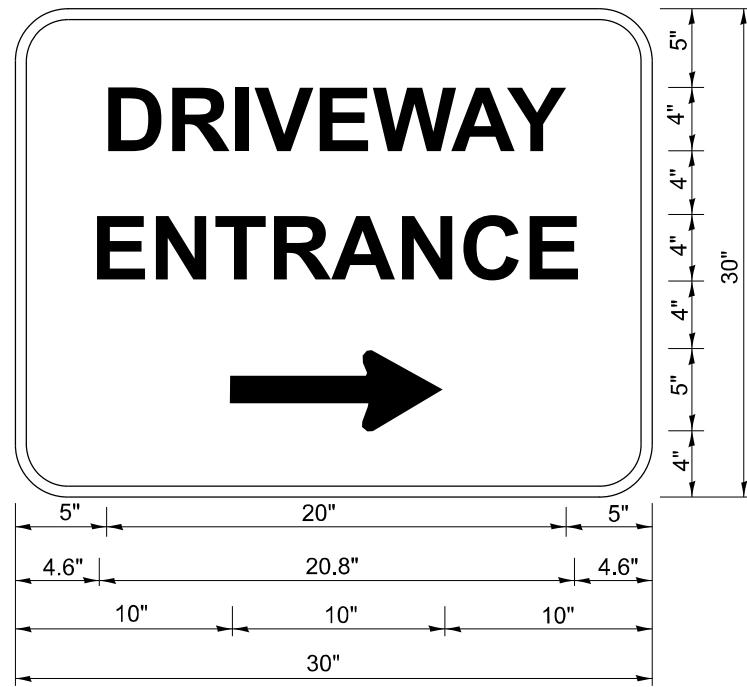


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

USER NAME - Baraa.alsmadi	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE. 334	SECTION 2025-1096-RS	COUNTY DUPAGE	TOTAL SHEETS 28	SHEET NO. 26
	DRAWN -	REVISED - R. MIRS 12-11-97							
	CHECKED -	REVISED - T. RAMMACHER 02-02-99							
PLOT DATE - 12/5/2025	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	1 OF 1 SHEETS STA.	TO STA.	TC-22	CONTRACT NO. 80B08	ILLINOIS FED. AID PROJECT



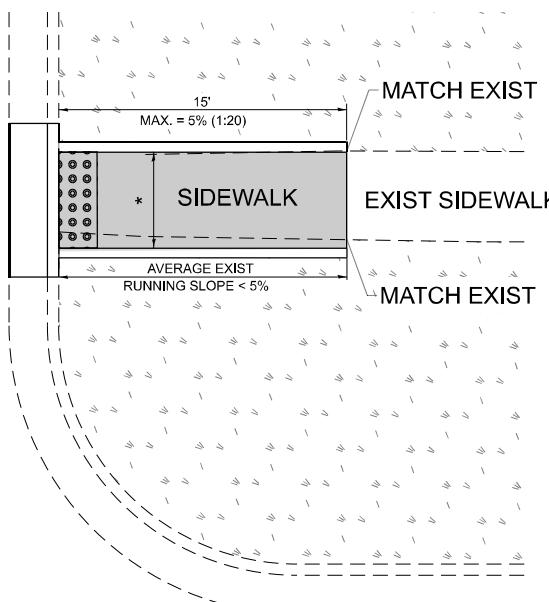
3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

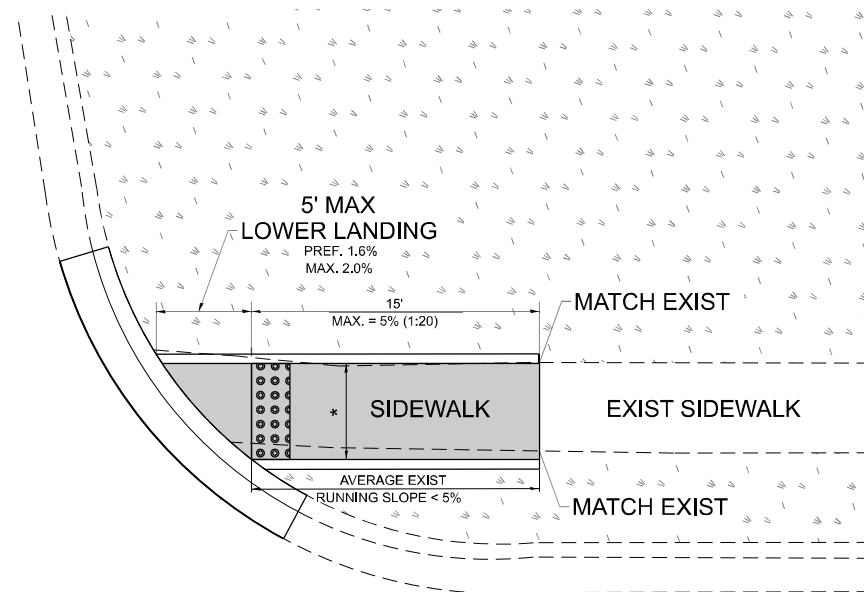
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

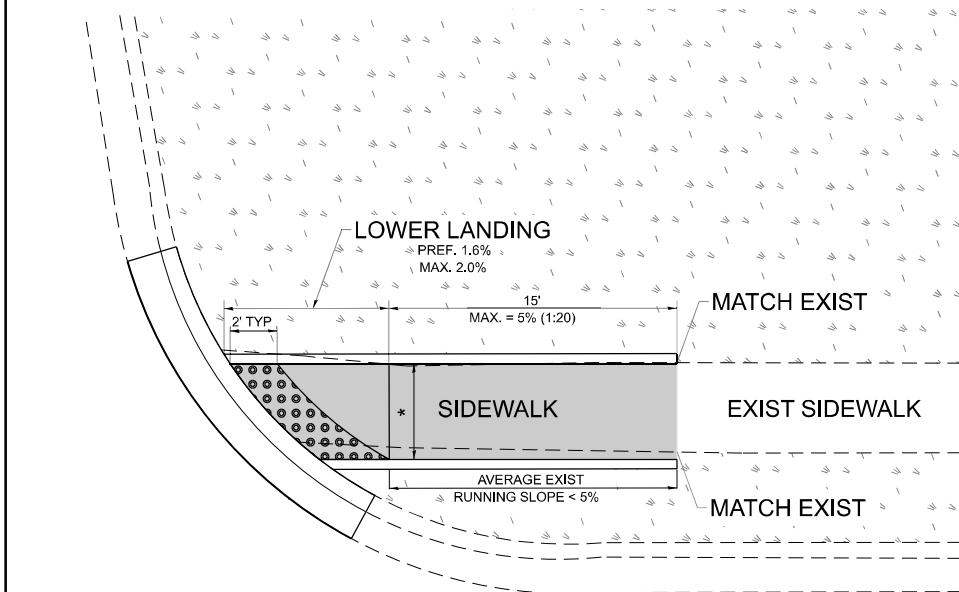
PD-01A



PD-01



PD-01C



LEGEND



PROPOSED SIDE CUP



DETECTABLE WARNING

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-01 [Sheet 1 of 1]

USER NAME = Baraa.alsmadi	DESIGNED -
	DRAWN -
	CHECKED -
PLOT DATE = 12/5/2025	DATE -

	REVISED	-	
MA	REVISED	-	
	REVISED	-	
9	REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS (PD-01)

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	28
	PD-01		CONTRACT NO. 80B08	
	ILLINOIS	FED. AID PROJECT		