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INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-6	TYPICAL SECTIONS
7-9	ROADWAY PLANS
10-11A	ADA IMPROVEMENT DETAILS
12-16	PROPOSED SIGNAL PLANS
17	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
18	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
19	CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
20	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
21	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
22	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
23	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
24	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
25	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
26	ARTERIAL ROAD INFORMATION SIGN (TC-22)
27	DRIVEWAY ENTRANCE SIGNING (TC-26)
28	PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS (PD-01)

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-09	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-04	CLASS C and D PATCHES
701101-05	OFF-RD OPERATIONS, MULTILANE 15' (4.5 m) TO 24" (600 mm)FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE MORE THAN 15' (4.5 m) AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701422-10	LANE CLOSURE MULTILANE, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≥ 45 MPH
701601-09	URBAN LANE CLOSURE MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-11	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES, ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT MR. EMAD ALHUSSEINI AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- ALL PAVEMENT MARKINGS ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING DETAIL TC-13.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ALL RAISED REFLECTIVE PAVEMENT MARKERS (RRPM) SHALL BE PLACED THROUGHOUT THE IMPROVEMENT AND INSTALLED ACCORDING TO DISTRICT STANDARDS, AS SHOWN IN THE IDOT D1 RRPM DETAIL TC-11.
- CONTRACTOR MUST NOT ENCROACH, PERFORM ANY CONSTRUCTION ACTIVITIES, OR PARK ANY CONSTRUCTION EQUIPMENT OR VEHICLES IN THE AREA WITHIN 30' FROM THE CENTERLINE OF THE NEAREST RAILROAD TRACK.
- PAVEMENT MARKING, TYPE IV TAPE SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- STORM SEWERS AND CULVERT BOX CLEANING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, VILLAGE OF BENSENVILLE, AND DUPAGE COUNTY.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- TEMPORARY PAVEMENT MARKINGS ON SHORT-TERM PAVEMENT MARKINGS ON INTERMEDIATE SURFACES SHALL NOT BE REMOVED UNLESS DIRECTED BY THE ENGINEER.

	USER NAME = Baraa.alsmadi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES S OF BRYN MAWR AVE TO IL-19 (IRVING PARK RD)			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					334	2025-1096-RS	DUPAGE	28	2
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	PLOT DATE = 1/7/2026	DATE -	REVISED -		SCALE:	SHEET	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

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SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	ROADWAY	SIGNALS			
					80% FED 20% STATE	100% STATE	80% FED 20% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0044	0021			
	20200100	EARTH EXCAVATION	CU YD	10	10					
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	22	22					
	25200110	SODDING, SALT TOLERANT	SQ YD	22	22					
	25200200	SUPPLEMENTAL WATERING	UNIT	1	1					
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	33063	33063					
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	29120	29120					
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	111	111					
	40600405	MATERIAL TRANSFER DEVICE	TON	6950	6950					
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	330	330					
	40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	321	321					
	40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	1279	1279					
	40605036	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, MIX "F", N80	TON	6950	6950					
	42001300	PROTECTIVE COAT	SQ YD	165	165					
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	280	280					
	42400800	DETECTABLE WARNINGS	SQ FT	80	80					
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	73472	73472					
	44000600	SIDEWALK REMOVAL	SQ FT	294	294					
	44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	3146	3146					
	44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	184	184					
	44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	654	654					
	44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	1103	1103					
	44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	919	919					
	48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	981	981					

SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	ROADWAY	SIGNALS			
					80% FED 20% STATE	100% STATE	80% FED 20% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0044	0021			
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1					
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	3	3					
Δ	60905305	BOX CULVERTS TO BE CLEANED	FOOT	125		125				
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10	10					
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1					
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1					
*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2	2					
	67100100	MOBILIZATION	L SUM	1	1					
	70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1					
	70100320	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422	L SUM	1	1					
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
	70102635	TRAFFIC CONTROLAND PROTECTION, STANDARD 701701	L SUM	1	1					
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
	70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	13581	13581					
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2264	2264					
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	535	535					
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	33671	33671					
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	2942	2942					
	70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	793	793					

* SPECIALTY ITEM

	USER NAME = Baraa.alsmadi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				F.A.P RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 12/5/2025	DATE -	REVISED -		SCALE:	SHEET	OF 2 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: SHEET OF 2 SHEETS STA. TO STA.

REV-SEP

LEGEND

1. EXISTING P.C.C. BASE, ±9"
2. EXISTING HOT-MIX ASPHALT, ±3"
3. EXISTING HOT-MIX ASPHALT SHOULDER,±10"
4. EXISTING CURB AND GUTTER TYPE B-6.12 OR B-6.24
5. EXISTING LANDSCAPE OR CONCRETE MEDIAN SURFACES 5"
6. PROPOSED HMA SURFACE REMOVAL 2"
7. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 12.5, MIX "F", N80, 2"
8. PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"
9. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
10. PROPOSED GRADING AND SHAPING SHOULDERS

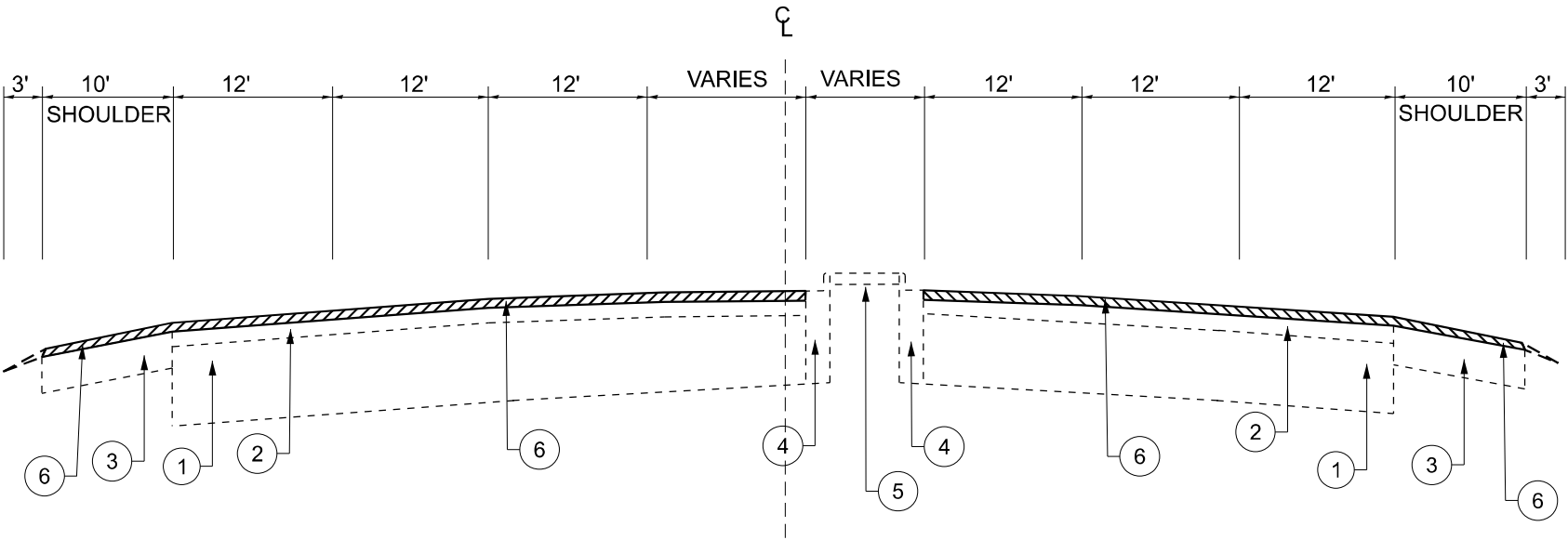
NOTE:

1. CONTRACTOR SHALL PATCH BEFORE MILLING
2. LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QMP	MTD (YES/NO)
MIXTURE TYPE	AIR VOIDS @ Ndesign		
PAVEMENT RESURFACING			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, MIX "F", N80, 2"	3.5% @ 80 Gyr.	QCP	YES
HMA SHOULDER RESURFACING			
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	4% @ 70 Gyr.	QC/QA	NO
PATCHING			
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 90 Gyr.	QC/QA	NO
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 90 Gyr.	QC/QA	NO
QMP Designations: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)			

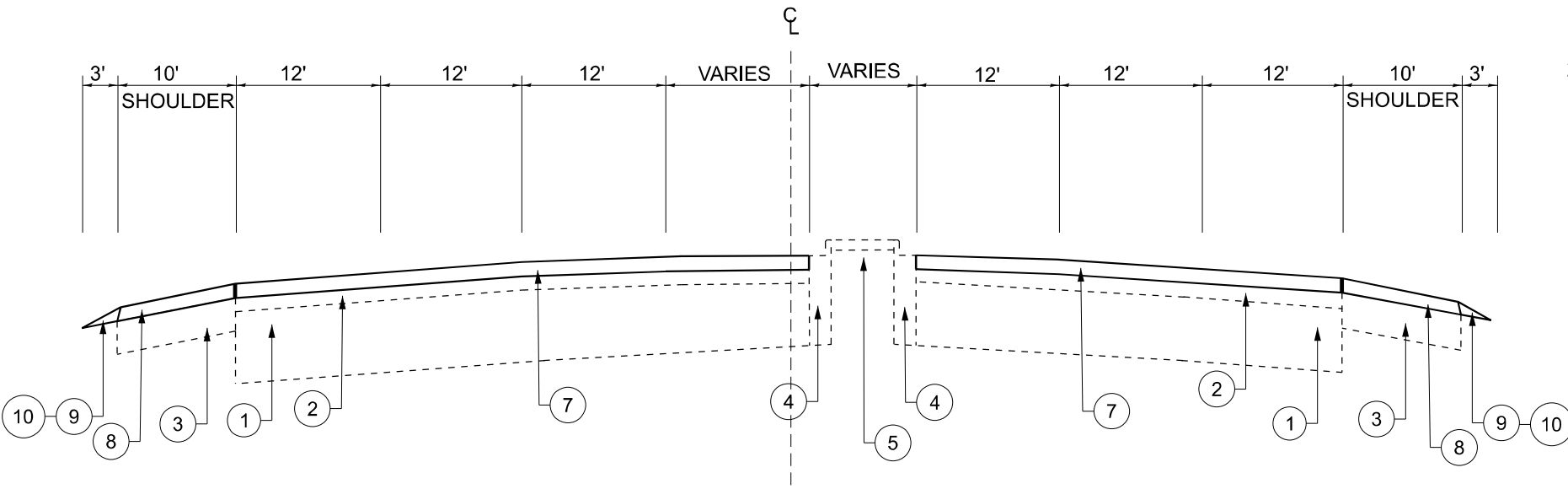
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS



IL ROUTE 83
STA. 15+76 TO STA. 59+48

EXISTING TYPICAL SECTION



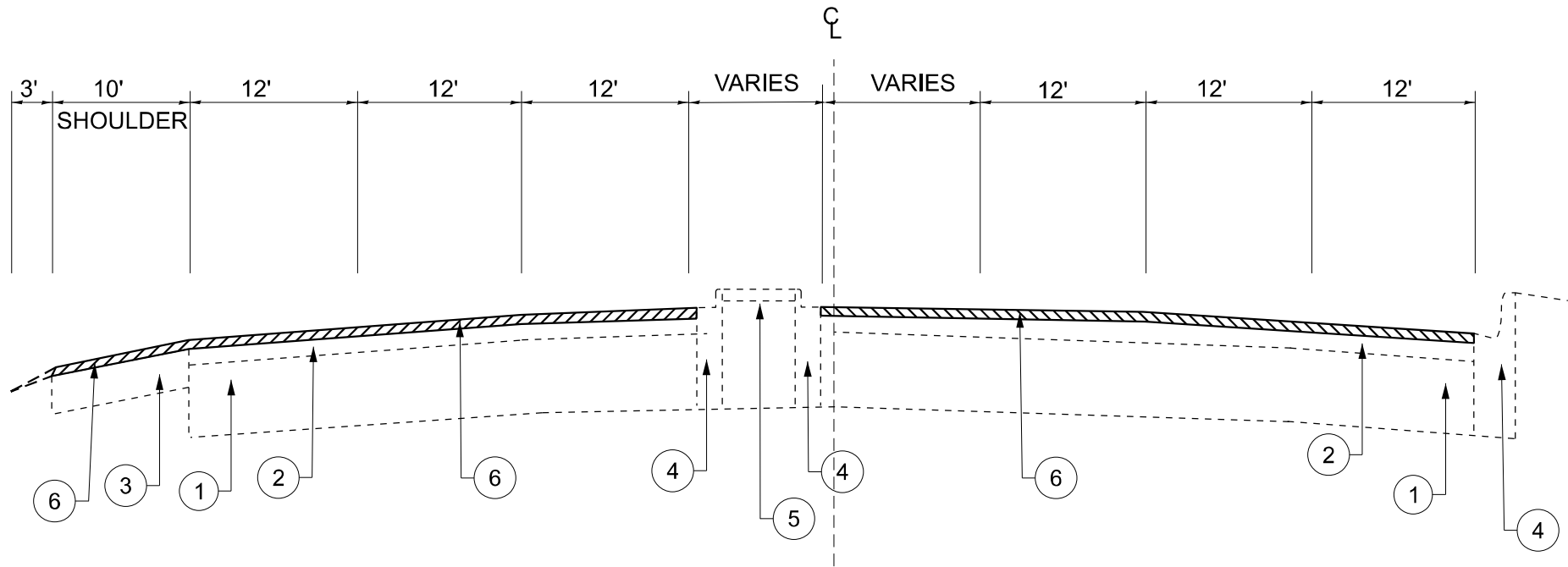
IL ROUTE 83
STA. 15+76 TO STA. 59+48

PROPOSED TYPICAL SECTION

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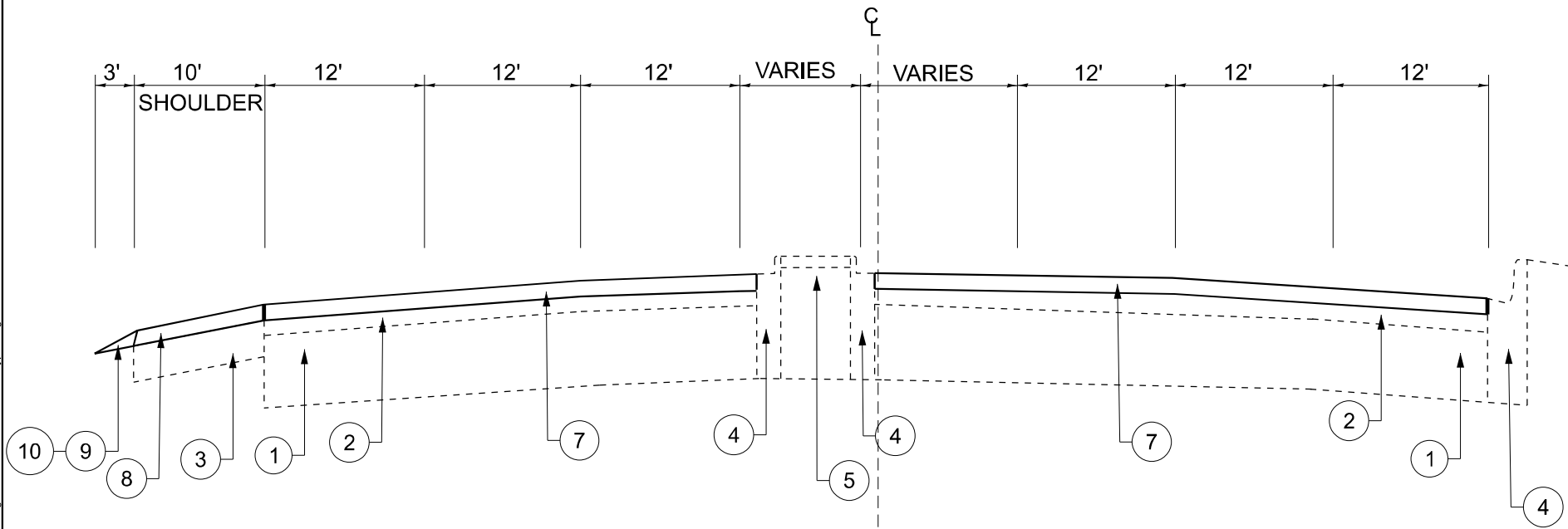
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		DRAWN -	REVISED -				334	2025-1096-RS	DUPAGE	28	5
		CHECKED -	REVISED -				CONTRACT NO. 80B08				
	PLOT DATE = 12/5/2025	DATE -	REVISED -				ILLINOIS FED. AID PROJECT				
SCALE:		SHEET 1 OF 2 SHEETS		STA.		TO STA.					

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IL ROUTE 83
STA. 59+48 TO STA. 82+01

EXISTING TYPICAL SECTION



IL ROUTE 83
STA. 59+48 TO STA. 82+01

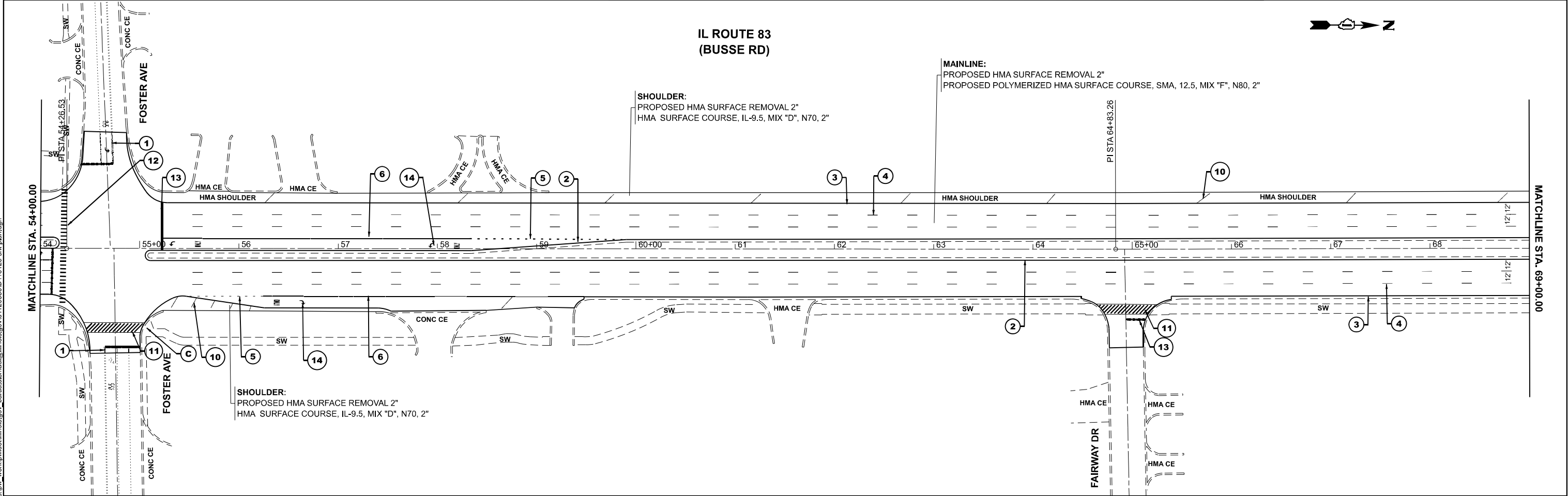
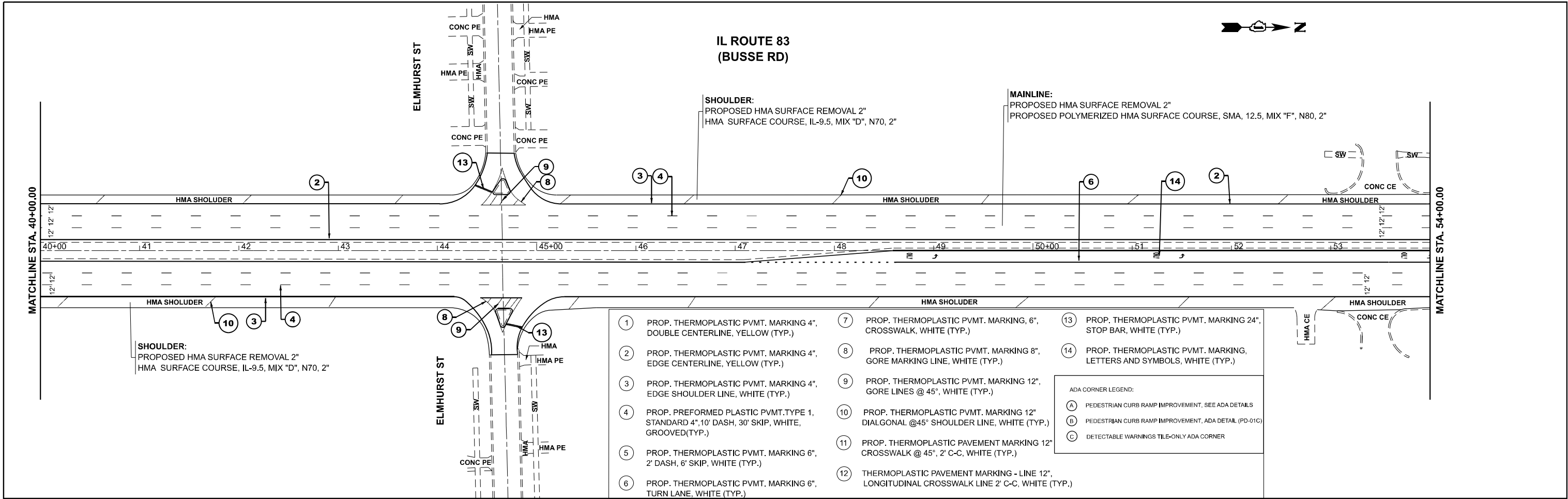
PROPOSED TYPICAL SECTION

LEGEND

- EXISTING P.C.C. BASE, $\pm 9"$
- EXISTING HOT-MIX ASPHALT, $\pm 3"$
- EXISTING HOT-MIX ASPHALT SHOULDER, $\pm 10"$
- EXISTING CURB AND GUTTER TYPE B-6.12 OR B-6.24
- EXISTING LANDSCAPE OR CONCRETE MEDIAN SURFACES 5"
- PROPOSED HMA SURFACE REMOVAL 2"
- PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 12.5, MIX "F", N80, 2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"
- PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- PROPOSED GRADING AND SHAPING SHOULDERS

	USER NAME = Baraa.alsmadi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS S OF BRYN MAWR AVE TO IL-19 (IRVING PARK RD)			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					334	2025-1096-RS	DUPAGE	28	6
		CHECKED -	REVISED -					CONTRACT NO. 80B08				
	PLOT DATE = 12/5/2025	DATE -	REVISED -		SCALE:	SHEET 2	OF 2	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

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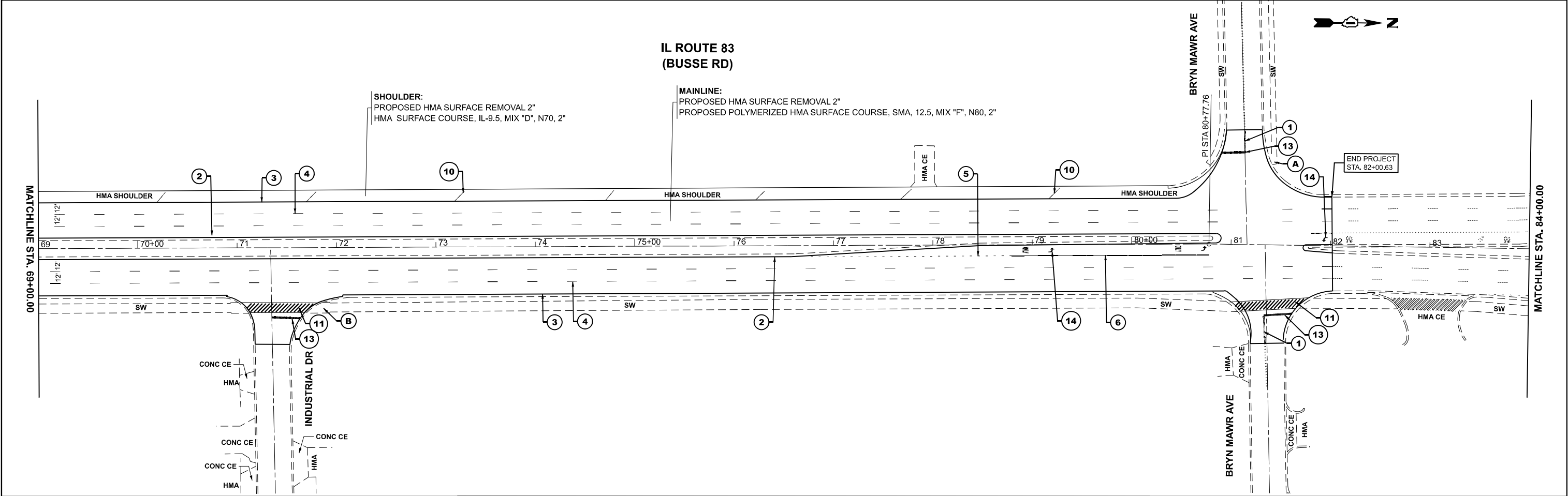
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PLOT DATE = 1/7/2026	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
S OF BRYN MAWR AVE TO IL-19 (IRVING PARK RD)

SCALE: 1"=50' SHEET 1 OF 6 SHEETS STA. 40+00.00 TO STA. 69+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	8
CONTRACT NO. 80B08				
ILLINOIS FED. AID PROJECT				



IL ROUTE 83
(BUSSE RD)

1 PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE CENTERLINE, YELLOW (TYP.)

2 PROP. THERMOPLASTIC PVMT. MARKING 4", EDGE CENTERLINE, YELLOW (TYP.)

3 PROP. THERMOPLASTIC PVMT. MARKING 4", EDGE SHOULDER LINE, WHITE (TYP.)

4 PROP. PREFORMED PLASTIC PVMT. TYPE 1, STANDARD 4", 10' DASH, 30' SKIP, WHITE, GROOVED(TYP.)

5 PROP. THERMOPLASTIC PVMT. MARKING 6", 2' DASH, 6' SKIP, WHITE (TYP.)

6 PROP. THERMOPLASTIC PVMT. MARKING 6", TURN LANE, WHITE (TYP.)

7 PROP. THERMOPLASTIC PVMT. MARKING, 6", CROSSWALK, WHITE (TYP.)

8 PROP. THERMOPLASTIC PVMT. MARKING 8", GORE MARKING LINE, WHITE (TYP.)

9 PROP. THERMOPLASTIC PVMT. MARKING 12", GORE LINES @ 45°, WHITE (TYP.)

10 PROP. THERMOPLASTIC PVMT. MARKING 12" DIALGONAL @45° SHOULDER LINE, WHITE (TYP.)

11 PROP. THERMOPLASTIC PAVEMENT MARKING 12" CROSSWALK @ 45°, 2' C-C, WHITE (TYP.)

12 THERMOPLASTIC PAVEMENT MARKING - LINE 12", LONGITUDINAL CROSSWALK LINE 2' C-C, WHITE (TYP.)

13 PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)

14 PROP. THERMOPLASTIC PVMT. MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)

ADA CORNER LEGEND:

A PEDESTRIAN CURB RAMP IMPROVEMENT, SEE ADA DETAILS

B PEDESTRIAN CURB RAMP IMPROVEMENT, ADA DETAIL (PD-01C)

C DETECTABLE WARNINGS TILE-ONLY ADA CORNER

USER NAME = Baraa.alsmadi	DESIGNED -	REVISED -
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PLOT DATE = 1/7/2026	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
S OF BRYN MAWR AVE TO IL-19 (IRVING PARK RD)

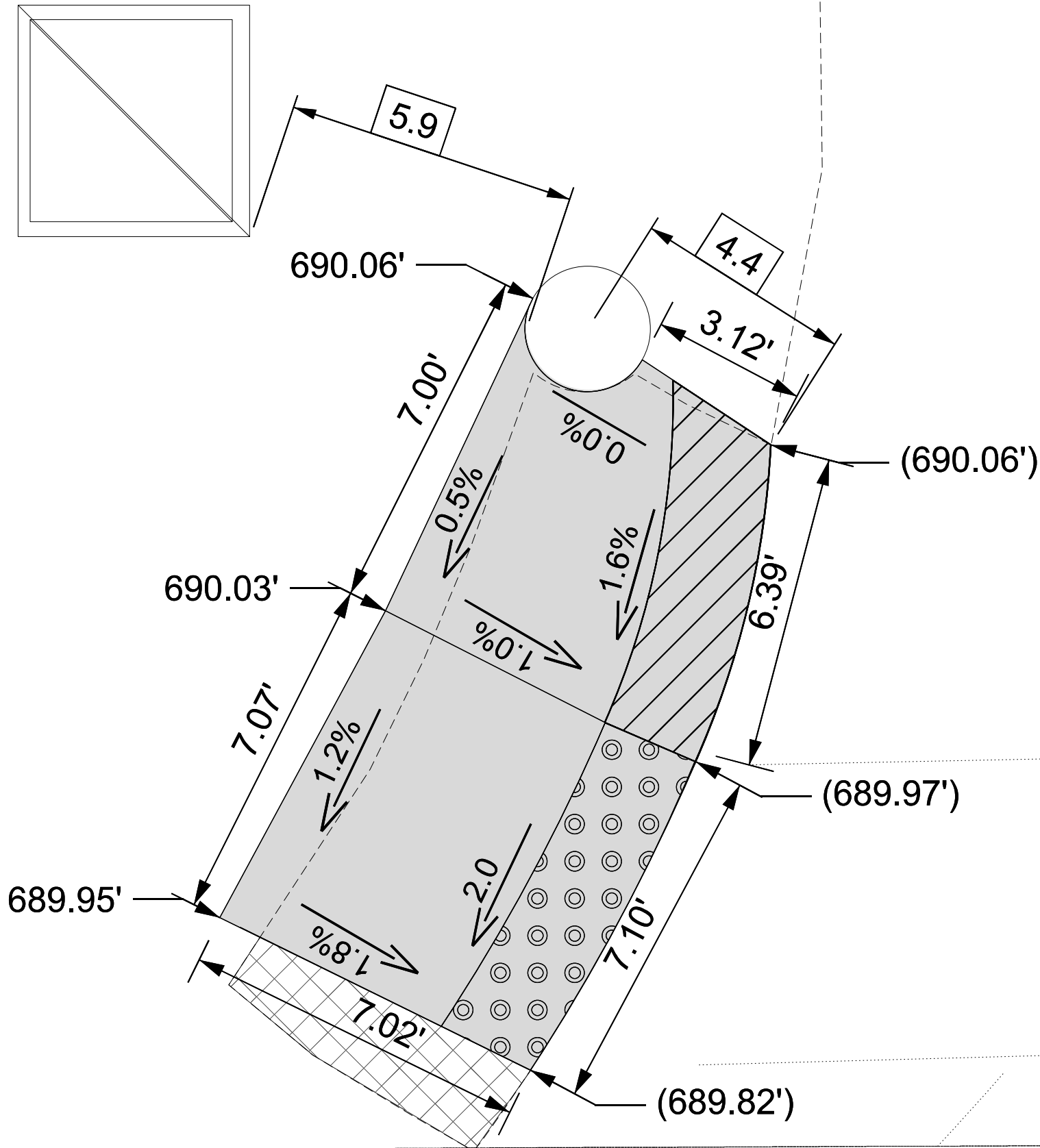
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SHEET 1 OF 6 SHEETS

STA. 69+00.00 TO STA. 99+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 80B08				
ILLINOIS FED. AID PROJECT				

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IL- ROUTE 83
(BUSSE RD.)

HILLSIDE DR.

LEGEND

xx.xx'

EXISTING LENGTH

PROPOSED SIDE CURB

() EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD



PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA IMPROVEMENT DETAILS
NW CORNER AT (HILLSIDE DR. & IL-83 (BUSSE RD.))

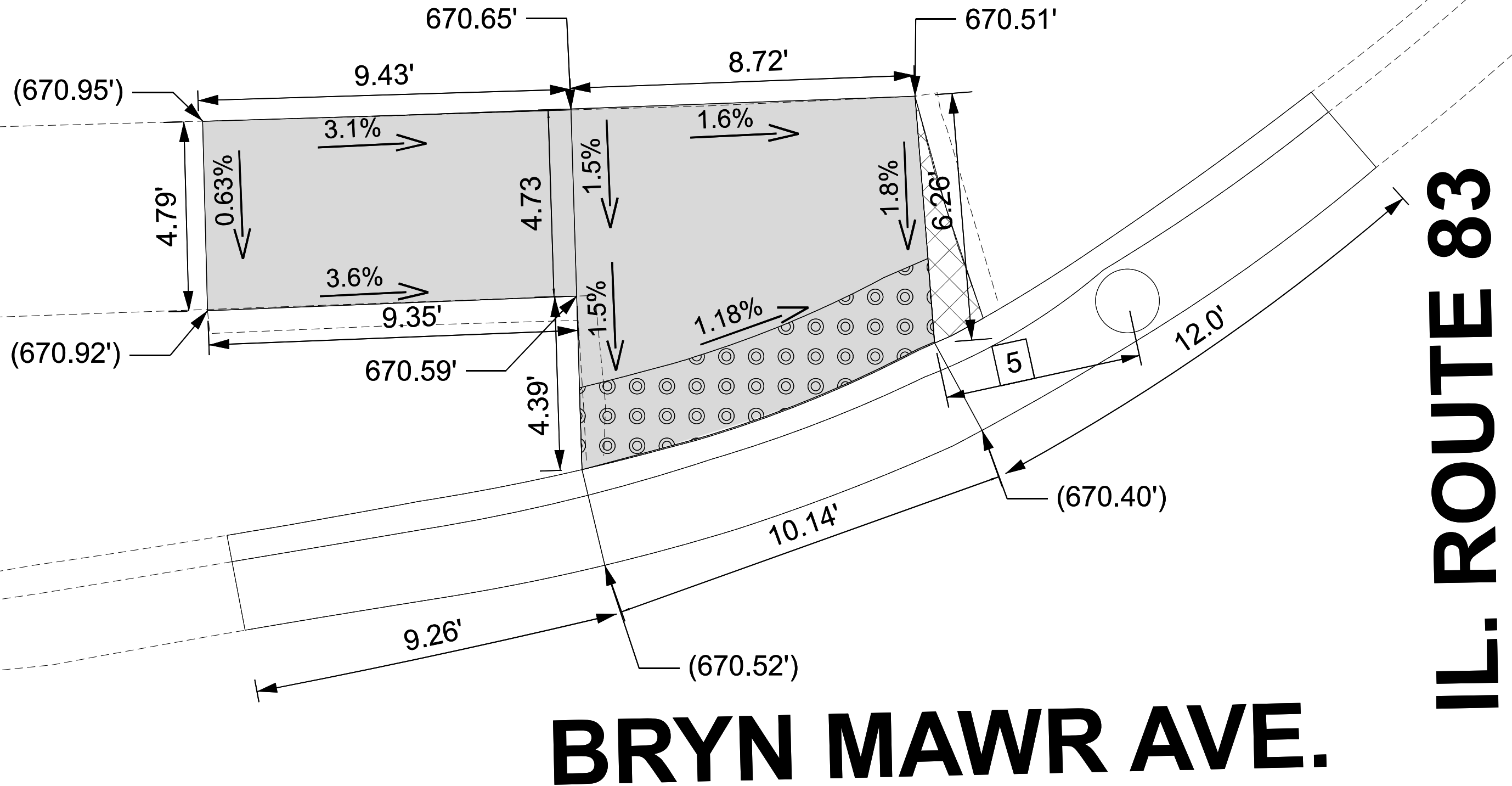
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	10
CONTRACT NO. 80B08				

ILLINOIS FED. AID PROJECT

BENSENVILLE

MODEL: Bryn Mawr Ave [Sheet]
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LEGEND

XX.XX'

EXISTING LENGTH

PROPOSED SIDE CURB

() EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

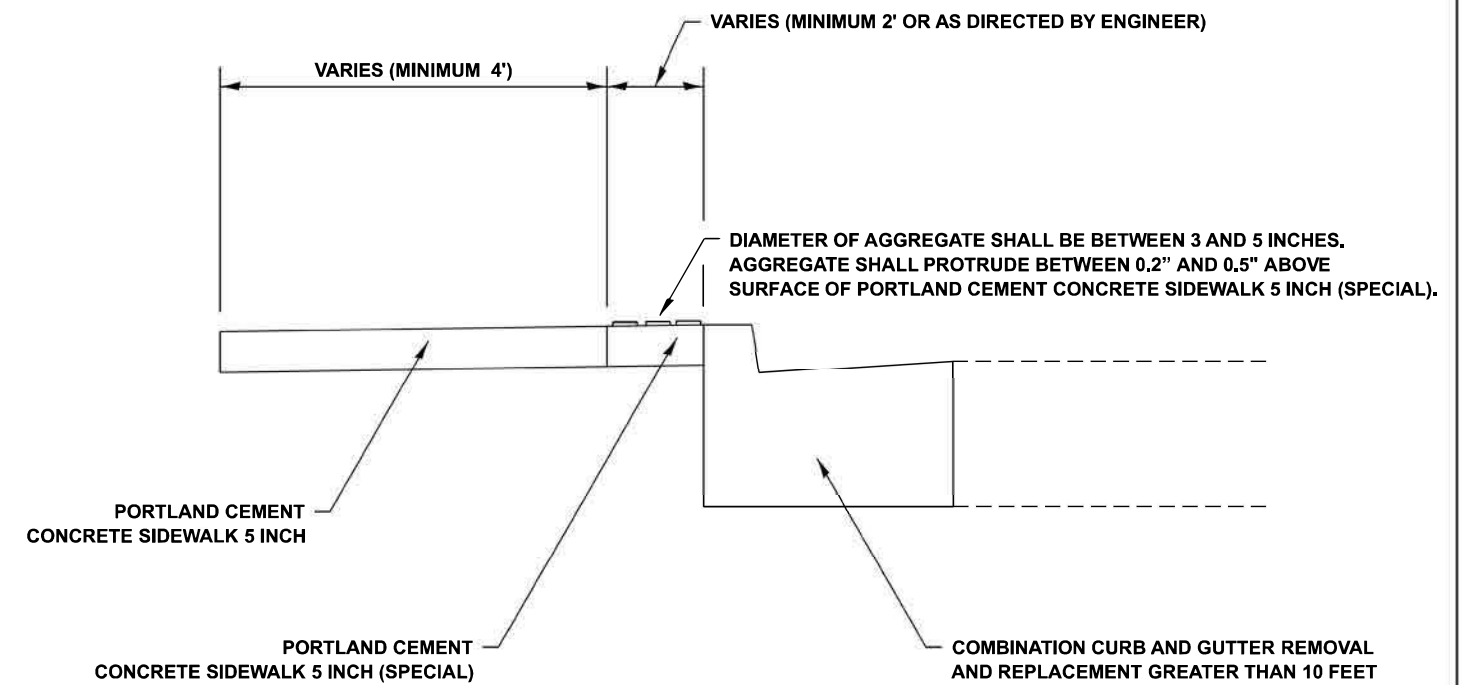
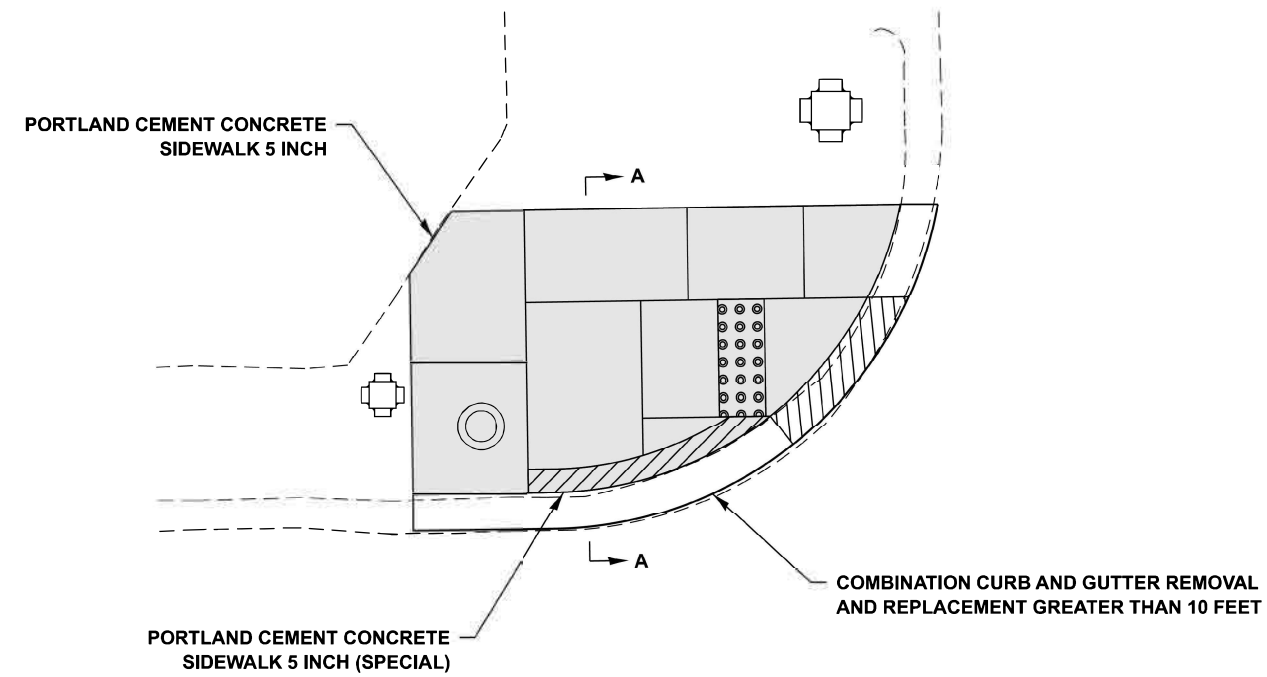
ADA IMPROVEMENT DETAILS
NW CORNER AT (BRYN MAWR AVE & IL-83 (BUSSE RD.))

SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	11
CONTRACT NO. 80B08				

ILLINOIS FED. AID PROJECT

BENSENVILLE



MODEL: ADA- Special [Sheet]
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USER NAME = Baraa Alsmadi	DESIGNED =	REVISED =
	DRAWN =	REVISED =
	CHECKED =	REVISED =
PLOT DATE = 1/6/2026	DATE =	REVISED =

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**DETAIL FOR PORTAND CEMENT CONCRETE SIDEWALK 5 INCH (SPECIAL)
NW CORNER AT (HILLSIDE DR. & IL-83 (BUSSE RD)**

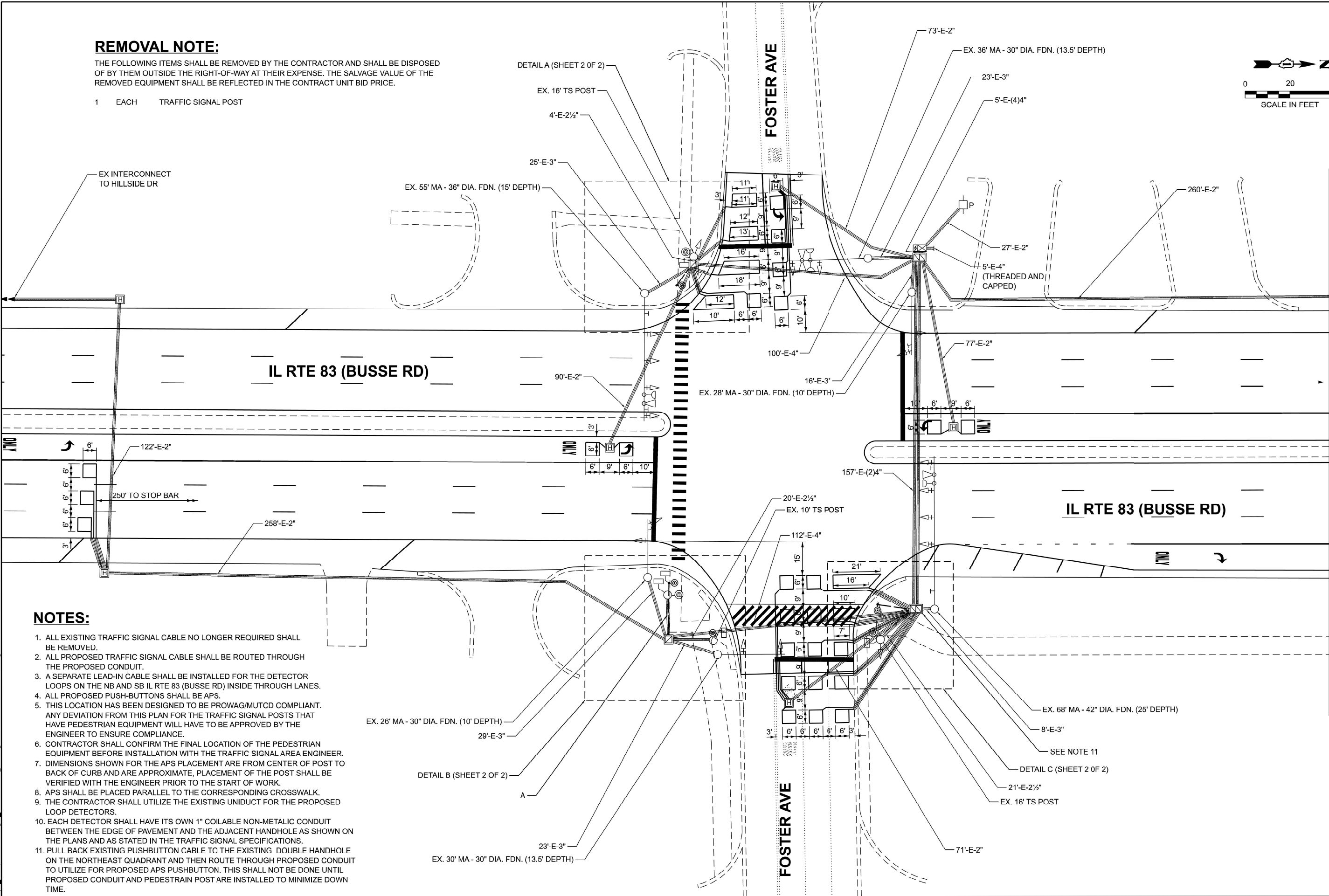
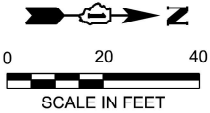
SCALE:	SHEET 1	OF 6	SHEETS	STA.	TO STA.
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	11A
CONTRACT NO. 80B08				
ILLINOIS		FED. AID PROJECT		

REMOVAL NOTE:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT UNIT BID PRICE.

1 EACH TRAFFIC SIGNAL POST



NOTES:

1. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.
2. ALL PROPOSED TRAFFIC SIGNAL CABLE SHALL BE ROUTED THROUGH THE PROPOSED CONDUIT.
3. A SEPARATE LEAD-IN CABLE SHALL BE INSTALLED FOR THE DETECTOR LOOPS ON THE NB AND SB IL RTE 83 (BUSSE RD) INSIDE THROUGH LANES.
4. ALL PROPOSED PUSH-BUTTONS SHALL BE APS.
5. THIS LOCATION HAS BEEN DESIGNED TO BE PROWAG/MUTCD COMPLIANT. ANY DEVIATION FROM THIS PLAN FOR THE TRAFFIC SIGNAL POSTS THAT HAVE PEDESTRIAN EQUIPMENT WILL HAVE TO BE APPROVED BY THE ENGINEER TO ENSURE COMPLIANCE.
6. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER.
7. DIMENSIONS SHOWN FOR THE APS PLACEMENT ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE, PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK.
8. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
9. THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.
10. EACH DETECTOR SHALL HAVE ITS OWN 1" COILABLE NON-METALIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
11. PULL BACK EXISTING PUSHBUTTON CABLE TO THE EXISTING DOUBLE HANDHOLE ON THE NORTHEAST QUADRANT AND THEN ROUTE THROUGH PROPOSED CONDUIT TO UTILIZE FOR PROPOSED APS PUSHBUTTON. THIS SHALL NOT BE DONE UNTIL PROPOSED CONDUIT AND PEDESTRAIN POST ARE INSTALLED TO MINIMIZE DOWN TIME.

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	USER NAME = Baraa.Alsyadi	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
	PLOT DATE = 1/20/2026	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

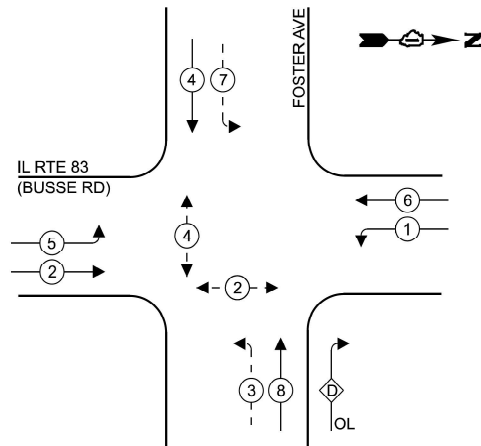
TRAFFIC SIGNAL MODERNIZATION PLAN
IL RTE 83 (BUSSE RD) AND FOSTER AVE - SHEET 1 OF 2

SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	12
CONTRACT NO. 80B08				
ILLINOIS FED. AID PROJECT				

TS 590
ECON 111

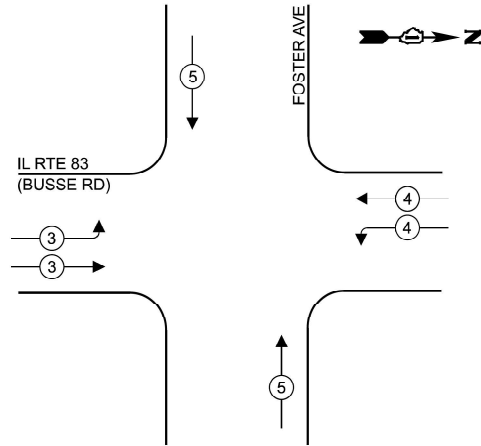
EXISTING
CONTROLLER PHASE DIAGRAM



RIGHT TURN OVERLAP
PHASE DESIGNATION

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
D	=	8 + 1

EXISTING
EMERGENCY VEHICLE
PREEMPTION PHASE DIAGRAM

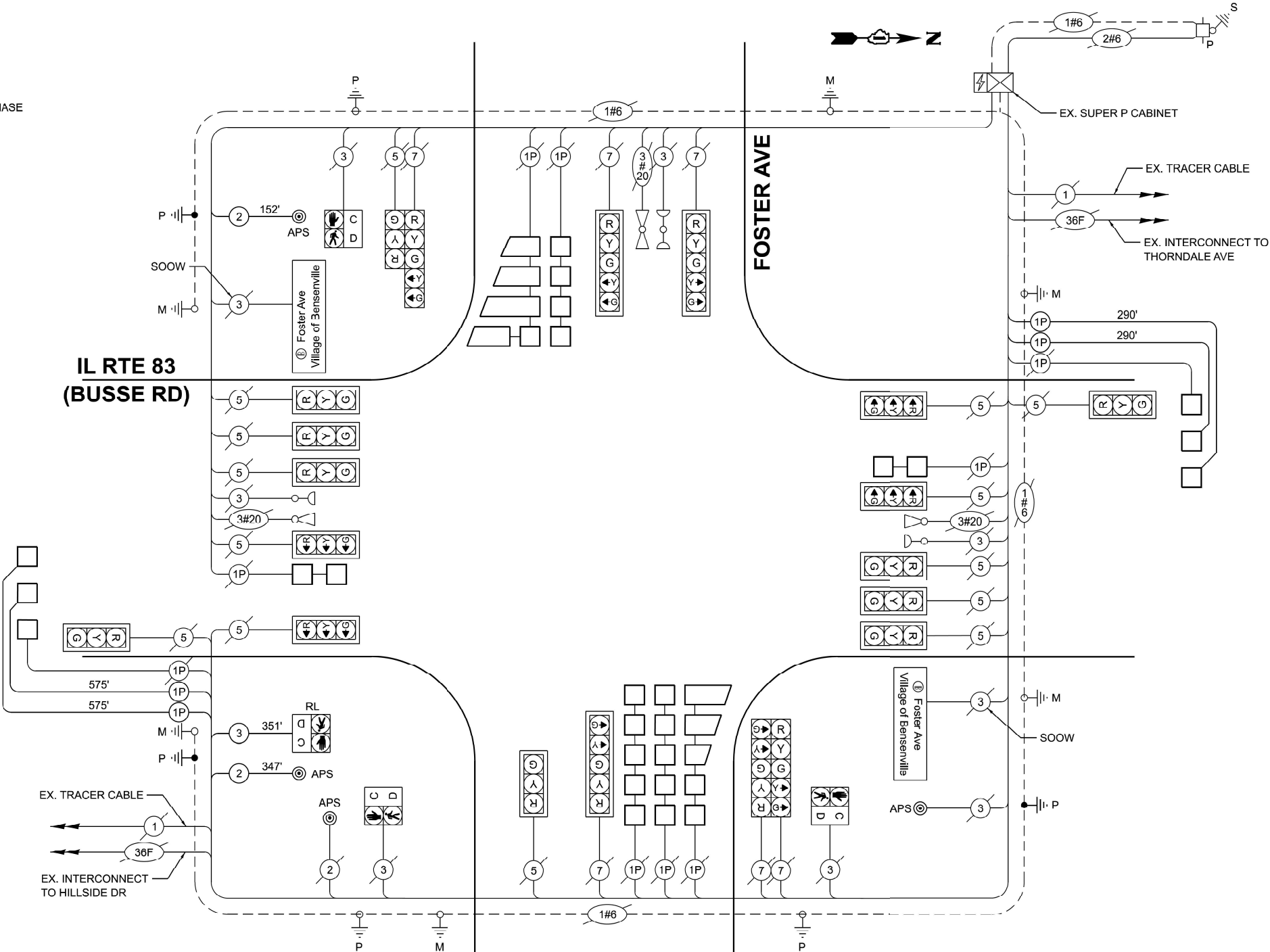


TRAFFIC SIGNAL ELECTRICAL
SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	14	11	154
4-SECTION	-	14	-
5-SECTION	6	13	78
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	4	15	60
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			467
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	2	120	240
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			1312

ENERGY COSTS TO:	
ILLINOIS DEPARTMENT OF TRANSPORTATION	
201 W. CENTER CT, SCHAUMBURG, IL 60196	
ENERGY SUPPLY:	
CONTACT: ANTONIO RIOS	
PHONE: (630)-696-6855	
COMPANY: COMED GLENBARD REGION	
ACCOUNT NUMBER: XXXXXXXXXX	
METER NUMBER:	

IL RTE 83
(BUSSE RD)



CABLE PLAN
(NOT TO SCALE)

PAY ITEM DESCRIPTION	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	54
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	499
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	351
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1PAIR	FOOT	1730
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	87
DRILL EXISTING HANDHOLE	EACH	3
INDUCTIVE LOOP DETECTOR	EACH	4
DETECTOR LOOP, TYPE I	FOOT	1397
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	1
REMOVE EXISTING ELECTRIC CABLE FROM CONDUIT	FOOT	695
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	21
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	2
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	12
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, EXISTING CONTROLLER PHASE DIAGRAM AND
EXISTING EMERGENCY VEHICLE PREEMPTION PHASE DIAGRAM
IL RTE 83 (BUSSE RD) AND FOSTER AVE

SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

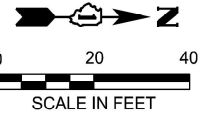
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	14
CONTRACT NO. 80B08				

ILLINOIS FED. AID PROJECT

TS 590
ECON 111

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT UNIT BID PRICE.

HILLSIDE DR

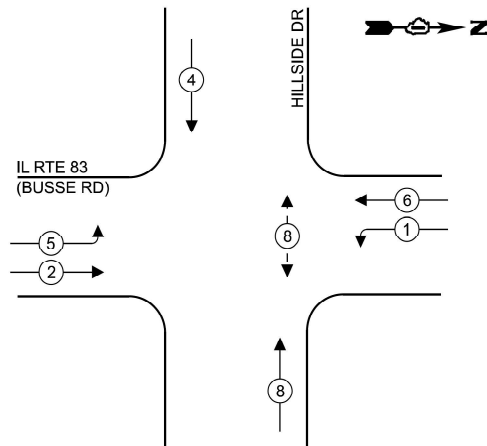


1. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.
2. ALL PROPOSED TRAFFIC SIGNAL CABLE SHALL BE ROUTED THROUGH THE PROPOSED CONDUIT.
3. A SEPARATE LEAD-IN CABLE SHALL BE INSTALLED FOR THE DETECTOR LOOPS ON THE NB AND SB I RTE 83 (BUSSE RD) INSIDE THROUGH LANES.
4. ALL PROPOSED PUSH-BUTTONS SHALL BE APS.
5. THIS LOCATION HAS BEEN DESIGNED TO BE PROWAG/MUTCD COMPLIANT. ANY DEVIATION FROM THIS PLAN FOR THE TRAFFIC SIGNAL POSTS THAT HAVE PEDESTRIAN EQUIPMENT WILL HAVE TO BE APPROVED BY THE ENGINEER TO ENSURE COMPLIANCE.
6. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL AREA ENGINEER.
7. DIMENSIONS SHOWN FOR THE APS PLACEMENT ARE FROM CENTER OF POST TO BACK OF CURB AND ARE APPROXIMATE, PLACEMENT OF THE POST SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE START OF WORK.
8. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
9. THE CONTRACTOR SHALL UTILIZE THE EXISTING UNIDUCT FOR THE PROPOSED LOOP DETECTORS.
10. EACH DETECTOR SHALL HAVE ITS OWN 1" COILABLE NON-METALIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

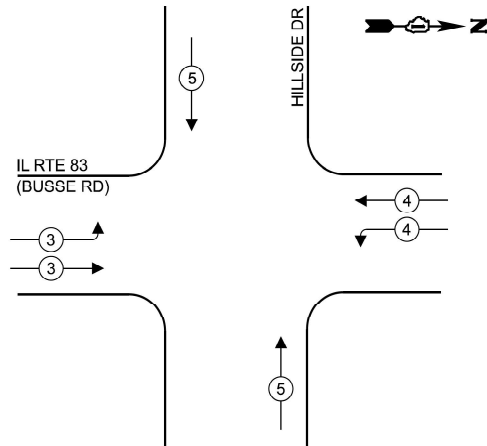
TS 600
ECON 111

USER NAME = Baraa.Alsmadi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODERNIZATION PLAN TE 83 (BUSSE RD) AND HILLSIDE DR				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED -						334	2025-1096-RS	DUPAGE	28	15
	CHECKED -	REVISED -						CONTRACT NO. 80B08				
PLOT DATE = 1/20/2026	DATE -	REVISED -		SCALE:	SHEET 1	OF 5	SHEETS	STA.	TO STA.			
				ILLINOIS FED. AID PROJECT								

EXISTING
CONTROLLER PHASE DIAGRAM



EXISTING
EMERGENCY VEHICLE
PREEMPTION PHASE DIAGRAM



TRAFFIC SIGNAL ELECTRICAL
SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT	WATTAGE	TOTAL
SIGNAL HEAD 1 OR 3-SECTION	21	11	231	
4-SECTION	-	14	-	
5-SECTION	-	13	-	
PROGRAMMABLE 3-SECTION	-	22	-	
4-SECTION	-	32	-	
5-SECTION	-	28	-	
PEDESTRIAN SIGNAL	2	15	30	
CONTROLLER	1	150	150	
MASTER CONTROLLER	-	100	-	
UPS	1	25	25	
DETECTION RADAR OR VIDEO	-	20	-	
BLANK-OUT SIGN	-	25	-	
NETWORK SWITCH II OR III	-	35	-	
CELLULAR MODEM	-	15	-	
TOTAL UPS SIZING			436	
UPS CHARGING	1	225	225	
BATTERY HEATER MAT	1	180	180	
CABINET HEATER	1	200	200	
FLASHER	-	15	-	
LED STREET NAME SIGN	2	120	240	
LUMINAIRE	-	240	-	
TOTAL SERVICE WIRE SIZING			1281	

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION

201 W. CENTER CT,
SCHAUMBURG, IL 60196

ENERGY SUPPLY:

CONTACT: ANTONIO RIOS

PHONE: (630)-696-6855

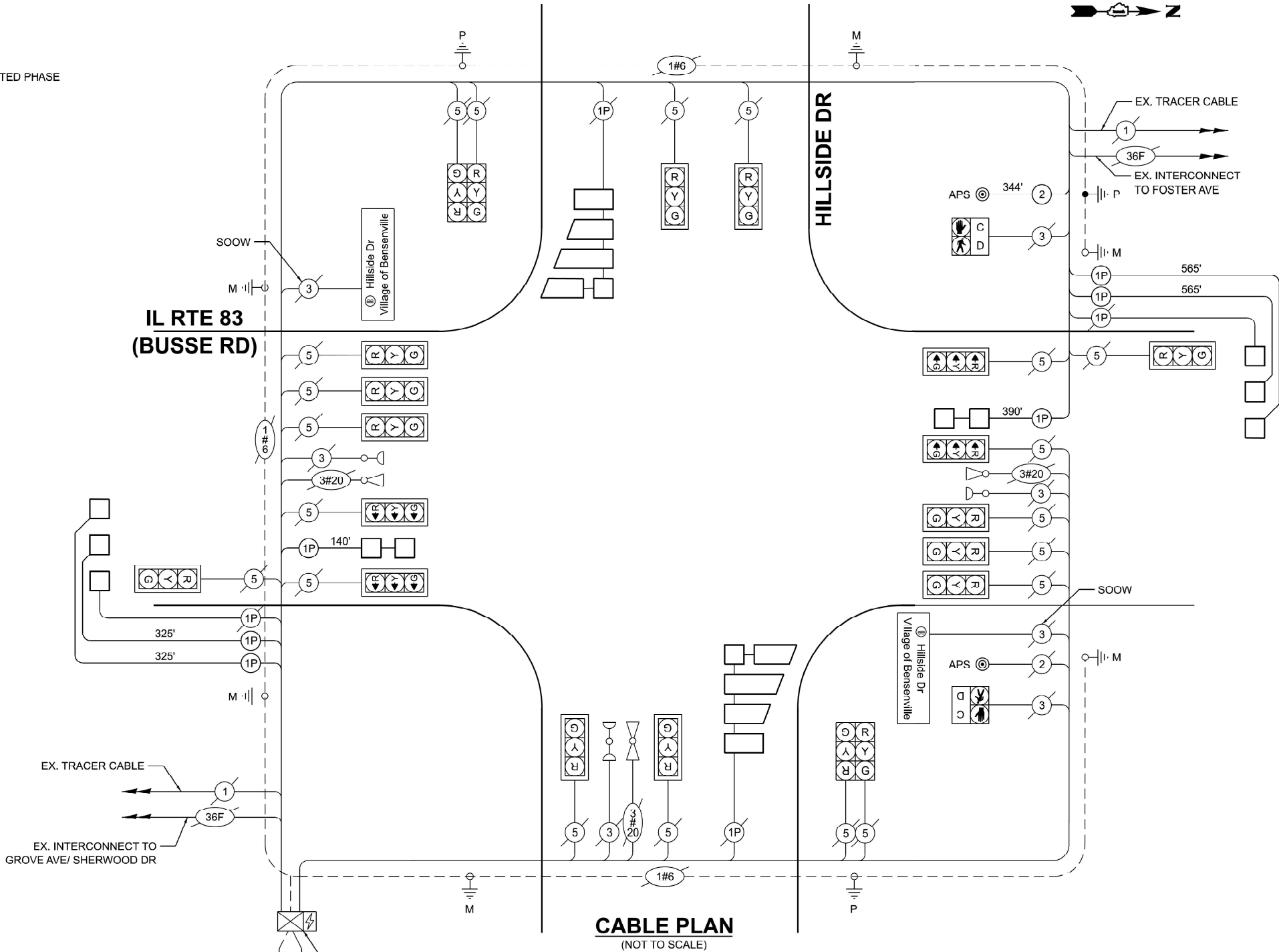
COMPANY: COMED GLENBARD REGION

ACCOUNT NUMBER: XXXXXXXXXX

METER NUMBER:

LEGEND

- ← (⊛) → PROTECTED PHASE
- ← (⊛) - PROTECTED/PERMITTED PHASE
- ← (⊛) → PEDESTRIAN PHASE



CABLE PLAN

(NOT TO SCALE)

PAY ITEM DESCRIPTION	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	139
HEAVY-DUTY HANDHOLE	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	344
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1PAIR	FOOT	2310
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	24
DRILL EXISTING HEAVY DUTY HANDHOLE	EACH	3
INDUCTIVE LOOP DETECTOR	EACH	4
DETECTOR LOOP, TYPE I	FOOT	798
REMOVE EXISTING ELECTRIC CABLE FROM CONDUIT	FOOT	572
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	2
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	4
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, EXISTING CONTROLLER PHASE DIAGRAM AND
EXISTING EMERGENCY VEHICLE PREEMPTION PHASE DIAGRAM
IL RTE 83 (BUSSE RD) AND HILLSIDE DR

SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	16
CONTRACT NO. 80B08				

TS 600
ECON 111

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FILE NAME: c:\paw\work\illinois\gov\baraa\almsadi\illinois\gov\1129386\0116125-sht-TS.dgn



1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

① SUB-BASE GRANULAR MATERIAL	⑥ FRAME AND LID (SEE NOTES)
② EXISTING PAVEMENT	⑦ CLASS PP-2* CONCRETE
③ 36 (900) DIAMETER METAL PLATE	⑧ PROPOSED HMA SURFACE COURSE
④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX	⑨ PROPOSED HMA BINDER COURSE
⑤ EXISTING STRUCTURE	

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

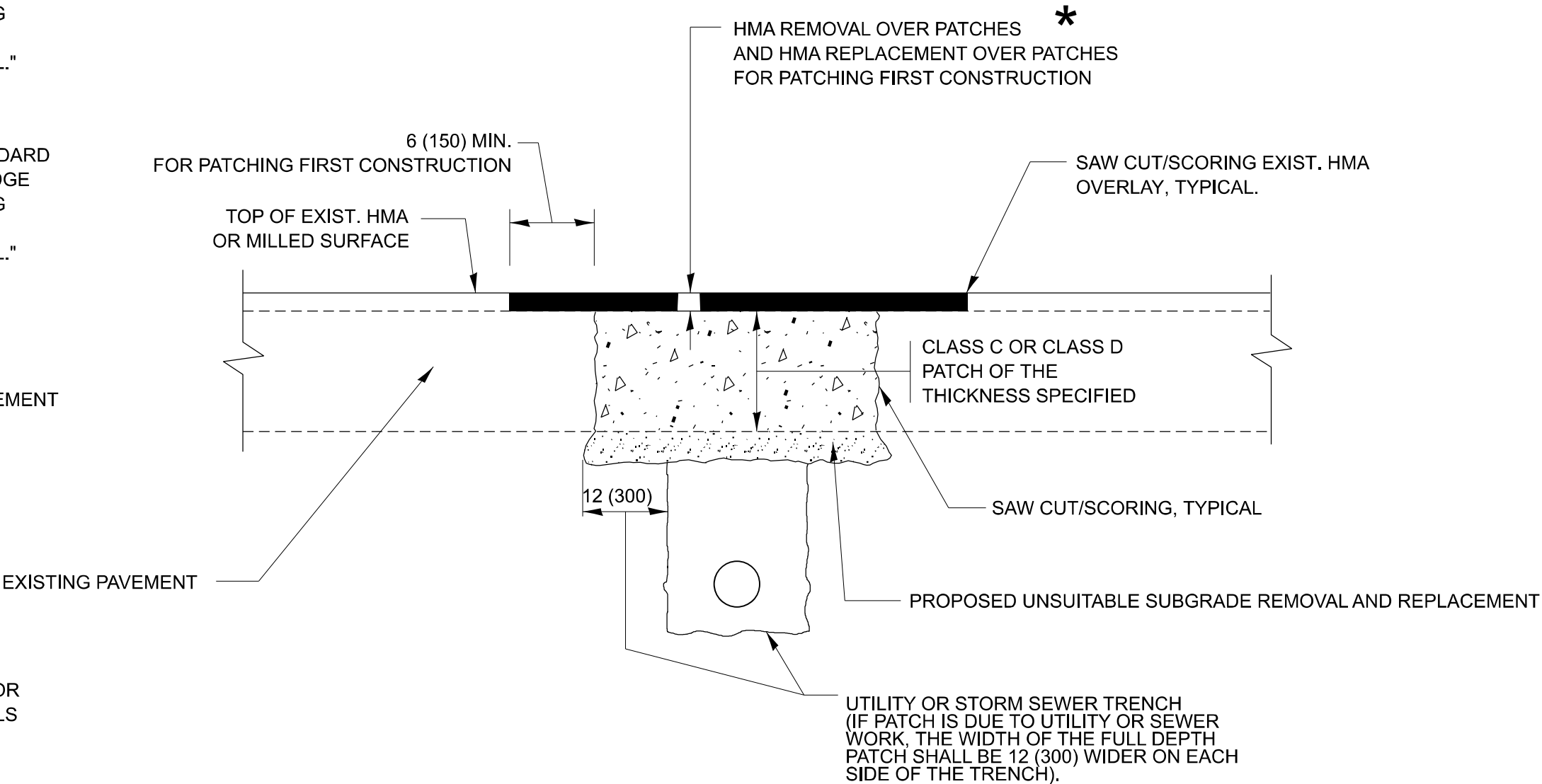
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					334	2025-1096-RS	DUPAGE	28	17
					BD600-03 (BD-08)			CONTRACT NO. 80B08	
SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.				

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

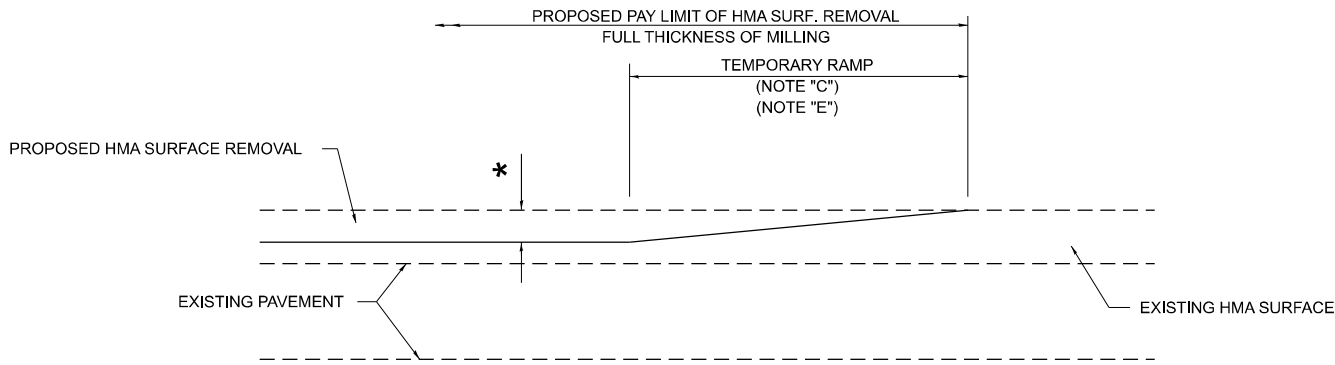
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

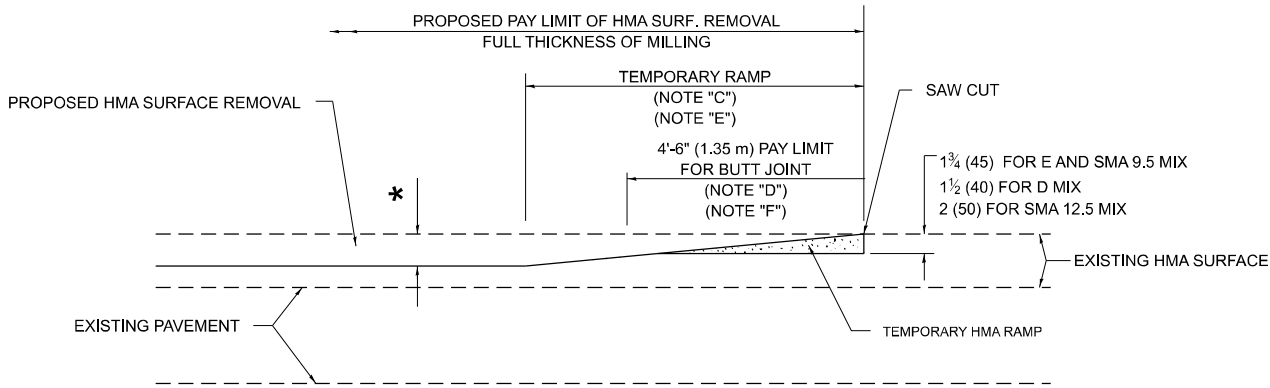
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	USER NAME = Baraa.alsmadi	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 09-04-07						334	2025-1096-RS	DUPAGE	28	18
		CHECKED -	REVISED - K. ENG 10-27-08						BD400-04 (BD-22)		CONTRACT NO. 80B08		
	PLOT DATE = 12/5/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		ILLINOIS FED. AID PROJECT								
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.			



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

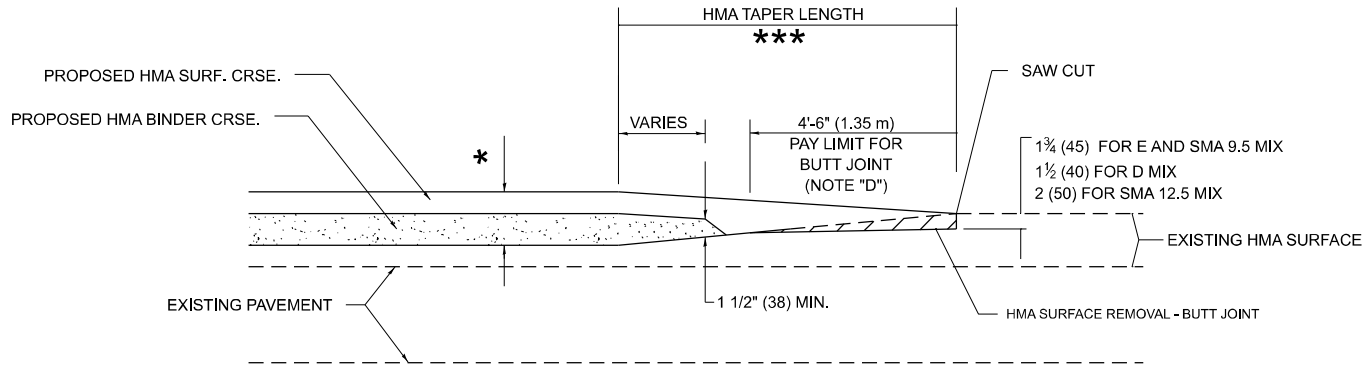
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

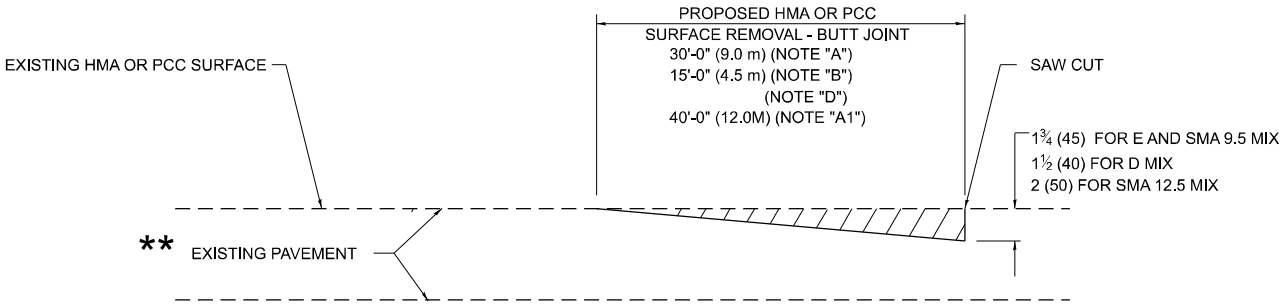
OPTION 2

TYPICAL TEMPORARY RAMP

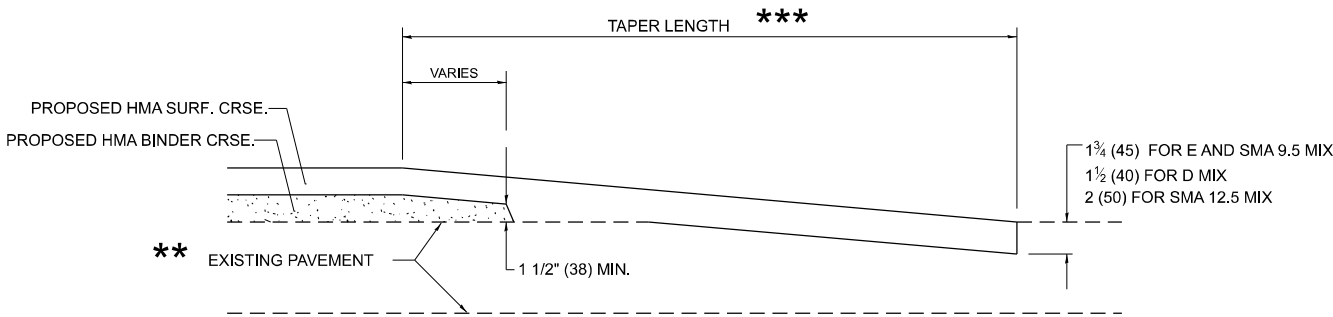


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: BD-32 [Sheet]
FILE NAME: c:\pav_work\pav\illinois.gov_baraa.alsmadi\129386\D116125-shd-DistSds.dgn

USER NAME = Baraa.alsmadi	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN - M. GOMEZ 04-06-01	REVISED - R. BORO 01-01-07
	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 12/5/2025	DATE - 06-13-90	

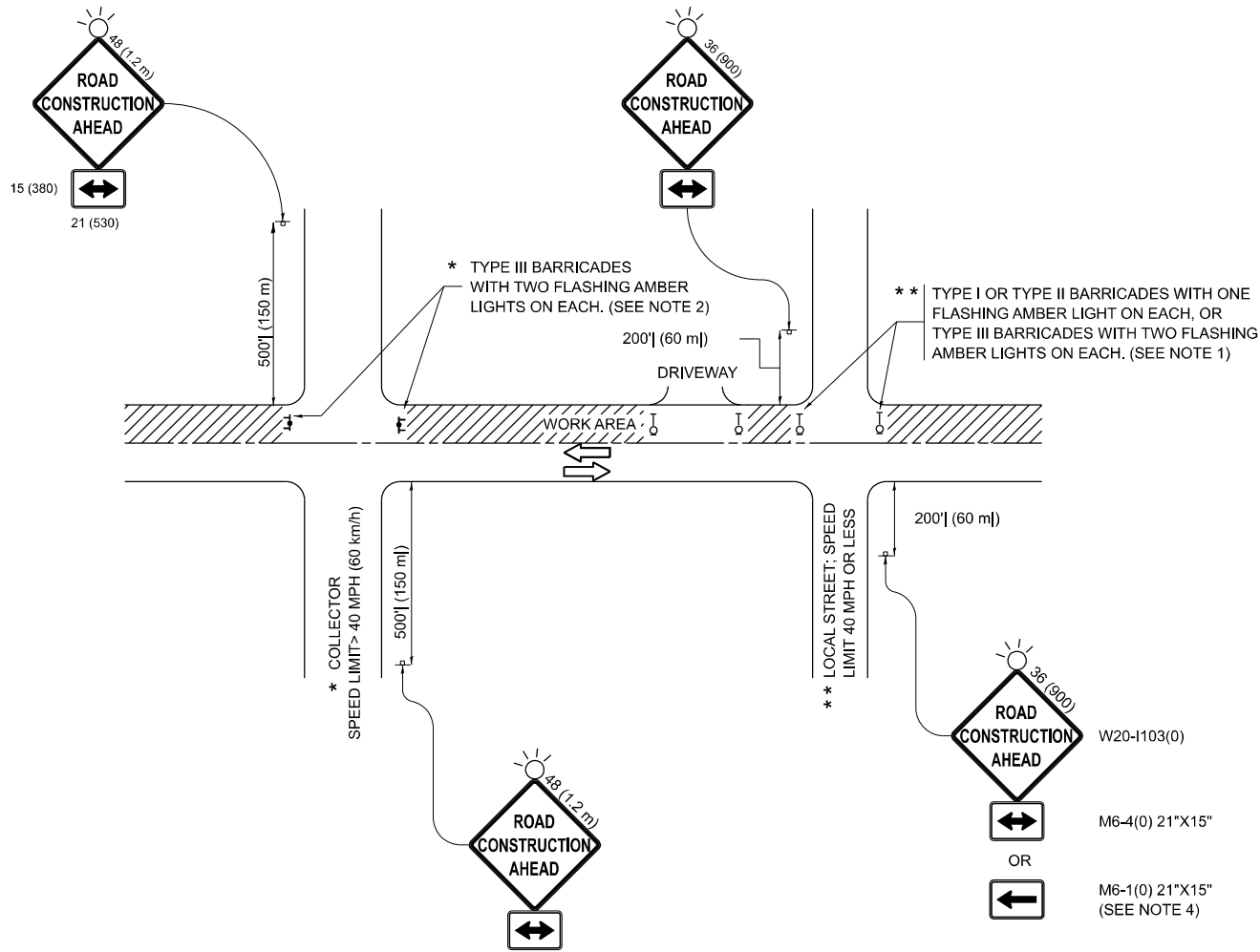
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	20
BD400-05 BD-32		CONTRACT NO. 80B08		
		ILLINOIS	FED. AID PROJECT	

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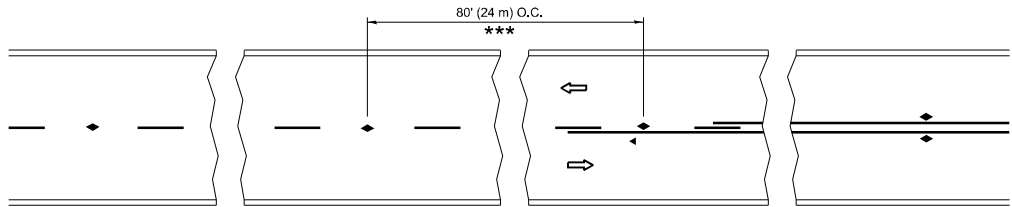
NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

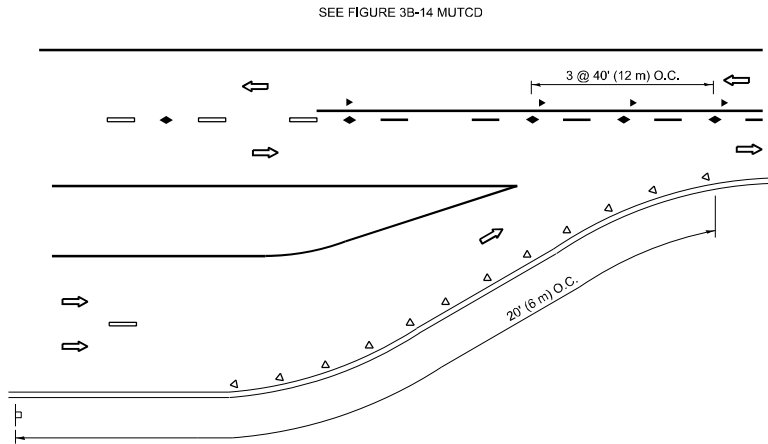
All dimensions are in inches (millimeters)
unless otherwise shown.

	USER NAME = Baraa.alsmadi	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. SCHUETZE 07-01-13						334	2025-1096-RS	DUPAGE	28	21
		CHECKED -	REVISED - A. SCHUETZE 09-15-06		TC-10				CONTRACT NO. 80B08				
	PLOT DATE = 12/5/2025	DATE - 06-89	REVISED - D. SENDERAK 05-03-24		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

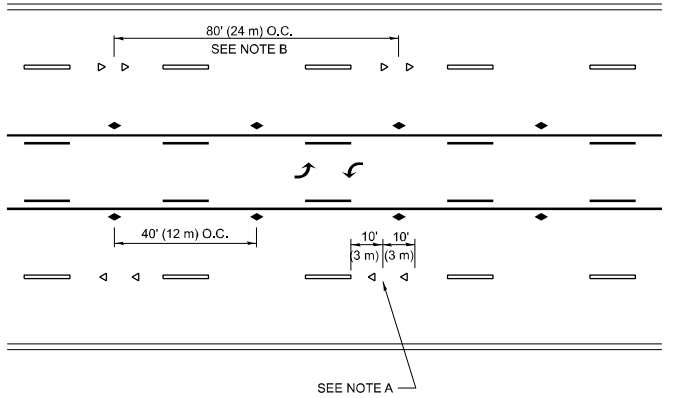
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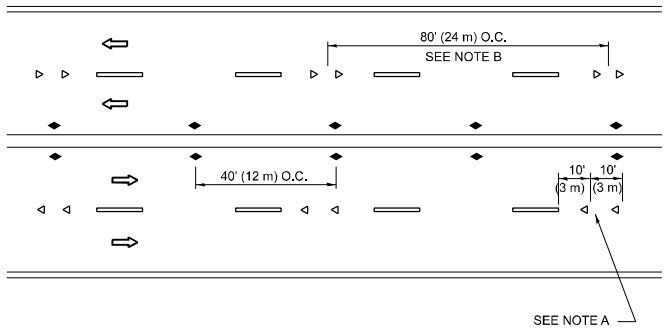
TWO-LANE/TWO-WAY



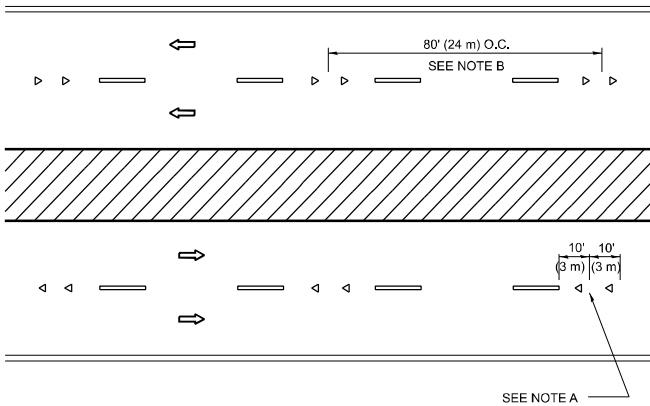
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

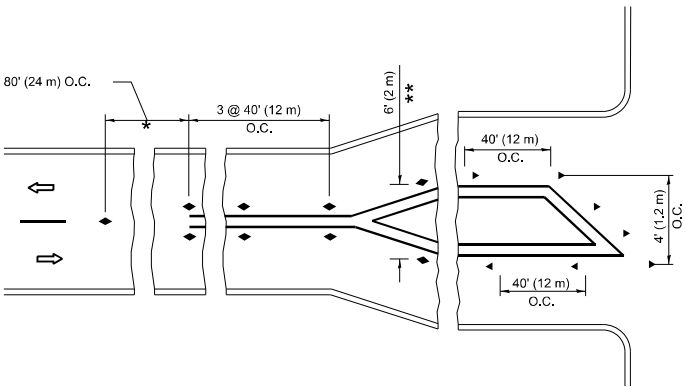
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

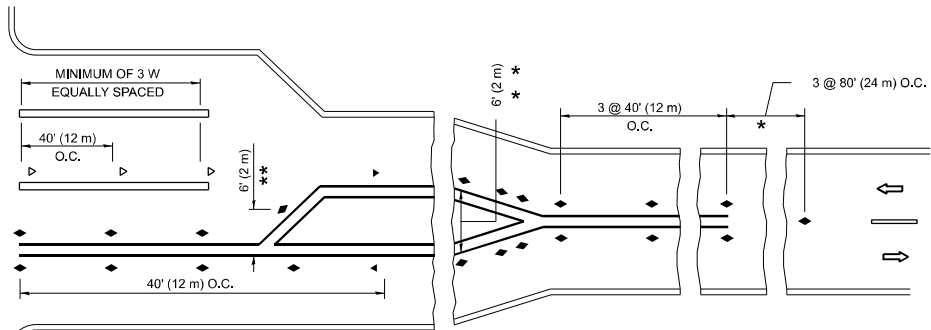
- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES



- * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = Baraa.alsmadi	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 12/5/2025	DATE -	REVISED - C. JUCIUS 07-01-13

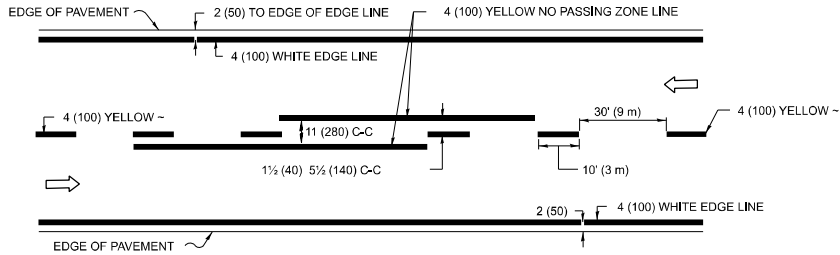
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

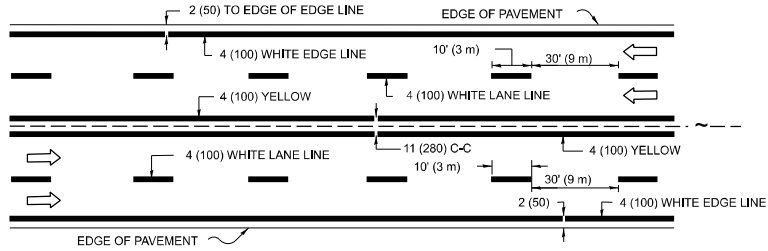
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11		CONTRACT NO. 80B08		
ILLINOIS		FED. AID PROJECT		

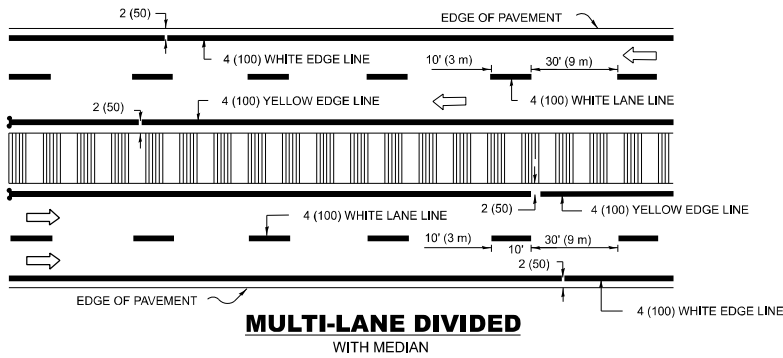
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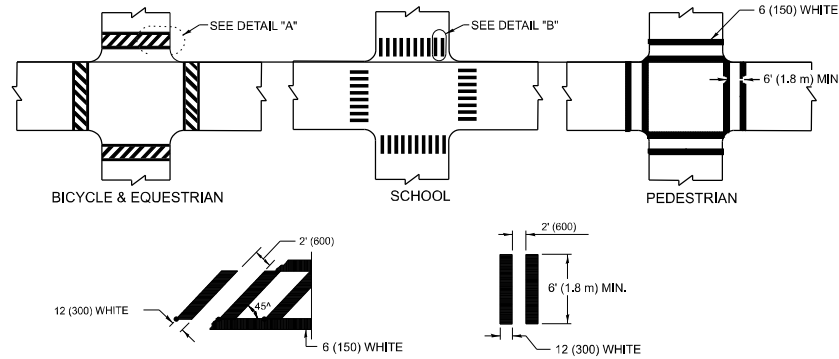
2-LANE ROADWAY



MULTI-LANE UNDIVIDED



TYPICAL LANE AND EDGE LINE MARKING

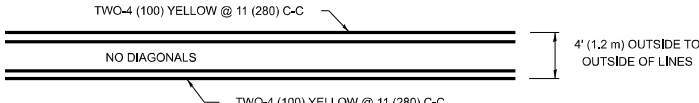


DETAIL "A"

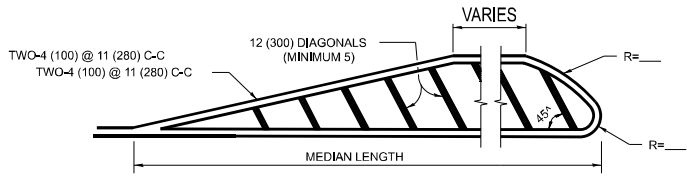
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

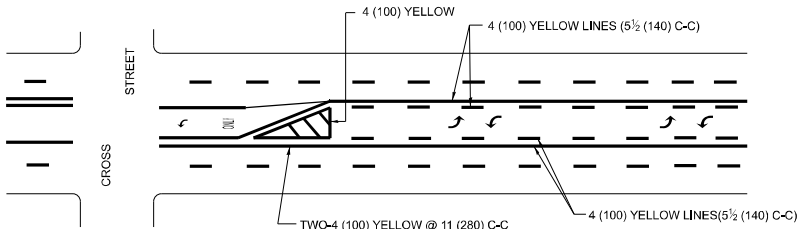


4' (1.2 m) WIDE MEDIANS ONLY

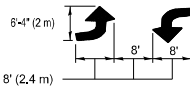


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

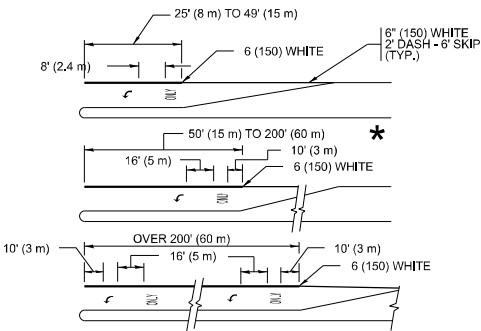


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

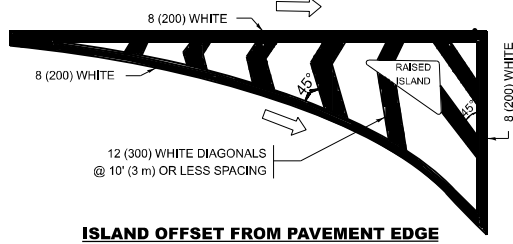


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

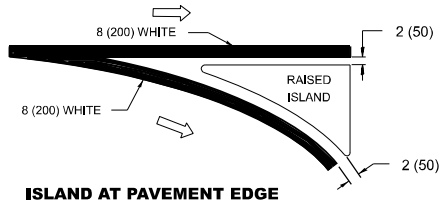
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

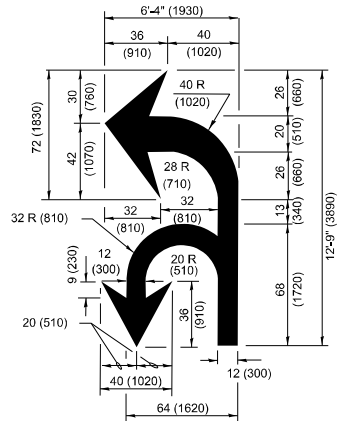


ISLAND OFFSET FROM PAVEMENT EDGE

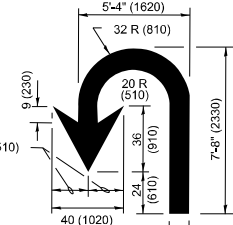


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = Baraa.alsmadi	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 12/5/2025	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	23
TC-13		CONTRACT NO. 80B08		
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

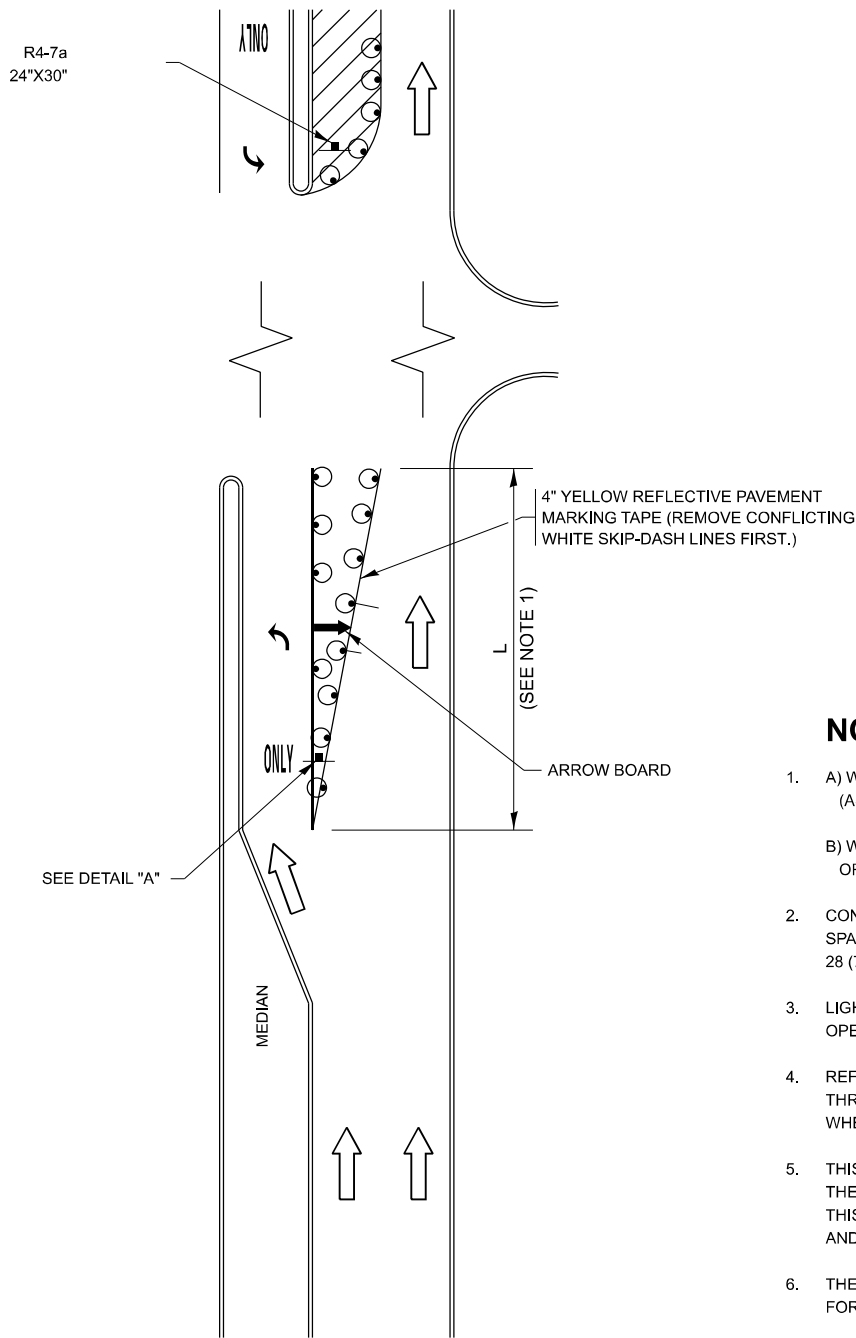
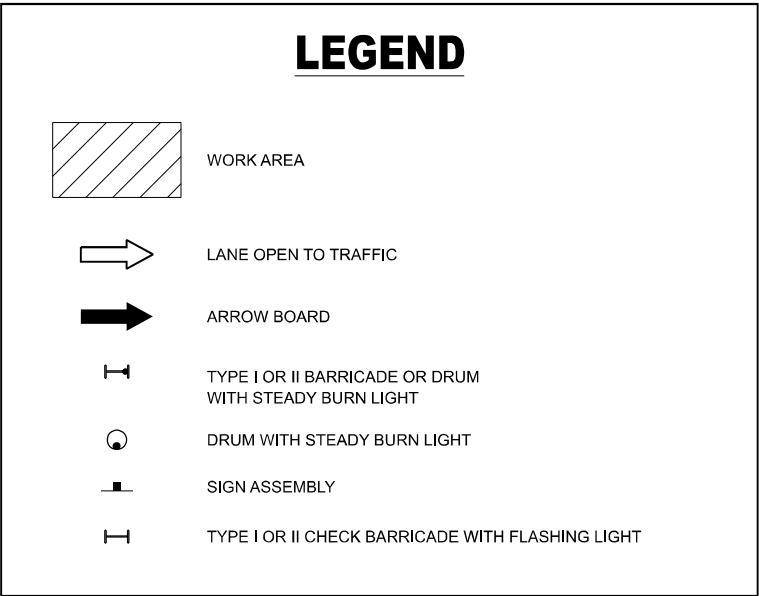


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

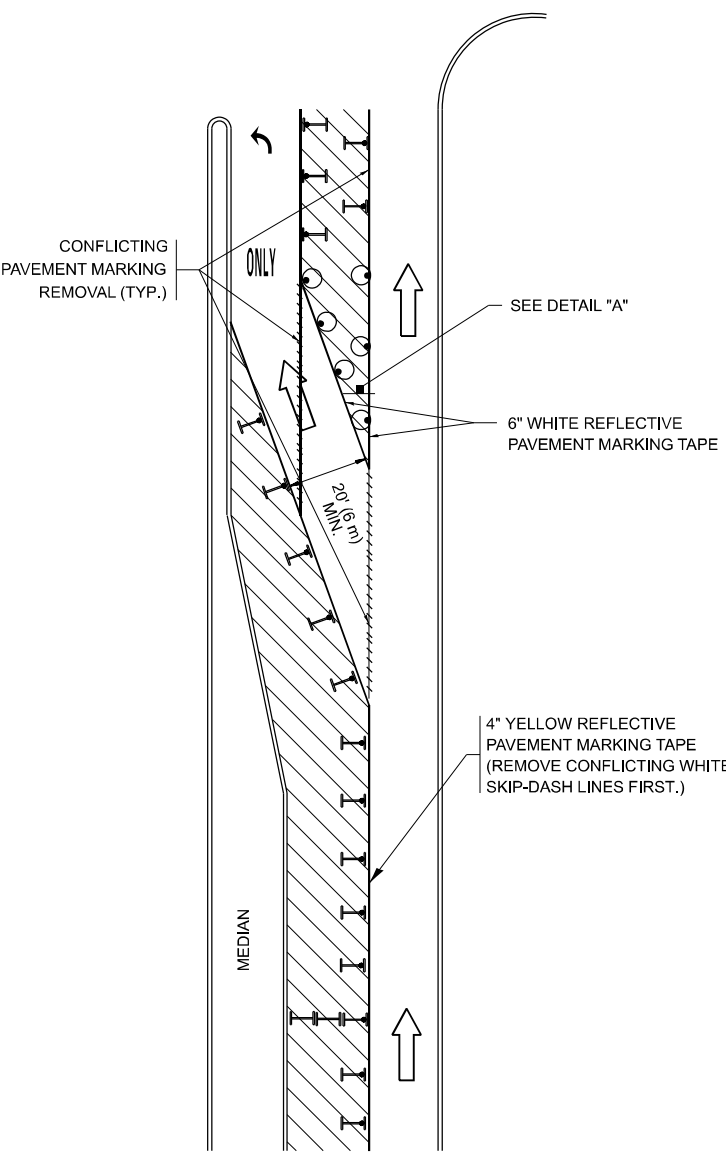
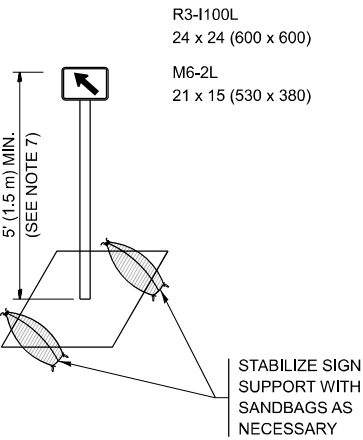


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
CHECKED	- A. HOUSEH 10-12-96
DATE	- T. RAMMACHER 01-06-00

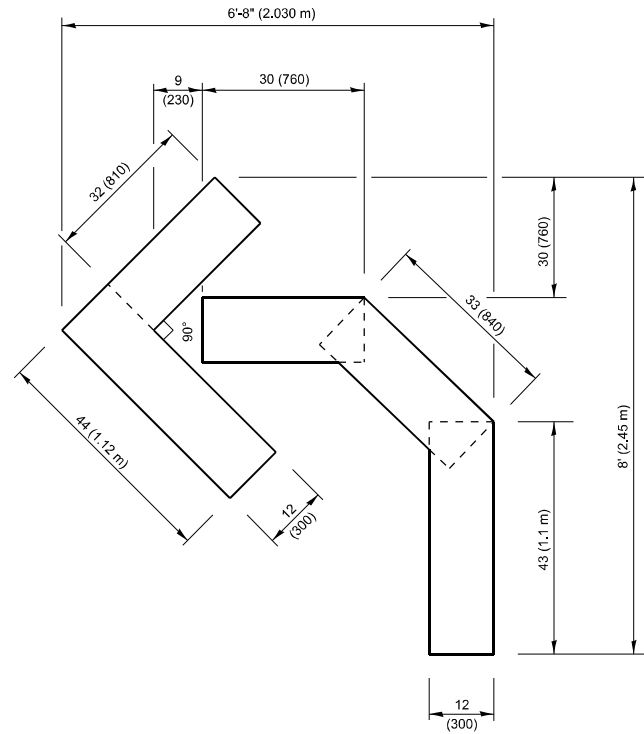
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REVISED	- A. SCHUETZE 07-01-13
REVISED	- A. SCHUETZE 09-15-16
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

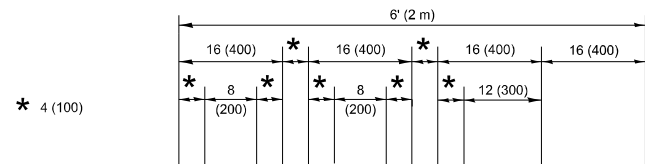
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	24
TC-14		CONTRACT NO. 80B08		
		ILLINOIS	FED. AID PROJECT	



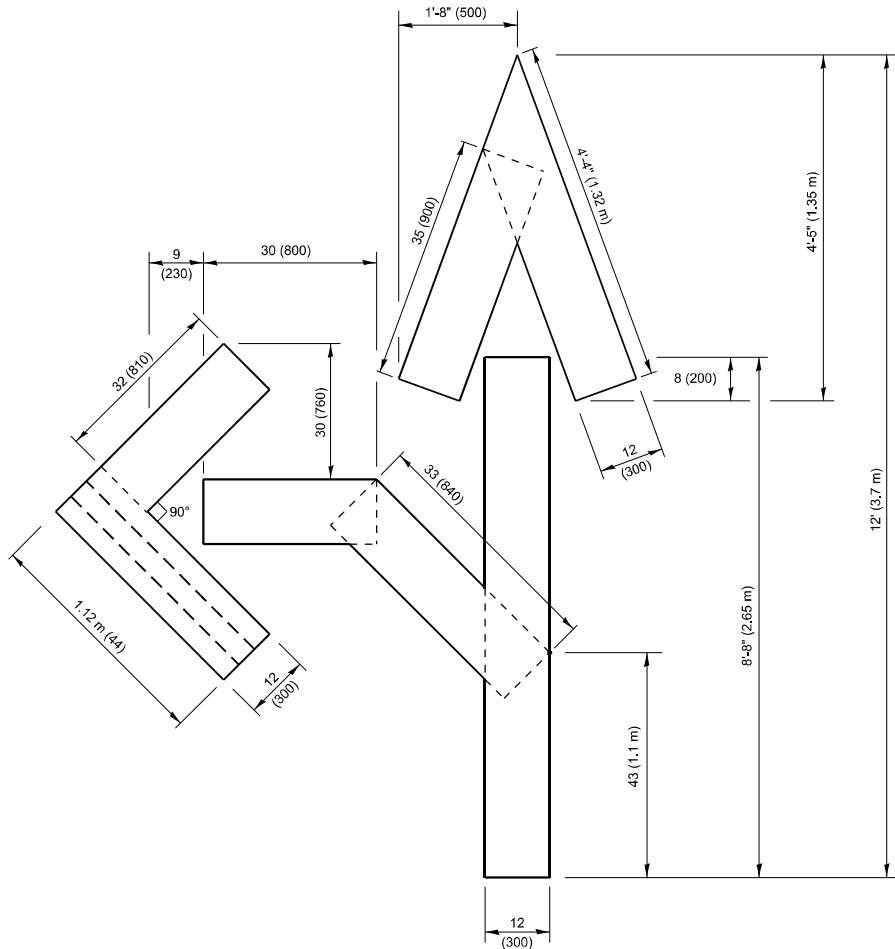
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

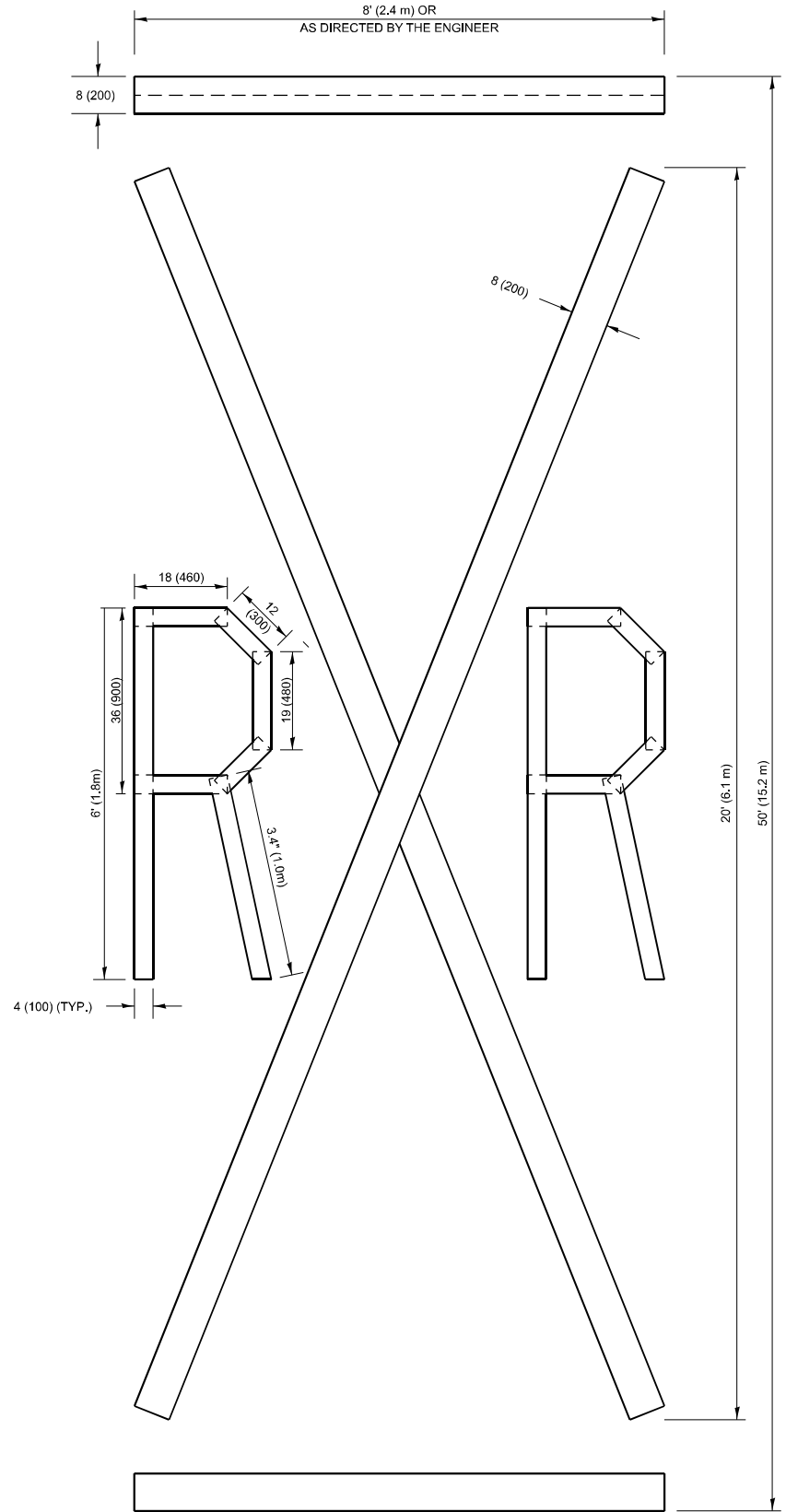


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

MODEL: TC-16 (Sheet)
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USER NAME	= Baraa.alsmadi	DESIGNED	-	REVISED	- T. RAMMACHER 03-02-98
		DRAWN	-	REVISED	- E. GOMEZ 08-28-00
		CHECKED	-	REVISED	- E. GOMEZ 08-28-00
PLOT DATE	= 12/5/2025	DATE	- 09-18-94	REVISED	- A. SCHUETZE 09-15-16

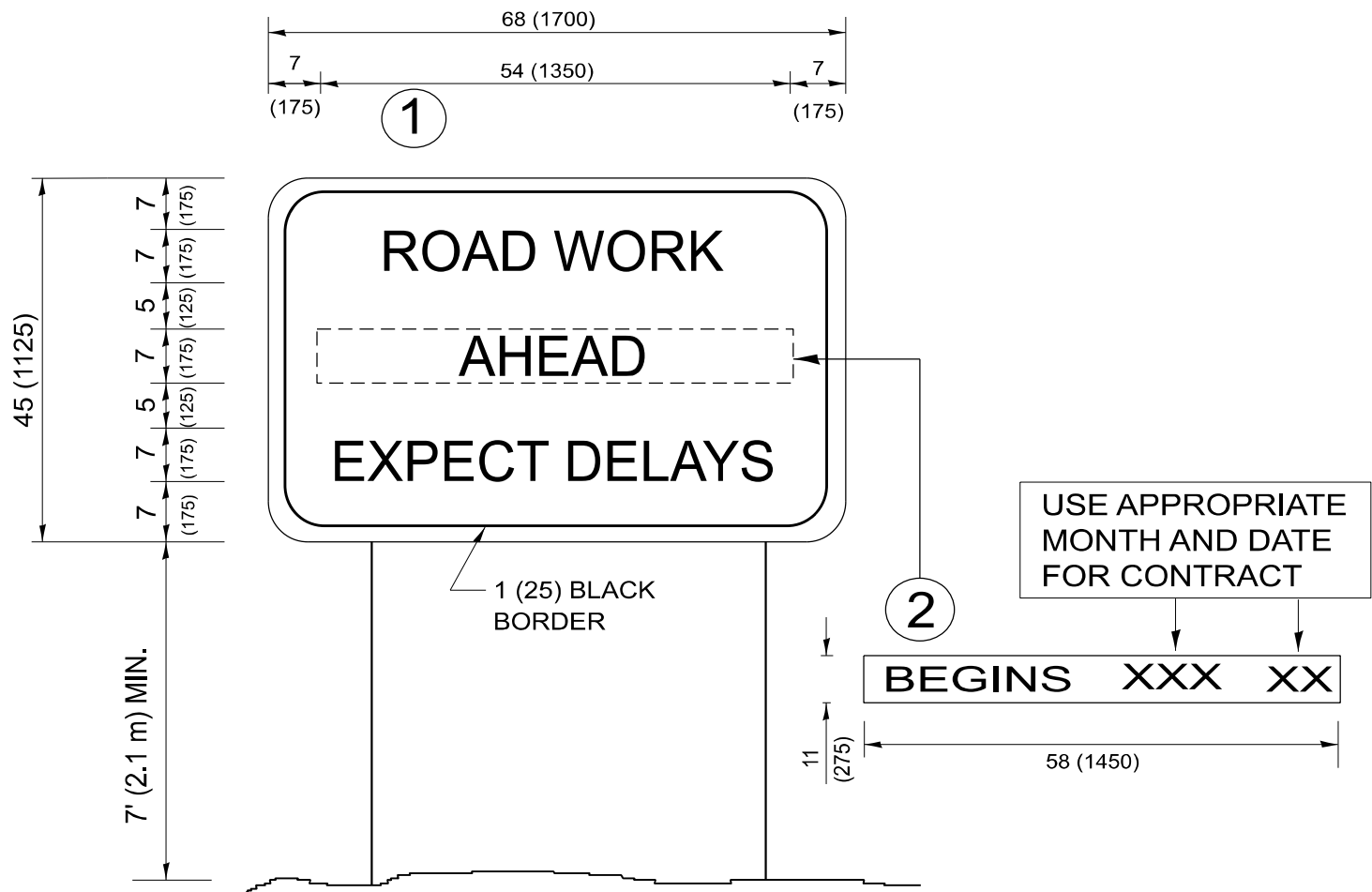
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	25
TC-16		CONTRACT NO. 80B08		
		ILLINOIS	FED. AID PROJECT	

MODEL: TC-22 [Sheet]
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NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = Baraa.alsmadi	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97						334	2025-1096-RS	DUPAGE	28	26
		CHECKED -	REVISED - T. RAMMACHER 02-02-99						TC-22		CONTRACT NO. 80B08		
	PLOT DATE = 12/5/2025	DATE -	REVISED - C. JUCIUS 01-31-07						ILLINOIS FED. AID PROJECT				
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.			



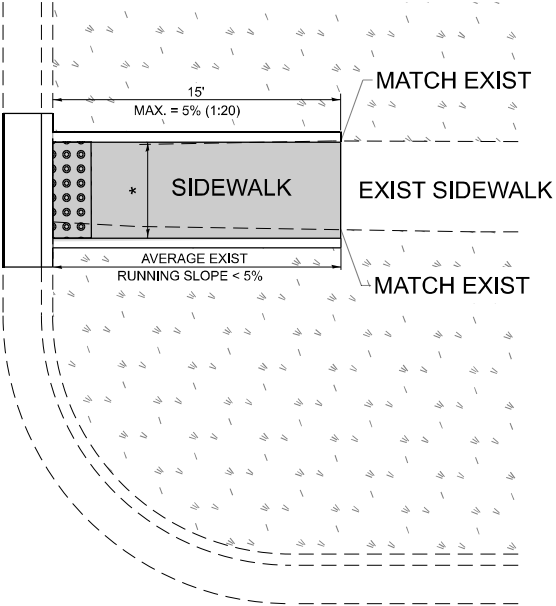
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

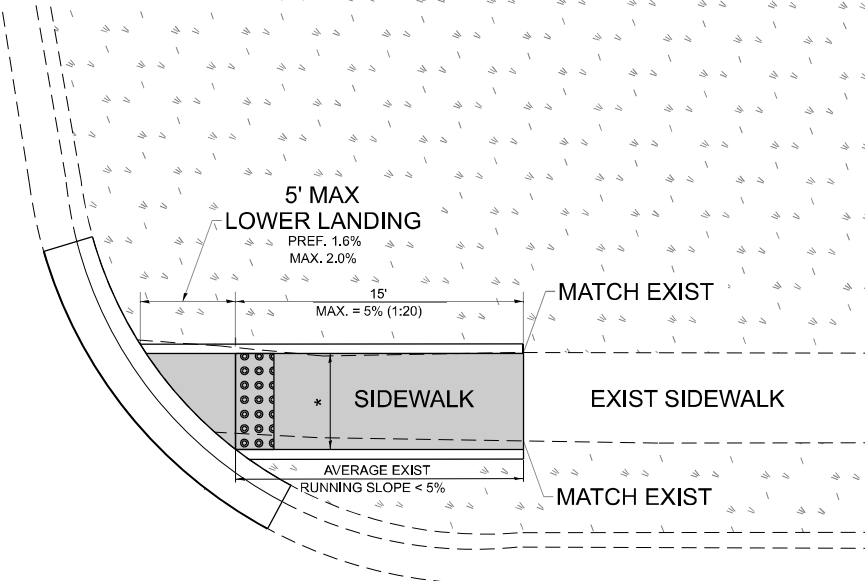
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	27
TC-26		CONTRACT NO. 80B08		
ILLINOIS FEDERAL AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

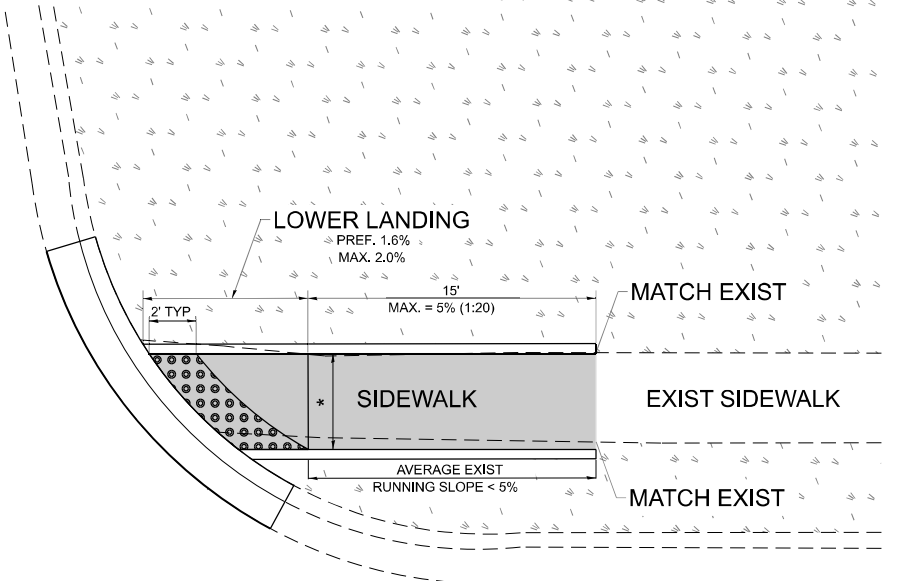
PD-01A



PD-01B



PD-01C



LEGEND



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

PROPOSED SIDE CURB

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

USER NAME = Baraa.alsmadi	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE - 10/02/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS (PD-01)

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	2025-1096-RS	DUPAGE	28	28
PD-01		CONTRACT NO. 80B08		
ILLINOIS		FED. AID PROJECT		

MODEL: PD-01 [Sheet]
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