02-27-2026 LETTING ITEM 064

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	1
	ILLINOIS	CONTRACT NO. 80B15		

* 50 + 1 = 51 TOTAL SHEETS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED
HIGHWAY PLANS

FAP ROUTE 347: IL 38 (ROOSEVELT RD)
E OF NICOLL WAY TO EDSON AVENUE
SECTION: 2025-1089-RS
PROJECT: NHPP-XYPQ(496)
STANDARD OVERLAY WITH ADA IMPROVEMENTS
DUPAGE COUNTY

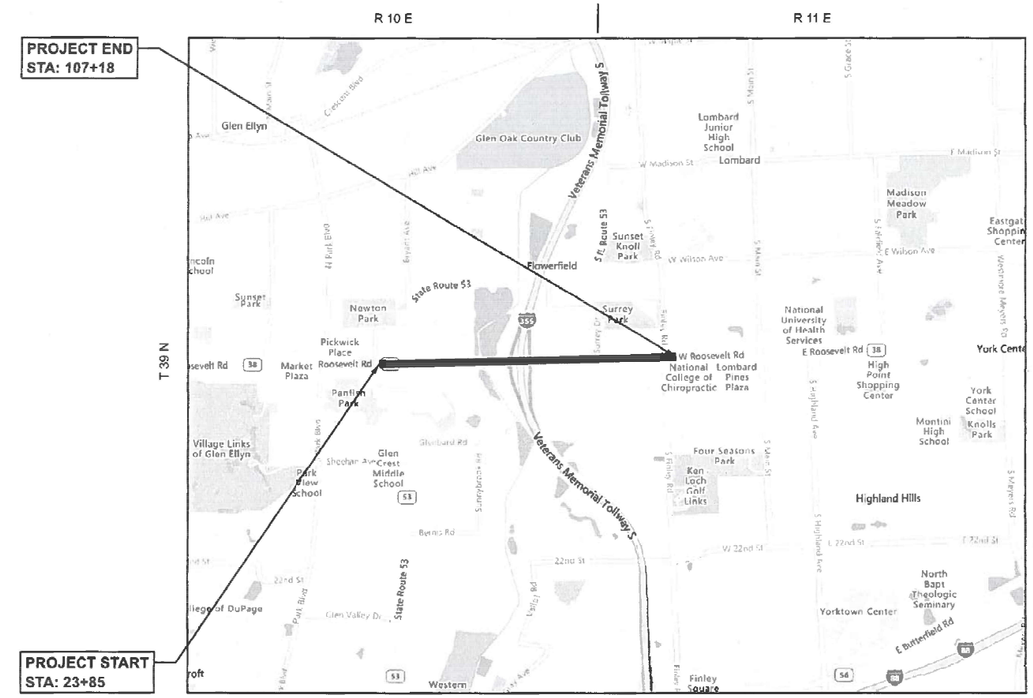
C-91-248-25

FOR INDEX OF SHEETS, SEE SHEET NO. 2

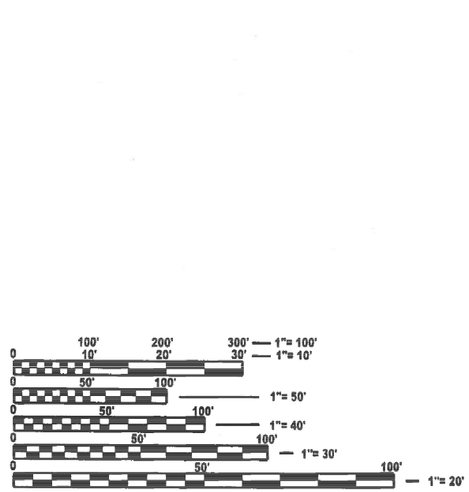
THIS PROJECT IS LOCATED IN THE VILLAGES OF GLEN ELLYN AND LOMBARD.

TRAFFIC DATA

FAP 347 - IL 38 (ROOSEVELT RD): E OF NICOLL WAY TO EDSON AVE
ADT: 37,700 (2023)
SPEED LIMIT: 35 - 45 MPH



MILTON & YORK TOWNSHIPS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

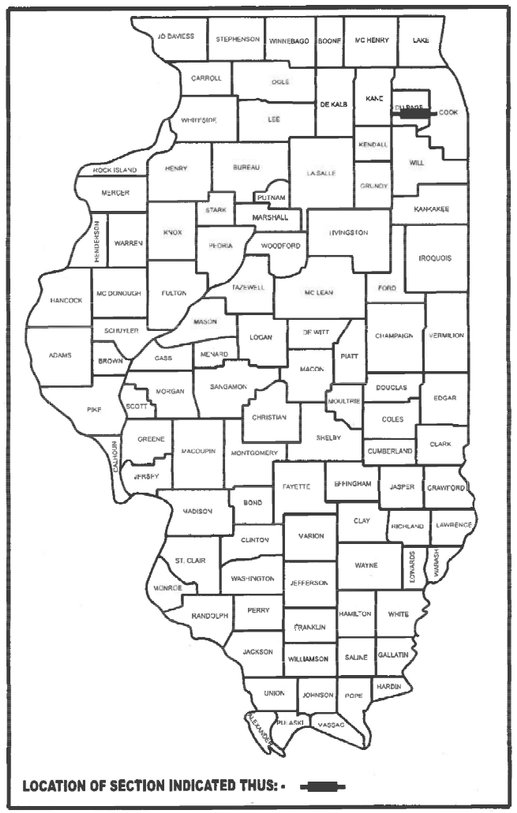
PROJECT ENGINEER: RODRIGO LEDEZMA (847) 705-4580
PROJECT MANAGER: J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 80B15

OMISSIONS:
STA. 30+35 TO STA. 31+18
STA. 50+86 TO STA. 51+25

GROSS LENGTH = 8,333 FT. = 1.578 MILE
NET LENGTH = 8,211 FT. = 1.555 MILE

D-91-168-25



LOCATION OF SECTION INDICATED THUS: ▪

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Dec 1st 20 25
REGIONAL ENGINEER

January 23 20 26
ENGINEER OF DESIGN AND ENVIRONMENT

January 23 20 26
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

REV-SEP

INDEX OF SHEETS

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-09	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-04	CLASS C&D PATCHES
606001-09	CONCRETE CURB TYPE B AND COMBINATION CONCRETE GURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701411-09	LANE CLOSURE, MULTILANE, ENTRANCE OR EXIT RAMP, 45 MPH OR MORE
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY 45 - 55 MPH
701422-10	LANE CLOSURE, MULITLANE, 45 - 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, 45 MPH OR MORE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, 40 MPH OR LESS
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W 2W WITH NONTRAVERSABLE MEDIAN
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE, INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-11	TRAFFIC CONTROL DEVICES

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGES OF LOMBARD AND GLEN ELLYN.
3. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
6. BUTT JOINTS SHALL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
7. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
8. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
9. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
10. SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER, UNLESS OTHERWISE SHOWN.
11. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
12. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
13. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BYSTATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
14. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL DELIVER THE RECORD TO THE ENGINEER.
15. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
16. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR KALPANA KANNAN-HOSADURGA AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
17. THE ENGINEER SHALL CONTACT EMAD ALHUSSEINI, AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
18. PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
19. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
20. THE AGGREGATE GRADATION FOR THE LOWER 9 INCHES OF AGGREGATE SUBGRADE IMPROVEMENT 12" SHALL BE CS 01 OR RR 1.
21. THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
22. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
23. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE WOVEN.
24. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECT BY THE ENGINEER AT CONTRACTOR EXPENSE.
25. TEMPORARY PAVEMENT MARKINGS OR SHORT TERM PAVEMENT MARKINGS ON INTERMEDIATE SURFACES SHALLNOT BE REMOVED, UNLESS DIRECTED BY THE ENGINEER.
26. WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICY MEMORANDUM 4-21. DROP-OFFS GREATER THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC, THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY AND ENSURE THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES/DRUMS AT THE END OF EACH WORKDAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. IF THE ABOVE REQUIREMENTS CAN'T BE MET, AND IT IS DETERMINED THAT OVERNIGHT LANE CLOSURES AND/OR TEMPORARY CONCRETE BARRIER WALL INSTALLATION WILL BE NECESSARY, THEN IDOT WRITTEN APPROVAL WILL BE REQUIRED PRIOR TO THE INSTALLATION OF THESE ITEMS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT. WHERE POSITIVE PROTECTION (TEMPORARY CONCRETE BARRIER PER STD. 704001) IS PROVIDED, THIS REQUIREMENT IS NULLIFIED.

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	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/4/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	2
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		

REV-SEP

SUMMARY OF QUANTITIES

					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SIGNALS	DRAINAGE			
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0044			
	20101400	NITROGEN FERTILIZER NUTRIENT	POUND	83	83					
	20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	83	83					
	20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	83	83					
	20200100	EARTH EXCAVATION	CU YD	1213	1213					
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	475	475					
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	4417	4417					
	25200110	SODDING, SALT TOLERANT	SQ YD	4417	4417					
	25200200	SUPPLEMENTAL WATERING	UNIT	45	45					
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	159	159					
	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	1898	1898					
	35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	410	410					
	35501328	HOT-MIX ASPHALT BASE COURSE, 11"	SQ YD	1845	1845					
	35600720	HOT-MIX ASPHALT BASE COURSE WIDENING, 11"	SQ YD	53	53					
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	48369	48369					
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	39175	39175					
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	108	108					
	40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	463	463					
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	3942	3942					
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	46	46					
	40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	7023	7023					
	42001300	PROTECTIVE COAT	SQ YD	8975	8975					
	42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	219	219					

SUMMARY OF QUANTITIES

					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SIGNALS	DRAINAGE			
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0044			
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1125	1125					
	42400800	DETECTABLE WARNINGS	SQ FT	90	90					
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	629	629					
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	15804	15804					
	44000600	SIDEWALK REMOVAL	SQ FT	1125	1125					
	44003100	MEDIAN REMOVAL	SQ FT	49523	49523					
	44201773	CLASS D PATCHES, TYPE I, 11 INCH	SQ YD	108	108					
	44201777	CLASS D PATCHES, TYPE II, 11 INCH	SQ YD	860	860					
	44201781	CLASS D PATCHES, TYPE III, 11 INCH	SQ YD	645	645					
	44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SQ YD	538	538					
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	6780	6780					
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	12645	12645					
	60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	22251	22251					
	60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	3930	3930					
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	1213	1213					
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	10	10					
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1					
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1					
*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	20	20					
	67100100	MOBILIZATION	L SUM	1	1					
	70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1					
	70100320	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422	L SUM	1	1					

* SPECIALTY ITEM

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	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: | SHEET 1 OF 3 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	3
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

	Code No.	Item	Unit	Total Quantity	TYPE CODE URBAN ROADWAY 80% FED 20% STATE 0005	URBAN SIGNALS 80% FED 20% STATE 0021	URBAN DRAINAGE 100% STATE 0044	URBAN	URBAN	URBAN
	70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4	4					
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
	70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					
	70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	L SUM	1	1					
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
	70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	18582	18582					
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	3097	3097					
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	1858	1858					
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	25334	25334					
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	6740	6740					
	70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	1464	1464					
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	799	799					
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	752	752					
	70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	9291	9291					
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1858	1858					
	78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	232	232					
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19913	19913					
	78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	2838	2838					
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	6740	6740					
	78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	2006	2006					
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1464	1464					
	78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	447	447					
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	799	799					

SUMMARY OF QUANTITIES

	Code No.	Item	Unit	Total Quantity	TYPE CODE URBAN ROADWAY 80% FED 20% STATE 0005	URBAN SIGNALS 80% FED 20% STATE 0021	URBAN DRAINAGE 100% STATE 0044	URBAN	URBAN	URBAN
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	752	752					
	78009018	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	24	24					
*	78004620	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 4"	FOOT	5421	5421					
	78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	180	180					
*	78011025	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	5421	5421					
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1054	1054					
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1054	1054					
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	147		147				
*	81400200	HEAVY-DUTY HANDHOLE	EACH	2		2				
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1		1				
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	464		464				
*	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	115		115				
*	87900200	DRILL EXISTING HANDHOLE	EACH	2		2				
*	88600100	DETECTOR LOOP, TYPE I	FOOT	372		372				
*	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	464		464				
*	89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	92		92				
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1		1				
*	89502380	REMOVE EXISTING HANDHOLE	EACH	2		2				
	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
	X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	895	895					
	X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	1386	1386					
	X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	1505	1505					
	X4405020	LONGITUDINAL PARTIAL DEPTH REMOVAL 2"	FOOT	3500	3500					
	X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	2500			2500			

* SPECIALTY ITEM

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USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: | SHEET 2 OF 3 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	4
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

REV-SEP

SUMMARY OF QUANTITIES

	Code No.	Item	Unit	Total Quantity	TYPE CODE URBAN ROADWAY 80% FED 20% STATE 0005	URBAN SIGNALS 80% FED 20% STATE 0021	URBAN DRAINAGE 100% STATE 0044	URBAN	URBAN	URBAN
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	52	52					
	X7010005	MAINTENANCE OF TRAFFIC (ILLINOIS TOLLWAY)	L SUM	1	1					
	X7010808	TRAFFIC CONTROL AND PROTECTION, STANDARD TC8	EACH	4	4					
	X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	282.7	282.7					
*	X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4		4				
*	X8860105	DETECTOR LOOP REPLACEMENT	FOOT	1705	1705					
	Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	122	122					
	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	20			20			
*	Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1		1				
	00000000	PORTLAND CEMENT CONCRETE SURFACE REMOVAL, 2 3/4"	SQ YD	1918		1918				
Ø	Z0076600	TRAINEES	HOUR	500	500					
Ø	Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500					

SUMMARY OF QUANTITIES

Code No.	Item	Unit	Total Quantity	TYPE CODE URBAN ROADWAY 80% FED 20% STATE 0005	URBAN SIGNALS 80% FED 20% STATE 0021	URBAN DRAINAGE 100% STATE 0044	URBAN	URBAN	URBAN

* SPECIALTY ITEM

Ø 0042

USER NAME = Jacob.Roth	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

SCALE: | SHEET 3 OF 3 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	5
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

MODEL: SOQ-03 [Sheet]
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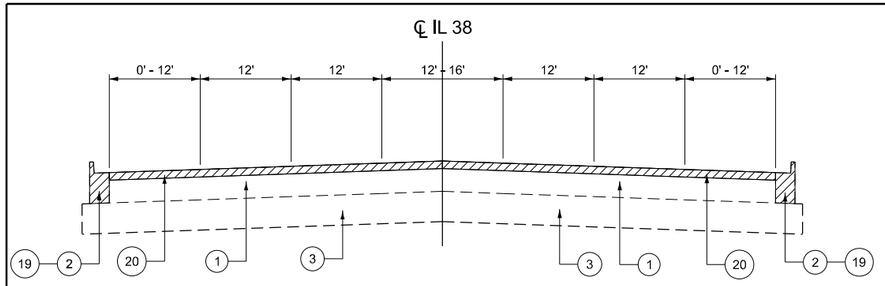
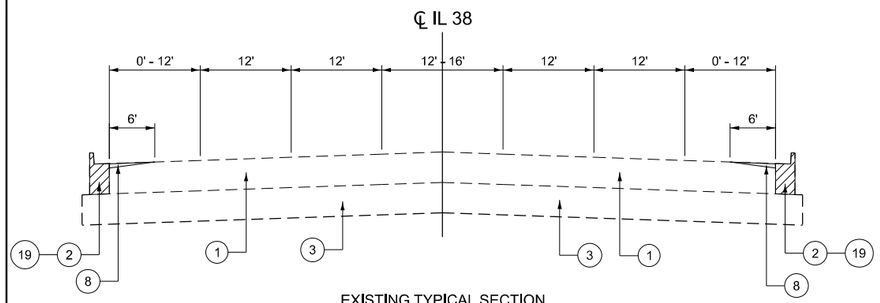
REV-SEP



EXISTING TYPICAL SECTION

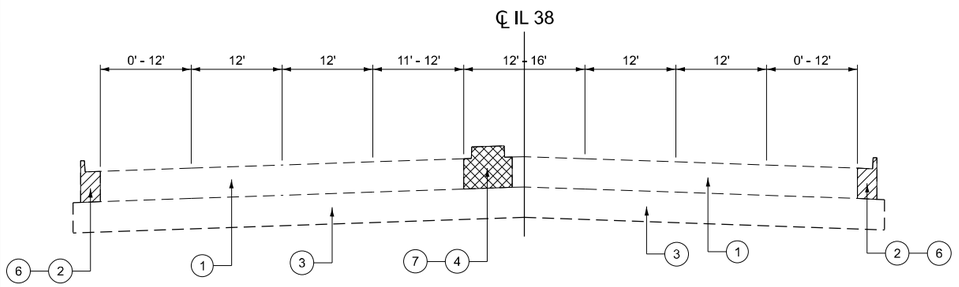
STA 23+86 TO STA 25+92



EXISTING TYPICAL SECTION

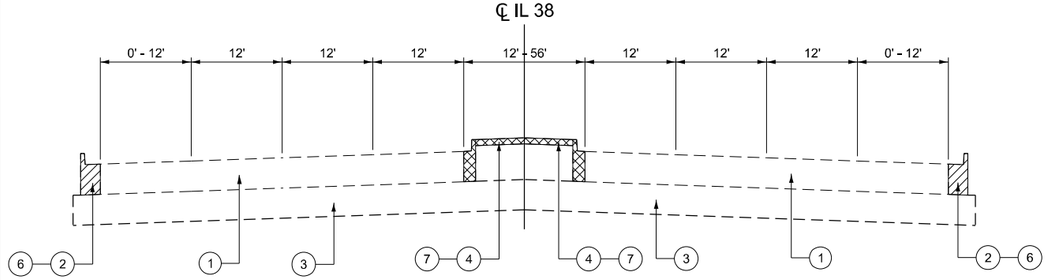
STA 25+92 TO STA 29+84

STA 101+50 TO STA 107+18



EXISTING TYPICAL SECTION

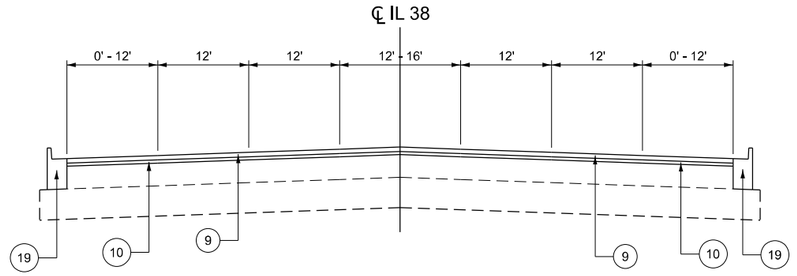
STA 32+93 TO STA 50+87



EXISTING TYPICAL SECTION

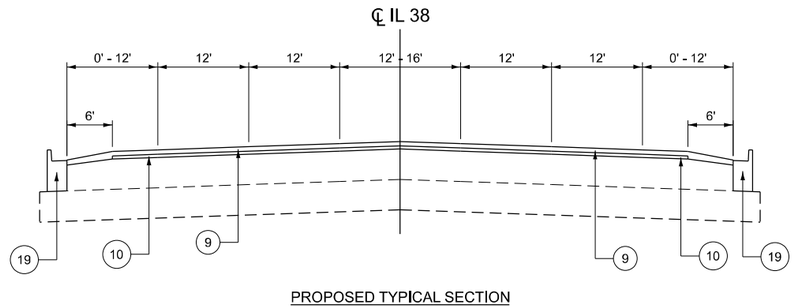
STA 51+25 TO STA 58+50

STA 63+50 TO STA 73+00



PROPOSED TYPICAL SECTION

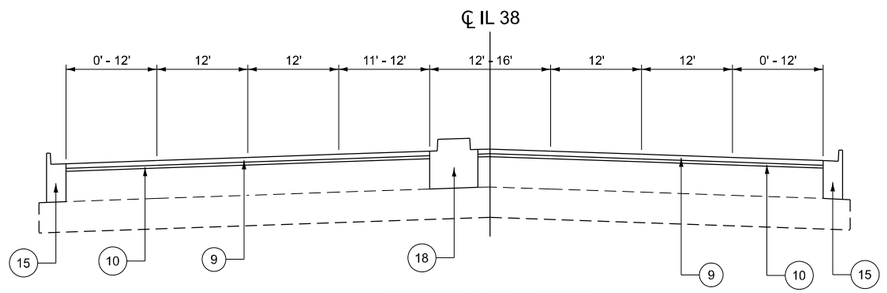
STA 23+86 TO STA 25+92



PROPOSED TYPICAL SECTION

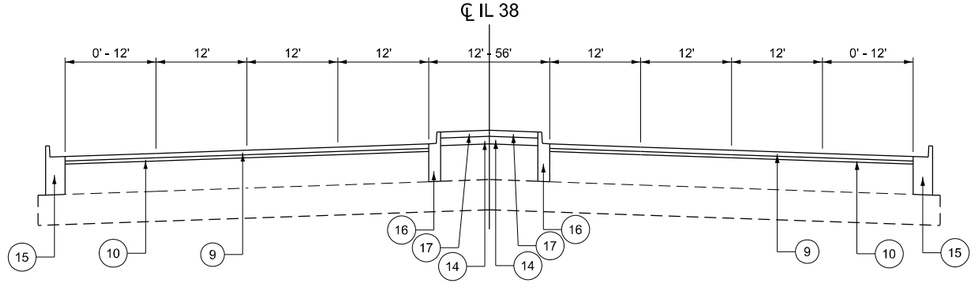
STA 25+92 TO STA 29+84

STA 101+50 TO STA 107+18



PROPOSED TYPICAL SECTION

STA 32+93 TO STA 50+87



PROPOSED TYPICAL SECTION

STA 51+25 TO STA 58+50

STA 63+50 TO STA 73+00

LEGEND

1. EXISTING PCC PAVEMENT, ±11"
2. EXISTING COMB. CONC. CURB AND GUTTER
3. EXISTING SUBBASE
4. EXISTING BARRIER MEDIAN
5. EXISTING MOUNTABLE MEDIAN
6. PROP. COMB. CONC. CURB AND GUTTER REMOVAL
7. PROP. MEDIAN REMOVAL
8. PROP. VARIABLE DEPTH MILLING
9. PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"
10. PROP. POLY. HMA BINDER COURSE, IL-4.75, N50, 1"
11. PROP. HMA BASE COURSE, 11"
12. PROP. HMA BASE COURSE WIDENING, 11"
13. PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
14. PROP. SUBBASE GRANULAR MATERIAL, TYPE B
15. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.24
16. PROP. COMB. COMC. CURB AND GUTTER, TYPE B-6.12
17. PROP. CONCRETE MEDIAN SURFACE, 4"
18. PROP. CONCRETE MEDIAN, TYPE SB-6.12
19. PROP. COMB. C&G REMOVAL AND REPLACEMENT > 10'
20. PROP. PCC SURFACE REMOVAL, 2¾"

NOTE:

1. THE CONTRACTOR SHALL PATCH BEFORE OVERLAY.
2. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50.

MODEL: TypicalSection 1 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\pd1143894\D116825-sht-typical.dgn

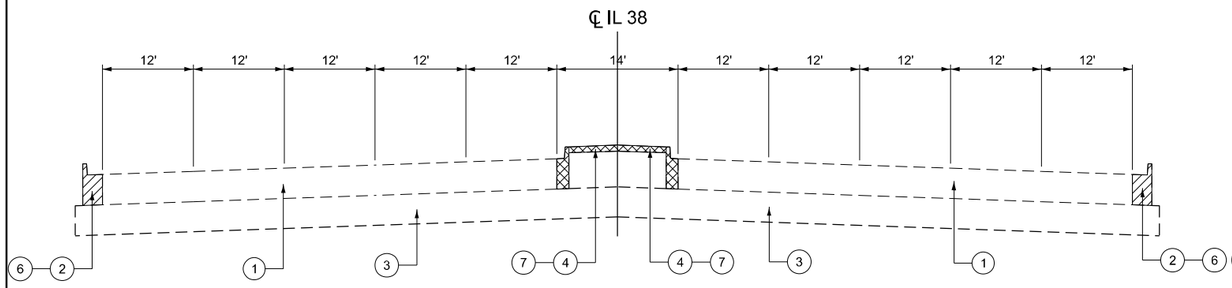
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

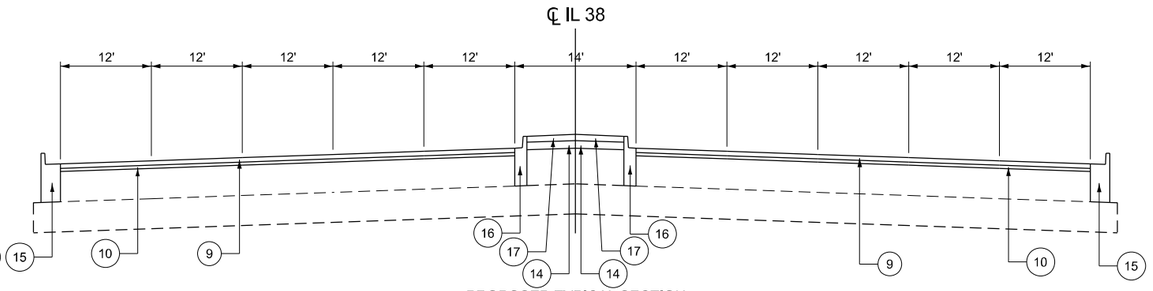
SCALE: | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	6
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		



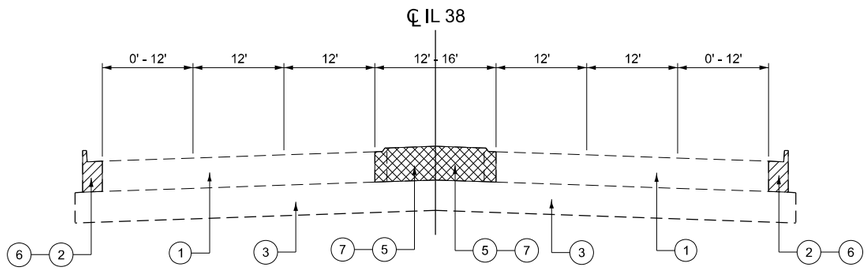
EXISTING TYPICAL SECTION

STA 58+50 TO STA 63+50



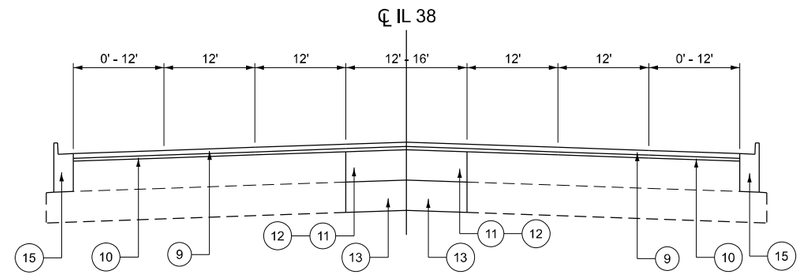
PROPOSED TYPICAL SECTION

STA 58+50 TO STA 63+50



EXISTING TYPICAL SECTION

STA 73+00 TO STA 101+50



PROPOSED TYPICAL SECTION

STA 73+00 TO STA 101+50

NOTE:

1. THE CONTRACTOR SHALL PATCH BEFORE OVERLAY.
2. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50.

LEGEND

1. EXISTING PCC PAVEMENT, ±11"
2. EXISTING COMB. CONC. CURB AND GUTTER
3. EXISTING SUBBASE
4. EXISTING BARRIER MEDIAN
5. EXISTING MOUNTABLE MEDIAN
6. PROP. COMB. CONC. CURB AND GUTTER REMOVAL
7. PROP. MEDIAN REMOVAL
8. PROP. VARIABLE DEPTH MILLING
9. PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"
10. PROP. POLY. HMA BINDER COURSE, IL-4.75, N50, 1"
11. PROP. HMA BASE COURSE, 11"
12. PROP. HMA BASE COURSE WIDENING, 11"
13. PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
14. PROP. SUBBASE GRANULAR MATERIAL, TYPE B
15. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.24
16. PROP. COMB. COMC. CURB AND GUTTER, TYPE B-6.12
17. PROP. CONCRETE MEDIAN SURFACE, 4"
18. PROP. CONCRETE MEDIAN, TYPE SB-6-12
19. PROP. COMB. C&G REMOVAL AND REPLACEMENT > 10'
20. PROP. PCC SURFACE REMOVAL, 2¾"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	**AIR VOIDS @ N_{design}**	**QMP**
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"	3.5% @ 80 Gyr.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N 50, 1"	3.5% @ 50 Gyr.	QCP
PAVEMENT WIDENING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"	3.5% @ 80 Gyr.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N 50, 1"	3.5% @ 50 Gyr.	QCP
HOT-MIX ASPHALT BASE COURSE, 11" (HMA BINDER IL-19.0)	4% @ 90 Gyr.	QC/QA
HOT-MIX ASPHALT BASE COURSE WIDENING, 11" (HMA BINDER IL-19.0)	4% @ 90 Gyr.	QC/QA
DRIVEWAYS		
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19.0), 8"	4% @ 50 Gyr.	QC/QA
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"	4% @ 50 Gyr.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 Gyr.	QC/QA
QMP Designations: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIAL SPECIFICATIONS.

MODEL: TypicalSection 2 [Sheet]
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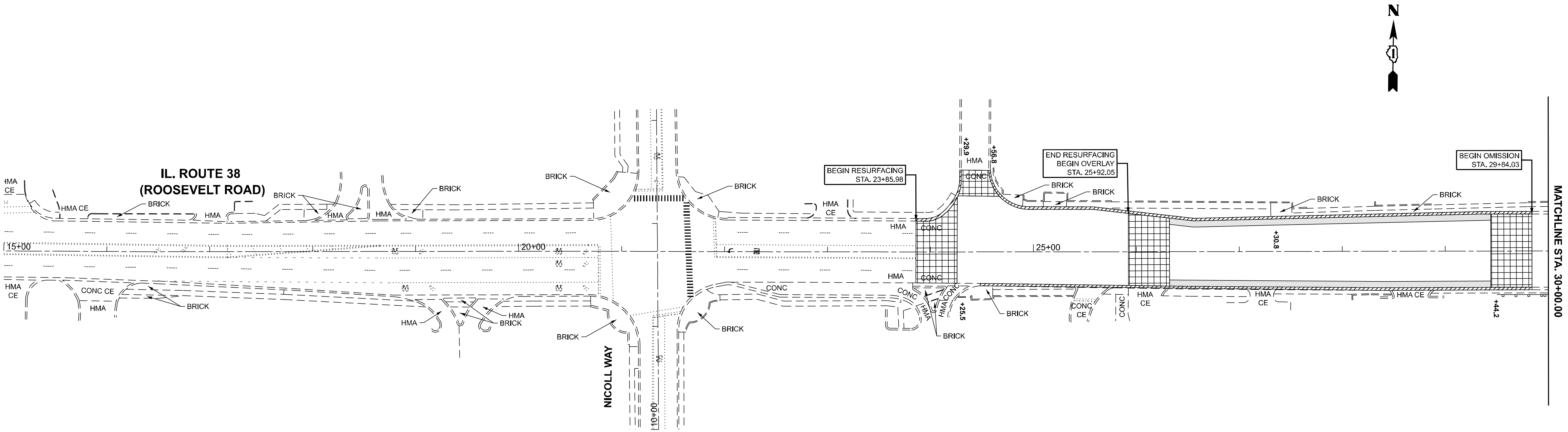
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

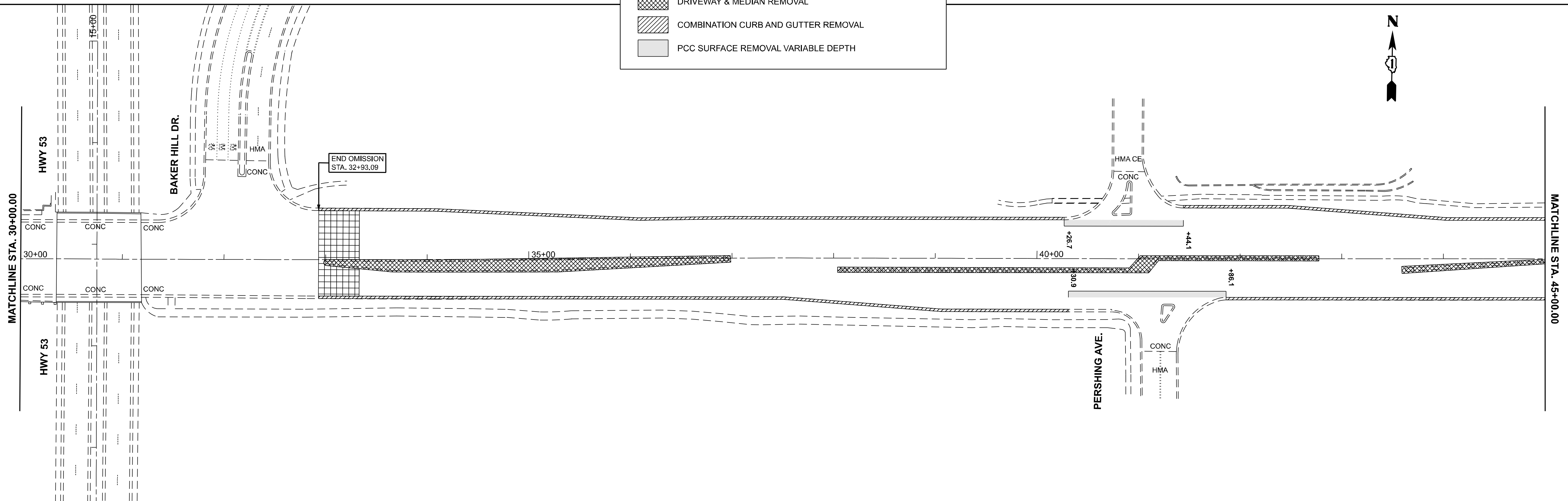
SCALE: | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	7
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



LEGEND

- PCC SURFACE REMOVAL - BUTT JOINT
- DRIVEWAY & MEDIAN REMOVAL
- COMBINATION CURB AND GUTTER REMOVAL
- PCC SURFACE REMOVAL VARIABLE DEPTH



MODEL: Removal Plan 1 - Removal Plan 1 [Sheet]
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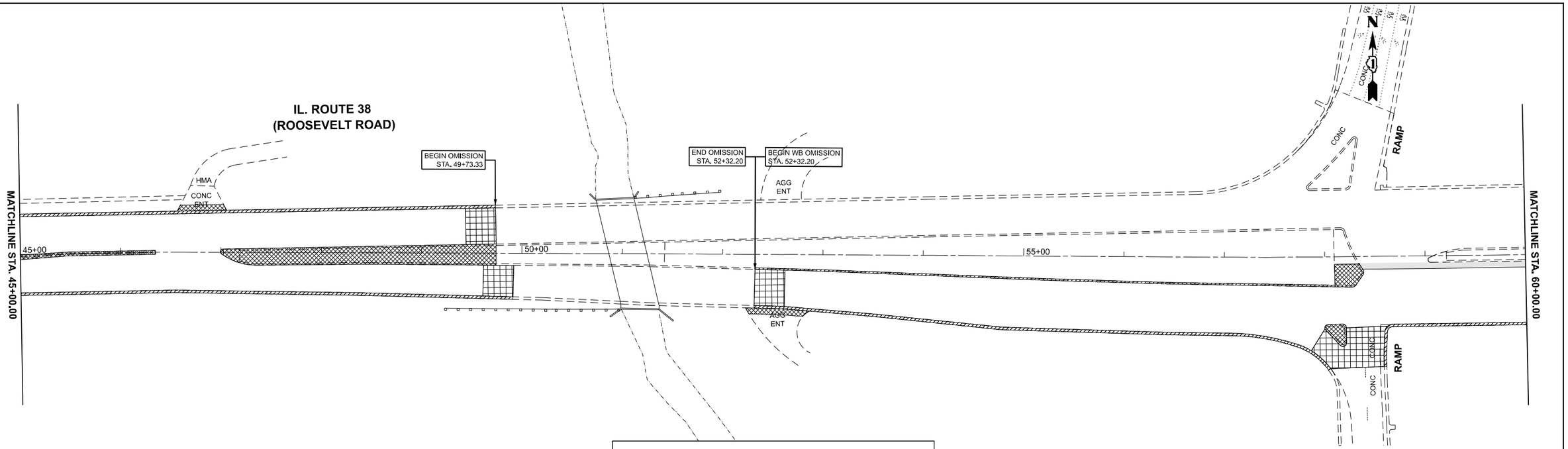
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

ROADWAY REMOVAL PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 1 OF 4 SHEETS | STA. 15+00.00 TO STA. 45+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	8
		CONTRACT NO. 80B15		
		ILLINOIS	FED. AID PROJECT	



MODEL: Removal Plan 1 - Removal Plan 3 [Sheet]
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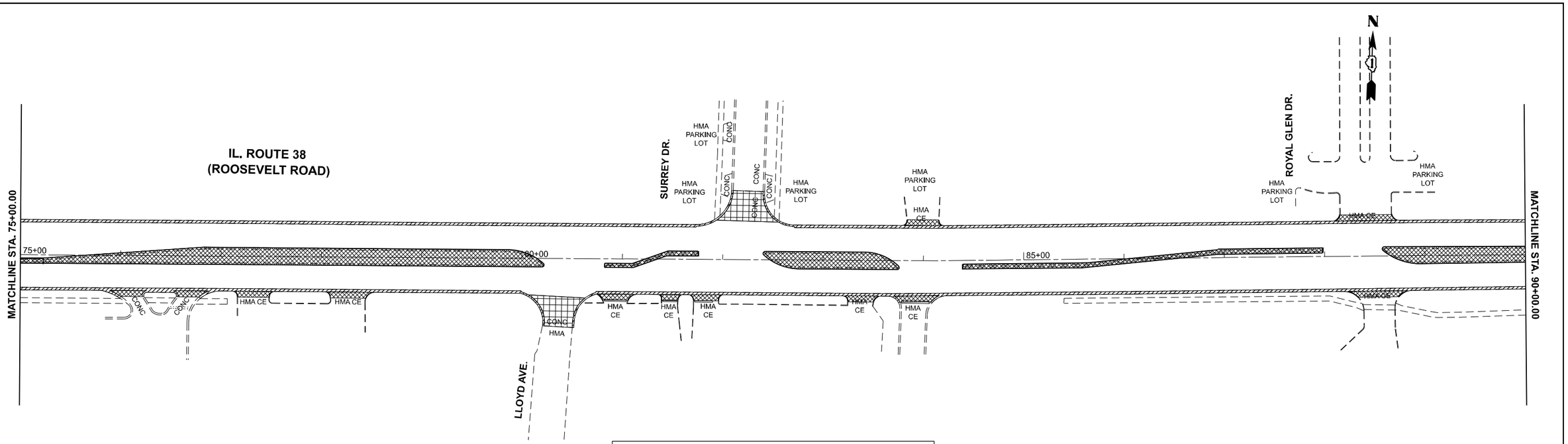
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY REMOVAL PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

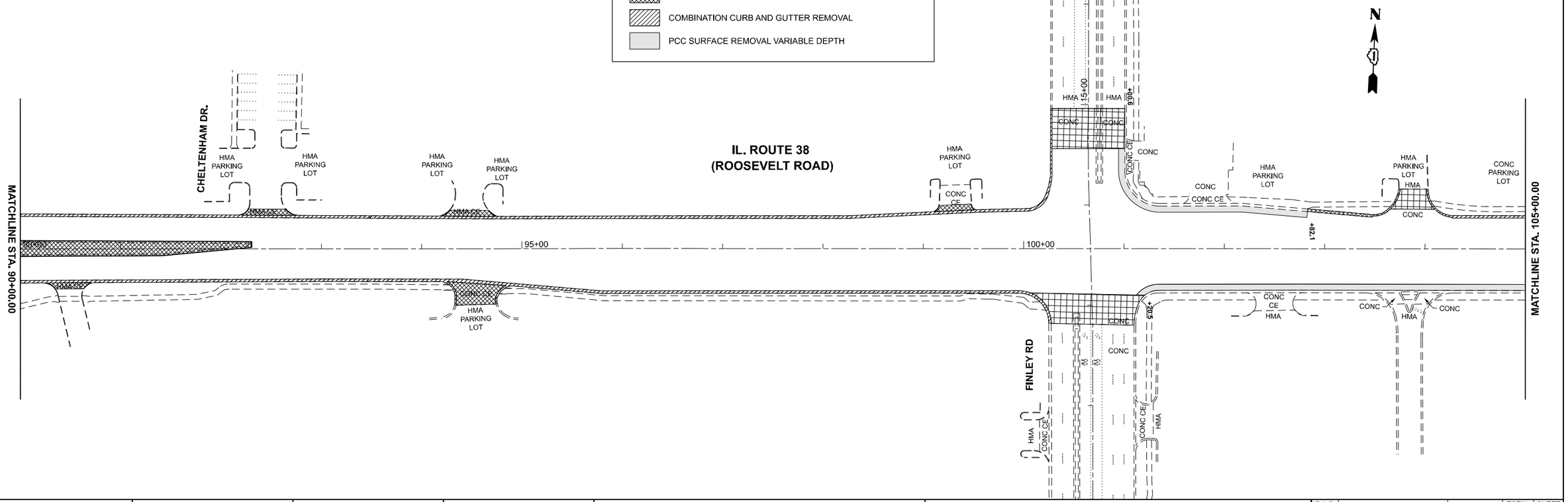
SCALE: 1"=50' | SHEET 2 OF 4 SHEETS | STA. 45+00.00 TO STA. 75+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	9
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



LEGEND

- PCC SURFACE REMOVAL - BUTT JOINT
- DRIVEWAY & MEDIAN REMOVAL
- COMBINATION CURB AND GUTTER REMOVAL
- PCC SURFACE REMOVAL VARIABLE DEPTH



MODEL: Removal Plan 1 - Removal Plan 5 [Sheet]
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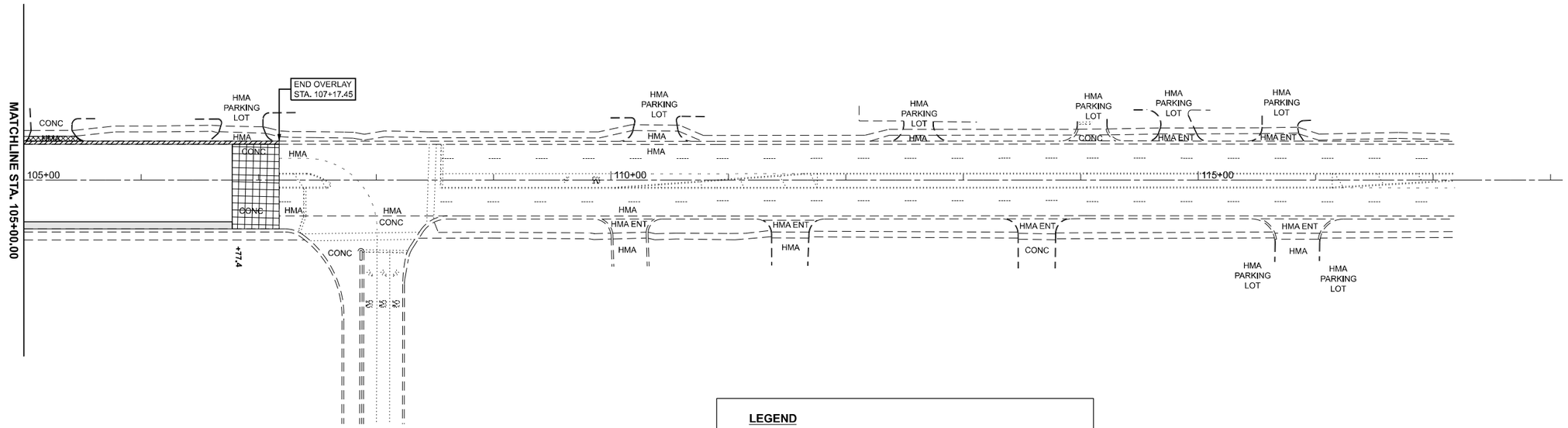
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY REMOVAL PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 3 OF 4 SHEETS | STA. 75+00.00 TO STA. 105+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	10
		CONTRACT NO. 80B15		
		ILLINOIS	FED. AID PROJECT	



LEGEND

- PCC SURFACE REMOVAL - BUTT JOINT
- DRIVEWAY & MEDIAN REMOVAL
- COMBINATION CURB AND GUTTER REMOVAL
- PCC SURFACE REMOVAL VARIABLE DEPTH

MODEL: Removal Plan 1 - Removal Plan 7 [Sheet]
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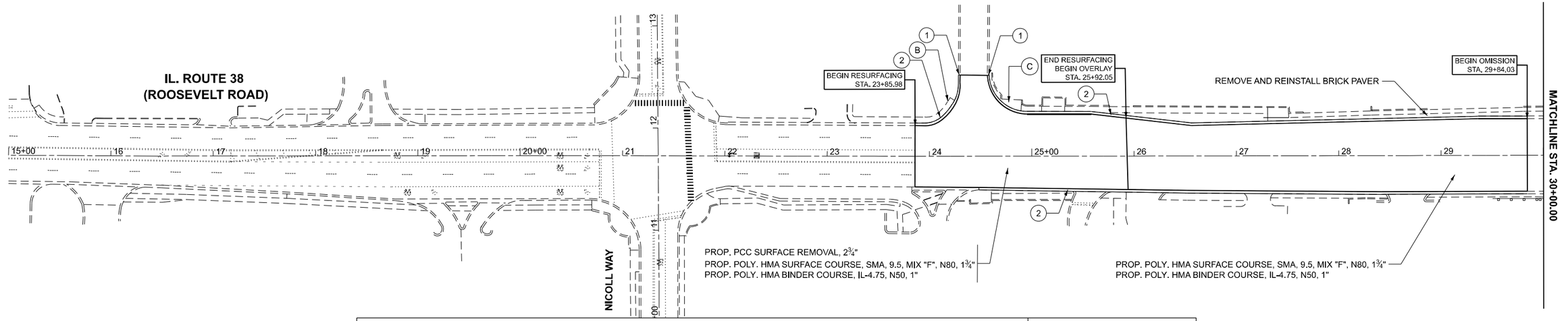
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY REMOVAL PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 4 OF 4 SHEETS | STA. 105+00.00 | TO STA. 120+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	11
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		

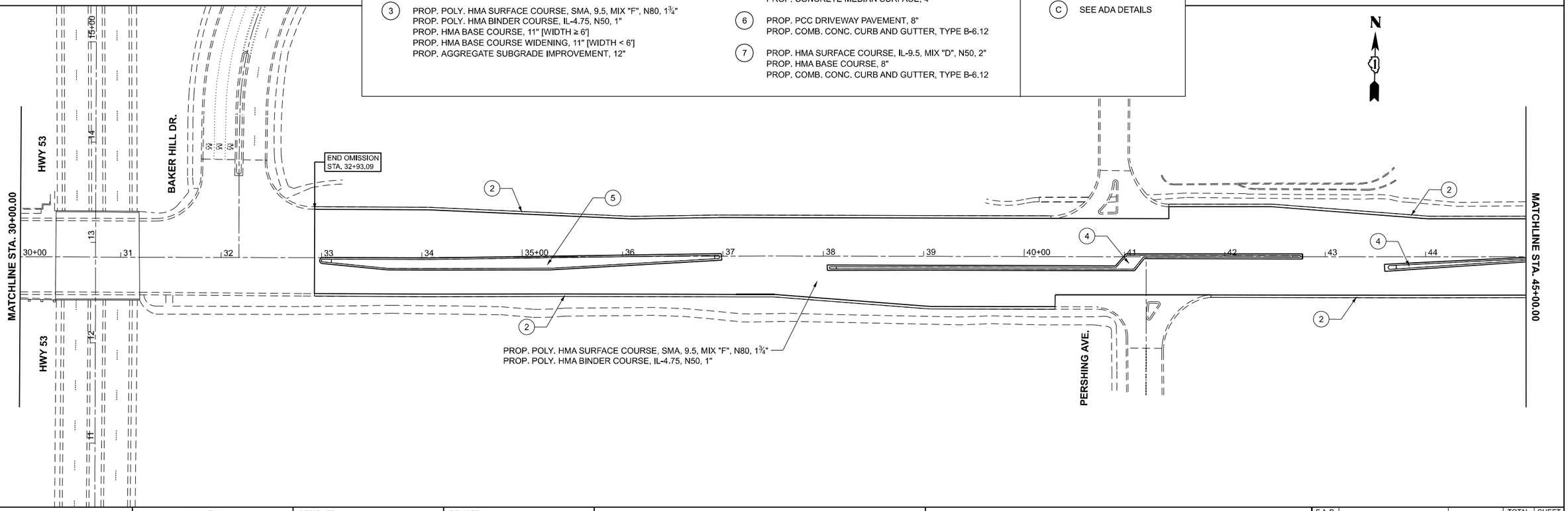


PAVEMENT LEGEND

1. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
2. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.24
3. PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"
   PROP. POLY. HMA BINDER COURSE, IL-4.75, N50, 1"
   PROP. HMA BASE COURSE, 11" [WIDTH ≥ 6']
   PROP. HMA BASE COURSE WIDENING, 11" [WIDTH < 6']
   PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
4. PROP. CONCRETE MEDIAN, TYPE SB-6.12
5. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
   PROP. CONCRETE MEDIAN SURFACE, 4"
6. PROP. PCC DRIVEWAY PAVEMENT, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
7. PROP. HMA SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
   PROP. HMA BASE COURSE, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12

SIDEWALK LEGEND

A. PD-01B
B. PD-06A
C. SEE ADA DETAILS



MODEL: IL 38 - Roadway Plan [Sheet]
FILE NAME: c:\pw_work\pwidot\rothjb\d1143894\D116825-sht-plan.dgn

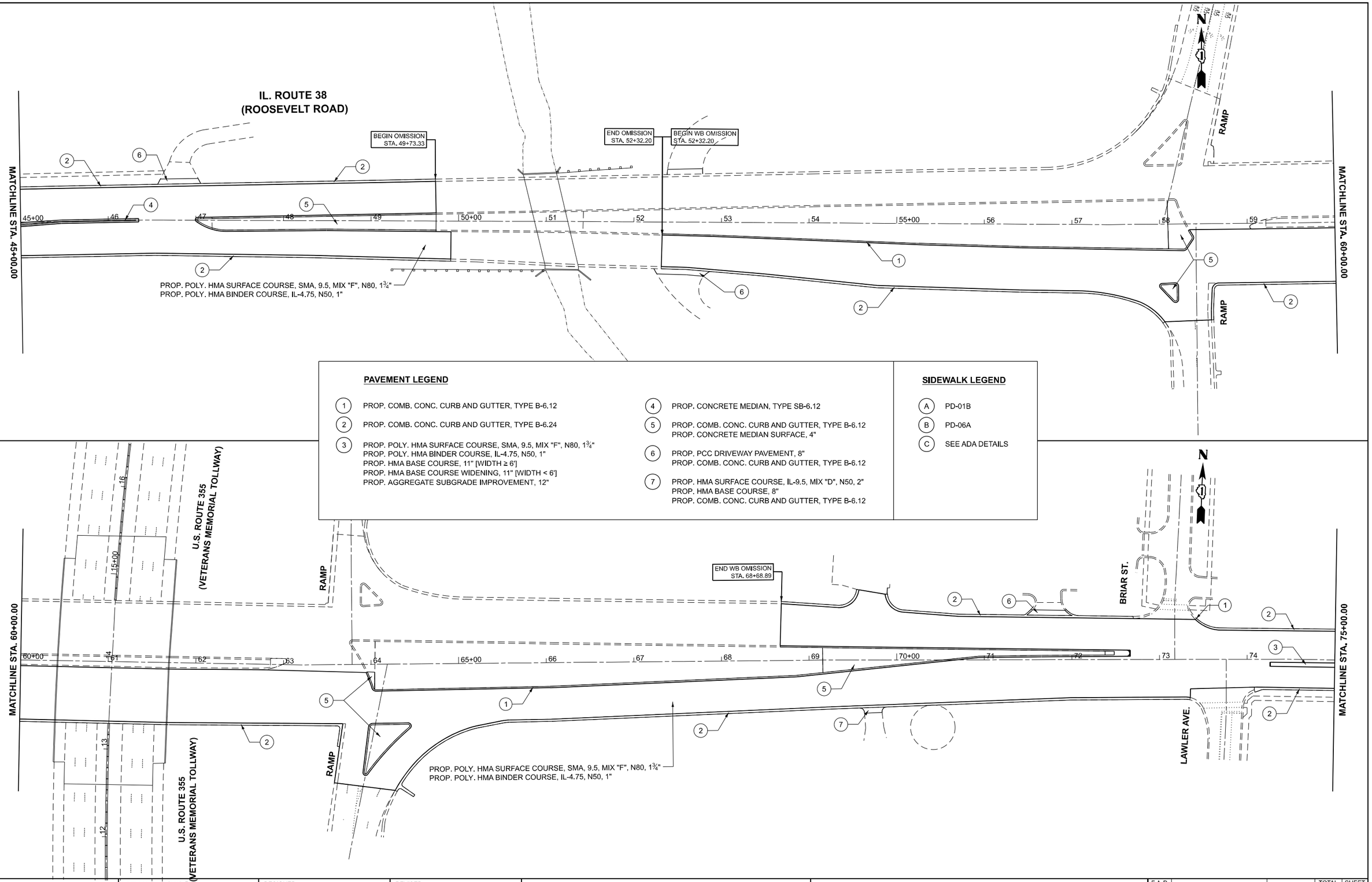
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED ROADWAY PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 1 OF 4 SHEETS | STA. 15+00.00 TO STA. 45+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	12
		CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		



PAVEMENT LEGEND

1. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
2. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.24
3. PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"
   PROP. POLY. HMA BINDER COURSE, IL-4.75, N50, 1"
   PROP. HMA BASE COURSE, 11" [WIDTH ≥ 6']
   PROP. HMA BASE COURSE WIDENING, 11" [WIDTH < 6']
   PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
4. PROP. CONCRETE MEDIAN, TYPE SB-6.12
5. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
   PROP. CONCRETE MEDIAN SURFACE, 4"
6. PROP. PCC DRIVEWAY PAVEMENT, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
7. PROP. HMA SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
   PROP. HMA BASE COURSE, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12

SIDEWALK LEGEND

- A PD-01B
- B PD-06A
- C SEE ADA DETAILS

MODEL: IL 38 - Roadway Plan-2 [Sheet]
FILE NAME: c:\pw_work\pwidot\rothjlp\d1143894\D116825-sht-plan.dgn

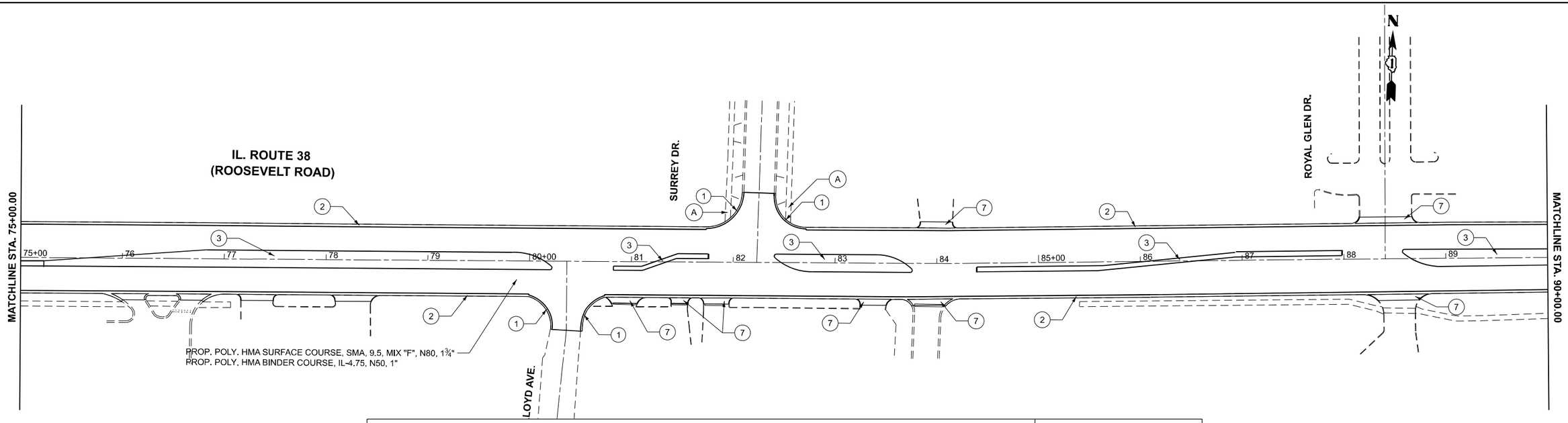
USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 1/16/2026	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED ROADWAY PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 2 OF 4 SHEETS | STA. 45+00.00 TO STA. 75+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	13
		CONTRACT NO. 80B15		
		ILLINOIS	FED. AID PROJECT	

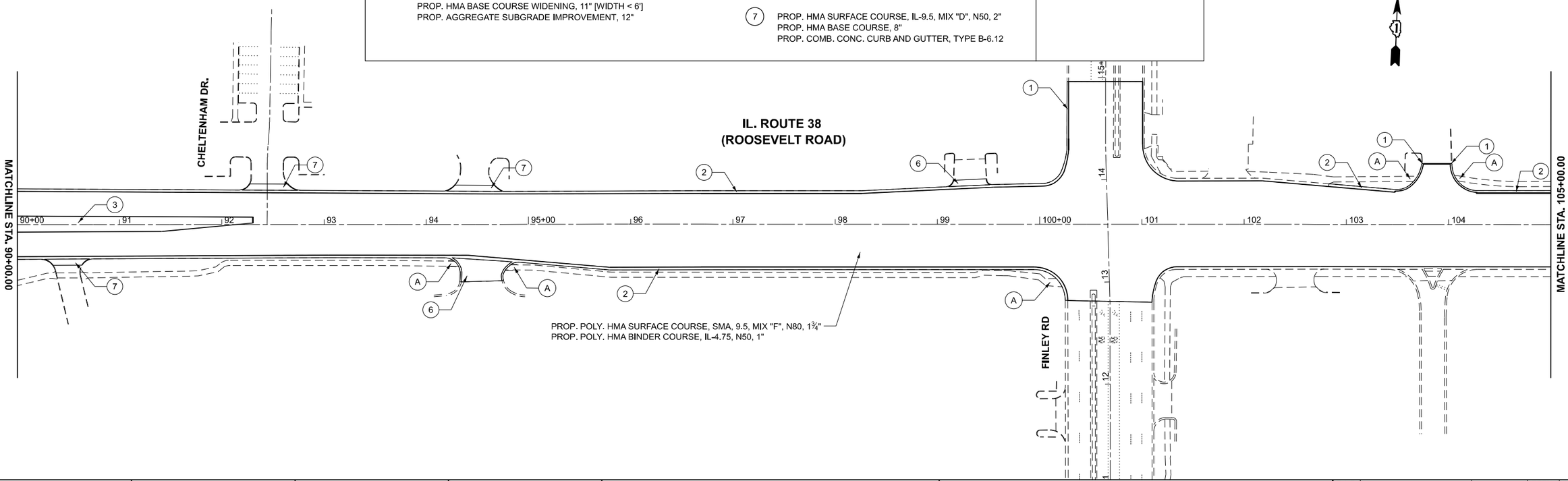


PAVEMENT LEGEND

1. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
2. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.24
3. PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"
   PROP. POLY. HMA BINDER COURSE, IL-4.75, N50, 1"
   PROP. HMA BASE COURSE, 11" [WIDTH ≥ 6']
   PROP. HMA BASE COURSE WIDENING, 11" [WIDTH < 6']
   PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
4. PROP. CONCRETE MEDIAN, TYPE SB-6.12
5. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
   PROP. CONCRETE MEDIAN SURFACE, 4"
6. PROP. PCC DRIVEWAY PAVEMENT, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
7. PROP. HMA SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
   PROP. HMA BASE COURSE, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12

SIDEWALK LEGEND

- A PD-01B
- B PD-06A
- C SEE ADA DETAILS



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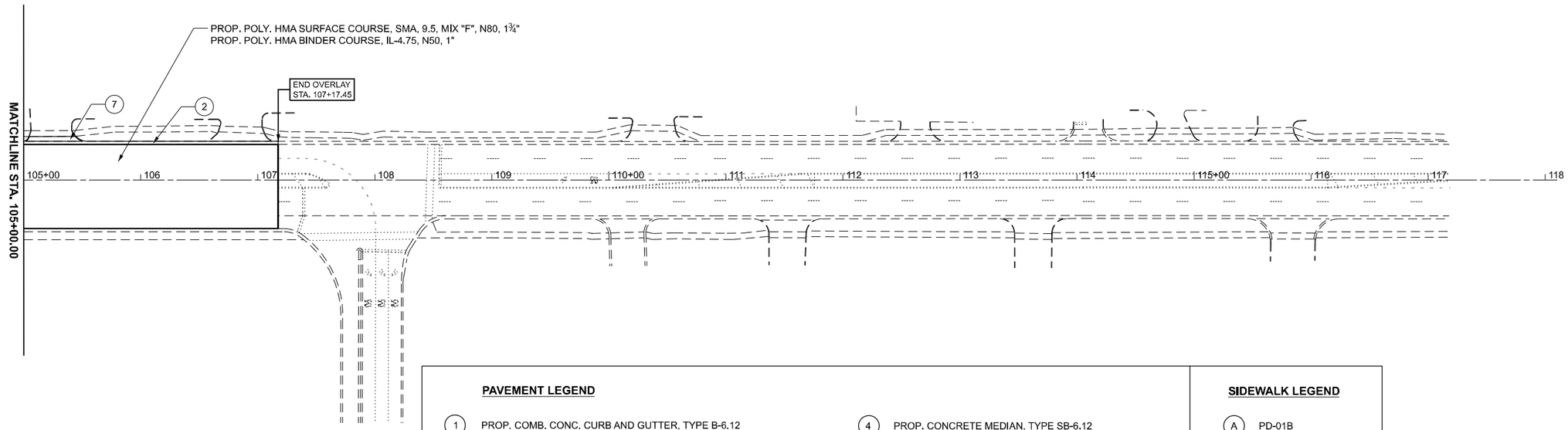
USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 1/16/2026	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED ROADWAY PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 3 OF 4 SHEETS | STA. 75+00.00 TO STA. 105+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	14
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



PAVEMENT LEGEND

1. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
2. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.24
3. PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1¾"
   PROP. POLY. HMA BINDER COURSE, IL-4.75, N50, 1"
   PROP. HMA BASE COURSE, 11" [WIDTH ≥ 6']
   PROP. HMA BASE COURSE WIDENING, 11" [WIDTH < 6']
   PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
4. PROP. CONCRETE MEDIAN, TYPE SB-6.12
5. PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
   PROP. CONCRETE MEDIAN SURFACE, 4"
6. PROP. PCC DRIVEWAY PAVEMENT, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12
7. PROP. HMA SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
   PROP. HMA BASE COURSE, 8"
   PROP. COMB. CONC. CURB AND GUTTER, TYPE B-6.12

SIDEWALK LEGEND

- A. PD-01B
- B. PD-06A
- C. SEE ADA DETAILS

MODEL: IL 38 - Roadway Plan-6 [Sheet]
FILE NAME: c:\pw_work\dwidotroth\pd1143894\D1116825-sht-plan.dgn

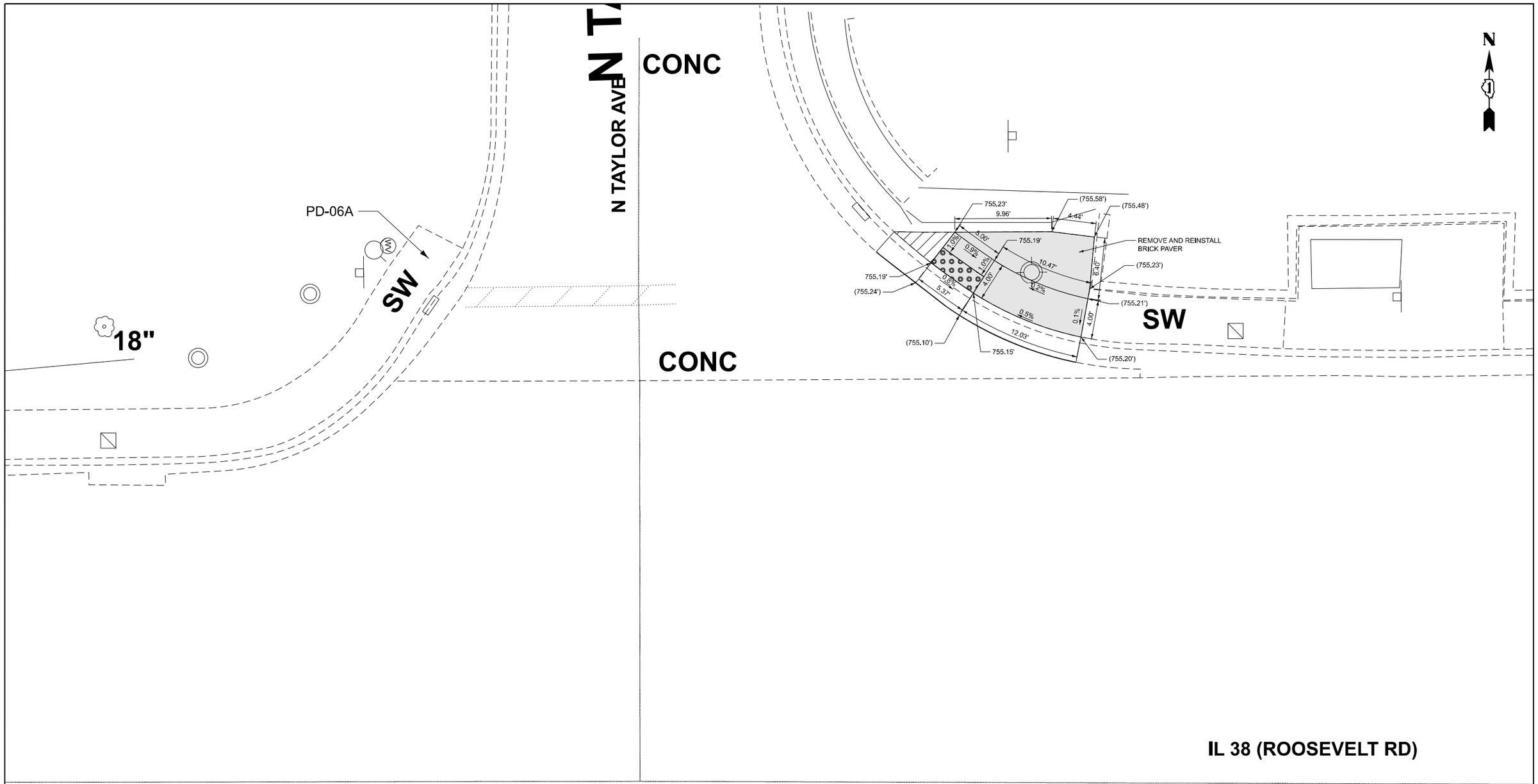
USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 1/16/2026	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED ROADWAY PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 4 OF 4 SHEETS | STA. 105+00.00 | TO STA. 120+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	15
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		



IL 38 (ROOSEVELT RD)

LEGEND

- XX.XX' — EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) — EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV.: 759.30'

BENCHMARK: "X" IN N.E. BOLT OF F.H.

LOCATION: N.W. CORNER OF IL 38 & TAYLOR AVE

USER NAME = Jacob.Roth	DESIGNED -	REVISED -
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PLOT DATE = 12/4/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA DETAILS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: | SHEET 1 OF 1 SHEETS | STA. 0+00.00 | TO STA. 0+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	16
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

MODEL: ADA-01 [Sheet]
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NOTES:

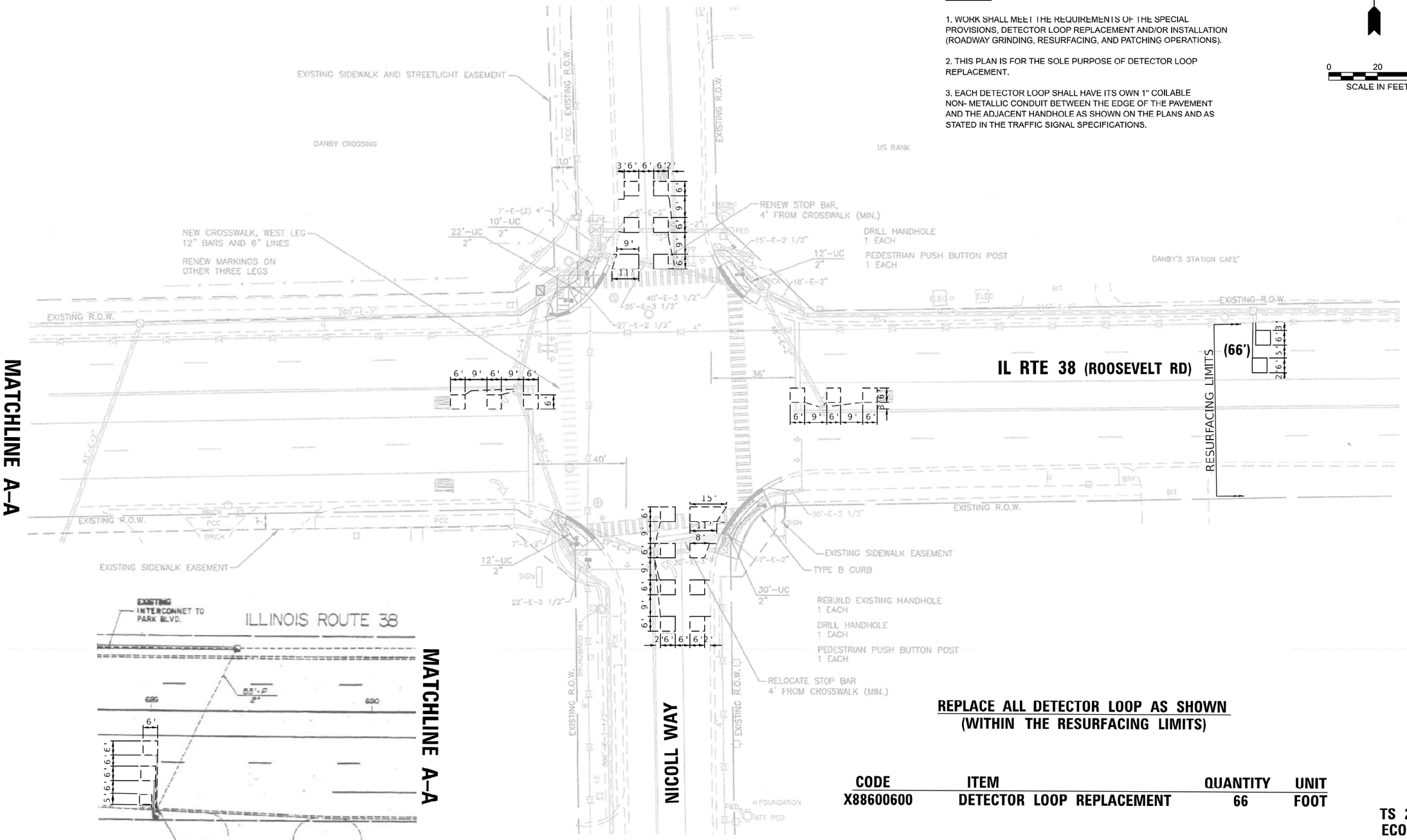
1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

3. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON- METALLIC CONDUIT BETWEEN THE EDGE OF THE PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

N

0 20 40
SCALE IN FEET



REPLACE ALL DETECTOR LOOP AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
X88600600	DETECTOR LOOP REPLACEMENT	66	FOOT

TS 20631
ECON 40

USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/4/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT NICOLL WAY

SCALE: | SHEET 1 OF 8 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	17
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		

MODEL: APS-01 [Sheet]
FILE NAME: c:\pw_work\pwidotrothjd\d1143894\D116825-shr-TS.dgn

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

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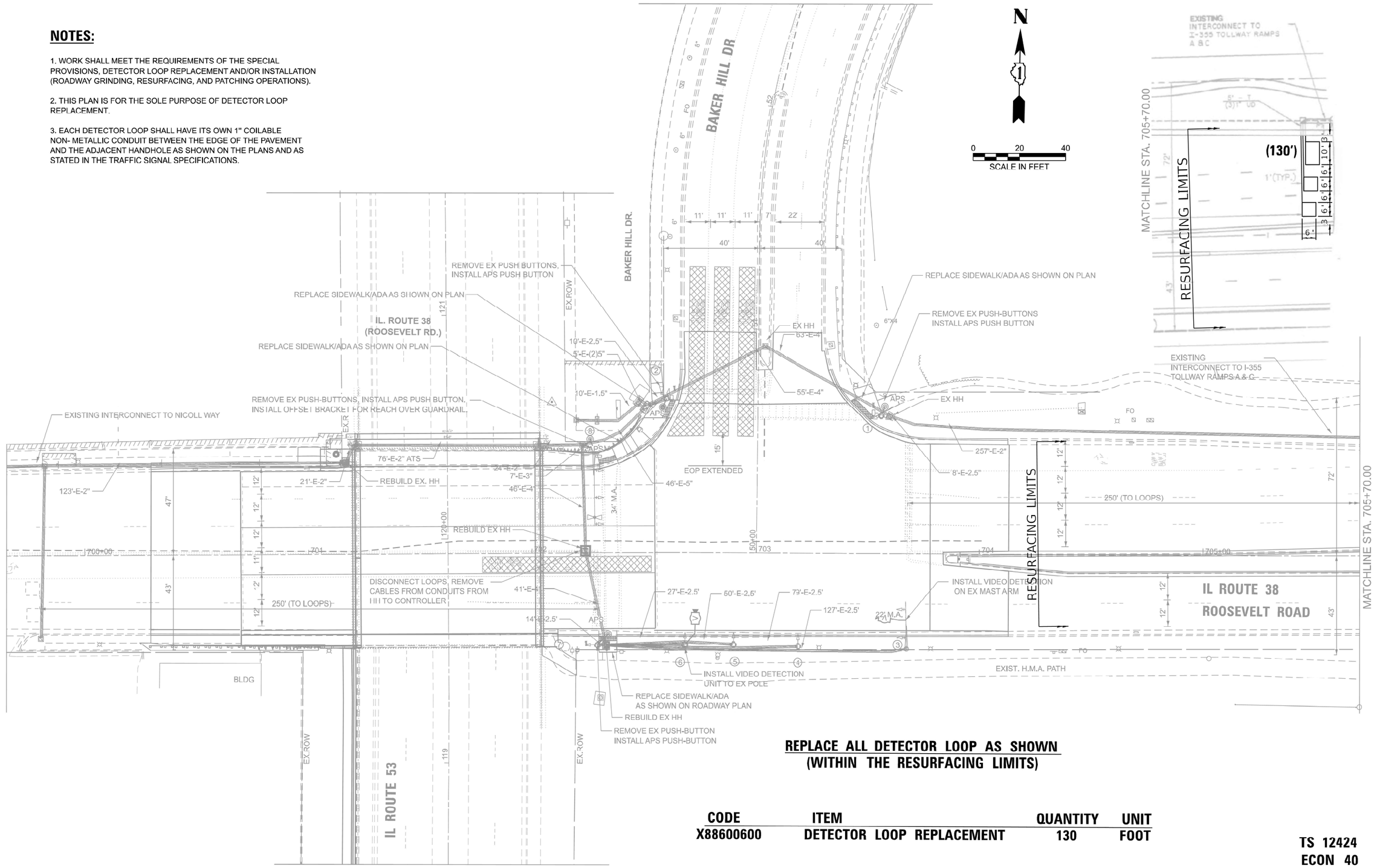


REPLACE ALL DETECTOR LOOP AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
X88600600	DETECTOR LOOP REPLACEMENT	130	FOOT

TS 12424
ECON 40

MODEL: APS-02 [Sheet]
FILE NAME: c:\pw_work\pwidotroth\d1143894\D116825-sht-TS.dgn

USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT NICOLL WAY

SCALE: | SHEET APS-02 OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	17A
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

2. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.

3. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.

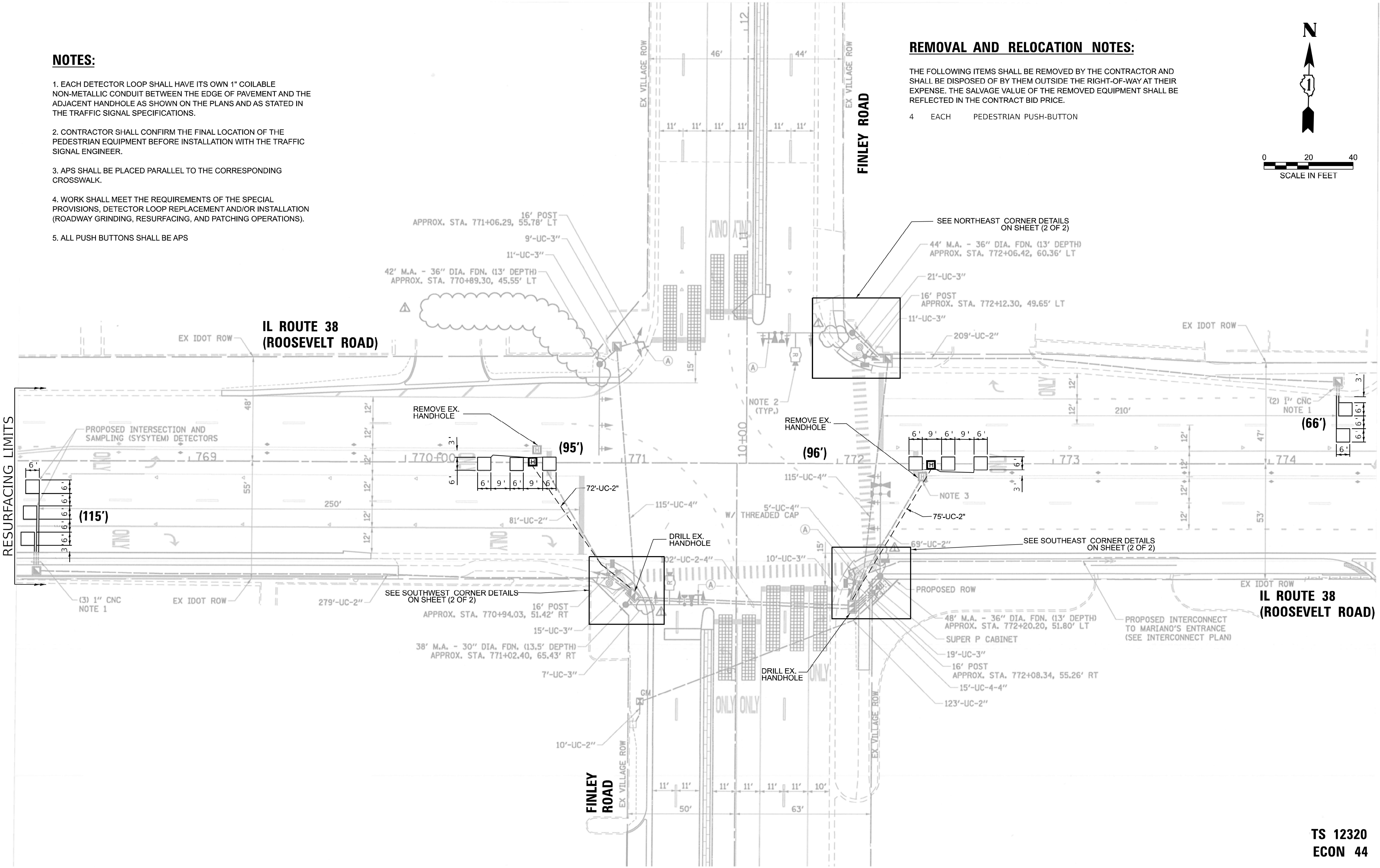
4. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

5. ALL PUSH BUTTONS SHALL BE APS

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH PEDESTRIAN PUSH-BUTTON



USER NAME = Jacob.Roth	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT FINLEY ROAD

SCALE: | SHEET 2 OF 8 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	18
		CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		

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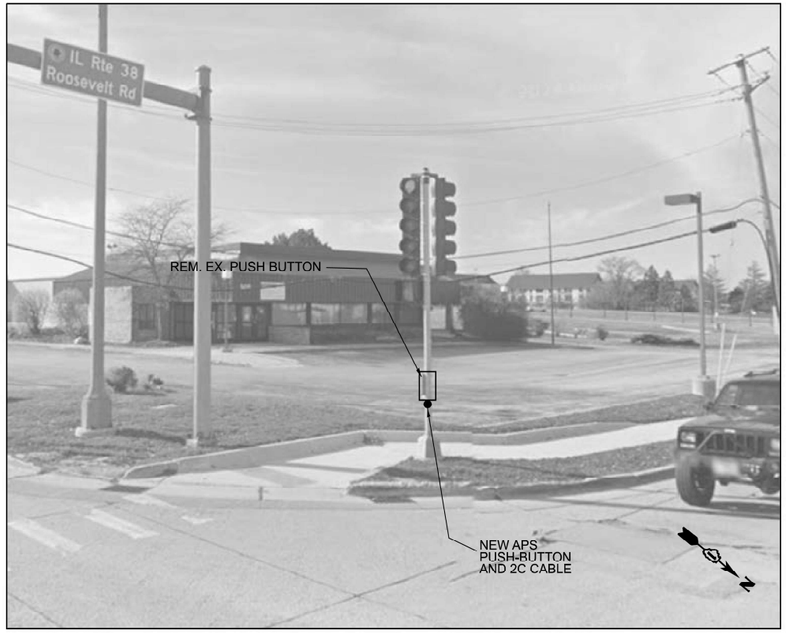


SOUTHWEST CORNER

NORTHEAST CORNER

SOUTHEAST CORNER

MODEL: APS-03 [Sheet]
FILE NAME: c:\pw_work\pwdot\rothjd\d1143894\D1116825-sht-TS.dgn

USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

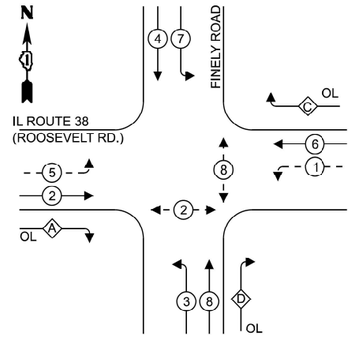
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT FINLEY ROAD

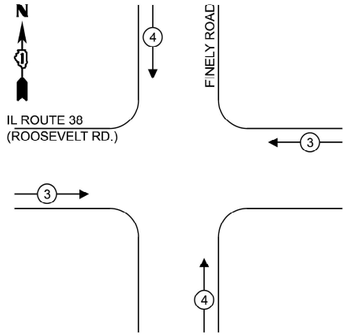
SCALE: | SHEET 3 OF 8 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	19
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

EXISTING CONTROLLER SEQUENCE



EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



RIGHT TURN OVERLAP PHASE DESIGNATION

OVERLAP LETTER		PERMISSIVE PHASE		PROTECTED PHASE
A	=	2	+	3
B	=	6	+	7
C	=	8	+	1

LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OVERLAP

SCHEDULE OF QUANTITIES

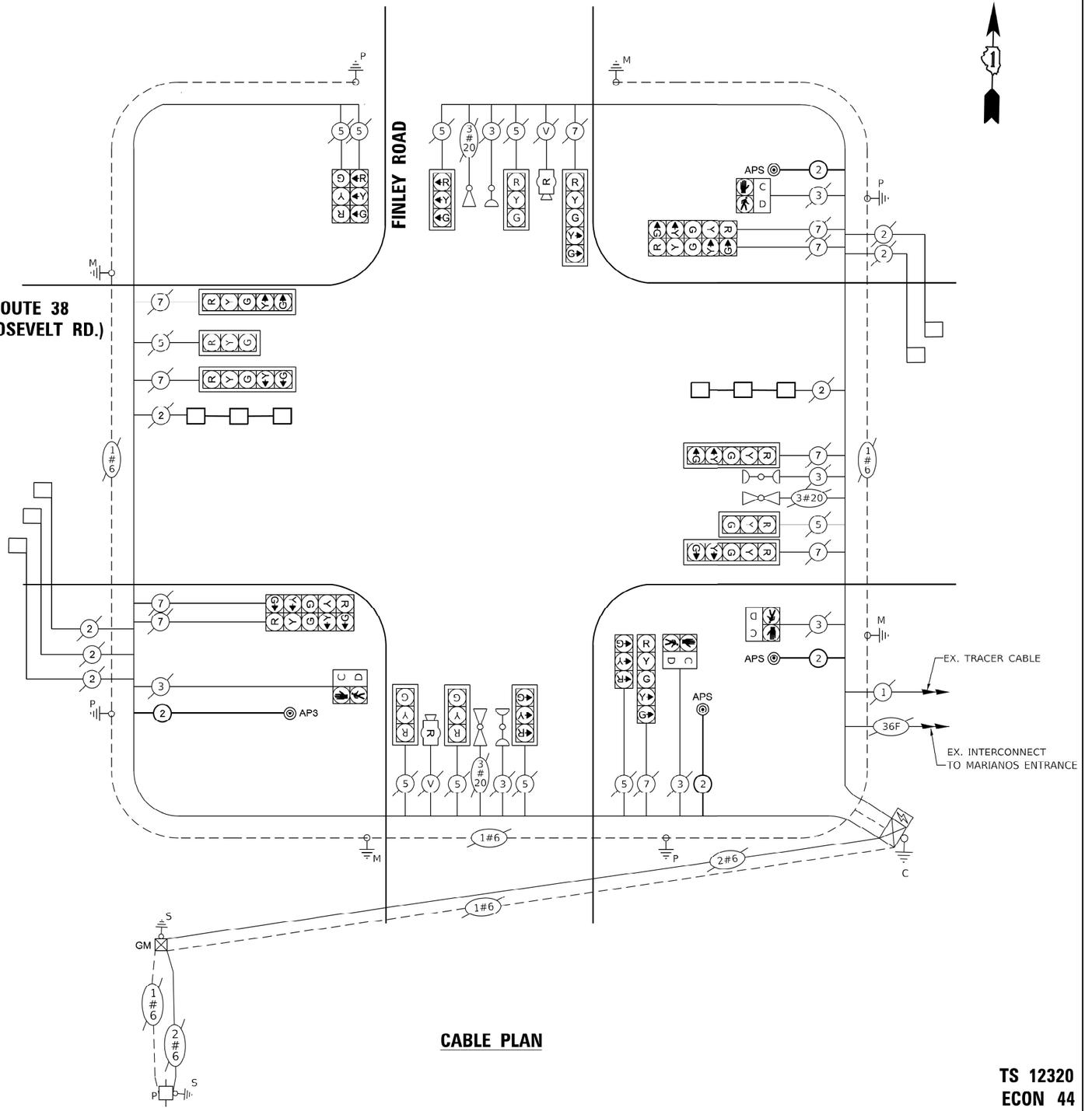
ITEM DESCRIPTION	UNITS	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	147
HEAVY-DUTY HANDHOLE	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	464
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	115
DRILL EXISTING HANDHOLE	EACH	2
DETECTOR LOOP, TYPE I	FOOT	372
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	464
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	92
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	2
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
RF-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE		QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD	1 OR 3-SECTION	10	11	110
	4-SECTION	-	14	-
	5-SECTION	10	13	130
PROGRAMMABLE	3-SECTION	-	22	-
	4-SECTION	-	32	-
	5-SECTION	-	28	-
PEDESTRIAN SIGNAL		4	15	60
CONTROLLER		1	150	150
MASTER CONTROLLER		-	100	-
UPS		1	25	25
DETECTION	RADAR	2	20	40
BLANK-OUT SIGN		-	25	-
NETWORK SWITCH	II OR III	-	35	-
CELLULAR MODEM		-	15	-
PTZ CAMERA		-	75	-
	TOTAL UPS SIZING			515
UPS CHARGING		1	225	225
BATTERY HEATER MAT		1	180	180
CABINET HEATER		1	200	200
FLASHER		-	15	-
LED STREET NAME SIGN		-	120	-
LUMINAIRE		-	240	-
TOTAL SERVICE WIRE SIZING				1120

ENERGY COSTS TO:

ENERGY SUPPLY: CONTACT: ANTONIO RIOS
PHONE: 630-696-6855
COMPANY: COMED
ACCOUNT NUMBER: -
METER NUMBER: ---



CABLE PLAN

TS 12320
ECON 44

MODEL: APS-04 [Sheet]
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USER NAME = Jacob.Roth	DESIGNED -	REVISED -
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	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT FINLEY ROAD

SCALE: | SHEET 4 OF 8 SHEETS | STA. | TO STA.

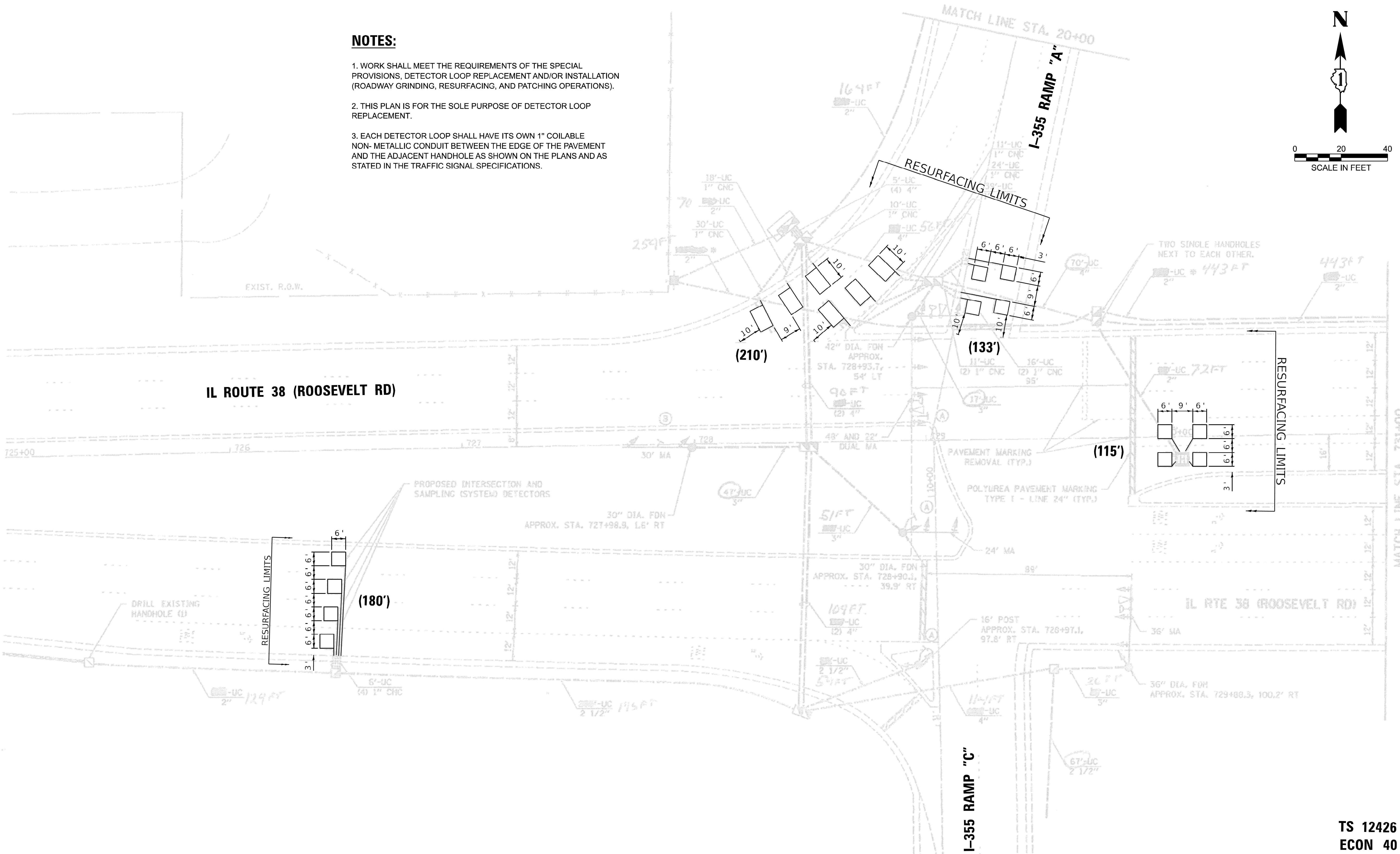
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	20
		CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

3. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON- METALLIC CONDUIT BETWEEN THE EDGE OF THE PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.



TS 12426
ECON 40

USER NAME = Jacob.Roth	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	APS & DETECTOR LOOP PLANS IL 38 (ROOSEVELT ROAD) AT I-355 SB RAMPS "A" AND "C"
	DRAWN -	REVISED -		
	CHECKED -	REVISED -		
PLOT DATE = 12/5/2025	DATE -	REVISED -		SCALE: SHEET 5 OF 8 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	21
CONTRACT NO. 80B15				
ILLINOIS FED. AID PROJECT				

MODEL: APS-05 [Sheet]
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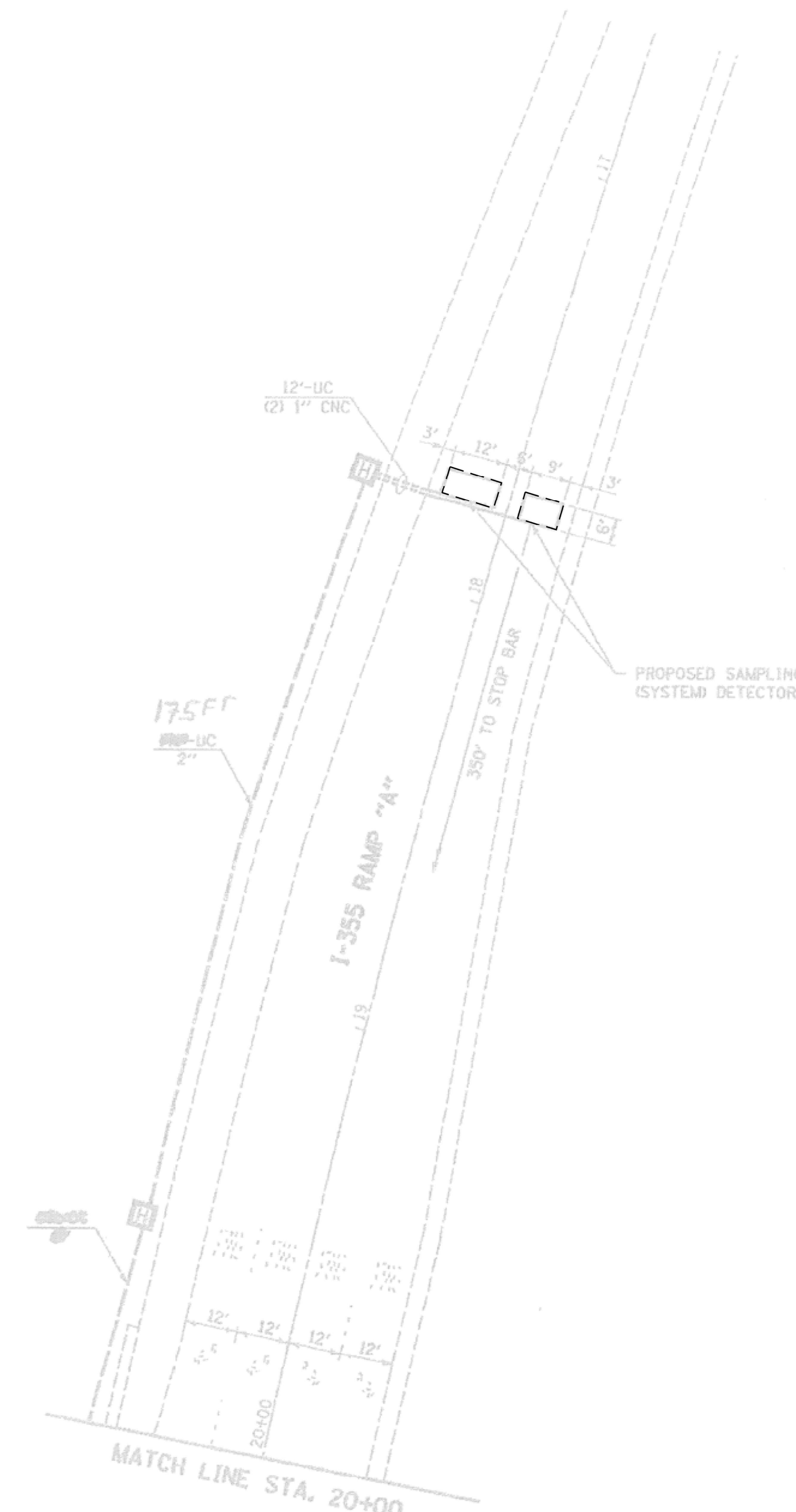

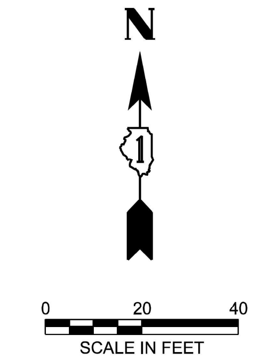

REPLACE ALL DETECTOR LOOP AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
X8860105	DETECTOR LOOP REPLACEMENT	638	FOOT

TS 12426
ECON 40

MODEL: APS-06 [Sheet]
FILE NAME: c:\pw_work\pwidot\rothjp\d1143894\D116825-sht-TS.dgn

USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT I-355 SB RAMPS "A" AND "C"

SCALE: | SHEET 6 OF 8 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	22
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		

N

0 20 40
SCALE IN FEET

I-355 RAMP "D"

I-355 VETERANS MEMORIAL TOLLWAY

** QUANTITY INCLUDED ON IL RTE 38 AT I-355 SB RAMPS "A" AND "C" INTERSECTION.

DRILL EXISTING HANDHOLE (4)

4'-UC (3) 1" CNC

225' TO STOP BAR

RESURFACING LIMITS

(118')

14' POST APPROX. STA. 734+32.6, 120.7' LT

LOCAL AND MASTER CONTROLLER

733+96.38 68.01' LT

36" DIA. FDN APPROX. STA. 733+90.5, 54.1' LT

36' MA

PAVEMENT MARKING REMOVAL (TYP.)

24' MA

16' POST APPROX. STA. 734+79.8, 53.4' LT

250' TO STOP BAR

IL RTE 38 (ROOSEVELT RD)

30" DIA. FDN APPROX. STA. 734+99.3, 3.5' RT

30" DIA. FDN APPROX. STA. 735+97.3, 38.5' RT

30' MA

48' AND 22' DUAL MA

42" DIA. FDN APPROX. STA. 735+07.5, 47.3' RT

MATCH LINE STA. 731+00

MATCH LINE STA. 736+50

RESURFACING LIMITS

(115')

250' TO STOP BAR

(117')

IL ROUTE 38 (ROOSEVELT RD)

(195')

(275')

POLYUREA PAVEMENT MARKING TYPE I - LINE 24" (TYP.)

PAVEMENT MARKING REMOVAL (TYP.)

I-355 RAMP "B"

RESURFACING LIMITS

MATCH LINE STA. 24+00

I-355 VETRANS MEMORIAL TOLLWAY

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

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3. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON- METALLIC CONDUIT BETWEEN THE EDGE OF THE PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

TS 12425
ECON 40

MODEL: APS-07 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\jb\d1143894\D1T6825-sht-TS.dgn

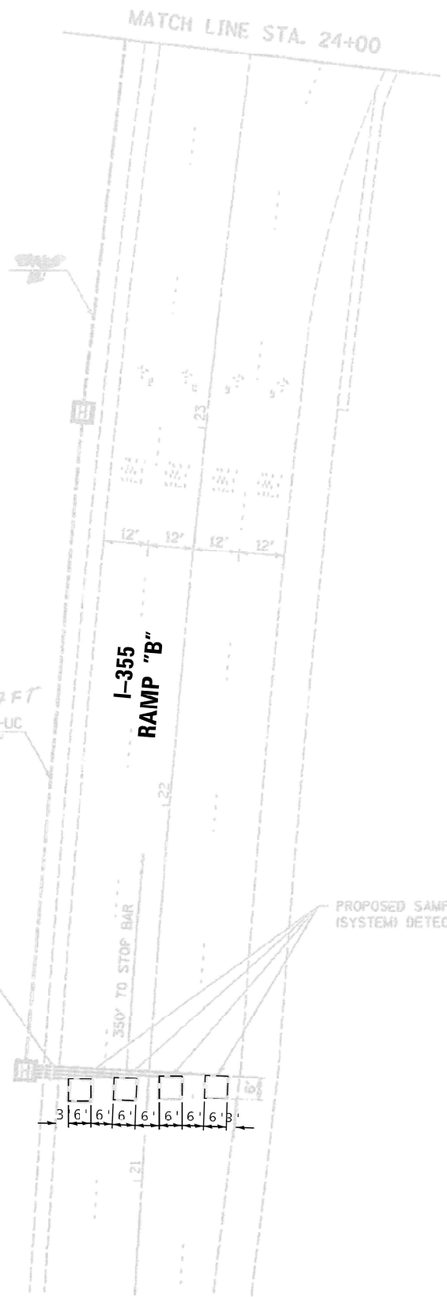
USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE -	REVISED -

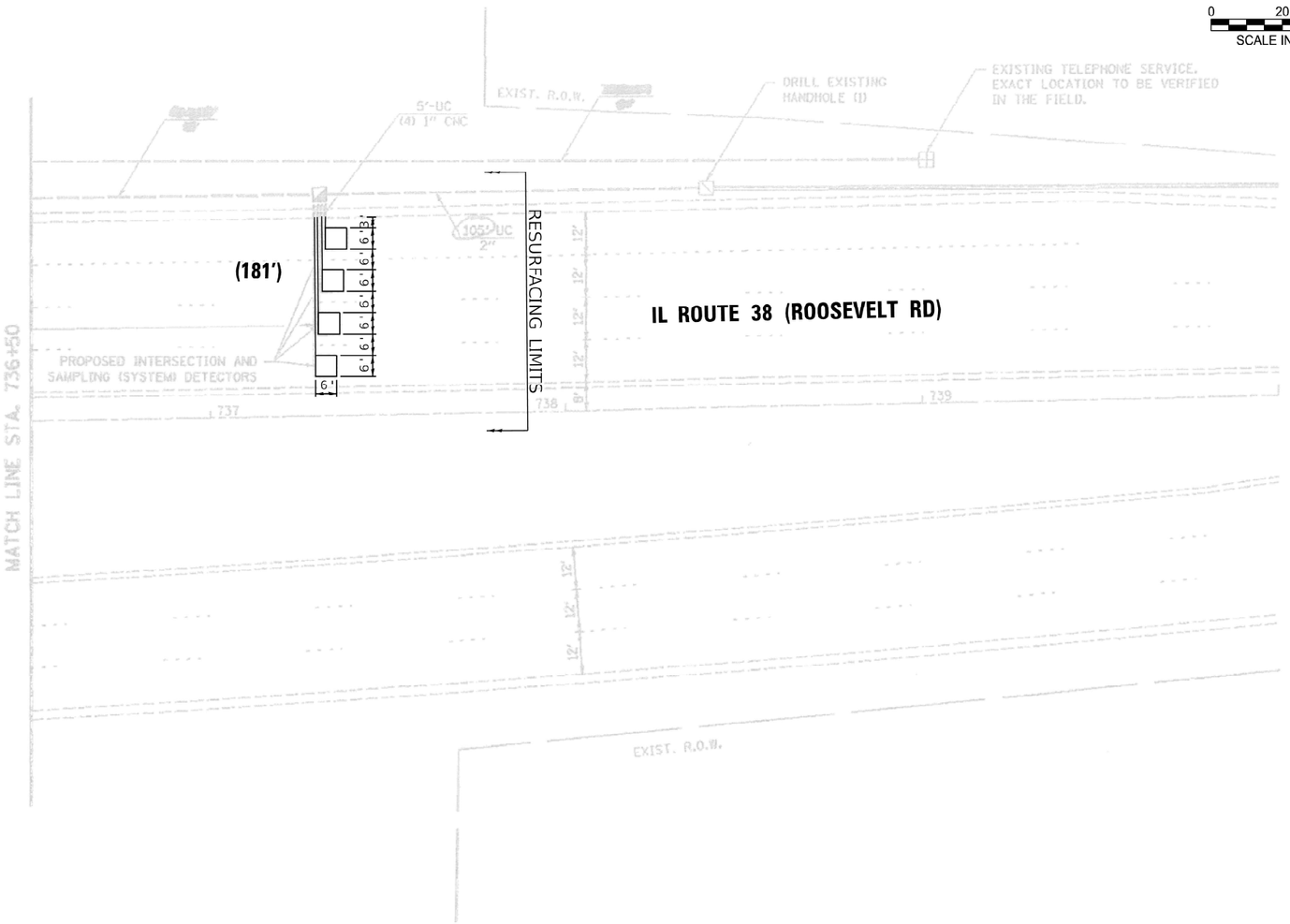
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT I-355 NB RAMPS "B" AND "D"

SCALE: | SHEET 7 OF 8 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	23
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		





REPLACE ALL DETECTOR LOOP AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
X88600600	DETECTOR LOOP REPLACEMENT	1001	FOOT

TS 12425
ECON 40

MODEL: APS-08 [Sheet]
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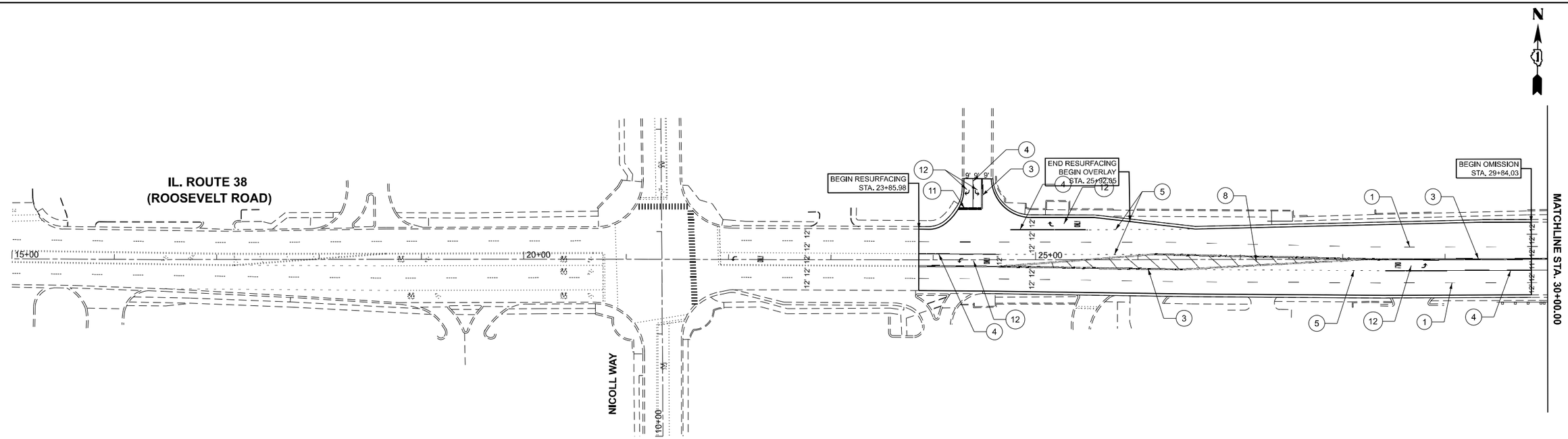
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APS & DETECTOR LOOP PLANS
IL 38 (ROOSEVELT ROAD) AT I-355 NB RAMPS "B" AND "D"

SCALE: | SHEET 8 OF 8 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	24
		CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

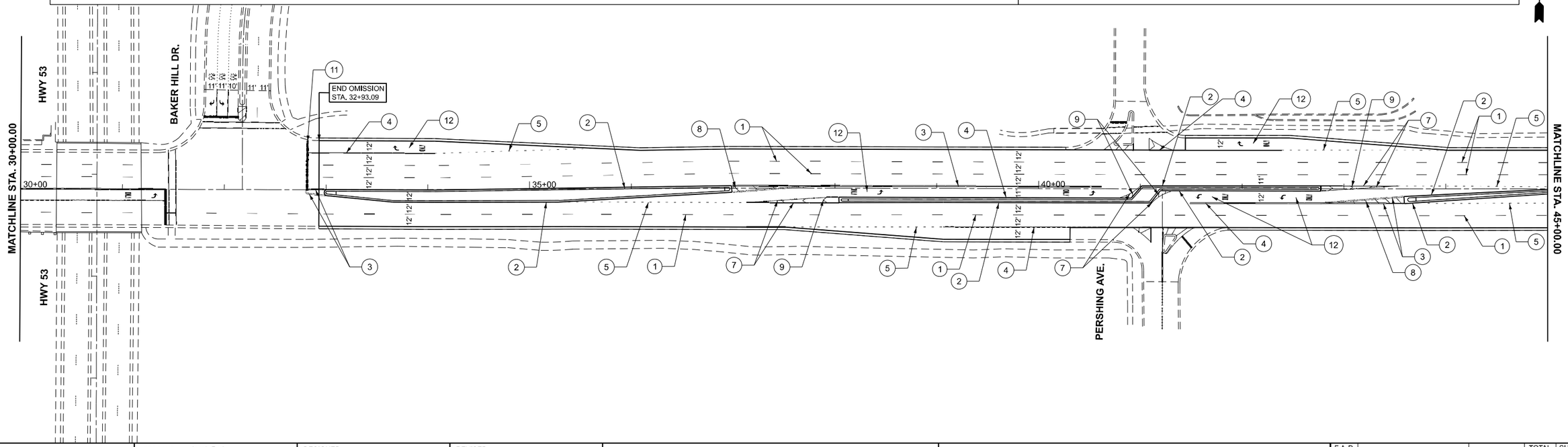


THERMOPLASTIC PAVEMENT MARKING LEGEND

1. PROP. THERMOPLASTIC PVMT. MARKING, 4" 10' DASH, 30' SKIP, WHITE (TYP.)
2. PROP. THERMOPLASTIC PVMT. MARKING, 4" SOLID LINE, YELLOW (TYP.)
3. PROP. THERMOPLASTIC PVMT. MARKING, 4" DOUBLE YELLOW @ 11" C-C (TYP.)
4. PROP. THERMOPLASTIC PVMT. MARKING, 6" TURN LANE LINE, WHITE (TYP.)
5. PROP. THERMOPLASTIC PVMT. MARKING, 6" 2' DASH, 6' SKIP, WHITE (TYP.)
6. PROP. THERMOPLASTIC PVMT. MARKING, 6" CROSSWALK @ 6' C-C, WHITE (TYP.)
7. PROP. THERMOPLASTIC PVMT. MARKING, 8" SOLID LINE, GORE, WHITE (TYP.)
8. PROP. THERMOPLASTIC PVMT. MARKING, 12" MEDIAN @ 45°, YELLOW (TYP.) (5 MINIMUM)
9. PROP. THERMOPLASTIC PVMT. MARKING, 12" MEDIAN @ 45°, WHITE (TYP.) (5 MINIMUM)
10. PROP. THERMOPLASTIC PVMT. MARKING, 12" CROSSWALK @ 90°, 2' C-C, WHITE (TYP.)
11. PROP. THERMOPLASTIC PVMT. MARKING, 24" STOP BAR, WHITE (TYP.)
12. PROP. THERMOPLASTIC PVMT. MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)

MODIFIED URETHANE PAVEMENT MARKING LEGEND

- (A) PROP. MODIFIED URETHANE PVMT. MARKING, 4" 10' DASH, 30' SKIP, WHITE (TYP.)
- (B) PROP. MODIFIED URETHANE PVMT. MARKING, 4" SOLID LINE, YELLOW (TYP.)
- (C) PROP. MODIFIED URETHANE PVMT. MARKING, 6" TURN LANE LINE, WHITE (TYP.)
- (D) PROP. MODIFIED URETHANE PVMT. MARKING, 8" SOLID LINE, GORE, WHITE (TYP.)
- (E) PROP. MODIFIED URETHANE PVMT. MARKING, 12" MEDIAN @ 45°, WHITE (TYP.) (20' C-C)
- (F) PROP. MODIFIED URETHANE PVMT. MARKING, 24" STOP BAR, WHITE (TYP.)
- (G) PROP. MODIFIED URETHANE PVMT. MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)



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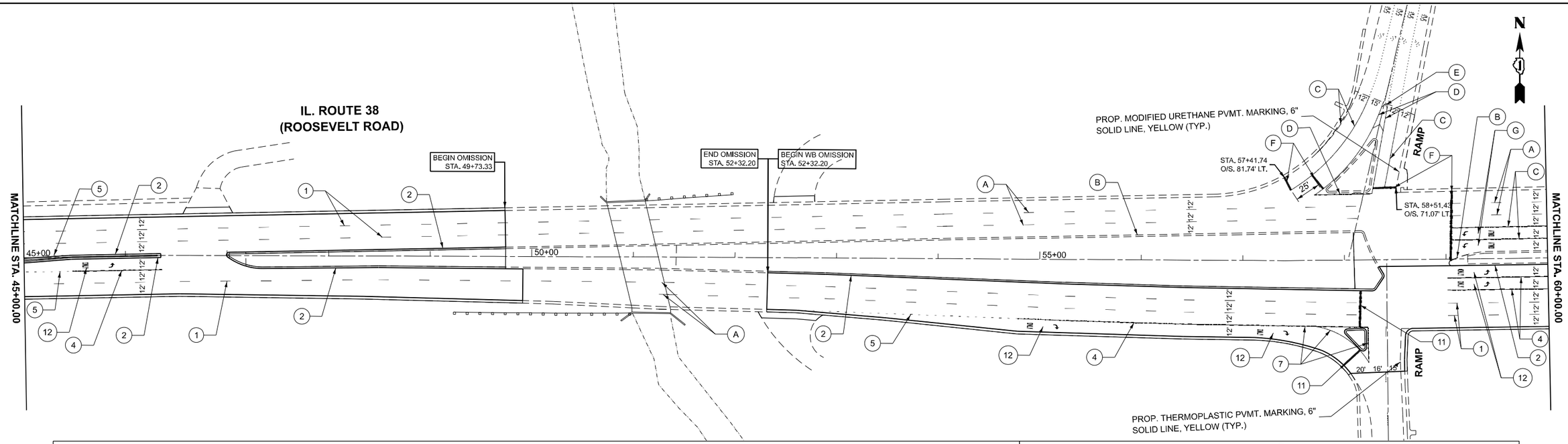
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 1 OF 4 SHEETS | STA. 15+00.00 TO STA. 45+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	25
		CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		



THERMOPLASTIC PAVEMENT MARKING LEGEND

1. PROP. THERMOPLASTIC PVMT. MARKING, 4" 10' DASH, 30' SKIP, WHITE (TYP.)
2. PROP. THERMOPLASTIC PVMT. MARKING, 4" SOLID LINE, YELLOW (TYP.)
3. PROP. THERMOPLASTIC PVMT. MARKING, 4" DOUBLE YELLOW @ 11" C-C (TYP.)
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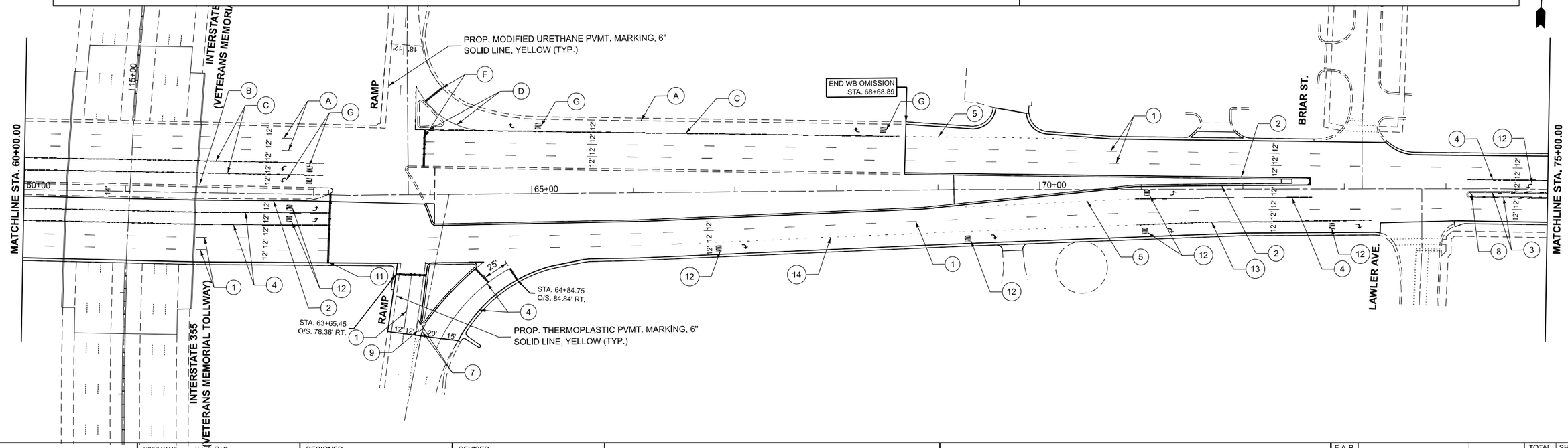
12. PROP. THERMOPLASTIC PVMT. MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)

MODIFIED URETHANE PAVEMENT MARKING LEGEND

- A. PROP. MODIFIED URETHANE PVMT. MARKING, 4" 10' DASH, 30' SKIP, WHITE (TYP.)
- B. PROP. MODIFIED URETHANE PVMT. MARKING, 4" SOLID LINE, YELLOW (TYP.)
- C. PROP. MODIFIED URETHANE PVMT. MARKING, 6" TURN LANE LINE, WHITE (TYP.)
- D. PROP. MODIFIED URETHANE PVMT. MARKING, 8" SOLID LINE, GORE, WHITE (TYP.)
- E. PROP. MODIFIED URETHANE PVMT. MARKING, 12" MEDIAN @ 45°, WHITE (TYP.) (20' C-C)
- F. PROP. MODIFIED URETHANE PVMT. MARKING, 24" STOP BAR, WHITE (TYP.)
- G. PROP. MODIFIED URETHANE PVMT. MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)



USER NAME = Jacob.Roth	DESIGNED -	REVISED -	**STATE OF ILLINOIS**
	DRAWN -	REVISED -	**DEPARTMENT OF TRANSPORTATION**
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

PAVEMENT MARKING PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 2 OF 4 SHEETS | STA. 45+00.00 | TO STA. 75+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	26
		CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		

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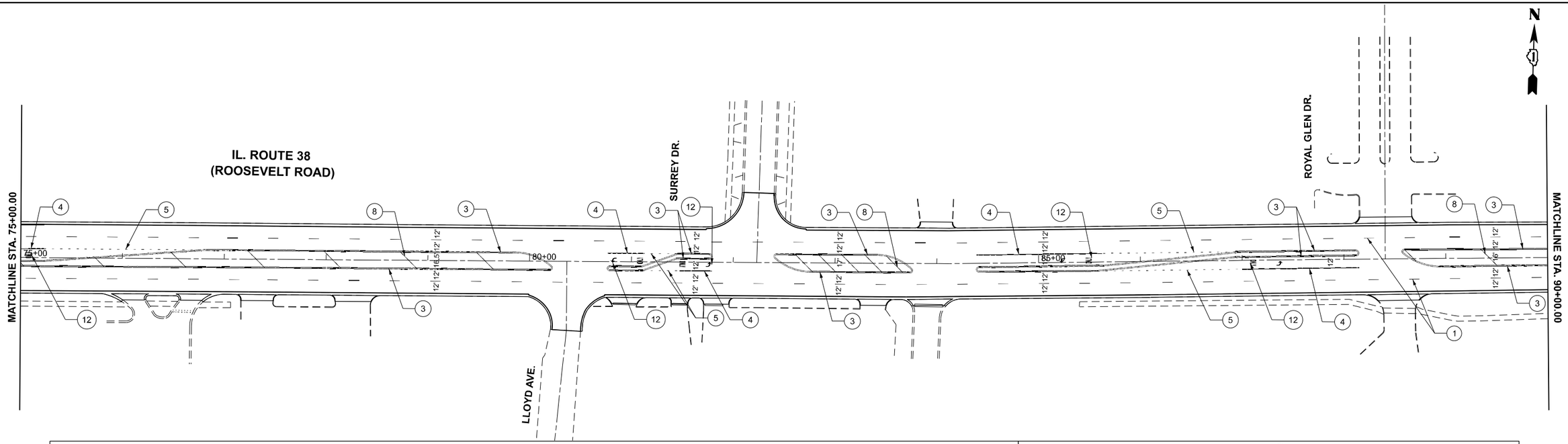

THERMOPLASTIC PAVEMENT MARKING LEGEND

1. PROP. THERMOPLASTIC PVMT. MARKING, 4" 10' DASH, 30' SKIP, WHITE (TYP.)
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MODIFIED URETHANE PAVEMENT MARKING LEGEND

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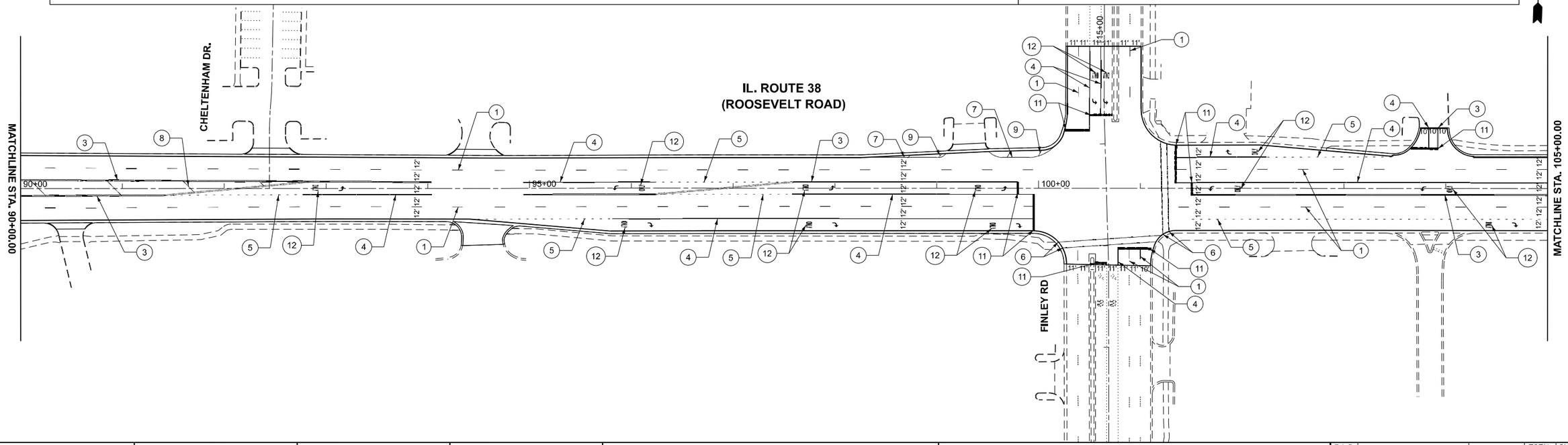

USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 3 OF 4 SHEETS | STA. 75+00.00 | TO STA. 105+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	27
		CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		

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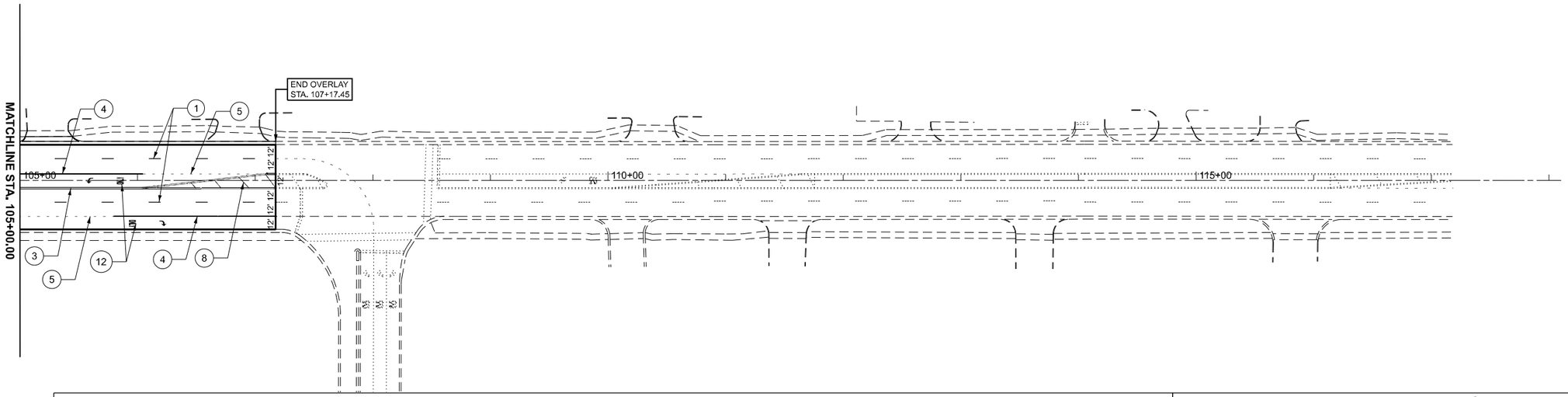

THERMOPLASTIC PAVEMENT MARKING LEGEND

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- (E) PROP. MODIFIED URETHANE PVMT. MARKING, 12" MEDIAN @ 45°, WHITE (TYP.) (20' C-C)
- (F) PROP. MODIFIED URETHANE PVMT. MARKING, 24" STOP BAR, WHITE (TYP.)
- (G) PROP. MODIFIED URETHANE PVMT. MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)

MODEL: IL 38 - PMK 7 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\pid1143894\D116825-sht-pmk.dgn

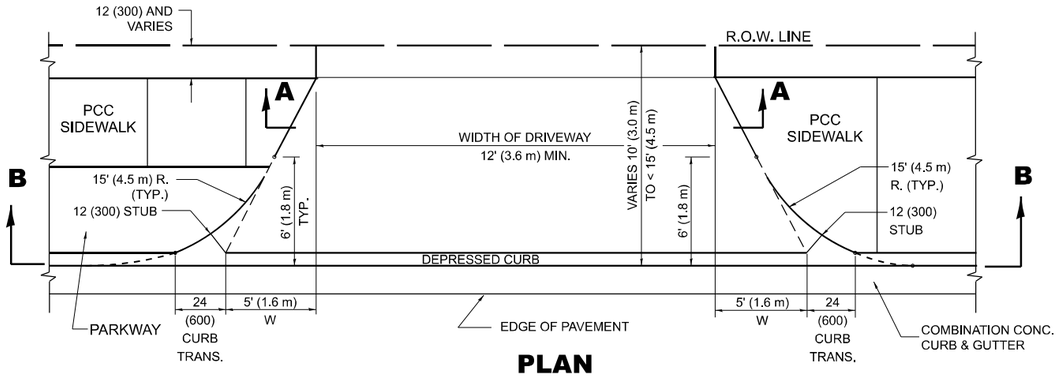
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 1/16/2026	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

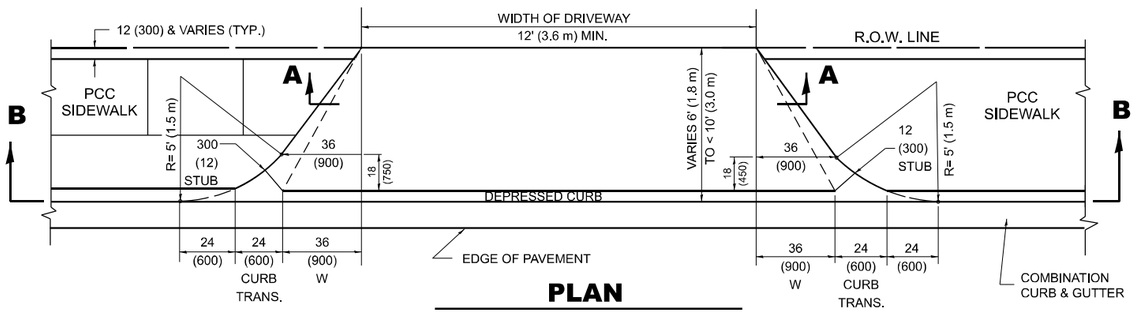
PAVEMENT MARKING PLANS
IL 38 - EAST OF NICOLL WAY TO EDSON AVE

SCALE: 1"=50' | SHEET 4 OF 4 SHEETS | STA. 105+00.00 | TO STA. 120+00.00

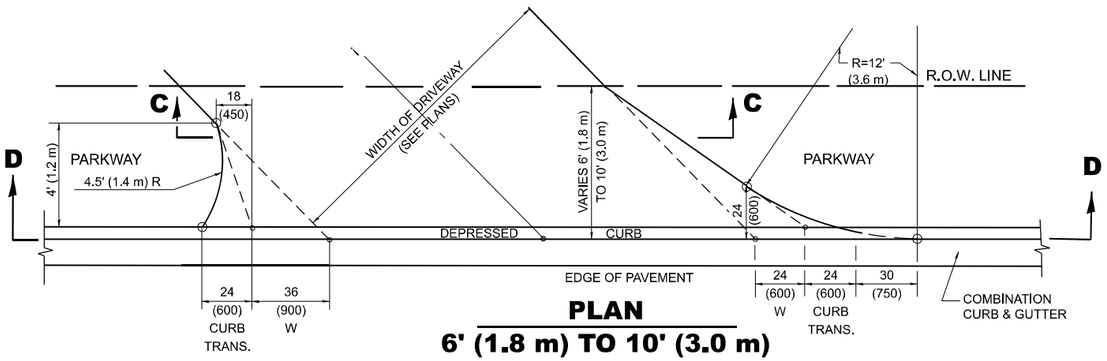
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	28
		CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		



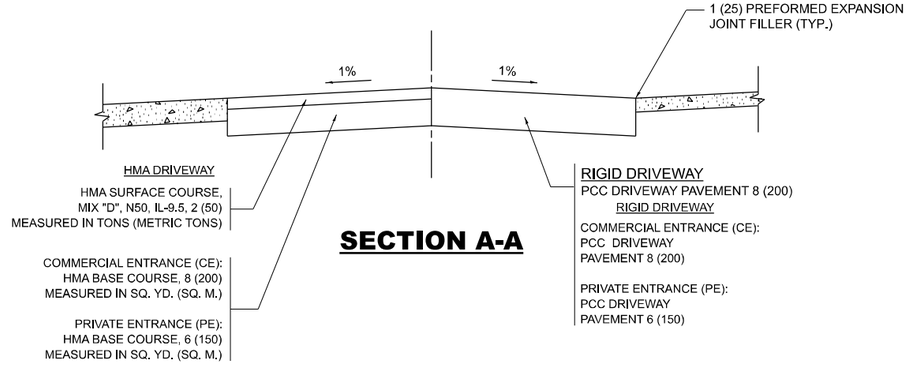
PLAN
10' (3.0 m) TO < 15' (4.5 m)



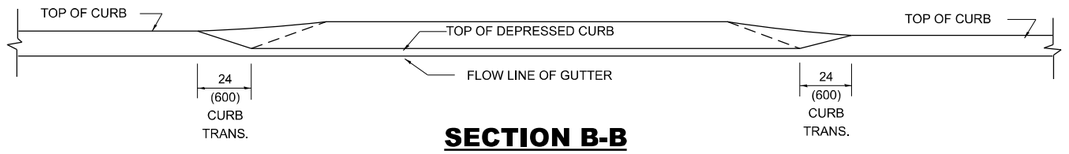
PLAN
6' (1.8 m) TO < 10' (3.0 m)



PLAN
6' (1.8 m) TO 10' (3.0 m)



SECTION A-A



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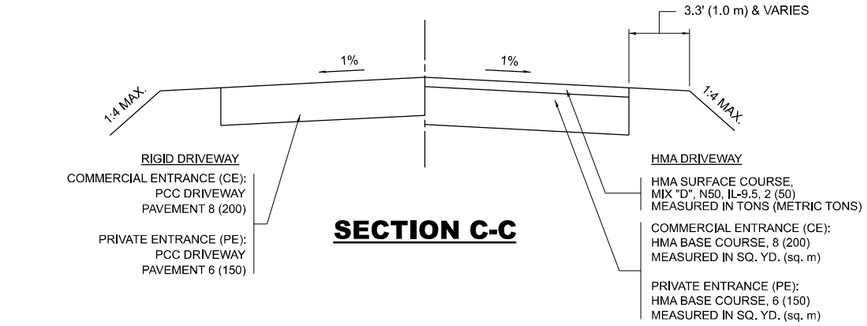

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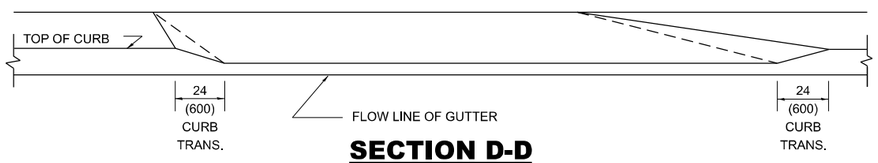


SECTION D-D

GENERAL NOTES

1. DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.
2. WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE PCC SIDEWALK SHALL EXTEND TO THE BACK OF CURB.
3. "W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

MODEL: BD-02 [Sheet]
FILE NAME: c:\pw_work\pwdot\roth\pd1143894\D1116825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	
	DRAWN -	REVISED - R. BORO 09-06-11	
	CHECKED -	REVISED - K. SMITH 08-27-19	
PLOT DATE = 12/5/2025	DATE - 11-06-95	REVISED - K. SMITH 11-18-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

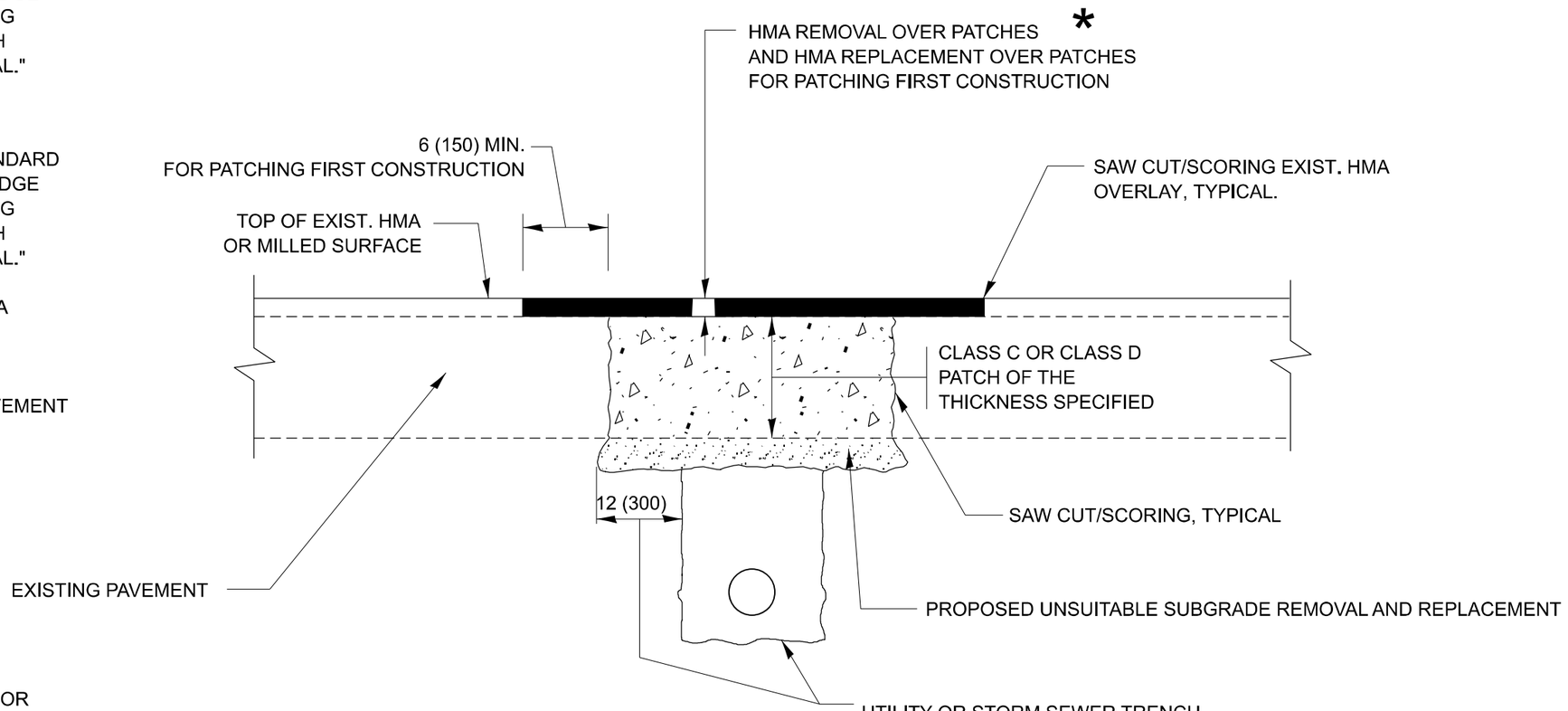
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	29
BD400-01 (BD-02)		CONTRACT NO. 80B15		
ILLINOIS	FED. AID PROJECT			

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 ½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: BD-22 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\pid1143894\D116825-sht-DistStds.dgn

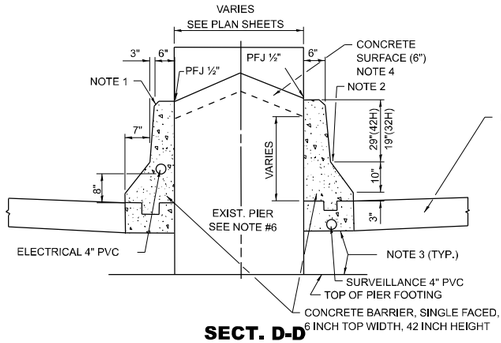
USER NAME = Jacob.Roth	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
	DRAWN -	REVISED - R. BORO 09-04-07	
	CHECKED -	REVISED - K. ENG 10-27-08	
PLOT DATE = 12/5/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22	

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

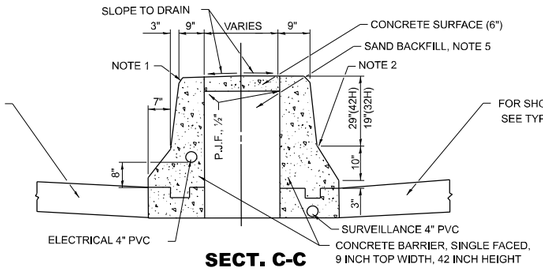
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	30
	BD400-04 (BD-22)	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

TANGENT CONDITION



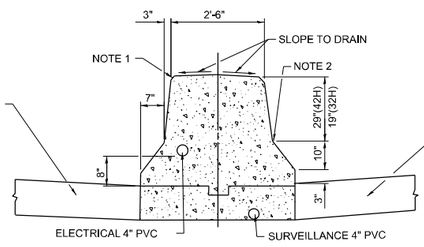
SECT. D-D

CONCRETE BARRIER



SECT. C-C

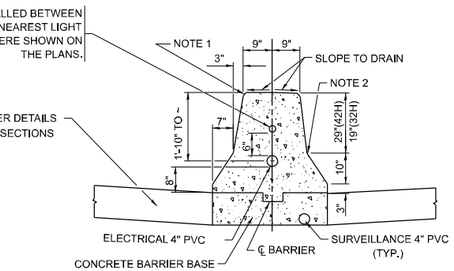
CONCRETE BARRIER



SECT. B-B

CONCRETE BARRIER

CONCRETE BARRIER, DOUBLE FACED
18 INCH VARIABLE TOP WIDTH
42 INCH HEIGHT

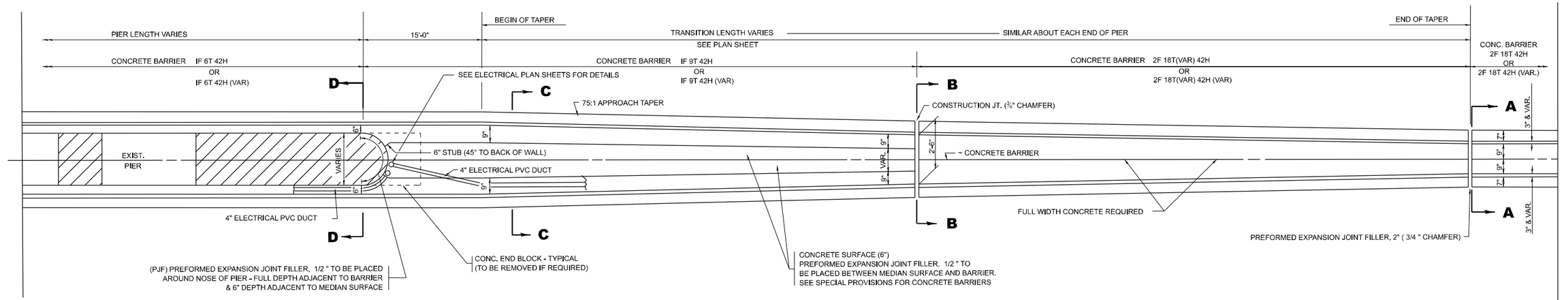


SECT. A-A

CONCRETE BARRIER

CONCRETE BARRIER, DOUBLE FACE,
18 INCH TOP WIDTH,
42 INCH HEIGHT

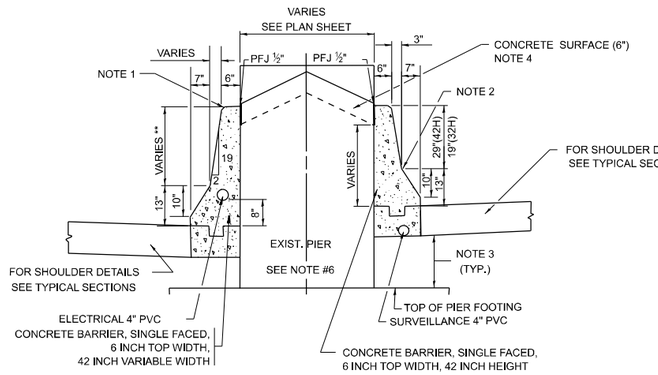
PLAN VIEW OF
CONCRETE BARRIER TRANSITION



(SUPER ELEVATION CONDITION)

SECT. D-D

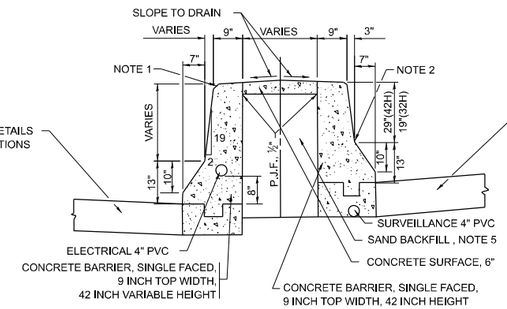
CONCRETE BARRIER



AROUND NOSE OF PIER - FULL DEPTH ADJACENT TO BARRIER
& 6" DEPTH ADJACENT TO MEDIAN SURFACE

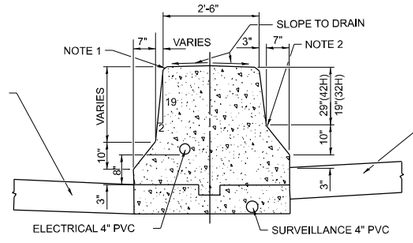
SECT.C-C

CONCRETE BARRIER



SECT. B-B

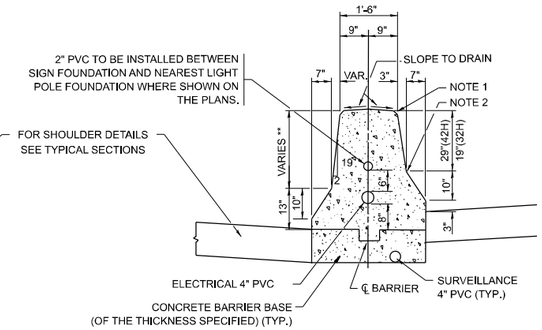
CONCRETE BARRIER



CONCRETE BARRIER, DOUBLE FACE,
18 INCH VARIABLE TOP WIDTH,
42 INCH VARIABLE HEIGHT

SECT. A-A

CONCRETE BARRIER



CONCRETE BARRIER, DOUBLE FACE,
18 INCH TOP WIDTH,
42 INCH VARIABLE HEIGHT

GENERAL NOTES

1. FOR UNDERDRAIN DETAILS SEE TYPICAL SECTIONS.
2. FOR KEYWAY (F) DIMENSIONS, SEE TYPICAL SECTIONS.
3. IF USING JOINTED CONCRETE BARRIER BASE, CONTRACTORS WILL HAVE THE OPTION OF USING A KEYWAY OR # TIE BARS AT O.C. ____
4. MAINTAIN SLOPE OF FACE AS SHOWN ON DETAIL. HEIGHT AND WIDTH OF BARRIER INCREASE WHERE A DIFFERENCE IN MEDIAN EDGE-OF-PAVEMENT GRADE ELEVATION EXISTS.

NOTES:

1. 3/4 " CHAMFER OR 1" RADIUS (OPTIONAL)
2. 10" RADIUS (OPTIONAL)
3. EXTEND BOTTOM OF BARRIER TO FOOTING ONLY WHEN DEPTH IS 6" OR LESS, OTHERWISE MAINTAIN SAME DEPTH AS BOTTOM OF SHOULDER
4. PIER FILLER MATERIAL TO BE CONCRETE IF MINIMUM 6" THICKNESS WILL BE MAINTAINED. IF 6" THICKNESS CANNOT BE MAINTAINED USE ASPHALT FILLER MATERIAL AS DIRECTED BY THE ENGINEER.
5. SAND BACKFILL AND CONCRETE SURFACE WILL BE REQUIRED. FILLING WITH CONCRETE WILL NOT BE ALLOWED.
6. IF PIER IS NEW CONSTRUCTION BARRIER WALL MAY BE MONOLITHIC

BASIS OF PAYMENT

1. COST OF SAND BACKFILL, CONCRETE SURFACE (6"), AND PIER FILLER MATERIAL WILL BE PAID FOR SEPERATELY.
2. PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE UNIT COST FOR THE CONCRETE BARRIER OF THE TYPE INVOLVED.
3. CONCRETE BARRIER BASE PAY ITEM IS TO BE INCLUDED IF THE BARRIER IS CONSTRUCTED MONOLITHIC OR JOINTED TO BASE. # TIE BARS AT O.C. ____
4. REMOVAL OF CONCRETE BLOCK SHALL BE INCLUDED IN THE UNIT COST OF CONCRETE BARRIER.

USER NAME = Jacob.Roth	DESIGNED - FORD	REVISED - FORD 12-06-88	
	DRAWN -	REVISED - K. SMITH 02-01-22	
	CHECKED -	REVISED - K. SMITH 11-18-22	
PLOT DATE = 12/5/2025	DATE - 09-09-88	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONCRETE BARRIER TRANSITION &
GENERAL DETAILS, CONCRETE BARRIER BASE

SCALE: | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	31
	BD-27	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

MODEL: BD-27 [Sheet]
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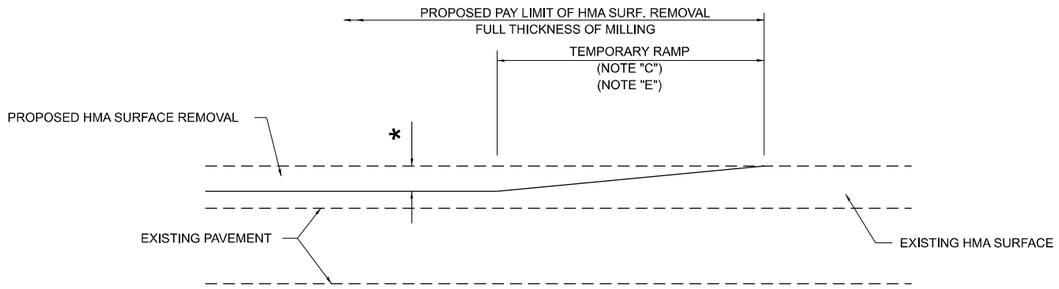
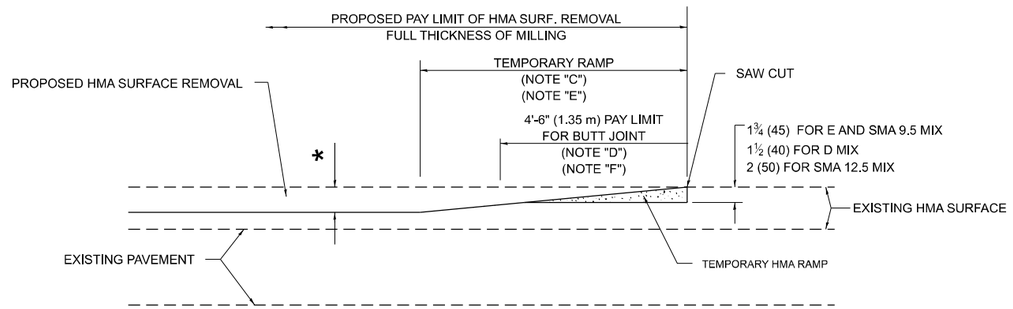


MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

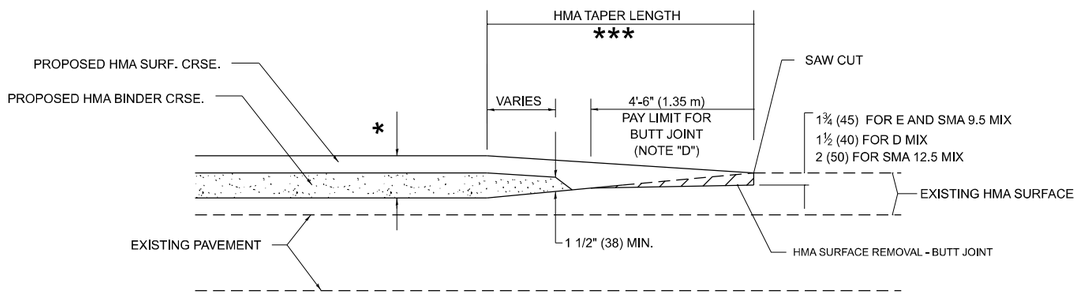


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

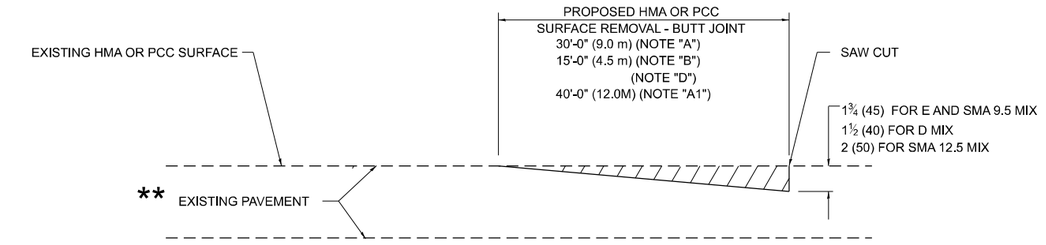
OPTION 2

TYPICAL TEMPORARY RAMP

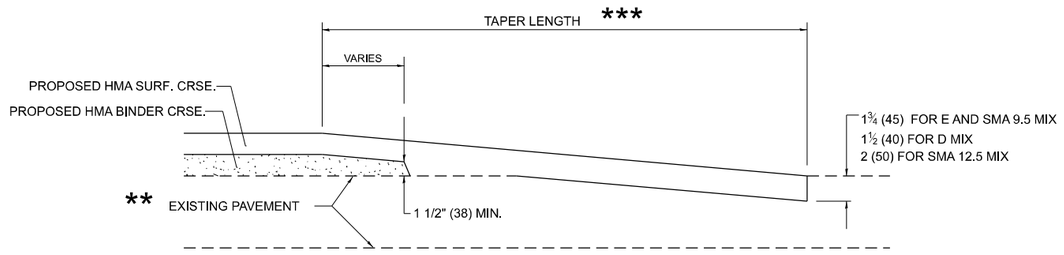


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.

A1. INTERSTATES

B. MINOR SIDE ROADS.

C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
   * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
   *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
   10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

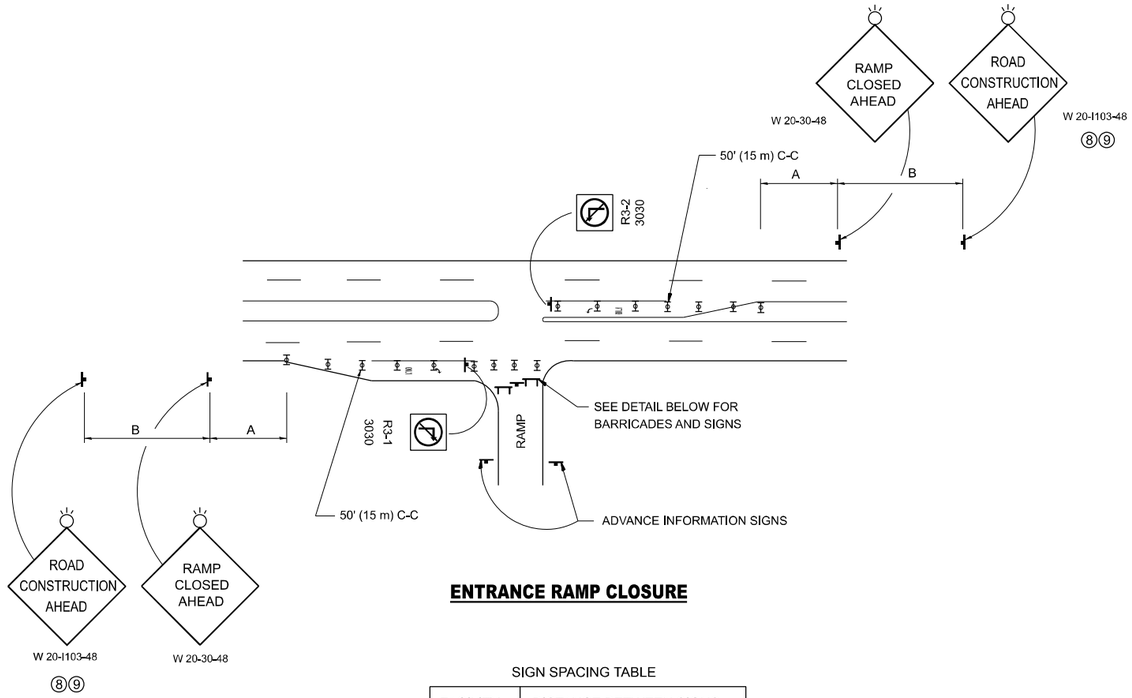
2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: BD-32 [Sheet]
FILE NAME: c:\pw_work\pwidot\rothjo\d1143894\D116825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS	
	DRAWN -	REVISED - M. GOMEZ 04-06-01			
	CHECKED -	REVISED - R. BORO 01-01-07			
PLOT DATE = 12/5/2025	DATE - 06-13-90	REVISED - K. SMITH 11-18-22		SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.	

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	32
	BD400-05 BD-32	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

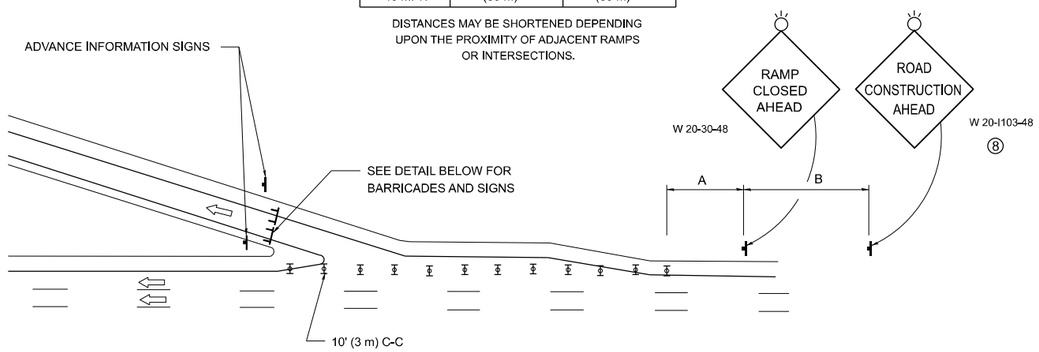


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

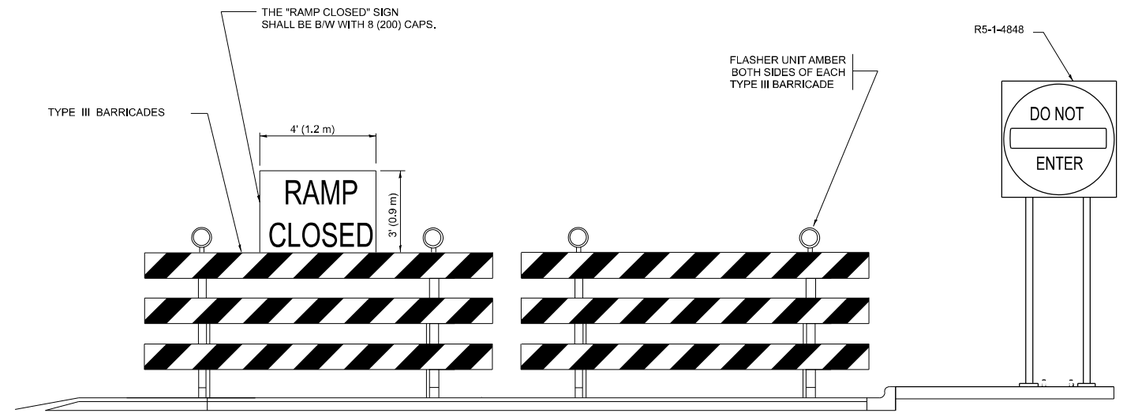


EXIT RAMP CLOSURE

SYMBOLS

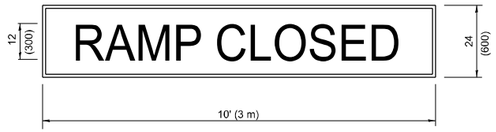
TYPE II BARRICADE OR DRUM

TYPE III BARRICADE WITH 2 FLASHING LIGHTS



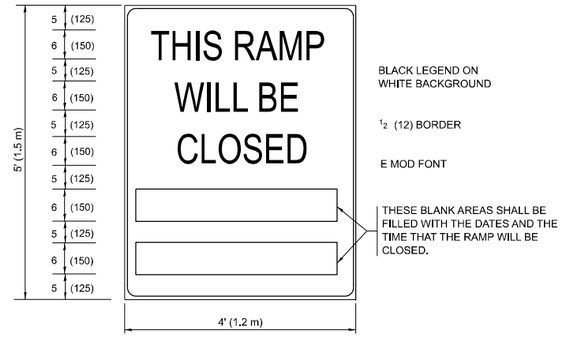
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

½ (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

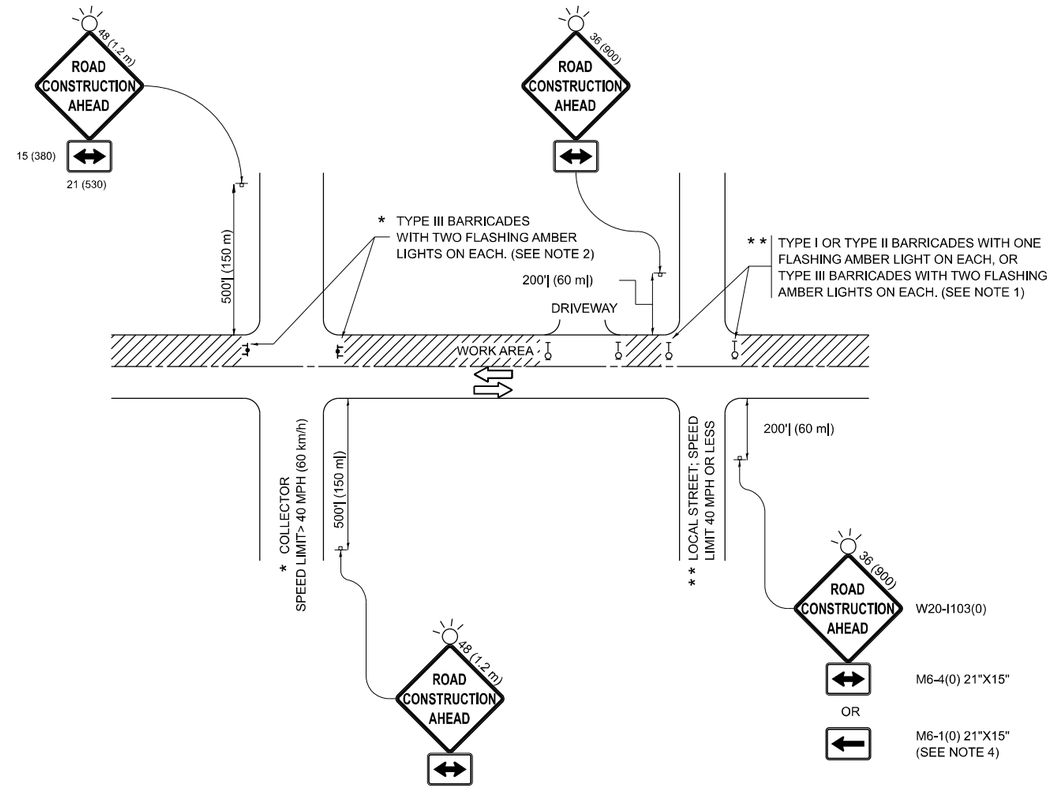
1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
2. VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEEDED BY A W20-7 FLAGGER WARNING SIGN.
4. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
5. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
7. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH
8. ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
9. ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: TC-08 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\pd1143894\D116825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED - D.W.S.	REVISED - S.P.B._12-09	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**ENTRANCE AND EXIT_RAMP CLOSURE DETAILS**	
	DRAWN -	REVISED - M.D._06-13			
	CHECKED -	REVISED - M.D._01-18			
PLOT DATE = 12/5/2025	DATE - 02-83	REVISED - D.S._05-24		SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.	

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	33
	TC-08	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-10 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\pjd1143894\D1116825-sht-DistStds.dgn

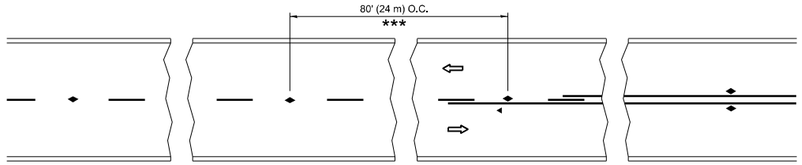
USER NAME = Jacob.Roth	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00	
	DRAWN -	REVISED - A. SCHUETZE 07-01-13	
	CHECKED -	REVISED - A. SCHUETZE 09-15-06	
PLOT DATE = 12/5/2025	DATE - 06-89	REVISED - D. SENDERAK 05-03-24	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: | SHEET OF SHEETS | STA. | TO STA.

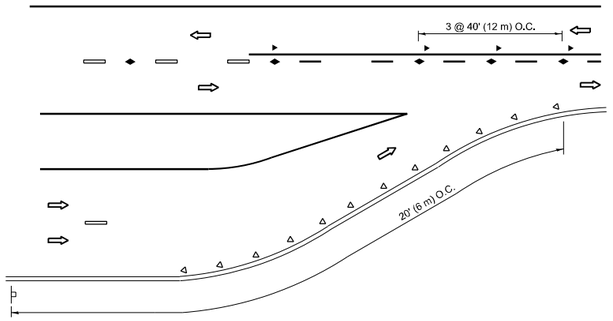
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	34
	TC-10	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



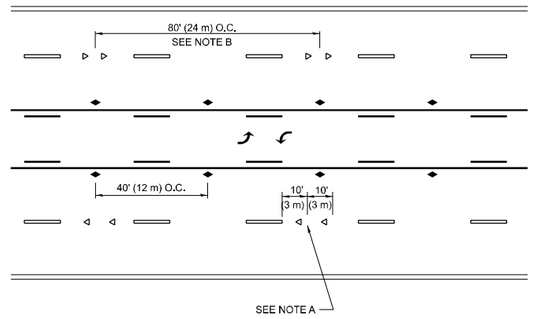
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

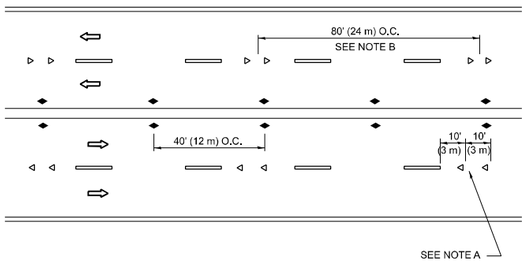
SEE FIGURE 3B-14 MUTCD



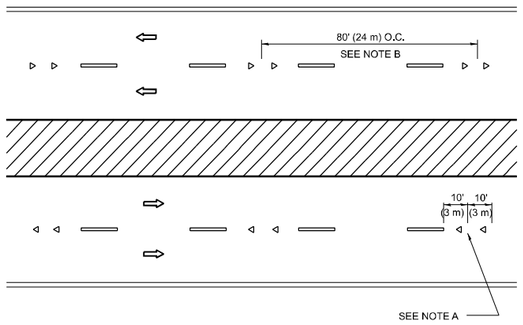
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

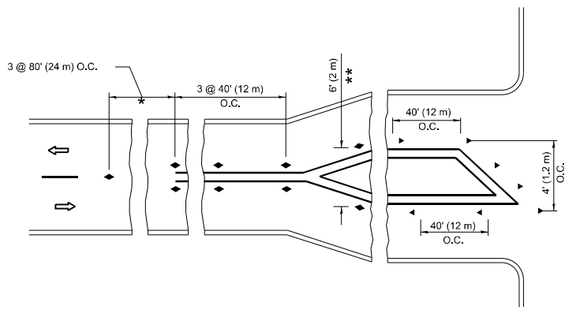
LANE MARKER NOTES

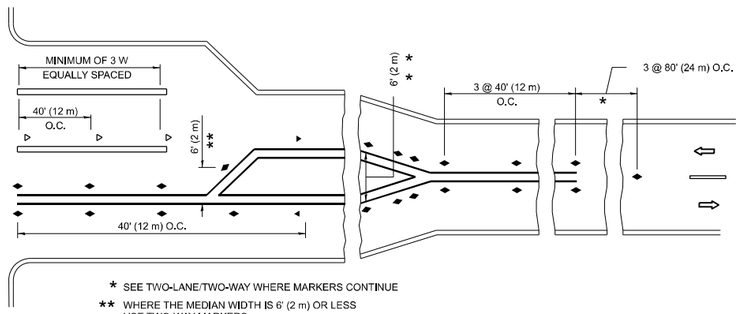
A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.





* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-11 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\pd1143994\D1116825-sht-DistStds.dgn

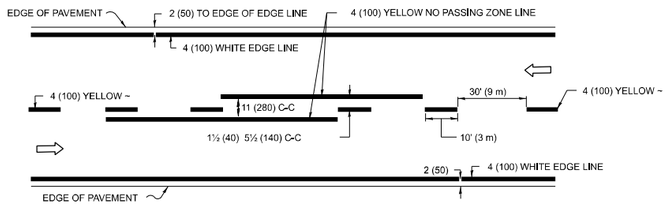
USER NAME = Jacob.Roth	DESIGNED -	REVISED - T. RAMMACHER 03-12-99	
	DRAWN -	REVISED - T. RAMMACHER 01-06-00	
	CHECKED -	REVISED - C. JUCIUS 09-09-09	
PLOT DATE = 12/5/2025	DATE -	REVISED - C. JUCIUS 07-01-13	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

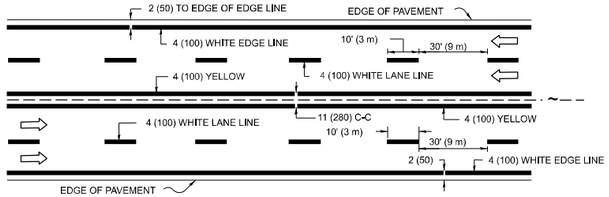
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

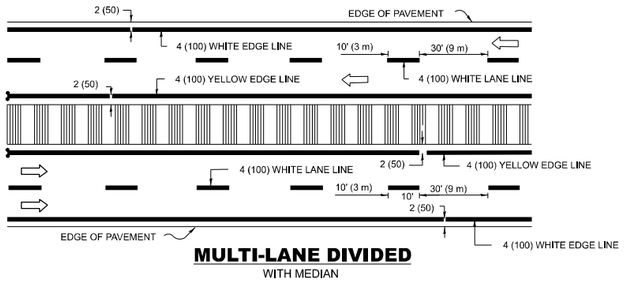
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	35
	TC-11	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



2-LANE ROADWAY

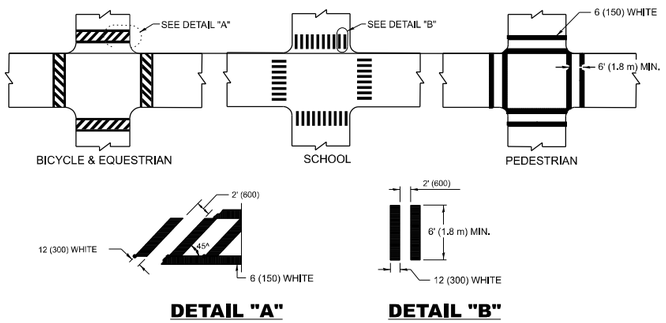


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED
WITH MEDIAN

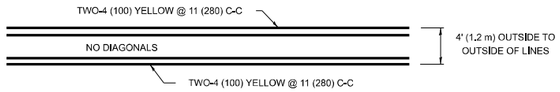
TYPICAL LANE AND EDGE LINE MARKING



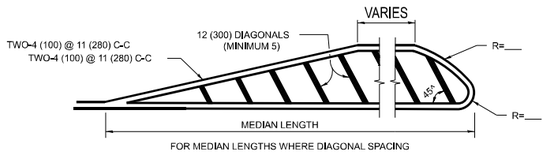
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



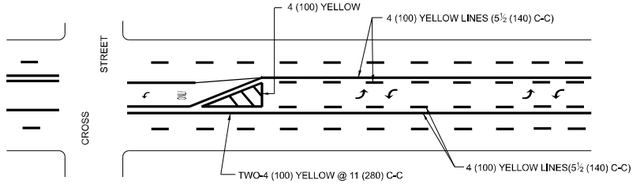
4' (1.2 m) WIDE MEDIANS ONLY



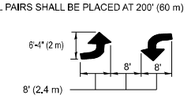
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

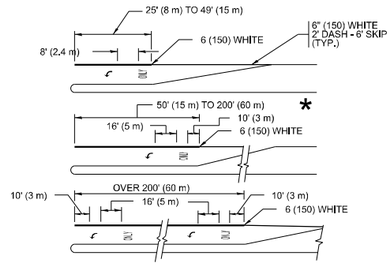


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

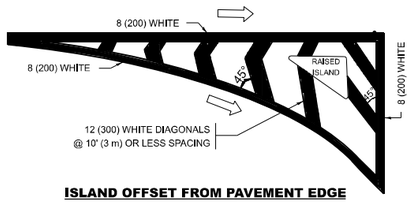


FULL SIZE LETTERS 8' (2,4 m) AND ARROWS SHALL BE USED.
AREA = 15,6 SQ. FT. (1,5 m²) **ONLY** AREA = 20,8 SQ. FT. (1,9 m²)

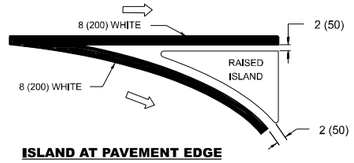
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

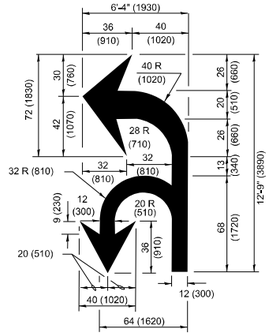


ISLAND OFFSET FROM PAVEMENT EDGE

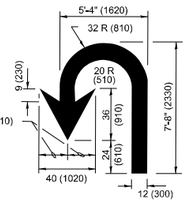


ISLAND AT PAVEMENT EDGE

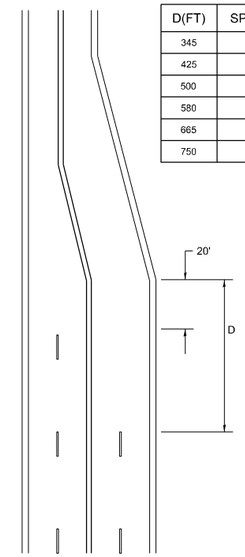
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN



LANE REDUCTION TRANSITION

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2,4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2,4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1,2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1,2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1,8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3,6 SQ. FT. (0,33 m²) EACH "X"=54,0 SQ. FT. (5,0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16,3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30,4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-13 [Sheet]
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USER NAME = Jacob.Roth	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09	
	DRAWN -	REVISED - C. JUCIUS 07-01-13	
	CHECKED -	REVISED - C. JUCIUS 12-21-15	
PLOT DATE = 12/5/2025	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	36
	TC-13	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

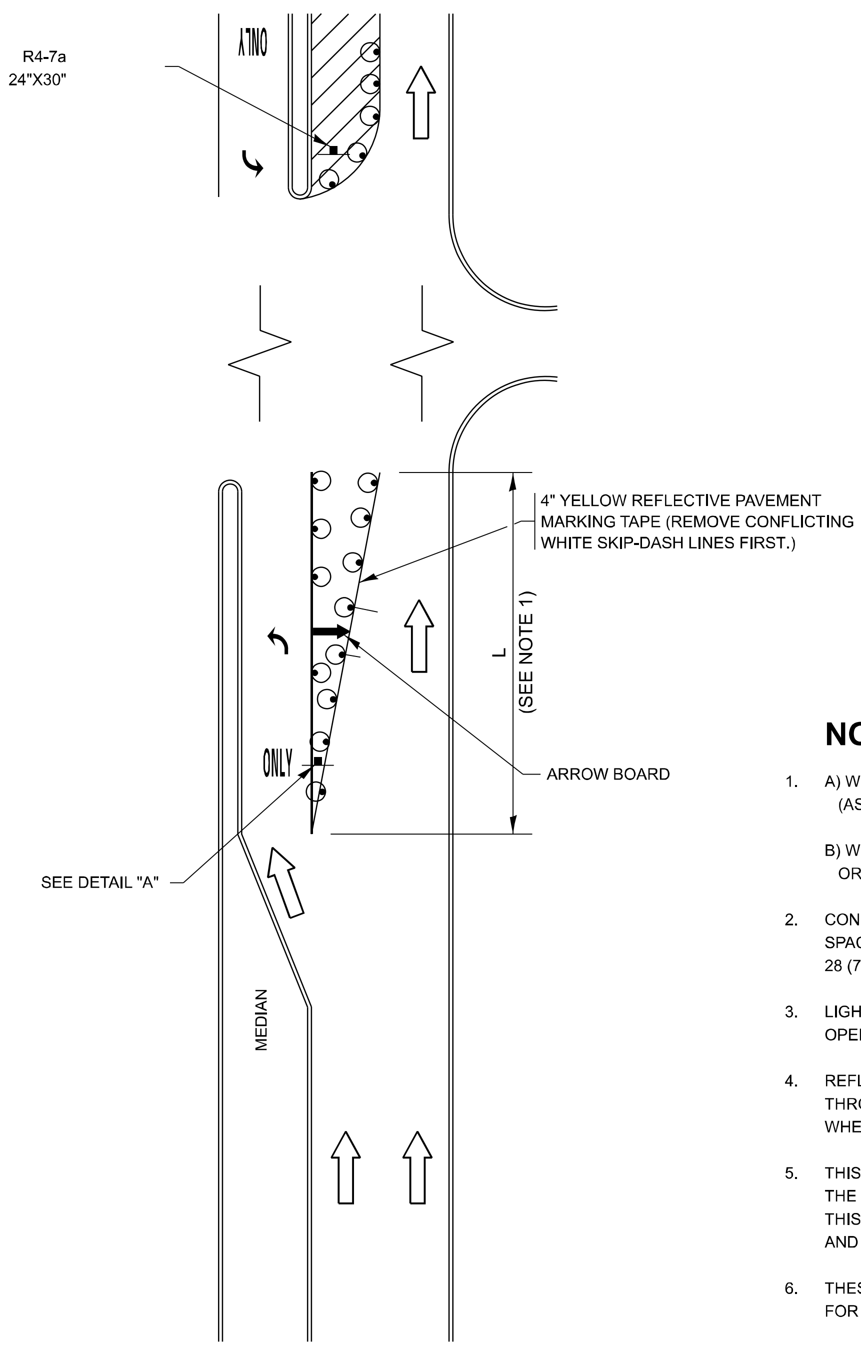


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

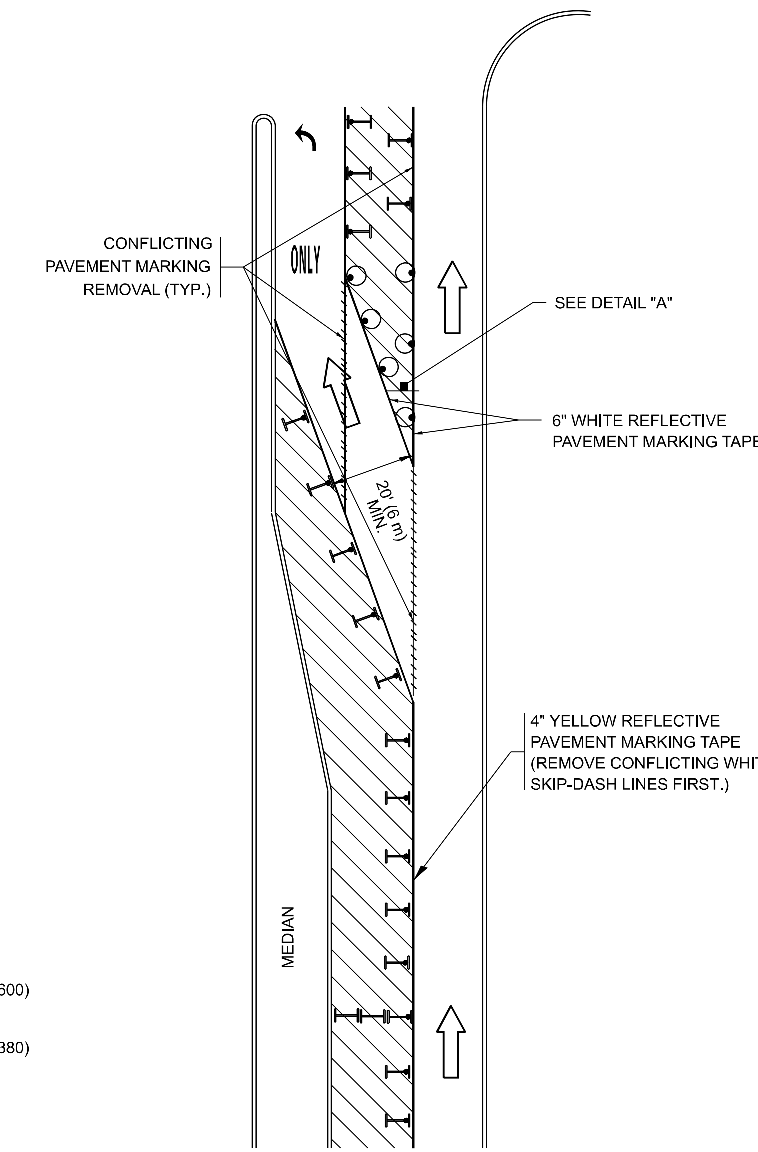


FIGURE 2

LEGEND

(hatched box)	WORK AREA
(open arrow)	LANE OPEN TO TRAFFIC
(solid arrow)	ARROW BOARD
(symbol)	TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
(symbol)	DRUM WITH STEADY BURN LIGHT
(symbol)	SIGN ASSEMBLY
(symbol)	TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

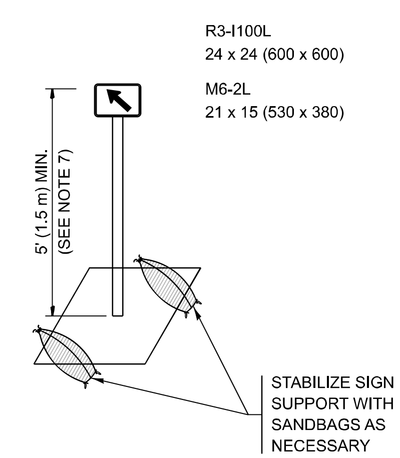
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



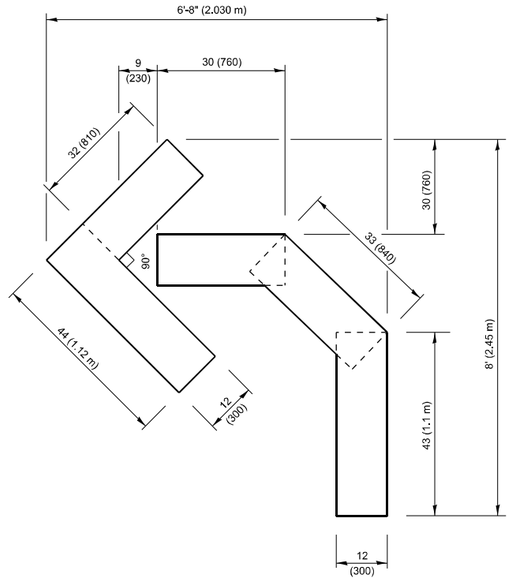
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-14 [Sheet]
FILE NAME: c:\pw_work\pwidot\rothjp\d1143894\D116825-sht-DistStds.dgn

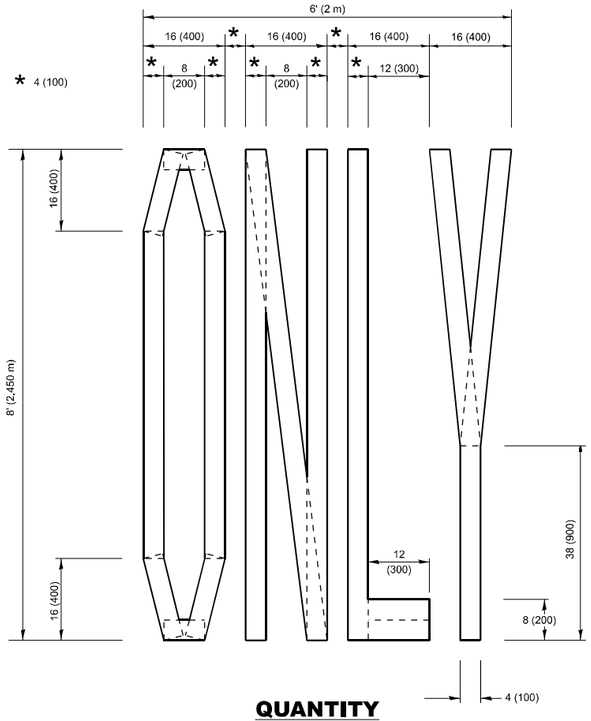
USER NAME = Jacob.Roth	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13		
	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		
PLOT DATE = 12/5/2025	DATE - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	37
	TC-14	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



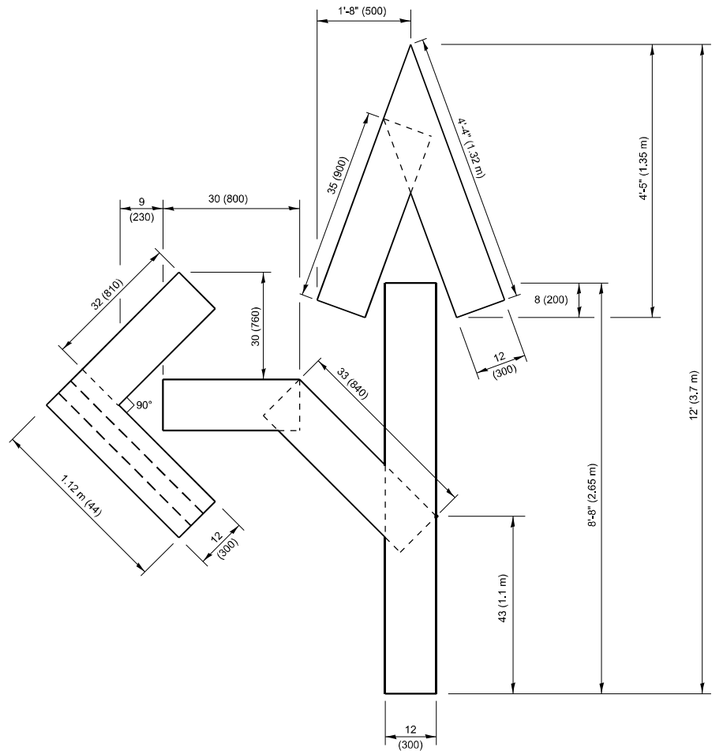
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

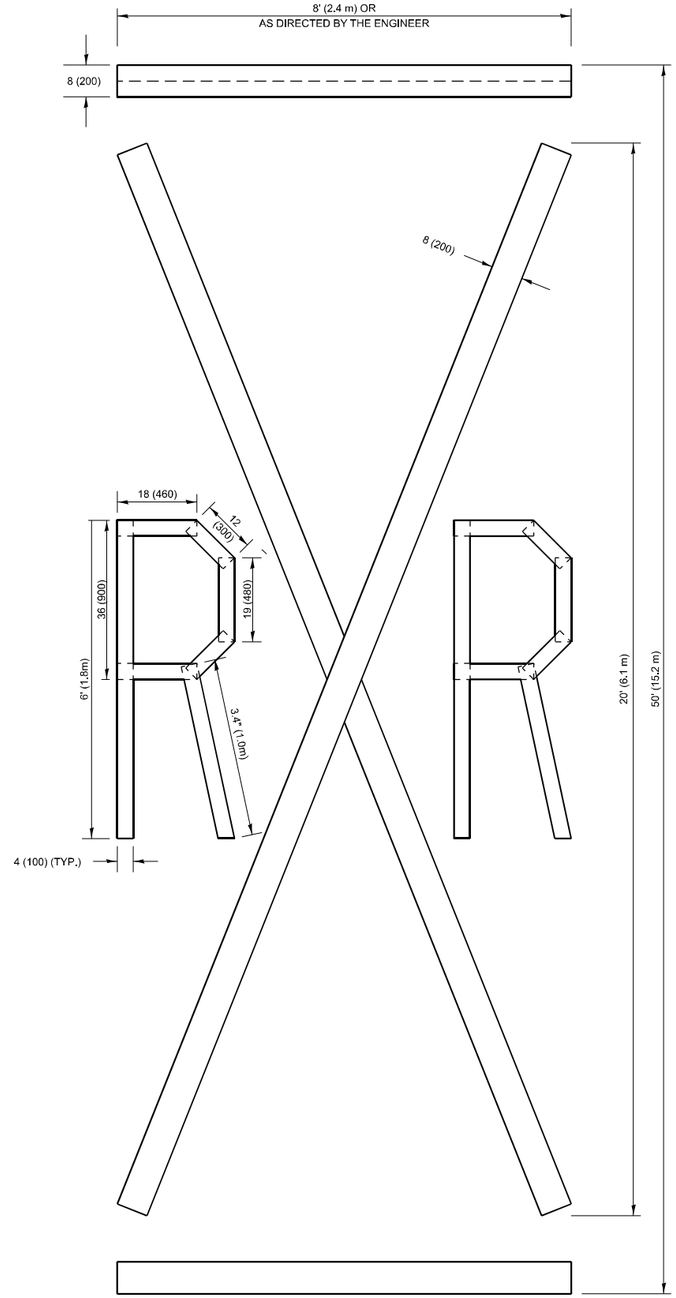


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-16 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\jd114394\D116825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 12/5/2025	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

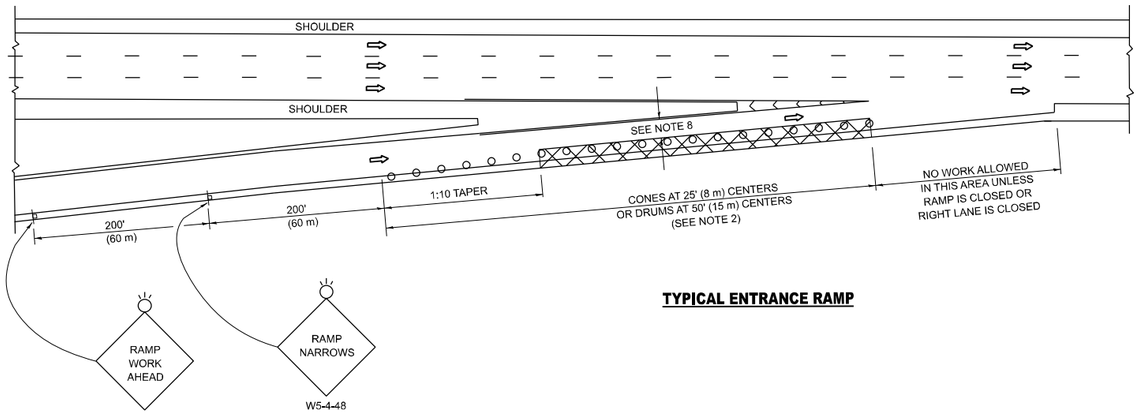
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

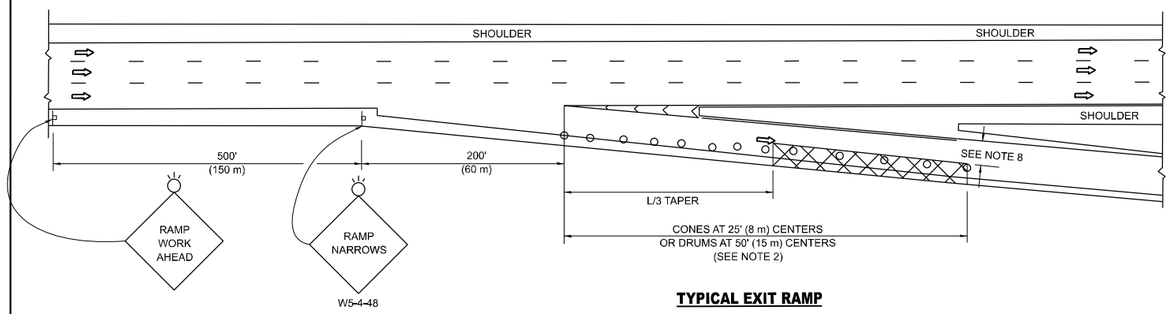
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	38
	TC-16	CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		

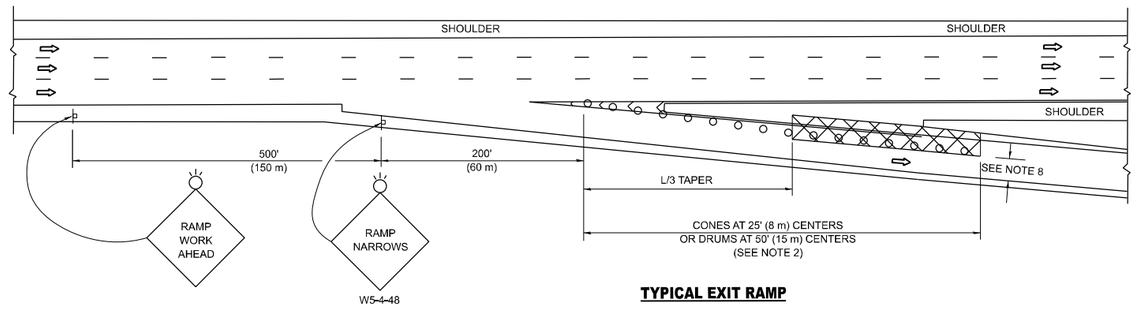
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP

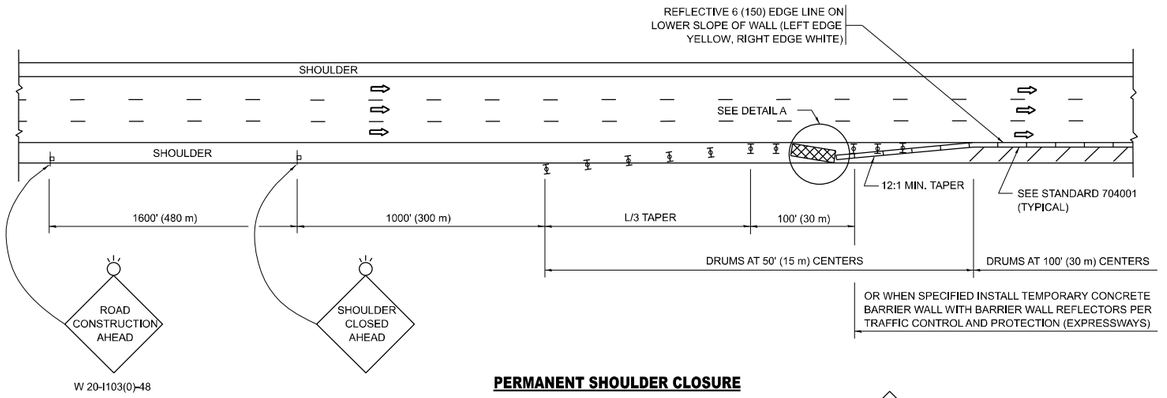


TYPICAL EXIT RAMP

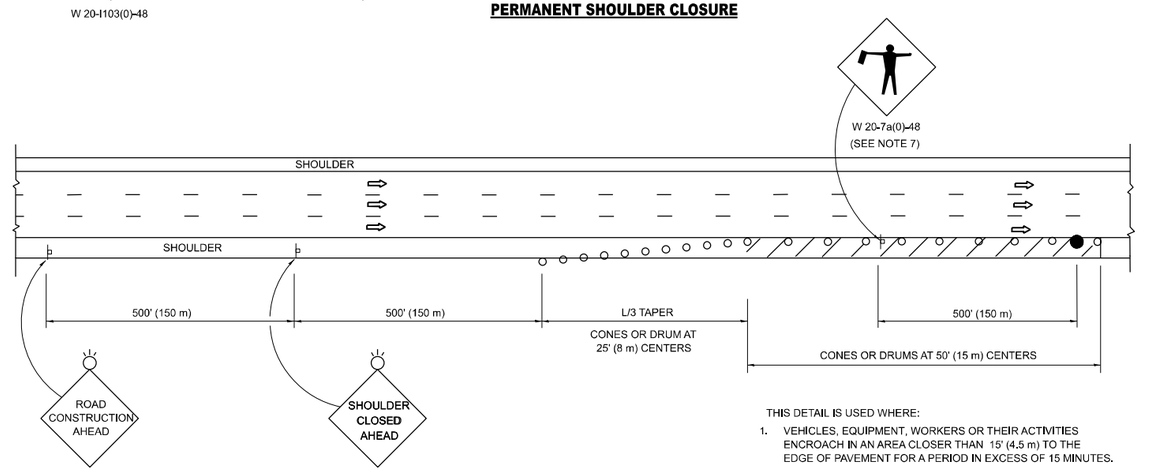


TYPICAL EXIT RAMP

SHOULDER CLOSURE DETAILS



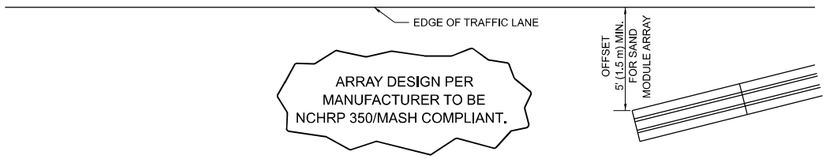
PERMANENT SHOULDER CLOSURE



THIS DETAIL IS USED WHERE:

1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

TEMPORARY SHOULDER CLOSURE



DETAIL "A"
IMPACT ATTENTUATOR, TEMPORARY
(SEE NOTE 5)

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES:

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS	
	METRIC	ENGLISH
45 mph (80 km/h) OR GREATER:	L=0.65(W)(S)	L=(W)(S)

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)

2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTICLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENTUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
   a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
   b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.

   THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
   16' MIN. WIDTH CURVE SECTION.
9. TEMPORARY SHOULDER AND NON-SYSTEM INTERCHANGE PARTIAL RAMP CLOSURES ARE ALLOWED WEEKDAYS BETWEEN 9:00 A.M. AND 3:00 P.M. AND BETWEEN 7:00 P.M. AND 5:00 A.M. OR AS APPROVED BY THE EXPRESSWAY TRAFFIC OPERATIONS ENGINEER. PERMANENT SHOULDER AND PARTIAL RAMP CLOSURES WILL ONLY BE PERMITTED IF CALLED FOR IN THE PLANS OR AS APPROVED BY THE EXPRESSWAY OPERATIONS ENGINEER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: TC-17 [Sheet]
FILE NAME: c:\pw_work\pwidot\rothjp\d1143894\D1116825-sht-DistStds.dgn

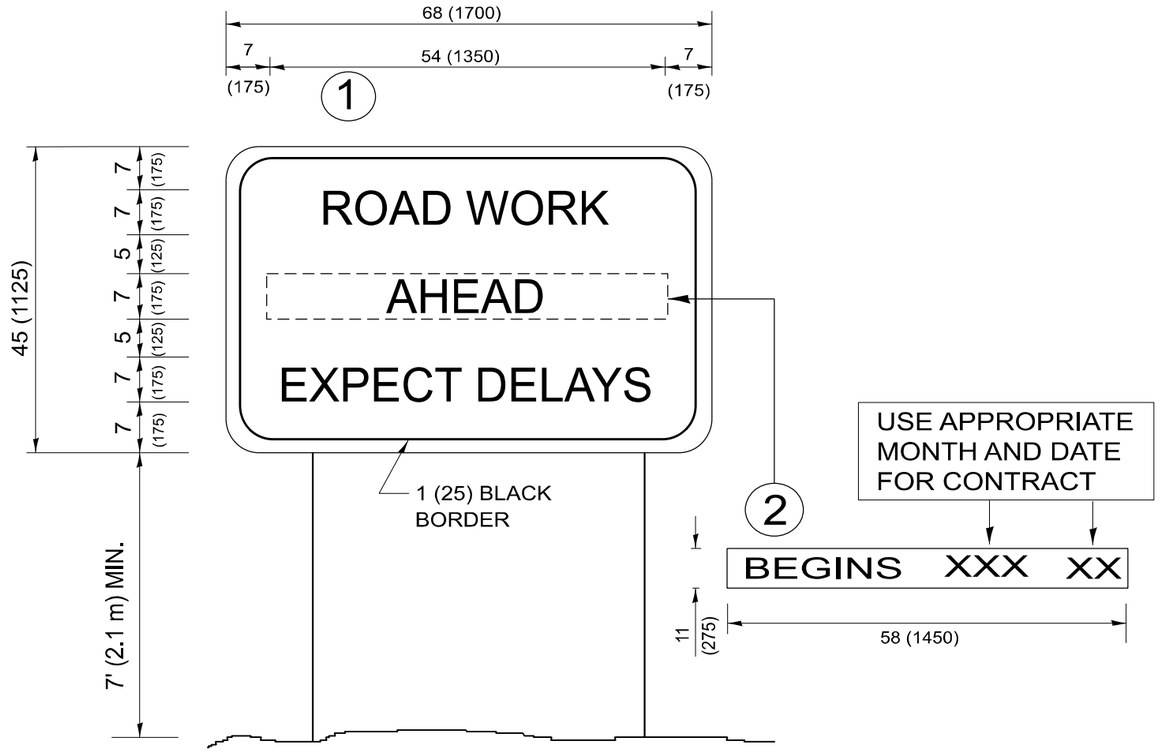
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	M.D. 06-13
	DRAWN - D.W.S.	REVISED -	M.D. 01-18
	CHECKED -	REVISED -	M.D. 10-20
PLOT DATE = 12/5/2025	DATE - 11-96	REVISED -	D.S. 05-24

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	39
	TC-17	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: TC-22 [Sheet]
FILE NAME: c:\dw_work\pwidot\roth\pid1143894\D1116825~sht~DistStds.dgn

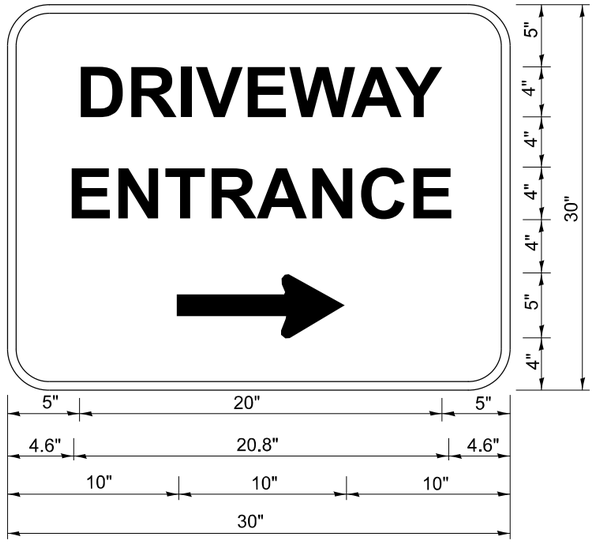
USER NAME = Jacob.Roth	DESIGNED -	REVISED - R. MIRS 09-15-97	
	DRAWN -	REVISED - R. MIRS 12-11-97	
	CHECKED -	REVISED - T. RAMMACHER 02-02-99	
PLOT DATE = 12/4/2025	DATE -	REVISED - C. JUCIUS 01-31-07	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	40
	TC-22	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: TC-26 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\p\d1143894\D116825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED -	REVISED - C. JUCIUS 02-15-07	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	41
	TC-26	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED
CONTROLLER CABINET		
COMMUNICATION CABINET	ECC	CC
MASTER CONTROLLER	EMC	MC
MASTER MASTER CONTROLLER	EMMC	MMC
UNINTERRUPTABLE POWER SUPPLY		
SERVICE INSTALLATION -(P) POLE MOUNTED	P	P
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED	G GM	G GM
TELEPHONE CONNECTION	ET	T
STEEL MAST ARM ASSEMBLY AND POLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY		BM
WOOD POLE		
GUY WIRE		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD OPTICALLY PROGRAMMED	P P	P P
FLASHER INSTALLATION -(FS) SOLAR POWERED	F FS F FS	F FS F FS
PEDESTRIAN SIGNAL HEAD		
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON	APS	APS
RADAR DETECTION SENSOR	R	R
VIDEO DETECTION CAMERA	V	V
RADAR/VIDEO DETECTION ZONE		
PAN, TILT, ZOOM (PTZ) CAMERA	PTZ	PTZ
EMERGENCY VEHICLE LIGHT DETECTOR		
CONFIMATION BEACON		
WIRELESS INTERCONNECT		
WIRELESS INTERCONNECT RADIO REPEATER	ERR	RR

ITEM	EXISTING	PROPOSED
HANDHOLE -SQUARE -ROUND		
HEAVY DUTY HANDHOLE -SQUARE -ROUND	H	H
DOUBLE HANDHOLE		
JUNCTION BOX	J	J
RAILROAD CANTILEVER MAST ARM		
RAILROAD FLASHING SIGNAL		
RAILROAD CROSSING GATE		
RAILROAD CROSSBUCK		
RAILROAD CONTROLLER CABINET	RR	RR
UNDERGROUND CONDUIT (UC), GALVANIZED STEEL		
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE		
SYSTEM ITEM	S	SP
INTERSECTION ITEM	I	IP
REMOVE ITEM		R
RELOCATE ITEM		RL
ABANDON ITEM		A
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF
MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF
SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF
DETECTOR LOOP, TYPE I		
PREFORMED DETECTOR LOOP	P	P
SAMPLING (SYSTEM) DETECTOR	S	S
INTERSECTION AND SAMPLING (SYSTEM) DETECTOR	IS	IS
QUEUE AND SAMPLING (SYSTEM) DETECTOR	QS	QS
WIRELESS DETECTOR SENSOR	W	W
WIRELESS ACCESS POINT		

ITEM	EXISTING	PROPOSED
SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD	R Y G ◄Y ◄G P	R Y G ◄Y ◄G P
SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE	R Y G ◄Y ◄G P RB	R Y G ◄Y ◄G P RB
PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER	C D	C D
ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED	5	5
GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)	1#6	1#6
ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C	1	1
COAXIAL CABLE	C	C
VENDOR CABLE	V	V
COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED	6#18	6#18
FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F	12F 24F 36F	12F 24F 36F
GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE	C M P S	C M P S

MODEL: TS-05a [Sheet]
FILE NAME: c:\pw_work\pwidot\rothjp\d1143894\D116825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED - IP	REVISED -	
	DRAWN - IP	REVISED -	
	CHECKED - LP	REVISED -	
PLOT DATE = 12/5/2025	DATE - 9/29/2016	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 1 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	42
	TS-05	CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

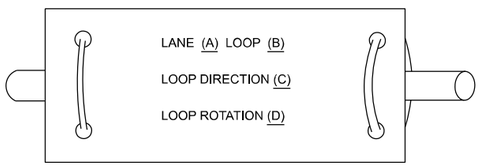
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

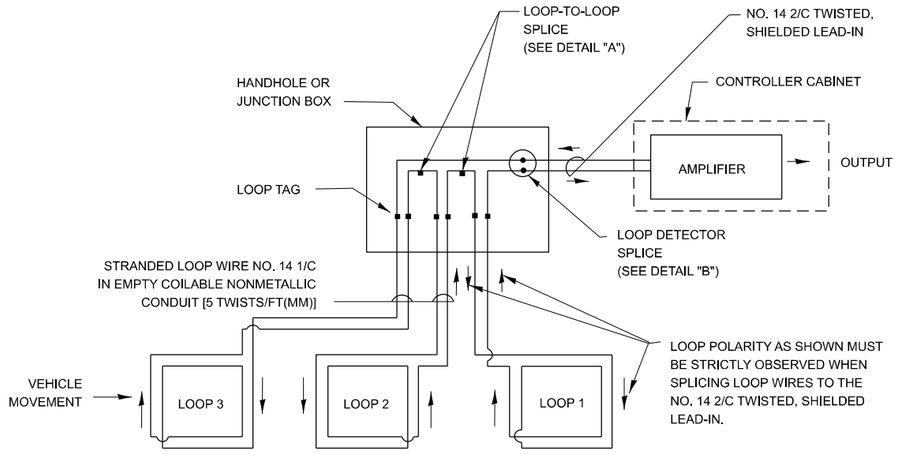


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

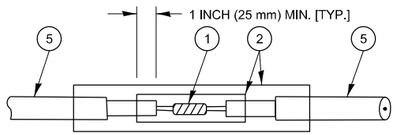
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

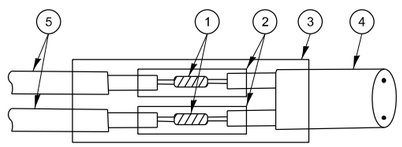


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

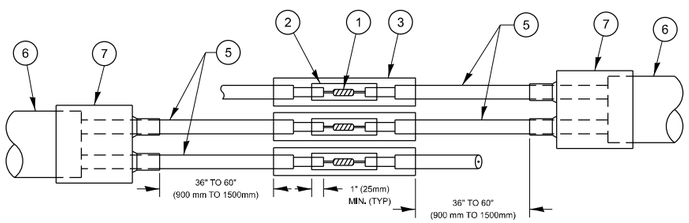


DETAIL "A"
LOOP-TO-LOOP SPLICE



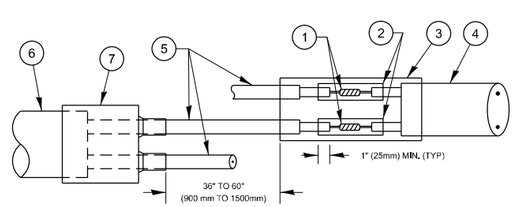
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP

(6) XL POLYOLEFIN 2 CONDUCTOR

(7) BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

MODEL: TS-05b [Sheet]
FILE NAME: c:\dw_work\pwidot\roth\pjd1143894\D116825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

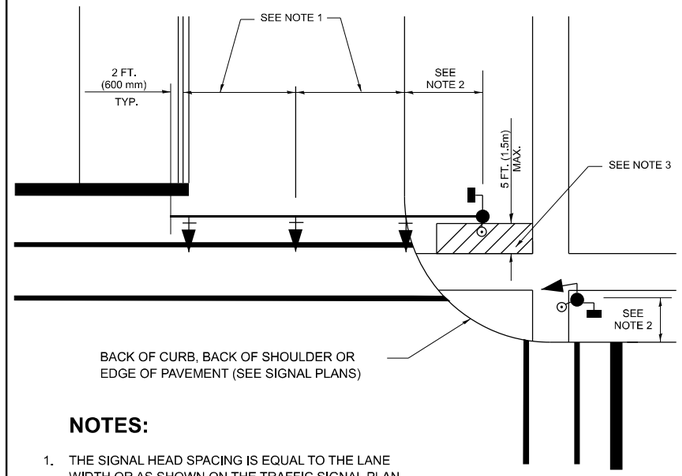
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 2 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	43
	TS-05	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

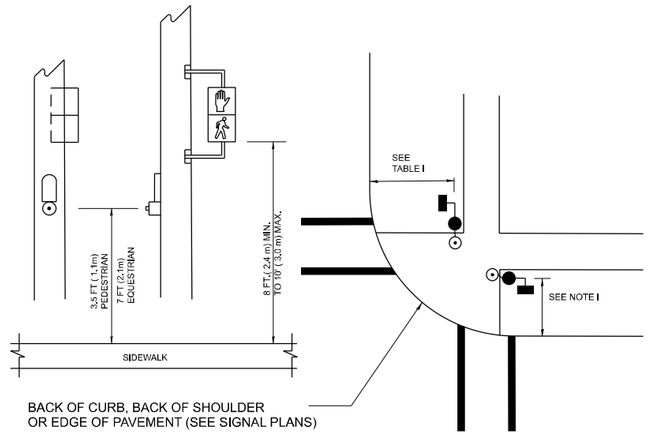
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

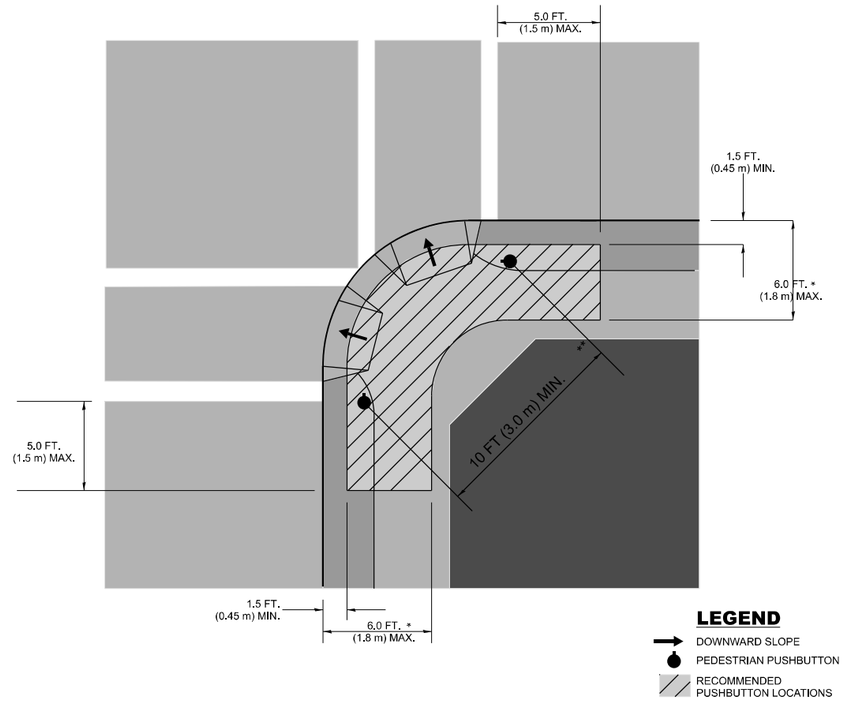
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

MODEL: TS-05c [Sheet]
FILE NAME: c:\pw_work\pwdot\roth\jobd1143894\D116825-sht-DistStds.dgn

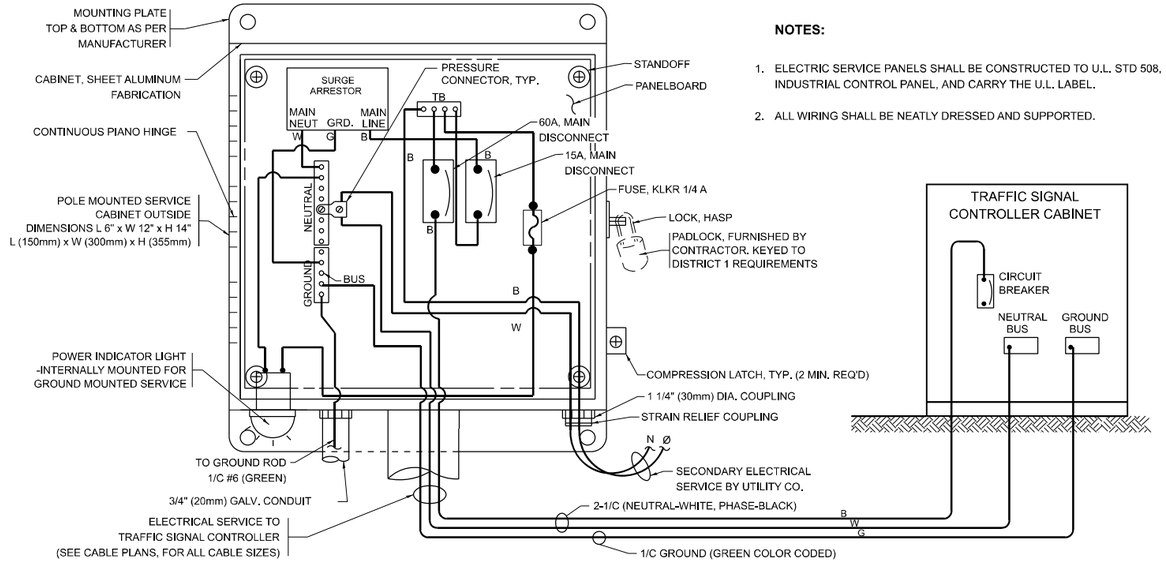
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 3 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	44
	TS-05	CONTRACT NO. 80B15		
	ILLINOIS	FED. AID PROJECT		

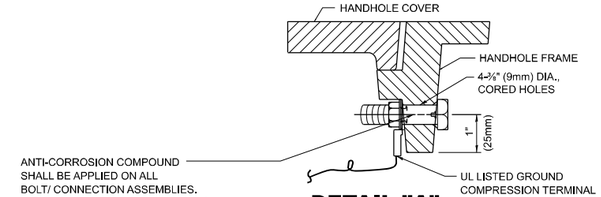


NOTES:

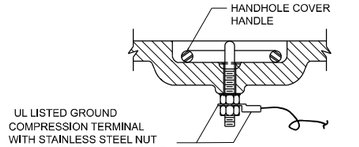
1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ELECTRICAL SERVICE - PANEL DIAGRAM
(TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)



DETAIL "A"

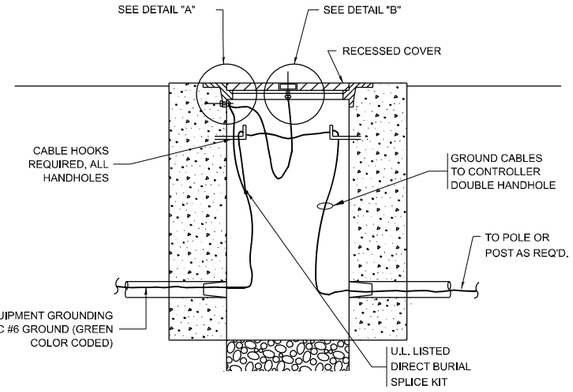


DETAIL "B"

NOTES:

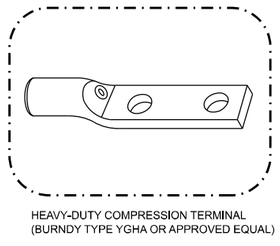
GROUNDING SYSTEM

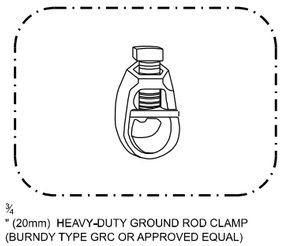
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



HANDHOLE COVER & FRAME - GROUNDING DETAIL

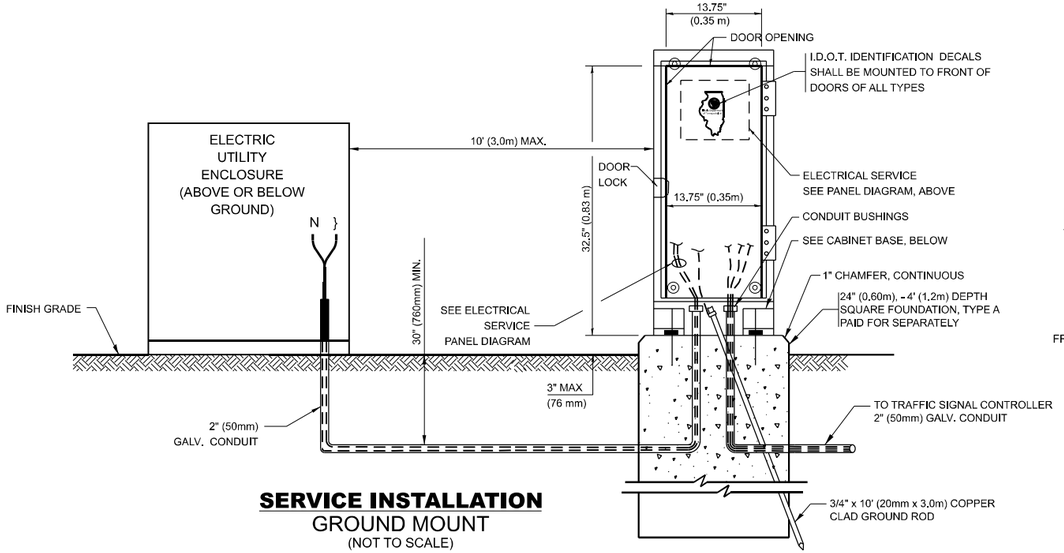
(NOT TO SCALE)



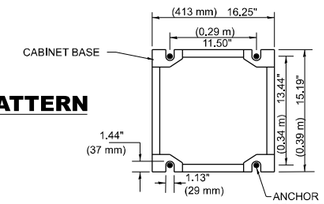


NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

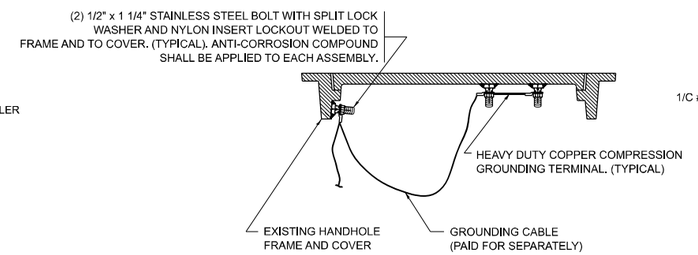


SERVICE INSTALLATION
GROUND MOUNT
(NOT TO SCALE)



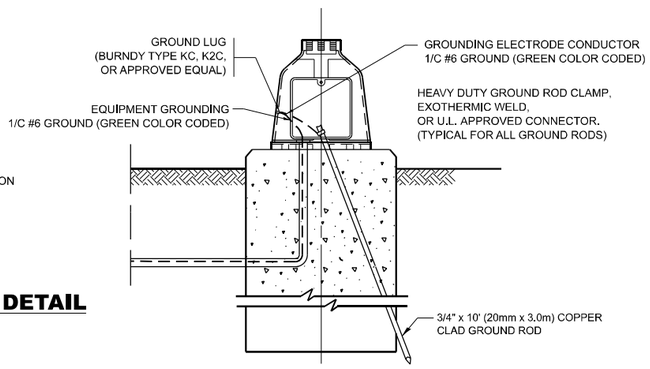
CABINET - BASE BOLT PATTERN

(NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL

(NOT TO SCALE)



MAST ARM POLE / POST-GROUNDING DETAIL

(NOT TO SCALE)

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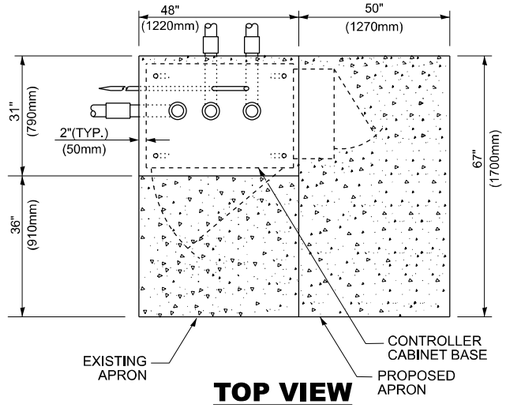
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

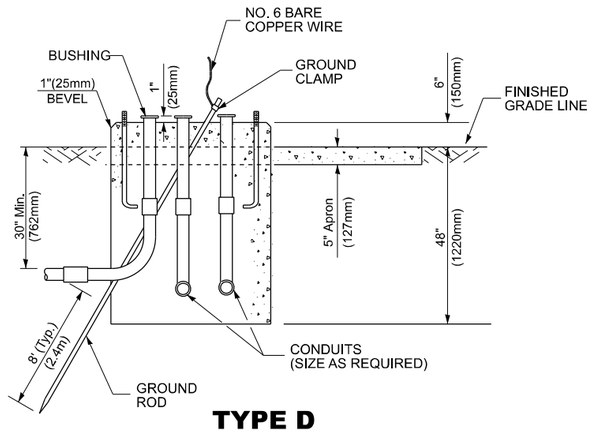
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 4 OF 7 SHEETS | STA. | TO STA.

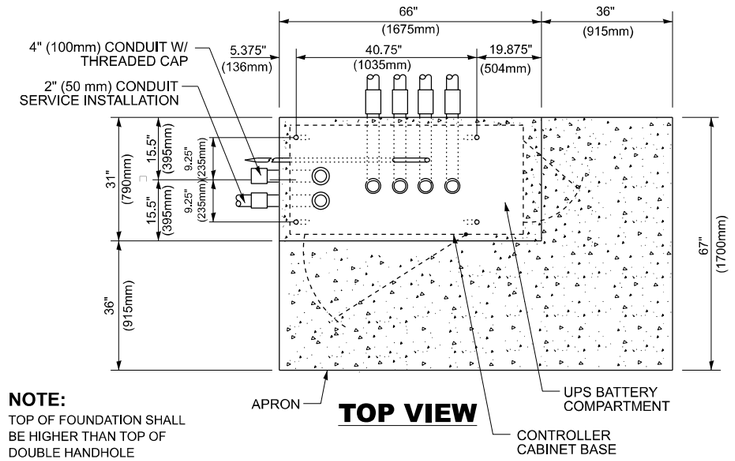
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	45
	TS-05	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			



TOP VIEW

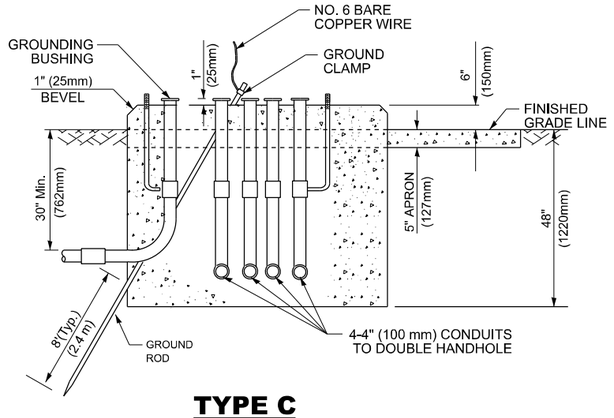


TYPE D
FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET

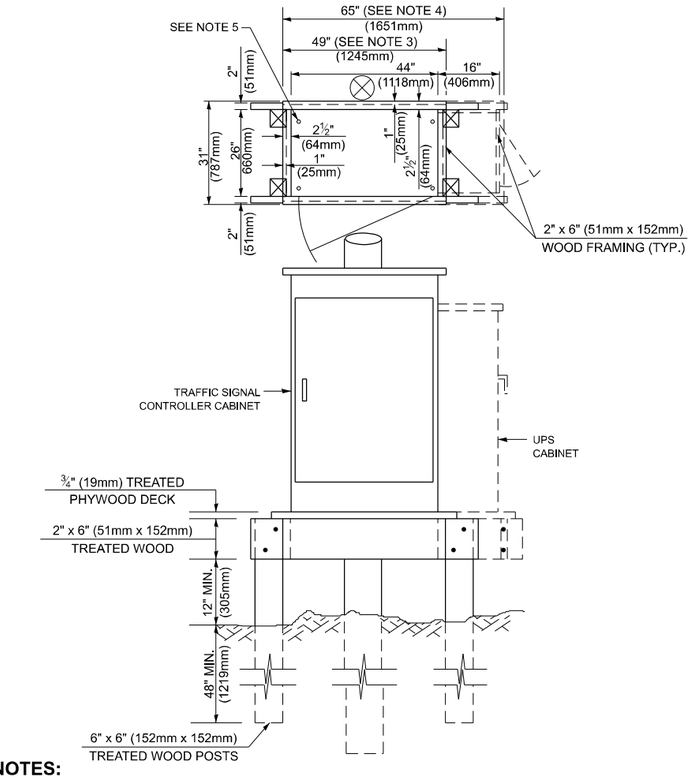


NOTE:
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE

TOP VIEW



TYPE C
FOR GROUND MOUNTED SUPER P (TYPE IV) AND SUPER R (TYPE V) CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy day, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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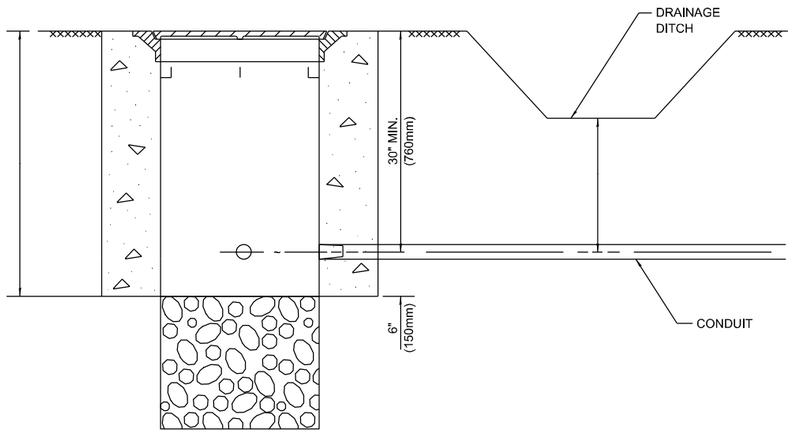
USER NAME = Jacob.Roth	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
	CHECKED -	REVISED -	
PLOT DATE = 12/5/2025	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 5 OF 7 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	46
	TS-05	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

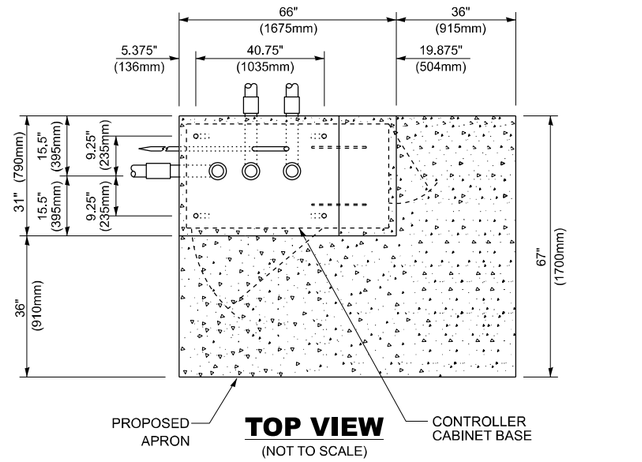


NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

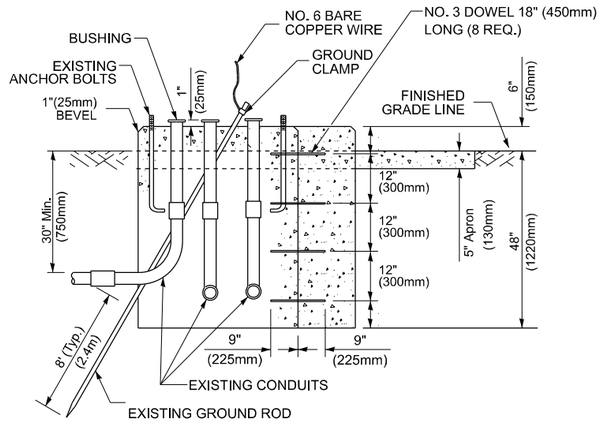
HANDHOLE WITH MINIMUM CONDUIT DEPTH

(NOT TO SCALE)



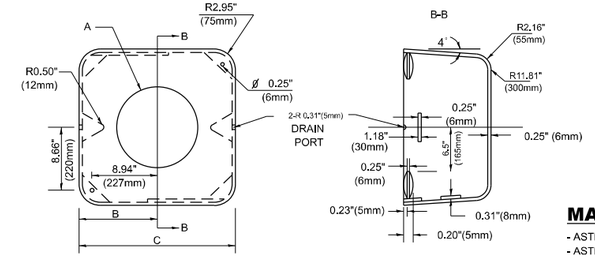
TOP VIEW

(NOT TO SCALE)



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

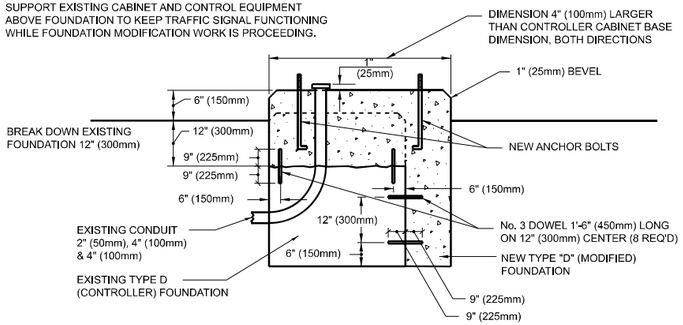
SHROUD

NOTES:

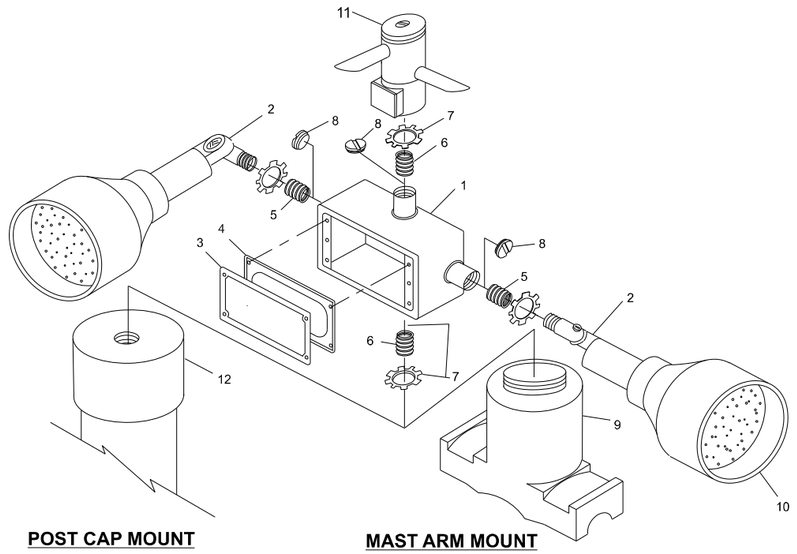
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



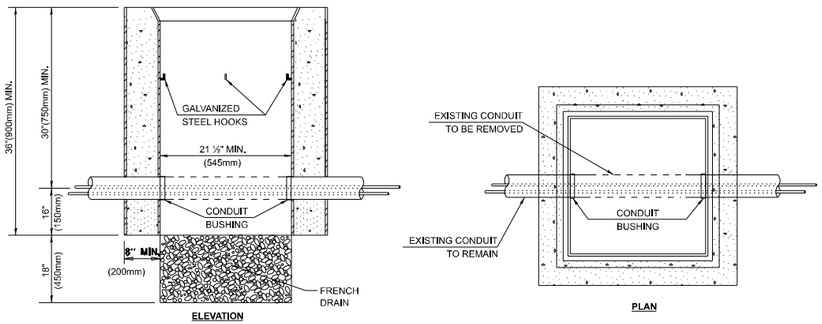
POST CAP MOUNT **MAST ARM MOUNT**

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾" (19 mm) CLOSE NIPPLE
7	¾" (19 mm) LOCKNUT
8	¾" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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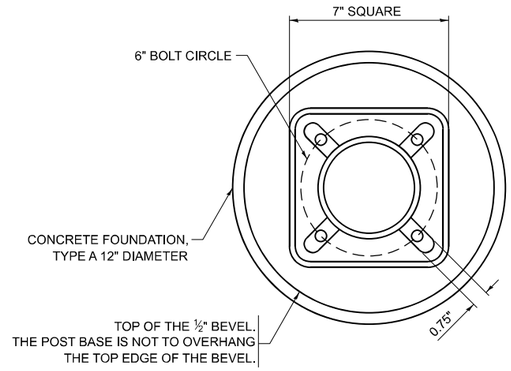
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 6 OF 7 SHEETS | STA. | TO STA.

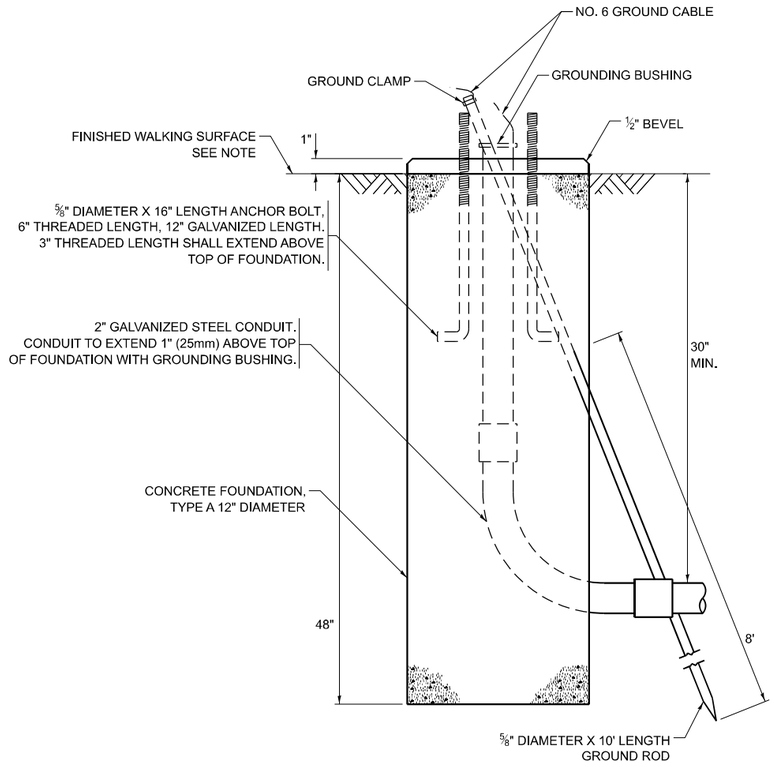
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	47
	TS-05	CONTRACT NO. 80B15		
		ILLINOIS FED. AID PROJECT		



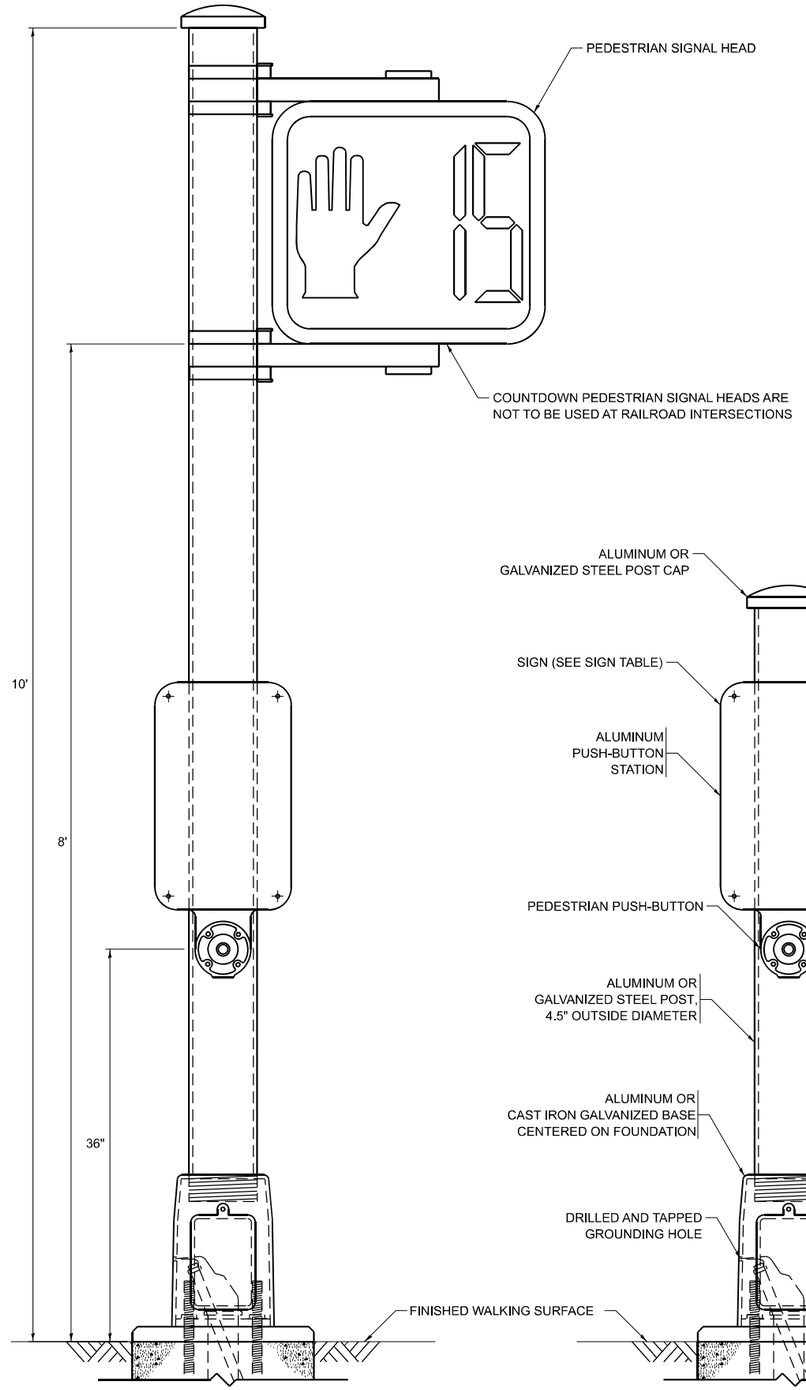
BOLT PATTERN

NOTE:

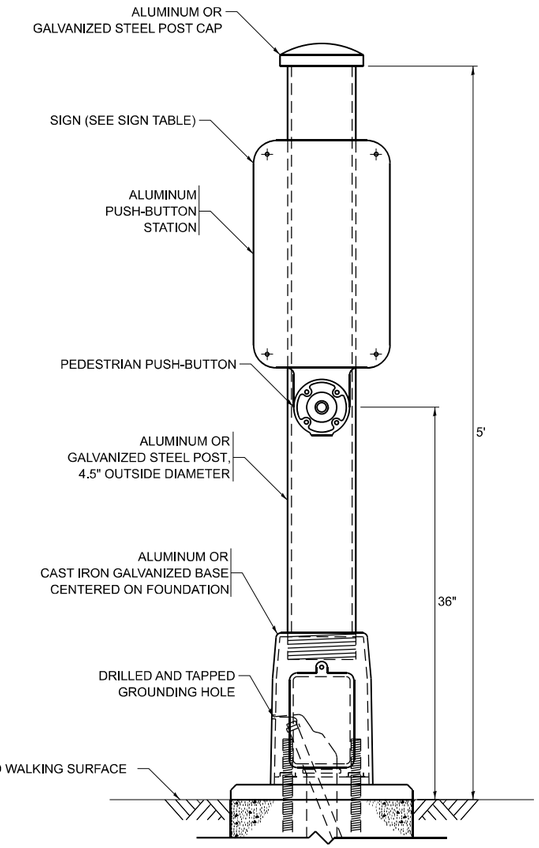
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



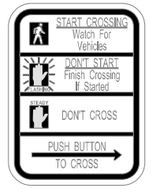
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER



PEDESTRIAN SIGNAL POST, 10 FT.

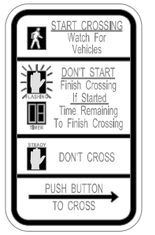


PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b

R10-3d



R10-3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

MODEL: TS-05g [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\jpd11143894\D1\16825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED - IP	REVISED - 10-15-2020	
	DRAWN - IP	REVISED -	
	CHECKED - LP	REVISED -	
PLOT DATE = 12/5/2025	DATE - 10-15-2018	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

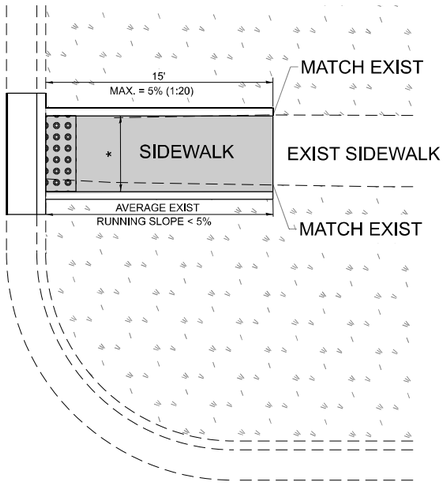
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 7 OF 7 SHEETS | STA. | TO STA.

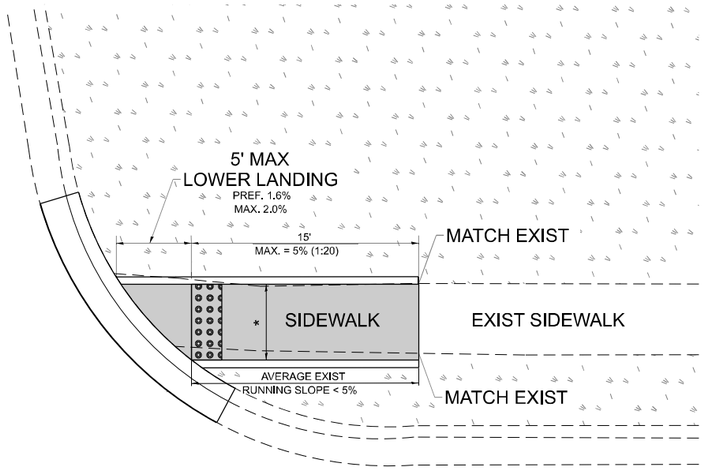
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	48
	TS-05	CONTRACT NO. 80B15		
		ILLINOIS	FED. AID PROJECT	

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

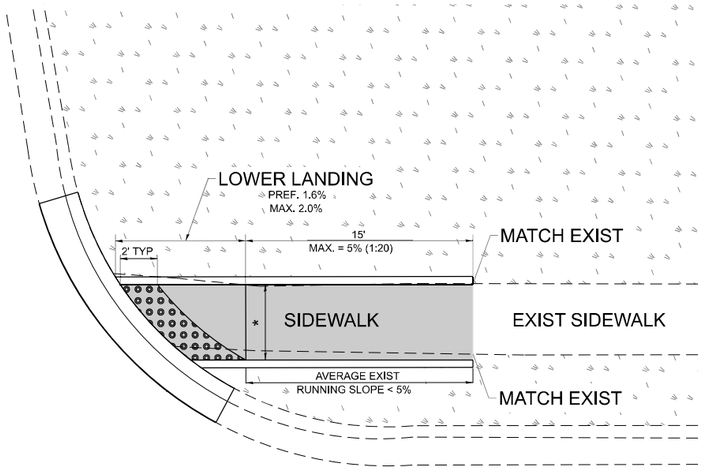
PD-01A



PD-01B



PD-01C



LEGEND

PROPOSED SIDE CURB

EXIST. GRASS

PROPOSED SIDEWALK

DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-01 [Sheet]
FILE NAME: c:\pw_work\dwidot\rothjp\d1143894\D16825-sht-DistStds.dgn

USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE - 10/02/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

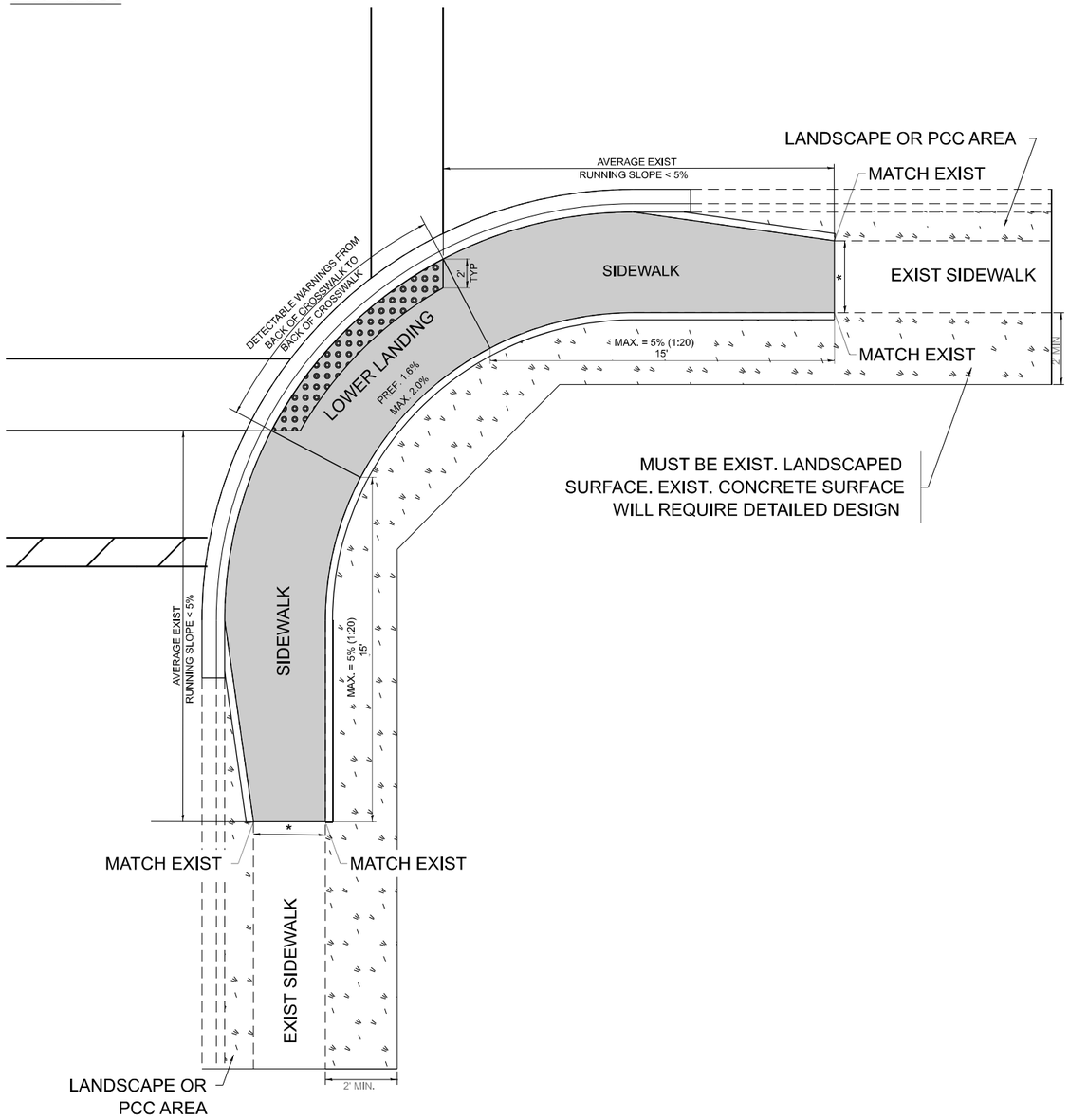
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS (PD-01)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

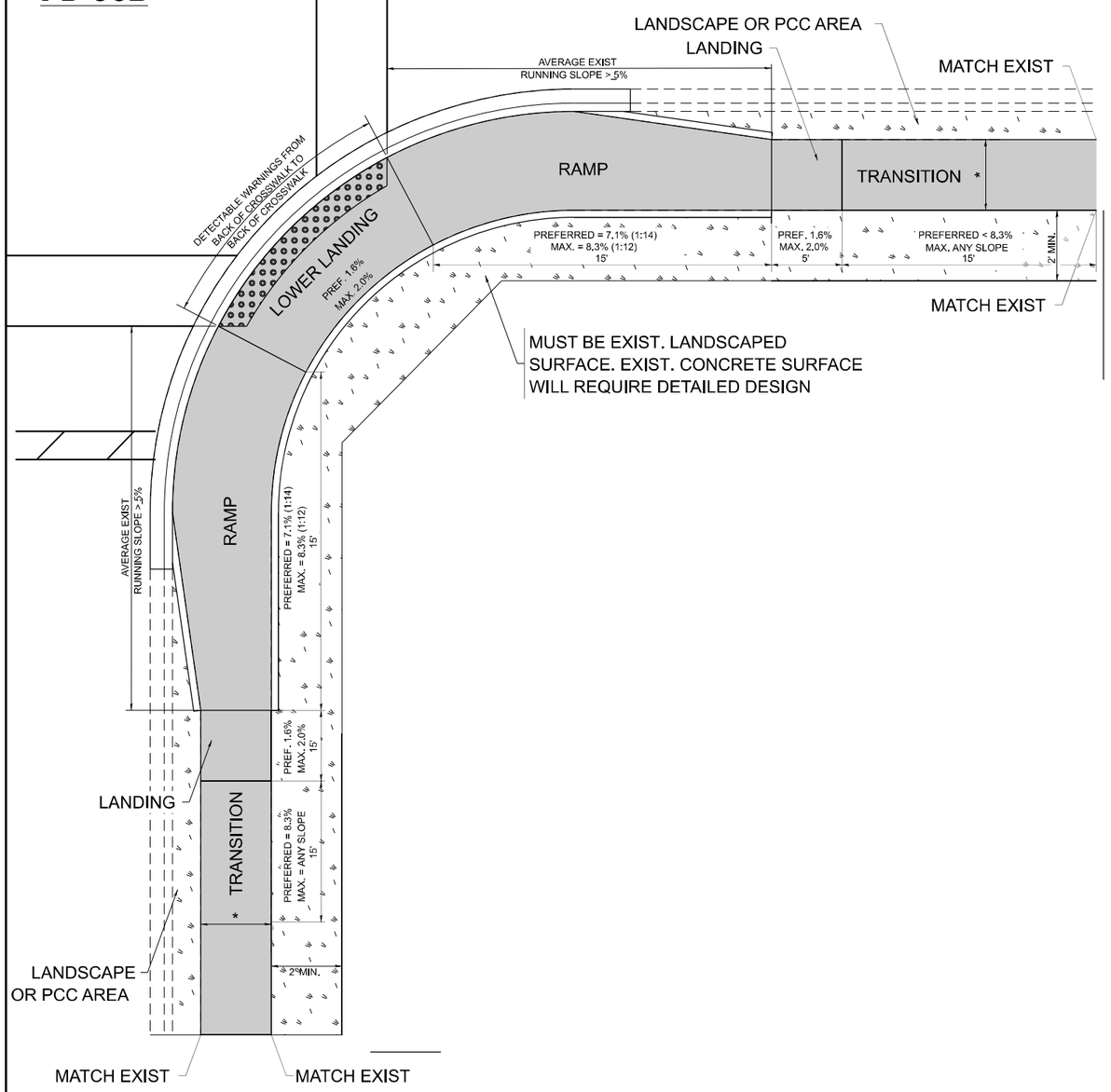
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	49
	PD-01	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

USER NAME = Jacob.Roth	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/5/2025	DATE - 10/02/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR PARALLEL CURB RAMPS
(PD-06)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2025-1089-RS	DUPAGE	50	50
	PD-06	CONTRACT NO. 80B15		
	ILLINOIS FED. AID PROJECT			

MODEL: PD-06 [Sheet]
FILE NAME: c:\pw_work\pwidot\roth\pjd1143894\D1116825-sht-DistStds.dgn