

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1008	13-00028-00-RS	COOK	17	1
		ILLINOIS	CONTRACT NO. 61A06	

02-28-14 LETTING ITEM 009

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PROPOSED
HIGHWAY PLANS**

FAU 1008 (CLAIRE BOULEVARD)
I-294 TO 139TH STREET
RESURFACING
SECTION NO.: 13-00028-00-RS
PROJECT NO.: M-4003(255)
VILLAGE OF ROBBINS
COOK COUNTY
JOB NO.: C-91-131-14

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. X2881739



CONTACT JULIE AT 811 OR 800-892-0123
WITH THE FOLLOWING:

COUNTY = COOK
CITY-TWNSHP. = ROBBINS-BREMEN
SEC. & 1/4 SEC. NO. = S2 SW & SE 1/4 T36N R13E
48 HOURS (2 working days) BEFORE YOU DIG



LOCATION OF SECTION INDICATED THUS: —

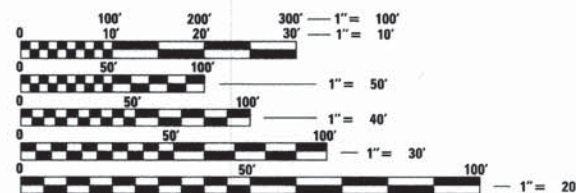
TRAFFIC DATA

CLAIRE BOULEVARD
POSTED AND DESIGN SPEED = 30 MPH
2008 ADT = 8,100

DESIGN DESIGNATION

ROADWAY CLASSIFICATION: COLLECTOR

BAXTER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. 184-001121
EXPIRES 4/30/2015



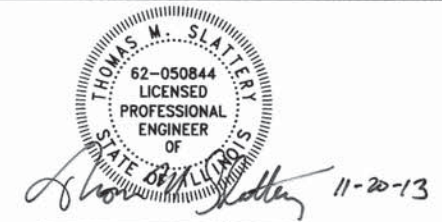
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



FAU 1008 CLAIRE BOULEVARD
IMPROVEMENT BEGINS
STA 19+40

BREMEN TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 2,923 LF OR 0.554 MILES
NET LENGTH OF IMPROVEMENT = 2,923 LF OR 0.554 MILES

FAU 1008 CLAIRE BOULEVARD
IMPROVEMENT ENDS
STA 48+63



PROJECT ENGINEER
"LICENSE EXPIRES 11-30-2015"

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	<i>Ernestine Paul Sullivan</i> 11-22-13
VILLAGE OF ROBBINS VILLAGE ADMINISTRATOR	
PASSED	DECEMBER 10, 2013 <i>C. Holt</i> C.J. HOLT
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS	
RELEASING FOR BID BASED ON LIMITED REVIEW	December 17, 2013 <i>John F. Woodman</i>
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER	

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OF THE STATE OF ILLINOIS**

B&W PROJECT NO.: 130097.40



CONTRACT NO. 61A06

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 ILLINOIS PROFESSIONAL ENGINEERING FIRM
 LICENSE NO. 184-001121 EXPIRES 4/30/2015
 PROJECT NO. 13-00028-00-RS
 PROGRAM AND OFFICE ENGINEER: CHARLES RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

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DESIGNED - JDM	REVISED -
DRAWN - KAR	REVISED -
CHECKED - MWP	REVISED -
DATE - 10-18-13	FILE - 130097SHT-500.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: NONE

STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1008	13-00028-00-R5	COOK	17	3
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT NO. 40032551			CONTRACT NO. 61A06	

SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	0005 QUANTITY
20200100	EARTH EXCAVATION	CU YD	5	5
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	43	43
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	4	4
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	171	171
25200100	SODDING	SQ YD	171	171
25200200	SUPPLEMENTAL WATERING	UNIT	11	11
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	112	112
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	6	6
40600300	AGGREGATE (PRIME COAT)	TON	53	53
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	2	2
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	599	599
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	221	221
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,161	1,161
42001300	PROTECTIVE COAT	SQ YD	103	103
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	791	791
42400800	DETECTABLE WARNINGS	SQ FT	86	86
# 44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	13,020	13,020
44000600	SIDEWALK REMOVAL	SQ FT	1,086	1,086
44201811	CLASS D PATCHES, TYPE I, 14 INCH	SQ YD	260	260
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SQ YD	521	521
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SQ YD	781	781
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SQ YD	1,042	1,042
# 48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	204	204
* # 56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	2	2
# 60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	5
# 60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	9	9
67100100	MOBILIZATION	L SUM	1	1
# 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
# 70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
# 70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1

* INDICATES SPECIALTY ITEM

INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE

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SUMMARY OF QUANTITIES			URBAN	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	0005 QUANTITY
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	3,824	3,824
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	319	319
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	37	37
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12,660	12,660
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	315	315
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	730	730
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	37	37
* # 88600600	DETECTOR LOOP REPLACEMENT	FOOT	152	152
# X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	5	5
# X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	9	9
# Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	68	68
# Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	283	283

* INDICATES SPECIALTY ITEM
 # INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE



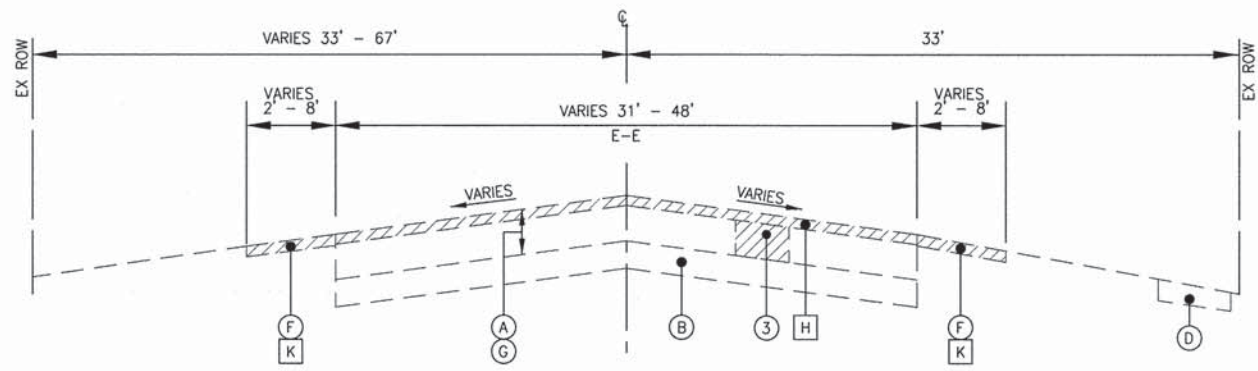
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

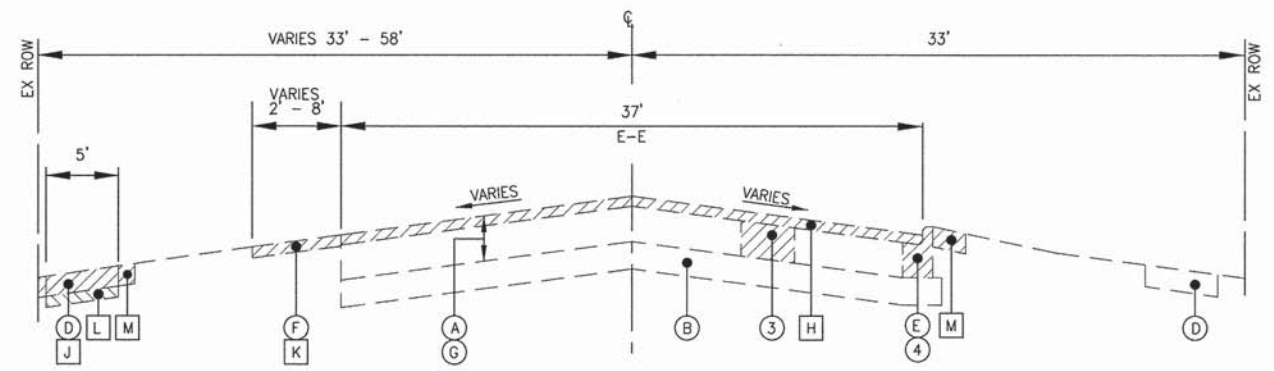
SUMMARY OF QUANTITIES

SCALE: NONE STA. TO STA.

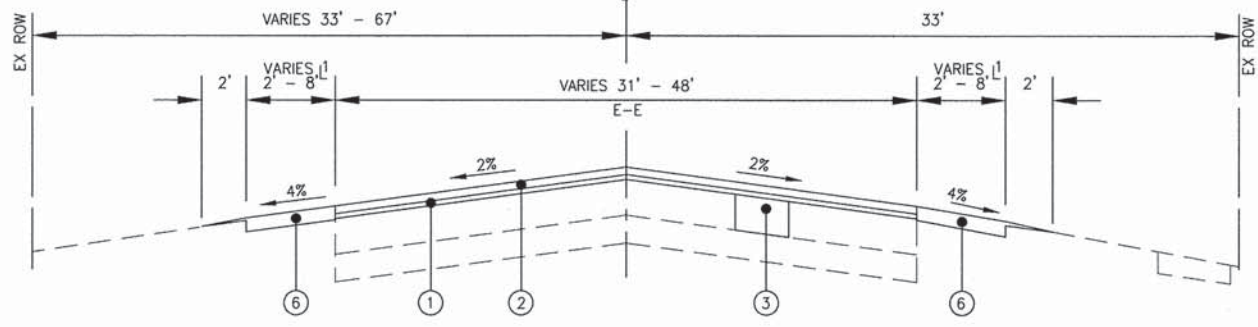
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1008	13-00028-00-RS	COOK	17	4
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-4003(255)			CONTRACT NO. 61A06	



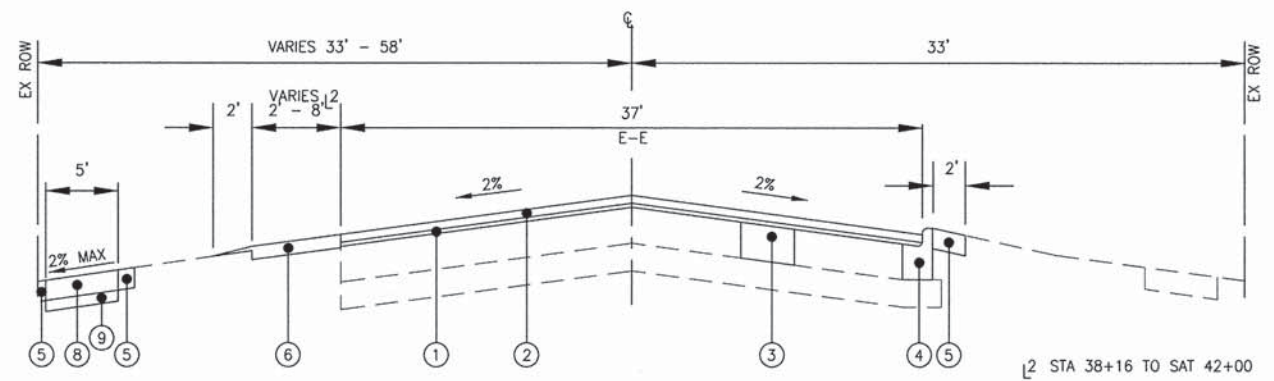
EXISTING TYPICAL SECTION
CLAIRE BOULEVARD
STA 19+40 TO STA 26+04
STA 42+00 TO STA 47+33



EXISTING TYPICAL SECTION
CLAIRE BOULEVARD
STA 26+04 TO STA 42+00



PROPOSED TYPICAL SECTION
CLAIRE BOULEVARD
STA 19+40 TO STA 26+04
STA 42+00 TO STA 47+33



PROPOSED TYPICAL SECTION
CLAIRE BOULEVARD
STA 26+04 TO STA 42+00

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm); 1 1/2"	4% @ 50 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 Gyr.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm), 14" (5 LIFTS)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY THE DISTRICT ONE SPECIAL PROVISIONS.

FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL MILL BEFORE PATCHING

EXISTING PAVEMENT DATA

CORE NUMBER	HMA THICKNESS	PCC BASE THICKNESS
B-8	6.5	6
B-9	6.5	10
B-10	5.25	10.25
B-12	5	10
B-13	5.5	9.75
B-14	4	10
B-16	4	11.5

LEGEND

- | EXISTING | PROPOSED |
|---|--|
| (A) EXISTING HOT-MIX ASPHALT SURFACE | (1) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4" |
| (B) EXISTING AGGREGATE BASE | (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2" |
| (C) RESERVED | (3) CLASS D PATCHES - 8" (AS DETERMINED BY THE ENGINEER) |
| (D) EXISTING SIDEWALK | (4) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER) |
| (E) EXISTING COMBINATION CONCRETE CURB AND GUTTER | (5) TOPSOIL FURNISH AND PLACE, 4"; SODDING |
| (F) EXISTING AGGREGATE SHOULDER | (6) AGGREGATE WEDGE SHOULDER, TYPE B |
| (G) EXISTING PCC BASE COURSE | (7) RESERVED |
| (H) HOT-MIX ASPHALT SURFACE REMOVAL - 2" | (8) PCC SIDEWALK, 5-INCH (AS DETERMINED BY THE ENGINEER) |
| (J) SIDEWALK REMOVAL | (9) AGGREGATE BASE COURSE, TYPE B, 4" |
| (K) EXCAVATING AND GRADING OF EXISTING SHOULDER (AS DETERMINED BY THE ENGINEER) | |
| (L) AGGREGATE BASE COURSE REMOVAL (NOT PAID FOR SEPARATELY) | |
| (M) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL) (NOT PAID FOR SEPARATELY WHEN ADJACENT TO EXISTING SIDEWALK OR CURB AND GUTTER) | |
| (Hatched) ITEMS TO BE REMOVED | |

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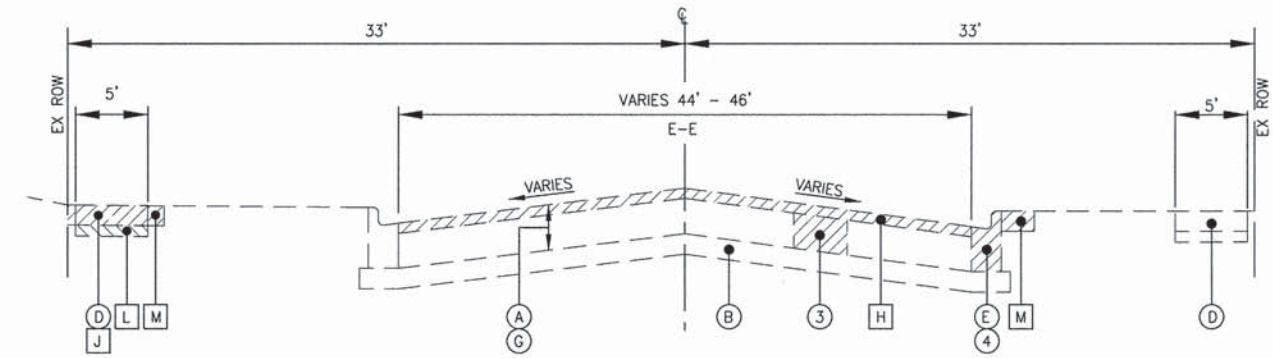


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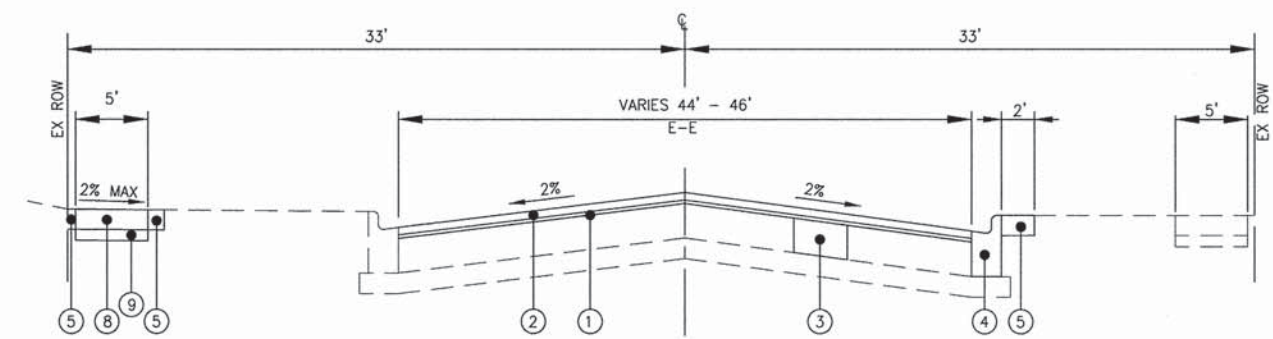
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
 SCALE: NONE STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1008	13-00028-00-RS	COOK	17	5
CONTRACT NO. 61A06				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-40032551				



EXISTING TYPICAL SECTION
CLAIRE BOULEVARD
STA 47+33 TO STA 48+63



PROPOSED TYPICAL SECTION
CLAIRE BOULEVARD
STA 47+33 TO STA 48+63

LEGEND

EXISTING

- (A) EXISTING HOT-MIX ASPHALT SURFACE
- (B) EXISTING AGGREGATE BASE
- (C) RESERVED
- (D) EXISTING SIDEWALK
- (E) EXISTING COMBINATION CONCRETE CURB AND GUTTER
- (F) EXISTING AGGREGATE SHOULDER
- (G) EXISTING PCC BASE COURSE
- (H) HOT-MIX ASPHALT SURFACE REMOVAL - 2"
- (J) SIDEWALK REMOVAL
- (K) EXCAVATING AND GRADING OF EXISTING SHOULDER (AS DETERMINED BY THE ENGINEER)
- (L) AGGREGATE BASE COURSE REMOVAL (NOT PAID FOR SEPARATELY)
- (M) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL) (NOT PAID FOR SEPARATELY WHEN ADJACENT TO EXISTING SIDEWALK OR CURB AND GUTTER)
- [Hatched Box] ITEMS TO BE REMOVED

PROPOSED

- (1) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (3) CLASS D PATCHES - 8" (AS DETERMINED BY THE ENGINEER)
- (4) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- (5) TOPSOIL FURNISH AND PLACE, 4"; SODDING
- (6) AGGREGATE WEDGE SHOULDER, TYPE B
- (7) RESERVED
- (8) PCC SIDEWALK, 5-INCH (AS DETERMINED BY THE ENGINEER)
- (9) AGGREGATE BASE COURSE, TYPE B, 4"

EXISTING PAVEMENT DATA

CORE NUMBER	HMA THICKNESS	PCC BASE THICKNESS
B-8	6.5	6
B-9	6.5	10
B-10	5.25	10.25
B-12	5	10
B-13	5.5	9.75
B-14	4	10
B-16	4	11.5

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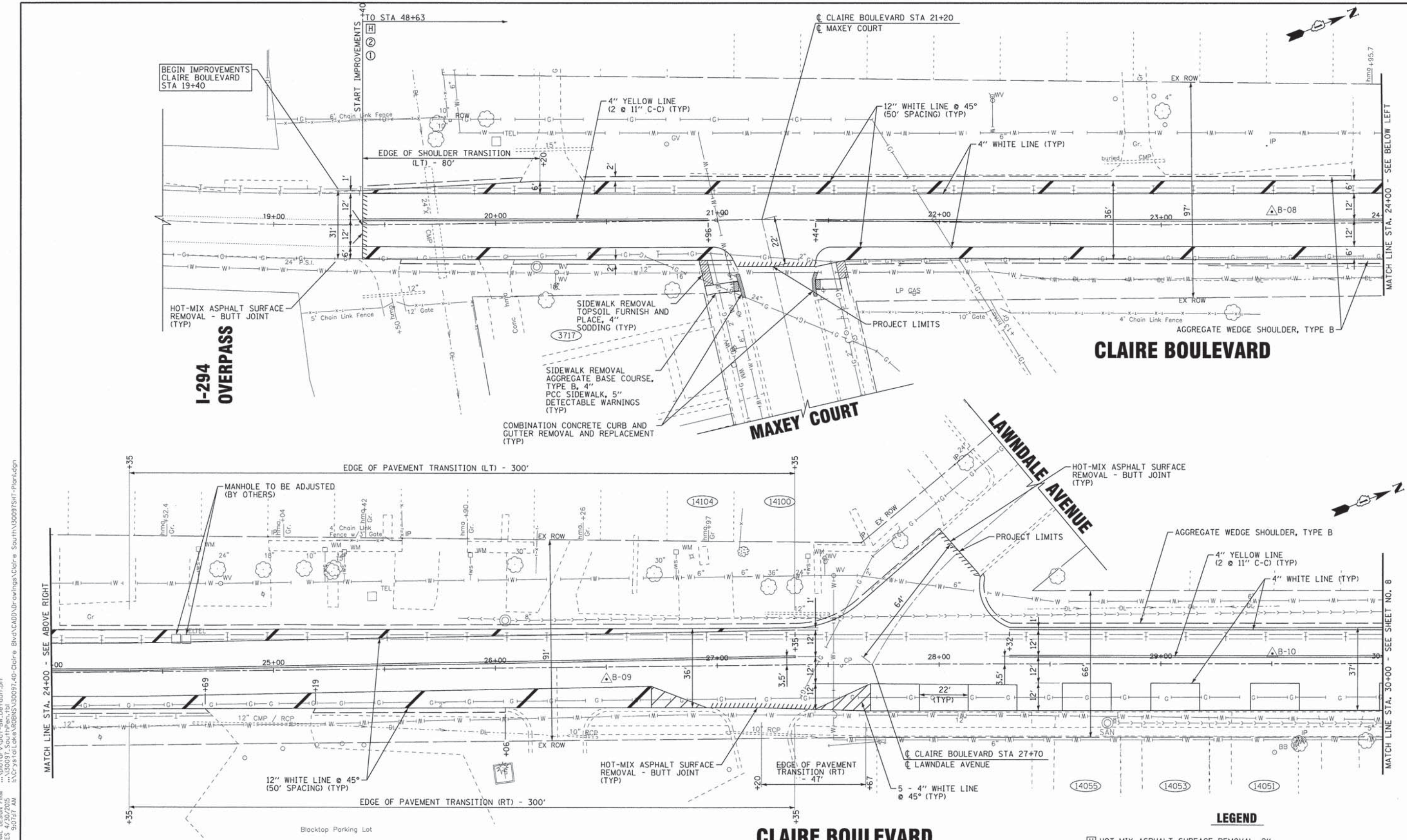


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS	
SCALE: NONE	STA. TO STA.

F.A.U. RTE. 1008	SECTION 13-00028-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 6
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT M-4003(255)			CONTRACT NO. 61A06	



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 PROJECT: 13-00028-00-RS, CLARE BLVD. STA. 19+40 TO STA. 30+00
 DRAWING: 13-00028-00-RS-PLAN-1

BAXTER & WOODMAN
Consulting Engineers

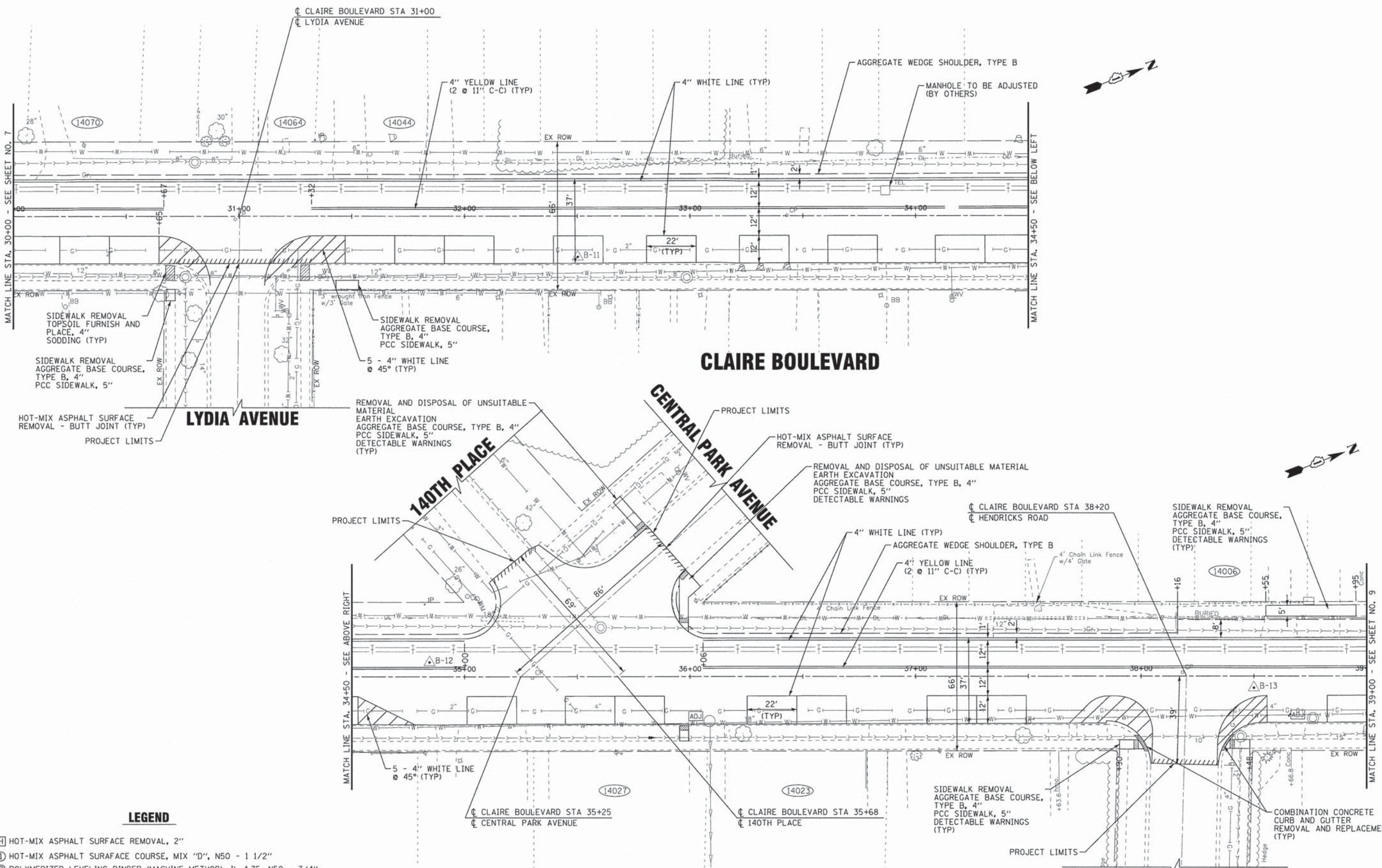
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN CLAIRE BOULEVARD	
SCALE: 1" = 20'	STA. 19+00 TO STA. 30+00

F.A.U. RTE. 1008	SECTION 13-00028-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 7
CONTRACT NO. 61A06				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT M-40032553				

- LEGEND**
- [H] HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
 - ② POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"



- LEGEND**
- [H] HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - [1] HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
 - [2] POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"

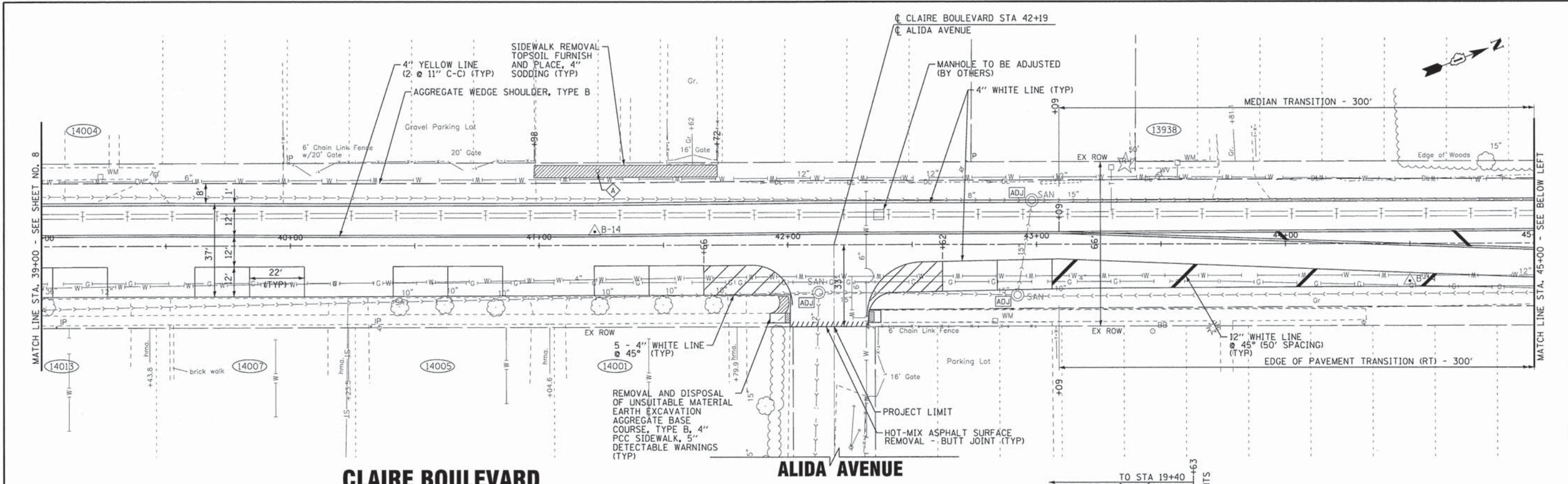
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	CHECKED - MWP	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

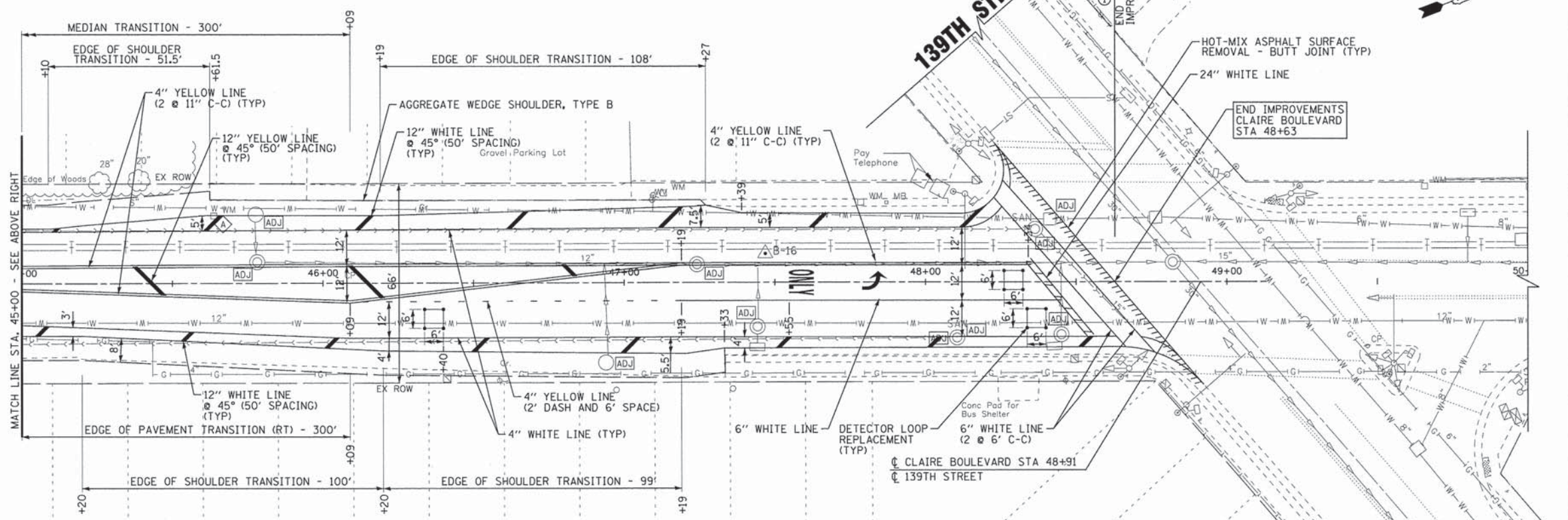
PLAN CLAIRE BOULEVARD	
SCALE: 1" = 20'	STA. 30+00 TO STA. 39+00

F.A.U. RTE. 100B	SECTION 13-00028-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 8
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT M-40032955		



CLAIRE BOULEVARD

ALIDA AVENUE



CLAIRE BOULEVARD

139TH STREET

LEGEND

- H HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- ② POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"

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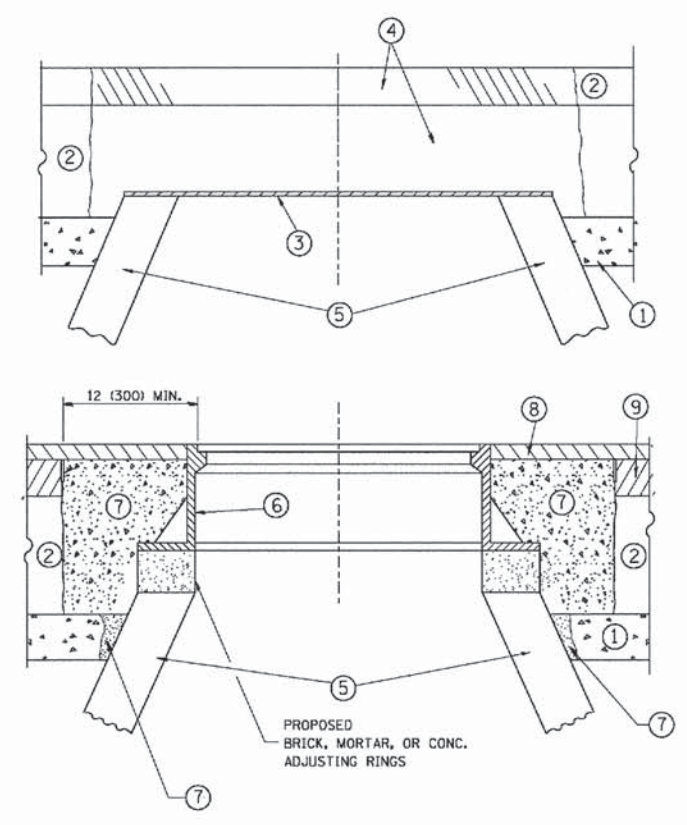
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CLAIRE BOULEVARD		1008	13-00028-00-RS	COOK	17	9
SCALE: 1" = 20'		STA. 39+00	TO STA. 50+00	CONTRACT NO. 61A06		

FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT M-4003255
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 DESIGNED - R. SHAH
 REVISED - R. WIEDEMAN 05-14-04
 DRAWN -
 REVISED - R. BORO 01-01-07
 CHECKED -
 REVISED - R. BORO 03-09-11
 DATE - 10-25-94
 REVISED - R. BORO 12-06-11
 PLOT SCALE = 1/8" = 1'-0"
 PLOT DATE = 12/6/2011



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1# CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1# CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

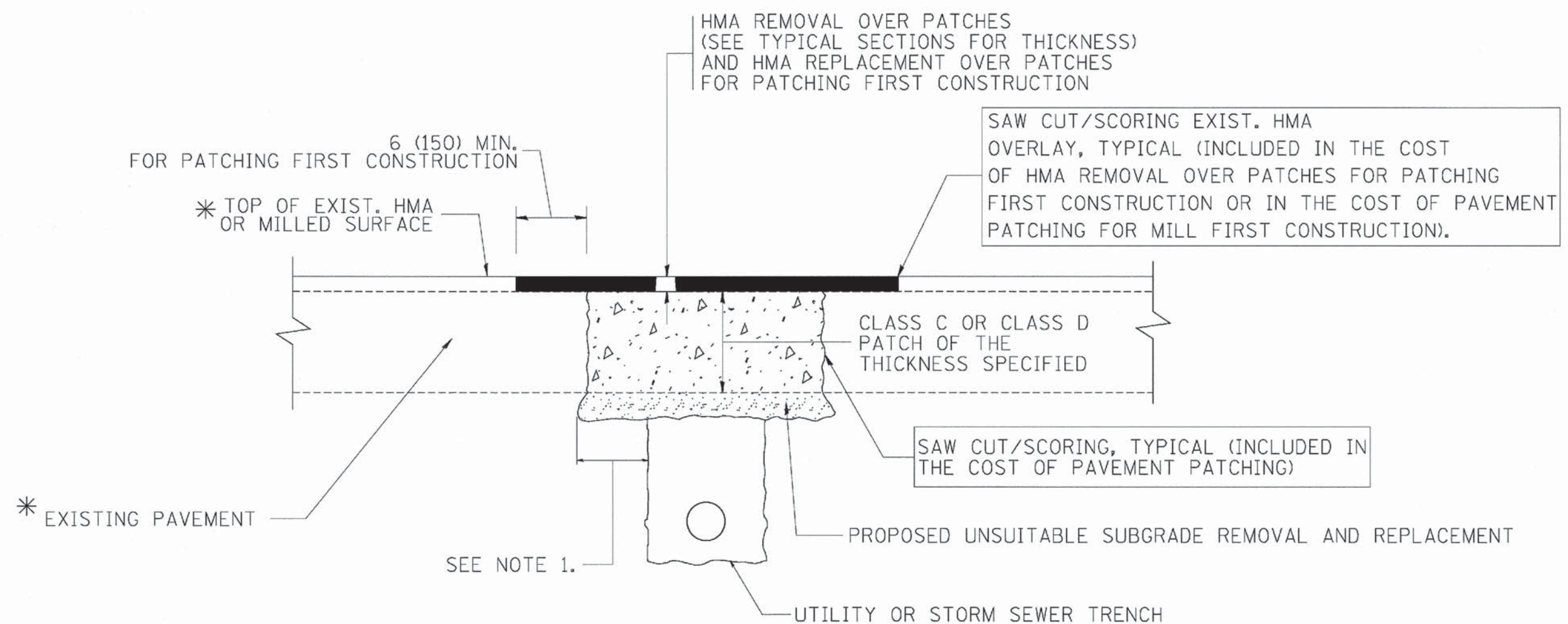
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bevard	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at\pwork\pwork\bevard\0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	13-00028-00-RS	COOK	17	10
PLOT SCALE = 1/8" = 1'-0"		CHECKED -	REVISED - R. BORO 03-09-11					BD600-03 (BD-8)			CONTRACT NO. 61A06	
PLOT DATE = 12/6/2011		DATE - 10-25-94	REVISED - R. BORO 12-06-11					FED. ROAD DIST. NO. 1			ILLINOIS FED. AID PROJECT M-4003-(255)	

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* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

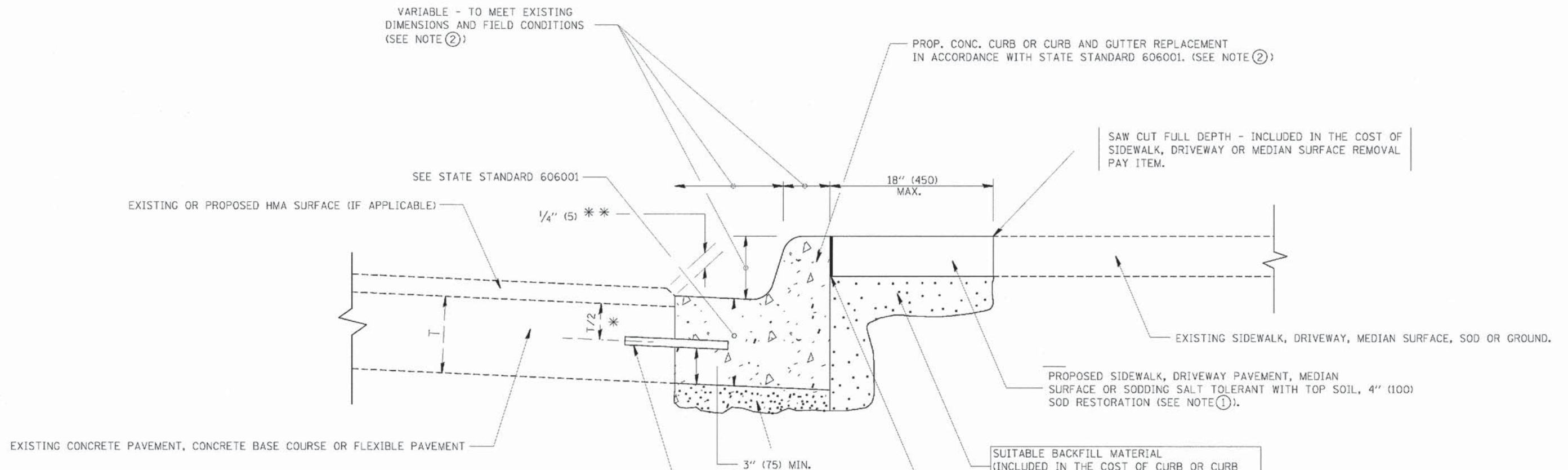
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
e:\projects\distatd22x34\bd22.dgn		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	1008	13-00028-00-RS	COOK	17	11
		PLOT SCALE = 50.000' / 1".	REVISED - R. BORO 09-04-07					BD400-04 (BD-22)		CONTRACT NO. 61A06			
		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(255)					



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

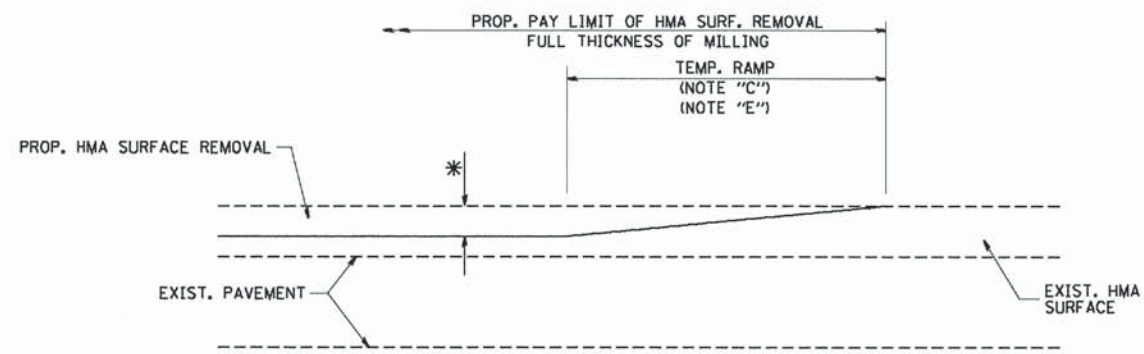
BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

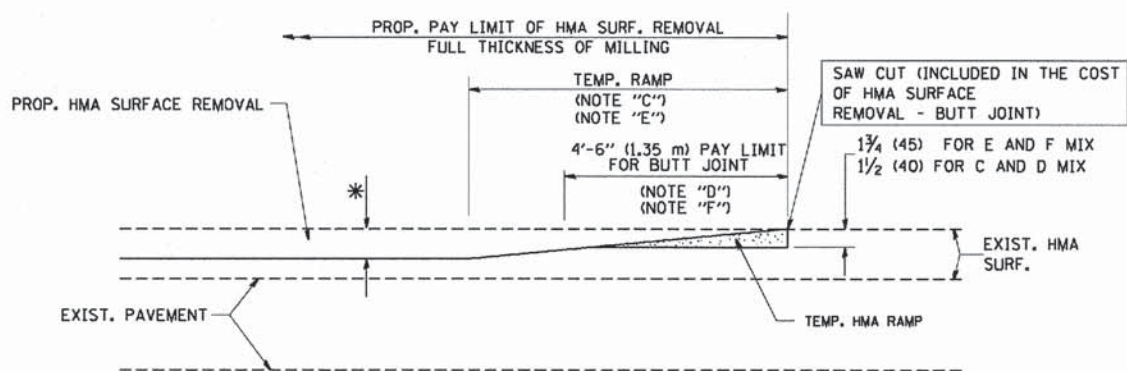
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DESIGNED	A. HOUSEH	REVISED	R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	NO.	
DRAWN	-	REVISED	A. ABBAS 03-21-97			1008	13-00028-00-RS	COOK	17	12	
CHECKED	-	REVISED	M. GOMEZ 01-22-01			BD600-06 (BD-24)					
DATE	03-11-94	REVISED	R. BORO 12-15-09			CONTRACT NO. 61A06					
PLCT DATE	12/15/2009	REVISED	-			FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003-(255)					



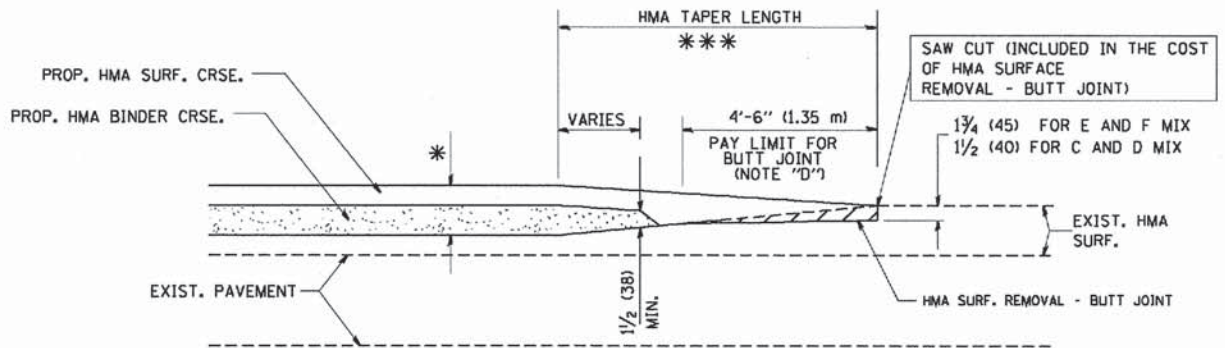
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



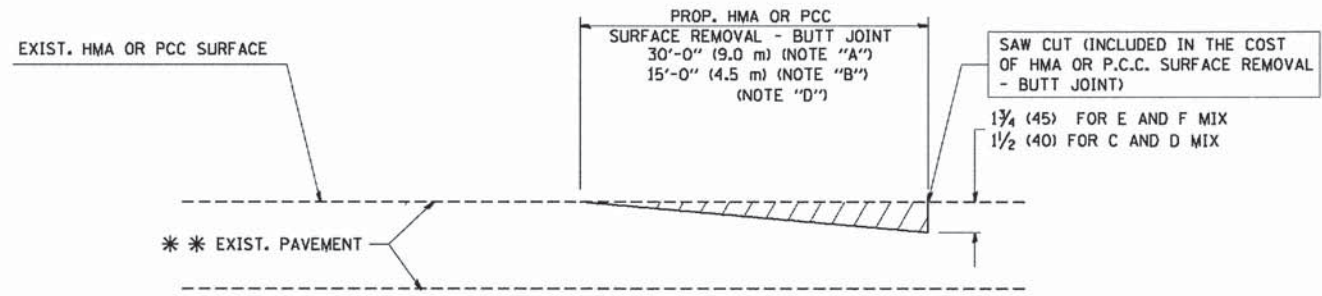
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2
TYPICAL TEMPORARY RAMP**

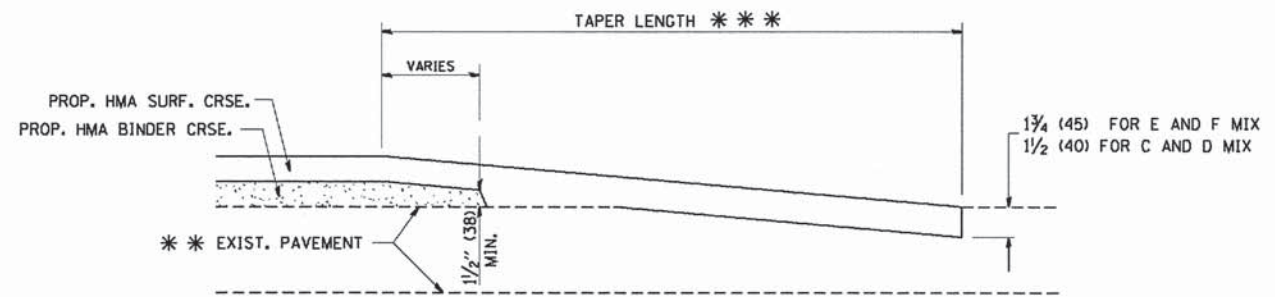


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME =	USER NAME = geglennob	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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	PLOT SCALE = 58.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1008	13-00028-00-RS	COOK	17	13
BD400-05 BD32		CONTRACT NO. 61A06		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003-(255)				

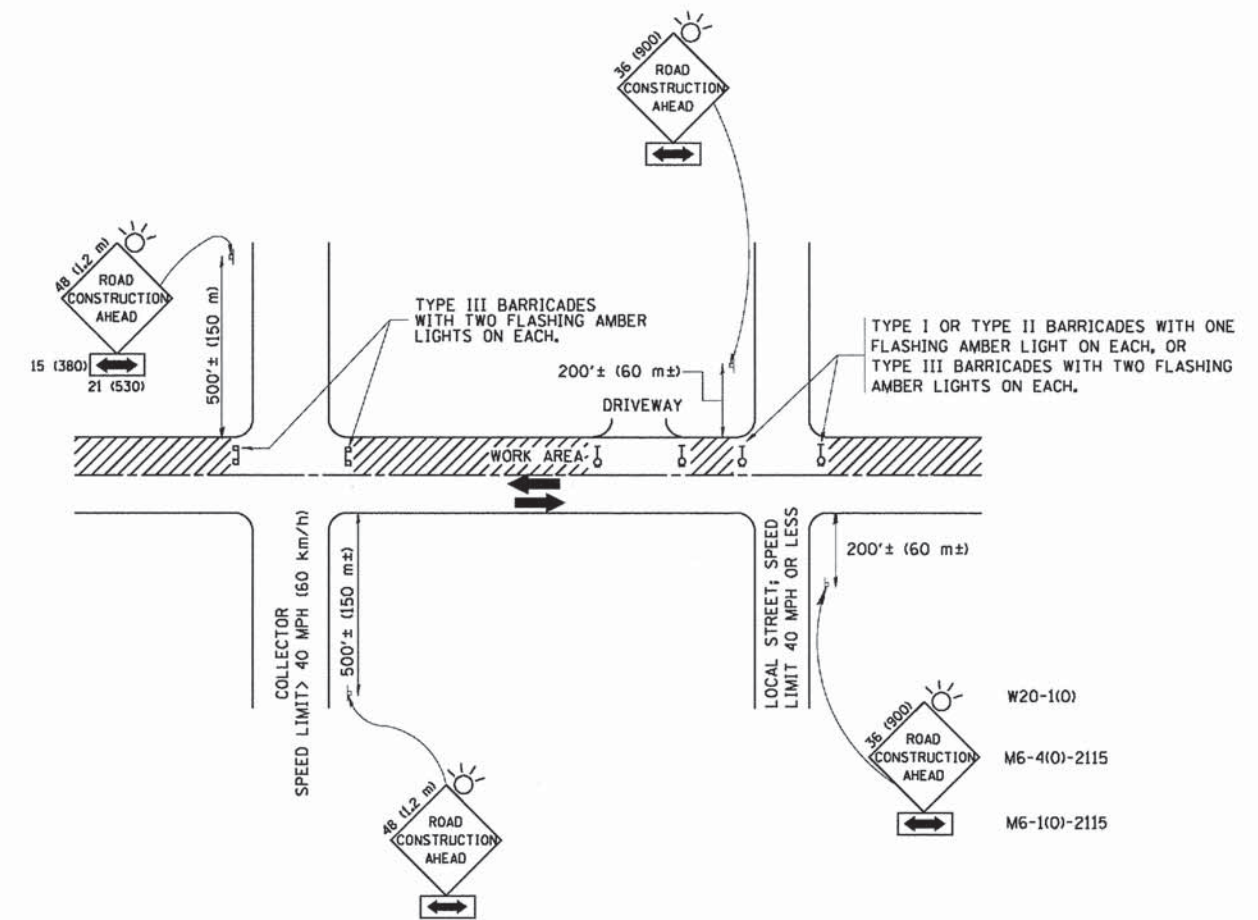
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FILE NAME = W:\dist1\22x34\1018.dgn	USER NAME = gogirenots	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1008	13-00028-00-RS	COOK	17	14
TC-10			CONTRACT NO. 61A06	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-4003(255)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

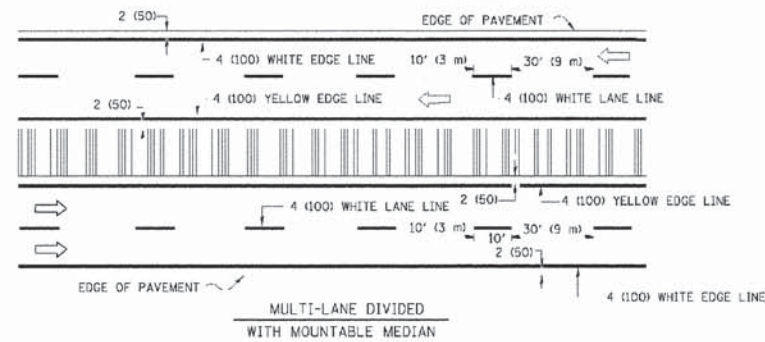
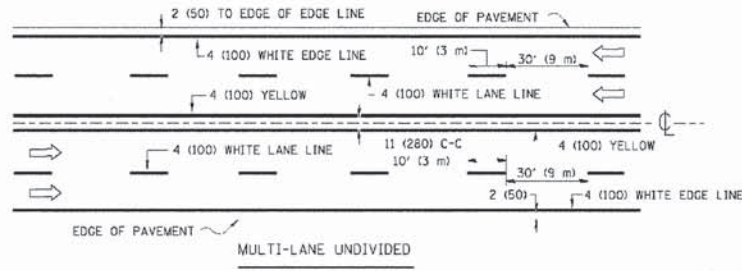
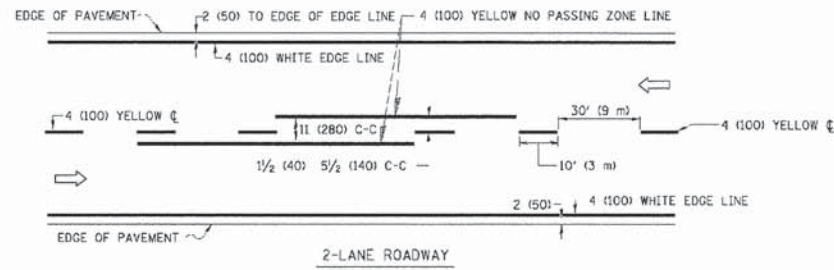
A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

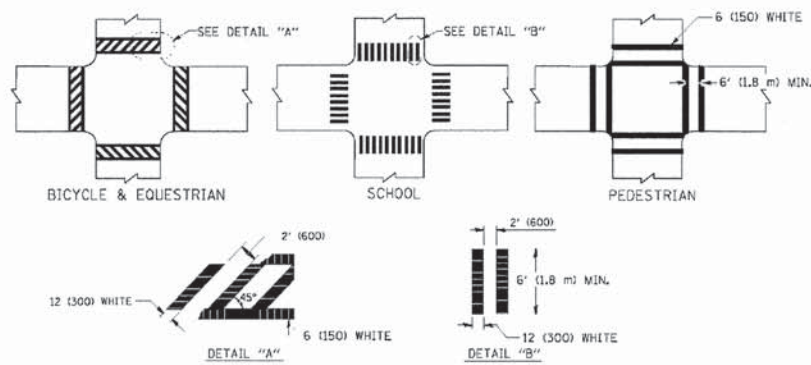
B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

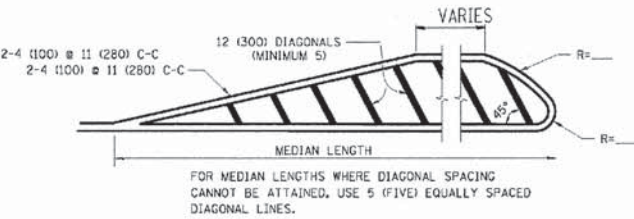
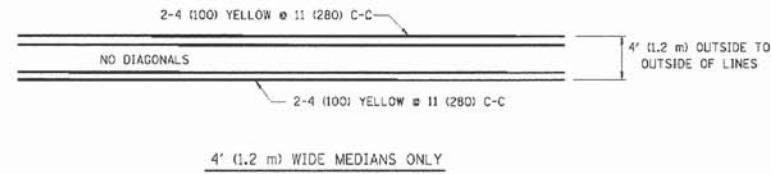
All dimensions are in millimeters (inches) unless otherwise shown.



TYPICAL LANE AND EDGE LINE MARKING



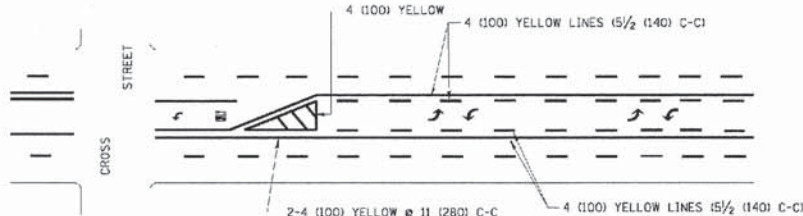
TYPICAL CROSSWALK MARKING



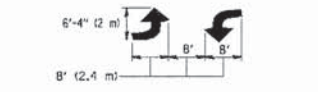
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

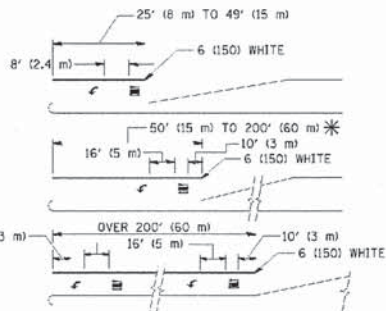
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



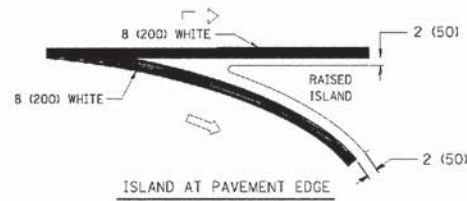
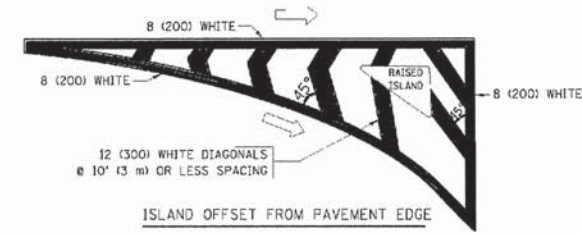
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

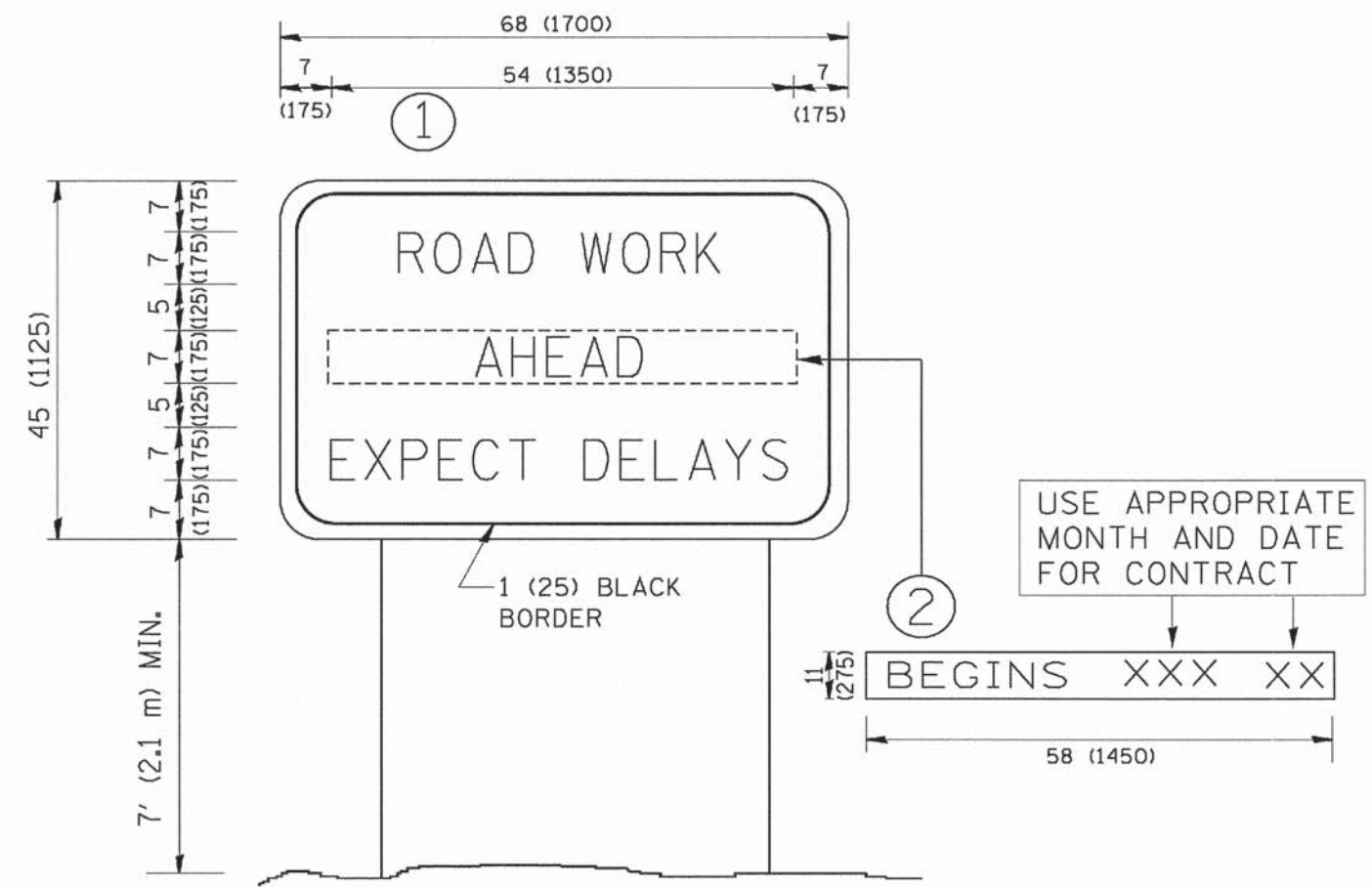
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 LICENSE NO. 184-00021 - EXPRESS - 3/25/2003
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DESIGNED	EVERS	REVISED	T. RAMMACHER 10-27-94
DRAWN	-	REVISED	-C. JUCIUS 09-09-09
CHECKED	-	REVISED	-
DATE	03-19-90	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEET NO.
TYPICAL PAVEMENT MARKINGS		1008	13-00028-00-RS	COOK	17 15
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		CONTRACT NO.	61A06
STA. TO STA.		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT		M-4003(255)	

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 LICENSE NO. 184-0007 EXPRESS EXPIRES 6/30/2015
 PROJECT: STATE OF ILLINOIS - SOUTH DISTRICT ONE DETAILS - TC-111.dgn
 USER: goglenobs
 FILE NAME: W:\dist1\22x34\1\to22.dgn



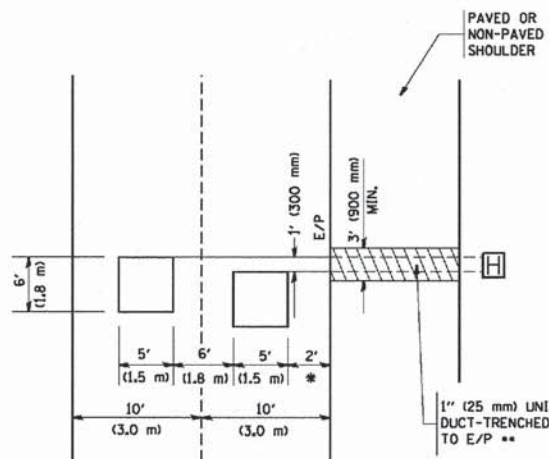
- NOTES:**
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
 3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
 4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dist1\22x34\1\to22.dgn	USER NAME = goglenobs	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A.U. RTE. 1008	SECTION 13-00028-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 16	
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	SCALE: NONE		SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	TC-22		CONTRACT NO. 61A06
PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(255)									

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

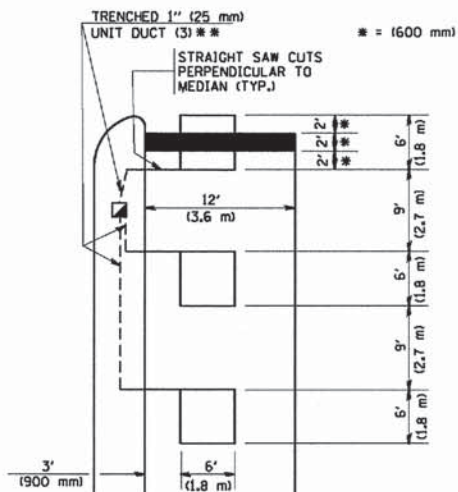


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

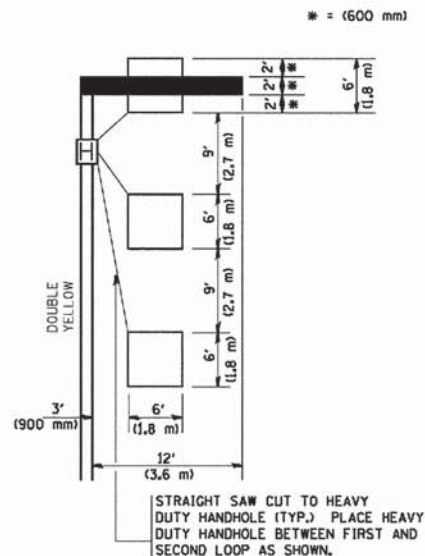
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD B14001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

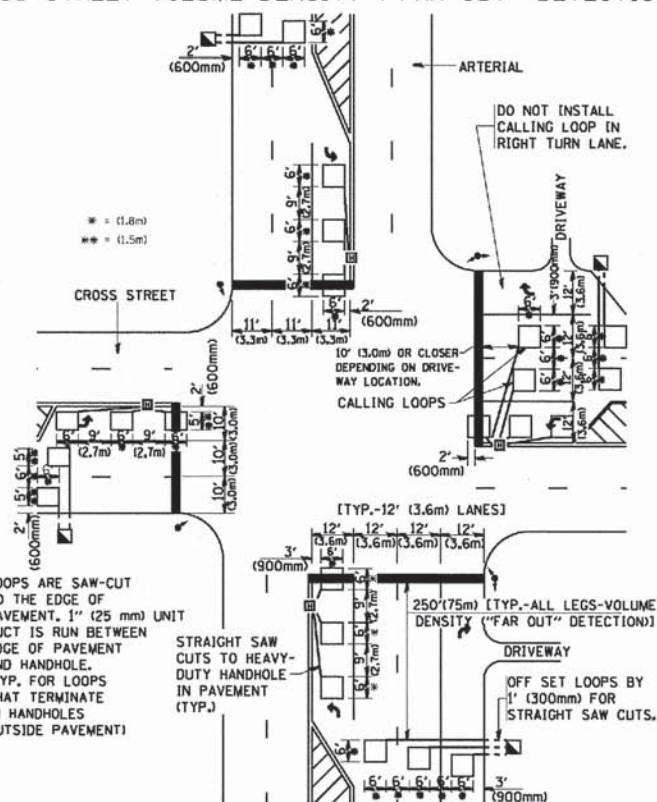
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



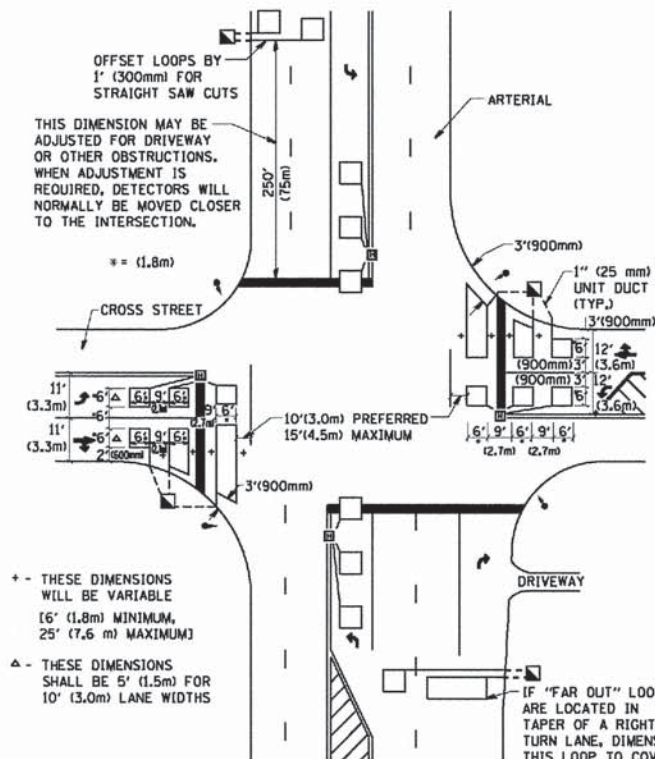
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1008	13-00028-00-RS	COOK	17	17
TS-07			CONTRACT NO. 61A06	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(255)				