

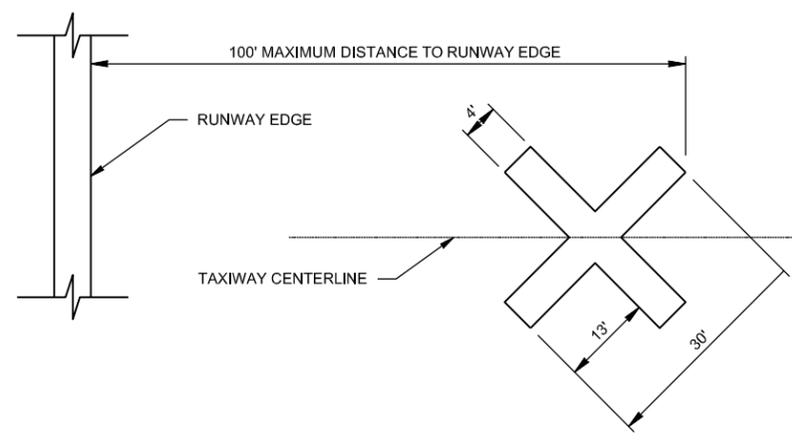
LEGEND

	PHASE 3A
	PHASE 3B
	PHASE 3C
	EXISTING
	NEW WORK AREA
	CONTRACTOR ACCESS
	BARRICADE LINE
	PORTABLE RUNWAY MARKER. (PROVIDED BY AIRPORT).
	CONTRACTOR'S FLAGMAN
	TEMPORARY TAXIWAY CLOSURE MARKER

UNICOM FREQUENCY - 122.70

PHASE 3 NOTES

1. WORK IN PHASES 3A AND 3B SHALL BE CONCURRENT AND SHALL REQUIRE THE CLOSURE OF BOTH RUNWAYS 16/34 AND 3/21.
2. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT AS TO WHICH DAYS OF THE WEEK ARE BEST FOR THE CLOSURE AND SHALL PROVIDE THEM 14 DAYS NOTIFICATION PRIOR TO SCHEDULING THE WORK.
3. THE CONTRACTOR MUST PROVIDE A YELLOW "X" AT EACH END OF THE RUNWAY IN ORDER TO CLOSE THAT RUNWAY (SEE CONSTRUCTION ACTIVITY PLAN 2). THIS PHASE SHALL REQUIRE A TOTAL OF 4 CLOSED RUNWAY MARKERS. TWO PORTABLE LIGHTED MARKERS SHALL BE PROVIDED BY THE AIRPORT AND TWO MARKER AS DETAILED ON CONSTRUCTION ACTIVITY PLAN 2 SHALL BE PROVIDED BY THE CONTRACTOR.
4. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE BOTH RUNWAYS FOR 2 CONSECUTIVE DAYS TO COMPLETE THE WORK IN PHASES 3A AND 3B. ALL WORK IN PHASES 3A AND 3B SHALL BE COMPLETED WITHIN THIS CLOSURE PERIOD. AT THE END OF THE SECOND CALENDAR DAY RUNWAY 3/21 SHALL BE INSPECTED BY THE AIRPORT AND RE-OPENED. SHOULD THE CONTRACTOR COMPLETE THE WORK PRIOR TO THE END OF THE SECOND CALENDAR DAY, THE RUNWAY SHALL BE RE-OPENED UPON IMMEDIATE INSPECTION BY THE AIRPORT.
5. WORK IN PHASE 3C MAY BE CONDUCTED AS PART OF PHASE 2 AND/OR 3B PROVIDED THAT IT DOES NOT RESULT IN LONGER RUNWAY 3/21 CLOSURE TIME.
6. WORK IN PHASES 3B AND 3C SHALL BE CONTINGENT UPON THE AWARD OF ADDITIVE ALTERNATES 1 AND 2.
7. THE CONTRACTOR SHALL PROVIDE A FLAGGER WHEN OPERATIONS REQUIRE CROSSING ACTIVE RUNWAY 3/21. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK IN SUCH A MANNER THAT RUNWAY CROSSINGS ARE LIMITED TO THE FEWEST POSSIBLE CROSSINGS EACH DAY.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
9. WORK SHALL NOT BE COMPLETED WITHIN 250' FROM THE CENTERLINE OF AN ACTIVE RUNWAY.
10. BARRICADES SHOWN ON THIS SHEET SHALL BE PLACED AT 250' FROM A RUNWAY CENTERLINE OR 100' FROM A TAXIWAY CENTERLINE.



TYPICAL TAXIWAY CLOSURE MARKER DETAIL

N.T.S.

NOTES

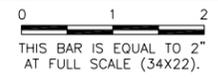
- 1.) THE MARKING CAN BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR CAN BE ANOTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND IS APPROVED BY THE AIRPORT
- 2.) PLACE OVER TAXIWAY CENTERLINE.
- 3.) YELLOW X SHALL BE ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH.
- 4.) TAXIWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND SHALL NOT BE MEASURED FOR PAYMENT.

FILE: CAP3.dwg
UPDATE BY: TJ Heavisides
PLOT DATE: 1/13/2014 10:48 AM

DNVBase_StatePlane
Baselines

VE049

REVISIONS		
NUMBER	BY	DATE



VERMILION REGIONAL AIRPORT AUTHORITY
VERMILION REGIONAL AIRPORT
DANVILLE, ILLINOIS

**REHABILITATE LIGHTS ON RUNWAY 16/34
CONSTRUCTION ACTIVITY PLAN PHASE 3**

© Copyright CMT, Inc.
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

DESIGN BY:	TJH
DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	JANUARY 3, 2014
JOB No:	13042-01
IL PROJ. NO.	DNV-4234
PROJ. NO.	3-17-SBGP-XX
SHEET	07 OF 14 SHEETS

K:\Drawings\1304201\Drawn\Sheets