

# CONSTRUCTION PLANS FOR VERMILION REGIONAL AIRPORT

VERMILION REGIONAL AIRPORT AUTHORITY  
 DANVILLE, ILLINOIS

ILLINOIS PROJECT DNV-4232  
 SBG PROJECT 3-17-SBGP-XX

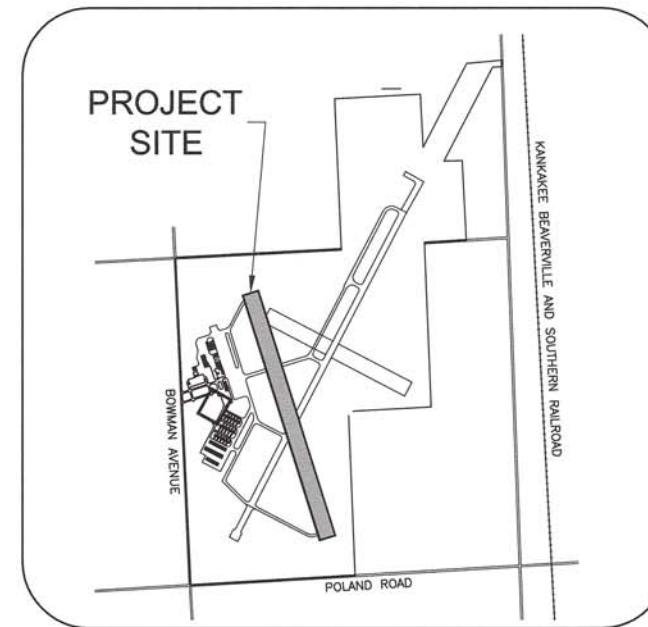
## REHABILITATE LIGHTS ON RUNWAY 16/34

ADDITIVE ALTERNATE #1: REHABILITATE LIGHTS ON TAXIWAY D  
 ADDITIVE ALTERNATE #2: INSTALL COUNTERPOISE ON TAXIWAY D  
 ADDITIVE ALTERNATE #3: LED UPGRADE

JANUARY 3, 2013



LOCATION MAP



SITE PLAN

**811** Know what's below. Call before you dig.

J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS  
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

MAXIMUM EQUIPMENT HEIGHT = 25'  
 GROUND FREQUENCY 122.70 MHz

**811** Know what's below. Call before you dig.

COMMON GROUND ALLIANCE  
 www.call811.com or  
 Phone: 811

VERMILION REGIONAL AIRPORT

TOWNSHIP: 20 NORTH  
 RANGE: 11 WEST  
 SECTION: 15  
 COUNTY: VERMILION  
 CIVIL TOWNSHIP:



VERMILION COUNTY AIRPORT AUTHORITY  
 VERMILION COUNTY AIRPORT  
 DANVILLE, ILLINOIS

APPROVED *[Signature]*  
 AIRPORT MANAGER

DATE 1-3-14

**CMT**  
 CRAWFORD MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 ■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *[Signature]*  
 DATE JANUARY 13, 2014

CMT JOB NUMBER 09042-02-00

## SUMMARY OF QUANTITIES

BASE BID			
ITEM #	DESCRIPTION	UNIT	QUANTITY
AR108158	1/C #8 5KV UG CABLE IN UD	L.F.	8,650
AR108706	1/C #6 COUNTERPOISE	L.F.	10,650
AR800250	2-1/C #8 5KV UG CABLE IN UD	L.F.	2,900
AR125415	MITL - BASE MOUNTED	EACH	3
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	4
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	2
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	2
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	1
AR125505	MIRL, STAKE MOUNTED	EACH	22
AR125510	MIRL, BASE MOUNTED	EACH	16
AR125545	MI THRESHOLD LIGHT BASE MOUNTED	EACH	14
AR125565	SPLICE CAN	EACH	3
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	43
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	8
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	9

ADDITIVE ALTERNATE 1			
ITEM #	DESCRIPTION	UNIT	QUANTITY
AS108158	1/C #8 5KV UG CABLE IN UD	L.F.	2,700
AS800250	2-1/C #8 5KV UG CABLE IN UD	L.F.	265
AS125410	MITL - STAKE MOUNTED	EACH	27
AS125415	MITL - BASE MOUNTED	EACH	3
AS125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	1
AS125901	REMOVE STAKE MOUNTED LIGHT	EACH	23
AS125902	REMOVE BASE MOUNTED LIGHT	EACH	4
AS125904	REMOVE TAXI GUIDANCE SIGN	EACH	1

ADDITIVE ALTERNATE 2			
ITEM #	DESCRIPTION	UNIT	QUANTITY
AT108706	1/C #6 COUNTERPOISE	LF	2,700

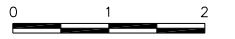
ADDITIVE ALTERNATE 3			
ITEM #	DESCRIPTION	UNIT	QUANTITY
AU125417	MITL LED UPGRADE	EACH	33
AU125462	SIGN LED UPGRADE	EACH	10
AU125511	MIRL LED UPGRADE	EACH	38
AU125546	MI THRESHOLD LIGHT LED UPGRADE	EACH	14

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SHEET NO.	SHEET TITLE
01	COVER SHEET
02	SUMMARY OF QUANTITIES
03	SITE PLAN
04	CONSTRUCTION SAFETY PHASING PLAN NOTES
05	CONSTRUCTION ACTIVITY PLAN PHASE 1
06	CONSTRUCTION ACTIVITY PLAN PHASE 2
07	CONSTRUCTION ACTIVITY PLAN PHASE 3
08	ELECTRICAL SITE PLAN 1
09	ELECTRICAL SITE PLAN 2
10	ELECTRICAL SITE PLAN 3
11	ELECTRICAL SITE PLAN 4
12	ELECTRICAL DETAILS 1
13	ELECTRICAL DETAILS 2
14	SIGN DETAIL AND SCHEDULE

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 UPDATE BY: TJ Heavisides  
 PLOT DATE: 1/23/2014 1:33 PM


**VE049**

REVISIONS		
NUMBER	BY	DATE

  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

VERMILION REGIONAL AIRPORT AUTHORITY  
 VERMILION REGIONAL AIRPORT  
 DANVILLE, ILLINOIS

REHABILITATE LIGHTS ON RUNWAY 16/34  
 SUMMARY OF QUANTITIES

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DESIGN BY: TJH  
 DRAWN BY: CMT  
 CHECKED BY: AJB  
 APPROVED BY: CET  
 DATE: JANUARY 3, 2014  
 JOB No: 13042-01

ILL. PROJ. NO. DNV-4234  
 PROJ. NO. 3-17-SBGP-XX

GENERAL NOTES

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION. AT NO POINT SHALL EQUIPMENT OR MATERIALS BE ALLOWED TO BE STORED ON RUNWAYS.
- MEN, VEHICLES AND EQUIPMENT WILL NOT BE ALLOWED WITHIN THE FOLLOWING AREAS:
  - 250' FROM THE CENTERLINE OF RUNWAY 3/21 WHEN ACTIVE.
  - 93' FROM THE CENTERLINE OF AN ACTIVE TAXIWAY.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING A TAXIWAY OR RUNWAY OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH AIR AND GROUND TRAFFIC AT ALL TIMES. CONTRACTOR SHALL PROVIDE THE ESCORT/FLAGMAN AND RADIO EQUIPMENT AT NO EXTRA COST TO THE CONTRACT.
- ABANDONED EQUIPMENT OR OTHER RUBBISH SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
  - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
  - THE CONTRACTOR SHALL MAINTAIN A COMPLETE LIST OF PERSONNEL AUTHORIZED BY THE CONTRACTOR TO WORK ON THE AIRFIELD. THIS LIST SHALL BE PROVIDED TO THE AIRPORT AND RESIDENT ENGINEER BY THE CONTRACTOR UPON REQUEST.
  - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER AND VRAA MAINTENANCE SUPERVISOR. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
  - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
  - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR CONTROL ACCESS AT THE GATE SHOULD THERE BE A NEED TO KEEP GATES OPEN AND UNLOCKED.
  - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
  - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES.

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VE049

REVISIONS

NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

VERMILION REGIONAL AIRPORT AUTHORITY  
 VERMILION REGIONAL AIRPORT  
 DANVILLE, ILLINOIS  
 REHABILITATE LIGHTS ON RUNWAY 16/34  
 SITE PLAN

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 SHEET 03 OF 14 SHEETS



AREA OF CONSTRUCTION

CONTRACTOR'S STAGING, STORAGE, AND PARKING

CONTRACTOR'S ACCESS

PARKING APRON

BOWMAN AVENUE

T-HANGAR TAXIWAY  
 TAXIWAY C

TAXIWAY C1

TAXIWAY D

TAXIWAY C

TAXIWAY B

TAXIWAY A

CRITICAL POINTS					
POINT ID	LATITUDE	LONGITUDE	GROUND ELEVATION	EQUIPMENT HEIGHT	GROUND ELEVATION
1	40°11'52.98"	87°35'50.40"	649'	25'	674'
2	40°11'47.60"	87°35'48.41"	660'	25'	685'
3	40°11'36.58"	87°35'55.72"	658'	25'	683'
4	40°12'09.51"	87°36'04.64"	670'	25'	695'
5	40°12'07.40"	87°36'01.07"	672'	25'	697'
6	40°11'58.63"	87°35'57.83"	661'	25'	686'
7	40°12'00.97"	87°35'47.06"	664'	25'	689'

CONTROL POINTS				
POINT ID	FIELD OBJECT	NORTHING	EASTING	ELEVATION
A	NGS MONUMENT	1,285,362.746	1,189,401.908	648.660'
B	NGS MONUMENT	1,286,988.989	1,189,984.259	660.278'
C	IRON PIN	1,286,354.603	1,189,892.825	657.770'
D	IRON PIN	1,287,026.341	1,189,695.216	658.940'
E	IRON PIN	1,288,370.389	1,189,301.610	674.470'

LEGEND

- AREA OF CONSTRUCTION
- EXISTING
- PROPOSED WORK AREA
- CONTRACTOR'S ACCESS
- CONTROL POINT
- CRITICAL POINT

UNICOM FREQUENCY - 122.70

**1. GENERAL**

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.
6. THE CONTRACTOR SHALL BACKFILL ALL SAFETY AREAS DISTURBED AT ACTIVE PAVEMENTS AT THE END OF EACH WORK DAY.

**2. COORDINATION**

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
4. IT IS ANTICIPATED THAT THE AIRPORT WILL HAVE MULTIPLE CONSTRUCTION PROJECTS THAT WILL REQUIRE RUNWAY CLOSURES. THE CONTRACTOR SHALL COORDINATE THE CONSTRUCTION SCHEDULE/PHASING CLOSELY WITH OTHER CONTRACTORS PRIOR TO THE START OF CONSTRUCTION. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.

**3. PHASING**

1. TOTAL CONTRACT TIME SHALL BE AS FOLLOWS:  
BASE BID = 48 CALENDAR DAYS.  
ADDITIVE ALTERNATE 1 = 13 CALENDAR DAYS  
ADDITIVE ALTERNATE 2 = 3 CALENDAR DAYS  
ADDITIVE ALTERNATE 3 = 0 CALENDAR DAYS
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

**4. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY**

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION ACTIVITY PLANS.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

**5. CONTRACTOR ACCESS**

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
3. CONTRACTOR SHALL MAINTAIN LIST OF NAMES, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
4. CONTRACTOR WORK CREWS MUST MONITOR THE UNICOM AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS. BE AWARE OF TENANT AIRCRAFT MOVEMENT NEAR THE WORK AREAS.
5. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
6. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
7. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
8. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
9. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A HANDHELD POWER AIR BLOWER SHALL BE ON SITE AT AND AVAILABLE FOR USED AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
10. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
11. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

**6. WILDLIFE MANAGEMENT**

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

**7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT**

1. THE CONTRACTOR SHALL PICK UP FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

**8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT**

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

**9. NOTIFICATION OF CONSTRUCTION ACTIVITIES**

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 14 DAYS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT AND THE AIRPORT MAY COORDINATE CLOSURES WITH TENANTS/FREQUENT AIRPORT USERS.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911 AND AIRPORT SAFETY.
5. CONTACTS FOR THIS PROJECT ARE AS DISCUSSED IN THE PRE-CONSTRUCTION MEETING.

**10. INSPECTION REQUIREMENTS**

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
  2. THE CONTRACTOR AND AIRPORT SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.
- 11. UNDERGROUND UTILITIES**
1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
  2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

**12. PENALTIES**

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

**13. SPECIAL CONDITIONS**

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

**14. RUNWAY AND TAXIWAY VISUAL AIDS**

1. PROJECT WILL REQUIRE THE CLOSURE OF RUNWAY 16/34 FOR THE DURATION OF THE CONTRACT. THE CONTRACTOR SHALL USE MARKING, LIGHTING, AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

**15. MARKING AND SIGNS FOR ACCESS ROUTES**

1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

**16. HAZARD MARKING AND LIGHTING**

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE AIRPORT.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

**17. PROTECTION**

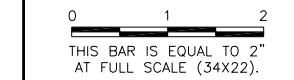
1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED INSIDE OF RUNWAY SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT AS INDICATED ON THE CONSTRUCTION ACTIVITY PLANS PRIOR TO THE REQUESTED CLOSURE TIME.

**18. OTHER LIMITATIONS ON CONSTRUCTION**

1. IF DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

**VE049**


REVISIONS		
NUMBER	BY	DATE



VERMILION REGIONAL AIRPORT AUTHORITY  
VERMILION REGIONAL AIRPORT  
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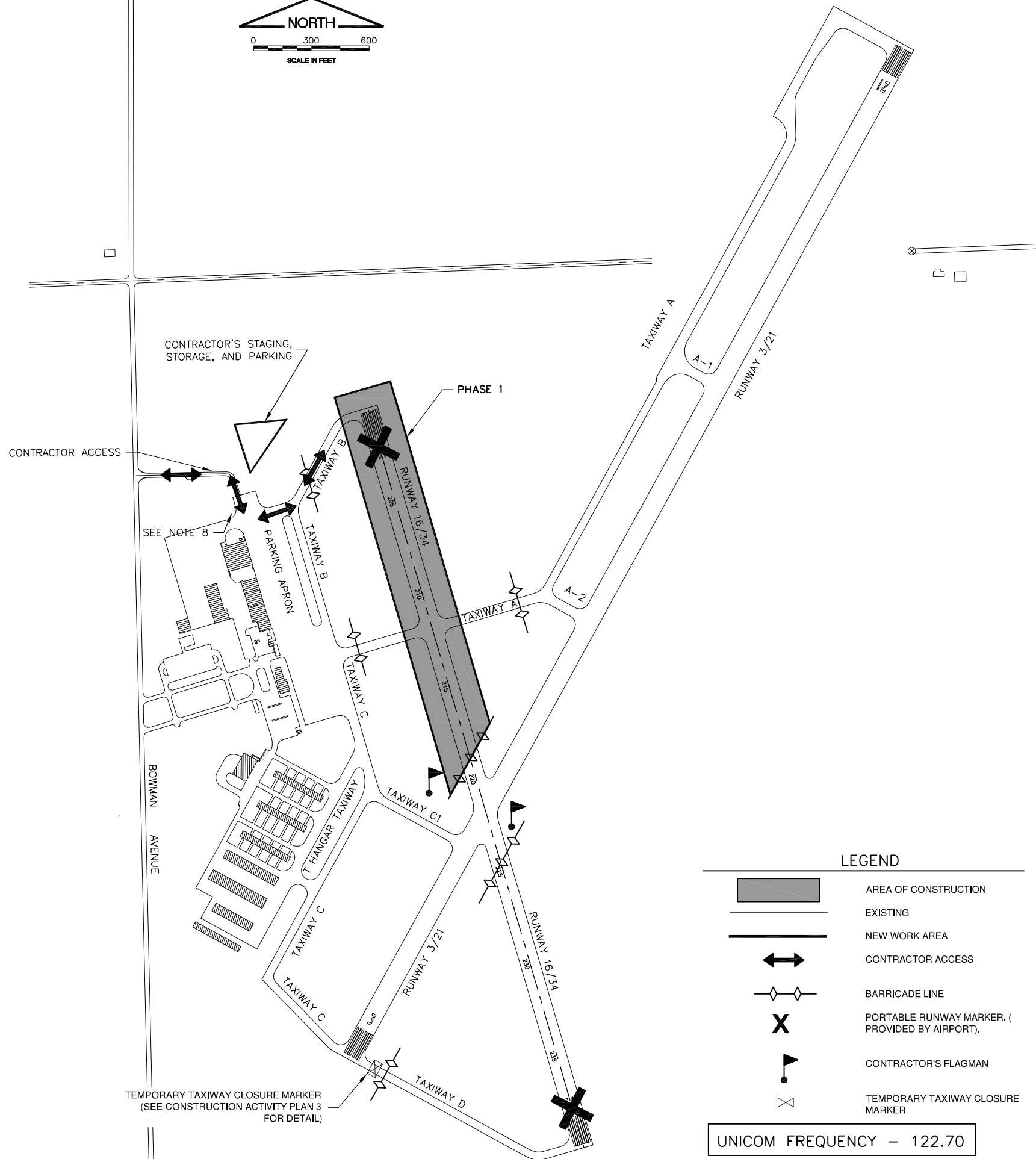
REHABILITATE LIGHTS ON RUNWAY 16/34  
CONSTRUCTION SAFETY PHASING PLAN  
NOTES

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JOB No:	13042-01
IL PROJ. NO. DNV-4234 PROJ. NO. 3-17-SBGP-XX	
SHEET 04 OF 14 SHEETS	



**LEGEND**

	AREA OF CONSTRUCTION
	EXISTING
	NEW WORK AREA
	CONTRACTOR ACCESS
	BARRICADE LINE
	PORTABLE RUNWAY MARKER. ( PROVIDED BY AIRPORT).
	CONTRACTOR'S FLAGMAN
	TEMPORARY TAXIWAY CLOSURE MARKER

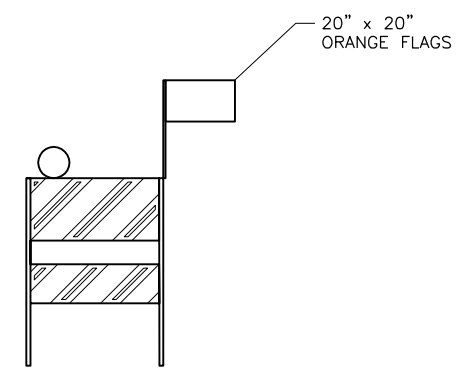
UNICOM FREQUENCY - 122.70

**GENERAL CONSTRUCTION SCHEDULING/PHASING NOTES**

1. THE AIRPORT IS SCHEDULED TO HAVE AN EVENT ON JUNE 17TH, 2014 WHICH REQUIRES THE FULL SERVICE OF THE AIRPORT WITHOUT DISRUPTION FROM CONSTRUCTION OPERATIONS. THEREFORE, JUNE 23RD SHALL BE THE EARLIEST DATE CONSIDERED FOR THE COMMENCEMENT OF THIS PROJECT.
2. THE CONTRACTOR SHALL COORDINATE THE SCHEDULE AND PHASING OF THIS PROJECT CLOSELY WITH OTHER CONTRACTORS ON SITE. IT IS OF PARTICULAR CONCERN THAT CONSTRUCTION SCHEDULING BE COORDINATED WITH THE AIRPORT AND THE CONTRACTOR FOR THE "RELOCATE TAXIWAY C AND WIDEN FILLETS" PROJECT AS THAT PROJECT REQUIRES THE CLOSURE OF RUNWAY 3/21. CONSTRUCTION OF THE TWO PROJECTS MAY NOT BE CONCURRENT IF THEY RESULT IN THE CLOSURE OF BOTH RUNWAY 3/21 AND RUNWAY 16/34 FOR LONGER THAN THE SPECIFIED RUNWAY 3/21 CLOSURE ON CONSTRUCTION ACTIVITY PLAN 3 OF THIS PROJECT.
3. PHASES 1 THROUGH 3 MAY BE INTERCHANGEABLE PROVIDED THAT THE CONTRACTOR PROVIDES A WRITTEN REQUEST TO REVISE THE PHASING WITH SUFFICIENT TIME FOR THE AIRPORT TO REVIEW AND ISSUE APPROPRIATE NOTIFICATIONS BASED ON CLOSURE REQUIREMENTS. THE AIRPORT SHALL HAVE THE FINAL SAY REGARDING CONSTRUCTION PHASING.
4. SHOULD THE "RELOCATE TAXIWAY C AND WIDEN FILLETS" PROJECT BEGIN PRIOR TO THE COMMENCEMENT OF THIS PROJECT, THE CONTRACTOR SHALL SCHEDULE TO COMPLETE PHASES 3A AND 3B DURING THE RUNWAY 3/21 CLOSURE FOR THE "RELOCATE TAXIWAY C AND WIDEN FILLETS" PROJECT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION TO COMPLETE PHASE 3C AT THAT TIME PROVIDED THAT WORK IN PHASES 3A AND 3B ARE COMPLETED WITHIN THE ALLOTTED RUNWAY 3/21 CLOSURE TIME (AND PROVIDED THAT THE RELATED ADDITIVE ALTERNATE WORK IS AWARDED). AFTER THE COMPLETION OF PHASE 3, THE PROJECT SHALL BE SUSPENDED UNTIL THE COMPLETION OF THE "RELOCATE TAXIWAY C AND WIDEN FILLETS" PROJECT AND RUNWAY 3/21 HAS BEEN RE-OPENED.

**PHASE 1 NOTES**

1. THE CONTRACTOR SHALL CLOSE RUNWAY 16/34 FOR THE DURATION OF THIS PHASE.
2. THE CONTRACTOR MUST PROVIDE A YELLOW "X" AT EACH END OF THE RUNWAY IN ORDER TO CLOSE THAT RUNWAY (SEE CONSTRUCTION ACTIVITY PLAN 2).
3. THE CONTRACTOR MUST PROVIDE A YELLOW "X" ON TAXIWAY D AT THE INTERSECTION OF RUNWAY 3/21 (SEE DETAIL ON CONSTRUCTION ACTIVITY PLAN 3).
4. THE CONTRACTOR SHALL PROVIDE A FLAGGER WHEN OPERATIONS REQUIRE CROSSING ACTIVE RUNWAY 3/21. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK IN SUCH A MANNER THAT RUNWAY CROSSINGS ARE LIMITED TO THE FEWEST POSSIBLE CROSSINGS EACH DAY.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
6. WORK SHALL NOT BE COMPLETED WITHIN 250' FROM THE CENTERLINE OF AN ACTIVE RUNWAY. WORK WITHIN 250' OF RUNWAY 3/21 SHALL REQUIRE THE CLOSURE OF THAT RUNWAY AND SHALL BE AS NOTED ON CONSTRUCTION ACTIVITY PLAN 3.
7. BARRICADES SHOWN ON THIS SHEET SHALL BE PLACED AT 250' FROM A RUNWAY CENTERLINE OR 100' FROM A TAXIWAY CENTERLINE.
8. THE CONTRACTOR SHALL PLACE A TEMPORARY STOP SIGN AT THE INTERSECTION OF THE AIRFIELD APRON TO ALERT CONSTRUCTION TRAFFIC TO YIELD TO AIRPORT OPERATIONS. ALL COSTS ASSOCIATED WITH THE SIGN SHALL BE INCIDENTAL TO THE PROJECT.



**FLASHER BARRICADE DETAIL**  
N.T.S.

**FLASHER BARRICADE NOTES**

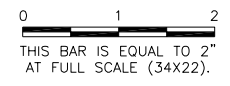
1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 15' INTERVALS.

FILE: CAP1.dwg  
UPDATE BY: TJ Heavisides  
PLOT DATE: 1/13/2014 10:48 AM

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**VERMILION REGIONAL AIRPORT AUTHORITY**  
**VERMILION REGIONAL AIRPORT**  
**DANVILLE, ILLINOIS**

**REHABILITATE LIGHTS ON RUNWAY 16/34**  
**CONSTRUCTION ACTIVITY PLAN PHASE 1**

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JOB No:	13042-01
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PROJ. NO.	3-17-SBGP-XX
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PHASE 2 NOTES

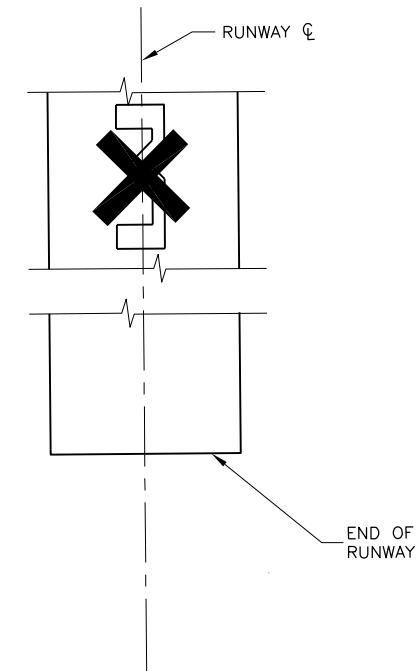
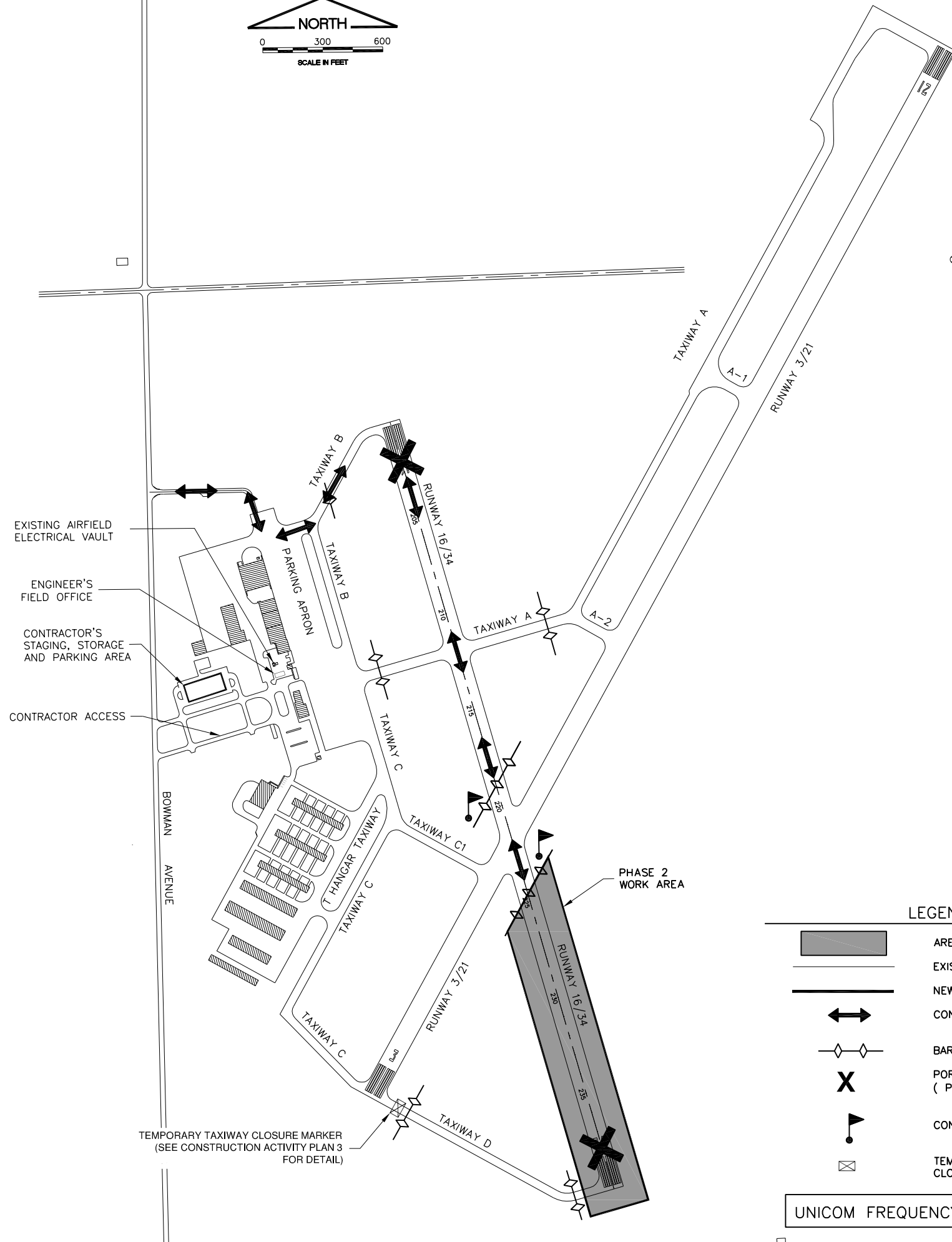
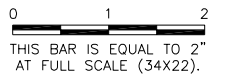
1. THE CONTRACTOR SHALL CLOSE RUNWAY 16/34 FOR THE DURATION OF THIS PHASE.
2. THE CONTRACTOR MUST PROVIDE A YELLOW "X" AT EACH END OF THE RUNWAY IN ORDER TO CLOSE THAT RUNWAY.
3. THE CONTRACTOR SHALL PROVIDE A FLAGGER WHEN OPERATIONS REQUIRE CROSSING ACTIVE RUNWAY 3/21. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK IN SUCH A MANNER THAT RUNWAY CROSSINGS ARE LIMITED TO THE FEWEST POSSIBLE CROSSINGS EACH DAY.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
5. WORK SHALL NOT BE COMPLETED WITHIN 250' FROM THE CENTERLINE OF AN ACTIVE RUNWAY. WORK WITHIN 250' OF RUNWAY 3/21 SHALL REQUIRE THE CLOSURE OF THAT RUNWAY AND SHALL BE AS NOTED ON CONSTRUCTION ACTIVITY PLAN 3.
6. BARRICADES SHOWN ON THIS SHEET SHALL BE PLACED AT 250' FROM A RUNWAY CENTERLINE OR 100' FROM A TAXIWAY CENTERLINE.
7. WORK IN PHASE 2 MAY BE COMPLETED CONCURRENTLY WITH WORK IN PHASE 1 PROVIDED THAT THE WORK IS MANAGED AS TWO SEPARATE PHASES AND THE NUMBER OF RUNWAY CROSSINGS ARE NOT INCREASED.

FILE: CAP2.dwg  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 1/13/2014 10:48 AM  
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CLOSED RUNWAY MARKER DETAIL

N.T.S.

NOTES

1. MARKERS SHALL BE SOLID YELLOW.
2. MARKERS SHALL BE PAINTED BURLAP, PLYWOOD OR OTHER APPROVED SOLID MATERIALS.
3. CONTRACTOR SHALL MAINTAIN MARKERS.
4. COST OF FURNISHING, INSTALLING, MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.
6. PORTABLE CLOSED RUNWAY MARKERS SHALL BE PROVIDED BY THE AIRPORT FOR USE ON RUNWAY 3/21. THE MARKERS SHALL BE INSTALLED, OPERATED AND MAINTAINED BY THE CONTRACTOR DURING THE LENGTH OF THE RUNWAY CLOSURE. ALL COSTS ASSOCIATED WITH THE PORTABLE CLOSED RUNWAY MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.

LEGEND

- AREA OF CONSTRUCTION
- EXISTING
- NEW WORK AREA
- CONTRACTOR ACCESS
- BARRICADE LINE
- PORTABLE RUNWAY MARKER. ( PROVIDED BY AIRPORT).
- CONTRACTOR'S FLAGMAN
- TEMPORARY TAXIWAY CLOSURE MARKER

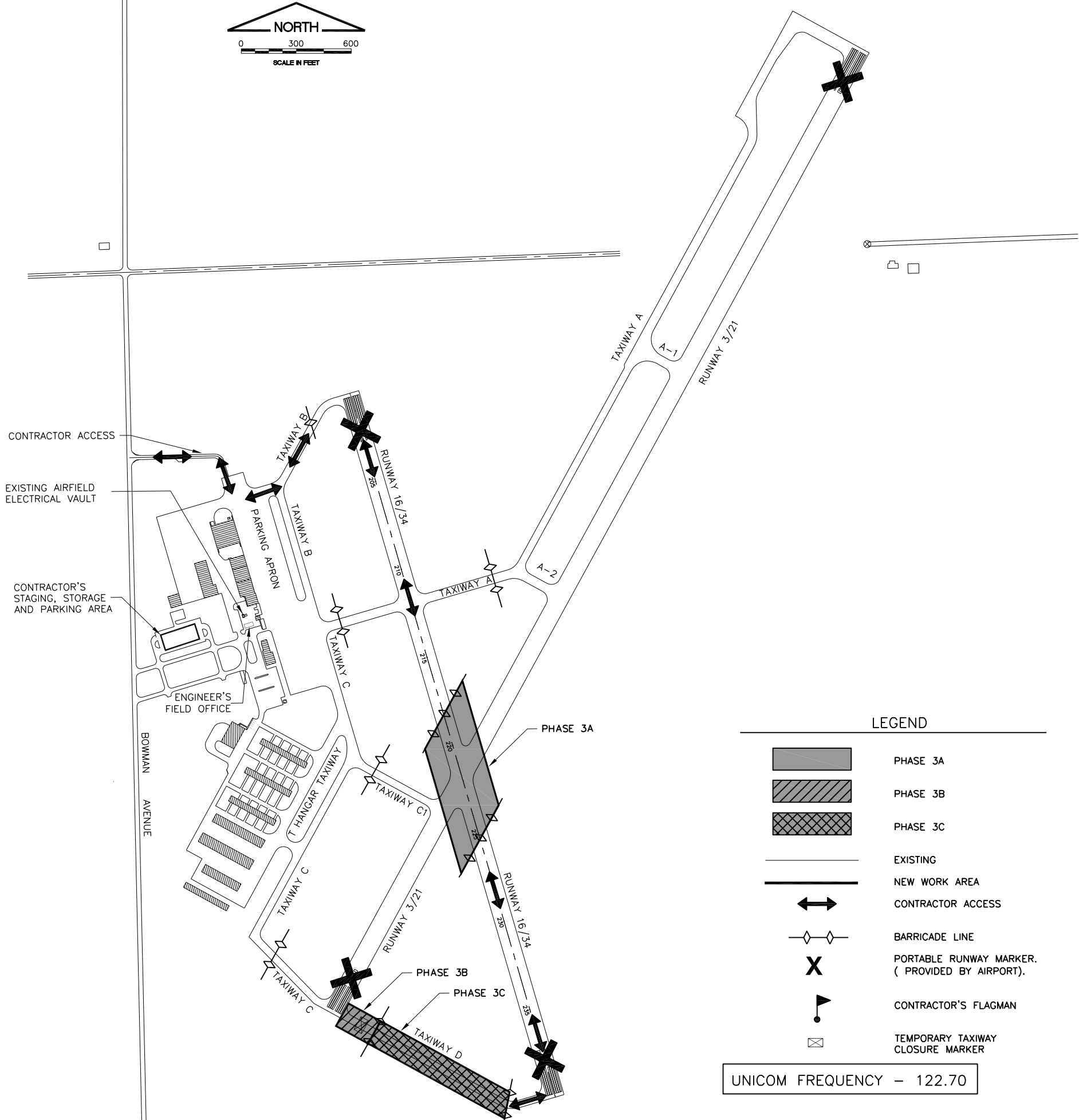
UNICOM FREQUENCY - 122.70

VERMILION REGIONAL AIRPORT AUTHORITY  
 VERMILION REGIONAL AIRPORT  
 DANVILLE, ILLINOIS

REHABILITATE LIGHTS ON RUNWAY 16/34  
 CONSTRUCTION ACTIVITY PLAN PHASE 2

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APPROVED BY:	CET
DATE:	JANUARY 3, 2014
JOB No:	13042-01
IL PROJ. NO.	DNV-4234
PROJ. NO.	3-17-SBGP-XX
SHEET	06 OF 14 SHEETS



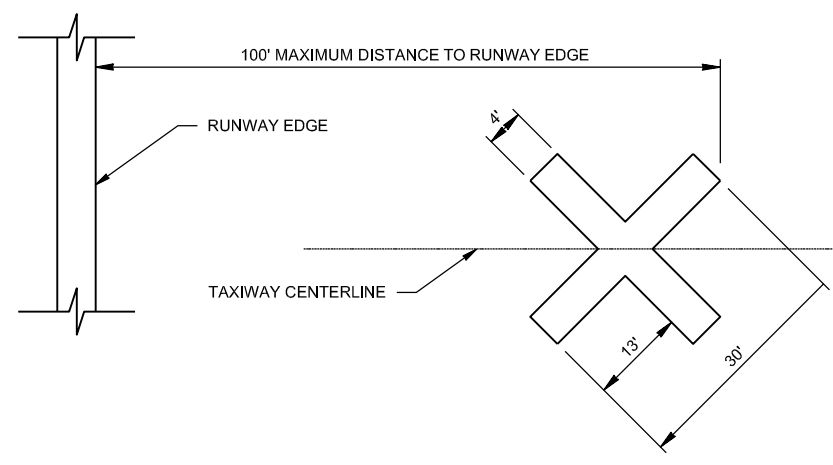
**LEGEND**

	PHASE 3A
	PHASE 3B
	PHASE 3C
	EXISTING
	NEW WORK AREA
	CONTRACTOR ACCESS
	BARRICADE LINE
	PORTABLE RUNWAY MARKER. ( PROVIDED BY AIRPORT).
	CONTRACTOR'S FLAGMAN
	TEMPORARY TAXIWAY CLOSURE MARKER

UNICOM FREQUENCY - 122.70

**PHASE 3 NOTES**

1. WORK IN PHASES 3A AND 3B SHALL BE CONCURRENT AND SHALL REQUIRE THE CLOSURE OF BOTH RUNWAYS 16/34 AND 3/21.
2. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT AS TO WHICH DAYS OF THE WEEK ARE BEST FOR THE CLOSURE AND SHALL PROVIDE THEM 14 DAYS NOTIFICATION PRIOR TO SCHEDULING THE WORK.
3. THE CONTRACTOR MUST PROVIDE A YELLOW "X" AT EACH END OF THE RUNWAY IN ORDER TO CLOSE THAT RUNWAY (SEE CONSTRUCTION ACTIVITY PLAN 2). THIS PHASE SHALL REQUIRE A TOTAL OF 4 CLOSED RUNWAY MARKERS. TWO PORTABLE LIGHTED MARKERS SHALL BE PROVIDED BY THE AIRPORT AND TWO MARKER AS DETAILED ON CONSTRUCTION ACTIVITY PLAN 2 SHALL BE PROVIDED BY THE CONTRACTOR.
4. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE BOTH RUNWAYS FOR 2 CONSECUTIVE DAYS TO COMPLETE THE WORK IN PHASES 3A AND 3B. ALL WORK IN PHASES 3A AND 3B SHALL BE COMPLETED WITHIN THIS CLOSURE PERIOD. AT THE END OF THE SECOND CALENDAR DAY RUNWAY 3/21 SHALL BE INSPECTED BY THE AIRPORT AND RE-OPENED. SHOULD THE CONTRACTOR COMPLETE THE WORK PRIOR TO THE END OF THE SECOND CALENDAR DAY, THE RUNWAY SHALL BE RE-OPENED UPON IMMEDIATE INSPECTION BY THE AIRPORT.
5. WORK IN PHASE 3C MAY BE CONDUCTED AS PART OF PHASE 2 AND/OR 3B PROVIDED THAT IT DOES NOT RESULT IN LONGER RUNWAY 3/21 CLOSURE TIME.
6. WORK IN PHASES 3B AND 3C SHALL BE CONTINGENT UPON THE AWARD OF ADDITIVE ALTERNATES 1 AND 2.
7. THE CONTRACTOR SHALL PROVIDE A FLAGGER WHEN OPERATIONS REQUIRE CROSSING ACTIVE RUNWAY 3/21. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK IN SUCH A MANNER THAT RUNWAY CROSSINGS ARE LIMITED TO THE FEWEST POSSIBLE CROSSINGS EACH DAY.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
9. WORK SHALL NOT BE COMPLETED WITHIN 250' FROM THE CENTERLINE OF AN ACTIVE RUNWAY.
10. BARRICADES SHOWN ON THIS SHEET SHALL BE PLACED AT 250' FROM A RUNWAY CENTERLINE OR 100' FROM A TAXIWAY CENTERLINE.



**TYPICAL TAXIWAY CLOSURE MARKER DETAIL**

N.T.S.

**NOTES**

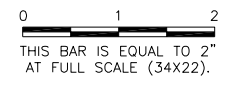
- 1.) THE MARKING CAN BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR CAN BE ANOTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND IS APPROVED BY THE AIRPORT
- 2.) PLACE OVER TAXIWAY CENTERLINE.
- 3.) YELLOW X SHALL BE ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH.
- 4.) TAXIWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND SHALL NOT BE MEASURED FOR PAYMENT.

FILE: CAP3.dwg  
UPDATE BY: TJ Heavisides  
PLOT DATE: 1/13/2014 10:48 AM

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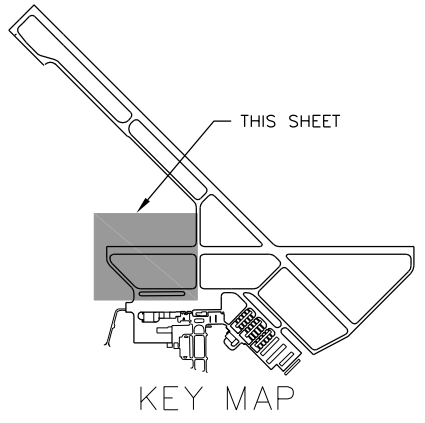
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DANVILLE, ILLINOIS

**REHABILITATE LIGHTS ON RUNWAY 16/34  
CONSTRUCTION ACTIVITY PLAN PHASE 3**

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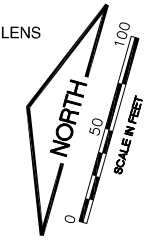
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PROJ. NO.	3-17-SBGP-XX
SHEET	07 OF 14 SHEETS

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- ### NOTES
- LIGHTS I-3, I-4, I-8, I-15 AND I-20 ON THIS SHEET ARE TO BE BASE MOUNTED RUNWAY LIGHTS.
  - ALL NEW LIGHTS AND SIGNS ARE TO BE INSTALLED IN THE SAME LOCATION AS THE EXISTING LIGHTS AND SIGNS WITH THE EXCEPTIONS OF LIGHTS I-3 AND I-19.
  - THESE LIGHTS SHALL BE REMOVED FROM THEIR EXISTING LOCATION AND THE PROPOSED LIGHTS I-3 AND I-19 SHALL BE INSTALLED IN THE NEW LOCATION SHOWN. LIGHTS I-3 AND I-19 SHALL BE 14" TALL DUE TO THEIR CLOSER PROXIMITY TO THE PAVEMENT. ALL OTHER LIGHTS SHALL BE 24" TALL AS SHOWN ON THE ELECTRICAL DETAILS.
  - ALL NEW LIGHTS SHALL BE INSTALLED 10' OFF THE EDGE OF PAVEMENT. LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE.
  - CABLE RUNNING FROM THE NEAREST RUNWAY LIGHT TO THE NEW HOLD SIGN SHALL HAVE COUNTERPOISE INSTALLED 4" ABOVE THE TOP OF THE RUNWAY CIRCUIT UNIT DUCT.
  - ALL BASE MOUNTED LIGHTS WITH CABLE RUNS TO A SIGN SHALL HAVE 3 HUBS.
  - INSTALL NEW CABLE TO EXISTING SIGN TO REMAIN IN PLACE.
  - ALL WORK SHOWN ON THIS SHEET SHALL BE PART OF THE BASE BID CONSTRUCTION.
  - ALL RUNWAY THRESHOLD LIGHTS SHALL HAVE HALF GREEN AND HALF RED LENSES. THE HALF RED LENSES SHALL BE FACING THE RUNWAY PAVEMENT.

- ### LEGEND
- RR [Symbol] EXISTING RUNWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW STAKE MOUNTED RUNWAY LIGHT
  - RR [Symbol] EXISTING RUNWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW BASE MOUNTED RUNWAY LIGHT
  - R [Symbol] REMOVE EXISTING RUNWAY LIGHT
  - RR [Symbol] EXISTING RUNWAY THRESHOLD LIGHT TO BE REMOVED AND REPLACED
  - RR [Symbol] EXISTING AIRFIELD SIGN TO BE REMOVED AND REPLACED
  - NEW RUNWAY 16/34 CIRCUIT (1/C #8 5KV UG CABLE IN U.D.)
  - NEW SIGN AND HOMERUN CABLE (2-1/C 5KV UG CABLE IN U.D.)
  - NEW COUNTERPOISE CABLE (1/C #6 COUNTERPOISE)
  - [Symbol] EXISTING ELECTRICAL DUCT
  - [Symbol] LIGHT NUMBER
  - [Symbol] SIGN NUMBER
  - [Symbol] NEW COUNTERPOISE GROUND ROD
  - W/Y HALF WHITE LENS/ HALF YELLOW LENS
  - W/W ALL WHITE LENS
  - G/R HALF GREEN LENS/ HALF RED LENS



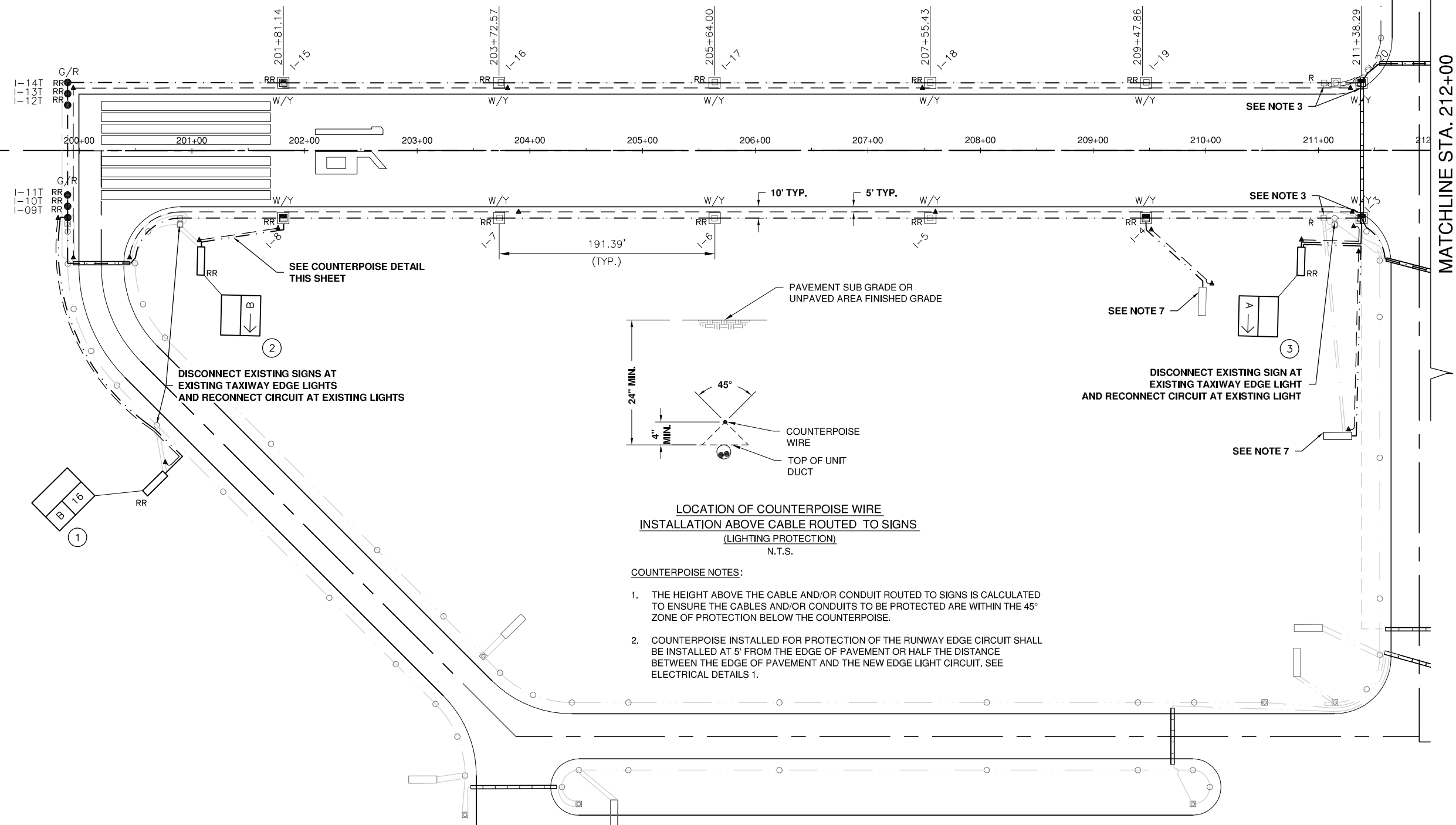
FILE: ELEC\_PLAN 01.dwg  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 1/13/2014 10:49 AM

BASE\_PROPELEC  
 KEYMAP  
 DNVBase\_StatePlane

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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



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REHABILITATE LIGHTS ON RUNWAY 16/34  
 ELECTRICAL SITE PLAN 1

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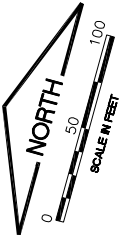
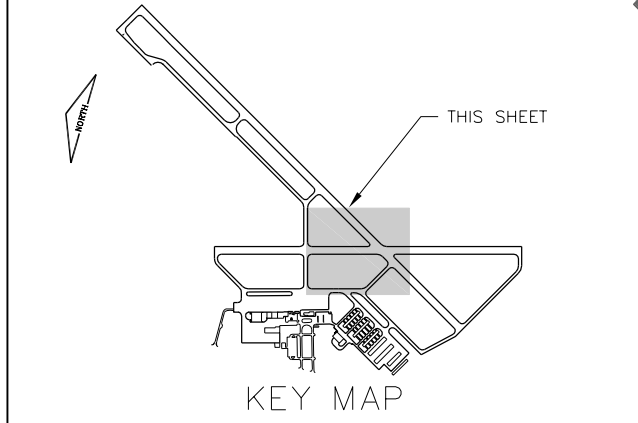
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PROJ. NO.	3-17-SBGP-XX

SHEET 08 OF 14 SHEETS

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**LEGEND**

- RR [Symbol] EXISTING RUNWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW STAKE MOUNTED RUNWAY LIGHT
- RR [Symbol] EXISTING RUNWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW BASE MOUNTED RUNWAY LIGHT
- RR [Symbol] EXISTING AIRFIELD SIGN TO BE REMOVED AND REPLACED
- NEW RUNWAY 16/34 CIRCUIT (1/C #8 5KV UG CABLE IN U.D.)
- NEW SIGN AND HOMERUN CABLE (2-1/C 5KV UG CABLE IN U.D.)
- NEW COUNTERPOISE CABLE (1/C #6 COUNTERPOISE)
- [Symbol] EXISTING ELECTRICAL DUCT
- [Symbol] 1-17 LIGHT NUMBER
- [Symbol] (1) SIGN NUMBER
- [Symbol] NEW COUNTERPOISE GROUND ROD
- [Symbol] NEW SPLICE CAN
- W/Y HALF WHITE LENS/ HALF YELLOW LENS
- W/W ALL WHITE LENS
- [Symbol] EXISTING SPLICE CAN

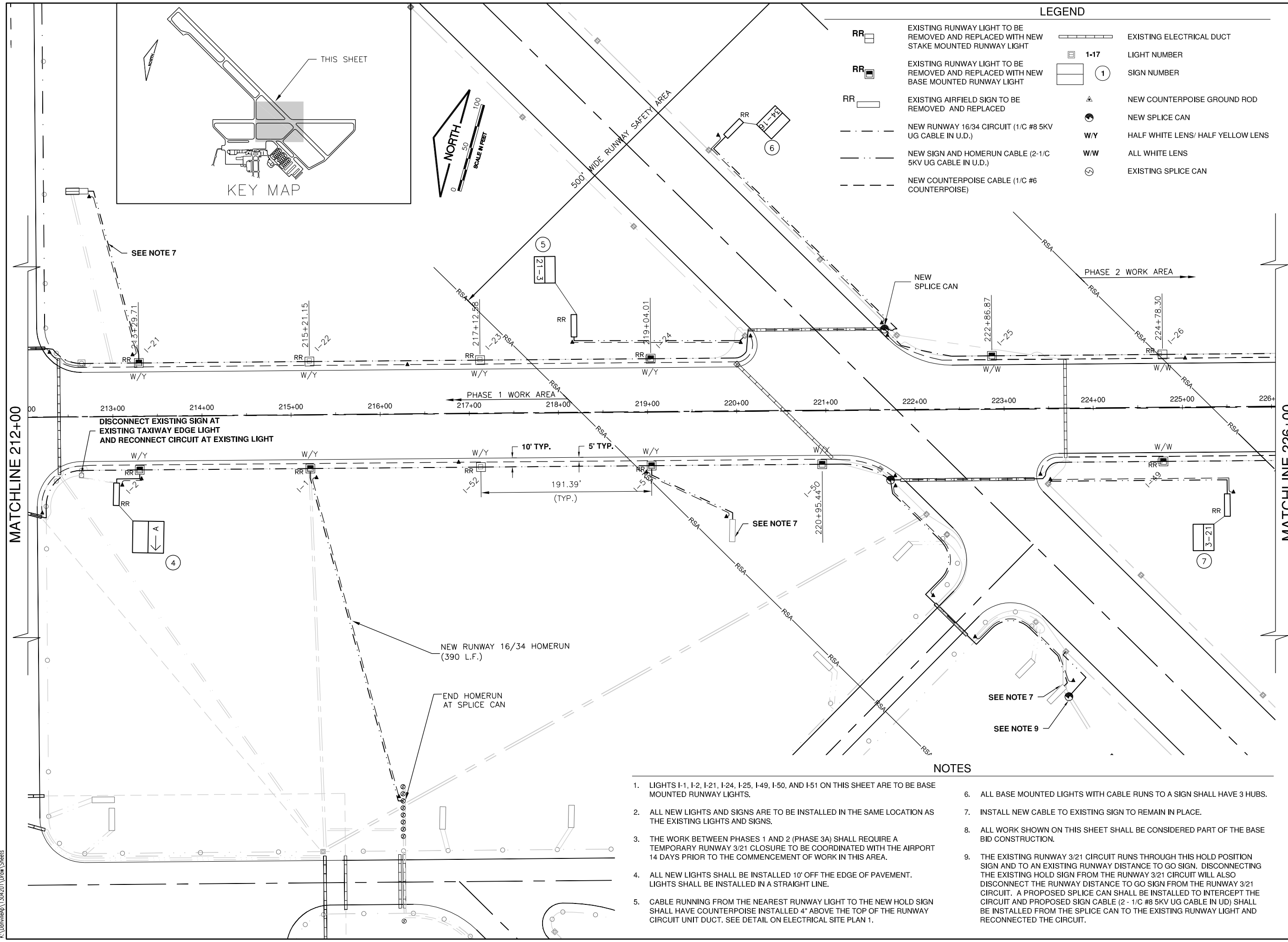
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 UPDATE BY: TJ Heavisides  
 PLOT DATE: 1/13/2014 10:49 AM

BASE\_PROPELEC  
 KEYMAP  
 DNVBase\_StatePlane

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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



**NOTES**

- LIGHTS I-1, I-2, I-21, I-24, I-25, I-49, I-50, AND I-51 ON THIS SHEET ARE TO BE BASE MOUNTED RUNWAY LIGHTS.
- ALL NEW LIGHTS AND SIGNS ARE TO BE INSTALLED IN THE SAME LOCATION AS THE EXISTING LIGHTS AND SIGNS.
- THE WORK BETWEEN PHASES 1 AND 2 (PHASE 3A) SHALL REQUIRE A TEMPORARY RUNWAY 3/21 CLOSURE TO BE COORDINATED WITH THE AIRPORT 14 DAYS PRIOR TO THE COMMENCEMENT OF WORK IN THIS AREA.
- ALL NEW LIGHTS SHALL BE INSTALLED 10' OFF THE EDGE OF PAVEMENT. LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE.
- CABLE RUNNING FROM THE NEAREST RUNWAY LIGHT TO THE NEW HOLD SIGN SHALL HAVE COUNTERPOISE INSTALLED 4" ABOVE THE TOP OF THE RUNWAY CIRCUIT UNIT DUCT. SEE DETAIL ON ELECTRICAL SITE PLAN 1.
- ALL BASE MOUNTED LIGHTS WITH CABLE RUNS TO A SIGN SHALL HAVE 3 HUBS.
- INSTALL NEW CABLE TO EXISTING SIGN TO REMAIN IN PLACE.
- ALL WORK SHOWN ON THIS SHEET SHALL BE CONSIDERED PART OF THE BASE BID CONSTRUCTION.
- THE EXISTING RUNWAY 3/21 CIRCUIT RUNS THROUGH THIS HOLD POSITION SIGN AND TO AN EXISTING RUNWAY DISTANCE TO GO SIGN. DISCONNECTING THE EXISTING HOLD SIGN FROM THE RUNWAY 3/21 CIRCUIT WILL ALSO DISCONNECT THE RUNWAY DISTANCE TO GO SIGN FROM THE RUNWAY 3/21 CIRCUIT. A PROPOSED SPLICE CAN SHALL BE INSTALLED TO INTERCEPT THE CIRCUIT AND PROPOSED SIGN CABLE (2 - 1/C #8 5KV UG CABLE IN UD) SHALL BE INSTALLED FROM THE SPLICE CAN TO THE EXISTING RUNWAY LIGHT AND RECONNECTED THE CIRCUIT.

VERMILION REGIONAL AIRPORT AUTHORITY  
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REHABILITATE LIGHTS ON RUNWAY 16/34  
 ELECTRICAL SITE PLAN 2

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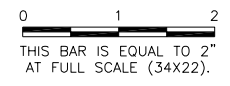
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IL PROJ. NO.	DNV-4234
PROJ. NO.	3-17-SBGP-XX
SHEET	09 OF 14 SHEETS

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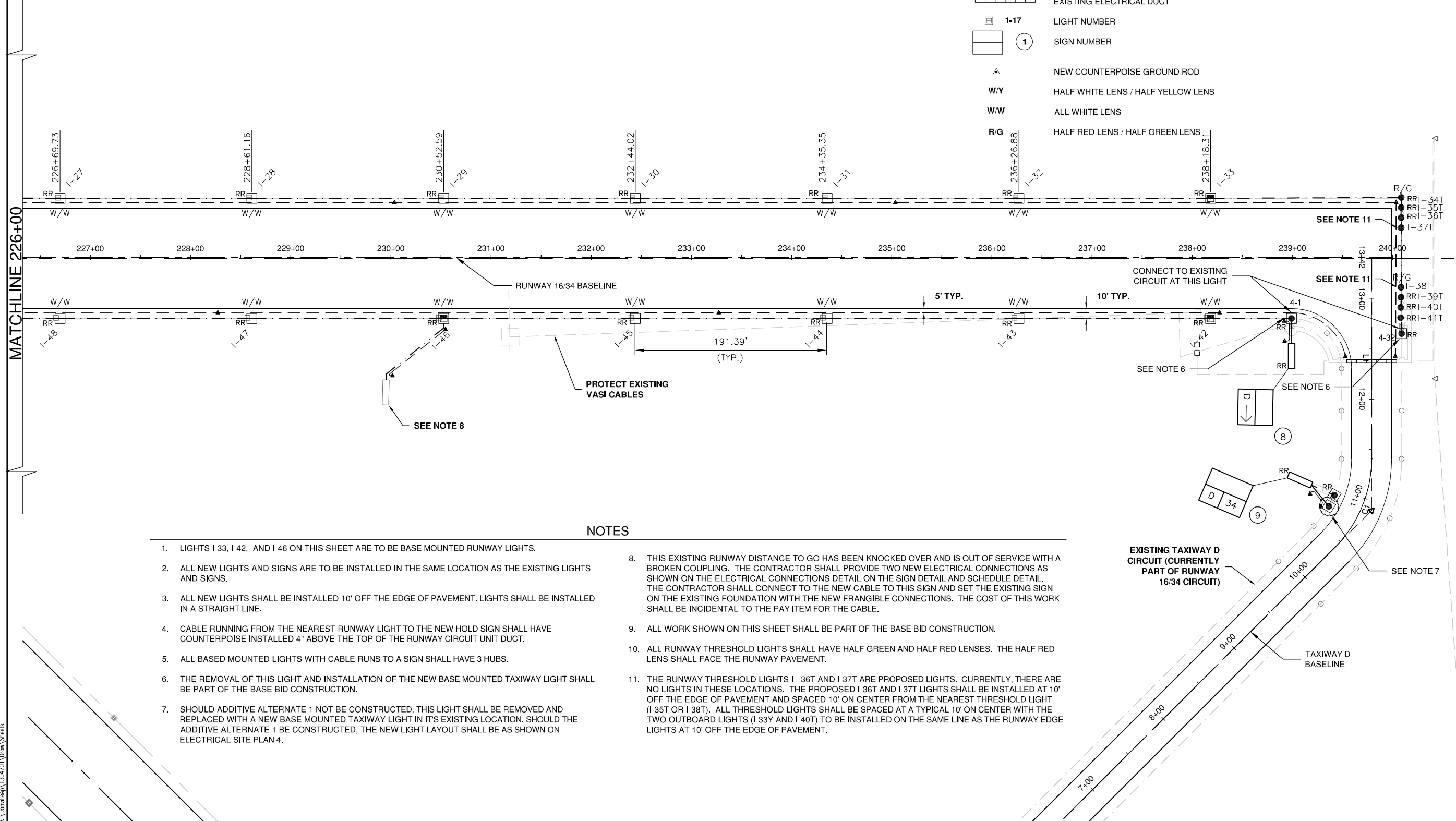
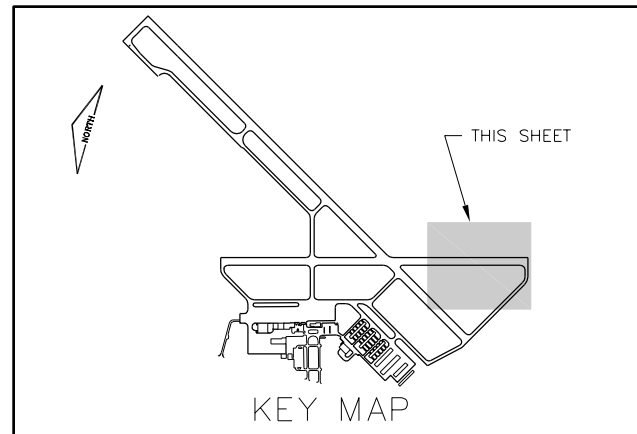
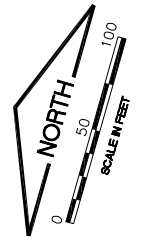
**VE049**

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NUMBER	BY	DATE



**LEGEND**

- RR EXISTING RUNWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW STAKE MOUNTED RUNWAY LIGHT
- RR EXISTING RUNWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW BASE MOUNTED RUNWAY LIGHT
- RR EXISTING RUNWAY THRESHOLD LIGHT TO BE REMOVED AND REPLACED
- RR EXISTING TAXIWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW BASE MOUNTED TAXIWAY LIGHT
- RR EXISTING AIRFIELD SIGN TO BE REMOVED AND REPLACED
- - - - NEW RUNWAY 16/34 CIRCUIT (1/C #8 5KV UG CABLE IN U.D.)
- - - - NEW SIGN AND HOMERUN CABLE (2-1/C 5KV UG CABLE IN U.D.)
- - - - NEW COUNTERPOISE CABLE (1/C #6 COUNTERPOISE)
- EXISTING ELECTRICAL DUCT
- LIGHT NUMBER
- SIGN NUMBER
- NEW COUNTERPOISE GROUND ROD
- W/Y HALF WHITE LENS / HALF YELLOW LENS
- W/W ALL WHITE LENS
- R/G HALF RED LENS / HALF GREEN LENS



**NOTES**

1. LIGHTS I-33, I-42, AND I-46 ON THIS SHEET ARE TO BE BASE MOUNTED RUNWAY LIGHTS.
2. ALL NEW LIGHTS AND SIGNS ARE TO BE INSTALLED IN THE SAME LOCATION AS THE EXISTING LIGHTS AND SIGNS.
3. ALL NEW LIGHTS SHALL BE INSTALLED 10' OFF THE EDGE OF PAVEMENT. LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE.
4. CABLE RUNNING FROM THE NEAREST RUNWAY LIGHT TO THE NEW HOLD SIGN SHALL HAVE COUNTERPOISE INSTALLED 4" ABOVE THE TOP OF THE RUNWAY CIRCUIT UNIT DUCT.
5. ALL BASED MOUNTED LIGHTS WITH CABLE RUNS TO A SIGN SHALL HAVE 3 HUBS.
6. THE REMOVAL OF THIS LIGHT AND INSTALLATION OF THE NEW BASE MOUNTED TAXIWAY LIGHT SHALL BE PART OF THE BASE BID CONSTRUCTION.
7. SHOULD ADDITIVE ALTERNATE 1 NOT BE CONSTRUCTED, THIS LIGHT SHALL BE REMOVED AND REPLACED WITH A NEW BASE MOUNTED TAXIWAY LIGHT IN ITS EXISTING LOCATION. SHOULD THE ADDITIVE ALTERNATE 1 BE CONSTRUCTED, THE NEW LIGHT LAYOUT SHALL BE AS SHOWN ON ELECTRICAL SITE PLAN 4.
8. THIS EXISTING RUNWAY DISTANCE TO GO HAS BEEN KNOCKED OVER AND IS OUT OF SERVICE WITH A BROKEN COUPLING. THE CONTRACTOR SHALL PROVIDE TWO NEW ELECTRICAL CONNECTIONS AS SHOWN ON THE ELECTRICAL CONNECTIONS DETAIL ON THE SIGN DETAIL AND SCHEDULE DETAIL. THE CONTRACTOR SHALL CONNECT TO THE NEW CABLE TO THIS SIGN AND SET THE EXISTING SIGN ON THE EXISTING FOUNDATION WITH THE NEW FRANGIBLE CONNECTIONS. THE COST OF THIS WORK SHALL BE INCIDENTAL TO THE PAY ITEM FOR THE CABLE.
9. ALL WORK SHOWN ON THIS SHEET SHALL BE PART OF THE BASE BID CONSTRUCTION.
10. ALL RUNWAY THRESHOLD LIGHTS SHALL HAVE HALF GREEN AND HALF RED LENSES. THE HALF RED LENS SHALL FACE THE RUNWAY PAVEMENT.
11. THE RUNWAY THRESHOLD LIGHTS I-36T AND I-37T ARE PROPOSED LIGHTS. CURRENTLY, THERE ARE NO LIGHTS IN THESE LOCATIONS. THE PROPOSED I-36T AND I-37T LIGHTS SHALL BE INSTALLED AT 10' OFF THE EDGE OF PAVEMENT AND SPACED 10' ON CENTER FROM THE NEAREST THRESHOLD LIGHT (I-35T OR I-38T). ALL THRESHOLD LIGHTS SHALL BE SPACED AT A TYPICAL 10' ON CENTER WITH THE TWO OUTBOARD LIGHTS (I-33Y AND I-40T) TO BE INSTALLED ON THE SAME LINE AS THE RUNWAY EDGE LIGHTS AT 10' OFF THE EDGE OF PAVEMENT.

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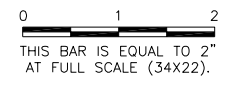
**REHABILITATE LIGHTS ON RUNWAY 16/34  
 ELECTRICAL SITE PLAN 3**

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DATE:	JANUARY 3, 2014
JOB No:	13042-01
IL PROJ. NO.	DNV-4234
PROJ. NO.	3-17-SBGP-XX
SHEET	10 OF 14 SHEETS

**VE049**

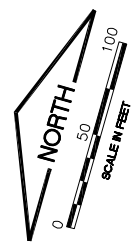
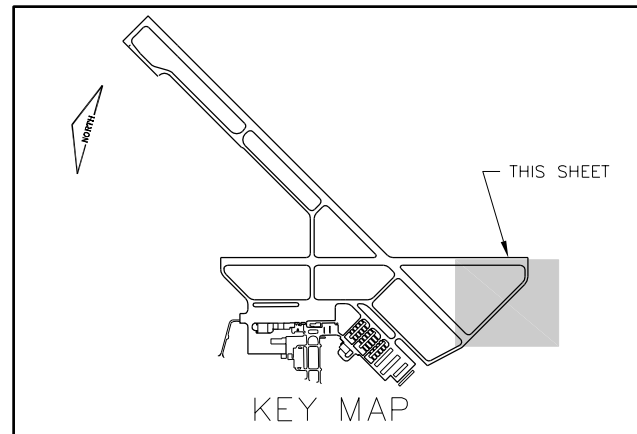
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**REHABILITATE LIGHTS ON RUNWAY 16/34**  
**ELECTRICAL SITE PLAN 4**

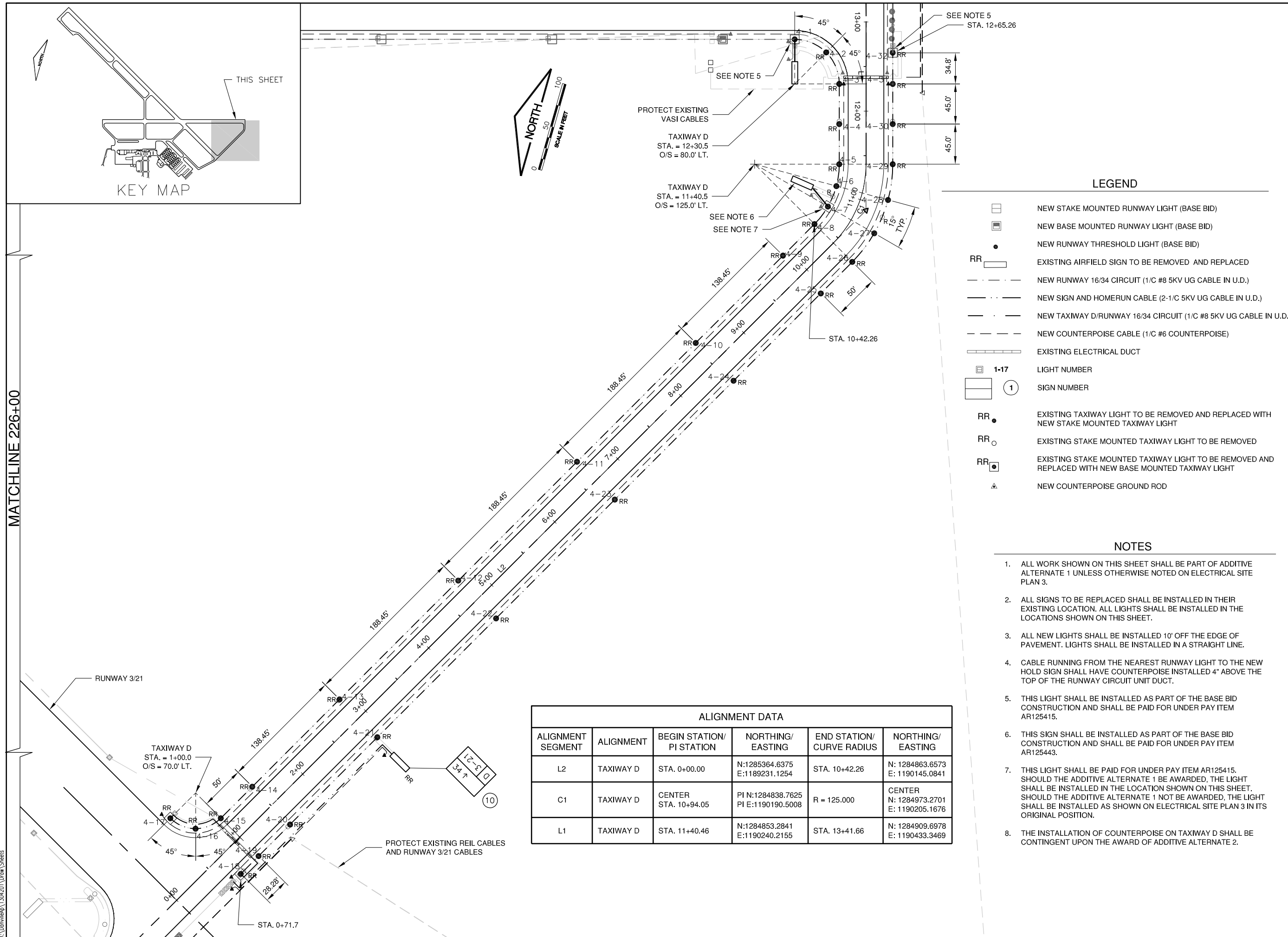
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LEGEND	
	NEW STAKE MOUNTED RUNWAY LIGHT (BASE BID)
	NEW BASE MOUNTED RUNWAY LIGHT (BASE BID)
	NEW RUNWAY THRESHOLD LIGHT (BASE BID)
	EXISTING AIRFIELD SIGN TO BE REMOVED AND REPLACED
	NEW RUNWAY 16/34 CIRCUIT (1/C #8 5KV UG CABLE IN U.D.)
	NEW SIGN AND HOMERUN CABLE (2-1/C 5KV UG CABLE IN U.D.)
	NEW TAXIWAY D/RUNWAY 16/34 CIRCUIT (1/C #8 5KV UG CABLE IN U.D.)
	NEW COUNTERPOISE CABLE (1/C #6 COUNTERPOISE)
	EXISTING ELECTRICAL DUCT
	1-17 LIGHT NUMBER
	(1) SIGN NUMBER
	RR ● EXISTING TAXIWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW STAKE MOUNTED TAXIWAY LIGHT
	RR ○ EXISTING STAKE MOUNTED TAXIWAY LIGHT TO BE REMOVED
	RR ◻ EXISTING STAKE MOUNTED TAXIWAY LIGHT TO BE REMOVED AND REPLACED WITH NEW BASE MOUNTED TAXIWAY LIGHT
	▲ NEW COUNTERPOISE GROUND ROD

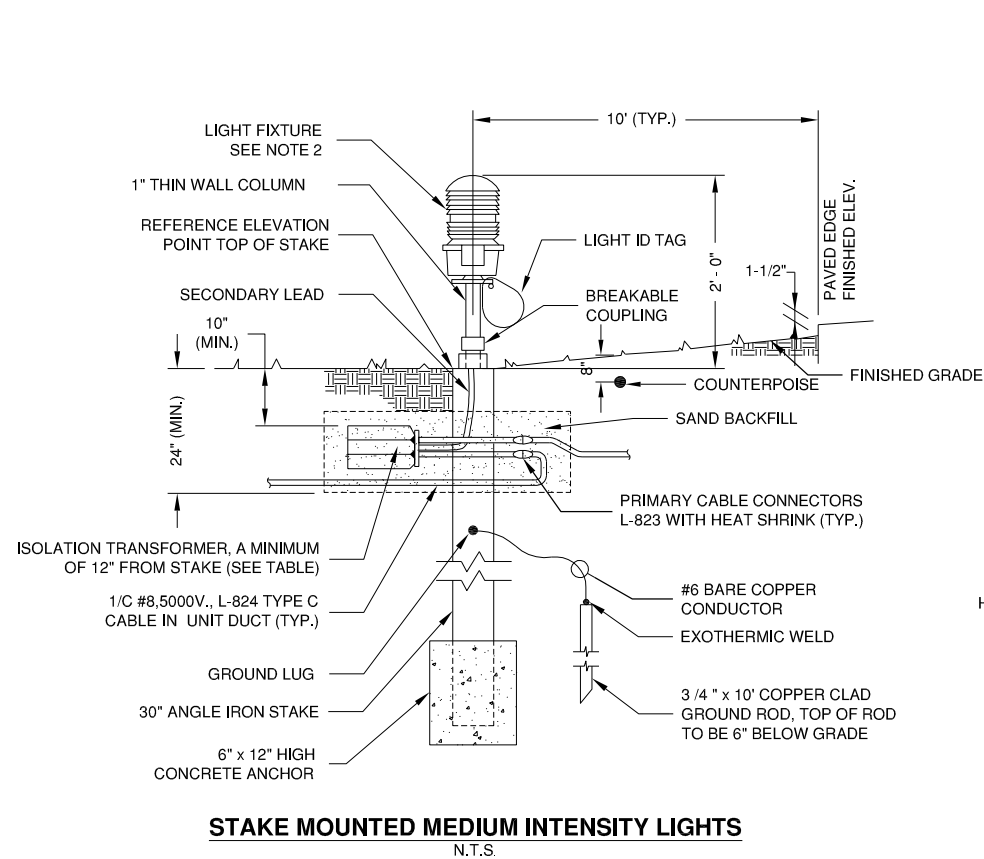
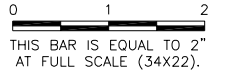
**NOTES**

- ALL WORK SHOWN ON THIS SHEET SHALL BE PART OF ADDITIVE ALTERNATE 1 UNLESS OTHERWISE NOTED ON ELECTRICAL SITE PLAN 3.
- ALL SIGNS TO BE REPLACED SHALL BE INSTALLED IN THEIR EXISTING LOCATION. ALL LIGHTS SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THIS SHEET.
- ALL NEW LIGHTS SHALL BE INSTALLED 10' OFF THE EDGE OF PAVEMENT. LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE.
- CABLE RUNNING FROM THE NEAREST RUNWAY LIGHT TO THE NEW HOLD SIGN SHALL HAVE COUNTERPOISE INSTALLED 4" ABOVE THE TOP OF THE RUNWAY CIRCUIT UNIT DUCT.
- THIS LIGHT SHALL BE INSTALLED AS PART OF THE BASE BID CONSTRUCTION AND SHALL BE PAID FOR UNDER PAY ITEM AR125415.
- THIS SIGN SHALL BE INSTALLED AS PART OF THE BASE BID CONSTRUCTION AND SHALL BE PAID FOR UNDER PAY ITEM AR125443.
- THIS LIGHT SHALL BE PAID FOR UNDER PAY ITEM AR125415. SHOULD THE ADDITIVE ALTERNATE 1 BE AWARDED, THE LIGHT SHALL BE INSTALLED IN THE LOCATION SHOWN ON THIS SHEET. SHOULD THE ADDITIVE ALTERNATE 1 NOT BE AWARDED, THE LIGHT SHALL BE INSTALLED AS SHOWN ON ELECTRICAL SITE PLAN 3 IN ITS ORIGINAL POSITION.
- THE INSTALLATION OF COUNTERPOISE ON TAXIWAY D SHALL BE CONTINGENT UPON THE AWARD OF ADDITIVE ALTERNATE 2.

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L2	TAXIWAY D	STA. 0+00.00	N:1285364.6375 E:1189231.1254	STA. 10+42.26	N: 1284863.6573 E: 1190145.0841
C1	TAXIWAY D	CENTER STA. 10+94.05	PI N:1284838.7625 PI E:1190190.5008	R = 125.000	CENTER N: 1284973.2701 E: 1190205.1676
L1	TAXIWAY D	STA. 11+40.46	N:1284853.2841 E:1190240.2155	STA. 13+41.66	N: 1284909.6978 E: 1190433.3469

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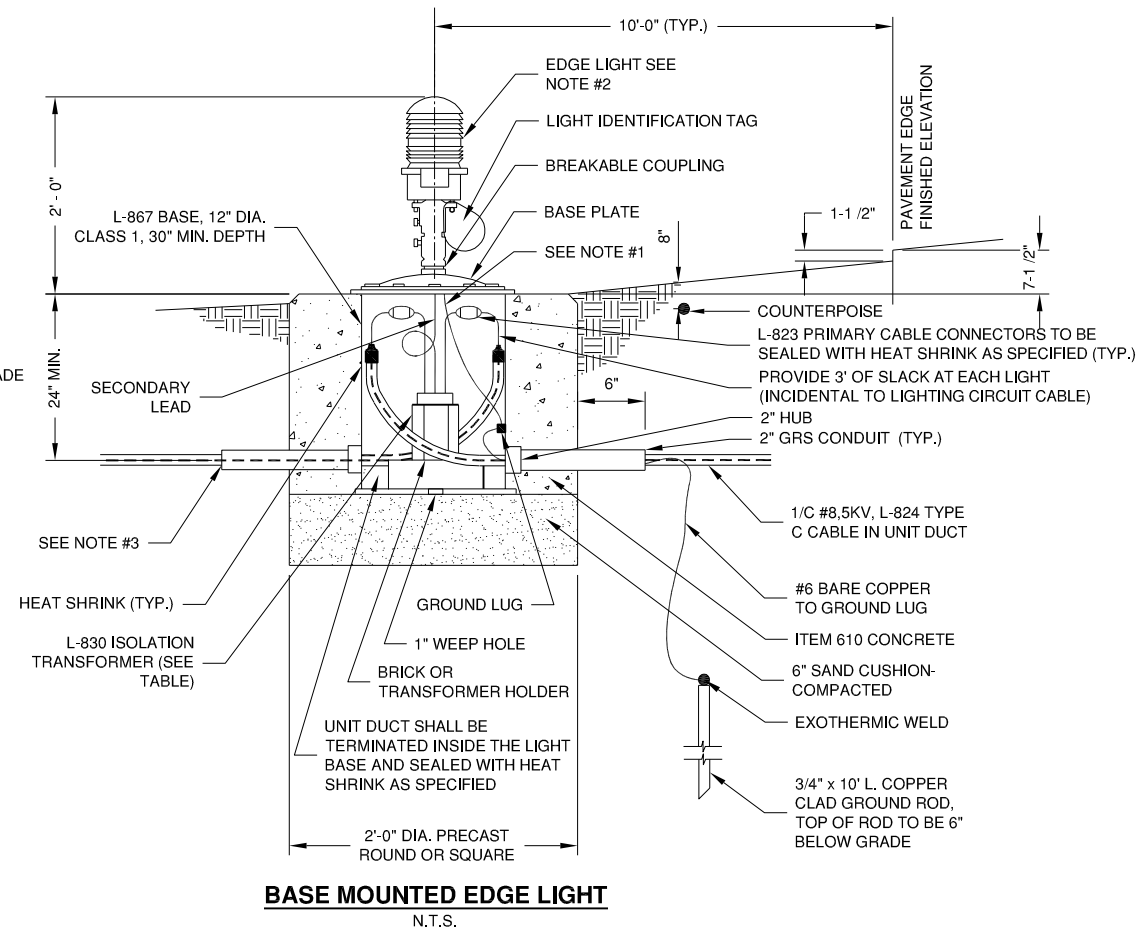
**STAKE MOUNTED MEDIUM INTENSITY LIGHTS**  
N.T.S.

**NOTES**

- CABLES SHALL NOT BE LESS THAN 24" DEEP. ENCASE ISOLATION TRANSFORMER, L-823 CONNECTORS, AND 2 FOOT MINIMUM OF L-824 CABLE SLACK IN SAND BACKFILL.
- LIGHT FIXTURES SHALL BE AS INDICATED IN THE TABLE ON THIS SHEET AND SHALL BE CONTINGENT UPON THE AWARD OF EACH ADDITIVE ALTERNATE.
- UNIT DUCT SHALL BE TERMINATED AT L-823 CONNECTORS AND SEALED TO MAKE WATERTIGHT.
- DO NOT INSTALL ANGLE IRON STAKE BY DRIVING. MAKE ELECTRICAL CONNECTIONS AND BACKFILL AROUND THE STAKE WITH EARTH PASSING THE 1-INCH SIEVE. COMPACT AS REQUIRED TO PROVIDE FIRM SUPPORT FOR STAKE, AND TO THE SATISFACTION OF THE RESIDENT ENGINEER.
- COUNTERPOISE SHALL BE INSTALLED HALF WAY BETWEEN THE NEW EDGE LIGHT CIRCUIT AND THE EDGE OF THE RUNWAY/TAXIWAY PAVEMENT.
- SEE VEGETATION CONTROL RING DETAIL ON ELECTRICAL DETAILS SHEET 2 TO BE PLACED AROUND EACH LIGHT.

LIGHT DESCRIPTION	FAA LIGHT FIXTURE TYPE	LAMP DESCRIPTION	TRANSFORMER TYPE	WATTAGE	TRANSFORMER PRI/SEC	CONTRACT
MEDIUM INTENSITY RUNWAY LIGHT	L-861	INCAN. 6.6A	L-830-1	30/45	6.6A/6.6A	BASE BID
MEDIUM INTENSITY RUNWAY LIGHT (LED)	L-861(L)	LED W/HEATER	L-830-1	30/45	6.6A/6.6A	ADD. ALT. 3
MEDIUM INTENSITY TAXIWAY LIGHT	L-861T	INCAN. 6.6A	L-830-1	30/45	6.6A/6.6A	ADD. ALT. 1
MEDIUM INTENSITY TAXIWAY LIGHT (LED)	L-861T(L)	LED W/HEATER	L-830-1	30/45*	6.6A/6.6A	ADD. ALT. 3

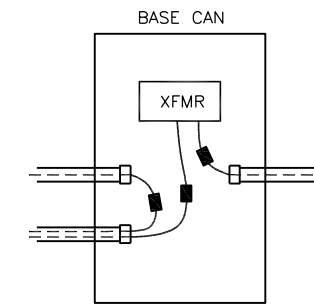
\*NOTE: WATTAGE MAY BE 20/25 AS RECOMMENDED BY THE MANUFACTURER.



**BASE MOUNTED EDGE LIGHT**  
N.T.S.

**NOTES**

- THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
- LIGHT FIXTURES SHALL BE AS INDICATED IN THE TABLE ON THIS SHEET AND SHALL BE CONTINGENT UPON THE AWARD OF EACH ADDITIVE ALTERNATE.
- UNIT DUCT SHALL BE TERMINATED AT L-823 CONNECTORS AND SEALED TO MAKE WATERTIGHT.
- COUNTERPOISE SHALL BE INSTALLED HALF WAY BETWEEN THE NEW EDGE LIGHT CIRCUIT AND THE EDGE OF THE RUNWAY/TAXIWAY PAVEMENT.
- SEE VEGETATION CONTROL RING DETAIL ON ELECTRICAL DETAILS SHEET 2 TO BE PLACED AROUND EACH LIGHT.

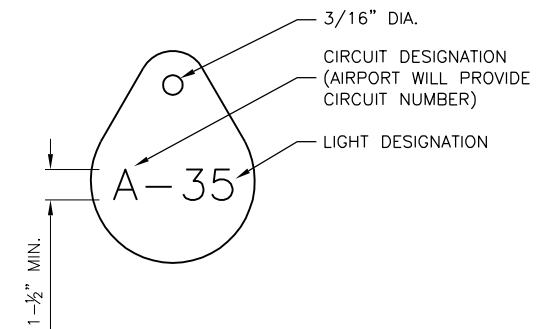


**NOTES:**

- CONTRACTOR SHALL CONNECT HOMERUN CABLES TO XFMR & SINGLE CONNECTOR IN UNIT DUCT W/ L-823 CONNECTORS IN PROPOSED BASE MOUNTED LIGHT.
- CONTRACTOR SHALL PROVIDE ADDITIONAL HUB ON EDGE LIGHT CAN.

**SCHEMATIC**

N.T.S.



**LIGHT IDENTIFICATION DETAIL**

N.T.S.

**NOTES**

INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.

LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.

THE CONTRACTOR SHALL NUMBER THE EXISTING/PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.

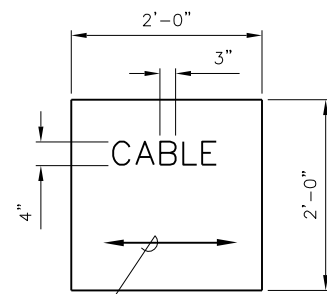
AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.

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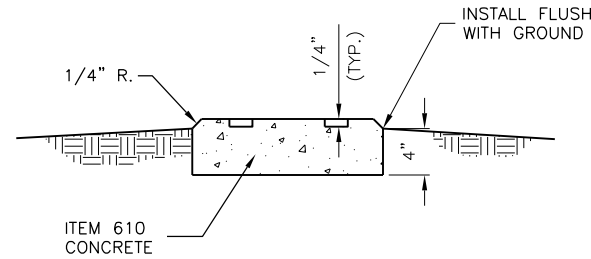
REHABILITATE LIGHTS ON RUNWAY 16/34  
 ELECTRICAL DETAILS 1

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PROJ. NO.	3-17-SBGP-XX
SHEET	12 OF 14 SHEETS



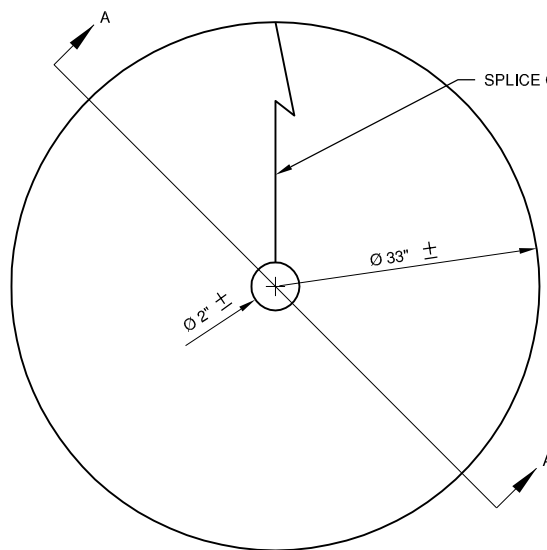
ARROW TO INDICATE THE DIRECTION OF THE CABLE RUN



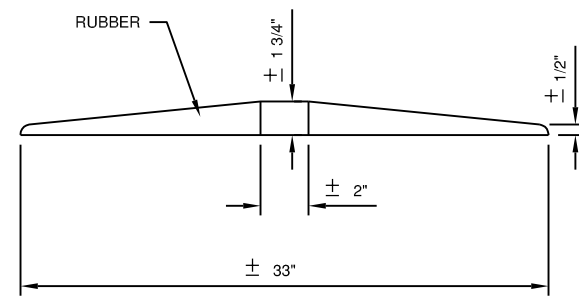
**TURF CABLE / SPLICE MARKER**  
N.T.S.

**NOTES**

1. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
2. ITEM 610 CONCRETE SHALL BE USED.
3. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
4. THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
5. 0.049 CU. YD. CONCRETE PER MARKER.
6. A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.

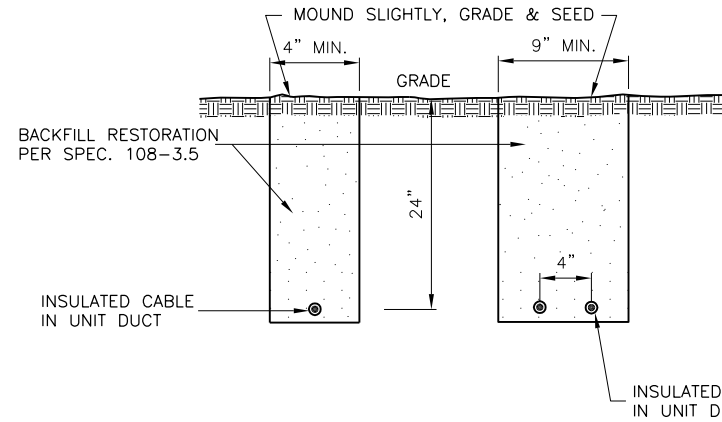


**PLAN VIEW**  
N.T.S.



**SECTION A-A**  
N.T.S.

**VEGETATION CONTROL RING DETAIL**  
N.T.S.

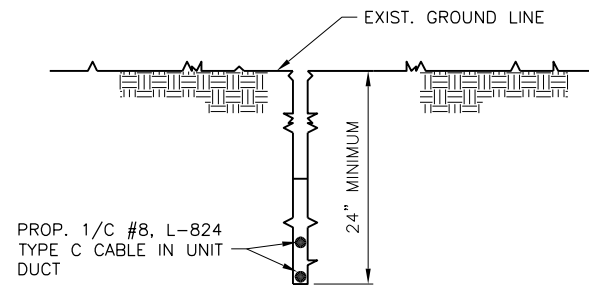


**TRENCH DETAIL**  
N.T.S.

**NOTES**

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.

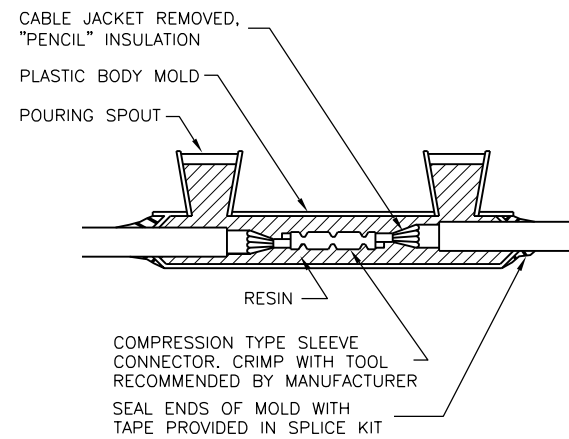
NOTE: AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN LIEU OF TRENCHING



PROP. 1/C #8, L-824  
TYPE C CABLE IN UNIT  
DUCT

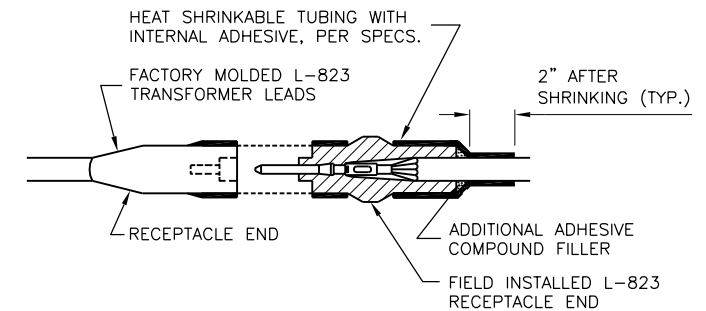
NOTE: WHERE TWO UNIT DUCTS ARE TO BE INSTALLED, CONTRACTOR MAY PLOW UNIT DUCTS SEPERATELY (SIDE BY SIDE)

**CABLE IN UNIT-DUCT - PLOWED**  
N.T.S.



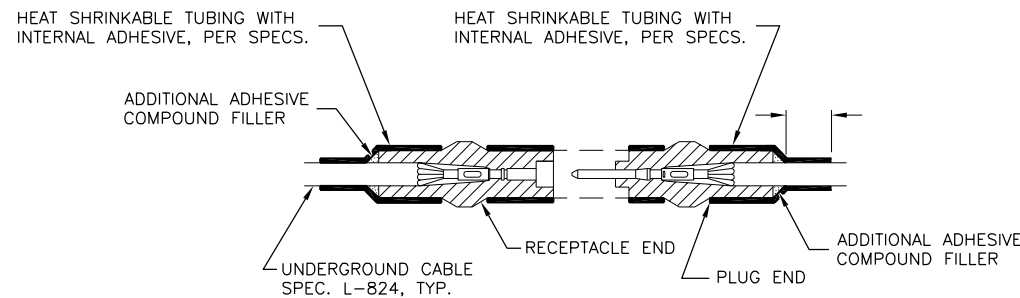
FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTIONS.

**TYPE A**



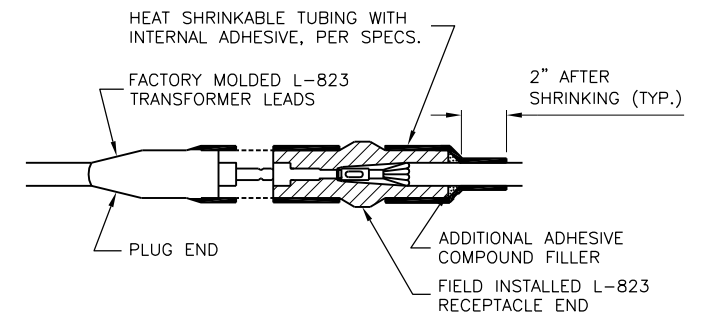
FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

**TYPE C**



NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER

**TYPE B**



FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

**TYPE D**

**CABLE SPLICES**

N.T.S.

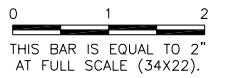
**NOTES**

1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
2. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
3. THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

**VE049**

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REHABILITATE LIGHTS ON RUNWAY 16/34  
 ELECTRICAL DETAILS 2

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### SIGNAGE SCHEDULE

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
1	NE SW	B 16	16		B	3	RWY 16/34	NEW SIGN
2	N S	← B		← B		2	RWY 16/34	NEW SIGN
3	N S	A →		A →		2	RWY 16/34	NEW SIGN
4	N S	← A		← A		2	RWY 16/34	NEW SIGN
5	N S	21-3	21-3			4	RWY 3/21	NEW SIGN
6	NE SW	34-16	34-16			5	RWY 16/34	NEW SIGN
7	N S	3-21	3-21			4	RWY 3/21	NEW SIGN
8	N S	D →		D →		2	RWY 16/34	NEW SIGN
9	E W	D 34	34		D	3	RWY 16/34	NEW SIGN
10	NW SE	34 ↑ D 3-21	3-21	34 ↑	D	5	RWY 3/21	NEW SIGN (ADD. ALT. 1)

#### AIRFIELD SIGN NOTES

1. TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
3. SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2.
4. SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.
5. RUNWAY DISTANCE REMAINING SIGNS SHALL BE SIZE "X" SIGNS.

#### NOTES

1. SIGN SCHEDULE MAY BE SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
2. CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO 1 ABOVE.
3. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.
4. EXISTING SIGNS ARE LUMACURVE.
5. RUNWAY 3/21 CIRCUIT IS A STYLE 3 CIRCUIT. ALL OTHER SIGNS NOT INSTALLED ON THE RUNWAY 3/21 CIRCUIT SHALL BE STYLE 2.

#### VE049

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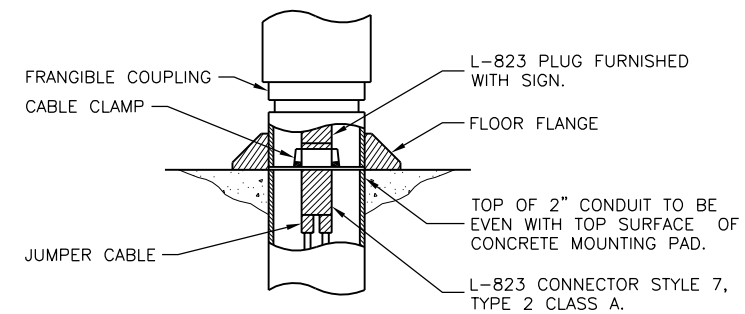
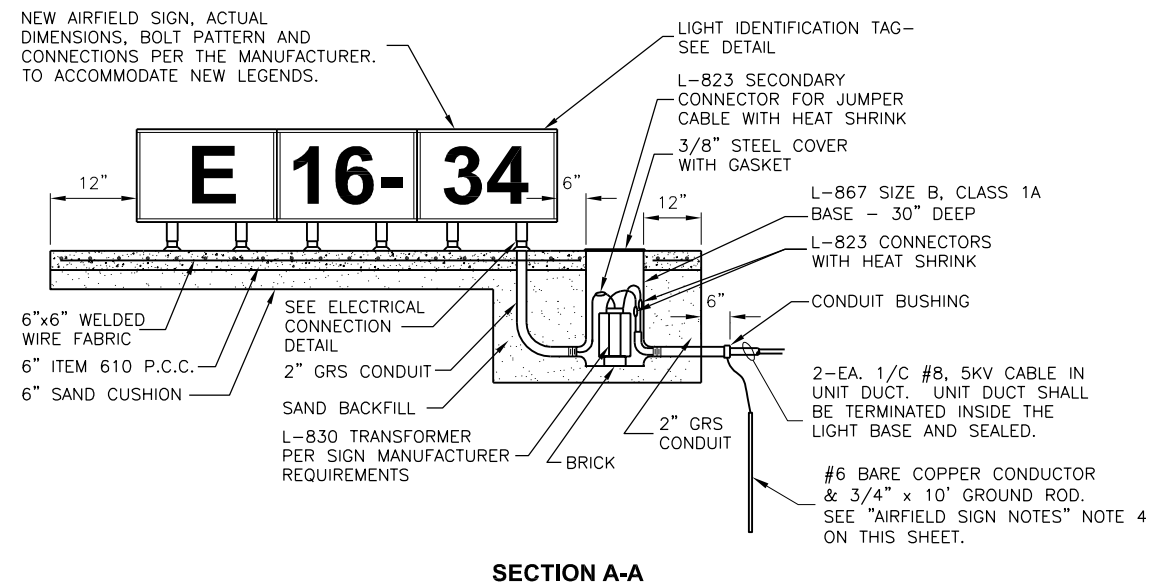
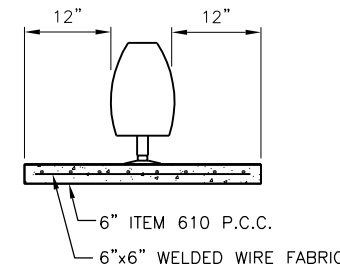
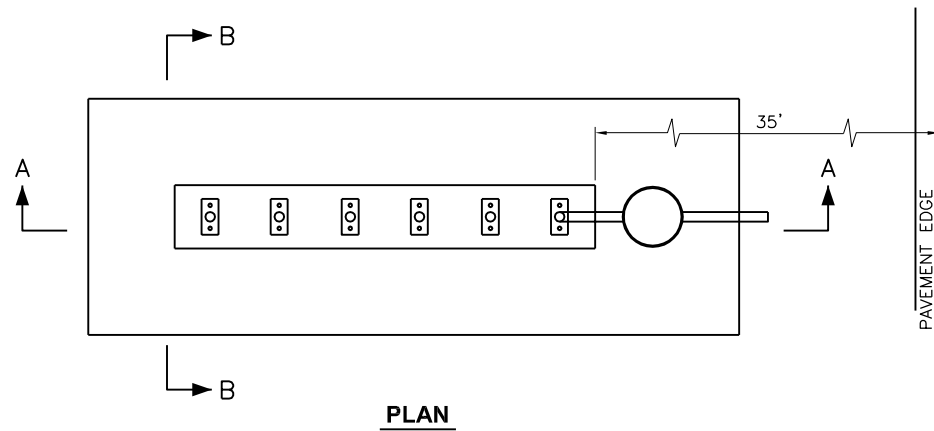
0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

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REHABILITATE LIGHTS ON RUNWAY 16/34  
 SIGN DETAIL AND SCHEDULE

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#### L-858 AIRFIELD SIGN DETAILS

N.T.S.