# ITEM 04A 03-03-2017 LETTING

AR152410 UNCLASSIFIED EXCAVATION
AR152480 SHOULDER ADJUSTMENT

DETECTOR LOOP

BITUMINOUS CRACK REPAIR

SOIL STABILIZATION FABRIC

CRACK CONTROL FABRIC

12" AGGREGATE BASE COURSE

CRUSHED AGG. BASE COURSE - 6"

CLEAN & SEAL BITUMINOUS CRACKS

AR152540

AR162570

AR201660

AR201661

AR201670

AR208612

AR209606

AR501506 AR501604 AR501690

AR501900

AR705546 AR705620

AR705645

AR751001 AR751410 SUMMARY OF QUANTITIES

UNIT QUANTITY

2 500

2 700

1.150

500

850

1,725

150

360

250

1.800

160

460

0.2

1,000

SY

SY

EACH

EACH

LF

EACH AC

SY

## TA006 TOTAL SHEETS: 22

# **CONSTRUCTION PLANS FOR**

# TAYLORVILLE MUNICIPAL AIRPORT

CITY OF TAYLORVILLE TAYLORVILLE, ILLINOIS

ILLINOIS PROJECT TAZ - 4342 S.B.G. PROJECT 3-17-SBGP-105

BITUMINOUS OVERLAY/RECONSTRUCTION OF NORTH AND SOUTHWEST APRON: INCLUDING CLEAN AND SEAL, CRACK REPAIR, MARKING, TIEDOWN ADJUSTMENT, FULL DEPTH RECONSTRUCTION OF AREAS, INSTALL TRENCH DRAINS

**JANUARY 6, 2017** 

		Sheet List Table
Sheet No.	Sheet Index	Sheet List Table Sheet Title
1	GC000	COVER SHEET
2	GC001	SITE PLAN
3	GC002	CONSTRUCTION SAFETY PHASING PLAN
4	GC003	CONSTRUCTION ACTIVITY PLAN 1
5	GC004	CONSTRUCTION ACTIVITY PLAN 2
6	GC005	CONSTRUCTION SAFETY PHASING PLAN NOTES
7	CD101	EXISTING CONDITIONS AND REMOVALS
8	CD501	EXISTING TYPICAL SECTIONS
9	CP100	PROPOSED TYPICAL SECTIONS
10	CP101	PROPOSED IMPROVEMENTS 1
11	CP102	PROPOSED IMPROVEMENTS 2
12	CP103	PROPOSED IMPROVEMENTS 3
13	CP501	PAVEMENT REPAIR DETAILS
14	CU501	DRAINAGE DETAILS
15	CU502	UNDERDRAIN DETAILS
16	CS101	PROPOSED STAKING PLAN 1
17	CS102	PROPOSED STAKING PLAN 2
18	CM101	PAVEMENT MARKING & TURFING PLAN 1
19	CM102	PAVEMENT MARKING & TURFING PLAN 2
20	CM501	PAVEMENT MARKING AND TIE DOWN DETAILS
21	CJ501	JOINTING DETAILS
22	El501	DETECTOR LOOP DETAILS

Know what's below. Call before you dig.

J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

PAVEMENT MARKING-WATERBORNE

PAVEMENT MARKING-BLACK BORDER
6" NON PERFORATED UNDERDRAIN

UNDERDRAIN END SECTION

UNDERDRAIN CONNECTION
TRENCH DRAIN

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPOSIBILITY TO ETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTORS TO UNDERGROUND UTILITIES. PIDIO TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS OBTAIN FROM RESPECTIVE UTILITY COMPANIES OF HIS OPERATIONAL PLANS OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL MIMEDIATELY OF SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND ADJUSTMENT OF SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND ADJUSTMENT OF ALL POLITIONS.

CALL J.U.L.I.E FOR UTILITY INFORMATION AT 811.

#### DESIGN INFORMATION

GEOMETRIC CRITERIA

AIRPLANE DESIGN GROUP II TAXIWAY DESIGN GROUP II PAVEMENT DESIGN CRITERIA

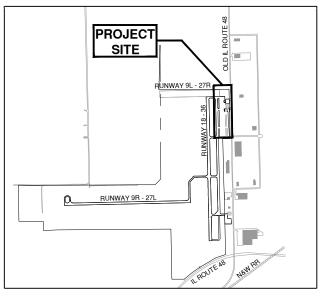
AIRCRAFT SINGLE WHEEL GEAR DEPARTURE WEIGHT = 12,500 LBS. 100 ANNUAL DEPARTURES

> CALL J.U.L.I.E. BEFORE EXCAVATING 1-800-892-0123

TOWNSHIP: 13 NORTH RANGE: 2 WEST OF THE 4TH P.M. SECTION: 32 COUNTY: CHRISTIAN CIVIL TOWNSHIP: TAYLORVILLE

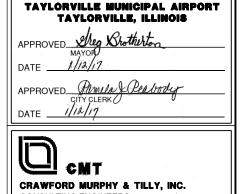


**LOCATION MAP** 

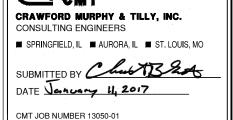


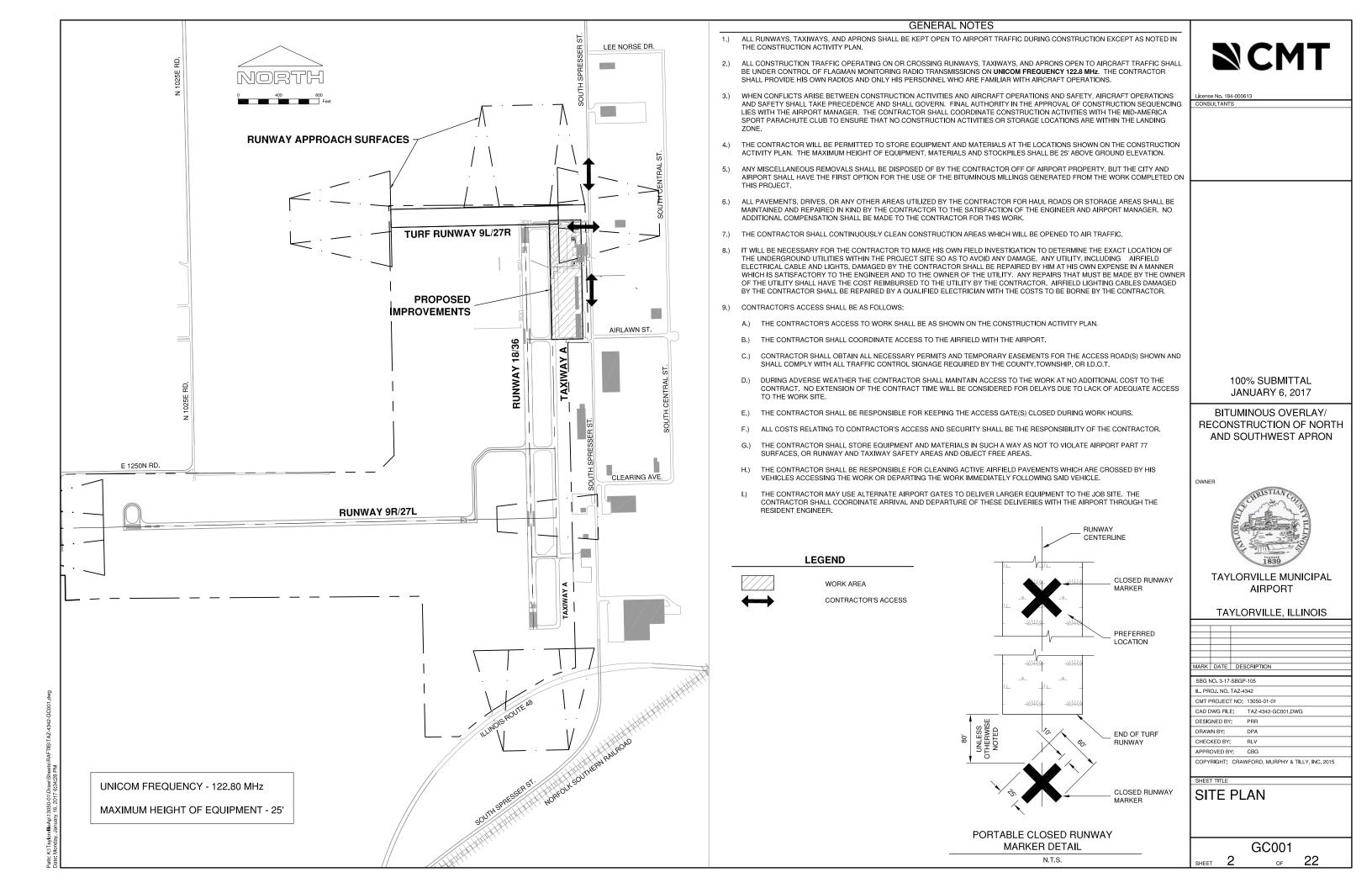
SITE PLAN

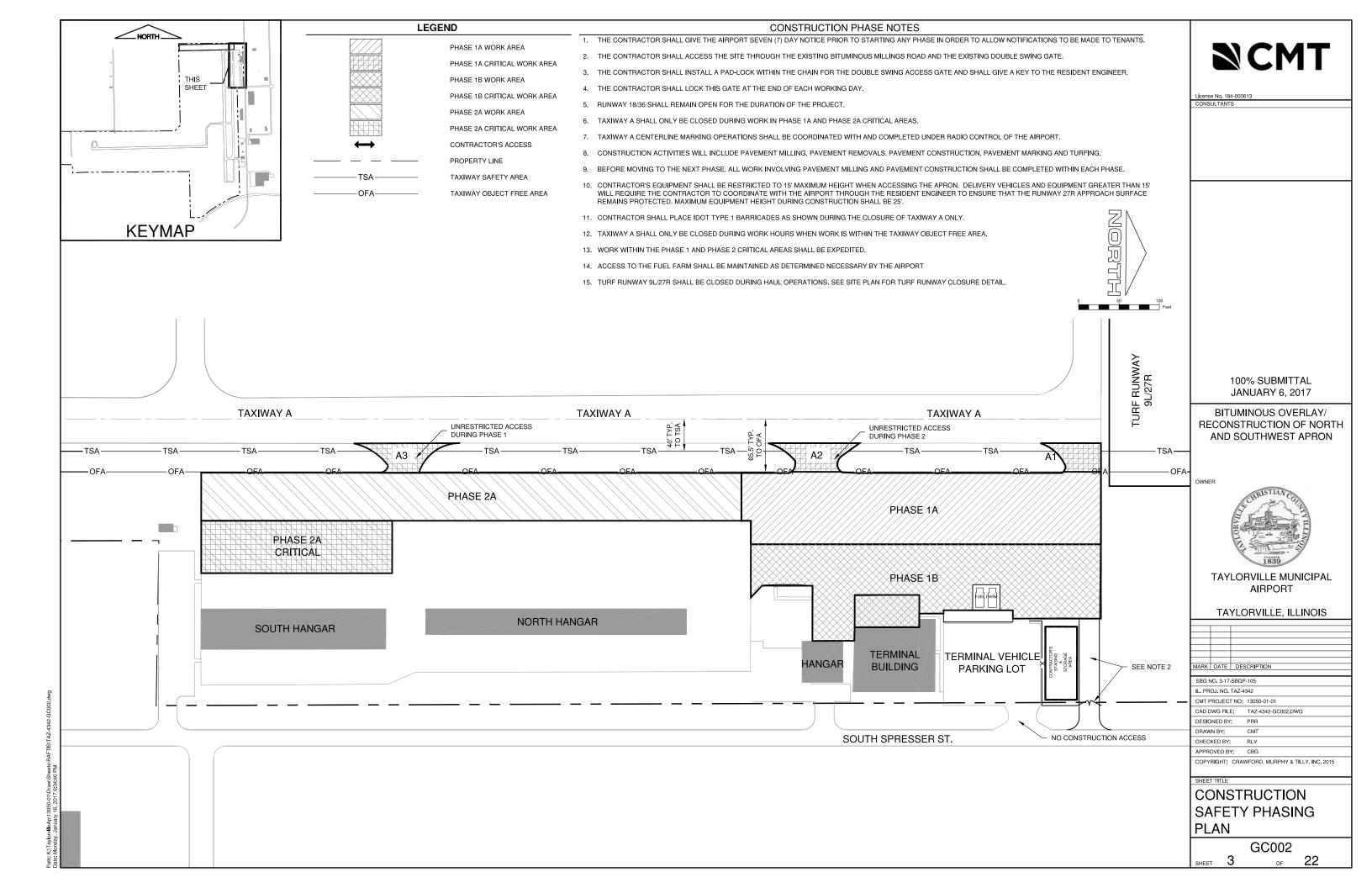


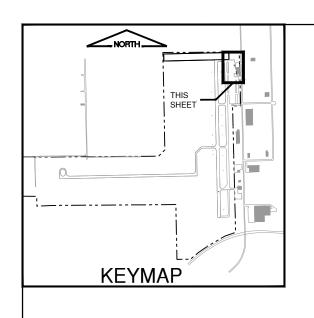


CITY OF TAYLORVILLE









**POINT** 

Α

**IDOT TYPE 1** 

NTS

LATITUDE

N39° 32' 11.84"

N39° 32' 12.17"

**LEGEND** 

DESCRIPTION

WORK LIMITS

WORK LIMITS

PHASE 1A WORK AREA

PHASE 1B WORK AREA

CONTRACTOR'S ACCESS

PROPERTY LINE

- PHASE 1A/1B

PHASE 1A CRITICAL WORK AREA

PHASE 1B CRITICAL WORK AREA

CONSTRUCTION SET BACK STAKES

LOW PROFILE BEAM BARRICADES

IDOT TYPE 1 W/ ORANGE FLAG AND

RED OMNI-DIRECTIONAL LIGHT

TAXIWAY OBJECT FREE AREA

TAXIWAY SAFETY AREA

1. THE CONTRACTOR SHALL GIVE THE AIRPORT SEVEN (7) DAY NOTICE PRIOR TO STARTING ANY PHASE IN ORDER TO ALLOW NOTIFICATIONS TO BE MADE TO TENANTS

**CONSTRUCTION PHASE NOTES** 

2. THE CONTRACTOR SHALL ACCESS THE SITE THROUGH THE EXISTING BITUMINOUS MILLINGS ROAD AND THE EXISTING DOUBLE SWING GATE

3. THE CONTRACTOR SHALL INSTALL A PAD-LOCK WITHIN THE CHAIN FOR THE DOUBLE SWING ACCESS GATE AND SHALL GIVE A KEY TO THE RESIDENT ENGINEER.

4. THE CONTRACTOR SHALL LOCK THIS GATE AT THE END OF EACH WORKING DAY

5. RUNWAY 18/36 SHALL REMAIN OPEN FOR THE DURATION OF THE PROJECT

6. TAXIWAY A SHALL ONLY BE CLOSED DURING WORK IN PHASE 1A CRITICAL AREAS. WORK WITHIN PHASE 1A CRITICAL SHALL BE LESS THAN 3 CONSECUTIVE CALENDAR DAYS.

7. CONSTRUCTION ACTIVITIES WILL INCLUDE PAVEMENT MILLING, PAVEMENT REMOVALS, PAVEMENT CONSTRUCTION, PAVEMENT MARKING AND TURFING.

BEFORE MOVING TO THE NEXT PHASE, ALL WORK INVOLVING PAVEMENT MILLING AND PAVEMENT CONSTRUCTION SHALL BE COMPLETED WITHIN EACH PHASE.

CONTRACTOR'S EQUIPMENT SHALL BE RESTRICTED TO 15' MAXIMUM HEIGHT WHEN ACCESSING THE APRON. DELIVERY VEHICLES AND EQUIPMENT GREATER THAN 15' WILL REQUIRE THE CONTRACTOR TO COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER TO ENSURE THAT THE RUNWAY 27R APPROACH SURFACE REMAINS PROTECTED. MAXIMUM EQUIPMENT HEIGHT DURING CONSTRUCTION SHALL BE 25'.

10. CONTRACTOR SHALL PLACE HIGH PROFILE/FLASHER BARRICADES AS SHOWN DURING THE CLOSURE OF TAXIWAY A ONLY

11. TAXIWAY A SHALL ONLY BE CLOSED DURING WORK HOURS WHEN WORK IS WITHIN THE TAXIWAY OBJECT FREE AREA

12. WORK WITHIN THE PHASE 1 AND PHASE 2 CRITICAL AREAS SHALL BE EXPEDITED

RUNWAY L/27R 13. ACCESS TO THE FUEL FARM SHALL BE MAINTAINED AS DETERMINED NECESSARY BY THE AIRPORT IDOT TYPE 1 BARRICADES ACROSS TAXIWAY A - SEE NOTE 6 14. TURF RUNWAY 9L/27R SHALL BE CLOSED DAILY DURING PHASE 1A OPERATIONS. SEE SITE PLAN FOR TURF RUNWAY CLOSURE DETAIL PHASE 1A BARRICADES CONSTRUCTION SET WHEN NOT WORKING IN WHEN NOT WORKING IN BACK STAKES CRITICAL AREA TAXIWAY A CRITICAL AREA IDOT TYPE 1 (W/ ORANGE FLAG AND RED OMNI-DIRECTIONAL LIGHT) BARRICADES ACROSS TAXIWAY A AT **RUNWAY 18/36** HOLDLINE - SEE NOTE 6 CRITICAL CRITICAL 499+00 500±00

100% SUBMITTAL **JANUARY 6, 2017** 

**NCMT** 

icense No. 184-000613

CONSULTANTS

BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

PHASE 1B BARRICADES

PHASE 1A

FXISTING

MILLINGS ACCESS

EXISTING DOUBLE SWING

ACCESS AND LOCKED WITH

GATE TO BE USED FOR

CONTRACTOR LOCK

BARRICADES



**AIRPORT** 

TAYLORVILLE. ILLINOIS

RK	DATE	DES	SCRIPTION		
3G NO. 3-17-SBGP-105					
PROJ. NO. TAZ-4342					
ИT F	ROJEC	ΓNO:	13050-01-01		

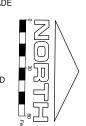
CAD DWG FILE: TAZ-4342-GC003.DWG PRR

DRAWN BY: CHECKED BY: RI V

PPROVED BY: COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015

CONSTRUCTION **ACTIVITY PLAN 1** 

GC003



OPERATED OMNI DIRECTIONAL LENS TO BE RED. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND JET OR PROP BLAST. 3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM COSTS SHALL BE CONSIDERED INCIDENTAL TO THE 4. PLACE AT 15' INTERVALS. BARRICADE DETAIL ONE 20" x 20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE AND

С N39° 32' 13.28" W89° 19' 35.42' 615.50' 25' WORK LIMITS N39° 32' 15.49" W89° 19' 35.45' 615.00' 25' WORK LIMITS N39° 32' 16.23" W89° 19' 35.47' 615.00' 25' WORK LIMITS N39° 32' 16.26" W89° 19' 32.69' 614.30' 25' WORK LIMITS G N39° 32' 11.97" W89° 19' 32.96' 614.70' 25' WORK LIMITS N39° 32' 11.84" W89° 19' 34.17 614.30' 25' WORK LIMITS N39° 32' 15.58" W89° 19' 32.56' 616.00' 25' STAGING AND STORAGE .I N39° 32' 15.97" W89° 19' 32.56' 616 00' 25' STAGING AND STORAGE K N39° 32' 15.98" W89° 19' 31.41' 616.00' 25' STAGING AND STORAGE М N39° 32' 12.15" W89° 19' 35.79 616 00' 25 BARRICADES Ν N39° 32' 16.06" W89° 19' 36.39' 615.00' 25' BARRICADES 0 N39° 32' 07.57" W89° 19' 34.11 616 00' 25' WORK LIMITS N39° 32' 07.57" W89° 19' 33.28' 616 00' 25' WORK LIMITS Q N39° 32' 05.24" W89° 19' 33.24' 616.00' 25' WORK LIMITS R N39° 32' 05.22" W89° 19' 34.83' 616.00' 25' WORK LIMITS N39° 32' 07.09" W89° 19' 35.32' 615.50' 25' WORK LIMITS N39° 32' 08.38" W89° 19' 35 35' 615 50' 25' WORK LIMITS 25' BARRICADES N39° 32' 05.75" W89° 19' 35.69' 616 00' NOTES STEADY BURN TO BE BATTERY

**CRITICAL POINT TABLE** 

ELEVATION

614.20'

615.50'

AGL

25'

25'

LONGITUDE

W89° 19' 34.93'

W89° 19' 35.40'

PHASE 1A BARRICADES XPHASE 1B BARRICADES PHASE 1B CRITICAL AREA BARRICADES

**HANGAR** 

PHASE 1A

PHASE 1B

PHASE 18 CRITICAL

EXISTING ELECTRIC GATE NO CONSTRUCTION ACCESS **TERMINAL** 

**TERMINAL VEHICLE** BUILDING **PARKING LOT** 

> EXISTING 4" CHAIN LINK FENCE

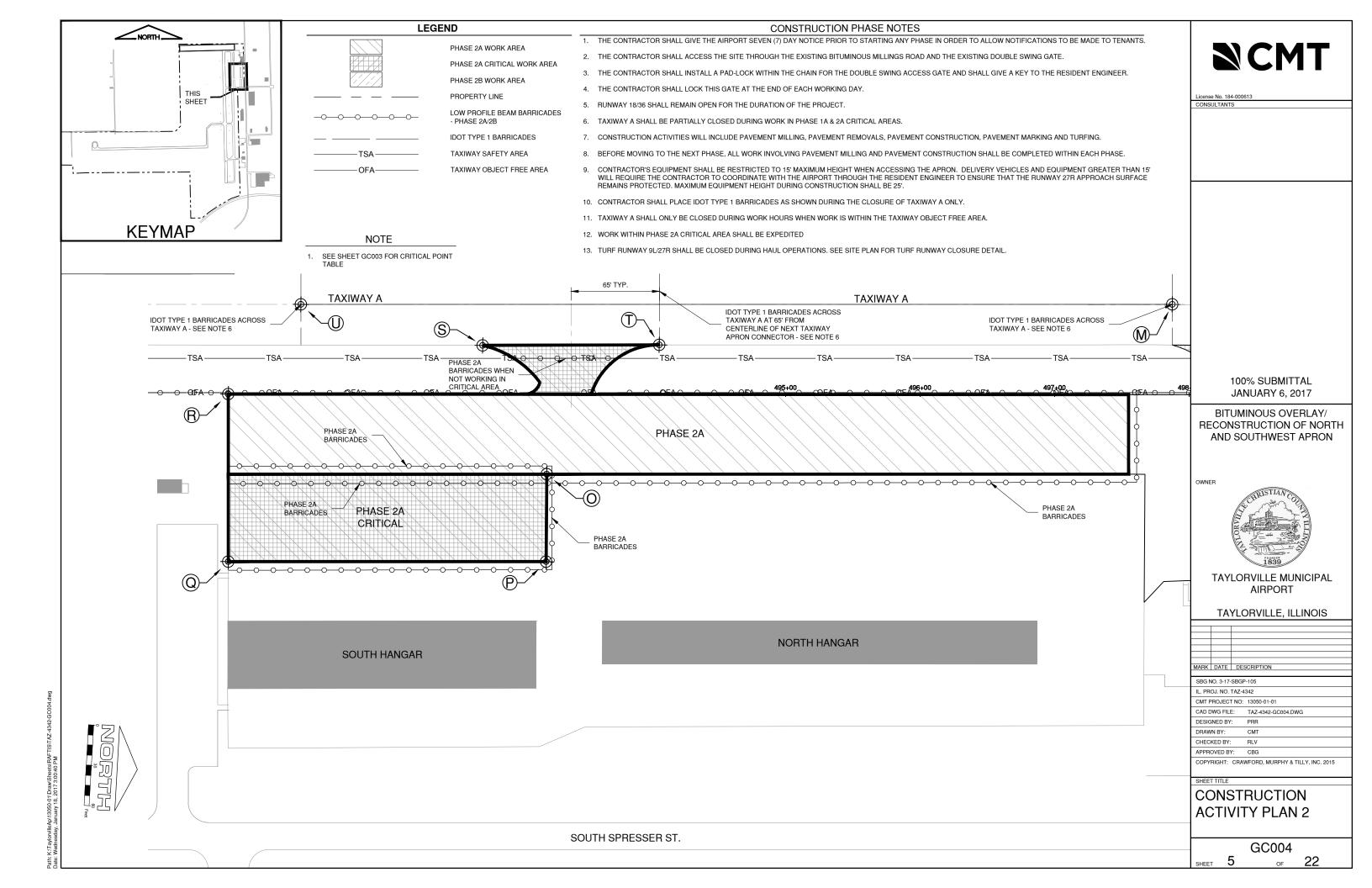
FUEL HARM

PUBLIC AIRPORT ENTRANCE - NO

ACCESS

CONSTRUCTION

SOUTH SPRESSER ST.



#### GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) FOR THIS PROJECT, FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED (NTP) AND THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT (THROUGH THE ENGINEER) FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. AIRPORT APPROVAL OF THE SPCD SHALL BE REQUIRED PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED AND THE START OF CONSTRUCTION ACTIVITY.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE STORM WATER POLLUTION PREVENTION PROGRAM (SWPPP) CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

#### 1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT. ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON AS NEEDED BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A COORDINATION MEETINGS WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

#### 2. PHASING

PHASING SHALL BE AS NOTED AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

#### 3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL BLINWAYS TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

#### 4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

THE CONTRACTOR SHALL REMAIN CLEAR OF THE NAVAIDS FACILITIES AT

#### 5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE ENTRANCE
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES. DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL BOADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR
- 10. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO

#### 6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

#### 7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS
- THE CONTRACTOR SHALL HAVE A POWER BROOM ON SITE FOR FOD MANAGEMENT. MILLED SURFACES SHALL BE SWEPT FOLLOWING CONSTRUCTION ACTIVITY.

#### 8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JORSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

#### 9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTICE TO AIRMEN (NOTAMS) MAY BE ISSUED BY
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL
- 5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATOR
BILL NEWBERRY - AIRPORT MANAGER

CHRIS GROTH - PROJECT ENGINEER (217) 787-8050 CMT - RESIDENT ENGINEER (217) 787-8050

#### 10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH CONSTRUCTION PHASE'S WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

#### 11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN. FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

#### 12. PENALTIES

13. SPECIAL CONDITIONS

INFORMATION.

NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE

CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL

CONSULTANTS

#### 14. RUNWAY AND TAXIWAY VISUAL AIDS

- THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

#### 15. MARKING AND SIGNS FOR ACCESS ROUTES

BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

#### 16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT

(217) 827-2704

- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 65.5' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED, THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- THE CONTRACTOR SHALL MAINTAIN RADIO CONTACT WITH THE AIRPORT WHEN CONDUCTING OPERATIONS WITHIN THE TAXIWAY OBJECT FREE AREA

#### 18. OTHER LIMITATIONS ON CONSTRUCTION

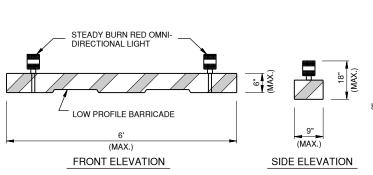
- IF. DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

100% SUBMITTAL **JANUARY 6, 2017** 

### BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON



TAYLORVILLE. ILLINOIS

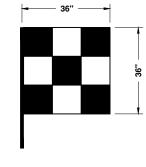


**BEAM BARRICADE DETAILS** 

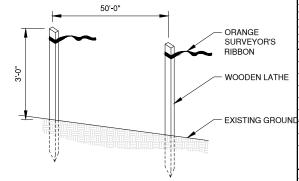
STEADY BURN RED OMNI-REFLECTIVE MATERIAL DIRECTIONAL LIGHT ORANGE FLORESCENT AND WHITE FLORESCENT DIAGONAL SHALL BE PLACED ON ALL FACES MATERIAL SHALL BE REFLECTIVE MATERIAL INSTALLATION PINS AS DIRECTED **ISOMETRIC** 

#### **NOTES**

- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS



TRUCK SIGNAL FLAG



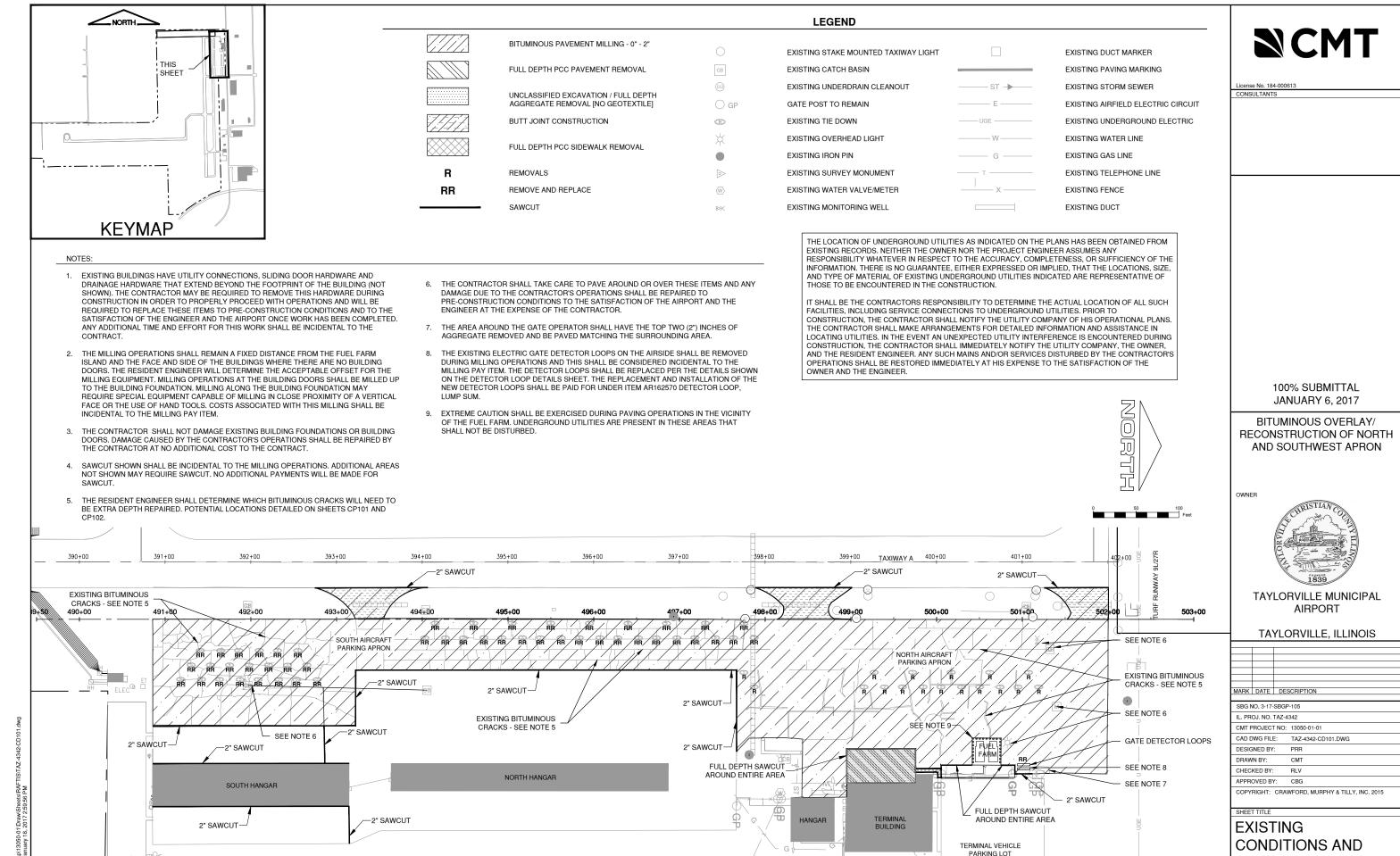
CONSTRUCTION SETBACK LINE DETAIL

MARK DATE DESCRIPTION SBG NO. 3-17-SBGP-105 II. PROJ. NO. TAZ-4342 CMT PROJECT NO: 13050-01-0 CAD DWG FILE: TAZ-4342-GC005.DWG DRAWN BY: CHECKED BY: RLV CBG PPROVED BY: COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015

CONSTRUCTION SAFETY PHASING PLAN NOTES

GC005

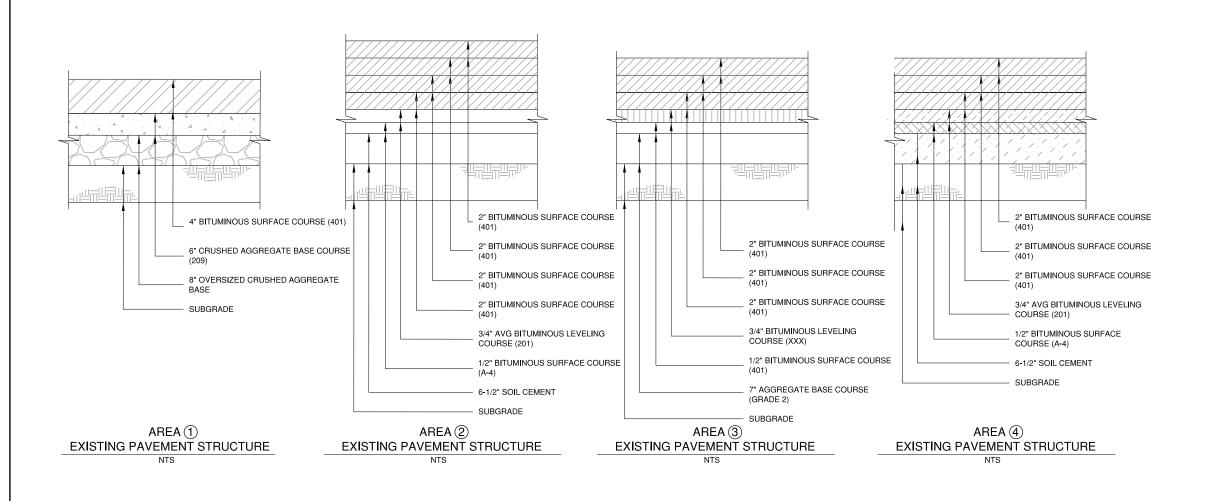
4.) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

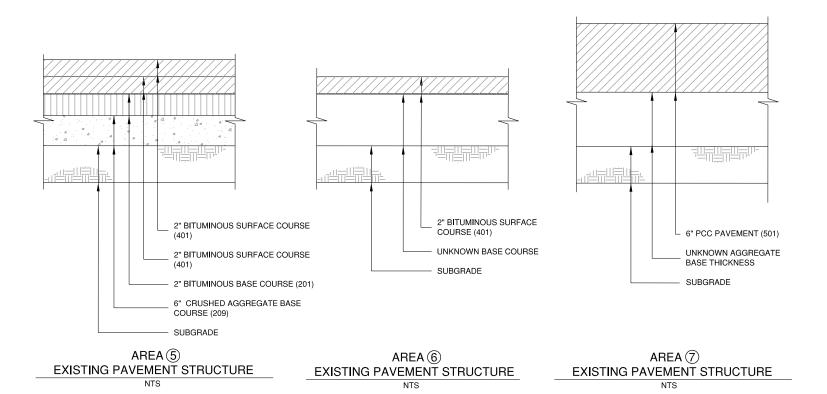


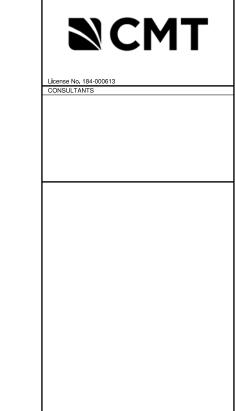
CD101

**REMOVALS** 

22







100% SUBMITTAL **JANUARY 6, 2017** 

BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

OWNER



**AIRPORT** 

TAYLORVILLE, ILLINOIS

MARK DATE DESCRIPTION

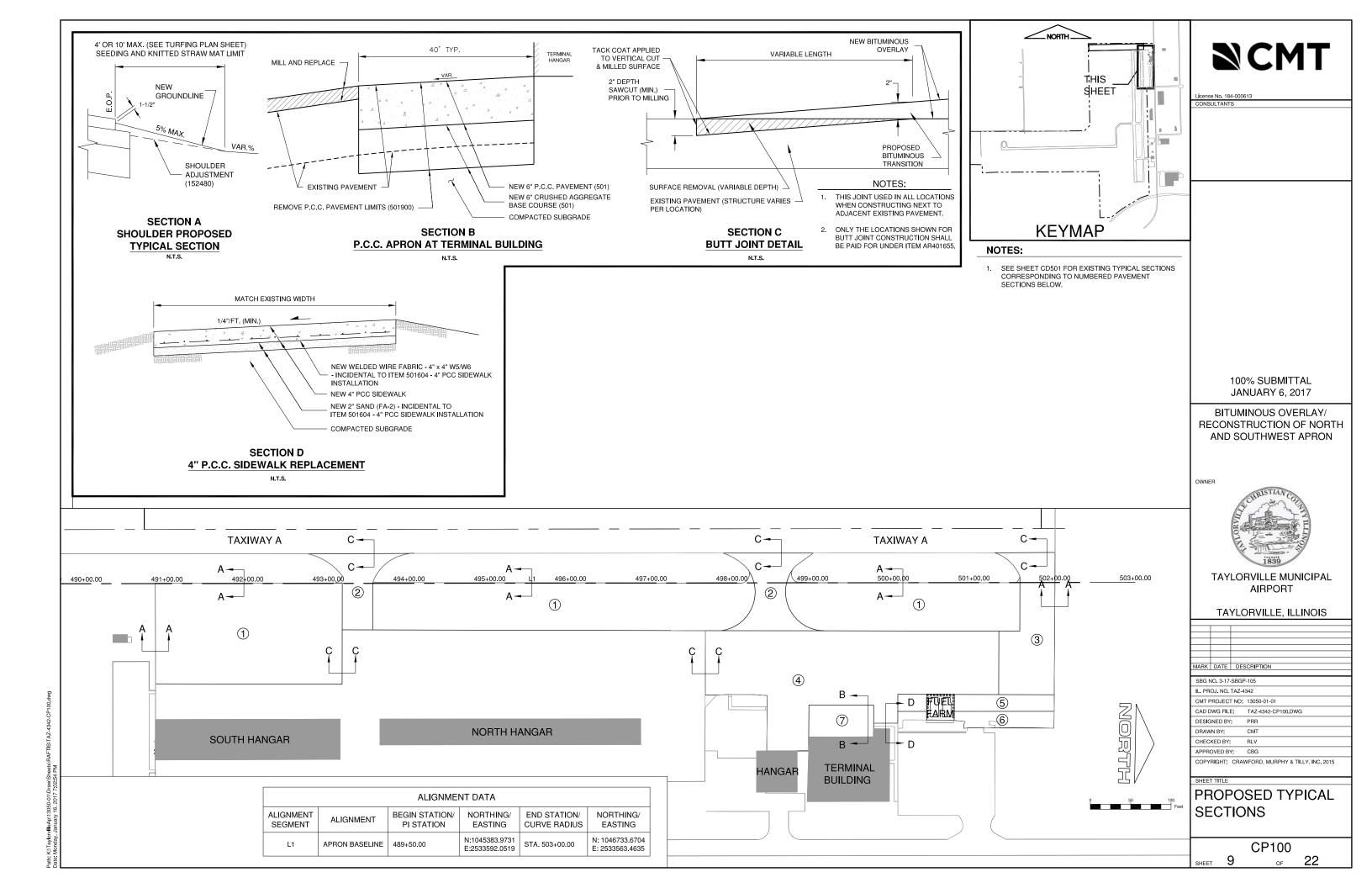
SBG NO. 3-17-SBGP-105 IL. PROJ. NO. TAZ-4342 CMT PROJECT NO: 13050-01-01 CAD DWG FILE: TAZ-4342-CD501.DWG DESIGNED BY: DRAWN BY: CHECKED BY: RLV APPROVED BY: CBG

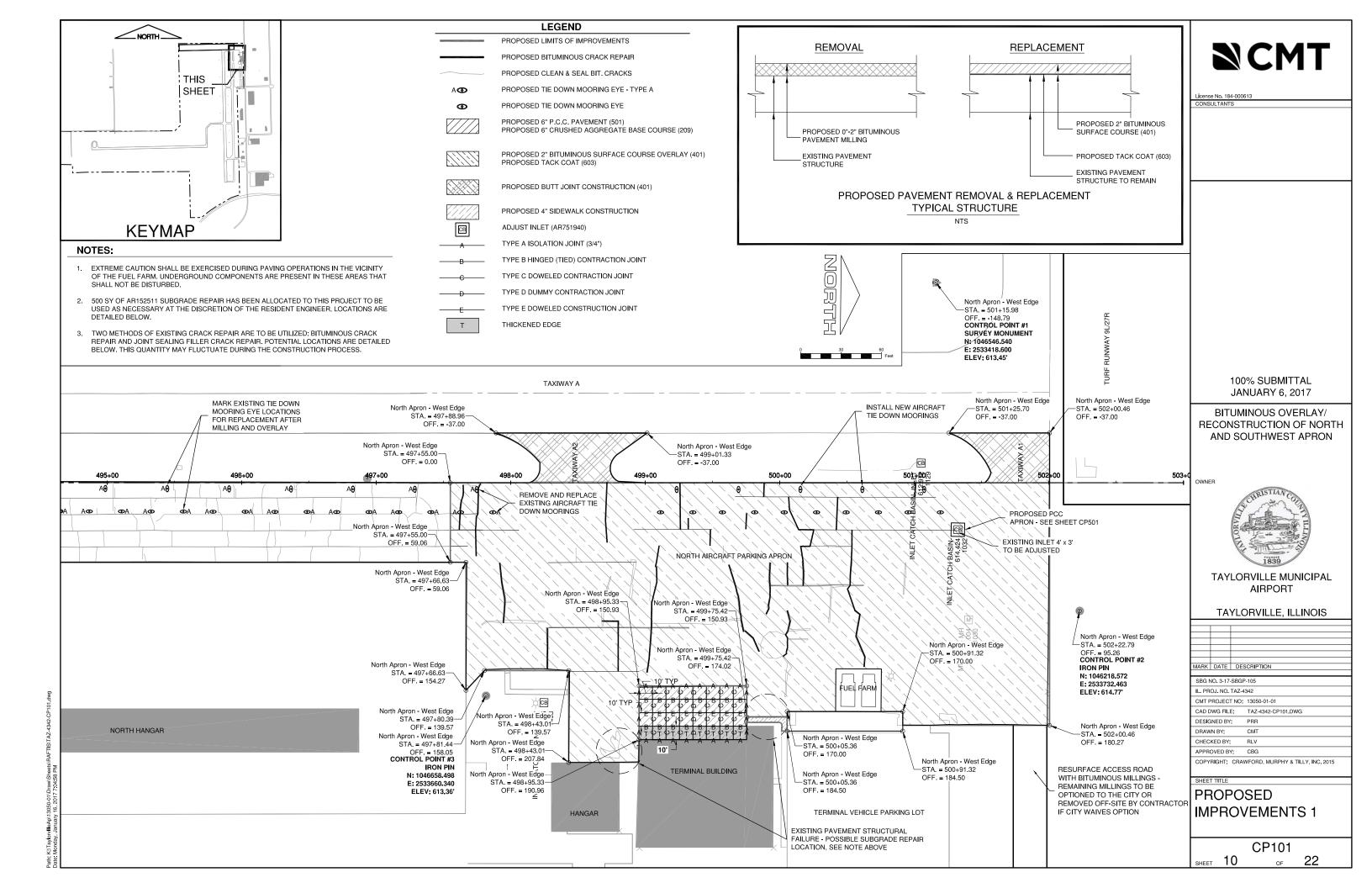
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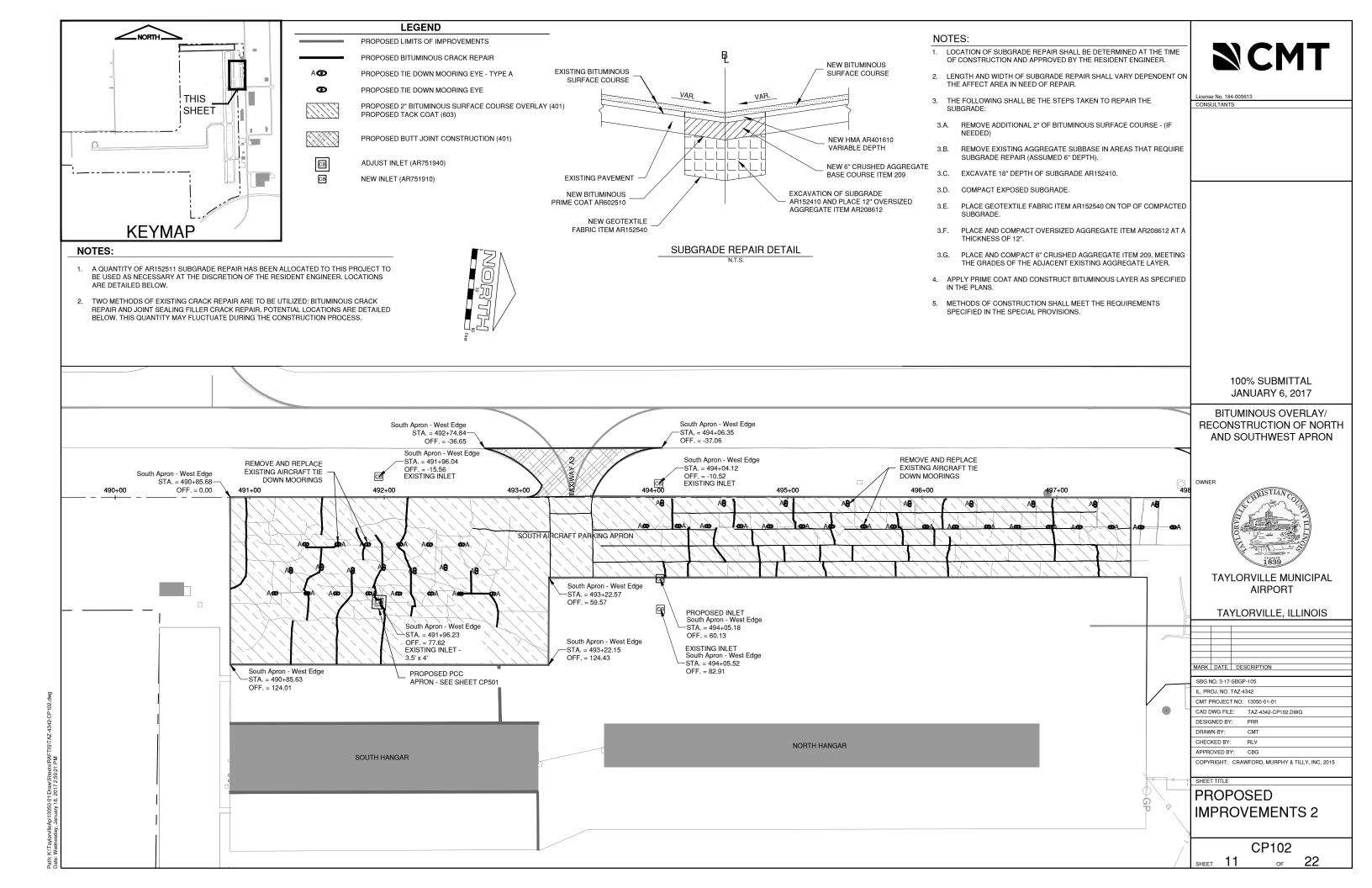
**EXISTING TYPICAL** 

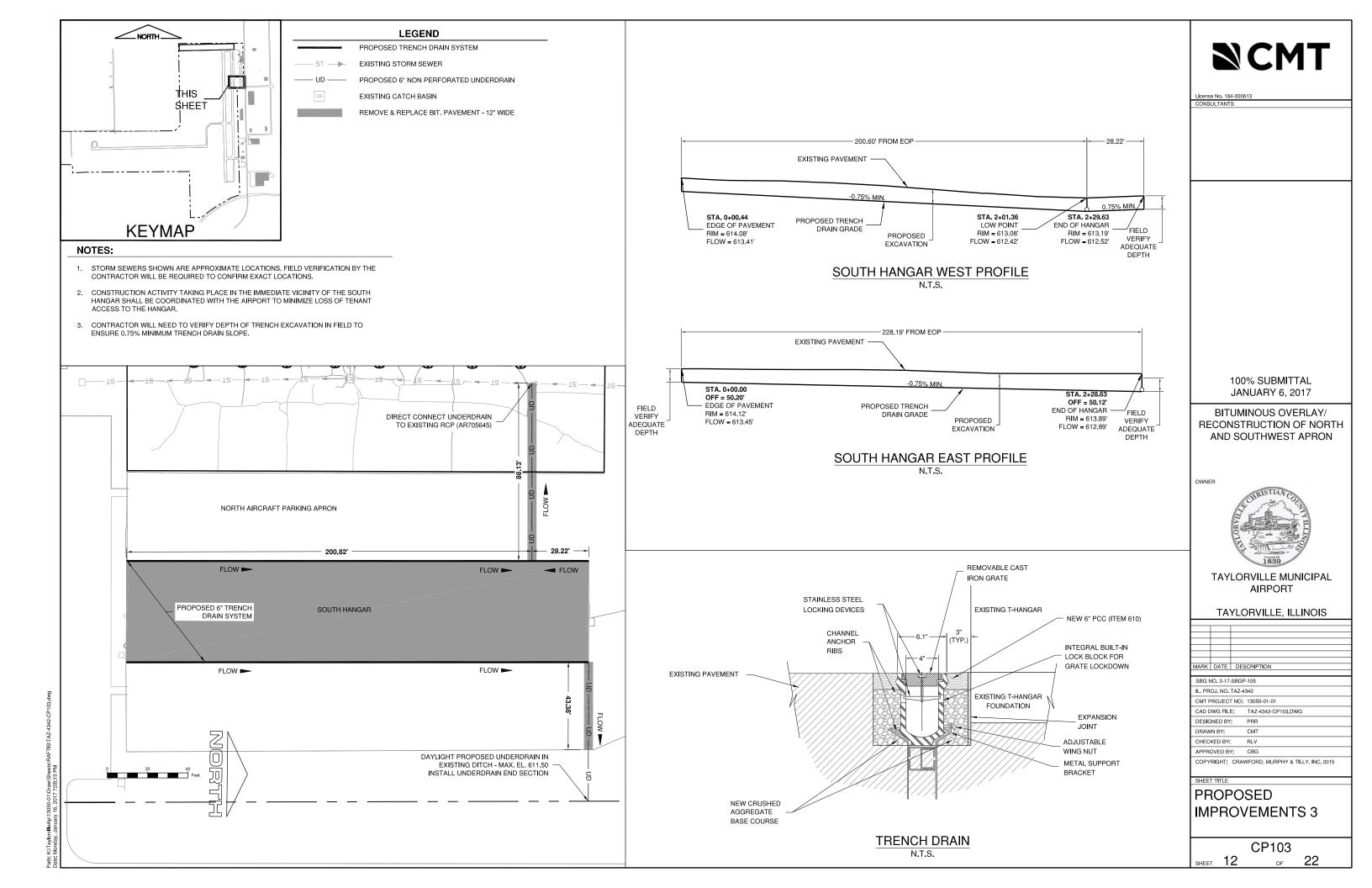
**SECTIONS** 

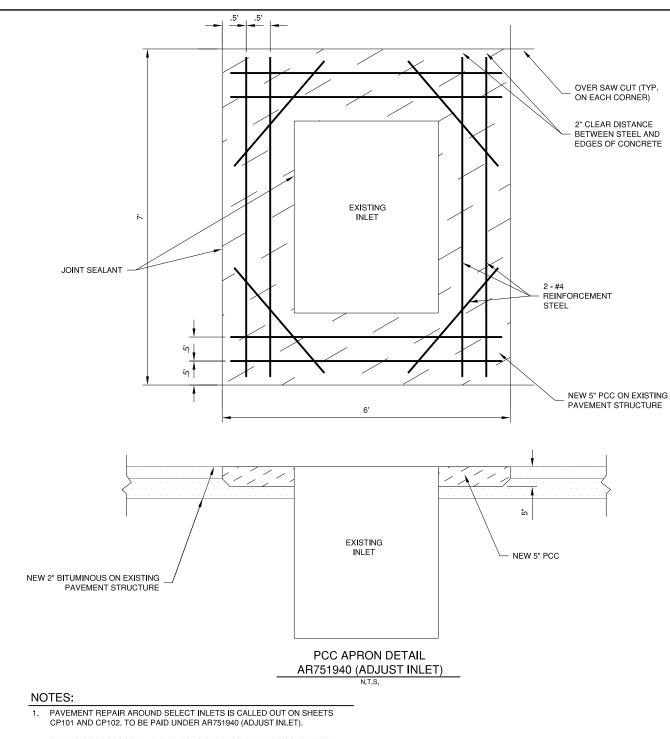
CD501 SHEET 8 22





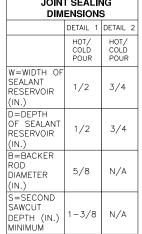


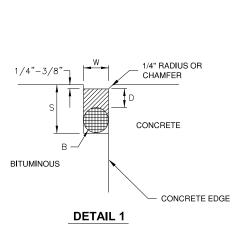




- 2. PAVING OF PROPOSED 2" BITUMINOUS SURFACE SHALL BE COMPLETED AND FINISHED TO EDGE OF INLET PRIOR TO CONSTRUCTING THIS DETAIL.
- 3. PCC APRON SHALL EXTEND 18" FURTHER THAN INLET EXTENTS IN ALL DIRECTIONS, AS SHOWN IN DETAIL ABOVE.
- 4. REMOVE ADDITIONAL 3" OF EXISTING PAVEMENT STRUCTURE. USE CARE TO NOT UNDERMINE NEW BITUMINOUS PAVEMENT SURFACE. SEE SHEET CD501 FOR EXISTING TYPICAL SECTIONS.
- 5. PLACE EXPANSION JOINT MATERIAL AROUND INLET FRAME.
- PLACE STEEL AS SHOWN IN THE DETAIL AND PLACE 5" PCC LAYER. REBAR SHALL BE PLACED AT A DEPTH OF T/2, T = THICKNESS OF PCC PAVEMENT. KEEP 2" CLEAR DISTANCE BETWEEN REBAR AND EDGES OF PCC
- 7. ENSURE THAT GRADES ARE MET ALONG EXISTING EDGES AND INLET WHILE FINISHING THE CONCRETE.
- 8. AFTER CONSTRUCTION OF CONCRETE LAYER, SAW CUT THE BITUMINOUS PAVEMENT ALONG THE EDGE OF THE CONCRETE. DIMENSIONS OF SAW
- 9. PLACE BACKER ROD AND SEAL WITH JOINT SEALANT AS CALLED OUT IN THE SPECIAL PROVISIONS. DETAIL OF JOINT IS SHOWN ON THIS SHEET. JOINT SHALL BE SAWED IN BITUMINOUS PAVEMENT ADJACENT TO THE PCC

	T SEALII ENSION:	
	DETAIL 1	DETAIL 2
	HOT/ COLD POUR	HOT/ COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	1/2	3/4
D=DEPTH OF SEALANT RESERVOIR (IN.)	1/2	3/4
B=BACKER ROD DIAMETER (IN.)	5/8	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	1-3/8	N/A





NEW 4' WIDE CRACK CONTROL FABRIC (AR201670) OVER EXTRA

MILLED SURFACE PRIOR

TO EXTRA DEPTH REPAIR

APPROX. 3"

ADDITIONAL

VARIABLE ROUT WIDTH (W)

ROUT DEPTH

EXISTING VARIABLE DEPTH

CRACK SEALANT MATERIAL

BITUMINOUS PAVEMENT

(SEE NOTE 1)

HMA CRACK SEALING DETAIL

REMOVAL

CRACKS TO BE REPAIRED SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.

GENERAL DIRECTION SHALL BE TO BEPAIR BY THIS METHOD ANY CRACKS.

THE DEPTH FOR ADDITIONAL REMOVAL MAY INCREASE AT THE DESIGNATION OF THE RESIDENT ENGINEER BASED ON THE SEVERITY OF THE CRACK.
MINIMUM DEPTH OF THIS ADDITIONAL REMOVAL SHALL BE THREE (3") INCHES.

DEPTH REMOVAL TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR

PARALLEL CRACKS LOCATED LESS THAN THREE (3') FEET APART SHALL BE

GREATER THAN ONE (1") INCH IN WIDTH THAT REMAIN AFTER MILLING.

3. THE CONTRACTOR SHALL REMOVE ALL LOOSE DEBRIS FROM THE EXTRA

CONSIDERED ONE (1) CRACK FOR MEASUREMENT

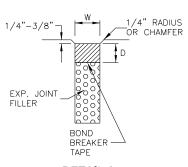
EXISTING CRACK

DEPTH REPLACEMENT

NOTES:

TO APPLYING TACK COAT.

BELOW PAVEMENT



**DETAIL 2** 

## **JOINT NOTES**

NOTES

1.) THE CRACK ROUTING DIMENSIONS SHALL PROVIDE A WIDTH TO DEPTH RATIO OF 1:1 FOR SEALANT

2.) CRACKS TO BE ROUTED, CLEANED AND SEALED AT LOCATIONS

DETERMINED BY THE ENGINEER PRIOR TO

MATERIAL.

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- SAW CUT FOR JOINT SEALANT SHALL BE CONSTRUCTED IN THE BITUMINOUS PAVEMENT MATERIAL ADJACENT TO THE PCC PAVEMENT.
- 3. EXPANSION JOINT MATERIAL SHALL BE PLACED AROUND FRAME PRIOR TO CONCRETE POUR.
- JOINTING SHALL BE CONSIDERED INCIDENTAL TO CATCH BASIN APRON PAVING. NO ADDITIONAL PAYMENT WILL BE MADE.

**N**CMT

NEW 2" BITUMINOUS MILLING

NEW 2" BITUMINOUS SURFACE

COURSE (401)

NEW TACK COAT ON ALL SIDES OF

**EXISTING BITUMINOUS** 

CRACK 1" OR GREATER IN

FULL DEPTH BITUMINOUS SURFACE COURSE - TO BE PAID

FOR UNDER ITEM AR201660

BITUMINOUS CRACK REPAIR

-REMOVAL - TO BE PAID FOR UNDER ITEM AR201660 BITUMINOUS CRACK REPAIR

CRACK CONTROL FABRIC - 4' WIDE

EXTRA DEPTH

REPAIR - 2' WIDE

**BITUMINOUS CRACK REPAIR (AR201660)** 

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BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

OWNER



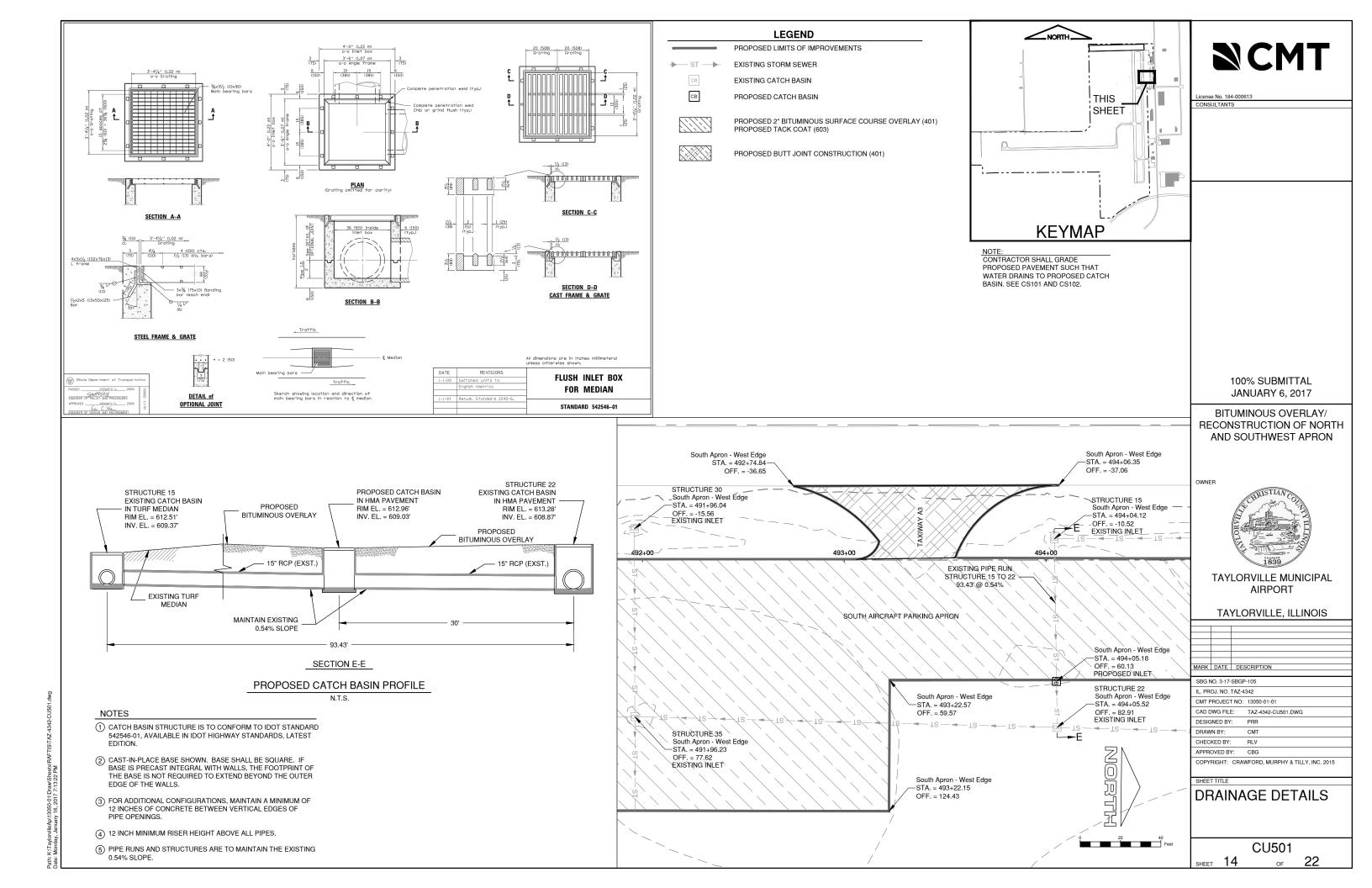
TAYLORVILLE MUNICIPAL **AIRPORT** 

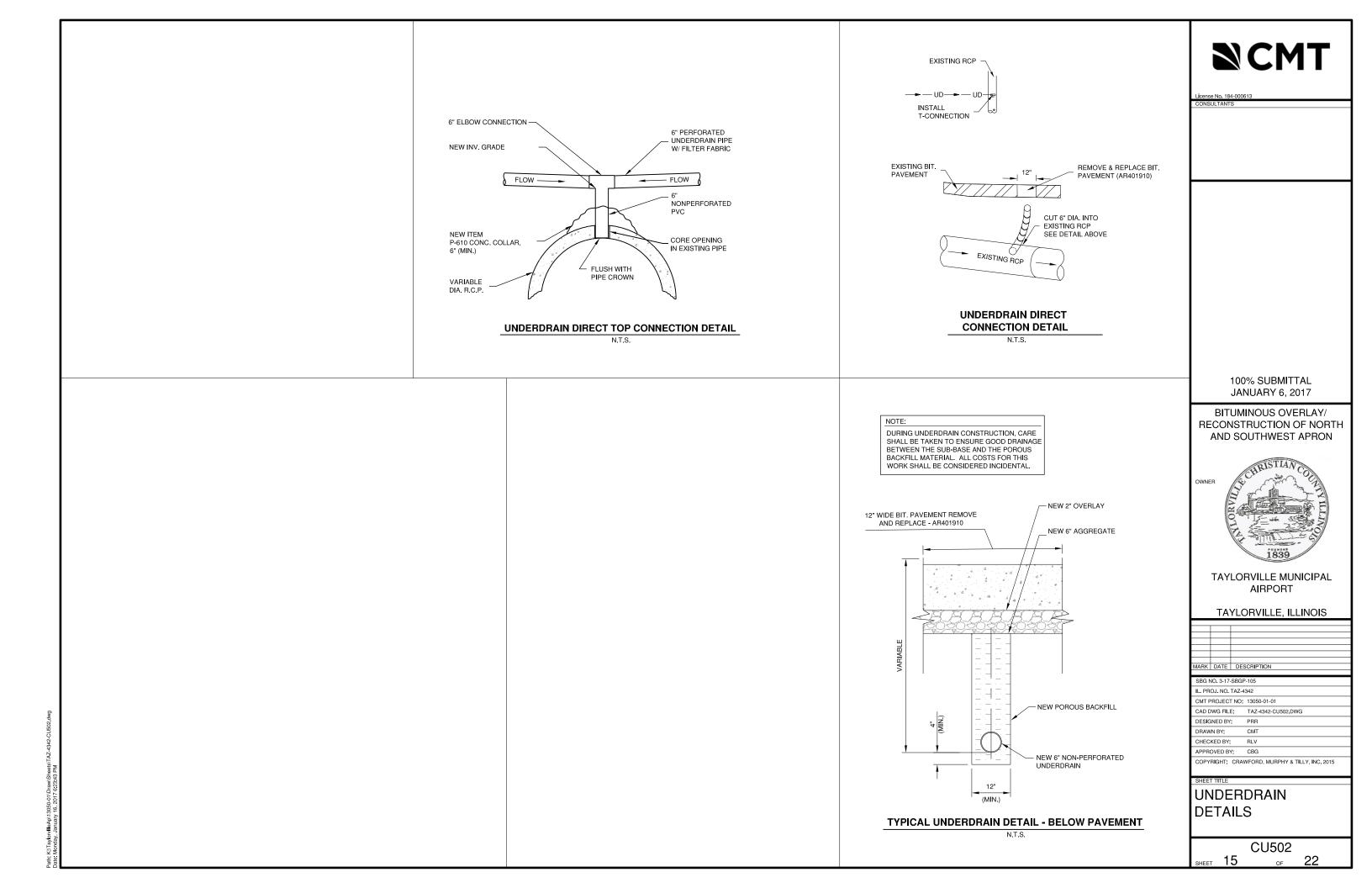
TAYLORVILLE, ILLINOIS

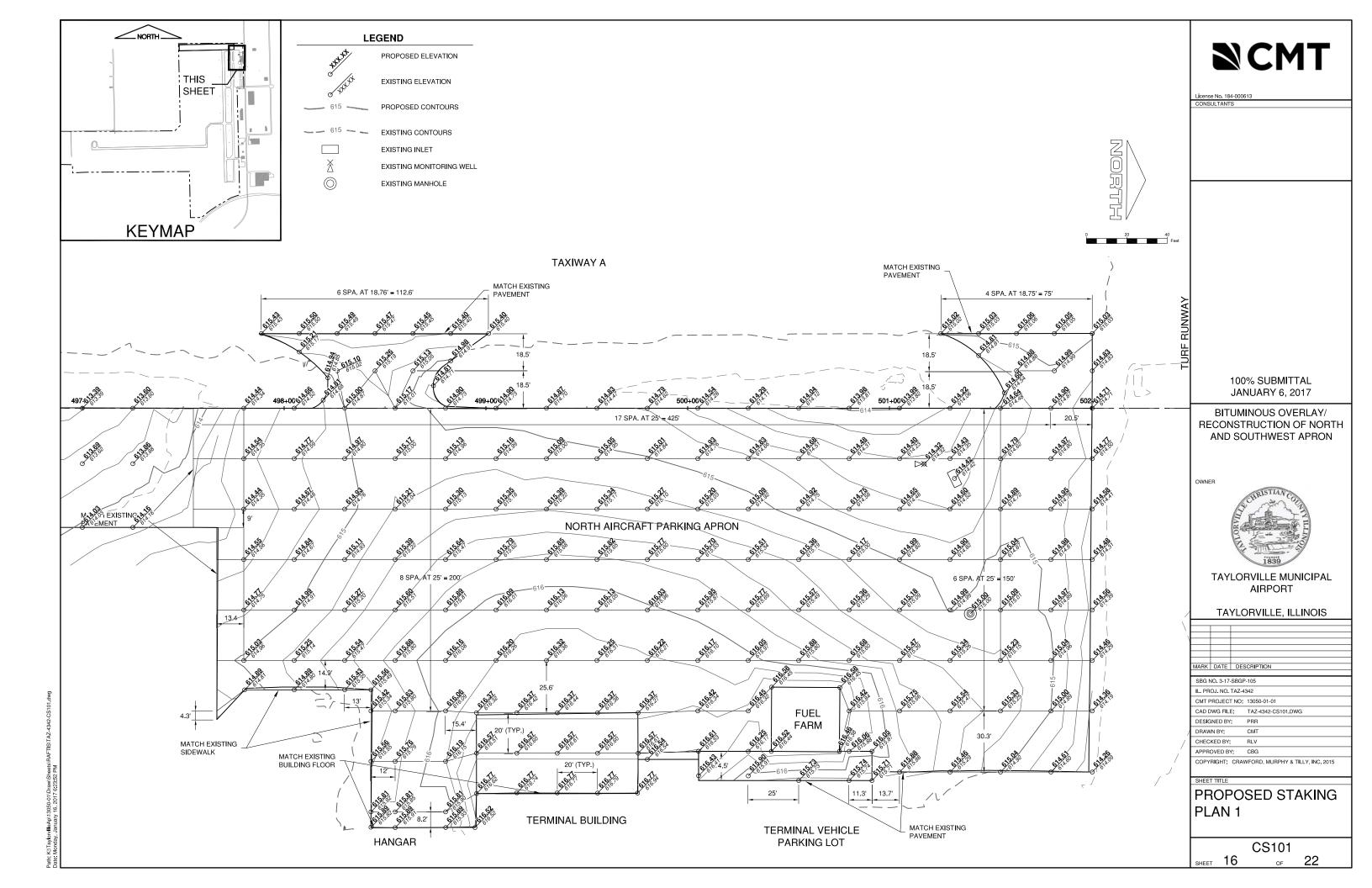
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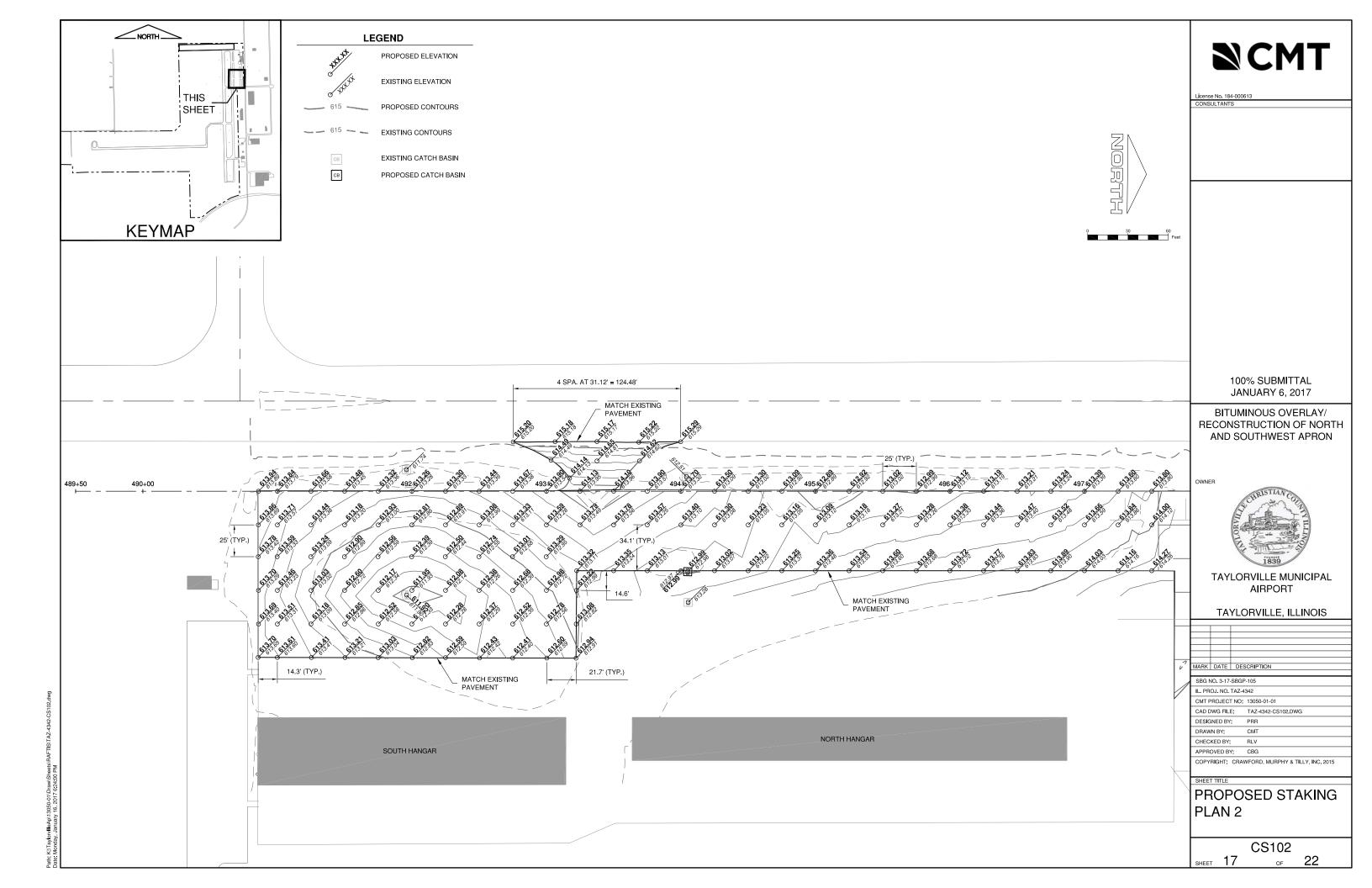
PAVEMENT REPAIR **DETAILS** 

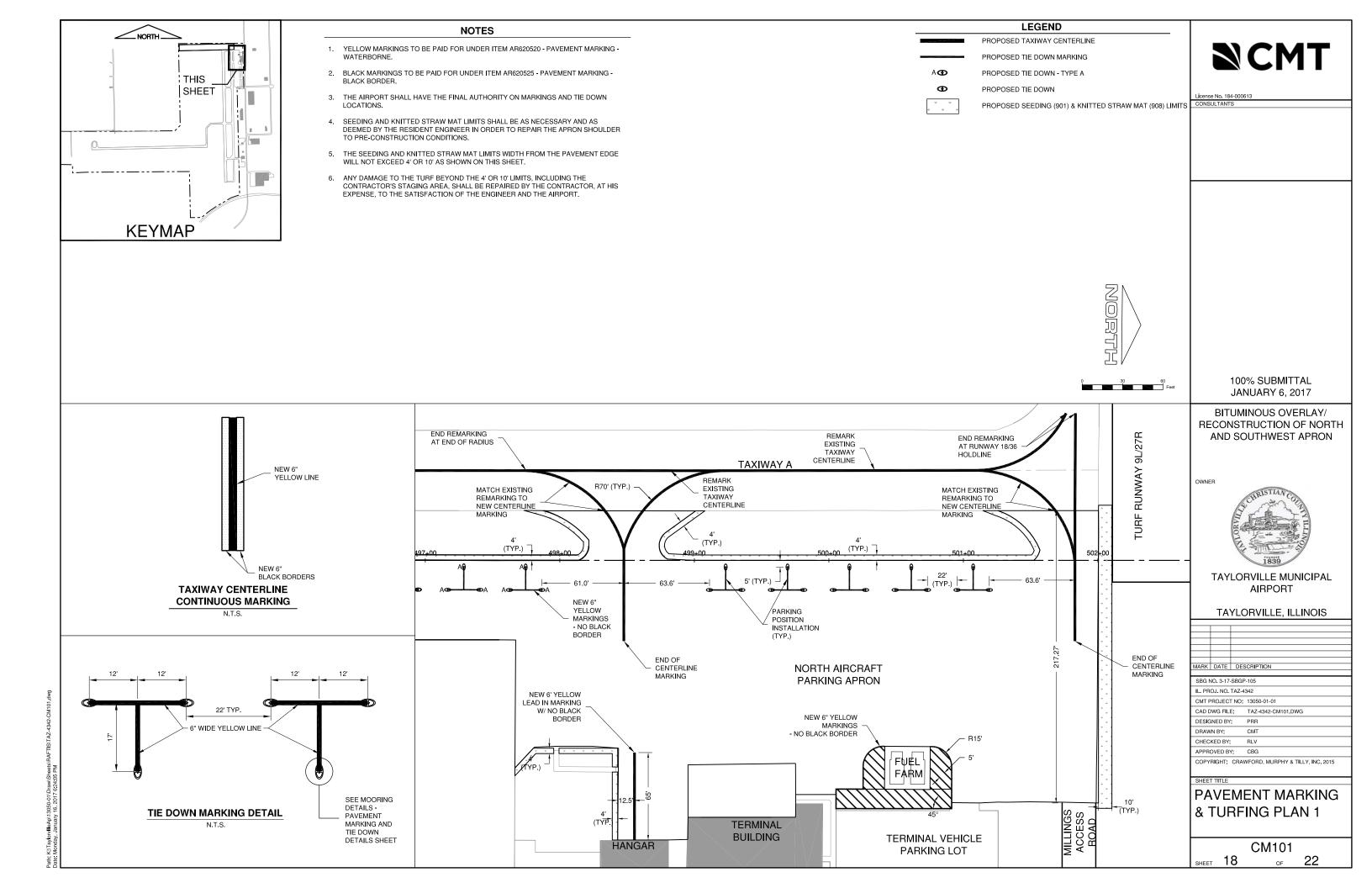
**CP501** 22 SHEET 13

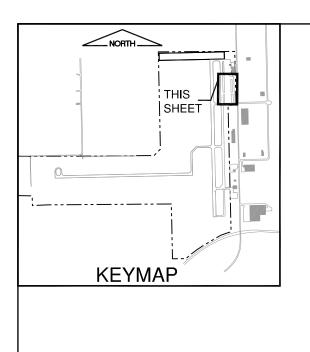












#### NOTES

- YELLOW MARKINGS TO BE PAID FOR UNDER ITEM AR620520 PAVEMENT MARKING WATERBORNE
- 2. BLACK MARKINGS TO BE PAID FOR UNDER ITEM AR620525 PAVEMENT MARKING BLACK BORDER.
- 3. THE AIRPORT SHALL HAVE THE FINAL AUTHORITY ON MARKINGS AND TIE DOWN
- 4. SEE MISCELLANEOUS DETAILS 2 FOR FURTHER INFORMATION ON THE TIE DOWN INSTALLATION.
- 5. SEEDING AND KNITTED STRAW MAT LIMITS SHALL BE AS NECESSARY AND AS DEEMED BY THE RESIDENT ENGINEER IN ORDER TO REPAIR THE APRON SHOULDER TO PRE-CONSTRUCTION CONDITIONS.
- 6. THE SEEDING AND KNITTED STRAW MAT LIMITS WIDTH FROM THE PAVEMENT EDGE WILL NOT EXCEED 4' OR 10' AS SHOWN ON THIS SHEET.
- 7. ANY DAMAGE TO THE TURF BEYOND THE 4' OR 10' LIMITS, INCLUDING THE CONTRACTOR'S STAGING AREA, SHALL BE REPAIRED BY THE CONTRACTOR, AT HIS EXPENSE, TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.

MATCH

EXISTING REMARKING

TO NEW CENTERLINE

NEW 6" YELLOW

MARKINGS

BORDER

- NO BLACK

NEW 6" YELLOW MARKINGS

END OF CENTERLINE MARKING

NO BLACK BORDER

END REMARKING AT END OF RADIUS

SOUTH AIRCRAFT

PARKING APRON

(TYP.)

(TYP.)

LEGEND

PROPOSED TAXIWAY CENTERLINE
PROPOSED TIE DOWN MARKING

PROPOSED TIE DOWN - TYPE A

PROPOSED TIE DOWN
PROPOSED SEEDING (901) & KNITTED STRAW MAT (908) LIMITS

PROPOSED SEEDING (901) & CONSULTANTS

100% JANL

(TYP.)

100% SUBMITTAL JANUARY 6, 2017

BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

NER CIRUS

TAYLORVILLE MUNICIPAL AIRPORT

TAYLORVILLE, ILLINOIS

MARK DATE DESCRIPTION

SBG NO. 3-17-SBGP-105

IL. PROJ. NO. TAZ-4342

CMT PROJECT NO: 13050-01-01

CMT PROJECT NO: 13050-01-01

CAD DWG FILE: TAZ-4342-CM102.DWG

DESIGNED BY: PRR

DRAWN BY: CMT
CHECKED BY: RLV
APPROVED BY: CBG

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SHEET TITLE

PAVEMENT MARKING & TURFING PLAN 2

CM102 SHEET 19 OF 22

SOUTH HANGAR

TAXIWAY A

PARKING

POSITION

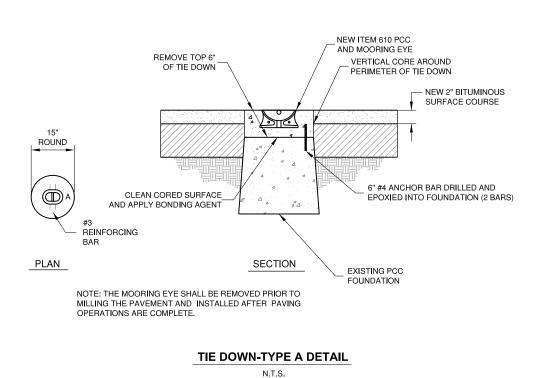
**NORTH HANGAR** 

INSTALLATION

REMARK EXISTING TAXIWAY CENTERLINE

- 393.92'

/IlleAp\13050-01\Draw\Sheets\RAFTIS\TAZ-4342-CM102.dwg January 16, 2017 6:24:11 PM



EXISTING TIE DOWNS 12" SQ. ARE FILLED WITH GROUT **EXISTING TIE DOWN DETAIL** N.T.S. CONTROL FABRIC (AR201670) OVER TIEDOWN REMOVALS - TO BE PAID FOR UNDER ITEM AR201670 CRACK CONTROL **BITUMINOUS CAP** 

DEPTH TO MILLED SURFACE

NEW CRUSHED AGGREGATE BASE COURSE (20

REMOVED TIE DOWN VOID

TIE DOWN BACKFILL DETAIL

N.T.S.

NEW 2" BITUMINOUS PAVEMENT (401

NEW BITUMINOUS TACK COAT (603)

NEW 4' x 4' SHEET CRACK

**FABRIC** 

**JANUARY 6, 2017** BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

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TAYLORVILLE MUNICIPAL AIRPORT

TAYLORVILLE, ILLINOIS



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PAVEMENT MARKING AND TIE DOWN **DETAILS** 

CM501 SHEET 20 22

VERTICAL CORE AROUND PERIMETER OF MOORING EYE MOORING EYE — CLEAN CORED SURFACE AND APPLY BONDING NEW 2" BITUMINOUS SURFACE COURSE ROUND EXISTING VARIABLE THICKNESS BITUMINOUS PAVEMENT ITEM 610 PCC REINFORCING 20" ROUND PLAN SECTION

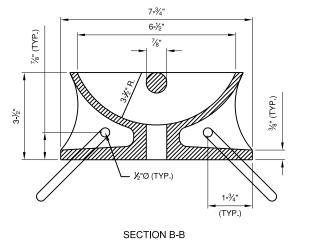
> NOTE: THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE.

#### **TIE DOWN DETAIL**

N.T.S.

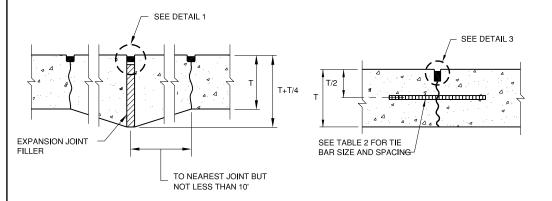
#### **MOORING NOTES**

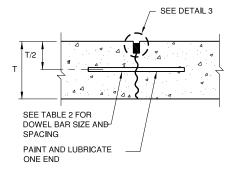
- MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. EJ5996 OR APPROVED
- 2. ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45 DEGREES.
- 3. THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE BY CORING THROUGH NEW PAVEMENT.

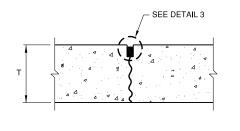


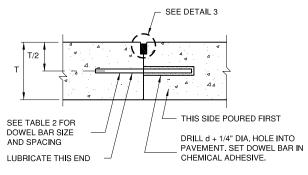
MOORING EYE DETAILS

N.T.S.









# **N**CMT

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TYPE A THICKENED ISOLATION

TYPE B HINGED (TIED) CONTRACTION

SYMBOL B

TYPE C DOWELED CONTRACTION

SYMBOL — C

TYPE D DUMMY CONTRACTION SYMBOL ———————

TYPE E DOWELED CONSTRUCTION

SYMBOL

TABLE 1				
	IADLE I			
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOIN INITIAL SAW CUT I, INCHES I=(T/3) ±1/4"			
5	1.67"			
6	2.00"			
7	2.33"			
8	2.67"			
9	3.00"			
10	3.33"			
11	3.67"			
12	4.00"			
13	4.33"			
14	4.67"			
15	5.00"			
16	5.33"			
17	5.67"			
18	6.00"			

TABLE 2						
PAVEMENT THICKNESS	DOW	EL BAR DE	AILS	TIE BAR DETAILS		
T - INCHES	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT/ COLD POUR	HOT/ COLD POUR	SILICONE	PRE FORMED	HOT/ COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2	3/8" COMPRESSED	3/16"
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/4	N/A	I
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	1-1/2	N/A

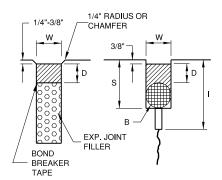
#### JOINT NOTES

- 1.) ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- 2.) THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE
- 3.) ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- 4.) TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.

- DOWEL

(SEE NOTES 2 & 3)

5.) THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE



DETAIL 1 DETAIL 3

JOINT SEALING DETAILS

ALTERNATE DOWEL BARS ARC WELDED TO TOP SPACER BARS (SEE NOTES 2 & 3) TOP SPACER BAR ✓ SHOWS ARC WELDING CHAIR " SHOWS RESISTANCE WELDING - BOTTOM SPACER BAR

### TYPICAL DOWEL BASKET ELEVATION **DETAIL SHOWING CHAIR**

#### **DOWEL BASKET DETAILS** N.T.S.

#### **DOWEL BASKET NOTES**

- 1. #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- 2. DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN
- 3. DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL.
  IMMEDIATELY PRIOR TO PAVING. THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE

100% SUBMITTAL **JANUARY 6, 2017** 

BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

OWNER



TAYLORVILLE, ILLINOIS

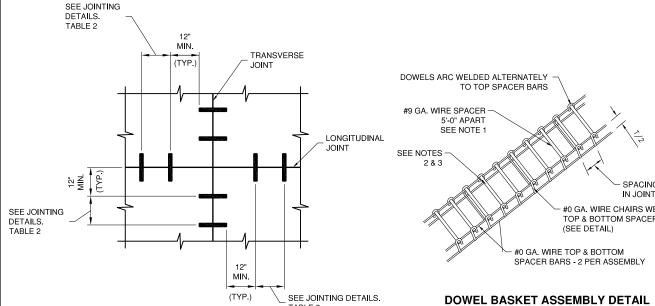
**AIRPORT** 

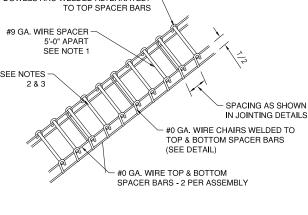
MARK	DATE	DES	SCRIPTION		
SBG N	NO. 3-17-	SBGF	P-105		
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CMT PROJECT NO: 13050-01-01					
CAD DWG FILE:		Ξ:	TAZ-4342-CJ501.DWG		
DESIGNED BY:			PRR		
DRAWN BY:			CMT		
CHECKED BY:			RLV		
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JOINTING DETAILS

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CJ501 22 SHEET 21



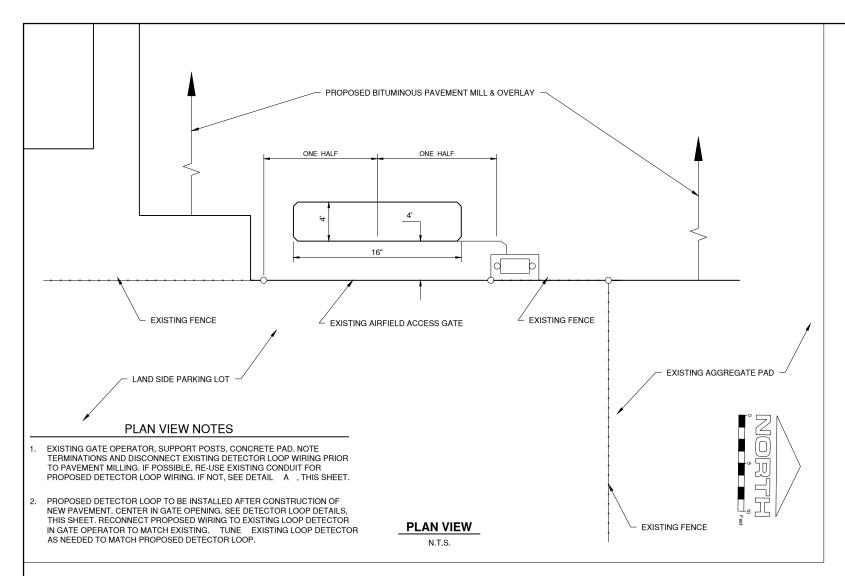


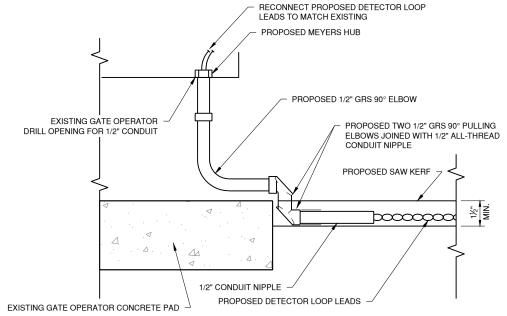
DRIVE STAKES 12" TO воттом-WIRE SPACER SPACER TO BE REMOVED 18" APART ON EACH PRIOR TO PAVING SIDE OF ASSEMBLY

TOP SPACER BAR

DOWEL BAR INSTALLATION DETAIL N.T.S.

DOWEL PLACEMENT DETAIL





**DETAIL A** 

BITUMINOUS OVERLAY/

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**NCMT** 

License No. 184-000613 CONSULTANTS

RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

OWNER



TAYLORVILLE, ILLINOIS

MARK DATE DESCRIPTION

SBG NO. 3-17-SBGP-105

IL. PROJ. NO. TAZ-4342

CMT PROJECT NO: 13050-01-01

CAD DWG FILE: TAZ-4342-EI501.DWG

DESIGNED BY: WDP

DRAWN BY: CMT

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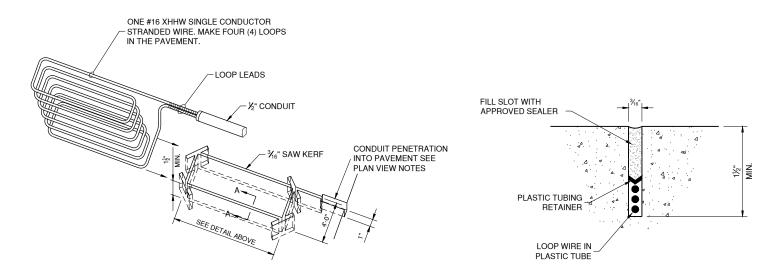
RLV

SHEET TITL

CHECKED BY:

DETECTOR LOOP DETAILS

El501 2 <sub>of</sub> 22



PERSPECTIVE VIEW OF DETECTOR
LOOP SAW SLOT

SECTION A-A
N.T.S.

DETECTOR LOOP DETAILS

N.T.S.

#### NOTES

- 1. LOOP LEADS ARE LIMITED TO 100 FEET.
- 2. LOOP LEADS MUST HAVE SIX (6) TWISTS PER FOOT.
- 3. LOOP AND LOOP LEADS MUST BE LOCATED, AT LEAST, 18" FROM ANY ELECTRICAL POWER SERVICE OR STEEL REINFORCEMENT.
- 4. LOOP LEADS MUST BE IN SEPARATE CONDUIT BETWEEN LOOP AND DETECTOR. THEY MUST NOT SHARE CONDUIT WITH OTHER WIRING OR LEADS FROM OTHER LOOPS.
- 5. WIRE SHALL BE #16 XHHW SINGLE CONDUCTOR STRANDED WIRE.
- 6. ALL WIRE SHALL BE CONTINOUS WITHOUT SPLICING.

#### CAUTION:

- DO NOT SPLICE WIRE.
- DO NOT FRACTURE WIRE INSULATION. LOOPS SHORTED TO GROUND WILL CAUSE DETECTOR MALFUNCTION. WHEN PLACING WIRE IN THE SLOT. DO NOT USE SCREWDRIVER OR OTHER SHARP TOOLS.

#### TYPICAL LAYOUT FOR LOOP:

- SAW SLOT 3/16" WIDE x 1-1/2" DEEP. MAKE RECTANGULAR SHAPE TO SPECIFIED LOOP DIMENSIONS PLUS SLOT FOR LEAD CONDUIT.
- GROUT WITH NO. 202 WEATHERBAN SEALANT (A PRODUCT OF 3M CO.)
  OR APPROVED EQUIVALENT ... (EXAMPLE: DE WITTS NO. 99 BLACK
  MASTIC CAULK).