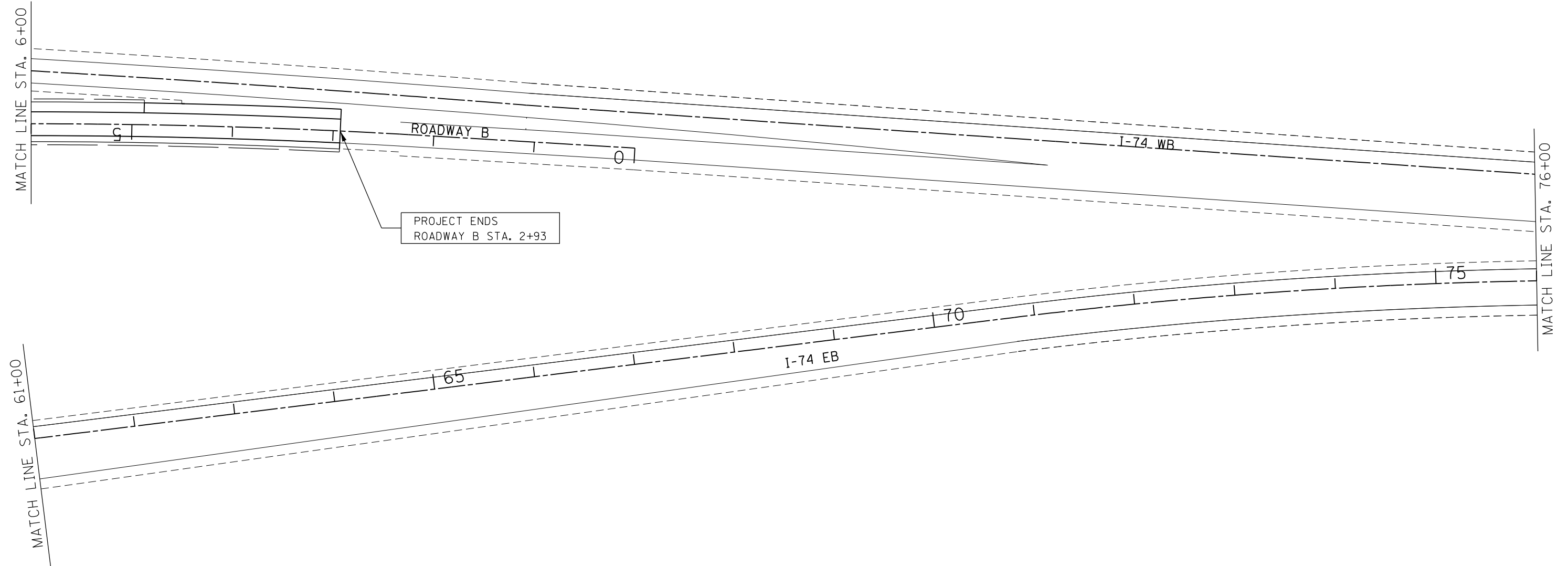




EROSION CONTROL PLAN	
TEMPORARY EROSION CONTROL	
	INLET AND PIPE PROTECTION
	PERIMETER EROSION BARRIER
	TEMPORARY DITCH CHECKS
PERMANENT EROSION CONTROL	
	AGGREGATE DITCH CHECKS
	STONE DUMPED RIPRAP CLASS B3 OR B4
	HEAVY DUTY EROSION CONTROL BLANKET
	SEEDING

PRIOR TO BEGINNING EARTHWORK, PERIMETER EROSION BARRIER SHALL BE PLACED AS SHOWN ABOVE IN ACCORDANCE WITH THE TYPICAL APPLICATION OF SILT FILTER FENCE DETAIL AND SECTION 280 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND AS DIRECTED BY THE RESIDENT ENGINEER.



FILE NAME = S:\GEN\DRIFT\STD&PLNS\SQUAD 0\1-474 3R	USER NAME = lababidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EROSION CONTROL PLANS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Project\english files\Erosion control Plan	DRAWN -	REVISED -	474					90-6,7RS-1	TAZEWELL	101	101	
Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -		CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/11/2015	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	

GENERAL NOTES

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

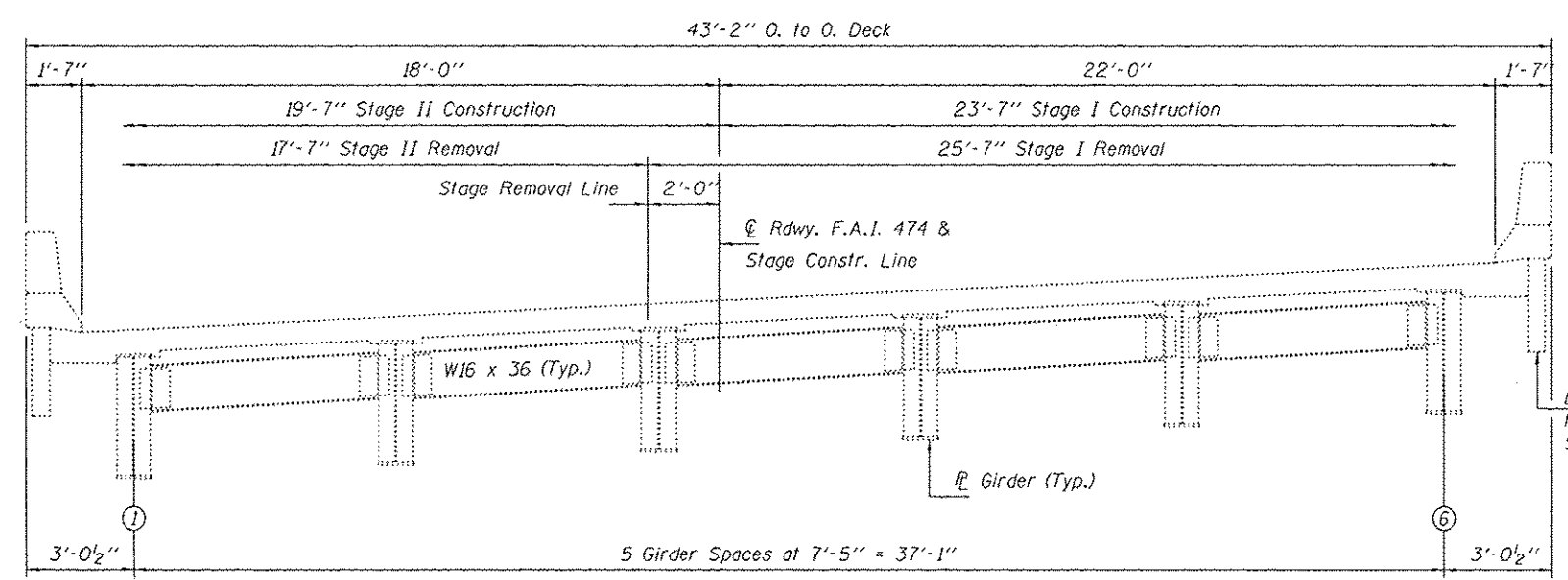
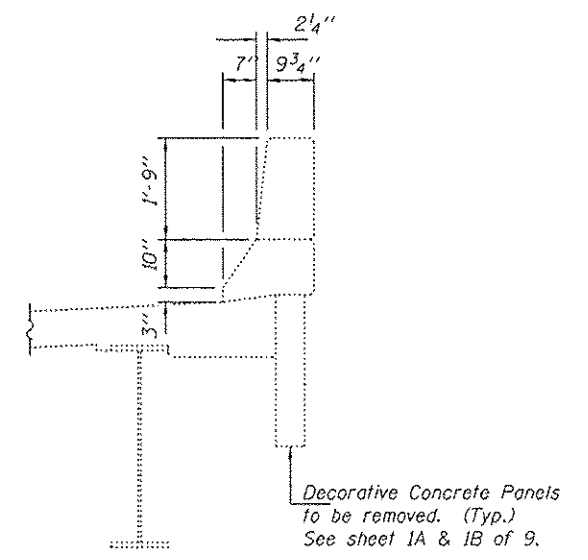
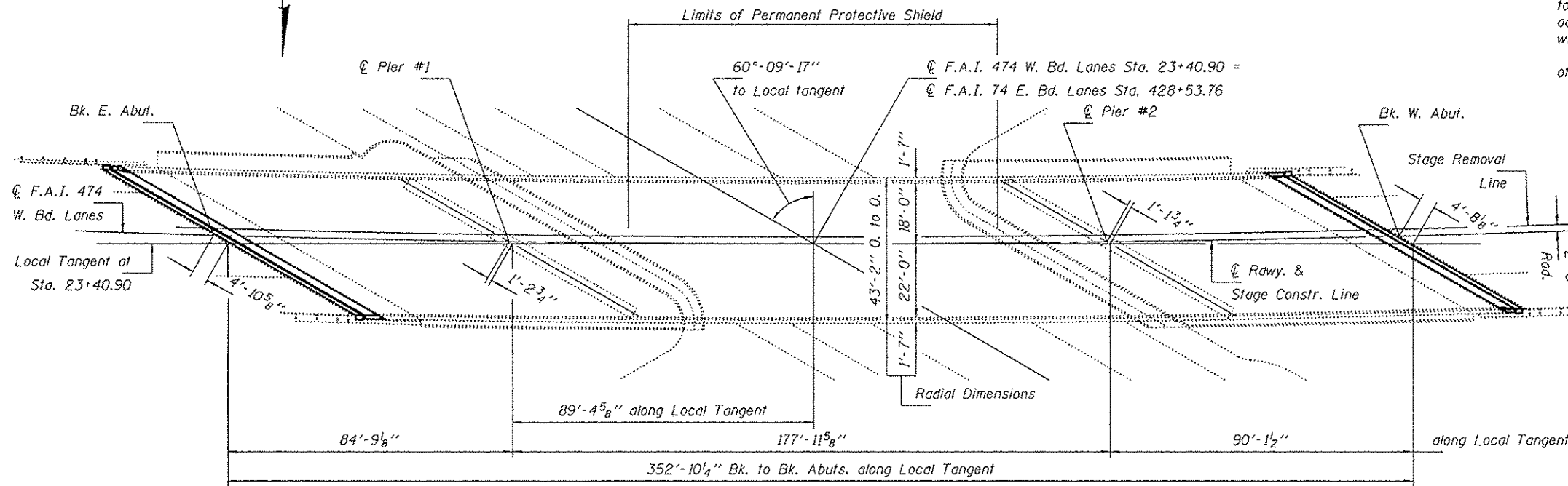
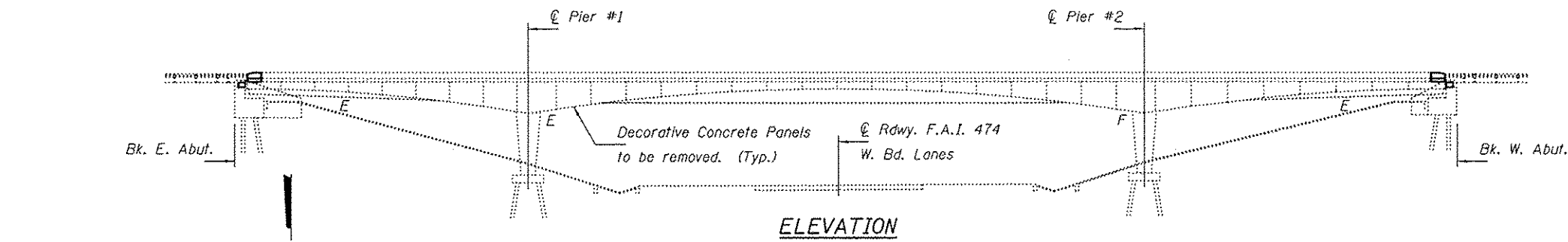
Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The deck surface shall have its final finish lined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.



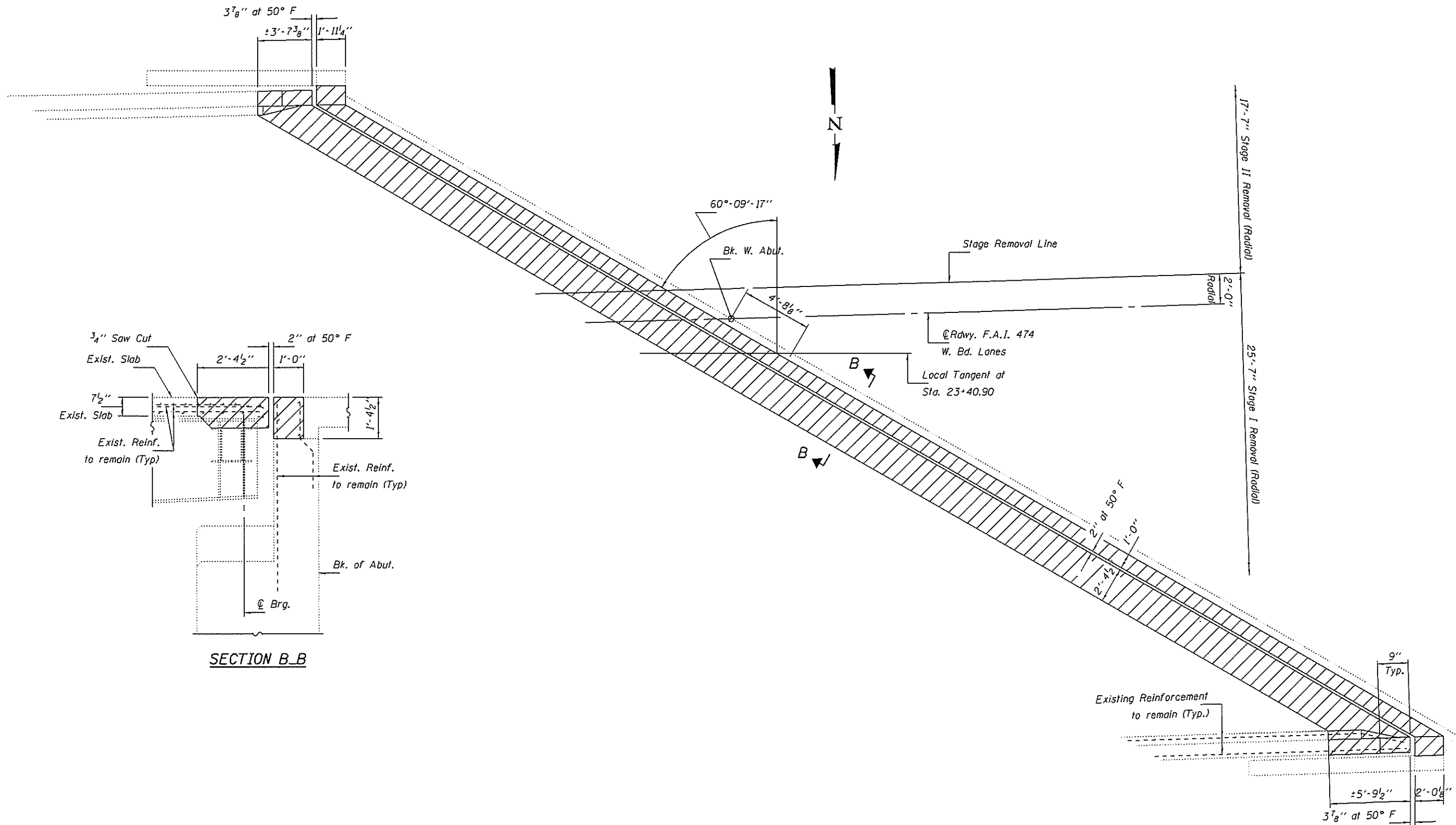
TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	24.7
Concrete Superstructure	Cu. Yd.	24.7
Preformed Joint Strip Seal	Foot	176
Reinforcement Bars, Epoxy Coated	Pound	3180
Bar Splicers	Each	28
Anchor Bolts 1"φ	Each	24
Furnishing and Erecting Structural Steel	Pound	1980
Jack & Remove Existing Bearings	Each	12
Elastomeric Bearings, Type II	Each	12
Concrete Sealer	Sq. Ft.	697
Deck Slab Repair (Partial Depth)	Sq. Yd.	13.9
Decorative Concrete Panel Removal	Sq. Ft.	3010
Protective Shield (Permanent)	Sq. Yd.	411

* On new concrete areas of deck, top, front and back of parapets only



EXPIRES 11-30-2016



SECTION B-B

JOINT REMOVAL PLAN WEST ABUTMENT

DESIGNED *SMR*
 CHECKED *CCC*
 DRAWN *J. Schneller*
 CHECKED *SMR CCC*

PASSED

J. Carl Perry
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016

REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

JOINT REMOVAL WEST ABUTMENT
 SN 090-0102

SHEET NO. 3 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6, TRS-1	TAZEWELL	245	104
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	

* 4-#5 d₁(E) bars at ±11" cts. I.F.
 * 4-#5 d(E) bars at ±11" cts. I.F.
 * 7-#4 d₃(E) bars at ±12" cts. O.F.

±1'-2⁵/₈"
 ±7" at 50° F
 ±7'-1"

* 5 x 2-#5 a₁(E) bars
 Top & Bottom of Slab
 See Sec. D-D

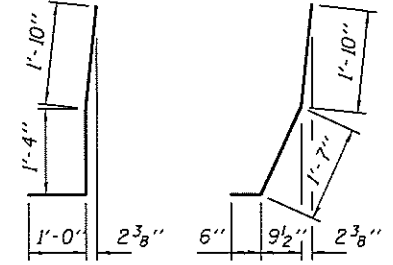
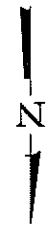
* 4 x 2-#6 h₁(E) bars
 See Sec. D-D

Bk. E. Abut.
 4 Bar Splicers for #6 bars
 See Sec. D-D

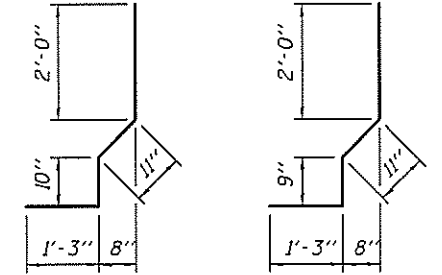
5 Bar Splicers for #5 bars
 Top & Bottom of Slab
 See Sec. D-D

⊙ Rdwy. F.A.I. 474 W. Bd. Lanes
 & Stage Construction Line
 Local Tangent at
 Sta. 23+40.90

19'-7" Stage II Construction (Radial)
 2'-0" Radial
 23'-7" Stage I Construction (Radial)



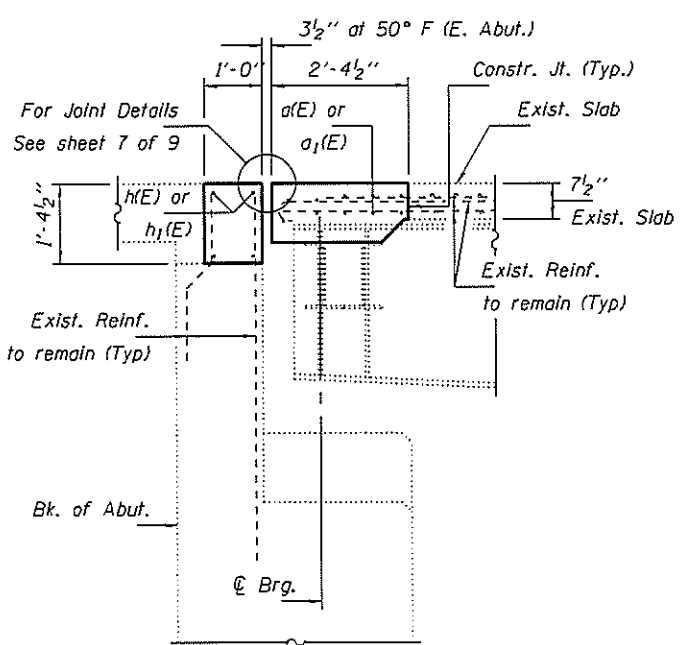
BAR d₁(E) BAR d(E)



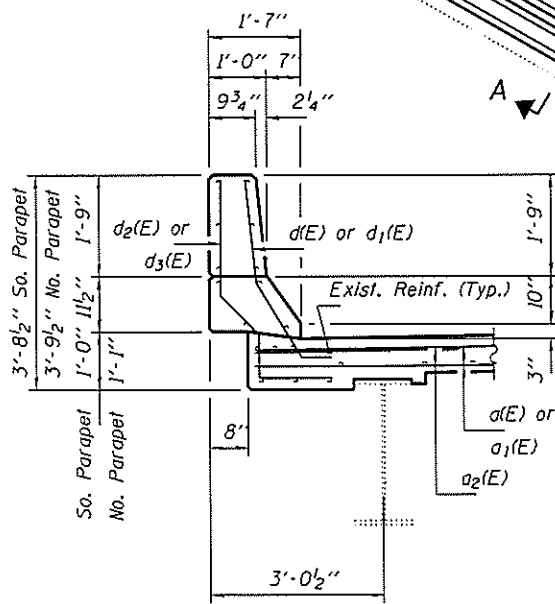
BAR d₃(E) BAR d₂(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	20	#5	23'-10"	—
a ₁ (E)	20	#5	19'-10"	—
a ₂ (E)	6	#6	4'-9"	—
d(E)	4	#5	3'-11"	J
d ₁ (E)	8	#5	4'-2"	J
d ₂ (E)	4	#4	4'-11"	J
d ₃ (E)	7	#4	5'-0"	J
h(E)	8	#6	24'-8"	—
h ₁ (E)	8	#6	20'-8"	—
Concrete Removal			Cu. Yd.	12.5
Concrete Superstructure			Cu. Yd.	12.5
Reinforcement Bars, Epoxy Coated			Lbs.	1590



SECTION A-A



SECTION THRU PARAPET

JOINT REPLACEMENT PLAN EAST ABUTMENT

* 5 x 2-#5 d(E) bars
 Top & Bottom of Slab
 See Sec. D-D

* 4 x 2-#6 h(E) bars
 See Sec. D-D

* Bend in field
 if required.

3-#6 a₂(E) bars
 Top of Slab

Existing Reinforcement
 to remain (Typ.)

MINIMUM BAR LAP
 #5 bar = 2'-5"
 #6 bar = 2'-11"

DESIGNED SMR
 CHECKED CCC
 DRAWN J. Schneller
 CHECKED SMR CCC

PASSED
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

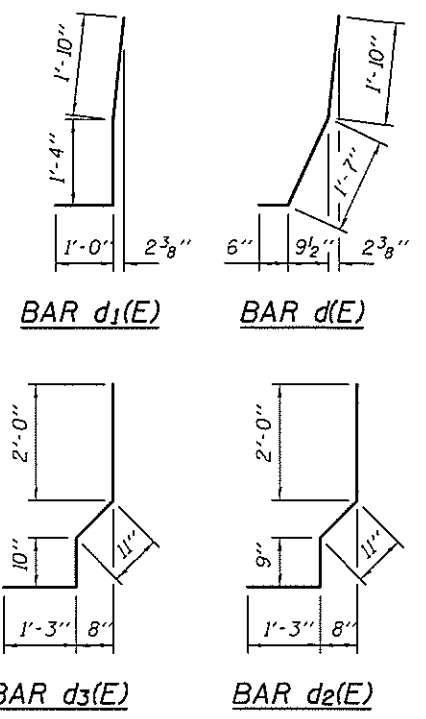
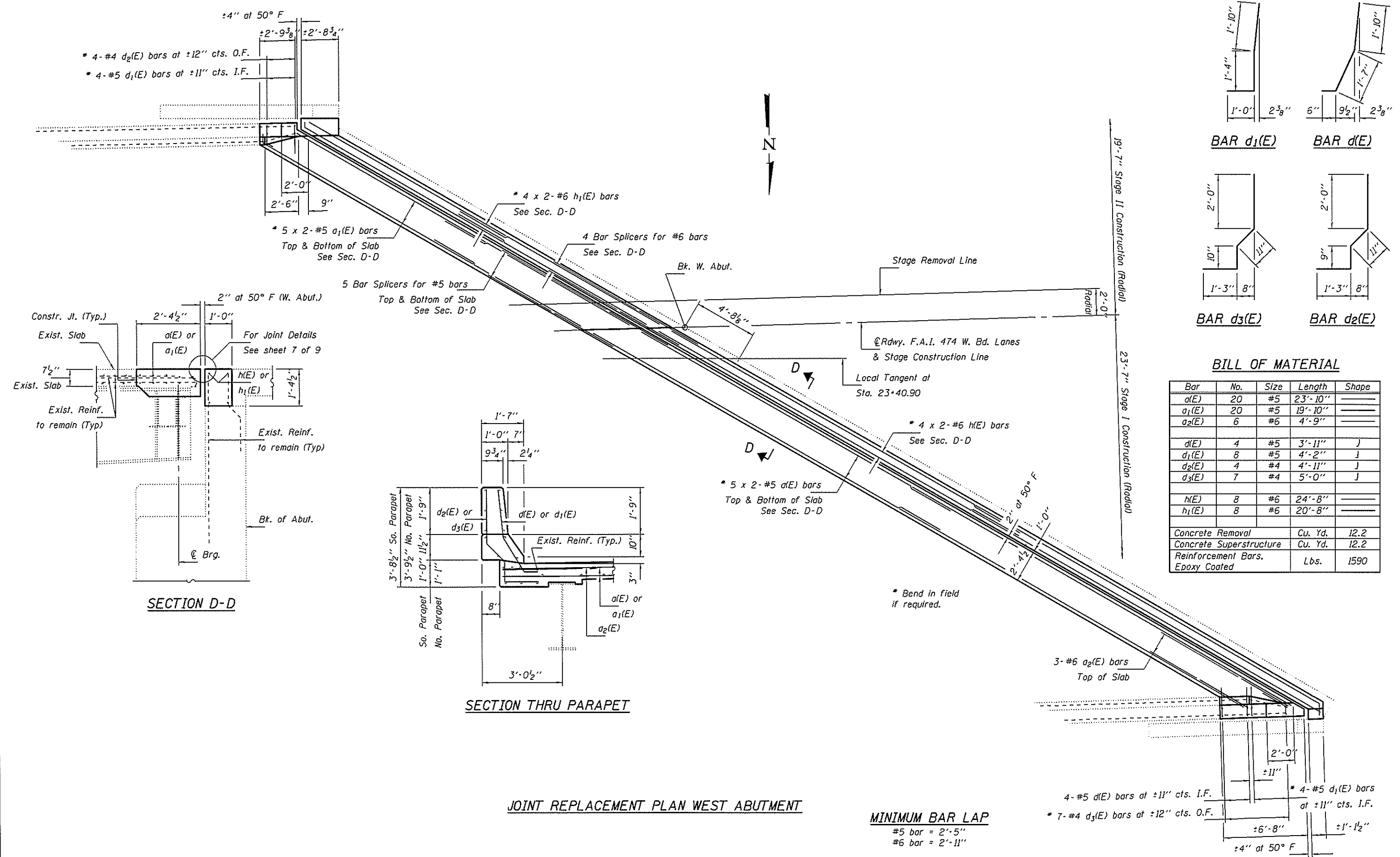
DATE JANUARY 27, 2016
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

JOINT REPLACEMENT EAST ABUTMENT
 SN 090-0102

SHEET NO. 4 OF 9 SHEETS

F.A.I. RTE. 474	SECTION 90-6, TRS-1	COUNTY TAZEWELL	TOTAL SHEETS 245	SHEET NO. 105
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



BILL OF MATERIAL

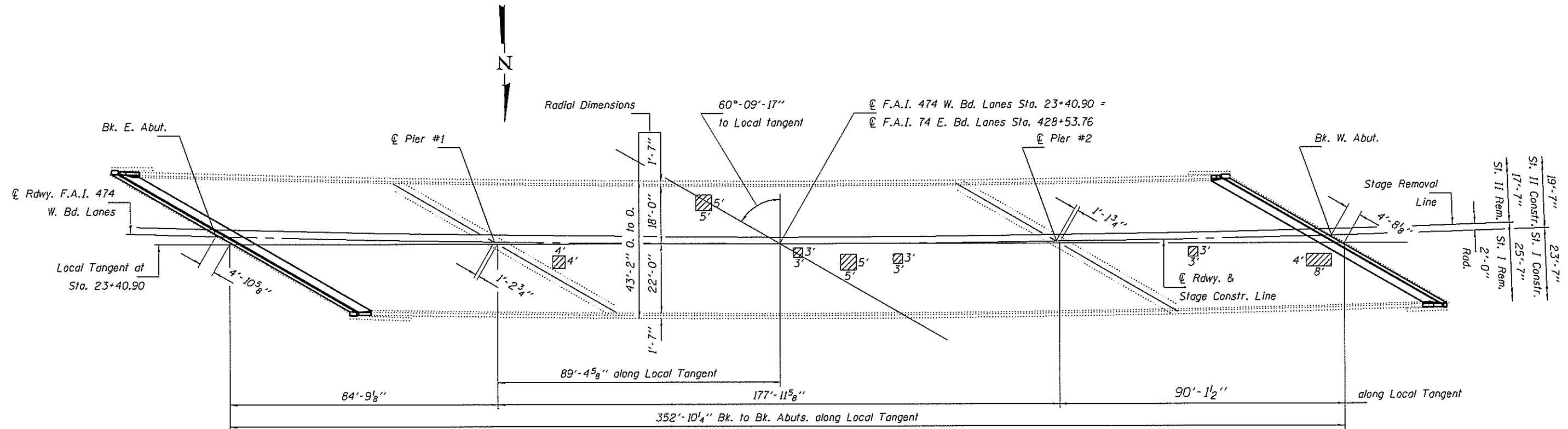
Bar	No.	Size	Length	Shape
a(E)	20	#5	23'-10"	—
a1(E)	20	#5	19'-10"	—
a2(E)	6	#6	4'-9"	—
d(E)	4	#5	3'-11"	J
d1(E)	8	#5	4'-2"	J
d2(E)	4	#4	4'-11"	J
d3(E)	7	#4	5'-0"	J
h(E)	8	#6	24'-8"	—
h1(E)	8	#6	20'-8"	—
Concrete Removal			Cu. Yd.	12.2
Concrete Superstructure			Cu. Yd.	12.2
Reinforcement Bars, Epoxy Coated			Lbs.	1590

SECTION D-D



SECTION THRU PARAPET

JOINT REPLACEMENT PLAN WEST ABUTMENT

MINIMUM BAR LAP
 #5 bar = 2'-5"
 #6 bar = 2'-11"




PLAN

-  - Deck Slab Repair (Partial).
-  - Concrete Superstructure. See sheets 4 & 5 of 9 for summarized repair quantity.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Deck Slab Repair (Partial Depth)	Sq. Yd.	13.9

DESIGNED *SMR*
 CHECKED *CCC*
 DRAWN *J. Schneller*
 CHECKED *SMR CCC*

PASSED

 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016
 REVISED
 REVISED

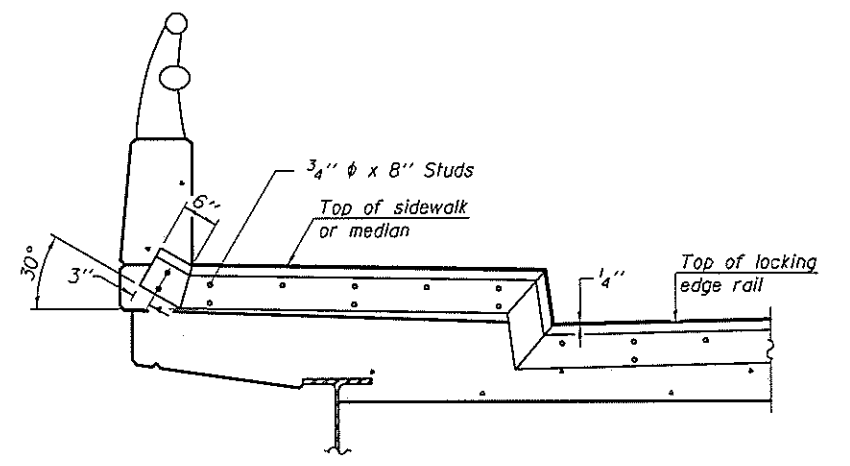
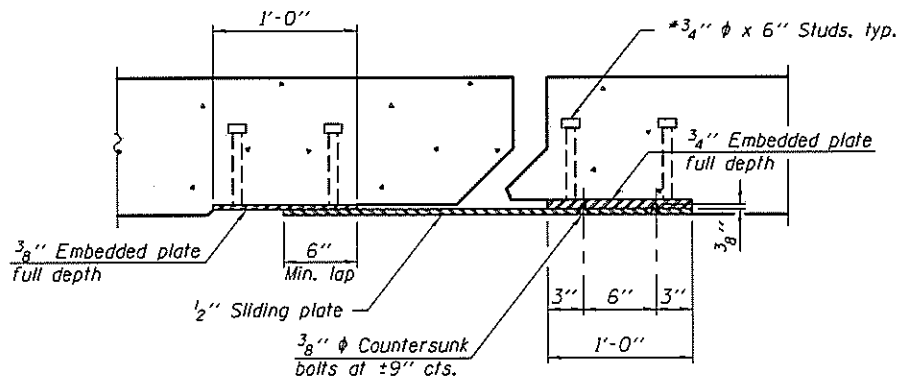
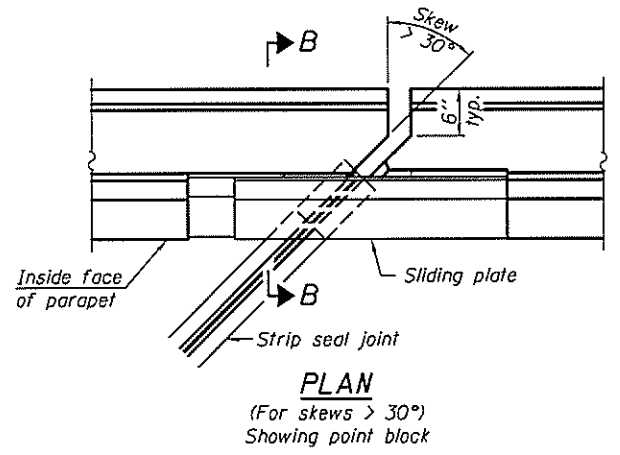
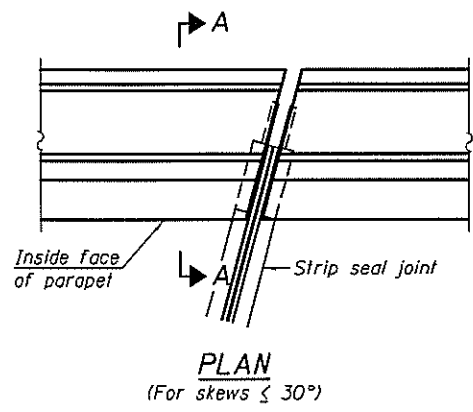
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DECK SLAB REPAIRS
 SN 090-0102**

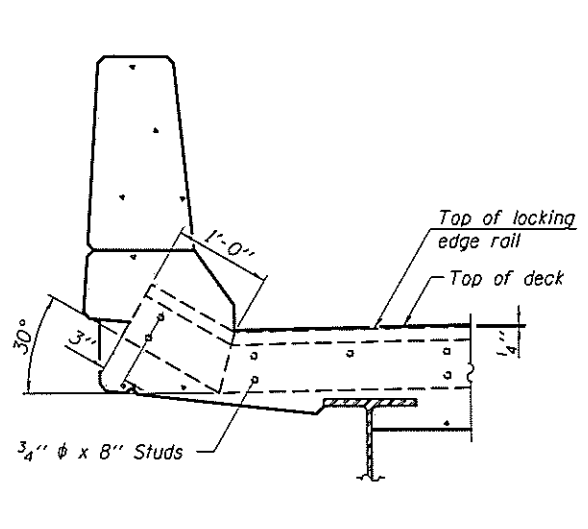
SHEET NO. 6 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6.TRS-1	TAZEWELL	245	101

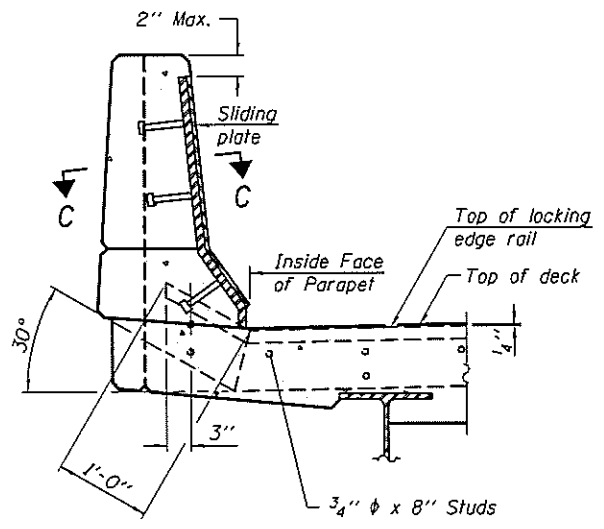
CONTRACT NO. 68606
 ILLINOIS FED. AID PROJECT



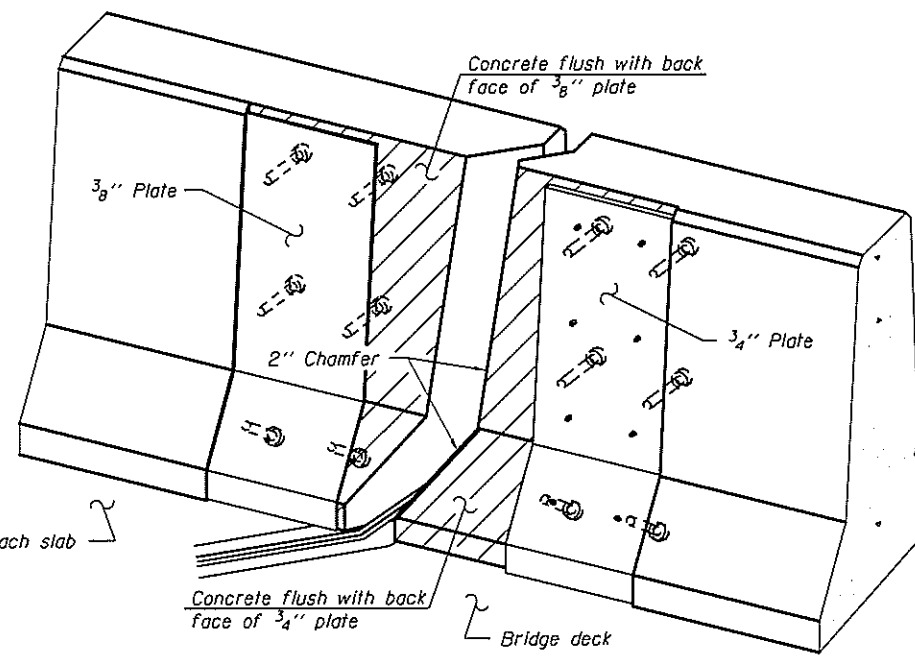
TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN
 Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



SECTION A-A

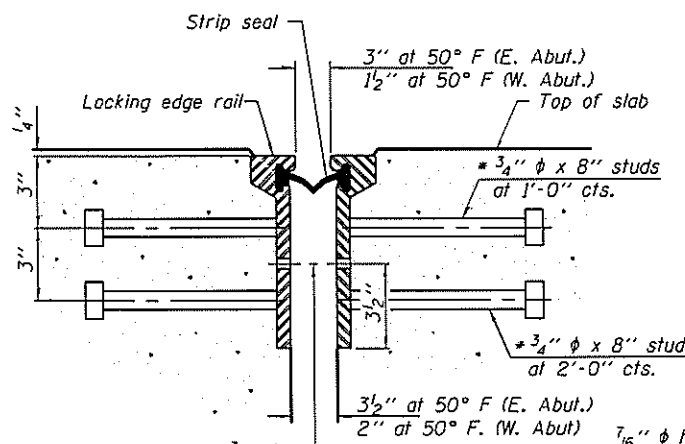


SECTION B-B

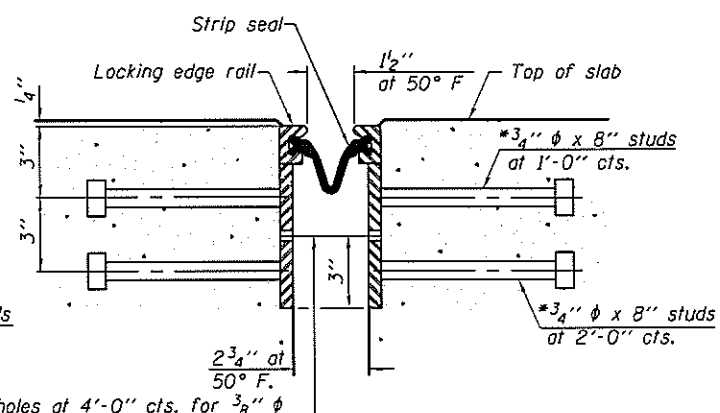


TRIMETRIC VIEW (Showing back plates only)

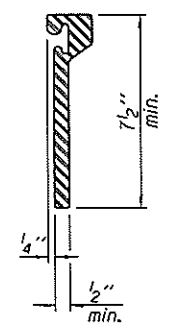
Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
 The manufacturer's recommended installation methods shall be followed.
 The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments shall be 3/16". Sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.
 Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.



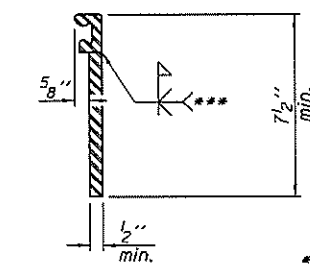
SECTION THRU ROLLED RAIL JOINT



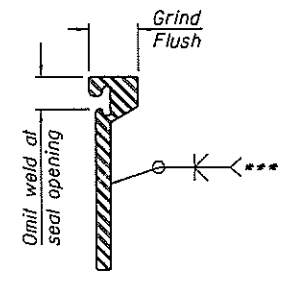
SECTION THRU WELDED RAIL JOINT



ROLLED EXTRUDED RAIL



WELDED RAIL



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
 Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	176

EJ-SSJ

1-27-12

DESIGNED SMR	CHECKED CCC
DRAWN J. Schneller	CHECKED SMR CCC

PASSED
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

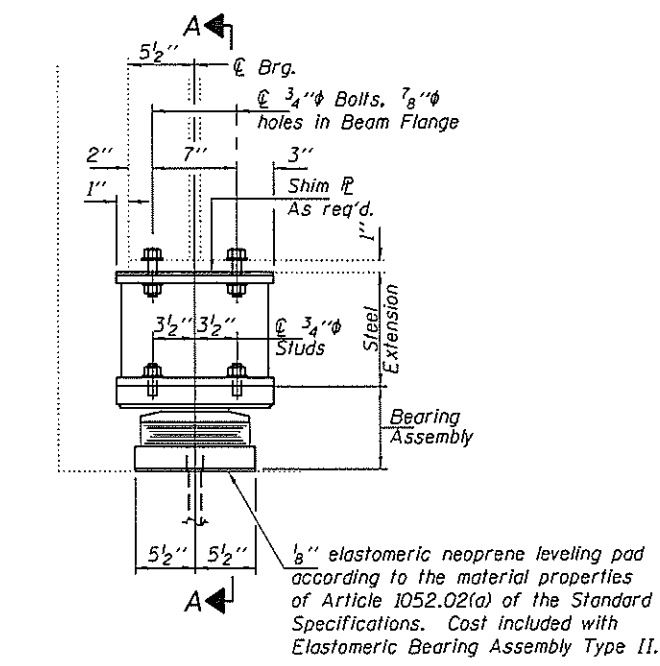
DATE JANUARY 27, 2016	REVISED
	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

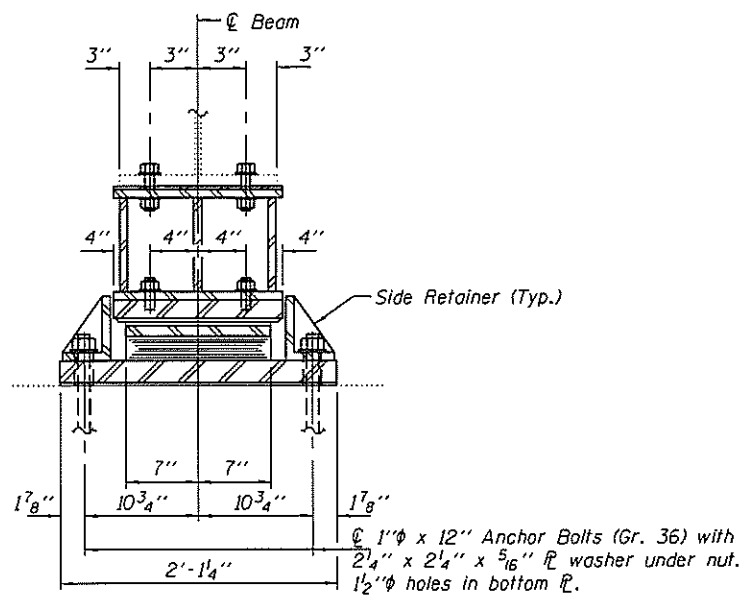
PREFORMED JOINT STRIP SEAL
 SN 090-0102

SHEET NO. 7 OF 9 SHEETS

F.A.J. RTE. 474	SECTION 90-6.7RS-1	COUNTY TAZEWELL	TOTAL SHEETS 245	SHEET NO. 109
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				



ELEVATION AT ABUTMENT



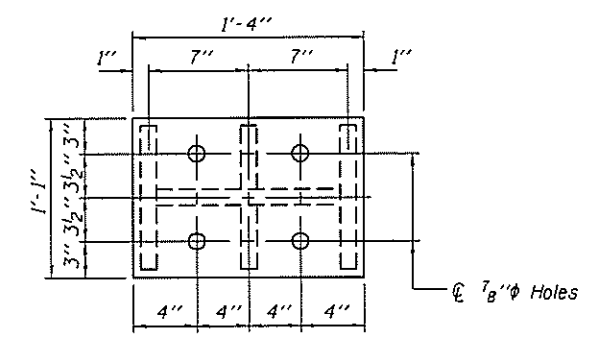
SECTION A-A

TYPE II TFE ELASTOMERIC EXP. BRG.

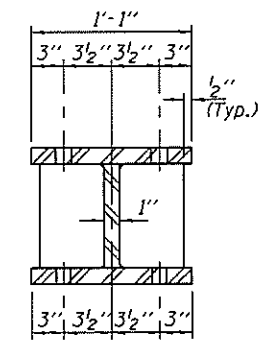
BEAM REACTIONS

R _P	(K)	21.3
R _E	(K)	40.5
Imp.	(K)	9.7
R (Total)	(K)	71.5

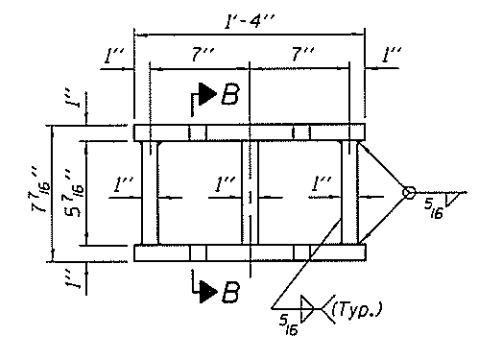
Notes:
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
 Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Min. jack capacity = 35 Tons.
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (F_y=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Anchor bolts for Type II bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type II.
 The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.
 Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.



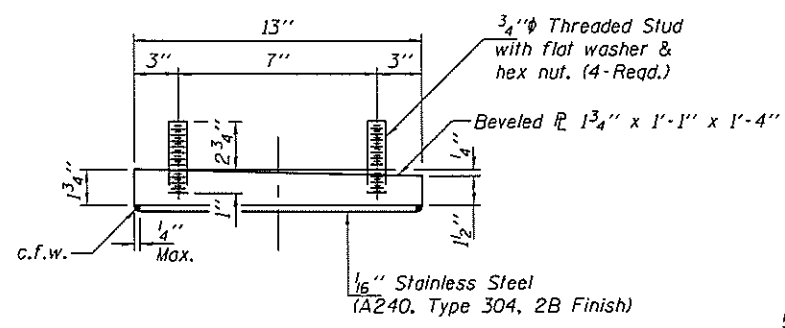
PLAN TOP AND BOTTOM PLATE



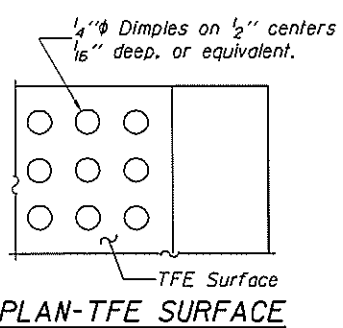
SECTION B-B



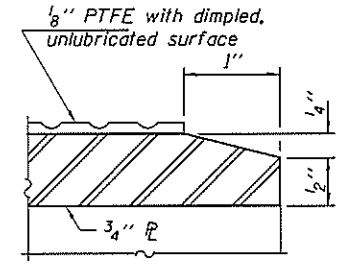
STEEL EXTENSION DETAIL



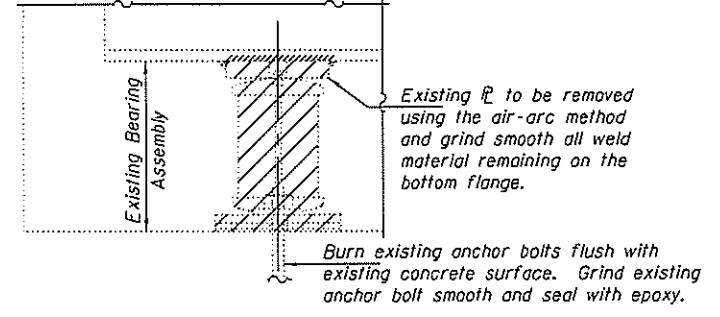
TOP BEARING ASSEMBLY



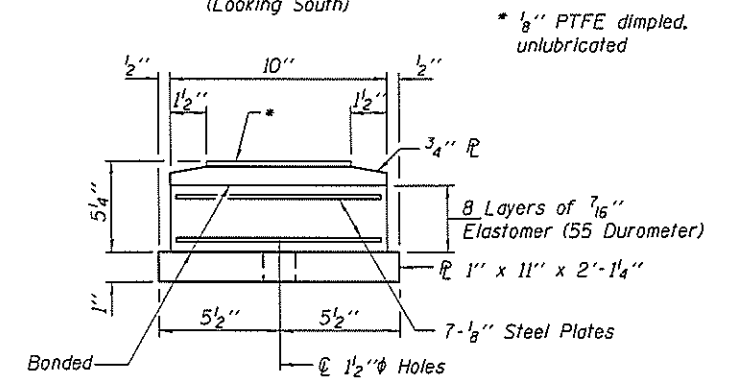
PLAN-TFE SURFACE



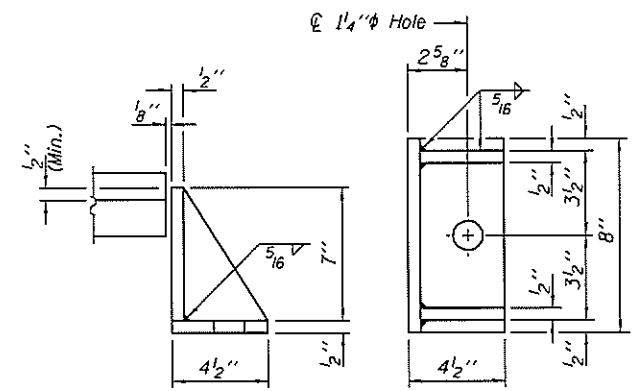
SECTION THRU TFE



EXISTING BEARING REMOVAL DETAIL

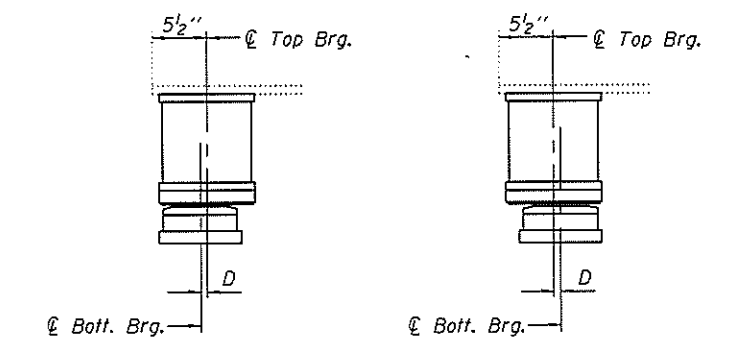


BOTTOM BEARING ASSEMBLY



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly Type II	Each	6
Jack and Remove Existing Bearings	Each	6
Furnishing and Erecting Structural Steel	Pound	1160
Anchor Bolts 1"φ	Each	12

TYII/REPS 12-03-2008

DESIGNED	SMR
CHECKED	CCC
DRAWN	J. Schneller
CHECKED	SMR CCC

PASSED
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

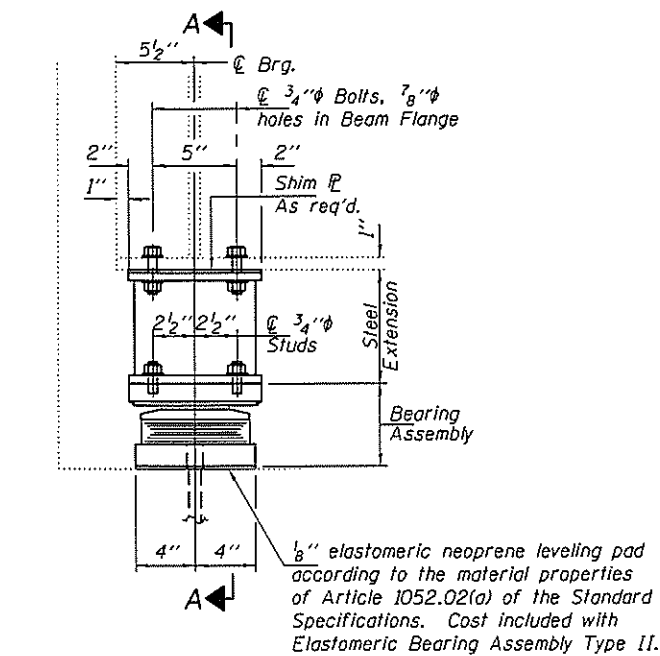
DATE	JANUARY 27, 2016
REVISED	
REVISED	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

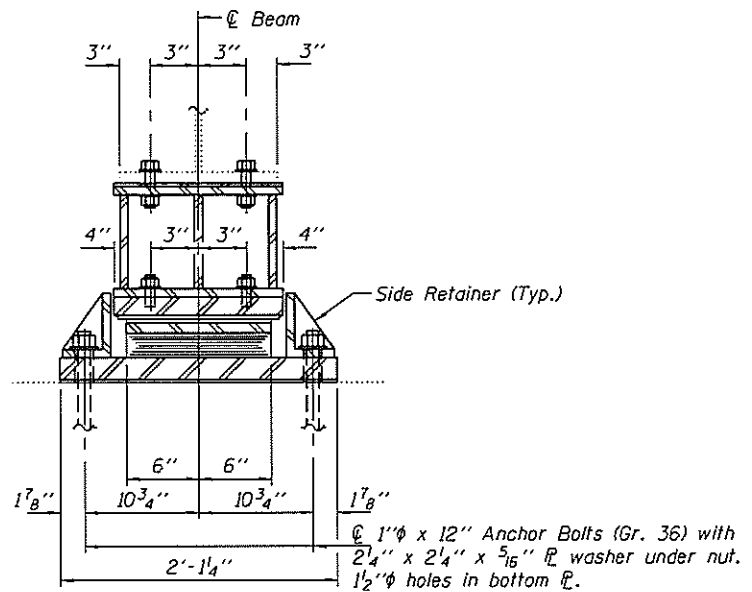
EAST ABUTMENT BEARING REPLACEMENT DETAILS
 SN 090-0102

SHEET NO. TA OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZENELL	245	108A
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				



ELEVATION AT ABUTMENT



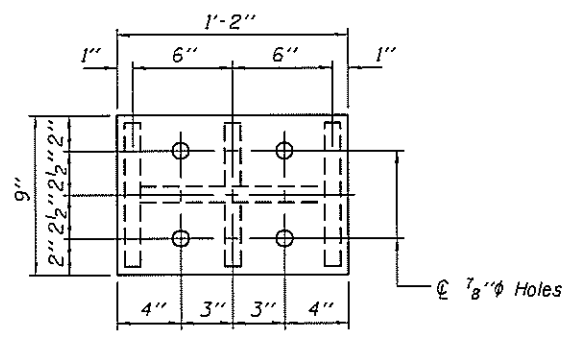
SECTION A-A

TYPE II TFE ELASTOMERIC EXP. BRG.

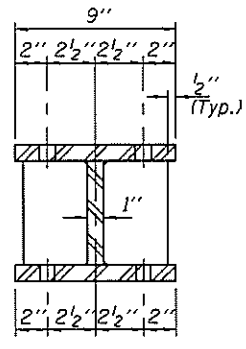
BEAM REACTIONS

RP	(K)	21.3
RL	(K)	40.5
Imp.	(K)	9.7
R (Total)	(K)	71.5

Notes:
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
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 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Anchor bolts for Type II bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type II.
 The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.
 Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

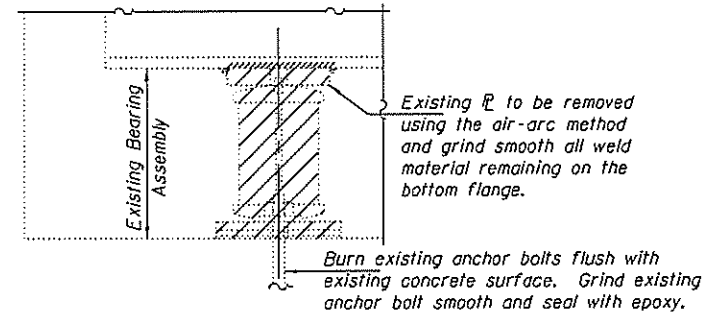


PLAN TOP AND BOTTOM PLATE

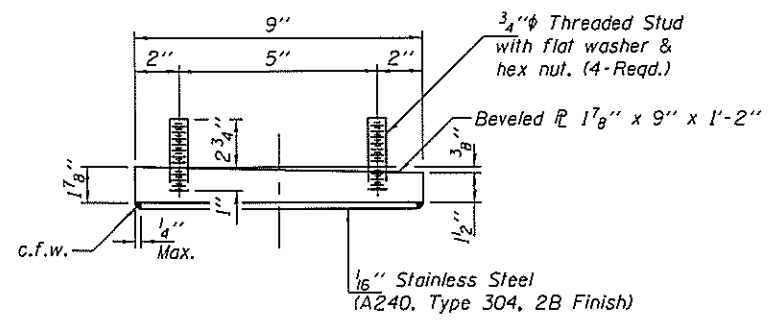


SECTION B-B

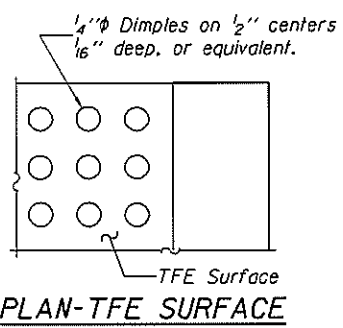
STEEL EXTENSION DETAIL



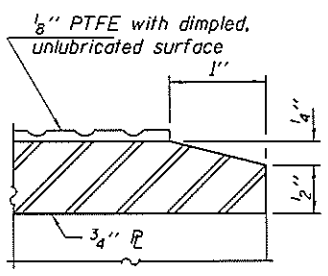
EXISTING BEARING REMOVAL DETAIL
 Cost included with Jack and Remove Existing Bearings.



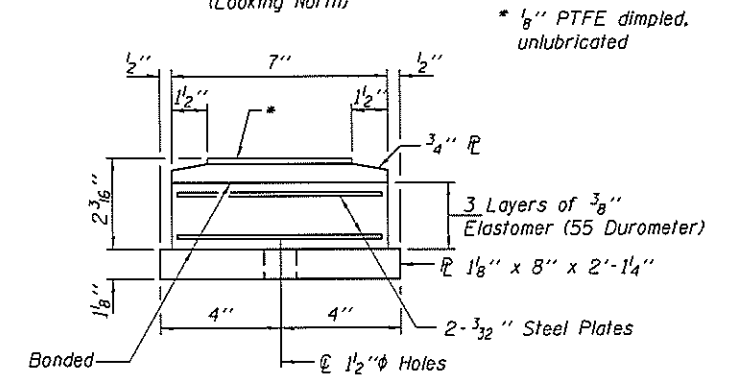
TOP BEARING ASSEMBLY
 (Looking North)



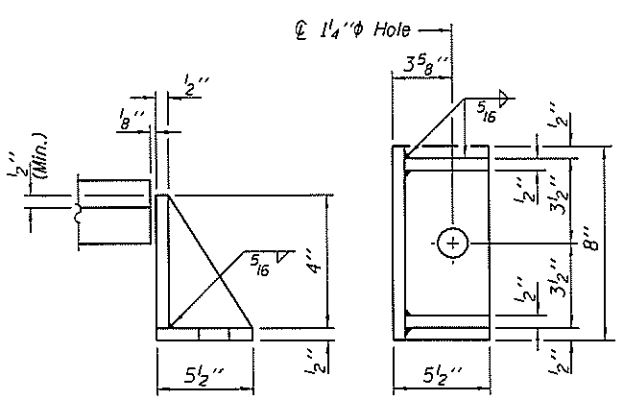
PLAN-TFE SURFACE



SECTION THRU TFE

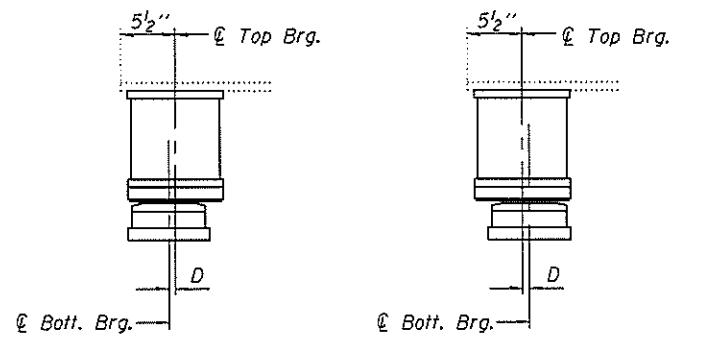


BOTTOM BEARING ASSEMBLY



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly Type II	Each	6
Jack and Remove Existing Bearings	Each	6
Furnishing and Erecting Structural Steel	Pound	820
Anchor Bolts 1" φ	Each	12

TYII/REPS 12-03-2008

DESIGNED	SMR
CHECKED	CCC
DRAWN	J. Schneller
CHECKED	SMR CCC

PASSED
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

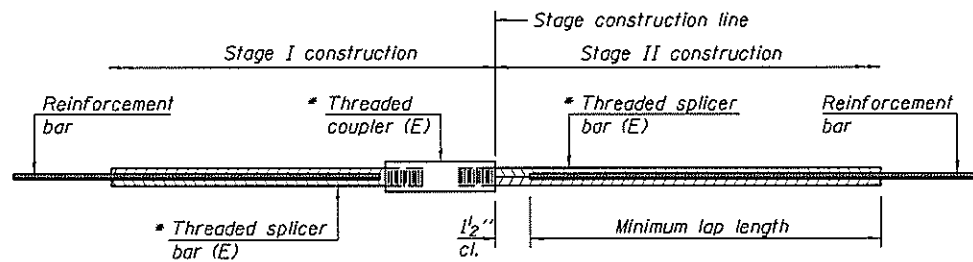
DATE	JANUARY 27, 2016
REVISED	
REVISED	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT BEARING REPLACEMENT DETAILS
 SN 090-0102

SHEET NO. 78 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6, TRS-1	TAZEWELL	245	1088
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				

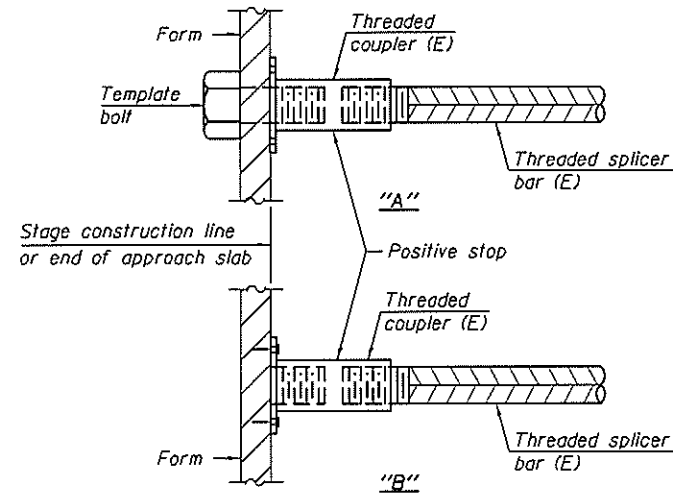


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

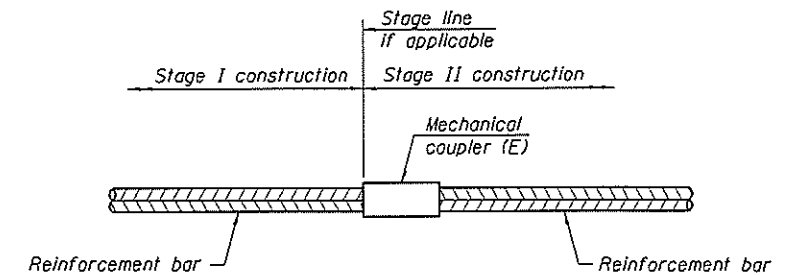
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
E. Abut. Hatch Block	#6	4	4'-0"
E. End of Deck	#5	10	3'-6"
W. Abut. Hatch Block	#6	4	4'-0"
W. End of Deck	#5	10	3'-6"



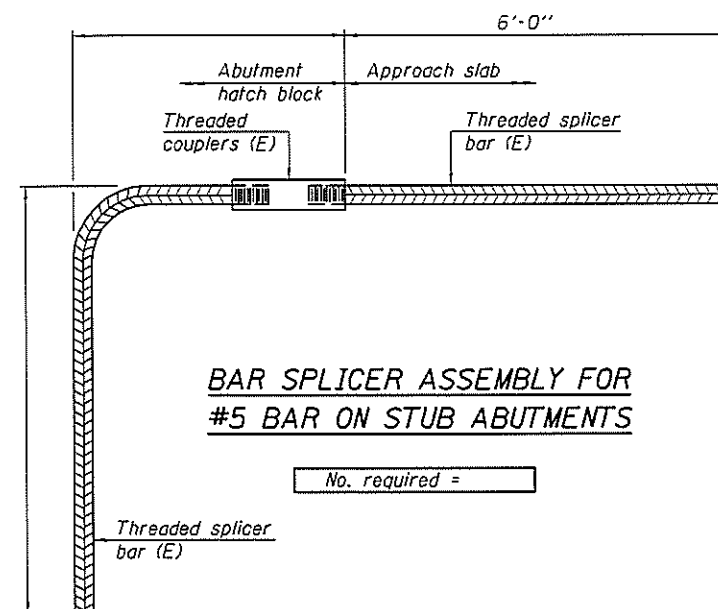
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 6-8-15

DESIGNED SMR
 CHECKED CCC
 DRAWN J. Schneller
 CHECKED SMR CCC

PASSED

 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

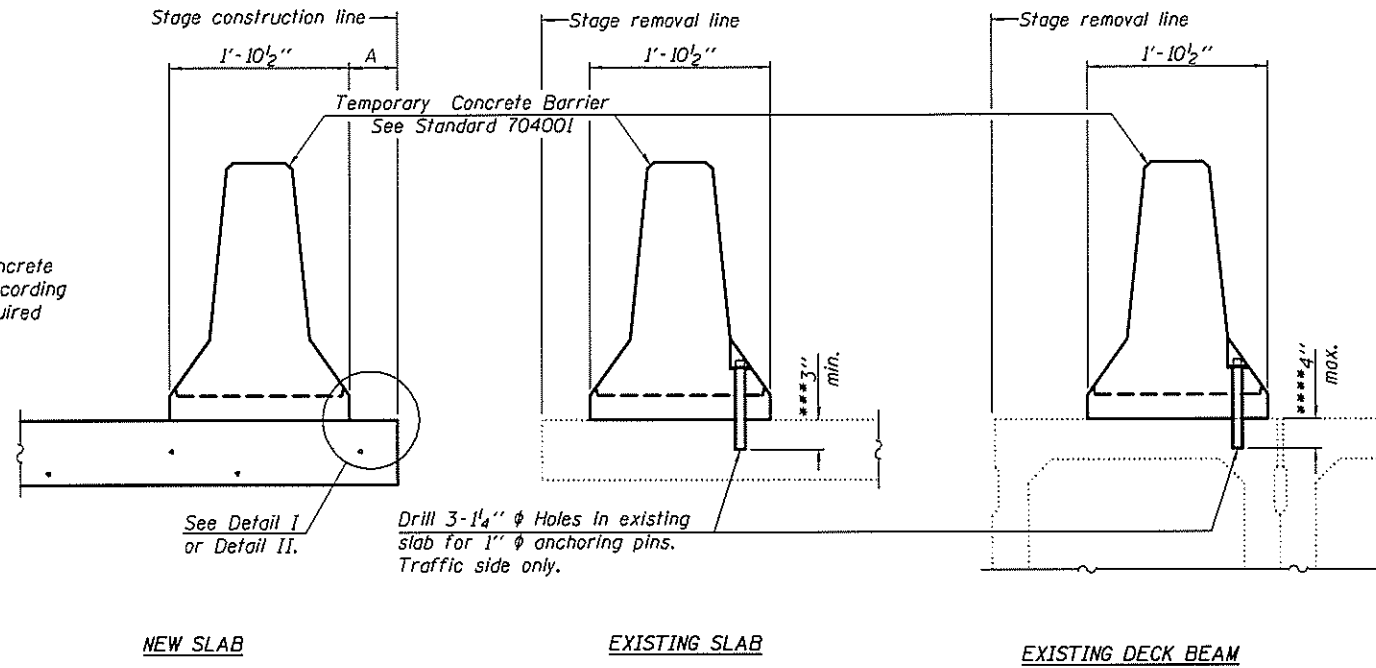
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 SN 090-0102

SHEET NO. 8 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZEWELL	245	109

CONTRACT NO. 68606
 ILLINOIS FED. AID PROJECT

When "A" is 3'-1" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-1".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

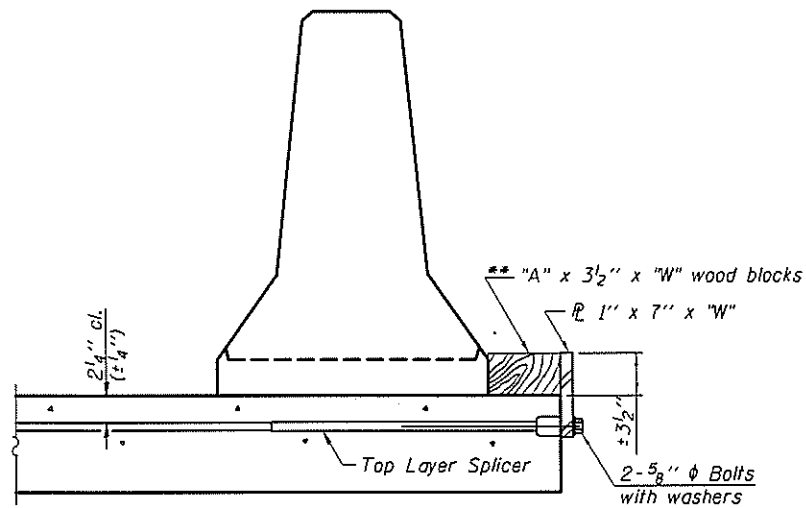
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel \bar{R} to the top layer of couplers with 2- $\frac{5}{8}$ " ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel \bar{R} to the concrete slab or concrete wearing surface with 2- $\frac{5}{8}$ " ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

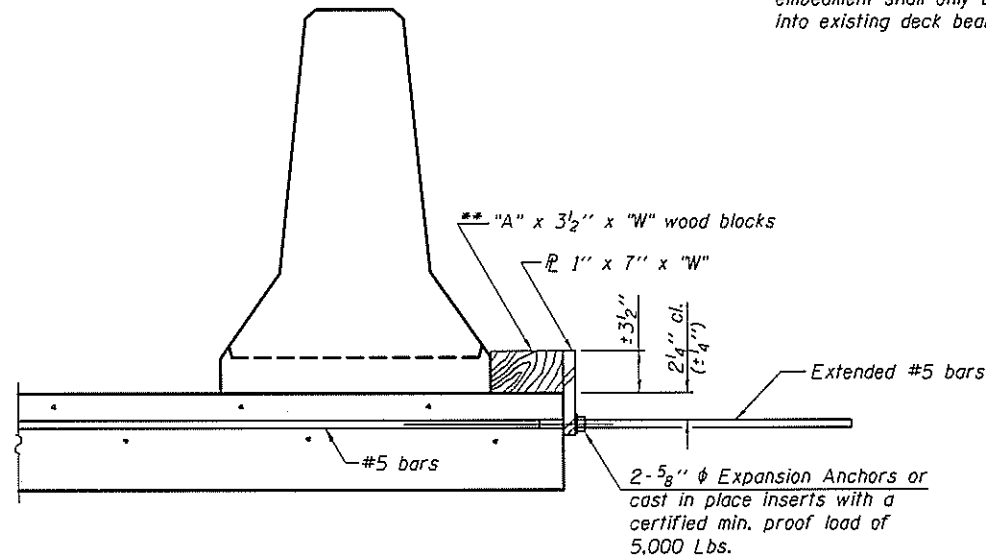
Cost of retainer assembly is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

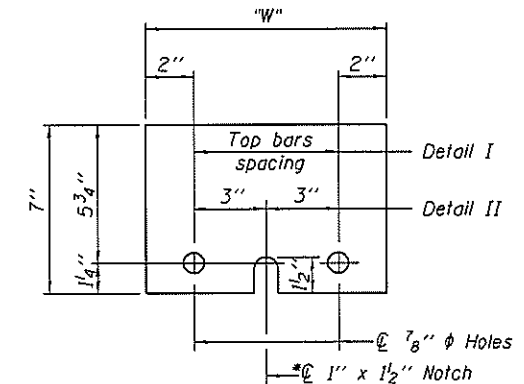
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{R} 1" x 7" x "W"

* Required only with Detail II

RETAINER ASSEMBLY

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

R-27

1-12-15

DESIGNED SMR	DATE JANUARY 27, 2016	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION SN 090-0102		F.A.J. RTE. 474	SECTION 90-5.7RS-1	COUNTY TAZEWELL	TOTAL SHEETS 245	SHEET NO. 110
CHECKED CCC	REVISOR	ACTING ENGINEER OF BRIDGES AND STRUCTURES		SHEET NO. 9 OF 9 SHEETS		CONTRACT NO. 68606		ILLINOIS FED. AID PROJECT		
DRAWN J. Schneller	REVISOR	PASSED								
CHECKED SMR CCC	REVISOR									

GENERAL NOTES

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

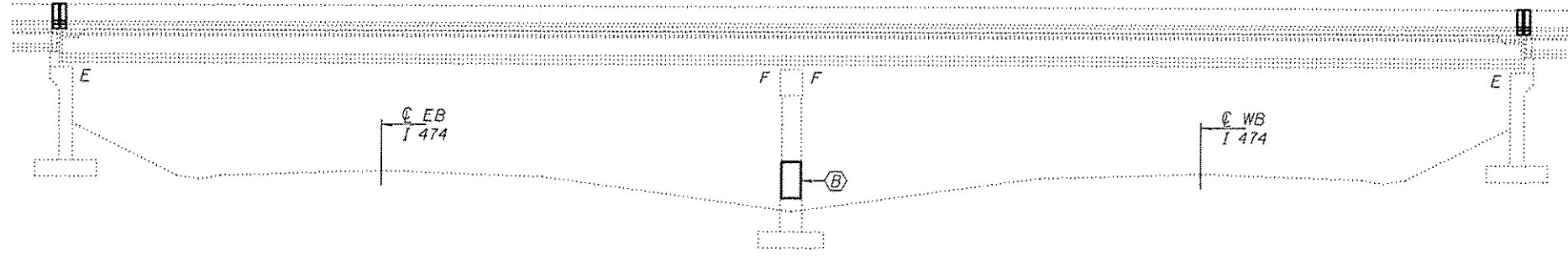
Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

Reinforcement bars designated (E) shall be epoxy coated.

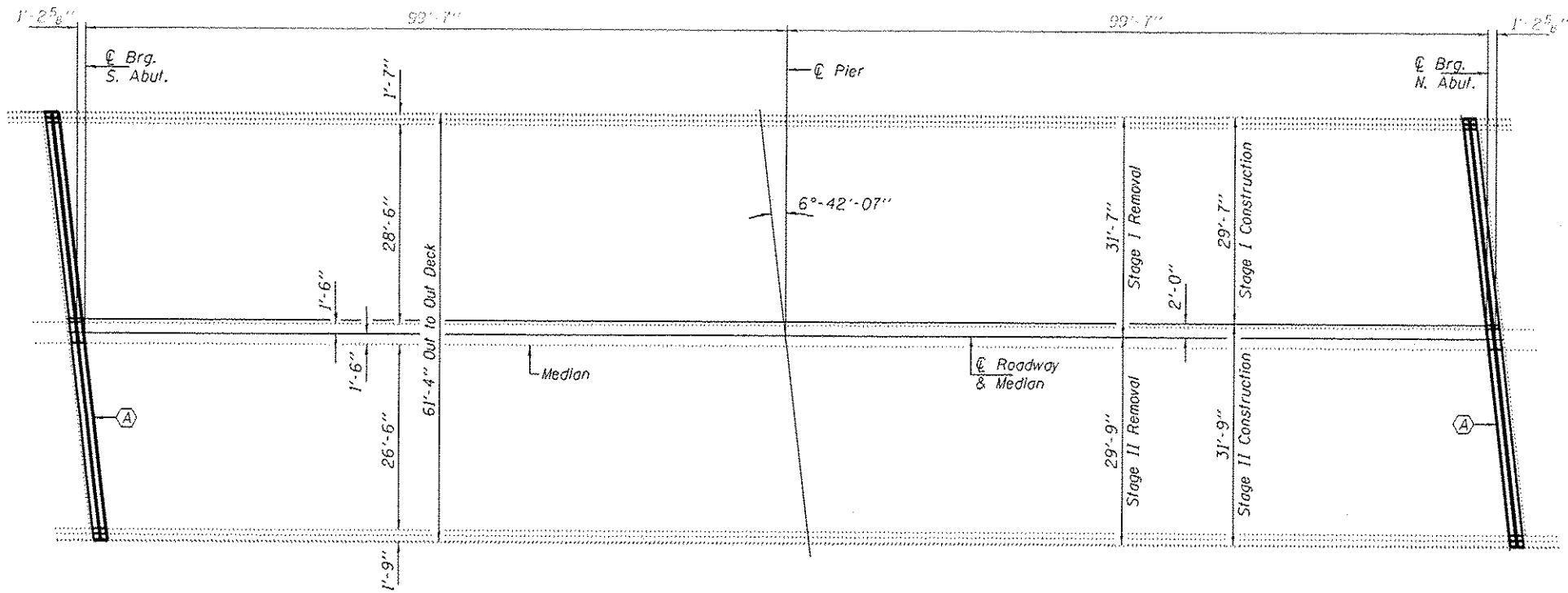
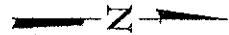
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The deck surface shall have its final finish lined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beam.



ELEVATION



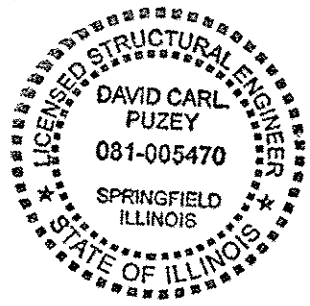
PLAN

- (A) - Joint replacement
- (B) - Crashwall extension

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	5.2
Concrete Superstructure	Cu. Yd.	5.2
Concrete Structures	Cu. Yd.	20.5
Preformed Joint Strip Seal	Foot	126
Reinforcement Bars, Epoxy Coated	Pound	2530
Bar Splicers	Each	8
* Concrete Sealer	Sq. Ft.	242

* New concrete areas of deck, top, front, and back of parapet only.



EXPIRES 11-30-2016

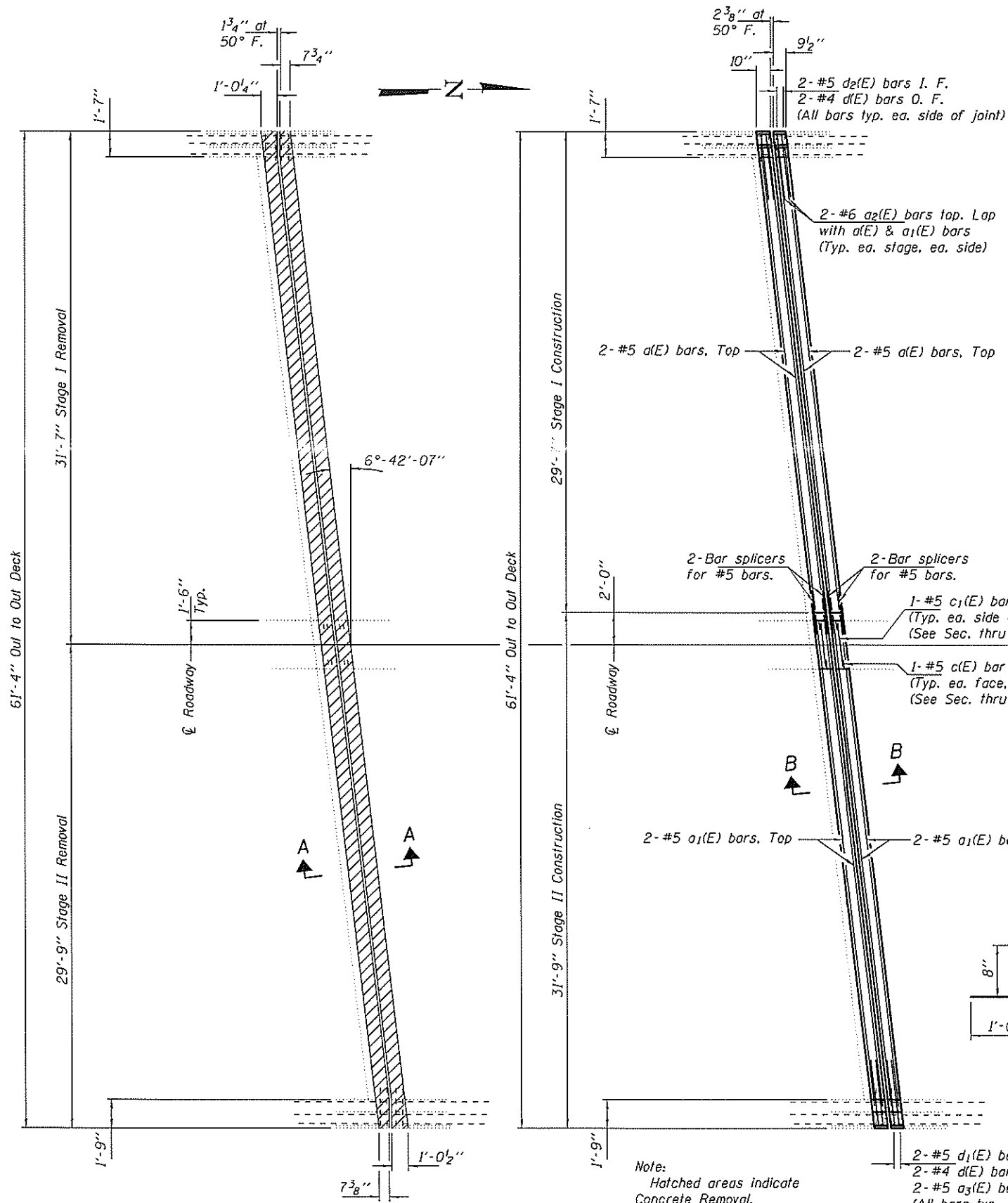
DESIGNED <i>David Carl Puze</i>	DATE JANUARY 27, 2016
CHECKED <i>Stephan M. Ryan</i>	REVISED
DRAWN <i>baliva</i>	REVISED
CHECKED <i>ccc smr</i>	

David Carl Puze
ACTING ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND ELEVATION
PINECREST DR. OVER FAI 474
SN 090-0103

F.A.I. RTE. 474	SECTION 90-6.1RS-1	COUNTY TAZEWELL	TOTAL SHEETS 245	SHEET NO. 111
CONTRACT NO. 68606			[ILLINOIS] FED. AID PROJECT	



CONCRETE REMOVAL PLAN

CONCRETE REPLACEMENT PLAN

Note:
Hatched areas indicate
Concrete Removal.

South abutment shown,
North abutment similar.

2- #5 d₂(E) bars I. F.
2- #4 d(E) bars O. F.
(All bars typ. ea. side of joint)

2- #6 a₂(E) bars top. Lap
with a(E) & a₁(E) bars
(Typ. ea. stage, ea. side)

2- #5 a(E) bars, Top

2-Bar splicers
for #5 bars.

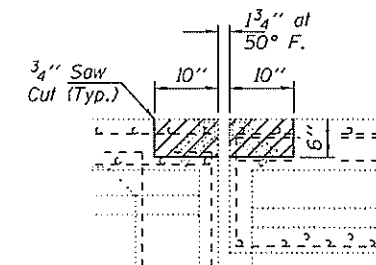
2-Bar splicers
for #5 bars.

1- #5 c₁(E) bar
(Typ. ea. side of jt.)
(See Sec. thru Median)

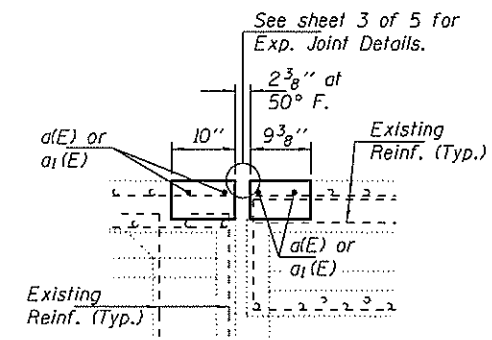
1- #5 c(E) bar
(Typ. ea. face, ea. side of jt.)
(See Sec. thru Median)

2- #5 a₁(E) bars, Top

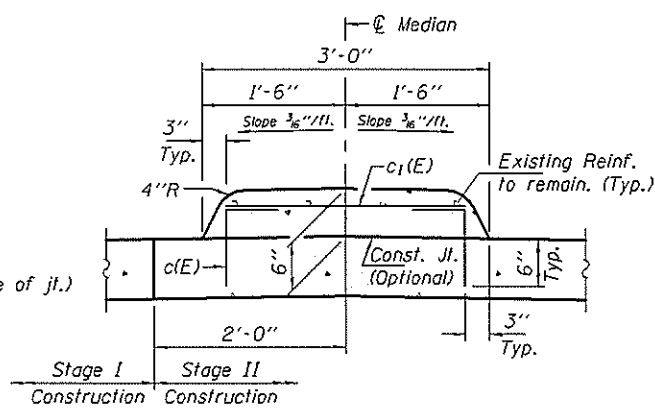
2- #5 d₁(E) bars I. F.
2- #4 d(E) bars O. F.
2- #5 a₃(E) bars.
(All bars typ. ea. side of joint)



SECTION A-A

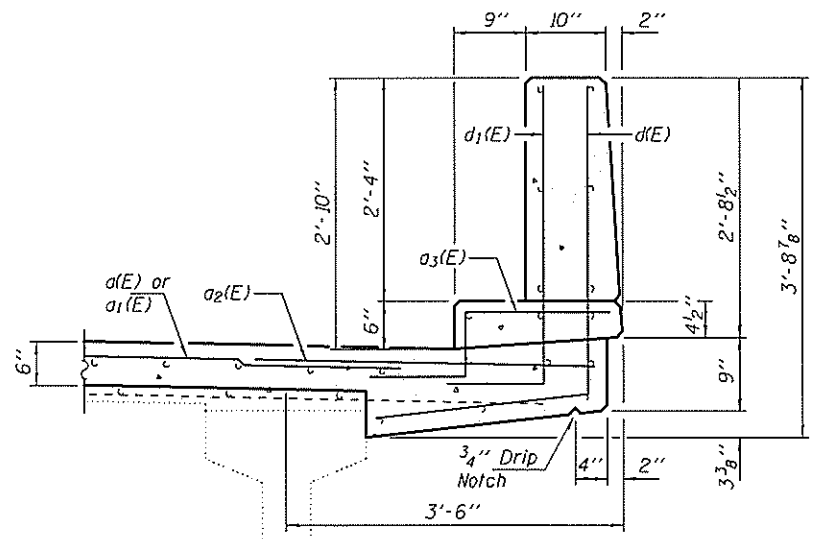


SECTION B-B

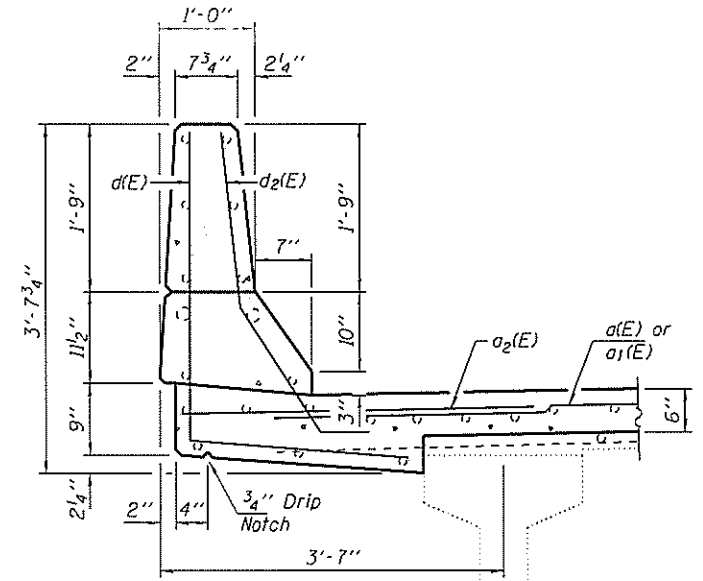


SECTION THRU MEDIAN

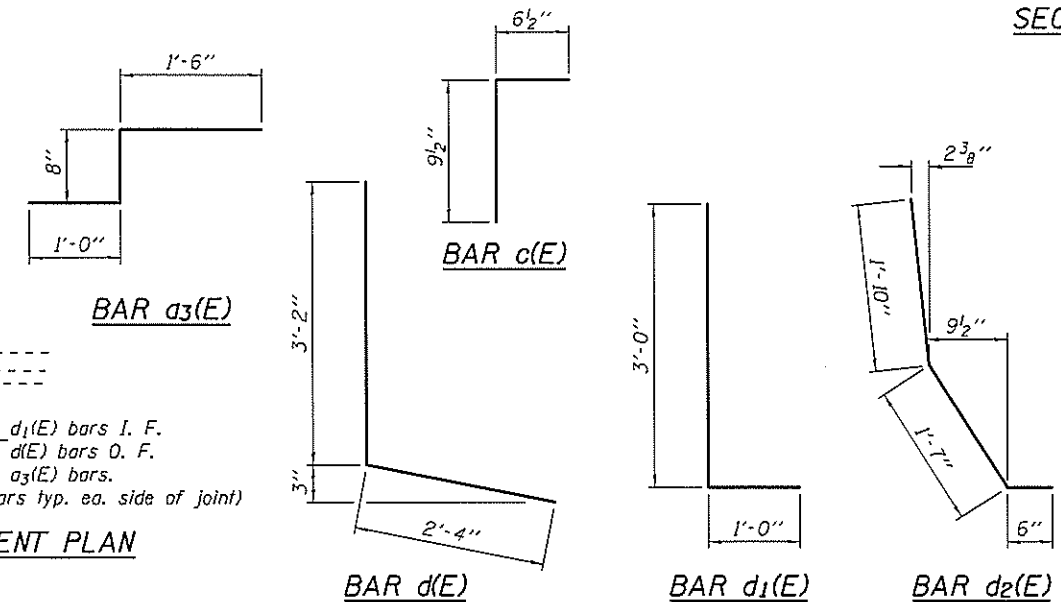
(Looking North)
Deck reinforcement not shown for clarity.



SECTION THRU EAST PARAPET



SECTION THRU WEST PARAPET



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	8	#5	29'-0"	—
a ₁ (E)	8	#5	31'-0"	—
a ₂ (E)	16	#6	4'-0"	—
a ₃ (E)	16	#5	3'-2"	—
c(E)	8	#5	1'-4"	┌
c ₁ (E)	4	#5	2'-6"	—
d(E)	16	#4	5'-6"	L
d ₁ (E)	8	#5	4'-0"	L
d ₂ (E)	8	#5	3'-11"	L
Concrete Removal			Cu. Yd.	5.2
Concrete Superstructure			Cu. Yd.	5.2
Reinforcement Bars, Epoxy Coated			Pound	800

DESIGNED CCC
CHECKED SMR
DRAWN baliva
CHECKED CCC SMR

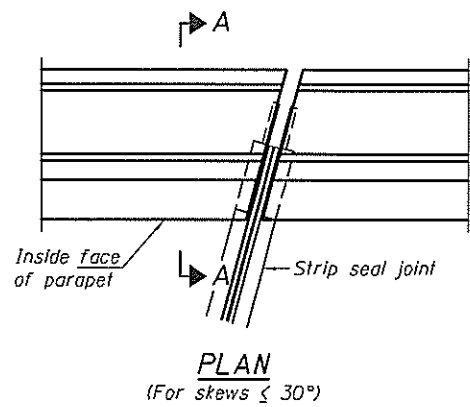
PASSED
ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016
REVISED
REVISED

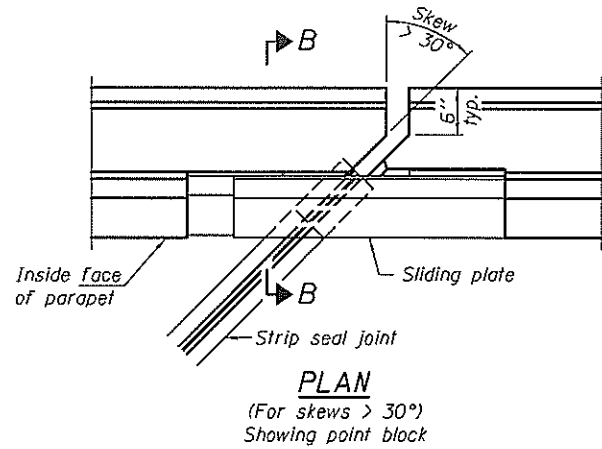
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REPAIR DETAILS
SN 090-0103
SHEET NO. 2 OF 5 SHEETS

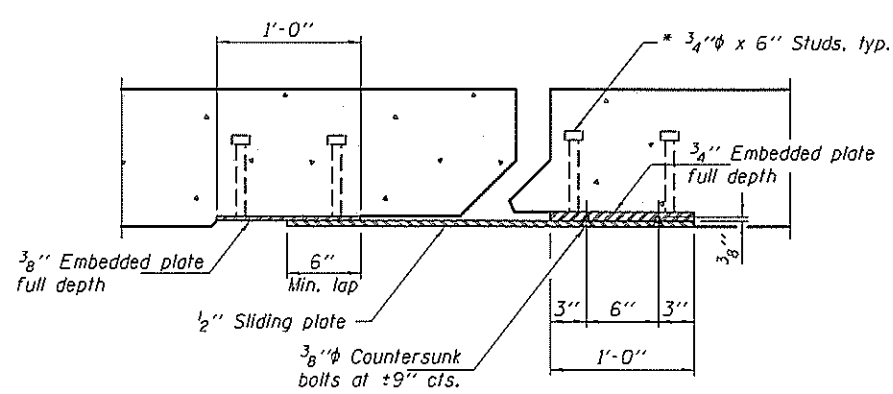
F.A.I. SECTION COUNTY TOTAL SHEETS SHEET NO.
414 90-6,785-1 TAZEWELL 245 112
CONTRACT NO. 68606
[ILLINOIS] FED. AID PROJECT



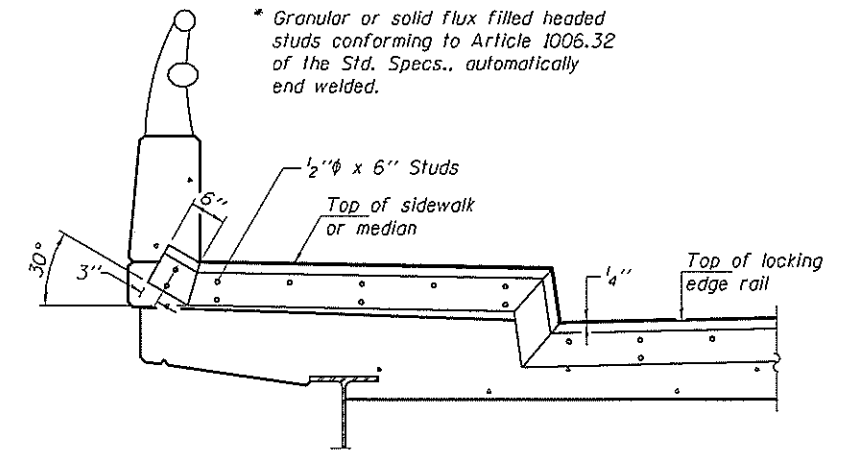
PLAN
(For skews $\le 30^\circ$)



PLAN
(For skews $> 30^\circ$)
Showing point block

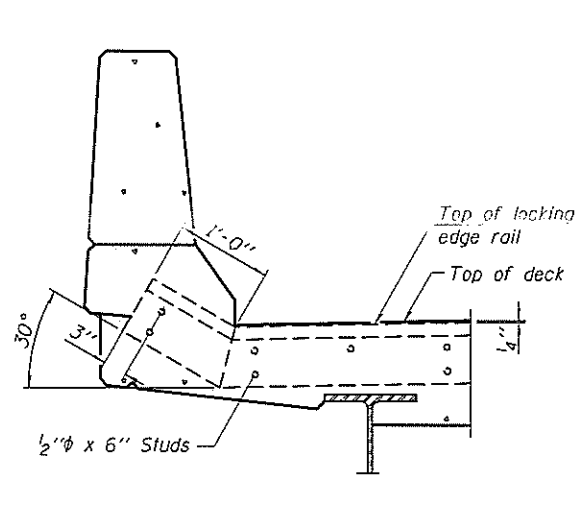


SECTION C-C

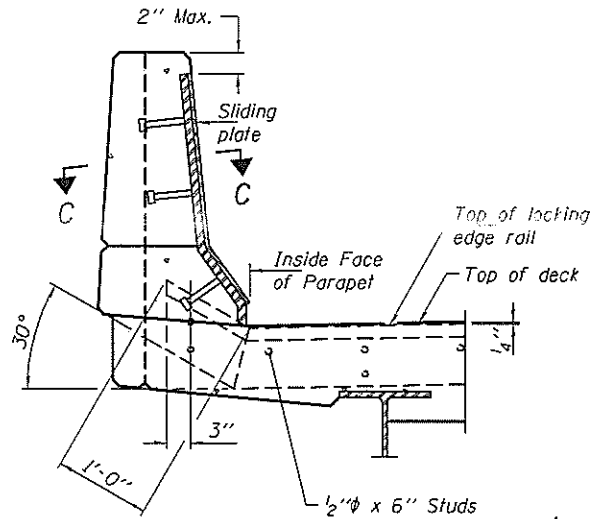


TYPICAL END TREATMENT AT SIDEWALK

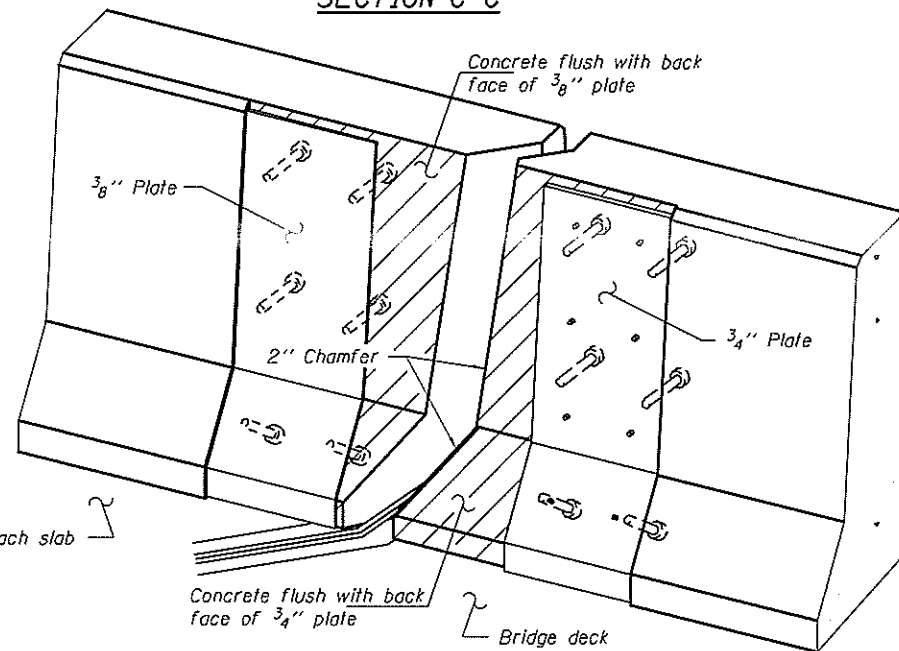
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



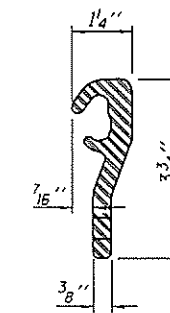
SECTION A-A



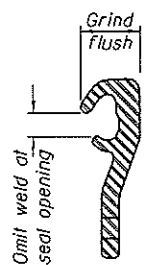
SECTION B-B



TRIMETRIC VIEW
(Showing back plates only)

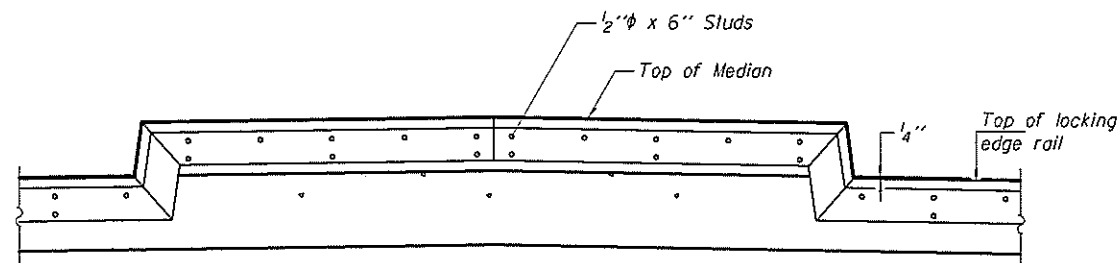


LOCKING EDGE RAIL



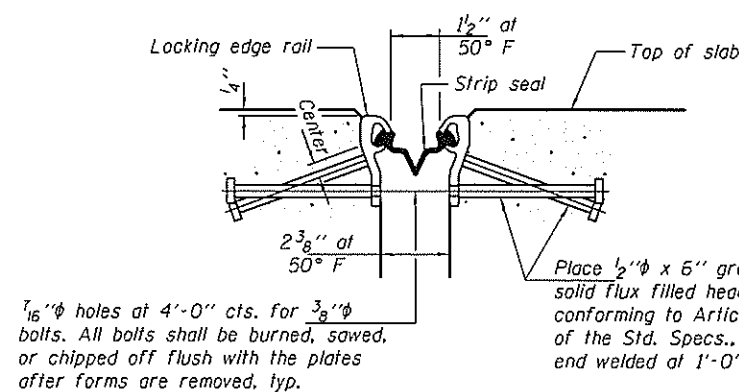
LOCKING EDGE RAIL SPLICE

Rolled rail shown, welded rail similar.



TYPICAL TREATMENT AT MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



SECTION THRU STRIP SEAL JOINT

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.

The inside of the Locking Edge Rail groove shall be free of weld residue. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	126

DESIGNED CCC
CHECKED SMR
DRAWN baliva
CHECKED CCC SMR

PASSED

[Signature]
ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016

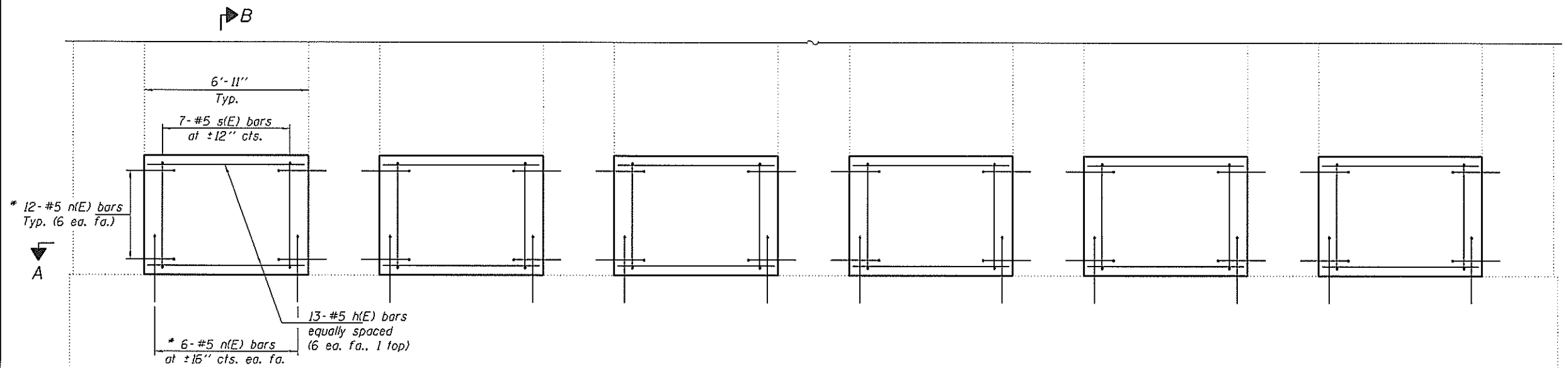
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

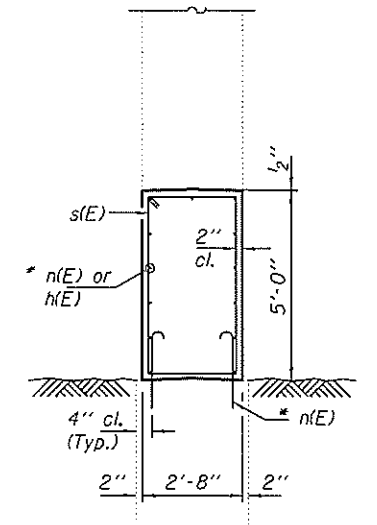
PREFORMED JOINT STRIP SEAL
SN 090-0103

SHEET NO. 3 OF 5 SHEETS

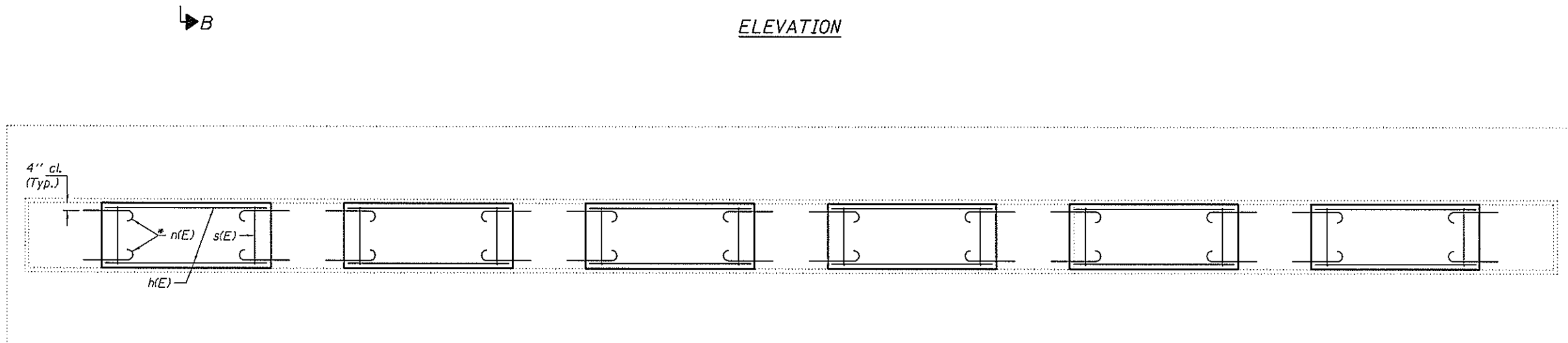
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-6, TRS-1	TAZEWELL	245	113
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



ELEVATION

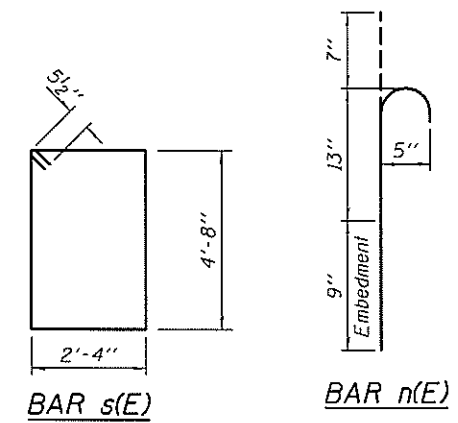


SECTION B-B



SECTION A-A

* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications. Cost included with Reinforcement Bars, Epoxy Coated.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	78	#5	6'-7"	—
n(E)	216	#5	2'-5"	U
s(E)	42	#5	14'-11"	□
Concrete Structures			Cu. Yd.	20.5
Reinforcement Bars, Epoxy Coated			Pound	1730

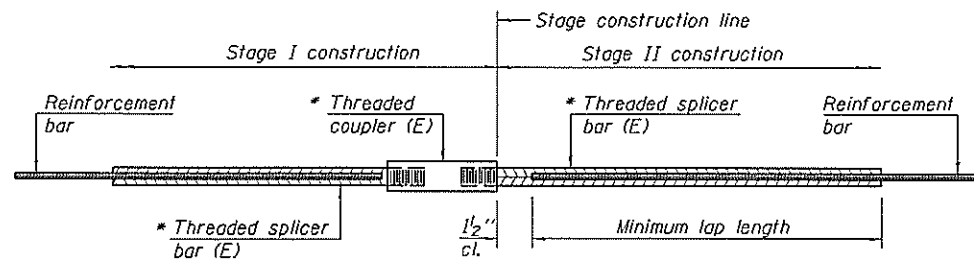
DESIGNED CCC
 CHECKED SMR
 DRAWN baliva
 CHECKED CCC SMR

DATE JANUARY 27, 2016
 PASSED *[Signature]*
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CRASHWALL EXTENSION
 SN 090-0103
 SHEET NO. 4 OF 5 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6, TRS-1	TAZENELL	245	114
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

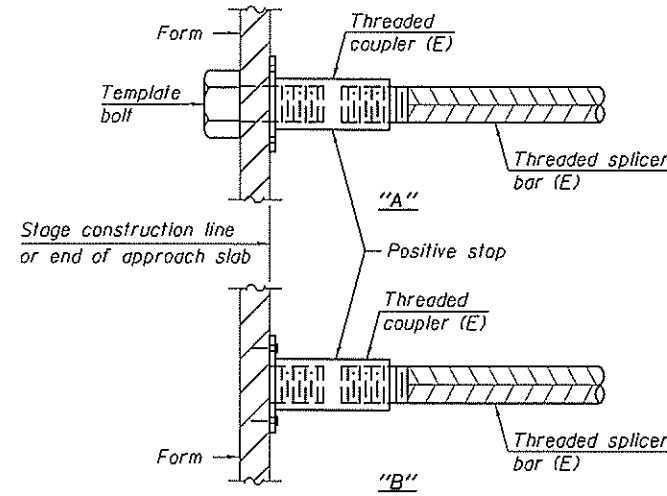


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
South Abutment	#5	4	3'-6"
North Abutment	#5	4	3'-6"

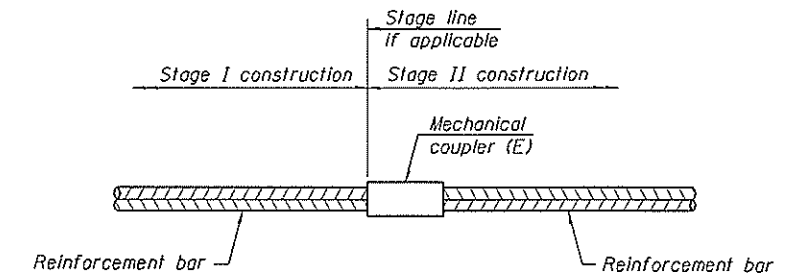


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

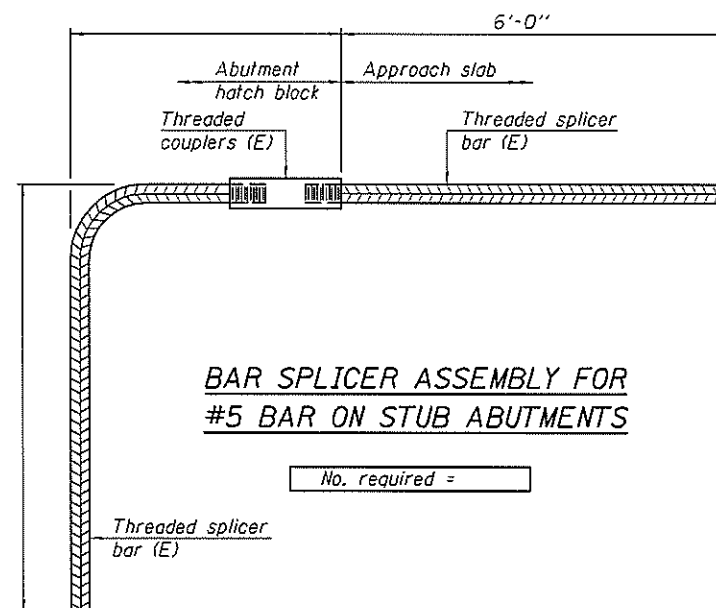
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 6-8-15

DESIGNED CCC
 CHECKED SMR
 DRAWN baliva
 CHECKED CCC SMR

DATE JANUARY 27, 2016
 PASSED *[Signature]*
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 SN 090-0103

SHEET NO. 5 OF 5 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-6, TRS-1	TAZENELL	245	115

CONTRACT NO. 68606
 ILLINOIS FED. AID PROJECT

GENERAL NOTES

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

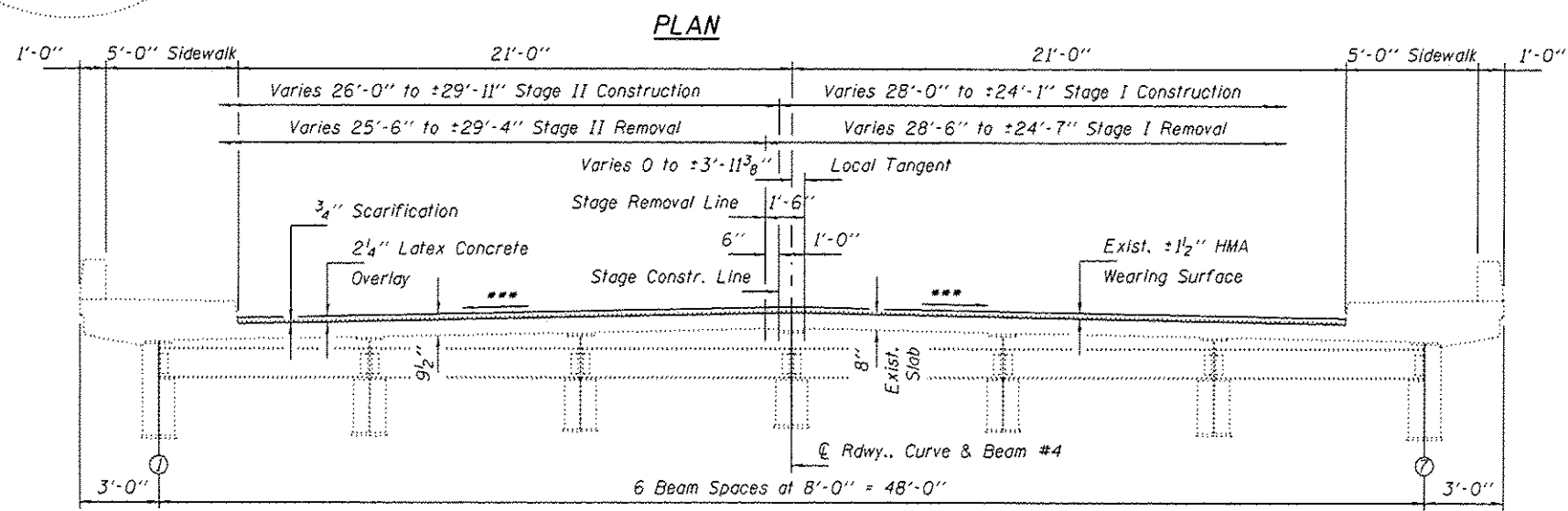
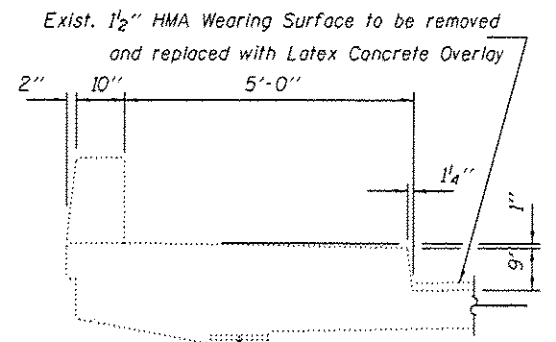
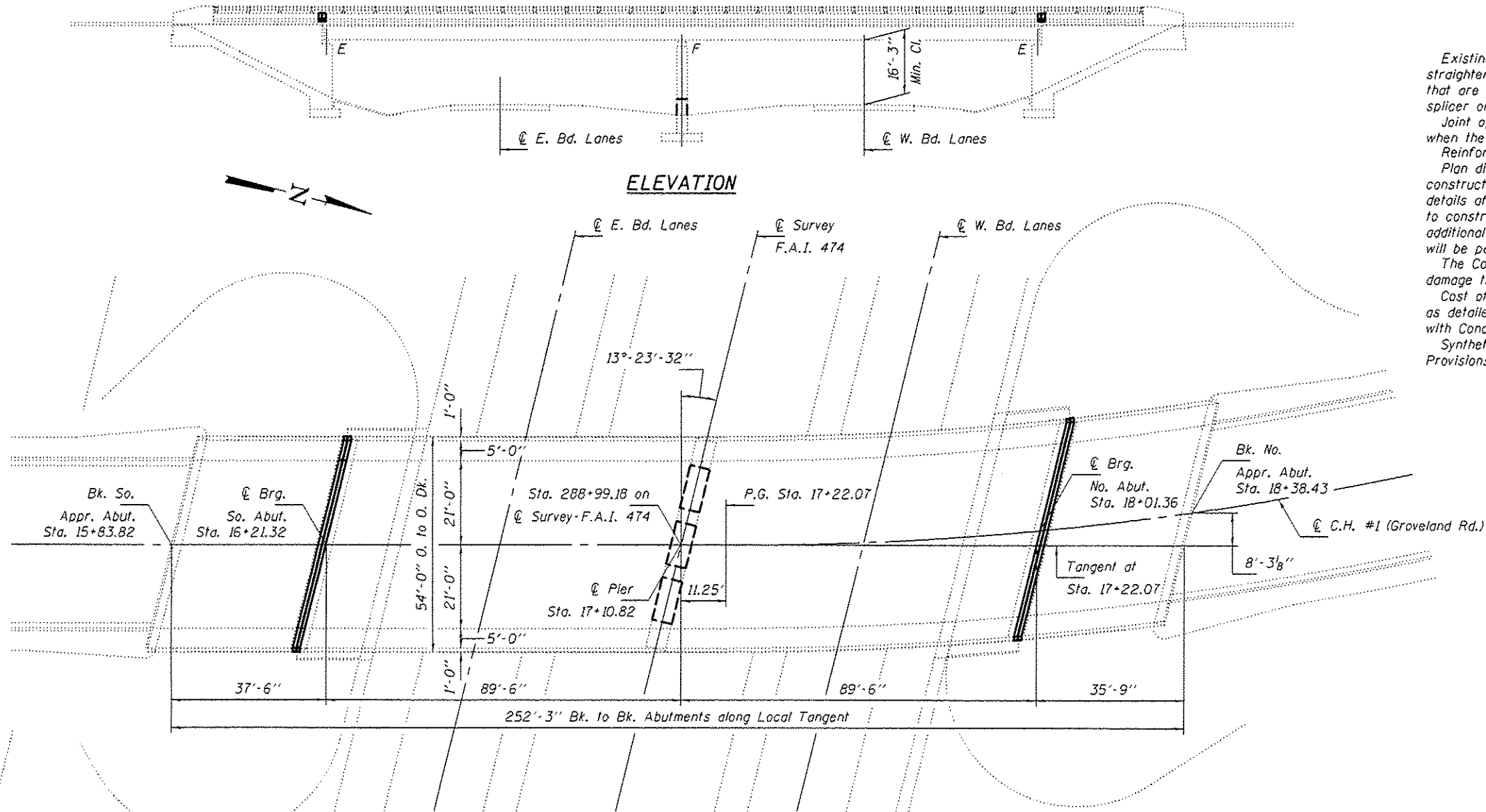
Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beam.

Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Concrete Removal.

Synthetic fibers shall be added to the bridge deck concrete overlay. See Special Provisions.



TOTAL BILL OF MATERIAL

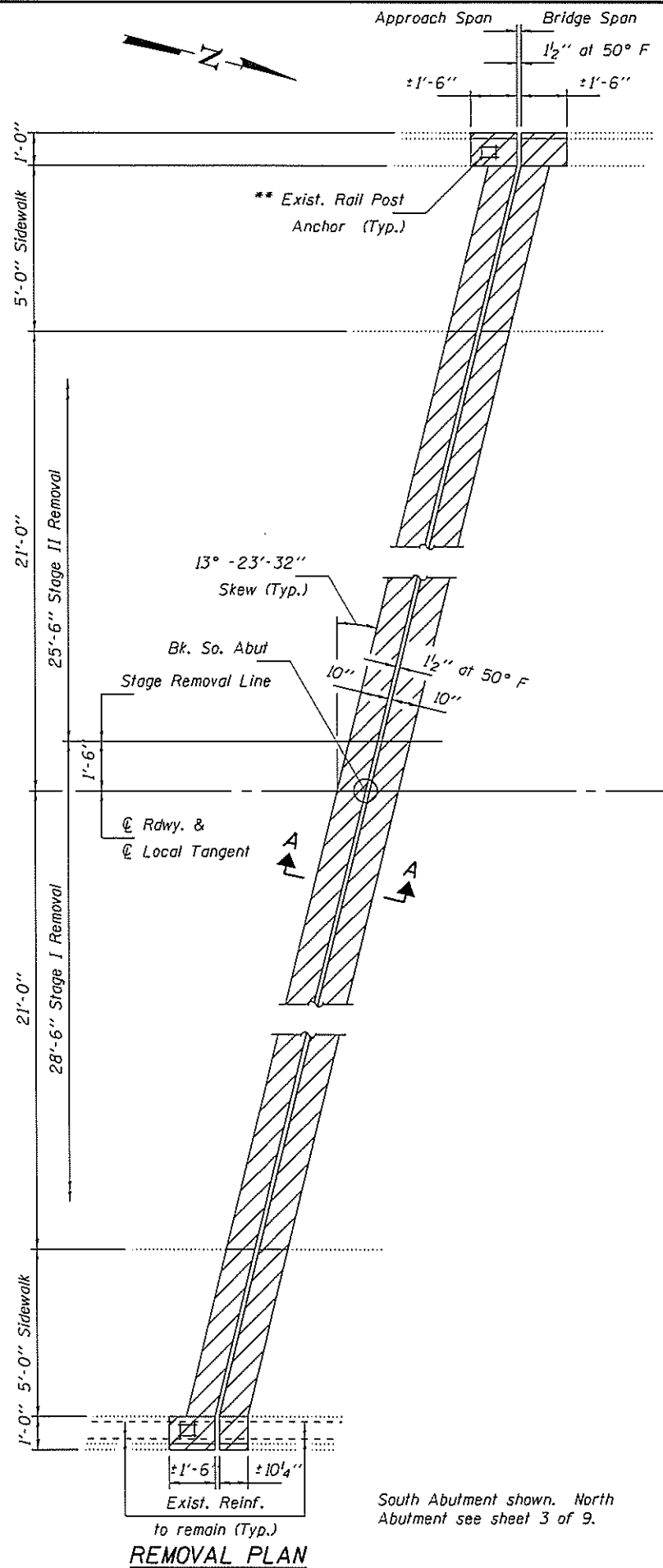
ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	10.1
Concrete Superstructure	Cu. Yd.	10.1
Concrete Structures	Cu. Yd.	15.0
Preformed Joint Strip Seal	Foot	118
Reinforcement Bars, Epoxy Coated	Pound	2410
Bar Splicers	Each	8
Concrete Sealer	Sq. Ft.	215
Bridge Deck Latex Concrete Overlay, 2 1/4"	Sq. Yd.	1191
Floor Drains	Each	9
Bridge Deck Scarification, 3/4"	Sq. Yd.	1191
HMA Surface Removal, 1 1/2"	Sq. Yd.	1213
Bridge Deck Grooving	Sq. Yd.	1136

* New Concrete Superstructures areas of deck, sidewalk top, front and back of parapets only

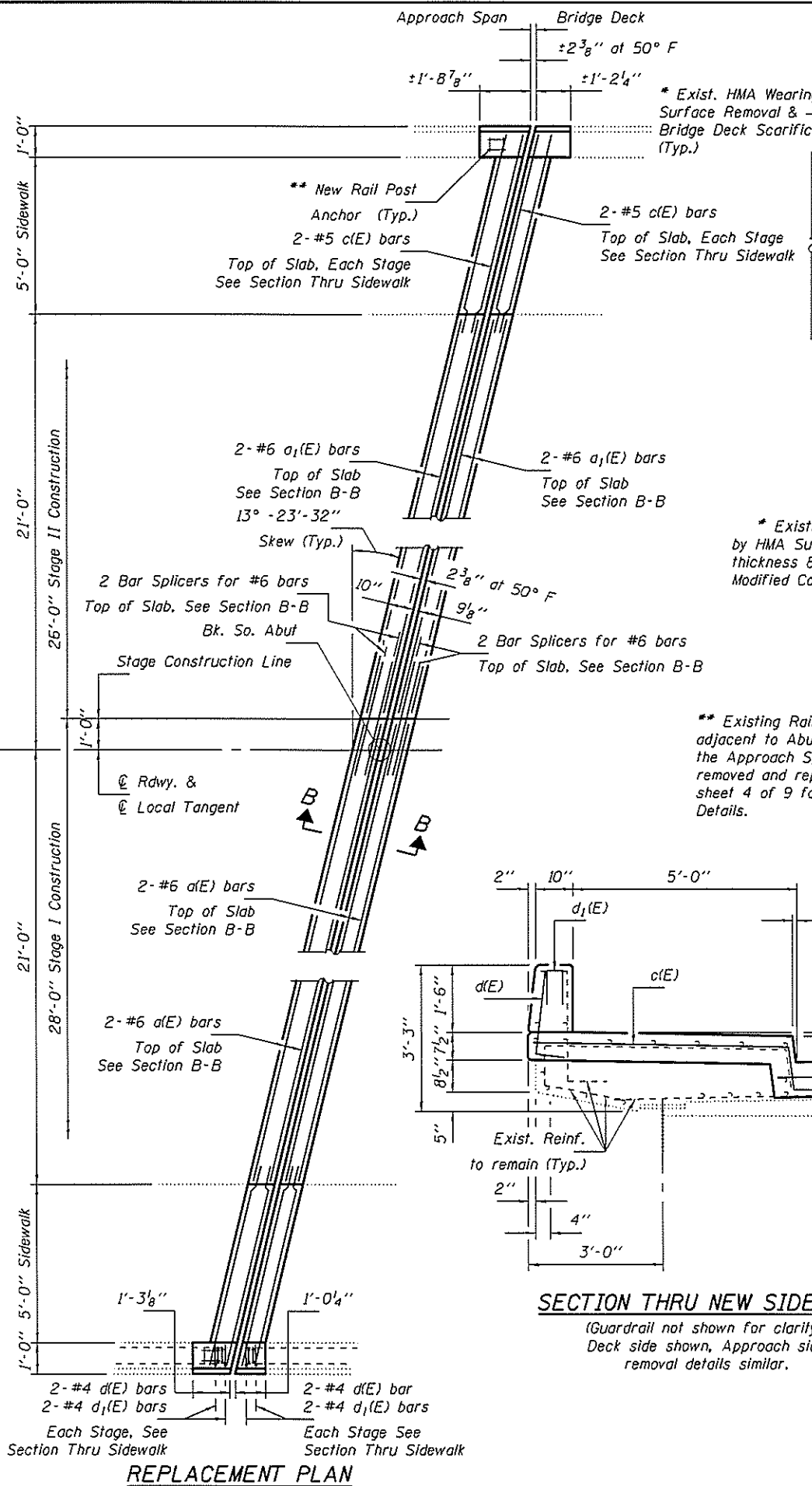


EXPIRES 11-30-2016

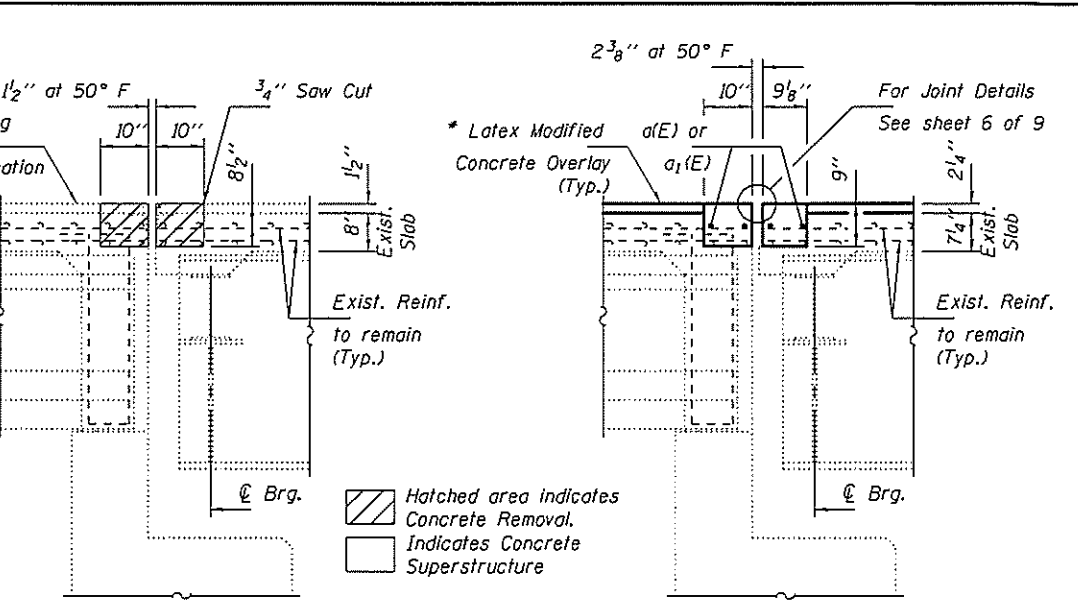
CROSS SECTION
(Looking North)
*** 3/16" / F1 Normal Crown
Super Elevation transition Sta. 16+22.07 to Sta. 17+72.07.
Full Super Elevation starts at Sta. 17+72.07 (0.07 ft./ft.).



REMOVAL PLAN



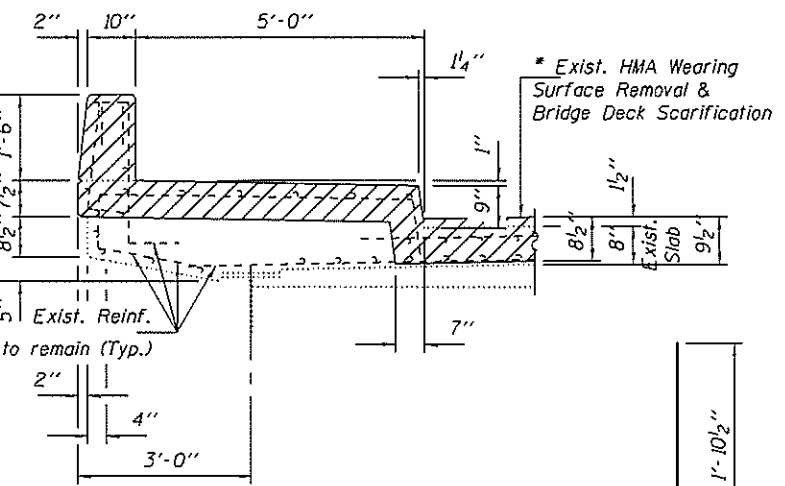
REPLACEMENT PLAN



SECTION A-A

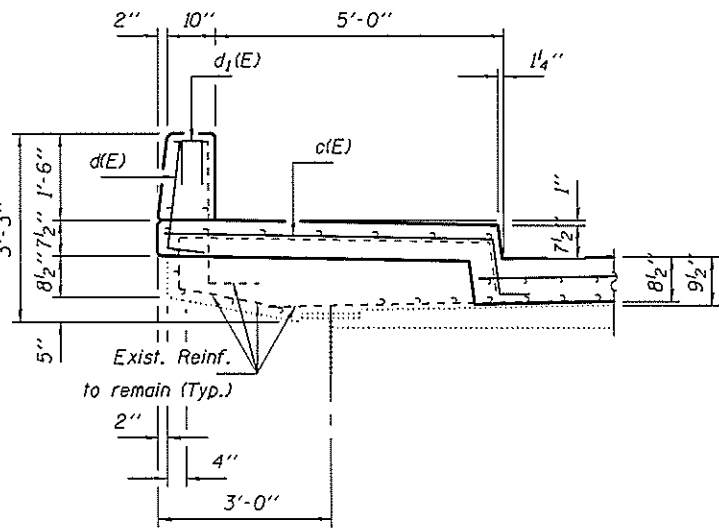
SECTION B-B

* Existing $\pm 1/2$ " HMA Wearing Surface is to be removed by HMA Surface Removal, $1/2$ ". The original deck slab thickness 8" scarified $3/4$ " and then replaced with Latex Modified Concrete Overlay, $2 1/4$ ".



SECTION THRU EXISTING SIDEWALK

(Guardrail not shown for clarity.) Deck side shown, Approach side removal details similar.



SECTION THRU NEW SIDEWALK

(Guardrail not shown for clarity.) Deck side shown, Approach side removal details similar.

BAR d1(E)

BAR d(E)

BAR c(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	4	#6	31'-11"	—
a1(E)	4	#6	29'-0"	—
c(E)	16	#5	7'-5"	—
d(E)	16	#4	2'-6"	L
d1(E)	16	#4	1'-11"	Π
Concrete Removal			Cu. Yd.	2.6
Concrete Superstructure			Cu. Yd.	2.6
Reinforcement Bars, Epoxy Coated			Lbs.	540

DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

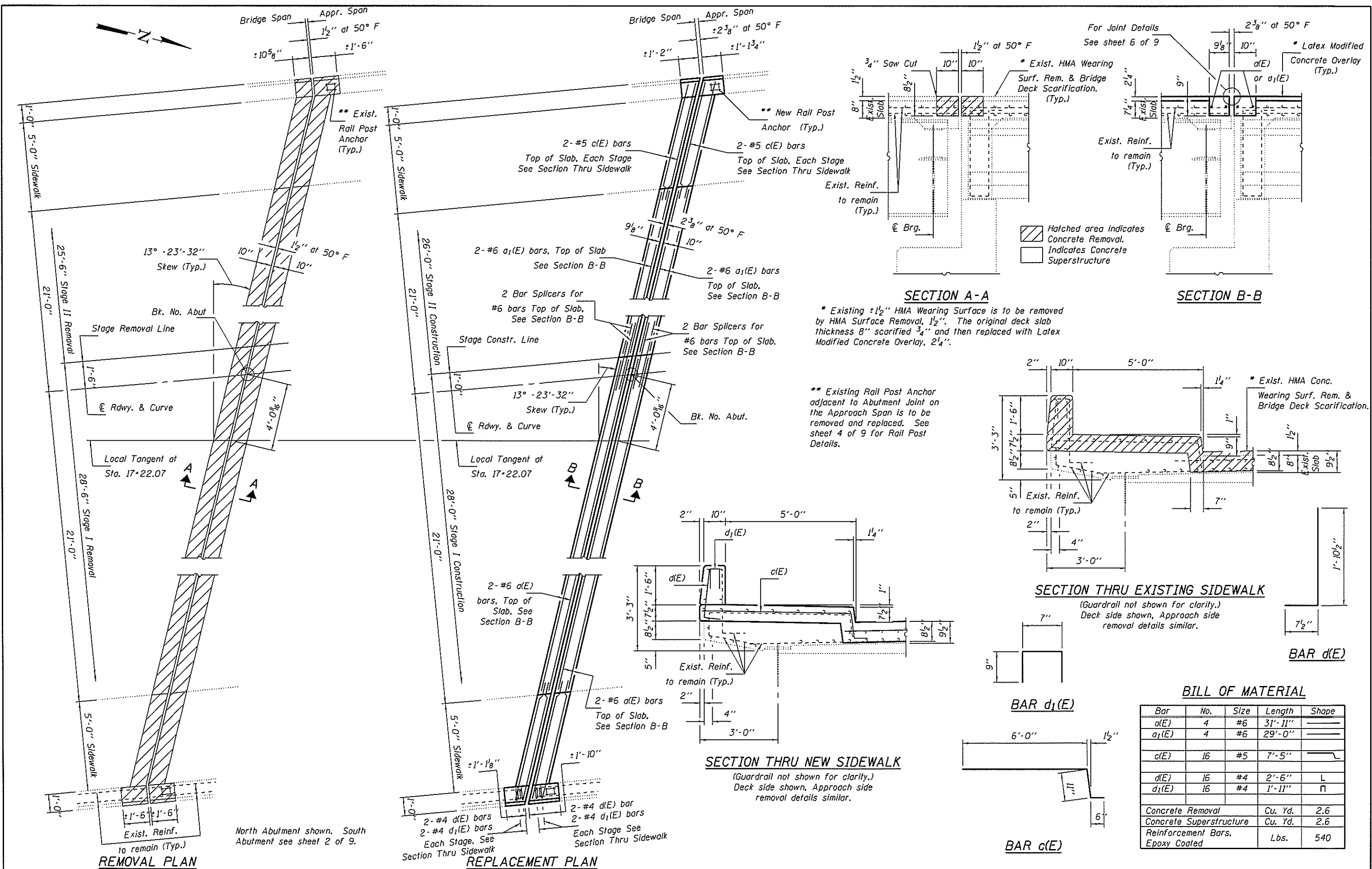
PASSED
ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT JOINT REPLACEMENT DETAILS
SN 090-0104
SHEET NO. 2 OF 9 SHEETS

F.A.I. RTE. 474
SECTION 90-6, TRS-1
COUNTY TAZEWELL
TOTAL SHEETS 245
SHEET NO. 111
CONTRACT NO. 68606
ILLINOIS FED. AID PROJECT



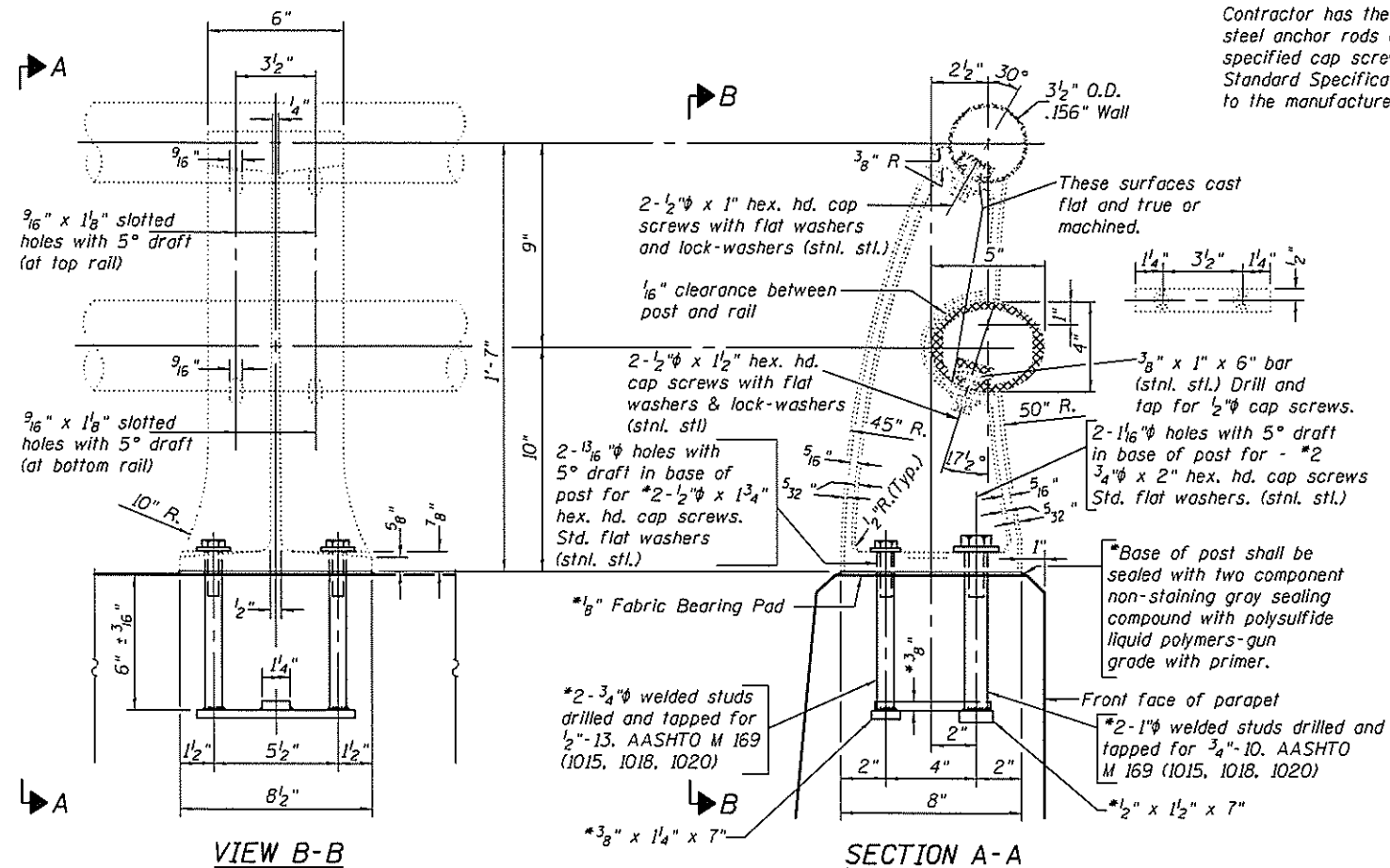
SECTION A-A
 * Existing $\pm 1\frac{1}{2}$ " HMA Wearing Surface is to be removed by HMA Surface Removal, $1\frac{1}{2}$ ". The original deck slab thickness 8" scarified $3\frac{1}{4}$ " and then replaced with Latex Modified Concrete Overlay, $2\frac{1}{4}$ ".

** Existing Rail Post Anchor adjacent to Abutment Joint on the Approach Span is to be removed and replaced. See sheet 4 of 9 for Rail Post Details.

Hatched area indicates Concrete Removal. Indicates Concrete Superstructure

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	4	#6	31'-11"	—
a1(E)	4	#6	29'-0"	—
c(E)	16	#5	7'-5"	—
d(E)	16	#4	2'-6"	L
d1(E)	16	#4	1'-11"	n
Concrete Removal		Cu. Yd.	2.6	
Concrete Superstructure		Cu. Yd.	2.6	
Reinforcement Bars, Epoxy Coated		Lbs.	540	



R20/REPS 1-27-2000

DESIGNED	SMR
CHECKED	CCC
DRAWN	J. Schneller
CHECKED	SMR CCC

PASSED	<i>J. Carl Perry</i> ACTING ENGINEER OF BRIDGES AND STRUCTURES
--------	-------------------------------------------------------------------

DATE	JANUARY 27, 2016
REVISED	
REVISED	

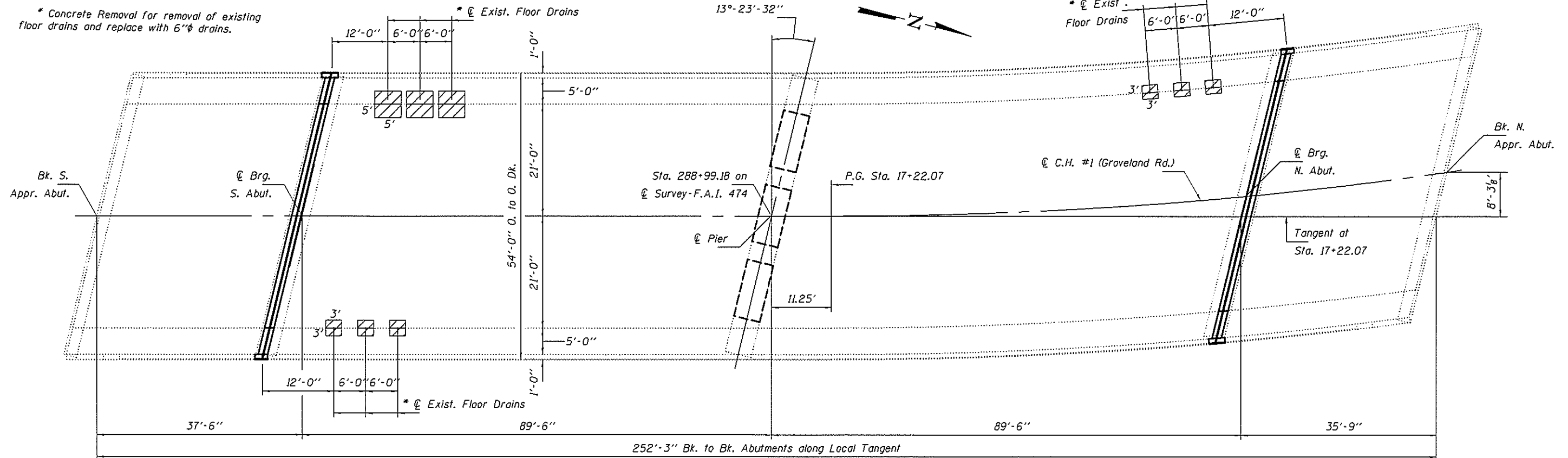
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RAIL POST ANCHOR DETAILS
SN 090-0104

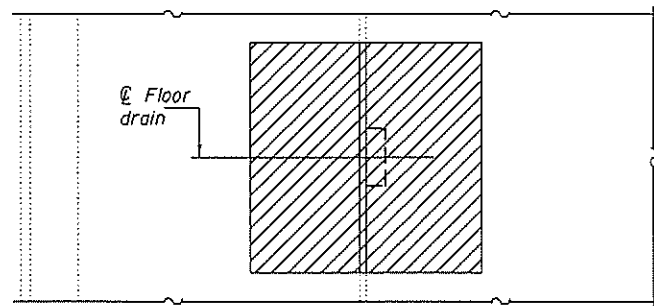
SHEET NO. 4 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6, TRS-1	TAZEWELL	245	119
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

* Concrete Removal for removal of existing floor drains and replace with 6"φ drains.

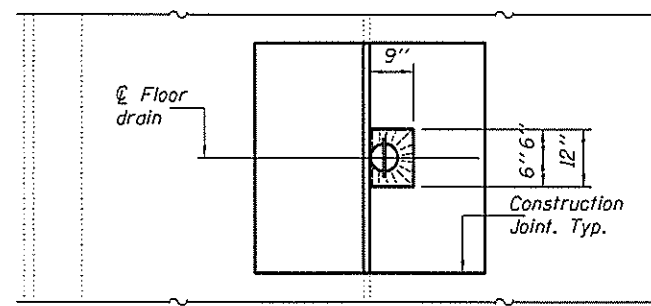


PLAN

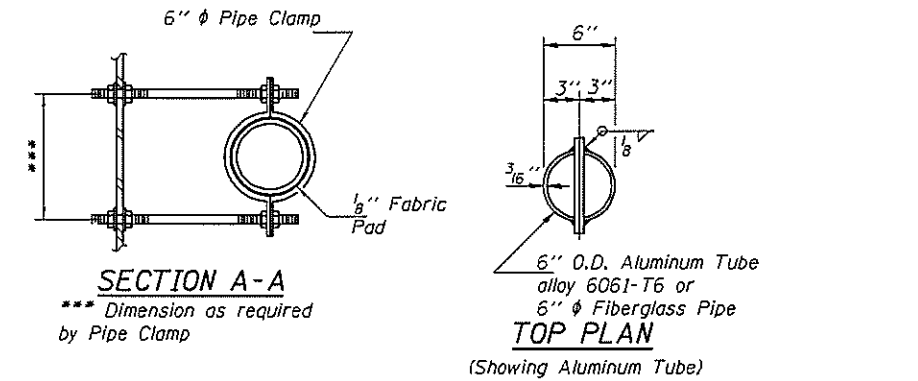


** TOP PLAN

** Hatched areas indicate Concrete Removal. For dimensions not shown, see Plan. Cost of replacement of concrete included with Concrete Superstructure.

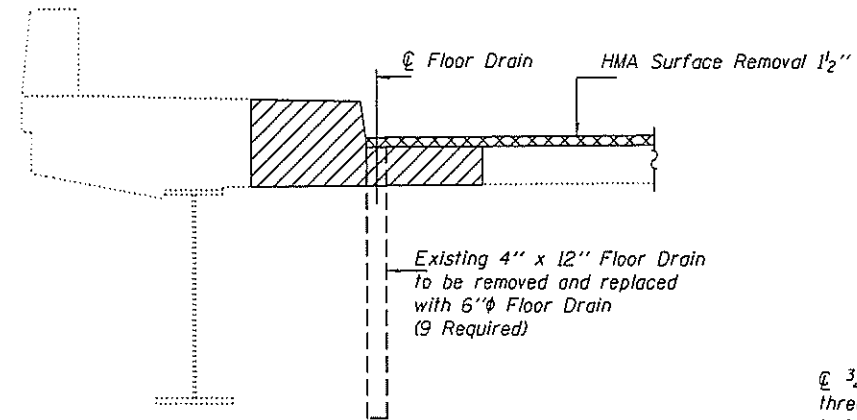


** TOP PLAN

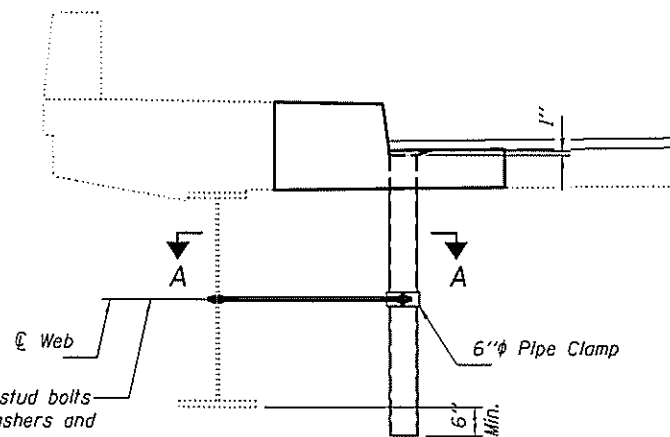


SECTION A-A
*** Dimension as required by Pipe Clamp

TOP PLAN
(Showing Aluminum Tube)



** CURB SECTION AT EXISTING DRAIN
(Bridge Deck Scarification not shown)



** CURB SECTION AT NEW DRAIN
(Bridge Deck Scarification not shown)

Notes:

- Drains shall be located clear of all diaphragms.
- The exterior surfaces of the floor drains shall be painted with the finish coat as specified in the special provisions for Cleaning and Painting New Metal Structures. The exterior surfaces of the drains shall be cleaned according to the Society of Protective Coatings' Spec. SSPC-SP1 prior to painting.
- Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
- Galvanize clamping device according to AASHTO M232. Cost of clamping device and inserts is included with Floor Drains.

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	4.8
Concrete Superstructure	Cu. Yd.	4.8

DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

PASSED

ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016

REVISED
REVISED

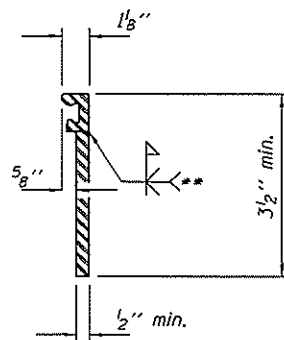
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FLOOR DRAINS DECK REPAIRS
SN 090-0104

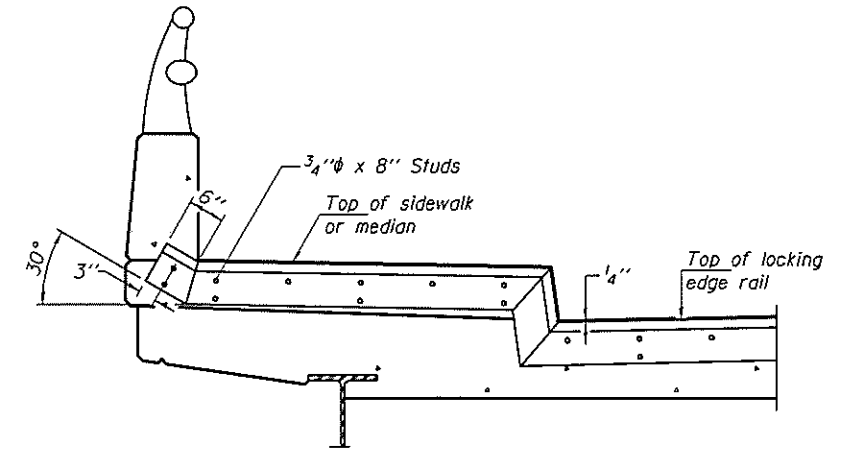
SHEET NO. 5 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZEWELL	245	120

CONTRACT NO. 68606
[ILLINOIS] FED. AID PROJECT

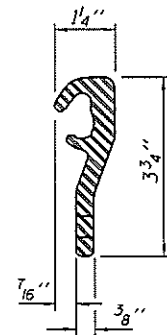


WELDED RAIL

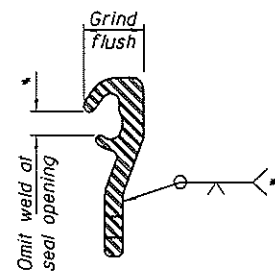


TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

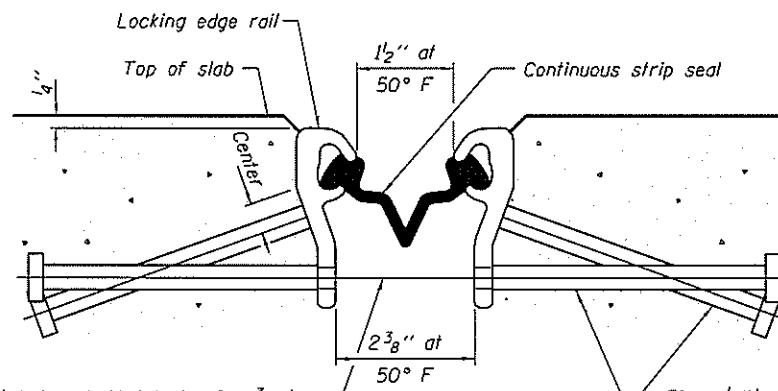
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



ROLLED (EXTRUDED) RAIL LOCKING EDGE RAIL



LOCKING EDGE RAIL SPLICE



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Place 1/2" ϕ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

SECTION THRU STRIP SEAL JOINT FOR OVERLAY AT ABUTMENT JOINTS

* Omit weld at seal opening.
** Back gouge not required if complete joint penetration is verified by mock-up.

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.

The inside of the Locking Edge Rail groove shall be free of weld residue. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skewers > 30° included in the cost of Preformed Joint Strip Seal.

The manufacturer's recommended installation methods shall be followed.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	118

DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

PASSED

J. Carl Prosser
ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016

REVISED
REVISED

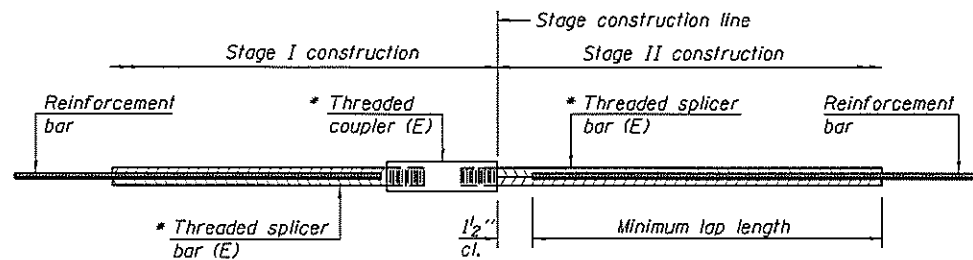
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
SN 090-0104

SHEET NO. 6 OF 9 SHEETS

FEM. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
A14	90-6,7RS-1	TAZERELL	245	121

CONTRACT NO. 68606
[ILLINOIS] FED. AID PROJECT

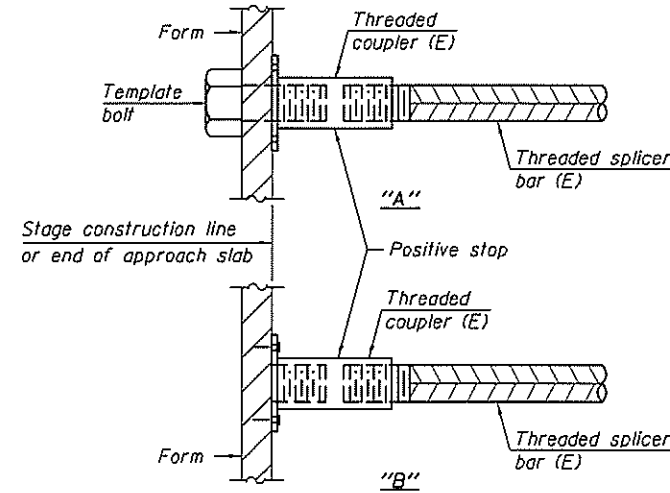


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1/2" + thread length

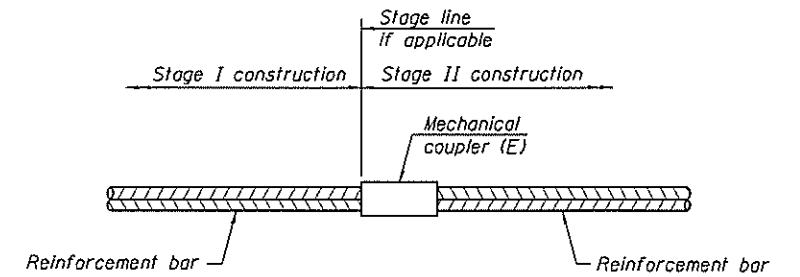
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
No. Abut.	#6	4	3'-7"
So. Abut.	#6	4	3'-7"



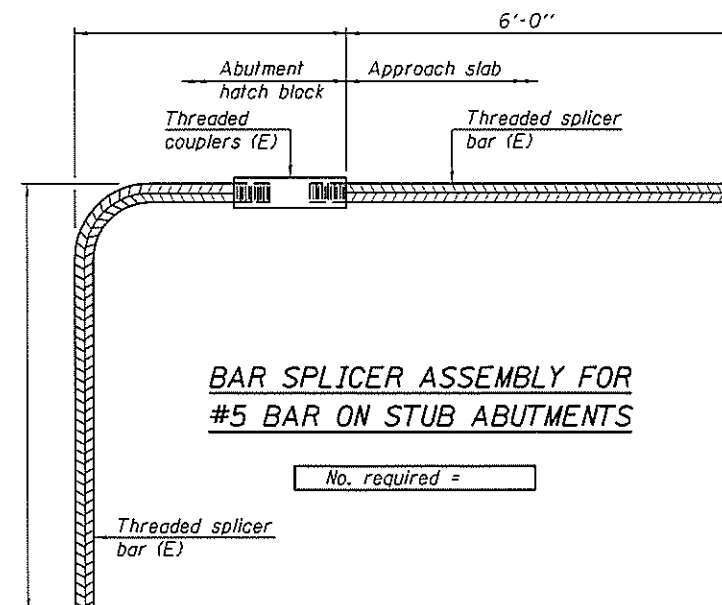
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

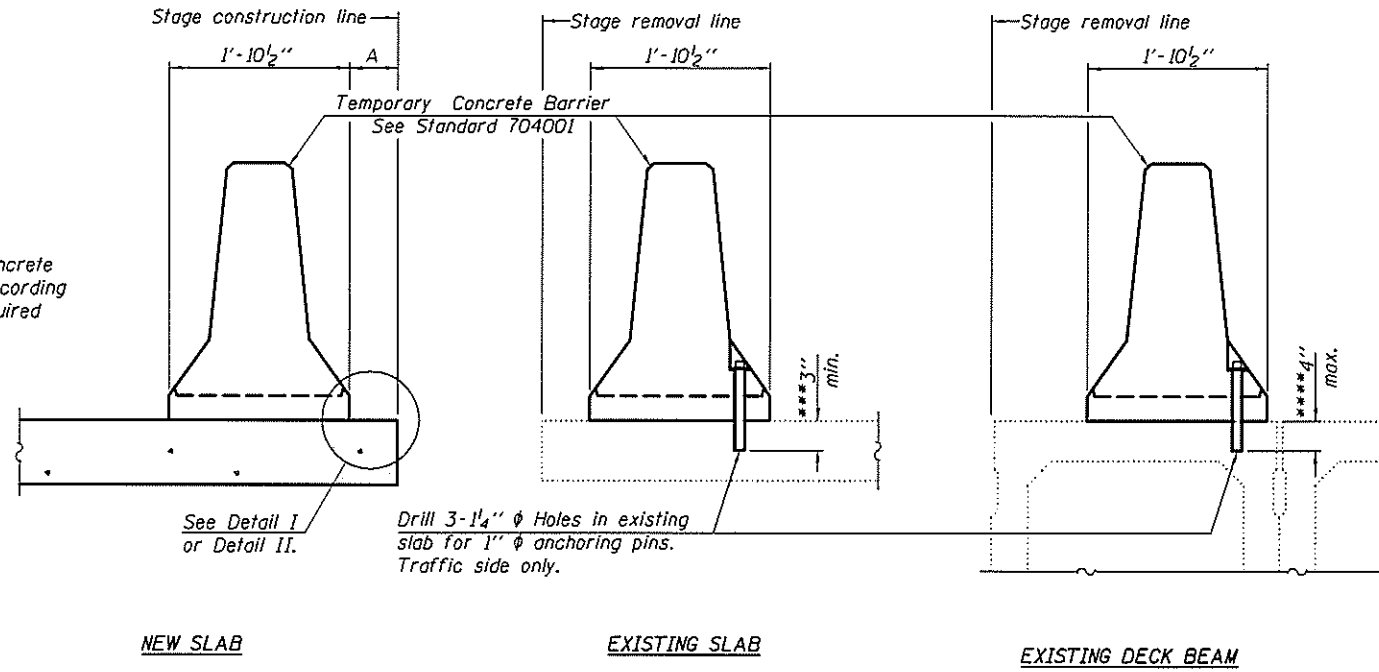
No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 6-8-15

When "A" is 3'-1" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-1".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

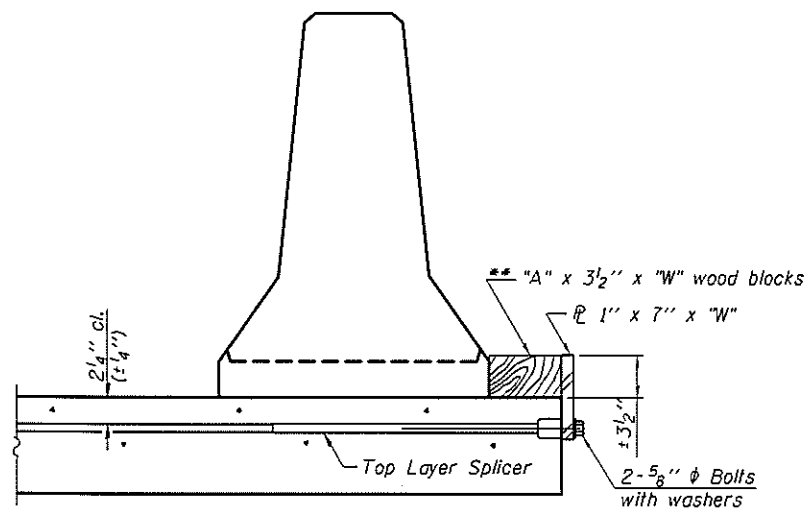
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel plate to the top layer of couplers with 2-5/8" diameter bolts screwed to coupler at approximate center of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel plate to the concrete slab or concrete wearing surface with 2-5/8" diameter Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate center of each barrier panel.

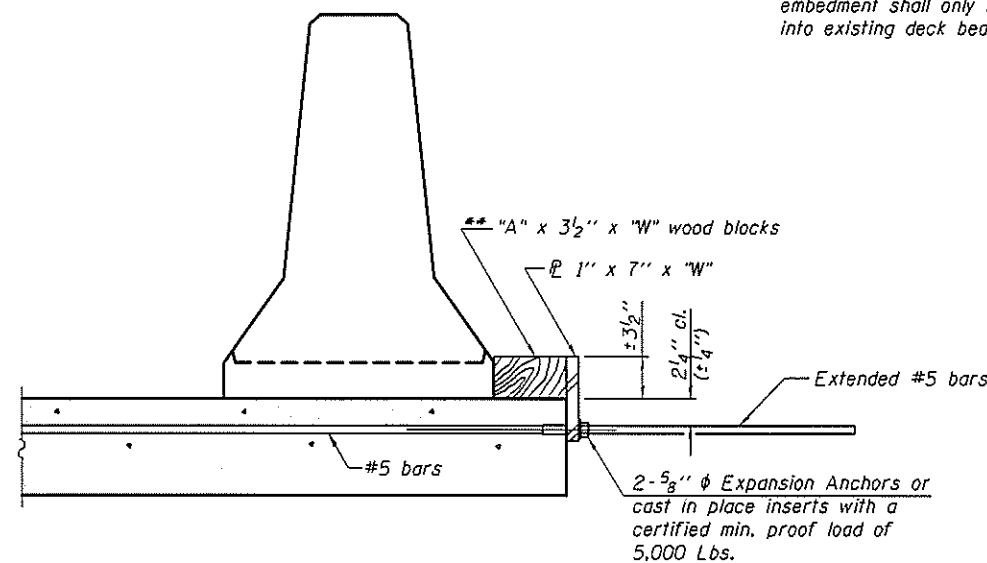
Cost of retainer assembly is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

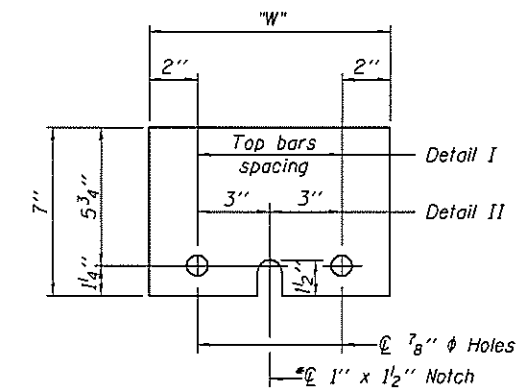
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER PLATE 1' x 7' x "W"

* Required only with Detail II

RETAINER ASSEMBLY

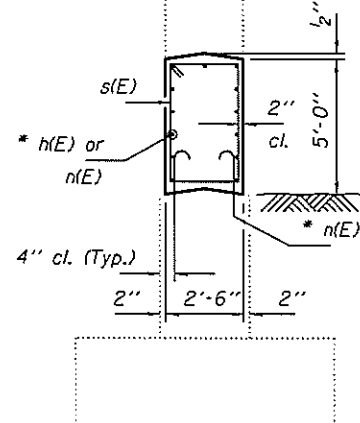
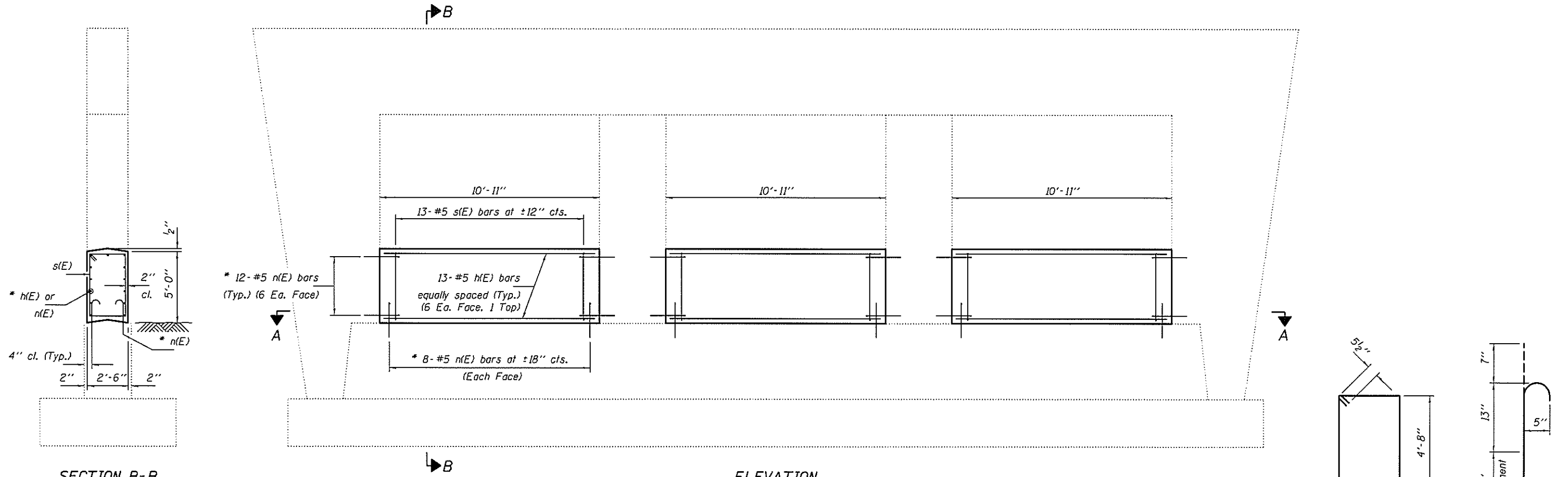
** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

R-27 1-12-15

DESIGNED SMR	DATE JANUARY 27, 2016	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION	F.A.I. RTE. 474	SECTION 90-6,7R5-1	COUNTY TAZEWELL	TOTAL SHEETS 245	SHEET NO. 123
CHECKED CCC	REVISOR		SN 090-0104	CONTRACT NO. 68606				
DRAWN J. Schneller	REVISOR		SHEET NO. 8 OF 9 SHEETS	ILLINOIS FED. AID PROJECT				
CHECKED SMR CCC	ACTING ENGINEER OF BRIDGES AND STRUCTURES							

NOTES

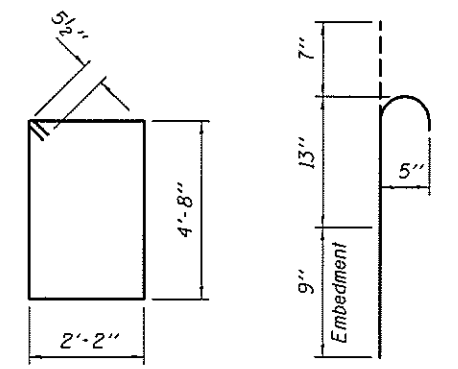
The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.



SECTION B-B

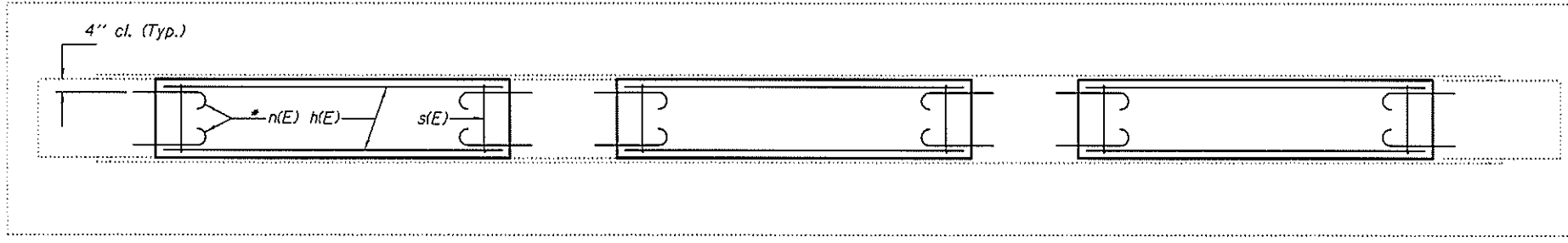
ELEVATION

* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.



BAR s(E)

BAR n(E)



SECTION A-A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
n(E)	39	#5	10'-8"	—
n(E)	120	#5	2'-5"	U
s(E)	39	#5	14'-7"	□
Concrete Structures			Cu. Yd.	15.0
Reinforcement Bars, Epoxy Coated			Pound	1330

DESIGNED SMR
 CHECKED CCC
 DRAWN J. Schneller
 CHECKED SMR CCC

DATE JANUARY 27, 2016
 PASSED *J. Carl Boyer*
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CRASHWALL EXTENSION
 SN 090-0104
 SHEET NO. 9 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-5, TRS-1	TAZEWELL	245	124
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

Reinforcement bars designated (E) shall be epoxy coated.

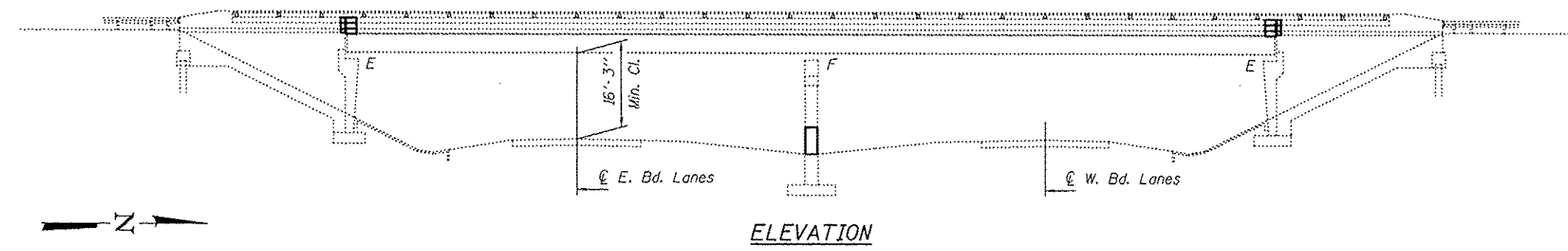
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beam.

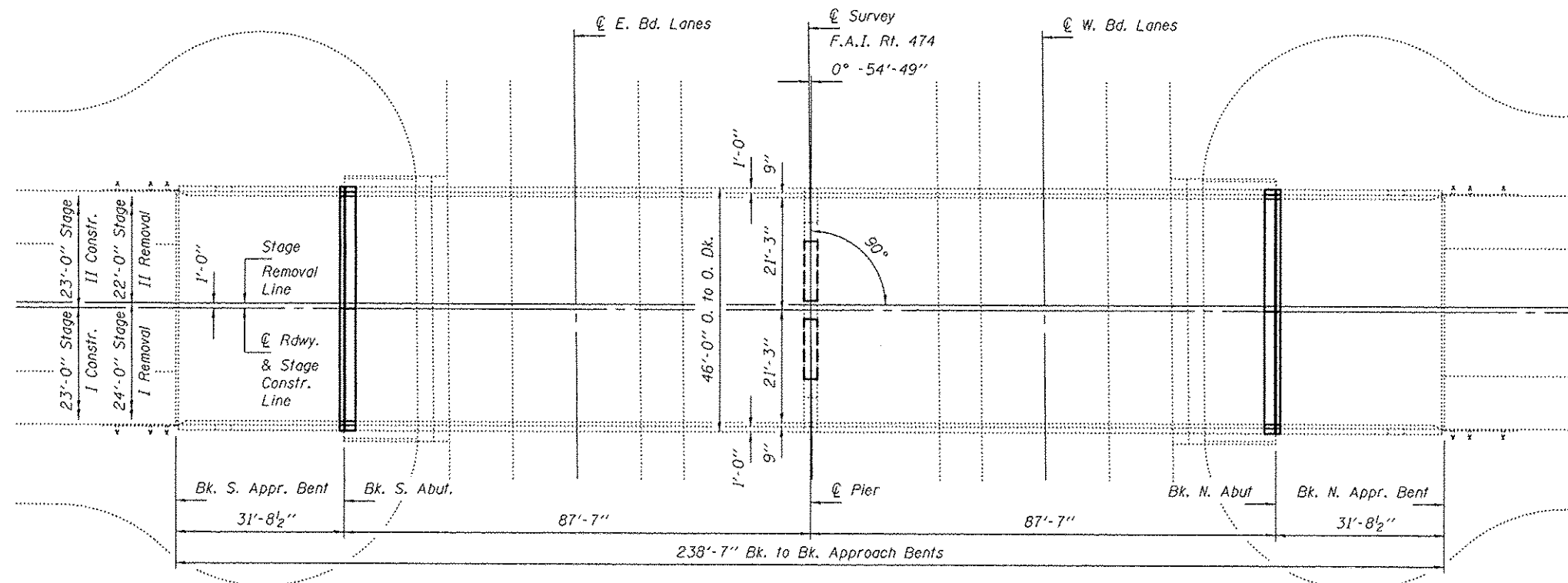
Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Concrete Removal.

Synthetic fibers shall be added to the bridge deck latex concrete overlay. See Special Provisions.

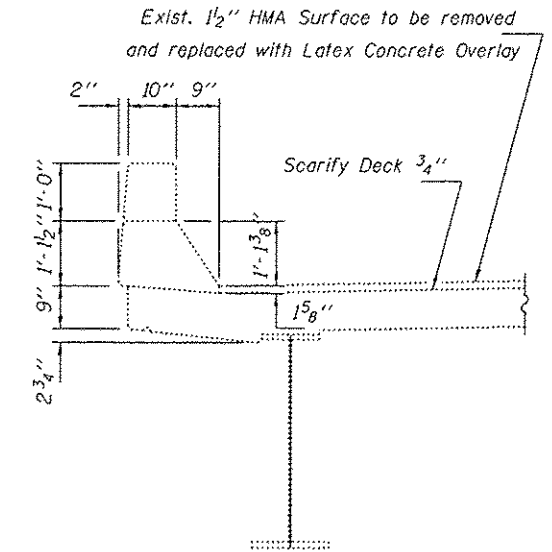
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.



ELEVATION



PLAN



SECTION THRU CURB

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	9.6
Concrete Superstructure	Cu. Yd.	9.9
Concrete Structures	Cu. Yd.	9.1
Preformed Joint Strip Seal	Foot	89
Reinforcement Bars, Epoxy Coated	Pound	2390
Bar Splicers	Each	20
* Concrete Sealer	Sq. Ft.	245
Bridge Deck Latex Concrete Overlay, 2 1/4"	Sq. Yd.	1094
Bridge Deck Scarification, 3/4"	Sq. Yd.	1094
HMA Surface Removal, 1/2"	Sq. Yd.	1094
Bridge Deck Grooving	Sq. Yd.	1068
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq. Ft.	361

* New concrete areas of deck, top, front and back of parapet only.



EXPIRES 11-30-2016

DESIGNED: *Stephan M Ryan*
 CHECKED: *J. Schneller*
 DRAWN: *J. Schneller*
 CHECKED: *CCC SMR*

PASSED: *David Carl Puzey*
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE: JANUARY 27, 2016

REVISED: _____
 REVISED: _____

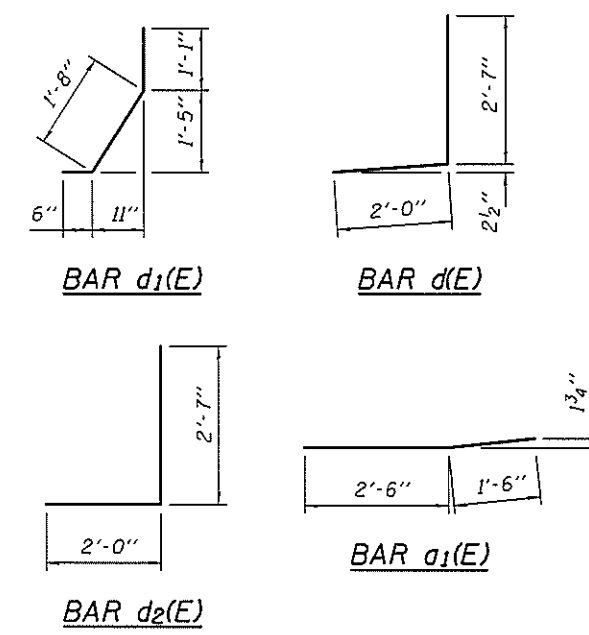
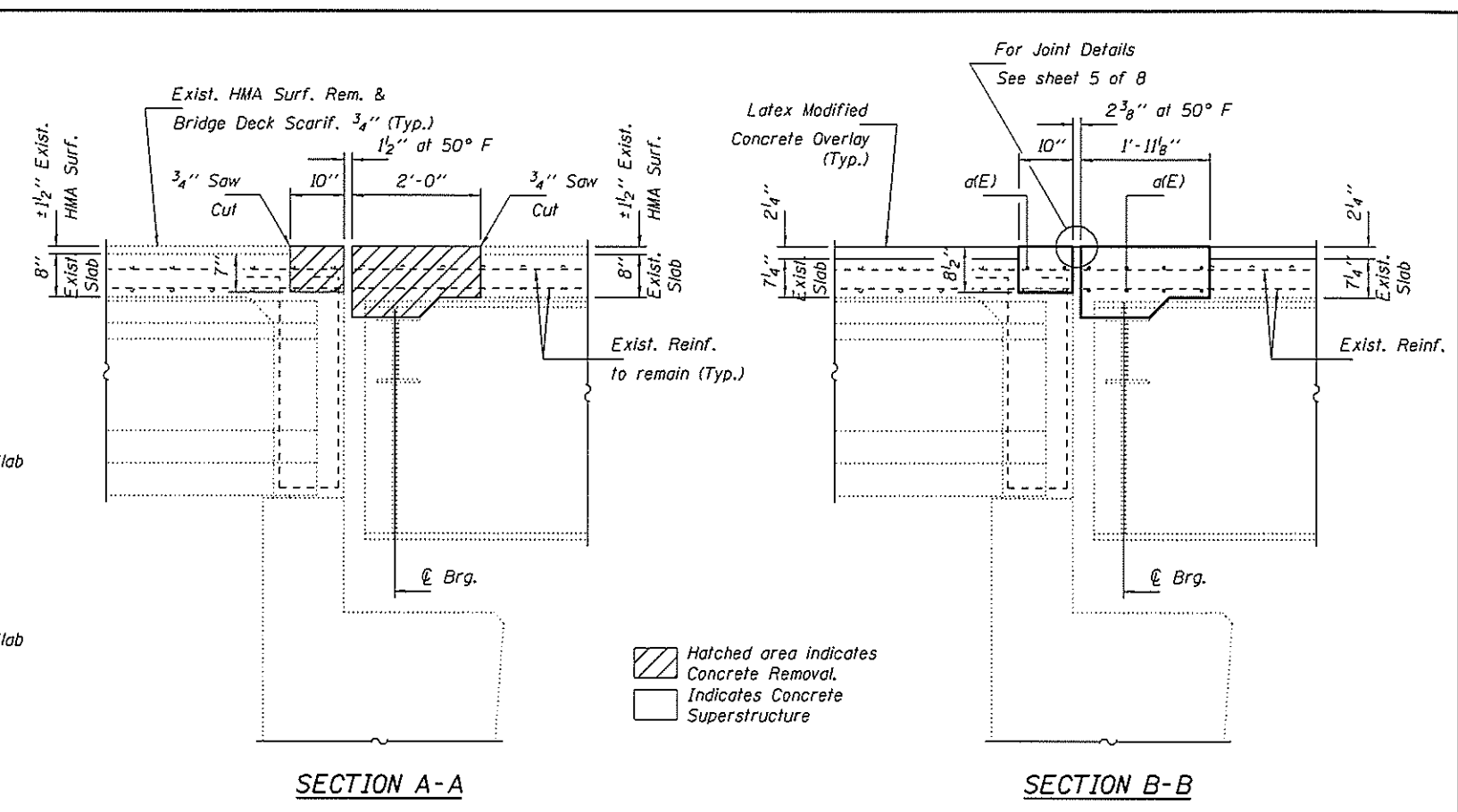
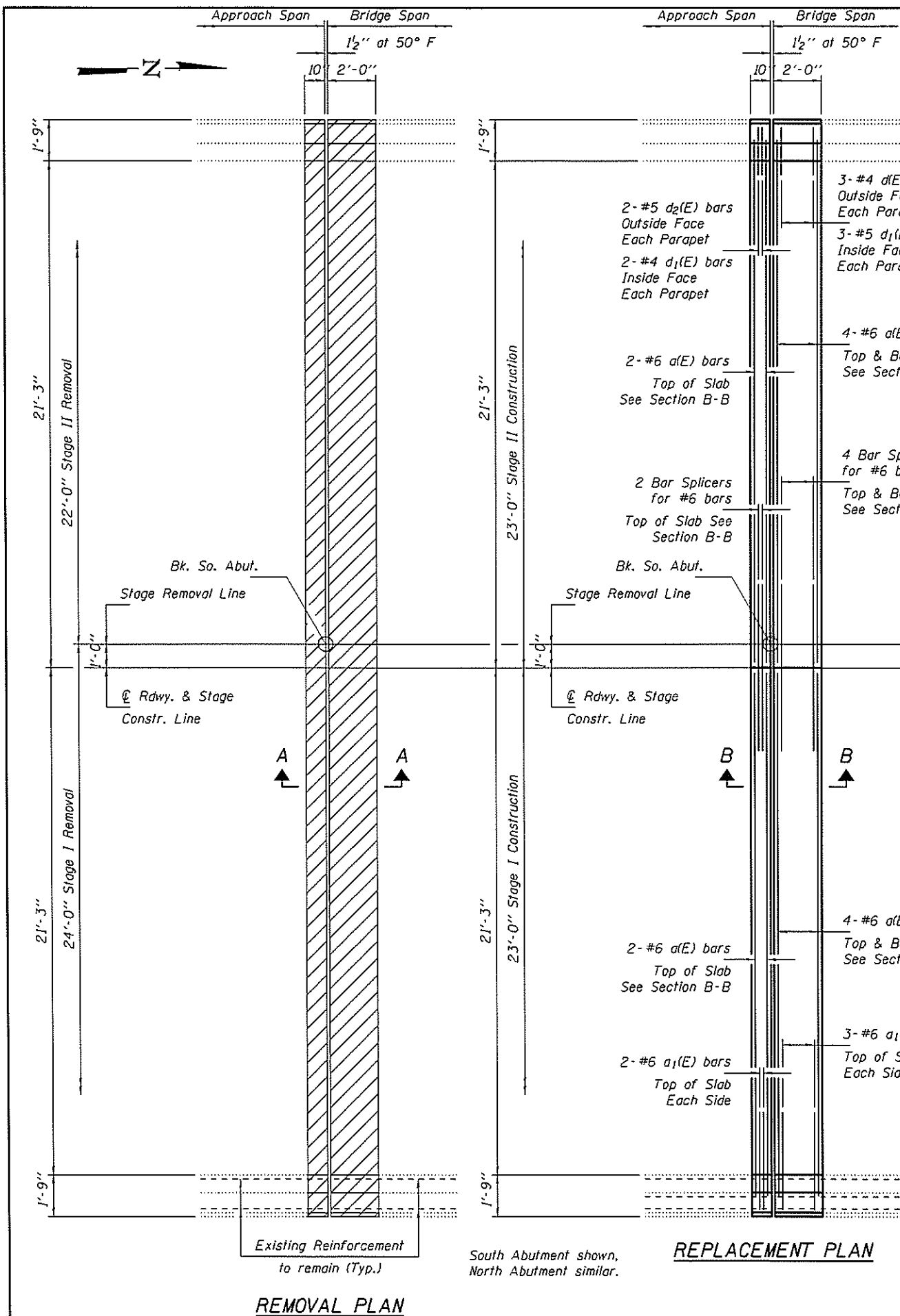
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN & ELEVATION
 T.R. RT 120 OVER F.A.I. RT. 474
 SN 090-0105

SHEET NO. 1 OF 8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	125

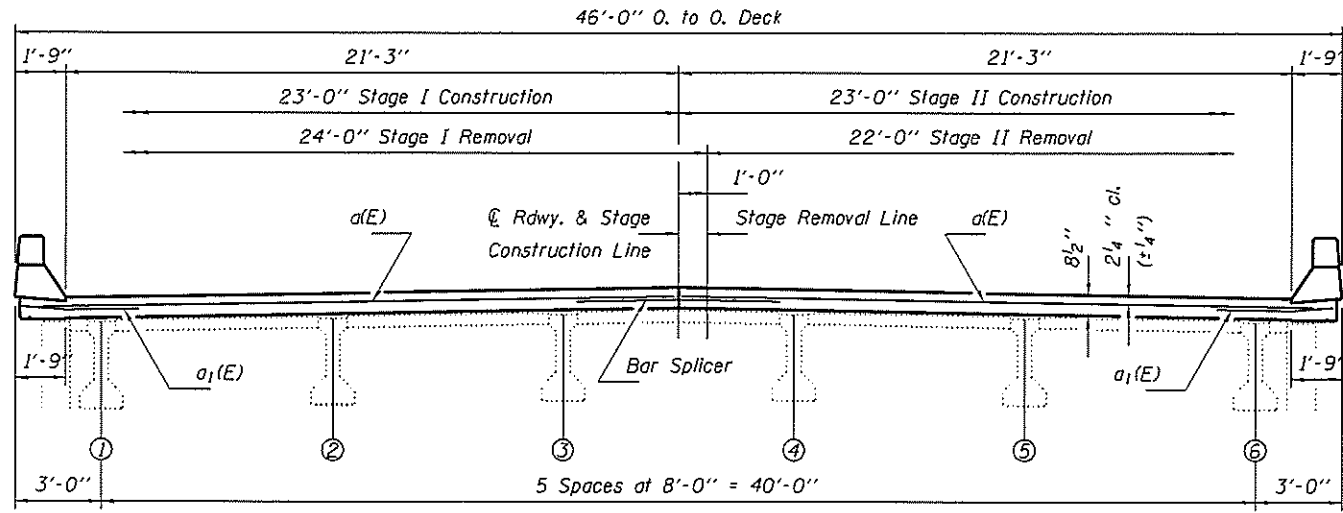
CONTRACT NO. 68606
 ILLINOIS FED. AID PROJECT



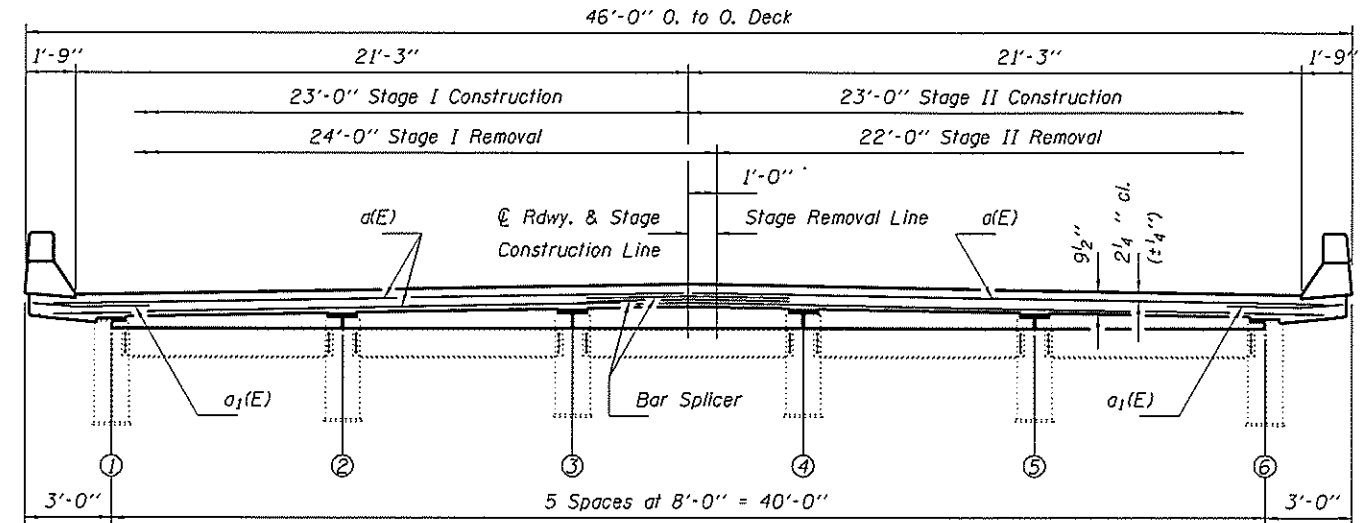
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	40	#6	22'-3"	—
a1(E)	20	#6	4'-0"	—
d(E)	12	#4	4'-7"	J
d1(E)	20	#5	3'-3"	J
d2(E)	8	#4	4'-7"	J
Concrete Removal			Cu. Yd.	9.6
Concrete Superstructure			Cu. Yd.	9.9
Reinforcement Bars, Epoxy Coated			Lbs.	1590
Bar Splicers			Each	20

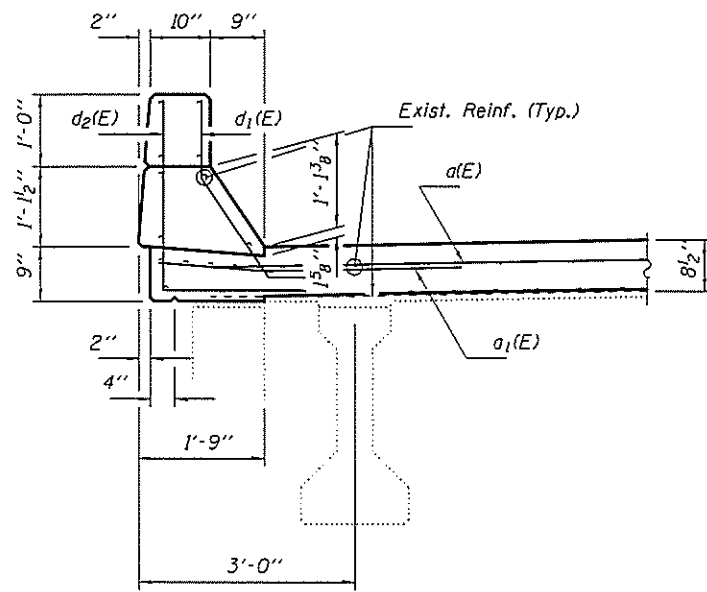
Notes:
For Cross Sections and Section thru Parapets see sheet 3 of 8.



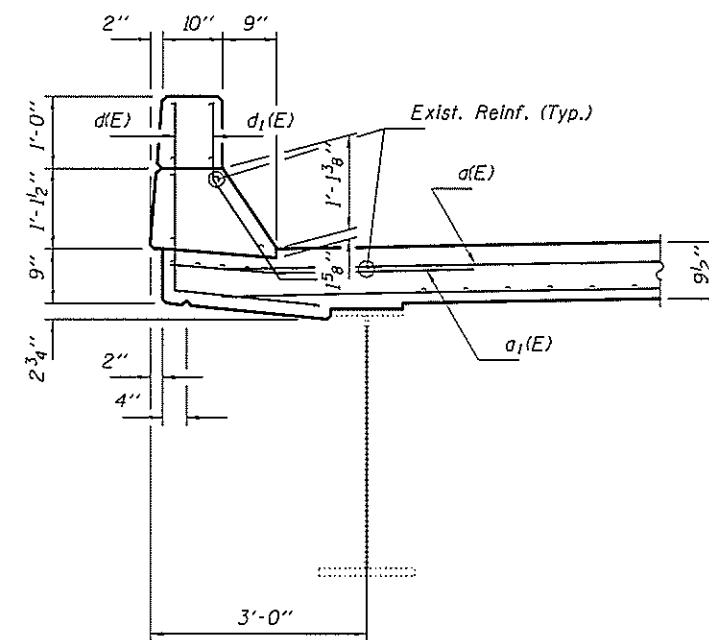
APPROACH CROSS SECTION
 (Looking South)
 (Existing Reinforcement not shown for clarity.)
 (See Section Thru Parapet for Parapet Reinforcement.)



BRIDGE CROSS SECTION
 (Looking South)
 (Existing Reinforcement not shown for clarity.)
 (See Section Thru Parapet for Parapet Reinforcement.)



SECTION THRU PARAPET APPROACH SPAN



SECTION THRU PARAPET BRIDGE SPAN

DESIGNED CCC
 CHECKED SMR
 DRAWN J. Schneller
 CHECKED CCC SMR

PASSED
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

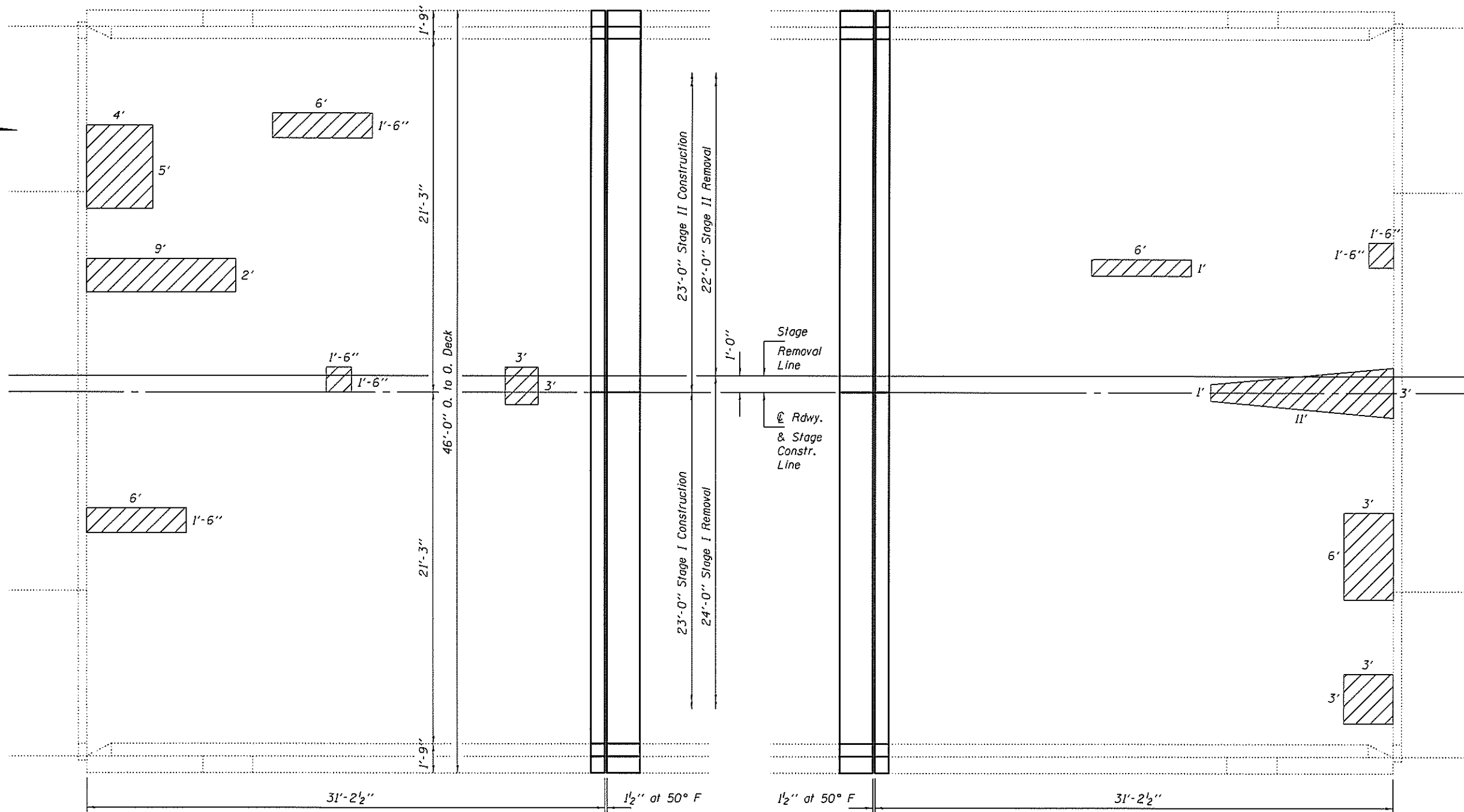
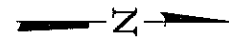
DATE JANUARY 27, 2016
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

JOINT REPLACEMENT DETAILS
 SN 090-0105

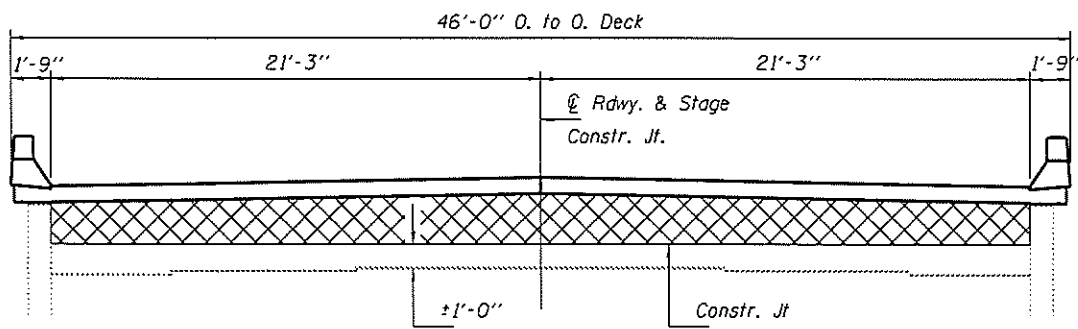
SHEET NO. 3 OF 8 SHEETS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-6,7R5-1	TAZEWELL	245	127
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



SOUTH APPROACH PLAN

NORTH APPROACH PLAN



ELEVATION

(Looking North, North Abutment & Looking South, South Abutment showing Diaphragm Repairs.)

- Deck Slab Repair (Partial). For Information only.
- Structural Repair of Concrete (Depth Equal to or Less than 5")
- Concrete Superstructure See sheet 2 of 8 for summarized repair quantity.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	361

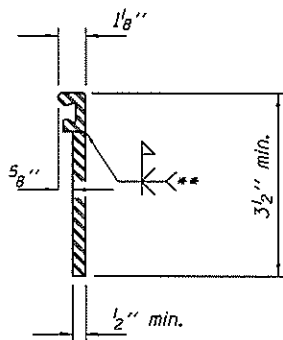
DESIGNED CCC	PASSED ACTING ENGINEER OF BRIDGES AND STRUCTURES	DATE JANUARY 27, 2016
CHECKED SMR		REVISED
DRAWN J. Schneller		REVISED
CHECKED CCC SMR		REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

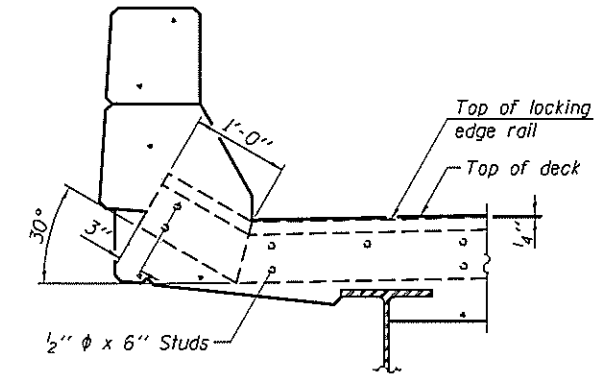
**DECK SLAB REPAIRS
SN 090-0105**

SHEET NO. 4 OF 8 SHEETS

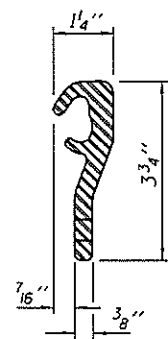
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZEWELL	245	128
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



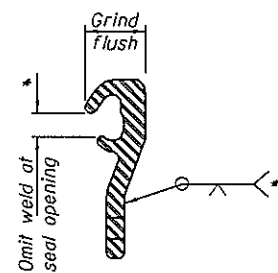
WELDED RAIL



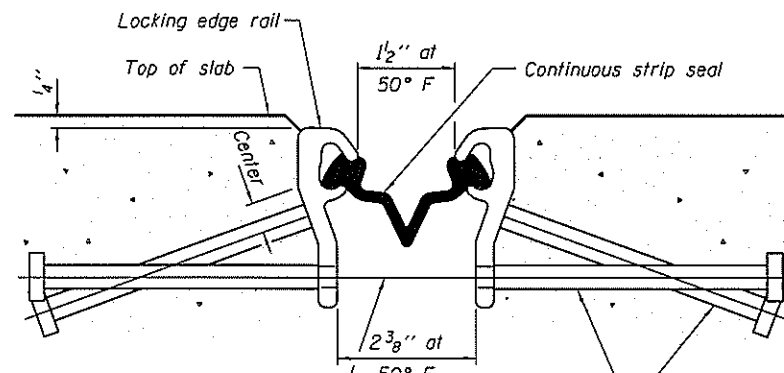
TYPICAL END TREATMENT AT PARAPETS



ROLLED (EXTRUDED) RAIL LOCKING EDGE RAIL



LOCKING EDGE RAIL SPLICE



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Place 1/2" ϕ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

SECTION THRU STRIP SEAL JOINT FOR OVERLAY AT ABUTMENT JOINTS

- * Omit weld at seal opening.
- ** Back gouge not required if complete joint penetration is verified by mock-up.

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.

The inside of the Locking Edge Rail groove shall be free of weld residue. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.

The manufacturer's recommended installation methods shall be followed.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	89

DESIGNED CCC
 CHECKED SMR
 DRAWN J. Schneller
 CHECKED CCC SMR

PASSED

Carl Propp
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016
 REVISED
 REVISED

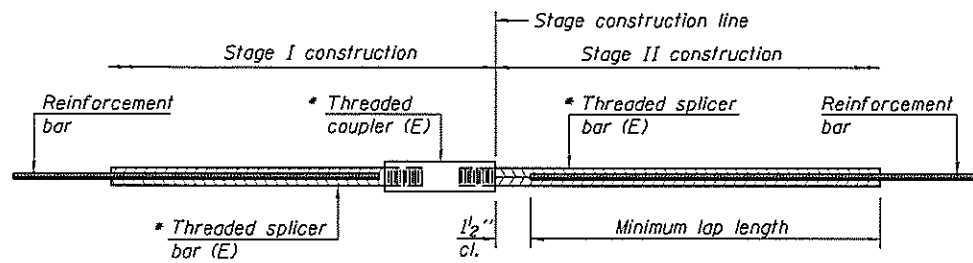
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
 SN 090-0105

SHEET NO. 5 OF 8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,1R5-1	TAZEWELL	245	129

CONTRACT NO. 68606
 [ILLINOIS] FED. AID PROJECT

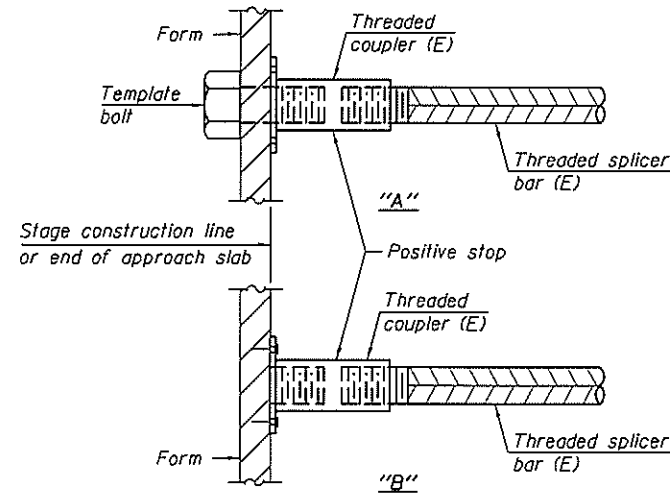


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1/2" + thread length

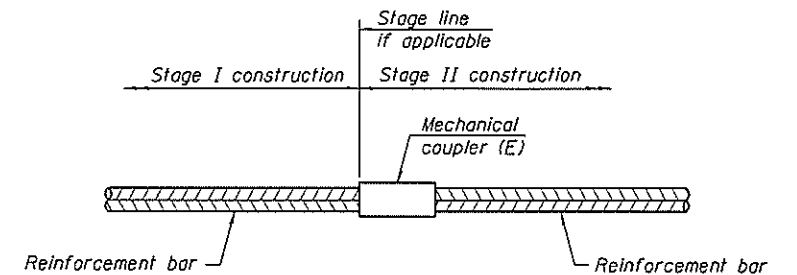
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S. Abut., S. Appr. Slab	#6	2	4'-0"
S. Abut., S. Bridge Slab	#6	8	4'-10"
N. Abut., N. Appr. Slab	#6	2	4'-0"
N. Abut., N Bridge Slab	#6	8	4'-10"



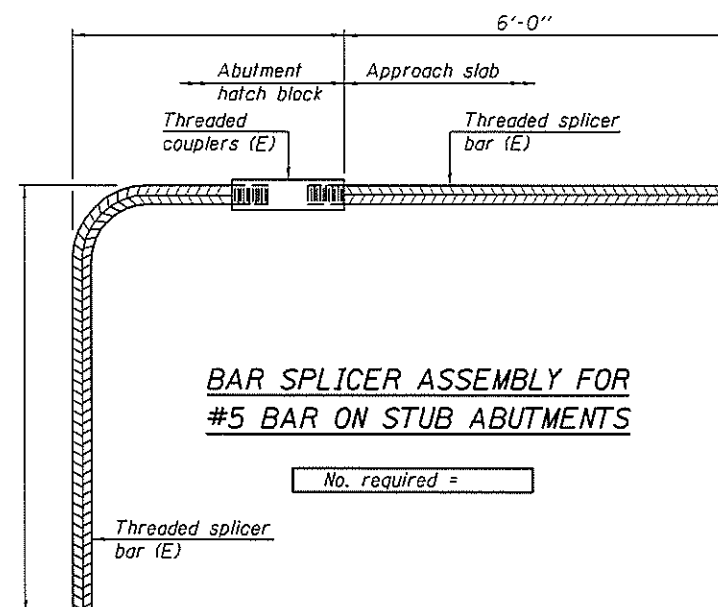
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 6-8-15

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DATE JANUARY 27, 2016
 PASSED *J. Carl Perry*
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

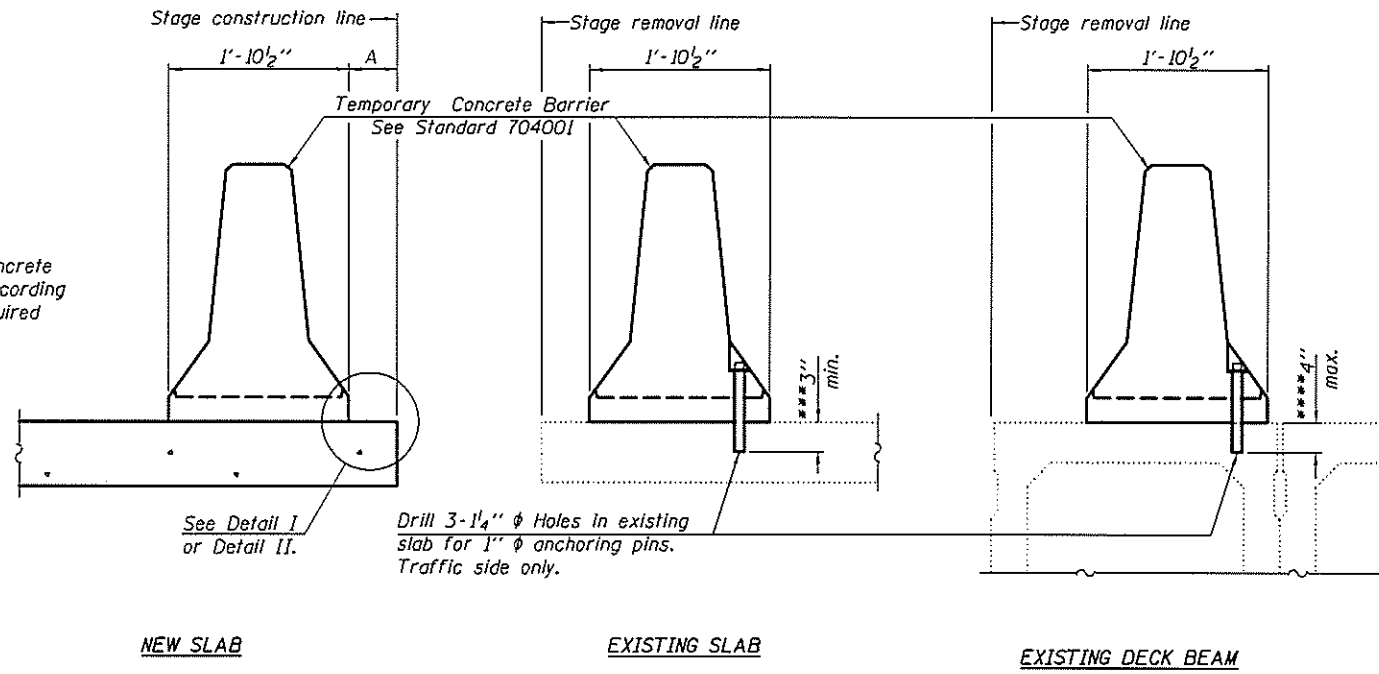
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 SN 090-0105

SHEET NO. 6 OF 8 SHEETS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7R5-1	TAZEWELL	245	130
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

When "A" is 3'-1" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-1".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

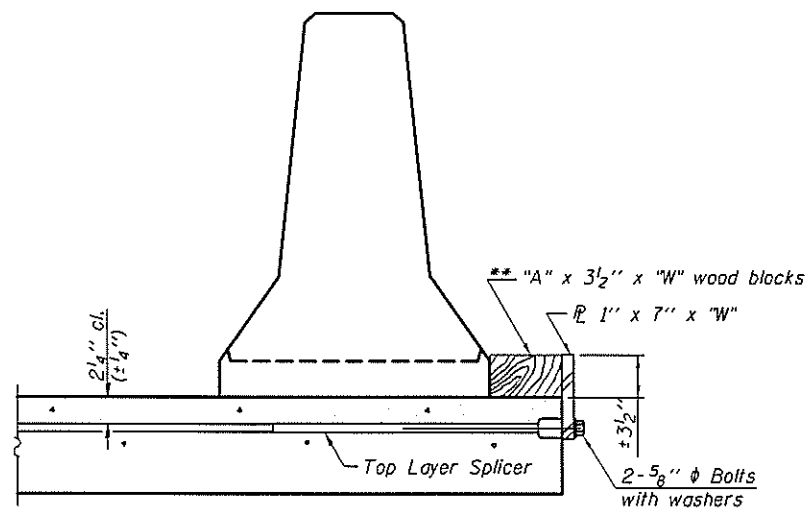
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel \bar{L} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place Inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

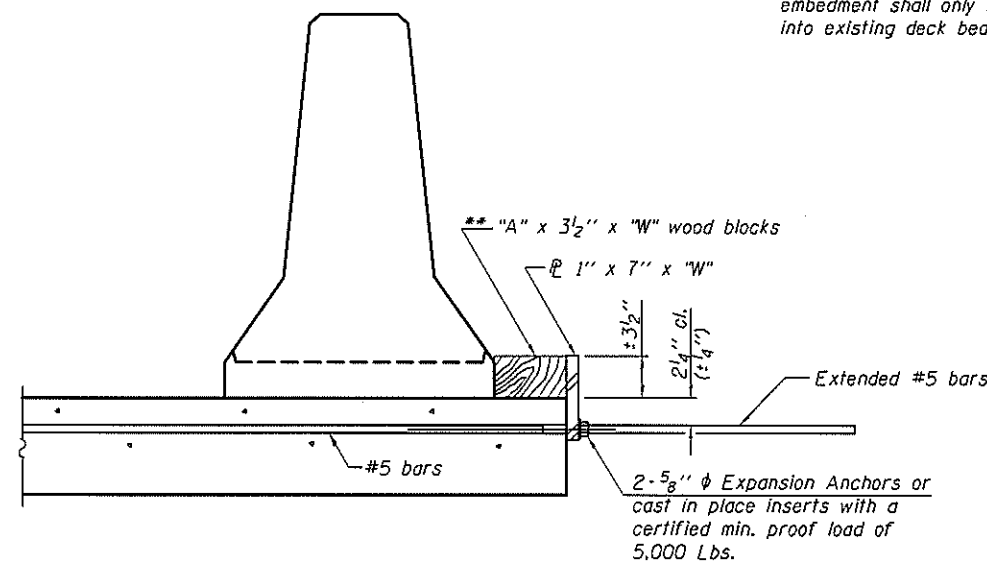
Cost of retainer assembly is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

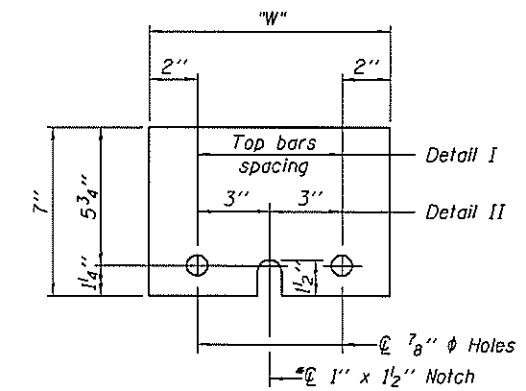
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{L} 1" x 7" x "W"

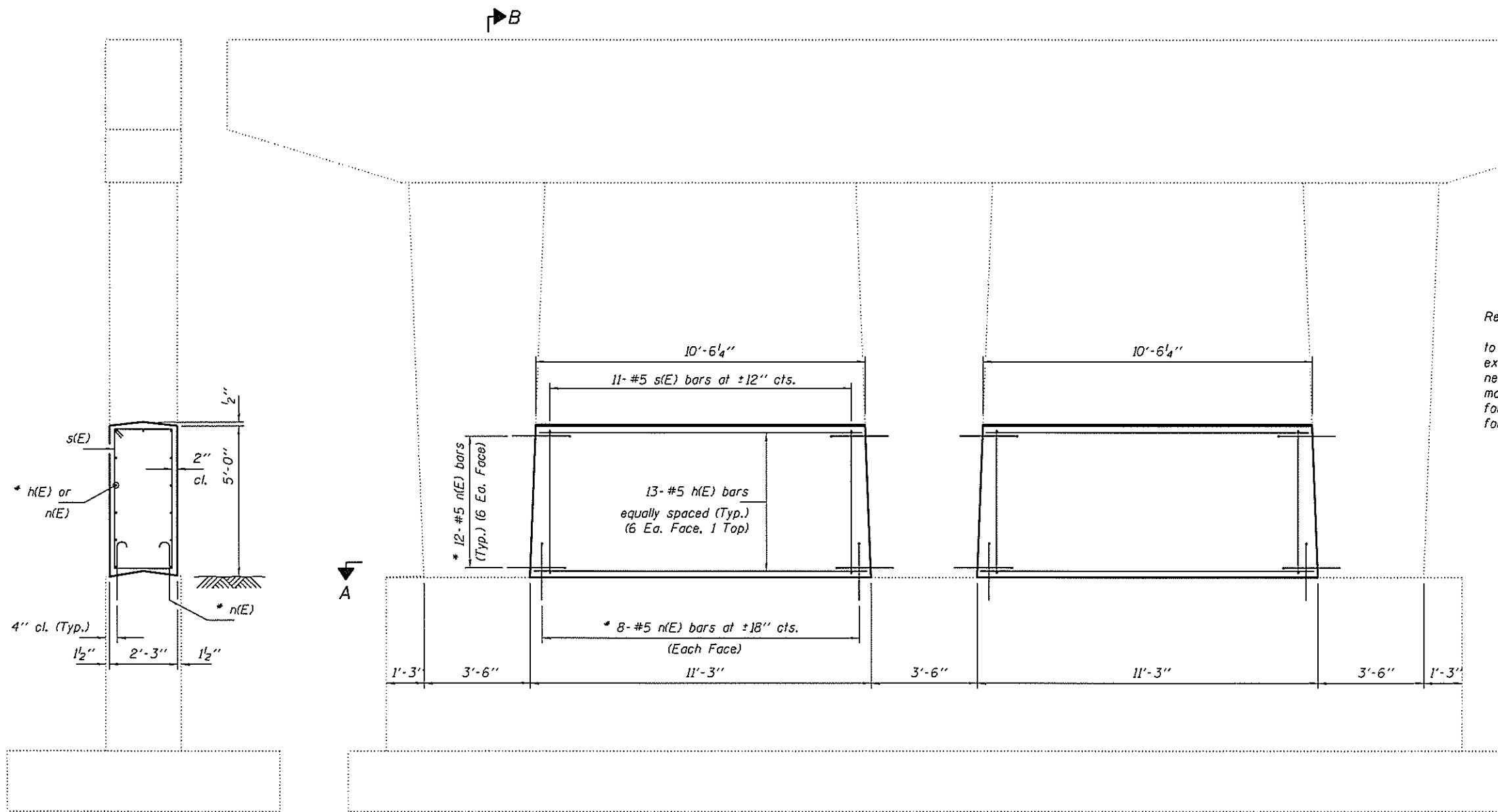
* Required only with Detail II

RETAINER ASSEMBLY

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

R-27 1-12-15

DESIGNED CCC	DATE JANUARY 27, 2016	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION SN 090-0105	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED SMR	REVISOR			474	90-6,7R5-1	TAZEWELL	245	131
DRAWN J. Schneller	REVISOR	SHEET NO. 7 OF 8 SHEETS		CONTRACT NO. 68606		ILLINOIS FED. AID PROJECT		
CHECKED CCC SMR	ACTING ENGINEER OF BRIDGES AND STRUCTURES							

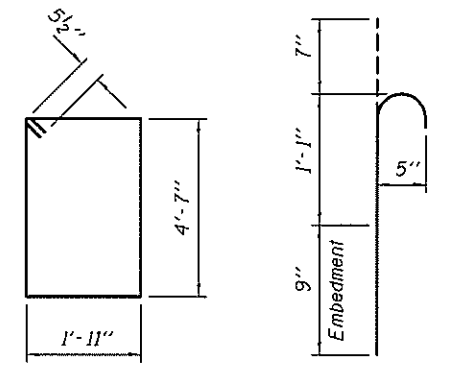


NOTES

The cost of epoxy grouting threaded rods shall be included with Reinforcement Bars, Epoxy Coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.

SECTION B-B

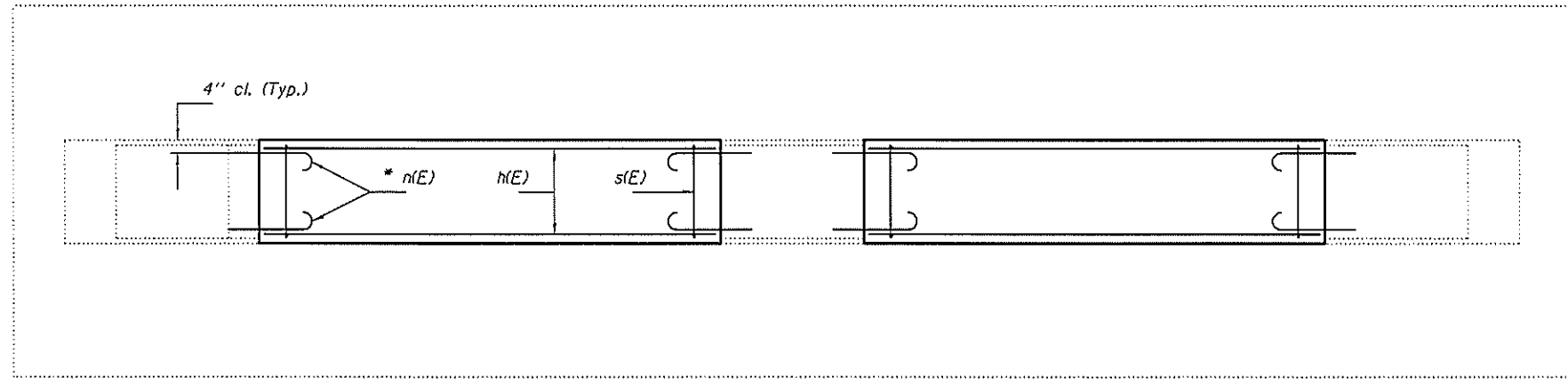
ELEVATION



BAR s(E)

BAR n(E)

* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.



SECTION A-A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
n(E)	26	#5	10'-3"	—
n(E)	80	#5	2'-5"	—
s(E)	22	#5	13'-11"	□
Concrete Structures			Cu. Yd.	9.1
Reinforcement Bars, Epoxy Coated			Pound	800

DESIGNED CCC
 CHECKED SMR
 DRAWN J. Schneller
 CHECKED CCC SMR

PASSED
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

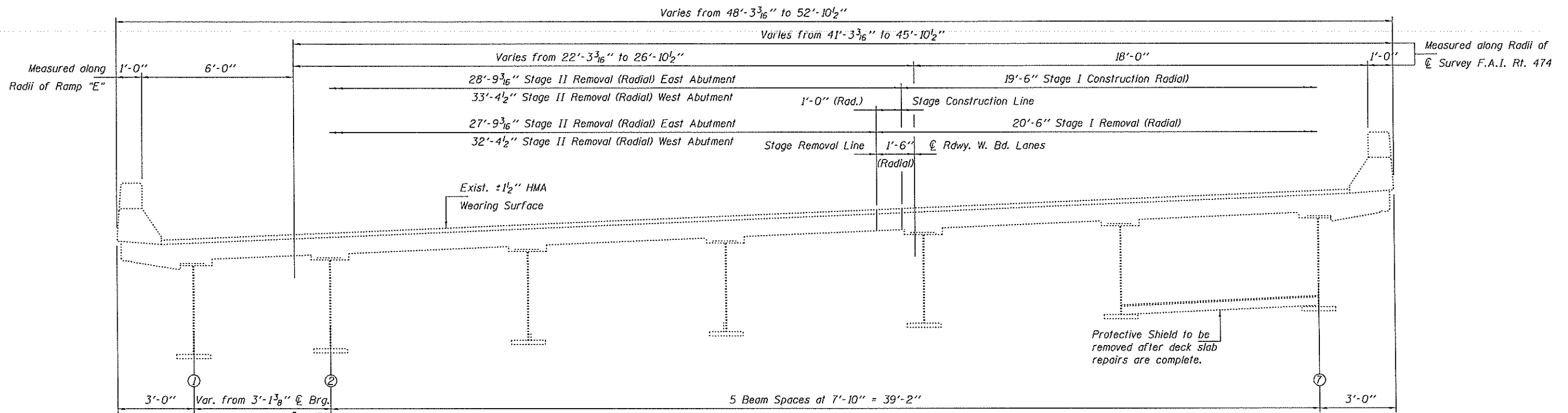
DATE JANUARY 27, 2016
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

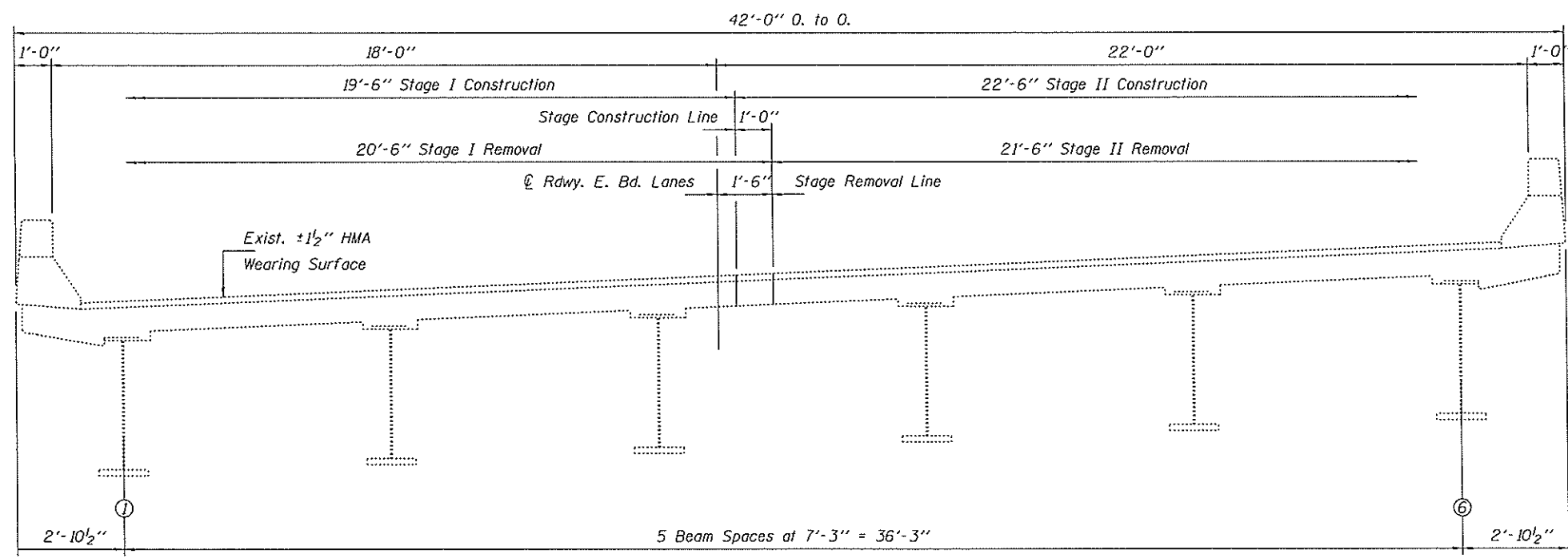
CRASHWALL EXTENSION
 SN 090-0105

SHEET NO. 8 OF 8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZEWELL	245	132
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				



CROSS SECTION WEST BOUND LANES
(Looking East)



CROSS SECTION EAST BOUND LANES
(Looking East)

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 CHECKED CCC
 DRAWN J. Schneller
 CHECKED SMR CCC

PASSED

DATE JANUARY 27, 2016

ACTING ENGINEER OF BRIDGES AND STRUCTURES

REVISED

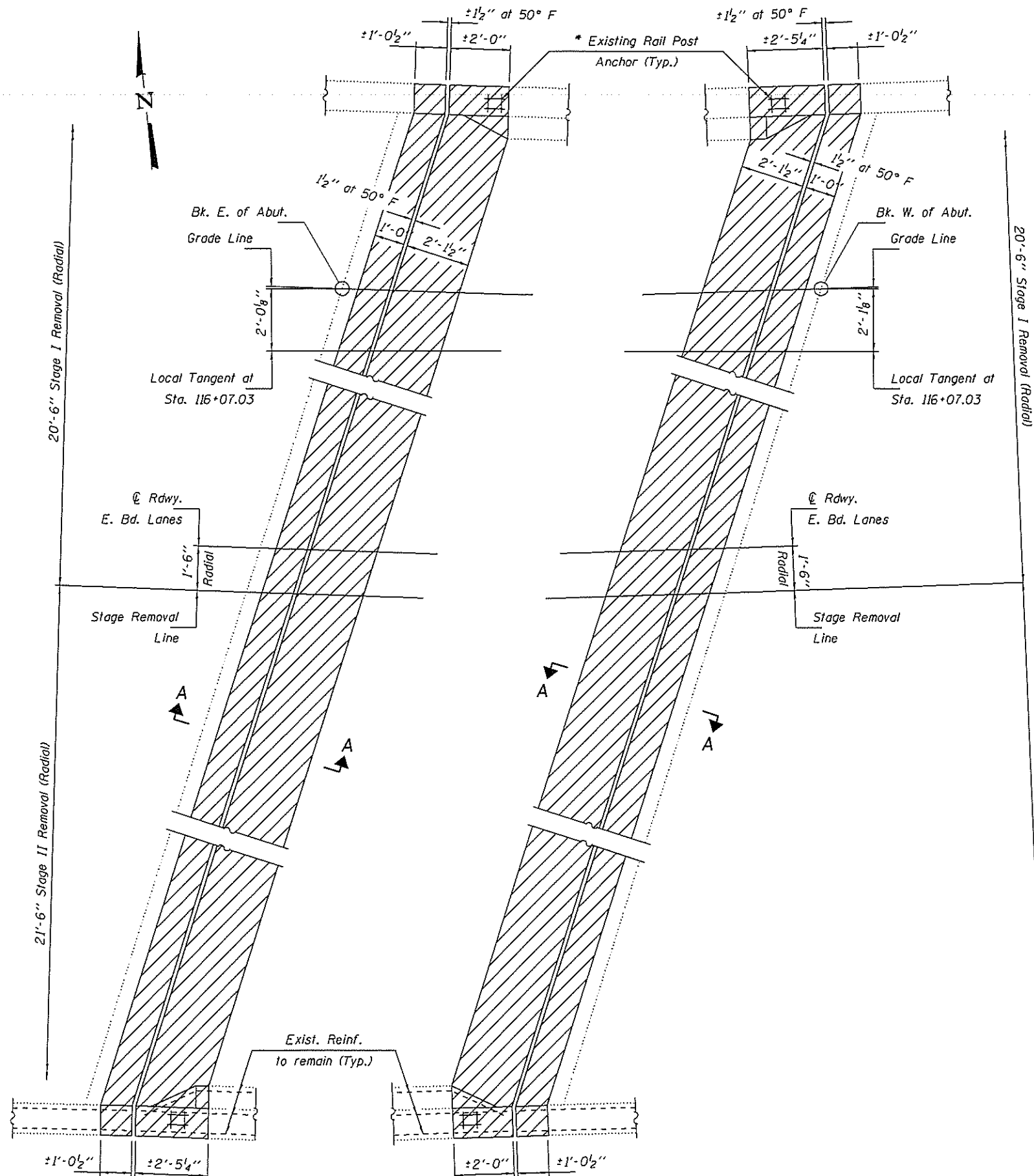
REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

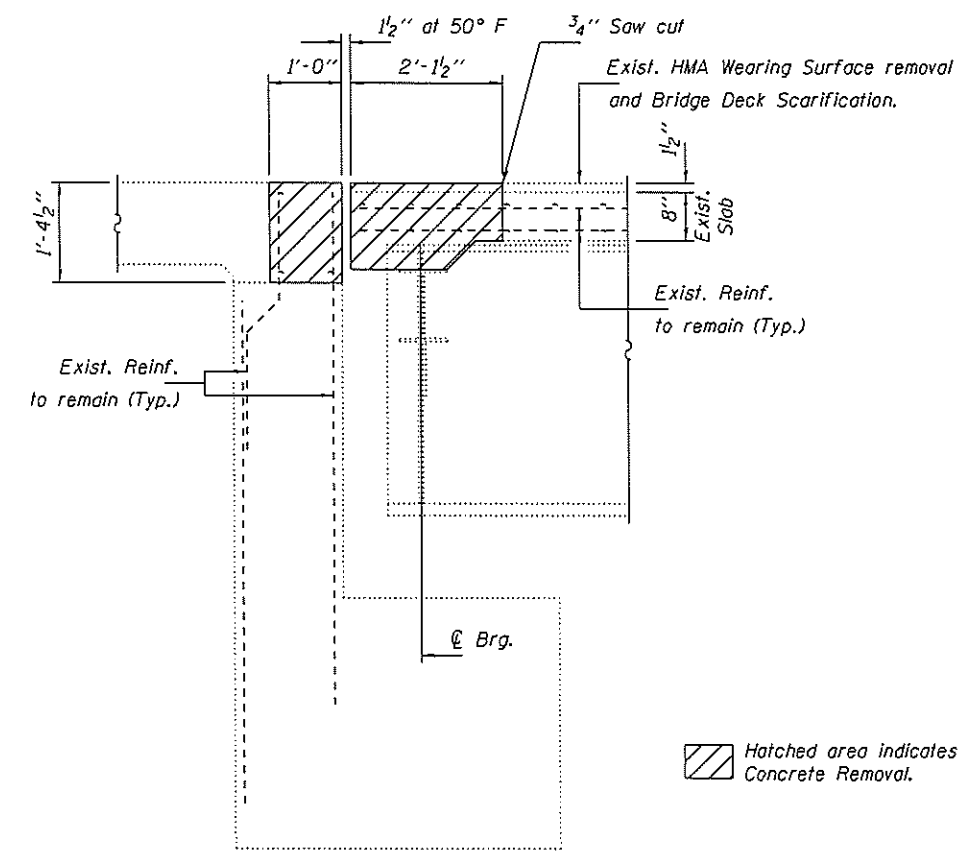
STAGE CONSTRUCTION DETAILS
 SN 090-0106 & 090-0107

SHEET NO. 2 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZENELL	245	134
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



* Existing Rail Post Anchor adjacent to Abutment Joint on the Bridge Span is to be removed and replaced. See sheet 7 of 11 for Rail Post Details.



Hatched area indicates Concrete Removal.

SECTION A-A

JOINT REMOVAL PLAN WEST ABUTMENT JOINT REMOVAL PLAN EAST ABUTMENT

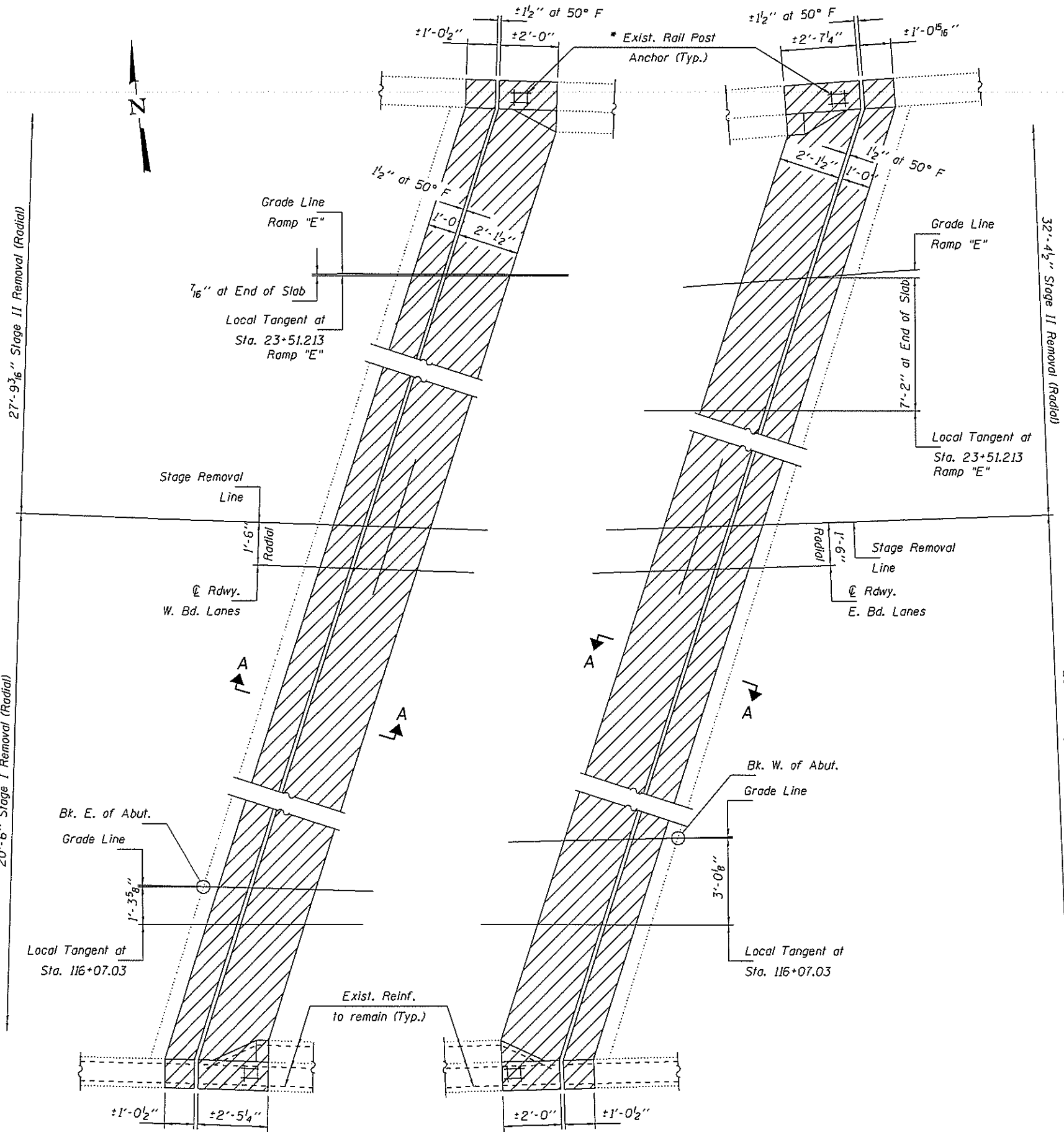
DESIGNED	SMR
CHECKED	CCC
DRAWN	J. Schneller
CHECKED	SMR CCC

PASSED	<i>J. Carl Pappas</i> ACTING ENGINEER OF BRIDGES AND STRUCTURES
DATE	JANUARY 27, 2016
REVISED	
REVISED	

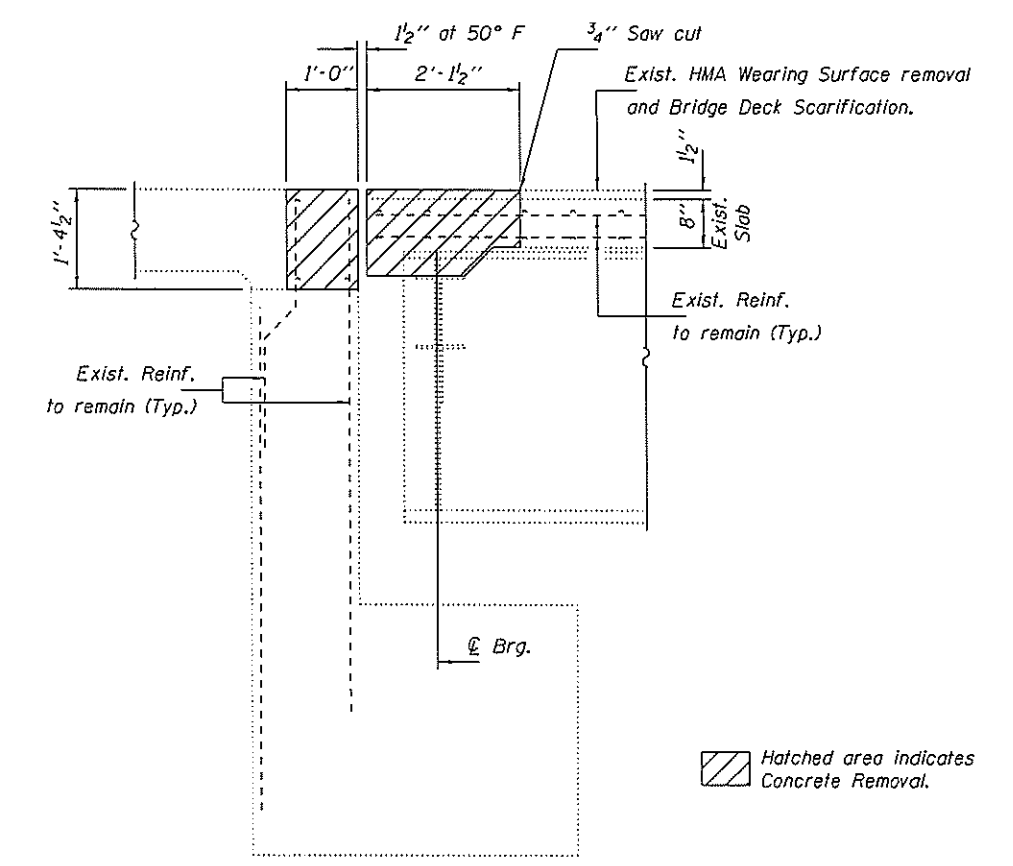
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JOINT REMOVAL DETAILS EAST BOUND LANES
SN 090-0106
SHEET NO. 3 OF 11 SHEETS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-5.7RS-1	TAZEWELL	245	135
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



* Existing Rail Post Anchor adjacent to Abutment Joint on the Bridge Span is to be removed and replaced. See sheet 7 of 11 for Rail Post Details.



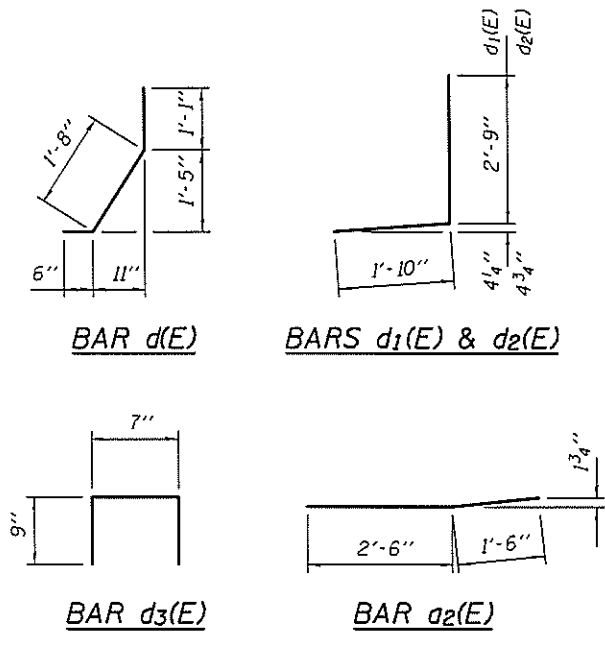
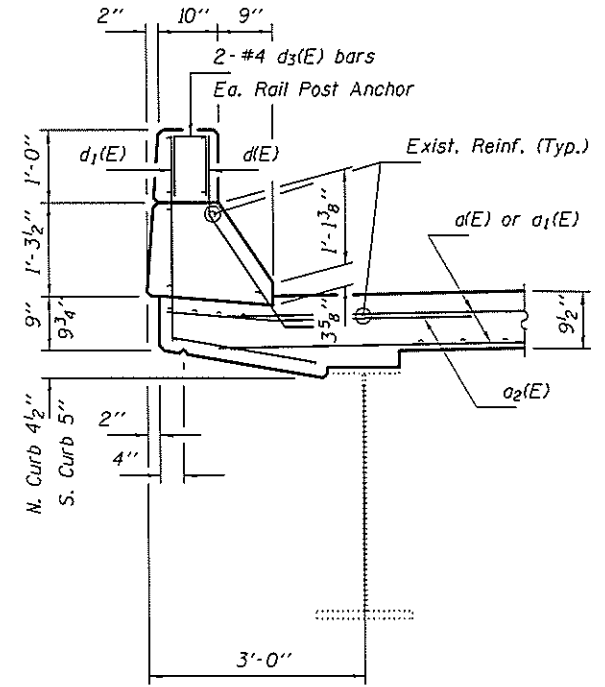
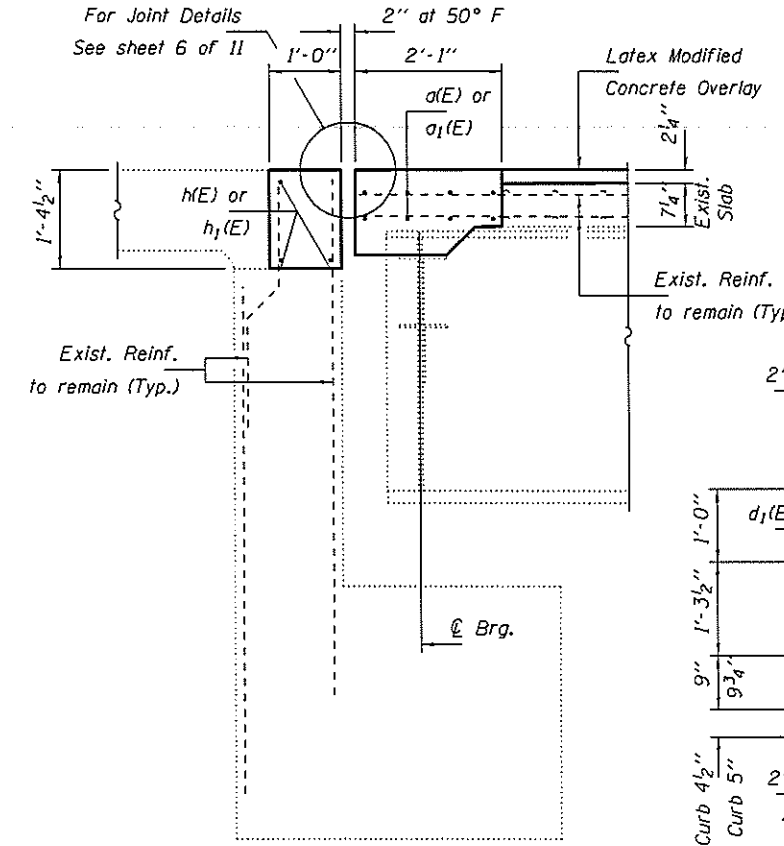
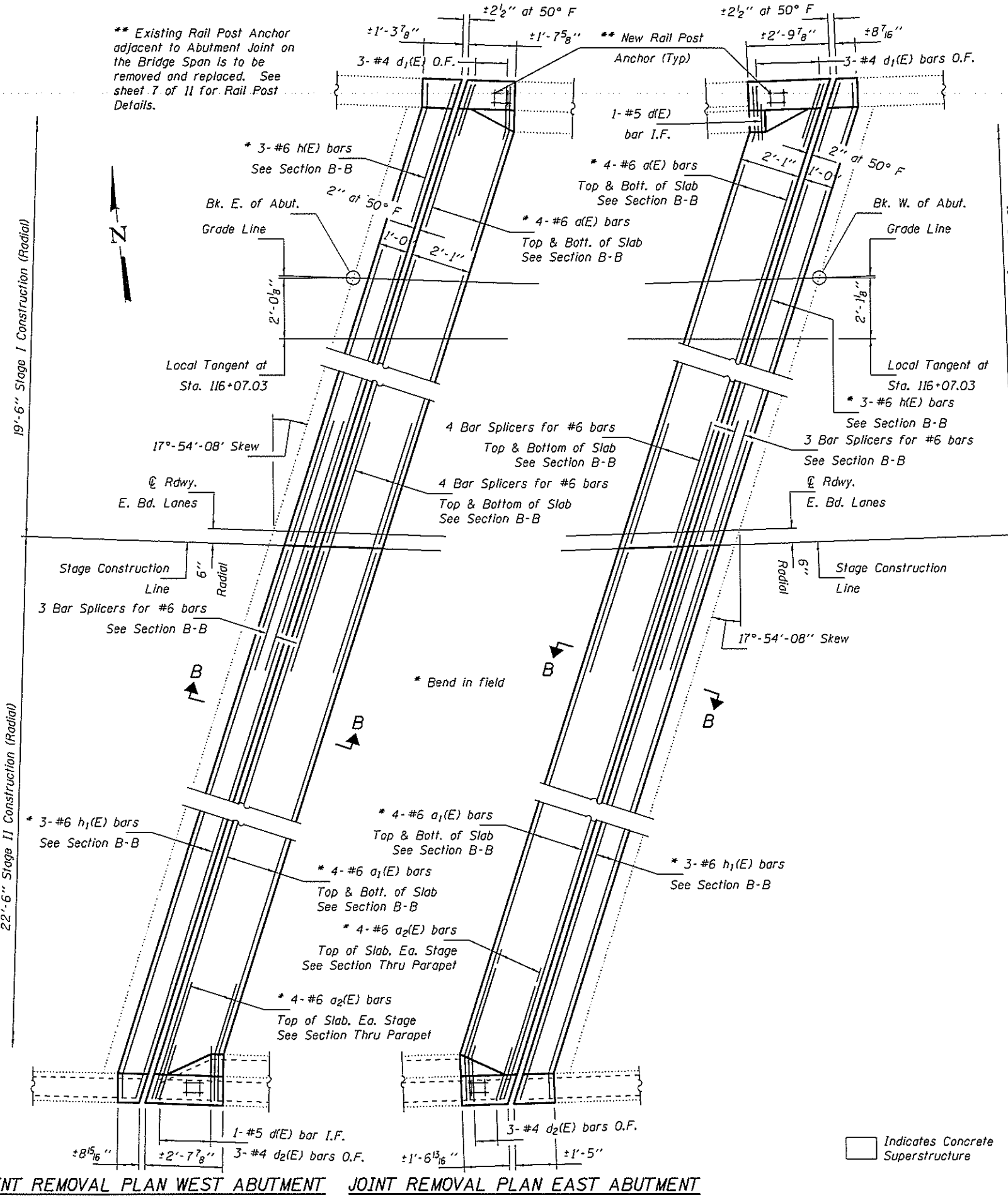
Hatched area indicates Concrete Removal.

SECTION A-A

JOINT REMOVAL PLAN WEST ABUTMENT JOINT REMOVAL PLAN EAST ABUTMENT

DESIGNED SMR	DATE JANUARY 27, 2016	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	JOINT REMOVAL DETAILS WEST BOUND LANES SN 090-0107		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED CCC	REVISED				414	90-6,785-1	TAZEWELL	245	136
DRAWN J. Schneller	REVISED				CONTRACT NO. 68606				
CHECKED SMR CCC	ACTING ENGINEER OF BRIDGES AND STRUCTURES				SHEET NO. 4 OF 11 SHEETS				
PASSED				ILLINOIS FED. AID PROJECT					

** Existing Rail Post Anchor adjacent to Abutment Joint on the Bridge Span is to be removed and replaced. See sheet 7 of 11 for Rail Post Details.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a1(E)	16	#6	20'-1"	—
a2(E)	16	#6	23'-2"	—
a2(E)	16	#6	4'-0"	—
h1(E)	6	#6	20'-3"	—
h2(E)	6	#6	23'-4"	—
d1(E)	2	#5	3'-3"	J
d1(E)	6	#4	4'-7"	J
d2(E)	6	#4	4'-7"	J
d3(E)	8	#4	2'-1"	U
Concrete Removal			Cu. Yd.	13.0
Concrete Superstructure			Cu. Yd.	13.0
Reinforcement Bars, Epoxy Coated			Lbs.	1580
Bar Splicers			Each	22

Notes:
For Cross Section see sheet 2 of 11.

JOINT REMOVAL PLAN WEST ABUTMENT JOINT REMOVAL PLAN EAST ABUTMENT

DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

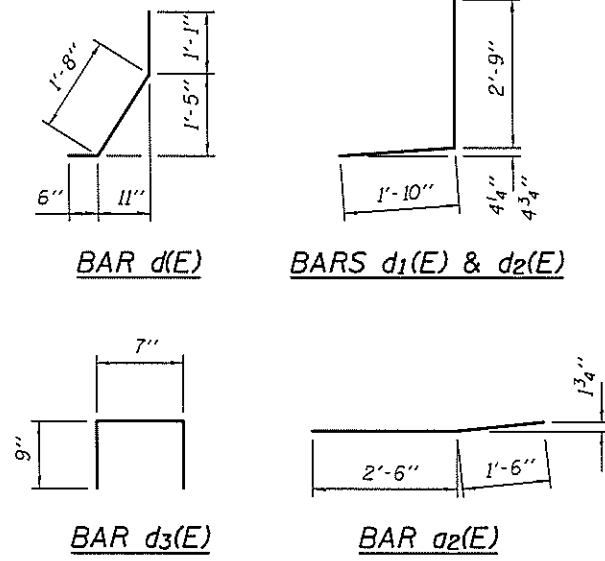
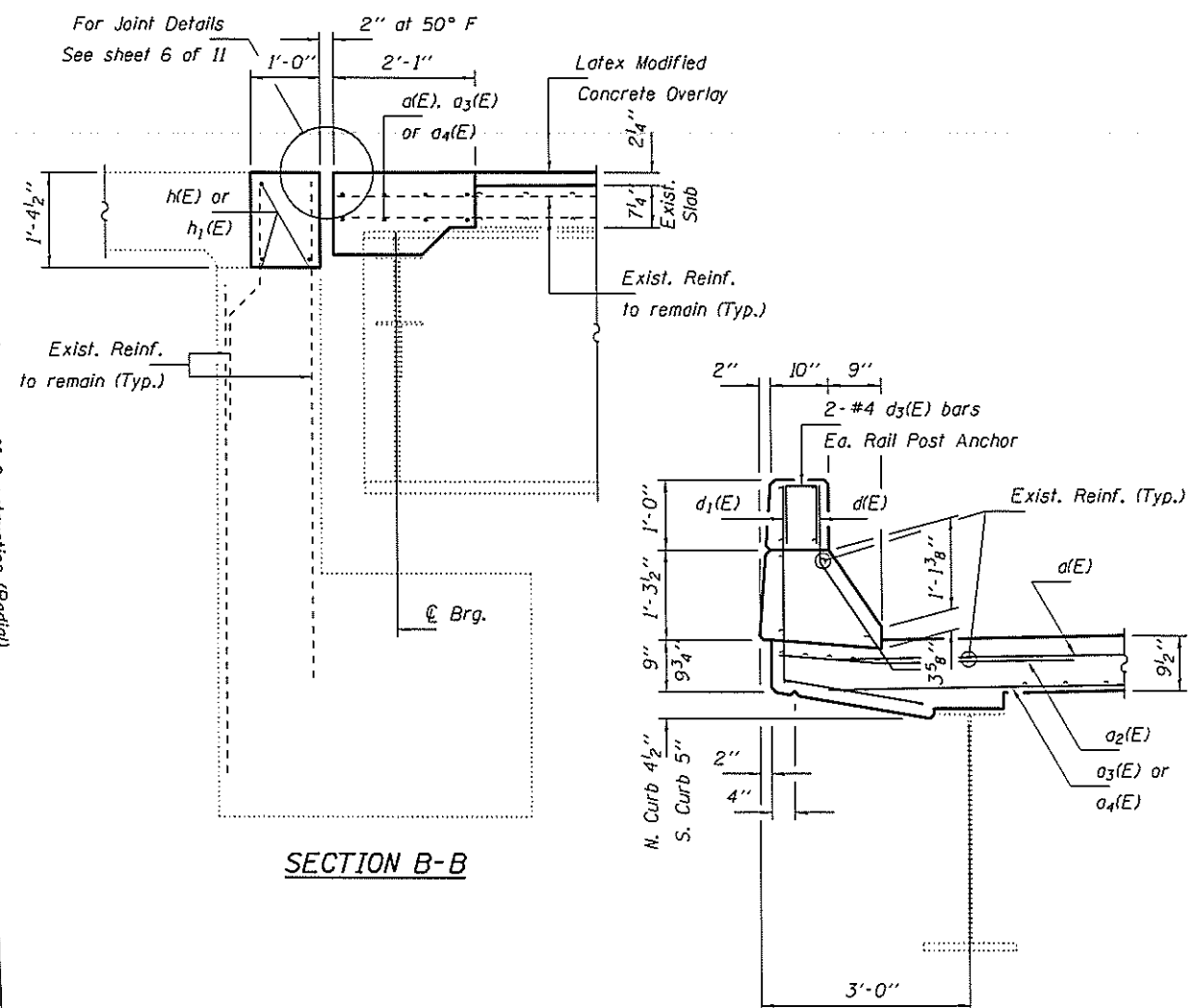
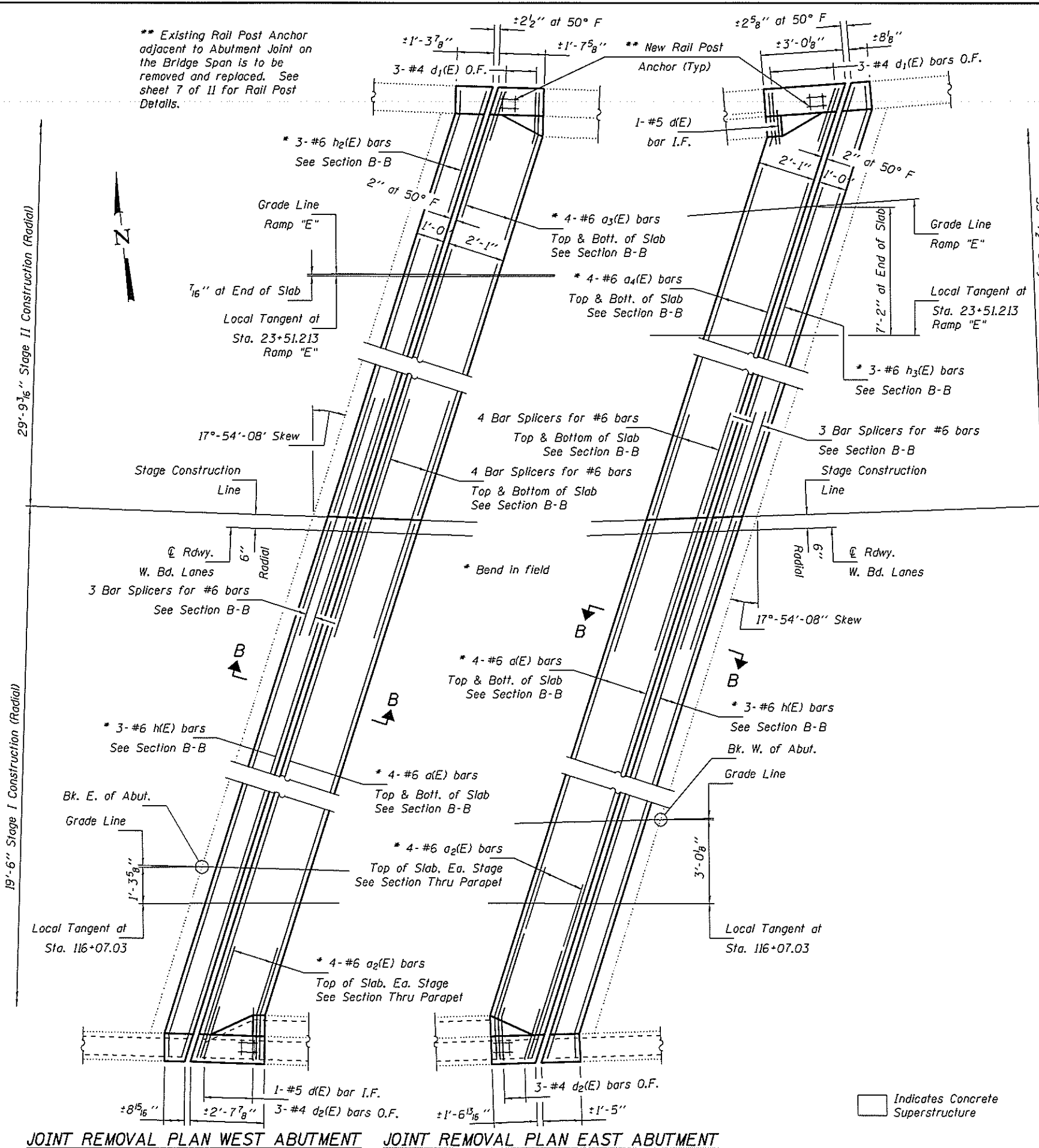
DATE JANUARY 27, 2016
PASSED
ACTING ENGINEER OF BRIDGES AND STRUCTURES
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JOINT REPLACEMENT DETAILS EAST BOUND LANES
SN 090-0106
SHEET NO. 5 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-6,785-1	TAZEWELL	245	137
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	

** Existing Rail Post Anchor adjacent to Abutment Joint on the Bridge Span is to be removed and replaced. See sheet 7 of 11 for Rail Post Details.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	16	#6	20'-1"	—
a2(E)	16	#6	4'-0"	—
a3(E)	8	#6	31'-0"	—
a4(E)	8	#6	34'-8"	—
h(E)	6	#6	20'-3"	—
h2(E)	3	#6	31'-1"	—
h3(E)	3	#6	34'-11"	—
d(E)	2	#5	3'-3"	J
d1(E)	6	#4	4'-7"	J
d2(E)	6	#4	4'-7"	J
d3(E)	8	#4	2'-1"	U
Concrete Removal		Cu. Yd.	14.3	
Concrete Superstructure		Cu. Yd.	14.3	
Reinforcement Bars, Epoxy Coated		Lbs.	1900	
Bar Splicers		Each	22	

Notes:
For Cross Section see sheet 2 of 11.

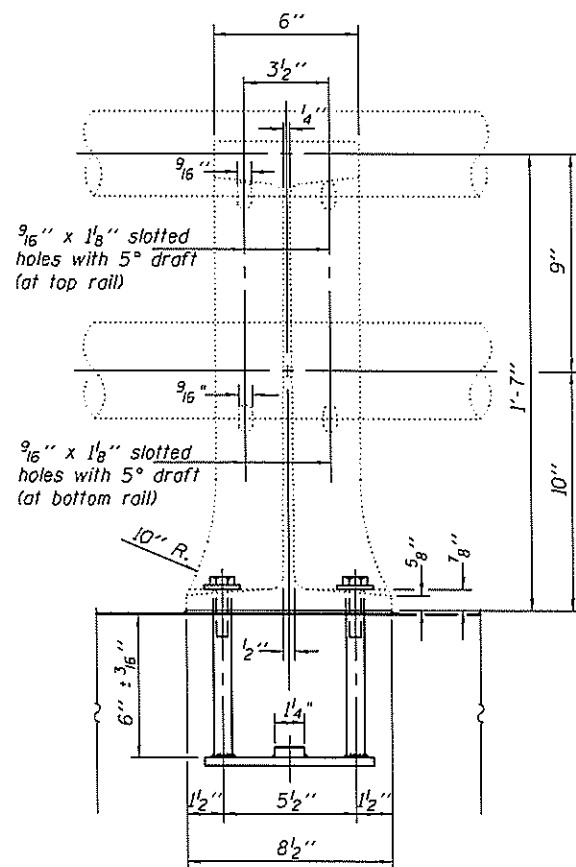
DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

DATE JANUARY 27, 2016
PASSED
ACTING ENGINEER OF BRIDGES AND STRUCTURES
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

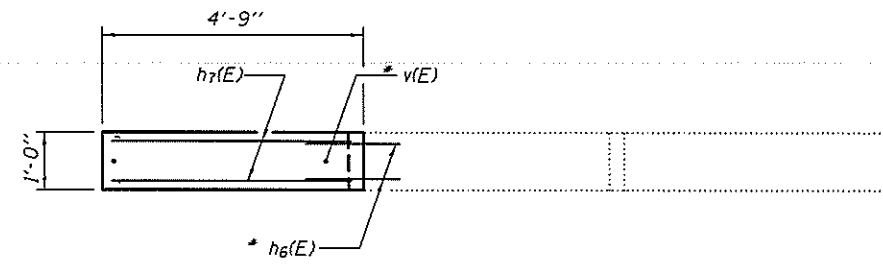
JOINT REPLACEMENT DETAILS WEST BOUND LANES
SN 090-0107
SHEET NO. 6 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-6,785-1	TAZEWELL	245	138
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	

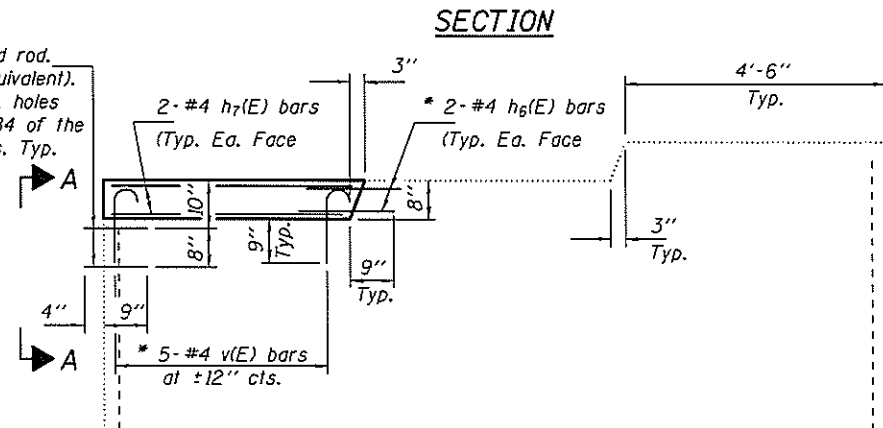


RAIL POST DETAILS
(8 Locations Required)

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications. Cost included with Concrete Superstructure.

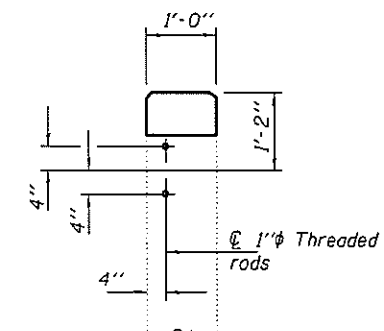


1"φ x 13" HS Threaded rod.
(ASTM A193, B7 or equivalent).
Epoxy grout in 9" min. holes
according to Article 584 of the
Standard Specifications. Typ.

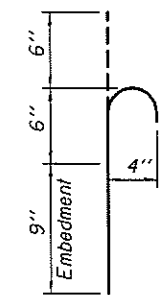


WINGWALL MODIFICATION
SN 090-0107 WEST ABUTMENT ONLY

* Epoxy grout $h_6(E)$ & $v(E)$ bars in 9" min. holes according to Article 584 of the Standard Specifications.



VIEW A-A



BAR v(E)

BILL OF MATERIAL
2 WINGWALLS

Bar	No.	Size	Length	Shape	
$h_6(E)$	8	#4	1'-9"	—	
$h_7(E)$	8	#4	3'-11"	—	
$v(E)$	10	#4	1'-9"	⌋	
Concrete Superstructure				Cu. Yd.	0.2
Reinforcement Bars, Epoxy Coated				Pound	40

DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

DATE JANUARY 27, 2016
PASSED
ACTING ENGINEER OF BRIDGES AND STRUCTURES

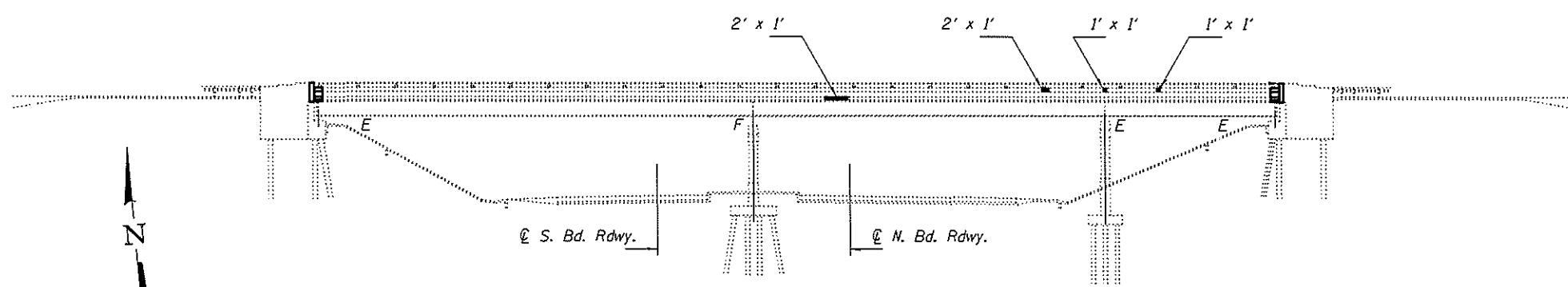
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

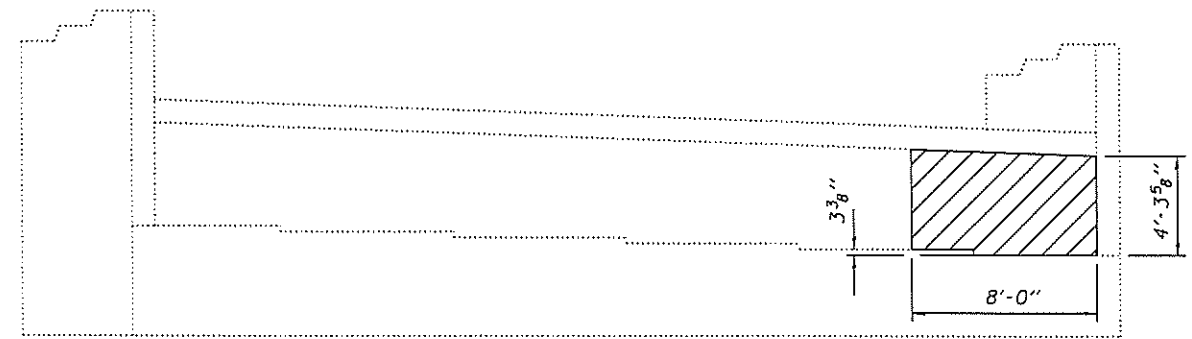
RAIL POST ANCHOR DETAILS
AND WINGWALL MODIFICATIONS
SN 090-0106 & 090-0107
SHEET NO. 7 OF 11 SHEETS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-6,785-1	TAZEWELL	245	139

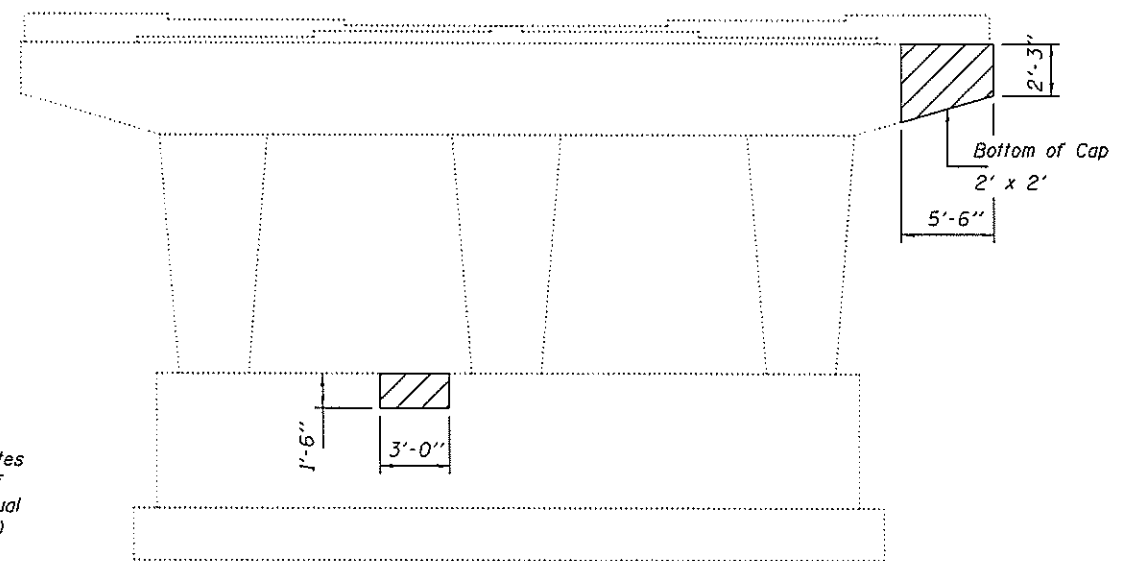
CONTRACT NO. 68606
ILLINOIS FED. AID PROJECT



ELEVATION WEST BD. LANES
(Looking North, Str. No. 090-0107)



ELEVATION WEST ABUTMENT EAST BD. LANES
(Looking West, Str. No. 090-0106)



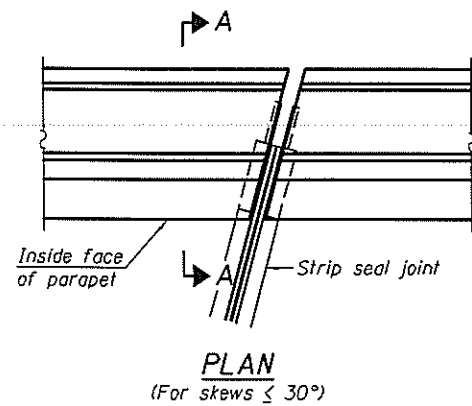
ELEVATION PIER #1 EAST BD. LANES
(Looking West, Str. No. 090-0106)

Hatched area indicates
Structural Repair of
Concrete (Depth Equal
to or Less than 5')

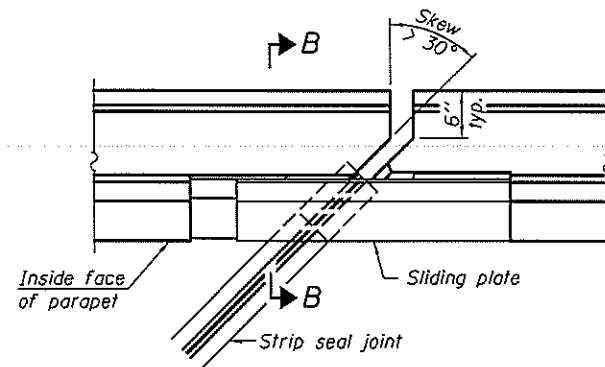
Note:
Areas of repairs shown are estimated. The Engineer shall show actual locations of repairs on As-built Plans.

BILL OF MATERIAL

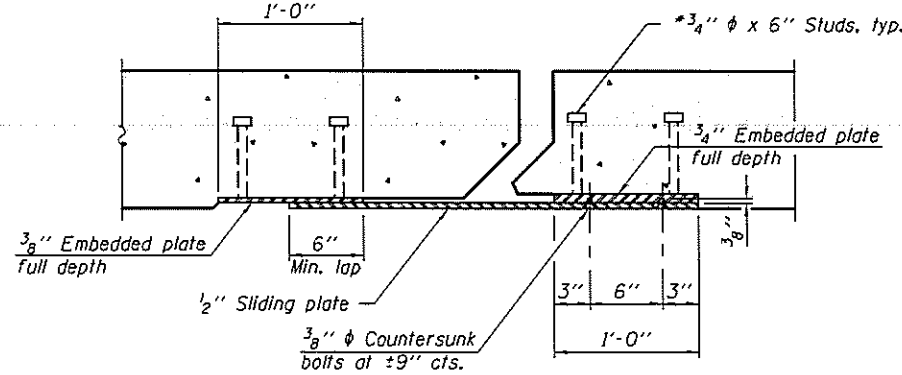
ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less than 5')	Sq. Ft.	56.8



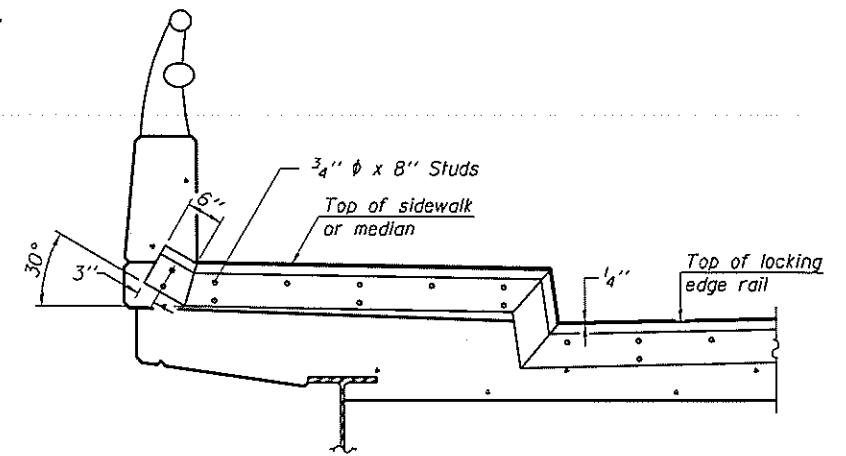
PLAN
(For skews $\leq 30^\circ$)



PLAN
(For skews $> 30^\circ$)
Showing point block

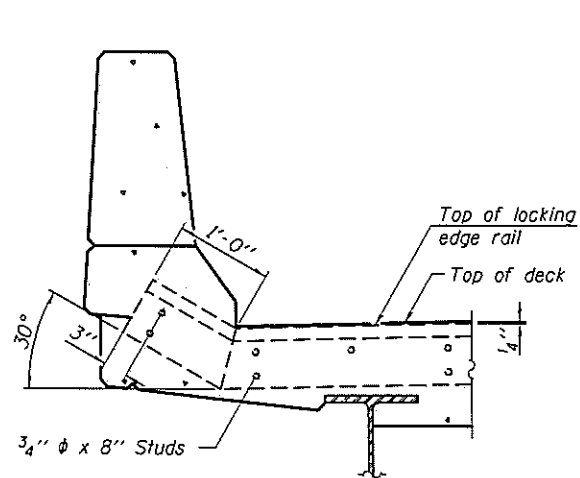


SECTION C-C

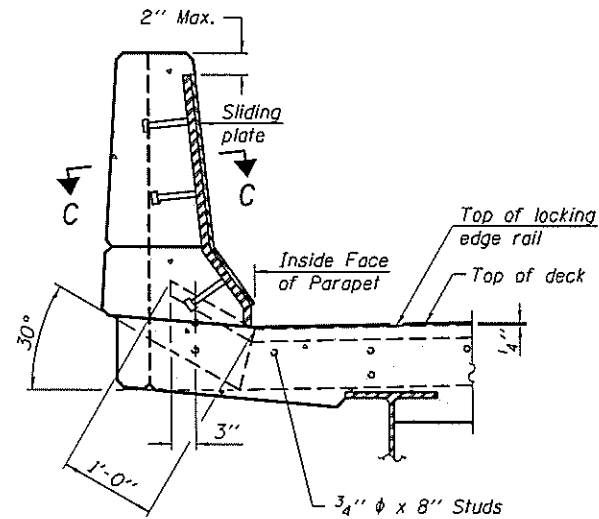


TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

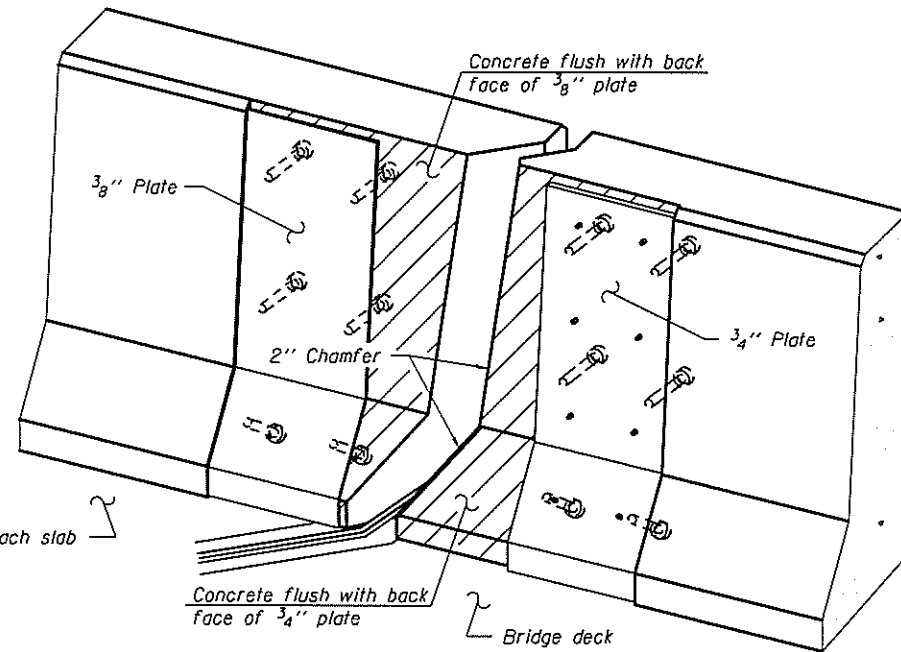
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



SECTION A-A



SECTION B-B

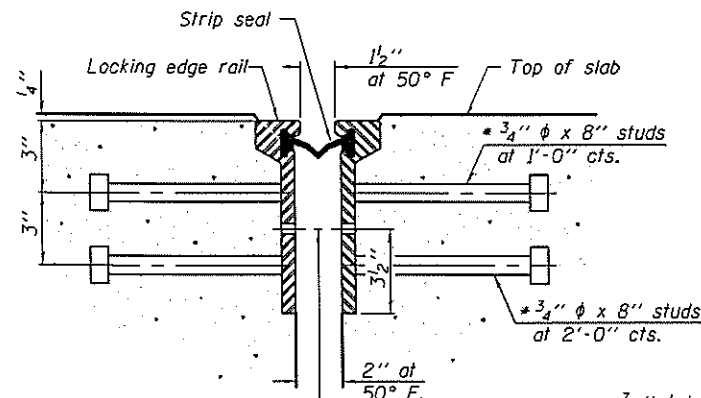


TRIMETRIC VIEW
(Showing back plates only)

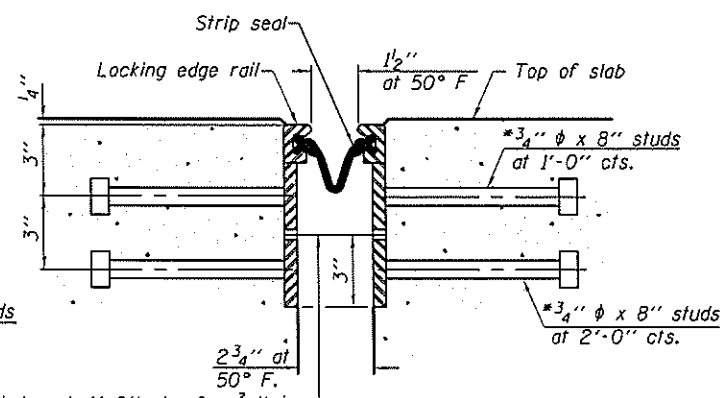
Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

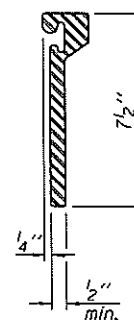
Parapet plates and anchorage studs for skews $> 30^\circ$ included in the cost of Preformed Joint Strip Seal.



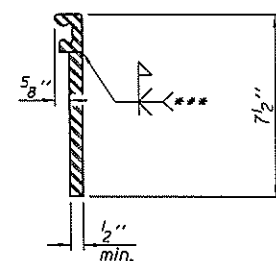
SECTION THRU ROLLED RAIL JOINT



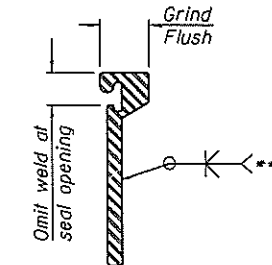
SECTION THRU WELDED RAIL JOINT



ROLLED EXTRUDED RAIL



WELDED RAIL



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

*** Back gauge not required if complete joint penetration is verified by mock-up.

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

LOCKING EDGE RAILS

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	190

EJ-SSJ 1-27-12

DESIGNED	SMR
CHECKED	CCC
DRAWN	J. Schneller
CHECKED	SMR CCC

PASSED	<i>A. Carl Papp</i>
	ACTING ENGINEER OF BRIDGES AND STRUCTURES

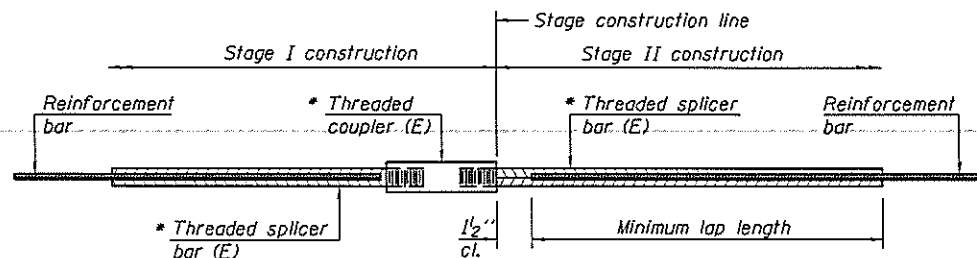
DATE	JANUARY 27, 2016
REVISED	
REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
SN 090-0106 & 090-0107

SHEET NO. 9 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
414	90-5,7RS-1	TAZEWELL	245	141
			CONTRACT NO. 68606	
[ILLINOIS] FED. AID PROJECT				

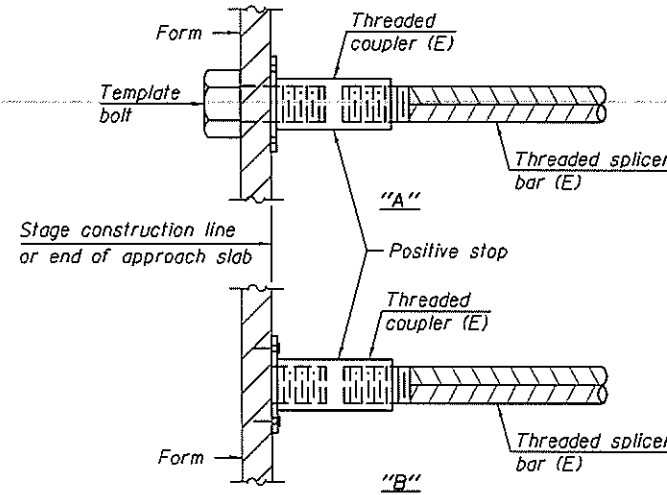


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1/2" + thread length

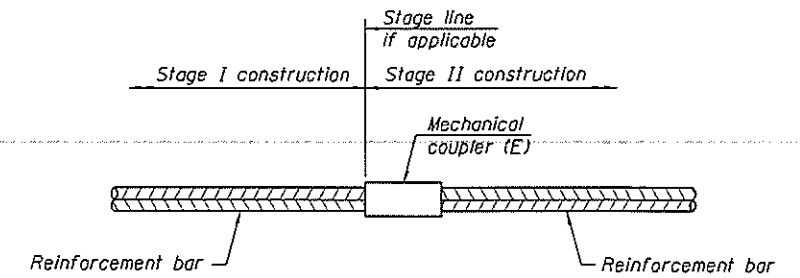
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
W. Abut. Hatched Block	#6	6	4'-0"
W. End of Deck	#6	16	4'-10"
E. Abut. Hatched Block	#6	6	4'-0"
E. End of Deck	#6	16	4'-10"



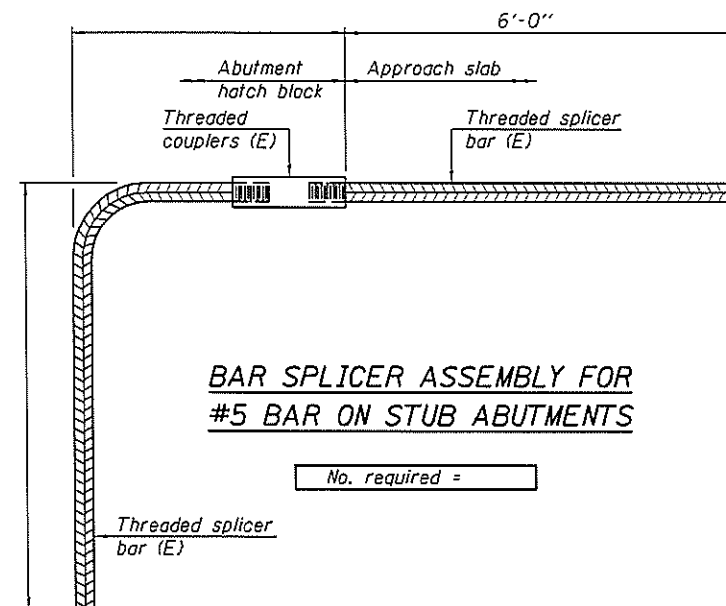
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 6-8-15

DESIGNED SMR
 CHECKED CCC
 DRAWN J. Schneller
 CHECKED SMR CCC

PASSED

 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 27, 2016
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

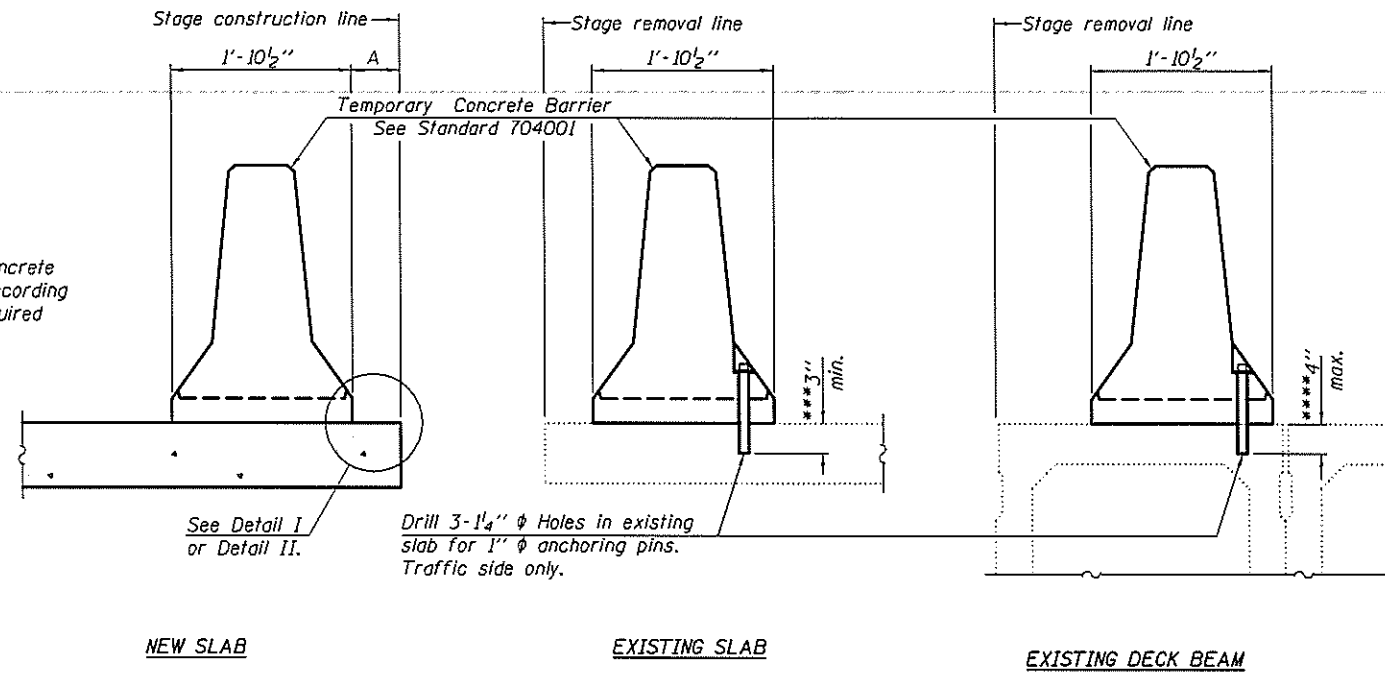
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 SN 090-0106 & 090-0107

SHEET NO. 10 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-5,785-1	TAZEWELL	245	142

CONTRACT NO. 68606
 ILLINOIS FED. AID PROJECT

When "A" is 3'-1" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-1".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

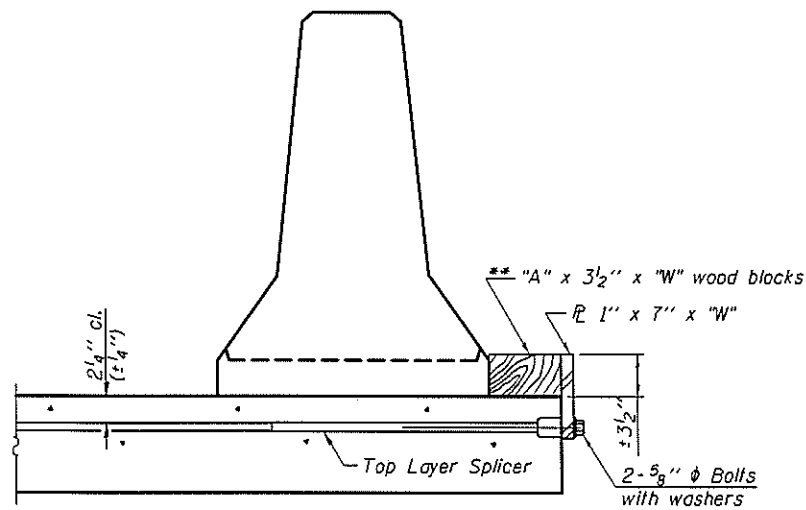
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel \bar{L} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

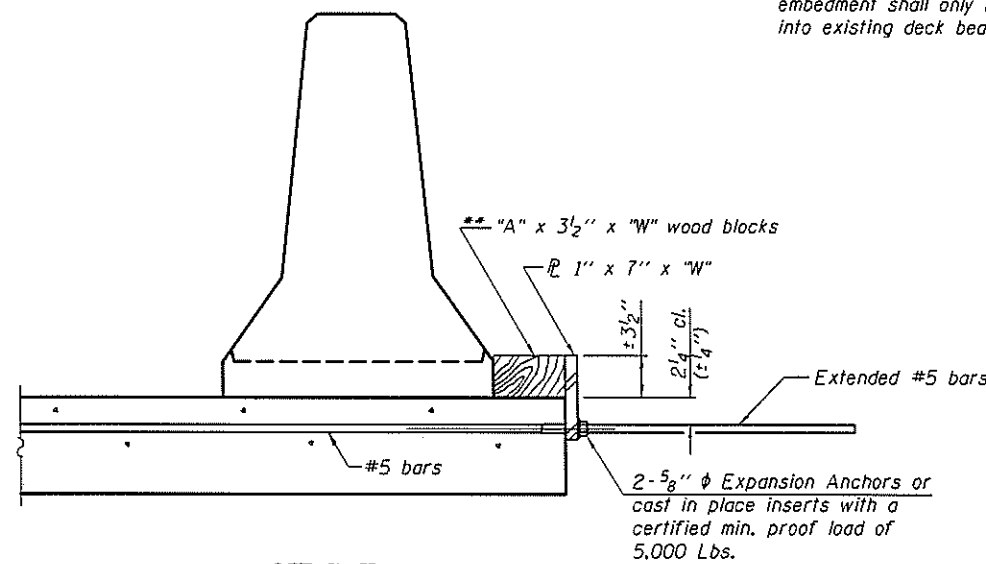
Cost of retainer assembly is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



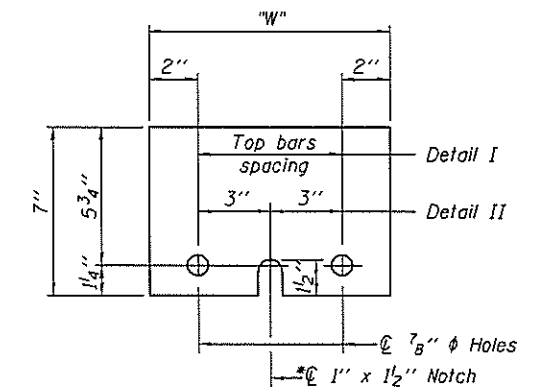
DETAIL I



DETAIL II

RETAINER ASSEMBLY

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.



STEEL RETAINER \bar{L} 1" x 7" x "W"

* Required only with Detail II

R-27

1-12-15

DESIGNED SMR	DATE JANUARY 27, 2016	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED CCC	REVISED		SN 090-0106 & 090-0107	414	90-5,785-1	TAZENELL	245	143	
DRAWN J. Schneller	REVISED		SHEET NO. 11 OF 11 SHEETS		CONTRACT NO. 68606		ILLINOIS FED. AID PROJECT		
CHECKED SMR CCC	ACTING ENGINEER OF BRIDGES AND STRUCTURES								

GENERAL NOTES

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

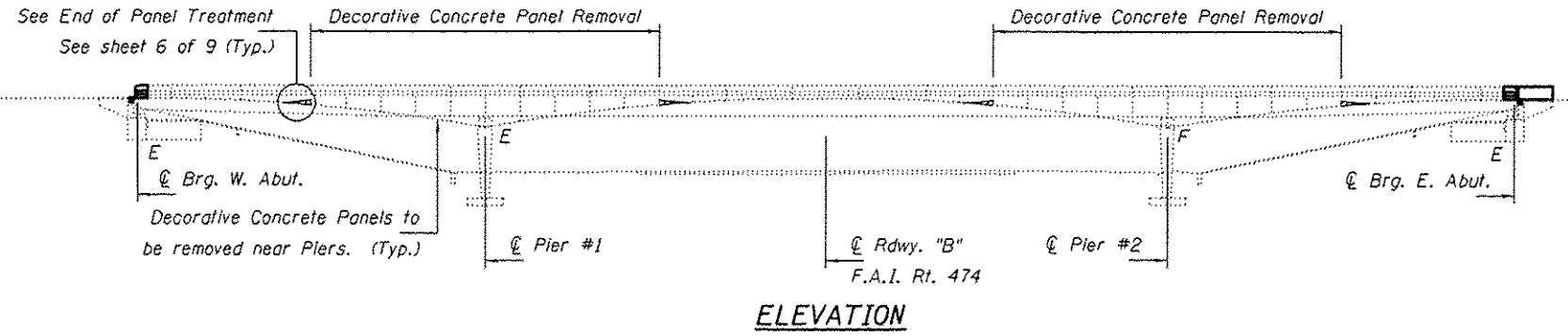
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Reinforcement bars designated (E) shall be epoxy coated.

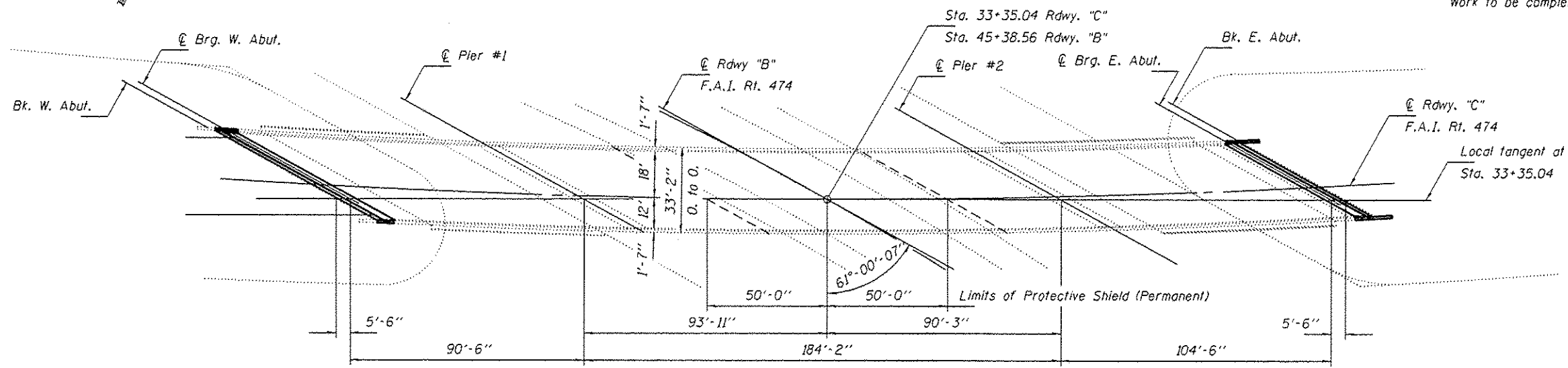
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The deck surface shall have its final finish lined according to Article 420.09(e)(1) of the Standard Specifications. Cost Included with Concrete Superstructure.

Work to be completed under bridge closure.

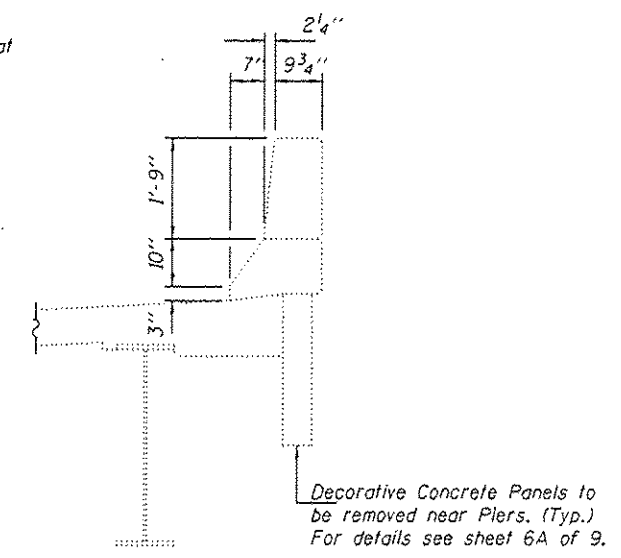


ELEVATION

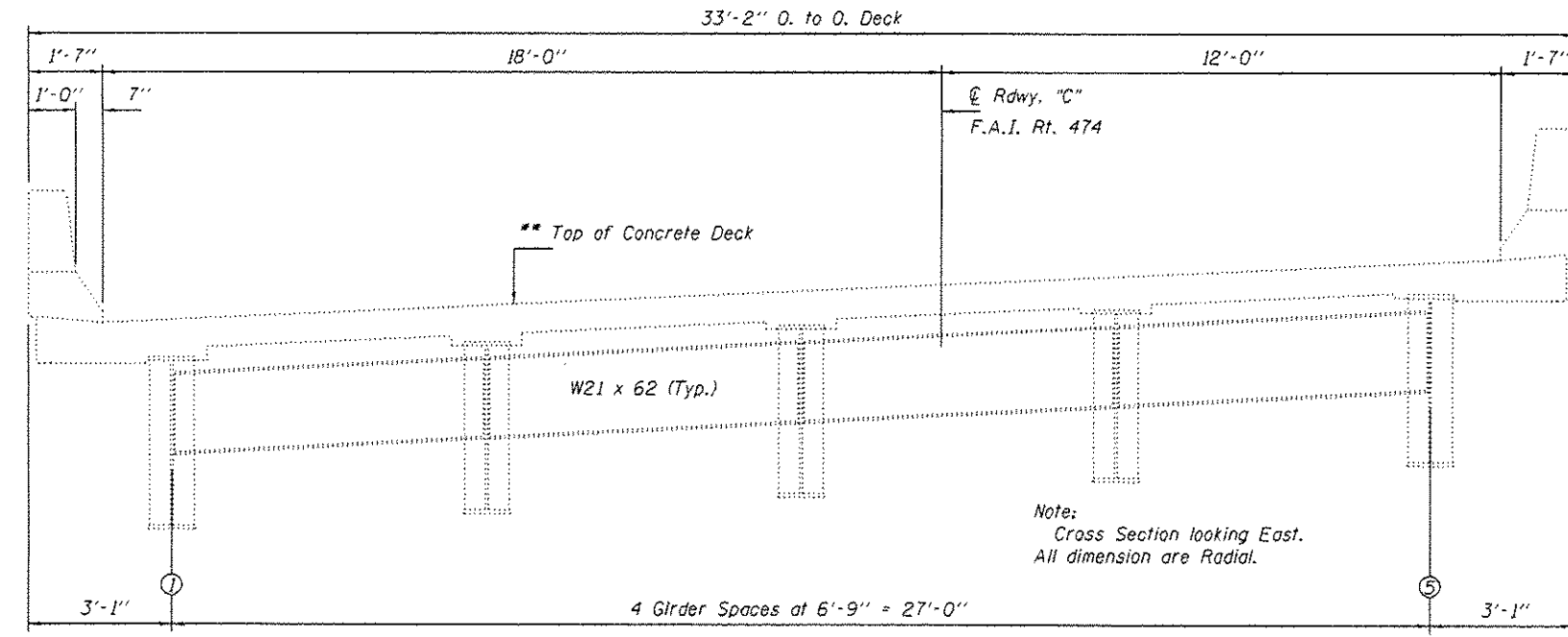


PLAN

Notes:
See sheet 7 of 9 for wingwall modification at E. Abut.



SECTION THRU PARAPET

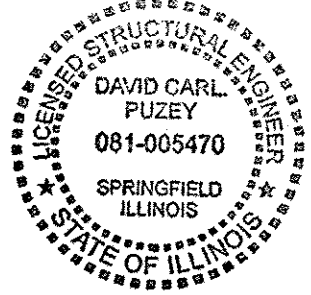


CROSS SECTION

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	20.5
Concrete Superstructure	Cu. Yd.	20.2
Concrete Structures	Cu. Yd.	2.0
Preformed Joint Strip Seal	Foot	133
Reinforcement Bars, Epoxy Coated	Pound	3020
Concrete Sealer	Sq. Ft.	650
Deck Slab Repair (Partial Depth)	Sq. Yd.	31.0
Protective Shield (Permanent)	Sq. Yd.	300
Decorative Concrete Panel Removal	Sq. Ft.	1092

* On new concrete areas of deck, top, front and back of parapets only.



DESIGNED *Adrian T. Holloway*
 CHECKED *Stephen M. Ryan*
 DRAWN *J. Schneller*
 CHECKED *ATH SML*

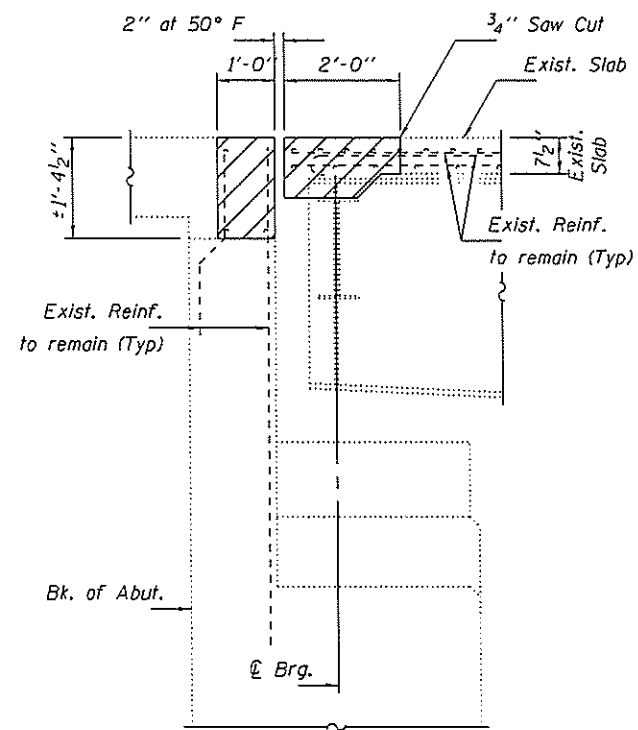
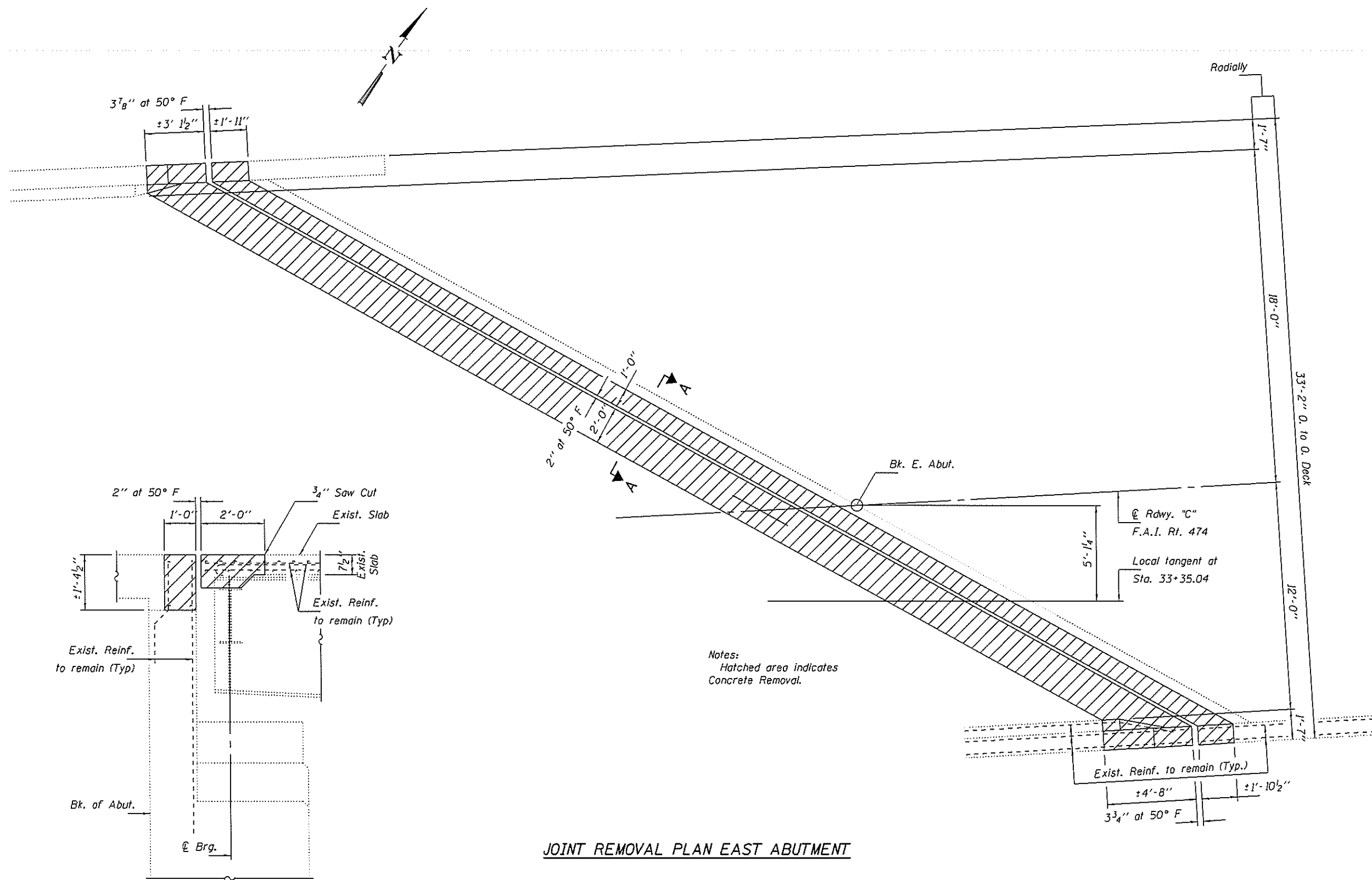
PASSED *D. Carl Puzey*
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 29, 2016
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN & ELEVATION
 F.A.I. Rt. 474 RDWY. "C" OVER F.A.I. RT. 474 RDWY. "B" (W.B.)
 SN 090-0118
 SHEET NO. 1 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZEWELL	245	144
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



SECTION A-A

JOINT REMOVAL PLAN EAST ABUTMENT

Notes:
Hatched area indicates
Concrete Removal.

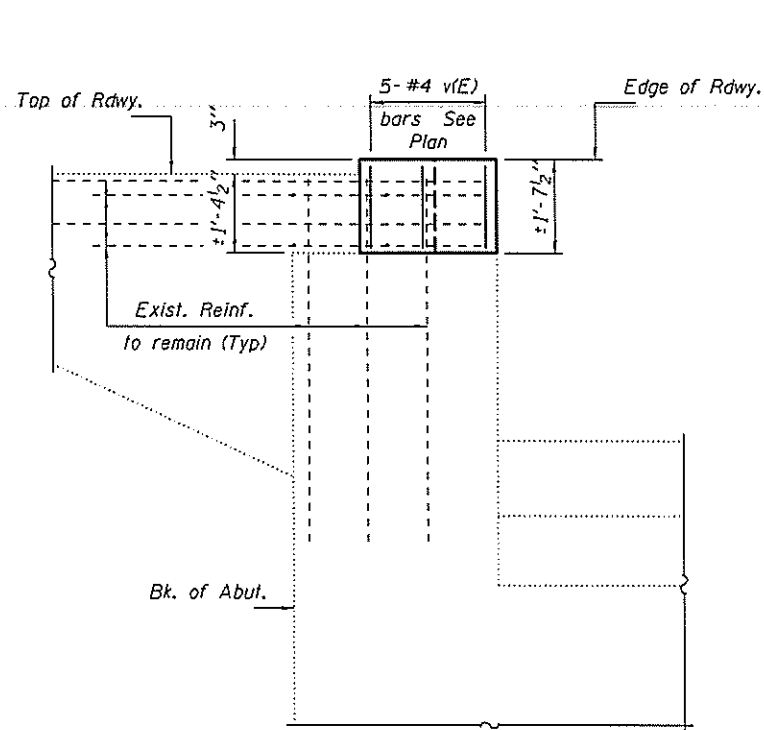
DESIGNED *ATH*
 CHECKED *SMR*
 DRAWN *J. Schneller*
 CHECKED *ATH SMR*

PASSED
 DATE JANUARY 29, 2016
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

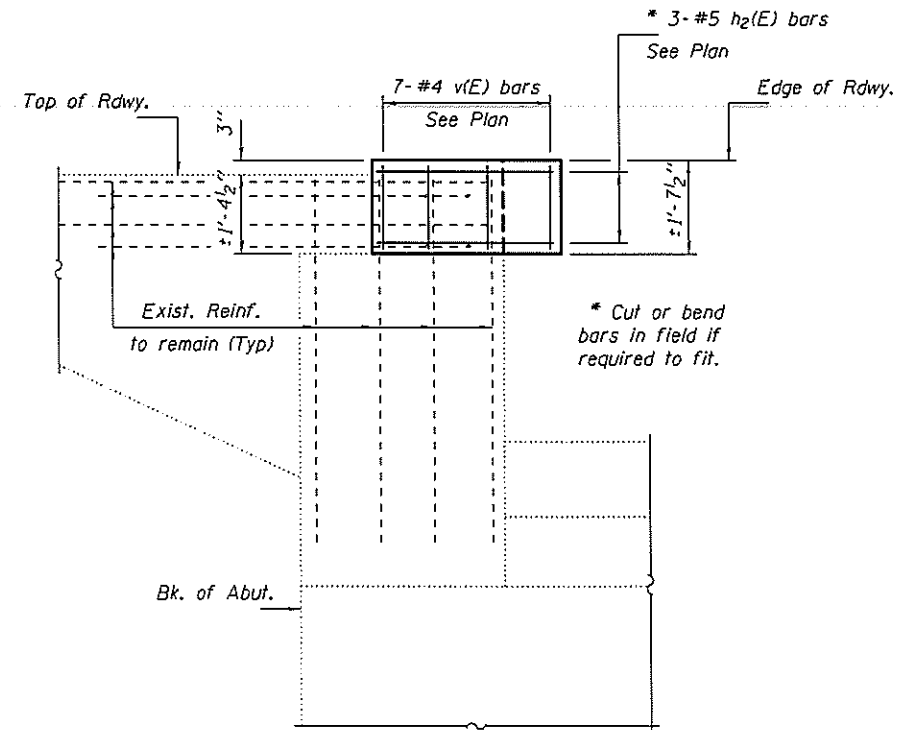
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

JOINT REMOVAL DETAILS EAST ABUTMENT
 SN 090-0118
 SHEET NO. 3 OF 9 SHEETS

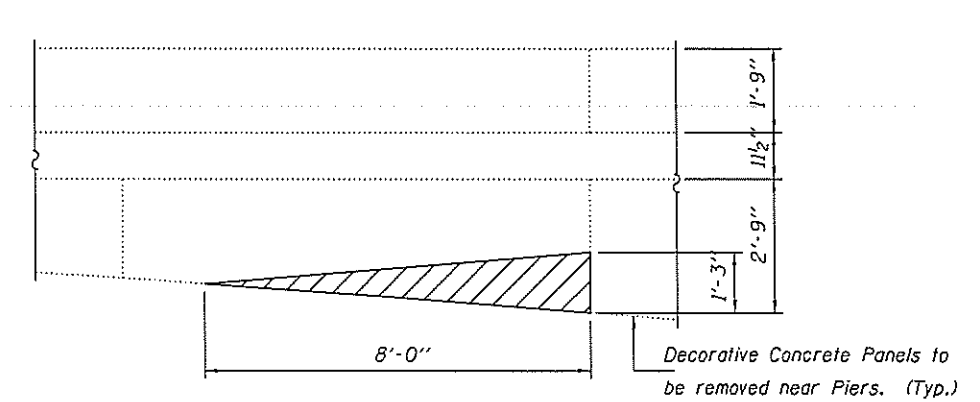
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6, TRS-1	TAZEWELL	245	146
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



VIEW A-A

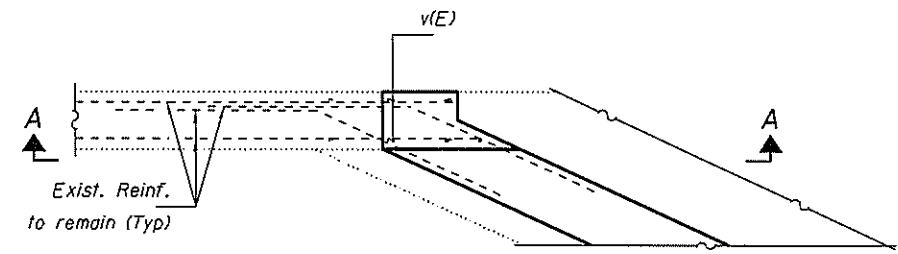


VIEW A-A



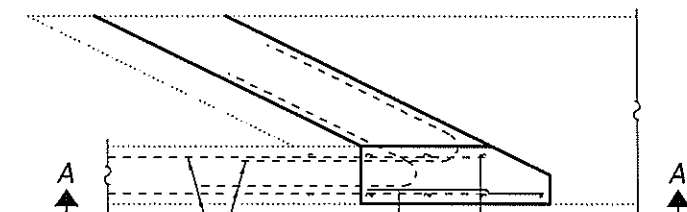
END PANEL TREATMENT

Hatched areas to be measured and paid for as Concrete Removal.



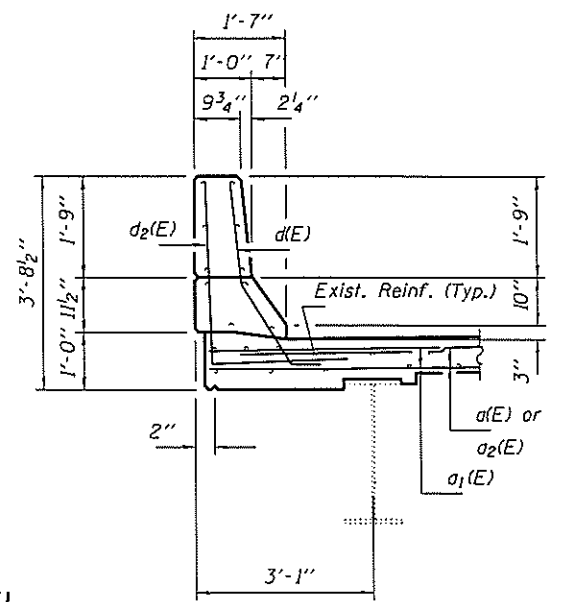
PLAN

NORTH WING WALL DETAILS
Showing North Wing of West Abutment



PLAN

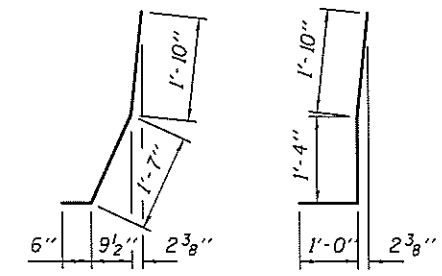
SOUTH WING WALL DETAILS
Showing South Wing of West Abutment



SECTION THRU PARAPET

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	24	#5	26'-6"	—
a1(E)	8	#6	8'-3"	—
a2(E)	16	#5	31'-3"	—
d(E)	3	#5	3'-11"	J
d1(E)	19	#5	4'-2"	J
d2(E)	20	#4	5'-6"	J
h(E)	18	#6	28'-0"	—
h1(E)	12	#6	33'-6"	—
h2(E)	3	#5	1'-4"	—
v(E)	12	#4	2'-6"	—
Concrete Removal			Cu. Yd.	20.3
Concrete Superstructure			Cu. Yd.	20.2
Reinforcement Bars, Epoxy Coated			Lbs.	2840

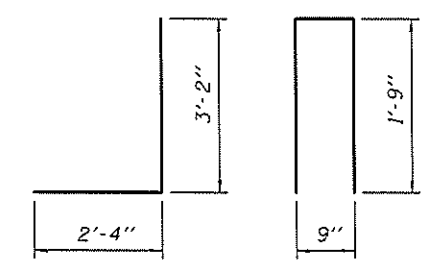


BAR d(E)

BAR d1(E)

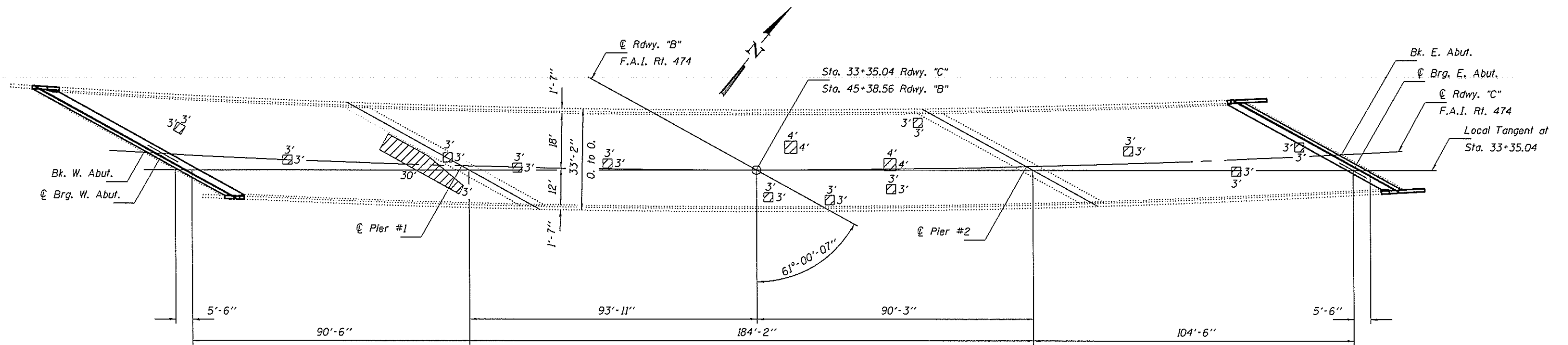
MINIMUM BAR LAP

#5 bar = 3'-6"
#6 bar = 4'-10"



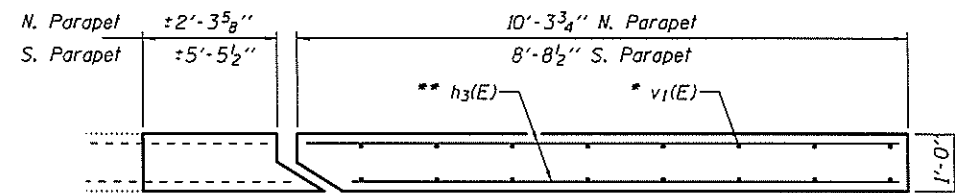
BAR d2(E)

BAR u(E)



PLAN

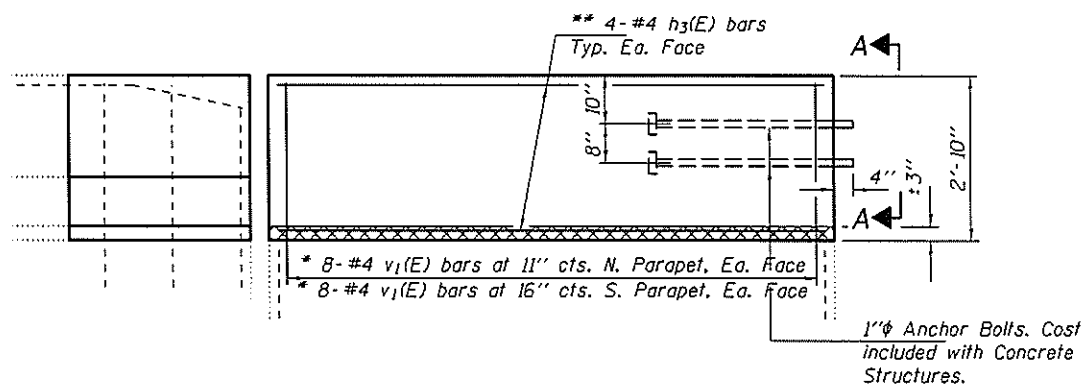
- Deck Slab Repair (Partial).
- Concrete Removal
- Concrete Superstructure. See sheet #6 of 9 for summarized repair quantity.



SECTION

**BILL OF MATERIAL
DECK SLAB REPAIR**

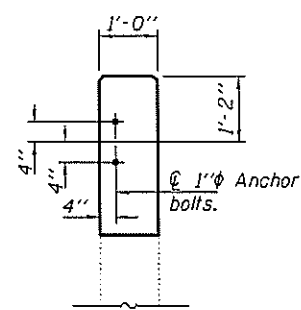
ITEM	UNIT	QUANTITY
Deck Slab Repair (Partial Depth)	Sq. Yd.	31



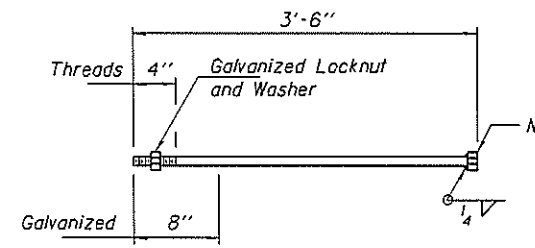
**WINGWALL MODIFICATION
EAST ABUTMENT ONLY**

* Epoxy grout v₁(E) & v₂(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.
** Cut reinforcement to fit.

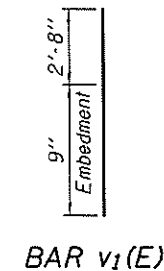
Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



VIEW A-A



1" ANCHOR BOLT



**BILL OF MATERIAL
2 WINGWALL MODIFICATIONS**

Bar	No.	Size	Length	Shape
h ₃ (E)	16	#4	9'-11"	—
v ₁ (E)	32	#4	3'-3"	—
Concrete Removal			Cu. Yd.	0.2
Concrete Structures			Cu. Yd.	2.0
Reinforcement Bars, Epoxy Coated			Pound	180

DESIGNED ATH
CHECKED SMR
DRAWN J. Schneller
CHECKED ATH SMR

PASSED
J. Carl Pappas
ACTING ENGINEER OF BRIDGES AND STRUCTURES

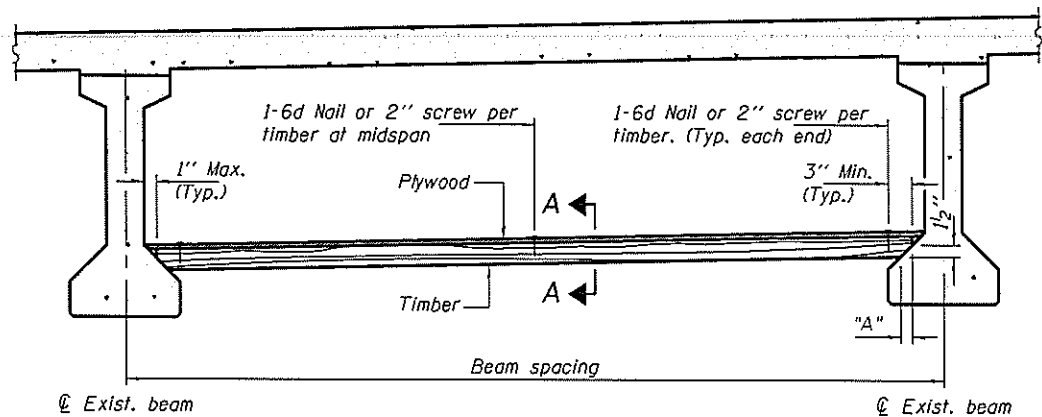
DATE JANUARY 29, 2016
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

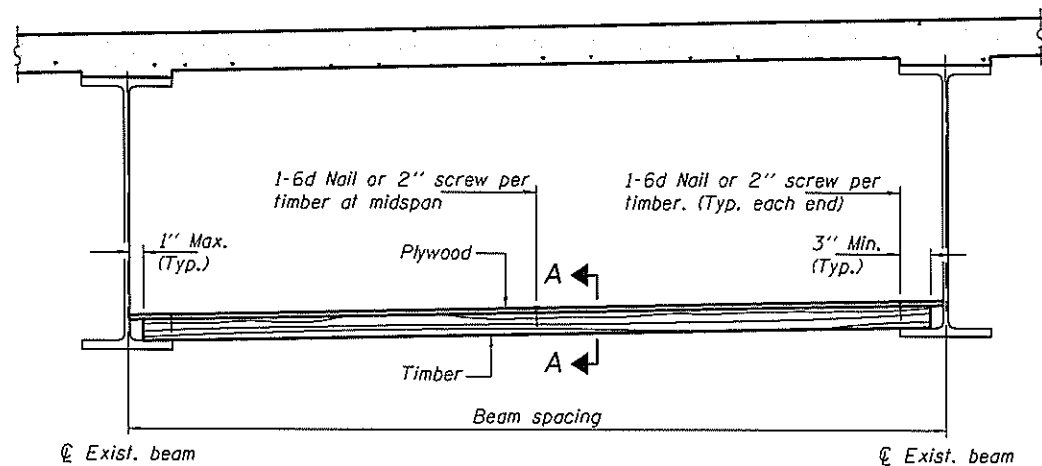
DECK SLAB REPAIRS AND
EAST ABUTMENT WINGWALL MODIFICATIONS
SN 090-0118
SHEET NO. 7 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-5,785-1	TAZEWELL	245	150

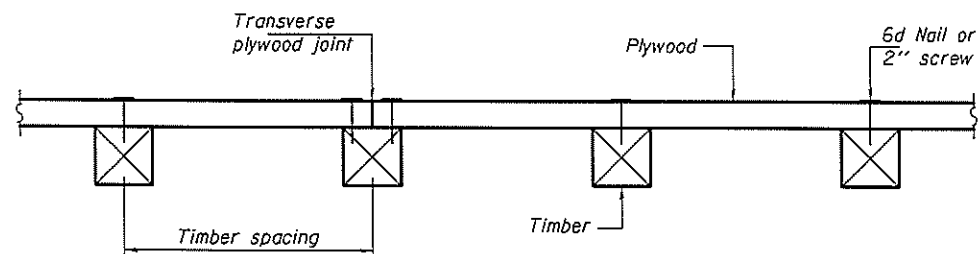
CONTRACT NO. 68606
ILLINOIS FED. AID PROJECT



PPC I-BEAMS AND BULB-T's



STEEL BEAMS



SECTION A-A

TIMBER SPACING

Beam Spacing (ft.)	Timber Sizes (in.)		
	4" x 4" with min. Fb=775 psi Fv=135 psi	4" x 6" with min. Fb=775 psi Fv=135 psi	6" x 6" with min. Fb=575 psi Fv=125 psi
	Maximum Timber Spacing (in.)		
4.5	16	16	16
4.75	16	16	16
5.0	16	16	16
5.25	16	16	16
5.5	16	16	16
5.75	16	16	16
6.0	16	16	16
6.25	12	16	16
6.5	12	16	16
6.75	12	16	16
7.0	8	16	16
7.25	8	16	16
7.5	8	16	16
7.75	8	16	16
8.0	8	12	16
8.25	8	12	16
8.5	6	12	12
8.75	6	12	12
9.0	6	8	12

PPC I-BEAMS AND BULB-T's

BEAM	"A"
36" I-Beam	1 1/2"
42" I-Beam	1 1/2"
48" I-Beam	1 1/2"
54" I-Beam	1 5/8"
63" Bulb-T	3 3/8"
72" Bulb-T	3 3/8"

Notes: See special provision for Permanent Protective Shield System.
 Timber sizes shown are nominal sizes. Rough sawn timber of the dimensions shown will also be considered acceptable.
 The minimum Fb and Fv values shown are the tabulated design values given in the National Design Specification for Wood Construction for No. 2 Spruce-Pine-Fir without adjustment factors applied. Better grades or other species with equal or higher allowable stresses will also be considered acceptable.
 The timber spacings shown have been determined using allowable stresses with all adjustment factors necessary for the anticipated service conditions.
 All timber shall be treated.
 Plywood shall be 5/8" rated Exterior type plywood by APA.
 Plywood shall be placed such that the face grain is perpendicular to the timber supports. When less than a full sheet (4' width) of plywood is used, the width of the strip used shall not be less than 2'.
 Transverse plywood joints shall be supported by timbers.
 When 4" x 6" timbers are used, they shall be placed such that the wide face is horizontal and the narrow face is vertical.
 Design load = 200 psf.

BILL OF MATERIAL

Item	Unit	Total
Protective Shield (Permanent)	Sq. Yd.	300

PPS-1 01-22-09

DESIGNED ATH
 CHECKED SMR
 DRAWN J. Schneller
 CHECKED ATH SMR

PASSED
 DATE JANUARY 29, 2016
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

REVISOR
 REVISOR

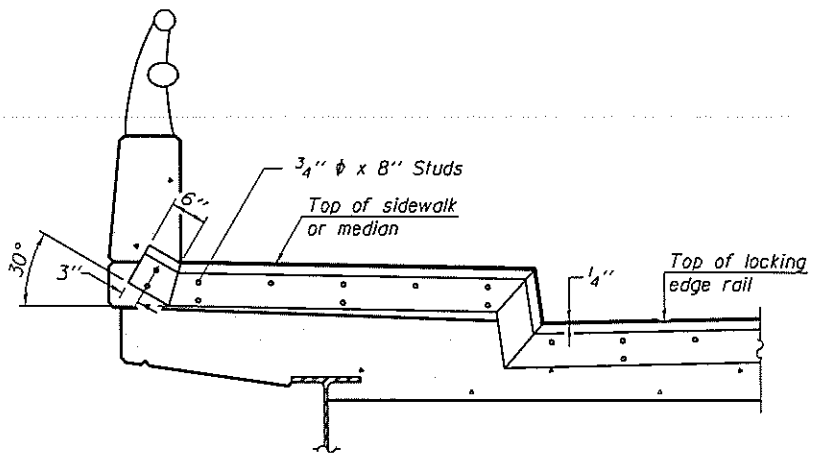
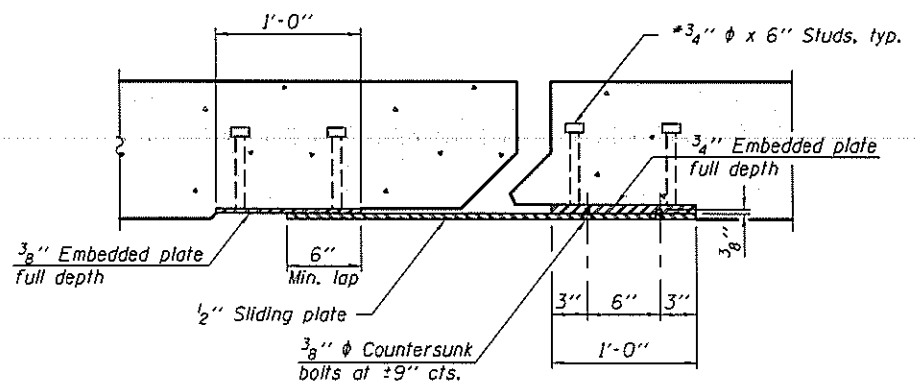
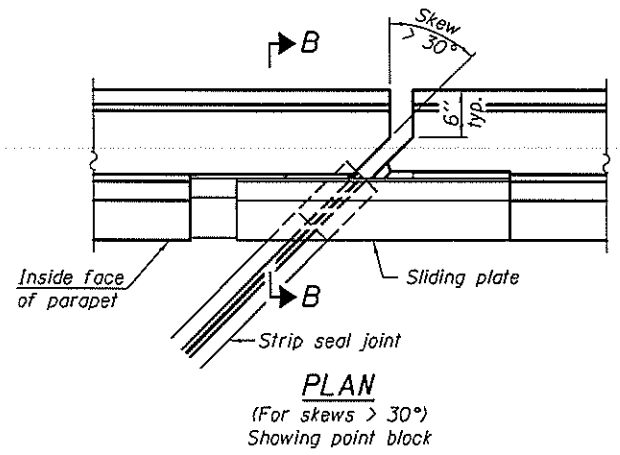
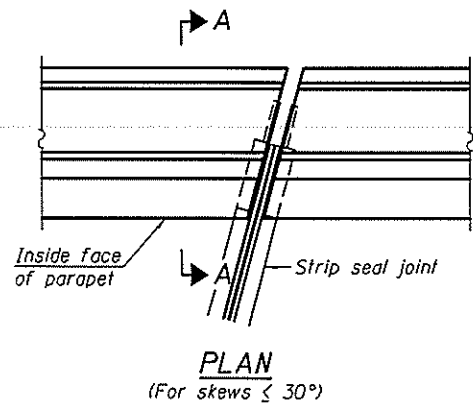
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PERMANENT PROTECTIVE SHIELD
 SN 090-0118

SHEET NO. 8 OF 9 SHEETS

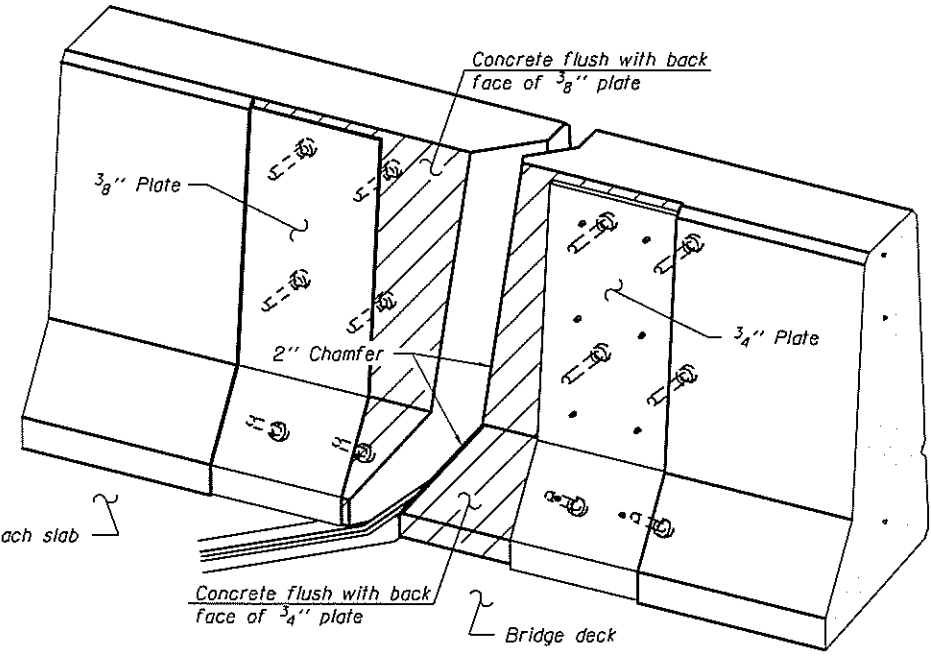
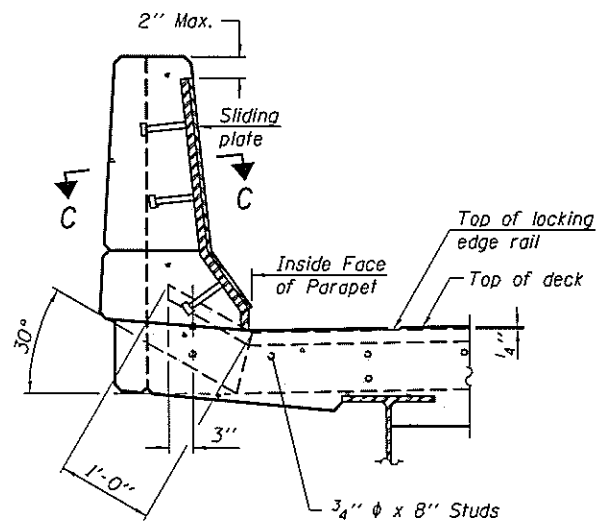
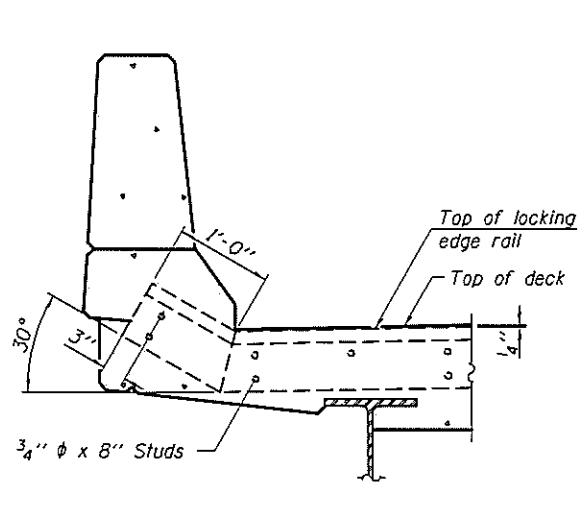
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-5,785-1	TAZENELL	245	151

CONTRACT NO. 68606
 ILLINOIS FED. AID PROJECT



TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

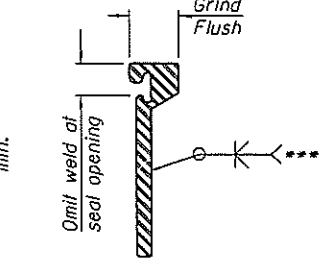
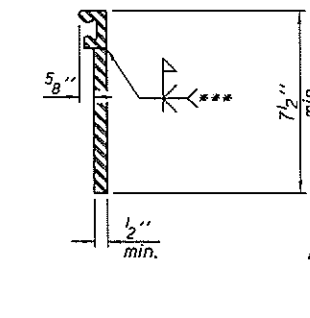
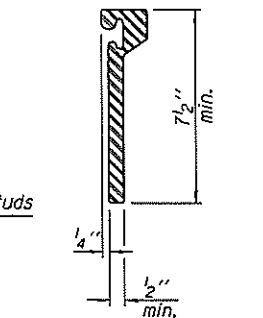
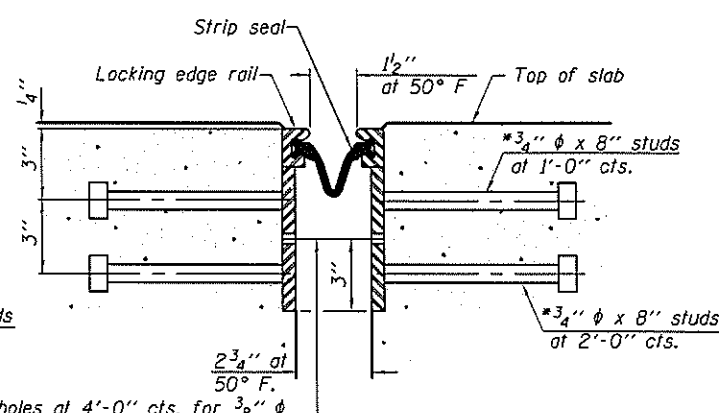
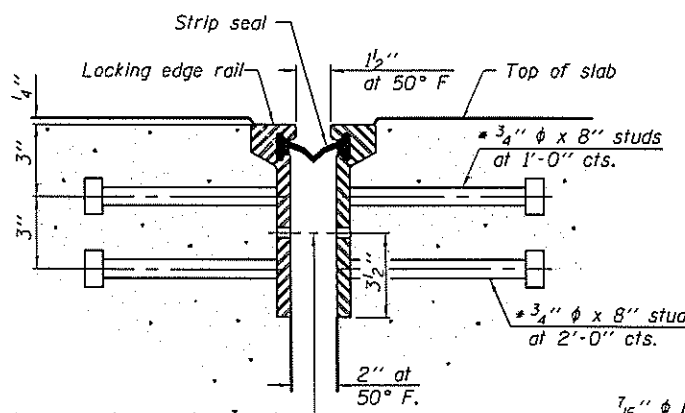
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews > 30 degrees included in the cost of Preformed Joint Strip Seal.



*** Back gauge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAILS

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	133

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

EJ-SSJ

1-27-12

DESIGNED ATH	DATE JANUARY 29, 2016
CHECKED SMR	
DRAWN J. Schneller	
CHECKED ATH SMR	

PASSED

 ACTING ENGINEER OF BRIDGES AND STRUCTURES

REVISOR	
REVISION	

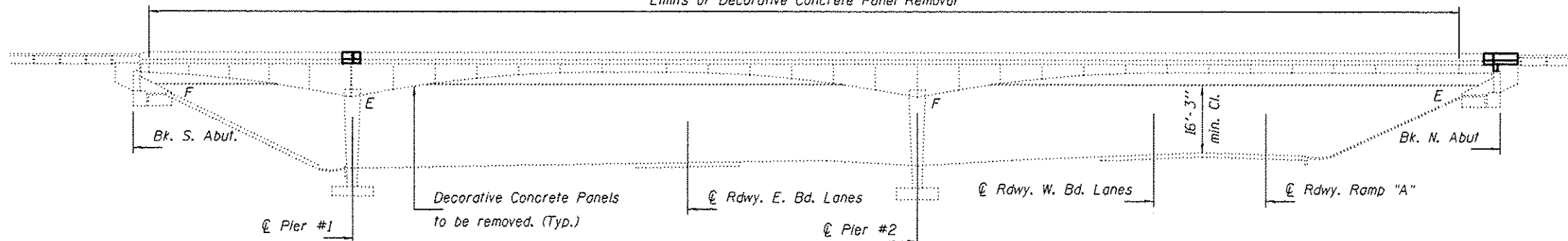
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
SN 090-0118

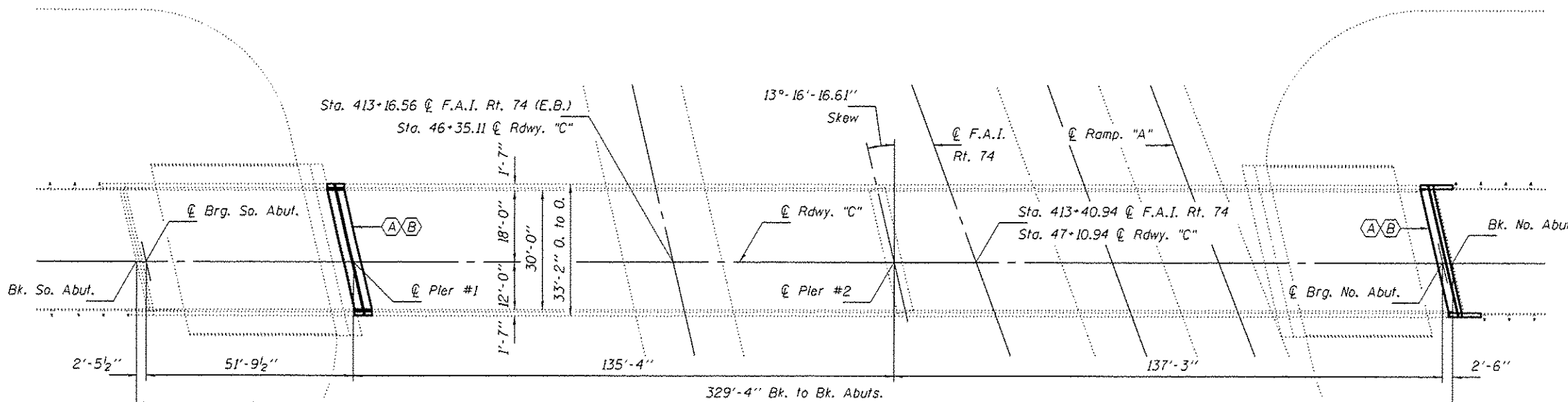
SHEET NO. 9 OF 9 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZEWELL	245	152
				CONTRACT NO. 68606
ILLINOIS FED. AID PROJECT				

Limits of Decorative Concrete Panel Removal



ELEVATION



PLAN

- (A) Remove and Replace Expansion Joint
- (B) Remove and Replace Bearings

Notes:
See sheet 4 of 8 for wingwall modification at N. Abut.

GENERAL NOTES

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

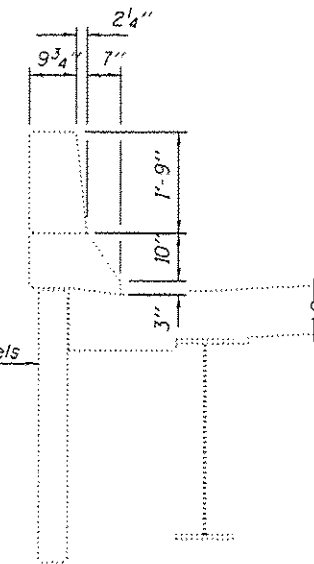
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

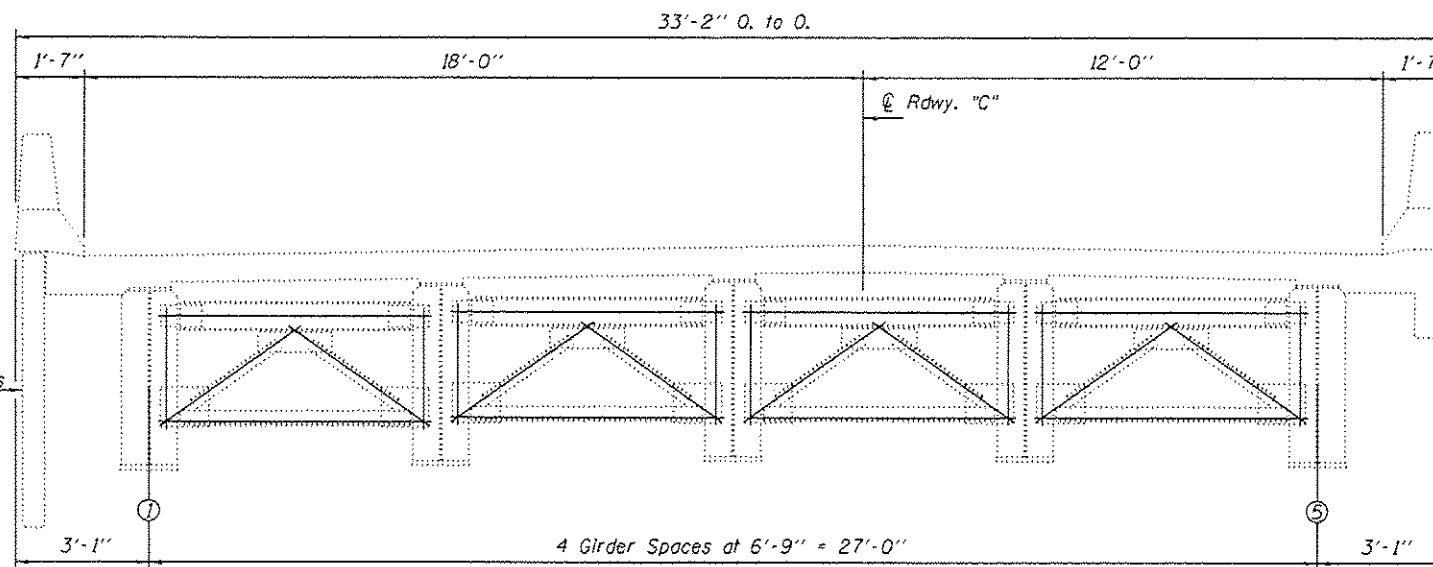
The deck surface shall have its final finish lined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.

Work to be completed under bridge closure.



SECTION THRU PARAPET

Decorative Concrete Panels to be removed. (Typ.)
See sheet 1A & 1B of B.



CROSS SECTION
(Looking North)

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	9.6
Concrete Superstructure	Cu. Yd.	9.7
Preformed Joint Strip Seal	Foot	66
Reinforcement Bars, Epoxy Coated	Pound	1420
Concrete Sealer	Sq. Ft.	318
Anchor Bolts 1"Ø	Each	30
Furnishing and Erecting Structural Steel	Pound	2600
Jack & Remove Existing Bearings	Each	15
Elastomeric Bearings, Type I	Each	15
Deck Slab Repair (Partial Depth)	Sq. Yd.	25.7
Decorative Concrete Panel Removal	Sq. Ft.	2236

* On new concrete areas of deck, top, front and back parapet only



Decorative Concrete Panels to be removed. (Typ.)
See sheet 1A & 1B of B.

DESIGNED: *Stephan Myron*
CHECKED: *Stephan Myron*
DRAWN: J. Schneller
CHECKED: *SMR* *CC*

PASSED

David Carl Puzey
ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE: JANUARY 29, 2016

REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN & ELEVATION
SN 090-0119

SHEET NO. 1 OF 8 SHEETS

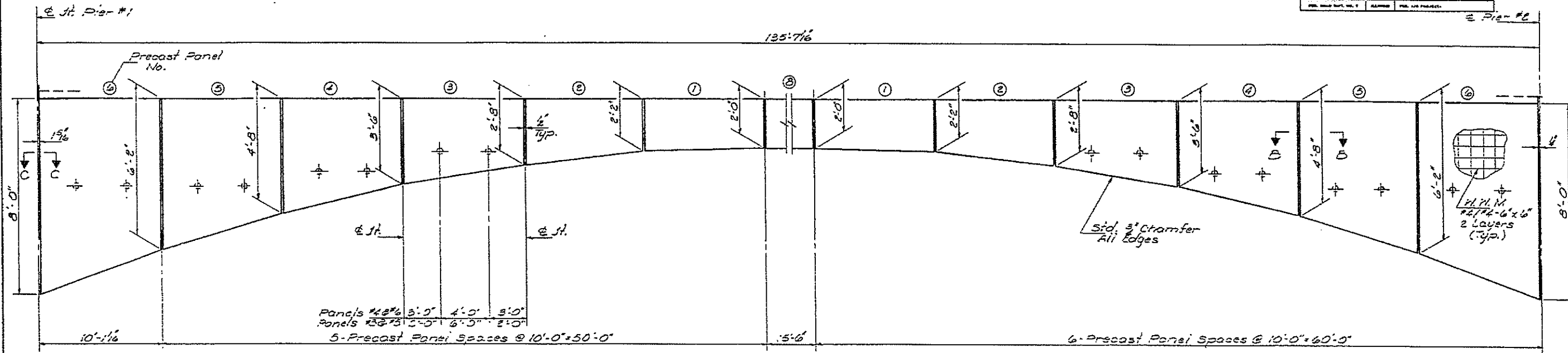
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-S, TRS-1	TAZEWELL	245	153

CONTRACT NO. 68606
ILLINOIS FED. AID PROJECT

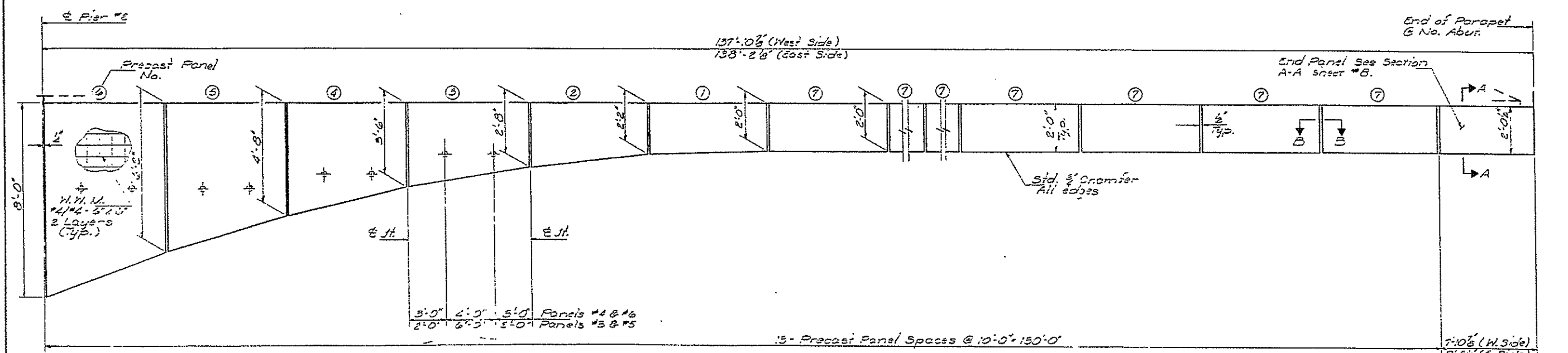
EXPIRES 11-30-2016

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	SHEET NO.	TOTAL SHEETS	SHEET NO.
90-1	Tazewell	126	41	20 SHEETS
474				



ELEVATION OF FASCIA PANELS
SPAN #2
(Showing outside face)



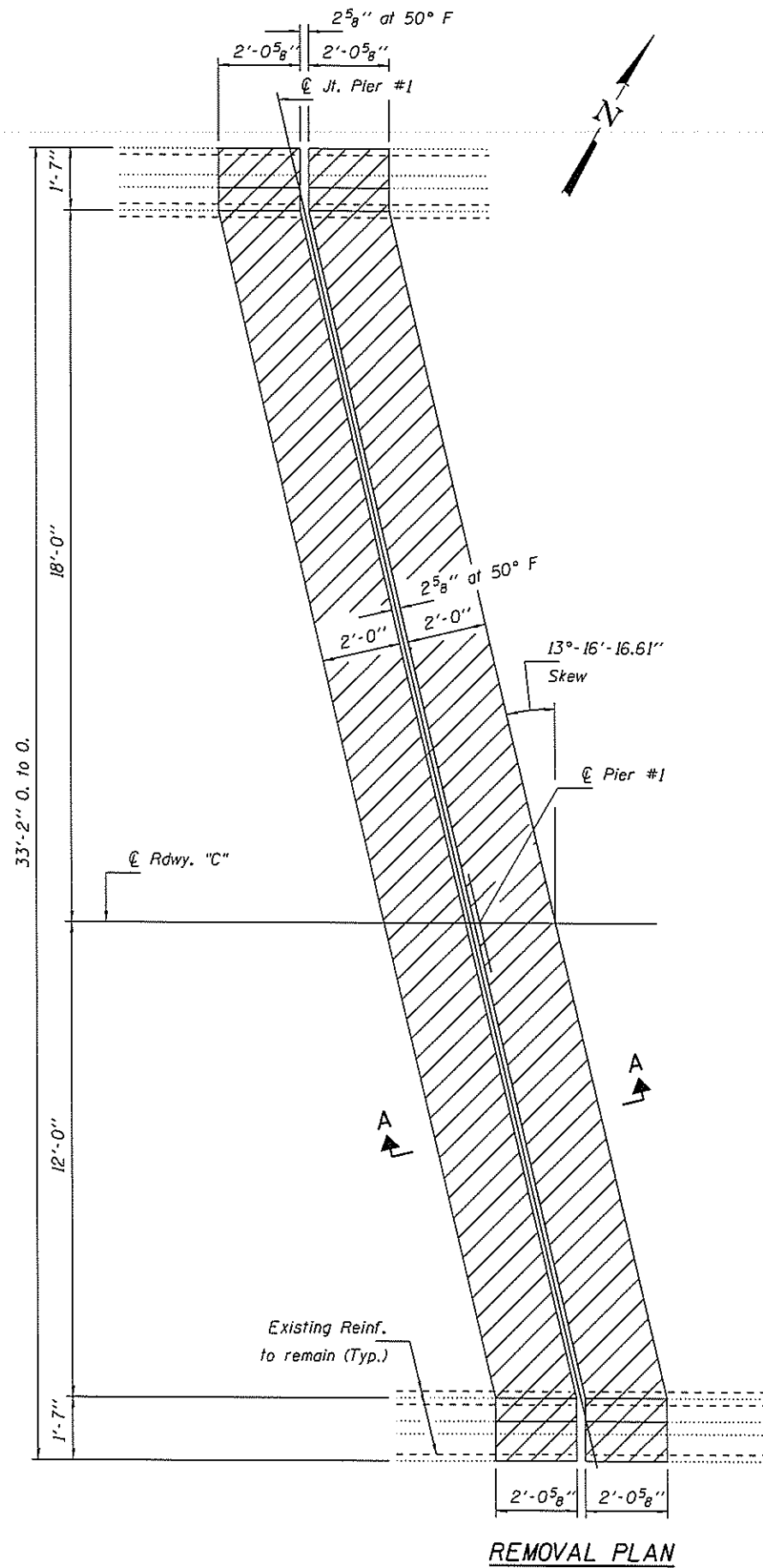
ELEVATION OF FASCIA PANELS
SPAN #3
(Showing outside face)

See sheet #8 for Sections A-A, B-B, C-C and all Details of Precast Panels.

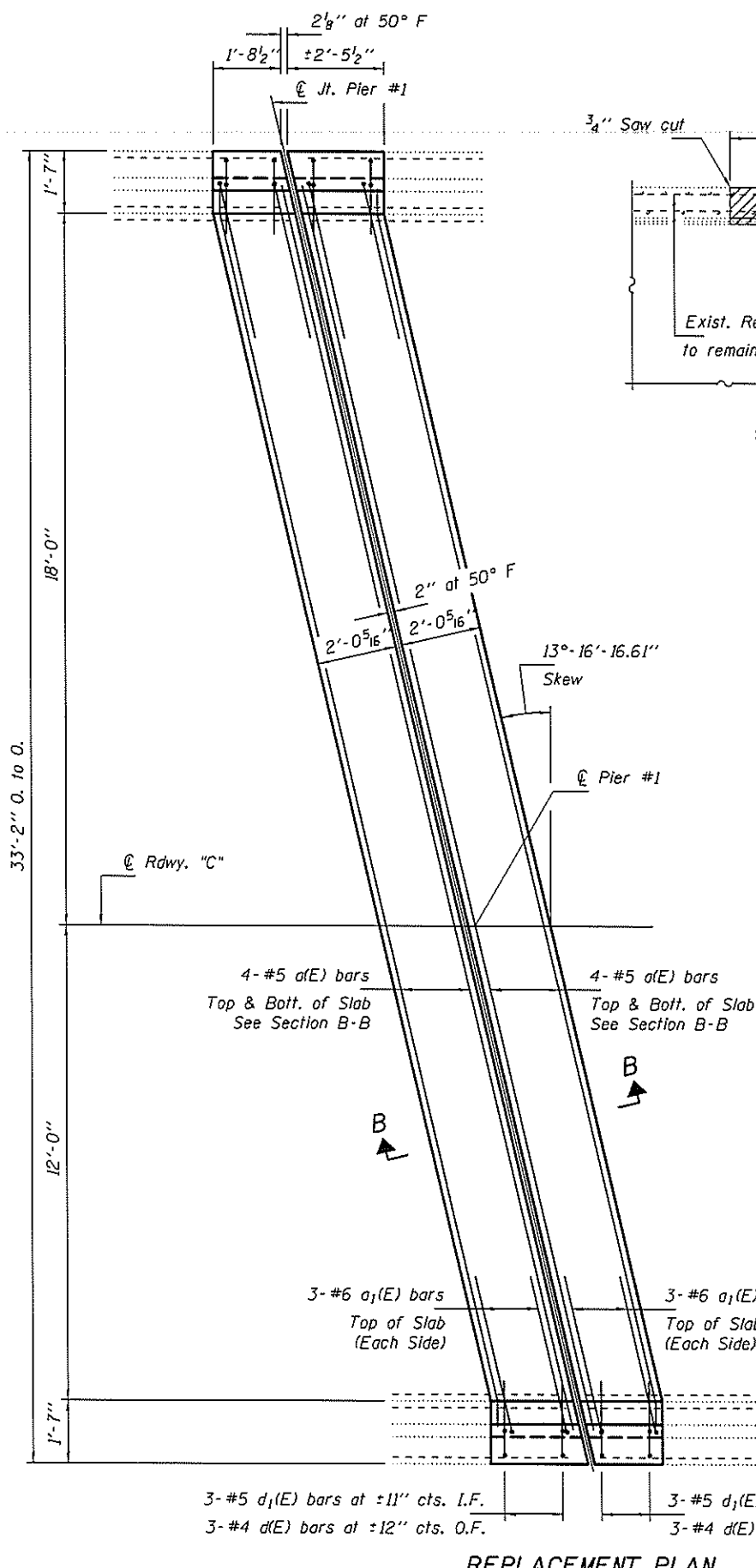
DESIGNED <i>Paul S. Miller</i>	EXAMINED <i>[Signature]</i>
CHECKED <i>[Signature]</i>	PASSED
DRAWN <i>[Signature]</i>	APPROVED
CHECKED <i>[Signature]</i>	

FOR INFORMATION ONLY

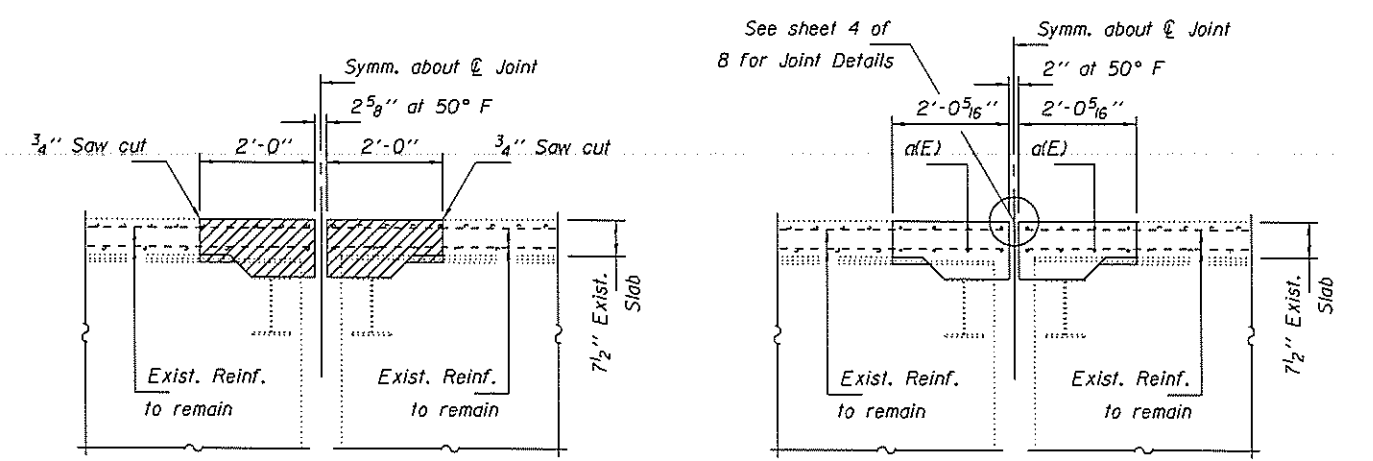
PRECAST PANELS
SPANS #2 & #3
E.A.I. RT. 474 SEC. 90-7-75-5
TAZEWELL COUNTY



REMOVAL PLAN



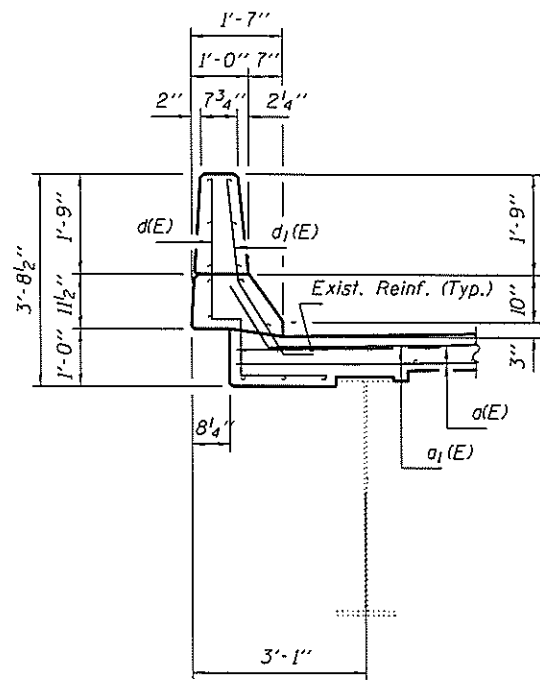
REPLACEMENT PLAN



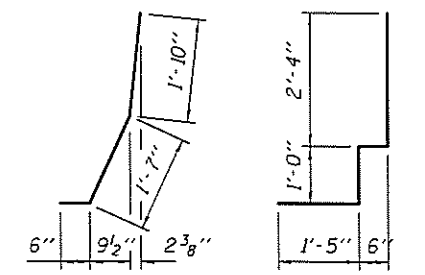
SECTION A-A

SECTION B-B

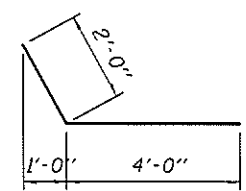
Concrete Removal
Concrete Superstructure



SECTION THRU PARAPET



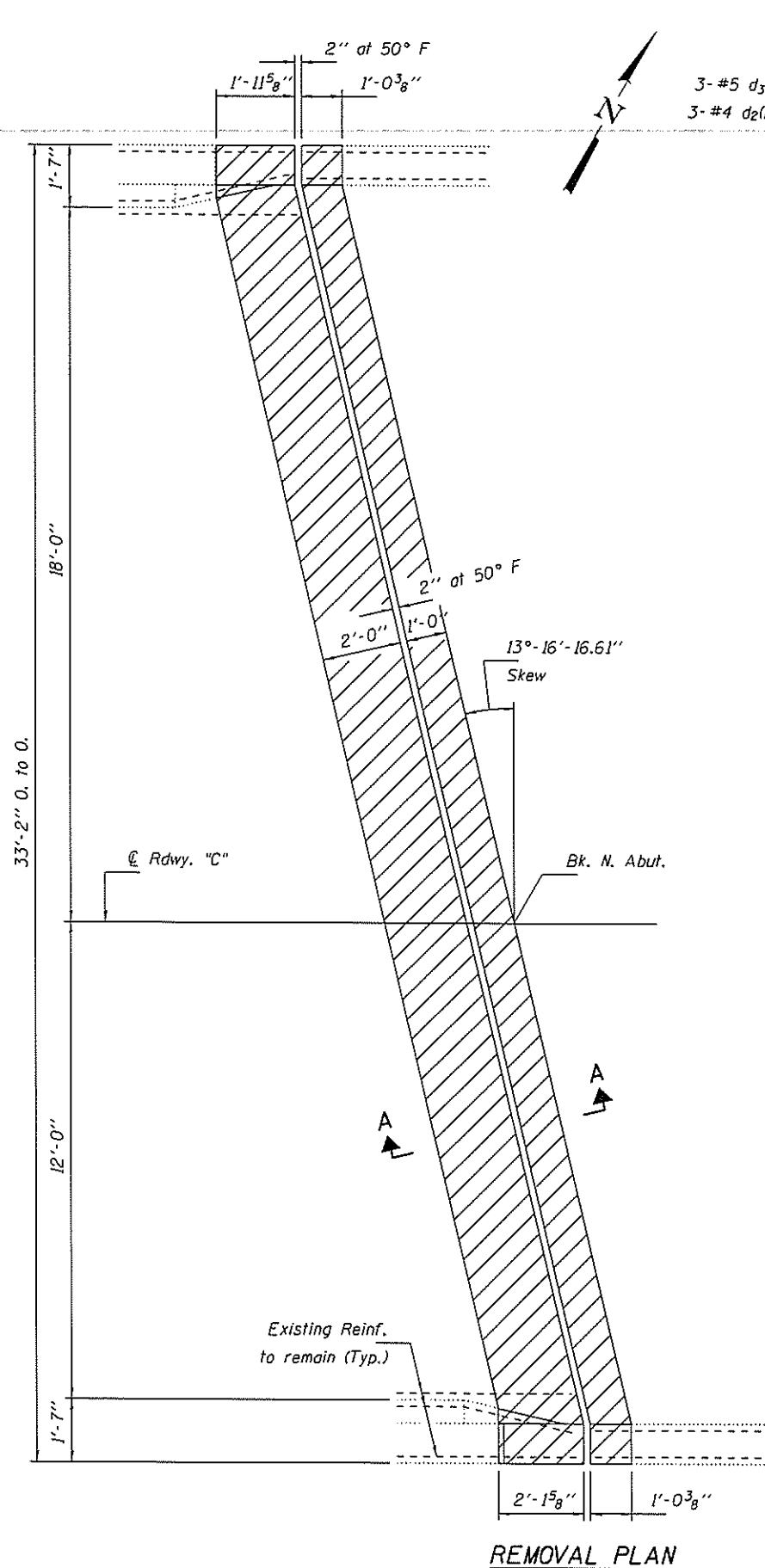
BAR d₁(E) BAR d(E)



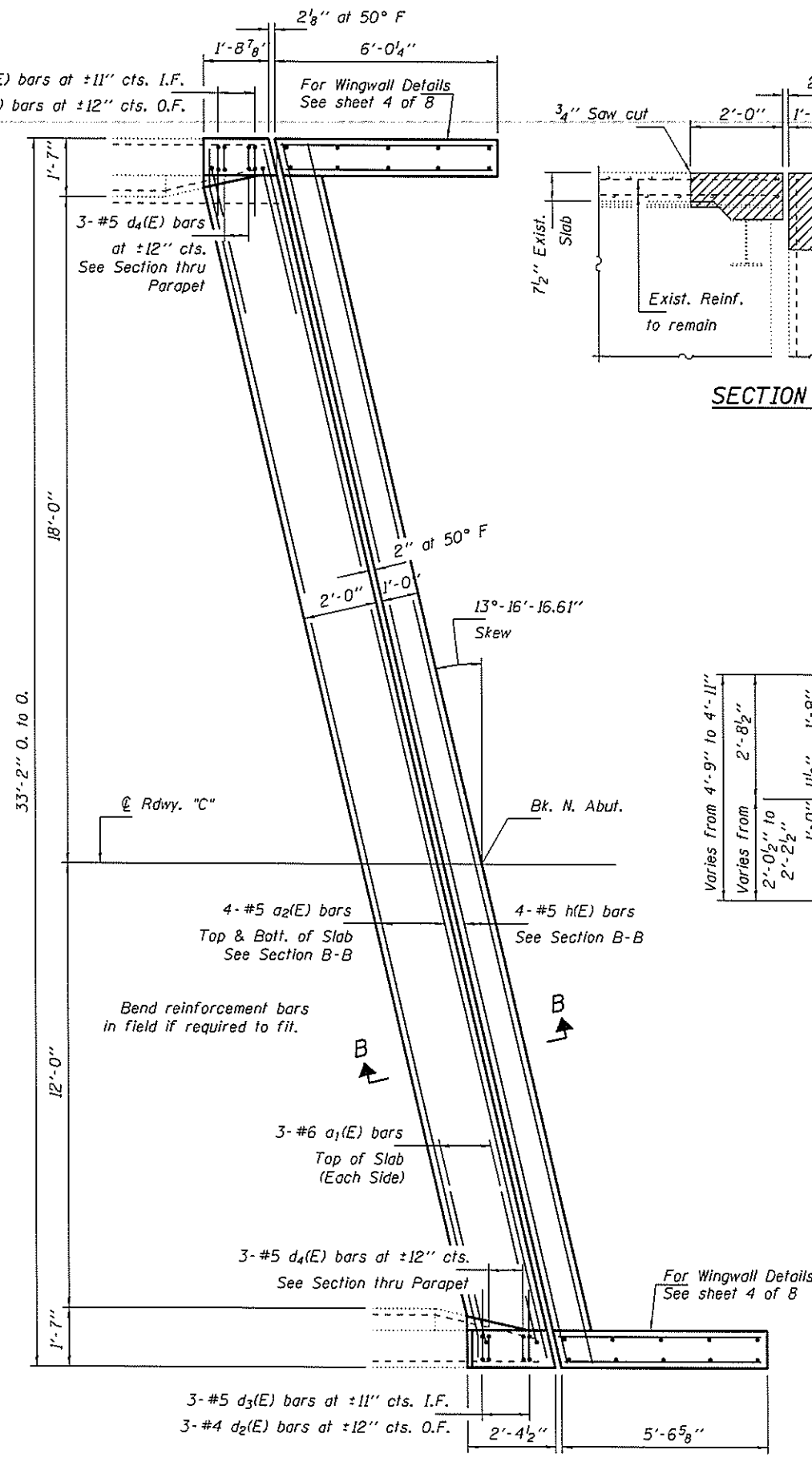
BAR a₁(E)

BILL OF MATERIAL

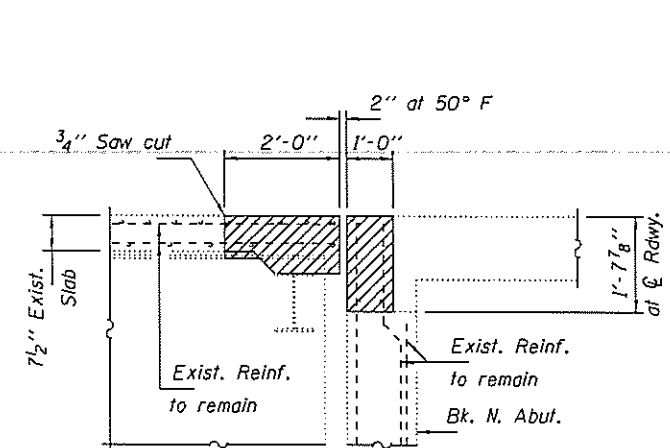
Bar	No.	Size	Length	Shape
a(E)	16	#5	32'-4"	
a ₁ (E)	12	#6	6'-0"	
d(E)	12	#4	5'-3"	
d ₁ (E)	12	#5	3'-11"	
Concrete Removal				Cu. Yd. 4.3
Concrete Superstructure				Cu. Yd. 4.4
Reinforcement Bars, Epoxy Coated				Lbs. 740



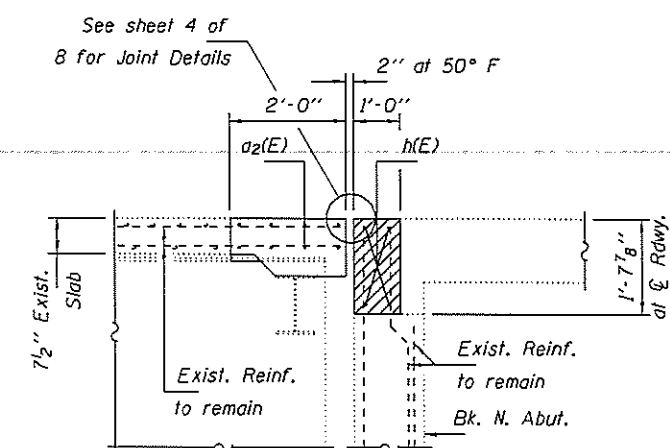
REMOVAL PLAN



REPLACEMENT PLAN

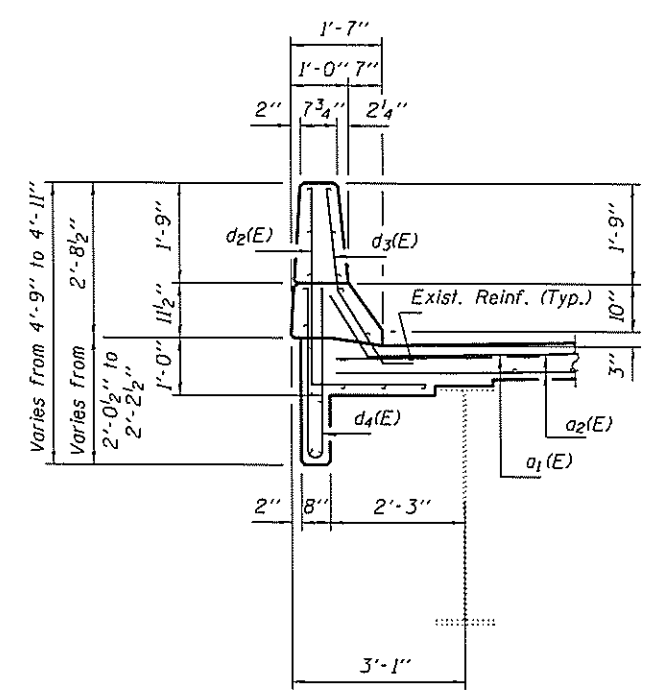


SECTION A-A

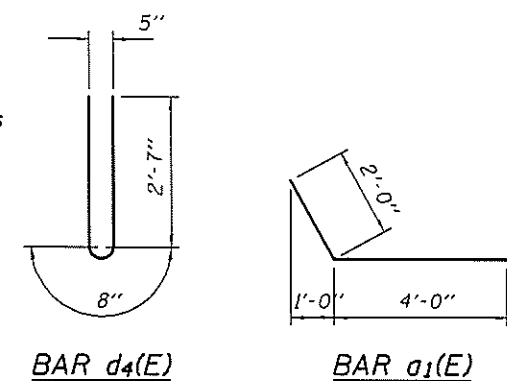
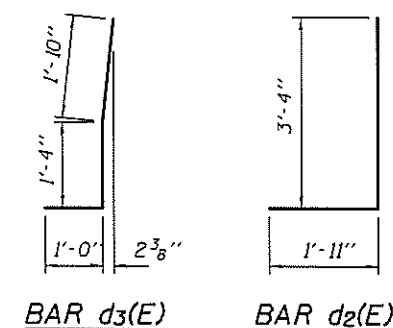


SECTION B-B

- Concrete Removal
 - Concrete Superstructure

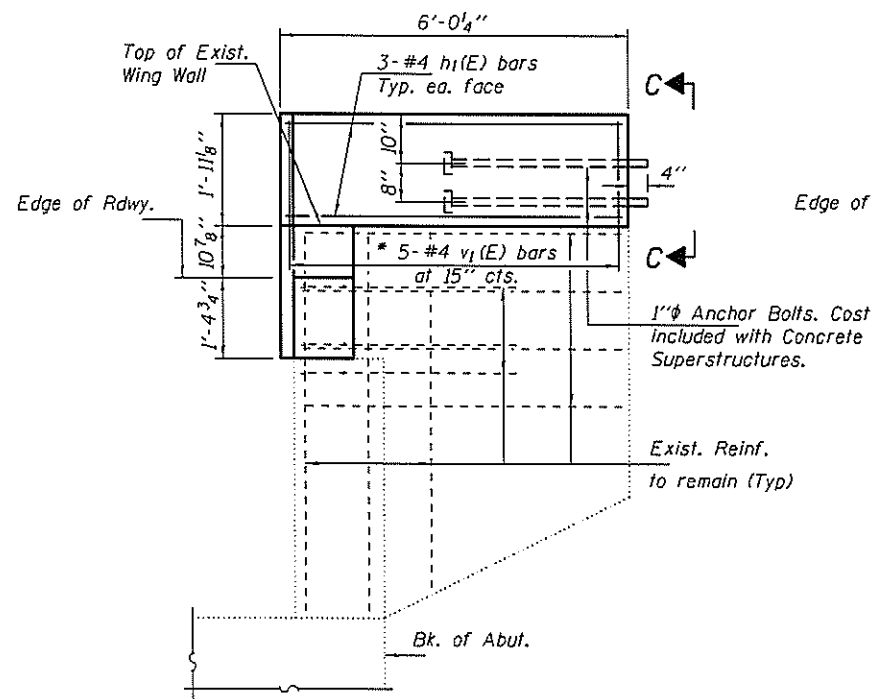


SECTION THRU PARAPET

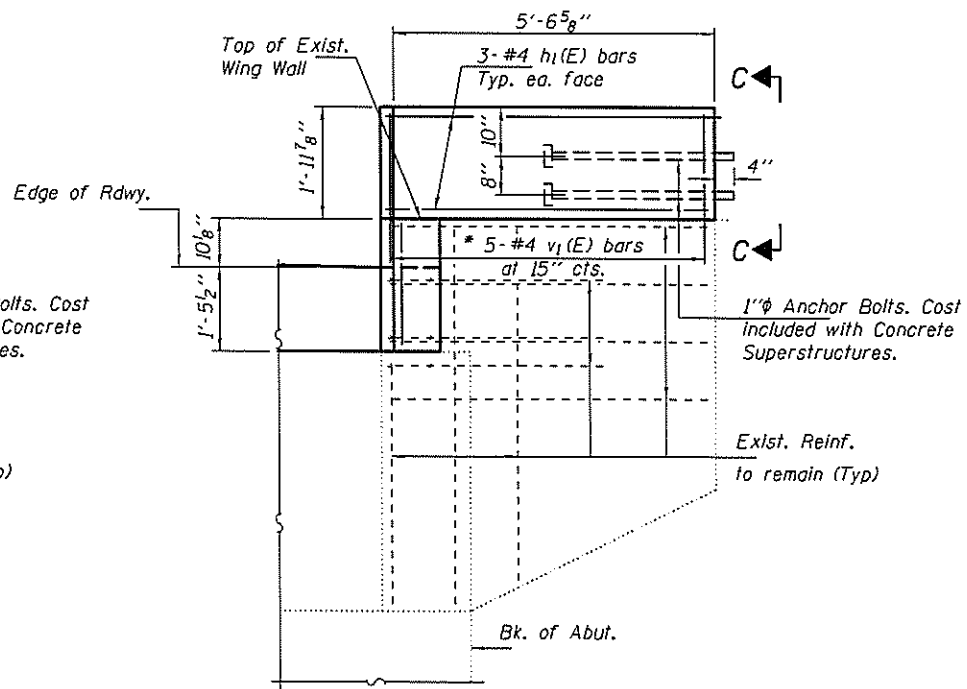


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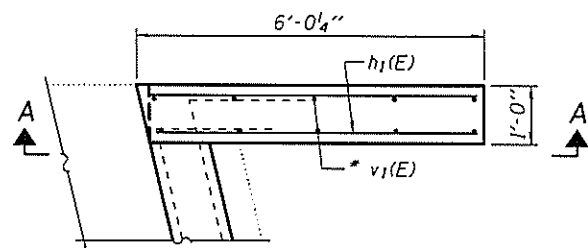
Bar	No.	Size	Length	Shape
a1(E)	6	#6	6'-0"	
a2(E)	8	#5	32'-0"	
d2(E)	6	#4	5'-3"	
d3(E)	6	#5	4'-2"	
d4(E)	6	#5	5'-10"	
h(E)	4	#6	31'-9"	
v(E)	1	#6	2'-0"	
Concrete Removal			Cu. Yd.	4.7
Concrete Superstructure			Cu. Yd.	4.7
Reinforcement Bars, Epoxy Coated			Lbs.	600



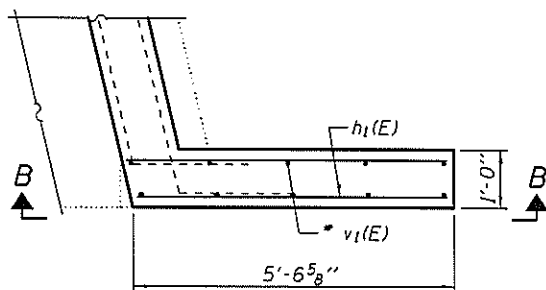
VIEW A-A



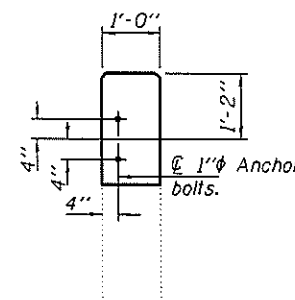
VIEW B-B



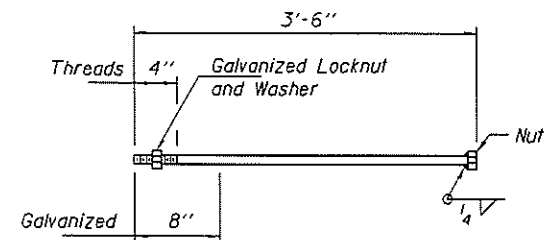
PLAN
WEST WING WALL DETAILS



PLAN
EAST WING WALL DETAILS



VIEW C-C



1" ANCHOR BOLT

BILL OF MATERIAL
2 WINGWALL MODIFICATIONS

Bar	No.	Size	Length	Shape
h ₁ (E)	12	#4	5'-5"	—
v ₁ (E)	20	#4	2'-7"	—
Concrete Removal			Cu. Yd.	0.30
Reinforcement Bars, Epoxy Coated			Pound	80

* Epoxy grout v₁(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

PASSED

ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE JANUARY 29, 2016

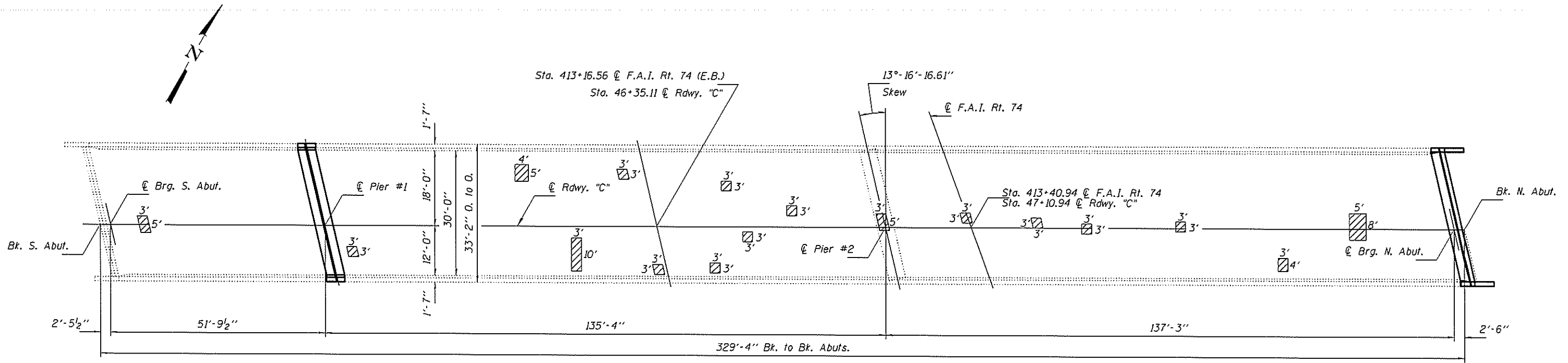
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION


NORTH ABUTMENT WINGWALL DETAILS
SN 090-0119

SHEET NO. 4 OF 8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,785-1	TAZEWELL	245	156
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



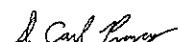
PLAN

 Deck Slab Repair (Partial).

**BILL OF MATERIAL
DECK SLAB REPAIRS**

ITEM	UNIT	QUANTITY
Deck Slab Repair (Partial Depth)	Sq. Yd.	25.7

DESIGNED SMR
 CHECKED CCC
 DRAWN J. Schneller
 CHECKED SMR CCC

PASSED 
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

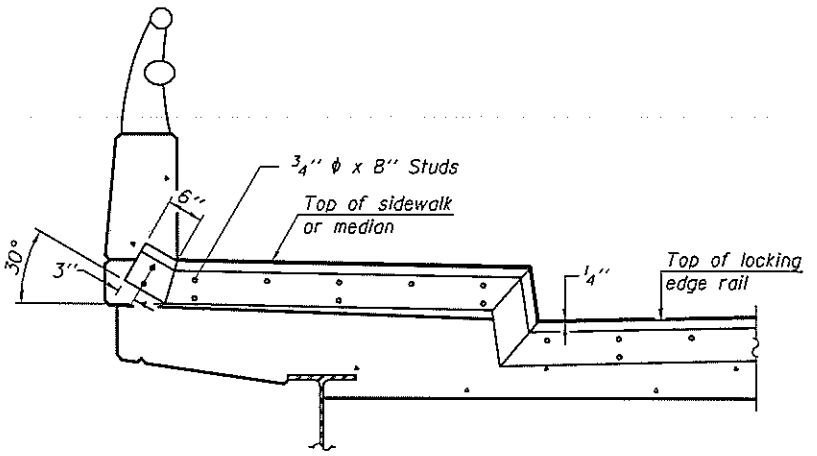
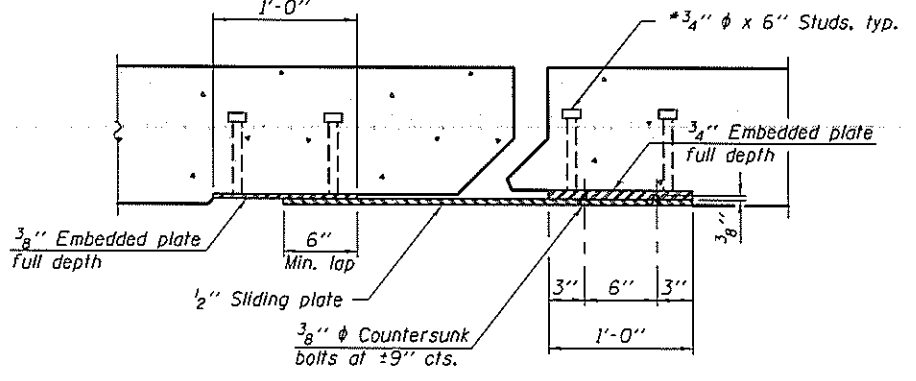
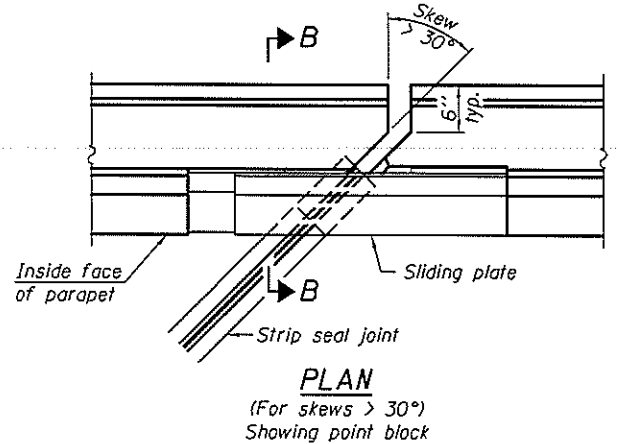
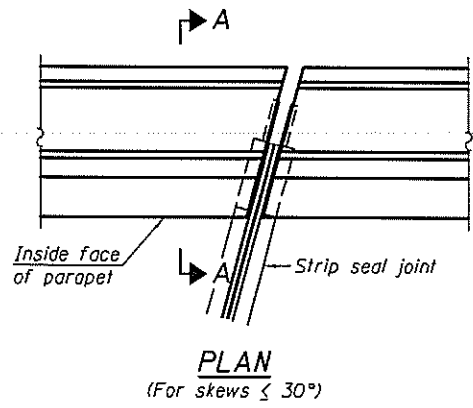
DATE JANUARY 29, 2016
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

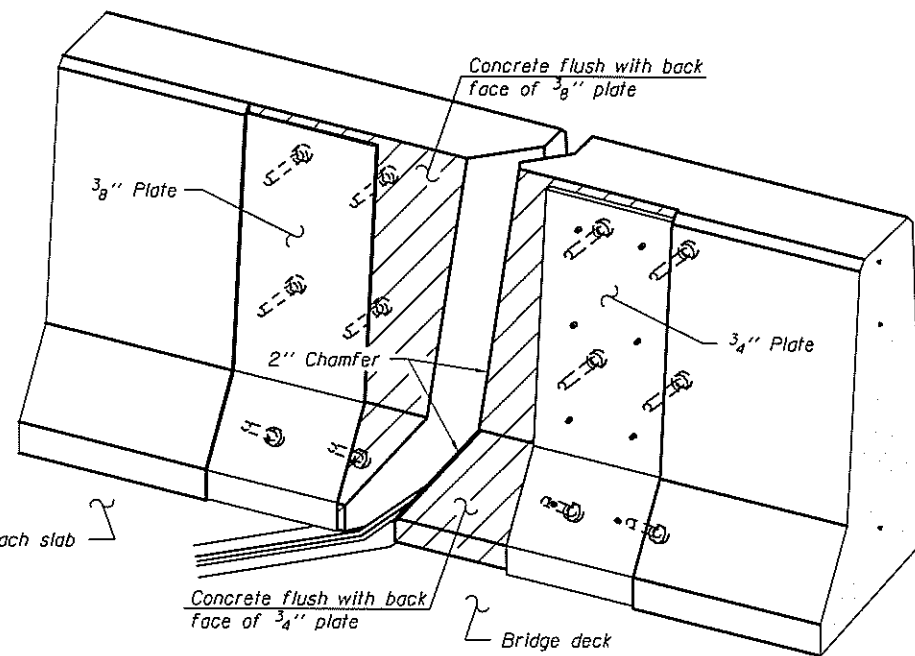
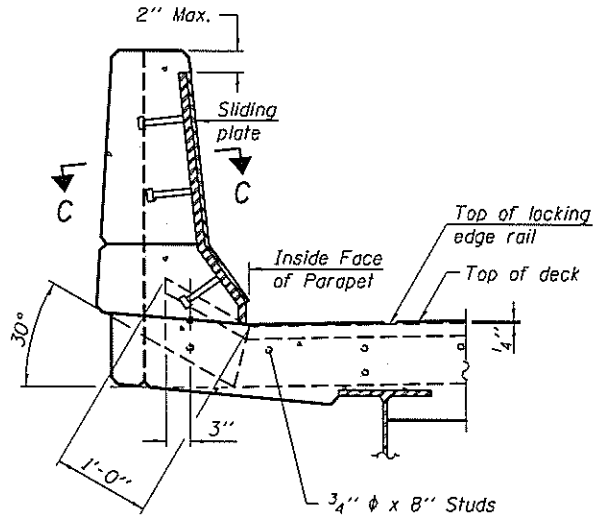
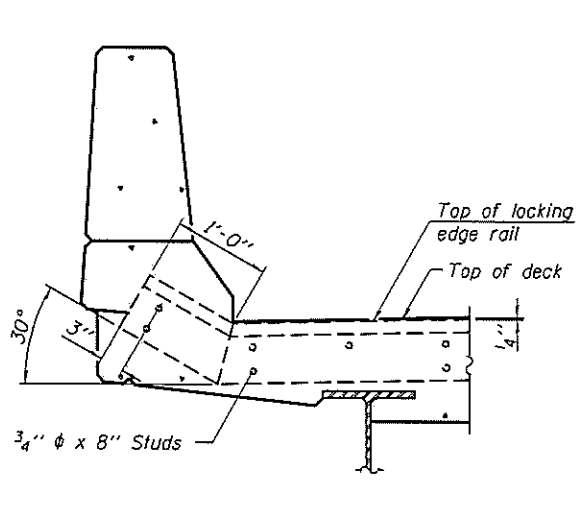
DECK SLAB REPAIRS
 SN 090-0119
 SHEET NO. 5 OF 8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	151

CONTRACT NO. 68606
 [ILLINOIS] FED. AID PROJECT

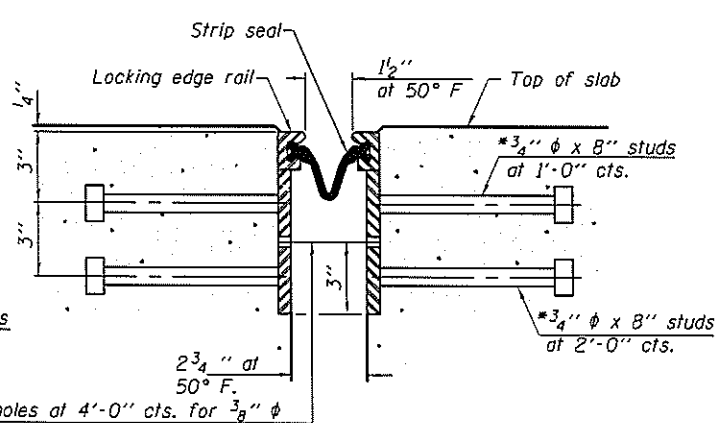
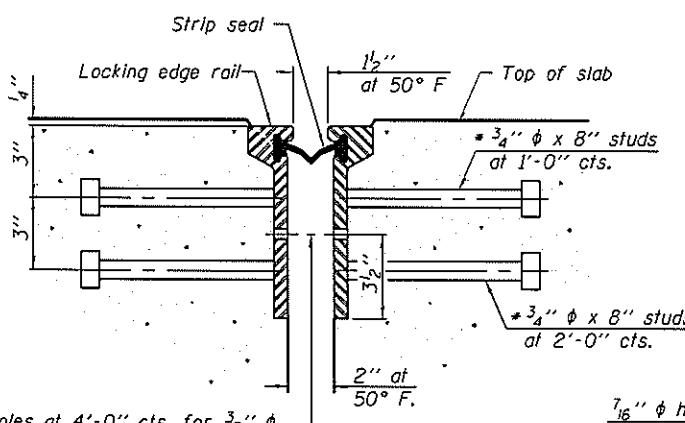


TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



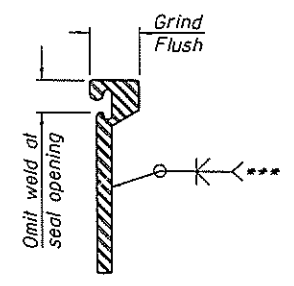
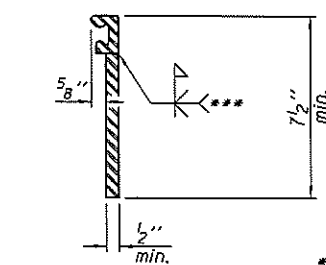
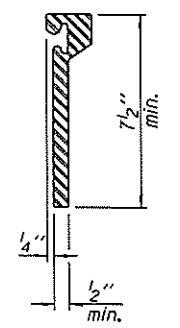
Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.
Parapet plates and anchorage studs for skews $> 30^\circ$ included in the cost of Preformed Joint Strip Seal.



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.



*** Back gouge not required if complete joint penetration is verified by mock-up.

ROLLED EXTRUDED RAIL

WELDED RAIL

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	66

EJ-SSJ

1-27-12

DESIGNED	SMR
CHECKED	CCC
DRAWN	J. Schneller
CHECKED	SMR CCC

PASSED
J. Carl Perry
ACTING ENGINEER OF BRIDGES AND STRUCTURES

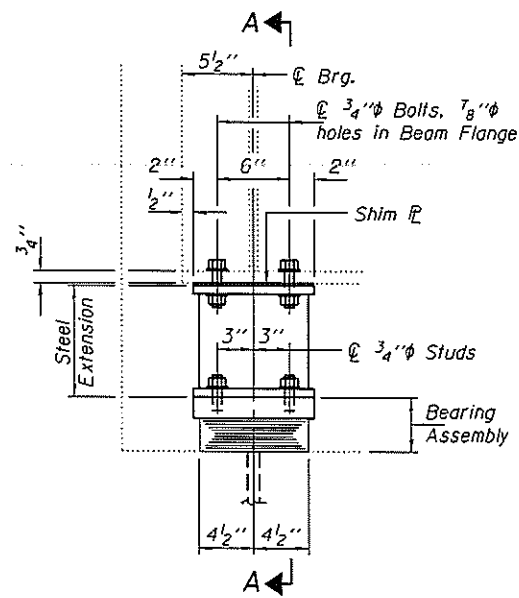
DATE	JANUARY 29, 2016
REVISED	
REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

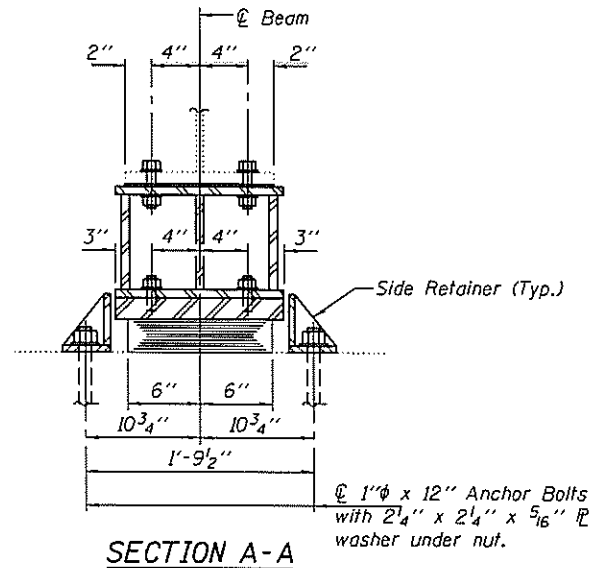
PREFORMED JOINT STRIP SEAL
SN 090-0119

SHEET NO. 6 OF 8 SHEETS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-5.7RS-1	TAZEWELL	245	158
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				

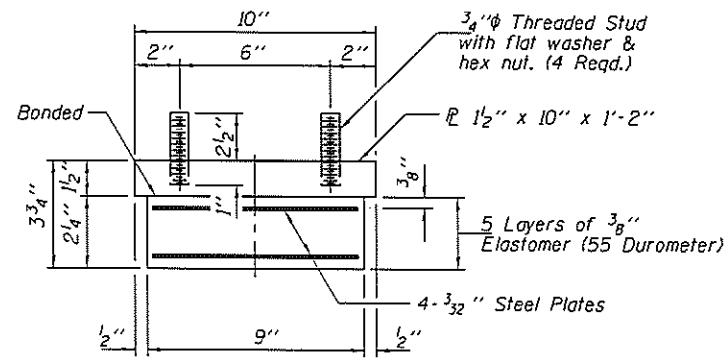


ELEVATION AT ABUTMENT



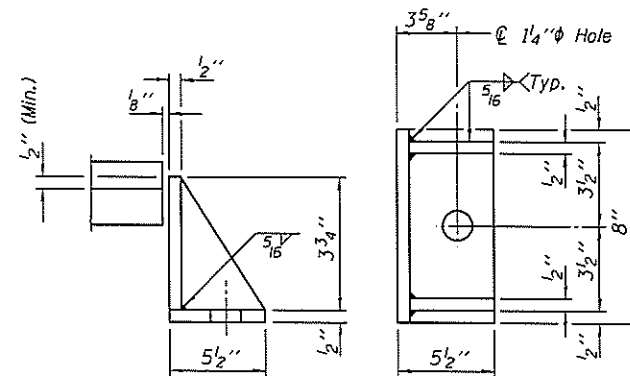
SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.



BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.



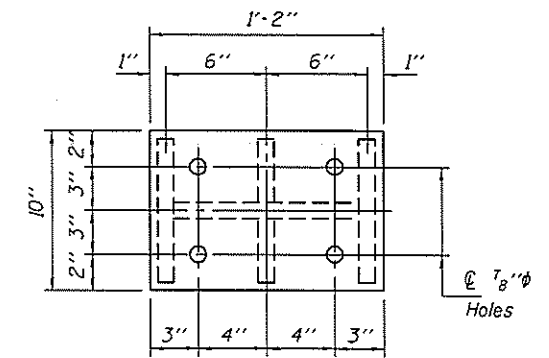
SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

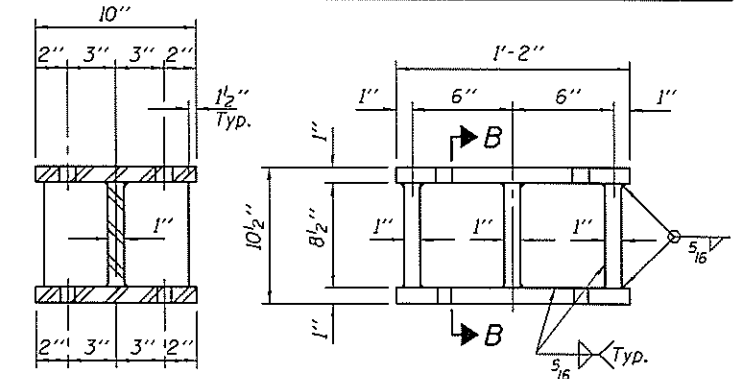
BEAM REACTIONS

RE	(K)	32.7
RL	(K)	35.9
Imp.	(K)	10.5
R (Total)	(K)	78.8

Notes:
Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Min. jack capacity = 45 Tons.
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled after the supported member is in place.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type I.

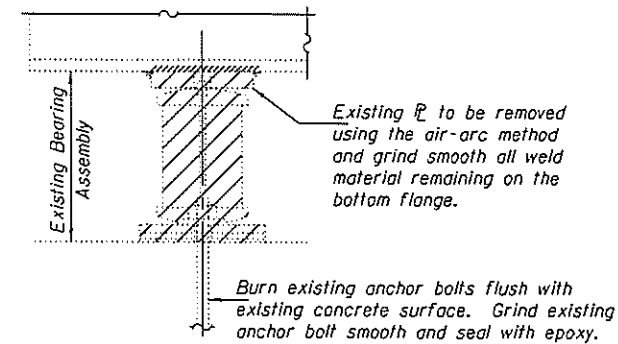


PLAN TOP AND BOTTOM PLATE



SECTION B-B

STEEL EXTENSION DETAIL



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	5
Jack and Remove Existing Bearings	Each	5
Furnishing and Erecting Structural Steel	Pound	850
Anchor Bolts 1"φ	Each	10

TYI/REPS 12-03-2008

DESIGNED SMR
CHECKED CCC
DRAWN J. Schneller
CHECKED SMR CCC

PASSED

ACTING ENGINEER OF BRIDGES AND STRUCTURES

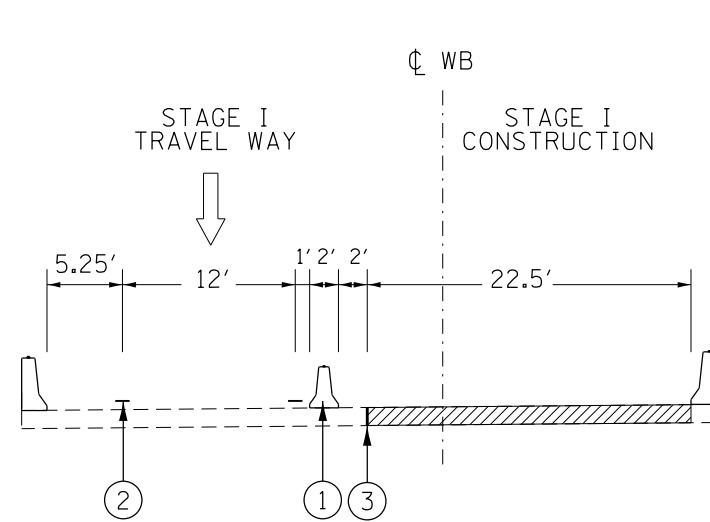
DATE JANUARY 29, 2016
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

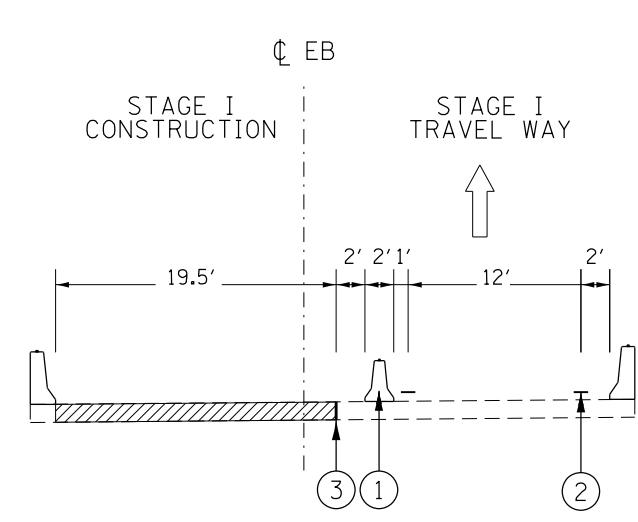
ELASTOMERIC BEARING DETAILS S. BRG P. 1
SN 090-0119

SHEET NO. 7 OF 8 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	159
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68606	

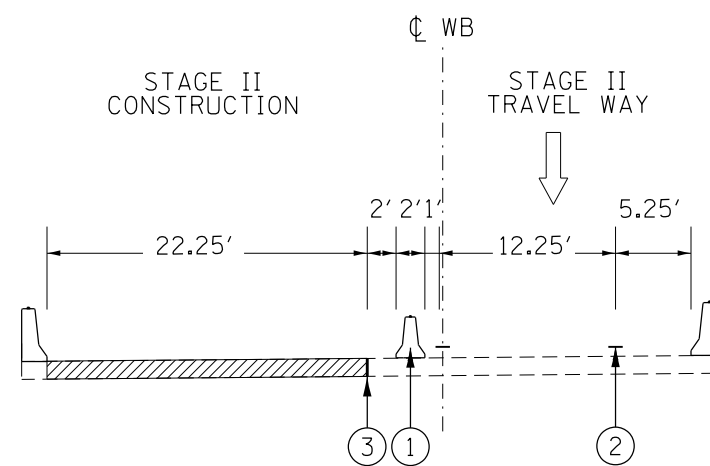


S.N. 090-0107

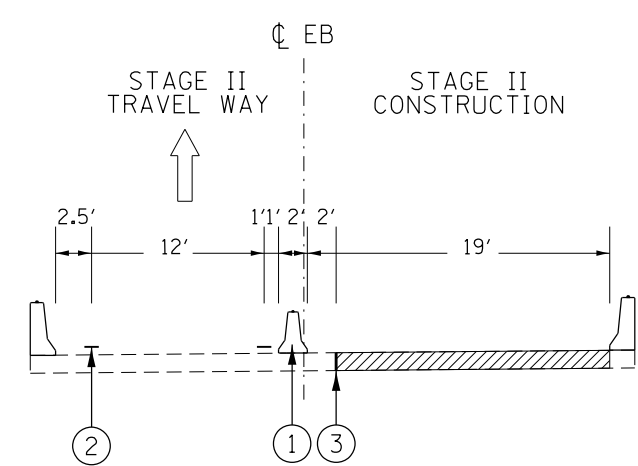


S.N. 090-0106

I-474 OVER IL 29
MAINTENANCE OF TRAFFIC - STAGE I



S.N. 090-0107



S.N. 090-0106

I-474 OVER IL 29
MAINTENANCE OF TRAFFIC - STAGE II

LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TYPE III PMK TAPE, 4"
- ③ STAGE CONSTRUCTION LINE

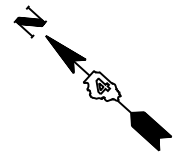
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

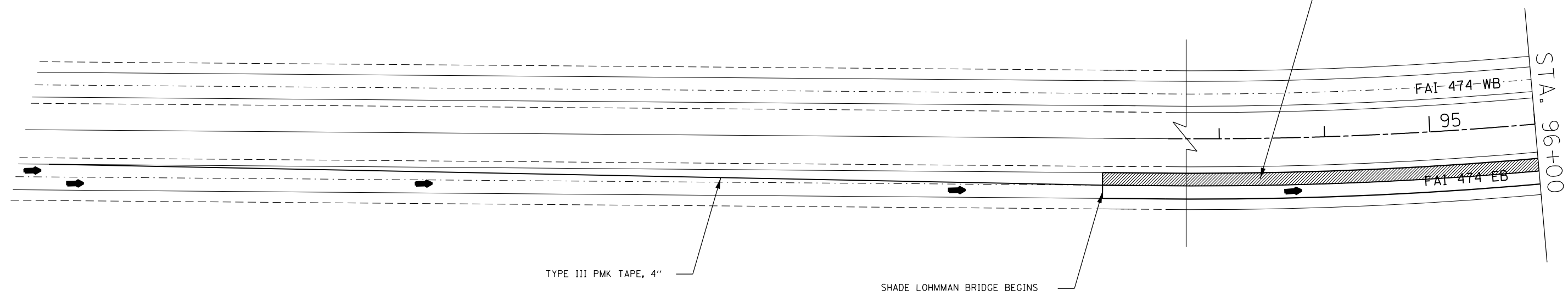
TRAFFIC CONTROL DETAIL STAGING FOR I-474 (E.B. & W.B.) OVER IL 29
S.N. 090-0106 & 090-0107

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	161
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68606	



THE PASSING LANE ON THE SHADE LOHMANN BRIDGE SHALL BE CLOSED AT ALL TIME DURING STAGE I AND II OF IL 29 BRIDGE WORK (SN 090-0106). TCP 701400 & 701401 SHALL BE APPLIED.



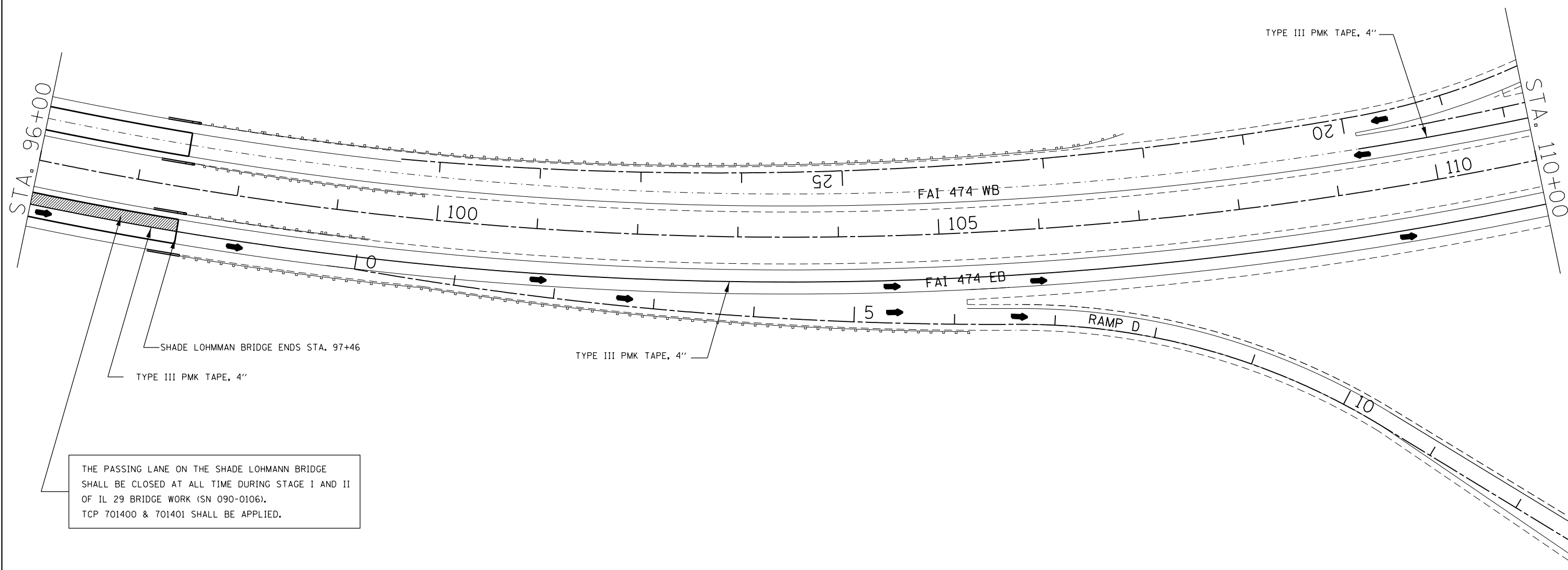
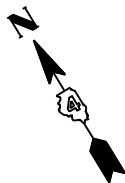
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING - STAGE I
S.N. 090-0106 & 090-0107**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	162
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68606	



THE PASSING LANE ON THE SHADE LOHMANN BRIDGE SHALL BE CLOSED AT ALL TIME DURING STAGE I AND II OF IL 29 BRIDGE WORK (SN 090-0106). TCP 701400 & 701401 SHALL BE APPLIED.

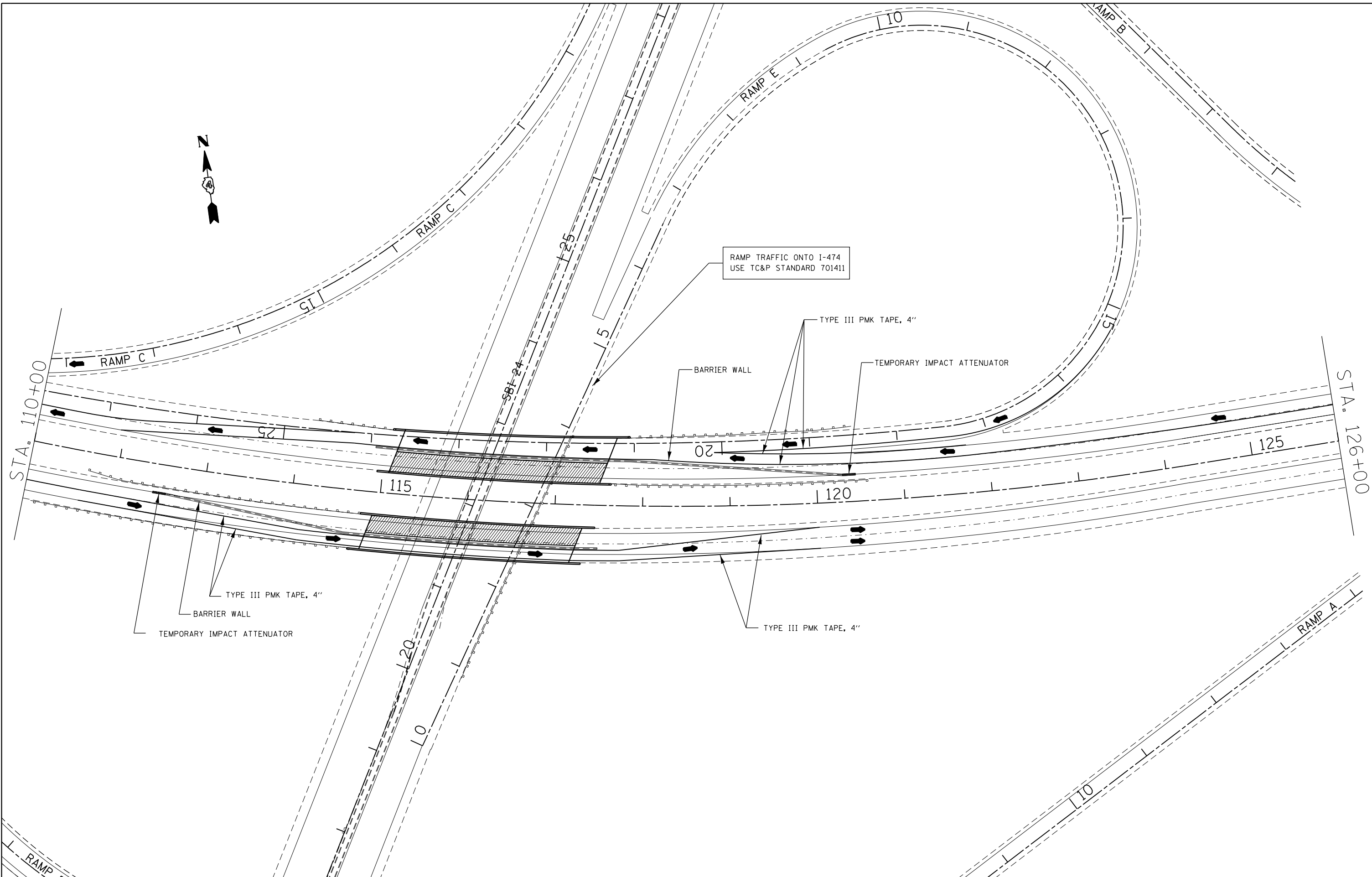
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING - STAGE I
S.N. 090-0106 & 090-0107**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	163
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL DETAIL STAGING - STAGE I
S.N. 090-0106 & 090-0107

SCALE: SHEET OF SHEETS STA. TO STA.

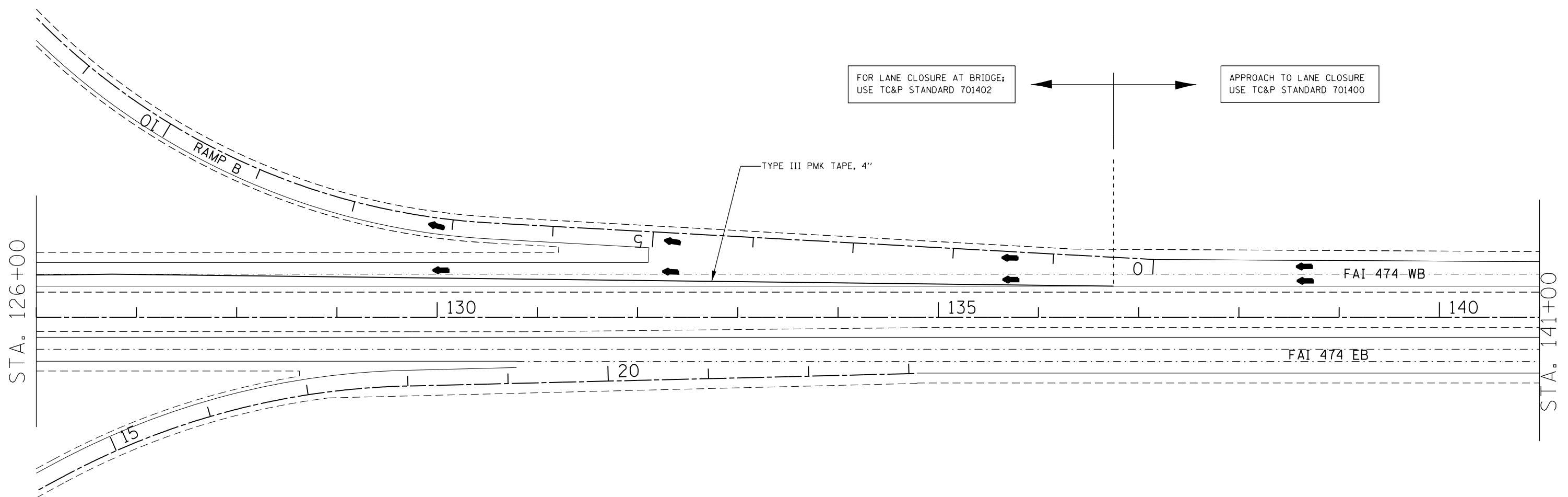
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474	90-6,7RS-1	TAZEWELL	245	164
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



FOR LANE CLOSURE AT BRIDGE;
USE TC&P STANDARD 701402

APPROACH TO LANE CLOSURE
USE TC&P STANDARD 701400

TYPE III PMK TAPE, 4"



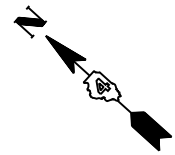
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

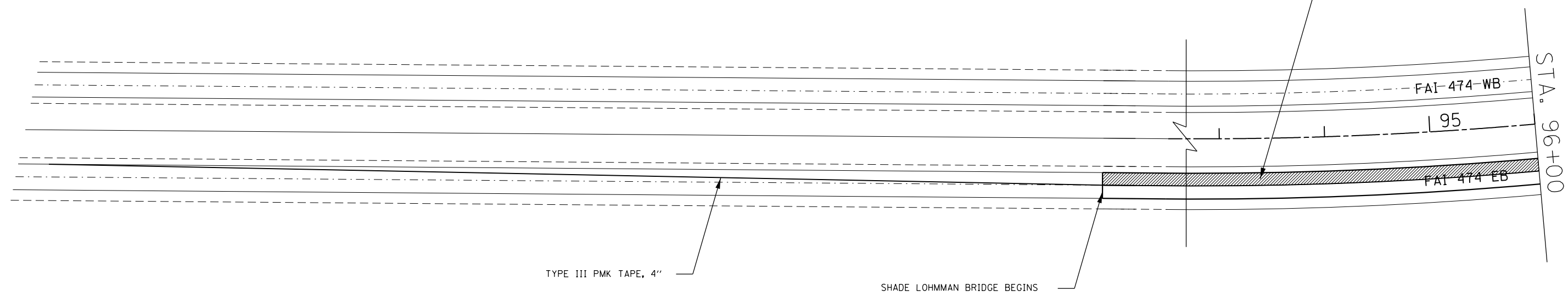
**TRAFFIC CONTROL DETAIL STAGING - STAGE I
S.N. 090-0106 & 090-0107**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	165
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



THE PASSING LANE ON THE SHADE LOHMANN BRIDGE SHALL BE CLOSED AT ALL TIME DURING STAGE I AND II OF IL 29 BRIDGE WORK (SN 090-0106). TCP 701400 & 701401 SHALL BE APPLIED.



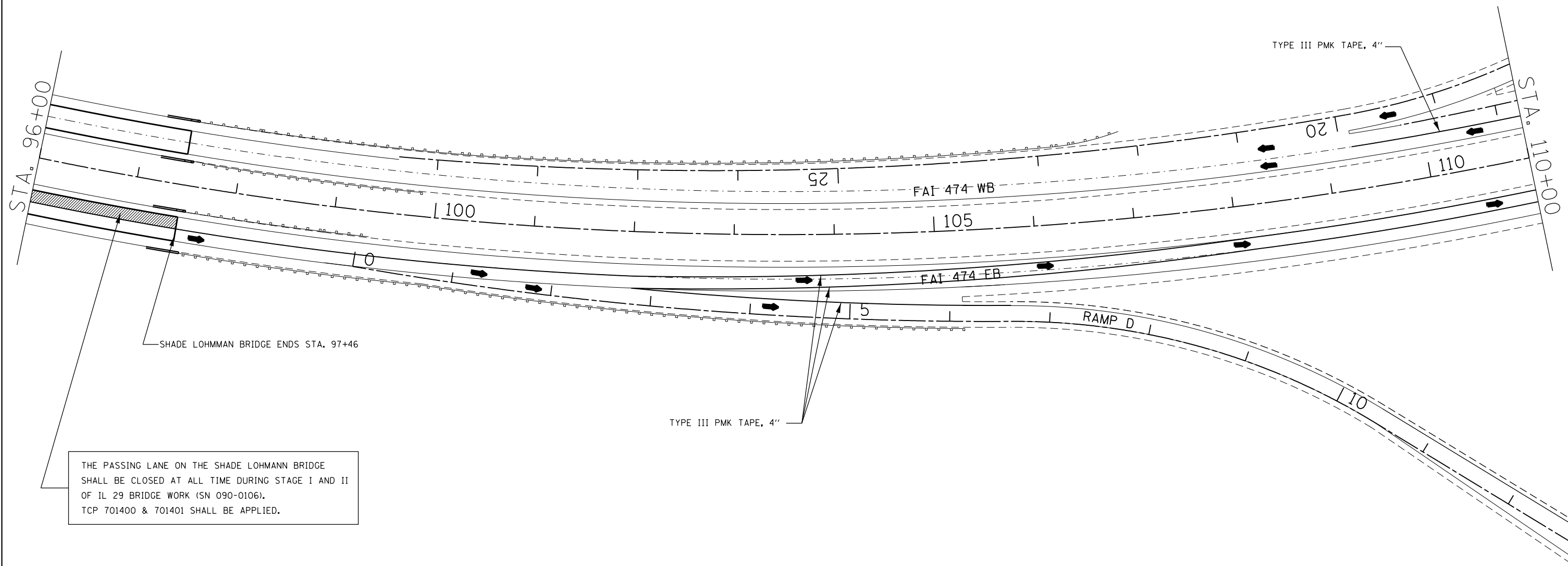
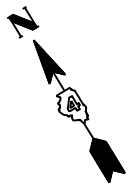
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	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING - STAGE II
S.N. 090-0106 & 090-0107**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	166
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



SHADE LOHMMAN BRIDGE ENDS STA. 97+46

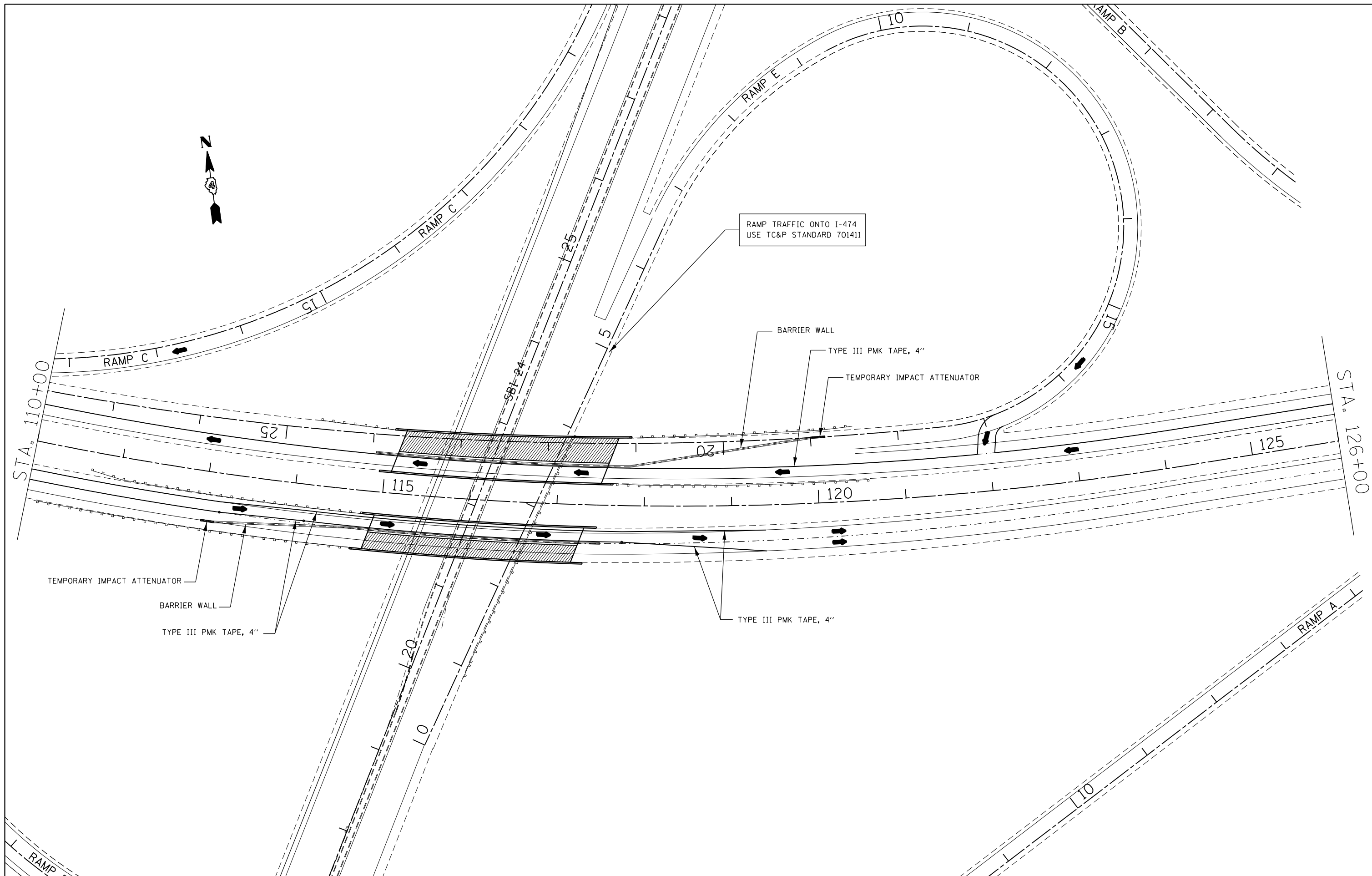
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL DETAIL STAGING - STAGE II			
S.N. 090-0106 & 090-0107			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAEWELL	245	167
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



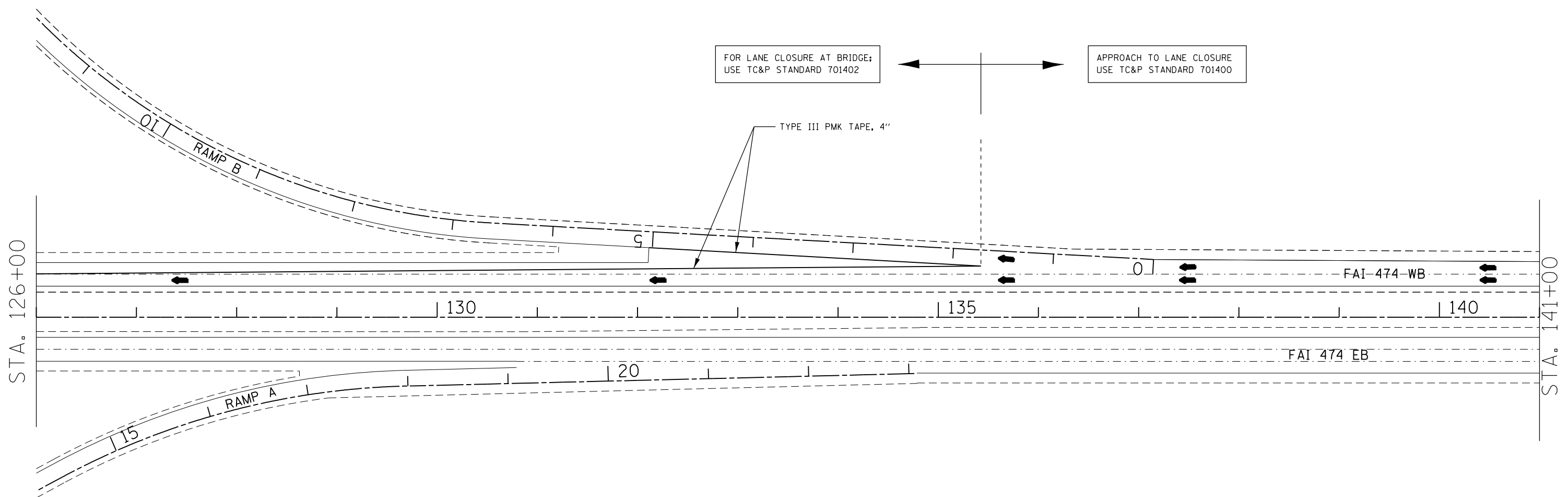
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL DETAIL STAGING - STAGE II
S.N. 090-0106 & 090-0107

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	168
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



FOR LANE CLOSURE AT BRIDGE;
USE TC&P STANDARD 701402

APPROACH TO LANE CLOSURE
USE TC&P STANDARD 701400

TYPE III PMK TAPE, 4"

STA. 126+00

STA. 141+00

130

135

140

FAI-474-WB

FAI-474-EB

20

RAMP A

15

RAMP B

01

01

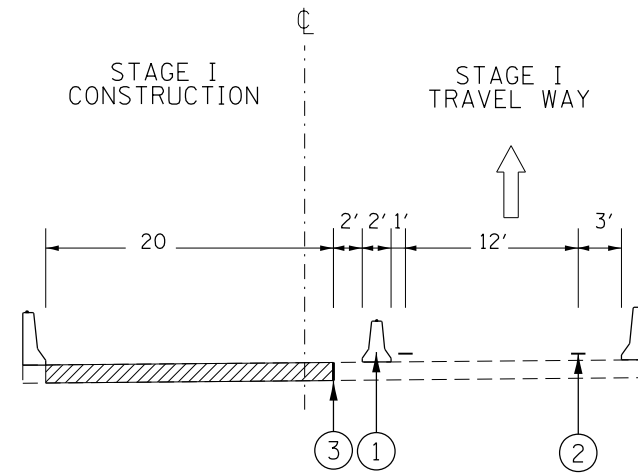
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

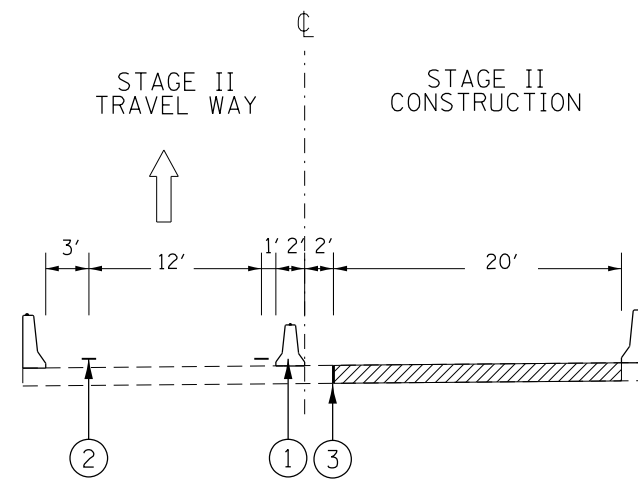
TRAFFIC CONTROL DETAIL STAGING - STAGE II
S.N. 090-0106 & 090-0107

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	169
CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT	



ROADWAY B OVER I-74
 MAINTENANCE OF TRAFFIC - STAGE I
 S.N. 090-0102



ROADWAY B OVER I-74
 MAINTENANCE OF TRAFFIC - STAGE II
 S.N. 090-0102

LEGEND	
①	TEMPORARY CONCRETE BARRIER
②	TYPE III PMK TAPE, 4"
③	STAGE CONSTRUCTION LINE

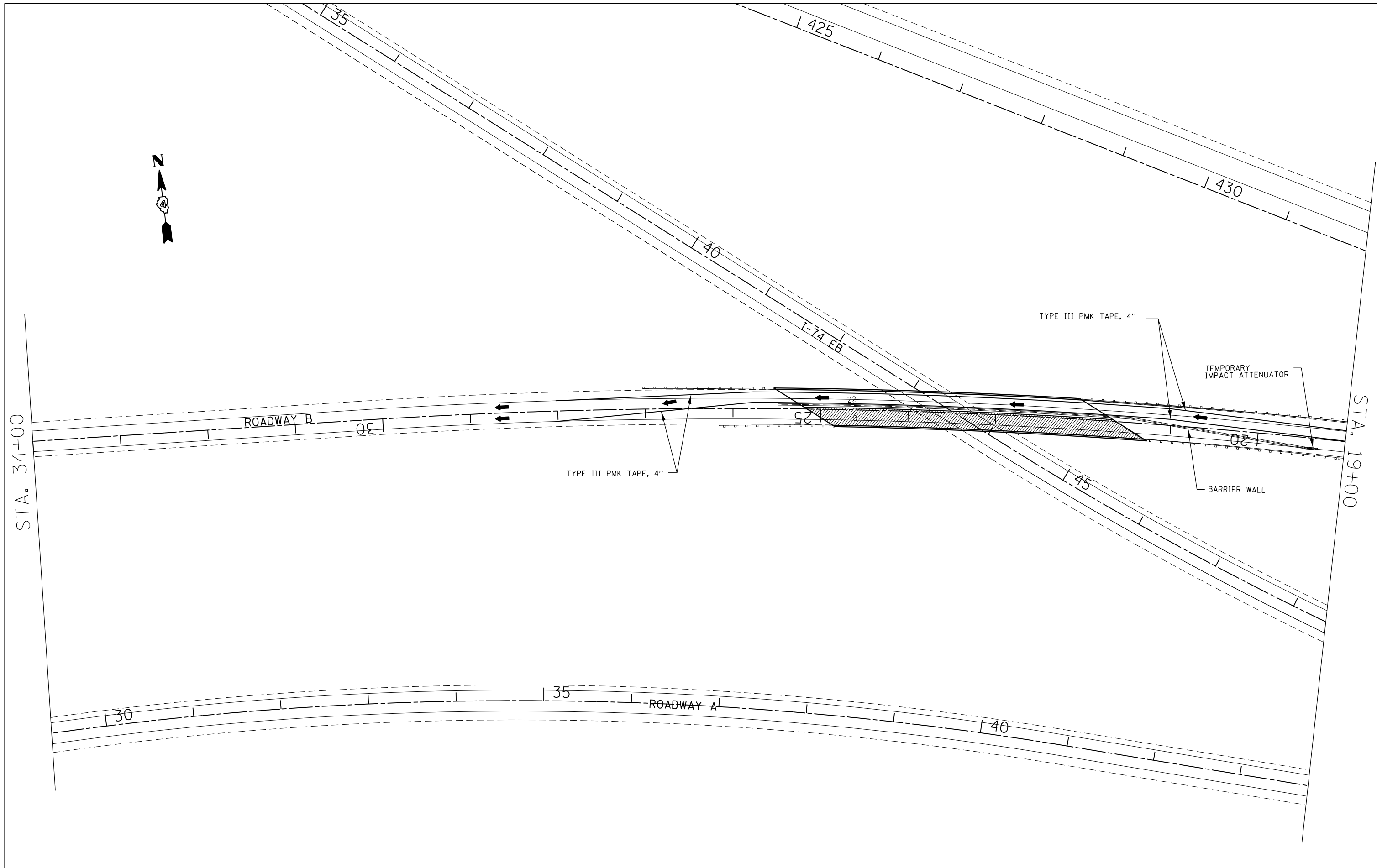
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Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING FOR (ROADWAY B OVER I-74)
 S.N. 090-0102**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAEWELL	245	170
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68606	



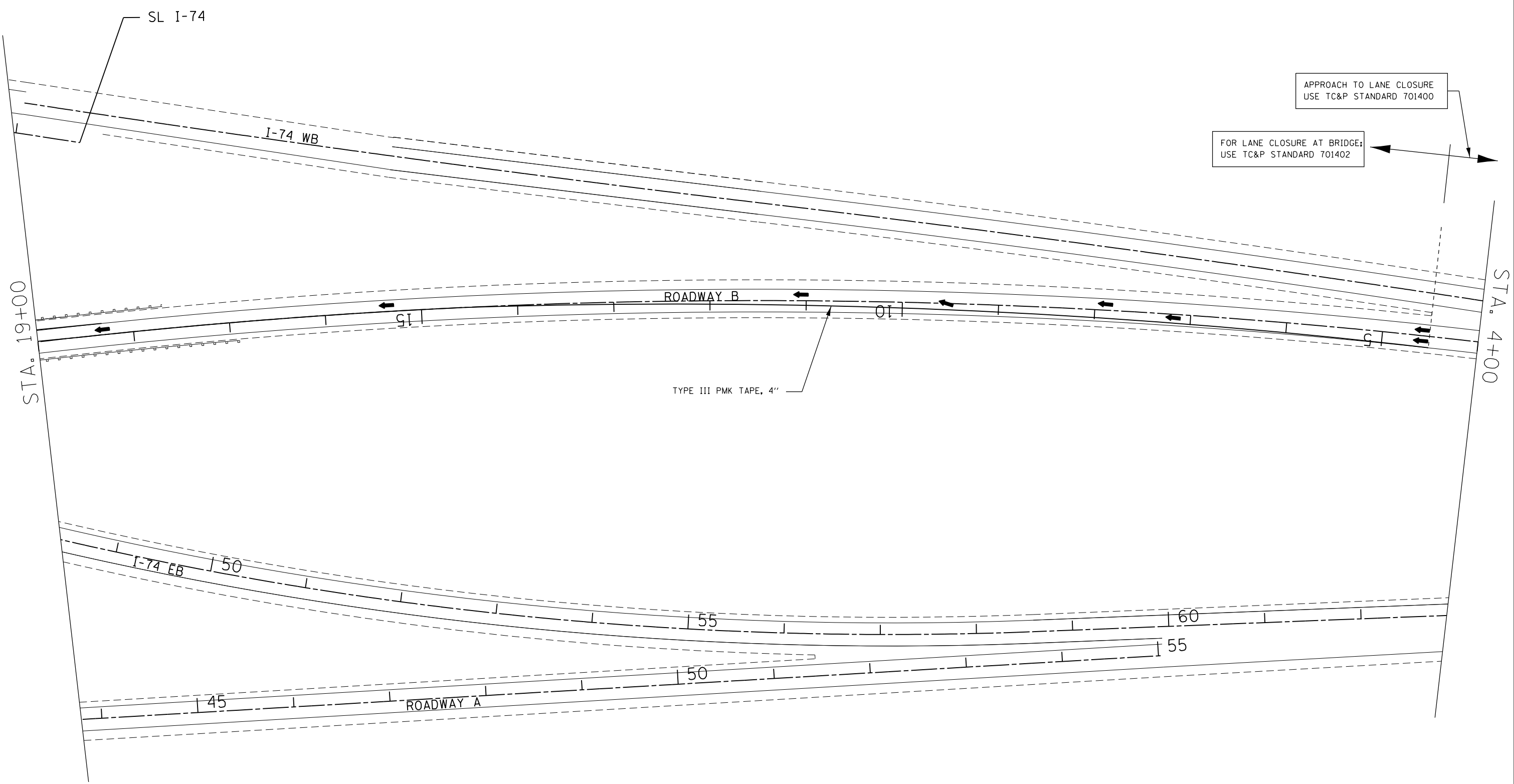
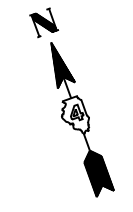
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Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING FOR (ROADWAY B OVER I-74)
S.N. 090-0102 STAGE I**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	171
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



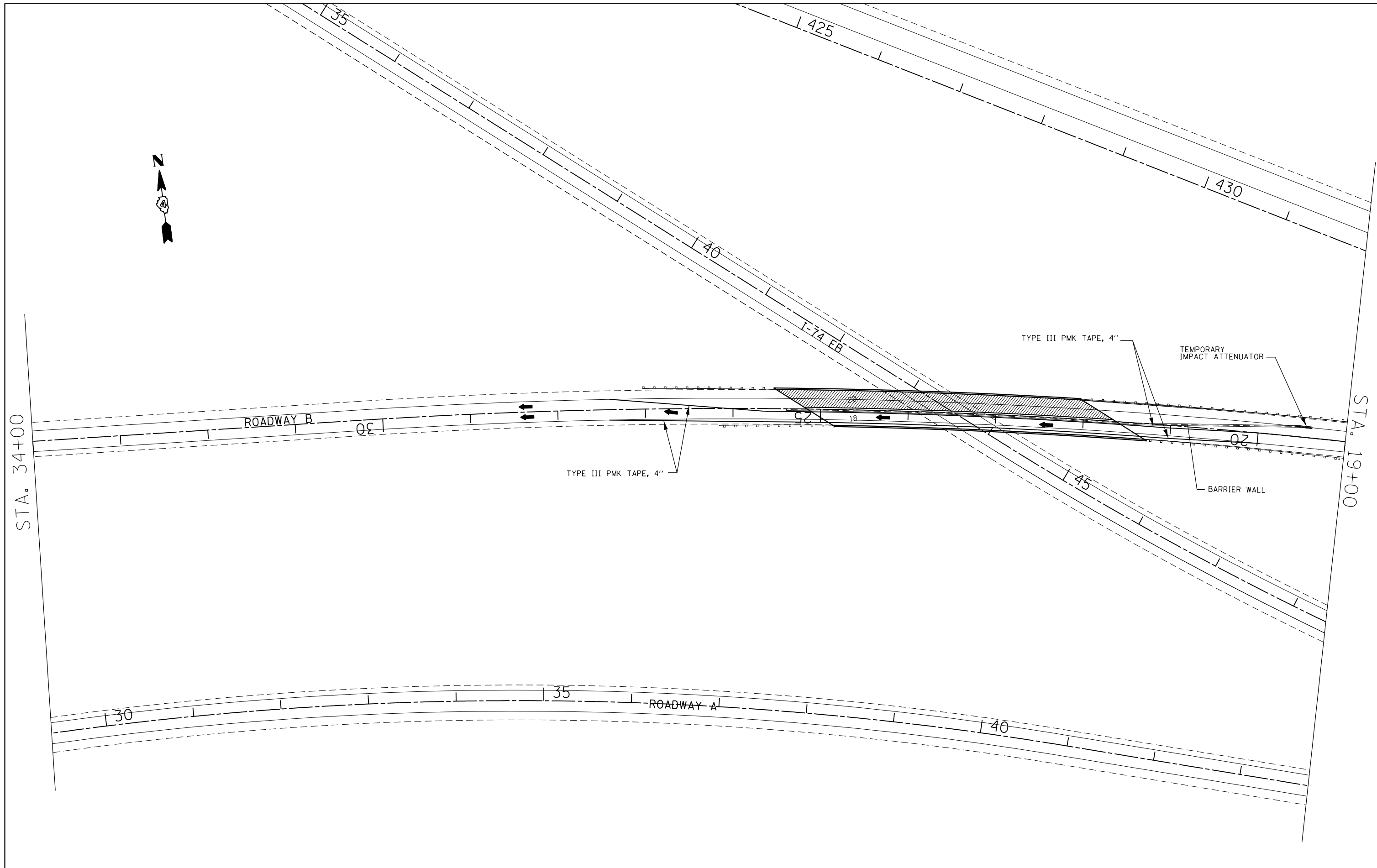
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Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING FOR (ROADWAY B OVER I-74)
S.N. 090-0102 STAGE I**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	172
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



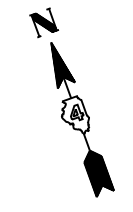
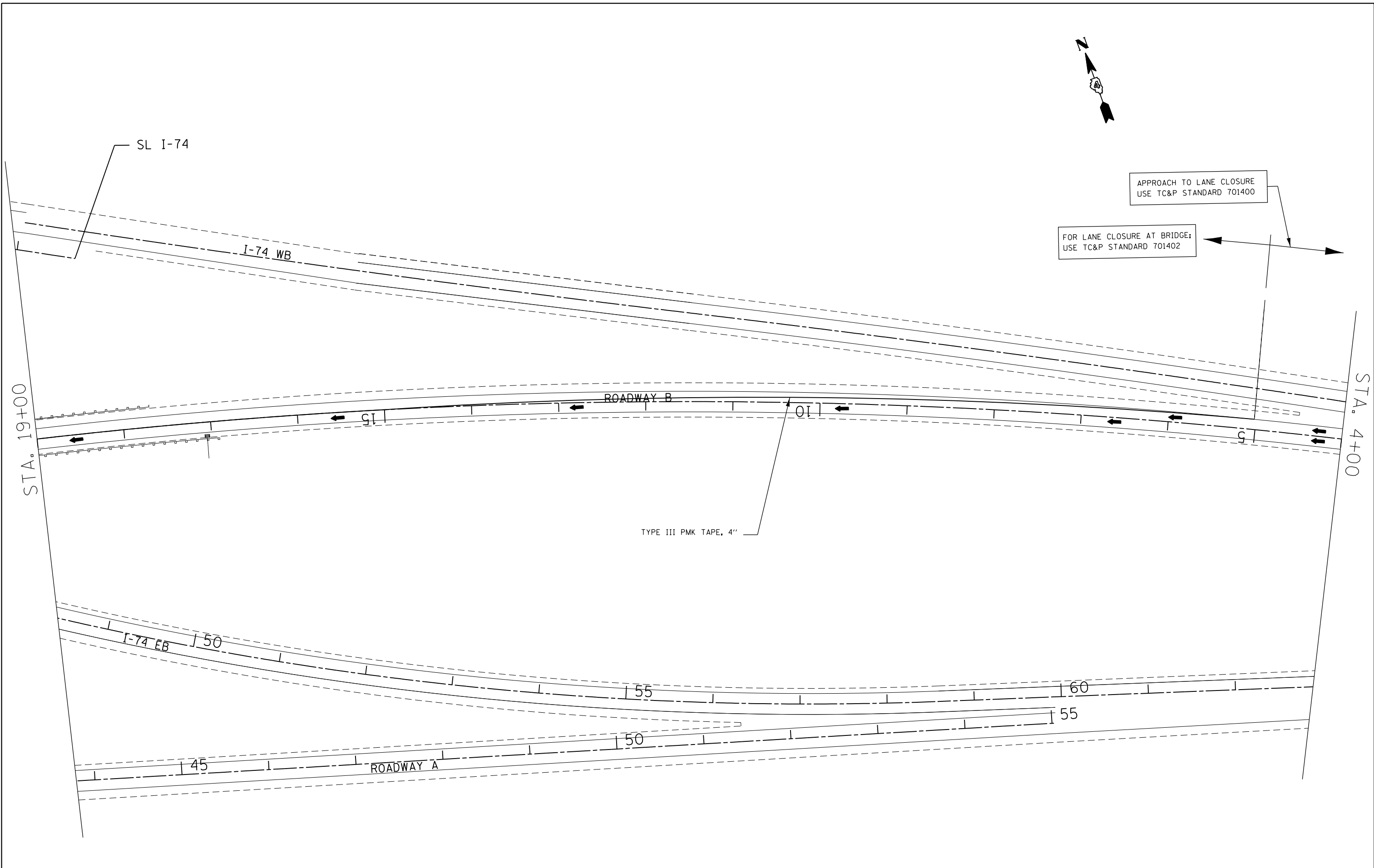
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Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING FOR (ROADWAY B OVER I-74)
S.N. 090-0102 STAGE II**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	173
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



APPROACH TO LANE CLOSURE
USE TC&P STANDARD 701400

FOR LANE CLOSURE AT BRIDGE:
USE TC&P STANDARD 701402

FILE NAME =	USER NAME = labobidism	DESIGNED -	REVISED -
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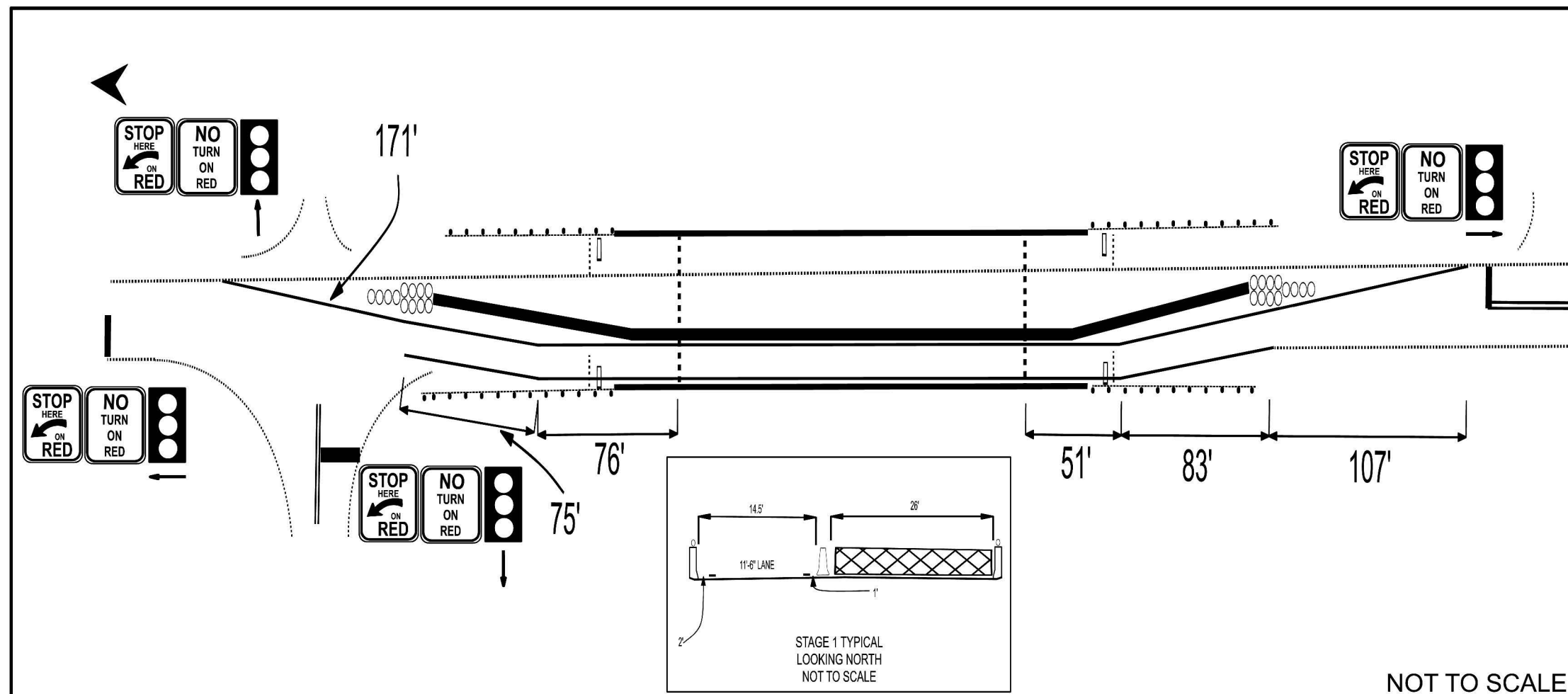
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAIL STAGING FOR (ROADWAY B OVER I-74)
S.N. 090-0102 STAGE II**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	174
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

TRAFFIC MANAGEMENT PLAN COLE STREET STAGE 1



NOT TO SCALE

STAGE 1 NOTES:

STD. 701321 SHALL BE USED ALONG WITH THE ABOVE DETAIL. DEVICES AND SIGNAGE NOT SHOWN FOR CLARIFICATION. ADDITIONAL SIGNS SHOWN ARE INCIDENTAL TO STD. 701321. STD. 701201 SHALL BE USED AS NEEDED. ADJUSTMENTS MAY NEED TO BE MADE TO FIT ACTUAL FIELD CONDITIONS. BLACKOUT TAPE SHALL NOT BE USED TO COVER CONFLICTING MARKINGS. ONE CHANGEABLE MESSAGE BOARD PER DIRECTION SHALL BE USED TO GIVE ADVANCE NOTICE TO PUBLIC THREE DAYS IN ADVANCE. A STATIC SIGN SHALL BE ALLOWED IF ANOTHER OVERPASS WILL START AT THE SAME TIME WITH NO ADDITIONAL COST ALLOWED.

ALL EXISTING PAVEMENT MARKING SHALL BE REMOVED PRIOR TO PLACING STAGE 1 CONSTRUCTION. THE YELLOW CENTERLINE ON THE BRIDGE SHALL REMAIN AND DOES NOT NEED TO BE REMOVED.

PAVEMENT MARKING REMOVAL	839 SQ. FT.
4" TYPE IV TAPE-WHITE	1428 LIN. FT.
4" TYPE IV -YELLOW	70 LIN. FT.
TEST LEVEL III TEMPORARY IMPACT ATTENUATORS NON-REDIRECTIVE	2 EACH
TEMPORARY CONCRETE BARRIER	450 FT.
TEMPORARY SIGNALS-STAGE 1 & 2 INCLUSIVE	1 EACH
STD. 701201-STAGE 1 & 2 INCLUSIVE	1 LUMP SUM
STD. 701321-STAGE 1 & 2 INCLUSIVE	1 EACH

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Default	PLOT SCALE = 100.000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC MANAGEMENT PLAN

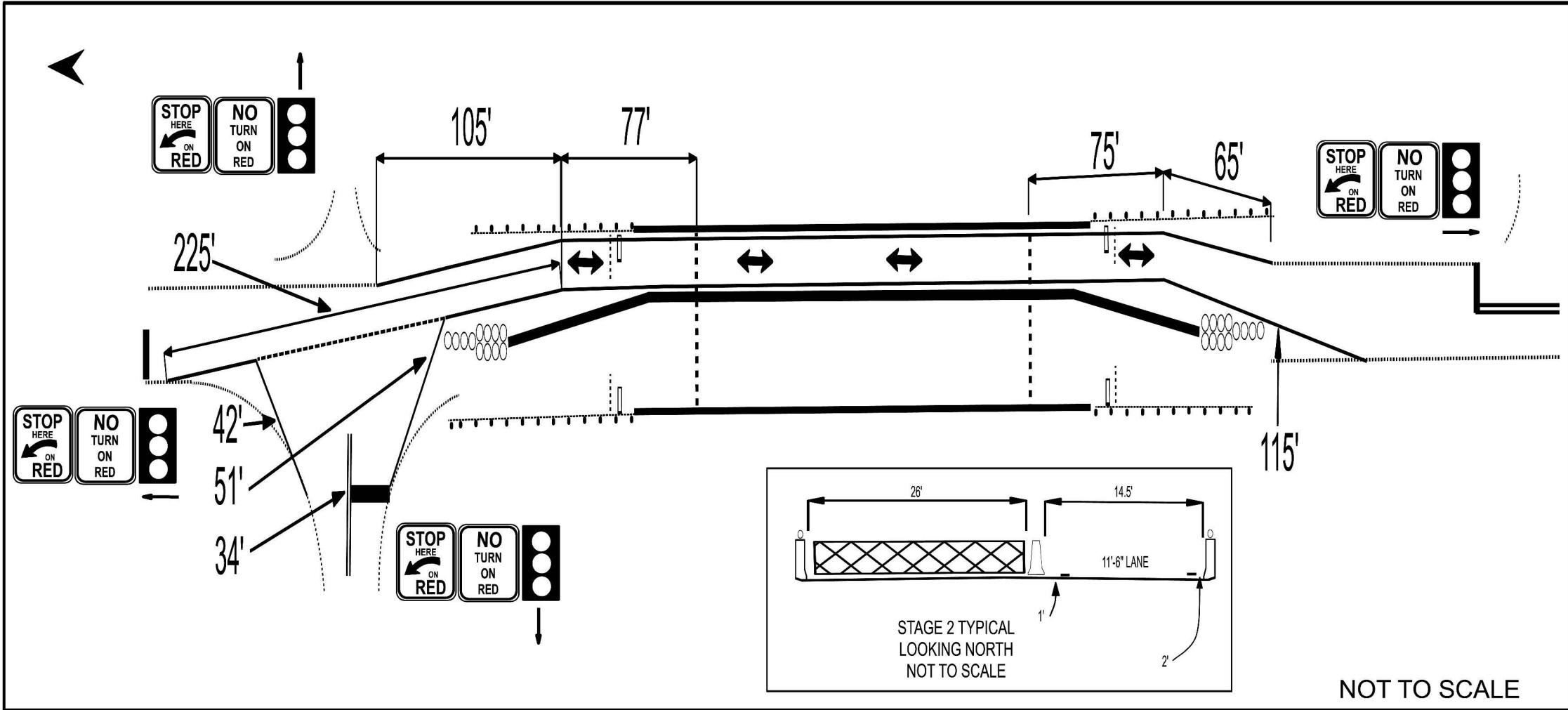
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	175
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

TRAFFIC MANAGEMENT PLAN

COLE STREET

STAGE 2



STAGE 1 NOTES:

STD. 701321 SHALL BE USED ALONG WITH THE ABOVE DETAIL. DEVICES AND SIGNAGE NOT SHOWN FOR CLARIFICATION. ADDITIONAL SIGNS SHOWN ARE INCIDENTAL TO STD. 701321. STD. 701201 SHALL BE USED AS NEEDED. ADJUSTMENTS MAY NEED TO BE MADE TO FIT ACTUAL FIELD CONDITIONS. BLACKOUT TAPE SHALL NOT BE USED TO COVER CONFLICTING MARKINGS. ONE CHANGEABLE MESSAGE BOARD PER DIRECTION SHALL BE USED TO GIVE ADVANCE NOTICE TO PUBLIC THREE DAYS IN ADVANCE. A STATIC SIGN SHALL BE ALLOWED IF ANOTHER OVERPASS WILL START AT THE SAME TIME WITH NO ADDITIONAL COST ALLOWED.

ALL EXISTING PAVEMENT MARKING SHALL BE REMOVED PRIOR TO PLACING STAGE 1 CONSTRUCTION. THE YELLOW CENTERLINE ON THE BRIDGE SHALL REMAIN AND DOES NOT NEED TO BE REMOVED.

WORK ZONE PAVEMENT MARKING REMOVAL-FROM STAGE 1	381 SQ. FT.
4" TYPE IV TAPE-WHITE	1275 LIN. FT.
RELOCATE TEST LEVEL III TEMPORARY IMPACT ATTENUATORS NON-REDIRECTIVE	2 EACH
RELOCATE TEMPORARY CONCRETE BARRIER	400 FT.
TEMPORARY SIGNALS-STAGE 1 & 2 INCLUSIVE	1 EACH
STD. 701201-STAGE 1 & 2 INCLUSIVE	1 LUMP SUM
STD. 701321-STAGE 1 & 2 INCLUSIVE	1 EACH
WORK ZONE PAVEMENT MARKING REMOVAL-STAGE 2	567 SQ. FT.

FILE NAME =	USER NAME = lebedism	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC MANAGEMENT PLAN

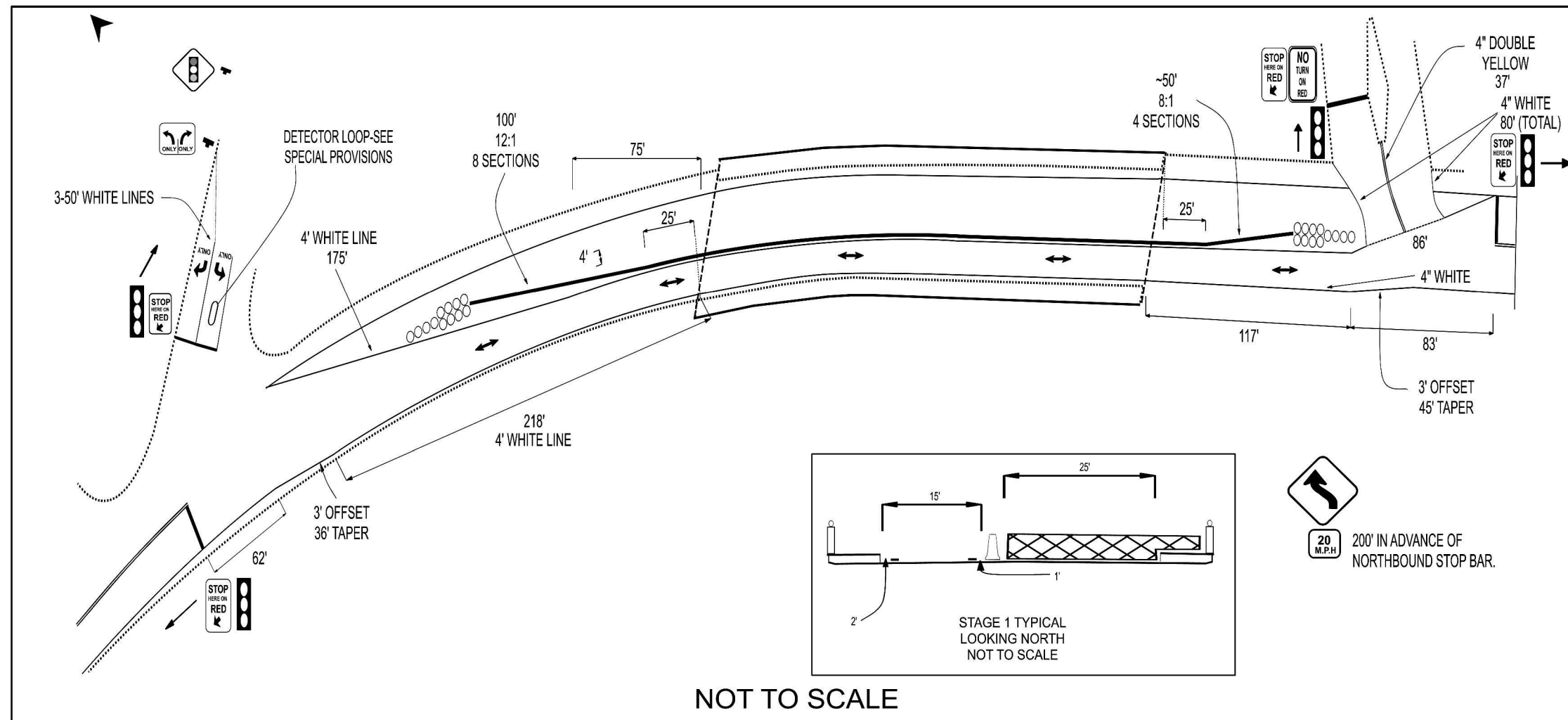
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	176
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

TRAFFIC MANAGEMENT PLAN

SPRINGFIELD ROAD

STAGE 1



STAGE 1 NOTES:

STD. 701321 SHALL BE USED ALONG WITH THE ABOVE DETAIL. DEVICES AND SIGNAGE NOT SHOWN FOR CLARIFICATION. ADDITIONAL SIGNS SHOWN ARE INCIDENTAL TO STD. 701321. STD. 701201 SHALL BE USED AS NEEDED. STD. 701801 SHALL BE USED TO CLOSE SIDEWALK. ADJUSTMENTS MAY NEED TO BE MADE TO FIT ACTUAL FIELD CONDITIONS. BLACKOUT TAPE SHALL NOT BE USED TO COVER CONFLICTING MARKINGS. ADVISORY SPEED LIMIT ON NORTHBOUND "ONE LANE ROAD AHEAD" SIGN SHALL BE 20 MPH. ONE CHANGEABLE MESSAGE BOARD PER DIRECTION SHALL BE USED TO GIVE ADVANCE NOTICE TO PUBLIC THREE DAYS IN ADVANCE. A STATIC SIGN SHALL BE ALLOWED IF ANOTHER OVERPASS WILL START AT THE SAME TIME WITH NO ADDITIONAL COST ALLOWED.

ALL EXISTING PAVEMENT MARKING SHALL BE REMOVED PRIOR TO PLACING STAGE 1 CONSTRUCTION. THE YELLOW CENTERLINE ON THE BRIDGE SHALL REMAIN AND DOES NOT NEED TO BE REMOVED.

SHOULDER EXCAVATION AND HMA SHOULDER REPLACEMENT SHALL BE COMPLETED ON ALL FOUR QUADRANTS PRIOR TO COMMENCING STAGE 1 WORK.

PAVEMENT MARKING REMOVAL	790 SQ. FT.
4" TYPE IV TAPE-WHITE	2185 LIN. FT.
4" TYPE IV -YELLOW	40 LIN. FT.
TEMPORARY PAVEMENT MARKING LETTER AND SYMBOLS	42 SQ. FT.
TEST LEVEL III TEMPORARY IMPACT ATTENUATORS NON-REDIRECTIVE	2 EACH
TEMPORARY CONCRETE BARRIER	475 FT.
TEMPORARY SIGNALS -STAGE 1 & 2 INCLUSIVE	1 EACH
STD. 701201-STAGE 1 & 2 INCLUSIVE	1 LUMP SUM
STD. 701801-STAGE 1 & 2 INCLUSIVE	1 LUMP SUM
STD. 701321-STAGE 1 & 2 INCLUSIVE	1 EACH

NOT TO SCALE

FILE NAME =	USER NAME = lebedisman	DESIGNED -	REVISED -
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	PLOT DATE = 12/11/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

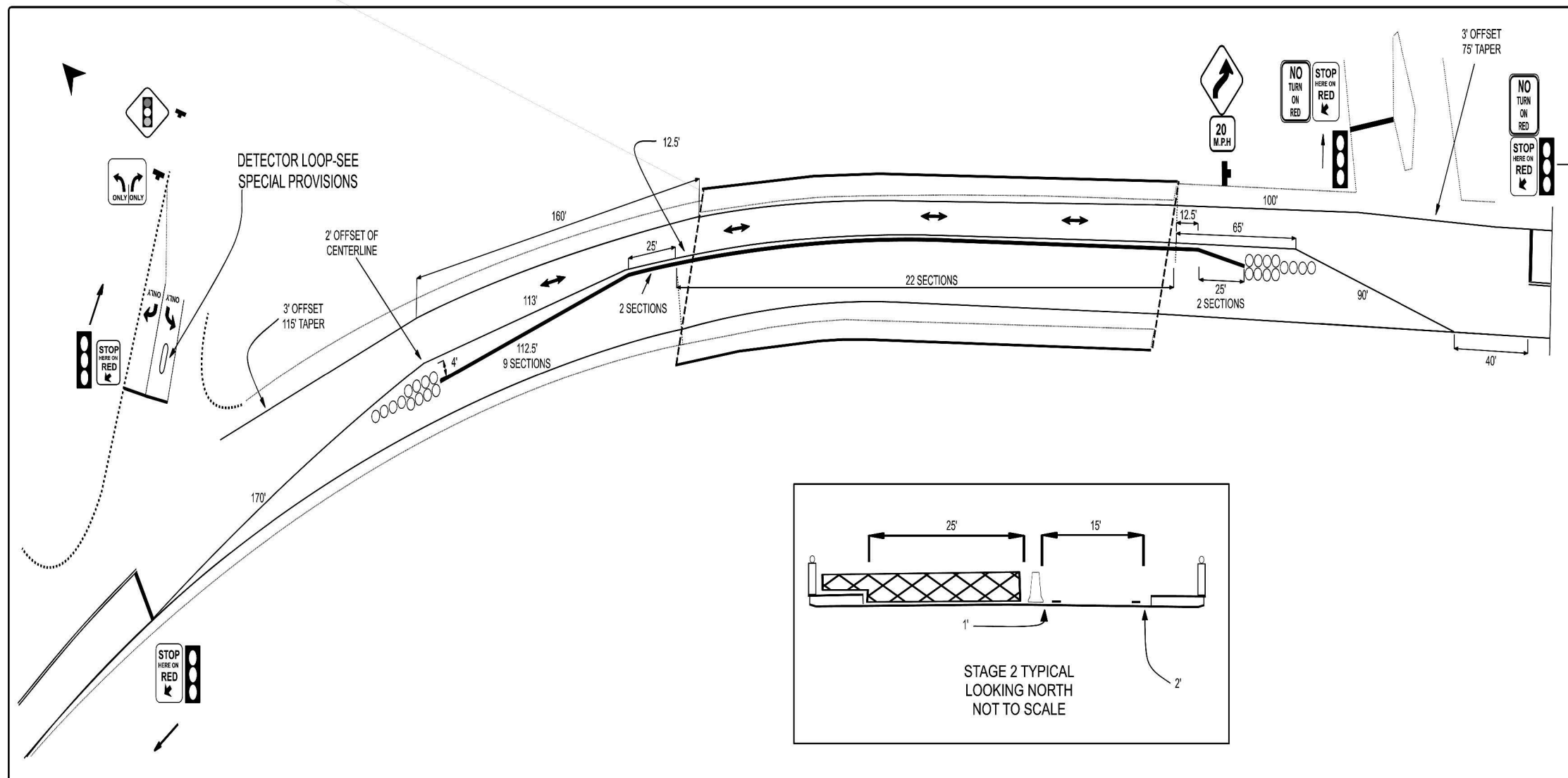
TRAFFIC MANAGEMENT PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	177
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

TRAFFIC MANAGEMENT PLAN

SPRINGFIELD ROAD STAGE 2



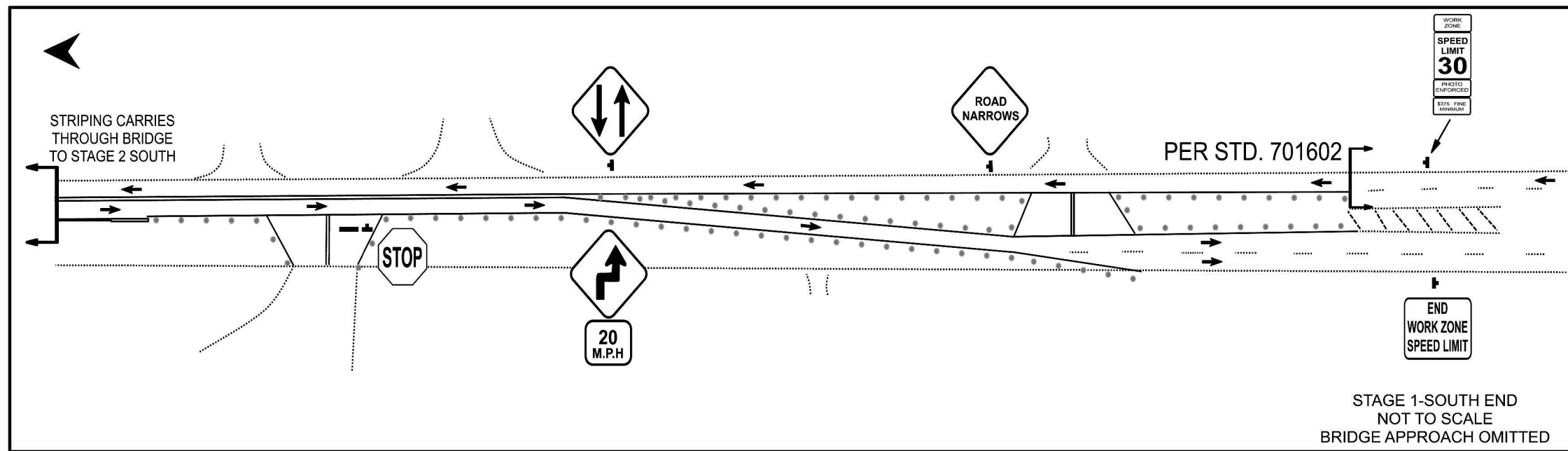
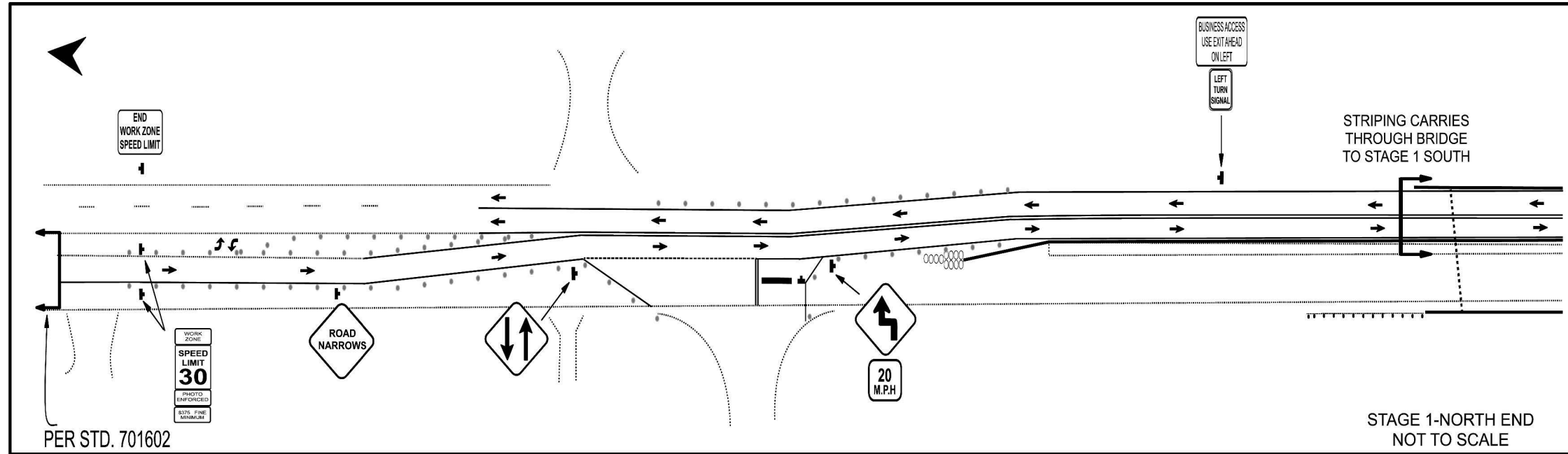
STAGE 2 NOTES:

STD. 701321 SHALL BE USED ALONG WITH THE ABOVE DETAIL. DEVICES AND SIGNAGE NOT SHOWN FOR CLARIFICATION. ADDITIONAL SIGNS SHOWN ARE INCIDENTAL TO STD. 701321. STD. 701201 SHALL BE USED AS NEEDED. STD. 701801 SHALL BE USED TO CLOSE SIDEWALK. ADJUSTMENTS MAY NEED TO BE MADE TO FIT ACTUAL FIELD CONDITIONS. BLACKOUT TAPE SHALL NOT BE USED TO COVER CONFLICTING MARKINGS. ADVISORY SPEED LIMIT ON NORTHBOUND "ONE LANE ROAD AHEAD" SIGN SHALL BE AS PER STANDARD FOR STAGE 2.

WORK ZONE PAVEMENT MARKING REMOVAL - FROM STAGE 1	510 SQ. FT.
4" TYPE IV TAPE-WHITE	1455 LIN. FT.
RELOCATE TEST LEVEL III TEMP. IMPACT ATTENUATORS NON-REDIRECTIVE	2 EACH
RELOCATE TEMPORARY CONCRETE BARRIER	475 FT.
WORK ZONE PAVEMENT MARKING REMOVAL - FROM STAGE 2	757 SQ. FT.

FILE NAME =	USER NAME = lebabidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC MANAGEMENT PLAN	F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
SA\GEN\DRIFT\STD&PLNS\SQUAD 0\1-474 3R	Project\english files\Staging sheets for I-474	DRAWN	REVISED			474	90-6,7RS-1	TAZEWELL	245	178	
Default	PLOT SCALE = 100.000' / 1in.	CHECKED -	REVISED -			CONTRACT NO. 68606					
	PLOT DATE = 12/11/2015	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO

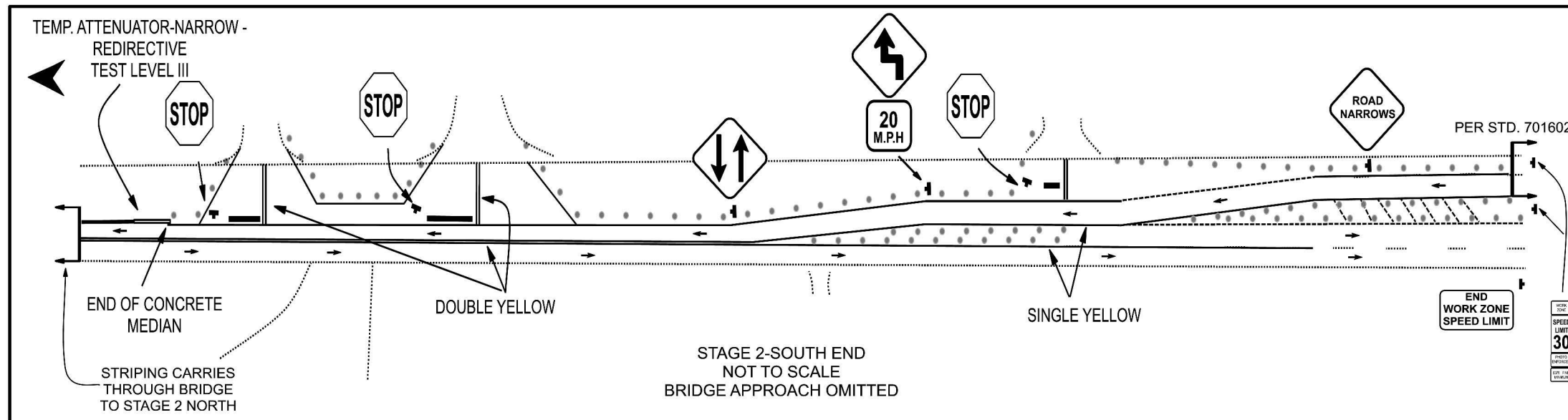
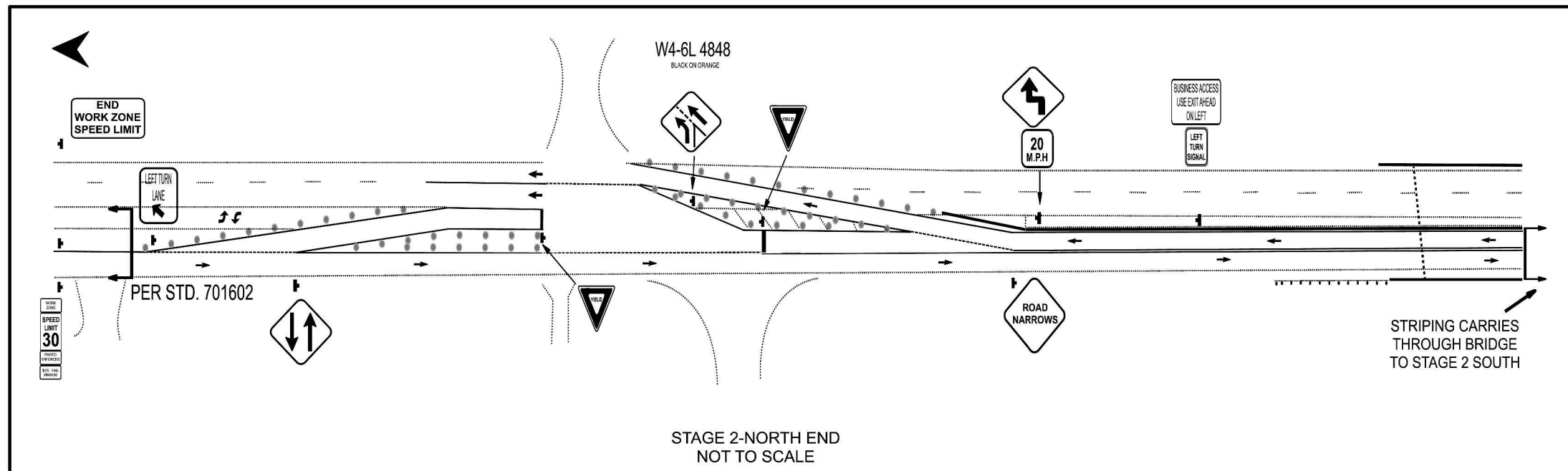
TRAFFIC MANAGEMENT PLAN PINECREST DRIVE STAGE 1



FILE NAME =	USER NAME = lebabidjan	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC MANAGEMENT PLAN			F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SA\GEN\DRIFT\STD&PLNS\SQUAD 0\1-474 3R	Project\english files\Staging sheets for I-474\DRAWN structures.dgn	CHECKED -	REVISED -								90-6,7RS-1	TAZEWELL
Default	PLOT SCALE = 100.000' / 1in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 68606				
	PLOT DATE = 12/11/2015				ILLINOIS FED. AID PROJECT							

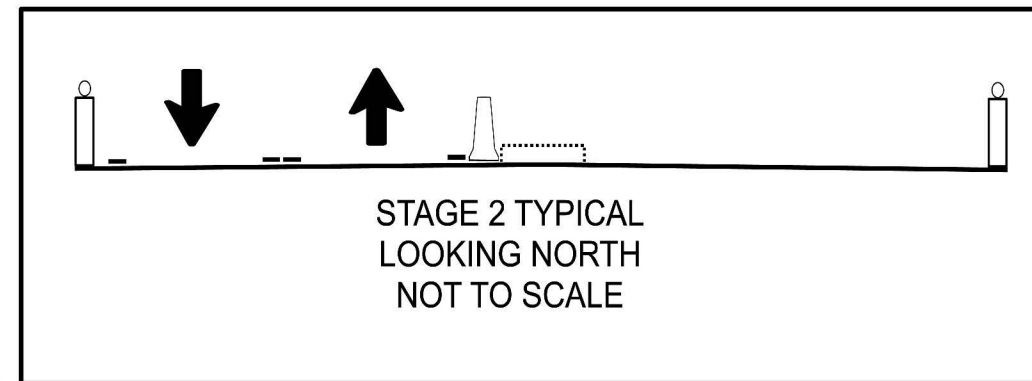
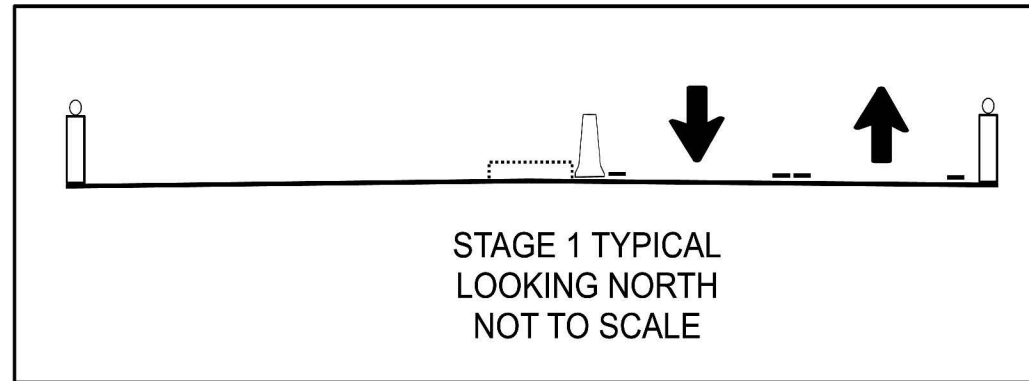
TRAFFIC MANAGEMENT PLAN

PINECREST DRIVE STAGE 2



FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC MANAGEMENT PLAN			F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SA\GEN\DRIFT\STD&PLNS\SQUAD 0\1-474 3R	Project\english files\Staging sheets for I-474	DRAWN structures.dgn	REVISED -					474	90-6,7RS-1	TAZEWELL	245	180
Default	PLOT SCALE = 100.000' / 1in.	CHECKED -	REVISED -		CONTRACT NO. 68606			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/11/2015	DATE -	REVISED -		SCALE:	SHEET	OF SHEETS	STA.	TO STA.			

**TRAFFIC MANAGEMENT PLAN
PINECREST DRIVE
GENERAL NOTES**



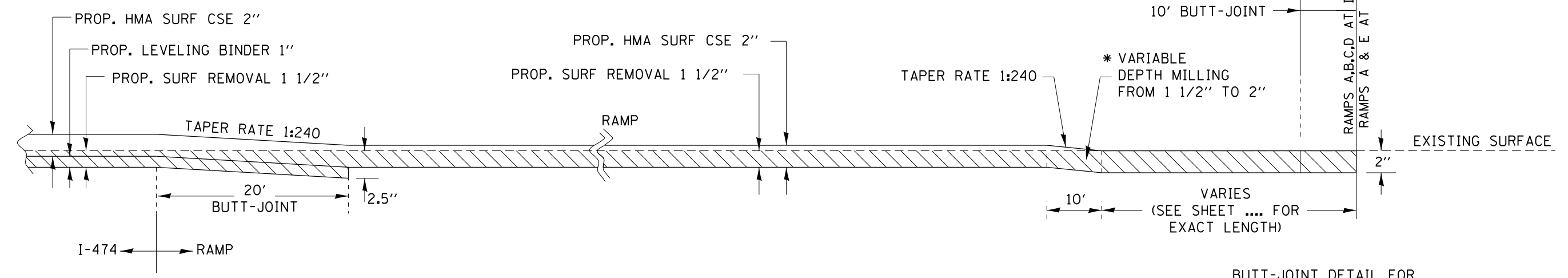
PAVEMENT MARKING REMOVAL	1,478	SQ. FT.
4" TYPE IV TAPE-WHITE	8,099	LIN. FT.
4" TYPE IV -YELLOW	11,936	LIN. FT.
TEST LEVEL III TEMP. IMPACT ATTENUATORS NON-REDIRECTIVE	1	EACH
TEST LEVEL III TEMP. IMPACT ATTENUATOR NARROW, REDIRECTIVE	1	EACH
RELOCATE-TEST LEVEL III TEMP. IMPACT ATTENUATOR NARROW, REDIRECTIVE	1	EACH
TEMPORARY CONCRETE BARRIER	1,037.5	FT.
RELOCATE TEMPORARY CONCRETE BARRIER	1,037.5	FT.
STD. 701602-STAGE 1 & 2 INCLUSIVE	1	LUMP SUM

GENERAL NOTES: PINECREST

STD. 701602 SHALL BE USED ALONG WITH DETAIL DRAWINGS AS SHOWN IN THE PLANS. ADDITIONAL SIGNAGE SHOWN SHALL BE INCIDENTAL TO STD. 701602
 PAVEMENT MARKING REMOVAL SHALL BE COMPLETED PRIOR TO STARTING STAGE 1 AND SHALL INCLUDE THE REMOVAL NECESSARY FOR STAGE 2.
 TYPE III BARRICADES (WITH LIGHTS AND ROAD CLOSED SIGNS) SHALL BE PLACED ON EACH END OF THE CLOSED SECTION. COST IS INCIDENTAL TO STD. 701602.
 IDOT STAFF WILL LAYOUT TRAFFIC CONTROL AND PAVEMENT MARKING REMOVAL LIMITS FOR STAGES 1 AND 2.
 ADJUSTMENTS MAY NEED TO BE MADE TO FIT ACTUAL FIELD CONDITIONS.

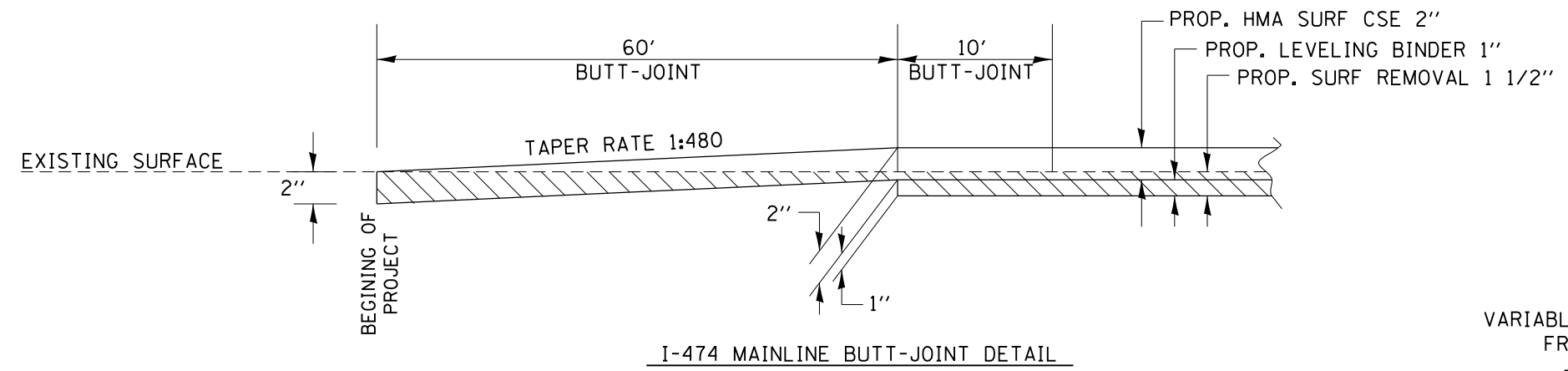
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S:\GEN\DRIFT\STD&PLNS\SQUAD 0\1-474 3R	Project\english files\Staging sheets for I-474	DRAWN structures.dgn	REVISED -					474	90-6,7RS-1	TAZEWELL	245	181
Default	PLOT SCALE = 100.000 / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 68606				
	PLOT DATE = 12/11/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

* (PAID FOR AS HMA SURF. REMOVAL 2")

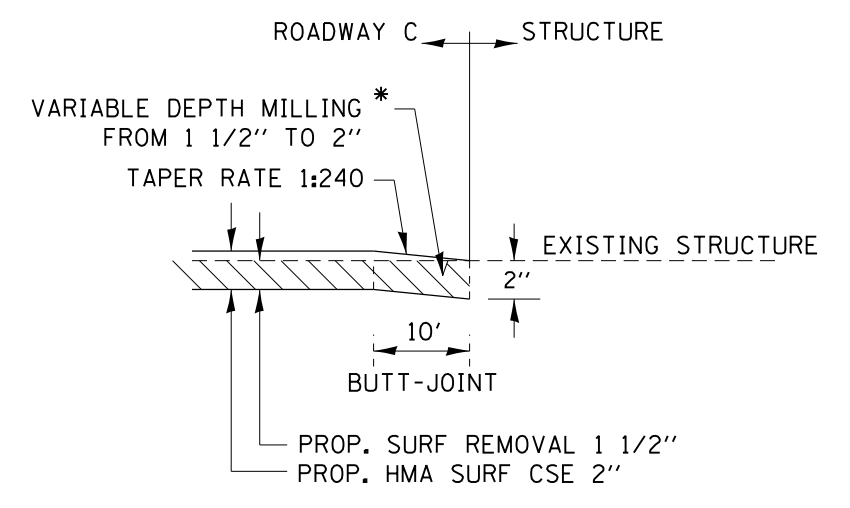


BUTT-JOINT DETAIL FOR I-474 TO IL-29 RAMPS, RAMP E, ROADWAY C

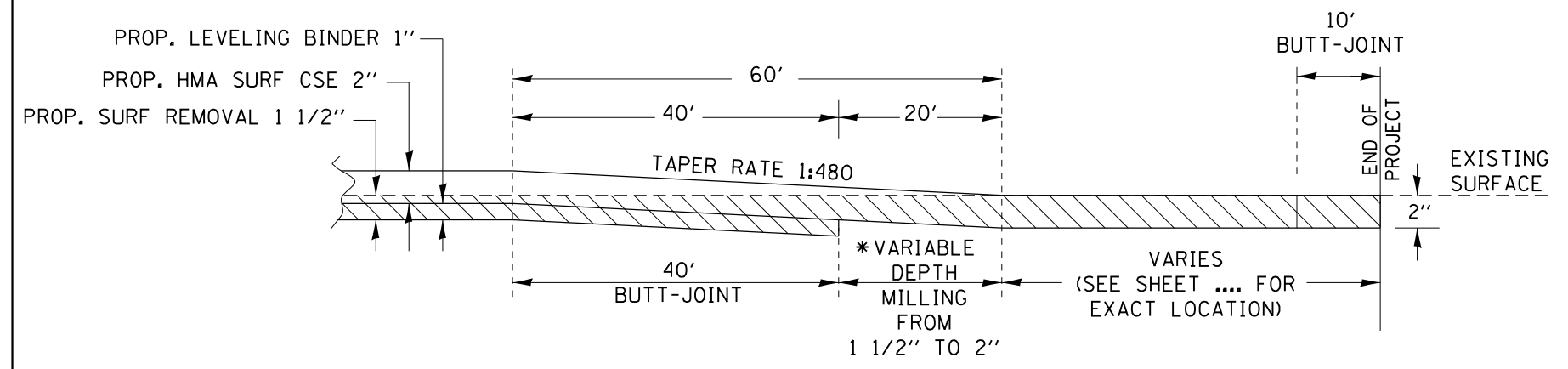
BUTT-JOINT DETAIL FOR RAMPS A,B,C,D AT IL 29 AND RAMPS A & E AT I-74



I-474 MAINLINE BUTT-JOINT DETAIL



BUTT-JOINT DETAIL FOR STRUCTURES ON ROADWAY C

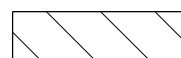
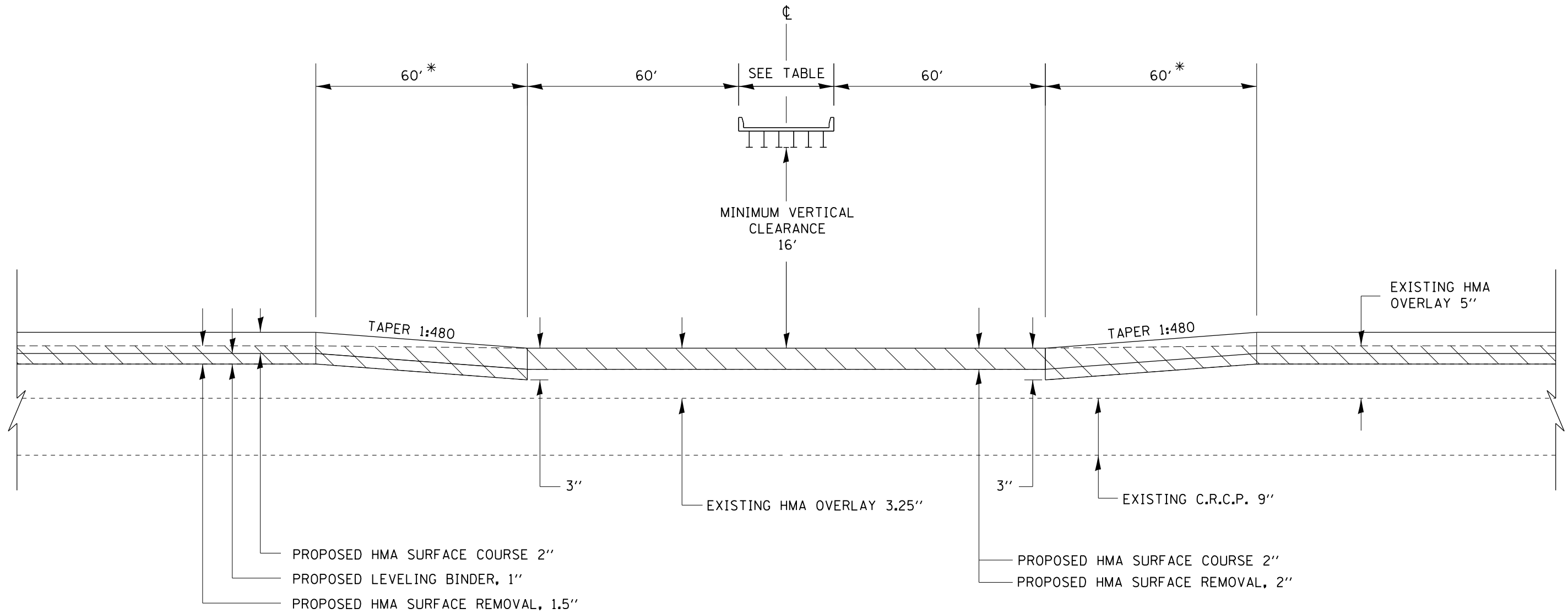


ROADWAY A & B (I-74) BUTT-JOINT DETAIL

NOT TO SCALE

FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS AND TAPER DETAIL			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S:\GEN\DRIFT\STD&PLNS\SQUAD 0\I-474 3R	Project\english Files\Cover-Line Diagram. Type	DRAWN -	REVISED -					474	90-6,7RS-1	TAZEWELL	245	182
Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -		CONTRACT NO. 68606							
	PLOT DATE = 12/11/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

STRUCTURE NUMBER/LOCATION	WIDTH (OUT TO OUT MEASUREMENT)
090-0105 / COLE STREET	45.5'
090-0104 / SPRINGFIELD ROAD	53.3'
090-0103 / PINECREST DRIVE	61'



PROPOSED HMA SURFACE REMOVAL

* PAY LIMITS FOR HMA SURFACE REMOVAL BUTT-JOINT

NOT TO SCALE

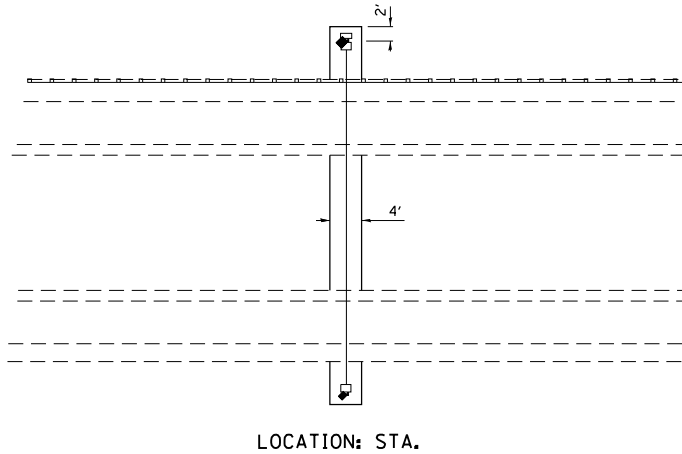
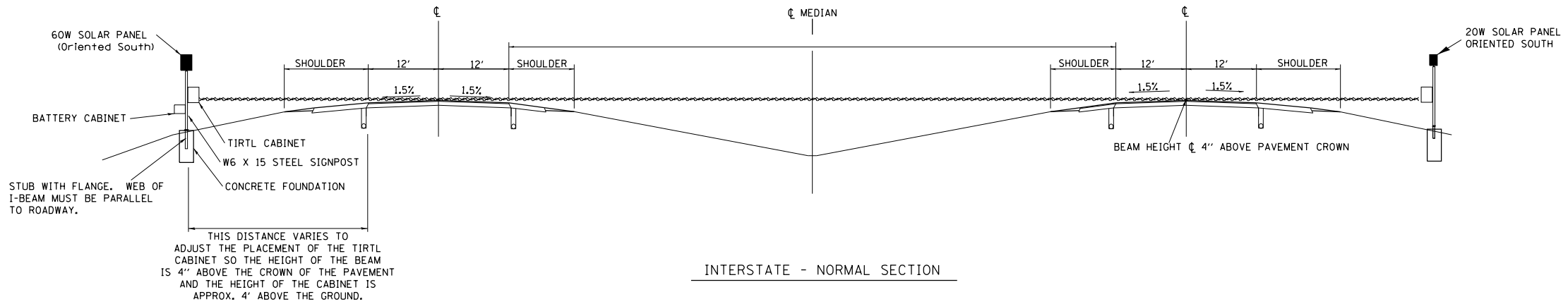
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Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINTS DETAIL UNDER STRUCTURES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	183
				CONTRACT NO. 68606
ILLINOIS FED. AID PROJECT				

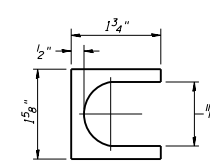
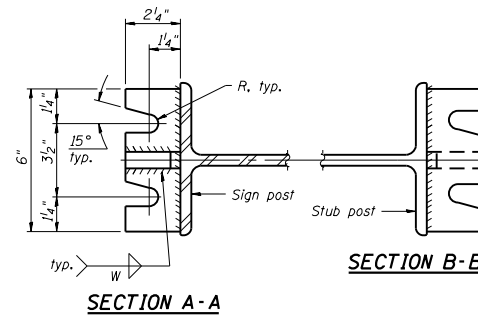
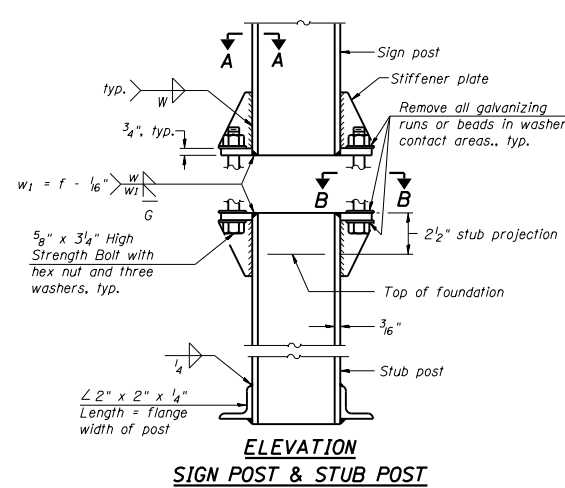
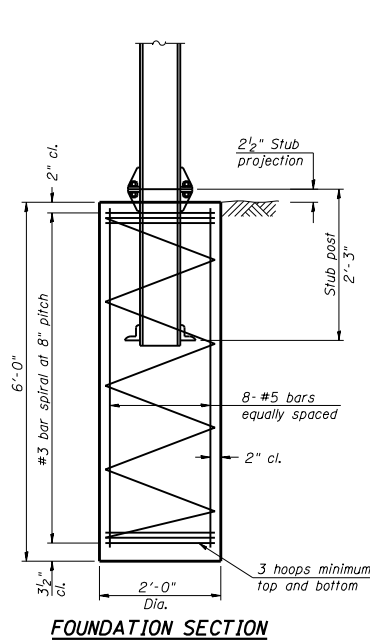


NOTES:

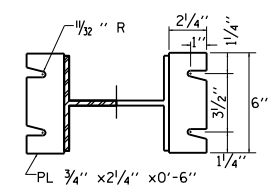
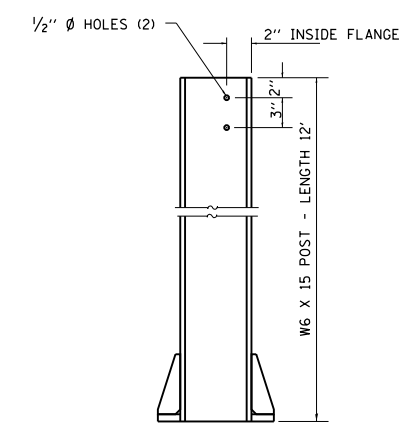
1. SYSTEM CONSISTS OF TWO W6 X 15 I-BEAM SIGNPOSTS WITH CONCRETE FOUNDATION AND A FLANGE WITH BREAKAWAY BOLTS
2. THE I-BEAMS ARE 12' LONG AND DRILLED AS PER THE I-BEAM DETAIL TO ACCOMODATE A PIPE TO ALLOW THE MOUNTING OF THE SOLAR PANEL.
3. THE WEB OF THE I-BEAM IS TO BE PLACED PARALLEL TO THE LANES.
4. THE TWO POSTS MUST BE PLACED DIRECTLY ACROSS FROM EACH OTHER AND PERPENDICULAR TO THE LANES.
5. TWO POSTS ARE REQUIRED LOCATED AS PER THE DRAWING.
6. THE CABINETS ARE ATTACHED TO THE I-BEAM USING 8" GALVANIZED J-BOLTS.
7. THE BOTTOM OF THE CABINET HEIGHT MUST BE NO LOWER THAN 4' ABOVE THE GROUND AND BE ABLE TO VIEW ACROSS ALL LANES AT THE CROWN OF THE PAVEMENT.
8. THE IDOT OFFICE OF PLANNING AND PROGRAMMING DATA MANAGEMENT LAB (RAMON TAYLOR or RICH MARX 217-782-2065) SHALL BE NOTIFIED TWO WEEKS PRIOR TO THE LAYOUT AND SHALL BE PRESENT DURING THE PLACEMENT OF THE POST FOUNDATIONS.

**TIRTL TRAFFIC COUNTER
SCHEDULE OF QUANTITIES
(FOR INFORMATION ONLY)**

CODE NUMBER	ITEM	UNIT	QUANTITY
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	520
73400100	CONCRETE FOUNDATIONS	CU YD	1.4
X0323388	TRAFFIC COUNTER	EACH	1
TIRTL TRAFFIC DATA COLLECTION SYSTEM INCLUDES:			
	Sierra Wireless LS300 EVDO Rev. A VZW	EACH	1
	PHANTOM DUAL BAND ANTENNA P/N RF-ART183055P/281	EACH	1
	TIRTL CABINET 15" X 27" X 16"	EACH	2
	BATTERY CABINET 16.5" X 16" X 11.5" BBA1M w/ #2 Police Lock	EACH	1
	SOLAR PANEL 60 W 12 VDC	EACH	1
	SOLAR PANEL 20 W 12 VDC	EACH	1
	SOLAR CHARGE REGULATOR 6 AMP 12 VDC	EACH	2
	BATTERY - 33 AH ABSORBED ELECTROLYTE 8" X 5.5" X 7" (DEEP CYCLE)	EACH	1
	BATTERY - 80 AH ABSORBED ELECTROLYTE 10" X 6" X 8" (DEEP CYCLE)	EACH	1



SHIM DETAIL
Furnish two 0.01" thick and two 0.03" thick stainless steel or brass (ASTM B36) shims per post.



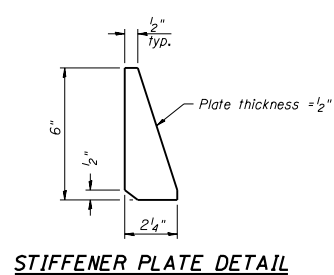
GENERAL NOTES

Posts shall be plumbed by using shims with post-to-stub post connection bolts snug tight only. Final tightening of all High Strength Bolts shall be in accordance with Article 727.05 and threads at the junction of the bolt and nut shall be burred or center punched to prevent the nut from loosening.

LOADING: 80 m.p.h. wind with 30% gust factor, normal to sign.

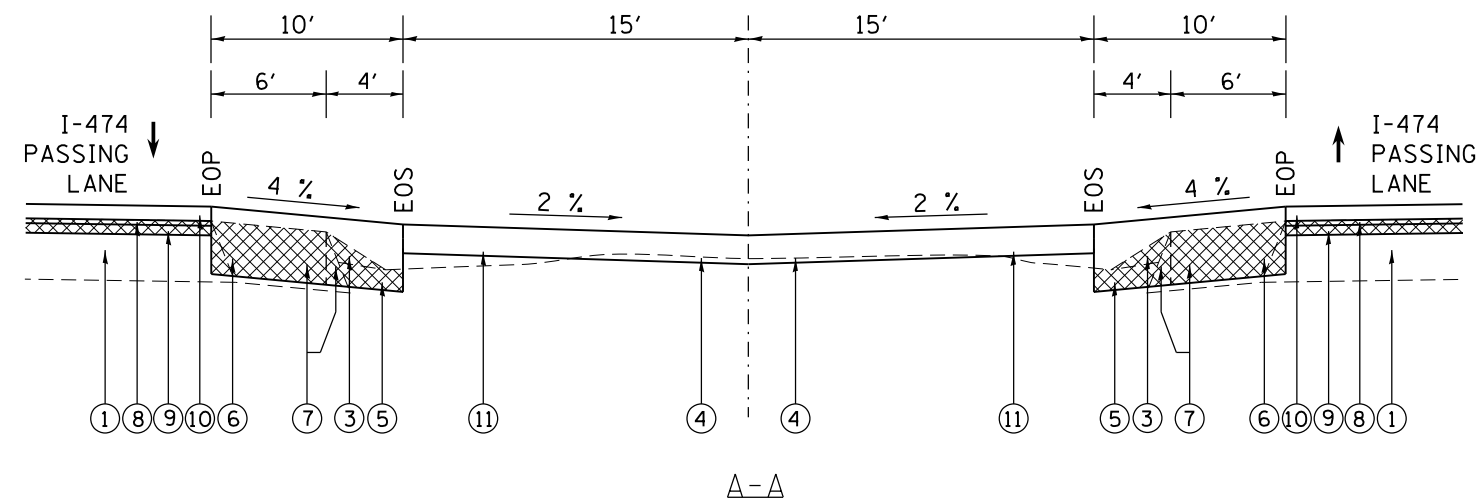
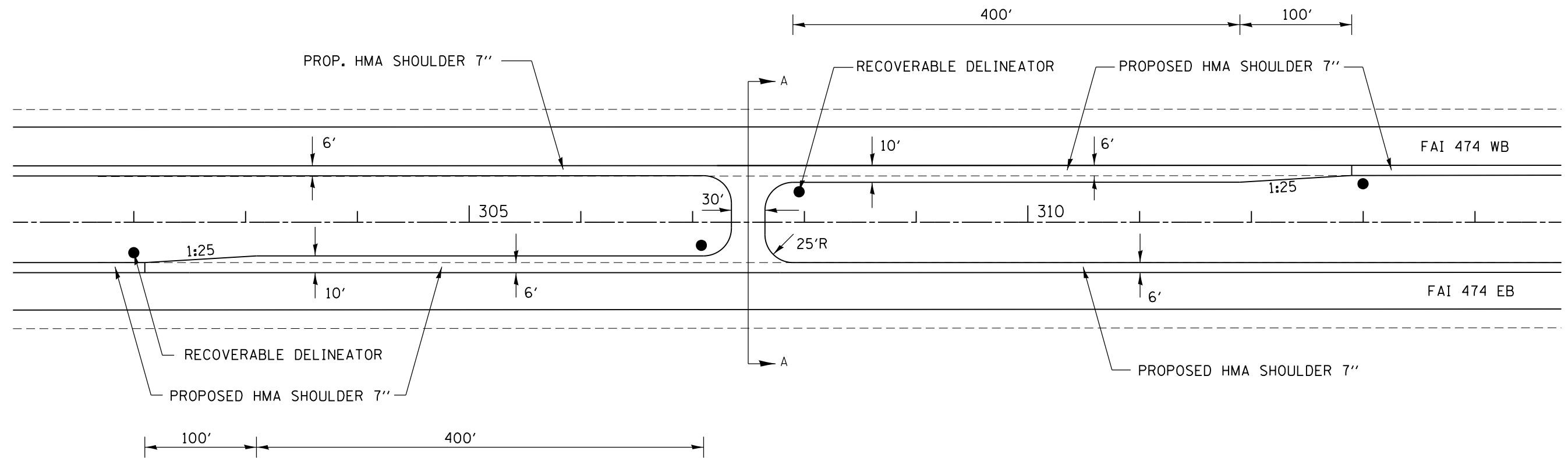
DESIGN STRESSES:
Structural steel - 20,000 p.s.i.
Reinforcing steel - 20,000 p.s.i.
Concrete - 1,400 p.s.i.
Footing soil pressure - 2,000 p.s.f.

After fabrication, the post and upper 6", min. of the stub post shall be hot-dip galvanized in accordance with AASHTO M111. All bolts, nuts and washers shall be hot-dip galvanized in accordance with AASHTO M232.



BREAK-AWAY WIDE FLANGE STEEL POST DETAILS

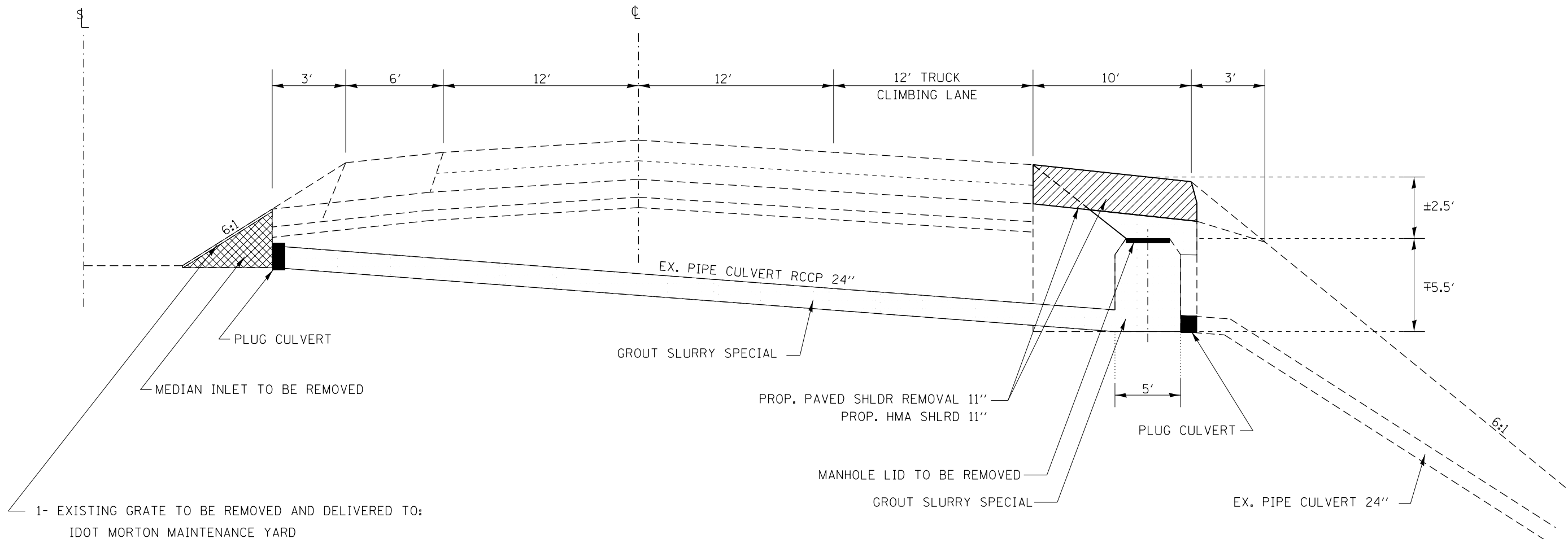
FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC COUNTER DETAIL	F.A.I R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -			CONTRACT NO. 68606					
	PLOT DATE = 12/11/2015	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



- ① EX. 9" CRC PCC
- ② EX. BAM SHOULDER
- ③ EX. AGG. SHOULDER
- ④ EX. AGG SURFACE 10"
- ⑤ PROP. EXCAVATING AND GRADING EXISTING SHOULDERS
- ⑥ PROP. HMA SHOULDER REMOVAL 5.5"
- ⑦ PROP. HMA SHOULDER 7"
- ⑧ PROPOSED SURFACE REMOVAL 1.5"
- ⑨ PROP. POLY LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 1"
- ⑩ PROP. POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 2"
- ⑪ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 3" (NOMINAL)

NOT TO SCALE

FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MEDIAN CROSSOVER DETAIL AT STA. 307+62	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -			CONTRACT NO. 68606					
	PLOT DATE = 12/11/2015	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

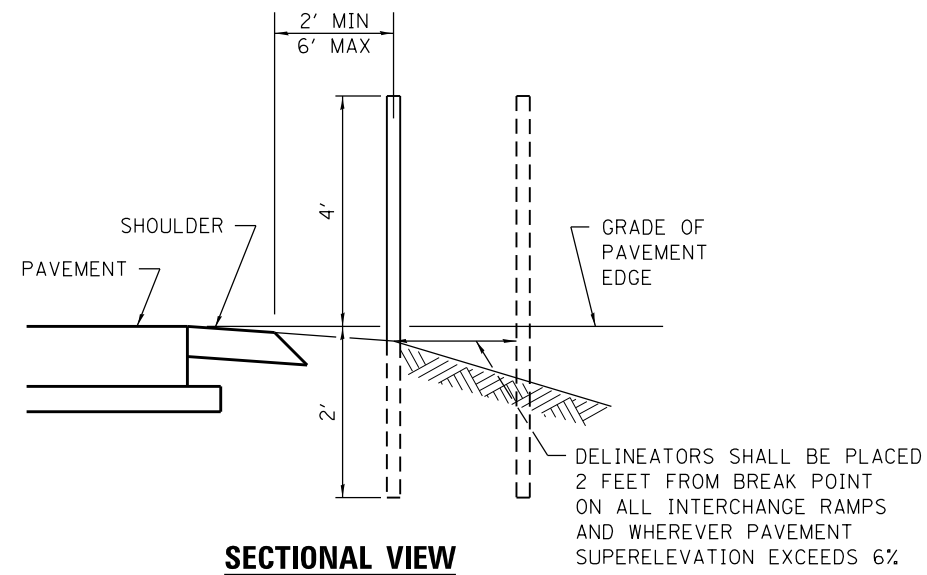


- 1- EXISTING GRATE TO BE REMOVED AND DELIVERED TO:
 IDOT MORTON MAINTENANCE YARD
 1971 N. MORTON AVE.
 MORTON IL 61550
 CONTACT DALE HILES AT 309-266-9441 FOR DELIVERY ARRANGEMENT
- 2- GRATE DELIVERY SHALL BE INCLUDED IN THE CONTRACT BID PRICE FOR "REMOVING INLETS"
- 3- GRADING AND SHAPING AFTER INLET REMOVAL SHALL BE INCLUDED IN THE CONTRACT BID PRICE FOR "REMOVING INLETS"
- 4- REMOVAL OF THE EXISTING MANHOLE LID (COVER) SHALL BE INCLUDED IN THE CONTRACT BID PRICE FOR "EARTH EXCAVATION"
- 5- ALL CONSTRUCTION ACTIVITIES ASSOCIATED WITH FILLING THE EXISTING MANHOLE SHALL BE DONE WHEN THE DRIVING LANE AND TRUCK CLIMBING LANE ARE CLOSED

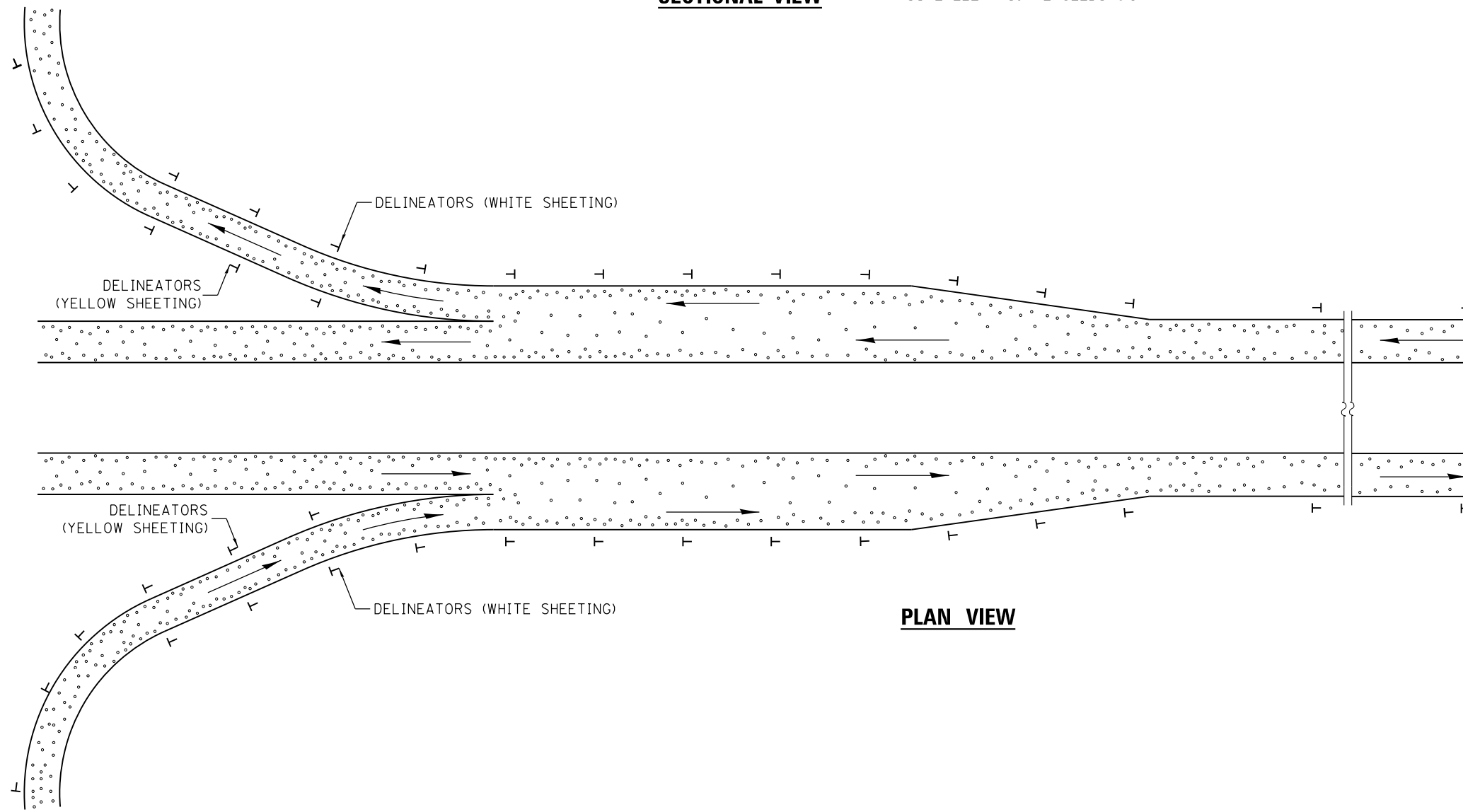
NOT TO SCALE

FILE NAME =	USER NAME = labobidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHOULDER SETTLEMENT TREATMENT DETAIL STA. 171 + 00				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S:\GEN\DRIFT\STD&PLNS\SQUAD 8\1-474 3R	Project\english files\Cover-Line Diagram. Type	DRAWN -	REVISED -						474	90-6,7RS-1	TAZEWELL	245	186
Default	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -		CONTRACT NO. 68606				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/11/2015	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		

**SPACING FOR
RECOVERABLE DELINEATORS
ON HORIZONTAL CURVES**



SECTIONAL VIEW



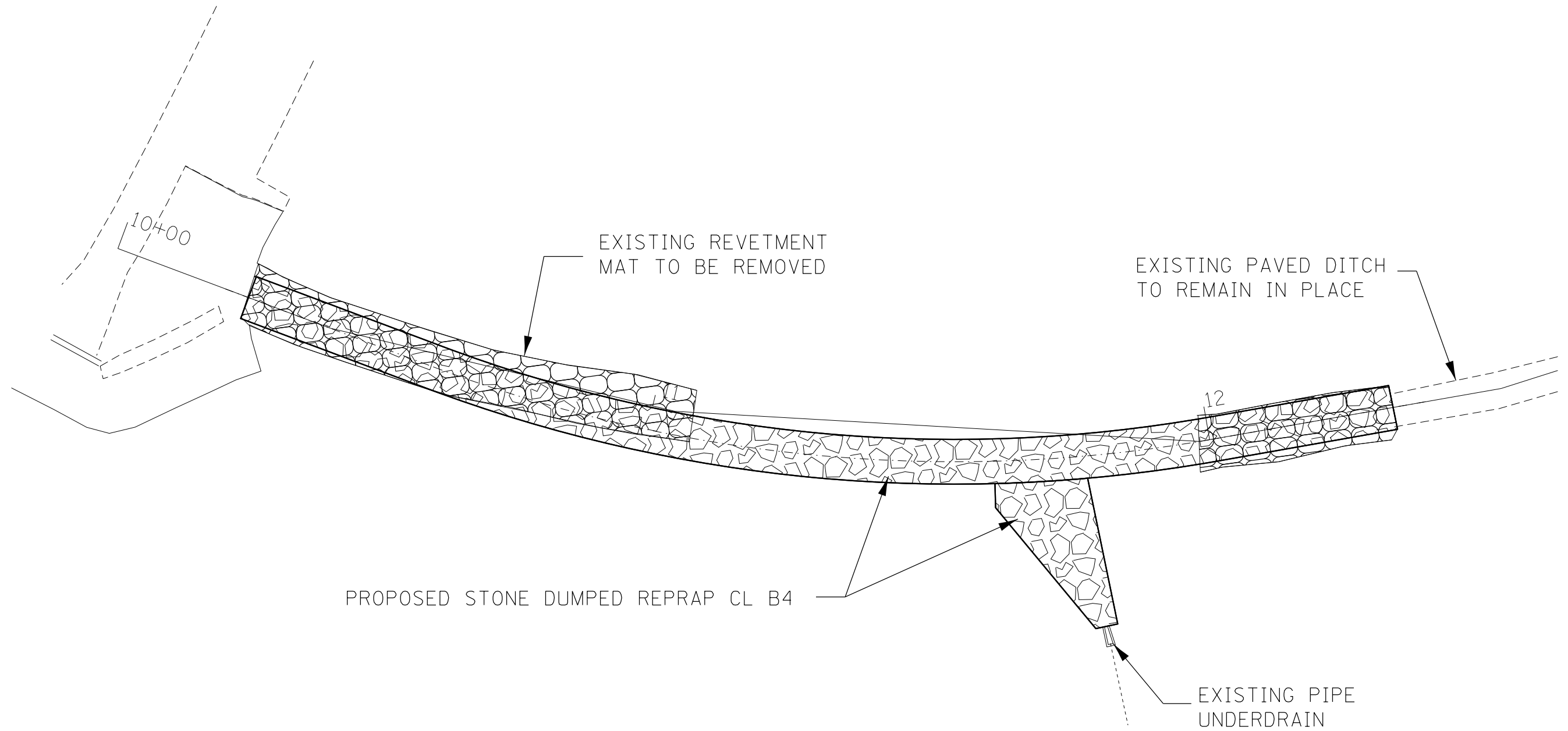
PLAN VIEW

LOCATION	RADIUS OF CURVE	SPACING ON CURVE	
		FT	FT
I-474 MAINLINE			
	CURVE 474P2-1	3,657	175
I-474 / IL 29 INTERCHANGE			
RAMP A			
	CURVE SBI24A-1	706	75
	CURVE SBI24A-2	764	80
RAMP B			
	CURVE SBI24B-1	690	75
	CURVE SBI24B-2	580	70
RAMP C			
	CURVE SBI24C-1	690	75
	CURVE SBI24C-2	3,601	175
RAMP D			
	CURVE SBI24D-1	3,706	175
	CURVE SBI24D-2	690	75
	CURVE SBI24D-3	516	65
RAMP E			
	CURVE SBI24E-1	430	55
	CURVE SBI24E-2	230	40
	CURVE SBI24E-3	3,601	175
I-474 / I-74 INTERCHANGE			
ROADWAY A			
	CURVE SBRDWYA-1	3,820	175
ROADWAY B			
	CURVE SBRDWYB-2	3,820	175
ROADWAY C			
	CURVE SBRDWYC-1	3,581	175
RAMP A			
	CURVE SBRAMPA1-1	642	70
	CURVE SBRAMPA2-1	263	40
	CURVE SBRAMPA2-2	263	40
	CURVE SBRAMPA2-3	477	65
RAMP E			
	CURVE SBRAMPE-1	674	75
	CURVE SBRAMPE-2	1,451	125
	CURVE SBRAMPE-3	764	80

GENERAL NOTES

DELINEATORS ON MAINLINE SHALL BE PLACED AT 400' SPACING ON TANGENT SECTIONS, AND 200' SPACING ON CURVED SECTIONS.

- DELINEATORS ON RAMP, SHALL BE PLACED AS FOLLOWS:
1. ON TANGENT SECTIONS, ACCELERATION LANES AND DECELERATION LANES AT MAXIMUM SPACING OF 100'.
 2. ON CURVED SECTIONS.
- (SEE TABLE ABOVE FOR SPACING)



FILE NAME =	USER NAME = labbidism	DESIGNED -	REVISED -
S:\GEN\DRAW\STD&PLNS\SQUAD 0\1-474 3R	Project\english files\revetment mat survey	DRAWN -	REVISED -
Default	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

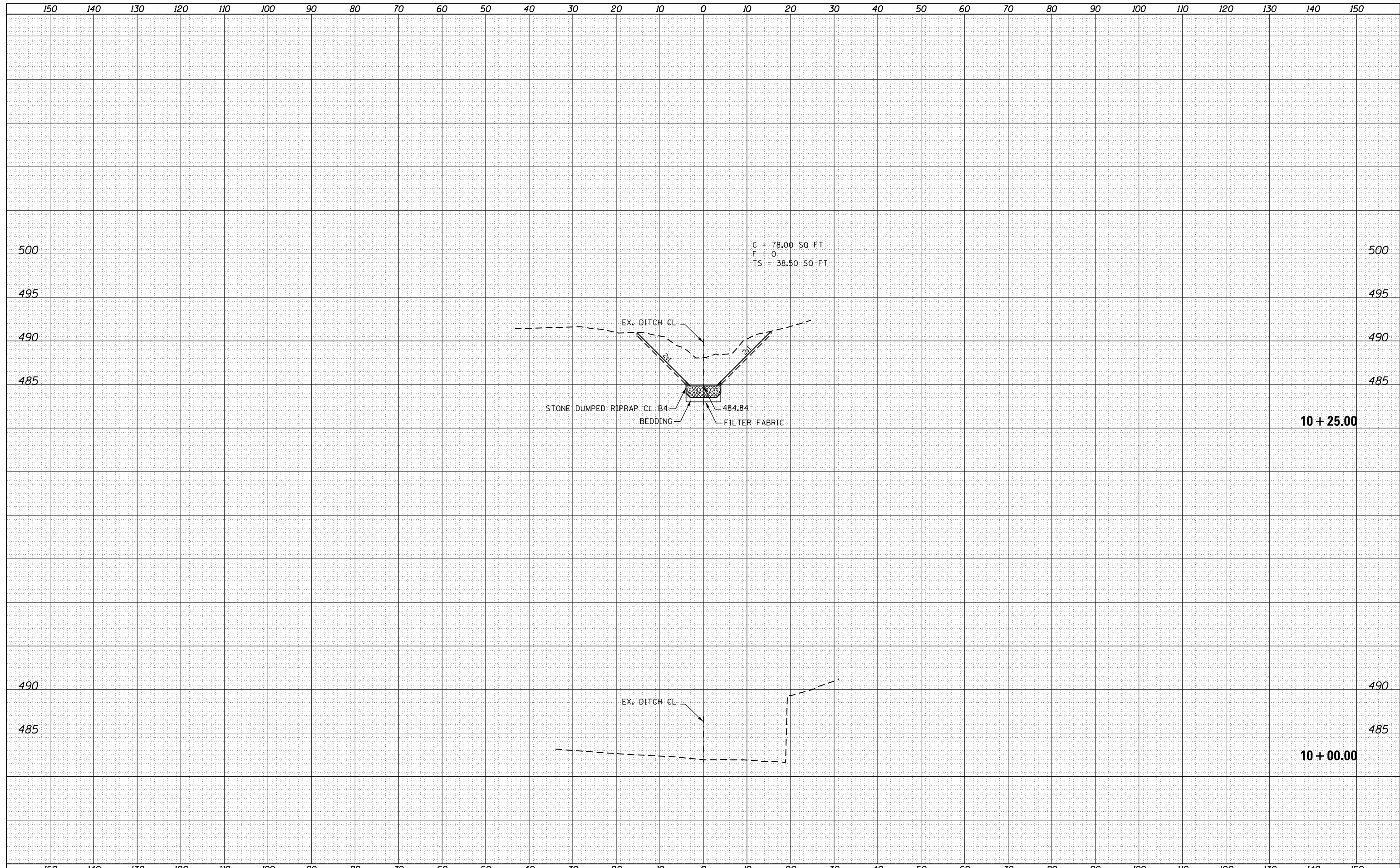
**DITCH REPAIR DETAIL
I-474 / IL29 RAMP A**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	145	188
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68606	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

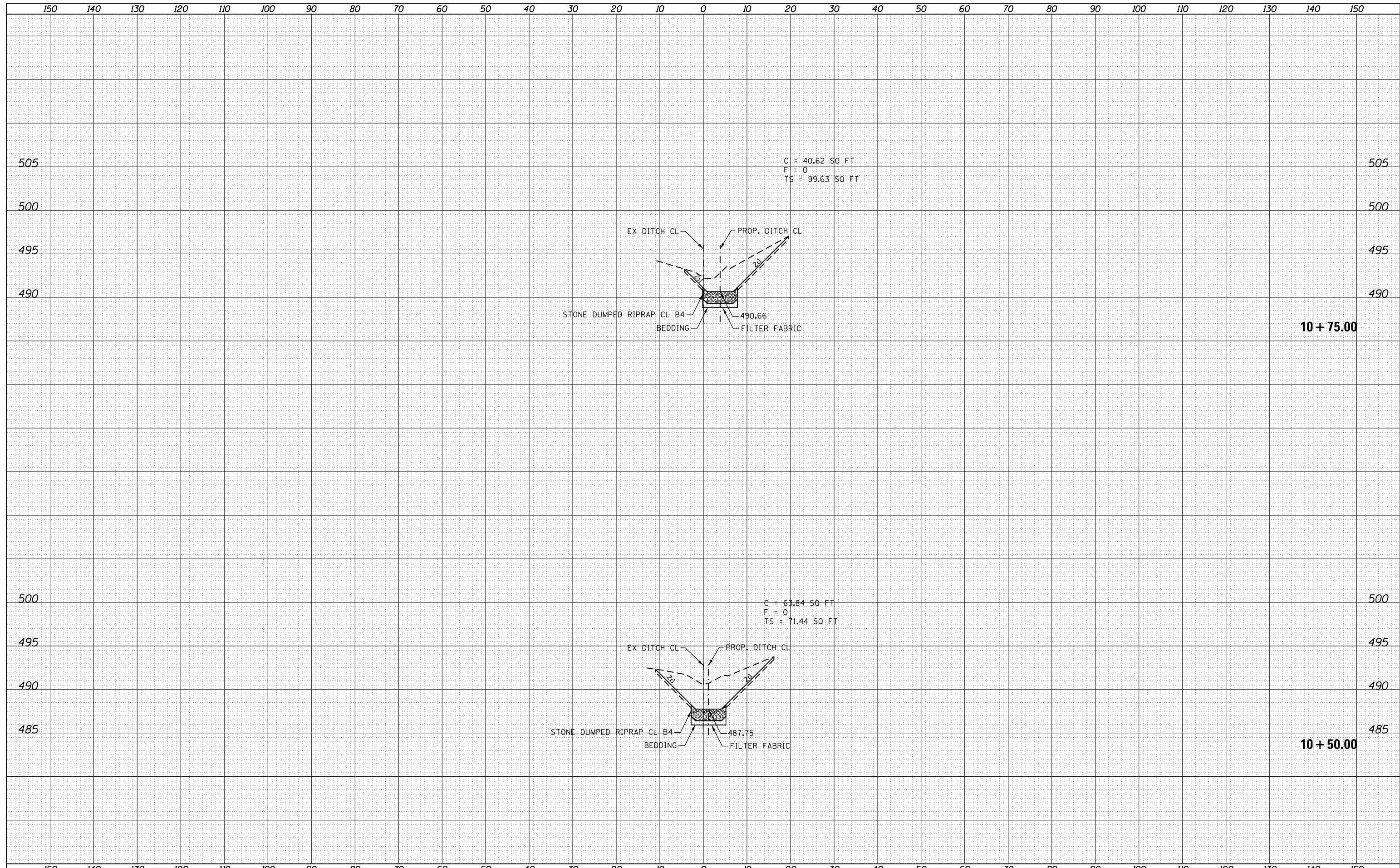
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BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



FILE NAME =	USER NAME = labobidism	DESIGNED -	REVISIED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP A AT I-474 & IL29 INTERCHANGE DITCH REPAIR CROSS SECTION	F.A.I	SECTION	COUNTY	TOTAL	SHEET
S:\GEN\DRAW\STD&PLNS\SQUAD 8\1-474 3R Project\English files\revetment mat survey from Key\DRAW\Sheet.dgn	DESIGNED -	REVISIED -	474			90-6,7RS-1	TAZEWELL	245	189	
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISIED -	CONTRACT NO. 68606							
Default	DATE = 12/11/2015	DATE -	SCALE:			SHEET	OF	SHEETS	STA. 10+00.00	TO STA. 10+25.00

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

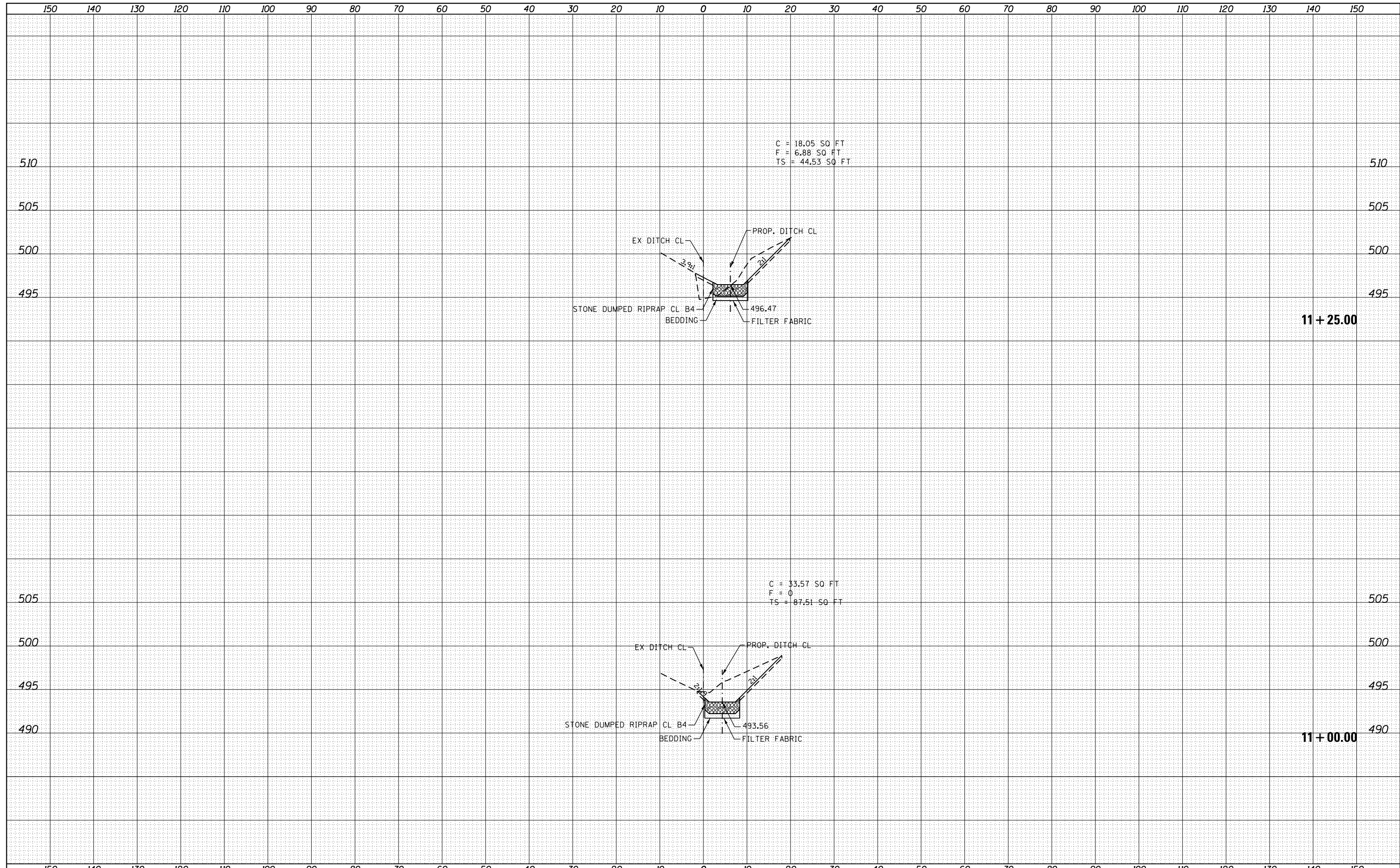
DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED



FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISIED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP A AT I-474 & IL29 INTERCHANGE DITCH REPAIR CROSS SECTION	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S:\GEN\DRIFT\STD&PLNS\SQUAD 8\1-474 3R Project\English files\revetment mat survey from Key\DRAW\Sheet.dgn	DESIGNED -	REVISIED -	474			90-6,7RS-1	TAZEWELL	245	190	
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISIED -	CONTRACT NO. 68606							
Default	DATE = 12/11/2015	DATE -	REVISIED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



C = 18.05 SQ. FT
 F = 6.88 SQ. FT
 TS = 44.53 SQ. FT

C = 33.57 SQ. FT
 F = 0
 TS = 87.51 SQ. FT

FILE NAME =	USER NAME = labobidism	DESIGNED -	REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

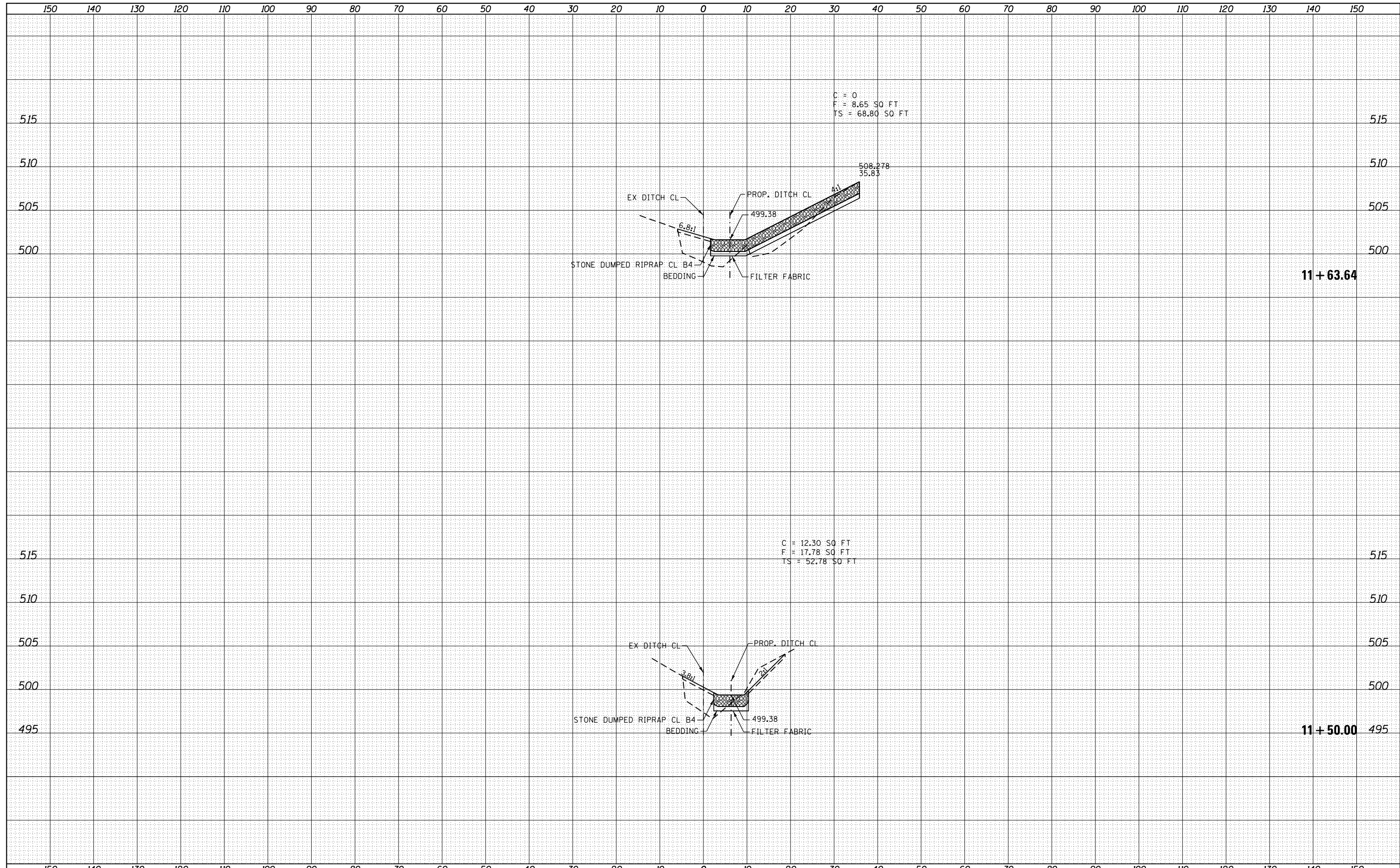
**RAMP A AT I-474 & IL29 INTERCHANGE
 DITCH REPAIR CROSS SECTION**

SCALE: SHEET OF SHEETS STA. 11+00.00 TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	191
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISIED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP A AT I-474 & IL29 INTERCHANGE DITCH REPAIR CROSS SECTION	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S:\GEN\DRAW\STD&PLNS\SQUAD B\1-474 3R Project\English files\revetment mat survey from Key\DRAW\Sheet.dgn	DESIGNED BY	REVISIED BY	474			90-6,7RS-1	TAZEWELL	245	192	
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Default	DATE = 12/11/2015	REVISIED -	SCALE:			SHEET	OF	SHEETS	STA. 11+50.00	TO STA.

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

DATE	
BY	
FINAL SURVEY NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	

510

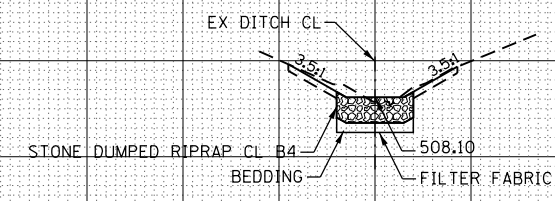
DATE	
BY	
ORIGINAL SURVEY NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	

515

510

505

C = 5.76 SQ FT
 F = 0
 TS = 16.78 SQ FT



12 + 25.00

515

510

505

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

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	PLOT DATE = 12/11/2015	DATE -	REVISOR -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**RAMP A AT I-474 & IL29 INTERCHANGE
 DITCH REPAIR CROSS SECTION**

SCALE: SHEET OF SHEETS STA. 12+25.00 TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	194
			CONTRACT NO. 68606	
ILLINOIS FED. AID PROJECT				

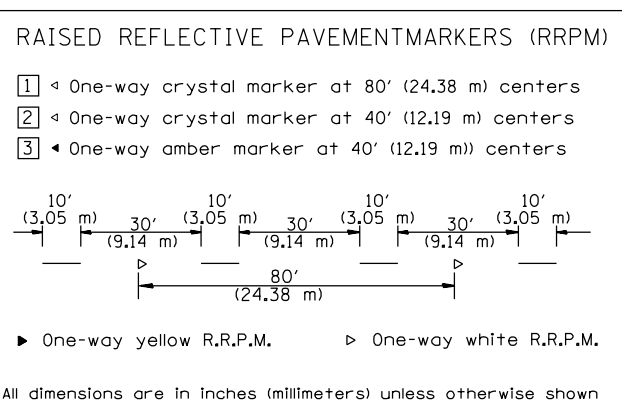
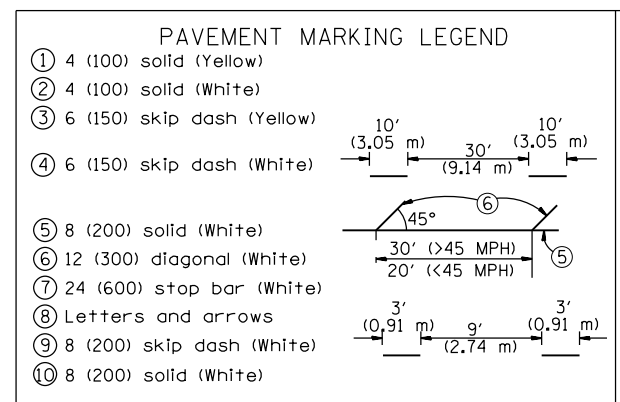
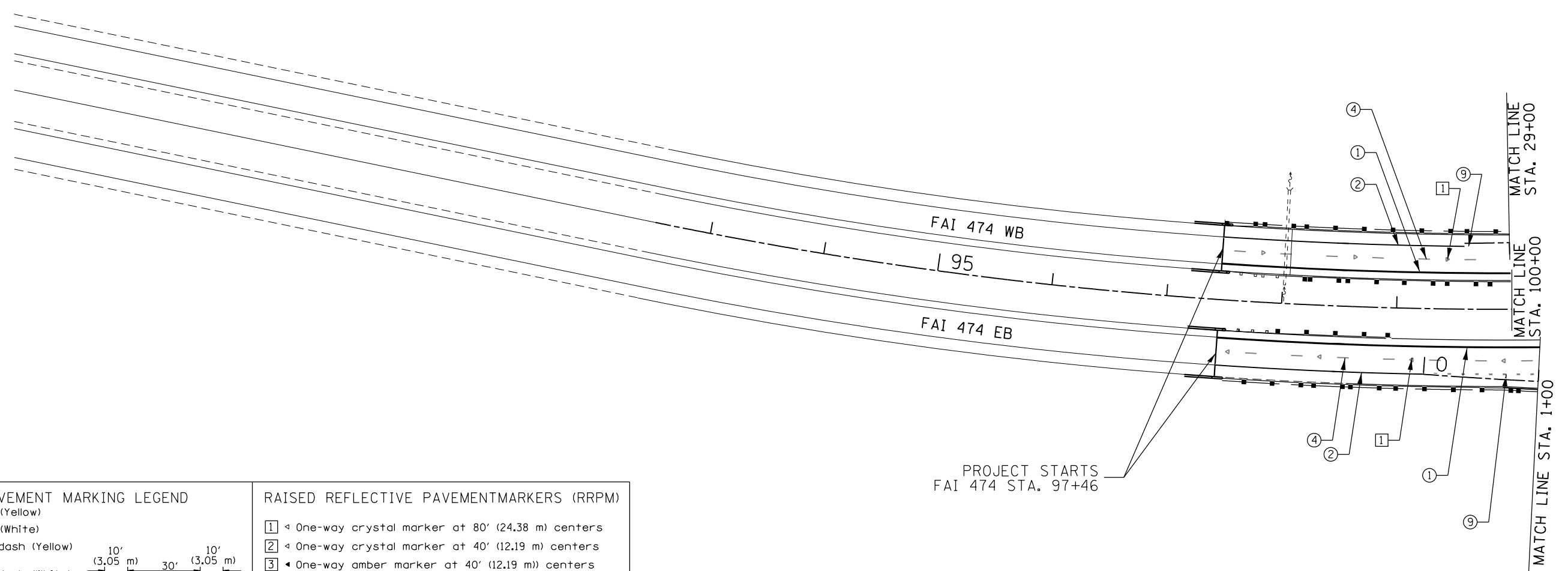
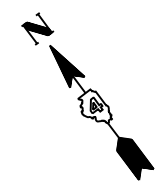


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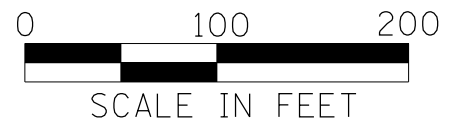
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ENTRANCE DETAIL AT STA. 199 + 00			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAEWELL	245	195
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



PROJECT STARTS
FAI 474 STA. 97+46

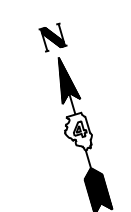
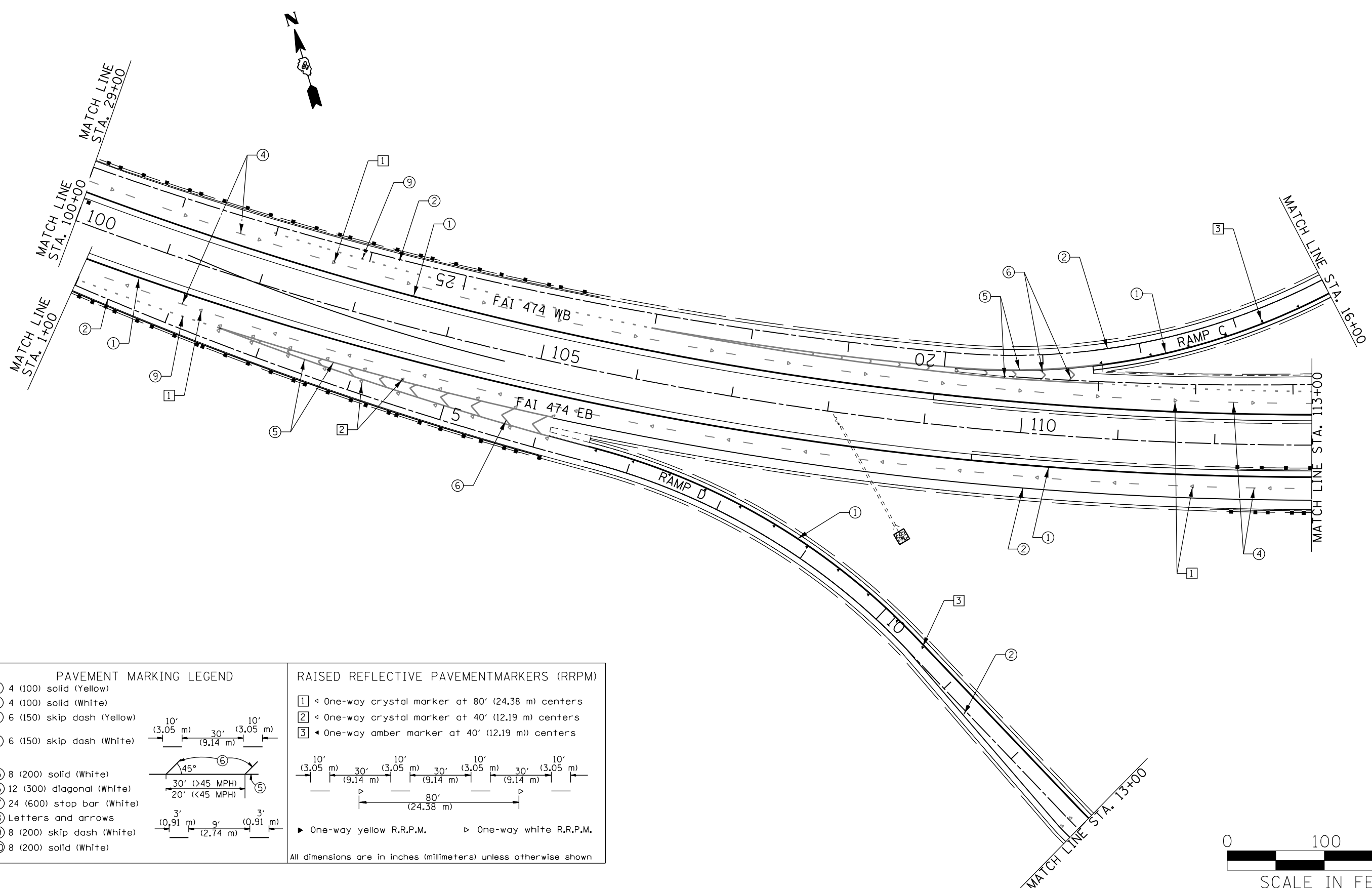


FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -
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Default	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKINGS DETAILS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	196
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



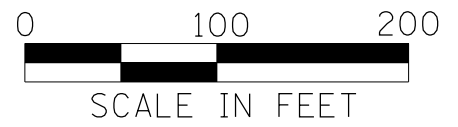
PAVEMENT MARKING LEGEND

① 4 (100) solid (Yellow)	
② 4 (100) solid (White)	
③ 6 (150) skip dash (Yellow)	
④ 6 (150) skip dash (White)	
⑤ 8 (200) solid (White)	
⑥ 12 (300) diagonal (White)	
⑦ 24 (600) stop bar (White)	
⑧ Letters and arrows	
⑨ 8 (200) skip dash (White)	
⑩ 8 (200) solid (White)	

RAISED REFLECTIVE PAVEMENT MARKERS (RRPM)

① ◀ One-way crystal marker at 80' (24.38 m) centers	
② ◀ One-way crystal marker at 40' (12.19 m) centers	
③ ◀ One-way amber marker at 40' (12.19 m) centers	
▶ One-way yellow R.R.P.M.	▷ One-way white R.R.P.M.

All dimensions are in Inches (millimeters) unless otherwise shown



FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -
S:\GEN\DRIFT\STD&PLNS\SQUAD 8\1-474 3R	Project\english files\Pavement marking Detail\DRAWN.dgn	CHECKED -	REVISED -
Default	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKINGS DETAILS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	197
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				

PAVEMENT MARKING LEGEND

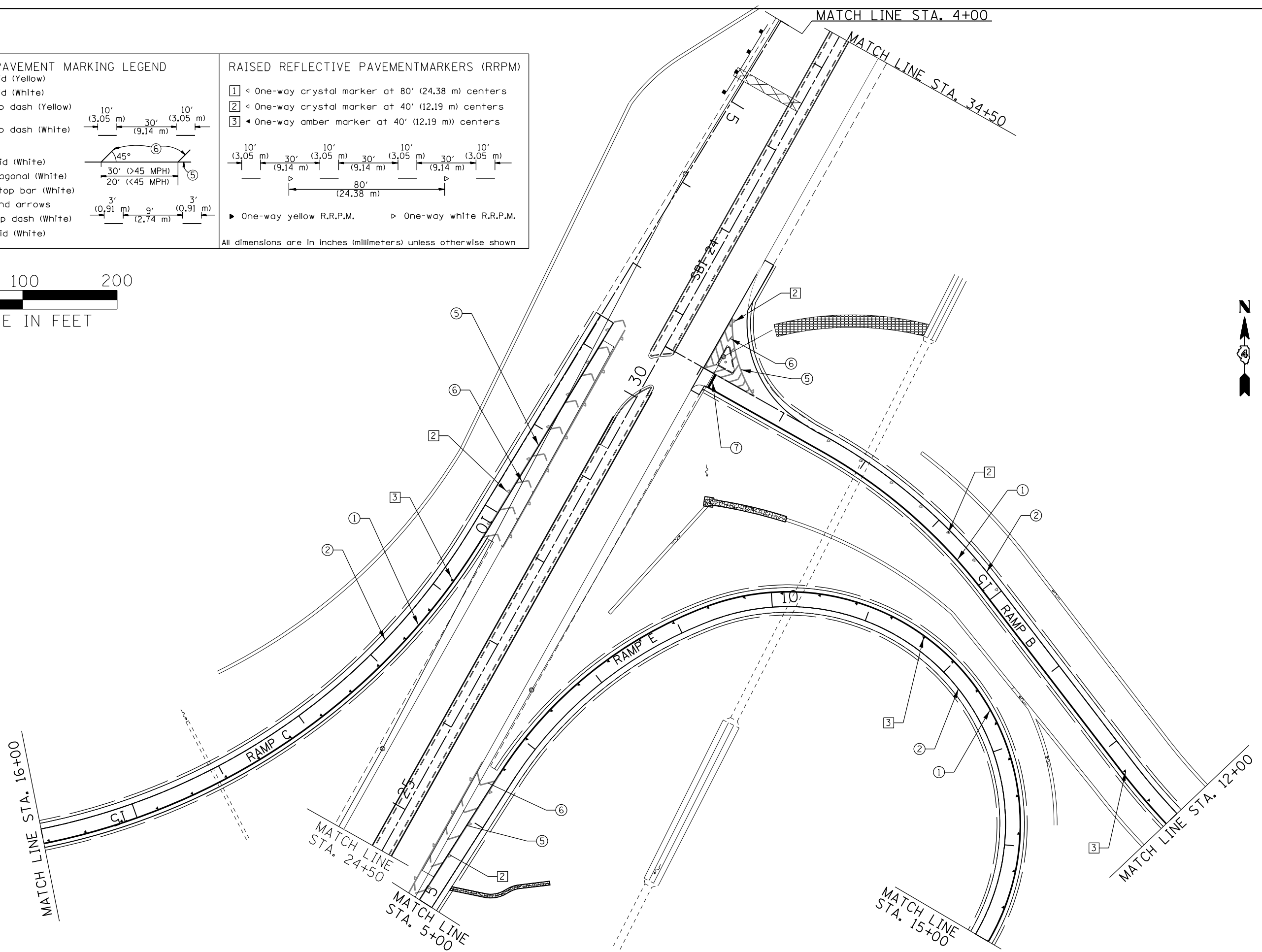
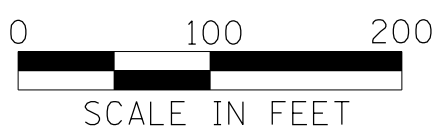
① 4 (100) solid (Yellow)	
② 4 (100) solid (White)	
③ 6 (150) skip dash (Yellow)	
④ 6 (150) skip dash (White)	
⑤ 8 (200) solid (White)	
⑥ 12 (300) diagonal (White)	
⑦ 24 (600) stop bar (White)	
⑧ Letters and arrows	
⑨ 8 (200) skip dash (White)	
⑩ 8 (200) solid (White)	

RAISED REFLECTIVE PAVEMENT MARKERS (RRPM)

① ◁ One-way crystal marker at 80' (24.38 m) centers	
② ◁ One-way crystal marker at 40' (12.19 m) centers	
③ ◁ One-way amber marker at 40' (12.19 m) centers	

▶ One-way yellow R.R.P.M. ▷ One-way white R.R.P.M.

All dimensions are in inches (millimeters) unless otherwise shown

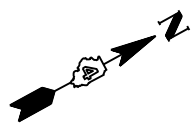


FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -
S:\GEN\DRIFT\STD&PLNS\SQUAD 8\1-474 3R	Project\english files\Pavement marking Details\DRAWN.dgn	CHECKED -	REVISED -
Default	PLOT DATE = 12/11/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKINGS DETAILS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	90-6,7RS-1	TAZEWELL	245	198
CONTRACT NO. 68606				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

① 4 (100) solid (Yellow)	
② 4 (100) solid (White)	
③ 6 (150) skip dash (Yellow)	
④ 6 (150) skip dash (White)	
⑤ 8 (200) solid (White)	
⑥ 12 (300) diagonal (White)	
⑦ 24 (600) stop bar (White)	
⑧ Letters and arrows	
⑨ 8 (200) skip dash (White)	
⑩ 8 (200) solid (White)	

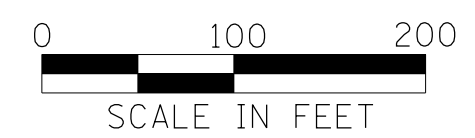
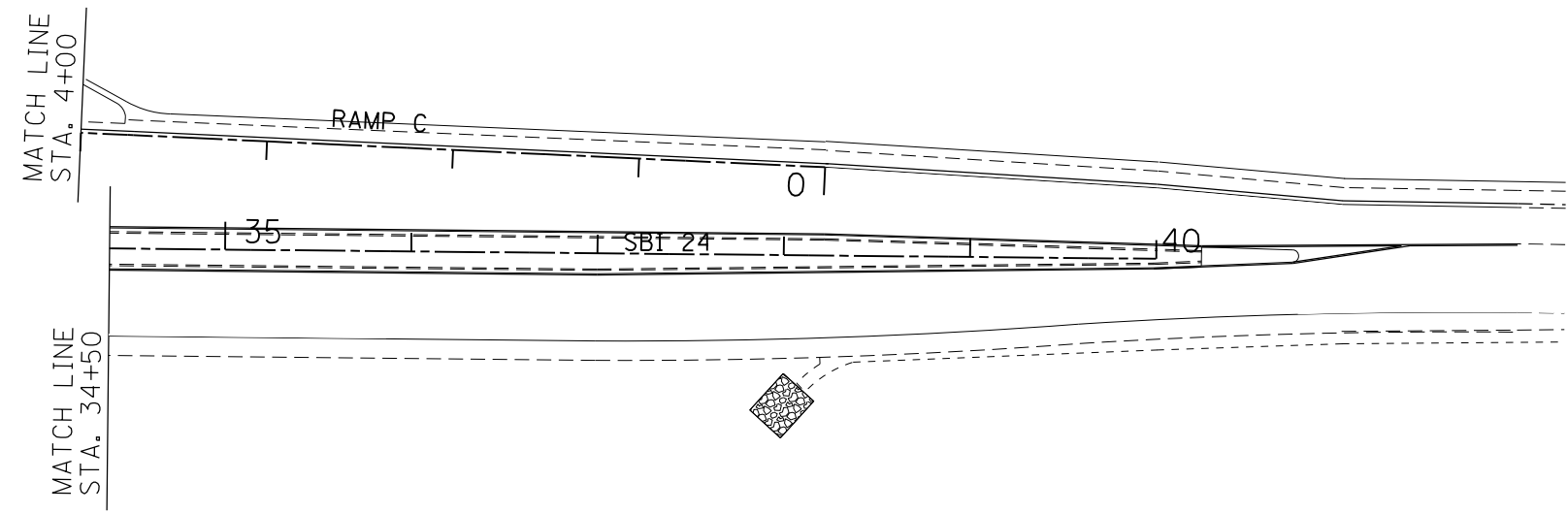
RAISED REFLECTIVE PAVEMENT MARKERS (RRPM)

① ◁ One-way crystal marker at 80' (24.38 m) centers	
② ◁ One-way crystal marker at 40' (12.19 m) centers	
③ ◁ One-way amber marker at 40' (12.19 m) centers	

80' (24.38 m)	

▶ One-way yellow R.R.P.M.	▷ One-way white R.R.P.M.
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All dimensions are in inches (millimeters) unless otherwise shown



FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKINGS DETAILS				F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
Default	Project\english files\Pavement marking Details\DRAWN.dgn	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	474	90-6,7RS-1	TAZEWELL	245	199
	PLOT SCALE = 100.000' / in.	DATE -	REVISED -						CONTRACT NO. 68606							
	PLOT DATE = 12/11/2015								ILLINOIS FED. AID PROJECT							

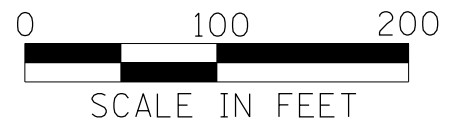
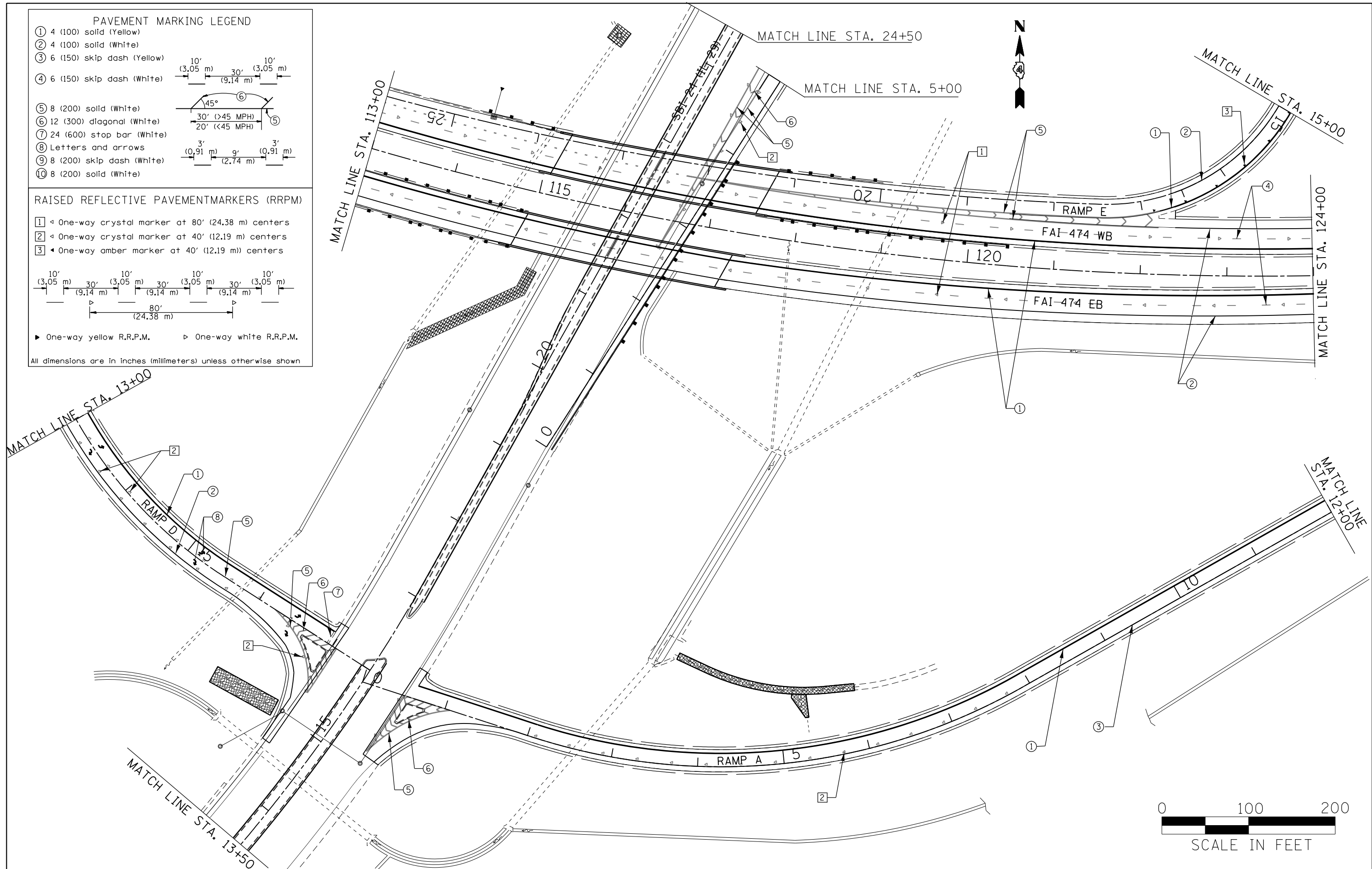
PAVEMENT MARKING LEGEND

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② 4 (100) solid (White)	
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④ 6 (150) skip dash (White)	
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RAISED REFLECTIVE PAVEMENT MARKERS (RRPM)

- ① < One-way crystal marker at 80' (24.38 m) centers
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 - ③ < One-way amber marker at 40' (12.19 m) centers
-
- ▶ One-way yellow R.R.P.M. ▷ One-way white R.R.P.M.

All dimensions are in inches (millimeters) unless otherwise shown



FILE NAME =	USER NAME = lababidism	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKINGS DETAILS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S:\GEN\DRAWING\STD&PLNS\SQUAD 8\1-474 3R	Project\english files\Pavement marking Details\DRAWN.dgn	DRAWN -	REVISED -						474	90-6,7RS-1	TAZEWELL	245	200
Default	PLOT SCALE = 100.0000 "/td> <td>CHECKED -</td> <td>REVISED -</td> <td colspan="4" style="text-align: center;">CONTRACT NO. 68606</td> <td colspan="2" style="text-align: center;">ILLINOIS FED. AID PROJECT</td>	CHECKED -	REVISED -		CONTRACT NO. 68606				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/11/2015	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		