

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

F. A. P. ROUTE 311
U. S. ROUTE 34 OVER IL. ROUTE 53
SECTION: 10HB-R
BRIDGE DECK REPLACEMENT & RESURFACE APPROACHES
STRUCTURE NO. 022-0033
DUPAGE COUNTY
PROJECT NO: **ACBHF-0311(041)**
C-91-023-07

FOR INDEX OF SHEETS, SEE SHEET NO. 2

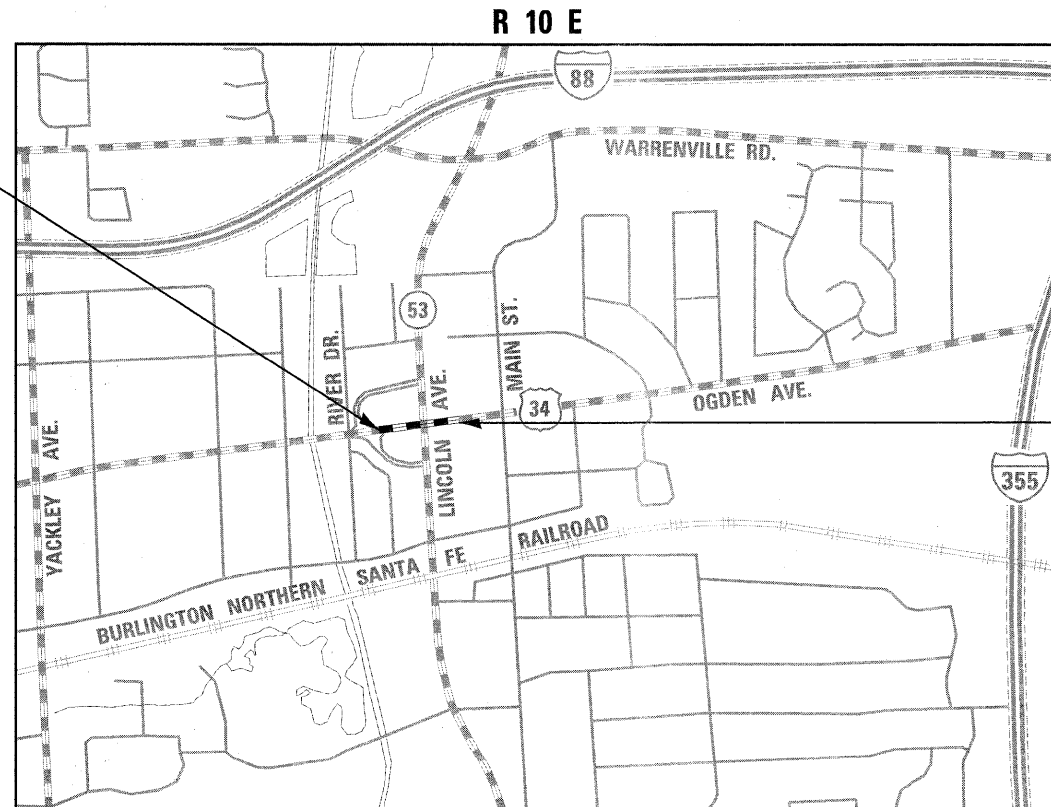
**PROJECT LOCATED IN
THE VILLAGE OF LISLE**

TRAFFIC DATA

2007 ADT (US RTE 34) = 34,400 VEHICLES PER DAY
2007 ADT (IL RTE 53) = 25,600 VEHICLES PER DAY
SPEED LIMIT = 40 MPH (US RTE 34)
35 MPH (IL RTE 53)

**PROJECT BEGINS
STA. 640 + 20**

T 38 N



LISLE TOWNSHIP

GROSS & NET LENGTH OF PROJECT = 930 FT = 0.18 MILES

LOCATION MAP

NOT TO SCALE

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: MICHELLE AQUINO (847) 705-4606
PROJECT MANAGER: RAJENDRA SHAH (847) 705-4555

CONTRACT NO. 60B92

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	1
FED. ROAD DIST. NO. 1	ILLINOIS	DUPAGE	CONTRACT NO. 60B92	

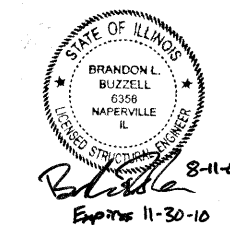
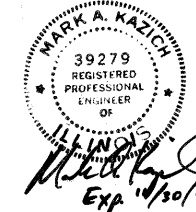
★53+1=54

D-91-023-07



LOCATION OF SECTION INDICATED THUS: - ■ -

**PROJECT ENDS
STA. 649 + 50**



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED AUGUST 14, 2009

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

October 2, 2009
Charles J. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT

October 2, 2009
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

rjngroup
Excellence through Ownership

200 West Front Street
Wheaton, IL 60187

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OF THE STATE OF ILLINOIS**

GENERAL NOTES - ROADWAY

INDEX OF SHEETS

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14	MAINTENANCE OF TRAFFIC - IL RTE 53 INSIDE LANE CLOSURE
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44	FRAMES AND LIDS ADJUSTMENT WITH MILLING
45	CURB OR CURB REMOVAL AND REPLACEMENT
46	BUTT JOINTS AND HMA TAPER DETAILS
47	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
48	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
49	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
50	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
51	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
52	ARTERIAL ROAD INFORMATION SIGN
53	DRIVEWAY ENTRANCE SIGNING

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
420401-00	BRIDGE APPROACH PAVEMENT CONNECTOR
515001-03	NAME PLATE FOR BRIDGES
606301-04	PCC ISLANDS AND MEDIANS
701601-06	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NONTRAVERSABLE MEDIUM
701602-04	URBAN LANE CLOSURE, MULTILANE 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
704001-06	TEMPORARY CONCRETE BARRIER

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL *J.U.L.I.E.* AT (800)892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED.)
- ALL PAVEMENT MARKING SHALL BE PLACED THROUGHOUT THE PROJECT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT, CLEANING OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE RESIDENT ENGINEER SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
- THE ENGINEER SHALL CONTACT MR. DON CHIARUGI, THE TRAFFIC FIELD ENGINEER, @ 847-741-9857 TWO (2) WEEKS PRIOR TO THE START OF THIS PROJECT SO THAT EXACT STATIONING OF NO PASSING ZONES AND OTHER PERMANENT PAVEMENT MARKINGS MAY BE ESTABLISHED.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL FOR TYPICAL APPLICATION OF RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) SHOWN IN THE PLANS.
- WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM BRIDGE INSPECTORS.
- SAW CUTTING OF PAVEMENTS, SHOULDERS, ETC., SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN, STRAIGHT EDGE ON THE PORTION REMAINING. THE COST OF SAW CUTTING REMOVAL ITEMS SHALL BE INCLUDED IN THE UNIT PRICES OF THESE ITEMS.

GENERAL NOTES - TRAFFIC CONTROL & PROTECTION

- THE MAINTENANCE OF TRAFFIC CONTROL (MOT) PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY IMPROVE OR MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MOT PLANS.
- ALL EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS IN CONFLICT WITH THE MAINTENANCE OF TRAFFIC STRIPING SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT, "PAVEMENT MARKING REMOVAL".
- THE CONTRACTOR SHALL REMOVE ALL TEMPORARY PAVEMENT MARKING TAPE WHICH CONFLICTS WITH THE NEXT STAGE OR FINAL STRIPING. REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT, "WORK ZONE PAVEMENT MARKING REMOVAL".
- ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC, AS DETAILED ON THE PLANS, OR HIGHWAY STANDARDS SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN MAINTENANCE OF TRAFFIC SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
- ALL DRUMS, VERTICAL PANELS AND BARRICADES ADJACENT TO THE EDGE OF TRAVELED WAY SHALL BE EQUIPPED WITH STEADY-BURNING LIGHTS.
- ALL EXISTING SIGNS WITHIN THE LIMITS OF MAINTENANCE OF TRAFFIC WHICH ARE OBTSCURED BY OR OTHERWISE INTERFERED WITH BY THE CONSTRUCTION OPERATIONS AND MAINTENANCE OF TRAFFIC SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS.
- TEMPORARY, OFF-PEAK HOUR LANE CLOSURES MUST BE REQUESTED THROUGH THE ENGINEER AND AS SPECIFIED IN THE SPECIAL PROVISIONS. WHEN OFF-PEAK HOUR OR WEEKEND LANE CLOSURES ARE REQUIRED, A PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE INSTALLED ONE WEEK PRIOR TO THE CLOSURE. THE MESSAGE SIGN WORDING AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
- ALL TEMPORARY INFORMATION SIGNS SHALL BE PAID FOR SEPARATELY AT THE CONTRACT UNIT PRICE PER SQUARE FEET FOR "TEMPORARY INFORMATION SIGNING".
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.
- ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.
- FOR ADDITIONAL BRIDGE CONSTRUCTION STAGING INFORMATION, SEE STRUCTURAL PLANS.

PLOT DATE = 8/7/2009
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 PLOT SCALE = 1/8"=1'-0"
 MODEL NAME = Defaul
 MODEL



200 West Front Street
Wheaton, IL 60187

DESIGNED -	REVISED -
DRAWN - MH	REVISED -
CHECKED - MK	REVISED -
DATE - 8/7/09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US RTE. 34 OVER IL RTE. 53
STRUCTURE NO. 022-0033**

**INDEX OF SHEETS, STANDARDS,
AND GENERAL NOTES**

SCALE: SHEET NO. 2 OF 53 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	2
				CONTRACT NO. 60892

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

URBAN 80% FED./20% STATE

SUMMARY OF QUANTITIES

CODE NO.	ITEM DESCRIPTION	UNIT	CONSTRUCTION TYPE CODE			
			TOTAL QUANTITY	ROADWAY I000-2A	BRIDGE SN 022-0033 X271-2A	VILLAGE OF LISLE X271-2A 100% L.A.
20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	432		432	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	4	4		
40600300	AGGREGATE (PRIME COAT)	TON	19	19		
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	524	524		
40600895	CONSTRUCTING TEST STRIP	EACH	1	1		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	62	62		
40603395	HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	443	443		
42001420	BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)	SQ YD	74	74		
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	4,520	4,520		
44000600	SIDEWALK REMOVAL	SQ FT	365	365		
44000700	APPROACH SLAB REMOVAL	SQ YD	440	440		
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	146	146		
44003700	MEDIAN REMOVAL (SPECIAL)	SQ FT	2,874	2,874		
50102400	CONCRETE REMOVAL	CU YD	60.6		60.6	
50104650	SLOPE WALL REMOVAL	SQ YD	5.6		5.6	
50104720	REMOVAL OF EXISTING CONCRETE DECK	EACH	1		1	
50157300	PROTECTIVE SHIELD	SQ YD	871		871	
50200100	STRUCTURE EXCAVATION	CU YD	489.4		489.4	
50300100	FLOOR DRAINS	EACH	8		8	
50300225	CONCRETE STRUCTURES	CU YD	73.6		73.6	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	713.1		713.1	
50300260	BRIDGE DECK GROOVING	SQ YD	1,200		1,200	
50300285	FORM LINER TEXTURED SURFACE	SQ FT	1,284			1,284
50300300	PROTECTIVE COAT	SQ YD	2,169	344	1,825	
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	330		330	
50500505	STUD SHEAR CONNECTORS	EACH	5,664		5,664	
50600300	CLEANING AND PAINTING STEEL BRIDGE	L SUM	1			1
50606600	CONTAINMENT AND DISPOSAL OF NON-LEAD PAINT CLEANING RESIDUES	L SUM	1			1
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	137,440		137,440	
50800515	BAR SPLICERS	EACH	797		797	
51100100	SLOPE WALL 4 INCH	SQ YD	5.6		5.6	
51500100	NAME PLATES	EACH	1		1	
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	24		24	
52100520	ANCHOR BOLTS, 1"	EACH	48		48	
59000200	EPOXY CRACK INJECTION	FOOT	12		12	
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	177		177	
60109580	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	177		177	
60619500	CONCRETE MEDIAN, TYPE SB-6.06 (SPECIAL)	SQ FT	2,874	2,874		
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4		

* SPECIALTY ITEM

80% FED./20% STATE

SUMMARY OF QUANTITIES

CODE NO.	ITEM DESCRIPTION	UNIT	CONSTRUCTION TYPE CODE			
			TOTAL QUANTITY	ROADWAY I000-2A	BRIDGE SN 022-0033 X271-2A	VILLAGE OF LISLE X271-2A 100% L.A.
63200310	GUARDRAIL REMOVAL	FOOT	179	179		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	8	8		
67100100	MOBILIZATION	L SUM	1	1		
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1		
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1,290	1,290		
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3,260	3,260		
70400100	TEMPORARY CONCRETE BARRIER	FOOT	520	520		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	488	488		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2,758	2,758		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	64	64		
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	621	621		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	116	116		
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	19	19		
* 78200520	BARRIER WALL MARKERS, TYPE B	EACH	42	42		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	1,003	1,003		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	116	116		
* 81100700	CONDUIT ATTACHED TO STRUCTURE, 2 1/2" DIA., GALVINIZED STEEL	FOOT	30	30		
* 81300835	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18"X18"X10"	EACH	2	2		
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2		
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	73	73		
* 89502200	MODIFY EXISTING CONTROLLER	EACH	2	2		
* 89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	2,950	2,950		
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	214	214		
X0323988	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	164		164	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	20		20	
X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH	FOOT	8,490	8,490		
* X0325938	TEMPORARY WIRELESS INTERCONNECT, COMPLETE	L SUM	1	1		
X0326096	ALUMINUM RAILING, SPECIAL	FOOT	300		300	
* XX003079	REMOVE JUNCTION BOX	EACH	2	2		
XX007023	STAINING CONCRETE STRUCTURES	SQ YD	186.2			186.2
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	1	1		
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	1	1		
Z0030390	IMPACT ATTENUATORS, REPLACEMENT (NON-REDIRECTIVE), TEST LEVEL 3	EACH	1	1		
Z0031200	JACKING AND CRIBBING	EACH	4			4
Δ Z0076600	TRAINEES	HOUR	500	500		

* SPECIALTY ITEM

Δ Y080

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Excellence through Ownership

200 West Front Street
 Wheaton, IL 60187

DESIGNED -	REVISED -
DRAWN - MH	REVISED -
CHECKED - MK	REVISED -
DATE - 8/7/09	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

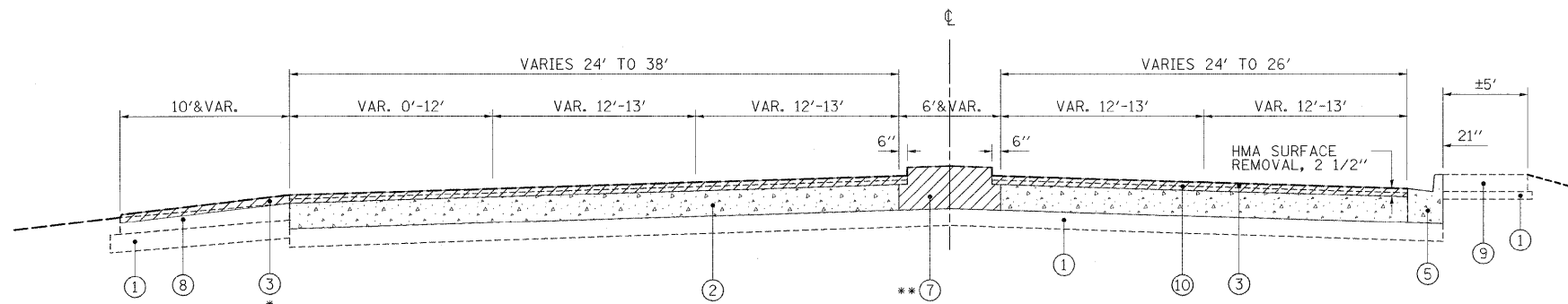
US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033

SUMMARY OF
 QUANTITIES

F.A.P. RTE. 311	SECTION 10HB-R	COUNTY DUPAGE	TOTAL SHEETS 53	SHEET NO. 3
SCALE: SHEET NO. 3 OF 53 SHEETS STA. TO STA.			CONTRACT NO. 60B92	

Rev.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

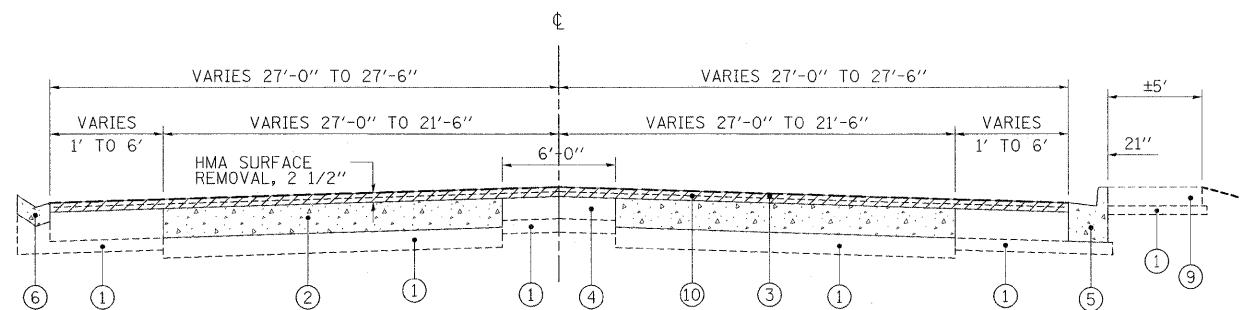


EXISTING TYPICAL SECTION OGDEN AVE.

STA. 640+20 TO STA. 643+83.4
 STA. 646+07.1 TO STA. 647+89

* STA. 646+07.1 TO STA. 646+67 (COMBINATION CONCRETE CURB & GUTTER)
 STA. 646+67 TO STA. 647+89 (CONCRETE GUTTER)

** MEDIAN REMOVAL BEGINS AT STA. 640+70



EXISTING TYPICAL SECTION OGDEN AVE.

STA. 647+89 TO STA. 649+50

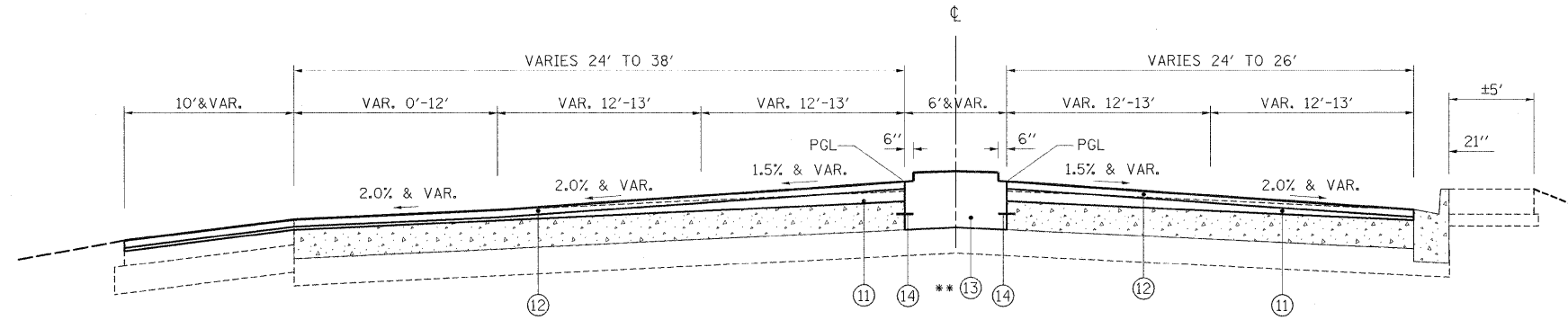
LEGEND

- ① EXISTING AGGREGATE BASE COURSE
- ② EXISTING PCC PAVEMENT, 9 1/4" (±)
- ③ EXISTING HOT-MIX ASPHALT OVERLAY, 3" (±)
- ④ EXISTING HOT-MIX ASPHALT BASE COURSE, 7 3/4"
- ⑤ EXISTING COMBINATION CURB AND GUTTER, TYPE B-6.18
- ⑥ EXISTING GUTTER, TYPE B
- ⑦ EXISTING CONCRETE MEDIAN REMOVAL (SPECIAL)
- ⑧ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑨ EXISTING PCC SIDEWALK
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑪ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, VARIABLE DEPTH (3/4" MIN.)
- ⑫ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
- ⑬ PROPOSED CONCRETE MEDIAN, TYPE SB 6.06 (SPECIAL)
- ⑭ TIE BARS, NO. 6, 24" LONG, EMBEDDED 8" INTO EXISTING PAVEMENT @ 24" O.C.

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 MODEL = Existing Ogden

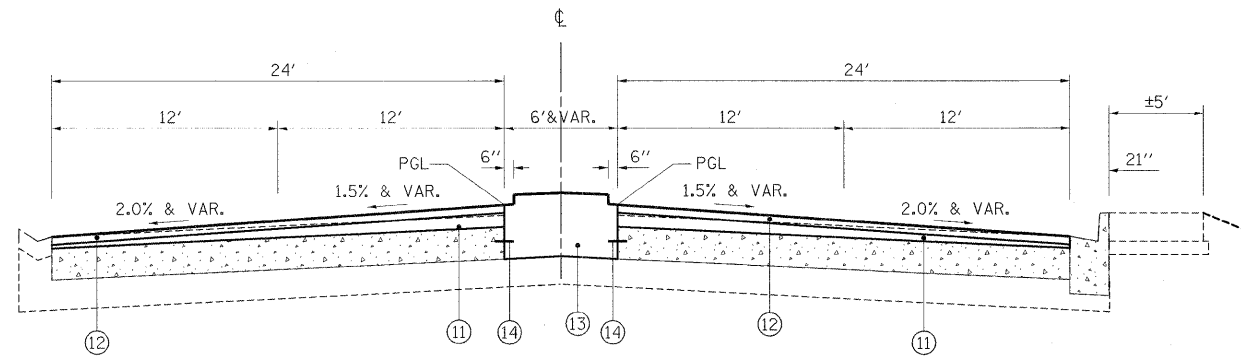
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DRAWN - JT	REVISD -
CHECKED - MK	REVISD -
DATE - 8/6/09	REVISD -

F.A.P. RTE. 311	SECTION 10HB-R	COUNTY DUPAGE	TOTAL SHEETS 53	SHEET NO. 4
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



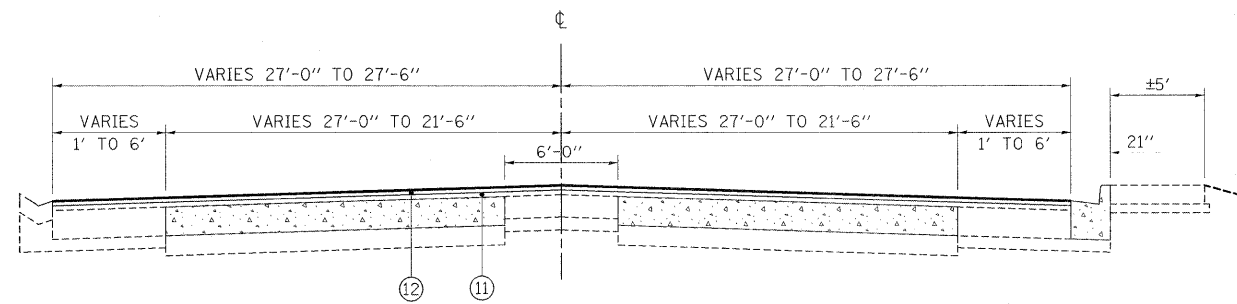
PROPOSED TYPICAL SECTION OGDEN AVE.

STA. 640+20 TO STA. 643+83.4
 ** PROPOSED MEDIAN BEGINS AT STA. 640+70



PROPOSED TYPICAL SECTION OGDEN AVE.

STA. 646+07.1 TO STA. 647+89



PROPOSED TYPICAL SECTION OGDEN AVE.

STA. 647+89 TO STA. 649+50

LEGEND

- ① EXISTING AGGREGATE BASE COURSE
- ② EXISTING PCC PAVEMENT, 9 1/4" (±)
- ③ EXISTING HOT-MIX ASPHALT OVERLAY, 3" (±)
- ④ EXISTING HOT-MIX ASPHALT BASE COURSE, 7 3/4"
- ⑤ EXISTING COMBINATION CURB AND GUTTER, TYPE B-6.18
- ⑥ EXISTING GUTTER, TYPE B
- ⑦ EXISTING CONCRETE MEDIAN REMOVAL (SPECIAL)
- ⑧ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑨ EXISTING PCC SIDEWALK
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑪ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, VARIABLE DEPTH (3/4" MIN.)
- ⑫ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
- ⑬ PROPOSED CONCRETE MEDIAN, TYPE SB 6.06 (SPECIAL)
- ⑭ TIE BARS, NO. 6, 24" LONG, EMBEDDED 8" INTO EXISTING PAVEMENT @ 24" O.C.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE USES	AC TYPE	VOIDS
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5mm)	SBS / SBR PG 70-22	4% @ 90 GYR.
LEVELING BINDER (MACHINE METHOD), N70, (IL-9.5mm)	PG 64-22 *	4% @ 70 GYR.

* RAP % NOTE UNDER THE CHART.

NOTES:
 THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.

* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

PLOT DATE = 8/6/2009
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 MODEL = Proposed Ogden



200 West Front Street
 Wheaton, IL 60187

DESIGNED -	REVISED -
DRAWN - JT	REVISED -
CHECKED - MK	REVISED -
DATE - 8/6/09	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033

PROPOSED TYPICAL SECTIONS

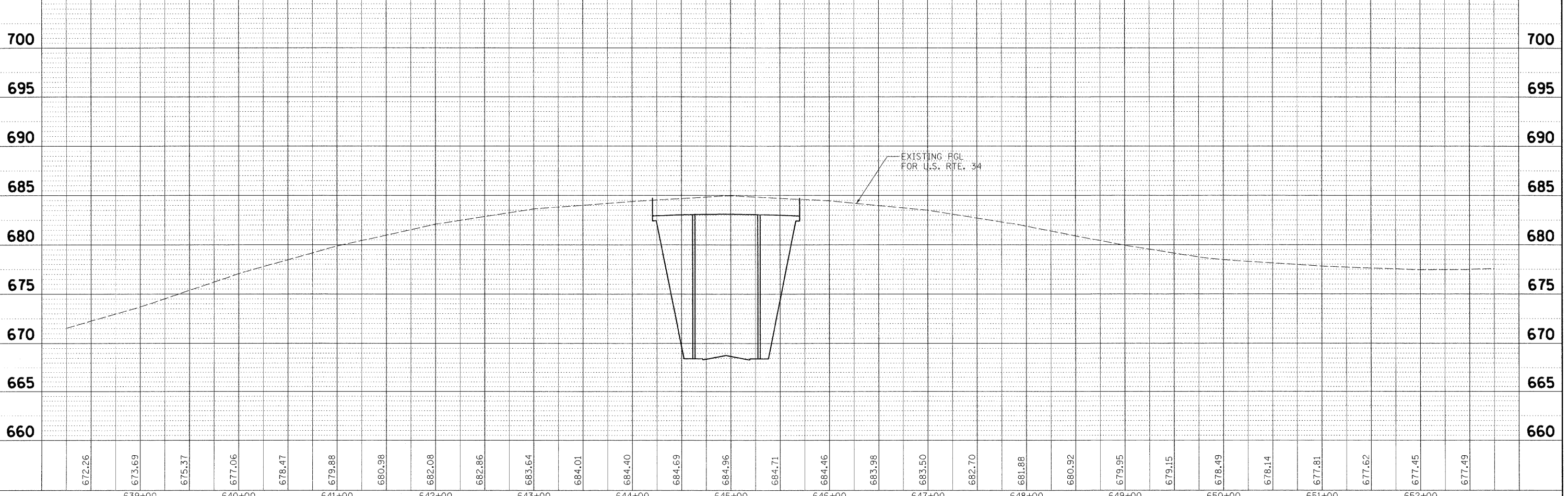
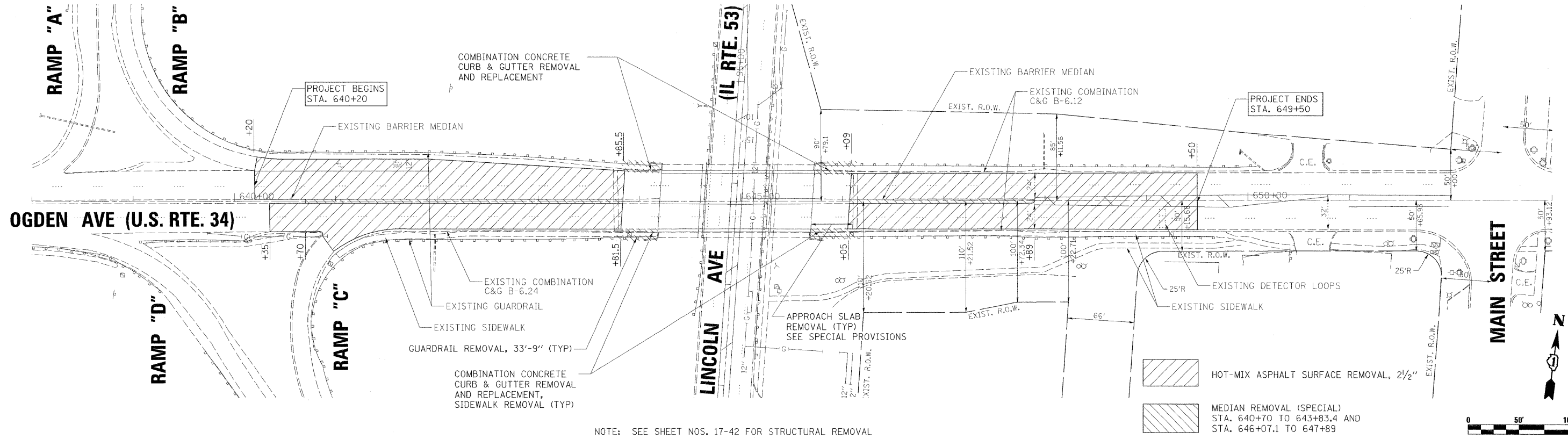
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311	10HB-R	DUPAGE	53	5
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. 5 OF 53 SHEETS STA. _____ TO STA. _____

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	NOTED		
	PLOTTED		
	CHECKED		
	PAID FILE NAME		

PROFILE	SURVEYED	BY	DATE
	NOTED		
	PLOTTED		
	CHECKED		
	STRUCTURE NOTATION CHRG		

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DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US RTE. 34 OVER IL RTE. 53 STRUCTURE NO. 022-0033	ROADWAY REMOVAL PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN - MH	REVISED -				311	10HB-R	DUPAGE	53	6
CHECKED -	REVISED -				CONTRACT NO. 60B92				
DATE - 8/7/09	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

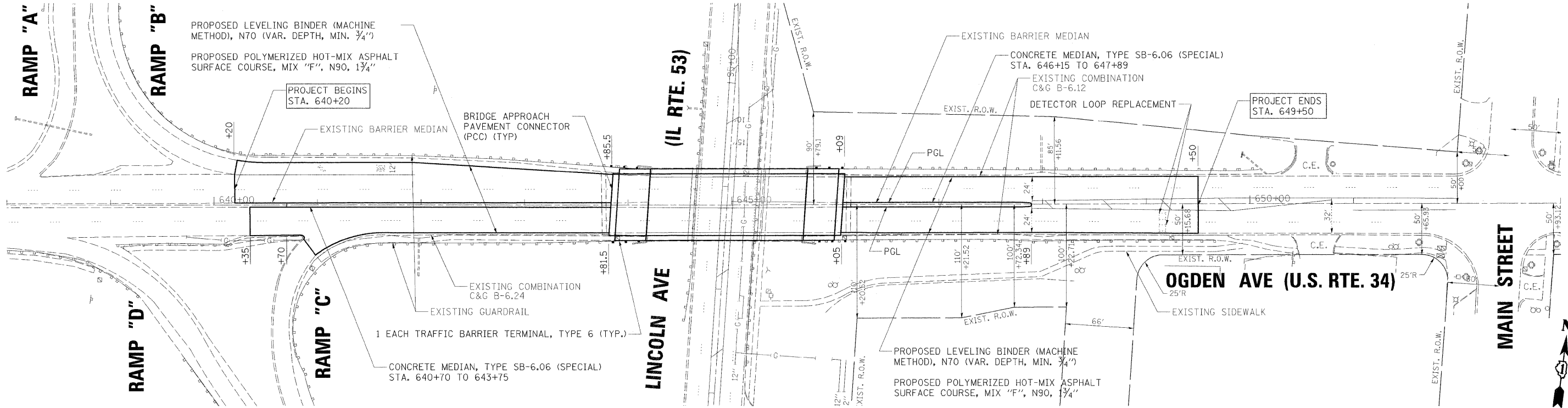
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 Wheaton, IL 60187

SCALE: 1:50 SHEET NO. 6 OF 53 SHEETS STA. _____ TO STA. _____

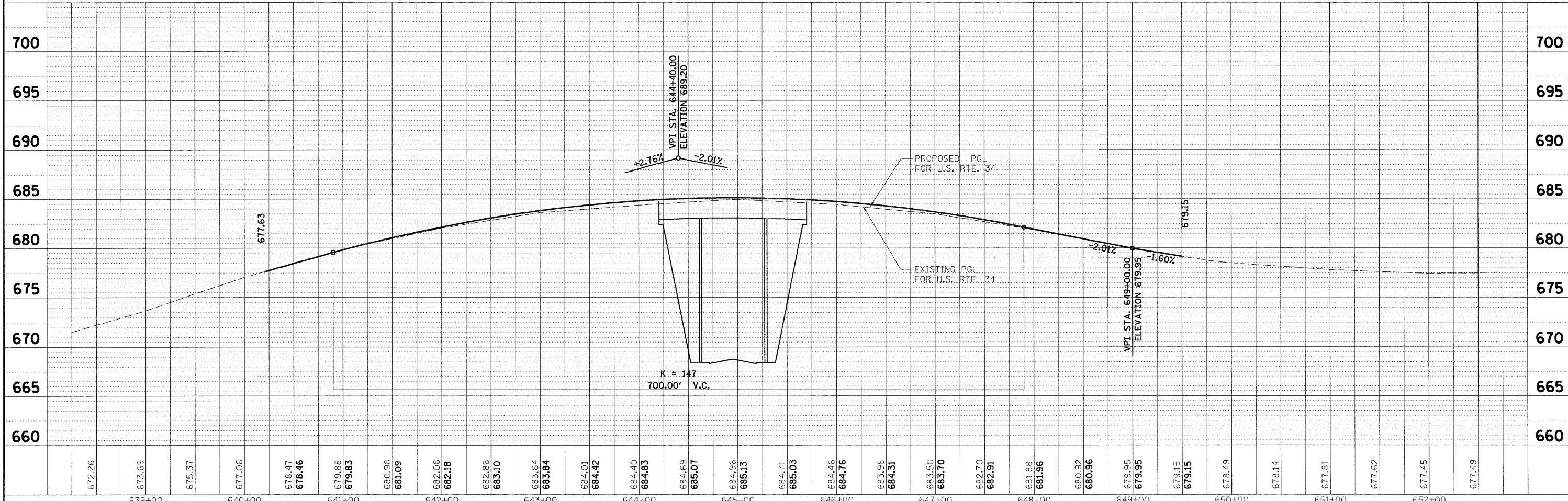
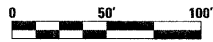
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NOTE: SEE SHEET NOS. 17-42 FOR STRUCTURAL IMPROVEMENTS AND SIDEWALK REPLACEMENT



672.26	673.69	675.37	677.06	678.47	679.88	680.98	681.09	682.08	682.18	682.86	683.10	683.64	683.84	684.01	684.42	684.40	684.83	684.69	685.07	684.96	685.13	684.71	685.03	684.46	684.76	683.98	684.31	683.50	683.70	682.70	682.91	681.88	681.96	680.92	680.96	679.95	679.95	679.15	679.15	678.49	678.14	677.81	677.62	677.45	677.49
639+00		640+00		641+00		642+00		643+00		644+00		645+00		646+00		647+00		648+00		649+00		650+00		651+00		652+00																			

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DESIGNED	-	REVISED	-
DRAWN	- MH/JT	REVISED	-
CHECKED	- MK	REVISED	-
DATE	- 8/7/09	REVISED	-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033

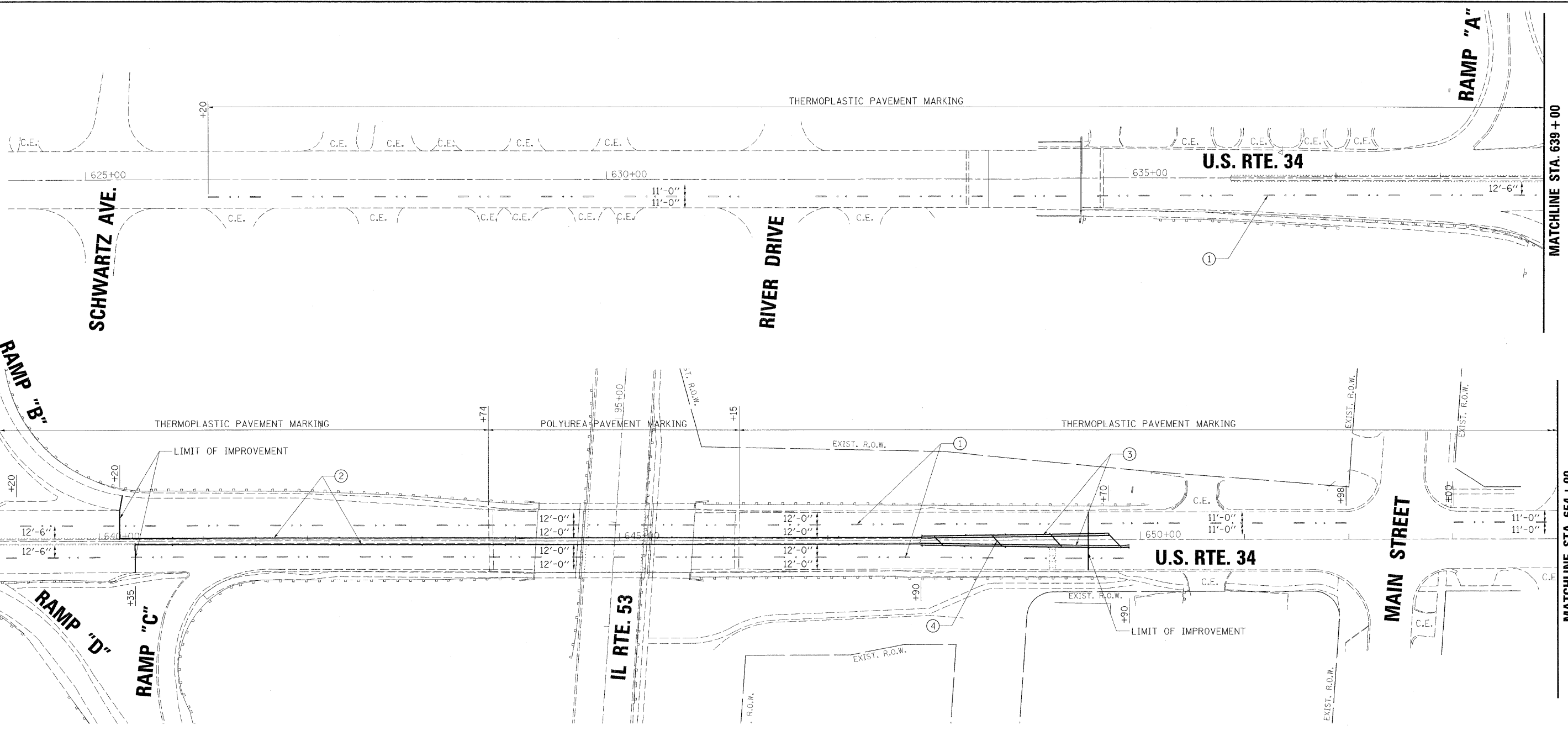
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	7
CONTRACT NO. 60892				

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STA. TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

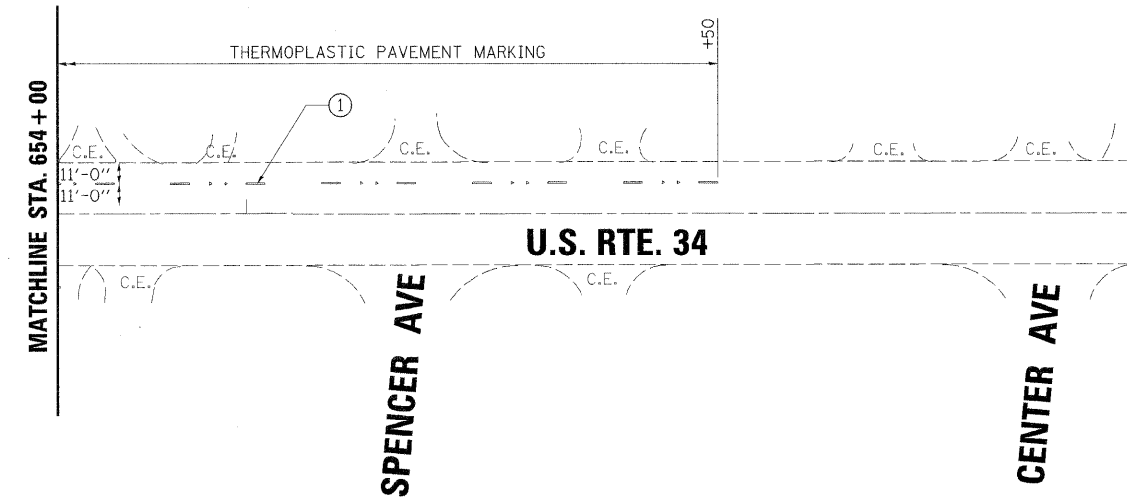
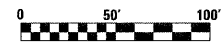


SYMBOLS LEGEND

- ◁ ONE-WAY CRYSTAL MARKER
- ◄ ONE-WAY AMBER MARKER

PAVEMENT MARKING LEGEND

- ① 4" WHITE LANE LINE, SKIP DASH (TYP)
- ② 4" YELLOW SOLID EDGE LINE
- ③ 4" DOUBLE YELLOW CENTERLINE (TYP)
- ④ 12" YELLOW DIAGONAL LINE, 75' O.C.



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DESIGNED -	REVISD -
DRAWN - MH	REVISD -
CHECKED - MK	REVISD -
DATE - 8/7/09	REVISD -

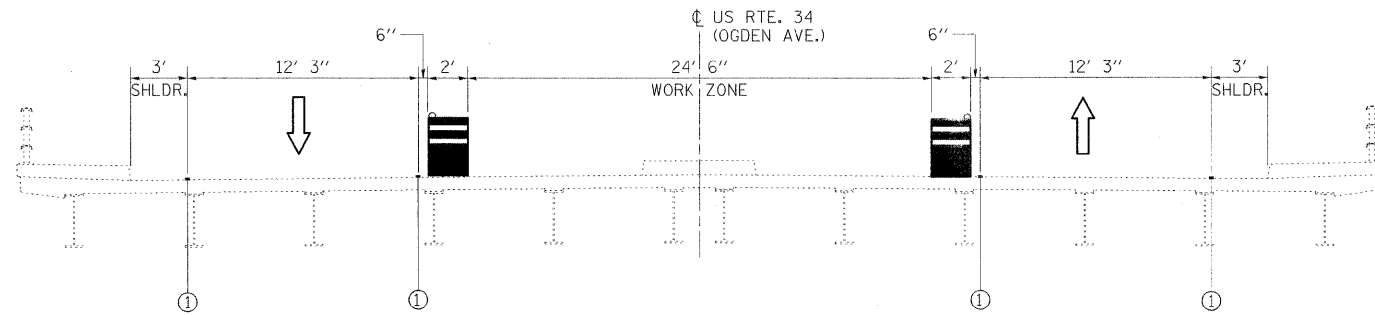
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033**

**PAVEMENT MARKING PLAN
 US RTE 34**

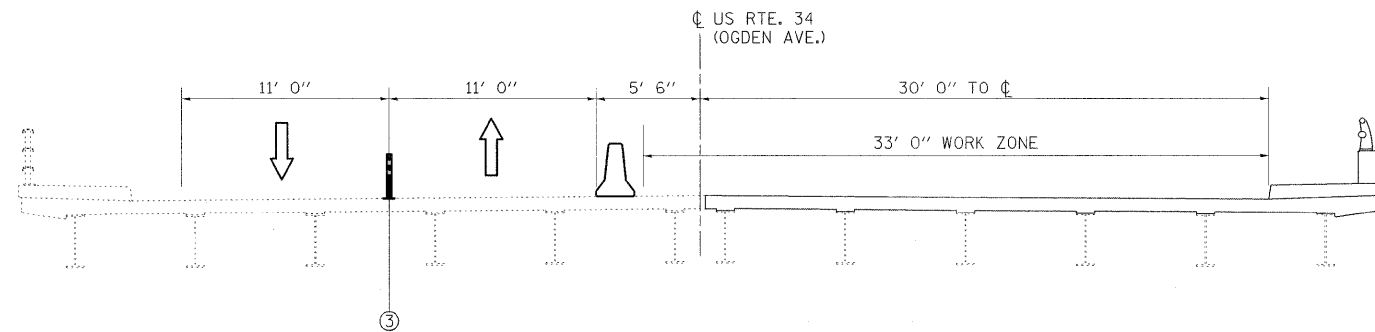
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F.A.P. RTE. 311	SECTION 10HB-R	COUNTY	TOTAL SHEETS 53	SHEET NO. 8
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



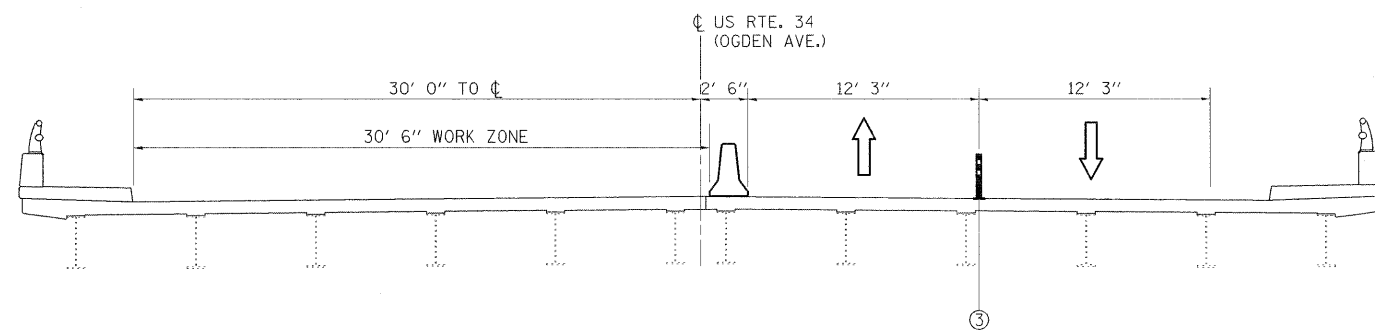
U.S. RTE. 34 - PRE-STAGE

(LOOKING EAST)



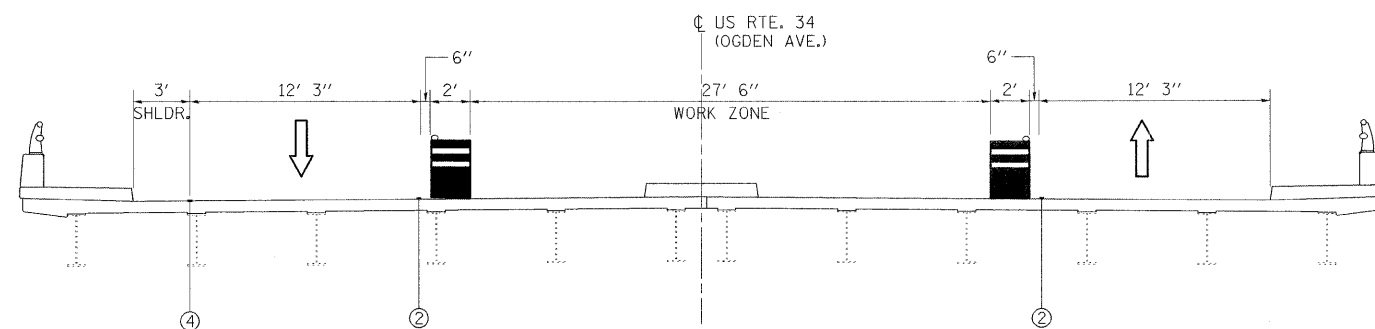
U.S. RTE. 34 - STAGE I

(LOOKING EAST)



U.S. RTE. 34 - STAGE II

(LOOKING EAST)



U.S. RTE. 34 - STAGE III

(LOOKING EAST)

PRE-STAGE

CONSTRUCTION:

- REMOVE CONCRETE MEDIAN STA. 640+70 TO 643+75 AND STA. 646+15 TO 647+89 TO BE FLUSH WITH ADJACENT PAVEMENT WITHIN THE LIMITS SHOWN.
- PLACE TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR, TEMPORARY.
- REMOVE CONFLICTING MARKINGS AND IMPLEMENT STAGE I MOT PAVEMENT MARKINGS WITHIN THE WORK ZONE.

MAINTENANCE OF TRAFFIC:

- CLOSE INSIDE LANE IN BOTH DIRECTIONS PER THE MOT PLANS.

STAGE I

CONSTRUCTION:

- PERFORM STRUCTURAL WORK ON SOUTH HALF OF DECK AND ABUTMENTS.
- PARTIAL REMOVAL AND RESURFACING OF ROADWAY (TO PERMIT TRAFFIC FOR STAGE II WORK).
- MISCELLANEOUS WORK ALONG THE SOUTH EDGE OF PAVEMENT.
- RELOCATE TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR, TEMPORARY.
- REMOVE CONFLICTING MARKINGS AND IMPLEMENT STAGE II MOT PAVEMENT MARKINGS WITHIN THE WORK ZONE.

MAINTENANCE OF TRAFFIC:

- U.S. 34 - TRAFFIC REDUCED TO ONE LANE IN EACH DIRECTION, AND SHIFTED TO THE NORTH SIDE OF THE EXISTING BRIDGE DECK.
- IL 53 - EITHER THE INSIDE OR OUTSIDE LANE(S) WILL BE CLOSED, PER THE MAINTENANCE OF TRAFFIC PLANS (PLANS SHOW WORK IN BOTH DIRECTIONS). CLOSURES ARE ANTICIPATED TO BE LIMITED TO OFF-PEAK HOURS, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

STAGE II

CONSTRUCTION:

- PERFORM STRUCTURAL WORK ON NORTH HALF OF DECK AND ABUTMENTS.
- PARTIAL REMOVAL AND RESURFACING OF ROADWAY (TO PERMIT TRAFFIC FOR STAGE III WORK).
- MISCELLANEOUS WORK ALONG THE NORTH EDGE OF PAVEMENT.
- REMOVE TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR, TEMPORARY.
- REMOVE CONFLICTING MARKINGS AND IMPLEMENT SHORT-TERM PAVEMENT MARKINGS.

MAINTENANCE OF TRAFFIC:

- U.S. 34 - TRAFFIC REDUCED TO ONE LANE IN EACH DIRECTION, AND SHIFTED TO THE SOUTH SIDE OF THE EXISTING BRIDGE DECK.
- IL 53 - EITHER THE INSIDE OR OUTSIDE LANE(S) WILL BE CLOSED, PER THE MAINTENANCE OF TRAFFIC PLANS (PLANS SHOW WORK IN BOTH DIRECTIONS). CLOSURES ARE ANTICIPATED TO BE LIMITED TO OFF-PEAK HOURS, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

STAGE III

CONSTRUCTION:

- COMPLETE MEDIAN REMOVAL AND RECONSTRUCT CONCRETE MEDIAN. ON DECK AND PAVEMENT.
- RESURFACE APPROACH AND DEPARTURE PAVEMENT IN BOTH DIRECTIONS.
- APPLY BOTH SHORT-TERM AND PERMANENT PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS.
- OTHER MISCELLANEOUS WORK.

MAINTENANCE OF TRAFFIC:

- U.S. 34 - CLOSE INSIDE LANE IN BOTH DIRECTIONS PER THE MOT PLANS. ONCE MEDIAN HAS BEEN RECONSTRUCTED AND THERE ARE NO DROP-OFFS TO THE PAVEMENT, DAILY LANE CLOSURES PER STD. 701601 MAY BE USED.
- IL 53 - EITHER THE INSIDE OR OUTSIDE LANE(S) WILL BE CLOSED, PER THE MAINTENANCE OF TRAFFIC PLANS (PLANS SHOW WORK IN BOTH DIRECTIONS). CLOSURES ARE ANTICIPATED TO BE LIMITED TO OFF-PEAK HOURS, UNLESS OTHERWISE APPROVED BY THE ENGINEER.

SEE SHEET 2 FOR TRAFFIC CONTROL AND PROTECTION NOTES.

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② TYPE III TEMPORARY PAVEMENT MARKING TAPE, FOR WET CONDITIONS, 4" SOLID YELLOW LINE (TYP)
- ③ TYPE III TEMPORARY PAVEMENT MARKING TAPE, FOR WET CONDITIONS, 4" DOUBLE YELLOW LINE @ 11" O.C. (TYP) STA. 43+25 TO STA. 46+65 FLEXIBLE DELINEATORS
- ④ TYPE III TEMPORARY PAVEMENT MARKING TAPE, FOR WET CONDITIONS, 4" SOLID WHITE LINE (TYP)

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 MODEL = MOT.Dwg Sections



200 West Front Street
Wheaton, IL 60187

DESIGNED -	REVISD -
DRAWN - MH	REVISD -
CHECKED - MK	REVISD -
DATE - 8/7/09	REVISD -

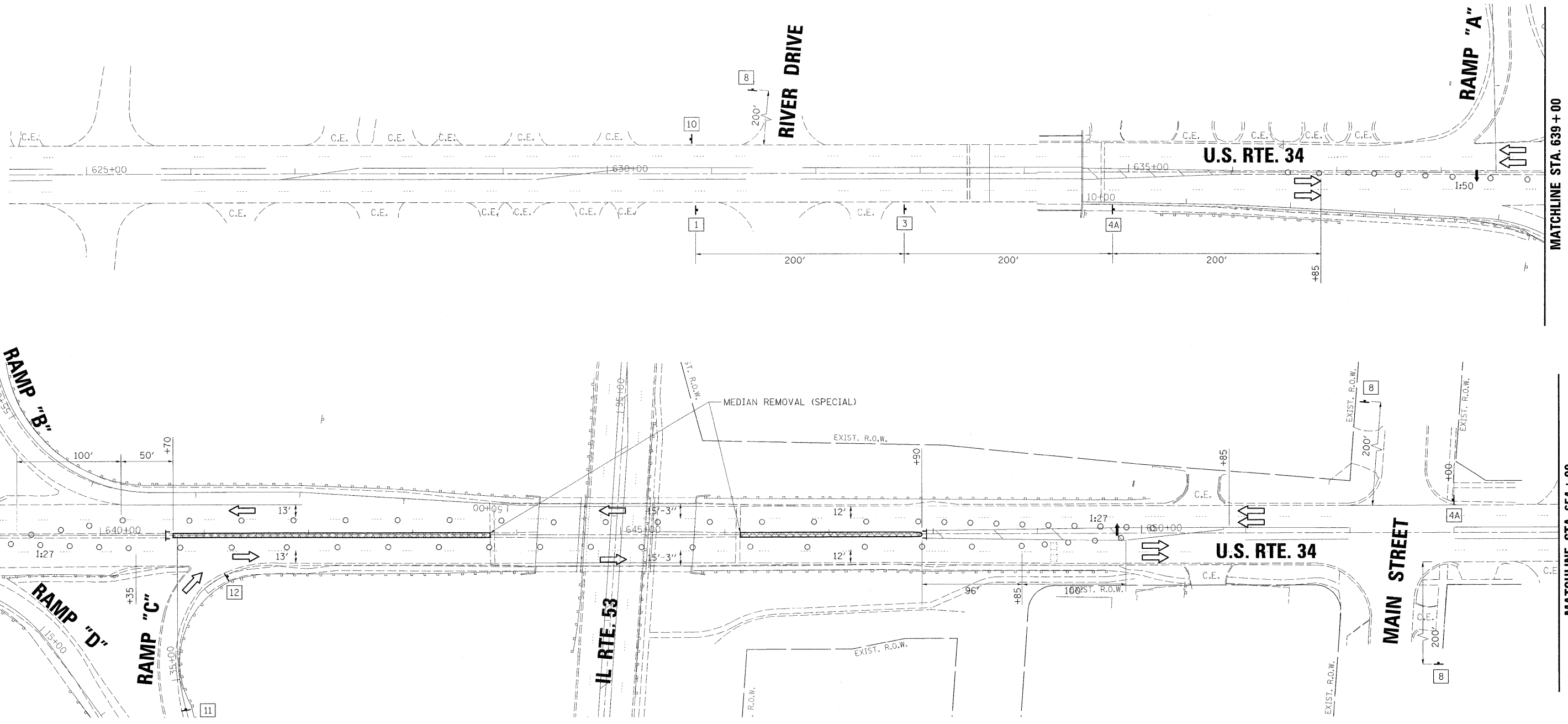
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US RTE. 34 OVER IL RTE. 53
STRUCTURE NO. 022-0033

MAINTENANCE OF TRAFFIC
TYPICAL SECTIONS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R		53	9
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. 9 OF 53 SHEETS STA. _____ TO STA. _____

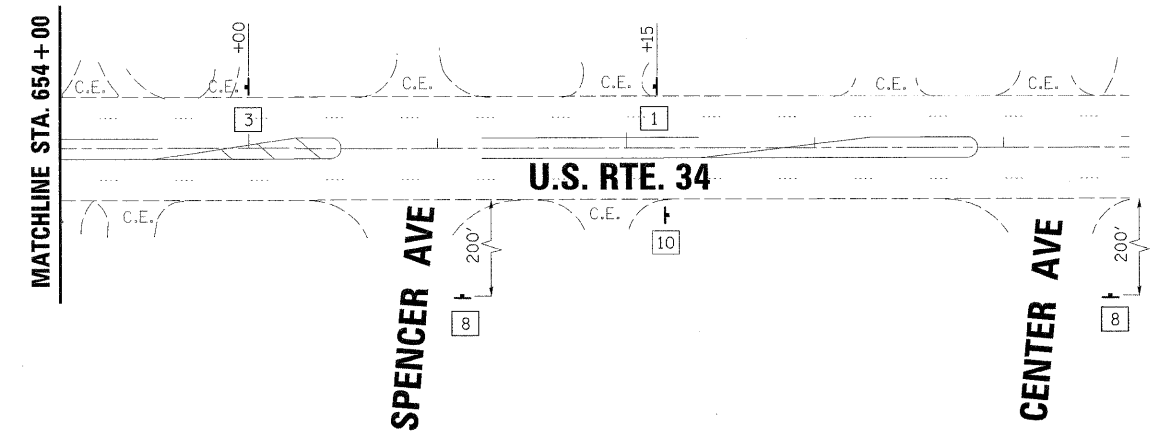


LEGEND

- WORK AREA
- SIGN
- TYPE II BARRICADE
 - o 50' C-C ON TANGENT
 - o 25' C-C ON TAPERS (TYP.)
- FLEXIBLE DELINEATORS @ 25' C-C
- TYPE III BARRICADE WITH FLASHING LIGHTS
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR (NON REDIRECTIVE) TEST LEVEL 3

8

1	ROAD CONSTRUCTION AHEAD W20-1103(O)-48	2	RIGHT LANE CLOSED AHEAD W-20-5R(O)-48	3	LEFT LANE CLOSED AHEAD W20-5L(O)-48	4A	W4-2L	4B	W4-2R
5	W1-4L-48	6	W1-4R-48	7	W-6-3R(O)-48	8	W20-1103(O)-36	8	W1-7-2115
9	SIDEWALK CLOSED WR11-1101-2418	11	YIELD AHEAD W3-2	12	YIELD R1-2	13	DRIVEWAY ENTRANCE (SEE TC-26)		
10	END CONSTRUCTION G20-2								



PLOT DATE = 8/7/09
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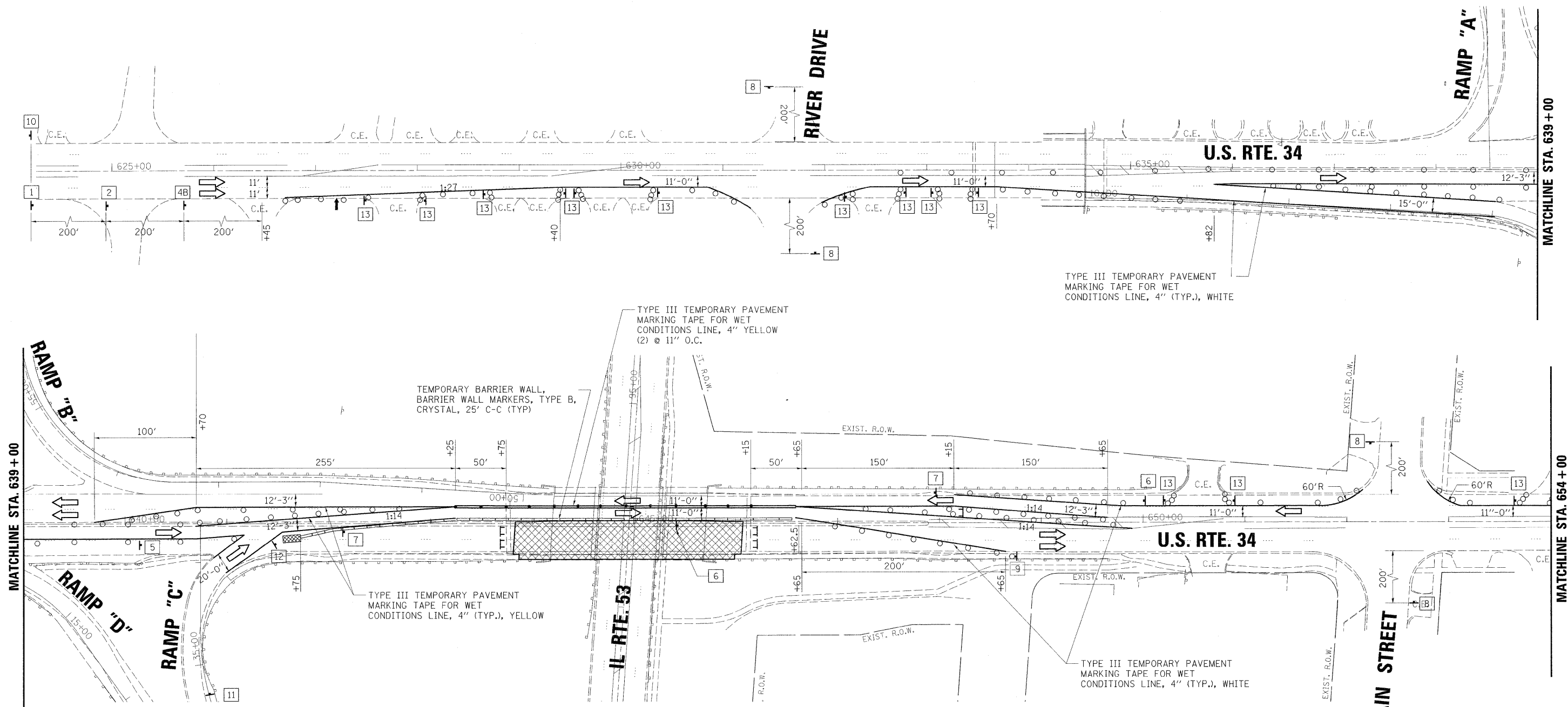
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CHECKED -	MK	REVISED -	---
DATE -	8/7/09	REVISED -	---

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033**
 SCALE: 1:50

**MAINTENANCE OF TRAFFIC
 US RTE 34 PRE-STAGE**
 SHEET NO. 10 OF 53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	10
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B92	



TYPE III TEMPORARY PAVEMENT
MARKING TAPE FOR WET
CONDITIONS LINE, 4" (TYP.), WHITE

TEMPORARY BARRIER WALL,
BARRIER WALL MARKERS, TYPE B,
CRYSTAL, 25' C-C (TYP)

TYPE III TEMPORARY PAVEMENT
MARKING TAPE FOR WET
CONDITIONS LINE, 4" YELLOW
(2) @ 11" O.C.

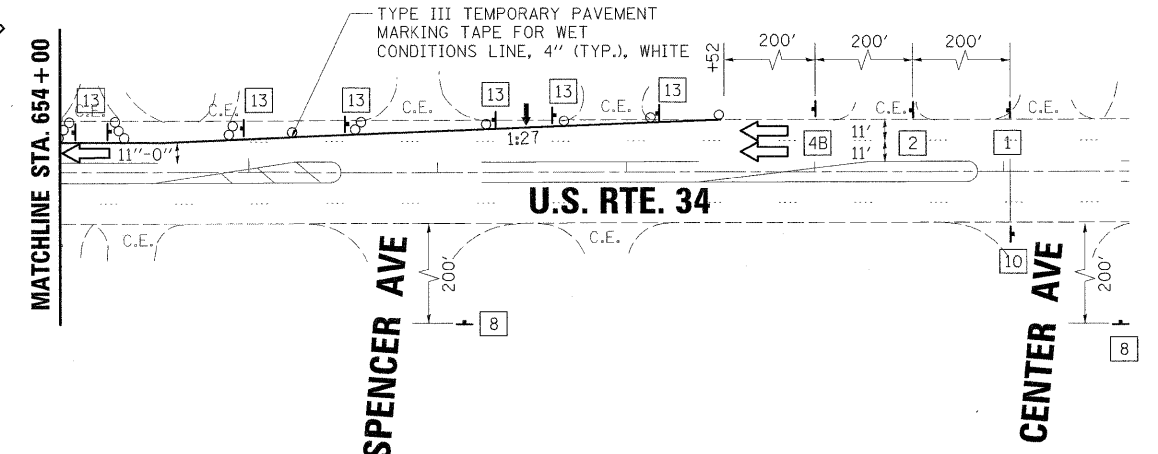
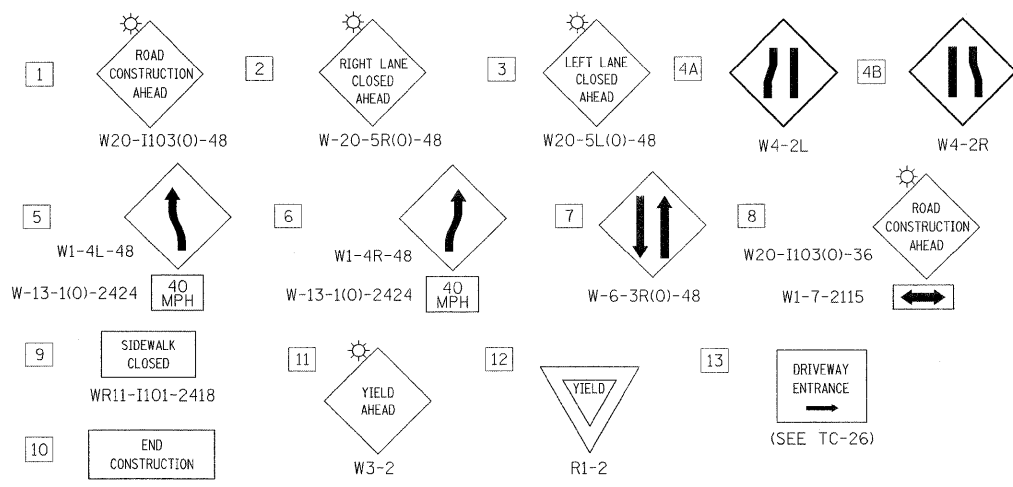
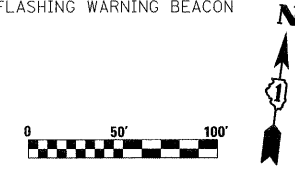
TYPE III TEMPORARY PAVEMENT
MARKING TAPE FOR WET
CONDITIONS LINE, 4" (TYP.), YELLOW

TYPE III TEMPORARY PAVEMENT
MARKING TAPE FOR WET
CONDITIONS LINE, 4" (TYP.), WHITE

TYPE III TEMPORARY PAVEMENT
MARKING TAPE FOR WET
CONDITIONS LINE, 4" (TYP.), WHITE

LEGEND

- WORK AREA
- SIGN
- TYPE II BARRICADE
@ 50' C-C ON TANGENT
@ 25' C-C ON TAPERS (TYP.)
- FLEXIBLE DELINEATORS
@ 25' C-C
- TYPE III BARRICADE WITH FLASHING LIGHTS
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR, TEMP. (NON-REDIRECTIVE), TEST LEVEL 3
- FLASHING WARNING BEACON



8/7/2009
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 USER: jbrucey
 MODEL: 1



200 West Front Street
Wheaton, IL 60187

DESIGNED -	MH	REVISED -	---
DRAWN -	MH	REVISED -	---
CHECKED -	MK	REVISED -	---
DATE -	8/7/09	REVISED -	---

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

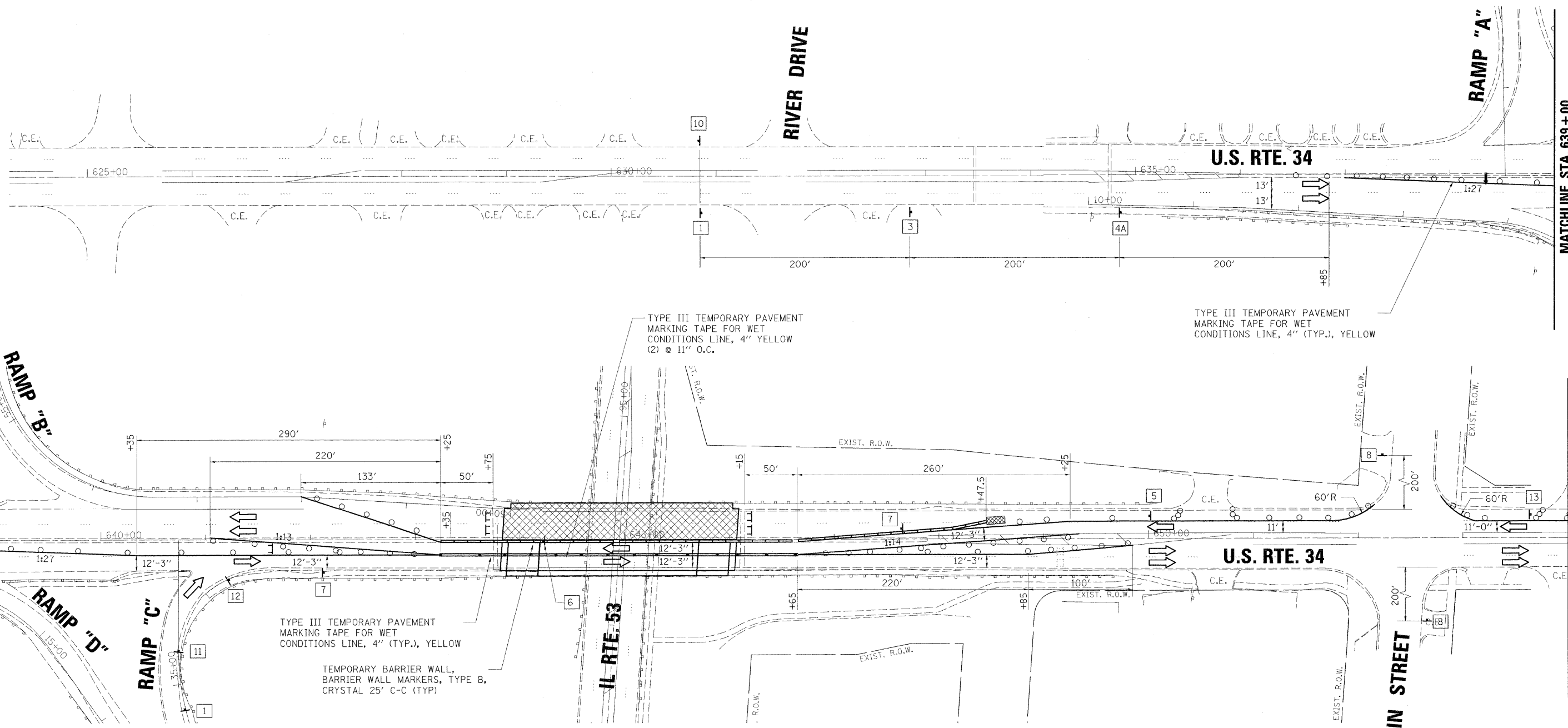
US RTE. 34 OVER IL RTE. 53
STRUCTURE NO. 022-0033

MAINTENANCE OF TRAFFIC
US RTE 34 STAGE I

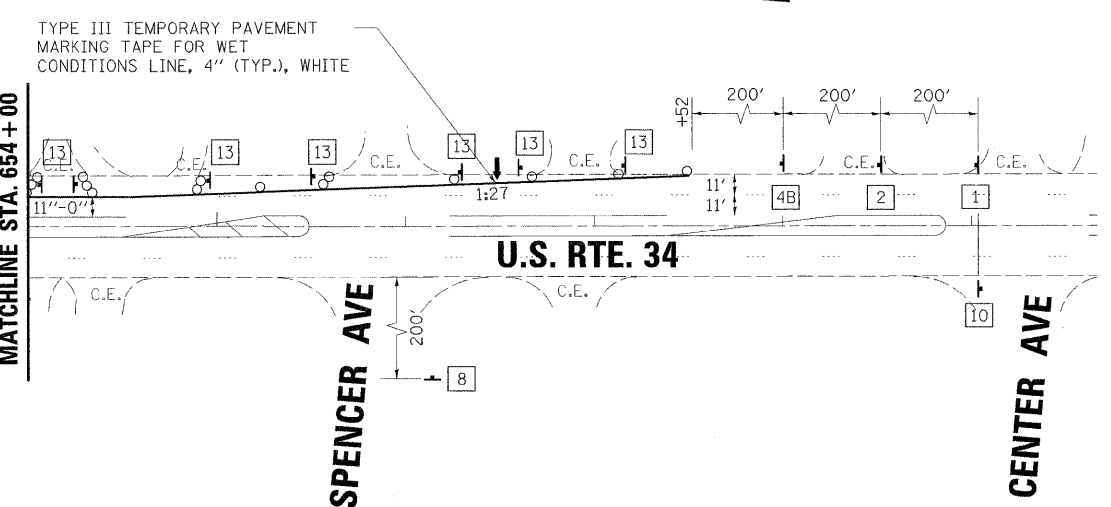
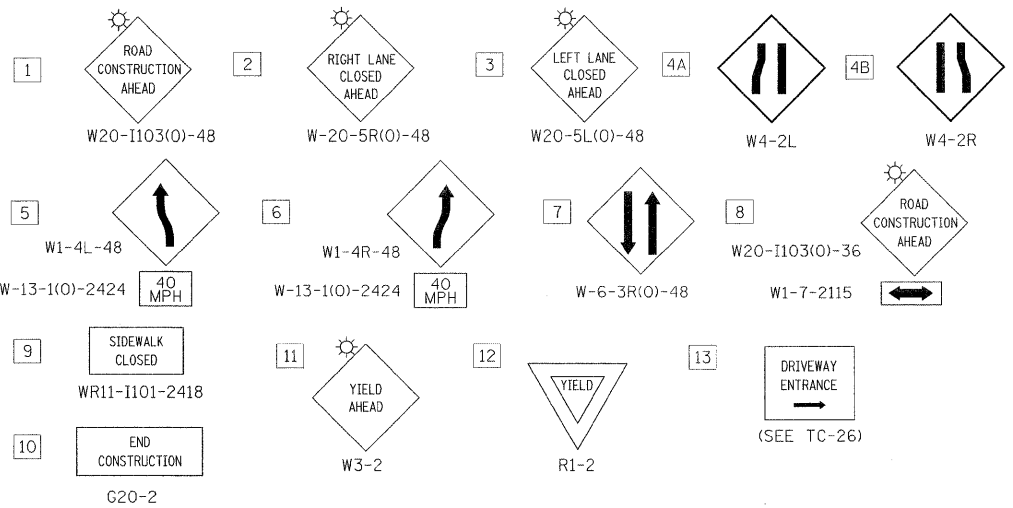
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SHEET NO. 11 OF 53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	11
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B92	



- LEGEND**
- WORK AREA
 - SIGN
 - TYPE II BARRICADE
 - ⊙ 50' C-C ON TANGENT
 - ⊙ 25' C-C ON TAPERS (TYP.)
 - FLEXIBLE DELINEATORS
 - ⊙ 25' C-C
 - TYPE III BARRICADE WITH FLASHING LIGHTS
 - ARROW BOARD
 - TEMPORARY CONCRETE BARRIER, RELOCATE
 - IMPACT ATTENUATOR, RELOCATE (NON REDIRECTIVE), TEST LEVEL 3



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 USER NAME = jbraney
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200 West Front Street
Wheaton, IL 60187

DESIGNED -	REVISOR -
DRAWN - MH	REVISIONS -
CHECKED - MK	REVISIONS -
DATE - 8/7/09	REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

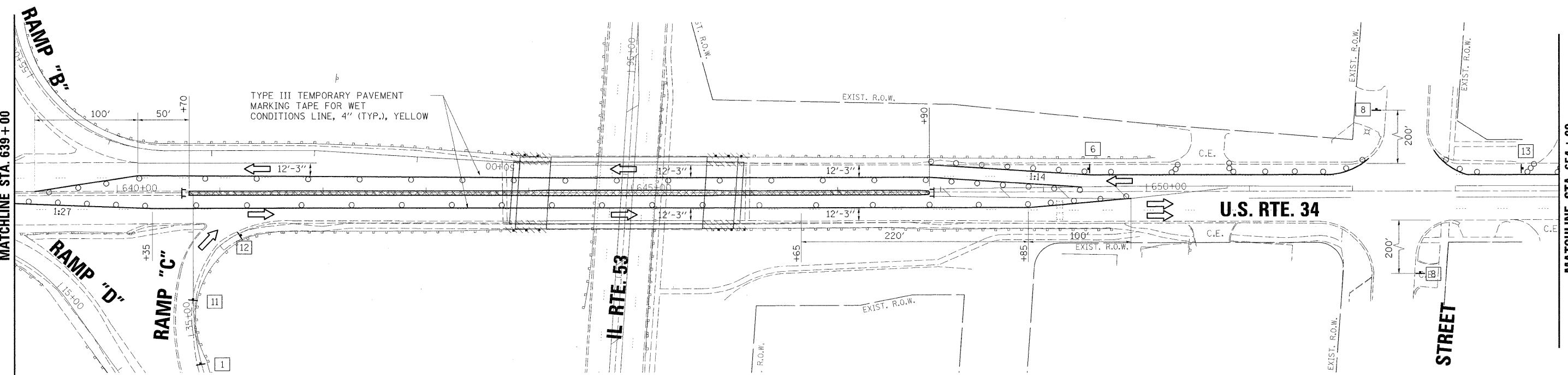
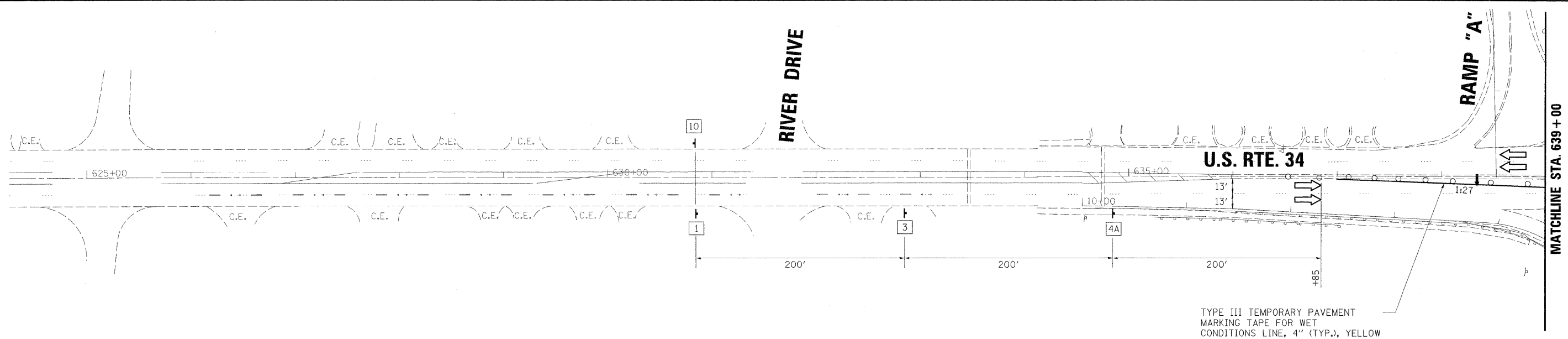
**US RTE. 34 OVER IL RTE. 53
STRUCTURE NO. 022-0033**

**MAINTENANCE OF TRAFFIC
US RTE 34 STAGE II**

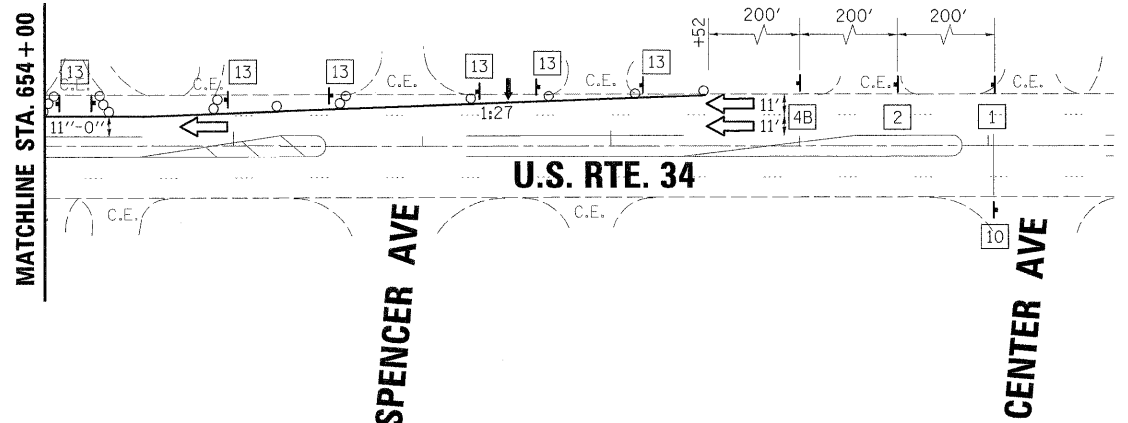
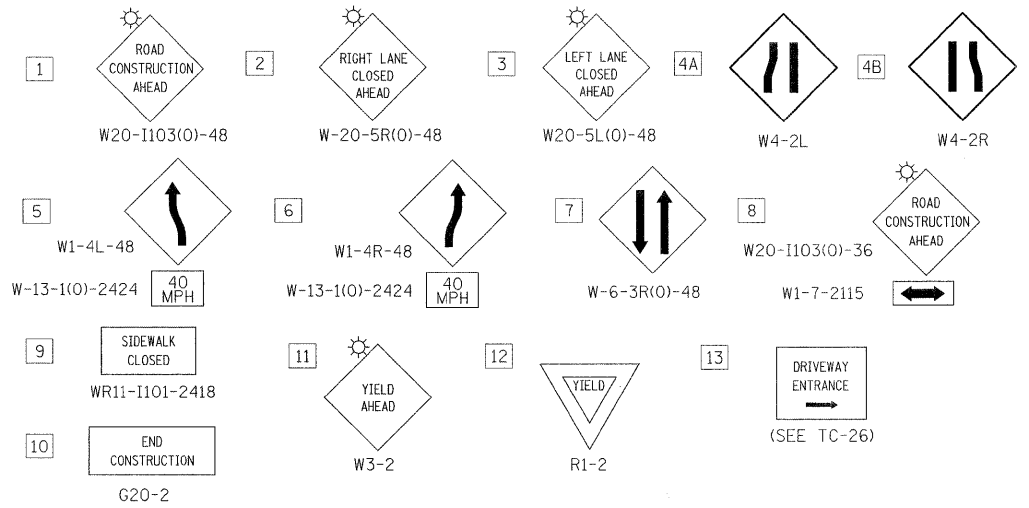
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	12
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B92	

SCALE: 1/8"=1'-0" SHEET NO. 12 OF 53 SHEETS

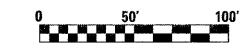
STA. _____ TO STA. _____



- LEGEND**
- WORK AREA
 - SIGN
 - TYPE II BARRICADE
 - o 50' C-C ON TANGENT
 - o 25' C-C ON TAPERS (TYP.)
 - FLEXIBLE DELINEATORS
 - o 25' C-C
 - TYPE III BARRICADE WITH FLASHING LIGHTS
 - ARROW BOARD



PLOT DATE = 8/7/2009
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 Wheaton, IL 60187

DESIGNED -	REVISOR -
DRAWN - MH	REVISIONS -
CHECKED - MK	REVISIONS -
DATE - 8/7/09	REVISIONS -

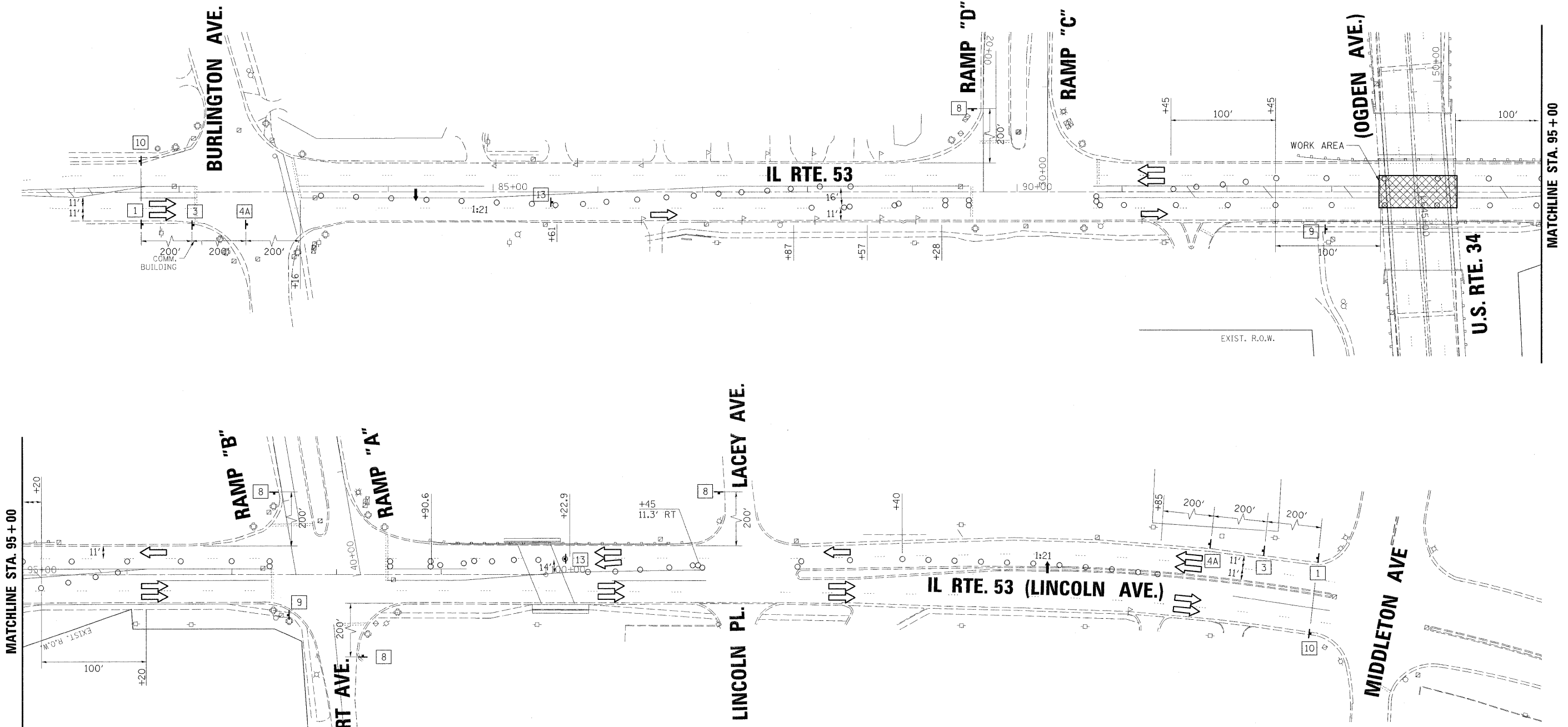
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033**

**MAINTENANCE OF TRAFFIC
 US RTE 34 STAGE III**

F.A.P. RTE. 311	SECTION 10HB-R	COUNTY	TOTAL SHEETS 53	SHEET NO. 13
DUPAGE			CONTRACT NO. 60B92	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: 1/8"=1'-0" SHEET NO. 13 OF 53 SHEETS STA. _____ TO STA. _____



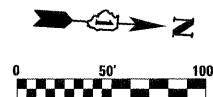
- LEGEND**
- WORK AREA
 - SIGN
 - TYPE II BARRICADE
 - ⊙ 50' C-C ON TANGENT
 - ⊙ 25' C-C ON TAPERS (TYP.)
 - ARROW BOARD

- 1 ROAD CONSTRUCTION AHEAD
W20-1103(O)-48
- 2 RIGHT LANE CLOSED AHEAD
W-20-5R(O)-48
- 3 LEFT LANE CLOSED AHEAD
W20-5L(O)-48
- 4A W4-2L
- 4B W4-2R
- 5 40 MPH
W1-4L-48
- 6 40 MPH
W1-4R-48
- 7 40 MPH
W-6-3R(O)-48
- 8 ROAD CONSTRUCTION AHEAD
W20-1103(O)-36
W1-7-2115
- 9 SIDEWALK CLOSED
WR11-1101-2418
- 10 END CONSTRUCTION
G20-2
- 11 YIELD AHEAD
W3-2
- 12 YIELD
R1-2
- 13 ** LEFT TURN LANE
R3-1100L-2424
M6-2L21-2115

NOTE: TRAFFIC CONTROL ALONG IL RTE 53 SHALL BE IN PLACE ONLY DURING OFF-PEAK HOURS.

** SEE DISTRICT 1 STANDARD TC14 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

8/7/2009
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 #PLOT SCALE#
 USER NAME
 MODEL



rjngroup
 Excellence through Ownership

200 West Front Street
 Wheaton, IL 60187

DESIGNED -	REVISED -
DRAWN - MH	REVISED -
CHECKED - MK	REVISED -
DATE - 8/7/09	REVISED -

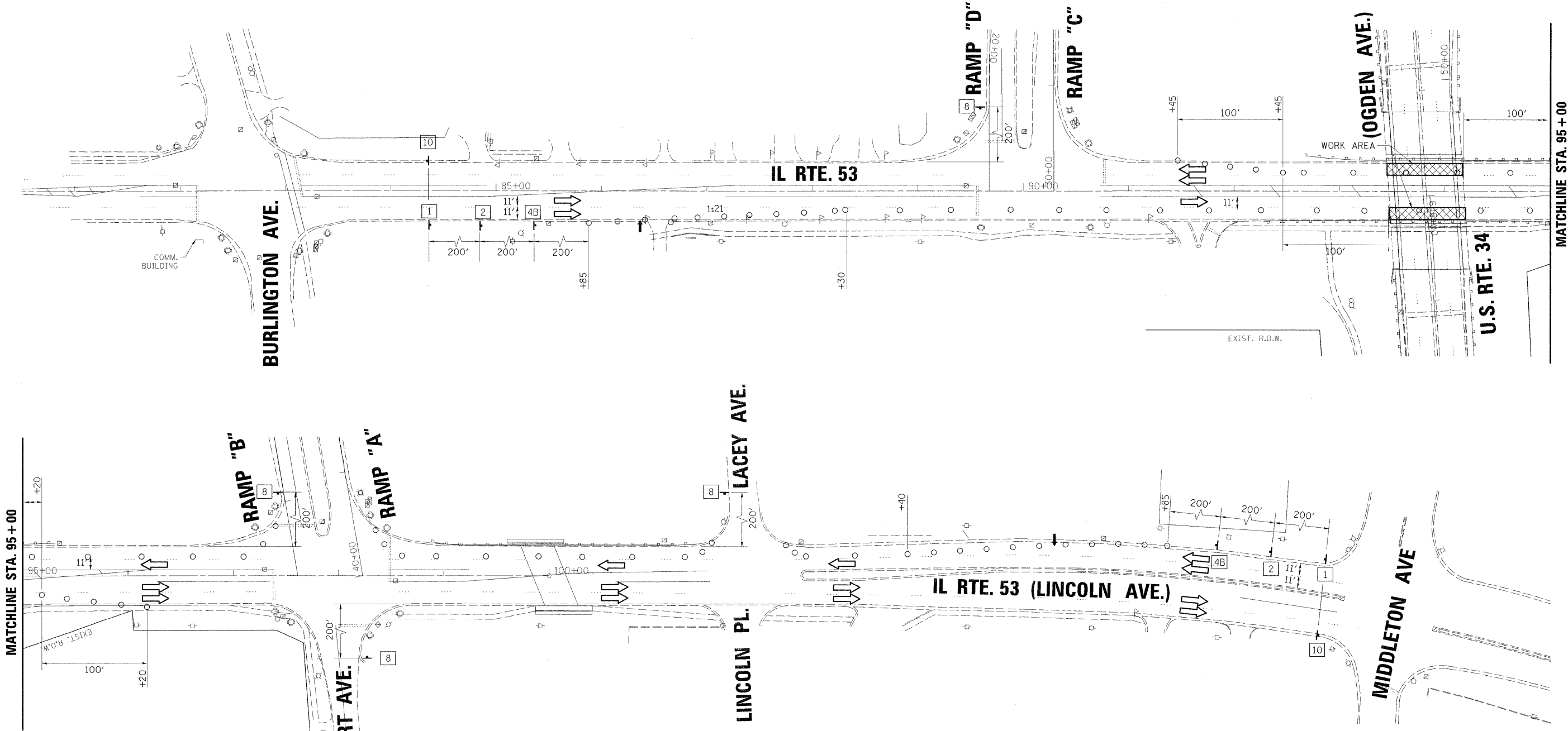
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033**

**MAINTENANCE OF TRAFFIC
 IL 53 INSIDE LANE CLOSURE**

SCALE: 1:50 SHEET NO. 14 OF 53 SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	14
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B92	



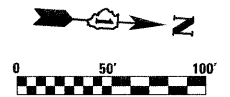
- LEGEND**
- WORK AREA
 - SIGN
 - TYPE II BARRICADE
 - ⊙ 50' C-C ON TANGENT
 - ⊙ 25' C-C ON TAPERS (TYP.)
 - ARROW BOARD

- 1 ROAD CONSTRUCTION AHEAD
W20-1103(O)-48
- 2 RIGHT LANE CLOSED AHEAD
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- 3 LEFT LANE CLOSED AHEAD
W20-5L(O)-48
- 4A W4-2L
- 4B W4-2R
- 5 W1-4L-48
- 6 W1-4R-48
- 7 W-6-3R(O)-48
- 8 ROAD CONSTRUCTION AHEAD
W20-1103(O)-36
- 9 SIDEWALK CLOSED
WR11-1101-2418
- 10 END CONSTRUCTION
G20-2
- 11 YIELD AHEAD
W3-2
- 12 YIELD
R1-2
- 13 LEFT TURN LANE
R3-1100L-2424
M6-2L21-2115

NOTE: TRAFFIC CONTROL ALONG IL RTE 53 SHALL BE IN PLACE ONLY DURING OFF-PEAK HOURS.

** SEE DISTRICT 1 STANDARD TC14 TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

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 MODEL =



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 Wheaton, IL 60187

DESIGNED -	REVISD -
DRAWN - MH	REVISD -
CHECKED - MK	REVISD -
DATE - 8/7/09	REVISD -

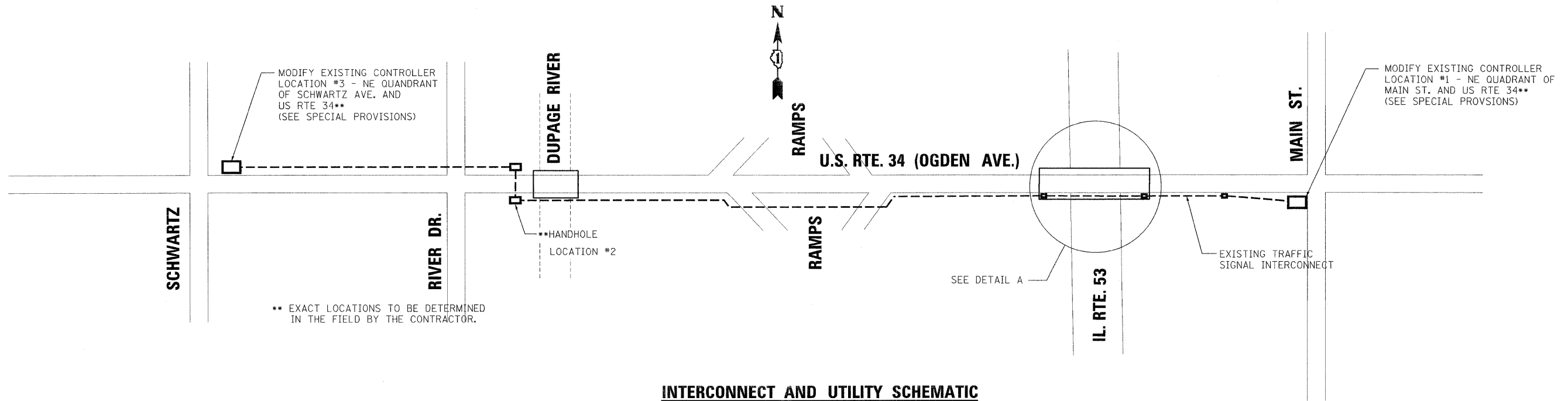
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US RTE. 34 OVER IL RTE. 53
 STRUCTURE NO. 022-0033**

**MAINTENANCE OF TRAFFIC
 IL 53 OUTSIDE LANE CLOSURE**

F.A.P. RTE. 311	SECTION 10HB-R	COUNTY	TOTAL SHEETS 53	SHEET NO. 15
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: 1:50 SHEET NO. 15 OF 53 SHEETS STA. _____ TO STA. _____



INTERCONNECT AND UTILITY SCHEMATIC

NOT TO SCALE

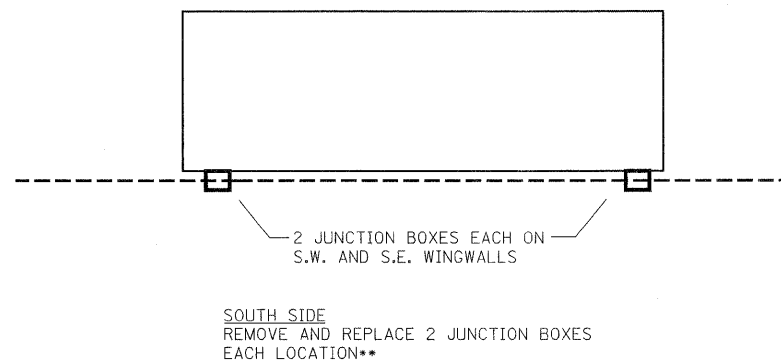
NOTES

- EXISTING FIBER OPTIC CABLE SHALL BE PULLED OUT OF THE EXISTING CONDUIT, STARTING AT THE EXISTING CONTROLLER BOX AT U.S. 34 AND MAIN STREET (LOCATION #1), ALL THE WAY TO THE FIRST HANDHOLE WEST OF THE BRIDGE (LOCATION #2). THIS CABLE SHALL BE SPOOLED UP NEAR LOCATION #2 AND PROTECTED ON-SITE FOR THE DURATION OF THE BRIDGE CONSTRUCTION. AFTER BRIDGE CONSTRUCTION IS COMPLETED AND NEW JUNCTION BOXES AND CONDUIT ARE IN PLACE, THE EXISTING CABLE SHALL BE REINSTALLED IN THE CONDUIT, ALL THE WAY BACK TO THE CONTROLLER BOX AT LOCATION #1. COST INCLUDED WITH "REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT".
- REMOVAL OF FIBER OPTIC CABLE SHALL NOT BEGIN UNTIL ALL COMPONENTS OF THE TEMPORARY WIRELESS INTERCONNECT SYSTEM ARE IN PLACE AND OPERATIONAL.
- ESTIMATED LENGTH OF INTERCONNECT CABLE TO BE REMOVED AND REINSTALLED = 2,950 FEET.
- A NOMINAL QUANTITY OF 15 FEET OF NEW CONDUIT HAS BEEN PROVIDED AT EACH OF THE TWO JUNCTION BOX LOCATIONS, IN THE EVENT THAT CONDUIT IN THESE LOCATIONS NEEDS TO BE REMOVED AND REPLACED. REMOVAL OF THE EXISTING ROADWAY LIGHTING CONDUIT, IF NEEDED, SHALL BE PAID FOR AS "ROADWAY LIGHTING CONDUIT REMOVAL". REMOVAL OF THE EXISTING TRAFFIC SIGNAL INTERCONNECT CONDUIT, IF NEEDED, IS INCLUDED WITH THE CONTRACT UNIT PRICE BID FOR "JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18"x10"x10" ".
- ANY HARDWARE OR ACCESSORIES REQUIRED TO ATTACH THE NEW CONDUIT TO THE NEW JUNCTION BOXES OR TO THE EXISTING CONDUIT WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED WITH THE CONTRACT UNIT PRICE BID FOR "CONDUIT ATTACHED TO STRUCTURE, 2 1/2" DIA., GALVANIZED STEEL".
- SEE TRAFFIC SIGNAL SPECIFICATIONS AND SPECIAL PROVISIONS FOR MORE INFORMATION.

BILL OF MATERIAL

81100700	CONDUIT ATTACHED TO STRUCTURE, 2 1/2" DIA., GALVANIZED STEEL	FOOT	30
81300835	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18"x18"x10"	EACH	2
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
89502200	MODIFY EXISTING CONTROLLER	EACH	2
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	2950
X0325938	TEMPORARY WIRELESS INTERCONNECT, COMPLETE	L SUM	1
XX003079	REMOVE JUNCTION BOX	EACH	2

• LARGE BOXES ARE FOR FIBER OPTIC INTERCONNECT CABLE;
SMALL BOXES ARE FOR ROADWAY LIGHTING CABLE



DETAIL A

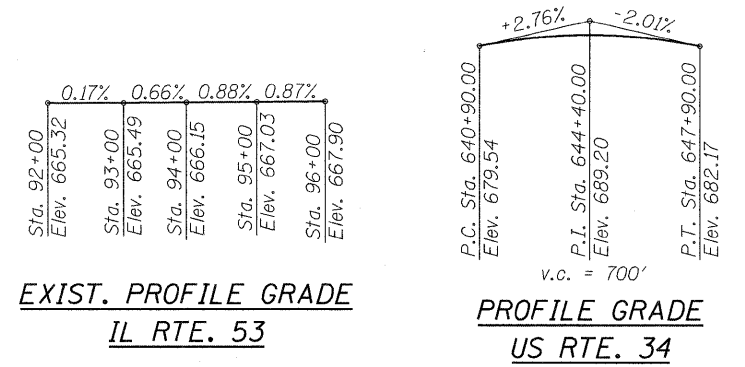
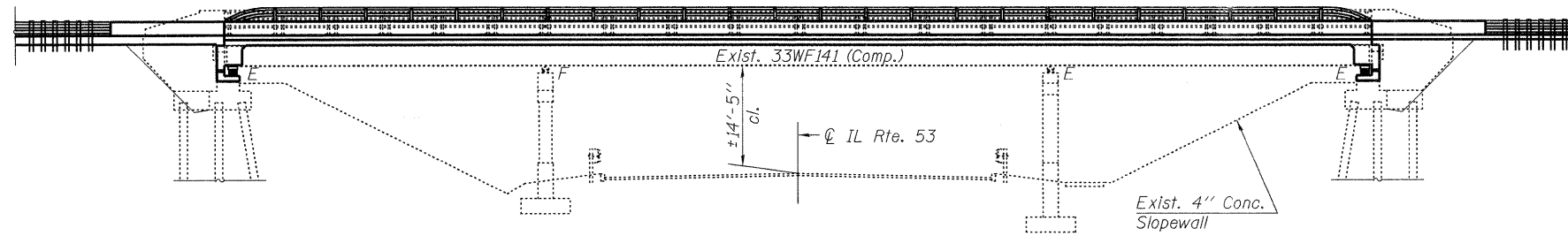
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 USER NAME = jersony
 MODEL = default

Benchmark: Square cut in W. end of Concrete Wingwall, N.W. corner of Rt. 34 bridge over Rt. 53. Elev. 685.16.

Existing Structure: S.N. 022-0033 built in 1960 as F.A.P. Route 311, Section 10-HB at Station 644+95.23. The superstructure consists of a R.C. deck 153'-4" back to back abutments by 71'-8" wide supported on three-span continuous steel wide flange beams, on stub abutments and multi-column piers. Traffic shall be maintained utilizing stage construction.

Existing Protective Shield and Bridge Railing to be Salvaged. See General Notes.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION

DESIGN SPECIFICATIONS

2002 AASHTO (17th Ed.)

LOADING HS20-44

Allow 50 #/Sq.Ft. for future wearing surface.

SEISMIC DATA

Seismic performance category (S.P.C.) = A
Bedrock acceleration coefficient (A) = .04g
Site coefficient (s) = 1.0

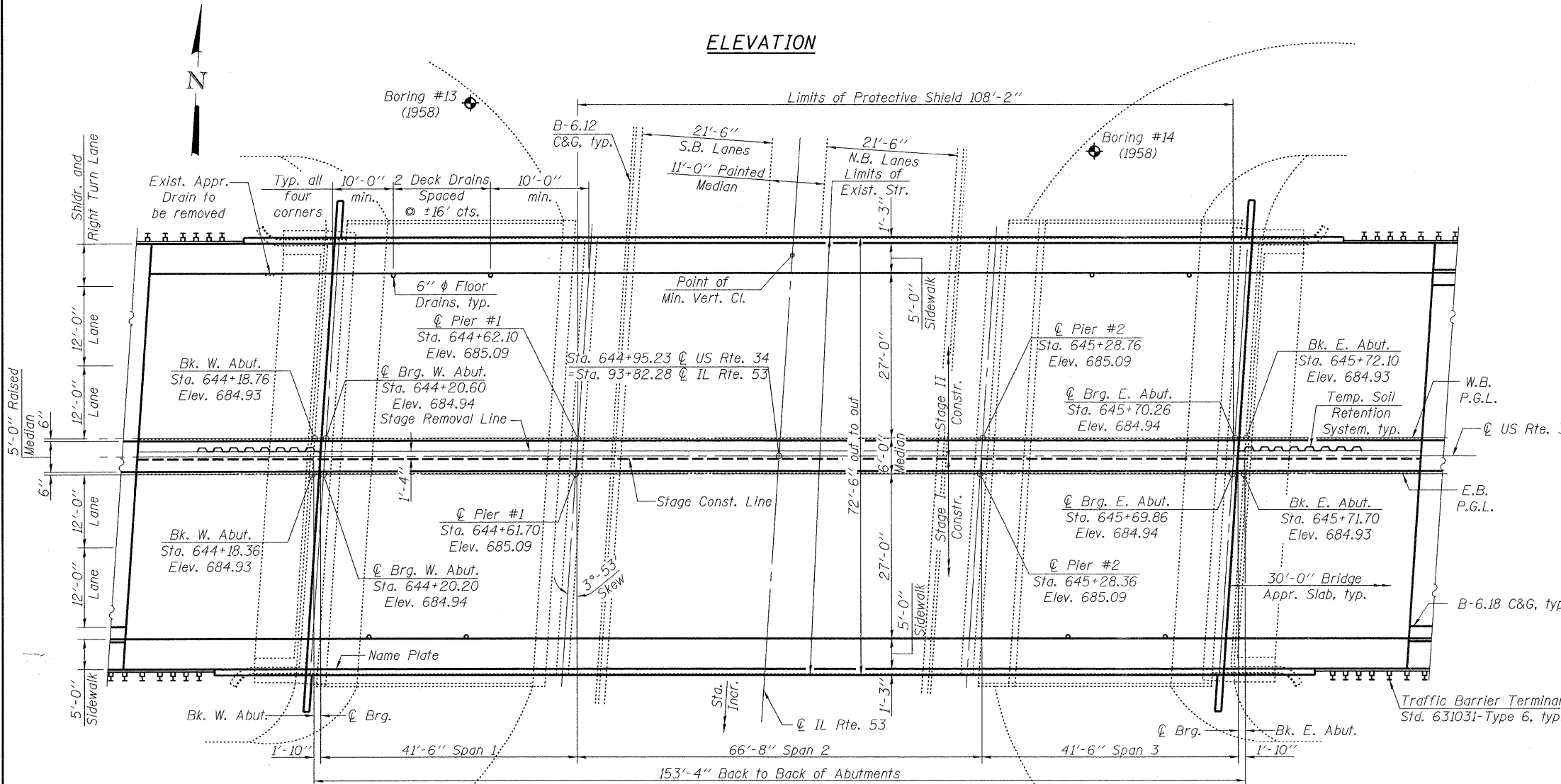
DESIGN STRESSES

NEW FIELD UNITS

$f_c = 3,500$ psi
 $f_y = 60,000$ psi (Reinf.)
 $f_y = 36,000$ psi (M270 Grade 36)

EXISTING CONSTRUCTION

$f_c = 1,400$ psi
 $f_s = 20,000$ psi (Reinf.)
 $f_s = 18,000$ psi



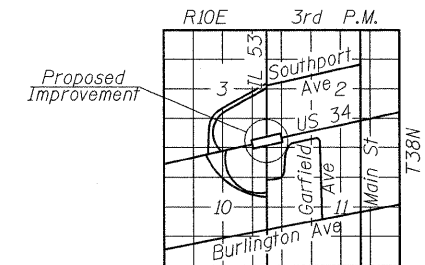
PLAN

STATION 644+95.23
REBUILT 20 BY
STATE OF ILLINOIS
F.A.P. RT. 311 SEC 10HB-R
LOADING HS-20
STRUCTURE NO. 022-0033

NAME PLATE

See Std. 515001

Existing Name Plate shall be cleaned and relocated next to new Name Plate. Cost included with Name Plates.



LOCATION SKETCH



Expires 11-30-10

APPROVED
FOR STRUCTURAL ADEQUACY ONLY

Robert E. Anderson (TND)
ENGINEER OF BRIDGES AND STRUCTURES

GENERAL PLAN AND ELEVATION
US RTE. 34 OVER IL RTE. 53
FAP 311, SECTION 10HB-R
DUPAGE COUNTY
STATION 644+95.23
STRUCTURE NO. 022-0033

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

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200 West Front Street
Wheaton, IL 60187

SHEET NO. 1 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	311	10HB-R	Du Page	53	17
CONTRACT NO. 60B92					
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

GENERAL NOTES

Fasteners shall be AASHTO M164 Type I, mechanically galvanized bolts.
Bolts 7/8" φ, holes 15/16" φ, unless otherwise noted.
No field welding is permitted except as specified in the contract documents.
Reinforcement bars shall conform to the requirements of ASTM A 706
Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.
Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete, including existing diaphragms at abutments. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Bearing seat surfaces shall be constructed or adjusted to their designated elevations within a tolerance of 1/8 inch (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.

Protective shield may exist in some locations. The Contractor with the approval of the Engineer may re-use the existing protective shield. If protective shield is to be re-utilized, the Contractor shall evaluate the existing protective shield, and demonstrate through calculations sealed by an Illinois Licensed Structural Engineer that the existing system meets or exceeds the design requirements specified in Article 501.03 of the Standard Specifications. Supplementing and/or replacement of the existing shield may be required to satisfy the Standard Specifications. The cost of maintaining and adjusting the protective shield shall be included in the cost of protective shield.

The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. See Special Provision for "Cleaning and Painting New Metal Structures".

Cleaning and painting of the existing fascia beams shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures." The exterior and bottom flange of the fascia beams shall be cleaned per Near White Blast Cleaning-SSPC-SP10. These surfaces shall be painted according to the requirements of Paint System 1-OZ/E/U. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Black, Munsell No. N-1.

The existing protective shield shall be salvaged when no longer required. The salvaged protective shield shall be delivered to IDOT District One Bridge Office, 1101 Blesterfield Road, Elk Grove Village, Illinois, 60007, telephone 847-956-1501. The cost of salvaging and delivering the existing protective shield shall be included in the cost of Protective Shield.

The existing handrail and posts shall be salvaged when no longer required. The salvaged handrail and posts shall be delivered to IDOT District One Bridge Office, 1101 Blesterfield Road, Elk Grove Village, Illinois, 60007, telephone 847-956-1501. The cost of salvaging and delivering the existing handrail and posts shall be included in the cost of Removal of Existing Concrete Deck.

The SSPC-QP1 and SSPC-QP2 Painting Contractor Certifications will be required for this bridge.

The Contractor shall submit Structural Assessment Report(s) as required for the Contractor's means and methods of construction. See Special Provisions.

Current Ratings on File for Existing Structure:

Inventory: HS 24.4
Operating: HS 36.7
Live Load Restrictions: No

Inventory and Operating Ratings and Live Load Restrictions are provided for information only. Inventory and Operating Ratings are based on HS loading and configuration. Live Load Restrictions are based on Illinois legal loads and configurations. The Ratings and Live Load Restrictions are not necessarily representative of capacities to support the Contractor's equipment.

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

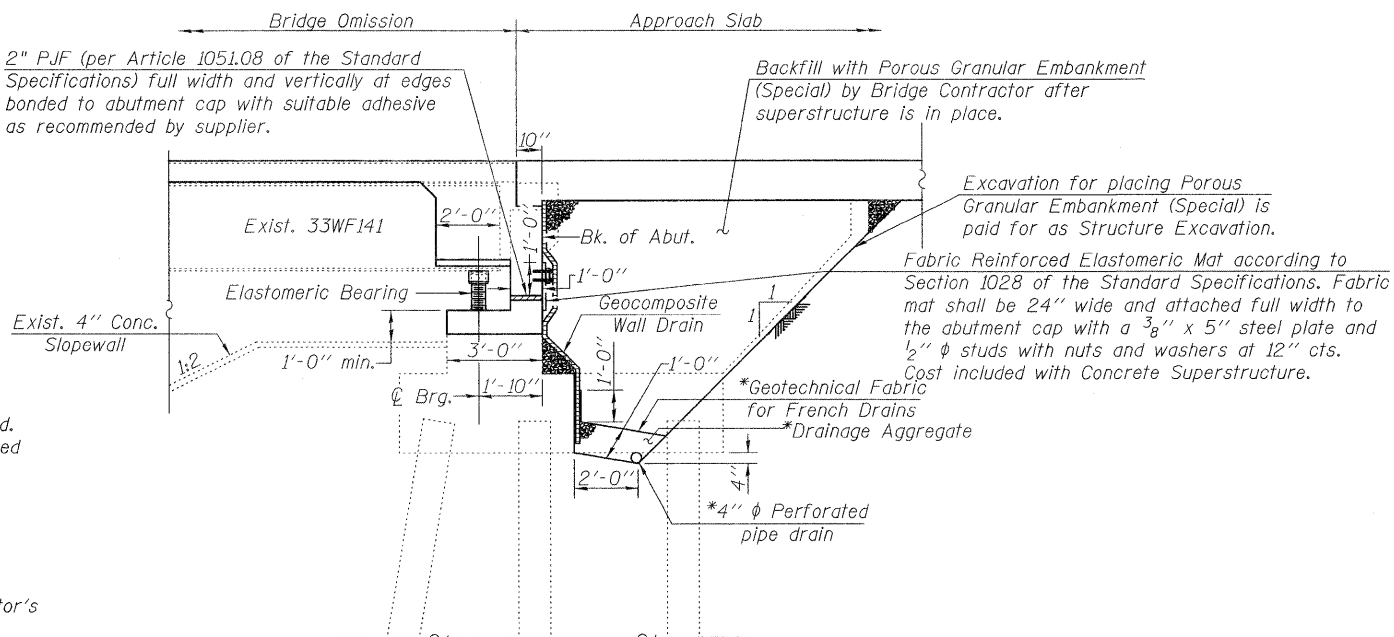
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Porous Granular Embankment, Special	Cu Yd	432		432
Protective Coat	Sq Yd	1825		1825
Concrete Removal	Cu Yd		60.6	60.6
Removal of Existing Concrete Deck	Each	1		1
Protective Shield	Sq Yd	871		871
Structure Excavation	Cu Yd		489.4	489.4
Floor Drains	Each	8		8
Concrete Structures	Cu Yd		73.6	73.6
Concrete Superstructure	Cu Yd	713.1		713.1
Cleaning and Painting Steel Bridge	L Sum	1		1
Bridge Deck Grooving	Sq Yd	1200		1200
Stud Shear Connectors	Each	5,664		5,664
Jacking and Cribbing	Each	24		24
Reinforcement Bars, Epoxy Coated	Pound	134,240	3,200	137,440
Bar Splacers	Each	785	12	797
Name Plates	Each	1		1
Elastomeric Bearing Assembly, Type I	Each	24		24
Geocomposite Wall Drain	Sq Yd		177	177
Pipe Underdrains for Structures 4"	Foot		177	177
Slope Wall Removal	Sq Yd		5.6	5.6
Slope Wall, 4 Inch	Sq Yd		5.6	5.6
Aluminum Railing, Special	Foot	300		300
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq Ft		20	20
Temporary Soil Retention System	Sq Ft		164	164
Containment and disposal of non-lead paint cleaning residues	L Sum	1		1
Form Liner Textured Surface	Sq Ft	1284		1284
Staining Concrete Structures	Sq Yd	186.2		186.2
Furnishing and Erecting Structural Steel	Pound	330		330
Anchor Bolts, 1 Inch	Each	48		48
Epoxy Crack Injection	Foot		12	12

INDEX OF SHEETS

- General Plan and Elevation
- General Data
- Stage Construction Details
- Top of Slab Elevations - I
- Top of Slab Elevations - II
- Top of Slab Elevations - III
- Top of Slab Elevations - IV
- Top of Slab Elevations - V
- Top of Slab Elevations - VI
- West Approach Slab Elevations
- East Approach Slab Elevations
- Approach Slab Details - I
- Approach Slab Details - II
- Deck Plan
- Deck Sections and Details
- Diaphragm Details
- Parapet Details
- Bridge Rail Details
- Framing Plan
- Beam Moment & Reaction Tables
- Bearing Details
- Abutment Details - I
- Abutment Details - II
- Pier Repairs
- Slope Wall Repairs
- Bar Splicer Details
- Temporary Concrete Barrier Details
- Soil Boring Logs



SECTION THRU SEMI-INTEGRAL ABUTMENT

(Horiz. dim. @ Rt. L's)

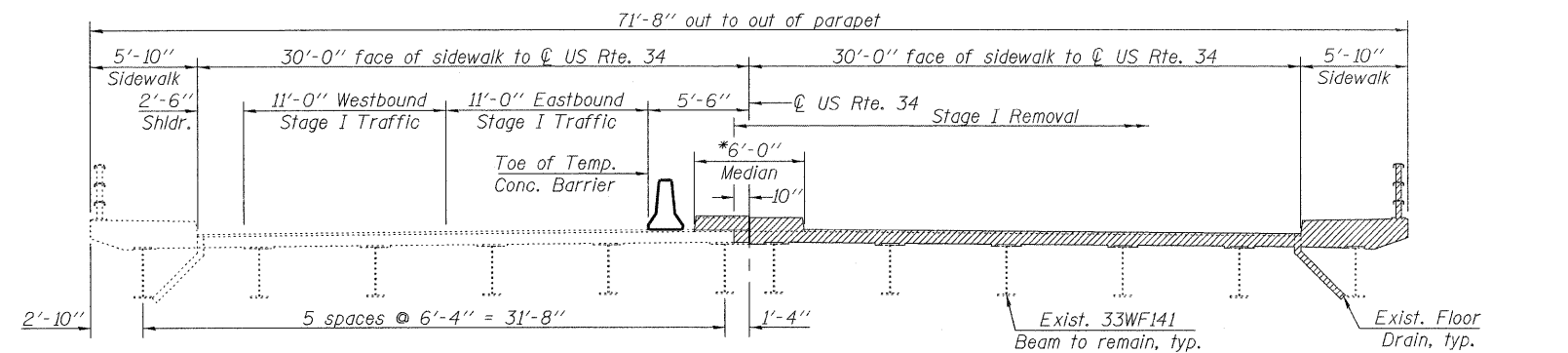
*Included in the cost of Pipe Underdrains for Structures.

Note:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

GENERAL DATA
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 2 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	10HB-R	Du Page	53	18
CONTRACT NO. 60B92						
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT						

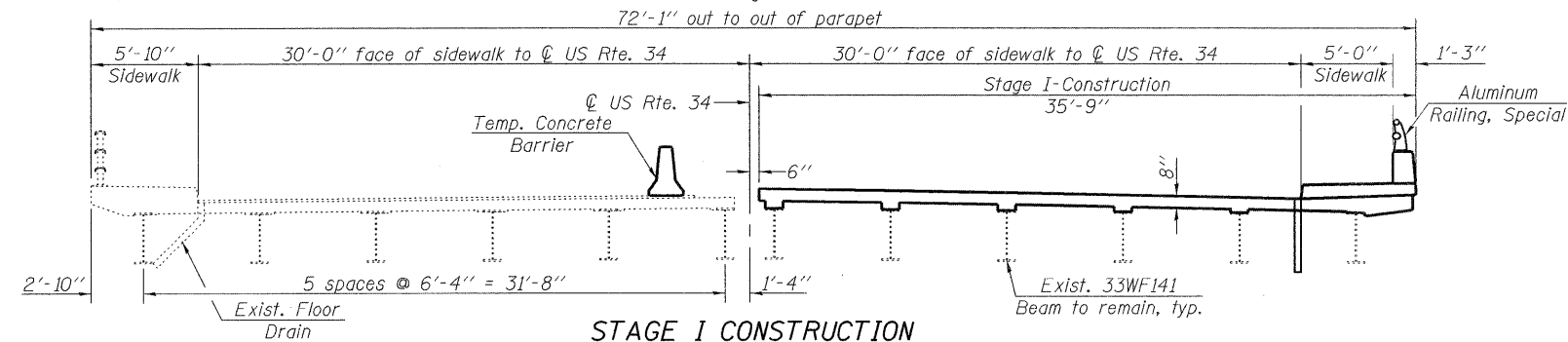
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



*Median to be removed during Stage I Removal.

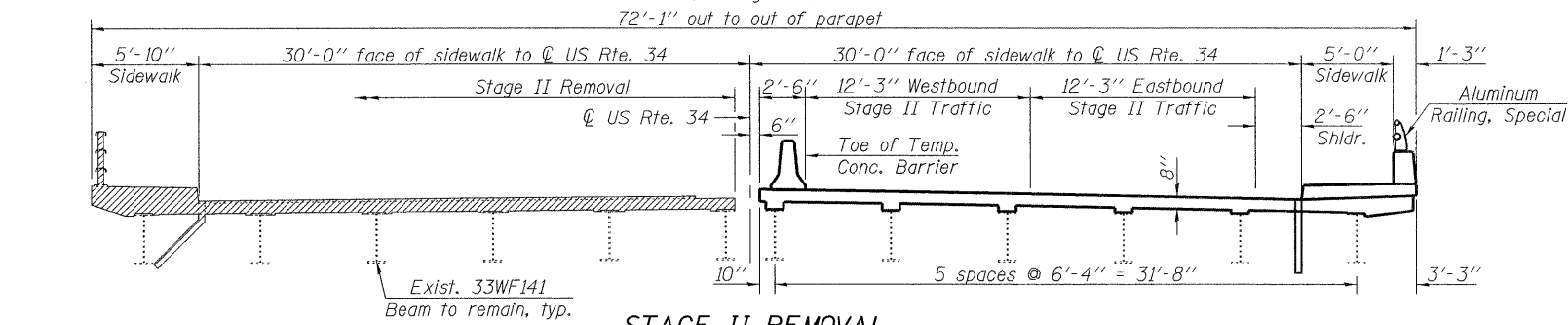
STAGE I REMOVAL

(Looking East)



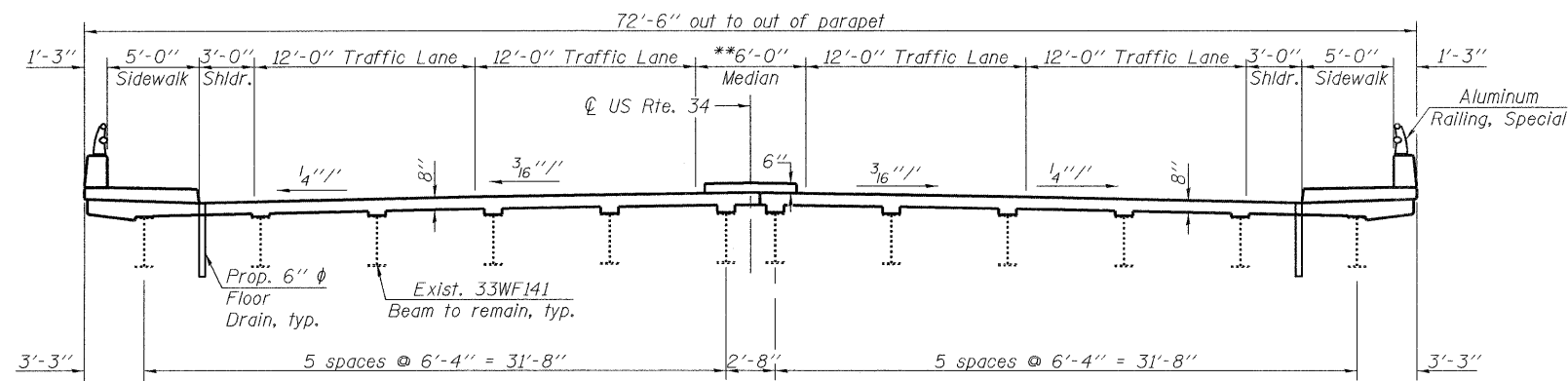
STAGE I CONSTRUCTION

(Looking East)



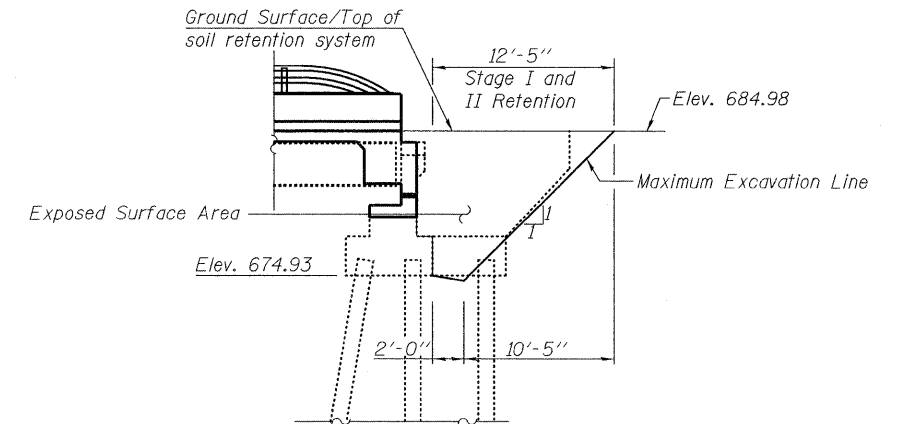
STAGE II REMOVAL

(Looking East)



STAGE II CONSTRUCTION

(Looking East)



TEMPORARY SOIL RETENTION SYSTEM

Slopes and distances shown parallel to roadway centerline.

NOTES:

- Removal of existing bridge rail and median is included with Removal of Existing Concrete Deck.
- See Sheet 27 of 27 for Temporary Concrete Barrier Details.
- Pay Item for "Temporary Concrete Barrier" is included with Roadway Plans.
- Stage Removal Line and Stage Construction Line at Abutments vary from what is shown on this sheet. See Sheets 22 and 23 of 27 for details.
- A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.

**Raised median to be constructed after completion of Stage II Construction.

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

LEGEND:

Removal of Existing Concrete Deck

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Wheaton, IL 60187

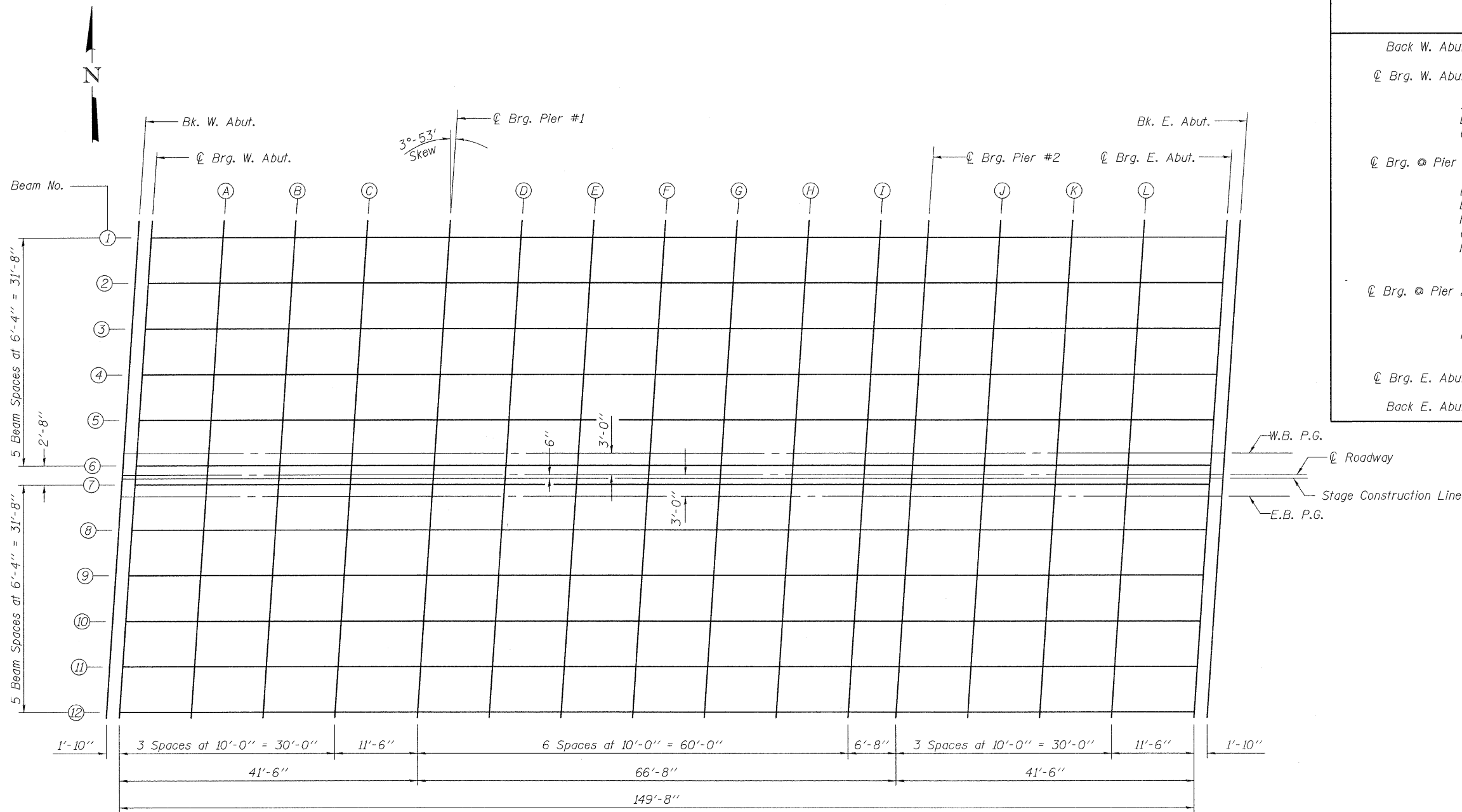
SHEET NO. 3
27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	Du Page	53	19
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

**STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 022-0033**

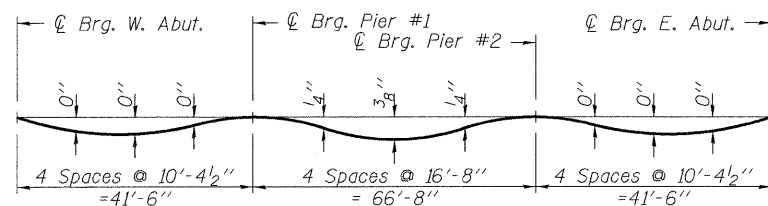
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM 1



Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+20.80	-33.00	684.38	684.38
☉ Brg. W. Abut.	644+22.64	-33.00	684.39	684.39
A	644+32.64	-33.00	684.43	684.44
B	644+42.64	-33.00	684.47	684.48
C	644+52.64	-33.00	684.51	684.50
☉ Brg. ☉ Pier 1	644+64.14	-33.00	684.53	684.53
D	644+74.14	-33.00	684.55	684.57
E	644+84.14	-33.00	684.56	684.60
F	644+94.14	-33.00	684.57	684.61
G	645+04.14	-33.00	684.56	684.61
H	645+14.14	-33.00	684.55	684.58
I	645+24.14	-33.00	684.54	684.55
☉ Brg. ☉ Pier 2	645+30.80	-33.00	684.52	684.52
J	645+40.80	-33.00	684.50	684.49
K	645+50.80	-33.00	684.46	684.46
L	645+60.80	-33.00	684.42	684.42
☉ Brg. E. Abut.	645+72.30	-33.00	684.36	684.36
Back E. Abut.	645+74.14	-33.00	684.35	684.35

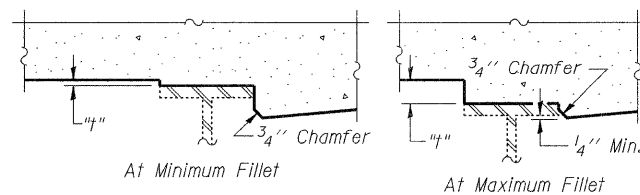
PLAN



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on the following sheets.



FILLET HEIGHTS

To determine "t": After all existing concrete has been removed, elevations of the top flanges of the beams shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on the following sheets, minus slab thickness, equals the fillet heights "t" above top flange of beams.

TOP OF SLAB ELEVATIONS - I
STRUCTURE NO. 022-0033

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 4 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	10HB-R	Du Page	53	20
				CONTRACT NO. 60B92		
FED. ROAD DIST. NO. _				ILLINOIS FED. AID PROJECT		

8/7/2009 ks:\112255\0\structures\final_plans\Deck Elevations.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+20.37	-26.67	684.51	684.51
☉ Brg. W. Abut.	644+22.21	-26.67	684.52	684.52
A	644+32.21	-26.67	684.56	684.57
B	644+42.21	-26.67	684.60	684.61
C	644+52.21	-26.67	684.64	684.64
☉ Brg. ☉ Pier 1	644+63.71	-26.67	684.67	684.67
D	644+73.71	-26.67	684.68	684.70
E	644+83.71	-26.67	684.69	684.73
F	644+93.71	-26.67	684.70	684.74
G	645+03.71	-26.67	684.70	684.74
H	645+13.71	-26.67	684.69	684.72
I	645+23.71	-26.67	684.67	684.68
☉ Brg. ☉ Pier 2	645+30.37	-26.67	684.66	684.66
J	645+40.37	-26.67	684.63	684.63
K	645+50.37	-26.67	684.59	684.60
L	645+60.37	-26.67	684.55	684.56
☉ Brg. E. Abut.	645+71.87	-26.67	684.50	684.50
Back E. Abut.	645+73.71	-26.67	684.49	684.49

BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+19.94	-20.33	684.64	684.64
☉ Brg. W. Abut.	644+21.78	-20.33	684.65	684.65
A	644+31.78	-20.33	684.69	684.70
B	644+41.78	-20.33	684.73	684.74
C	644+51.78	-20.33	684.77	684.77
☉ Brg. ☉ Pier 1	644+63.28	-20.33	684.80	684.80
D	644+73.28	-20.33	684.81	684.83
E	644+83.28	-20.33	684.83	684.86
F	644+93.28	-20.33	684.83	684.88
G	645+03.28	-20.33	684.83	684.87
H	645+13.28	-20.33	684.82	684.85
I	645+23.28	-20.33	684.80	684.82
☉ Brg. ☉ Pier 2	645+29.94	-20.33	684.79	684.79
J	645+39.94	-20.33	684.76	684.76
K	645+49.94	-20.33	684.73	684.73
L	645+59.94	-20.33	684.69	684.69
☉ Brg. E. Abut.	645+71.44	-20.33	684.63	684.63
Back E. Abut.	645+73.28	-20.33	684.62	684.62

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+19.51	-14.00	684.76	684.76
☉ Brg. W. Abut.	644+21.35	-14.00	684.77	684.77
A	644+31.35	-14.00	684.82	684.82
B	644+41.35	-14.00	684.86	684.86
C	644+51.35	-14.00	684.89	684.89
☉ Brg. ☉ Pier 1	644+62.85	-14.00	684.92	684.92
D	644+72.85	-14.00	684.94	684.96
E	644+82.85	-14.00	684.95	684.99
F	644+92.85	-14.00	684.96	685.00
G	645+02.85	-14.00	684.96	685.00
H	645+12.85	-14.00	684.95	684.98
I	645+22.85	-14.00	684.93	684.94
☉ Brg. ☉ Pier 2	645+29.51	-14.00	684.92	684.92
J	645+39.51	-14.00	684.89	684.89
K	645+49.51	-14.00	684.86	684.86
L	645+59.51	-14.00	684.82	684.82
☉ Brg. E. Abut.	645+71.01	-14.00	684.76	684.76
Back E. Abut.	645+72.85	-14.00	684.75	684.75

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

TOP OF SLAB ELEVATIONS - II
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 5	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	27 SHEETS	311	10HB-R	Du Page	53	21
			CONTRACT NO. 60B92			
		FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+19.08	-7.67	684.86	684.86
⊘ Brg. W. Abut.	644+20.92	-7.67	684.87	684.87
A	644+30.92	-7.67	684.92	684.92
B	644+40.92	-7.67	684.96	684.96
C	644+50.92	-7.67	684.99	684.99
⊘ Brg. ⊙ Pier 1	644+62.42	-7.67	685.02	685.02
D	644+72.42	-7.67	685.04	685.06
E	644+82.42	-7.67	685.05	685.08
F	644+92.42	-7.67	685.06	685.10
G	645+02.42	-7.67	685.05	685.10
H	645+12.42	-7.67	685.05	685.08
I	645+22.42	-7.67	685.03	685.04
⊘ Brg. ⊙ Pier 2	645+29.08	-7.67	685.02	685.02
J	645+39.08	-7.67	684.99	684.99
K	645+49.08	-7.67	684.96	684.96
L	645+59.08	-7.67	684.92	684.92
⊘ Brg. E. Abut.	645+70.58	-7.67	684.86	684.86
Back E. Abut.	645+72.42	-7.67	684.85	684.85

W.B. P.G.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.76	-3.00	684.93	684.93
⊘ Brg. W. Abut.	644+20.60	-3.00	684.94	684.94
A	644+30.60	-3.00	684.99	684.99
B	644+40.60	-3.00	685.03	685.03
C	644+50.60	-3.00	685.06	685.06
⊘ Brg. ⊙ Pier 1	644+62.10	-3.00	685.09	685.09
D	644+72.10	-3.00	685.11	685.13
E	644+82.10	-3.00	685.12	685.16
F	644+92.10	-3.00	685.13	685.17
G	645+02.10	-3.00	685.13	685.17
H	645+12.10	-3.00	685.12	685.15
I	645+22.10	-3.00	685.10	685.12
⊘ Brg. ⊙ Pier 2	645+28.76	-3.00	685.09	685.09
J	645+38.76	-3.00	685.06	685.06
K	645+48.76	-3.00	685.03	685.03
L	645+58.76	-3.00	684.99	684.99
⊘ Brg. E. Abut.	645+70.26	-3.00	684.94	684.94
Back E. Abut.	645+72.10	-3.00	684.93	684.93

BEAM 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.65	-1.33	684.96	684.96
⊘ Brg. W. Abut.	644+20.49	-1.33	684.97	684.97
A	644+30.49	-1.33	685.01	685.02
B	644+40.49	-1.33	685.05	685.06
C	644+50.49	-1.33	685.09	685.09
⊘ Brg. ⊙ Pier 1	644+61.99	-1.33	685.12	685.12
D	644+71.99	-1.33	685.14	685.16
E	644+81.99	-1.33	685.15	685.18
F	644+91.99	-1.33	685.16	685.20
G	645+01.99	-1.33	685.15	685.19
H	645+11.99	-1.33	685.15	685.18
I	645+21.99	-1.33	685.13	685.14
⊘ Brg. ⊙ Pier 2	645+28.65	-1.33	685.12	685.12
J	645+38.65	-1.33	685.09	685.09
K	645+48.65	-1.33	685.06	685.06
L	645+58.65	-1.33	685.02	685.02
⊘ Brg. E. Abut.	645+70.15	-1.33	684.96	684.96
Back E. Abut.	645+71.99	-1.33	684.95	684.95

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

TOP OF SLAB ELEVATIONS - III
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 6 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	10HB-R	Du Page	53	22
				CONTRACT NO. 60B92		
		FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.56	0.00	684.98	684.98
☉ Brg. W. Abut.	644+20.40	0.00	684.99	684.99
A	644+30.40	0.00	685.03	685.04
B	644+40.40	0.00	685.07	685.08
C	644+50.40	0.00	685.11	685.11
☉ Brg. ☉ Pier 1	644+61.90	0.00	685.14	685.14
D	644+71.90	0.00	685.16	685.18
E	644+81.90	0.00	685.17	685.20
F	644+91.90	0.00	685.18	685.22
G	645+01.90	0.00	685.17	685.22
H	645+11.90	0.00	685.17	685.20
I	645+21.90	0.00	685.15	685.16
☉ Brg. ☉ Pier 2	645+28.56	0.00	685.14	685.14
J	645+38.56	0.00	685.11	685.11
K	645+48.56	0.00	685.08	685.08
L	645+58.56	0.00	685.04	685.04
☉ Brg. E. Abut.	645+70.06	0.00	684.98	684.98
Back E. Abut.	645+71.90	0.00	684.97	684.97

STAGE CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.56	0.50	684.97	684.97
☉ Brg. W. Abut.	644+20.40	0.50	684.98	684.98
A	644+30.40	0.50	685.03	685.03
B	644+40.40	0.50	685.07	685.07
C	644+50.40	0.50	685.10	685.10
☉ Brg. ☉ Pier 1	644+61.90	0.50	685.13	685.13
D	644+71.90	0.50	685.15	685.17
E	644+81.90	0.50	685.16	685.20
F	644+91.90	0.50	685.17	685.21
G	645+01.90	0.50	685.17	685.21
H	645+11.90	0.50	685.16	685.19
I	645+21.90	0.50	685.13	685.13
☉ Brg. ☉ Pier 2	645+28.56	0.50	685.13	685.13
J	645+38.56	0.50	685.10	685.10
K	645+48.56	0.50	685.07	685.07
L	645+58.56	0.50	685.03	685.03
☉ Brg. E. Abut.	645+70.06	0.50	684.98	684.98
Back E. Abut.	645+71.90	0.50	684.97	684.97

BEAM 7

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.53	1.33	684.96	684.96
☉ Brg. W. Abut.	644+20.37	1.33	684.97	684.97
A	644+30.37	1.33	685.01	685.02
B	644+40.37	1.33	685.05	685.06
C	644+50.37	1.33	685.09	685.09
☉ Brg. ☉ Pier 1	644+61.87	1.33	685.12	685.12
D	644+71.87	1.33	685.14	685.16
E	644+81.87	1.33	685.15	685.18
F	644+91.87	1.33	685.16	685.20
G	645+01.87	1.33	685.15	685.19
H	645+11.87	1.33	685.15	685.18
I	645+21.87	1.33	685.13	685.14
☉ Brg. ☉ Pier 2	645+28.53	1.33	685.12	685.12
J	645+38.53	1.33	685.09	685.09
K	645+48.53	1.33	685.06	685.06
L	645+58.53	1.33	685.02	685.02
☉ Brg. E. Abut.	645+70.03	1.33	684.96	684.96
Back E. Abut.	645+71.87	1.33	684.95	684.95

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

TOP OF SLAB ELEVATIONS - IV
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 7	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	27 SHEETS	311	10HB-R	Du Page	53	23
			CONTRACT NO. 60B92			
		FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

E.B. P.G.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.47	3.00	684.93	684.93
⊕ Brg. W. Abut.	644+20.31	3.00	684.94	684.94
A	644+30.31	3.00	684.99	684.99
B	644+40.31	3.00	685.03	685.03
C	644+50.31	3.00	685.06	685.06
⊕ Brg. ⊙ Pier 1	644+61.81	3.00	685.09	685.09
D	644+71.81	3.00	685.11	685.13
E	644+81.81	3.00	685.12	685.16
F	644+91.81	3.00	685.13	685.17
G	645+01.81	3.00	685.13	685.17
H	645+11.81	3.00	685.12	685.15
I	645+21.81	3.00	685.11	685.12
⊕ Brg. ⊙ Pier 2	645+28.47	3.00	685.09	685.09
J	645+38.47	3.00	685.07	685.06
K	645+48.47	3.00	685.03	685.03
L	645+58.47	3.00	684.99	685.00
⊕ Brg. E. Abut.	645+69.97	3.00	684.94	684.94
Back E. Abut.	645+71.81	3.00	684.93	684.93

BEAM 8

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.36	7.67	684.86	684.86
⊕ Brg. W. Abut.	644+20.20	7.67	684.87	684.87
A	644+30.20	7.67	684.91	684.92
B	644+40.20	7.67	684.95	684.96
C	644+50.20	7.67	684.99	684.99
⊕ Brg. ⊙ Pier 1	644+61.70	7.67	685.02	685.02
D	644+71.70	7.67	685.04	685.06
E	644+81.70	7.67	685.05	685.08
F	644+91.70	7.67	685.06	685.10
G	645+01.70	7.67	685.05	685.10
H	645+11.70	7.67	685.05	685.08
I	645+21.70	7.67	685.03	685.04
⊕ Brg. ⊙ Pier 2	645+28.36	7.67	685.02	685.02
J	645+38.36	7.67	684.99	684.99
K	645+48.36	7.67	684.96	684.96
L	645+58.36	7.67	684.92	684.92
⊕ Brg. E. Abut.	645+69.86	7.67	684.87	684.87
Back E. Abut.	645+71.70	7.67	684.86	684.86

BEAM 9

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+18.04	14.00	684.76	684.76
⊕ Brg. W. Abut.	644+19.88	14.00	684.77	684.77
A	644+29.88	14.00	684.81	684.82
B	644+39.88	14.00	684.85	684.86
C	644+49.88	14.00	684.89	684.89
⊕ Brg. ⊙ Pier 1	644+61.38	14.00	684.92	684.92
D	644+71.38	14.00	684.94	684.96
E	644+81.38	14.00	684.95	684.98
F	644+91.38	14.00	684.96	685.00
G	645+01.38	14.00	684.96	685.00
H	645+11.38	14.00	684.95	684.98
I	645+21.38	14.00	684.93	684.95
⊕ Brg. ⊙ Pier 2	645+28.04	14.00	684.92	684.92
J	645+38.04	14.00	684.89	684.89
K	645+48.04	14.00	684.86	684.86
L	645+58.04	14.00	684.82	684.83
⊕ Brg. E. Abut.	645+69.54	14.00	684.77	684.77
Back E. Abut.	645+71.38	14.00	684.76	684.76

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

TOP OF SLAB ELEVATIONS - V
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 8	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	27 SHEETS	311	10HB-R	Du Page	53	24
			CONTRACT NO. 60B92			
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT						

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM 10

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+17.61	20.33	684.63	684.63
☉ Brg. W. Abut.	644+19.45	20.33	684.64	684.64
A	644+29.45	20.33	684.68	684.69
B	644+39.45	20.33	684.73	684.73
C	644+49.45	20.33	684.76	684.76
☉ Brg. ☉ Pier 1	644+60.95	20.33	684.79	684.79
D	644+70.95	20.33	684.81	684.83
E	644+80.95	20.33	684.82	684.86
F	644+90.95	20.33	684.83	684.87
G	645+00.95	20.33	684.83	684.87
H	645+10.95	20.33	684.82	684.85
I	645+20.95	20.33	684.81	684.82
☉ Brg. ☉ Pier 2	645+27.61	20.33	684.79	684.79
J	645+37.61	20.33	684.77	684.77
K	645+47.61	20.33	684.74	684.74
L	645+57.61	20.33	684.70	684.70
☉ Brg. E. Abut.	645+69.11	20.33	684.64	684.64
Back E. Abut.	645+70.95	20.33	684.63	684.63

BEAM 11

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+17.18	26.67	684.49	684.49
☉ Brg. W. Abut.	644+19.02	26.67	684.50	684.50
A	644+29.02	26.67	684.55	684.55
B	644+39.02	26.67	684.59	684.59
C	644+49.02	26.67	684.63	684.63
☉ Brg. ☉ Pier 1	644+60.52	26.67	684.66	684.66
D	644+70.52	26.67	684.68	684.70
E	644+80.52	26.67	684.69	684.73
F	644+90.52	26.67	684.70	684.74
G	645+00.52	26.67	684.70	684.74
H	645+10.52	26.67	684.69	684.72
I	645+20.52	26.67	684.68	684.69
☉ Brg. ☉ Pier 2	645+27.18	26.67	684.66	684.66
J	645+37.18	26.67	684.64	684.64
K	645+47.18	26.67	684.61	684.61
L	645+57.18	26.67	684.57	684.57
☉ Brg. E. Abut.	645+68.68	26.67	684.51	684.51
Back E. Abut.	645+70.52	26.67	684.50	684.50

BEAM 12

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back W. Abut.	644+16.75	33.00	684.36	684.36
☉ Brg. W. Abut.	644+18.59	33.00	684.37	684.37
A	644+28.59	33.00	684.42	684.42
B	644+38.59	33.00	684.46	684.46
C	644+48.59	33.00	684.49	684.49
☉ Brg. ☉ Pier 1	644+60.09	33.00	684.53	684.53
D	644+70.09	33.00	684.55	684.56
E	644+80.09	33.00	684.56	684.59
F	644+90.09	33.00	684.57	684.61
G	645+00.09	33.00	684.57	684.61
H	645+10.09	33.00	684.56	684.59
I	645+20.09	33.00	684.55	684.56
☉ Brg. ☉ Pier 2	645+26.75	33.00	684.53	684.53
J	645+36.75	33.00	684.51	684.51
K	645+46.75	33.00	684.48	684.48
L	645+56.75	33.00	684.44	684.44
☉ Brg. E. Abut.	645+68.25	33.00	684.38	684.38
Back E. Abut.	645+70.09	33.00	684.37	684.37

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

TOP OF SLAB ELEVATIONS - VI
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 9 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	10HB-R	Du Page	53	25
			CONTRACT NO. 60B92			
		FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+91.44	-30.00	684.26
A1	644+01.44	-30.00	684.33
A2	644+11.44	-30.00	684.39
E. End of W. Apr	644+21.44	-30.00	684.44

NORTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+91.23	-27.00	684.32
A1	644+01.23	-27.00	684.39
A2	644+11.23	-27.00	684.45
E. End of W. Apr	644+21.23	-27.00	684.51

W.B. P.G.

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+89.60	-3.00	684.75
A1	643+99.60	-3.00	684.82
A2	644+09.60	-3.00	684.88
E. End of W. Apr	644+19.60	-3.00	684.94

☉ ROADWAY

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+89.40	0.00	684.80
A1	643+99.40	0.00	684.86
A2	644+09.40	0.00	684.93
E. End of W. Apr	644+19.40	0.00	684.98

STAGE CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+89.37	0.50	684.79
A1	643+99.37	0.50	684.86
A2	644+09.37	0.50	684.92
E. End of W. Apr	644+19.37	0.50	684.97

E.B. P.G.

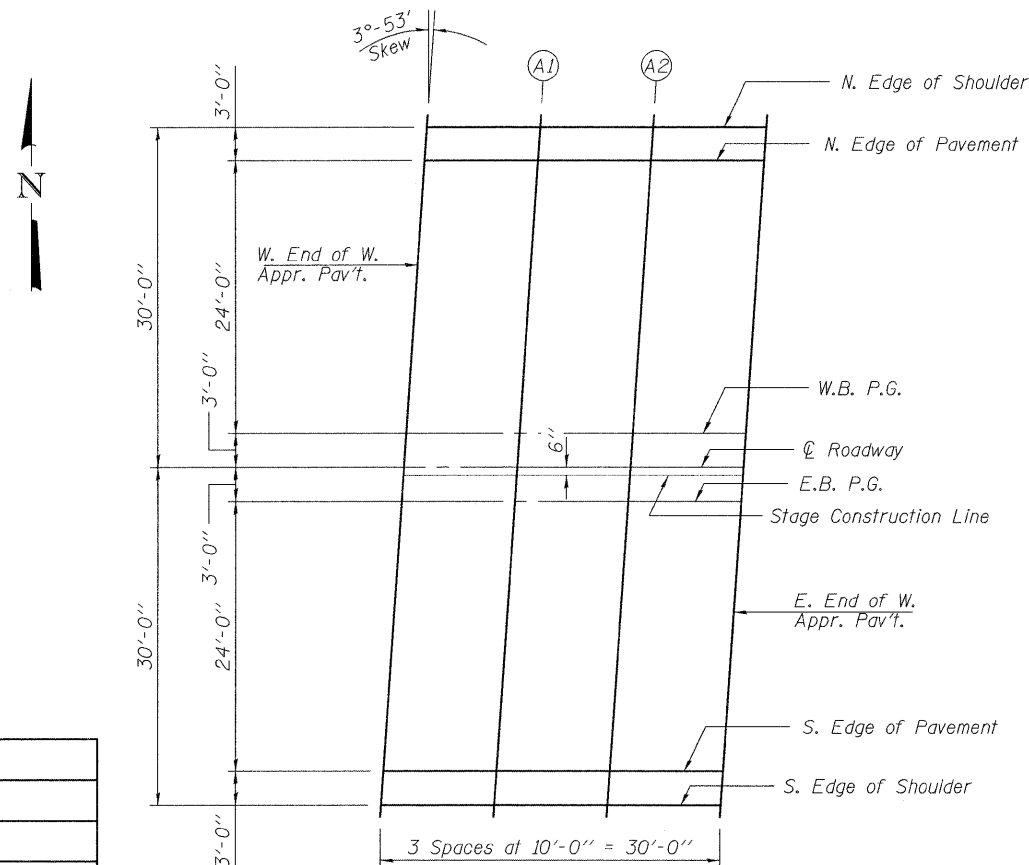
Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+89.20	3.00	684.75
A1	643+99.20	3.00	684.82
A2	644+09.20	3.00	684.88
E. End of W. Apr	644+19.20	3.00	684.93

SOUTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+87.57	27.00	684.30
A1	643+97.57	27.00	684.37
A2	644+07.57	27.00	684.43
E. End of W. Apr	644+17.57	27.00	684.49

SOUTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of W. Apr	643+87.36	30.00	684.23
A1	643+97.36	30.00	684.30
A2	644+07.36	30.00	684.37
E. End of W. Apr	644+17.36	30.00	684.42



PLAN

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

WEST APPROACH SLAB ELEVATIONS
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 10	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	27 SHEETS	311	10HB-R	Du Page	53	26
					CONTRACT NO. 60B92	
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT						

8/7/2009 K:\11225610\structures\final_plans\Approach_Slab.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+73.11	-30.00	684.42
A3	645+83.11	-30.00	684.37
A4	645+93.11	-30.00	684.30
E. End of E. Apr	646+03.11	-30.00	684.23

NORTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+72.90	-27.00	684.49
A3	645+82.90	-27.00	684.43
A4	645+92.90	-27.00	684.37
E. End of E. Apr	646+02.90	-27.00	684.30

W.B. P.G.

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+71.27	-3.00	684.93
A3	645+81.27	-3.00	684.88
A4	645+91.27	-3.00	684.81
E. End of E. Apr	646+01.27	-3.00	684.74

☉ ROADWAY

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+71.07	0.00	684.98
A3	645+81.07	0.00	684.92
A4	645+91.07	0.00	684.86
E. End of E. Apr	646+01.07	0.00	684.79

STAGE CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+71.04	0.50	684.97
A3	645+81.04	0.50	684.92
A4	645+91.04	0.50	684.85
E. End of E. Apr	646+01.04	0.50	684.79

E.B. P.G.

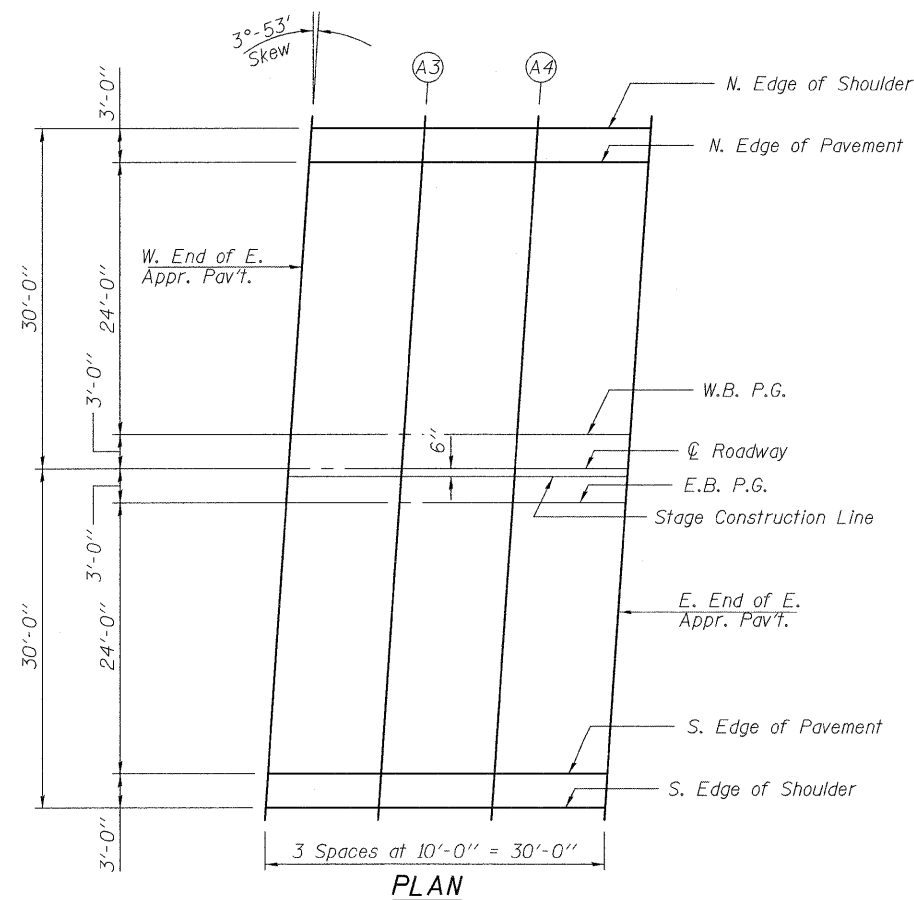
Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+70.87	3.00	684.93
A3	645+80.87	3.00	684.88
A4	645+90.87	3.00	684.82
E. End of E. Apr	646+00.87	3.00	684.75

SOUTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+69.24	27.00	684.50
A3	645+79.24	27.00	684.45
A4	645+89.24	27.00	684.39
E. End of E. Apr	645+99.24	27.00	684.32

SOUTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
W. End of E. Apr	645+69.03	30.00	684.44
A3	645+79.03	30.00	684.39
A4	645+89.03	30.00	684.33
E. End of E. Apr	645+99.03	30.00	684.26



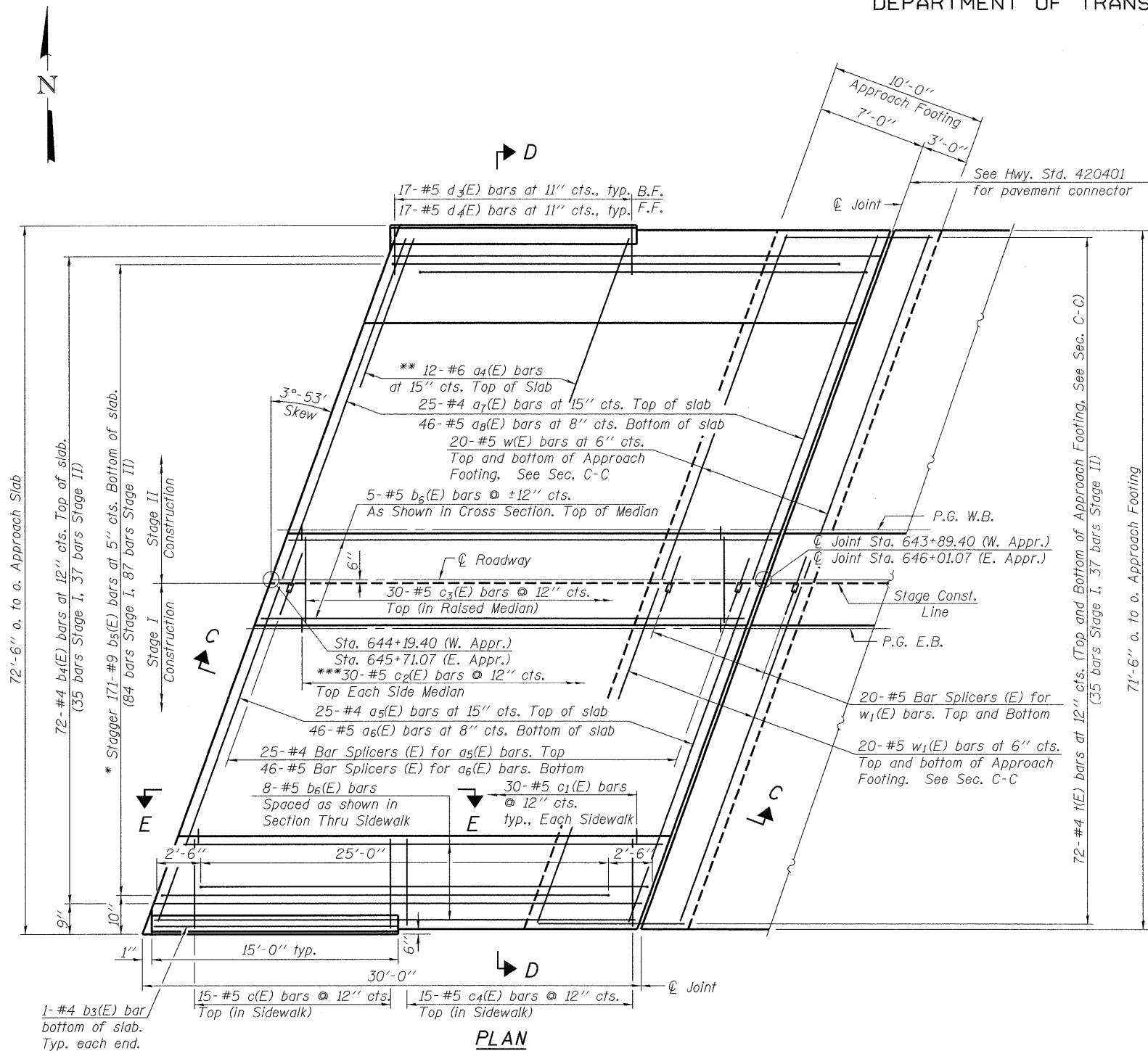
DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

EAST APPROACH SLAB ELEVATIONS
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 11	F.A.P. RTE. 311	SECTION 10HB-R	COUNTY Du Page	TOTAL SHEETS 53	SHEET NO. 27
	27 SHEETS	CONTRACT NO. 60B92			FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Notes:
See sheet 13 of 27 for Sections C-C & D-D and View E-E.
 $a_4(E)$ thru $a_8(E)$, $w(E)$, and $w_1(E)$ bar spacings measured perpendicular to $\text{C} \text{ Rdwy.}$



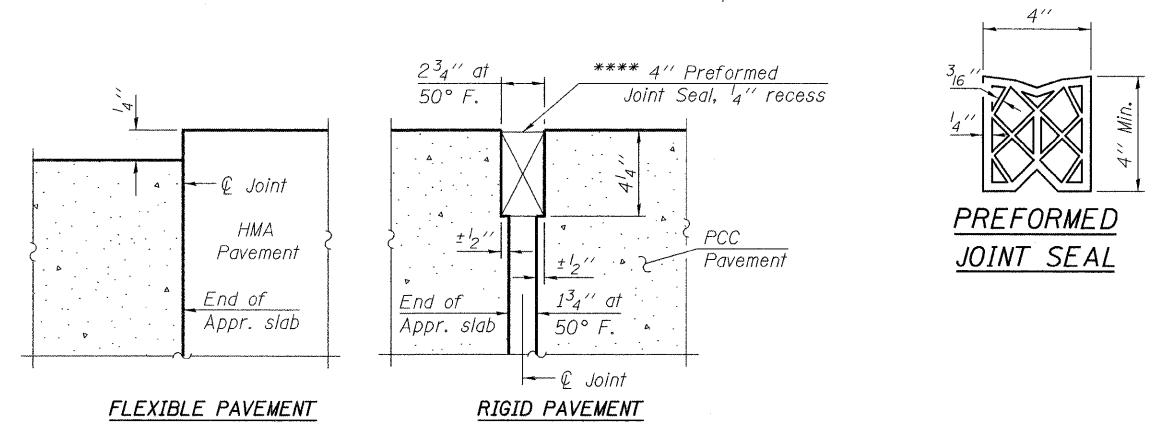
PLAN

East Approach Shown.

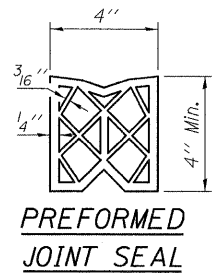
West Approach similar but opposite hand.

- * Tilt #9 $b_5(E)$ bars as required to maintain clearance.
- ** Alternate with $a_5(E)$ or $a_7(E)$ bars, typ. each parapet.
- *** $\frac{3}{4}$ " ϕ Galvanized expansion anchor or Ferrule Loop Slab Insert (Proof Load 6600 lb) for each $c_2(E)$ bar. Cost of anchor/inserts is included in the cost of Reinforcement Bars, Epoxy Coated. (120 total)

**** Cost included with Concrete Superstructure.



DETAIL A

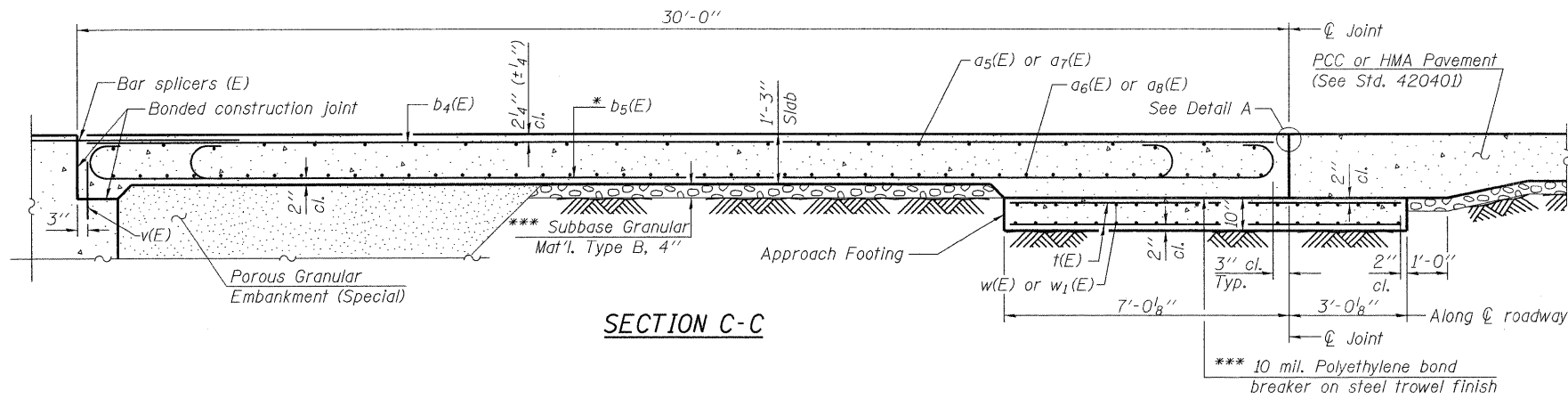


DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

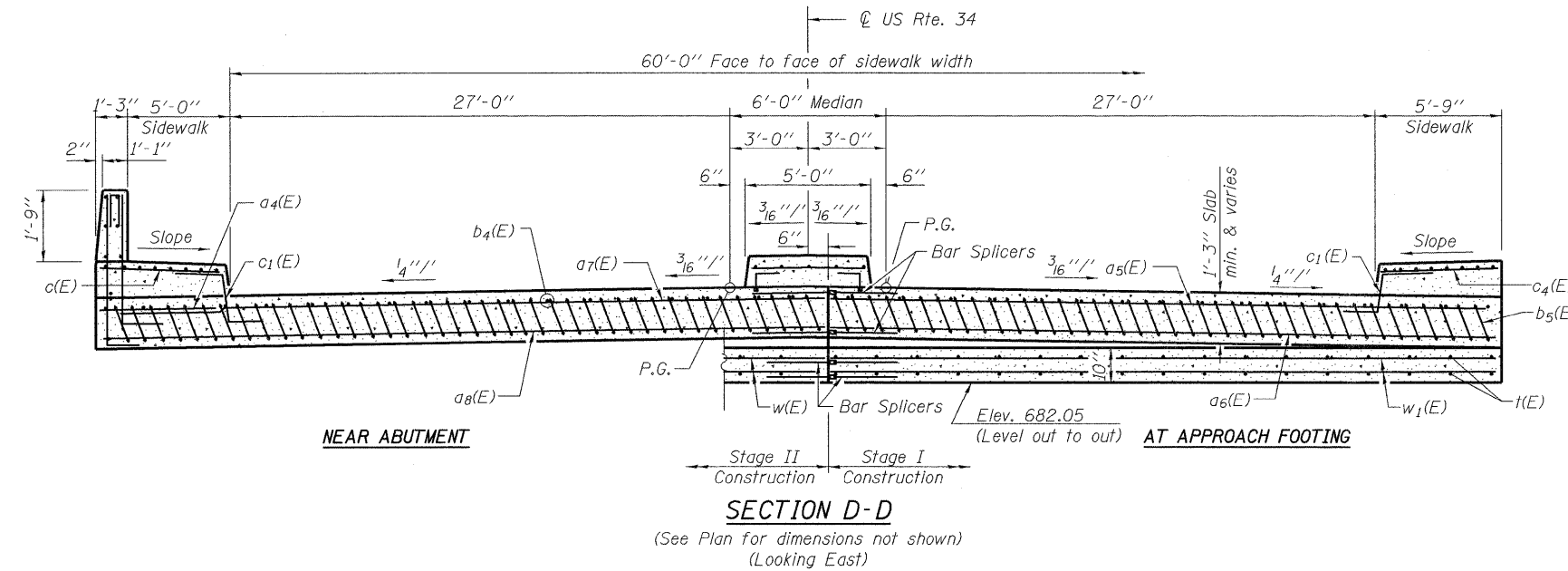
APPROACH SLAB DETAILS - I
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 12 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	10HB-R	Du Page	53	28
		CONTRACT NO. 60B92				
		FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

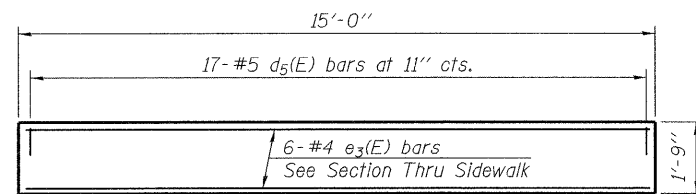


SECTION C-C

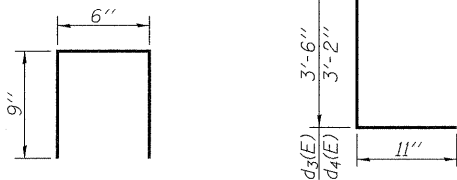


SECTION D-D

(See Plan for dimensions not shown)
(Looking East)

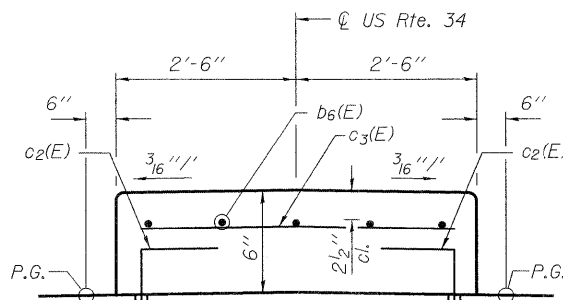


VIEW E-E



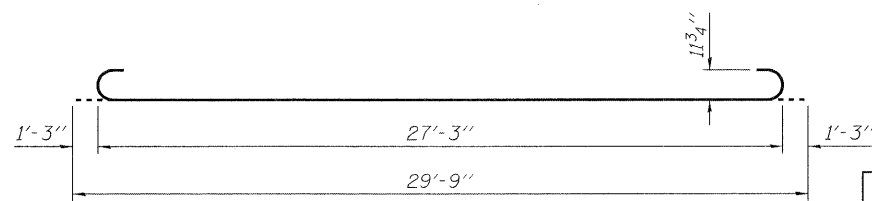
BAR d5(E)

BARS d3(E) and d4(E)

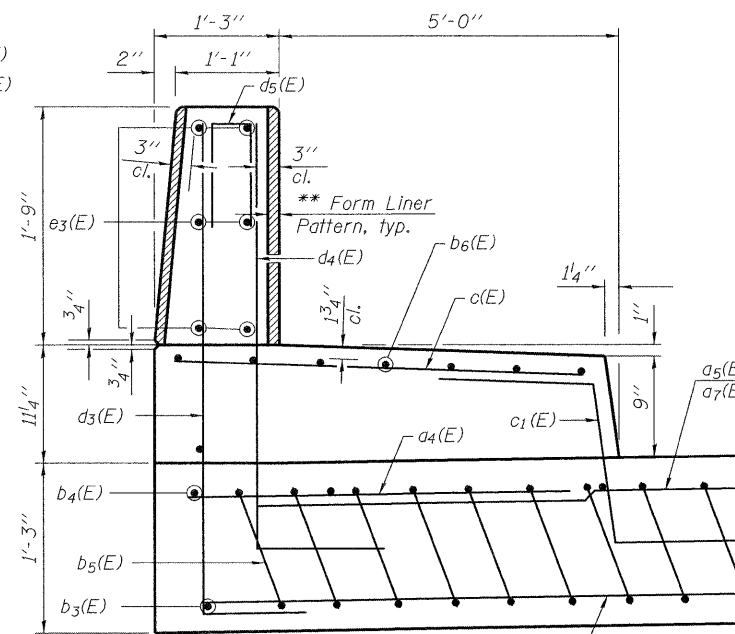


MEDIAN CROSS SECTION

3/4" ϕ Galvanized Expansion Anchor
or Ferrule Loop Slab Insert.
(Proof Load 6600 lb). Cost
included with Reinforcement
Bars, Epoxy Coated.



BAR b5(E)



SECTION THRU SIDEWALK

** Depth of form liner pattern shall not exceed 1 1/2".

Notes:

- See sheet 12 of 27 for Detail A.
- Approach slab, sidewalk and parapet concrete shall be paid for as Concrete Superstructure.
- Approach footing concrete shall be paid for as Concrete Structures.
- Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
- Approach Slab median to receive same treatment as Bridge median. See Sheet 15 of 27.
- For v(E) bar details, see sheet 17 of 27.
- The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
- For bar splicer details, see sheet 26 of 27.
- Cost of excavation for approach footing included with Concrete Structures.
- For Porous Granular Embankment (Special) and drainage treatment details, see sheet 2 of 27.
- See Sheet 15 of 27 for Form Liner note.
- See Sheet 17 of 27 for detail of c1(E) and c2(E) bars.
- Median to be constructed after completion of Stage II Construction.

* Tilt #9 b5(E) bars as required to maintain clearance.

*** Cost included with Concrete Superstructure.

TWO APPROACHES
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a4(E)	48	#6	6'-0"	—
a5(E)	50	#4	35'-4"	—
a6(E)	92	#5	35'-4"	—
a7(E)	50	#4	36'-4"	—
a8(E)	92	#5	36'-4"	—
b3(E)	4	#4	14'-8"	—
b4(E)	144	#4	29'-8"	—
b5(E)	342	#9	29'-9"	—
b6(E)	42	#5	29'-9"	—
c(E)	60	#5	5'-10"	—
c1(E)	120	#5	2'-5"	—
c2(E)	120	#5	1'-7"	—
c3(E)	60	#5	4'-9"	—
c4(E)	60	#5	5'-6"	—
d3(E)	68	#5	4'-5"	L
d4(E)	68	#5	4'-1"	L
d5(E)	68	#5	2'-0"	Π
e3(E)	24	#4	14'-9"	—
t(E)	288	#4	9'-9"	—
w(E)	80	#5	35'-10"	—
w1(E)	80	#5	34'-10"	—
Concrete Superstructure		Cu. Yd.	246.8	
Concrete Structures		Cu. Yd.	44.2	
Form Liner Textured Surface		Sq. Ft.	219	
Reinforcement Bars, Epoxy Coated		Pound	58,760	
Bar Splicers		Each	222	
Staining Concrete Structures		Sq. Yd.	31.5	

Curing compound shall not be applied to the concrete parapets before Staining Concrete Structures is complete. See Special Provisions.

APPROACH SLAB DETAILS - II
STRUCTURE NO. 022-0033

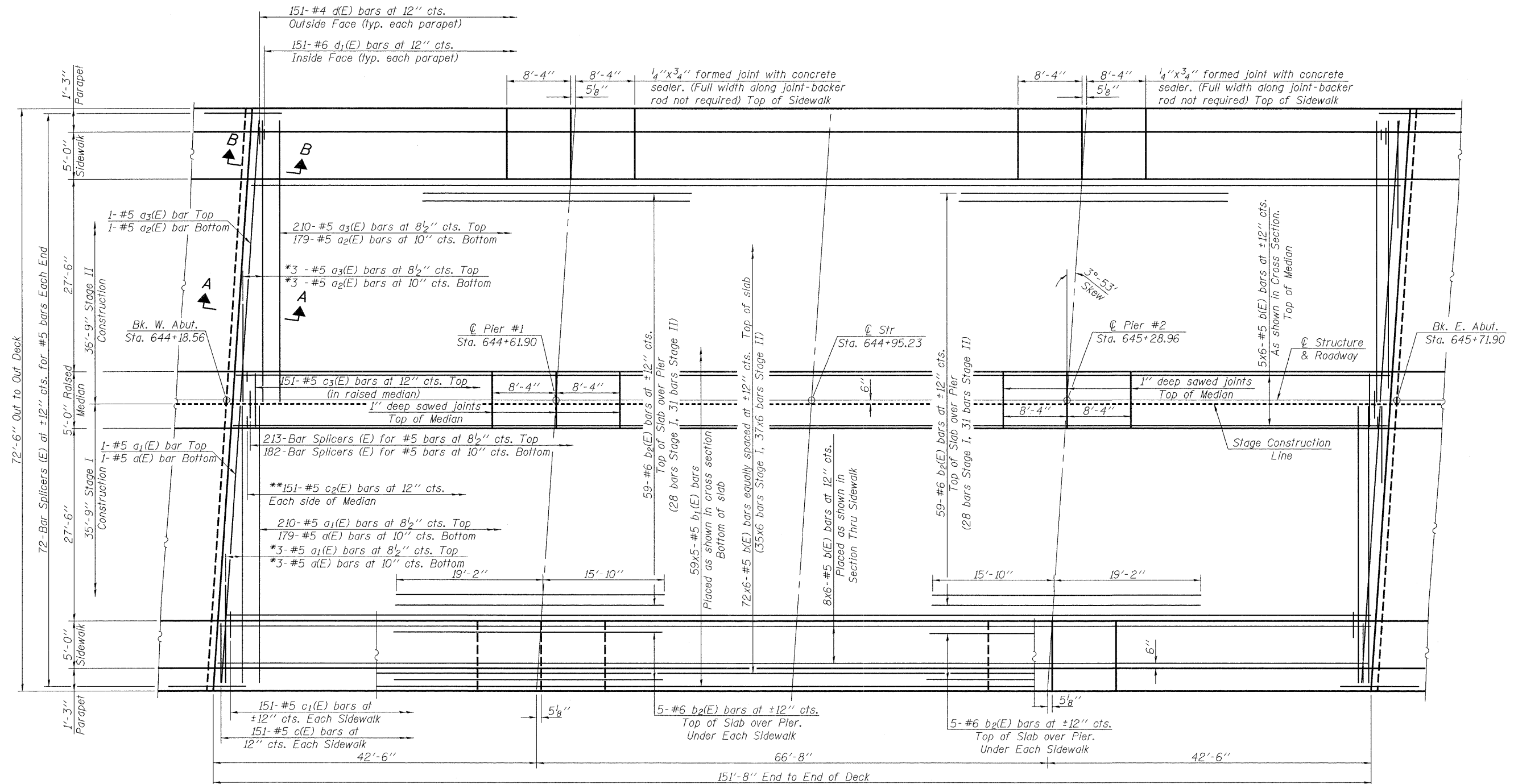
DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, IL 60187

SHEET NO. 13
27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	Du Page	53	29
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



PLAN

*Order a(E) thru a₃(E) bars full length.
Cut to fit skew and use remainder of bars in opposite end.

**3/4" φ Galvanized Expansion Anchor or Ferrule Loop Slab Insert.
(Proof Load 6600 lb) for each c₂(E) bar. Cost of anchor/inserts is included with Reinforcement Bars, Epoxy Coated.

NOTES

Minimum lap length for #5 bars = 2'-2".
See Sheets 15 and 17 of 27 for Superstructure Details, Parapet Details, Median Details, Section A-A, Section B-B and Bill of Material.
See Sheet 26 of 27 for Bar Splicer Details.
Bars indicated thus 6x5-#5 etc. indicates 6 lines of bars with 5 lengths per line.
See Sheet 1 of 27 for location of Deck Drains.
See Sheet 16 of 27 for Diaphragm Details.

DECK PLAN
STRUCTURE NO. 022-0033

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

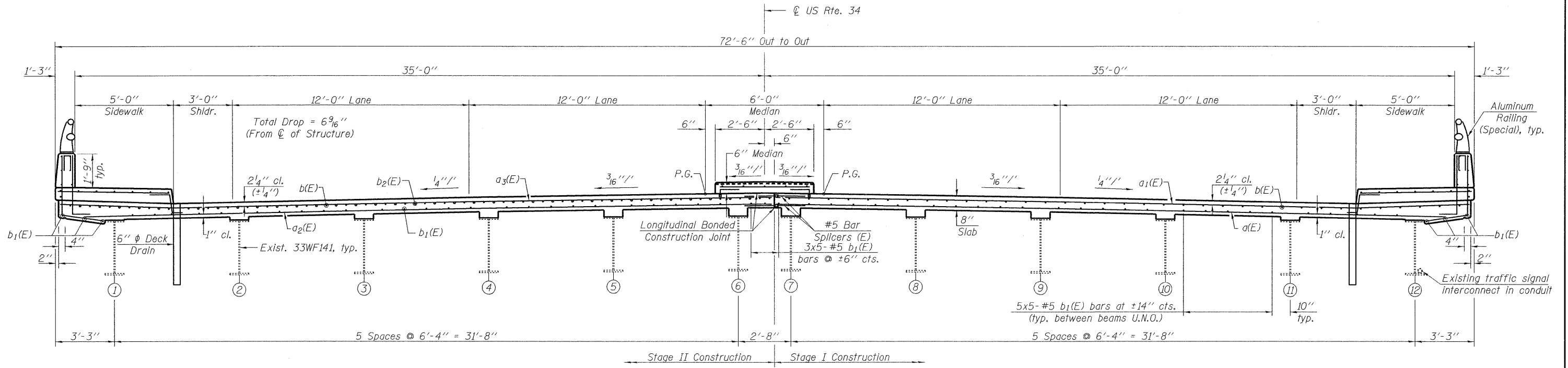
rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, IL 60187

SHEET NO. 14
27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	Du Page	53	30
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

8/7/2009 k:\1125510\structures\final_plans\Superstructure.dgn

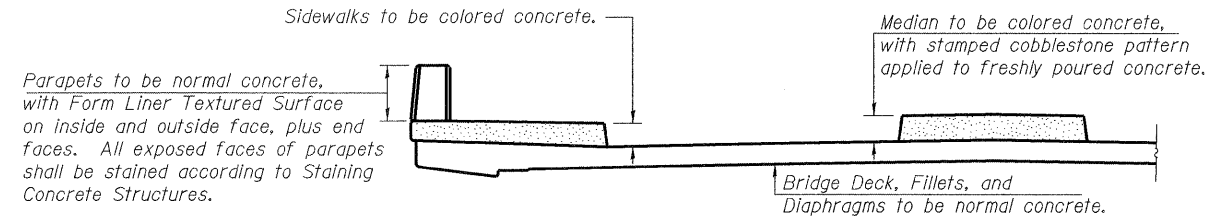
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



AT PIER

DECK CROSS SECTION
(Looking East)

AT MIDSPAN



CONCRETE TREATMENTS

CONCRETE TREATMENT NOTES

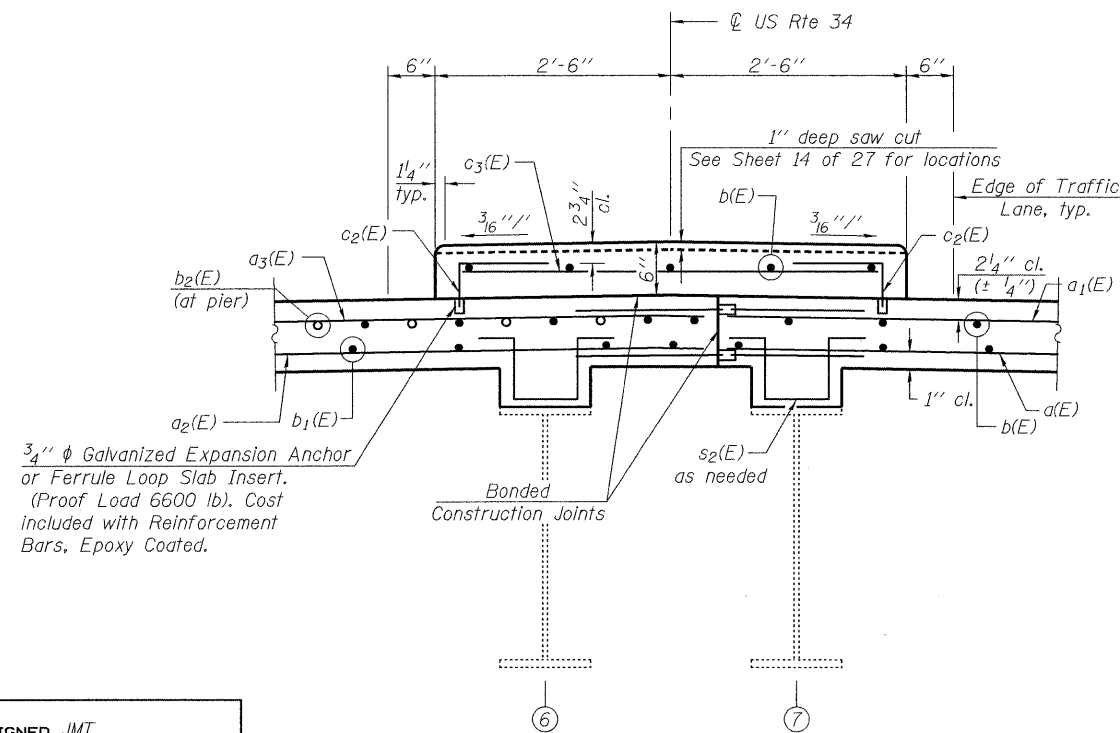
Coloring admixture shall be "Santa Fe Buff", color number L1447 by Butterfield Color, or approved equal. Contractor shall submit a sample for approval prior to construction according to the Special Provision for Concrete Median (Special). See Special Provision for more information.

Stamped pattern applied to top surface of Median shall be "Cobblestone - Random Interlocking" by Scofield Systems, or approved equal. Installation shall be according to manufacturer's recommendations. Contractor shall submit a sample for approval by the Village of Lisle, at least 30 days prior to pouring the median.

Color additive in bridge and approach slab (Median and Sidewalks), and stamped Cobblestone pattern on bridge and approach slab (Median), plus any required samples to be submitted for approval, will not be paid for separately, but shall be included with the unit price bid for Concrete Superstructure.

Form Liner on bridge and approach slab (Parapets) shall be Customrock pattern #12021 - "Ashlar", or approved equal. Contractor shall submit a sample for approval by the Village of Lisle according to the requirements of Article 503.06(a) of the Standard Specifications. This sample shall also display an example of the proposed staining method for Staining Concrete Structures. Cost included with Form Liner Textured Surface.

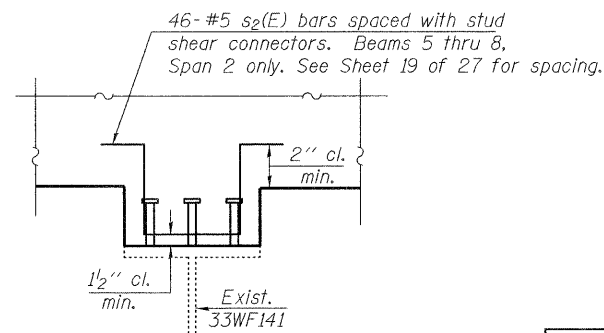
Staining of concrete parapets shall be done in accordance with the Special Provision for Staining Concrete Structures. Contractor shall exercise care when staining to avoid coloring the Aluminum Railing.



MEDIAN CROSS SECTION
(Looking East)

NOTES

Median to be constructed after completion of Stage II Construction. See Sheet 17 of 27 for Parapet Details. U.N.O. = Unless Noted Otherwise.



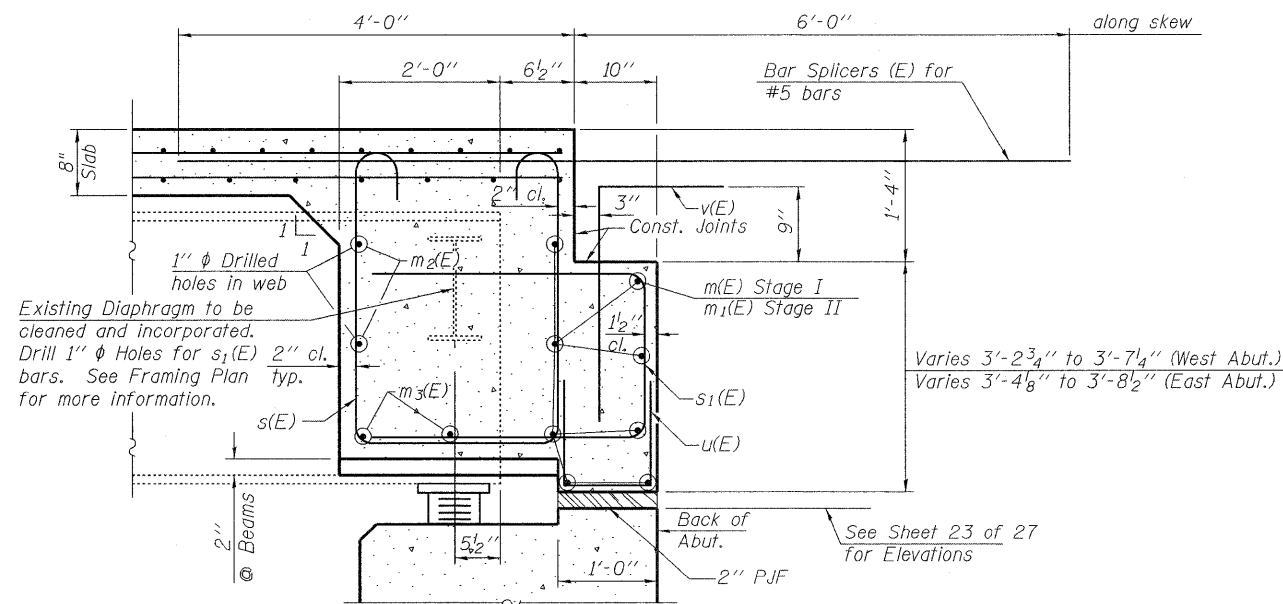
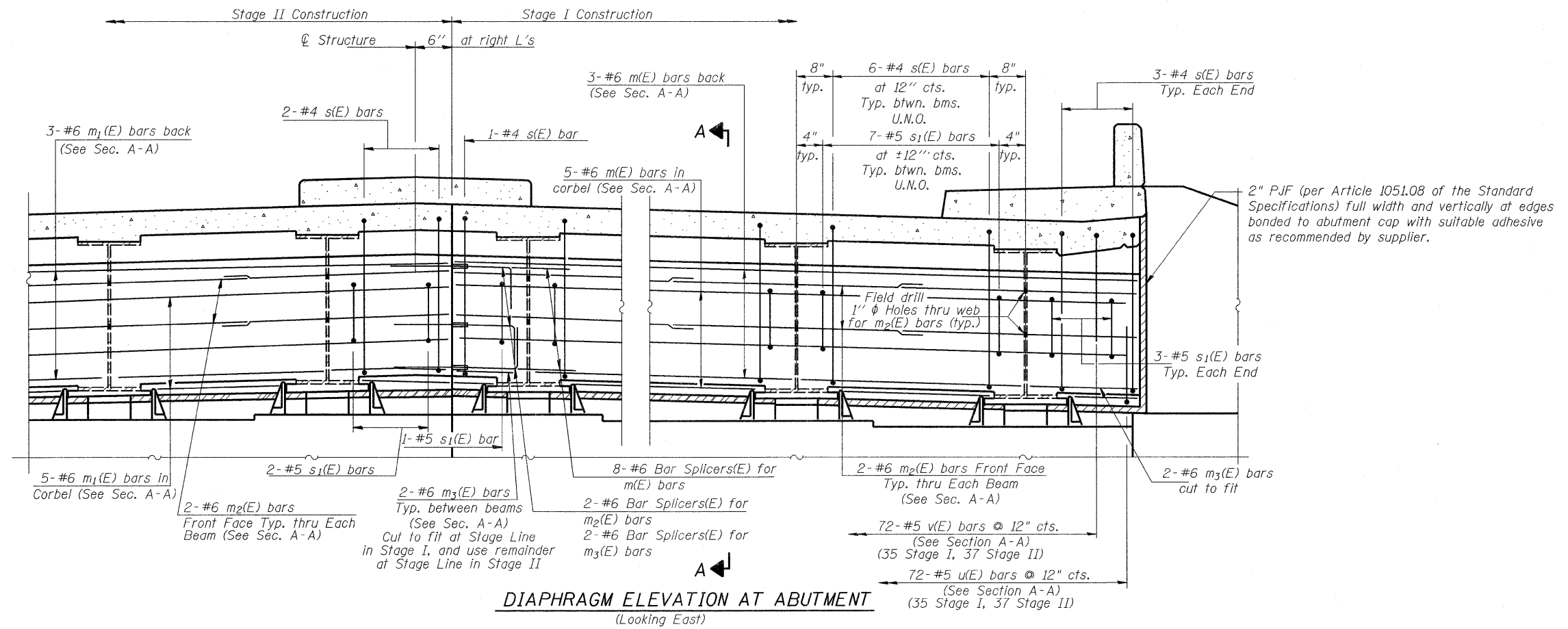
FILLET REINFORCEMENT DETAIL

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

DECK SECTIONS & DETAILS
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 15	F.A.P. RTE. 311	SECTION 10HB-R	COUNTY Du Page	TOTAL SHEETS 53	SHEET NO. 31
	27 SHEETS	CONTRACT NO. 60B92			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



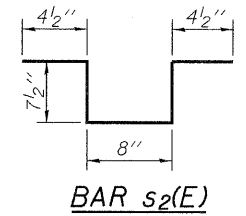
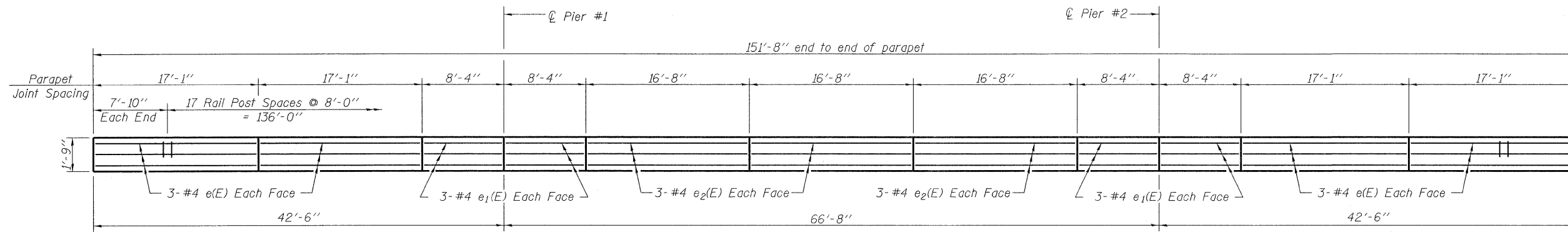
NOTES:
U.N.O. = Unless Noted Otherwise.

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

DIAPHRAGM DETAILS
STRUCTURE NO. 022-0033

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 16	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	27 SHEETS	311	10HB-R	Du Page	53	32
		CONTRACT NO. 60B92				
		FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

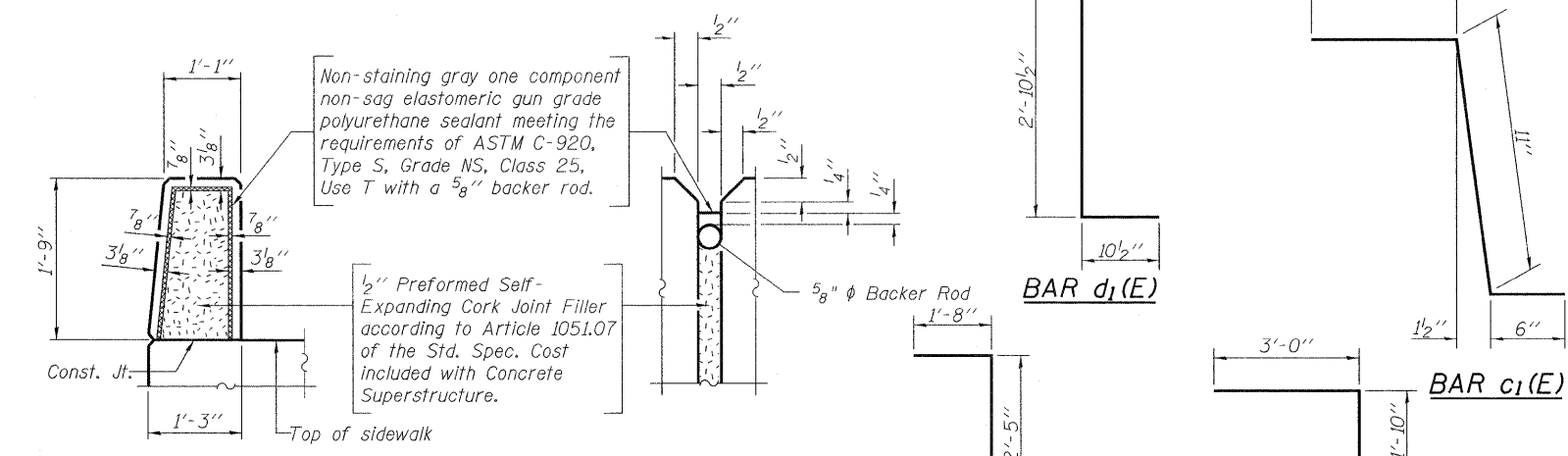
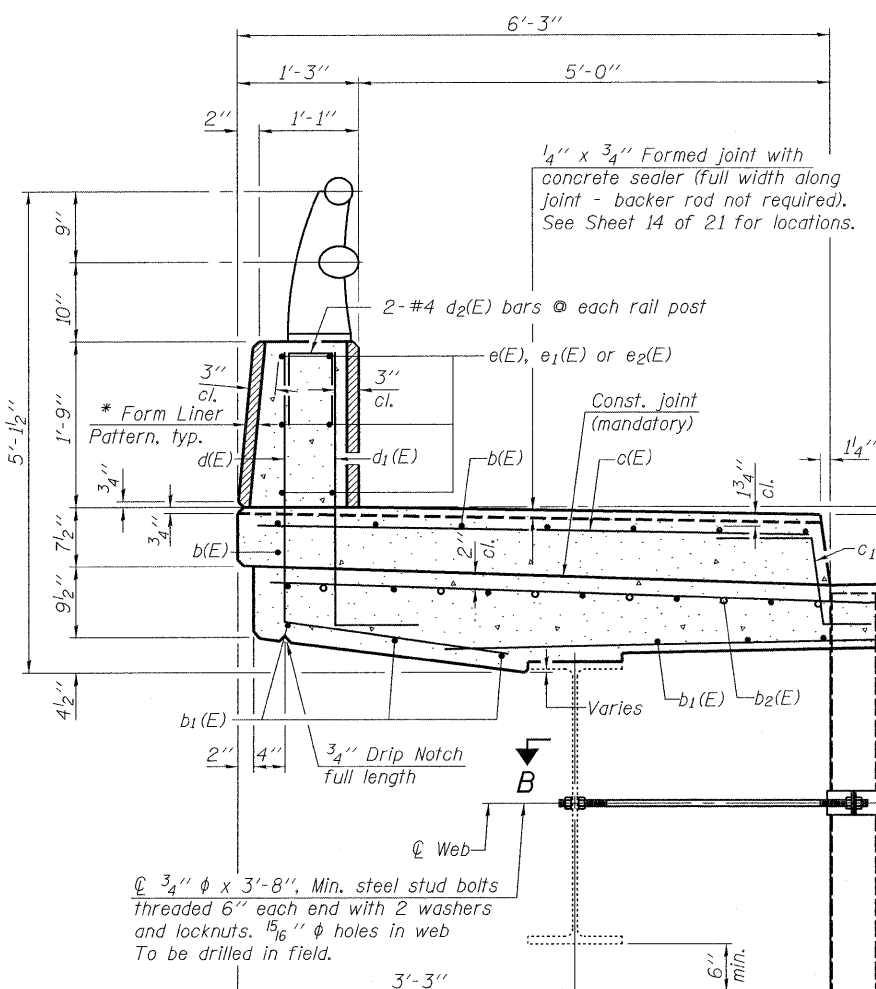
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



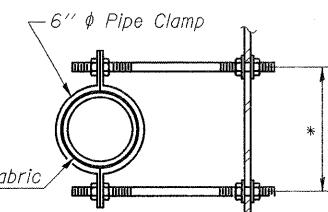
**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	182	#5	34'-3"	—
a1(E)	213	#5	35'-4"	—
a2(E)	182	#5	35'-3"	—
a3(E)	213	#5	36'-4"	—
b(E)	558	#5	26'-8"	—
b1(E)	295	#5	31'-8"	—
b2(E)	138	#6	35'-0"	—
c(E)	302	#5	5'-10"	—
c1(E)	302	#5	2'-5"	┌
c2(E)	302	#5	1'-7"	┌
c3(E)	151	#5	4'-9"	—
d(E)	302	#4	5'-4"	L
d1(E)	302	#6	3'-9"	L
d2(E)	72	#4	2'-0"	┐
e(E)	48	#4	16'-4"	—
e1(E)	48	#4	8'-0"	—
e2(E)	36	#4	16'-9"	—
m(E)	16	#6	35'-4"	—
m1(E)	16	#6	36'-4"	—
m2(E)	48	#6	8'-11"	—
m3(E)	52	#6	5'-0"	—
s(E)	138	#4	9'-8"	┐
s1(E)	158	#4	7'-10"	┐
s2(E)	184	#5	2'-8"	┌
u(E)	144	#5	2'-9"	┐
v(E)	144	#5	4'-1"	┌

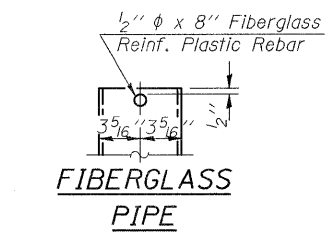
INSIDE ELEVATION OF PARAPET



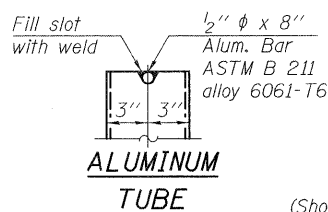
PARAPET JOINT DETAILS



SECTION B-B
*Dimension as required by Pipe Clamp



FIBERGLASS PIPE



ALUMINUM TUBE

TOP PLAN
(Showing Aluminum Tube)

SECTION THRU SIDEWALK

* Depth of Form Liner Pattern shall not exceed 1 1/2"

Notes:
Fiberglass pipe shall conform to ASTM D2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
Floor Drains need not be painted.

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

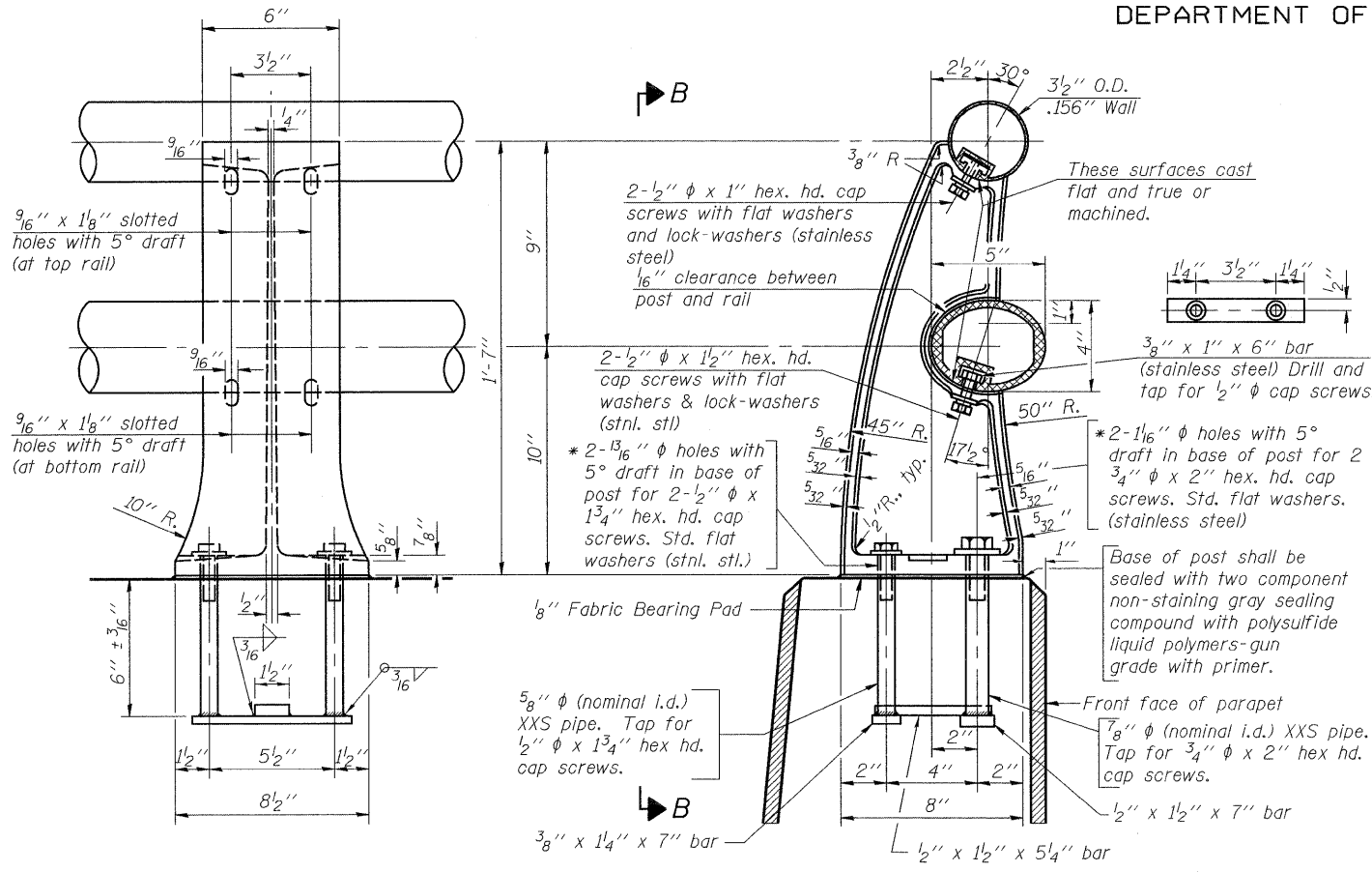
**PARAPET DETAILS
STRUCTURE NO. 022-0033**

Bars indicated thus 5 x 4 - #5 etc. indicates 5 lines of bars with 4 lengths per line.
See Sheet 1 of 27 for Deck Drain locations.
Curing compound shall not be applied to the concrete parapets before Staining Concrete Structures is complete. See Special Provisions.

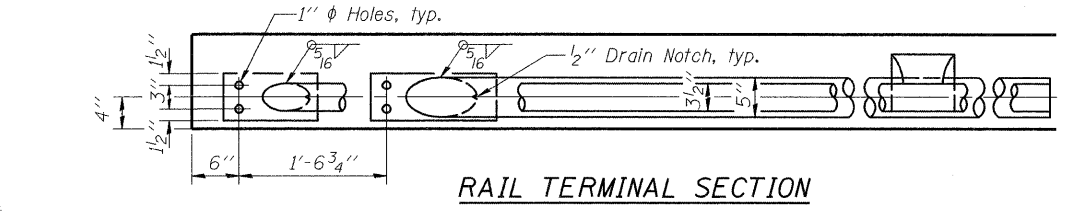
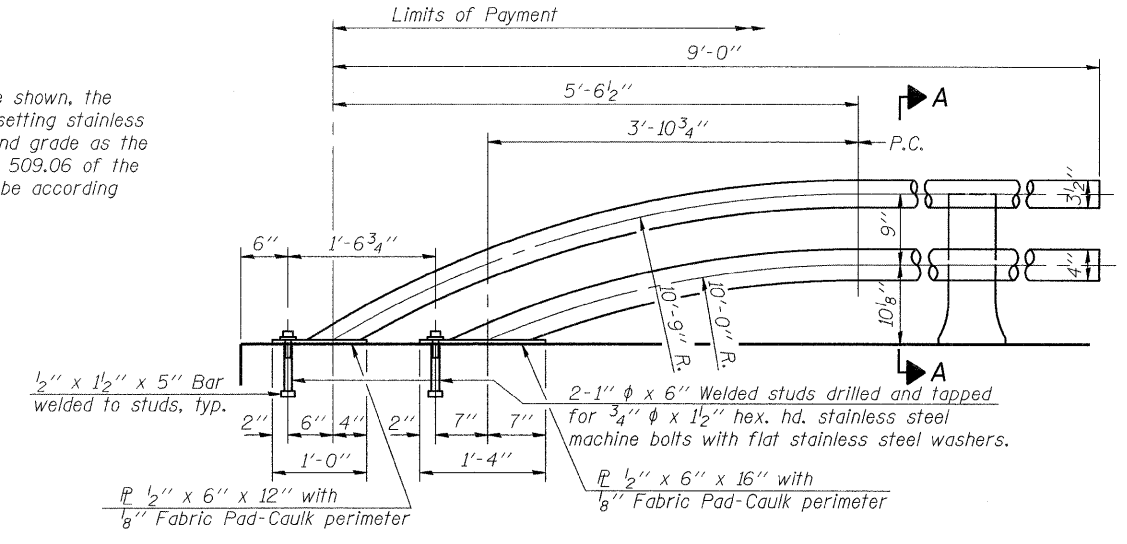
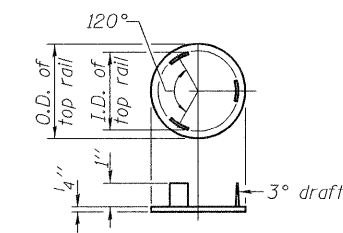
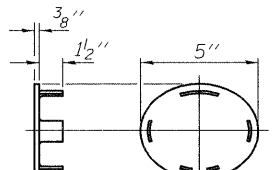
rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 17	F.A.P. RTE. 311	SECTION 10HB-R	COUNTY Du Page	TOTAL SHEETS 53	SHEET NO. 33
	27 SHEETS	CONTRACT NO. 60B92				
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT						

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

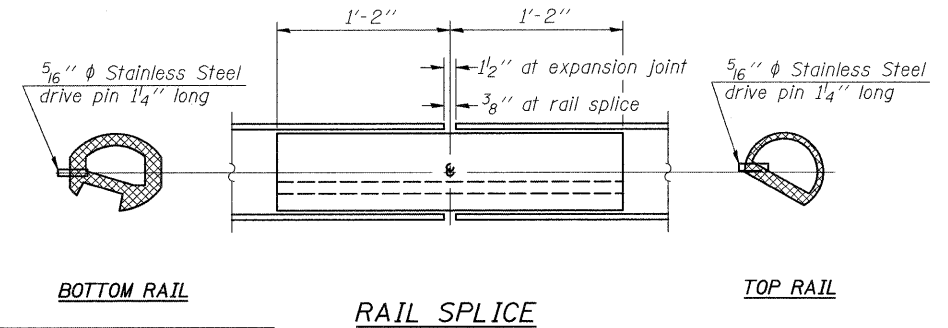
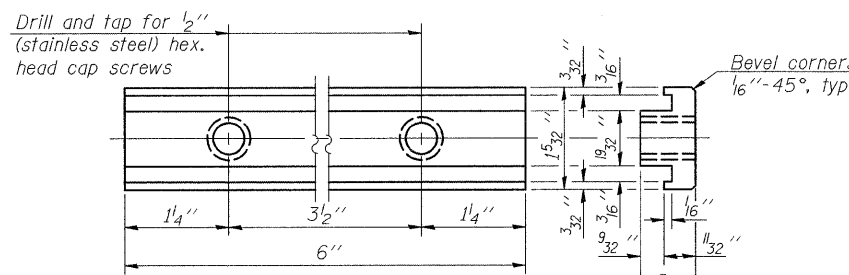


* In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

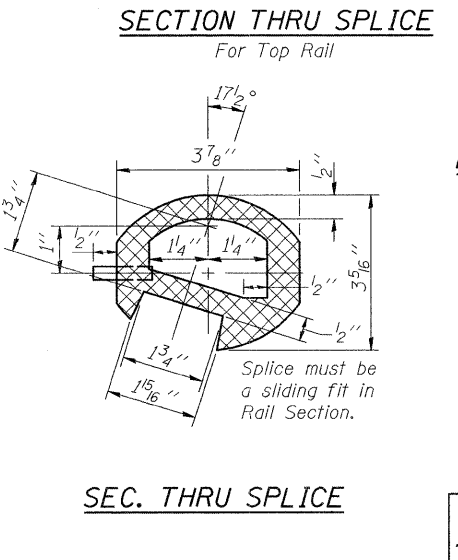
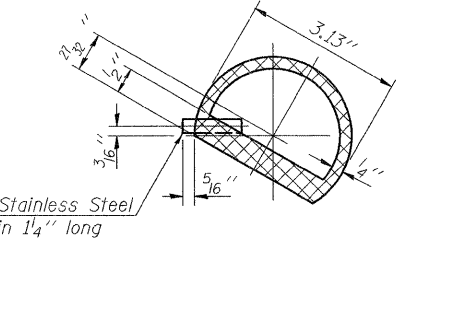
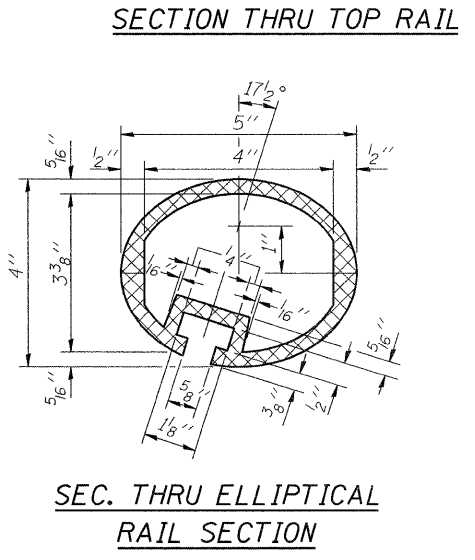
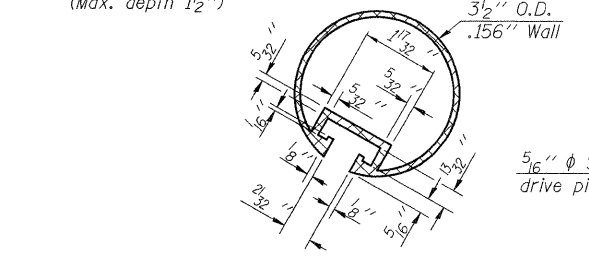


Note:
The end rail post shall be set back as required for the terminal rail section.

Notes:
All Posts shall be normal to parapet.
All joints in rail shall be spliced per detail.
All exposed rail ends shall be capped per detail.
Provide 1-1/8" and 2-1/8" Aluminum Shimls for 25% of the Posts.
Rail elements shall be parallel to Grade-high spots will be ground and low spots shimmed.
See sheet 17 of 27 for rail post spacing.
All aluminum components shall be colored Black. See Special Provision for Aluminum Railing, Special.



DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB



RAIL END TREATMENT FOR TYPE 5 AND 6 TERMINAL

BILL OF MATERIAL

Item	Unit	Quantity
Aluminum Railing, Special	Foot	300

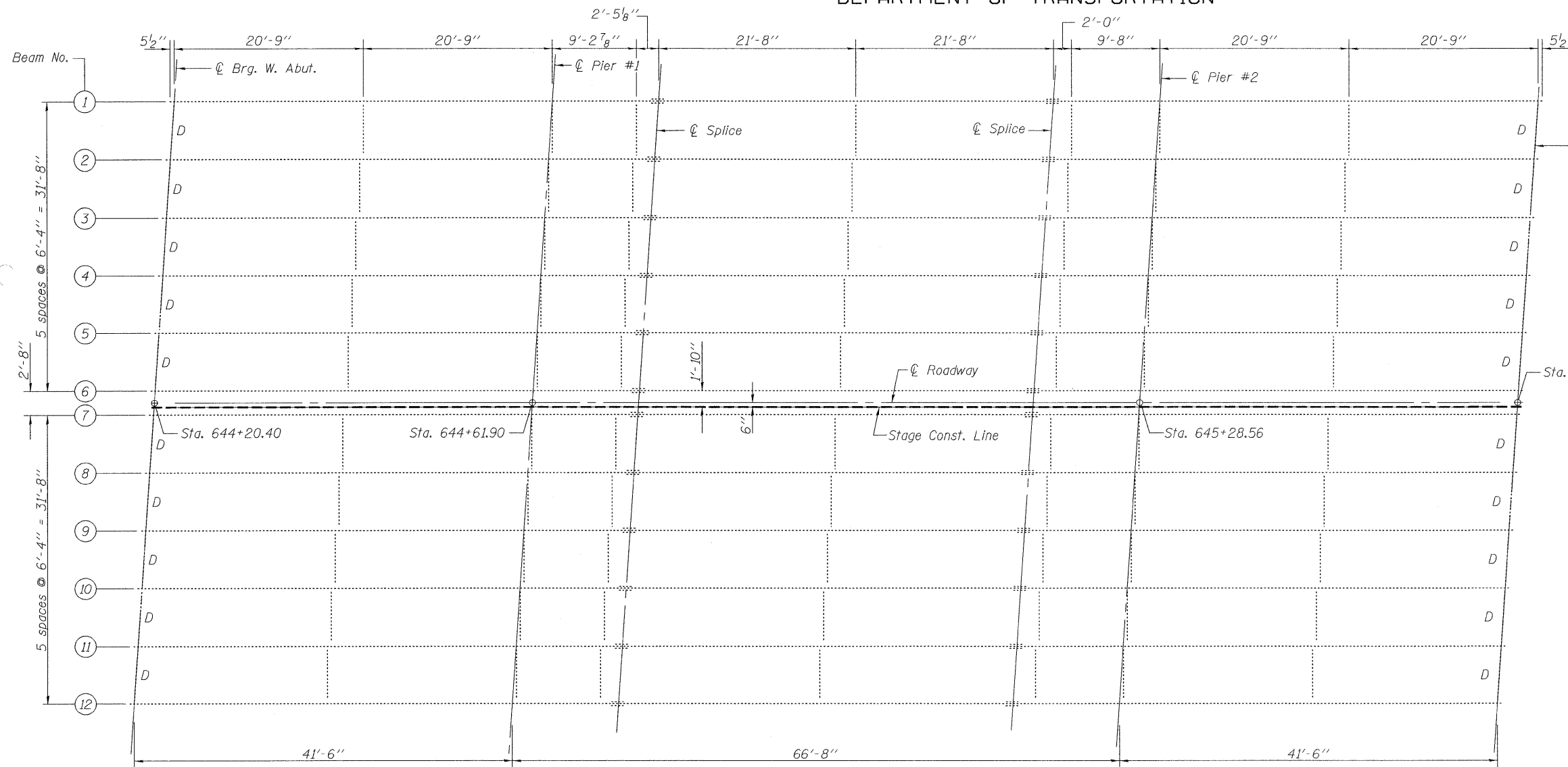
**BRIDGE RAIL DETAILS
STRUCTURE NO. 022-0033**

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 18	F.A.P. RTE. 311	SECTION 10HB-R	COUNTY Du Page	TOTAL SHEETS 53	SHEET NO. 34
	27 SHEETS	CONTRACT NO. 60B92			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

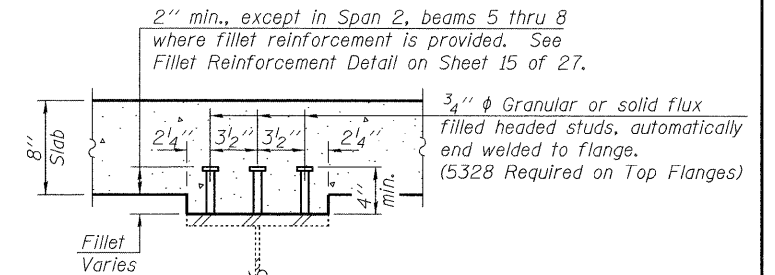
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NOTE

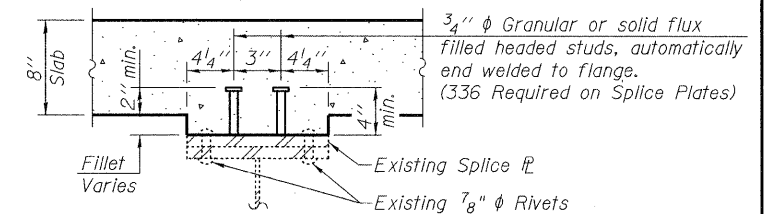
D - Indicates existing steel end diaphragms to be cleaned and incorporated into the concrete diaphragms at the abutments. Diaphragms are 12WF36, supported by 6"x4"x3/4" angles. This work will not be paid for separately, but shall be included with the cost of Concrete Removal. See General Notes for more information.



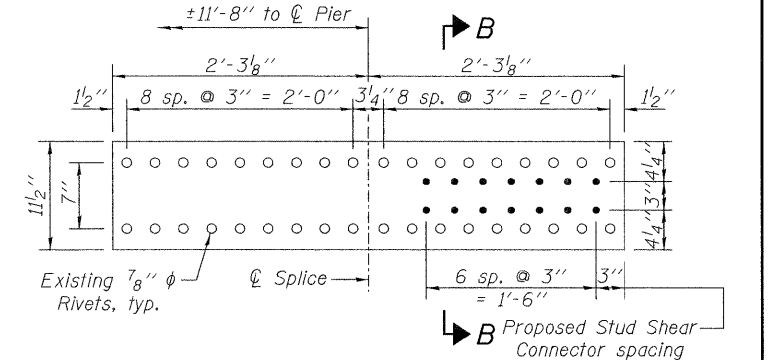
EXISTING FRAMING PLAN



SECTION A-A



SECTION B-B

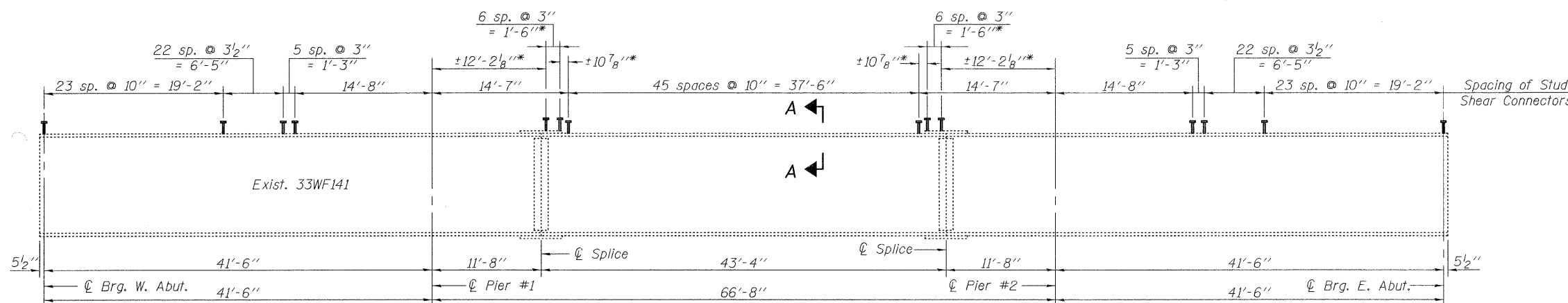


SPLICE PLATE DETAIL

Space Stud Shear Connectors along beam to maximize clearance from existing rivets.

BILL OF MATERIAL

Item	Unit	Quantity
Stud Shear Connectors	Each	5664



EXISTING GIRDER ELEVATION

* Place shear studs on top of Splice Plates according to Splice Plate Detail on this sheet.

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

**FRAMING PLAN
STRUCTURE NO. 022-0033**

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200 West Front Street
Wheaton, IL 60187

SHEET NO. 19
27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	Du Page	53	35
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERIOR GIRDER MOMENT TABLE						
		0.4 Sp. 1	Pier #1	0.5 Sp. 2	Pier #2	0.6 Sp. 3
I_s	(in ⁴)	7350	7350	7350	7350	7350
$I_c(n)$	(in ⁴)	25868	-	25868	-	25868
$I_c(3n)$	(in ⁴)	17806	-	17806	-	17806
S_s	(in ³)	442	442	442	442	442
$S_c(n)$	(in ³)	779	-	779	-	779
$S_c(3n)$	(in ³)	683	-	683	-	683
Z	(in ³)	-	-	-	-	-
ρ	(k/')	0.85	1.26	0.85	1.25	0.85
$M\rho$	(k)	66.0	363.0	197.8	363.0	66.0
$s\rho$	(k/')	0.41	-	0.41	-	0.41
$M_s\rho$	(k)	49.2	-	138.9	-	49.2
M_L	(k)	233.6	151.9	389.7	151.9	233.6
M_I	(k)	70.1	45.6	116.9	45.6	70.1
$^5_3 [M_L + I]$	(k)	506.2	329.2	844.33	329.2	506.2
M_o	(k)	807.8	899.9	1535.34	899.9	807.8
* M_u	(k)	2261	-	2114	-	2261
$f_s \rho$ non-comp	(ksi)	1.8	9.9	5.4	9.9	1.8
$f_s \rho$ (comp)	(ksi)	0.9	-	2.4	-	0.9
$f_s \ ^5_3 [M_L + M_I]$	(ksi)	7.8	8.9	13.0	8.9	7.8
f_s (Overload)	(ksi)	10.5	18.8	20.8	18.8	10.5
** f_s (Total)	(ksi)	-	24.4	-	24.4	-
VR	(k)	44.5	-	47.3	-	44.6

INTERIOR GIRDER REACTION TABLE					
		*** W. Abut.	Pier #1	Pier #2	*** E. Abut.
$R\rho$	(k)	17.2	76.8	76.8	17.2
R_L	(k)	35.4	42.7	42.7	35.4
R_I	(k)	10.6	12.8	12.8	10.6
R_{Total}	(k)	63.3	132.4	132.4	63.3

* Compact section
 ** Braced non-compact and partially braced section
 *** These reactions include 3 kips per foot dead load from the Approach Pavement.

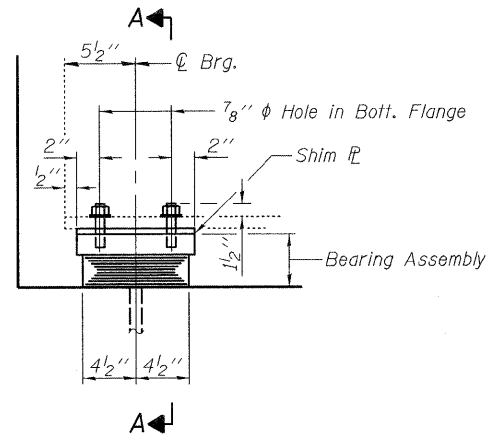
I_s, S_s : Non-composite moment of inertia and section modulus of the steel section used for computing f_s (Total and Overload) due to non-composite dead loads (in⁴ and in³).
 $I_c(n), S_c(n)$: Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing f_s (Total and Overload) due to short-term composite live loads (in⁴ and in³).
 $I_c(3n), S_c(3n)$: Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing f_s (Total and Overload) due to long-term composite (superimposed) dead loads (in⁴ and in³).
 ρ : Un-factored non-composite dead load (kips/ft.).
 $M\rho$: Un-factored moment due to non-composite dead load (kip-ft.).
 $s\rho$: Un-factored long-term composite (superimposed) dead load (kips/ft.).
 $M_s\rho$: Un-factored moment due to long-term composite (superimposed) dead load (kip-ft.).
 M_L : Un-factored live load moment (kip-ft.).
 M_I : Un-factored moment due to impact (kip-ft.).
 M_o : Factored design moment (kip-ft.).
 $1.3 [M\rho + M_s\rho + \frac{5}{3} (M_L + M_I)]$
 M_u : Compact composite moment capacity according to AASHTO LFD 10.50.1.1 or compact non-composite moment capacity according to AASHTO LFD 10.48.1 (kip-ft.).
 f_s (Overload): Sum of stresses as computed from the moments below (ksi).
 $M\rho + M_s\rho + \frac{5}{3} (M_L + M_I)$
 f_s (Total): Sum of stresses as computed from the moments below on non-compact section (ksi).
 $1.3 [M\rho + M_s\rho + \frac{5}{3} (M_L + M_I)]$
 VR: Maximum $L +$ impact horizontal shear range within the composite portion of the span for stud shear connector design (kips).

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

BEAM MOMENT & REACTION TABLES
STRUCTURE NO. 022-0033

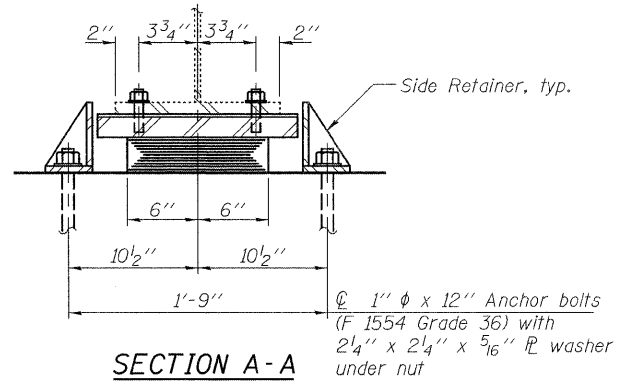
rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 20 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	10HB-R	Du Page	53	36
		CONTRACT NO. 60B92				
		FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION AT ABUT.

TYPE I ELASTOMERIC EXP. BRG.

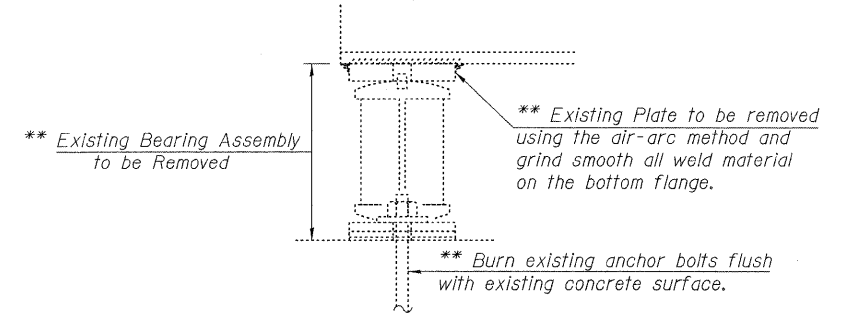


SECTION A-A

Beam No.	Shim Height (in)	
	West Abut	East Abut
1	-	-
2	5/8	3/4
3	-	-
4	5/8	3/4
5	1/8	-
6	5/8	5/8
7	5/8	5/8
8	-	1/8
9	3/4	5/8
10	-	-
11	3/4	5/8
12	-	-

SHIM TABLE

Match Plan dimensions of top bearing plate.
Cost Included with Furnishing and Erecting Structural Steel.



**Cost is Included with Jacking and Cribbing.

REMOVE EXISTING BEARINGS
AT ABUTMENTS

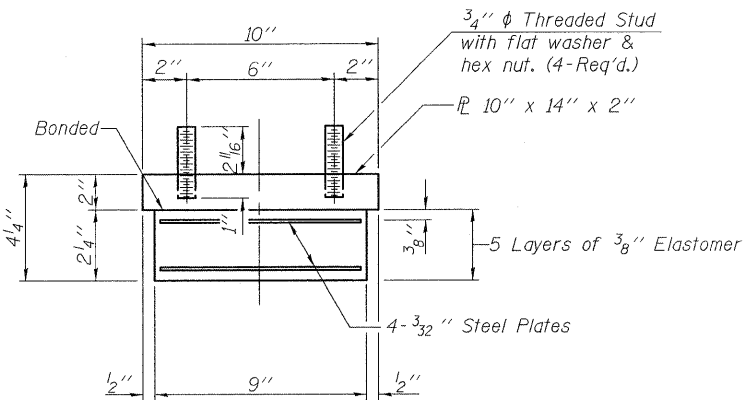
JACKING AND CRIBBING PROCEDURES

- Prior to commencing any work at the bearings, the contractor shall submit plans for Jacking and Cribbing for approval by the Engineer.

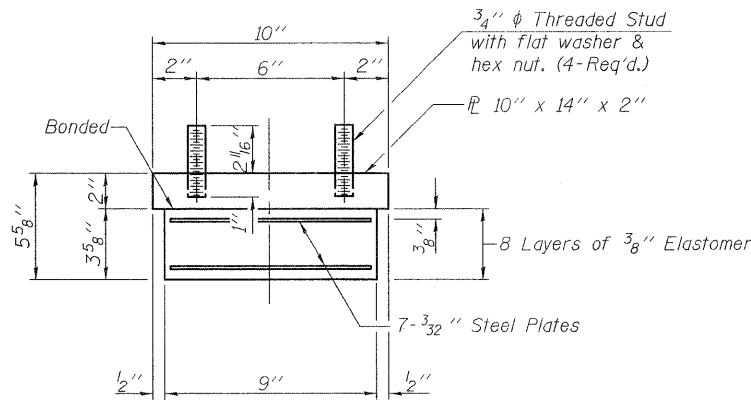
In each removal stage, Jacking and Cribbing shall be done after the existing deck is removed and before the new deck is poured.
- Jacking shall be limited so that the maximum lift transversely between adjacent beams is 1/8". See Special Provision for Jacking and Cribbing.
- Minimum Jack capacity is 12 Tons per bearing.

Notes:

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Anchor bolts for side retainers may be cast in place or installed in holes drilled before or after members are in place.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.
Prior to ordering any material, the contractor shall verify in the field all bearing height and shim thickness dimensions.
Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

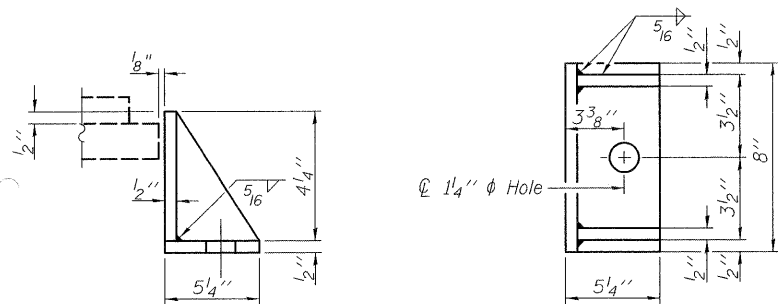


W. ABUT. BEARING ASSEMBLY



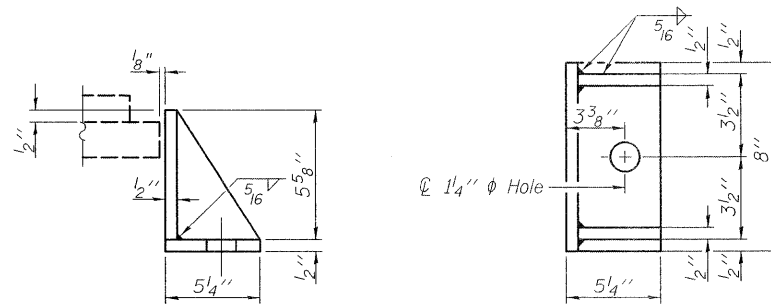
E. ABUT. BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.



W. ABUT. SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



E. ABUT. SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	24
Anchor Bolts, 1 inch	Each	48
Jacking and Cribbing	Each	24
Furnishing and Erecting Structural Steel	Pound	330

***Locations for Jacking and Cribbing include 12 beam ends at each Abutment.

BEARING DETAILS
STRUCTURE NO. 022-0033

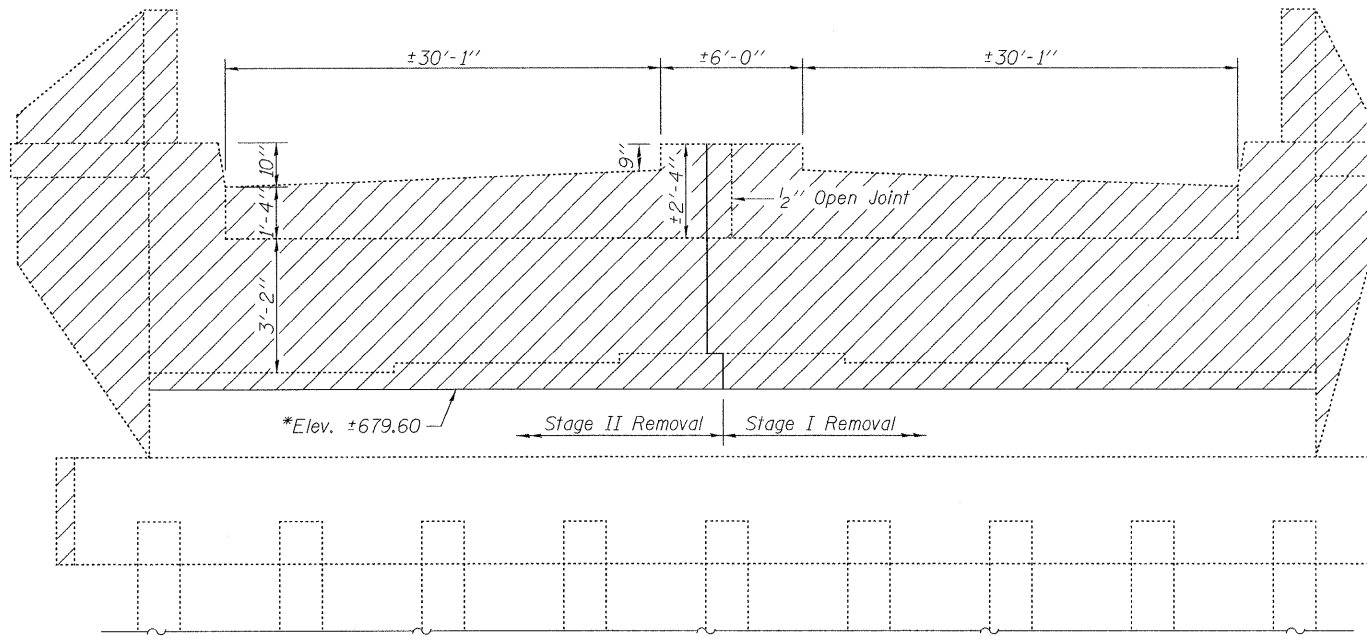
DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

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Wheaton, IL 60187

SHEET NO. 21 27 SHEETS	F.A.P. RTE. 311	SECTION 10HB-R	COUNTY Du Page	TOTAL SHEETS 53	SHEET NO. 37
	CONTRACT NO. 60B92			FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT	

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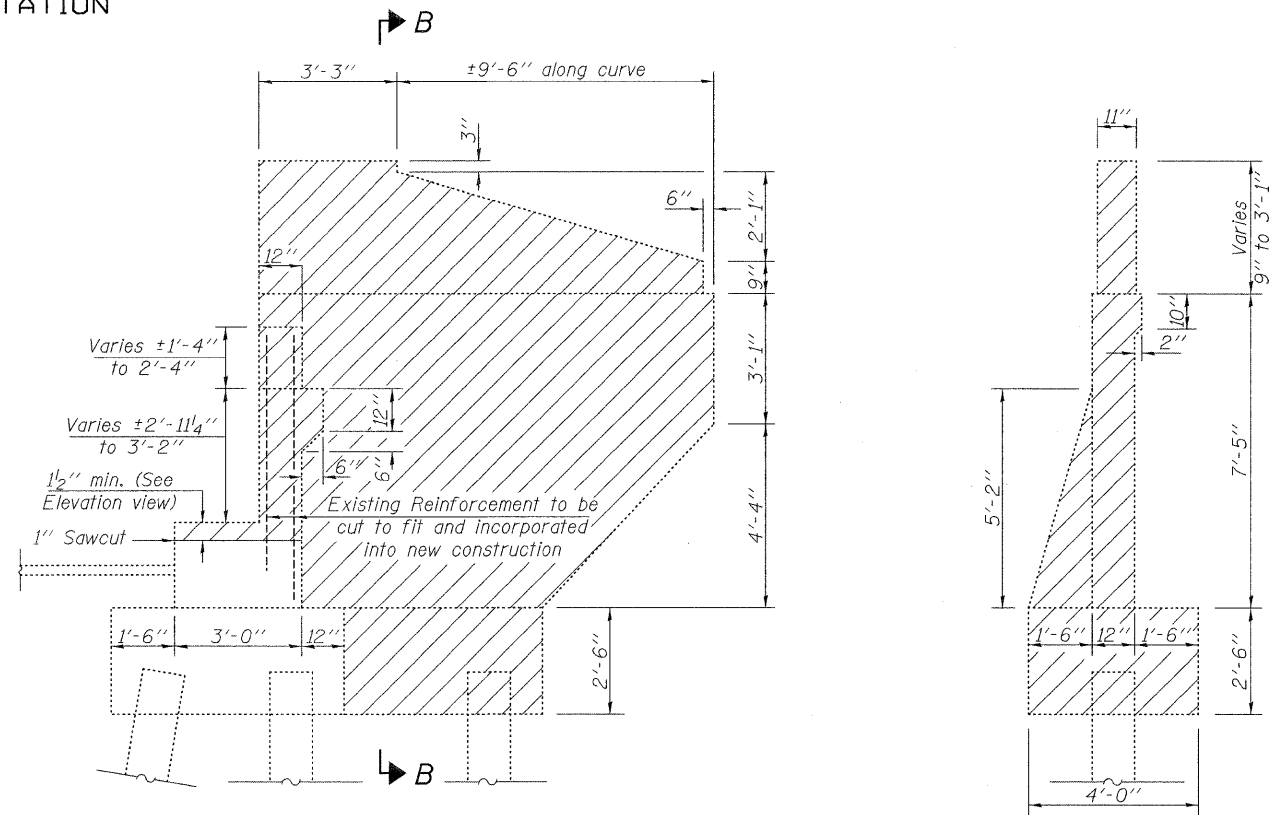
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION

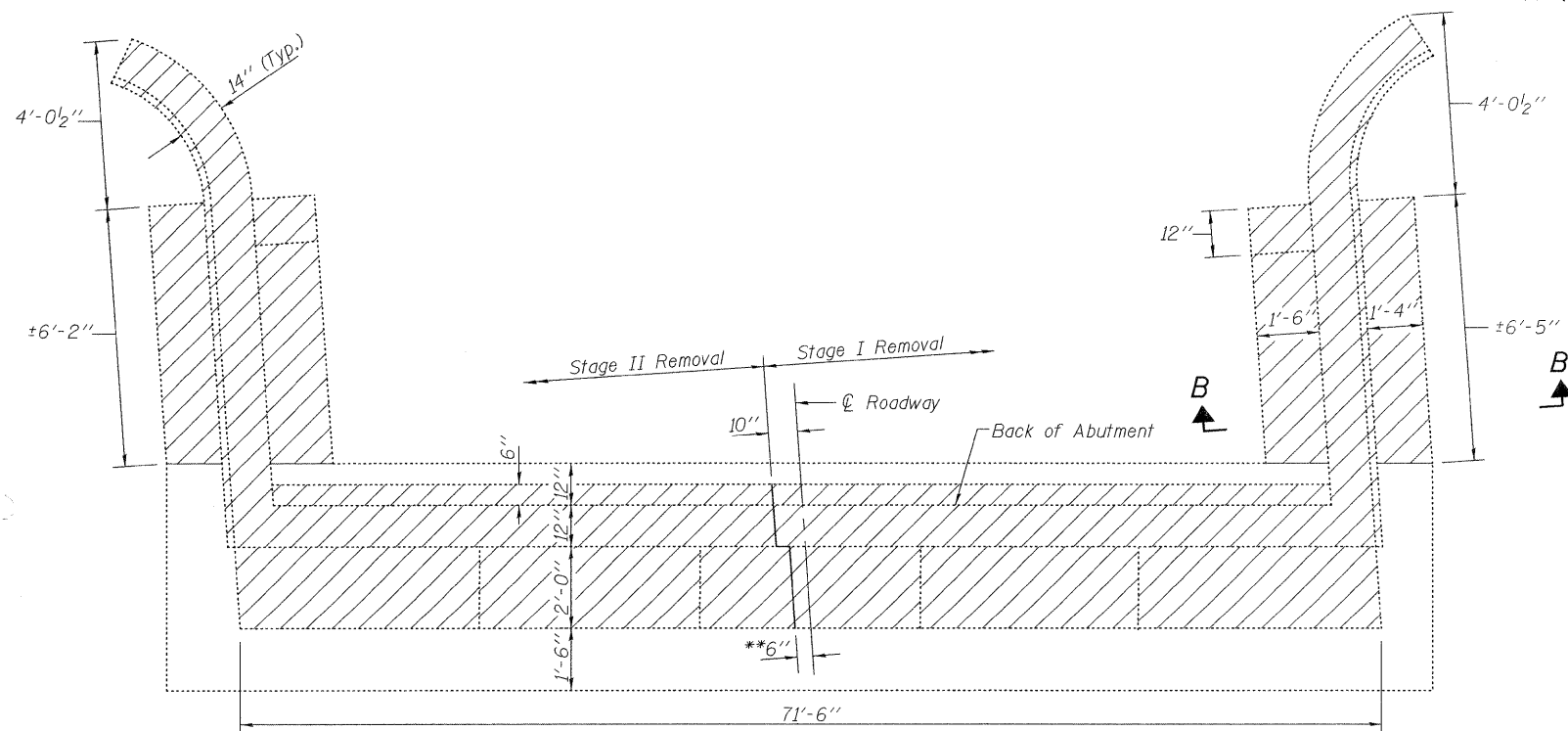
East Abutment shown, looking east.
West Abutment similar but opposite hand.

*Remove a minimum of 1 1/2" of existing concrete from existing abutment seats. If any unsound concrete remains, additional concrete shall be removed as directed by the Engineer. Additional removal to be paid as Concrete Removal.



VIEW A-A

SECTION B-B



PLAN

East Abutment shown.
West Abutment similar but opposite hand.

**Avoid damaging or undermining existing bearing plate during concrete removal.

**BILL OF MATERIAL
(TWO ABUTMENTS)**

Item	Unit	Total
Concrete Removal	Cu. Yd.	60.6

LEGEND

Concrete Removal

NOTES

Existing reinforcement bars extending into the removal area shall be blast cleaned to gray metal and straightened. Blast cleaning and straightening shall be included with the cost of Concrete Removal.

Existing reinforcement not extending into areas of new construction shall be cut at the removal line and removed. Cost included with Concrete Removal.

Existing reinforcement bars which have lost more than 25% of their original diameter shall be supplemented by new epoxy coated bars of the same diameter, spliced in place. Furnishing and placing supplemental epoxy coated reinforcement bars shall be included with the cost of Reinforcement Bars, Epoxy Coated.

Care shall be exercised by the contractor during and following removal operations to ensure that the existing rebar remaining in place are not damaged. All protruding rebar shall be cleaned, straightened, and properly positioned prior to concrete placement. Any rebar damaged during concrete removal shall be repaired or replaced using an approved Bar Splicer or Mechanical System. Cost included with Concrete Removal.

**ABUTMENT DETAILS - I
STRUCTURE NO. 022-0033**

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

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Wheaton, IL 60187

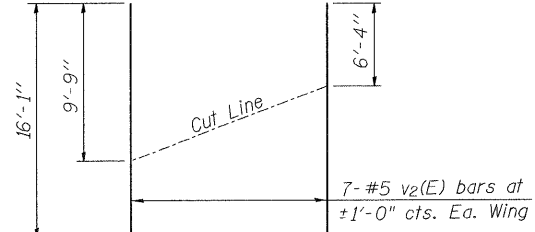
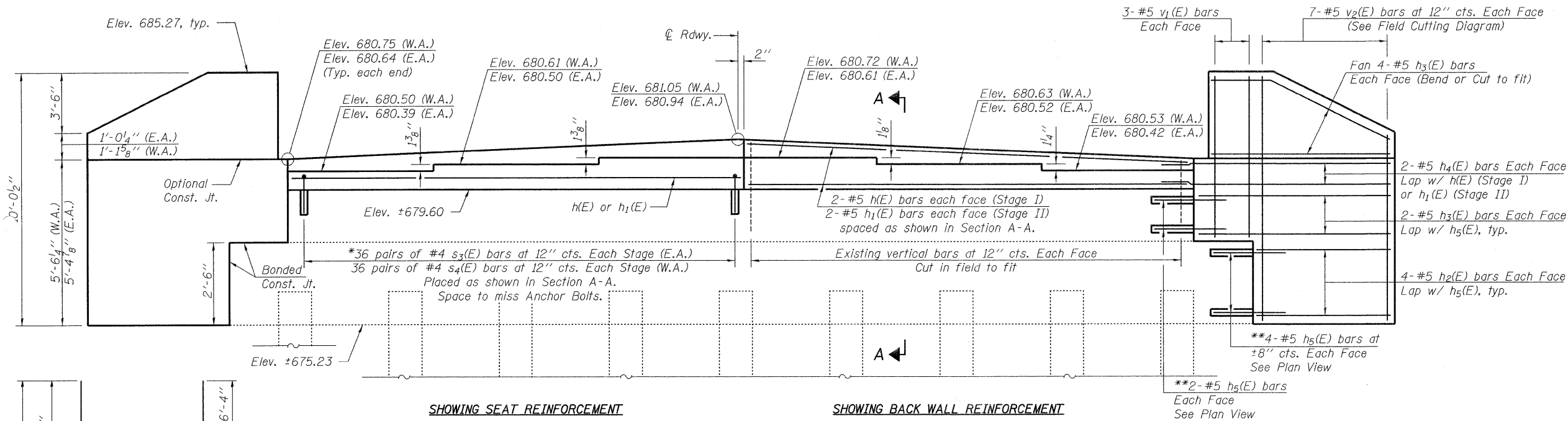
SHEET NO. 22
27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	Du Page	53	38
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

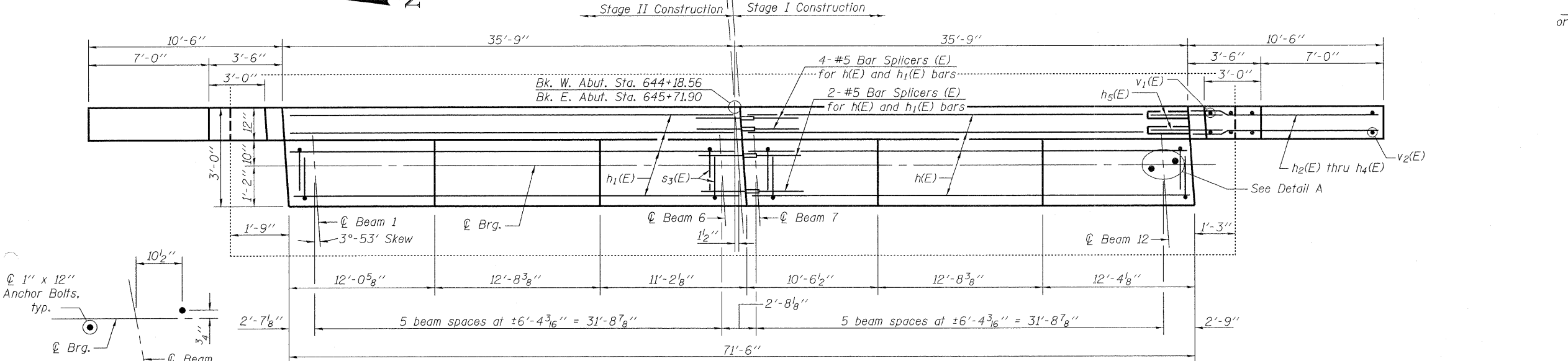
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

2 ABUTMENTS
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	12	#5	35'-4"	—
h ₁ (E)	12	#5	35'-8"	—
h ₂ (E)	32	#5	8'-0"	—
h ₃ (E)	48	#5	9'-8"	—
h ₄ (E)	16	#5	12'-0"	—
h ₅ (E)	48	#5	3'-5"	—
s ₃ (E)	144	#4	2'-8"	┘
s ₄ (E)	144	#4	2'-10"	┘
v ₁ (E)	24	#5	7'-3"	—
v ₂ (E)	28	#5	16'-1"	—
Reinforcement Bars, Epoxy Coated		Pound	3,200	
Concrete Structures		Cu. Yd.	29.4	
Bar Splicers		Each	12	



FIELD CUTTING DIAGRAM
Order v₂(E) bars full length.
Cut as shown and use remainder of bars in opposite face.



DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

DESIGNED JMT
CHECKED BLB
DRAWN JMT
CHECKED BLB

ELEVATION
East Abutment shown, looking east.
West Abutment similar but opposite hand.

LEGEND
(W.A.) - West Abutment
(E.A.) - East Abutment

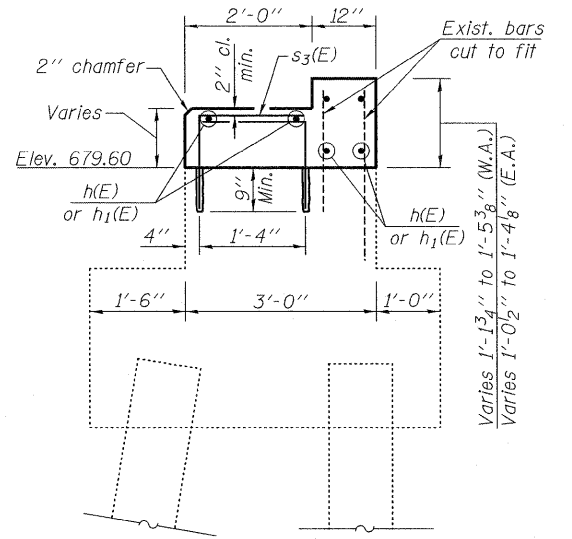
* Epoxy grout #4 s₃(E) bars into 3/4" φ x 9" drilled holes located as shown in Section A-A. See Section 584 of the Std. Specifications.

** Epoxy grout #5 h₅(E) bars into 7/8" φ x 9" drilled holes. Center of the hole shall be drilled a minimum of 4" from the face of the existing structure. See Section 584 of the Std. Specifications.

PLAN
East Abutment shown.
West Abutment similar but opposite hand.

NOTES
All construction joints between new and existing concrete shall be Bonded Construction Joints.
Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.

BAR s₃(E) and s₄(E)

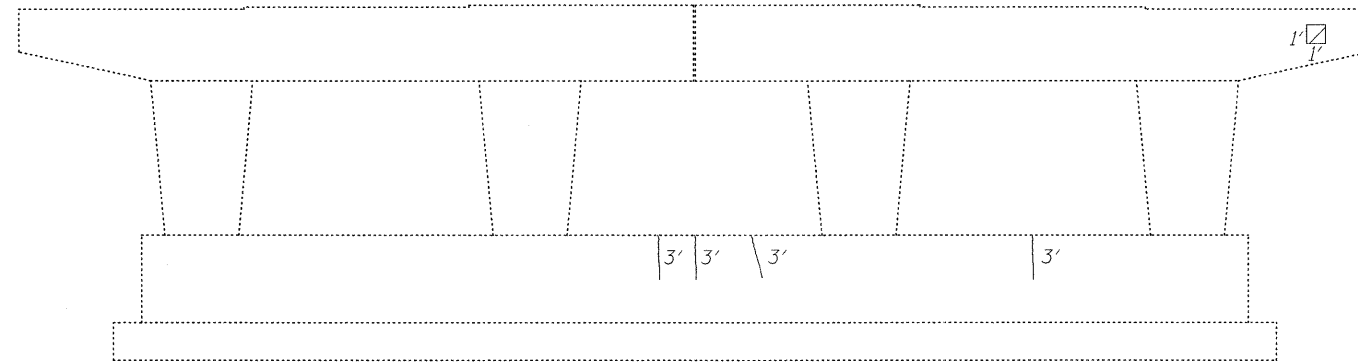


SECTION A-A

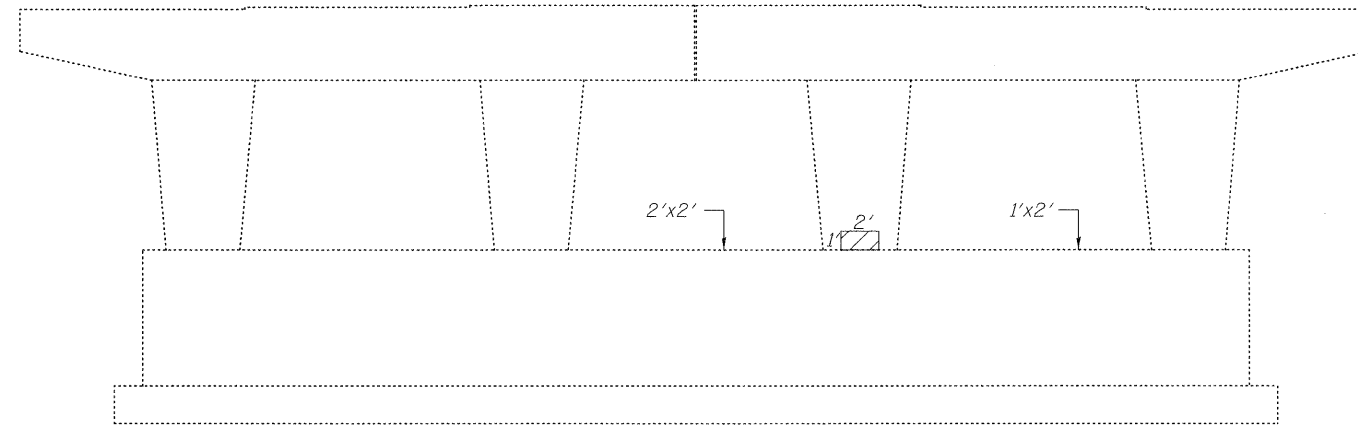
**ABUTMENT DETAILS - II
STRUCTURE NO. 022-0033**

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 23	F.A.P. RTE. 311	SECTION 10HB-R	COUNTY Du Page	TOTAL SHEETS 53	SHEET NO. 39
	27 SHEETS	CONTRACT NO. 60B92				
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT						

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION





PIER #1
(East Face has no deterioration)



PIER #2
(East Face has no deterioration)

LEGEND

 Structural Repair of Concrete
(Depth Equal to or Less Than 5')

 Epoxy Crack Sealing

BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair of Concrete (Depth Equal to or Less Than 5')	Sq. Ft.	20*
Epoxy Crack Injection	Foot	12

* Quantity has been increased to allow for additional repairs as directed by the Engineer.

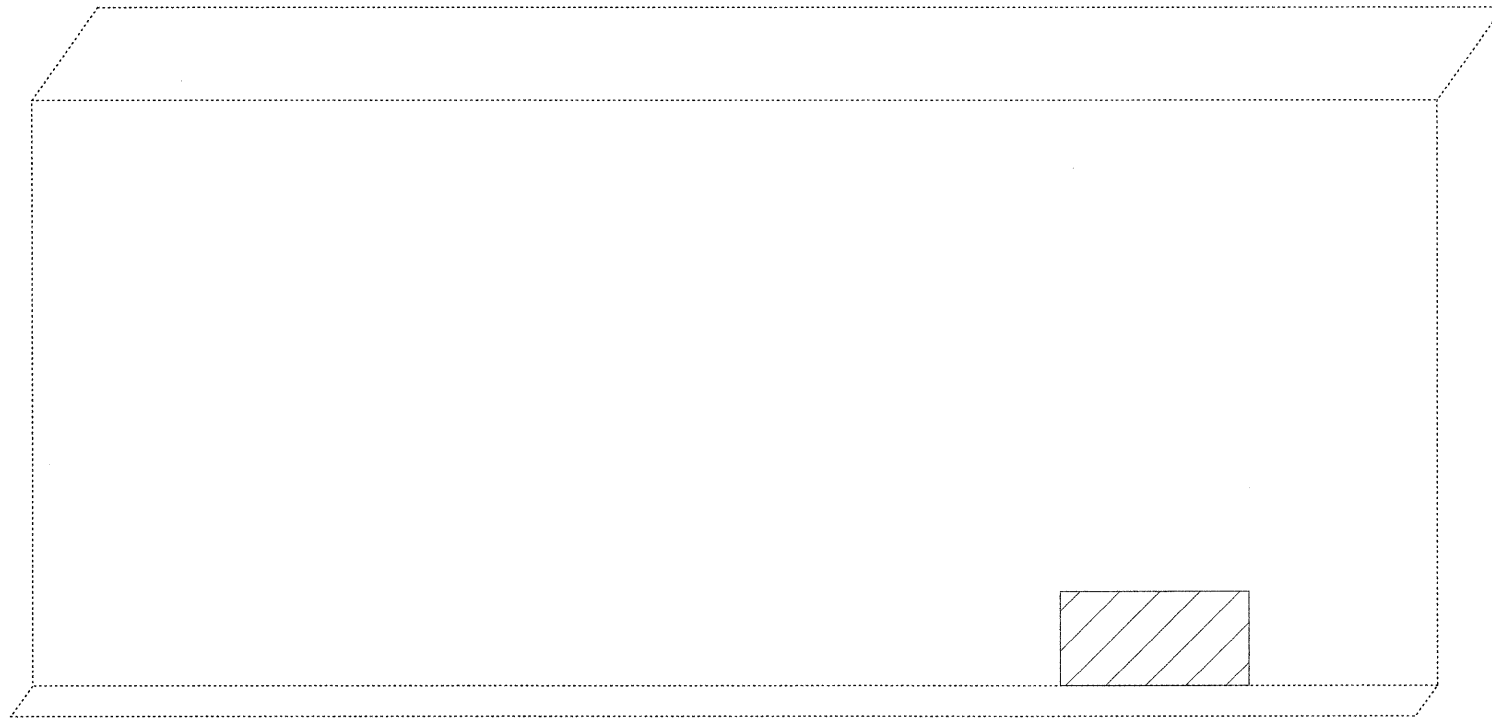
DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

PIER REPAIRS
STRUCTURE NO. 022-0033

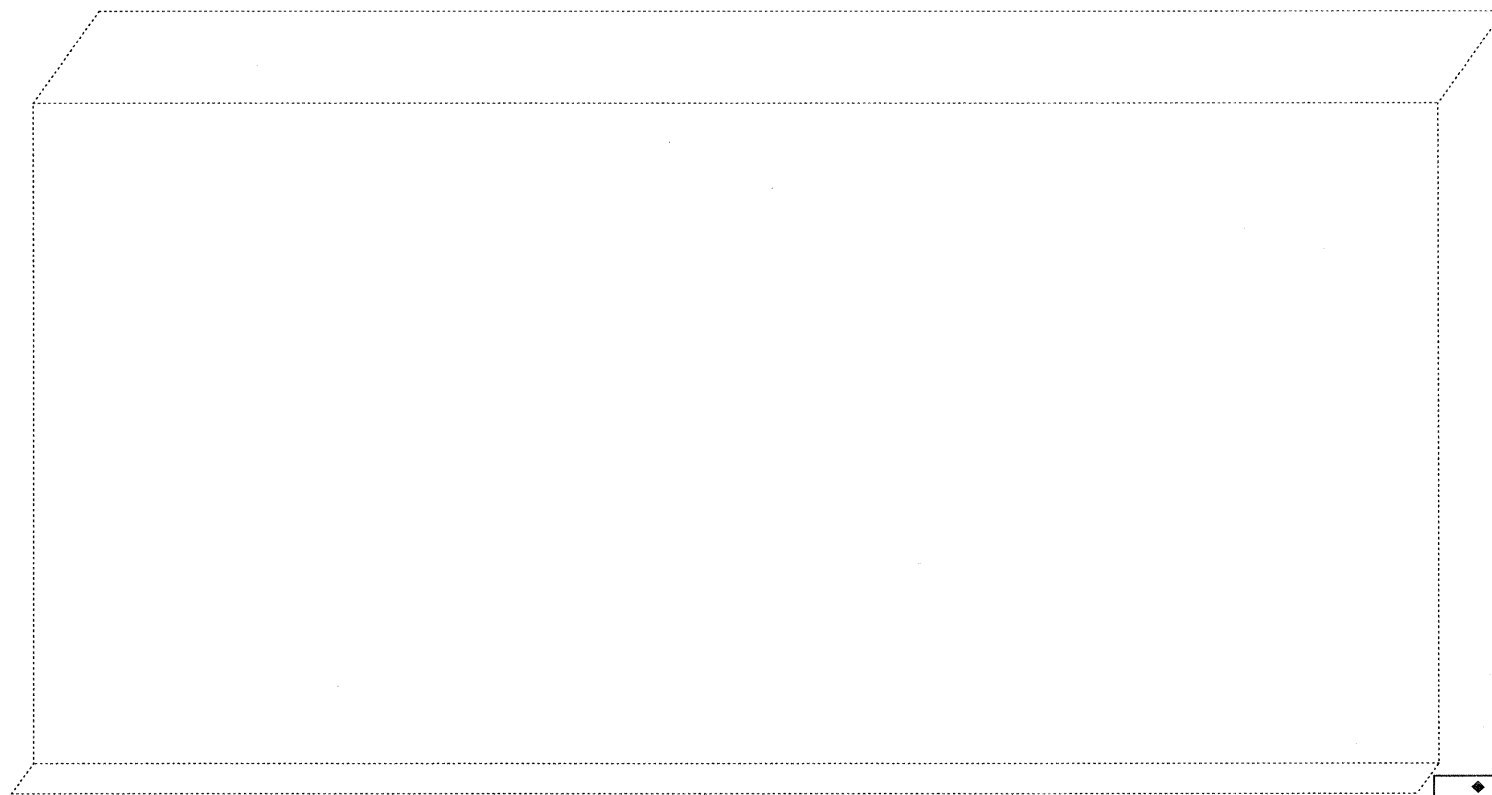
rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 24	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	27 SHEETS	311	10HB-R	Du Page	53	40
			CONTRACT NO. 60B92			
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT						

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



WEST SLOPE WALL



EAST SLOPE WALL

NOTE:

Sloped wall shall be reinforced with welded wire fabric, 6" x 6" - W4.0 x W4.0, weighing 58 lbs. per 100 sq. ft.

LEGEND



Slope Wall Repair

BILL OF MATERIAL

Item	Unit	Quantity
Slope Wall Removal	Sq. Yd.	5.6
Slope Wall, 4 Inch	Sq. Yd.	5.6

**SLOPE WALL REPAIRS
STRUCTURE NO. 022-0033**

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

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200 West Front Street
Wheaton, IL 60187

SHEET NO. 25
27 SHEETS

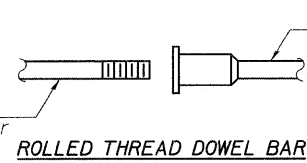
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	Du Page	53	41
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B92	

8/7/2009 K:\11225510\structures\Final Plans\Substructure.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

The diameter of this part is equal or larger than the diameter of bar spliced.

The diameter of this part is the same as the diameter of the bar spliced.

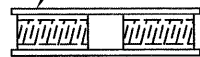


ROLLED THREAD DOWEL BAR



** ONE PIECE

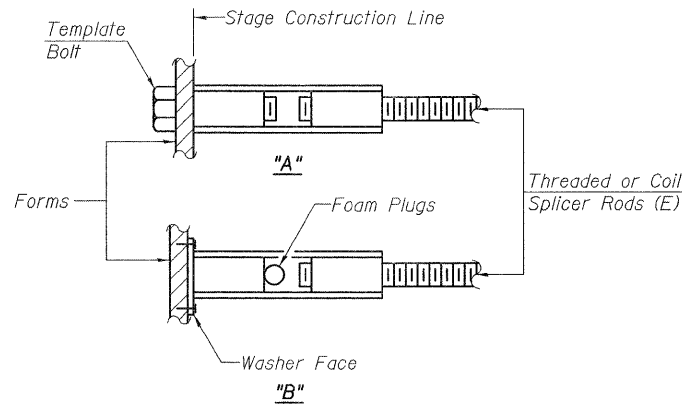
Wire Connector



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



INSTALLATION AND SETTING METHODS

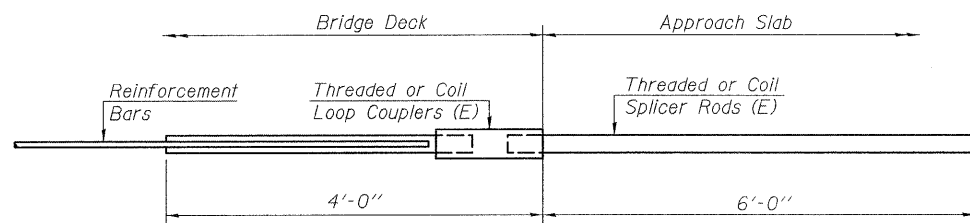
"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

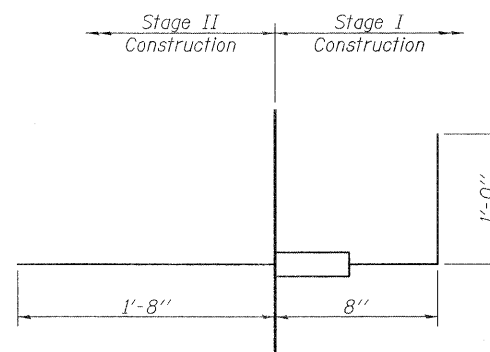
- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_t$
 - ② Minimum *Pull-out Strength (Tension in kips) = $0.66 \times f_y \times A_t$
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_t = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



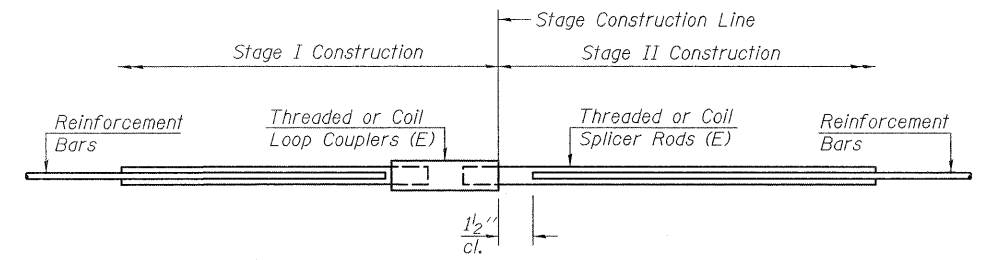
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 144



#6 BAR SPLICER (E) BETWEEN BEAMS 6 AND 7

SPECIAL SPLICER DETAIL
(Diaphragms)



STANDARD

Bar Size	No. Assemblies Required	Location
#5	395	Deck
#5	172	Approach
#6	24	Diaphragms
#5	12	Abutments
#4	50	Approach

BAR SPLICER DETAILS
STRUCTURE NO. 022-0033

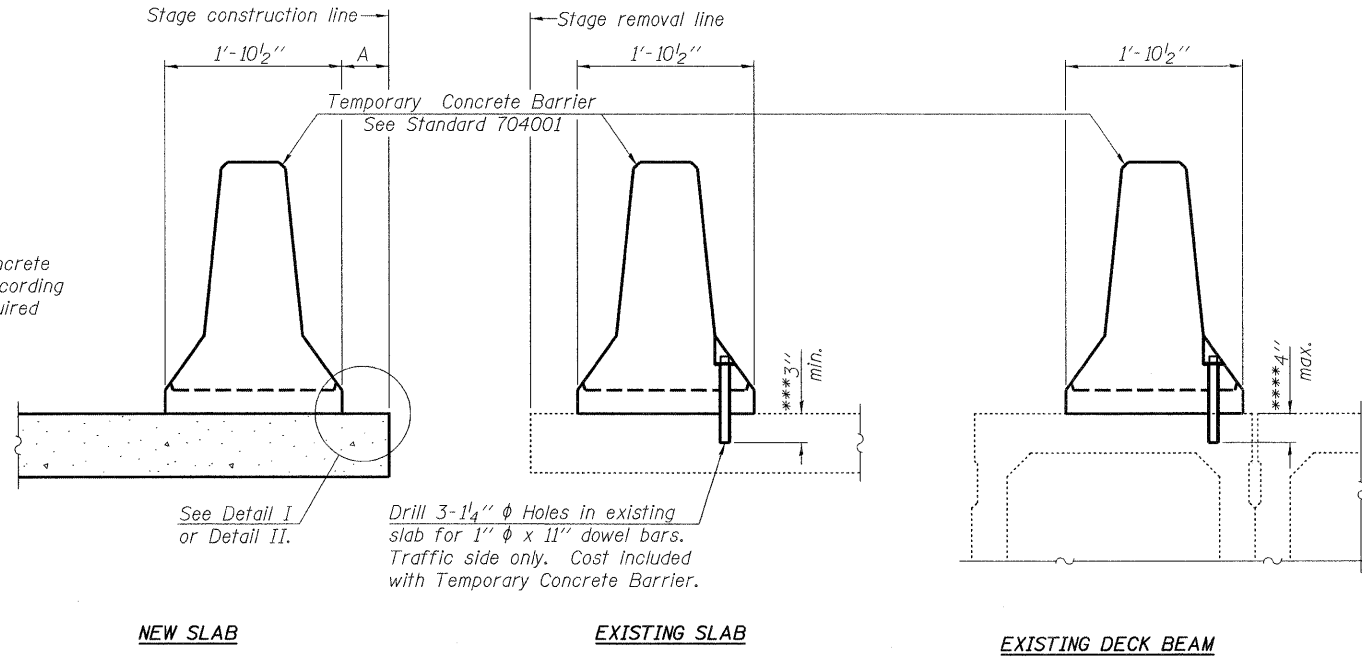
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DRAWN	JMT
CHECKED	BLB

rjngroup Excellence through Ownership 200 West Front Street Wheaton, IL 60187	SHEET NO. 26 27 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	10HB-R	Du Page	53	42
		CONTRACT NO. 60B92				
		FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT				

9/21/2009 K:\112253\Structures\Final Plans\Misc Bars Sheets.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

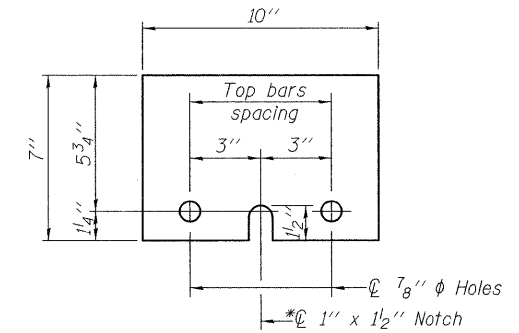
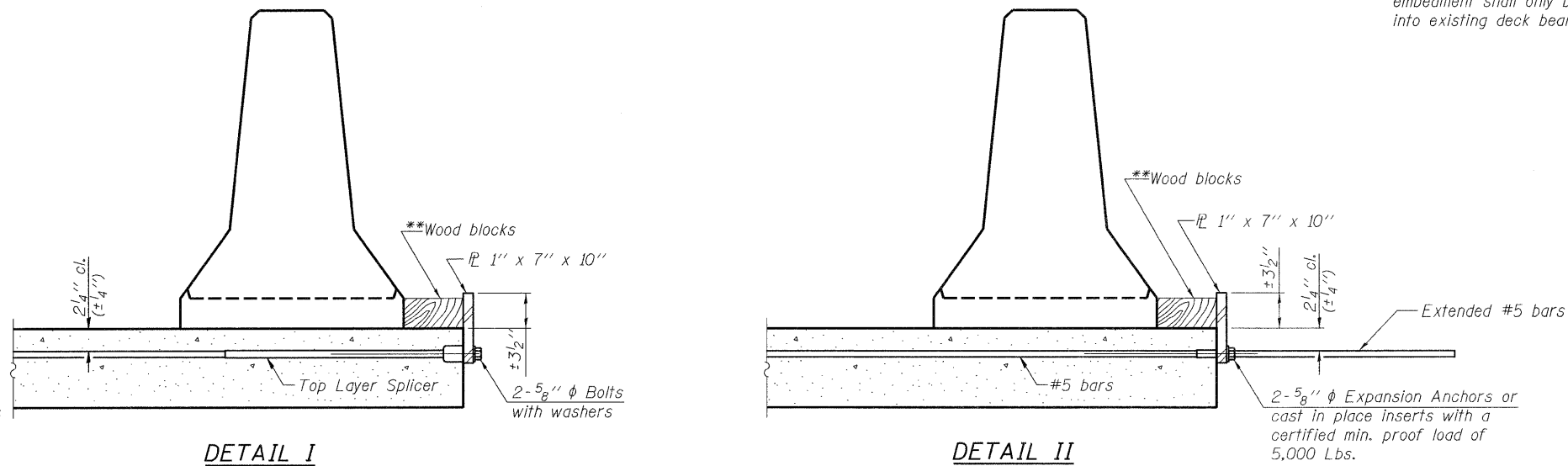
Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{L} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



STEEL RETAINER \bar{L} 1" x 7" x 10"

* Required only with Detail II

**Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

TEMPORARY CONCRETE BARRIER DETAILS
STRUCTURE NO. 022-0033

DESIGNED	JMT
CHECKED	BLB
DRAWN	JMT
CHECKED	BLB

R-27

10-1-08

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200 West Front Street
Wheaton, IL 60187

SHEET NO. 27
27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	Du Page	53	43
CONTRACT NO. 60B92				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SOIL BORING LOG

Page 1 of 1

DATE 1958

ROUTE F.A.P. 311 DESCRIPTION US-34 (OGDEN AVE.) OVER IL-53 LOGGED BY

SECTION 10HB-R LOCATION SE 14, SEC. 3, TWP. 38N, RNG. 10E, 3rd PM

COUNTY DUPAGE DRILLING METHOD HAMMER TYPE

STRUCT. NO. 022-0033 WEATHER COND. TEMP.
Station 644+95.23

BORING NO. 13	D	B	U	M	Surface Water Elev. _____ ft	D	B	U	M
Station 644+50	E	L	C	O	Stream Bed Elev. _____ ft	E	L	C	O
Offset 54 ft W of CL	P	O	S	I		P	O	S	I
Longitude/Northing _____	T	W	S	S	Groundwater Elev.:	T	W	S	S
Latitude/Easting _____	H	S	Qu	T	First Encounter _____ ft	H	S	Qu	T
Ground Surface Elev. _____ ft	(ft)	(6")	(tsf)	(%)	Upon Completion _____ ft	(ft)	(6")	(tsf)	(%)
					After _____ Hrs. _____ ft				

Boring begins at ground surface from 1958. No information available in embankment cone.									
1958 Ground Surface 667.6	0								
Medium Black Clayey Silt					-20				
665.8									
Stiff Brown Silty Sandy Clay									
663.8									
662.8	-5								
661.8		44							
660.3									
Very Dense Brown Angular Well Graded Gravel									
659.3		64							
657.8	-10								
656.8		58							
655.8									
655.3									
Dense Gray Uniform Fine Sand									
654.3		23							
653.8									
Very Dense Gray Angular Poorly Graded Gravel									
652.8	-15								
651.8		105							

SOIL BORING LOG

Page 1 of 1

DATE 1958

ROUTE F.A.P. 311 DESCRIPTION US-34 (OGDEN AVE.) OVER IL-53 LOGGED BY

SECTION 10HB-R LOCATION SE 14, SEC. 3, TWP. 38N, RNG. 10E, 3rd PM

COUNTY DUPAGE DRILLING METHOD HAMMER TYPE

STRUCT. NO. 022-0033 WEATHER COND. TEMP.
Station 644+95.23

BORING NO. 14	D	B	U	M	Surface Water Elev. _____ ft	D	B	U	M
Station 645+45	E	L	C	O	Stream Bed Elev. _____ ft	E	L	C	O
Offset 48 ft W of CL	P	O	S	I		P	O	S	I
Longitude/Northing _____	T	W	S	S	Groundwater Elev.:	T	W	S	S
Latitude/Easting _____	H	S	Qu	T	First Encounter _____ ft	H	S	Qu	T
Ground Surface Elev. _____ ft	(ft)	(6")	(tsf)	(%)	Upon Completion _____ ft	(ft)	(6")	(tsf)	(%)
					After _____ Hrs. _____ ft				

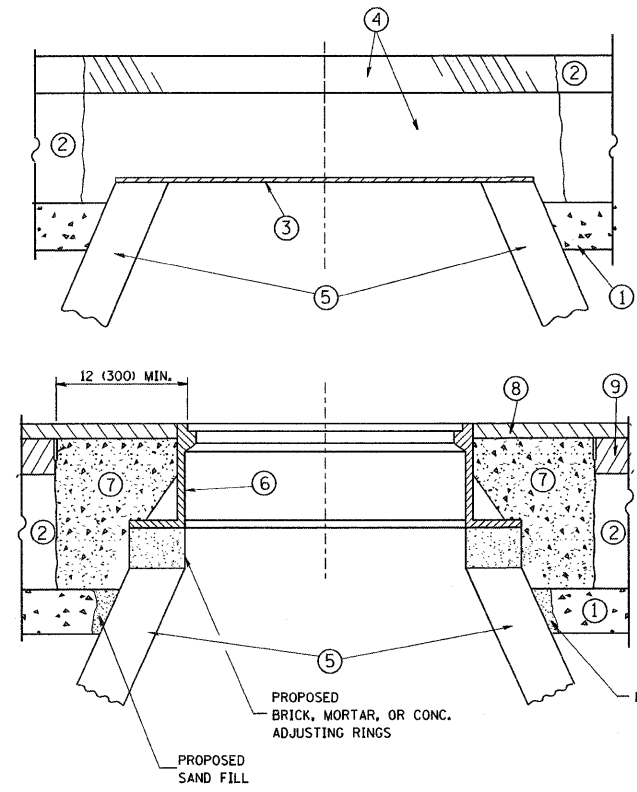
Boring begins at ground surface from 1958. No information available in embankment cone.									
1958 Ground Surface 662.5	0								
Medium Black Silty Gravelly Clay									
660.3									
658.8									
657.8	-5								
656.8		50							
655.8									
Very Dense Angular Gravel and Cobbles									
655.3									
Ruptured Sample		16							
654.3									
Very Stiff Gray Clay Till									
652.8	-10								
651.8		16							
651.3									
Free Water Level 650.8									
650.3									
649.3		66							
Very Dense Gray Poorly Graded Gravel									
647.8	-15								
646.8		55							

SOIL BORING LOGS
STRUCTURE NO. 022-0033

FOR INFORMATION ONLY

SHEET NO. 27A	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
27 SHEETS	311	10HB-R	DUPAGE	53	43A
			CONTRACT NO. 60B92		
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	44
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

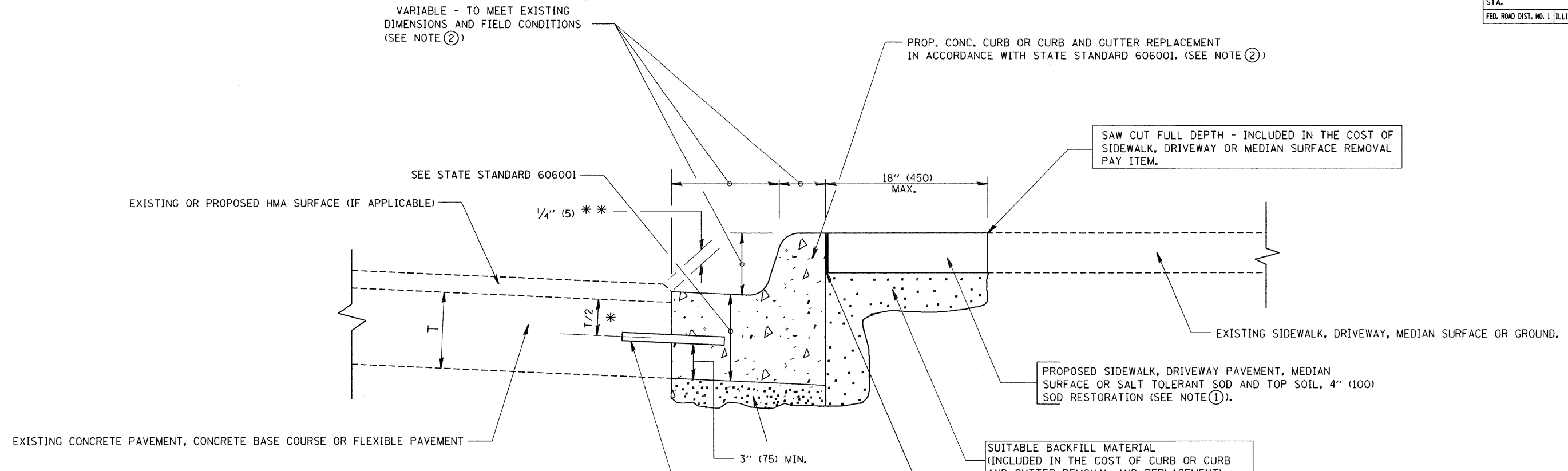
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	45
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

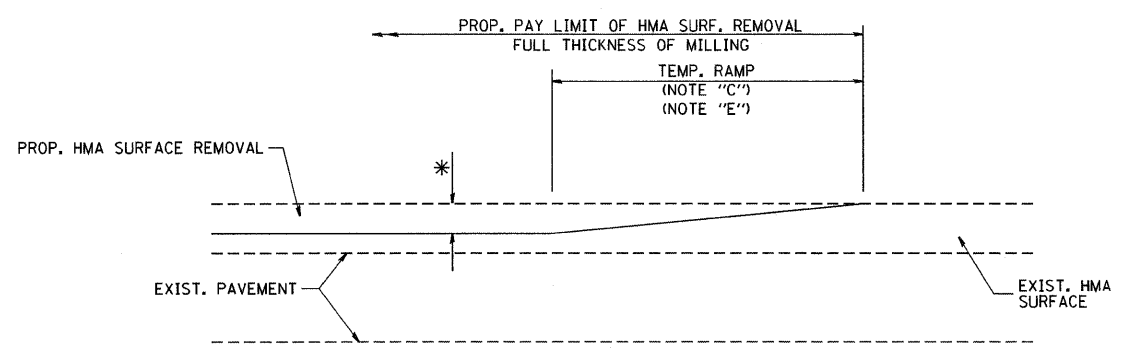
ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

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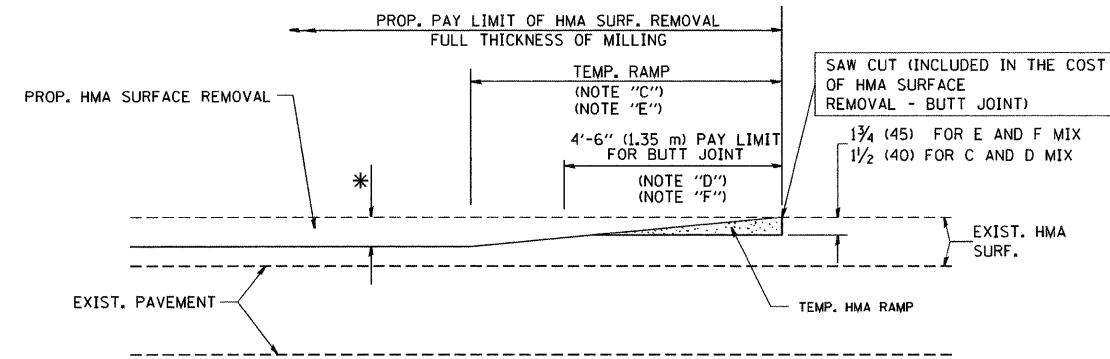
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USER NAME = bboard

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	46
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

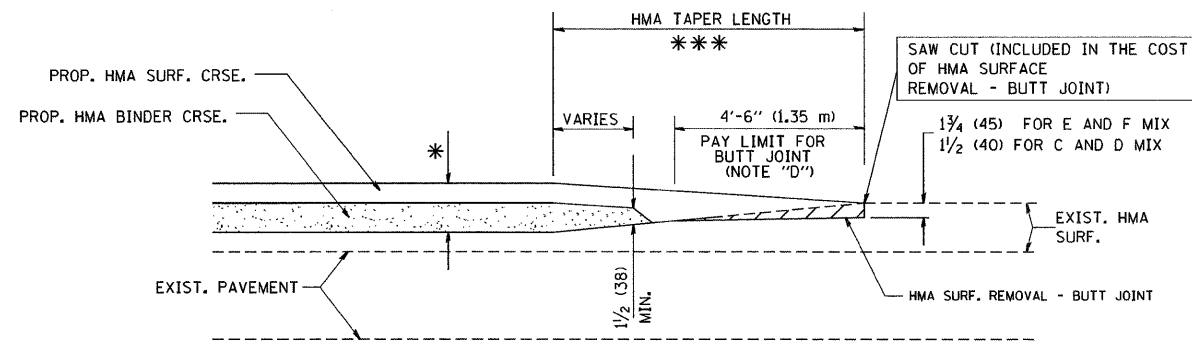
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

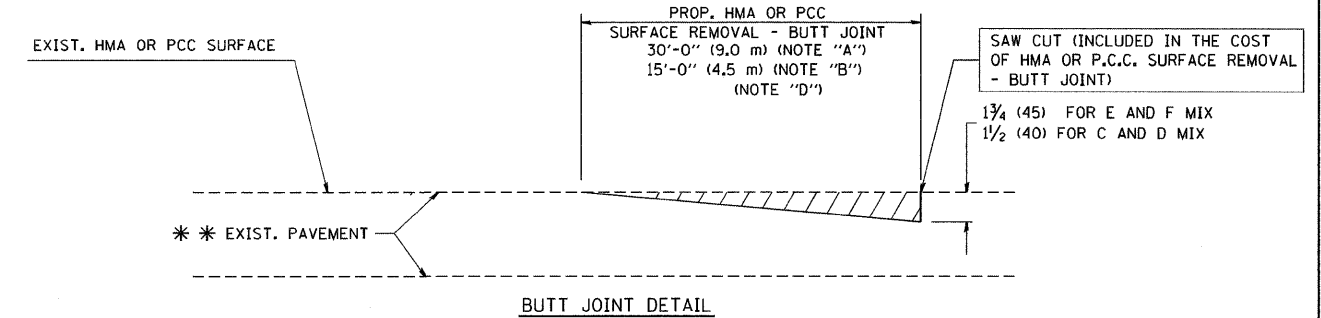
OPTION 2

TYPICAL TEMPORARY RAMP

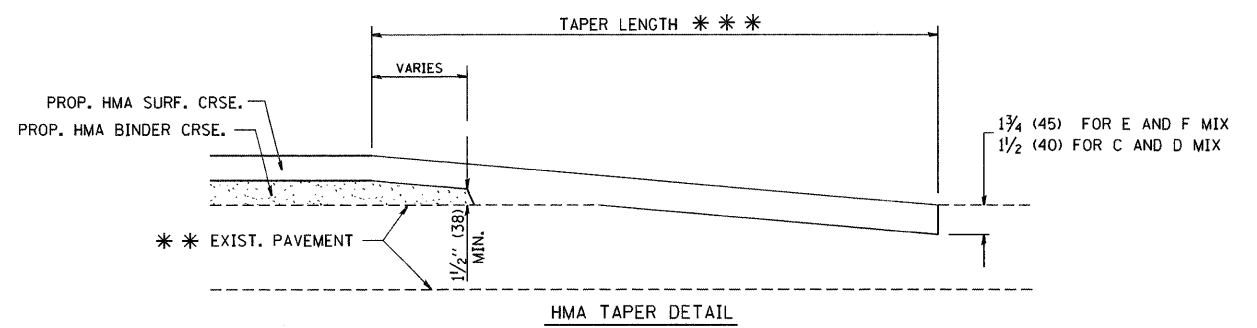


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE
HORIZ.

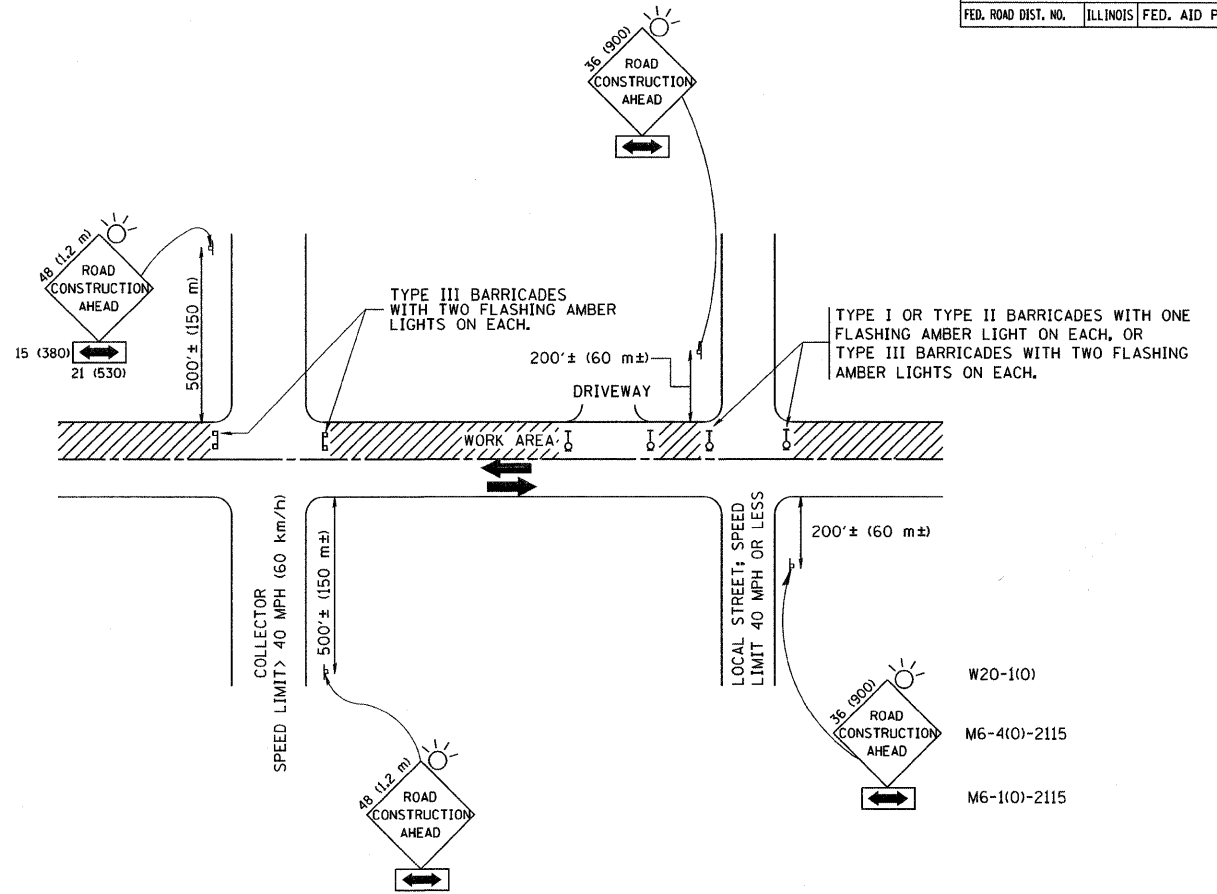
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BD400-05 (VI-BD32)

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USER NAME = bwardf

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	47
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

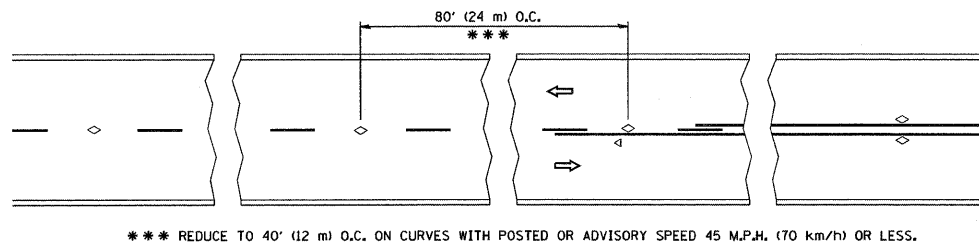
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

SCALE: NONE

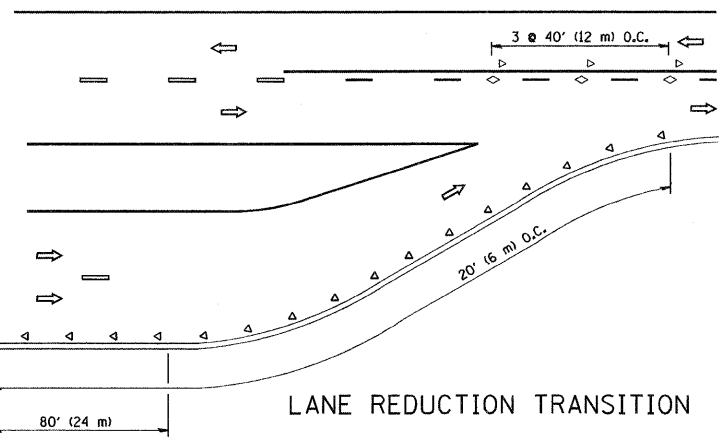
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 CHECKED BY
 TC-10

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

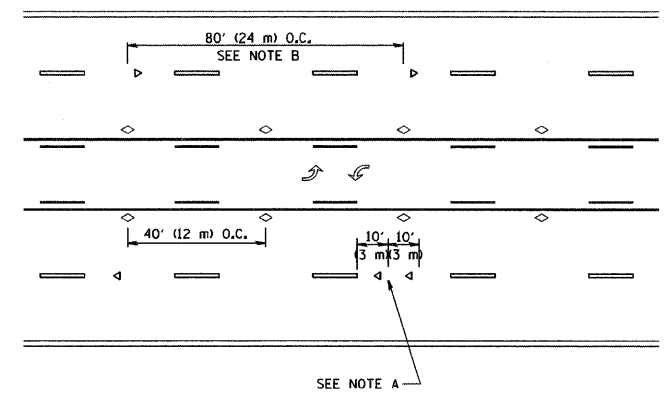


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

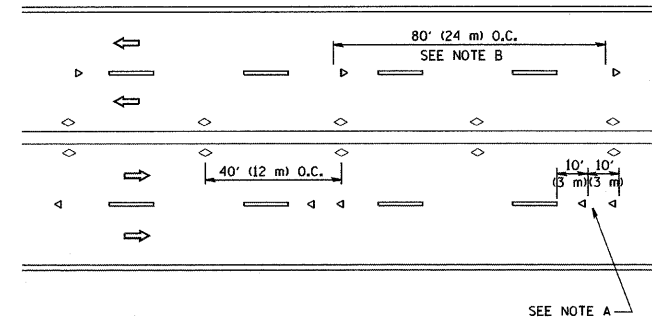
TWO-LANE/TWO-WAY



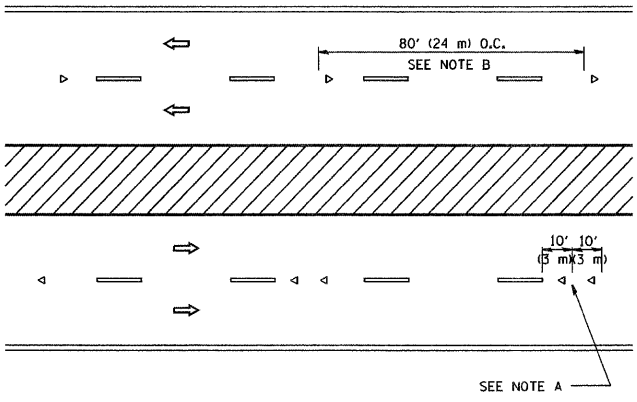
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

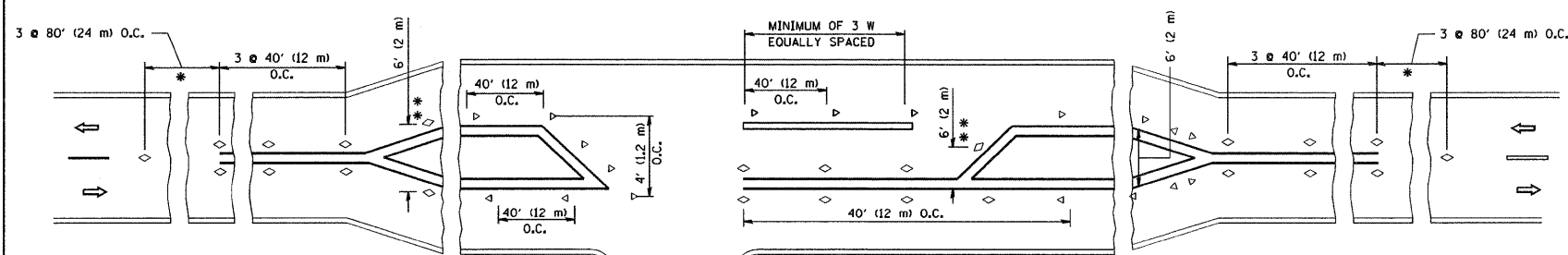
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◊ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

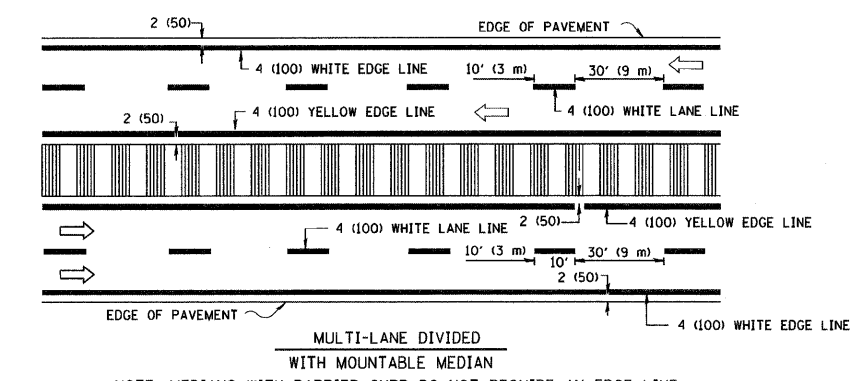
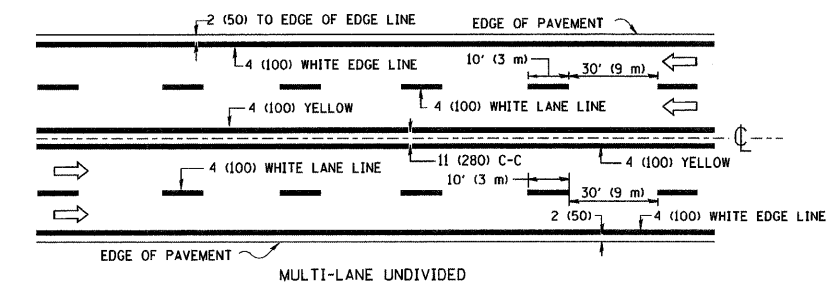
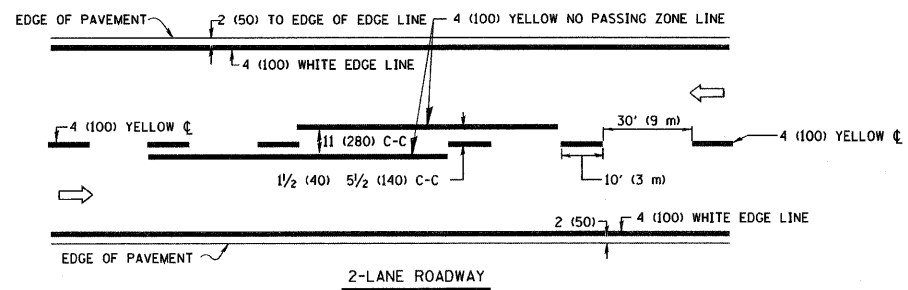
ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT
 MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE

DRAWN BY CADD
 CHECKED BY
 TC-11

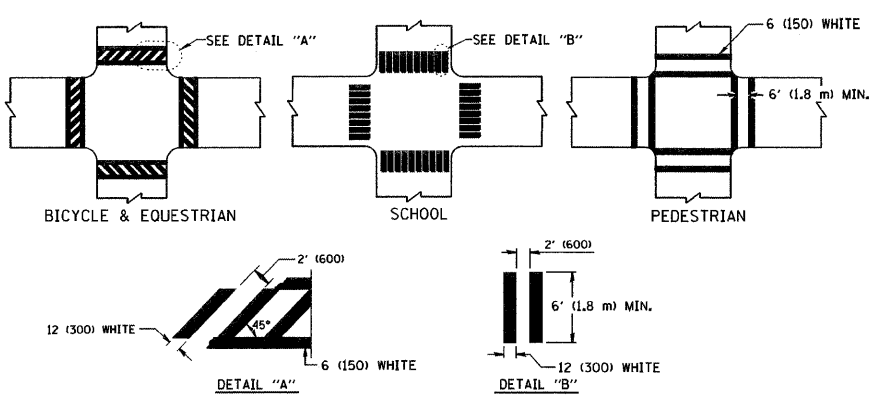
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

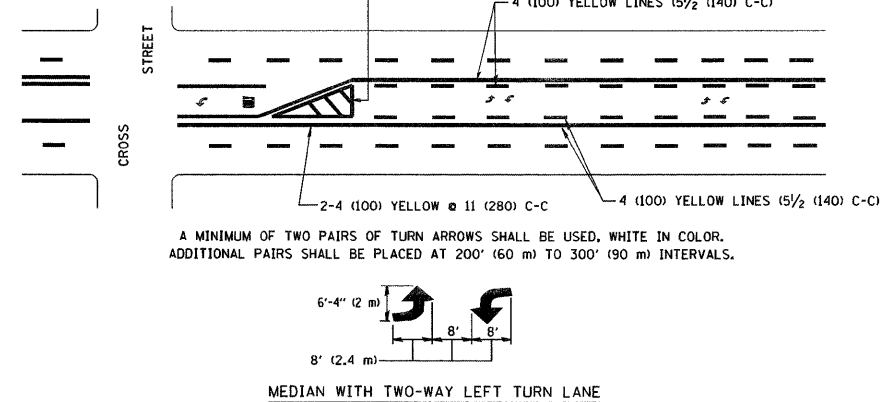
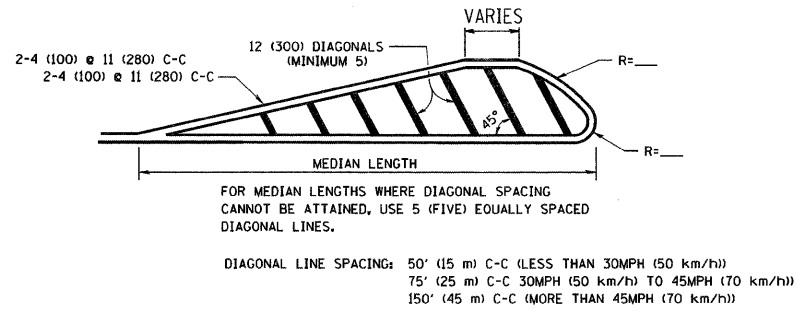
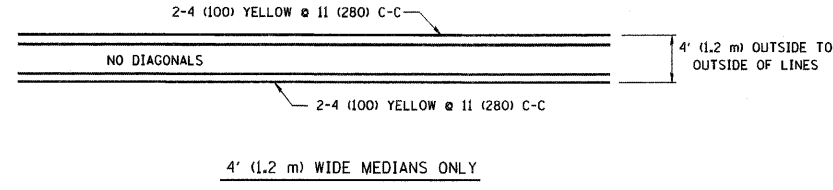


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

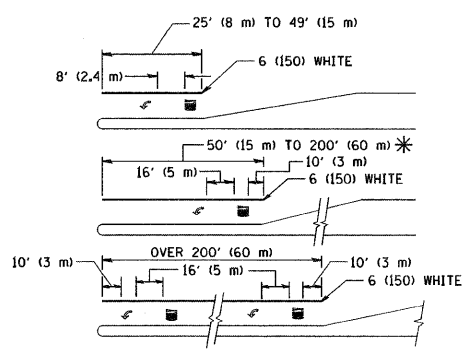
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



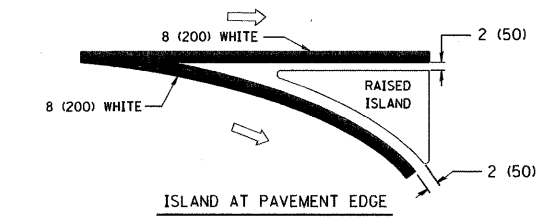
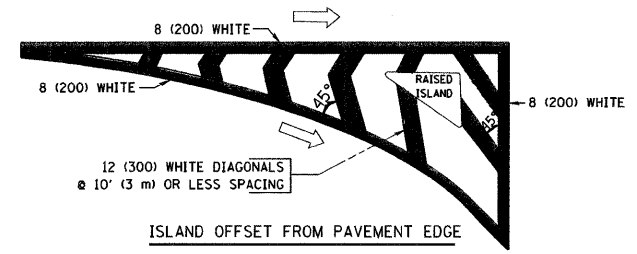
TYPICAL PAINTED MEDIAN MARKING



* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

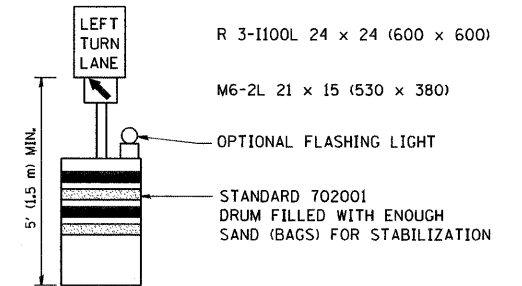
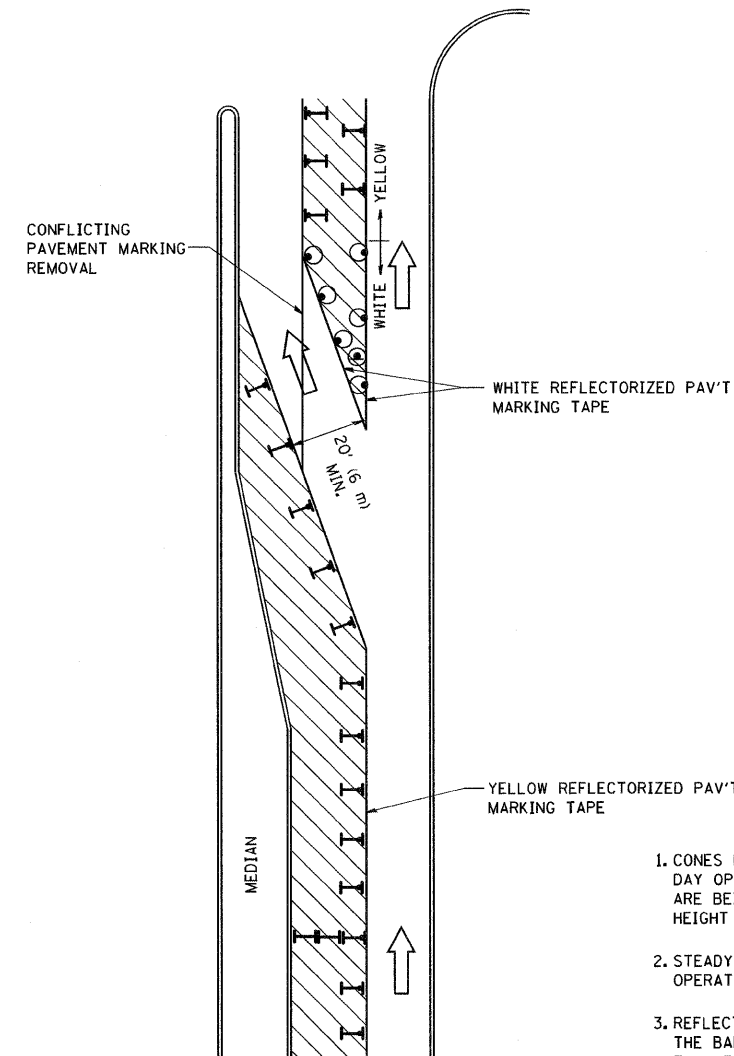
REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
**DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS**

SCALE: NONE
DRAWN BY CADD
CHECKED BY

PLOT DATE = 3/6/2007
FILE NAME = K:\ad\ad\ad\1349p
PLOT SCALE = 50,000 / 1in.
USER NAME = bwardl

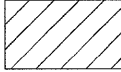





F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	50
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

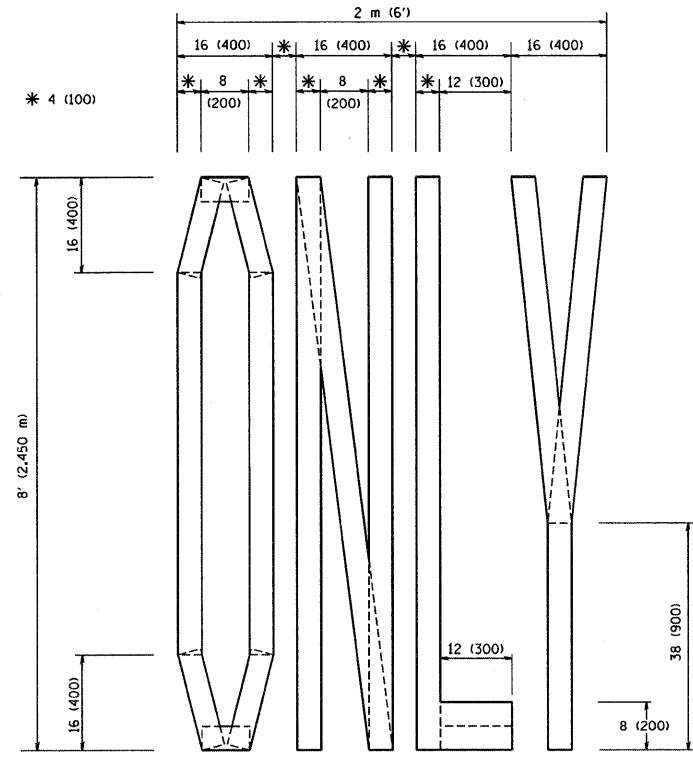
REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

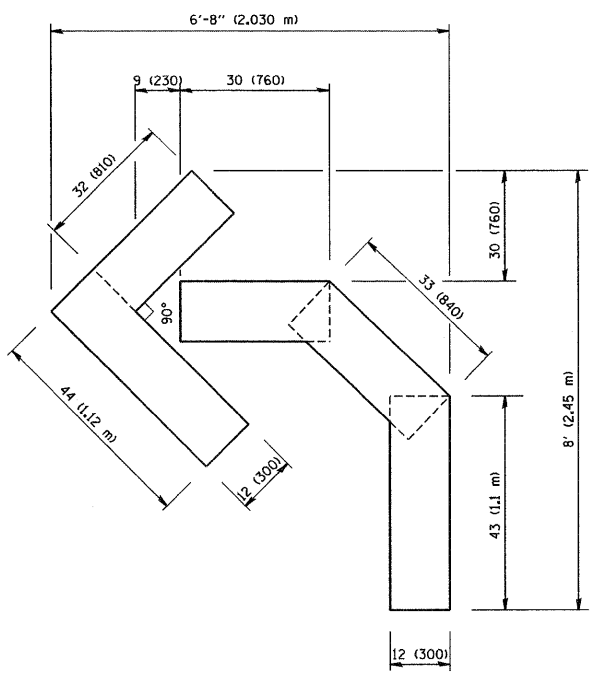
SCALE: NONE

DRAWN BY
 CHECKED BY LHA
 TC-14

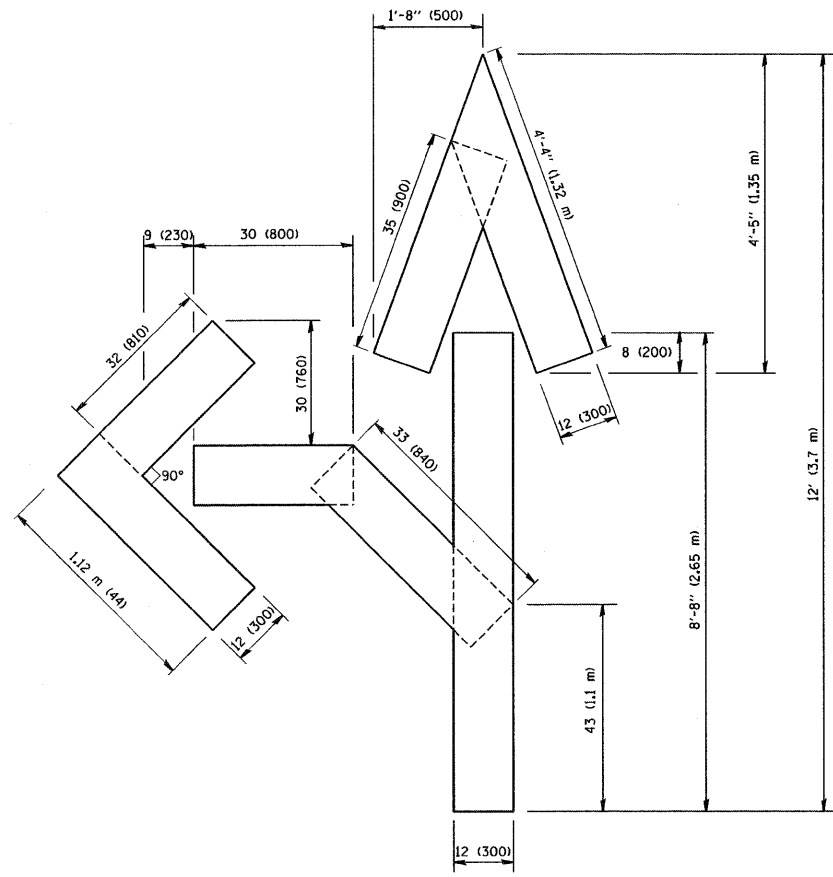
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	51
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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 PLOT SCALE = 50.0000 / IN.
 USER NAME = baundl

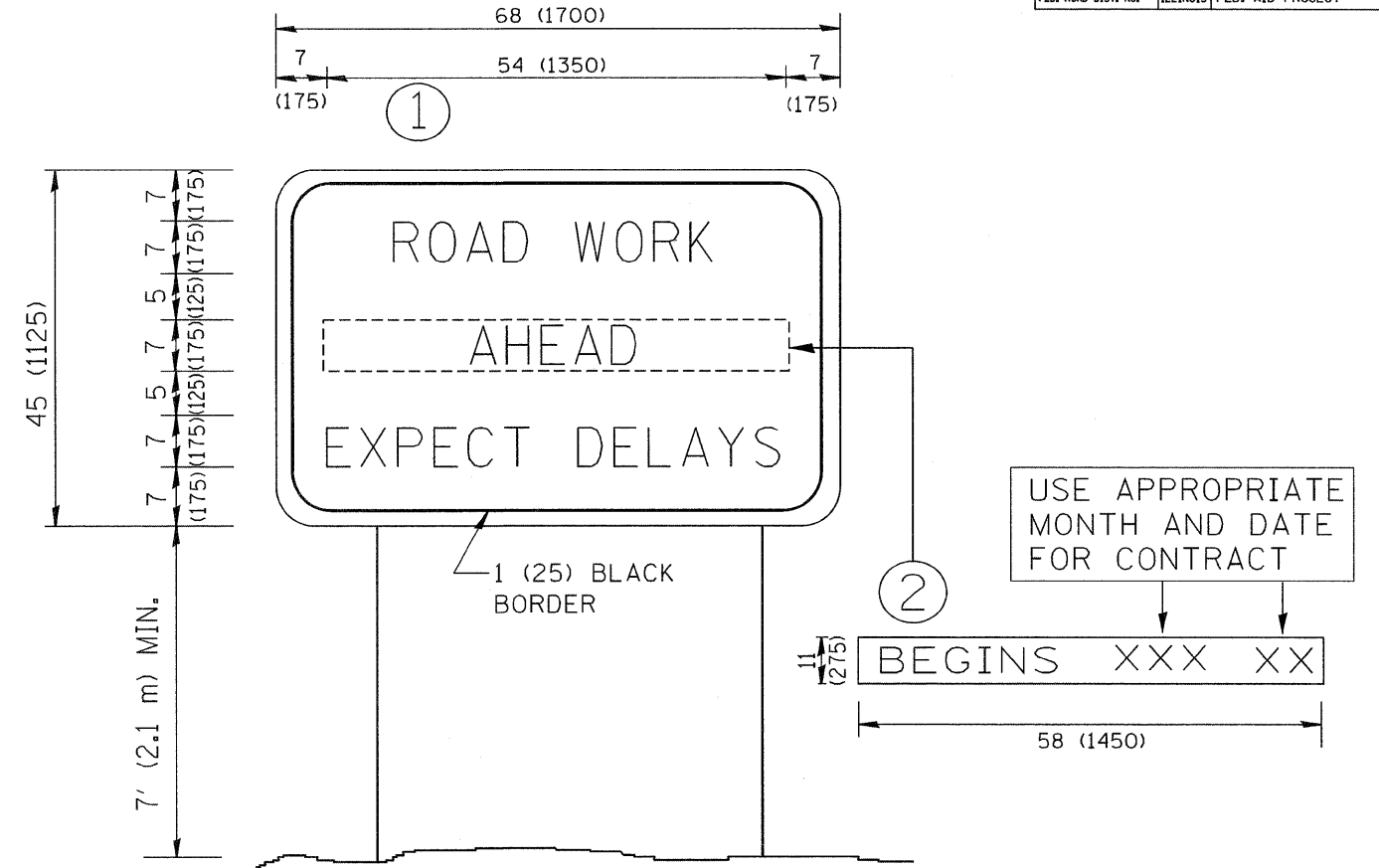
REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE
 DRAWN BY CADD
 CHECKED BY
 TC-16

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	52
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

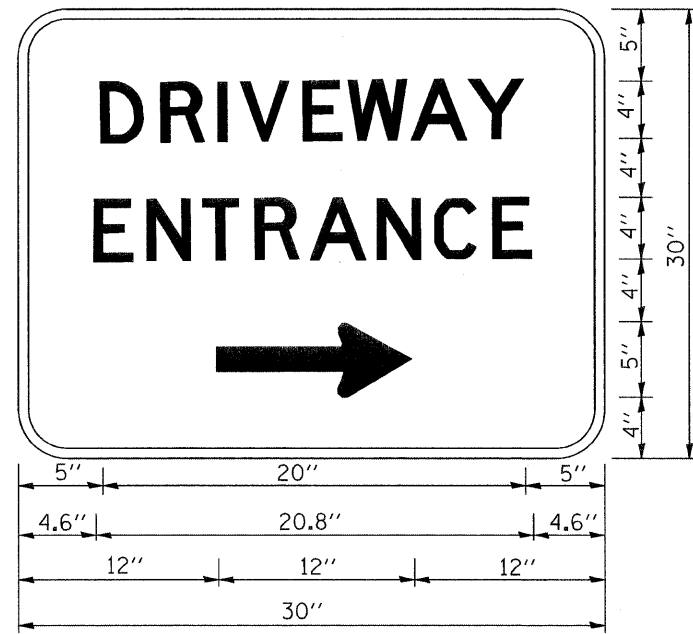
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DRAWN BY DESIGN

CHECKED BY

TC22

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	10HB-R	DUPAGE	53	53
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

PLOT DATE = 3/9/2007
 FILE NAME = K:\advised\2226.dgn
 PLOT SCALE = 50.000 / IN.
 USER NAME = bwardl

REVISIONS	
NAME	DATE
C. JUCIUS	02/15/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DRIVEWAY ENTRANCE
SIGNING**

SCALE: NONE
DATE

DRAWN BY R.H.
CHECKED BY