

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FAP ROUTE 348 : IL 43 (WAUKEGAN RD.)
WILLOW RD. TO IL 58 (GOLF RD.)
SECTION: 3271 (A&B) RS-4
RESURFACING

COOK COUNTY
C-91-841-09

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3271 (A&B) RS-4	COOK	35	1
ILLINOIS		CONTRACT NO. 60101		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN THE VILLAGES
OF GLENVIEW, NORTHFIELD,
MORTON GROVE & MAINE TOWNSHIP

TRAFFIC DATA:
2007 ADT = 27700
POSTED SPEED LIMIT = 30-40 MPH

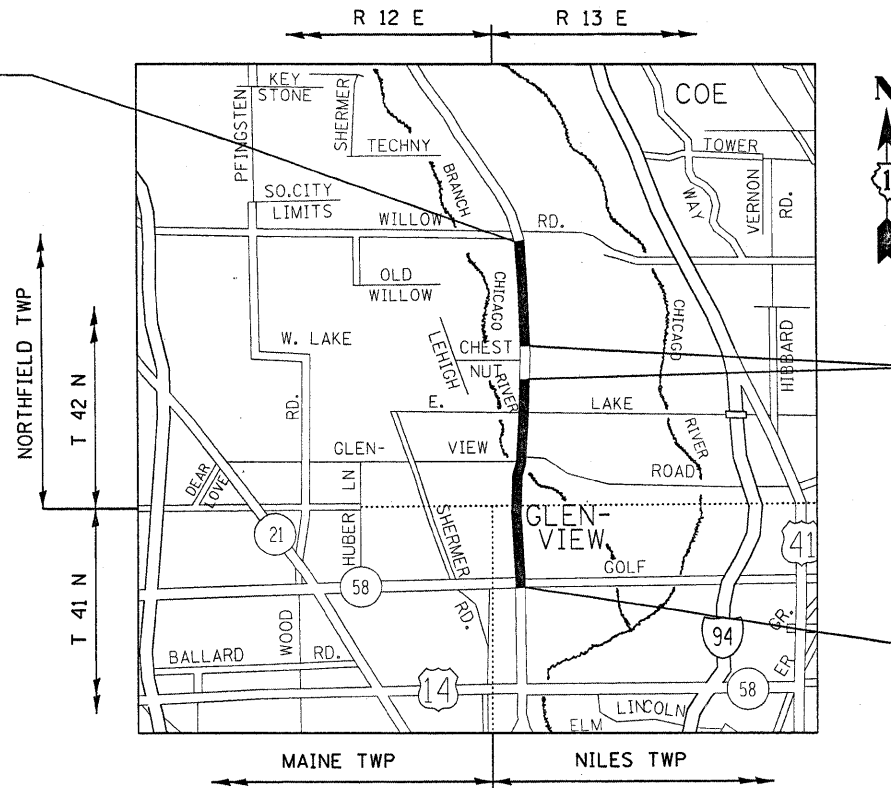
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER ROBERT BORO (847) 705-4178
PROJECT MANAGER KEN ENG

CONTRACT NO. 60101

IMPROVEMENT ENDS
STA. 187+09



OMMISSION
STA. 112+81
TO STA. 131+80

IMPROVEMENT BEGINS
STA. 3+33

MAINES, NORTHFILED & NILES TOWNSHIP

GROSS LENGTH OF IMPROVEMENT = 18376 FT. = 3.47 MILE
NET LENGTH OF IMPROVEMENT = 16477 FT. = 3.11 MILE



LOCATION OF SECTION INDICATED THIS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED DECEMBER 14, 2009

Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

January 29, 2010
Scott B. Stitt, P.E. AGING ENGINEER OF DESIGN AND ENVIRONMENT

January 29, 2010
Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

STATE STANDARDS

GENERAL NOTES

INDEX OF SHEETS

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442201-03	CLASS C AND D PATCHES
604001-03	FRAME AND LIDS TYPE 1
606001-04	CONCRETE CURB AND COMBINATION CURB AND GUTTER
701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEED ≥ 45 MPH
701601-06	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, VILLAGES OF GLENVIEW, NORTHFIELD, MORTON GROVE & MAINE TOWNSHIP.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 km/h). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

THE RESIDENT ENGINEER SHALL CONTACT WALLY CZARNY AREA TRAFFIC FIELD ENGINEER AT (773) 685-8386 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

BEFORE BEGINING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKINGS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXCEPT AS NOTED, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470. A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINING OF WORK.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISITING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE ENGINEER SHALL REPORT CLEARANCES UNDER BRIDGE AFTER RESURFACING.

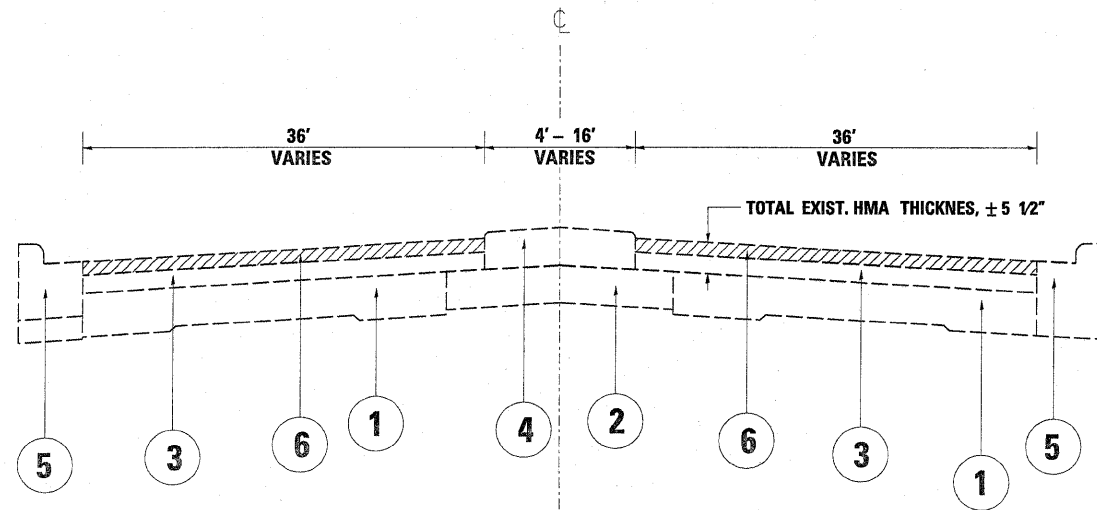
THERE ARE SOME LOCATIONS WHERE DRIVEWAYS ARE BEING CONSOLIDATED AND SHALL BE COORDINATED WITH THE VILLAGE OF GENVIEW. PLEASE CONTACT ADRIANNA WEBB, DESIGH ENGINEER FOR CAPITAL PROJECT DEPARTMENT OF THE VILLAGE OF GLENVIEW, AT (847) 998-9500 TO VERIFY LOCATIONS. ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS FOR THE FOLLOWING PAY ITEMS: PCC SIDEWALK 5" SPECIAL, PCC DRIVEWAY 8" SPECIAL, DRIVEWAY PAVEMENT REMOVAL, COMBINATION CURD & GUTTER REMOVAL & REPLACEMENT, TOPSOIL, SODDING (SALT TOLERANT) AND FERTILIZERS.

FILE NAME =	USER NAME = estimblek	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 43 (WAUKEGAN RD.) [IL 58 (GOLF RD.) TO WILLOW RD.] INDEX OF SHEET, STATE STANDARDS & GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 12/17/2009	CHECKED -	REVISED -			CONTRACT NO. 60101					
		DATE -	REVISED -			SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	100% STATE TOTAL QUANTITIES	URBAN 1000-2A					CODE NO	ITEM	UNIT	100% STATE TOTAL QUANTITIES	URBAN 1000-2A				
20101400	NITROGEN FERTILIZER NUTRIENT	POUND	32	32					70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	32	32					70300100	SHORT-TERM PAVEMENT MARKING	FOOT	6863	6863				
20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	32	32					70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	2143.5	2143.5				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	2125	2125					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	52000	52000				
25200110	SODDING, SALT TOLERANT	SO YD	2125	2125					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	18500	18500				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	94	94					70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	875	875				
40600300	AGGREGATE (PRIME COAT)	TON	468	468					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	6500	6500				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	176	176					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1100	1100				
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	4826	4826					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	2143.5	2143.5				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	52000	52000				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	775	775					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	18500	18500				
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	11464	11464					* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	875	875				
42001300	PROTECTIVE COAT	SO YD	1551	1551					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	6500	6500				
42300800	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL	SO YD	1000	1000					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1100	1100				
42400430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SO FT	4500	4500					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1326	1326				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	116977	116977					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1300	1300				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	1000	1000					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	4181	4181				
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3000	3000					X0322256	TEMPORARY INFORMATION SIGNING	SO FT	205.6	205.6				
44201785	CLASS D PATCHES, TYPE I, 12 INCH	SO YD	423	423					* X8950200	REBUILD EXISTING HANDHOLE	EACH	7	7				
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	3438	3438					Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	7	7				
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	1052	1052					Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	2106	2106													
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1													
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1													
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	45	45													
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	160	160													
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2													
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	52	52													
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4													
67100100	MOBILIZATION	L SUM	1	1													
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1													

* SPECIALTY ITEMS

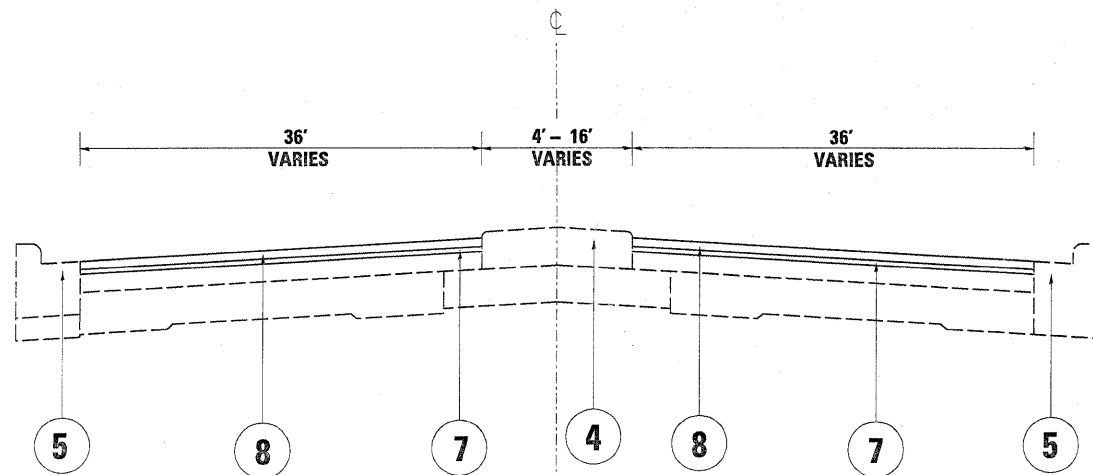
IL. 43 (WAUKEGAN RD.)



EXIST. TYPICAL SECTION

STA. 3+33 TO 18+27
 STA. 93+60 TO 105+68
 STA. 149+12 TO 187+09

IL. 43 (WAUKEGAN RD.)



PROP. TYPICAL SECTION

STA. 3+33 TO 18+27
 STA. 93+60 TO 105+68
 STA. 149+12 TO 187+09

LEGEND

- ① EXISTING PCC BASE COURSE, ± 9"
- ② EXISTING PCC BASE COURSE, ± 9"
- ③ EXISTING HOT-MIX ASPHALT AFTER MILLING, ± 3"
- ④ EXISTING BARRIER MEDIAN
- ⑤ EXISTING COMB. CURB AND GUTTER
- ⑥ PROP. HMA SURFACE REMOVAL, 2 1/2"
- ⑦ PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"

HMA MIXTURE REQUIREMENTS

MIXTURE USES	AIR VOIDS @ Ndes
POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm)	4% AT 90 GYR.
POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% AT 50 GYR.
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SQ YD/IN.

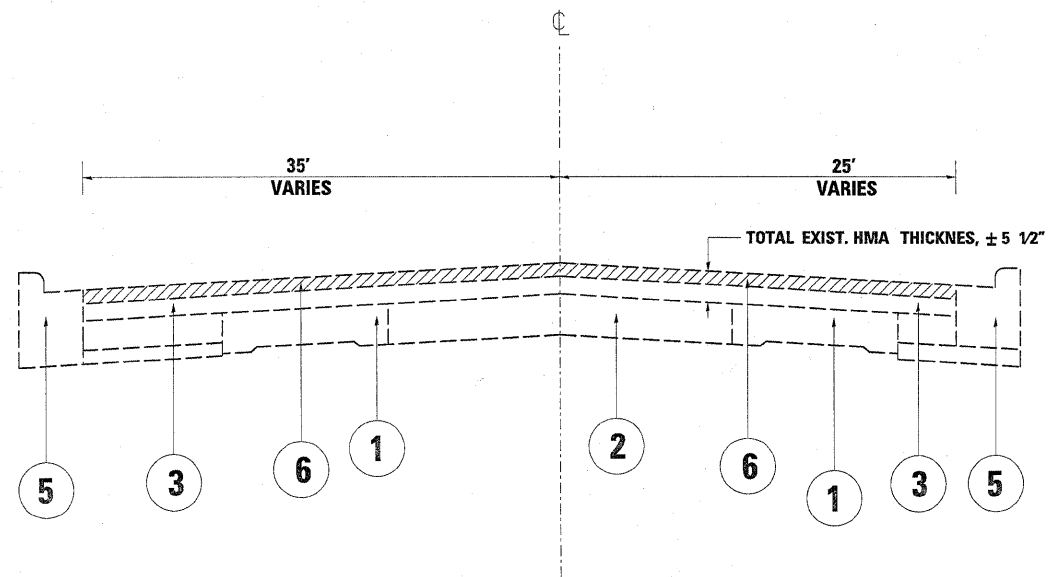
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22 AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

NOTE:

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ct:\pw\work\pwidot\estimblek\d2147666\034109-sht\plen.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -			348	3271 (A&B) RS-4	COOK	35	4	
PLOT DATE = 12/16/2009	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60101					
		DATE -	REVISED -			SCALE:	SHEET NO. 1 OF 5 SHEETS	STA. 1+89 TO STA. 18+27	ILLINOIS FED. AID PROJECT		

IL. 43 (WAUKEGAN RD.)

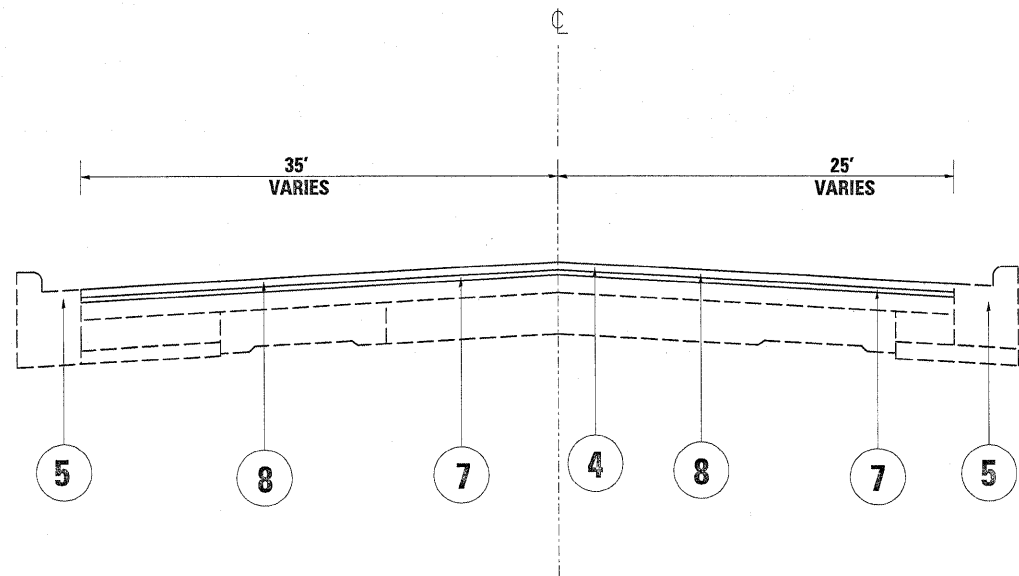


EXIST. TYPICAL SECTION
STA. 18+27 TO 29+00

LEGEND

- ① EXISTING PCC BASE COURSE, ± 9"
- ② EXISTING PCC BASE COURSE, ± 9"
- ③ EXISTING HOT-MIX ASPHALT AFTER MILLING, ± 3"
- ④ EXISTING BARRIER MEDIAN
- ⑤ EXISTING COMB. CURB AND GUTTER
- ⑥ PROP. HMA SURFACE REMOVAL, 2 1/2"
- ⑦ PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"

IL. 43 (WAUKEGAN)



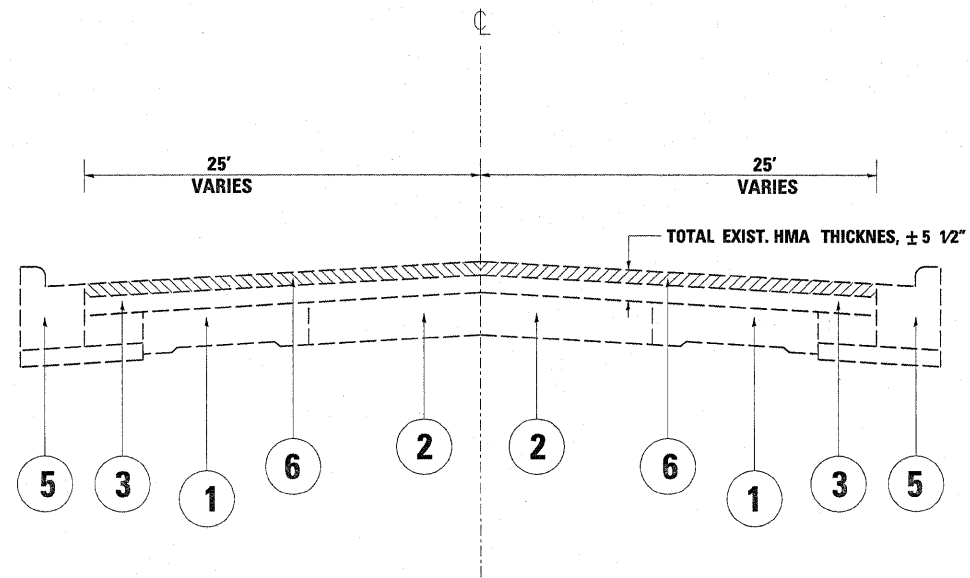
PROP. TYPICAL SECTION
STA. 18+27 TO 29+00

NOTE:

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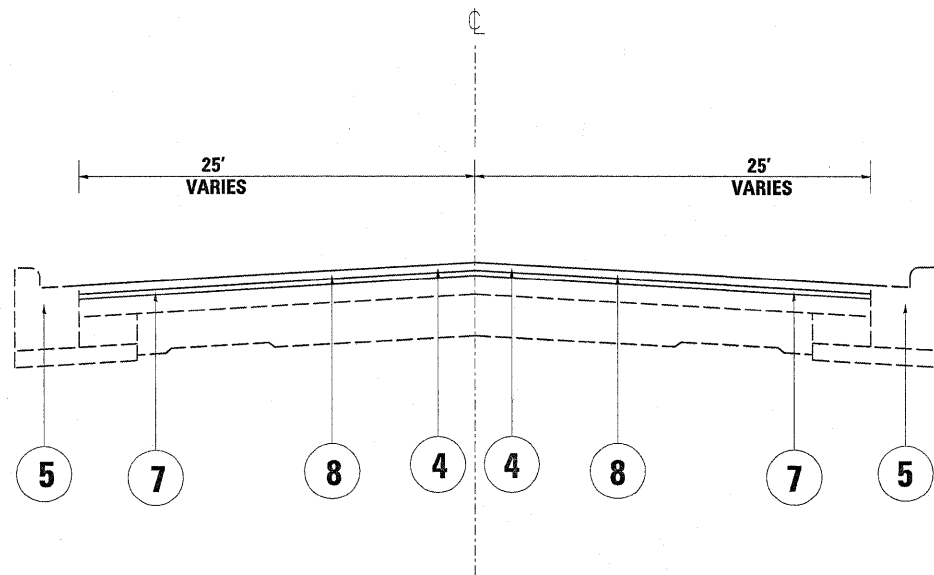
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					SCALE:	SHEET NO. 2 OF 5 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

IL. 43 (WAUKEGAN RD.)



EXIST. TYPICAL SECTION
STA. 29+00 TO 60+00

IL. 43 (WAUKEGAN)



PROP. TYPICAL SECTION
STA. 29+00 TO 60+00

LEGEND

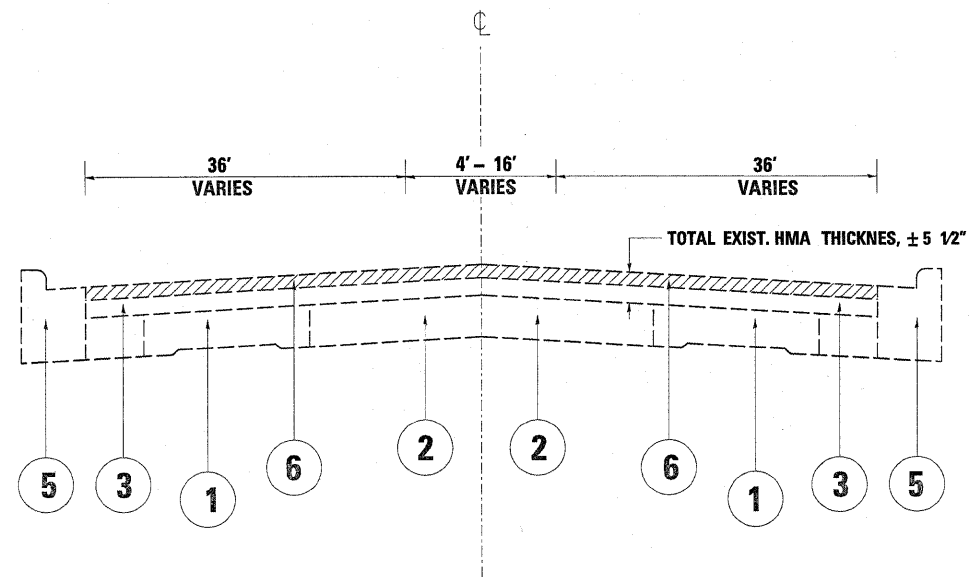
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- ⑧ PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"

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IL. 43 (WAUKEGAN RD.)



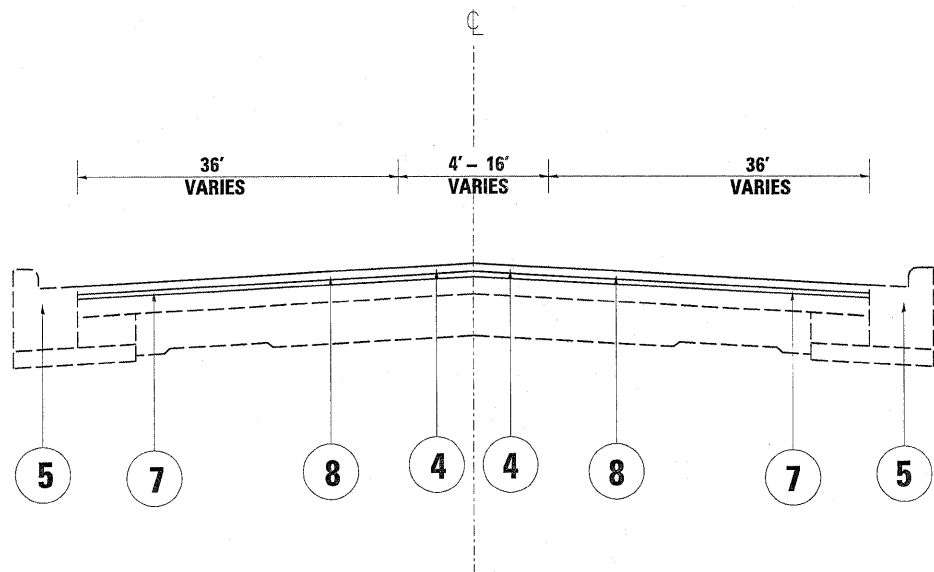
EXIST. TYPICAL SECTION

STA. 60+00 TO 93+60
STA. 105+68 TO 149+12

LEGEND

- ① EXISTING PCC BASE COURSE, ± 9"
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IL. 43 (WAUKEGAN)



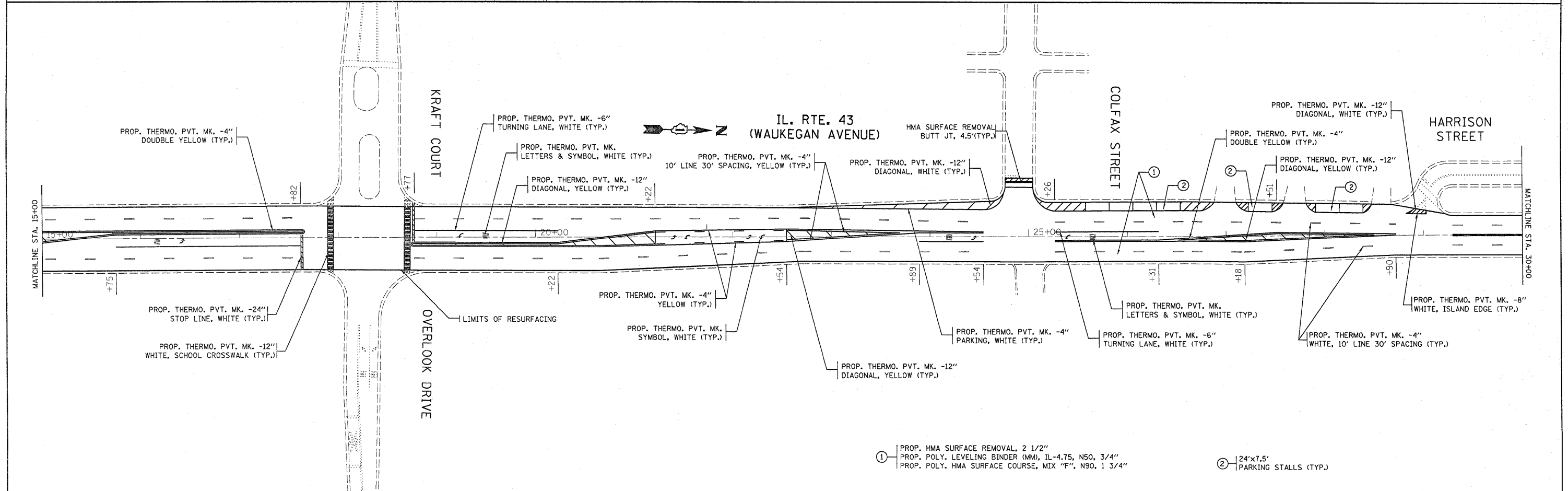
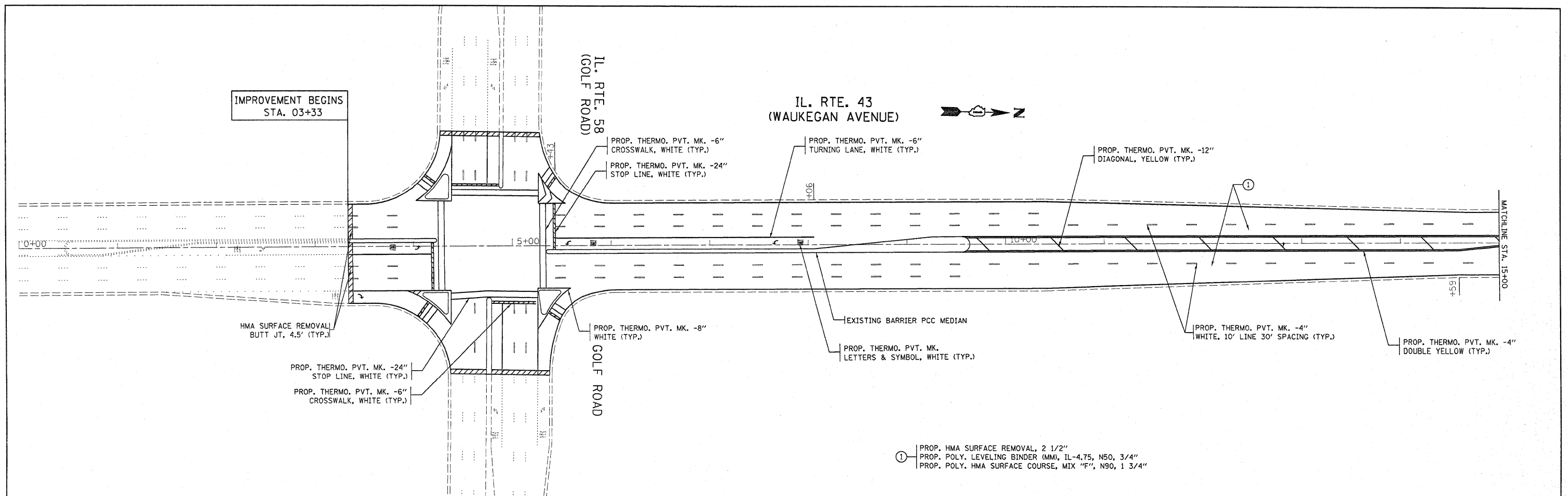
PROP. TYPICAL SECTION

STA. 60+00 TO 93+60
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PLOT DATE = 12/16/2009	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO. 5 OF 5 SHEETS	STA.	TO STA.			



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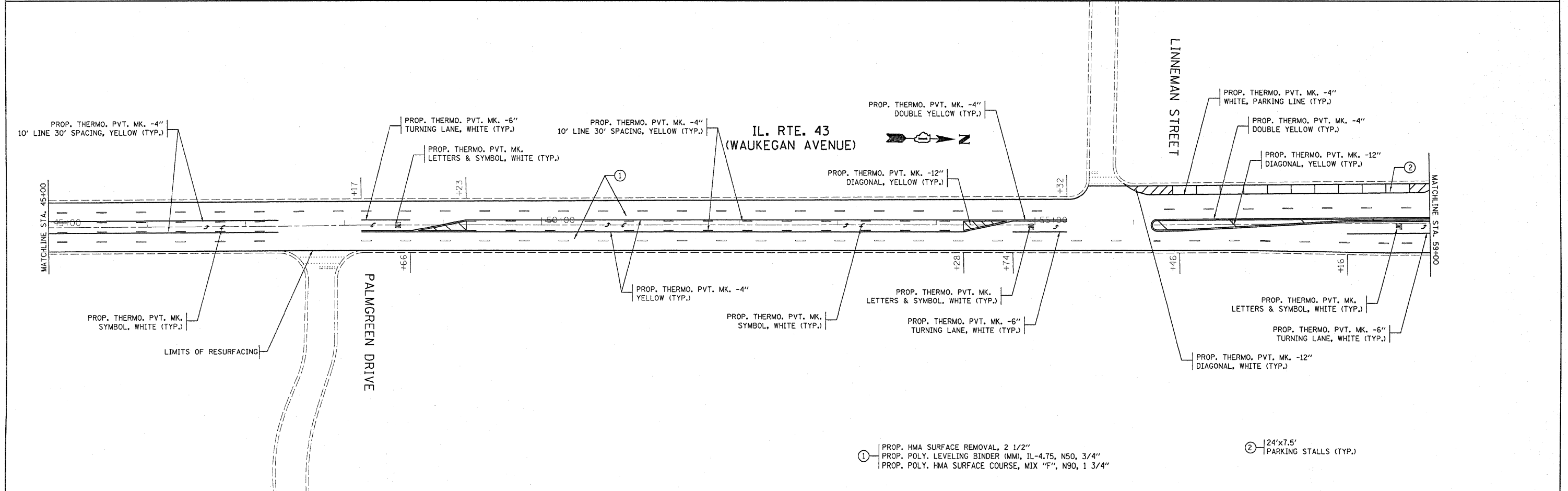
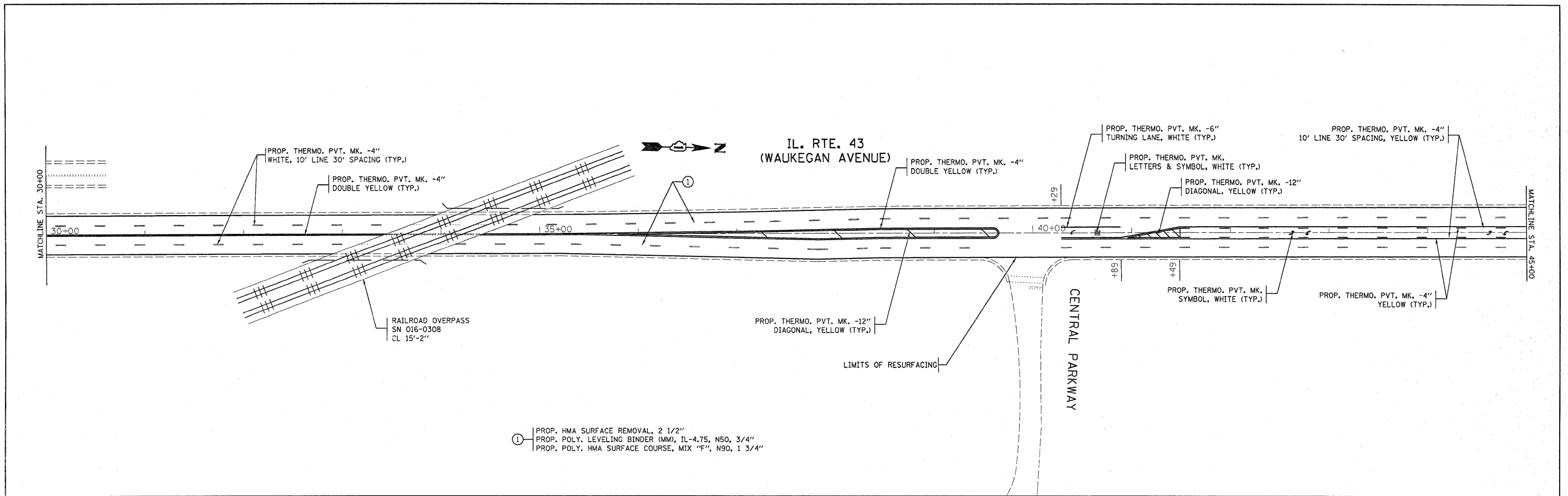
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CHECKED -	REVISED -
DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL. RTE. 43 (WAUKEGAN AVENUE)
(WILLOW ROAD TO IL. RTE. 58 (GOLF ROAD)
ROADWAY PLAN**

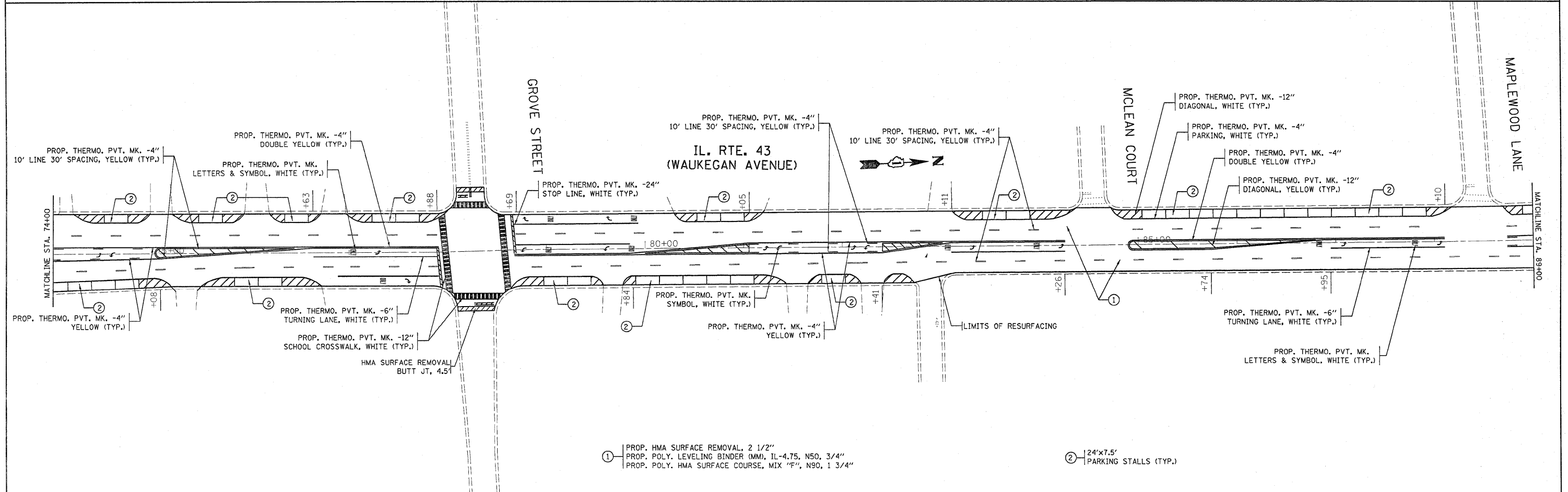
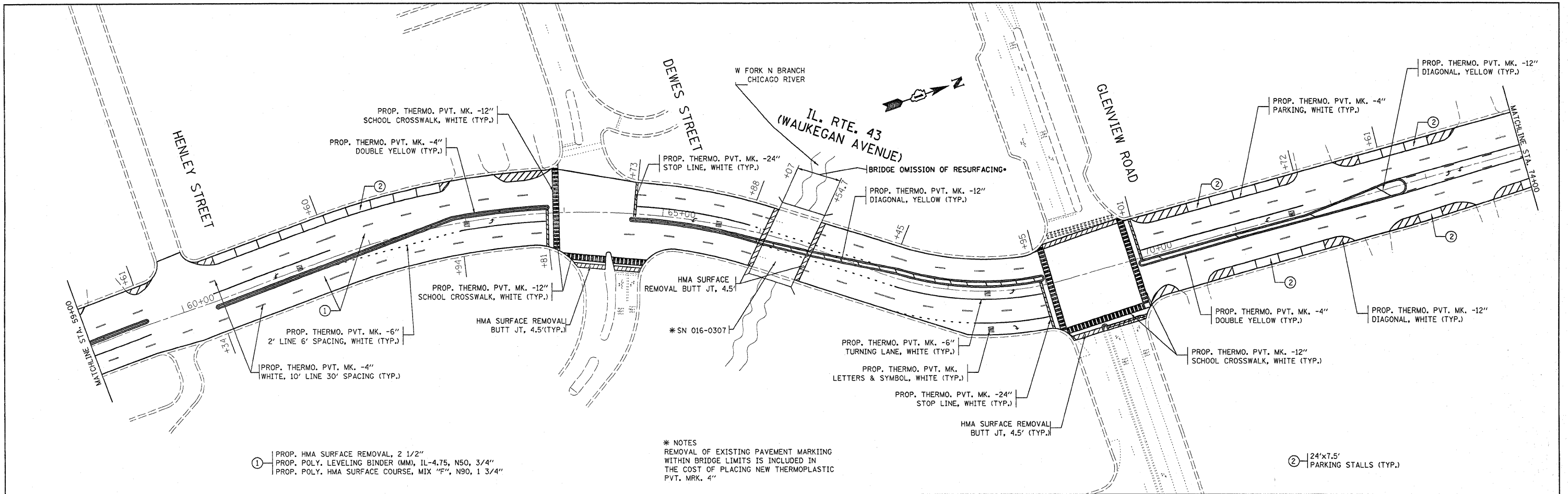
SCALE: 1" = 50' SHEET NO. 1 OF 7 SHEETS STA. 00+00.00 TO STA. 30+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3271 (A&B) RS-4	COOK	35	8
CONTRACT NO. 60101				
ILLINOIS FED. AID PROJECT				



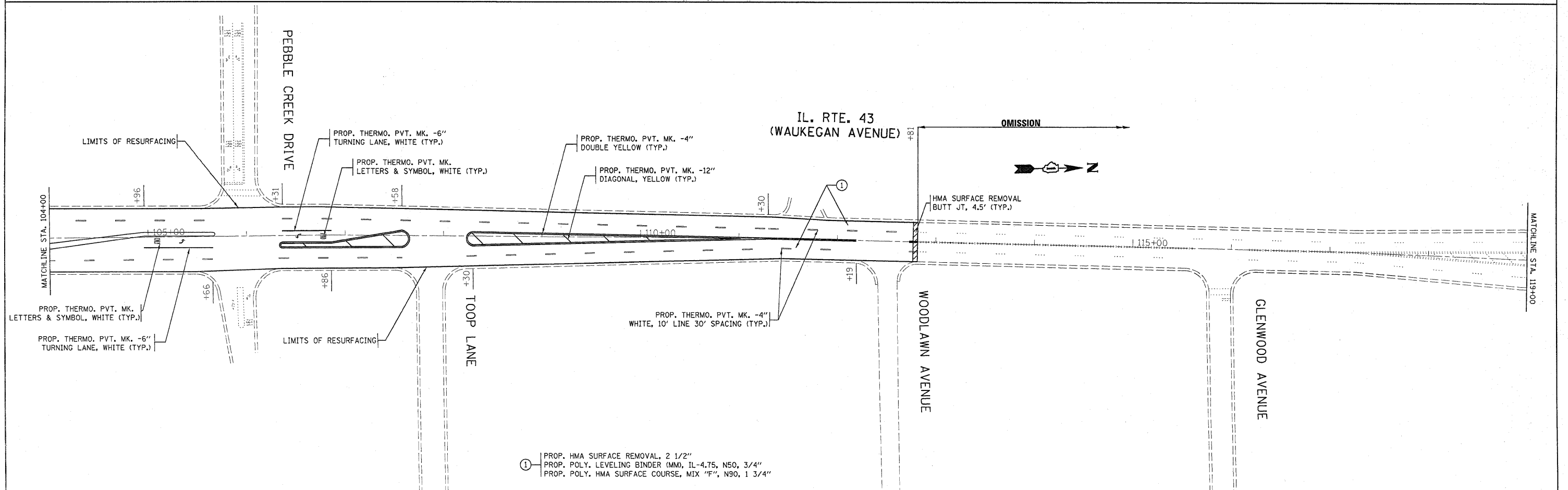
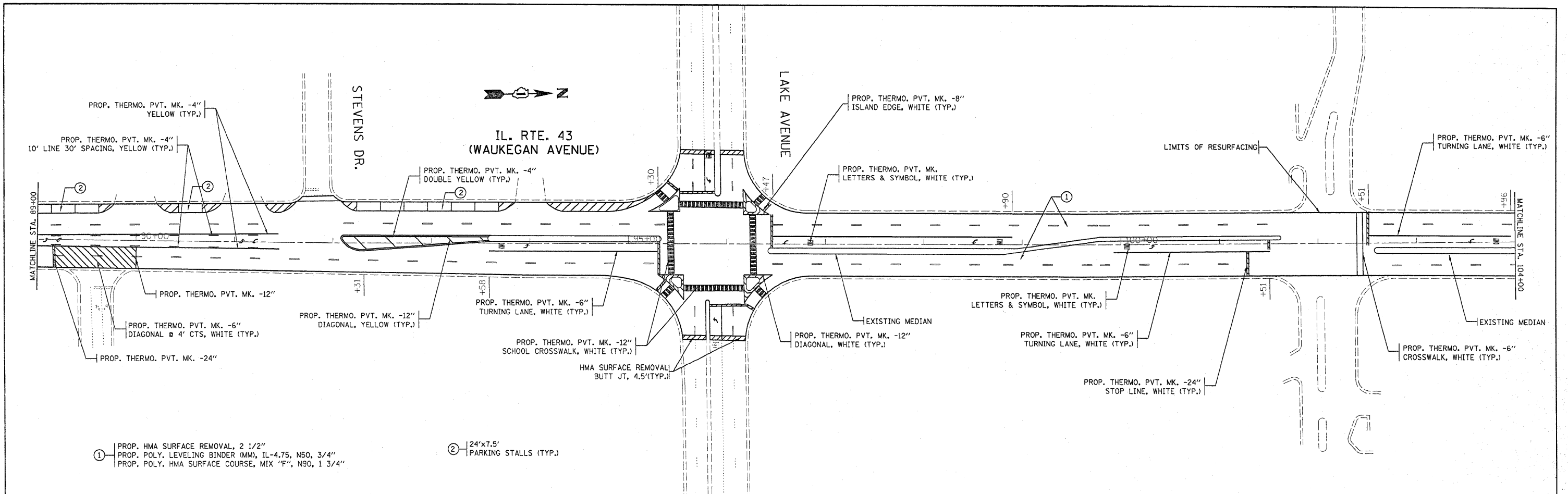
FILE NAME =	USER NAME = estmablek	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. RTE. 43 (WAUKEGAN AVENUE) (WILLOW ROAD TO IL. RTE. 58 (GOLF ROAD) ROADWAY PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\pwwid\estmablek\d0147666\0104109-shr-plan.dgn	12/16/2009	DRAWN -	REVISED -			348	3271 (A&B) RS-4	COOK	35	9	
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			SCALE: 1"= 50'		SHEET NO. 2 OF 7 SHEETS		STA. 30+00.00 TO STA. 59+00.00	
PLOT DATE = 12/16/2009		DATE -	REVISED -			CONTRACT NO. 60101					

ILLINOIS FED. AID PROJECT

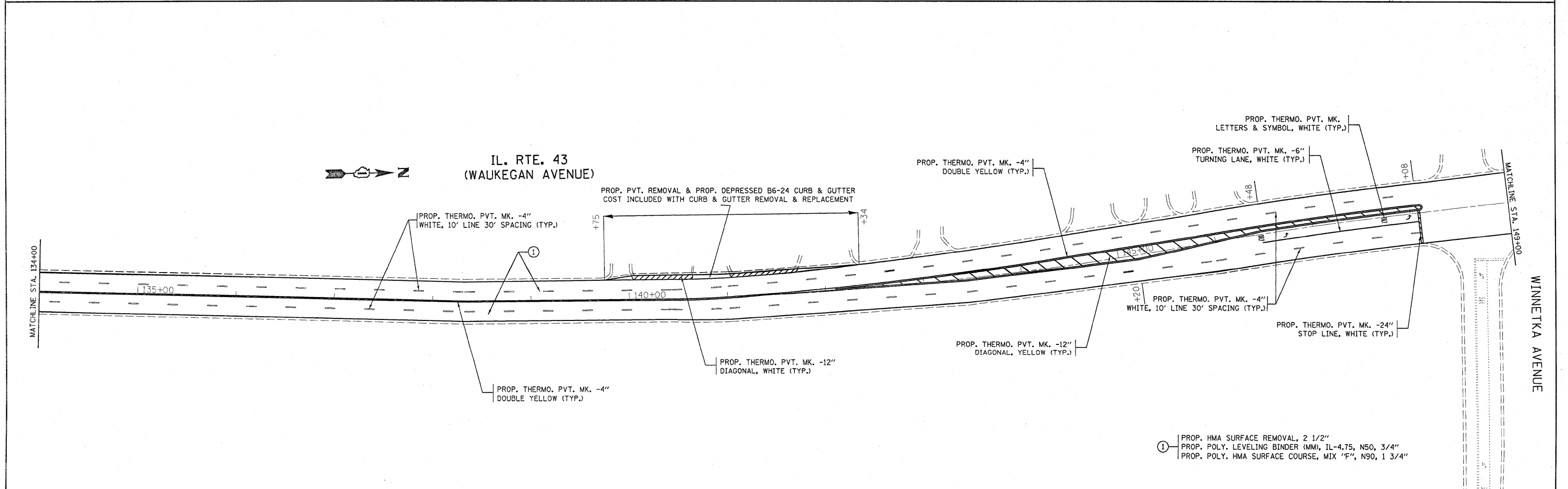
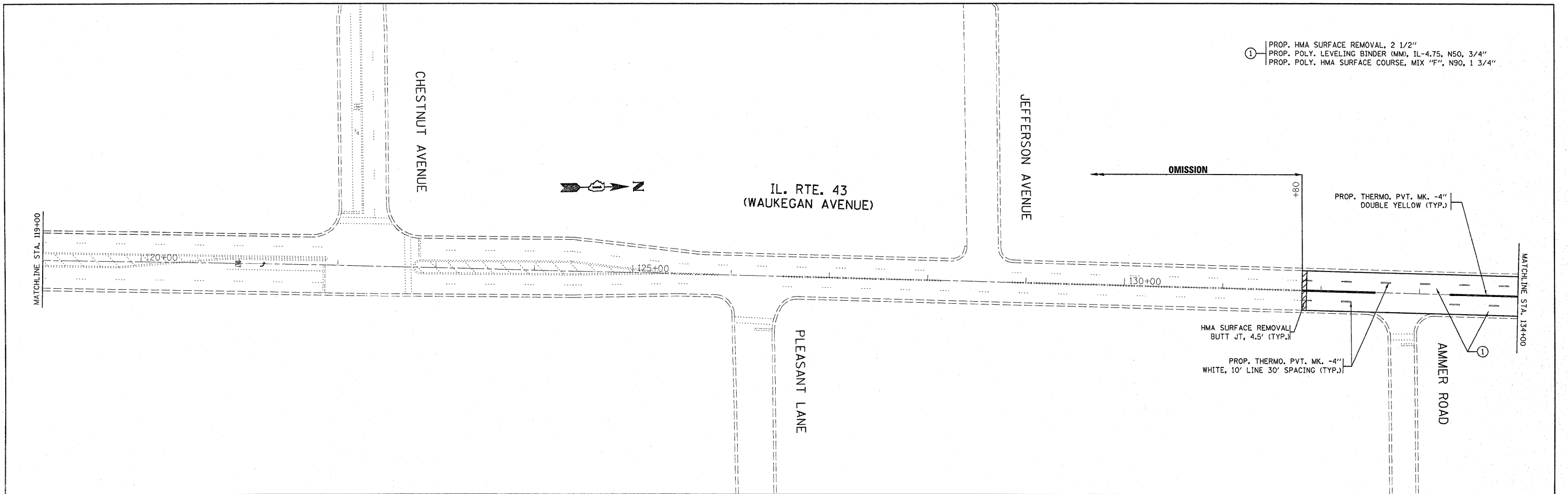


FILE NAME =	USER NAME = estimablek	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. RTE. 43 (WAUKEGAN AVENUE) (WILLOW ROAD TO IL. RTE. 58 (GOLF ROAD) ROADWAY PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw\work\pwidot\estimablek\d0147666\0194189-sht-plan.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -			348	3271 (A&B) RS-4	COOK	35	10	
PLOT DATE = 12/16/2009	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60101					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

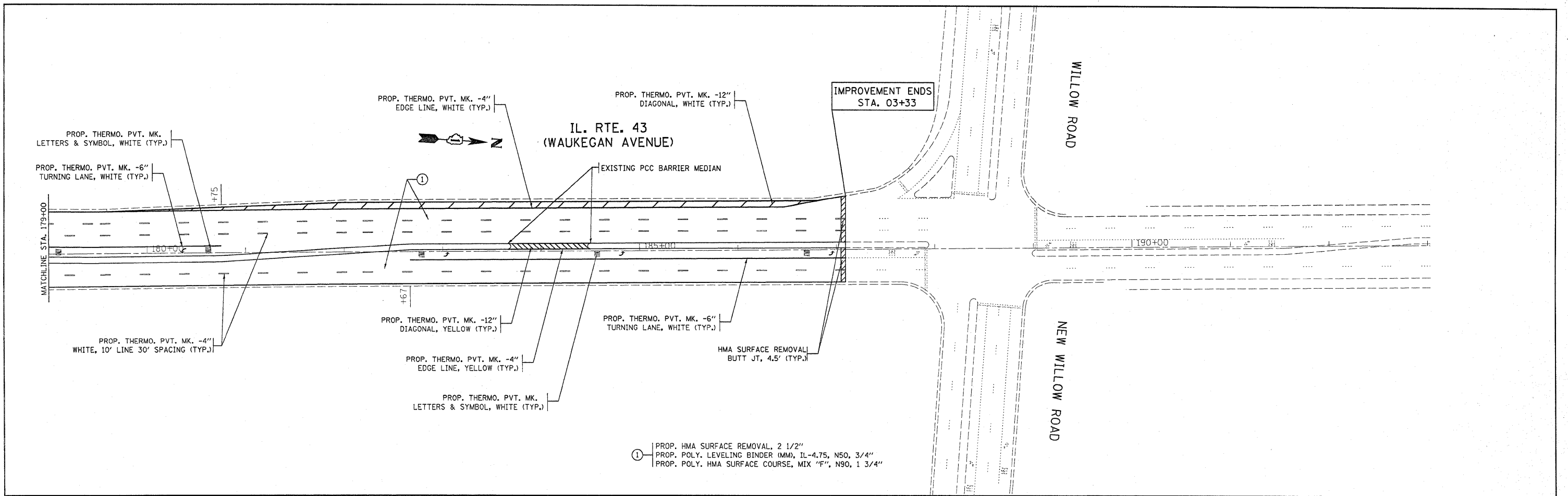
SCALE: 1"= 50' SHEET NO. 3 OF 7 SHEETS STA. 59+00.00 TO STA. 89+00.00



FILE NAME =	USER NAME = estimblek	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. RTE. 43 (WAUKEGAN AVENUE) (WILLOW ROAD TO IL. RTE. 58 (GOLF ROAD) ROADWAY PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pilot\estimblek\d0147666\1194109.sht-plan.dgn	PLOT SCALE = 50,0000 ' / IN.	DRAWN -	REVISED -			348	3271 (A&B) RS-4	COOK	35	11	
PLOT DATE = 12/16/2009	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60101					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



FILE NAME =	USER NAME = estimablek	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. RTE. 43 (WAUKEGAN AVENUE) (WILLOW ROAD TO IL. RTE. 58 (GOLF ROAD) ROADWAY PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw_work\pwsdot\estimablek\d0147666\014109-sht-plan.dgn		DRAWN -	REVISED -			348	3271 (A&B) RS-4	COOK	35	12	
PLOT SCALE = 50,0000 ' / IN.		CHECKED -	REVISED -			SCALE: 1"= 50'		SHEET NO. 5 OF 7 SHEETS		STA. 119+00.00 TO STA. 149+00.00	
PLOT DATE = 12/16/2009		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

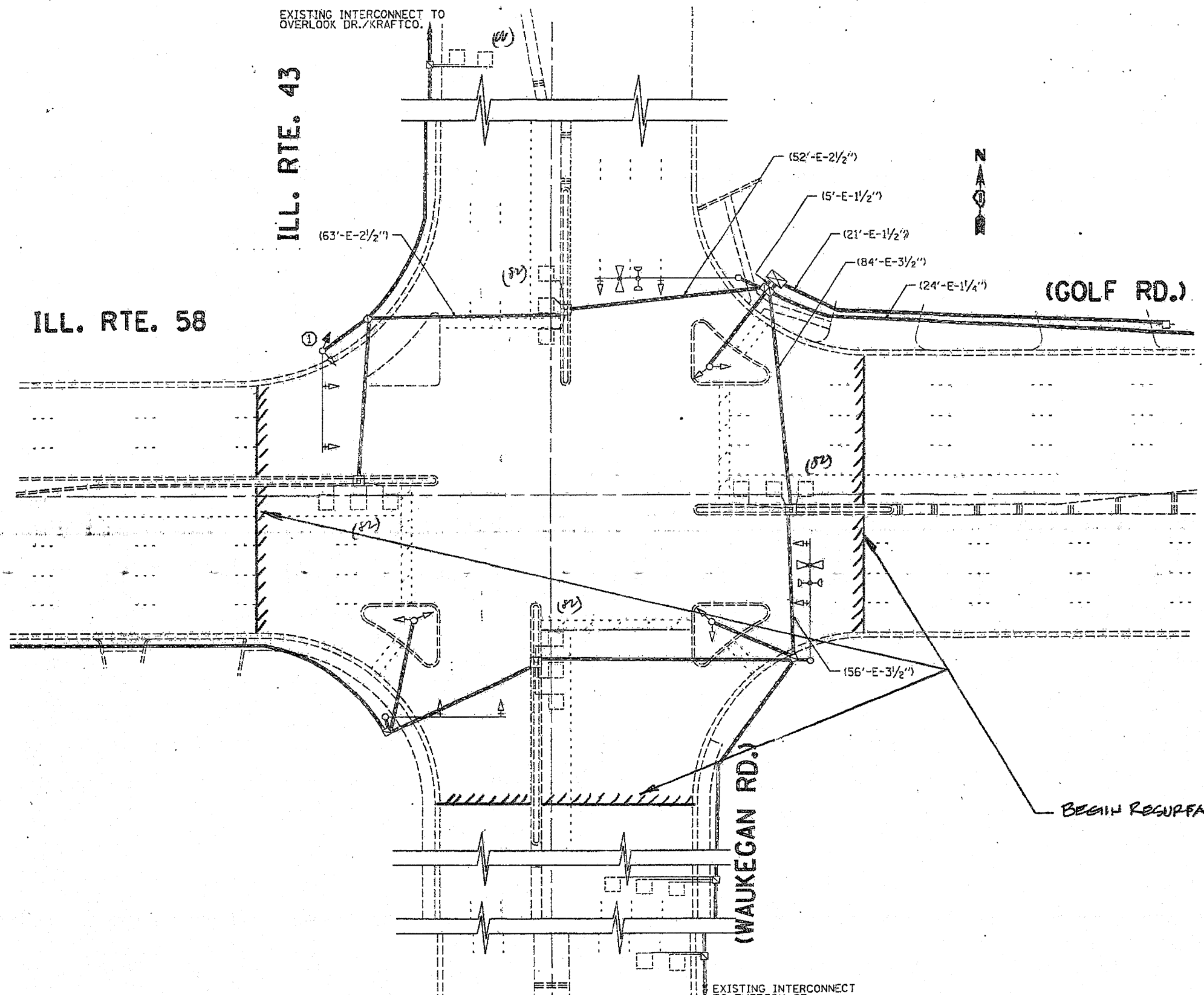


- ① PROP. HMA SURFACE REMOVAL, 2 1/2"
- PROP. POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- PROP. POLY. HMA SURFACE COURSE, MIX "F", N90, 1 3/4"

FILE NAME =	USER NAME = estimablek	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. RTE. 43 (WAUKEGAN AVENUE) (WILLOW ROAD TO IL. RTE. 58 (GOLF ROAD) ROADWAY PLAN	F.A.P RTE. 348	SECTION 3271 (A&B) RS-4	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 14	
ci:\pwork\puidot\estimablek\d0147665\01	94109-sht-plen.dgn	DRAWN -	REVISED -			SCALE: 1" = 50' SHEET NO. 7 OF 7 SHEETS STA. 179+00.00 TO STA. 193+02.75					
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			ILLINOIS FED. AID PROJECT					
PLOT DATE = 12/16/2009		DATE -	REVISED -			CONTRACT NO. 60101					

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	394	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kant@ephris.com	DESIGNED - BCK	REVISED -
g:\p\work\PM\DOT\KANT\PHIXAYBC\01126	traffic_legend_v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

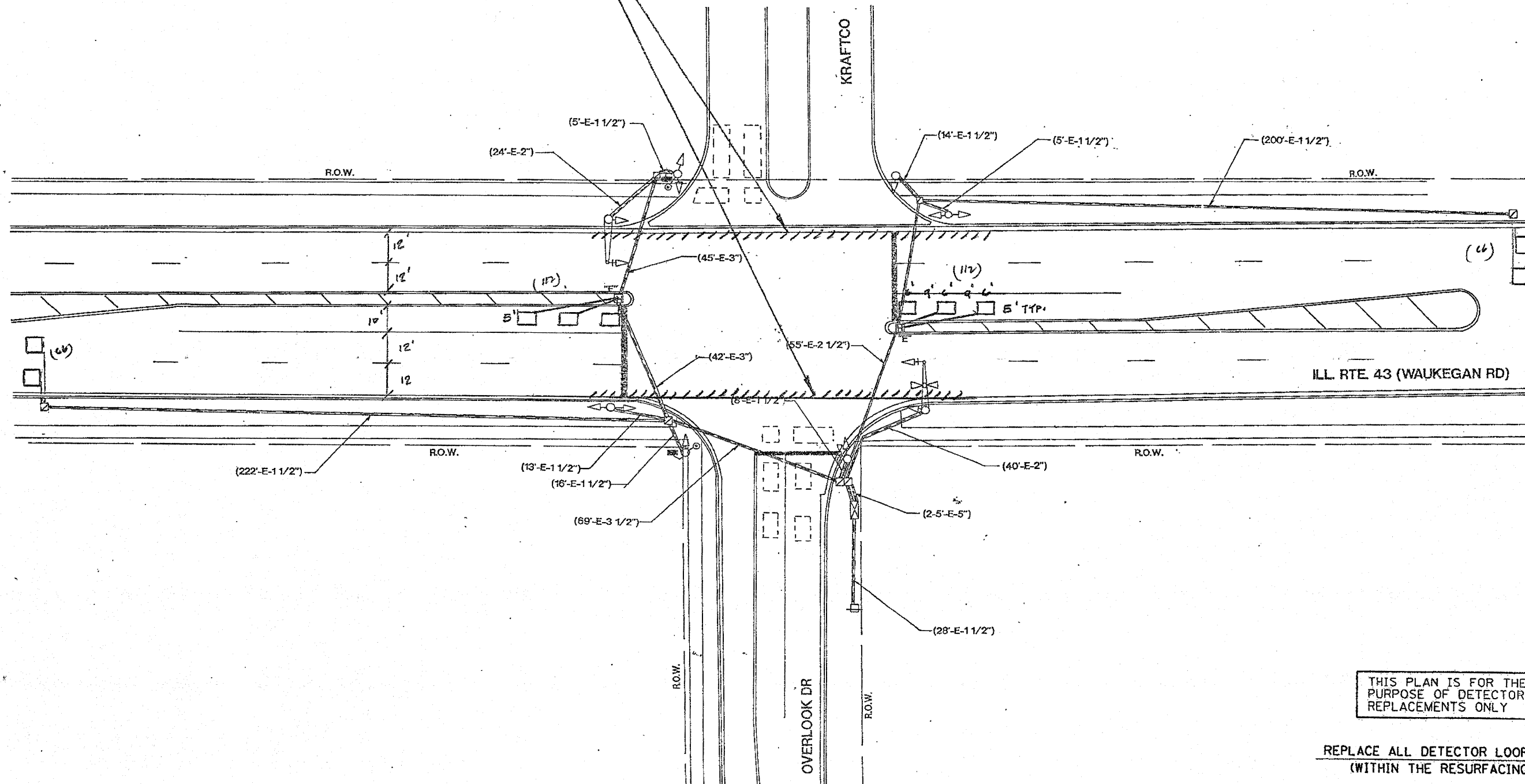
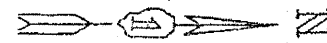
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. RTE. 43 (WAUKEGAN) @ ILL. RTE. 58

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348 (3071 (A16)	AS-4	COOK	35	15
CONTRACT NO. 6001				
SCALE: NONE SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		

RESURFACING LIMITS



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

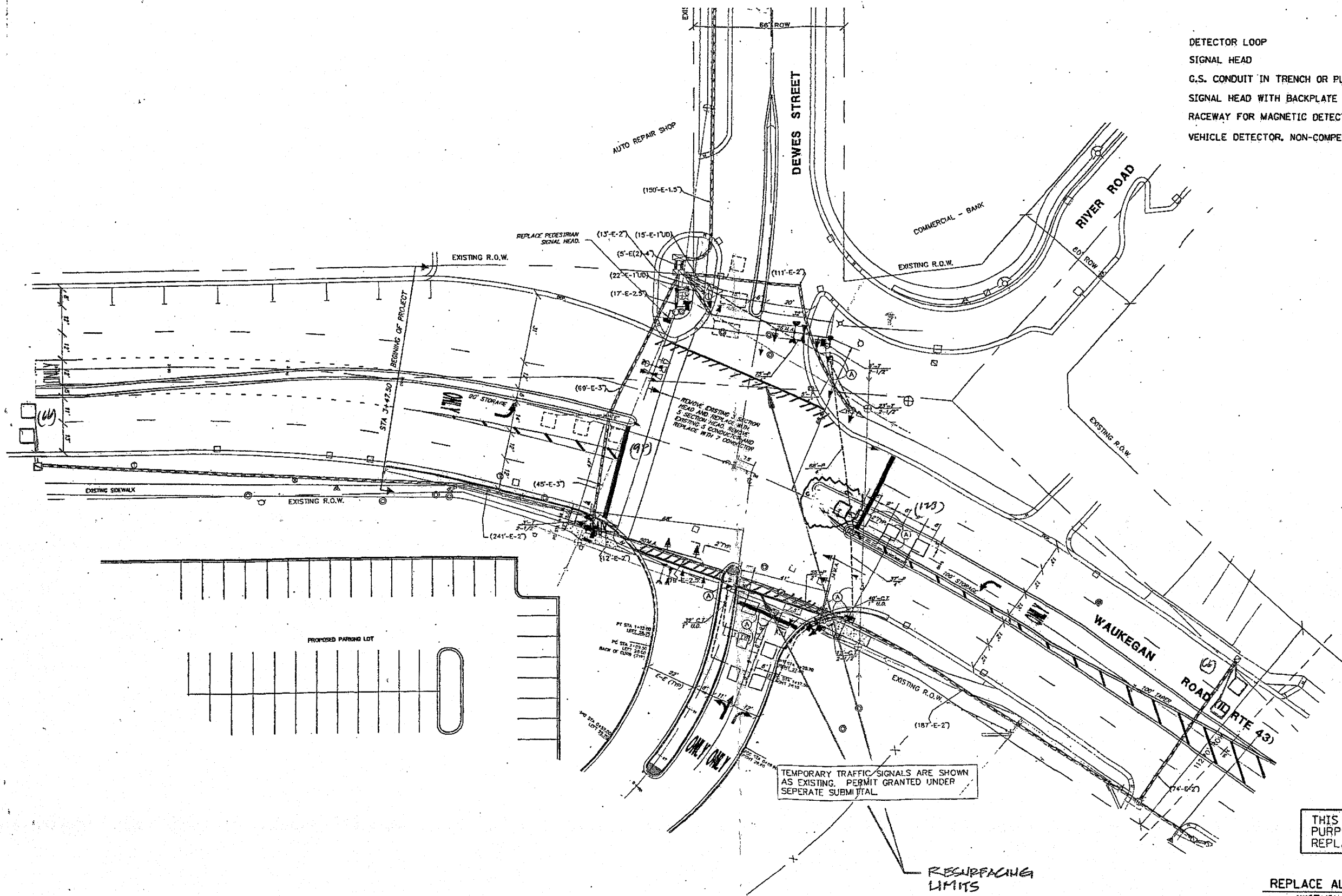
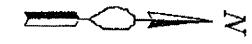
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	356	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = konthaphixaybo	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. RTE. 43 (WAUKEGAN) @ KRAFT DR.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
en\p\work\PWIDOT\KANTHAPHIXAYBO\01126	4\traf\Fic_Legend_v7.dgn	DRAWN - BCK	REVISED -			348	3271 (A8) R2-4	COOK	35	16	
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -			CONTRACT NO. 601					
	PLOT DATE = 4/3/2009	DATE	REVISED -			SCALE: NONE SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT					

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP	□	□
SIGNAL HEAD		△
C.S. CONDUIT IN TRENCH OR PUSHED	---	
SIGNAL HEAD WITH BACKPLATE		△
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		"E"
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



TEMPORARY TRAFFIC SIGNALS ARE SHOWN AS EXISTING. PERMIT GRANTED UNDER SEPERATE SUBMITTAL.

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	353	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kgnthphxaybo	DESIGNED - BCK	REVISED -
at\pk_work\PH\DOT\KANTHAPH\XAYBC\01126	4\traffic_legend_v7.dgn	DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

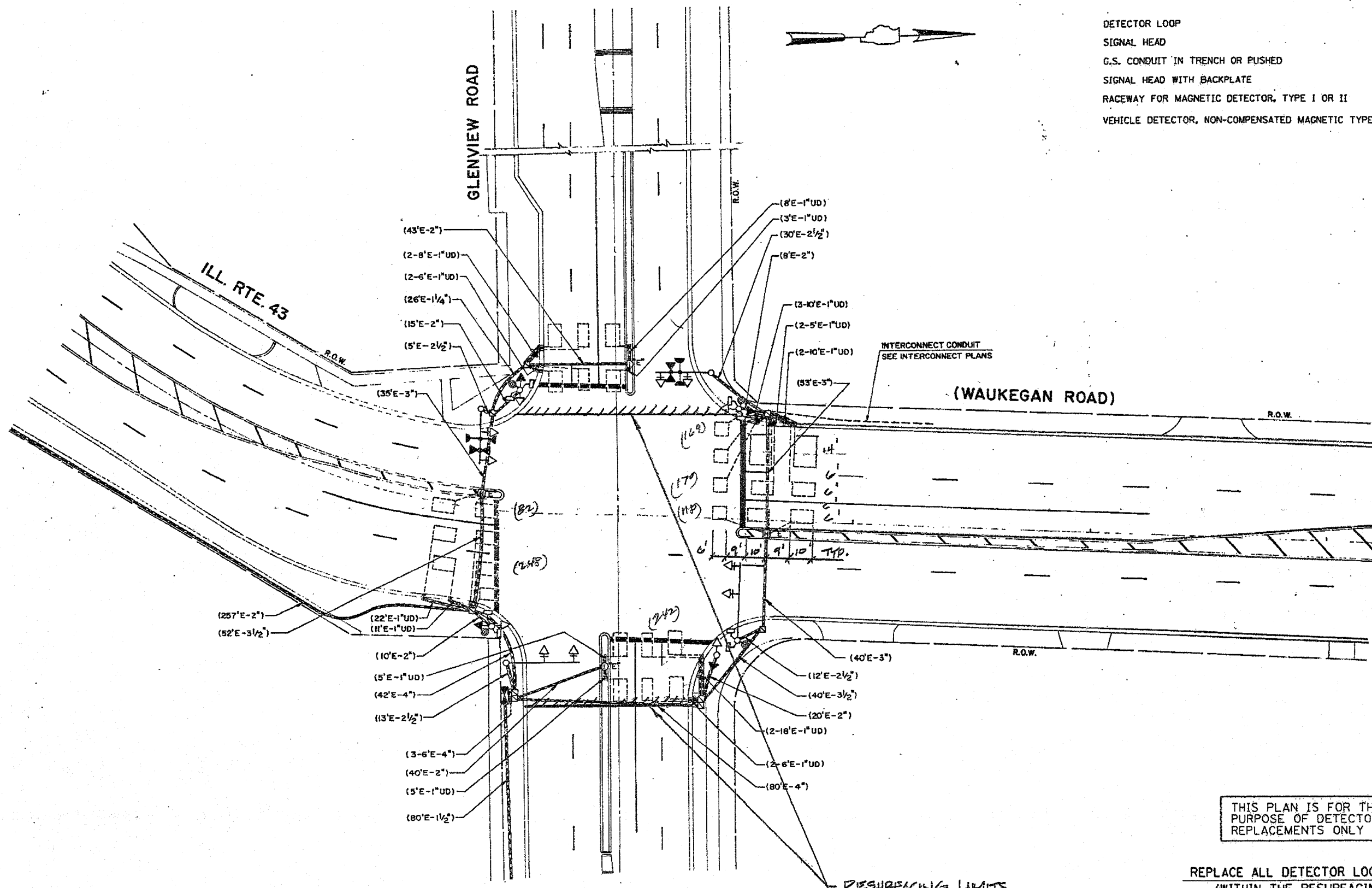
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. RTE. 43 (WAUKEGAN) @ DEWES STREET

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	0071 (A) KS-4	COOK	35	17
CONTRACT NO. 60901				

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	1,029	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = karnthophi.kaybc	DESIGNED - BCK	REVISED -
as:\pwwork\pwwork\KANTHOPHI\KAYBC\081126	4\1\office.legend.v7.dgn	DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

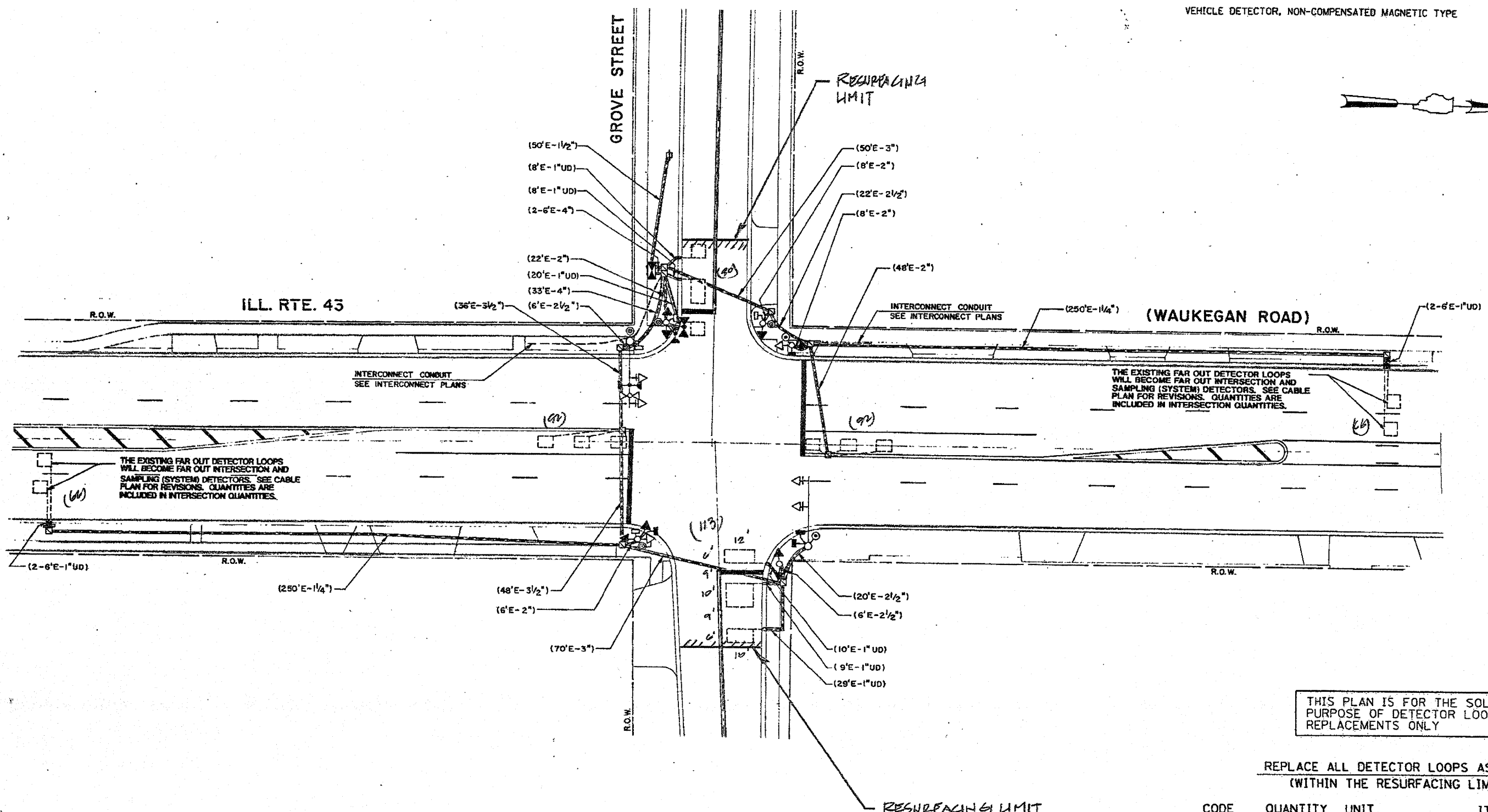
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. RTE. 43 (WAUKEGAN) @ GLENVIEW RD.

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3371 (A) RS-4	COOK	35	18
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

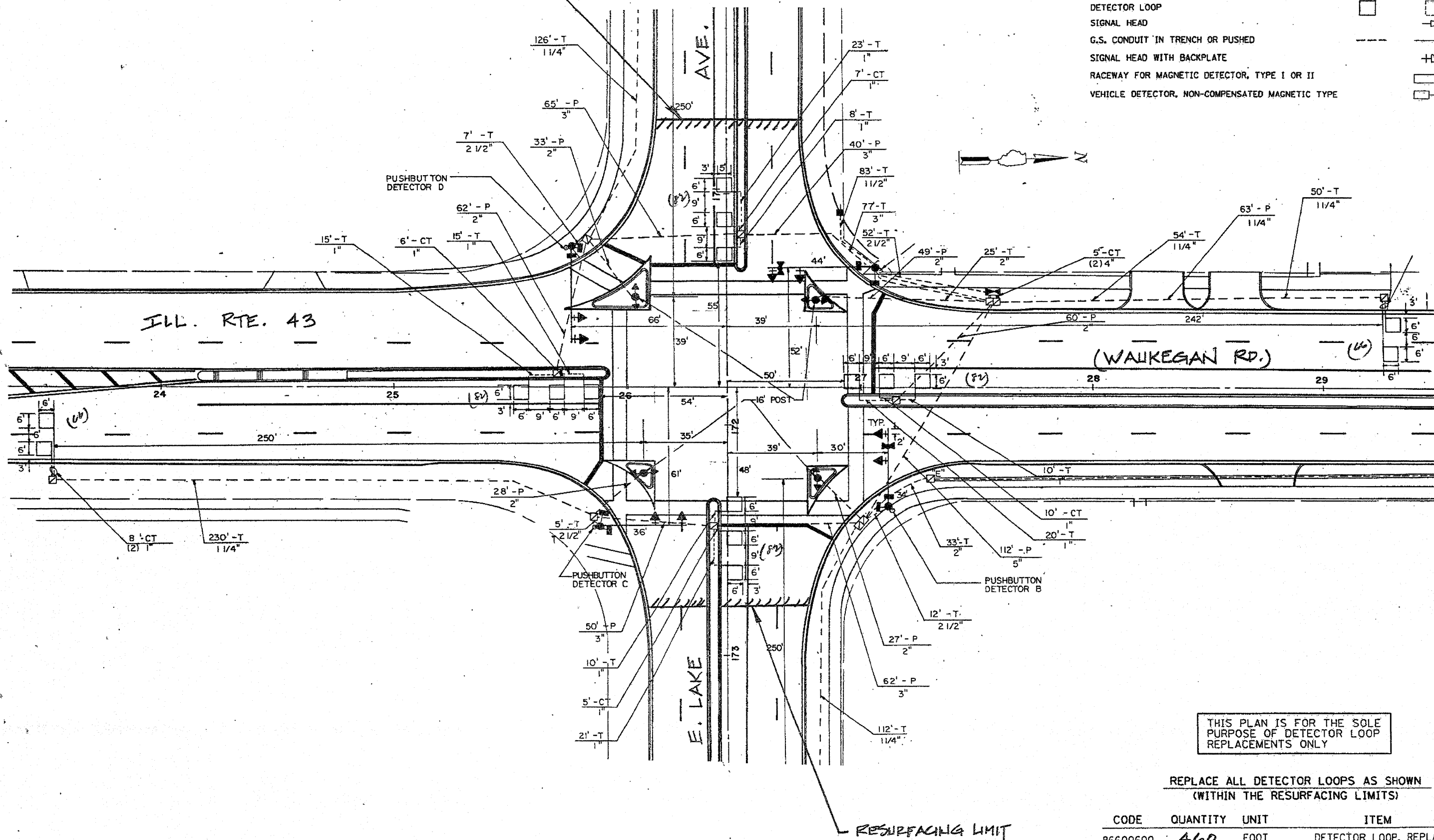
CODE	QUANTITY	UNIT	ITEM
86600600	519	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME *	USER NAME * kanthaphiroybo	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. RTE. 43 (WAUKEGAN) @ GROVE STREET	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
22\p\work\1\1\DOT\KANTHAPHIROYBO\1125	4\tr\of\tra_legend_v7.dgn	DRAWN - BCK	REVISED -			349	3871(44) 01-4	COOK	135	19	
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -			CONTRACT NO. 63201					
	PLOT DATE = 4/3/2007	DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		

RESURFACING LIMIT



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	460	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kanthaphixaybo	DESIGNED - BCK	REVISED -
at:\pwork\PW\DOT\KANTHAPHIXAYBO\01126	4\tr of fso.legend_v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1' - 0"	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

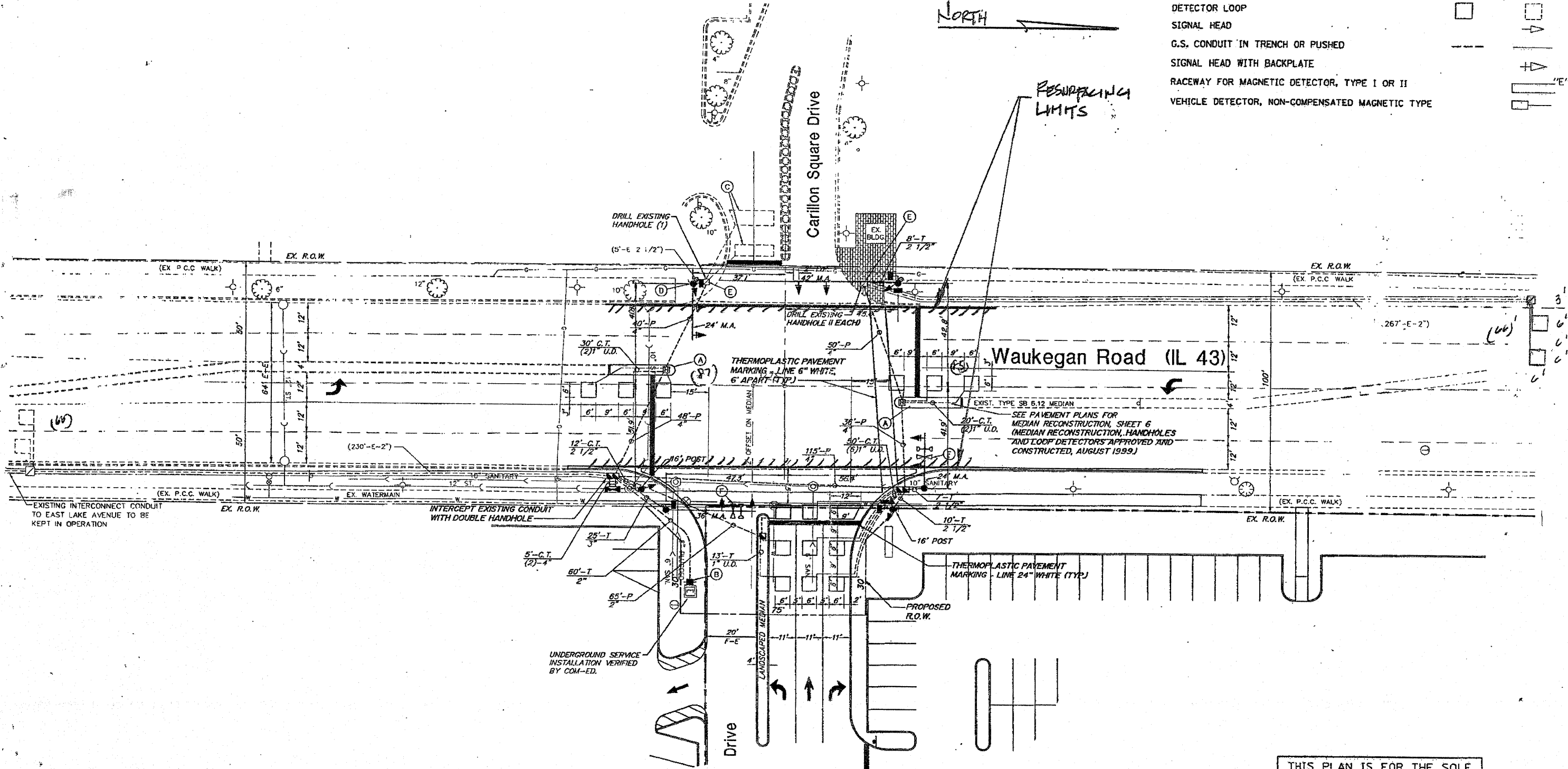
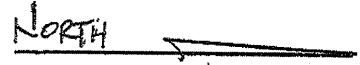
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 43 (WAUKEGAN) @ E. LAKE AV.

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
34E 2371 (A95) A3-4		COOK	35	30
SCALE: NONE		SHEET NO. OF SHEETS		STA. TO STA.
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

CONTRACT NO. 6001

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	304	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kenthaphixaybo	DESIGNED - BCK	REVISED -
4:\work\PH1\DOT\KANTHAPHIXAYBO\01126	4:\traffic\legend.v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

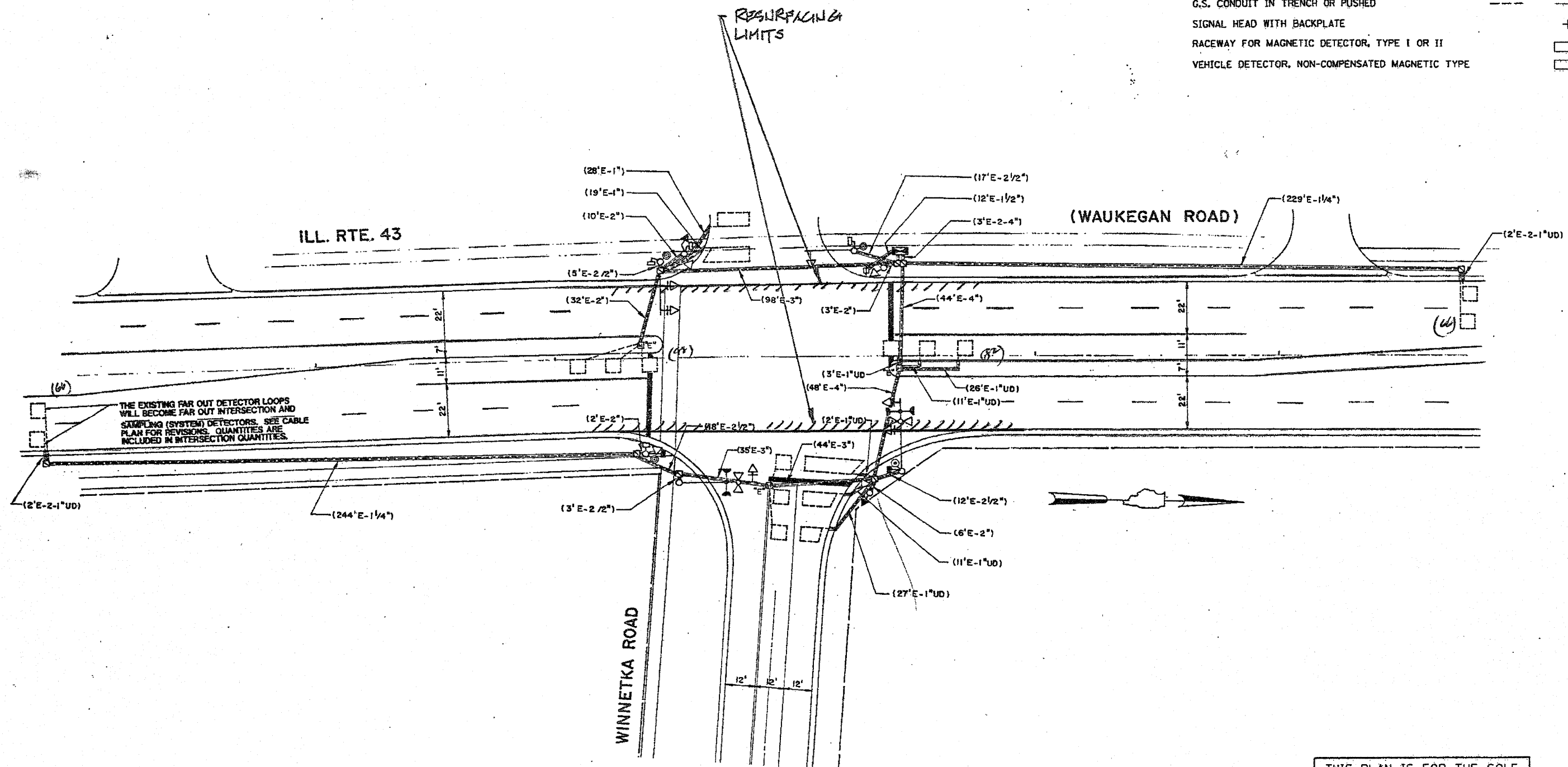
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 43 (WAUKEGAN) @ CARILLON SQ.**

SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			SR 3071 (R1) R1-4	LOOK	LOGK	35	31

CONTRACT NO. 60-01

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THE EXISTING FAR OUT DETECTOR LOOPS WILL BECOME FAR OUT INTERSECTION AND SAMPLING (SYSTEM) DETECTORS. SEE CABLE PLAN FOR REVISIONS. QUANTITIES ARE INCLUDED IN INTERSECTION QUANTITIES.

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	312	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = karrthephxojbo	DESIGNED - BCK	REVISED -
at\pwork\PIWIDOT\KANTHAPHIXAY8CV081128	4\traffic\legend_v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

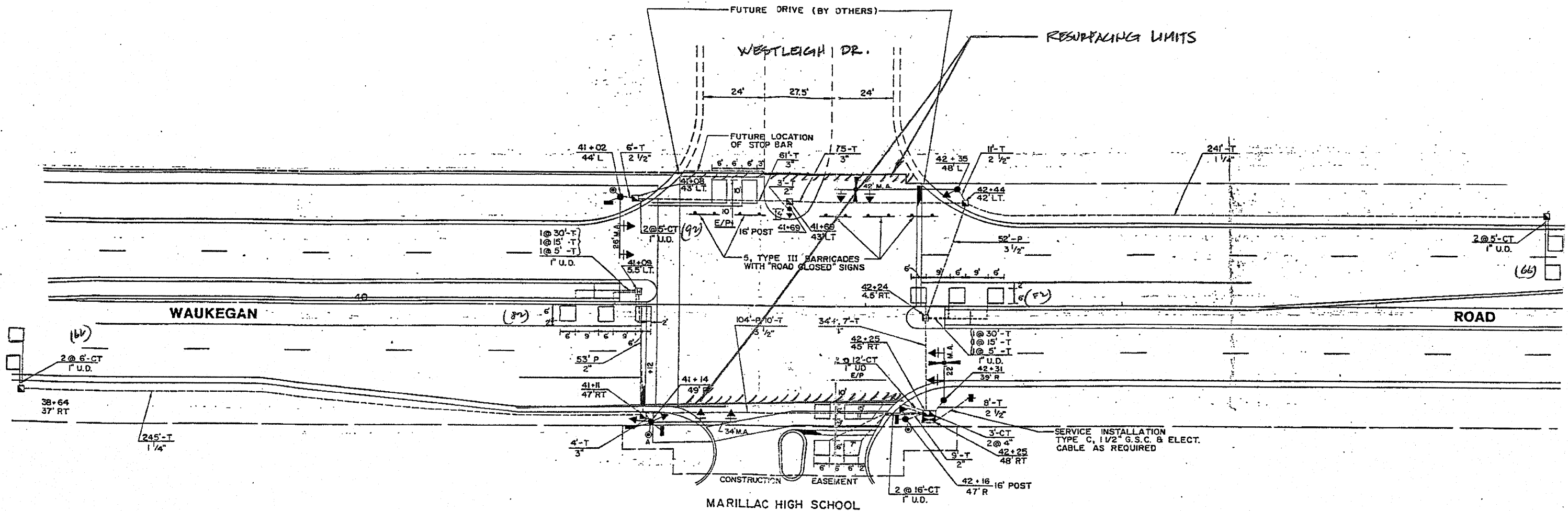
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 43 (WAUKEGAN) @ WINNETKA RD.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
306	1571 (40) AS-4	COOK	35	22
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 602-11	

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

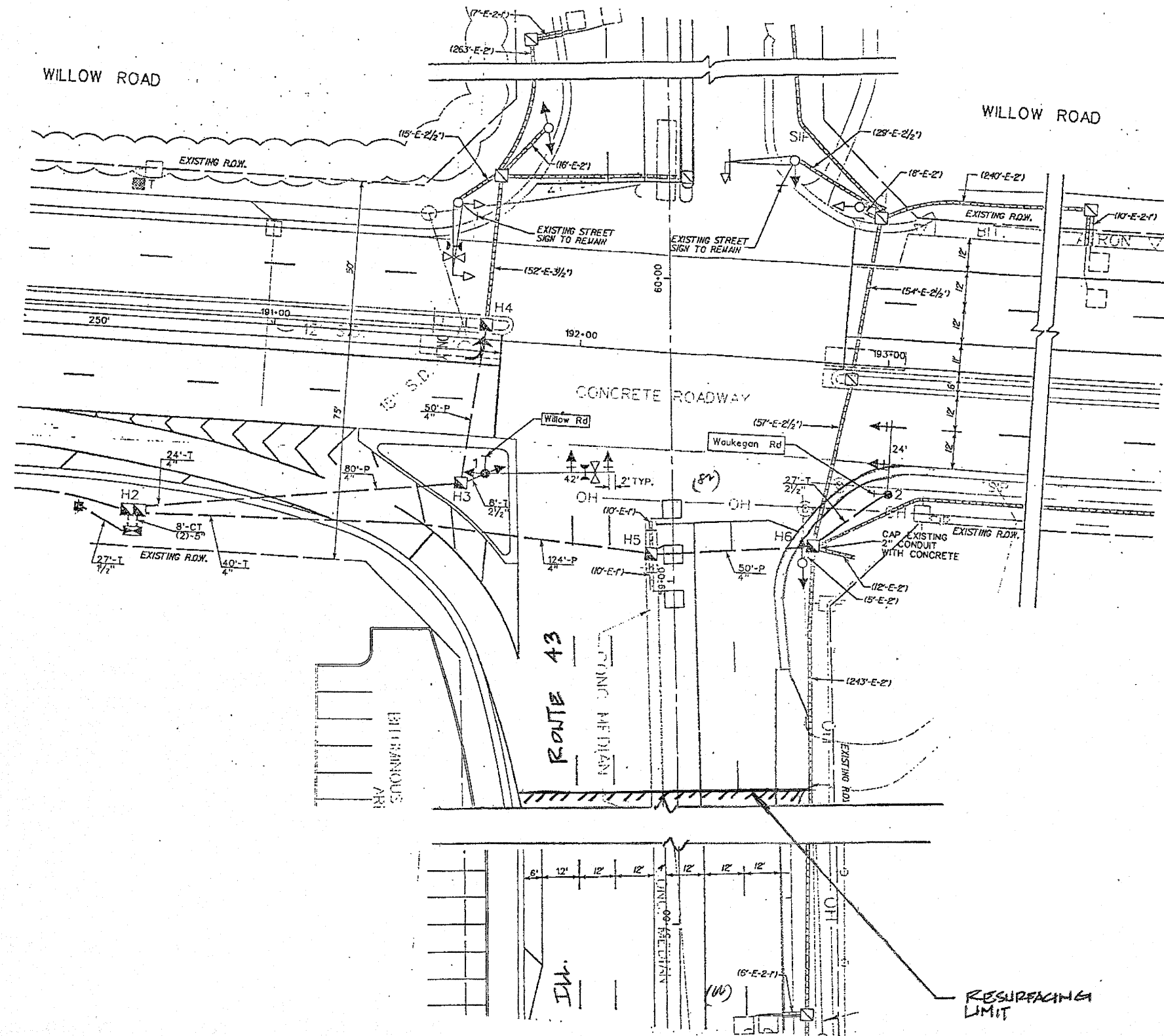
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	388	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kanthaphixaybo	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. RTE. 43 (WAUKEGAN) @ WESTLEIGH DR.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
or\p\work\PMIDOT\KANTHAPHIXAYBO\081126	4\tr-off\ca.legend.v7.dgn	DRAWN - BCK	REVISED -			248	5971 (R&D) RS-4	COOK	33	33	
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -			SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO.
	PLOT DATE = 4/3/2009	DATE -	REVISED -								

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



NOTE: WHERE REDLIGHT VIDEO ENFORCEMENT IS UTILIZED, IT SHALL BE THE RESPONSIBILITY OF THE RESIDENT ENGINEER TO NOTIFY THE OWNER OF THIS EQUIPMENT THEIR IN-PAVEMENT SENSORS MAY BE JEOPARDIZED DUE TO RESURFACING.

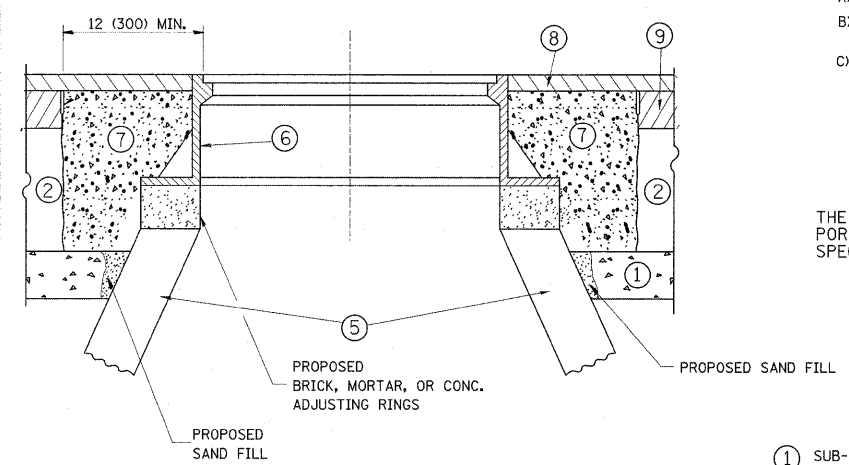
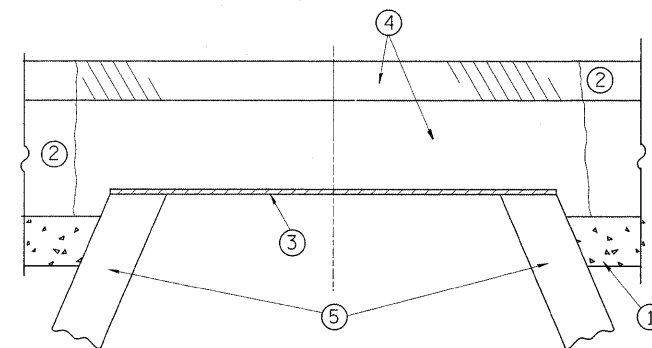
REDFLEX T.S. @ 1-480-607-0705

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	66	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kanthaphixeybo	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 43 (WAUKEGAN) @ WILLOW RD.	F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
as\p4_work\VP\DOT\KANTHAPHIXAY8C\091126	4\traffic.legend.v7.dgn	DRAWN - BCK	REVISED -			348	371 (244) R-4	COOK	35	24	
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -			CONTRACT NO. 63501					
	PLOT DATE = 4/3/2009	DATE -	REVISED -			SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

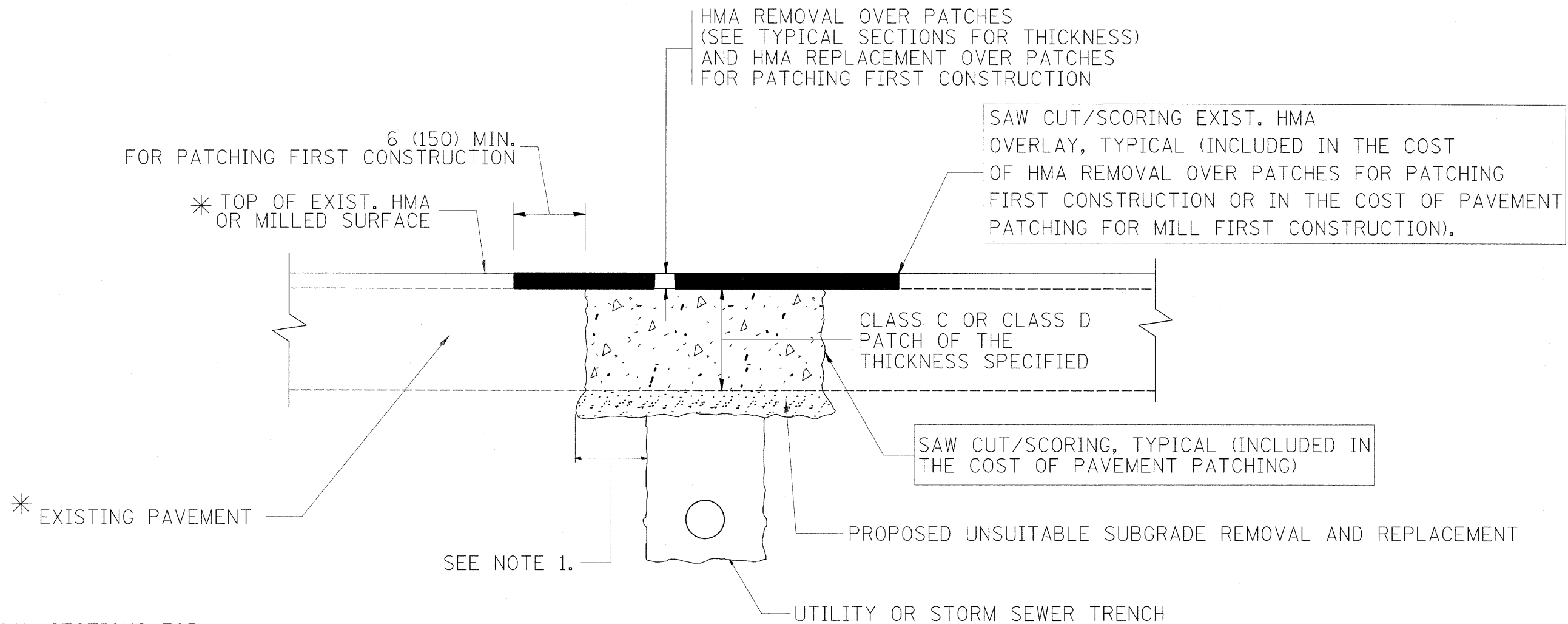
BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = est:mablek	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
est\pw_work\PWIDOT\ESTIMABLEK\d0147666\d0147666.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		348	3271 (A&B) RS-4	COOK	35	25			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - R. WIEDEMAN 05-14-04		BD600-03 (BD-8)			CONTRACT NO. 60101				
PLOT DATE = 12/17/2009		DATE - 10-25-94	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = estmablek	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw\work\VPWIDOT\ESTIMABLEK\d0147666\d0147666.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07			348	3271 (A&B) RS-4	COOK	35	26	
PLOT DATE = 12/17/2009	DATE - 10-25-94	CHECKED -	REVISED - R. BORO 09-04-07			BD400-04 (BD-22)		CONTRACT NO. 60101			
		DATE -	REVISED - K. ENG 10-27-08			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)
1/4" (5) **

18" (450) MAX.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

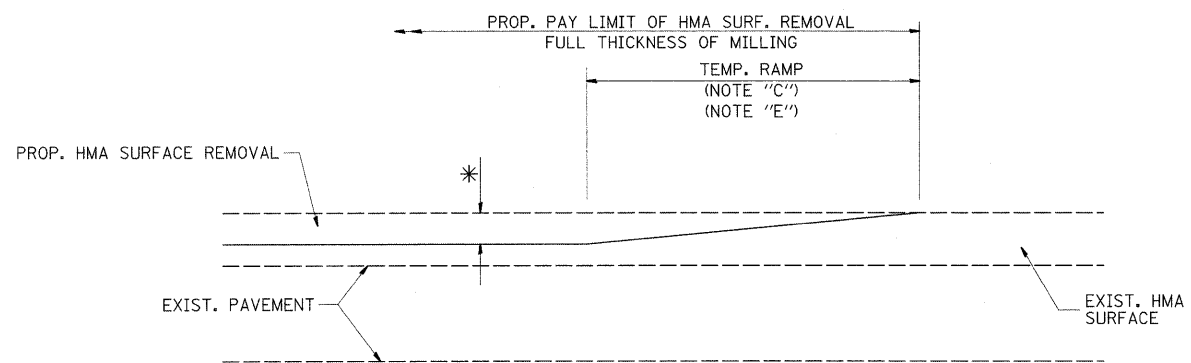
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

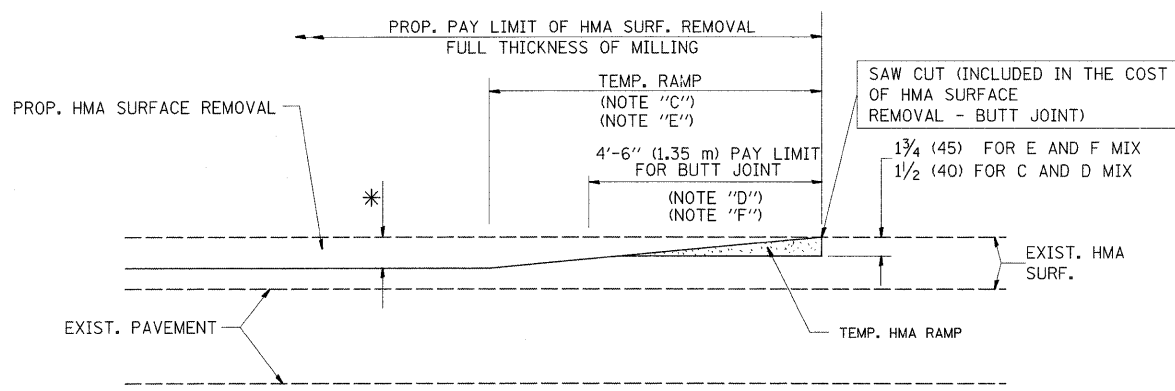
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = estmablek	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
est\pw\work\VPWIDOT\ESTIMABLEK\vd0147666\eststd.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	348			3271 (A&B) RS-4	COOK	35	27	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	BD600-06 (BD-24)			CONTRACT NO. 60101				
PLOT DATE = 12/17/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

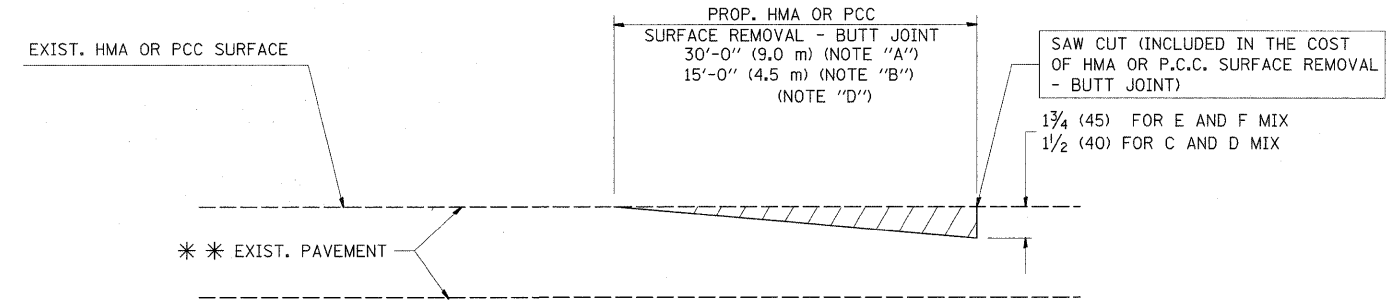
OPTION 1



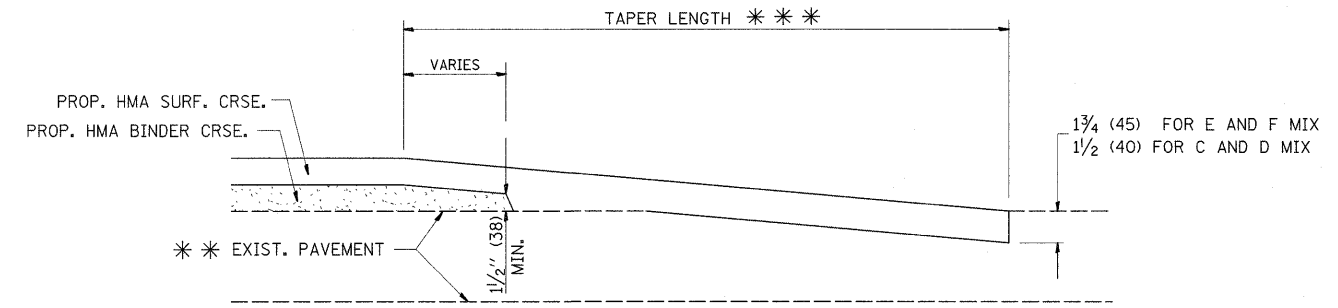
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

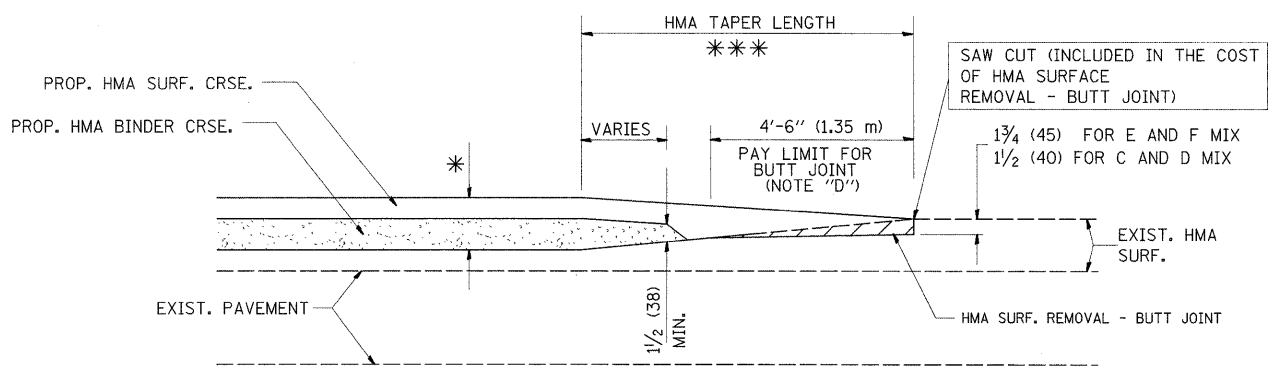
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

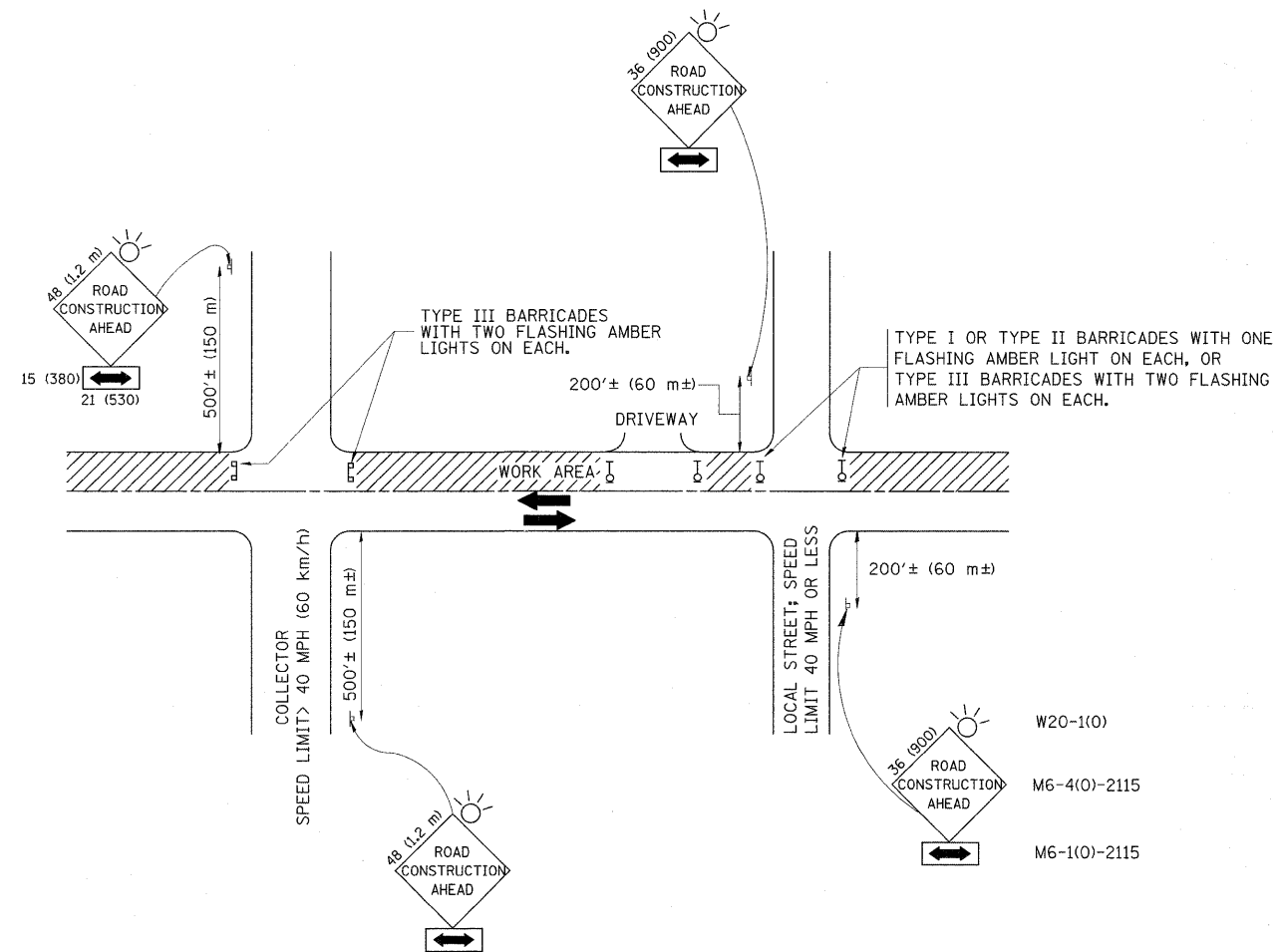
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = estmablek	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS		F.A.P. RTE. 348	SECTION 3271 (A&B) RS-4	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 28
est\pw_work\PMIDOT\ESTIMABLEK\d0147666\d01510.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD400-05 BD32		CONTRACT NO. 60101	
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 04-06-01				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT DATE = 12/17/2009		DATE - 06-13-90	REVISED - R. BORO 01-01-07								



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

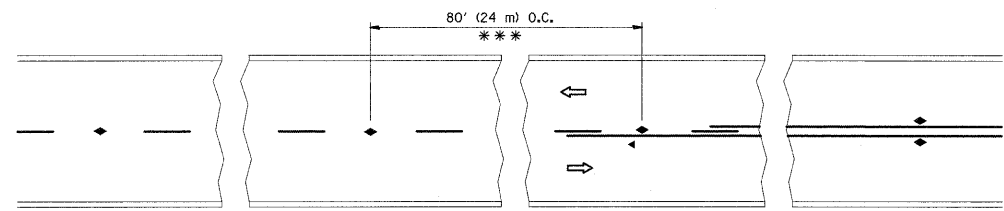
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

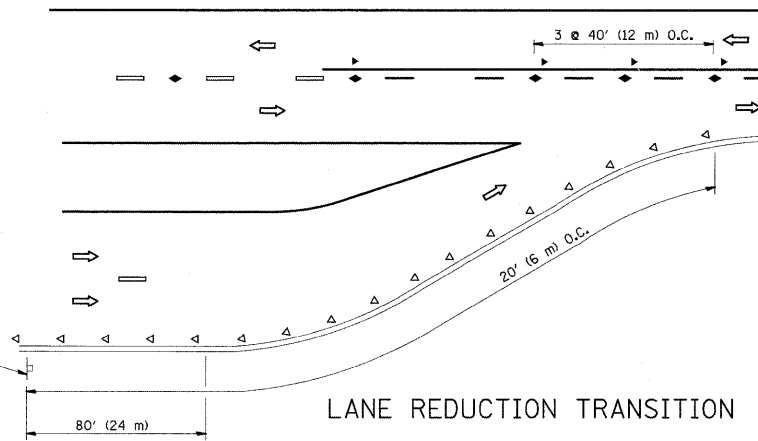
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = estumalek	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
est\pwwork\PWIDOT\ESTIMABLEK\d0147666\d015td.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96		348	3271 (A&B) RS-4	COOK	35	29				
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - A. HOUSEH 10-15-96		TC-10				CONTRACT NO. 60101				
PLOT DATE = 12/17/2009		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

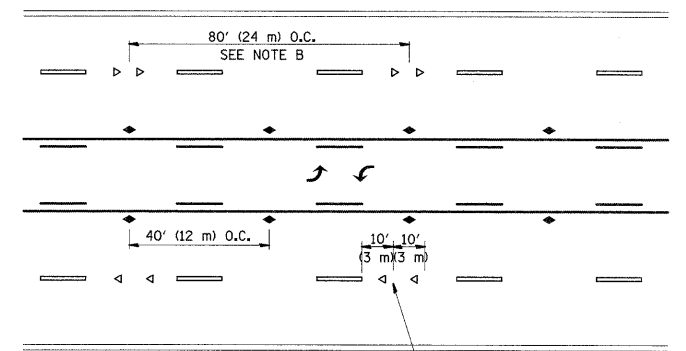


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

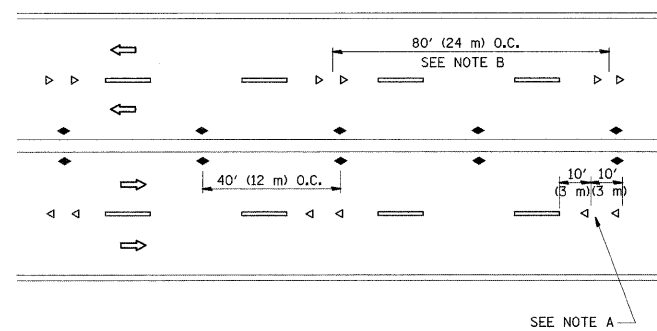
TWO-LANE/TWO-WAY



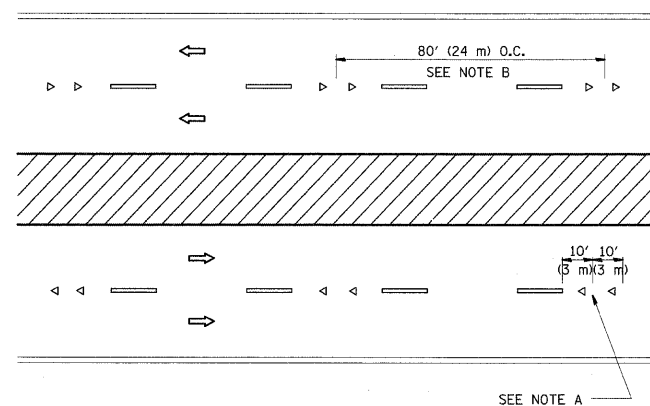
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

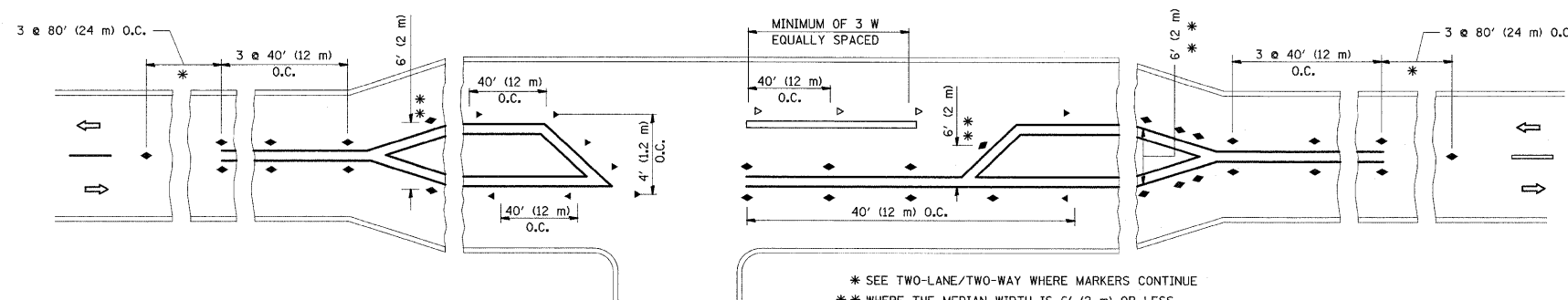
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

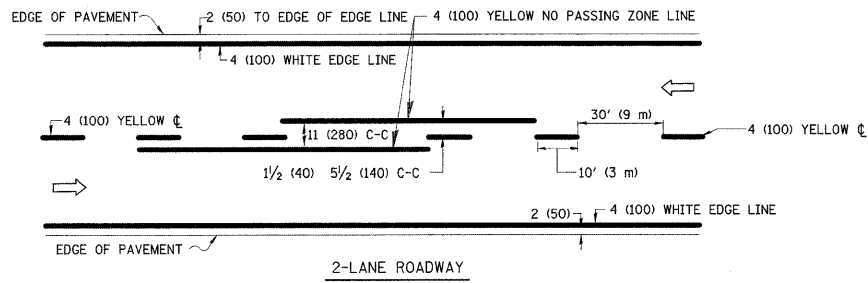
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



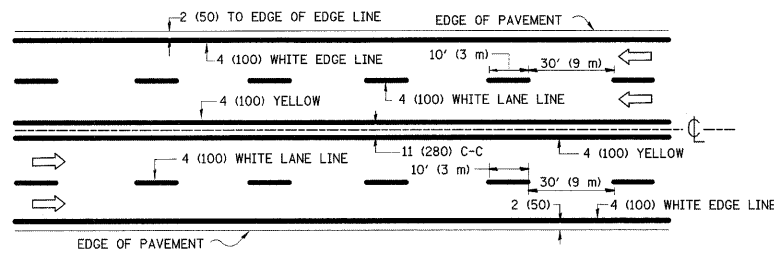
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

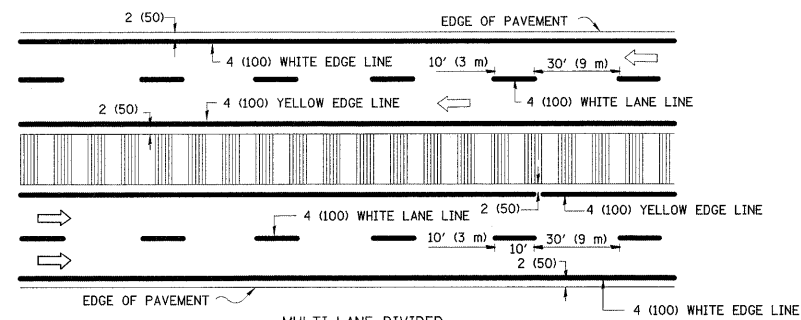
FILE NAME =	USER NAME = estumabjek	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pw_work\VPWIDOT\ESTIMABLEK\d0147666\est3std.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		348	3271 (A&B) RS-4	COOK	35	30			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - T. RAMMACHER 01-06-00		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			TC-11		CONTRACT NO. 60101		
PLOT DATE = 12/17/2009		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



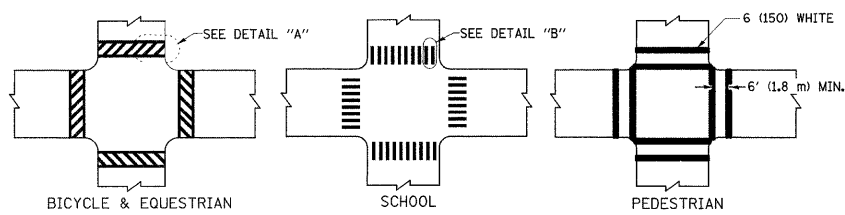
MULTI-LANE UNDIVIDED



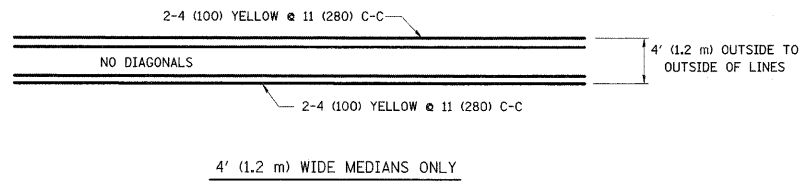
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

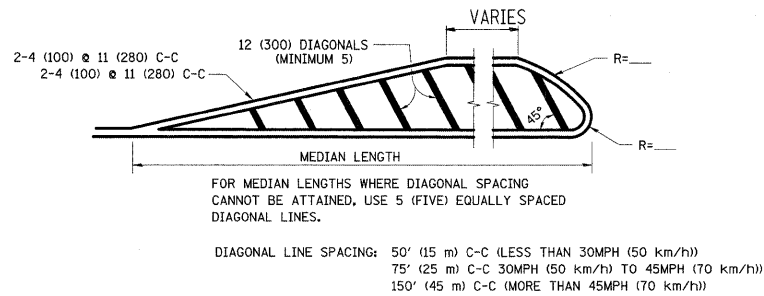
TYPICAL LANE AND EDGE LINE MARKING



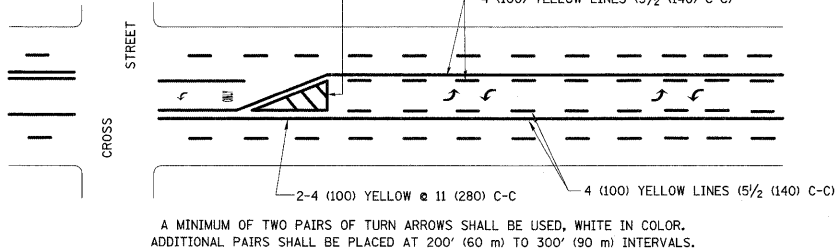
TYPICAL CROSSWALK MARKING



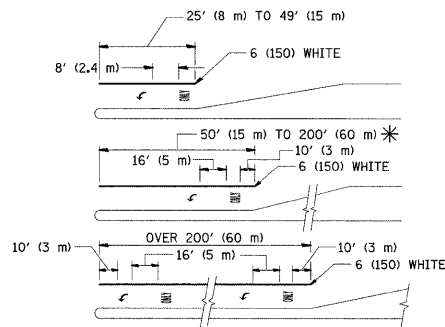
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



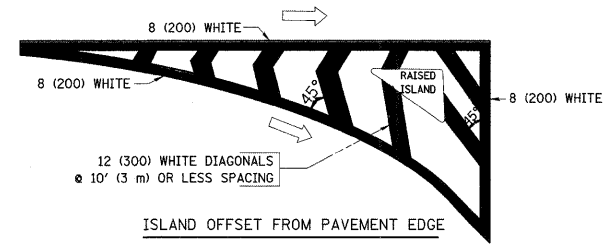
TYPICAL PAINTED MEDIAN MARKING



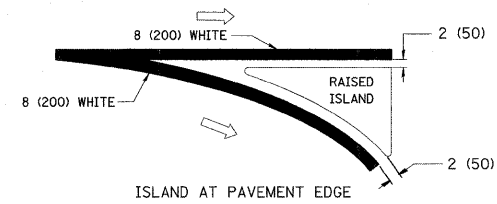
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

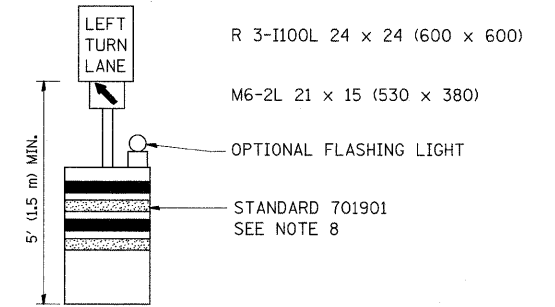
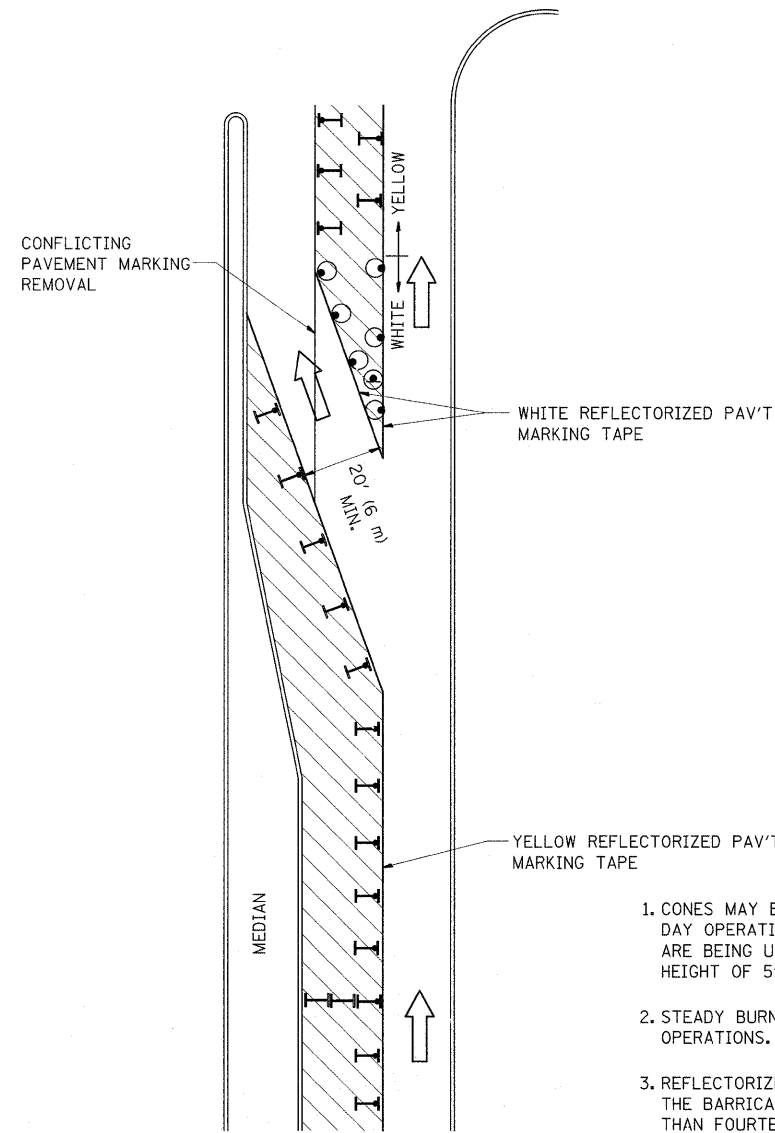
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 12/17/2009		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 348	SECTION 3271 (A&B) RS-4	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 31
TC-13		CONTRACT NO. 60101		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

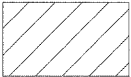







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

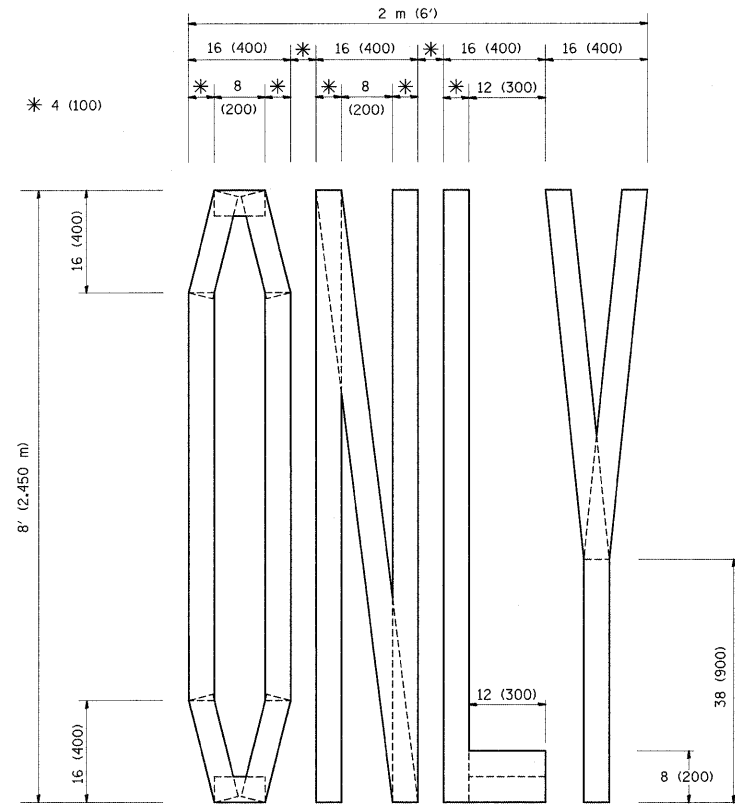
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	PLOT SCALE = 50.0000' / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 12/17/2009	REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

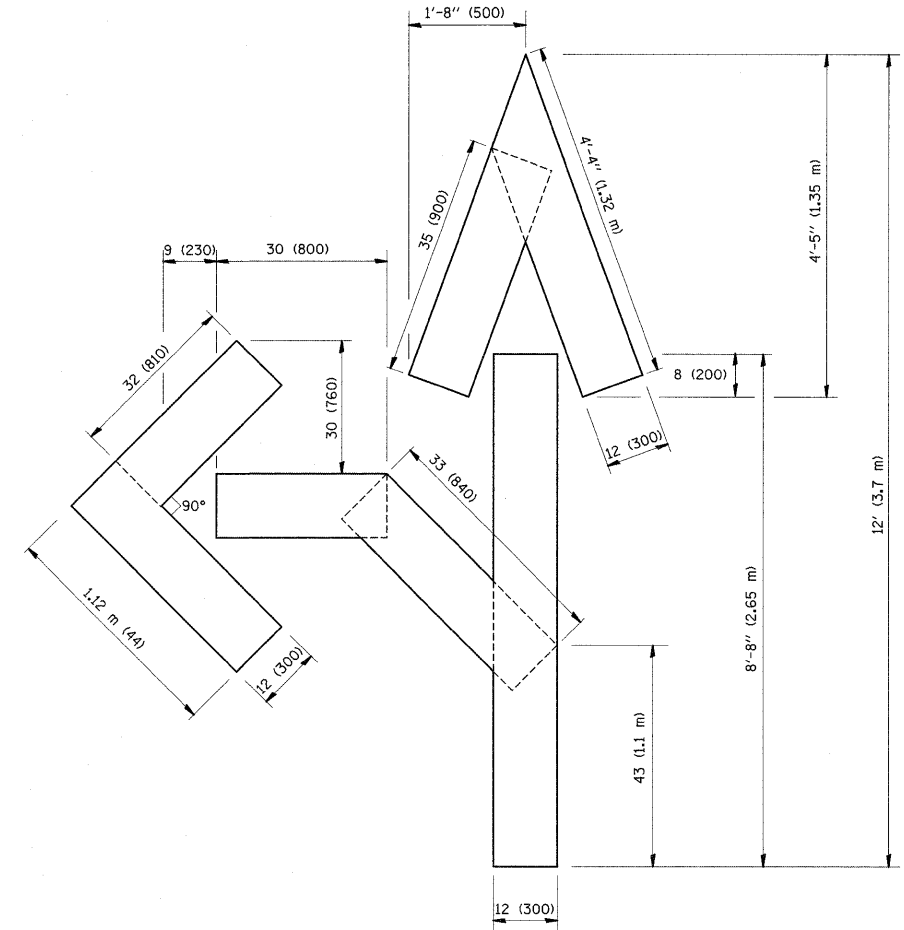
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

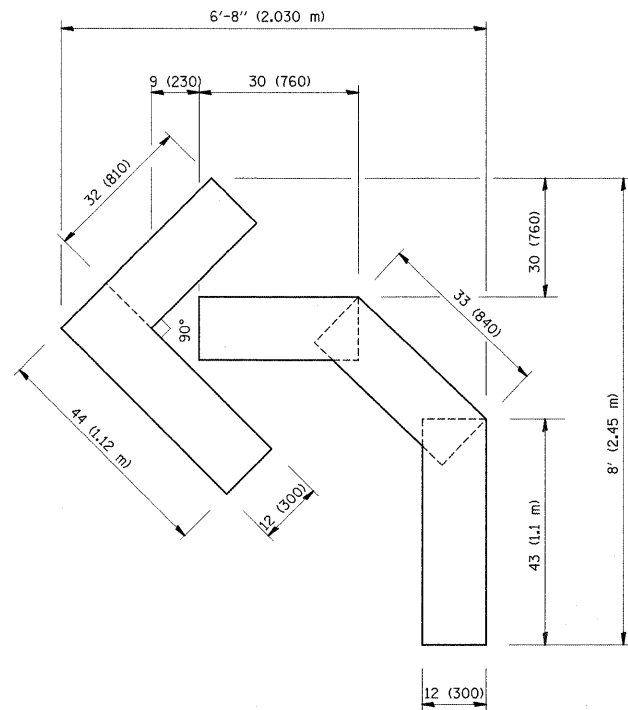
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348	3271 (A&B) RS-4	COOK	35	32
TC-14			CONTRACT NO. 60101	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

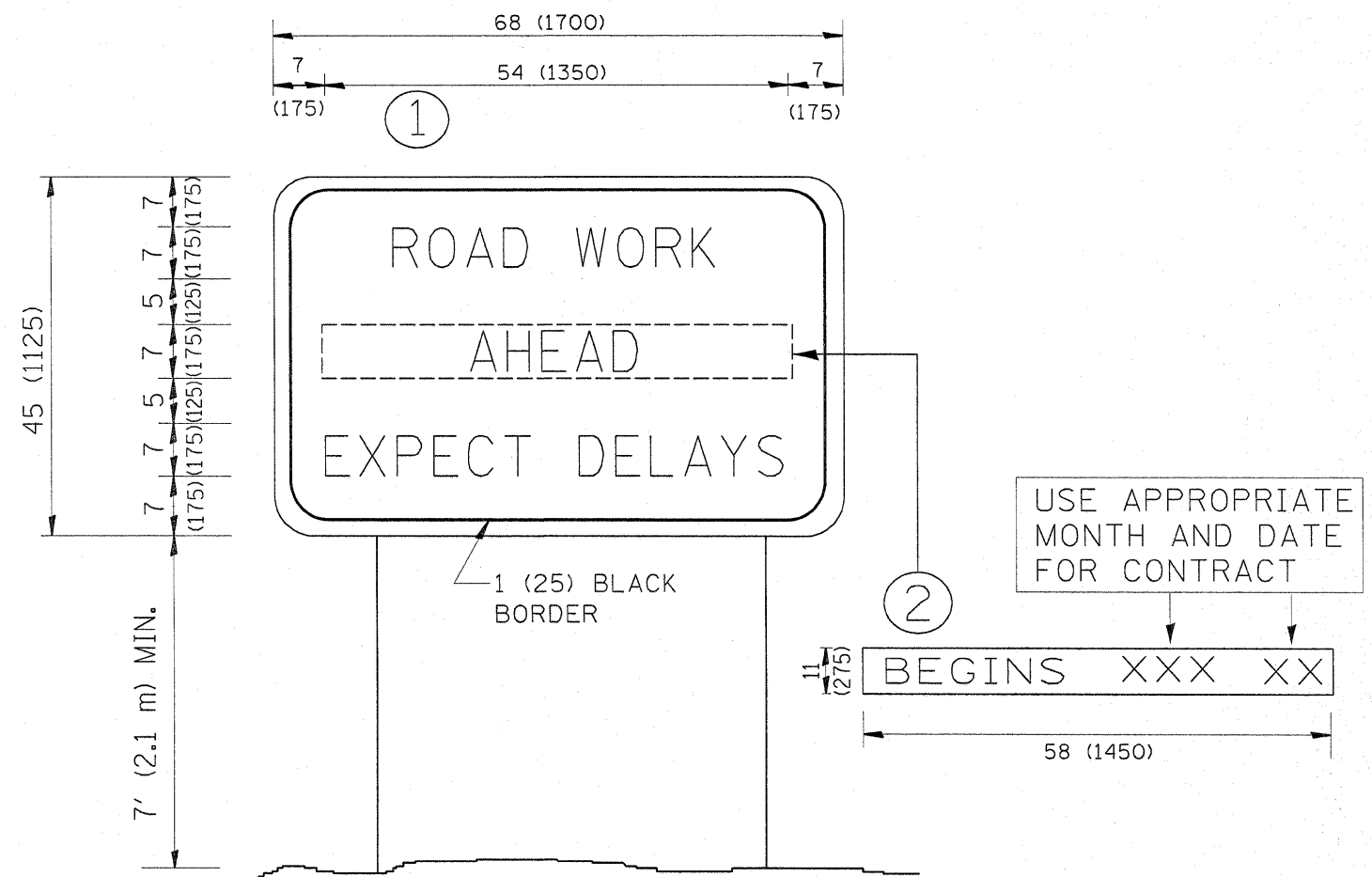
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PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -T. RAMMACHER 03-02-98
PLOT DATE = 12/17/2009		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3271 (A&B) RS-4	COOK	35	33
TC-16			CONTRACT NO. 60101	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

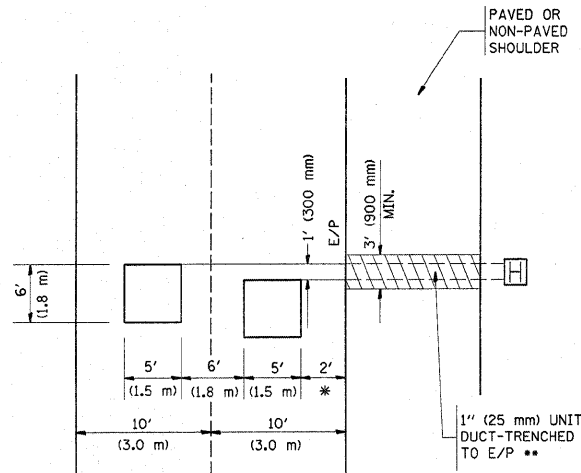
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = est:moblek	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 12/16/2009	CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 60101			
		DATE -	REVISED - C. JUCIUS 01-31-07			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

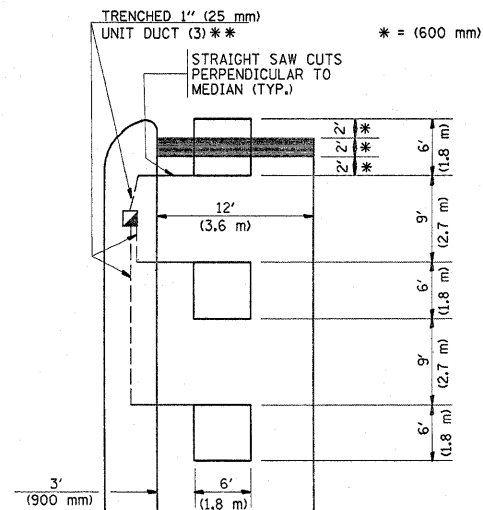


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

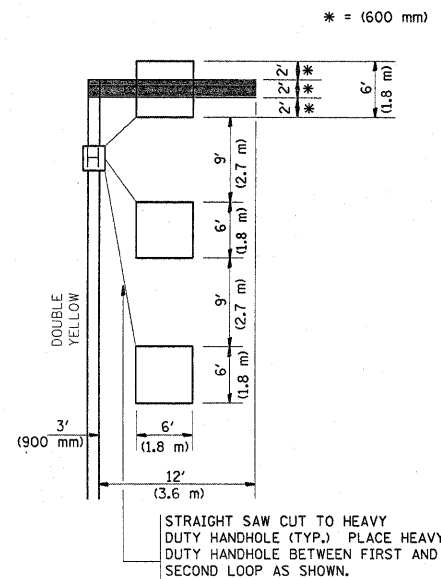


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

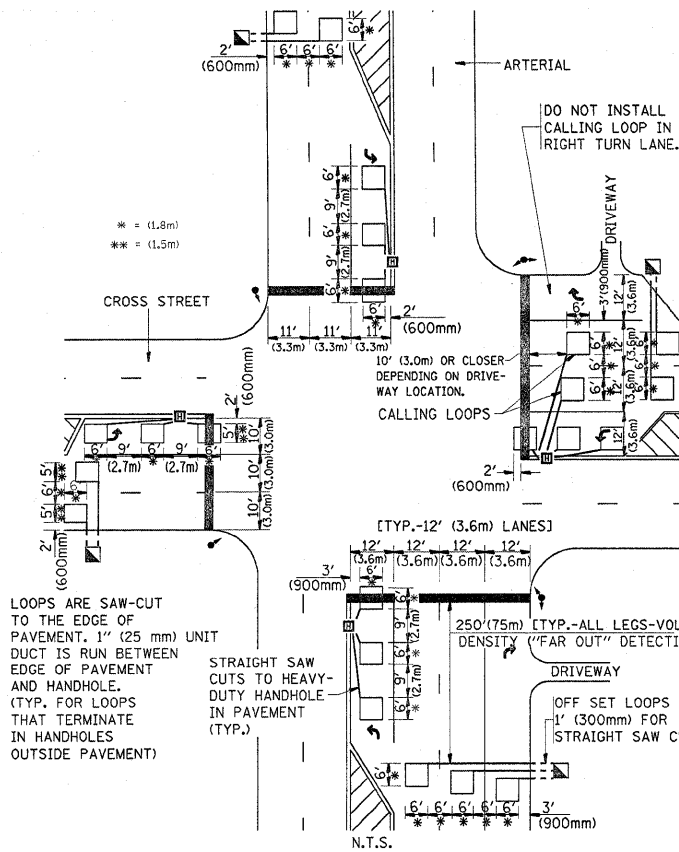
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



* = (600 mm)

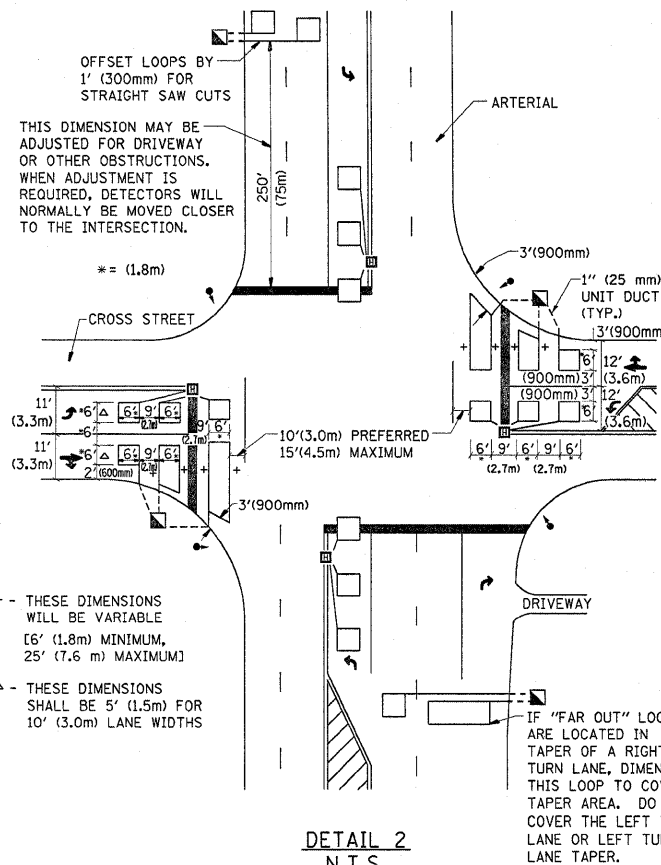
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = estimablek	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 12/16/2009		DATE -	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.			