

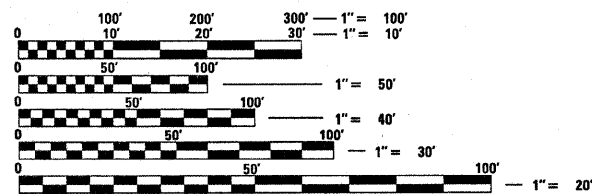
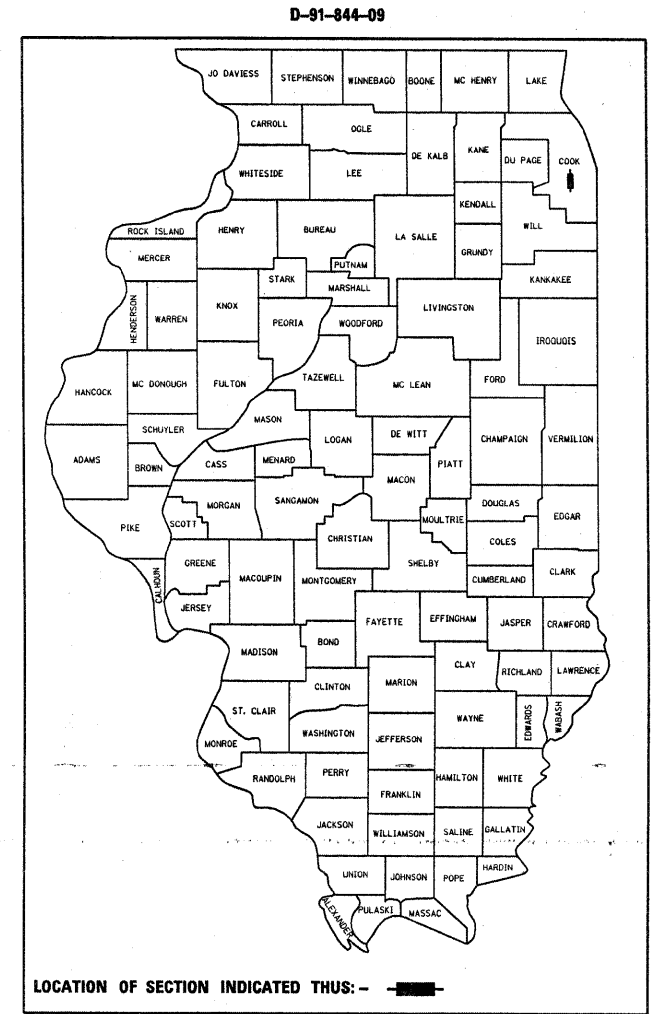
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2843	3248 RS-1	COOK	26	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 60104		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED HIGHWAY PLANS
FAU ROUTE 2843 : DIXIE HWY
US 6 (159th ST) TO GOVERNORS HWY
SECTION: 3248 RS-1
RESURFACING

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN THE VILLAGES OF EAST HAZEL CREST AND HAZEL CREST & THE CITIES OF MARKHAM AND HARVEY

COOK COUNTY
C-91-844-09



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

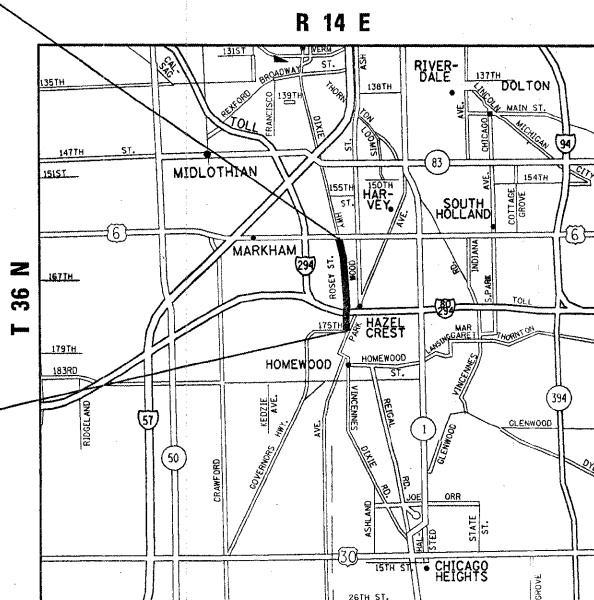
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER ROBERT BORO (847)705-4178
PROJECT MANAGER KEN ENG

CONTRACT NO. 60104

PROJECT ENDS
STA. 104 + 17

PROJECT BEGINS
STA. 2 + 47



BREMEN TOWNSHIP THORNTON TOWNSHIP

TRAFFIC DATA

ADT (2006) = 16600
POSTED SPEED LIMIT = 35 TO 45 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED DECEMBER 14, 20 09

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

January 29 20 10
Scott E. Stitt P.E.
REGIONAL ENGINEER OF DESIGN AND ENVIRONMENT

January 29 20 10
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

NET & GROSS LENGTH OF PROJECT = 10170 FEET = 1.93 MILES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF EAST HAZEL CREST, HAZEL CREST AND THE CITIES OF MARKHAM AND HARVEY.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h), OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 km/h). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1 : 3 (V : H).

UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

ALL PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705- 4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT PATRICE HARRIS AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE ENGINEER SHALL REPORT CLEARANCES UNDER THE BRIDGE AFTER RESURFACING.

THE CONTRACTOR SHALL VERIFY THE EXISTING TYPE/HEIGHT OF EXISTING GUARDRAIL BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR "GUARDRAIL REMOVAL"

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-5	TYPICAL SECTIONS
6-9	ROADWAY & PAVEMENT MARKING PLANS
10-14	DETECTOR LOOP REPLACEMENT PLANS
15	DETAILS FOR FRAMES & LIDS ADJUSTMENT WITH MILLING (BD-8)
16	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT (BD-22)
17	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
18	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)
19	DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJUCENT TO CURB AND GUTTER (BD-34)
20	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
21	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
22	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
23	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
24	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
25	ARTERIAL ROAD INFORMATION SIGN (TC-22)
26	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

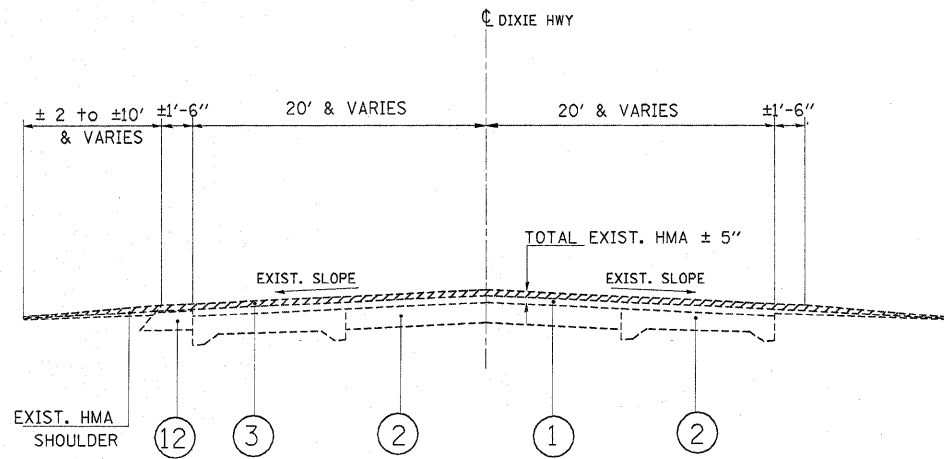
STANDARDS

STANDARD NO.	DESCRIPTION
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
701101-02	OFF ROAD OPERATIONS, MULTILANE, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701426-03	LANE CLOSURE, MULTILANE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701606-06	URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
701701-06	URBAN LANE CLOSURE MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

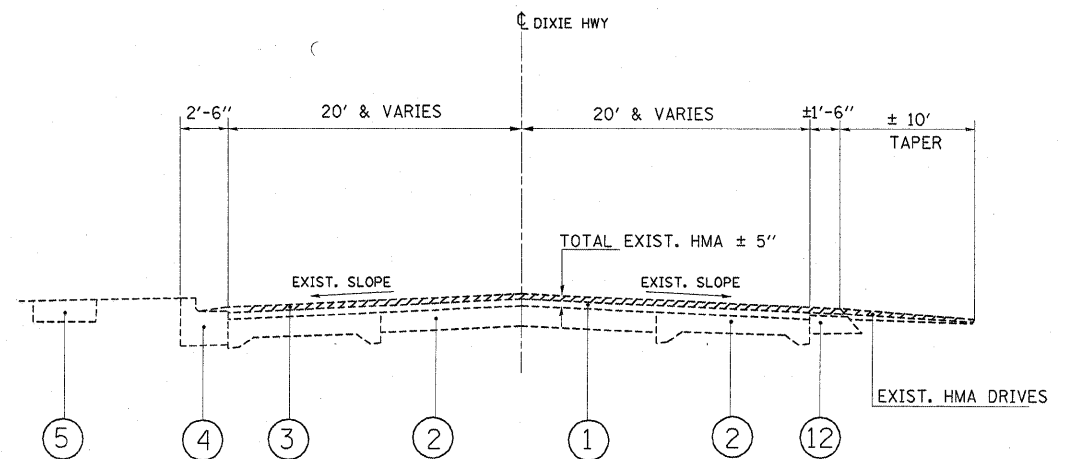
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DIXIE HWY (159th ST TO GOVERNORS HWY) INDEX OF SHEETS LIST OF STATE STANDARDS PLAN NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\pwsdot\gorengautab\d0147544\0184409-shit-plan.dgn	DRAWN -	REVISED -	2843			3248 RS-1	COOK	26	2	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60104							
PLOT DATE = 9/14/2009	DATE -	REVISED -	SCALE:			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS

SUMMARY OF QUANTITIES			URBAN 100% STATE CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			URBAN 100% STATE CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	I 000					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	I 000					
20201006	GRADING AND SHAPING SHOULDERS	UNIT	87	87					70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	40	40					70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
20400800	FURNISHED EXCAVATION	CU YD	160	160					70106800	CHANGEABLE MESSAGE SIGN	CAL MO	3	3					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	458	458					70300100	SHORT-TERM PAVEMENT MARKING	FOOT	11181	11181					
25000210	SEEDING, CLASS 2A	ACRE	0.09	0.09					70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	108.9	108.9					
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	8	8					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	37515	37515					
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	8	8					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	640	640					
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	8	8					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	520	520					
25100630	EROSION CONTROL BLANKET	SO YD	425	425					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	340	340					
25200110	SODDING, SALT TOLERANT	SO YD	33	33					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	108.9	108.9					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	48	48					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	37515	37515					
40600300	AGGREGATE (PRIME COAT)	TON	240	240					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	640	640					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	90	90					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	520	520					
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2375	2375					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	340	340					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	996	996					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	480	480					* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4836	4836					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	573	573					
42001300	PROTECTIVE COAT	SO YD	65	65					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	989	989					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	57568	57568					X0322256	TEMPORARY INFORMATION SIGNING	SO FT	154.2	154.2					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	200	200														
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	3585	3585														
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	1152	1152														
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	864	864														
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	864	864														
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SO YD	240	240														
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	686	686														
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10														
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	21	21														
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4														
63200310	GUARDRAIL REMOVAL	FOOT	200	200														
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4														
67100100	MOBILIZATION	L SUM	1	1														

*Specialty Items

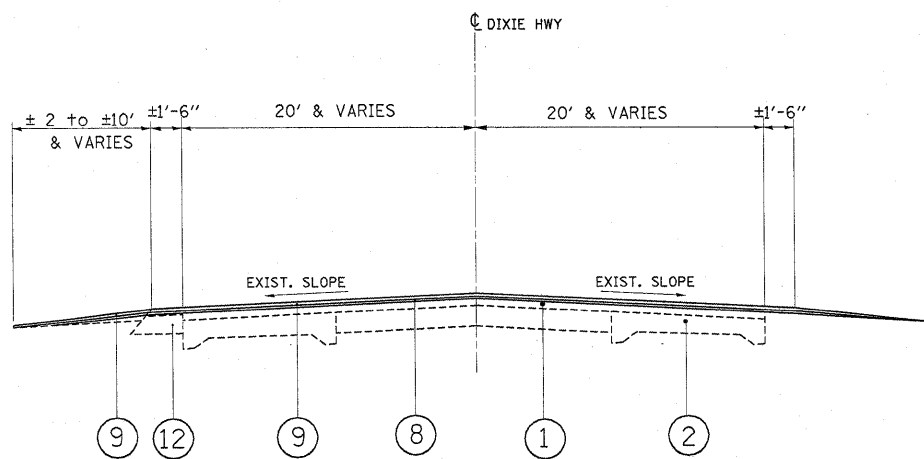


EXISTING TYPICAL SECTION

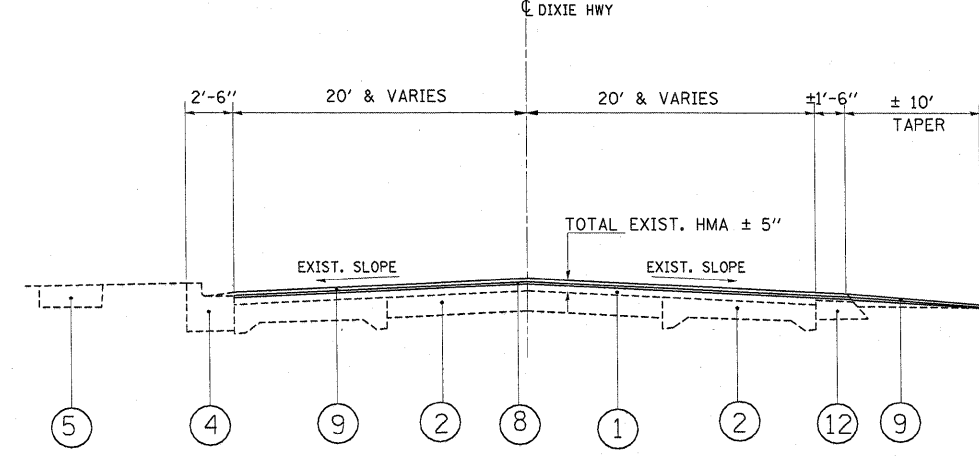


EXISTING TYPICAL SECTION

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING



PROPOSED TYPICAL SECTION



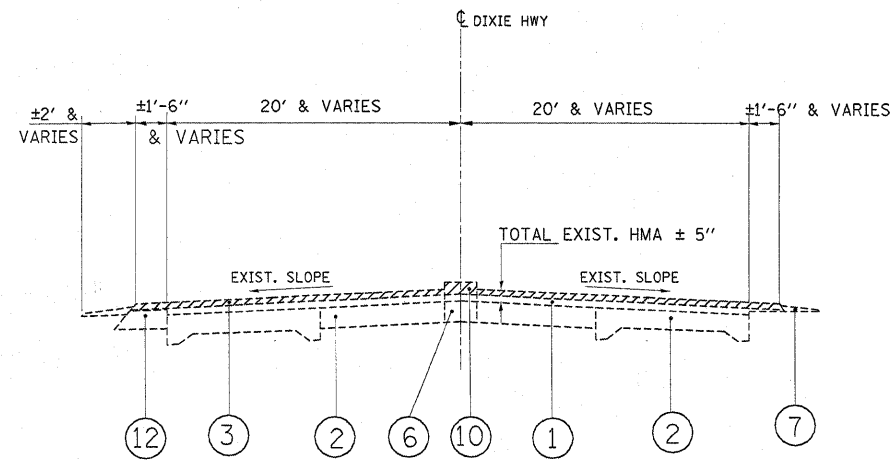
PROPOSED TYPICAL SECTION

LEGEND

- ① EXISTING HMA SURFACE COURSE ±5"
- ② EXISTING CONCRETE PAVEMENT FROM ±7" TO ±9"
- ③ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- ④ EXISTING COMBINATION CONC. CURB & GUTTER TYPE B 6.24 OR B6.12
- ⑤ EXISTING PCC SIDEWALK, 5"
- ⑥ EXISTING CORRUGATED CONCRETE MEDIAN
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2 "
- ⑩ PROP. CONCRETE MEDIAN REMOVAL, PARTIAL DEPTH
- ⑪ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ EXISTING HMA SHOULDER

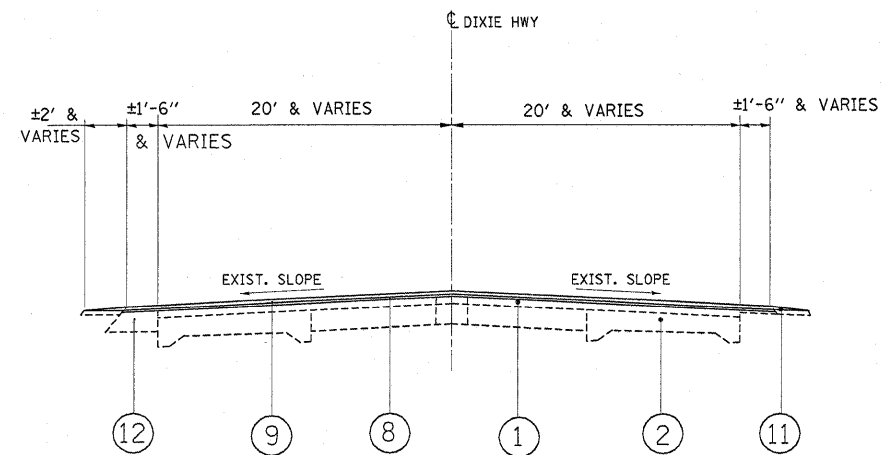
HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES, (HMA BINDER IL-19 mm)	4% @ 70 GYR.

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ.YD./IN
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
- THE "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISION.

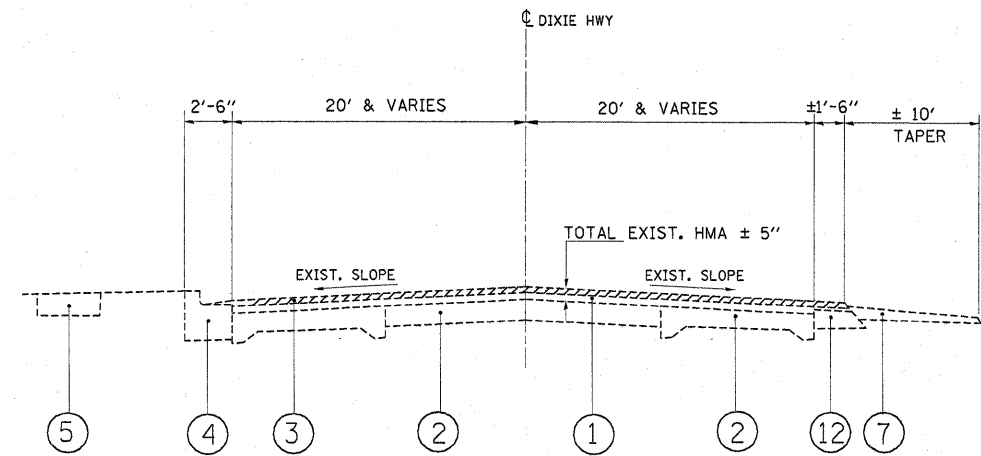


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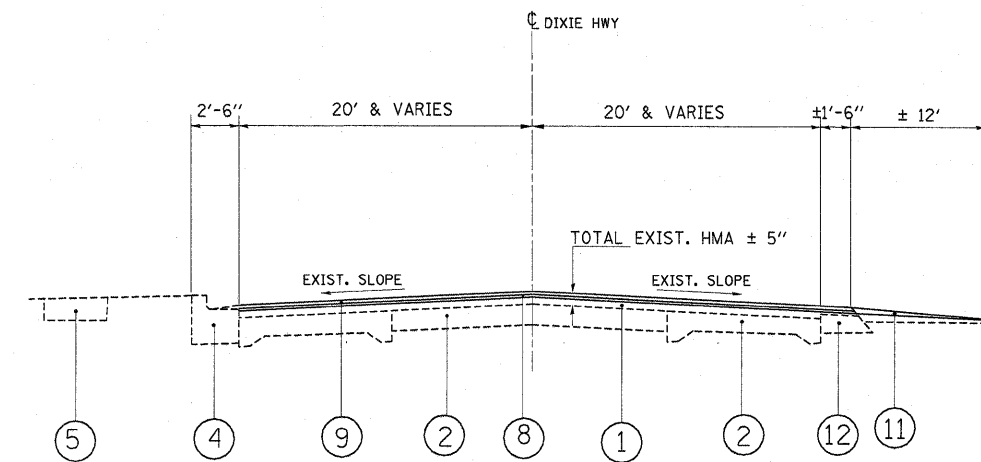
12



PROPOSED TYPICAL SECTION



EXISTING TYPICAL SECTION



PROPOSED TYPICAL SECTION

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- ⑪ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ EXISTING HMA SHOULDER

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		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

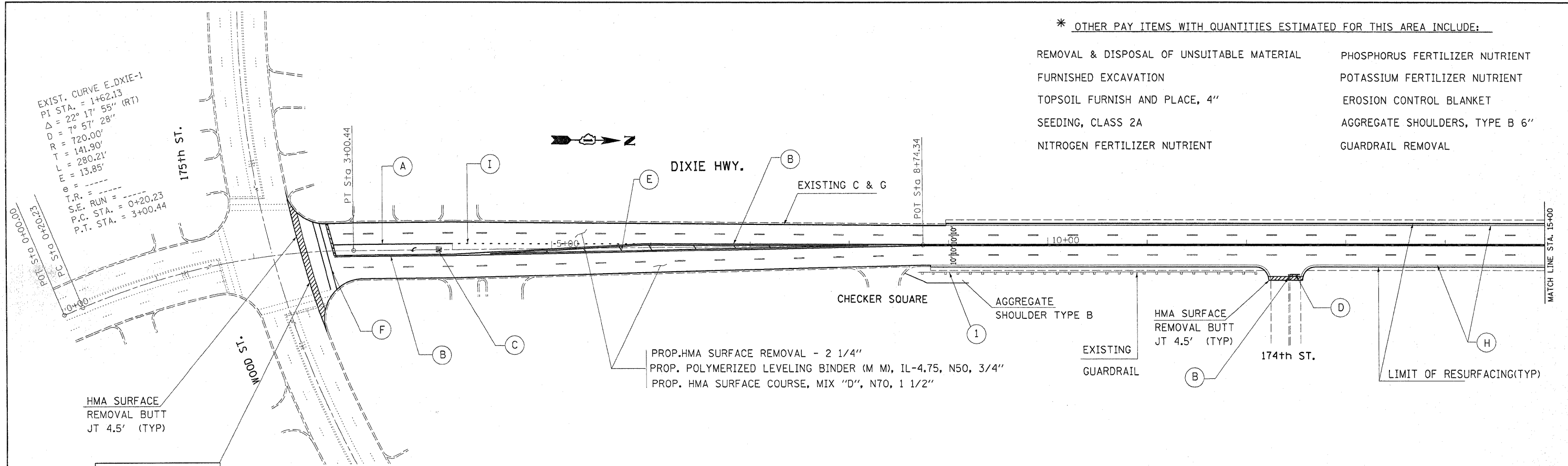
DIXIE HWY (159th ST TO GOVERNORS HWY)
EXISTING, PROPOSED TYPICAL SECTIONS AND
MIXTURE REQUIREMENT

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2843	3248 RS-1	COOK	26	5
CONTRACT NO. 60104				
ILLINOIS FED. AID PROJECT				

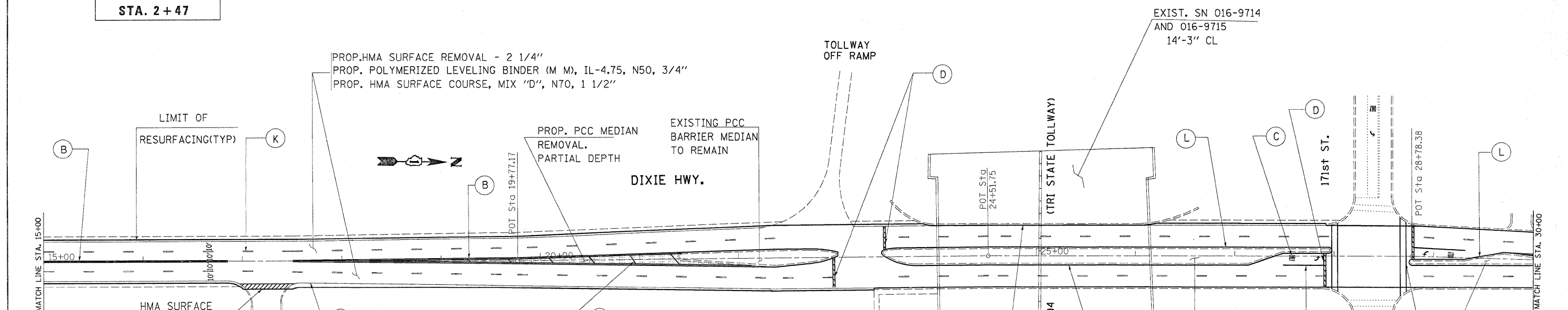
* OTHER PAY ITEMS WITH QUANTITIES ESTIMATED FOR THIS AREA INCLUDE:

- | | |
|---|--------------------------------|
| REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL | PHOSPHORUS FERTILIZER NUTRIENT |
| FURNISHED EXCAVATION | POTASSIUM FERTILIZER NUTRIENT |
| TOPSOIL FURNISH AND PLACE, 4" | EROSION CONTROL BLANKET |
| SEEDING, CLASS 2A | AGGREGATE SHOULDERS, TYPE B 6" |
| NITROGEN FERTILIZER NUTRIENT | GUARDRAIL REMOVAL |



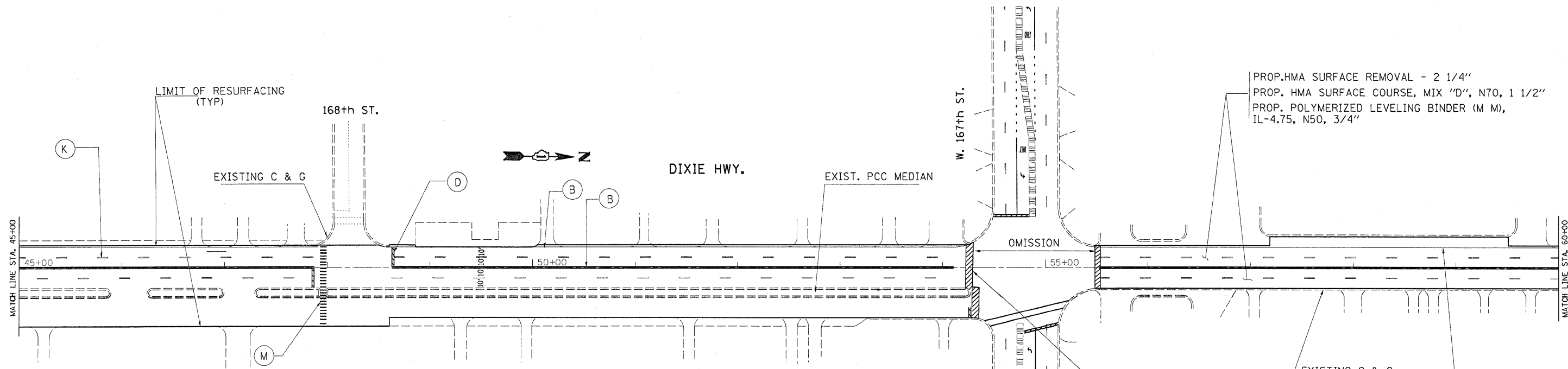
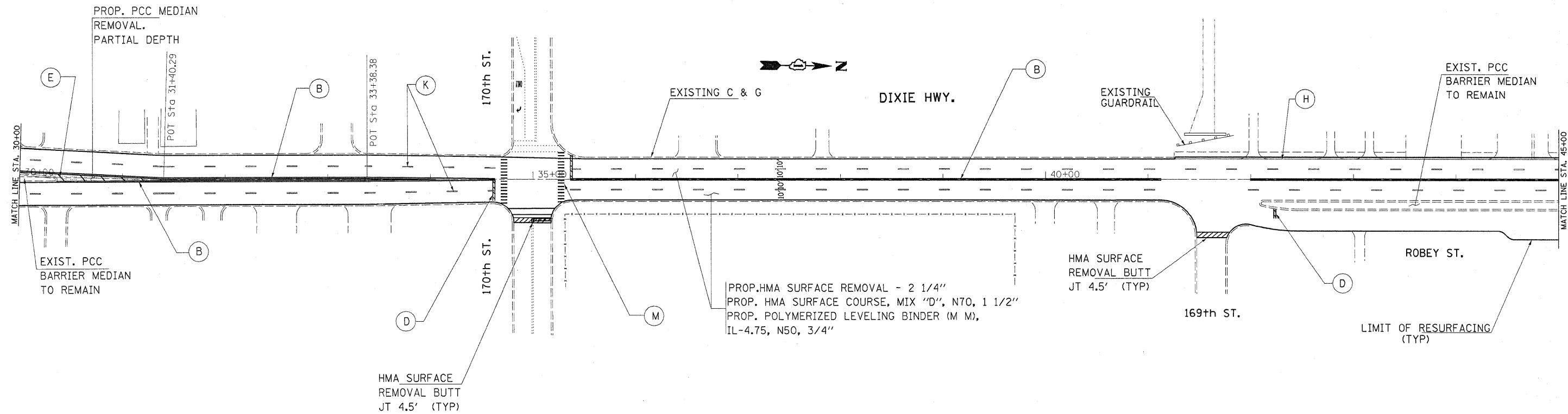
**PROJECT BEGINS
STA. 2 + 47**

PROP. HMA SURFACE REMOVAL - 2 1/4"
PROP. POLYMERIZED LEVELING BINDER (M M), IL-4.75, N50, 3/4"
PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"



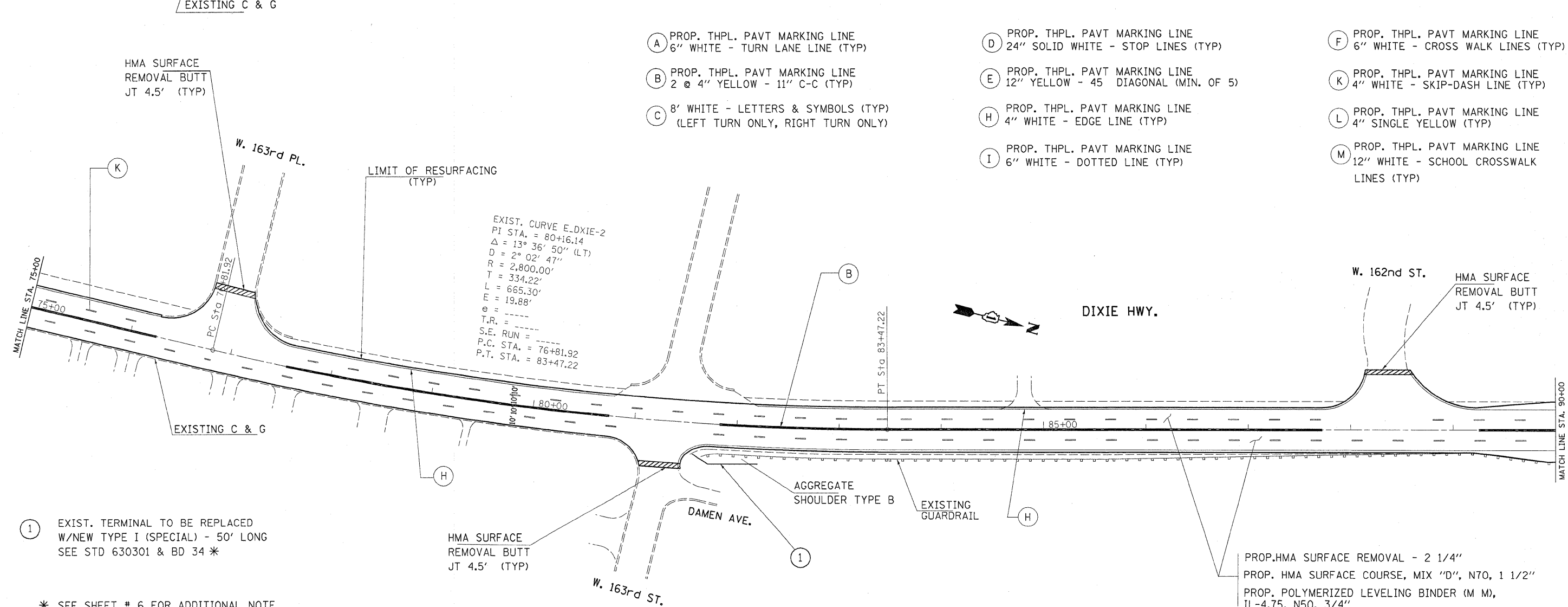
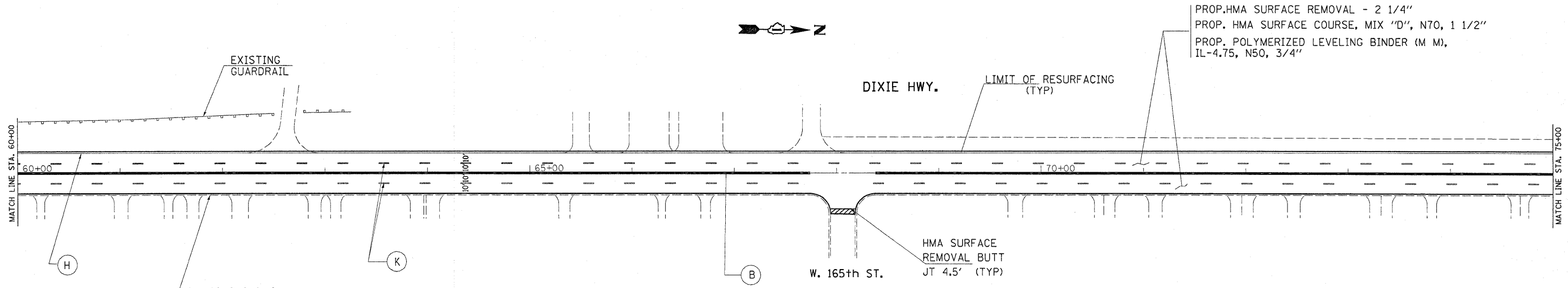
- | | | |
|---|---|--|
| (A) PROP. THPL. PAVT MARKING LINE
6" WHITE - TURN LANE LINE (TYP) | (D) PROP. THPL. PAVT MARKING LINE
24" SOLID WHITE - STOP LINES (TYP) | (F) PROP. THPL. PAVT MARKING LINE
6" WHITE - CROSSWALK LINES (TYP) |
| (B) PROP. THPL. PAVT MARKING LINE
2 @ 4" YELLOW - 11" C-C (TYP) | (E) PROP. THPL. PAVT MARKING LINE
12" YELLOW - 45° DIAGONAL (MIN. OF 5) | (K) PROP. THPL. PAVT MARKING LINE
4" WHITE - SKIP-DASH LINE (TYP) |
| (C) 8' WHITE - LETTERS & SYMBOLS (TYP)
(LEFT TURN ONLY, RIGHT TURN ONLY) | (H) PROP. THPL. PAVT MARKING LINE
4" WHITE - EDGE LINE (TYP) | (L) PROP. THPL. PAVT MARKING LINE
4" SINGLE YELLOW (TYP) |
| (I) PROP. THPL. PAVT MARKING LINE
6" WHITE - DOTTED LINE (TYP) | (M) PROP. THPL. PAVT MARKING LINE
12" WHITE - SCHOOL CROSSWALK LINES (TYP) | (1) EXIST. TERMINAL TO BE REPLACED
W/NEW TYPE I (SPECIAL) - 50' LONG
SEE STD 630301 & BD 34* |

FILE NAME =	USER NAME = gorengoutab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DIXIE HWY (159th ST TO GOVERNORS HWY) ROADWAY PLANS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw\work\pwidot\gorengoutab\d0147544	194489-sht-plan.dgn	DRAWN -	REVISED -			2843	3248 RS-1	COOK	26	6	
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60104					
	PLOT DATE = 12/11/2009	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



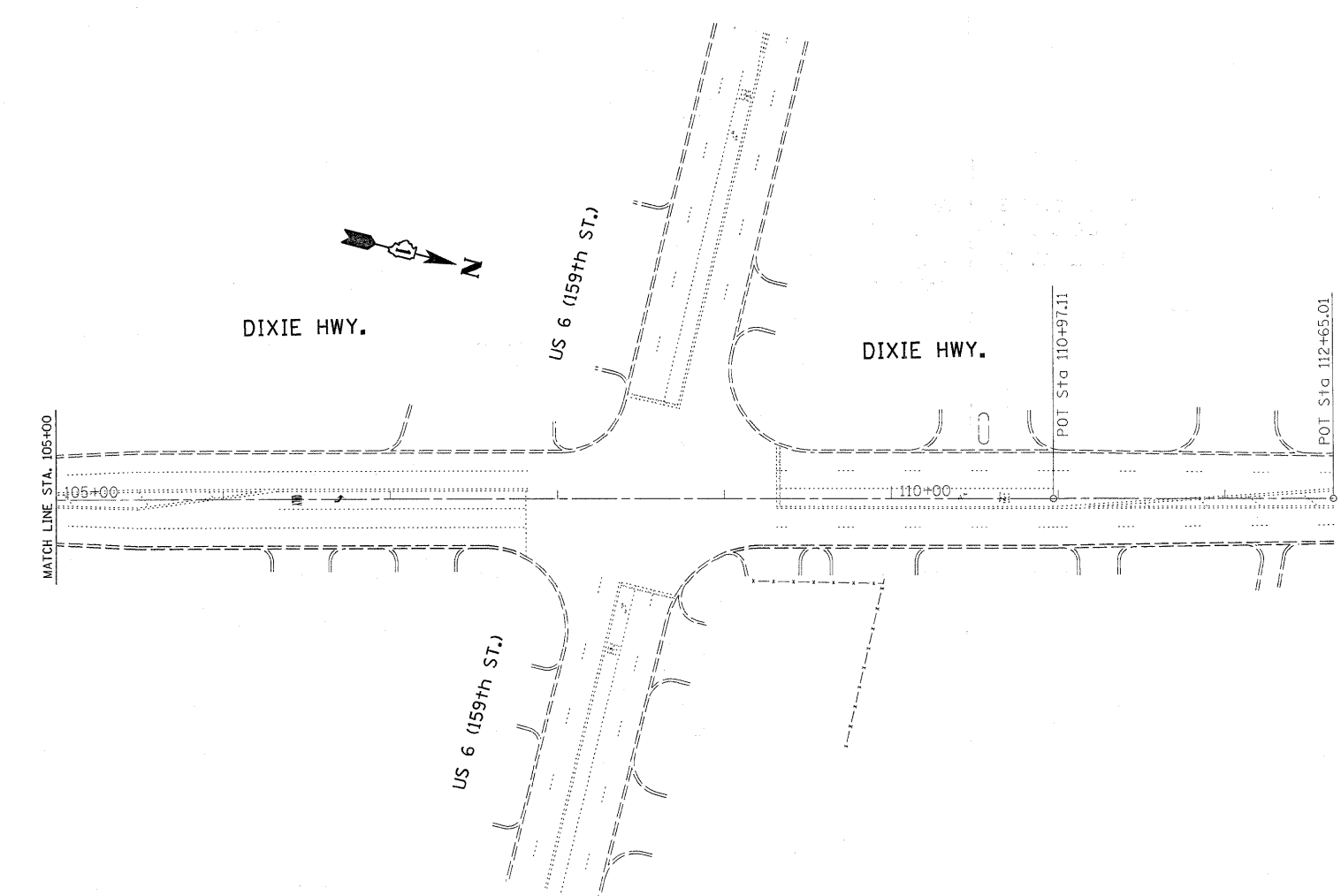
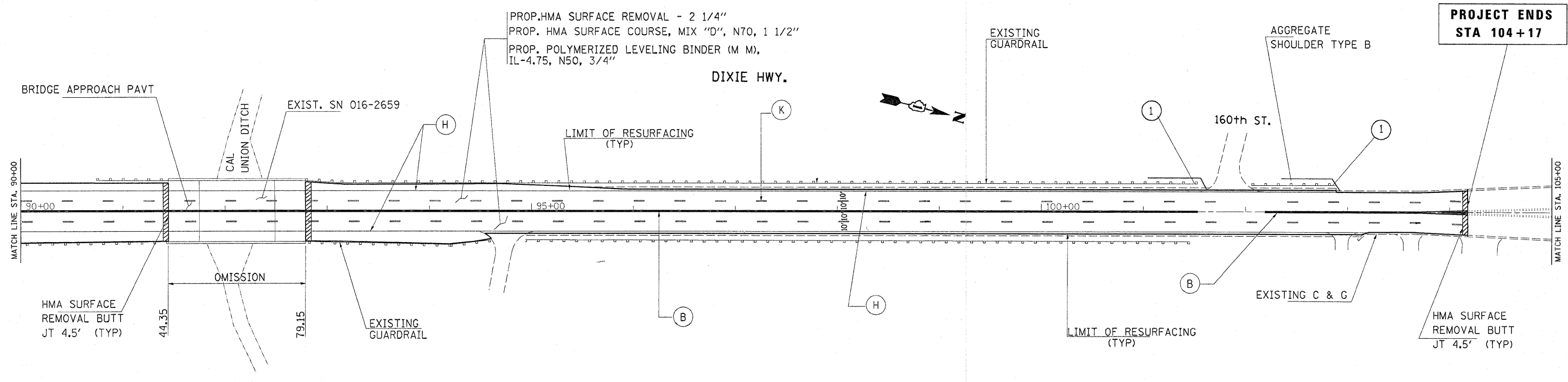
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6" WHITE - TURN LANE LINE (TYP)
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(LEFT TURN ONLY, RIGHT TURN ONLY)
- (D) PROP. THPL. PAVT MARKING LINE
24" SOLID WHITE - STOP LINES (TYP)
- (E) PROP. THPL. PAVT MARKING LINE
12" YELLOW - 45° DIAGONAL (MIN. OF 5)
- (H) PROP. THPL. PAVT MARKING LINE
4" WHITE - EDGE LINE (TYP)
- (I) PROP. THPL. PAVT MARKING LINE
6" WHITE - DOTTED LINE (TYP)
- (F) PROP. THPL. PAVT MARKING LINE
6" WHITE - CROSS WALK LINES (TYP)
- (K) PROP. THPL. PAVT MARKING LINE
4" WHITE - SKIP-DASH LINE (TYP)
- (L) PROP. THPL. PAVT MARKING LINE
4" SINGLE YELLOW (TYP)
- (M) PROP. THPL. PAVT MARKING LINE
12" WHITE - SCHOOL CROSSWALK LINES (TYP)

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DIXIE HWY (159th ST TO GOVERNORS HWY) ROADWAY PLANS	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\p\work\pwsdot\gorengautab\d2147544\184409-shr-plan.dgn		DRAWN -	REVISED -			2843	3248 RS-1	COOK	26	7
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60104				
PLOT DATE = 9/14/2009		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



EXIST. CURVE E. DIXIE-2
 PI STA. = 80+16.14
 $\Delta = 13^\circ 36' 50''$ (LT)
 $D = 2^\circ 02' 47''$
 $R = 2,800.00'$
 $T = 334.22'$
 $L = 665.30'$
 $E = 19.88'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 76+81.92$
 $P.T. STA. = 83+47.22$

FILE NAME =	USER NAME = gorangautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DIXIE HWY (159th ST TO GOVERNORS HWY) ROADWAY PLANS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.				
ct:\pwork\pwidot\gorangautab\d0147544\184489-sht-plan.dgn	PLDT SCALE = 50.0000' / IN.	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	2843	3248 RS-1	COOK	26	8
PLDT DATE = 9/14/2009	DATE -	CHECKED -	REVISED -						CONTRACT NO. 60104						
									ILLINOIS FED. AID PROJECT						



PROP. HMA SURFACE REMOVAL - 2 1/4"
 PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLYMERIZED LEVELING BINDER (M M),
 IL-4.75, N50, 3/4"

**PROJECT ENDS
 STA 104+17**

- (A) PROP. THPL. PAVT MARKING LINE
6" WHITE - TURN LANE LINE (TYP)
- (B) PROP. THPL. PAVT MARKING LINE
2 @ 4" YELLOW - 11" C-C (TYP)
- (C) 8' WHITE - LETTERS & SYMBOLS (TYP)
(LEFT TURN ONLY, RIGHT TURN ONLY)
- (D) PROP. THPL. PAVT MARKING LINE
24" SOLID WHITE - STOP LINES (TYP)
- (E) PROP. THPL. PAVT MARKING LINE
12" YELLOW - 45° DIAGONAL (MIN. OF 5)
- (H) PROP. THPL. PAVT MARKING LINE
4" WHITE - EDGE LINE (TYP)
- (I) PROP. THPL. PAVT MARKING LINE
6" WHITE - DOTTED LINE (TYP)
- (F) PROP. THPL. PAVT MARKING LINE
6" WHITE - CROSS WALK LINES (TYP)
- (K) PROP. THPL. PAVT MARKING LINE
4" WHITE - SKIP-DASH LINE (TYP)
- (L) PROP. THPL. PAVT MARKING LINE
4" SINGLE YELLOW (TYP)
- (M) PROP. THPL. PAVT MARKING LINE
12" WHITE - SCHOOL CROSSWALK LINES (TYP)

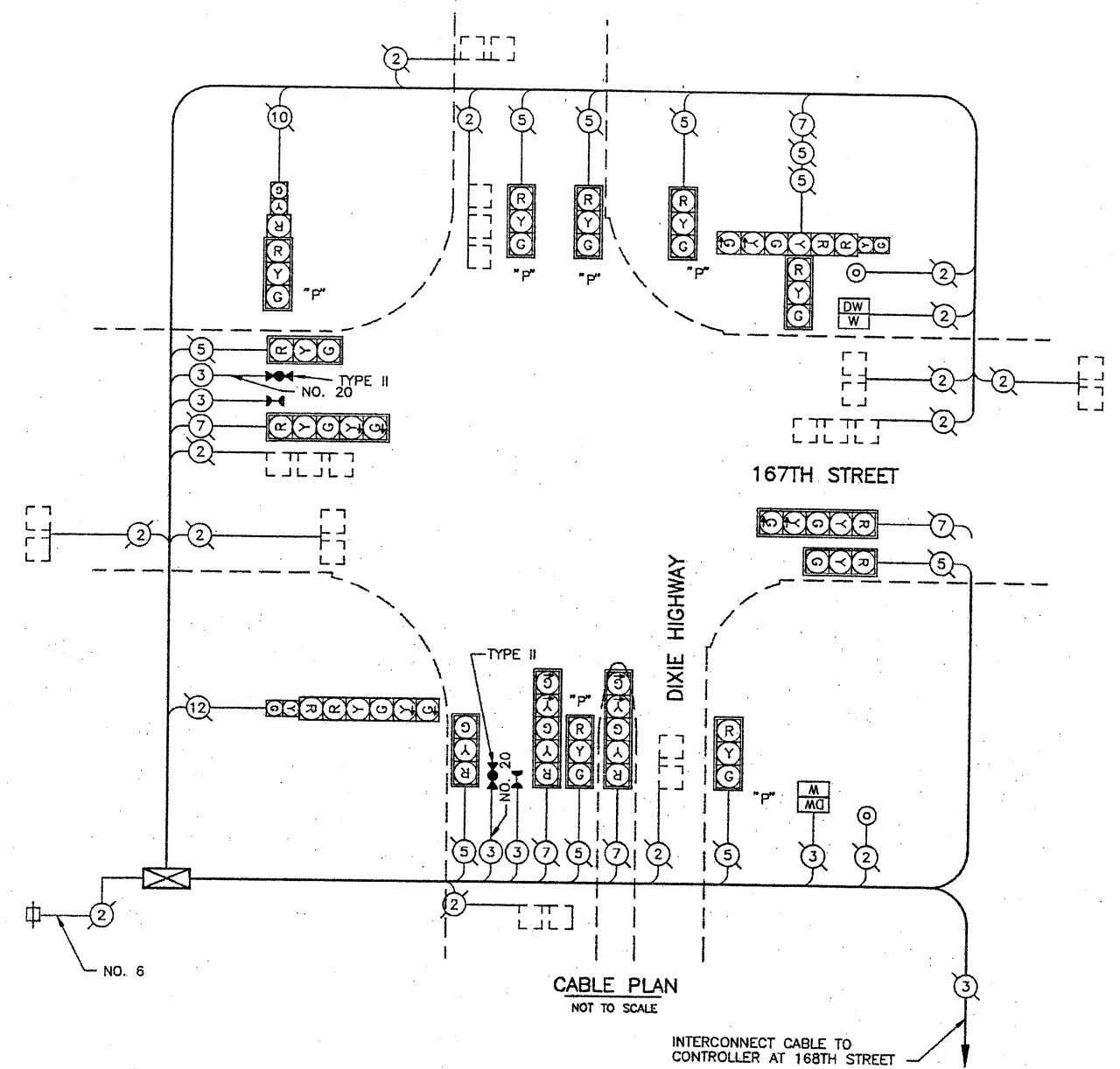
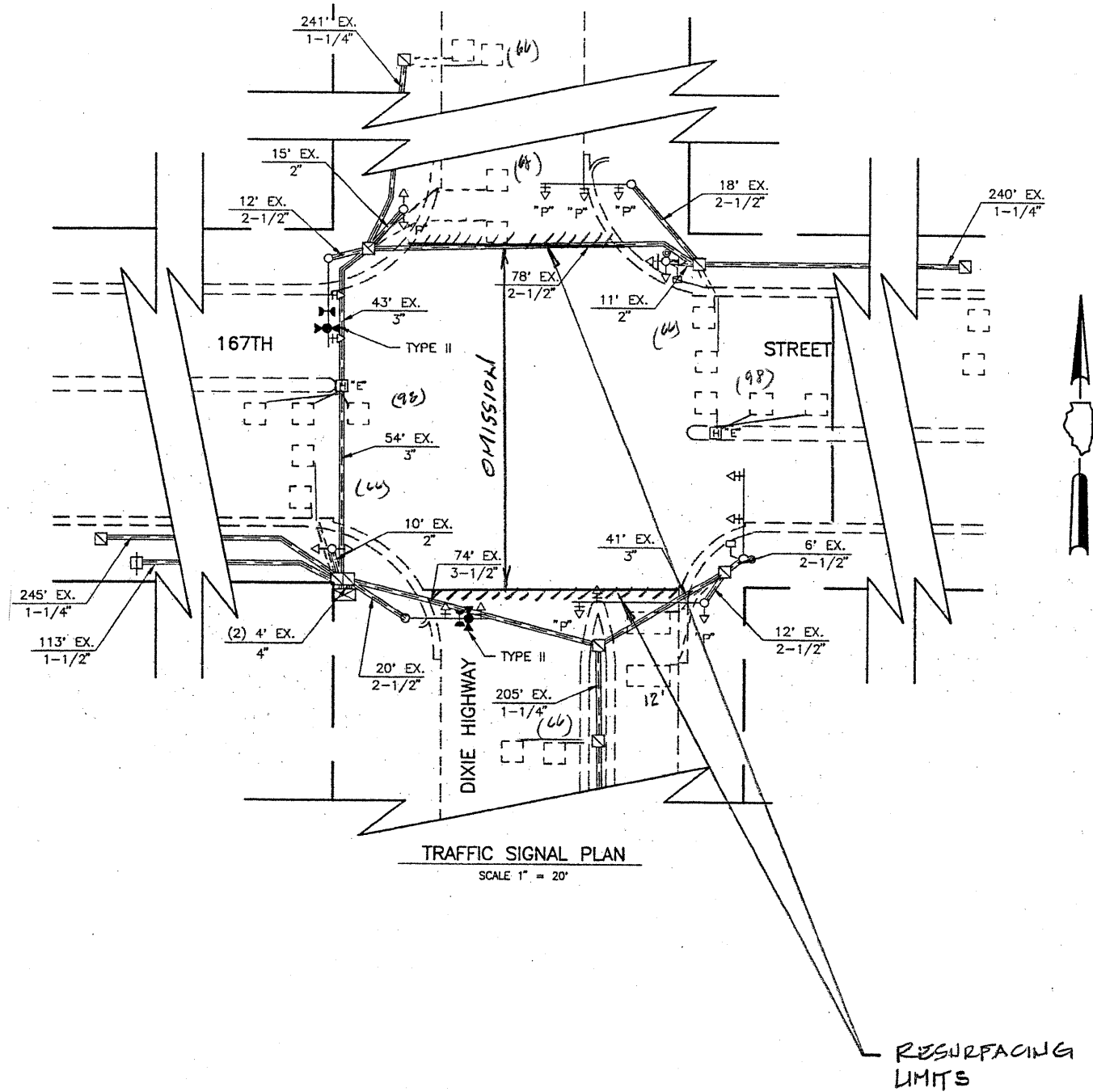
(1) EXIST. TERMINAL TO BE REPLACED
 W/NEW TYPE I (SPECIAL) - 50' LONG
 SEE STD 630301 & BD 34*

* SEE SHEET # 6 FOR ADDITIONAL NOTE

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DIXIE HWY (159th ST TO GOVERNORS HWY) ROADWAY PLANS				F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
ct:\pwork\pwork\dot\gorengautab\d0147544\	1184409-sh-t-plan.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	2843	3248 RS-1	COOK	26	9
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 60104										
	PLOT DATE = 9/14/2009	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

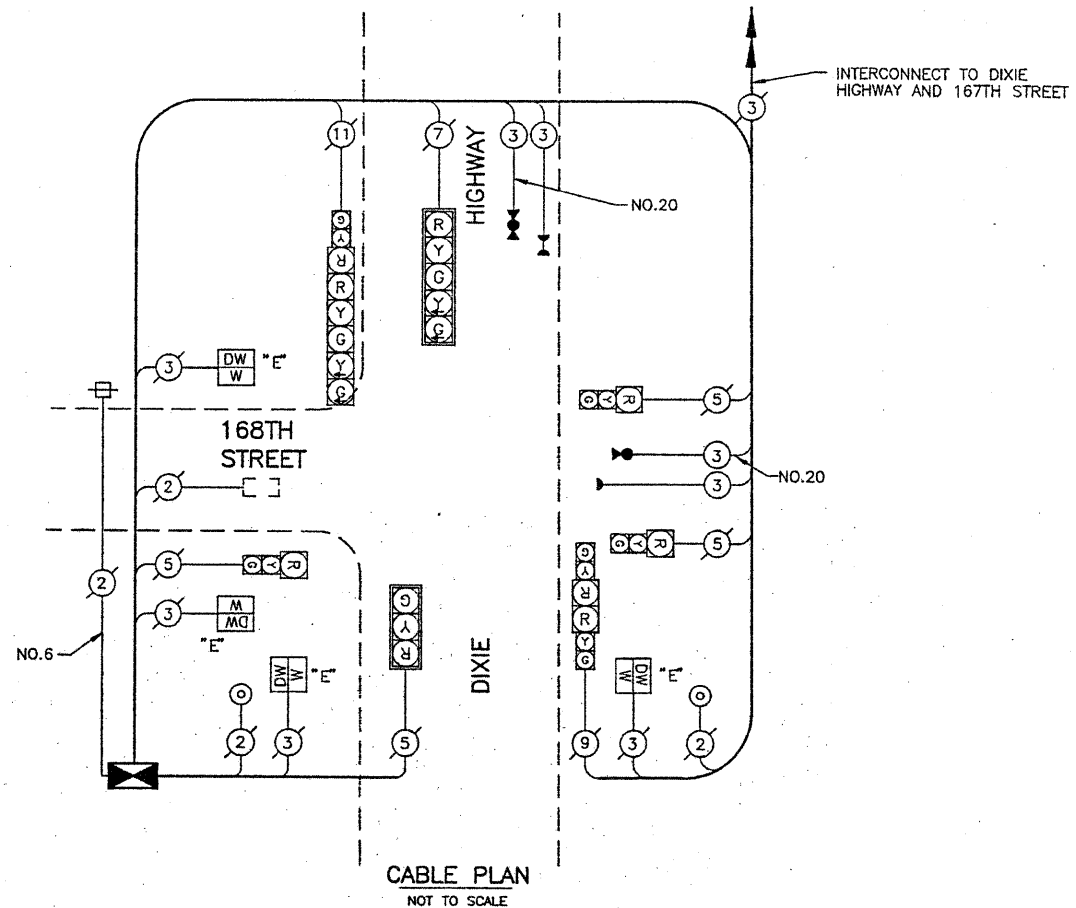
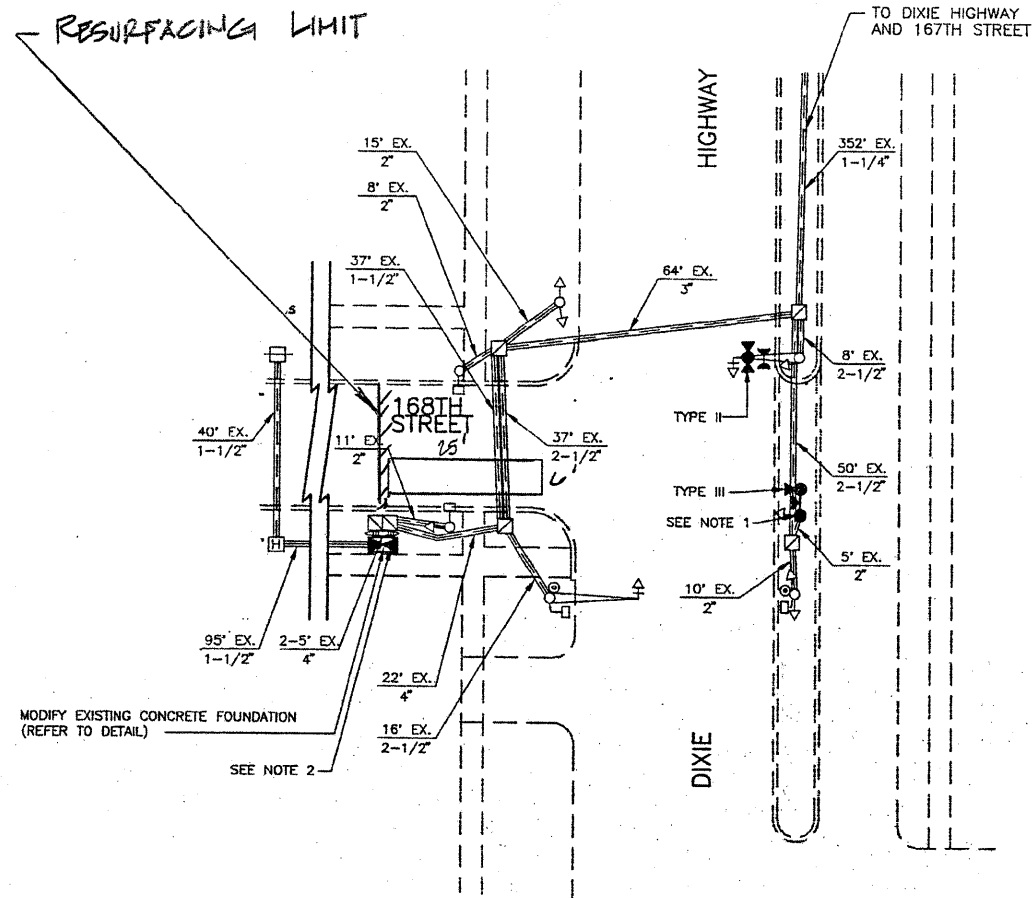
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	276	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kanthaphixaybo	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT DIXIE HIGHWAY @ 167TH STREET	F.A.U. RTE. 2843	SECTION 3248 RS-1	COUNTY COOK	TOTAL SHEETS 26	SHEET NO. 10		
CONTRACT NO. 60 I 04	PLLOT SCALE = 3/4" = 1'-0"	DRAWN - BCK	REVISED -			SCALE: NONE	SHEET NO. OF SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT				
	PLLOT DATE = 4/3/2009	CHECKED - DAD	REVISED -									
		DATE	REVISED -									

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	65	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kenthaphixaybc	DESIGNED - BCK	REVISED -
en:\pwork\VPW\DOT\KANTHAPHIXAYBC\d01126	4\traffic.legend.v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1'-0"	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
DIXIE HIGHWAY @ 168TH STREET

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2843	3248 RS-1	COOK	26	11
SCALE: NONE		SHEET NO. OF SHEETS		STA. TO STA.
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

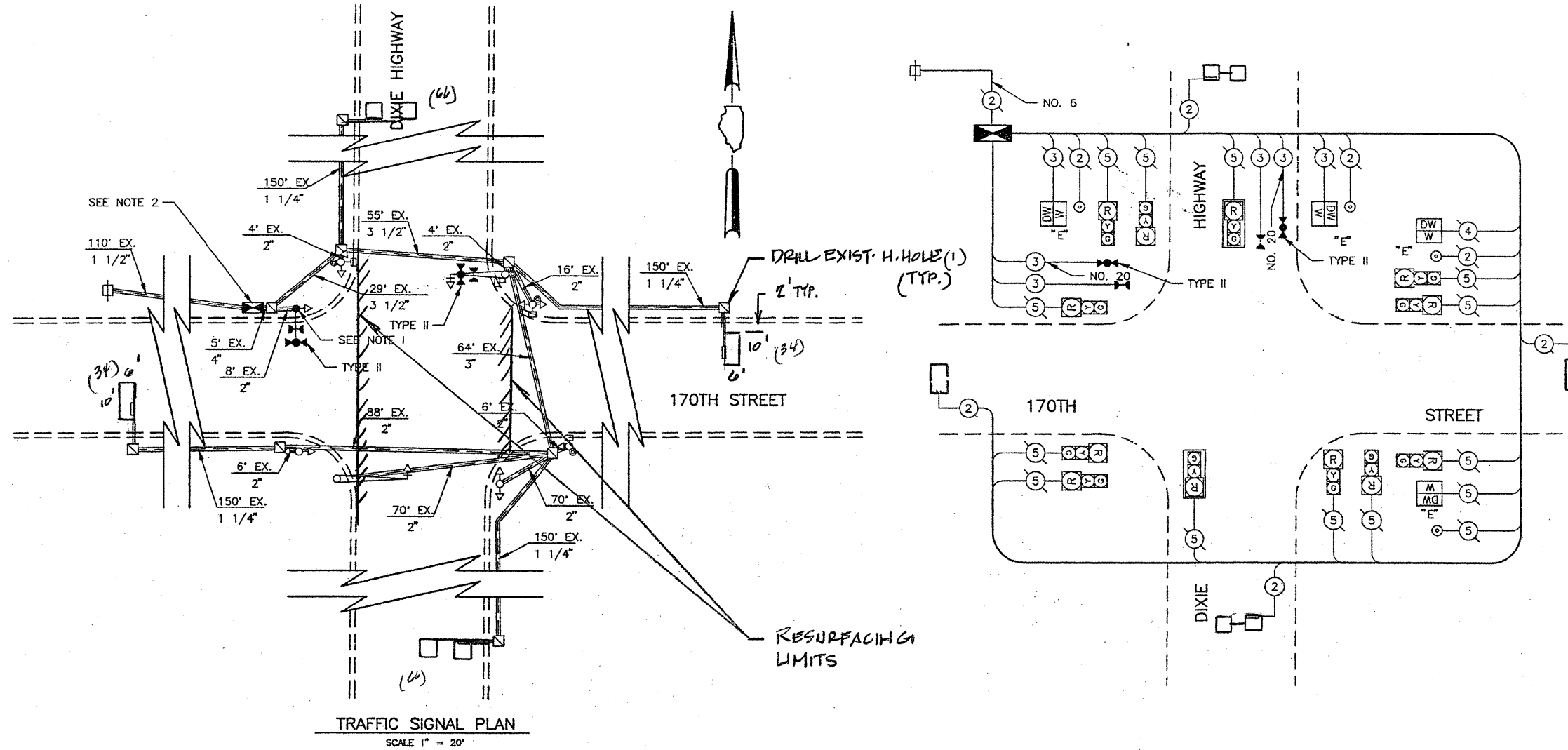
SCHEDULE OF QUANTITIES FOR MAGNETIC DETECTOR REPLACEMENTS
 (* When Quantities Below Are Shown)

CODE	QUANTITY	UNIT	ITEM
81900200	0	FOOT	Trench and Backfill for Electrical Work
85000200	1	EACH	Maintenance of Existing Traffic Signal Installation
87301305	1250	FOOT	Electric Cable in Conduit, Lead-In No. 14 1-Pair
87900200	4	EACH	Drill Existing Handhole
88500100	4	EACH	Inductive Loop Detector
88600100	68	FOOT	Detector Loop, Type 1
89502300	1100	FOOT	Remove Electric Cable from Conduit

All Magnetic Detectors will be replaced at a signalized intersection, even if only one magnetic detector is within the resurfacing limits. The "Pay Items" shown above will be paid as per the loop replacement specifications direct for "Magnetic Detectors" during roadway resurfacing projects.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	132	FOOT	DETECTOR LOOP, REPLACEMENT

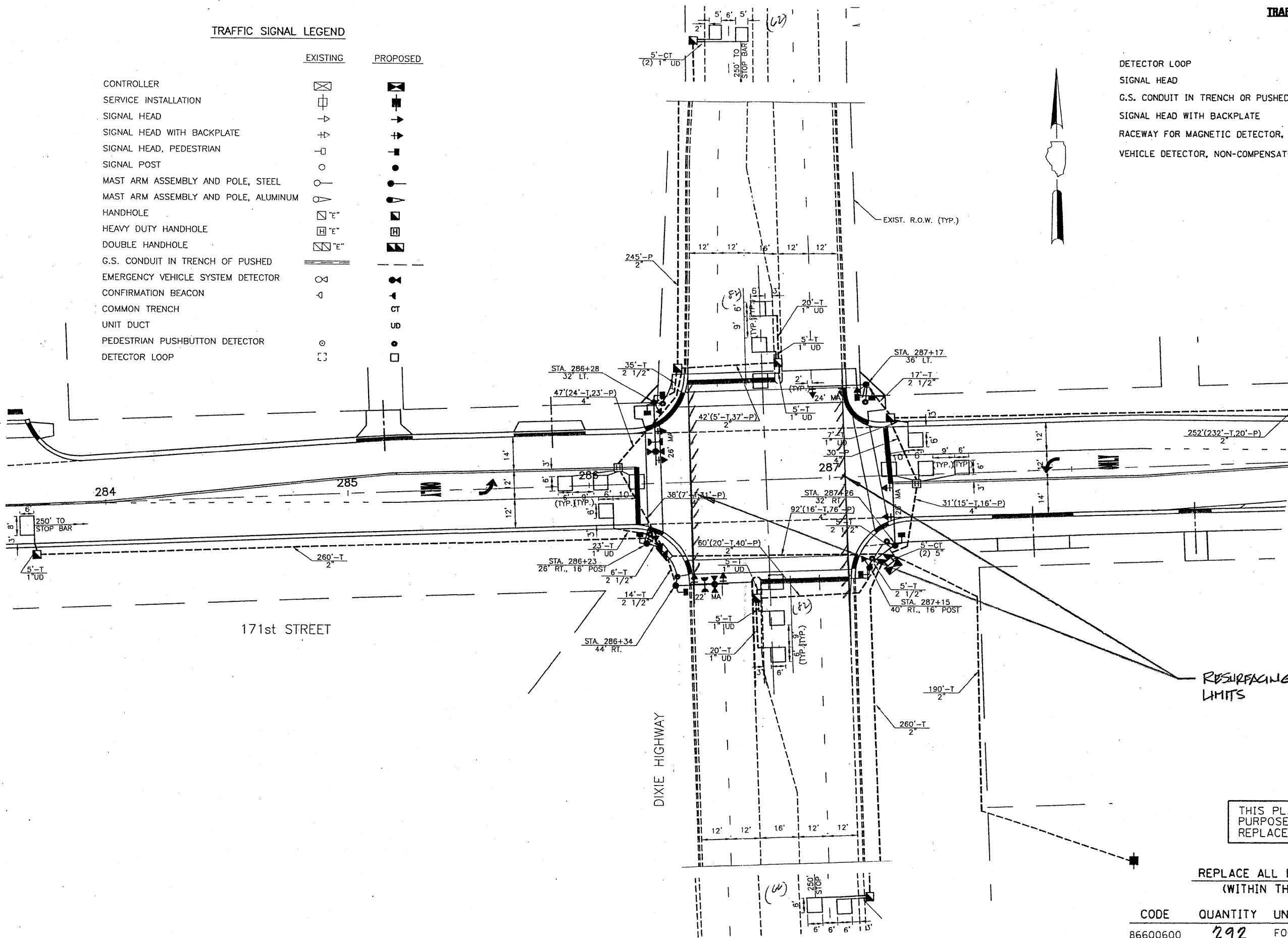
FILE NAME =	USER NAME = kanthaphixaybc	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT DIXIE HIGHWAY @ 170TH STREET	F.A.U. RTE. 2843	SECTION 3248 RS-1	COUNTY Cook	TOTAL SHEETS 25	SHEET NO. 12	
os:\pwork\pwork\KANTHAPHIXAYBC\d01126	4\traffic.legend.v7.dgn	DRAWN - BCK	REVISED -			SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60104
		CHECKED - DAD	REVISED -								
		DATE	REVISED -								

TRAFFIC SIGNAL LEGEND

	EXISTING	PROPOSED
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
MAST ARM ASSEMBLY AND POLE, ALUMINUM		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OF PUSHED		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
COMMON TRENCH		
UNIT DUCT		
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP		

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

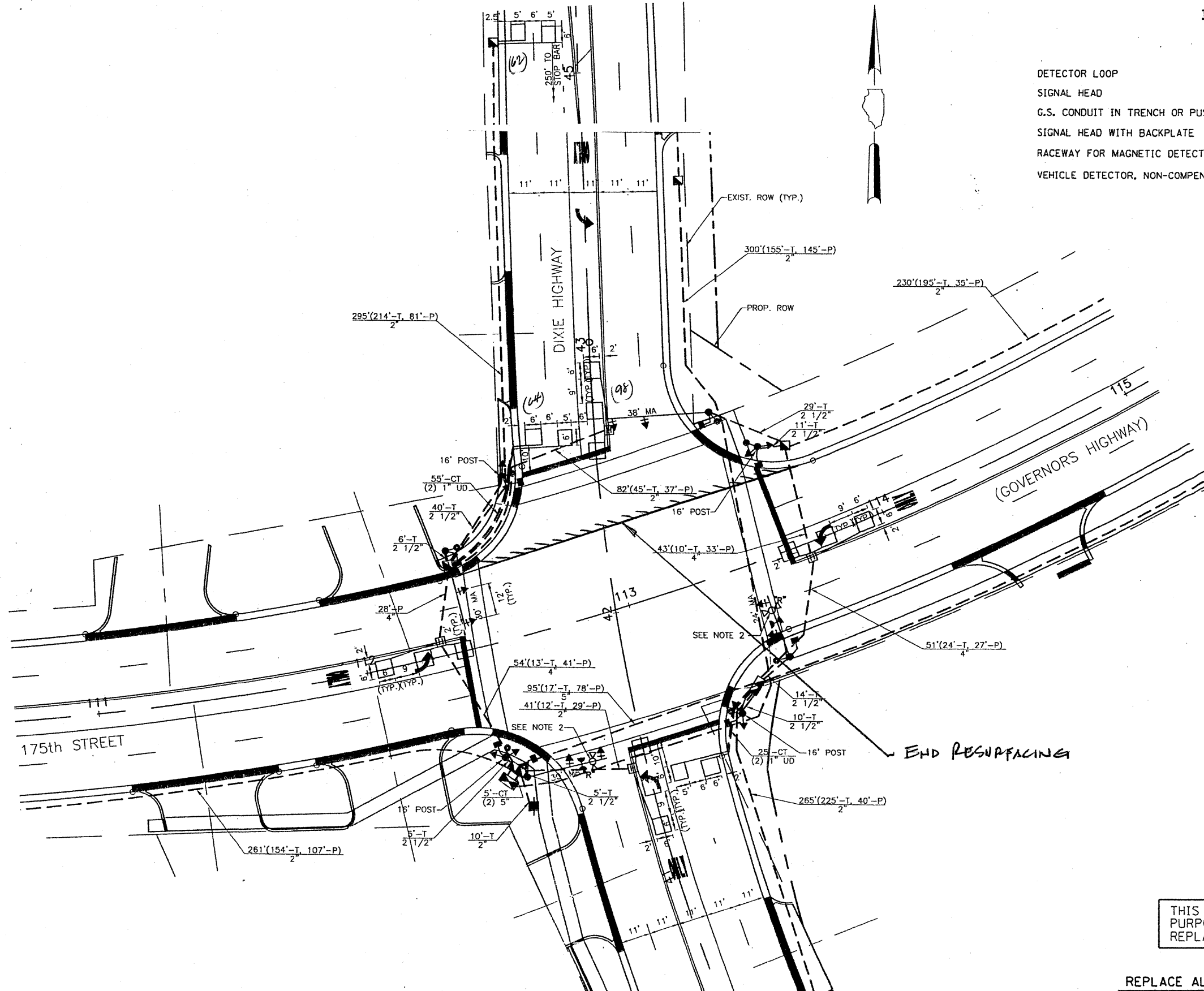
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	292	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME = USER NAME = kanthaphixaybo DESIGNED - BCK DRAWN - BCK PLOT SCALE = 3/4" = 1' IN. PLOT DATE = 4/3/2009	REVISIONS REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT DIXIE HIGHWAY @ 171st STREET	F.A.U. RTE. 2843 SECTION 3248 COUNTY COOK TOTAL SHEETS 26 SHEET NO. 13 CONTRACT NO. 60104
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TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	224	FOOT	DETECTOR LOOP, REPLACEMENT

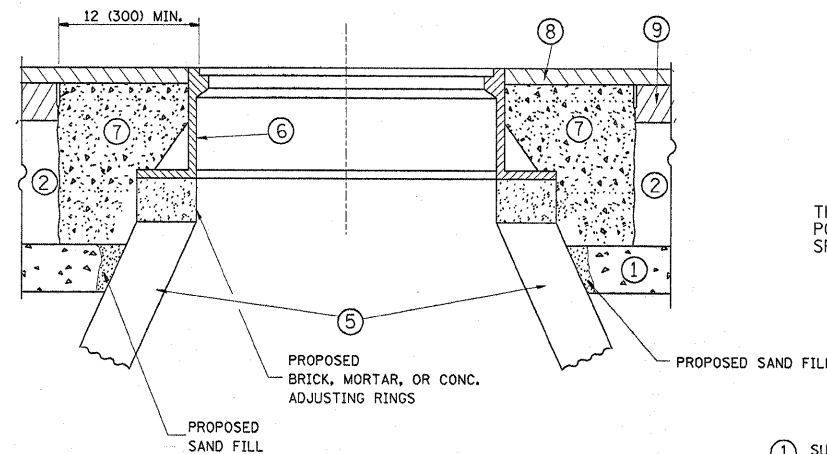
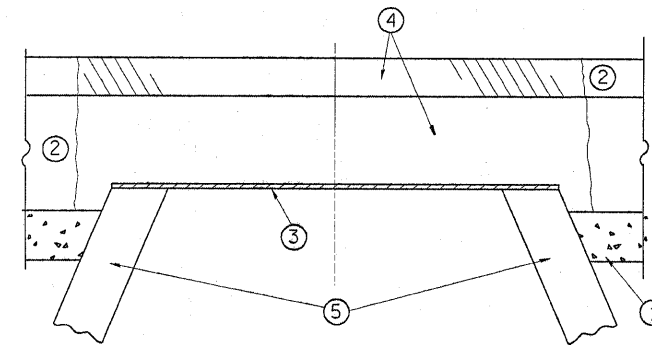
FILE NAME =	USER NAME = kanthapixaybc	DESIGNED - BCK	REVISED -
at:\pwork\p\1001\KANTHAPIXAYBC\d01126	11\traffic.legend_v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
DIXIE HIGHWAY @ GOVERNORS HIGHWAY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2843	3248 RS-1	COOK	26	14
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 6D104	

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

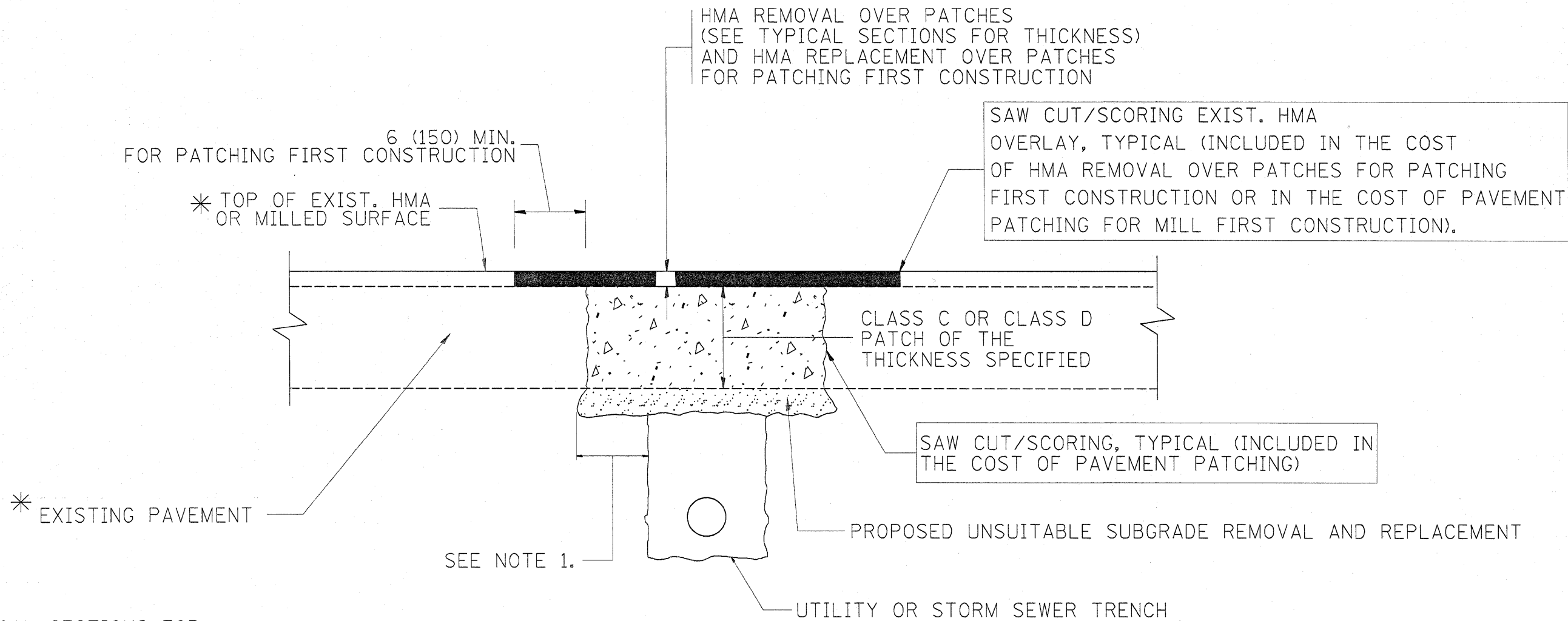
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gorengautab	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\PIW001\GORENGAUTAB\0147544	01stStd.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		2843	3248 RS-1	COOK	26	15			
PLOT SCALE = 50.0000' / 1" IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04			BD600-03 (BD-8)			CONTRACT NO. 60104				
PLOT DATE = 12/11/2009	DATE - 10-25-94	REVISED - R. BORO 01-01-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pwwork\PIWIDOT\GORENGAUTAB\0147544	DistStd.dgn	DRAWN -	REVISED - R. BORO 01-01-07		2843	3248 RS-1	COOK	26	16			
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08		BD400-04 (BD-22)			CONTRACT NO. 60104				
PLOT DATE = 12/11/2009	DATE - 10-25-94				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

18" (450) MAX.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

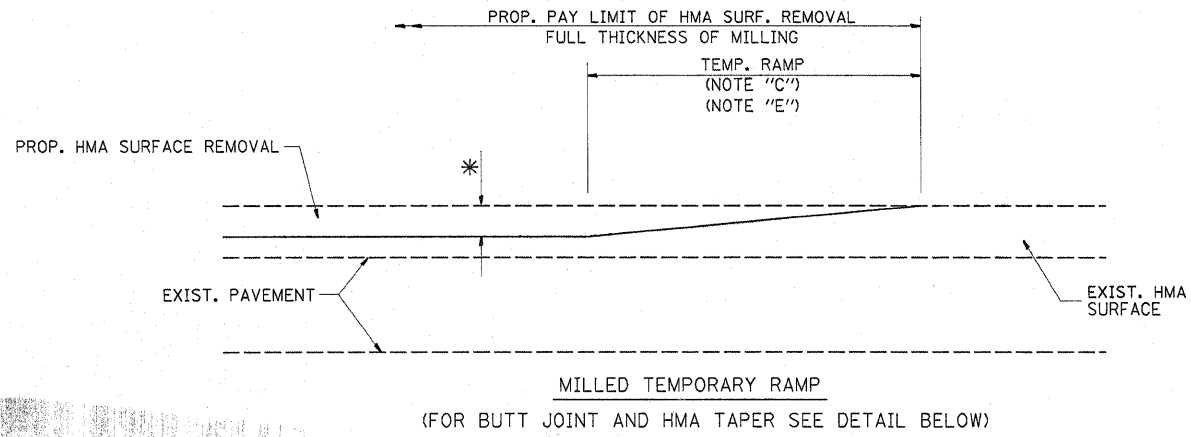
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

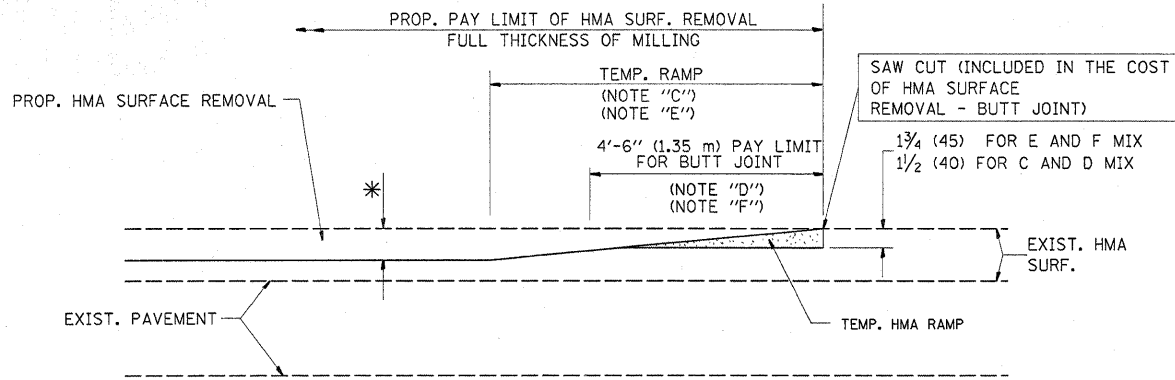
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengoutab	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 50.0000 ' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)			CONTRACT NO. 60104				
PLOT DATE = 12/16/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



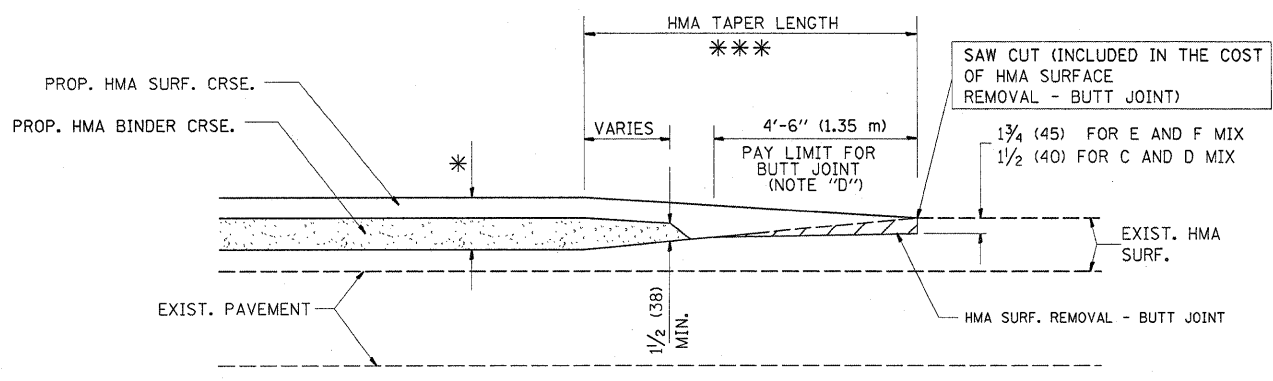
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

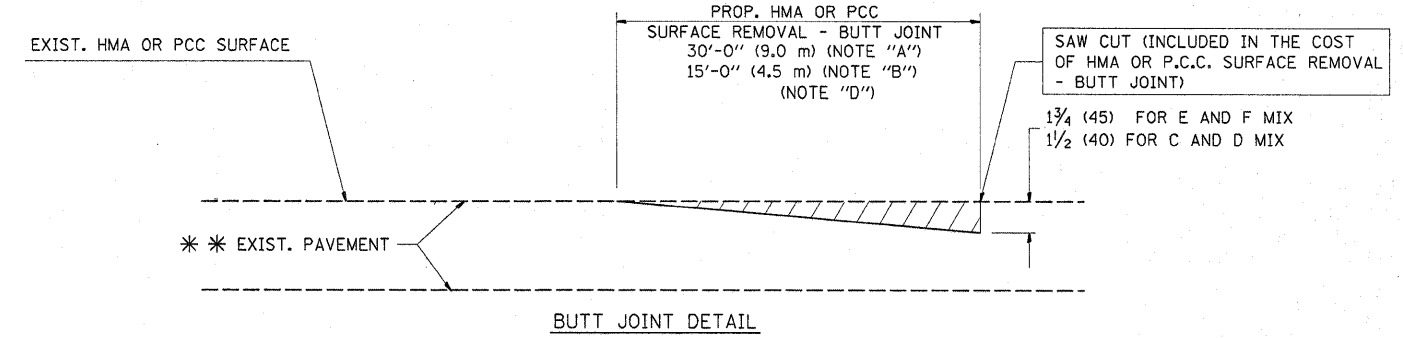


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

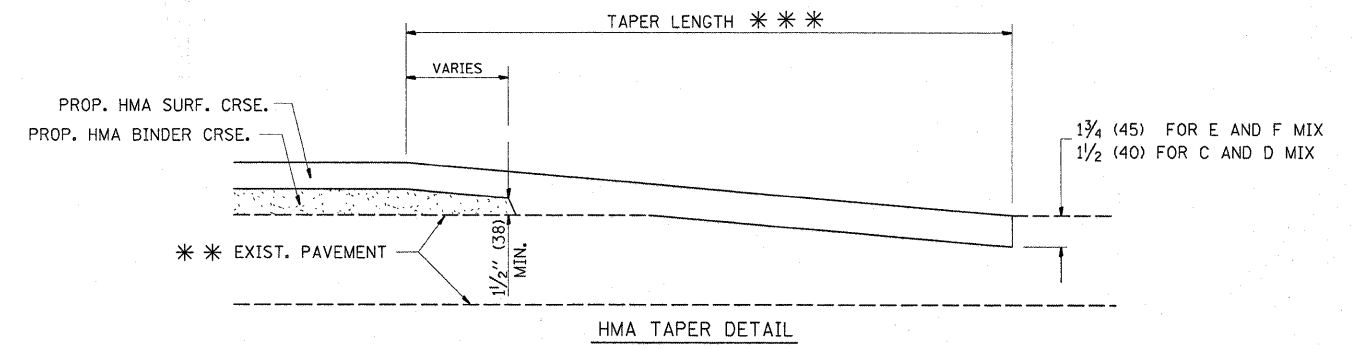
OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

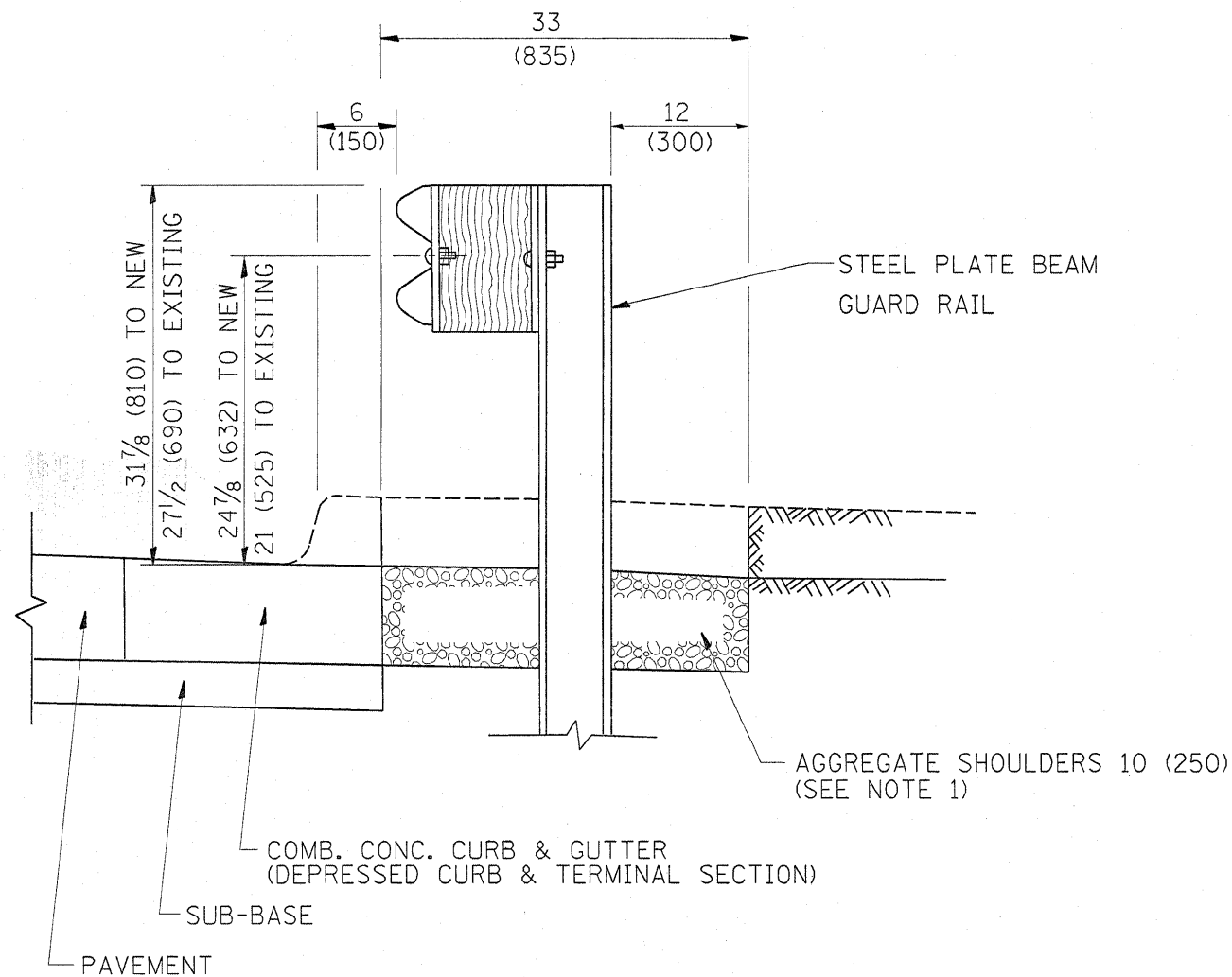
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

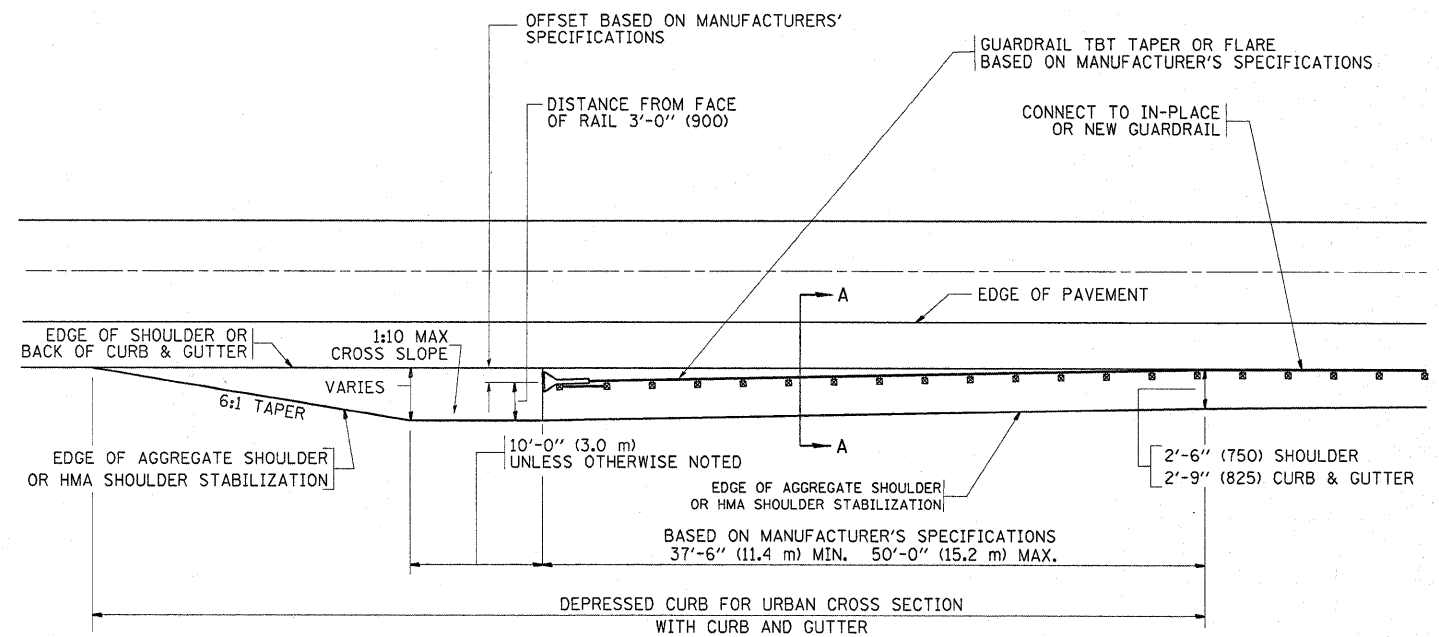
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es:\pwwork\pwwid\GORENGAUTAB\147544	DistStd.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		2843	3248 RS-1	COOK	26	18			
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISOR - M. GOMEZ 04-06-01	REVISED - R. BORO 01-01-07		BD400-05 BD32			CONTRACT NO. 60104				
PLOT DATE = 12/11/2009	DATE - 06-13-90				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.					



SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
 2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
 3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
 [FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
 ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

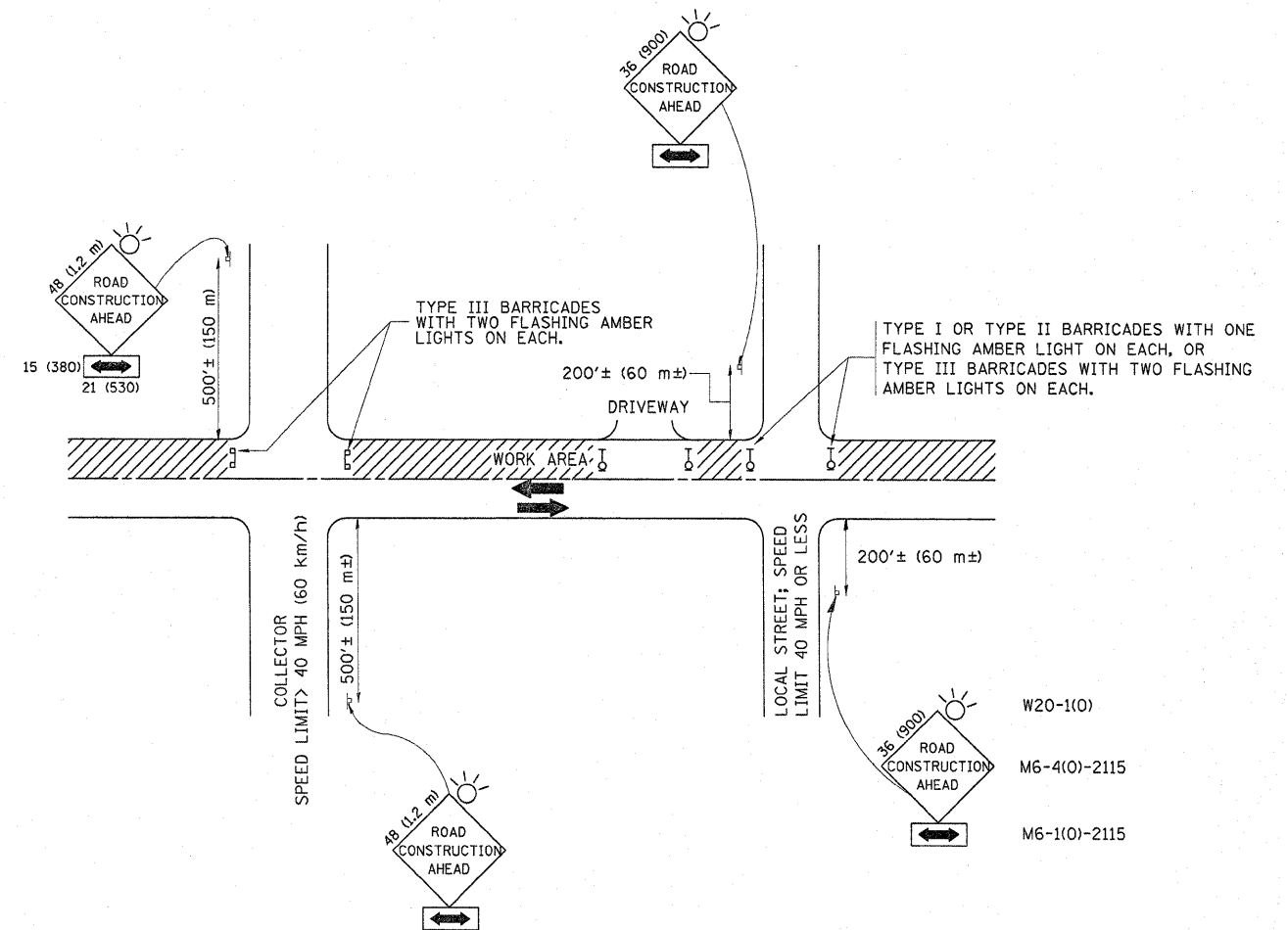
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 12-08-2008
	PLOT DATE = 12/11/2009	DATE - 09-22-90	REVISED - R. BORO 09-14-2009

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DETAILS FOR DEPRESSED CURB & GUTTER AND
 SHOULDER TREATMENT AT TBT TY 1 SPL.

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2843	3248 RS-1	COOK	26	19
BD600-10 (BD 34)		CONTRACT NO. 60104		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

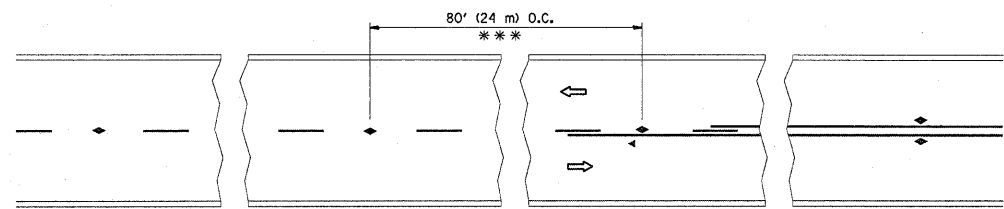
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	PLOT SCALE = 50.00000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 12/11/2009	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

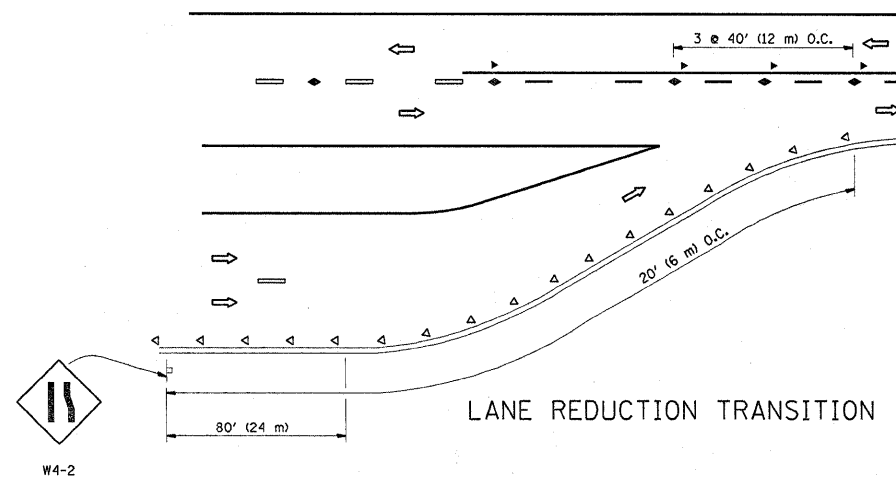
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60104	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

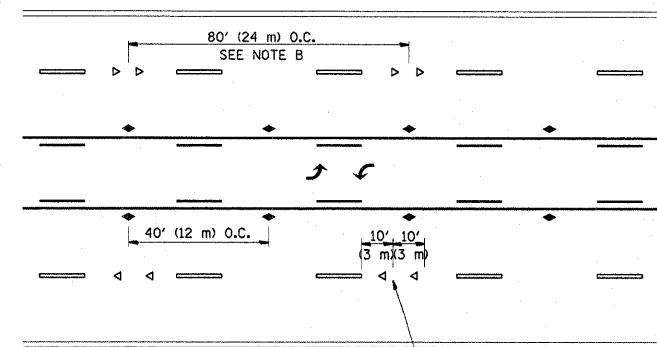


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

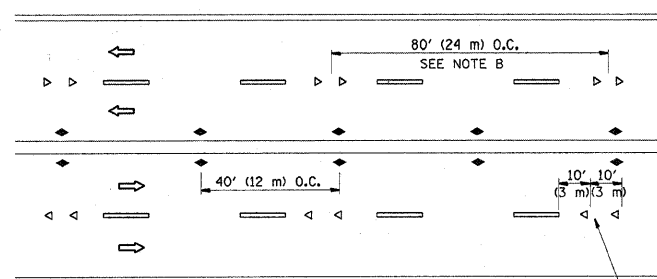


LANE REDUCTION TRANSITION



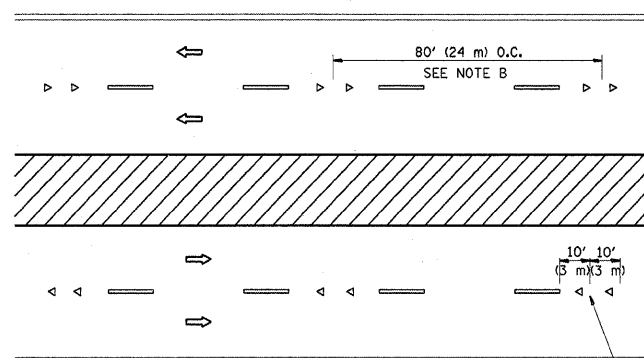
SEE NOTE A

TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

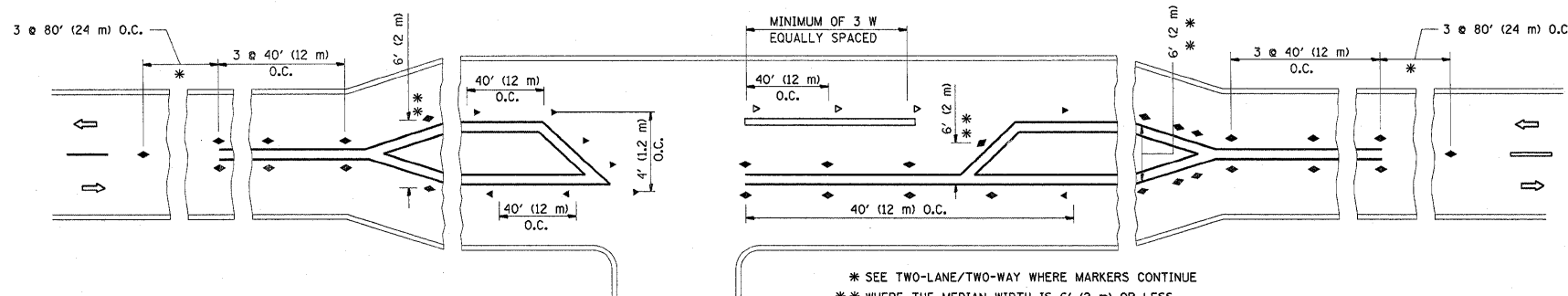
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

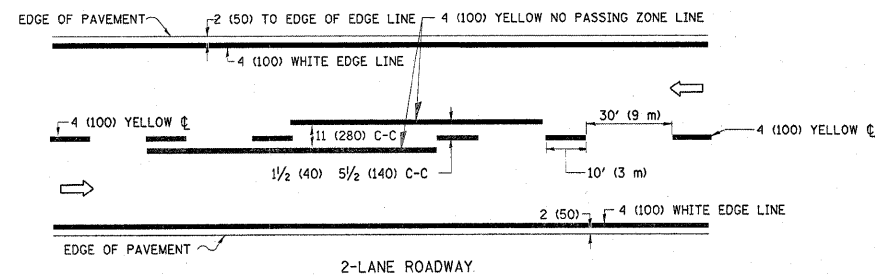


* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

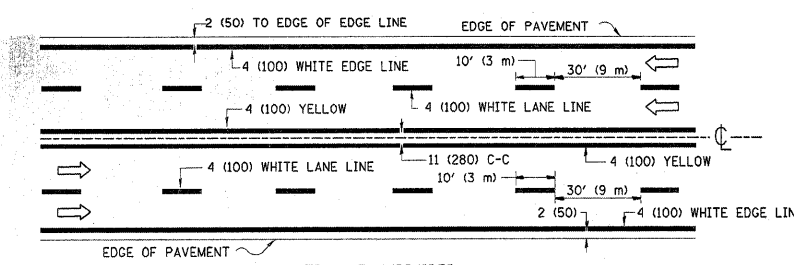
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

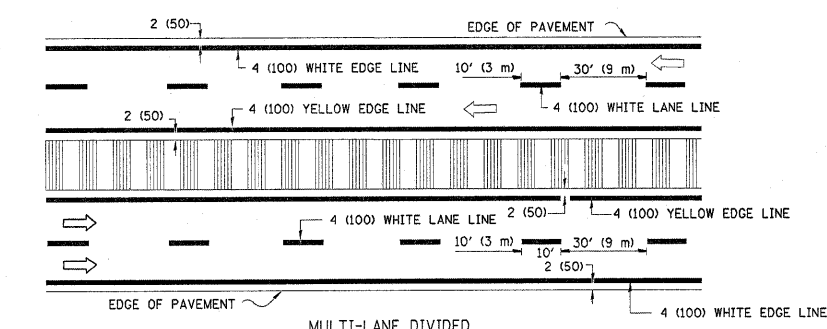
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	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		TC-11			CONTRACT NO. 60104				
	PLOT DATE = 12/11/2009	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



2-LANE ROADWAY



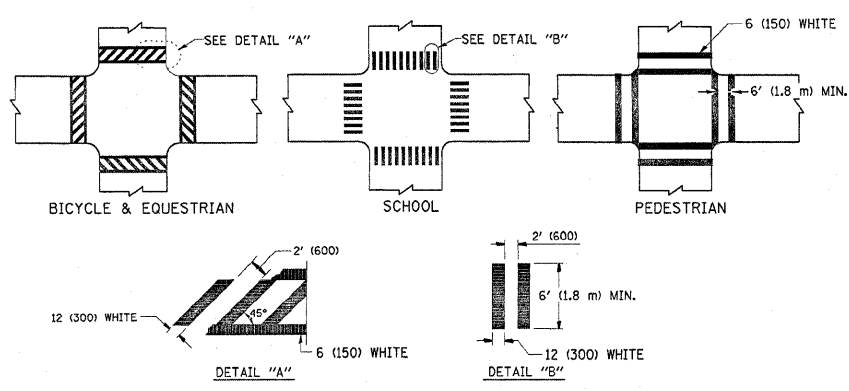
MULTI-LANE UNDIVIDED



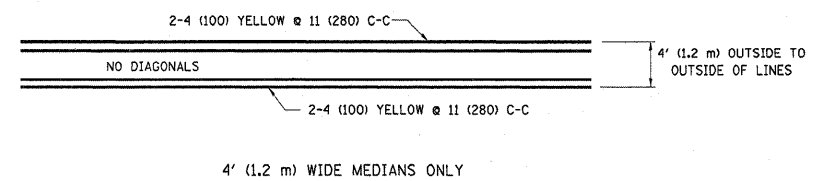
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

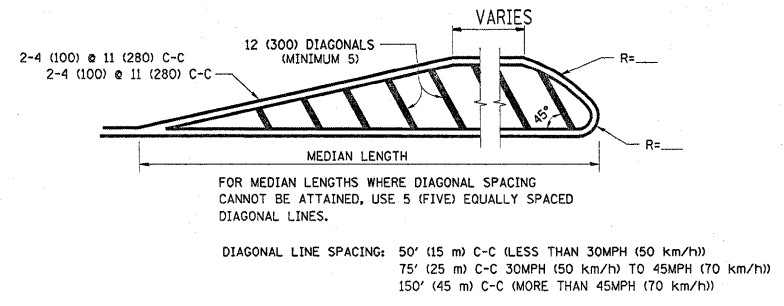
TYPICAL LANE AND EDGE LINE MARKING



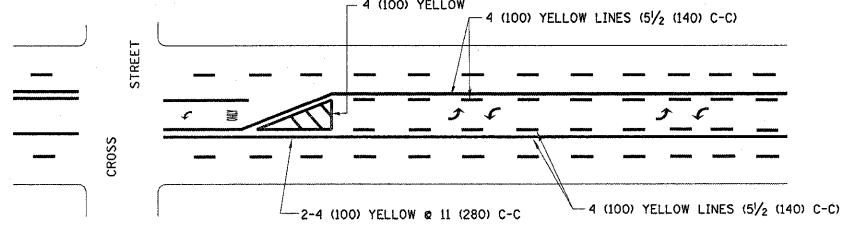
TYPICAL CROSSWALK MARKING



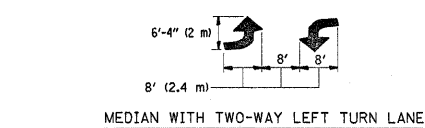
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

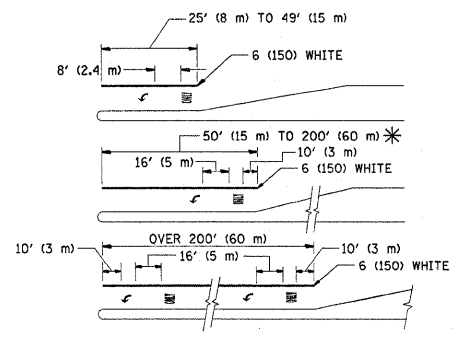


TYPICAL PAINTED MEDIAN MARKING

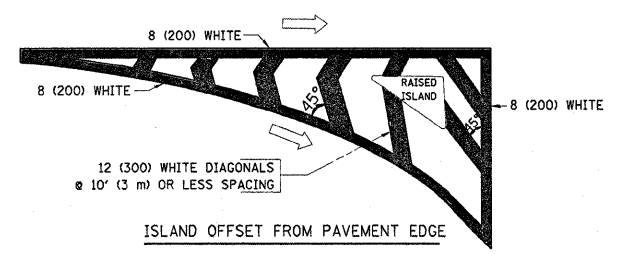


MEDIAN WITH TWO-WAY LEFT TURN LANE

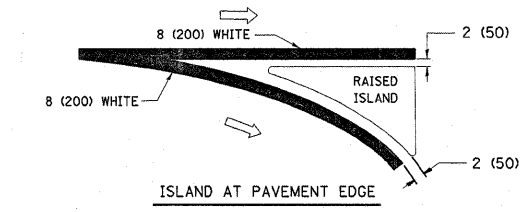
TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



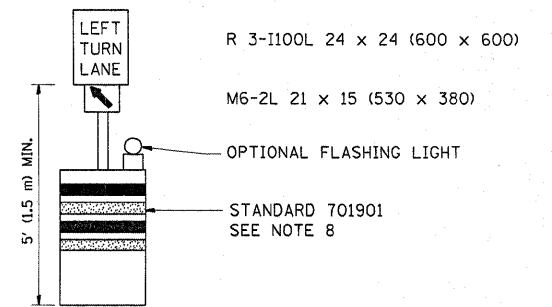
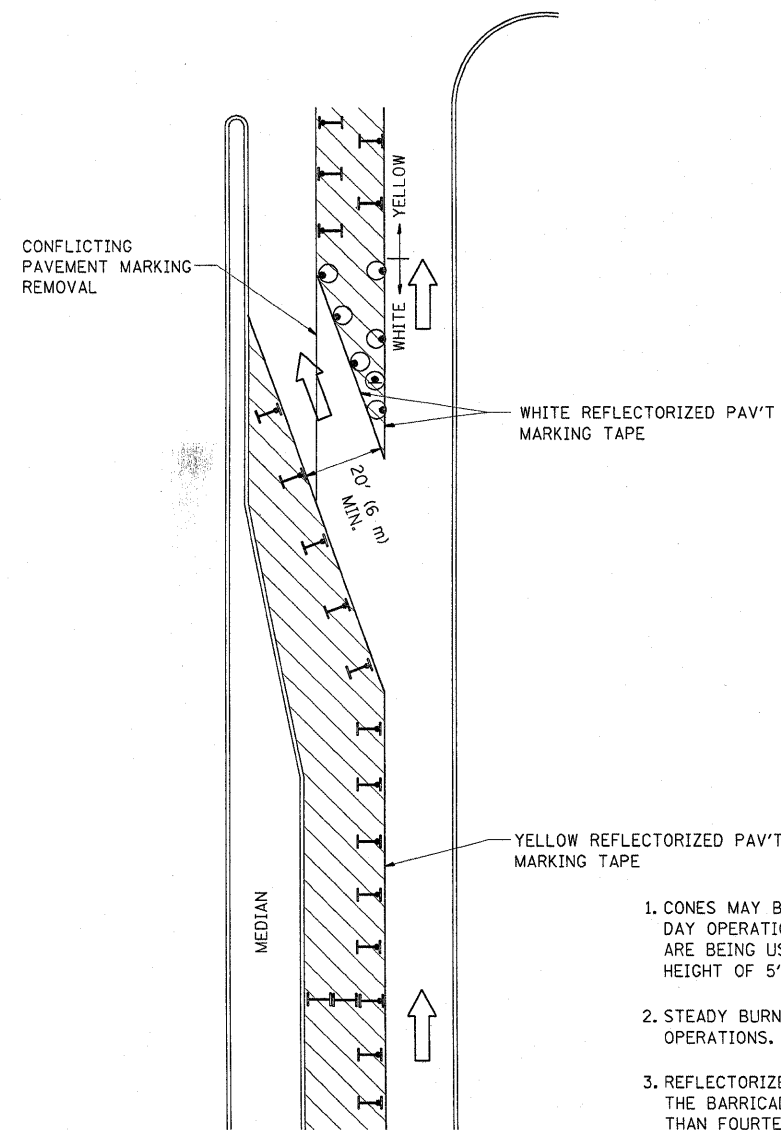
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

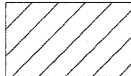







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

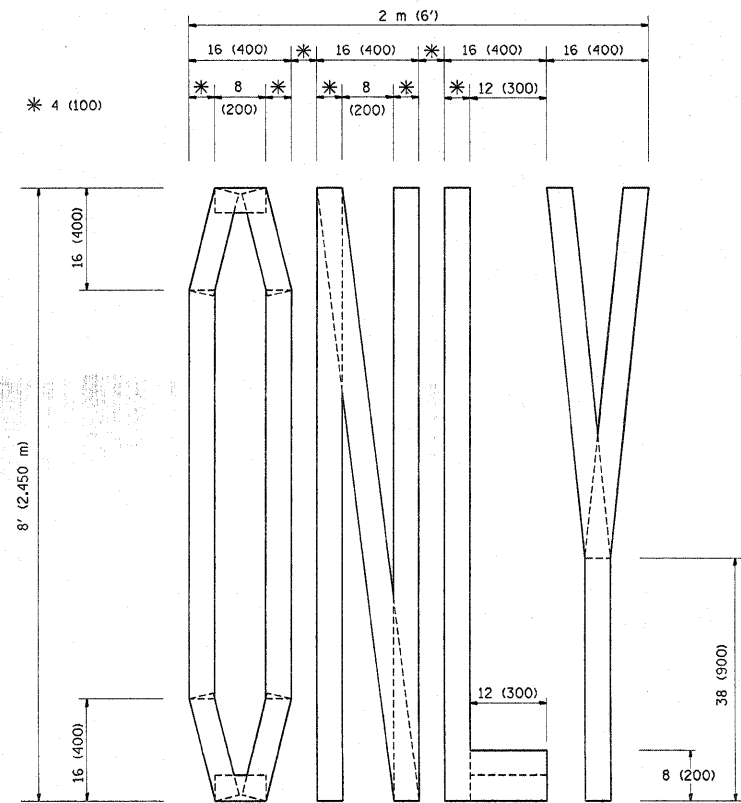
FILE NAME =	USER NAME = gorengautab	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
ct:\pwwork\pwwid\GORENGAUTAB\d8147544	D:\st\dgn	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 50,0000' / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 12/11/2009	REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

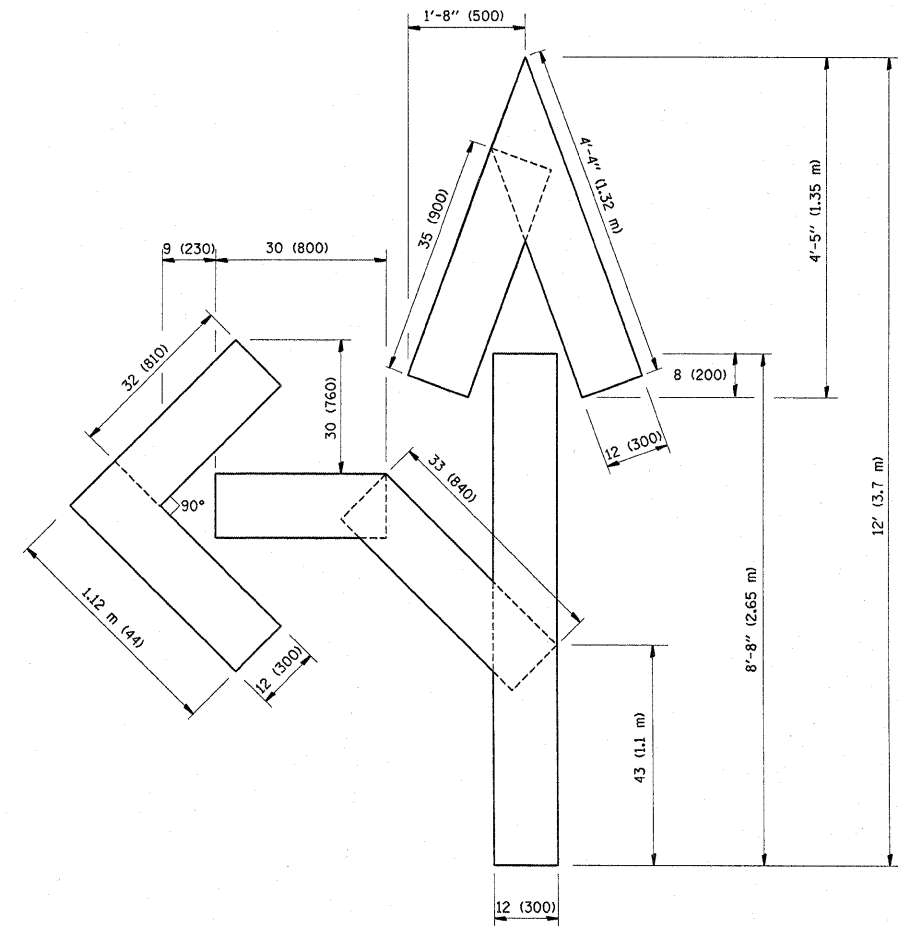
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

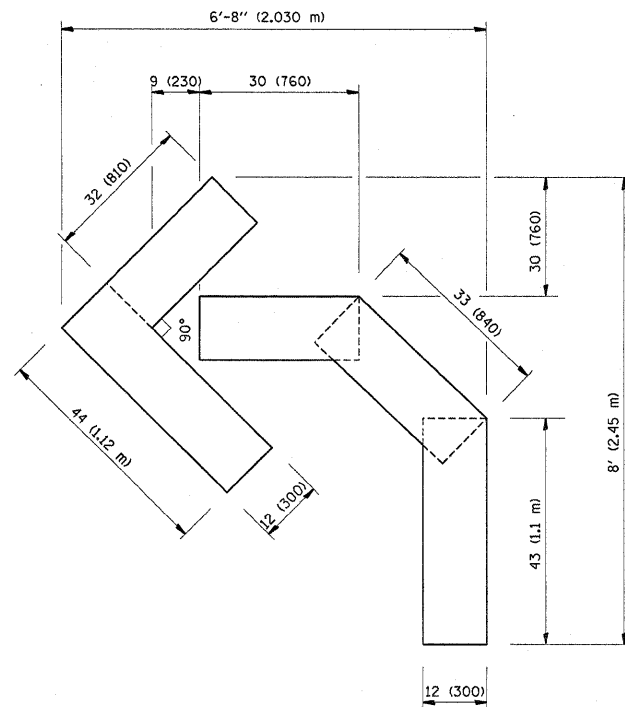
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2843	3248 RS-1	COOK	26	23
TC-14		CONTRACT NO. 60104		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

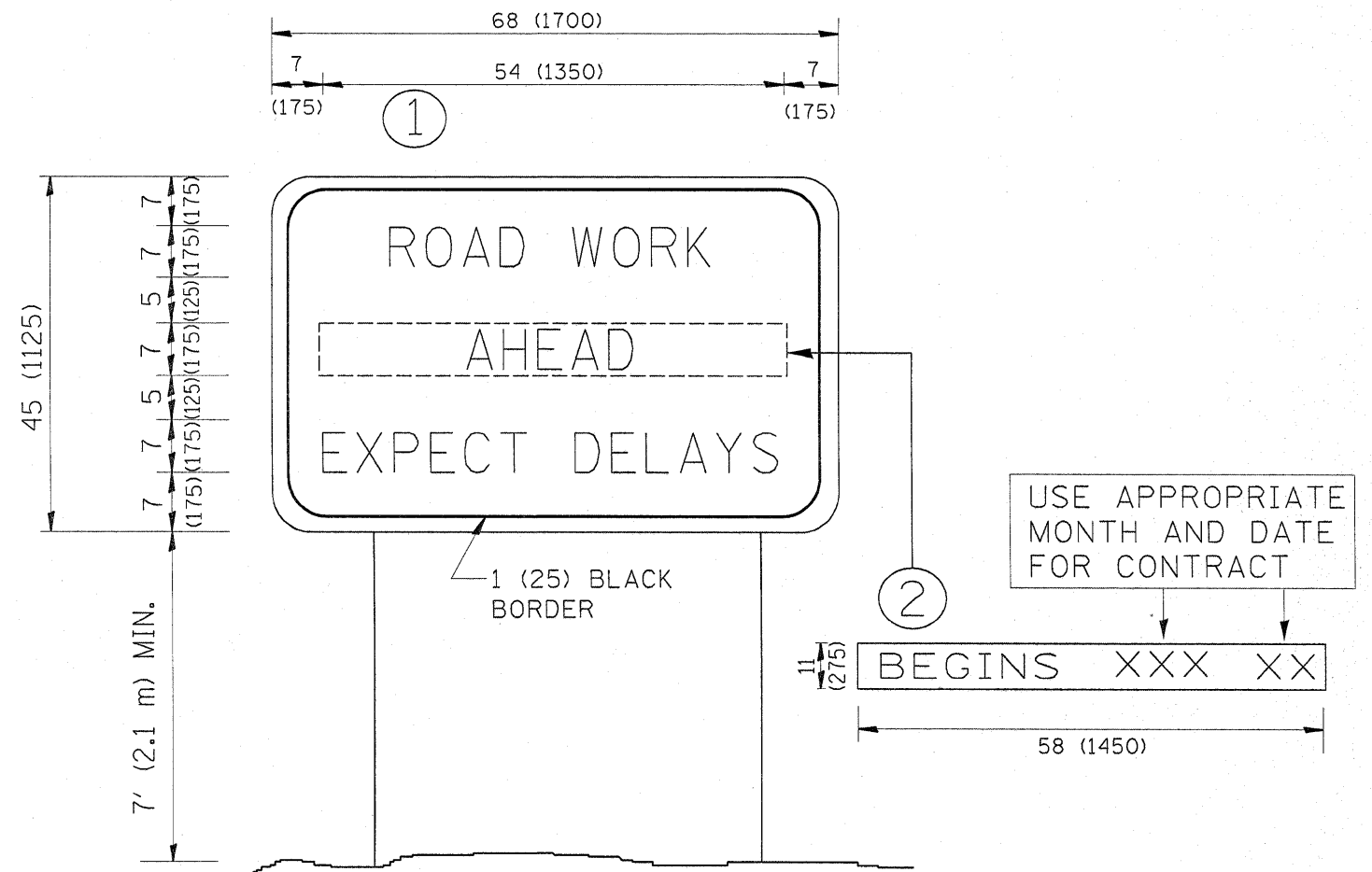
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
c:\pwork\pwwid\GORENGAUTAB\d0147544	DistStd.dgn	DRAWN -	REVISED -T. RAMMACHER 11-04-97
PLOT SCALE = 50,0000 "/ IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	
PLOT DATE = 12/11/2009	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2843	3248 RS-1	COOK	26	24
TC-16			CONTRACT NO. 60104	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

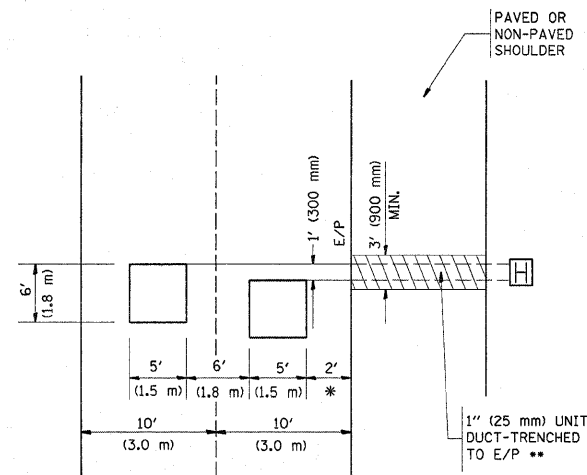
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
o:\pwork\PWIDOT\GORENGAUTAB\d0147544	DistStd.dgn	DRAWN -	REVISED - R. MIRS 12-11-97		2843	3248 RS-1	COOK	26	25		
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99		TC-22		CONTRACT NO. 60104				
	PLOT DATE = 12/11/2009	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

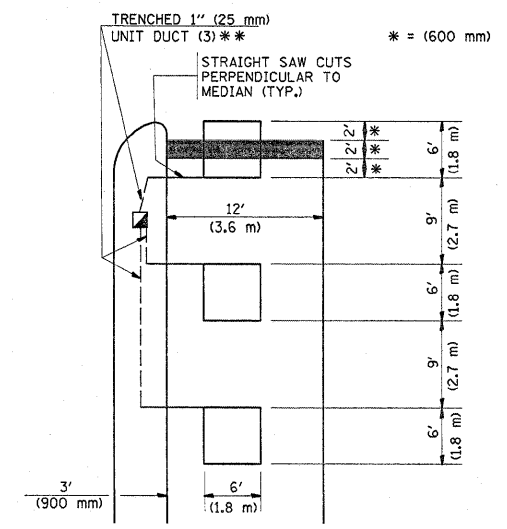


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

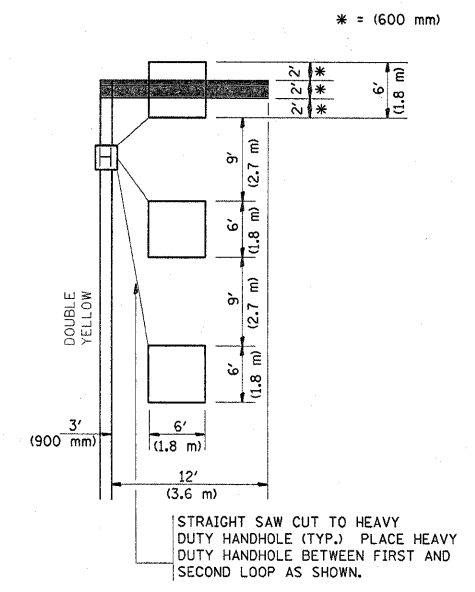


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



* = (600 mm)

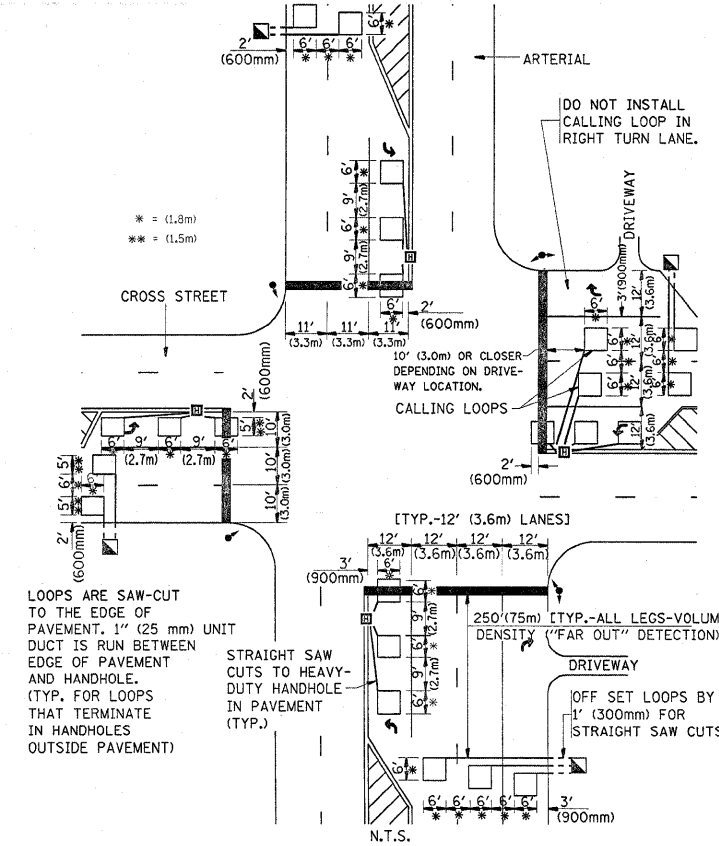
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

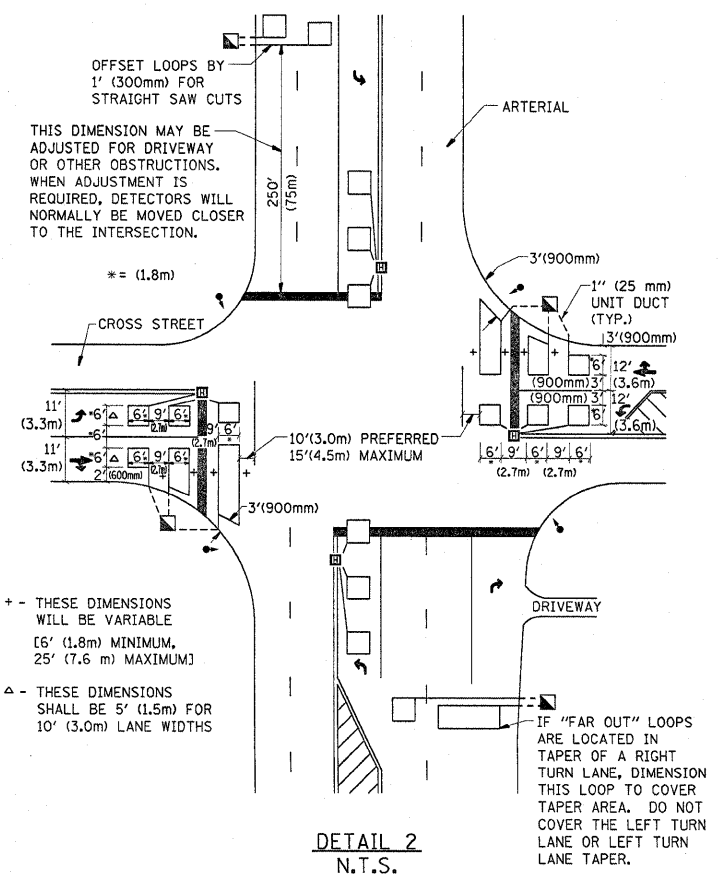
**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = garengautob	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwwork\PMIDOT\GORENGAUTAB\d0147544	DistStd.dgn	DRAWN -	REVISED -			2843	3248 RS-1	COOK	26	26
PLOT SCALE = 50.0000' / IN.		CHECKED - R.K.F.	REVISED -			TS-07				
PLOT DATE = 12/11/2009		DATE -	REVISED -			CONTRACT NO. 60104				
SCALE: NONE						SHEET NO. 1 OF 1 SHEETS		STA.		TO STA.
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				