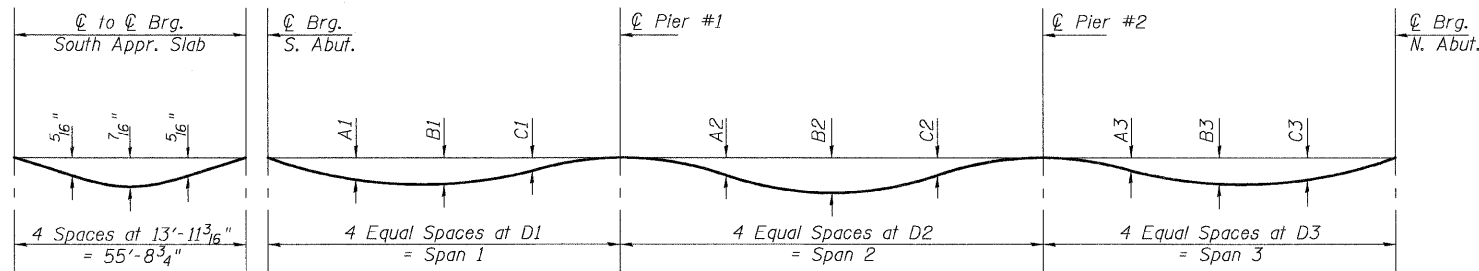


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ROUTE NO.	SECTION	COUNTY	PROJECT	SHEET NO.	SHEET NO. 5
60-15HB-3	*	MADISON	93	27	47 SHEETS
FED. ROAD DIST. NO. 7		FED. AID PROJECT		Contract No. 76706	

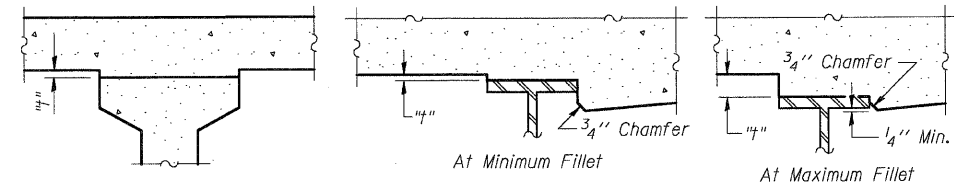


Location	A1	B1	C1	D1	Span 1	A2	B2	C2	D2	Span 2	A3	B3	C3	D3	Span 3
Girder 7	1 13/16"	2 1/16"	1 5/16"	48'-7 5/8"	194'-6 1/2"	7/8"	1 13/16"	1 3/16"	54'-11 1/8"	219'-8 1/2"	5/16"	7/8"	3/4"	38'-6 5/8"	154'-2 3/4"
Girder 8	1 5/16"	2 1/4"	1 1/8"	48'-7 5/8"	194'-6 3/8"	7/8"	1 13/16"	1 3/16"	55'-0"	219'-7 7/8"	3/8"	7/8"	1 3/16"	38'-6 3/8"	154'-1 3/4"
Girder 9	2 1/16"	2 7/16"	1 1/8"	48'-7 5/8"	194'-6 1/4"	7/8"	1 7/8"	1 3/16"	54'-10 3/4"	219'-7 1/4"	3/8"	1 5/16"	1 3/16"	38'-6 1/8"	154'-0 3/4"
Girder 10	2 1/4"	2 5/8"	1 1/4"	48'-7 1/2"	194'-6 1/8"	7/8"	1 7/8"	1 1/4"	54'-10 5/8"	219'-6 1/2"	3/8"	1 5/16"	7/8"	38'-5 7/8"	153'-11 3/4"
Girder 11	2 3/8"	2 13/16"	1 5/16"	48'-7 1/2"	194'-6"	1 3/16"	1 5/16"	1 1/4"	54'-10 1/2"	219'-5 7/8"	3/8"	1"	7/8"	38'-5 3/4"	153'-10 3/4"
Girder 12	2 9/16"	3"	1 7/16"	48'-7 1/2"	194'-5 7/8"	1 3/16"	1 5/16"	1 5/16"	54'-10 3/8"	219'-5 1/4"	3/8"	1 1/16"	1 5/16"	38'-5 1/2"	153'-9 7/8"
Baseline Ramp B & Profile Grade	2 3/8"	2 13/16"	1 5/16"	48'-7 1/2"	194'-6"	1 3/16"	1 5/16"	1 1/4"	54'-10 1/2"	219'-6"	3/8"	1"	7/8"	38'-5 3/4"	153'-10 7/8"

DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

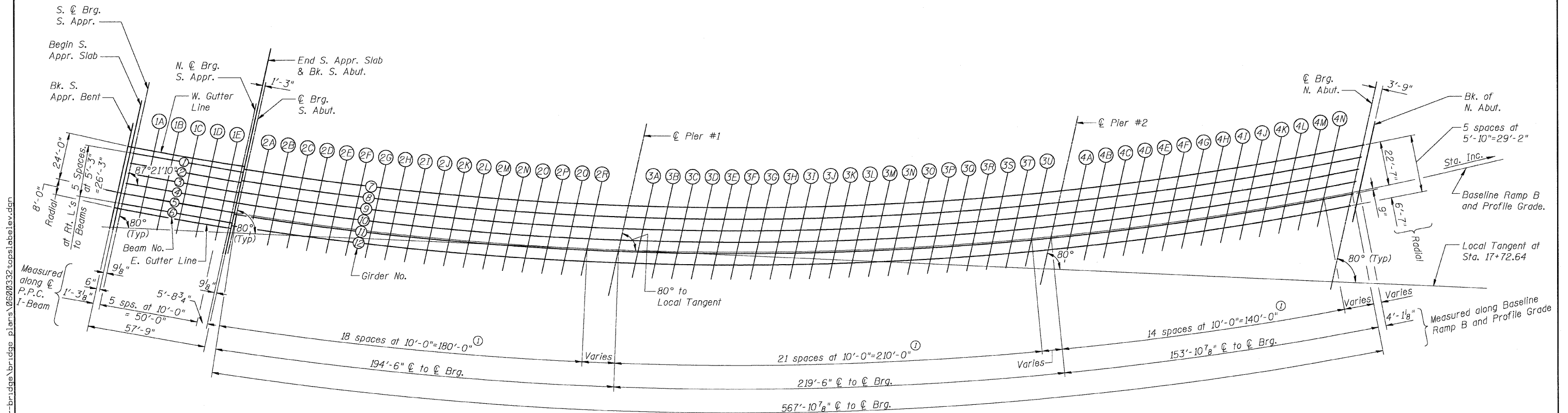
Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on sheets #6, #7 and #8 of 47.



To determine "h": After all structural steel and precast prestressed beams have been erected, elevations of the top flanges of the beams and girders shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets #6, #7 and #8 of 47, minus slab thickness, equals the fillet heights "h" above top flange of beams.

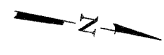
FILLET HEIGHTS

- Notes:
1. Screed points are located at 10'-0" intervals along each girder, and along Baseline Ramp B.
 2. Offsets are measured perpendicular to Baseline Ramp B.



DESIGNED	ADL
CHECKED	WLW
DRAWN	BGJ
CHECKED	WLW

PLAN



**TOP OF SLAB ELEVATIONS
RAMP B OVER FAP RTE 310
SECTION 60-15HB-3
MADISON COUNTY
STATION 17+72.64 (RAMP B)
SN 060-0332**