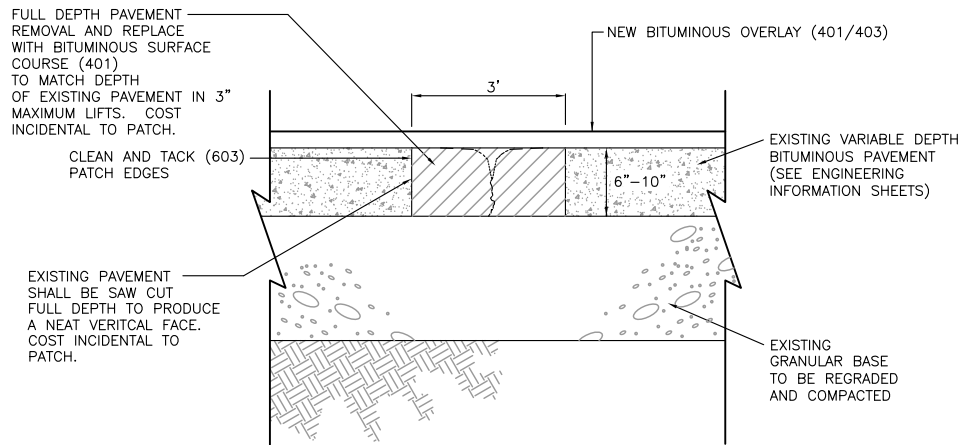


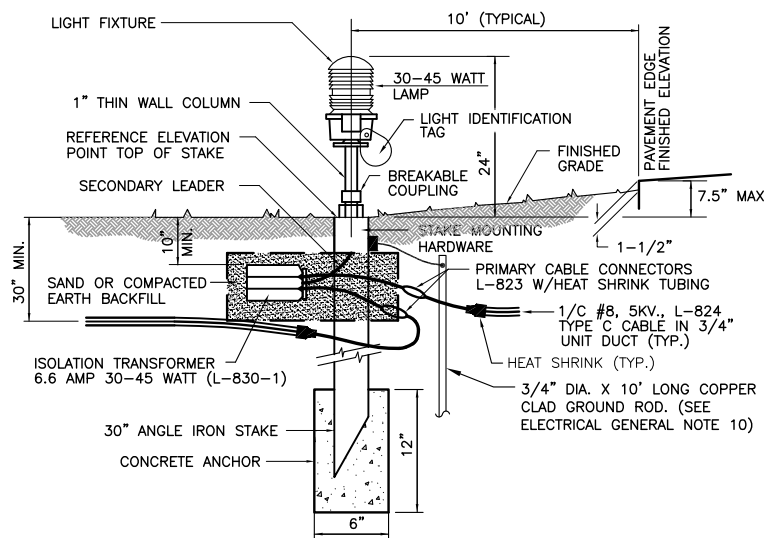
REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A
 NOT TO SCALE



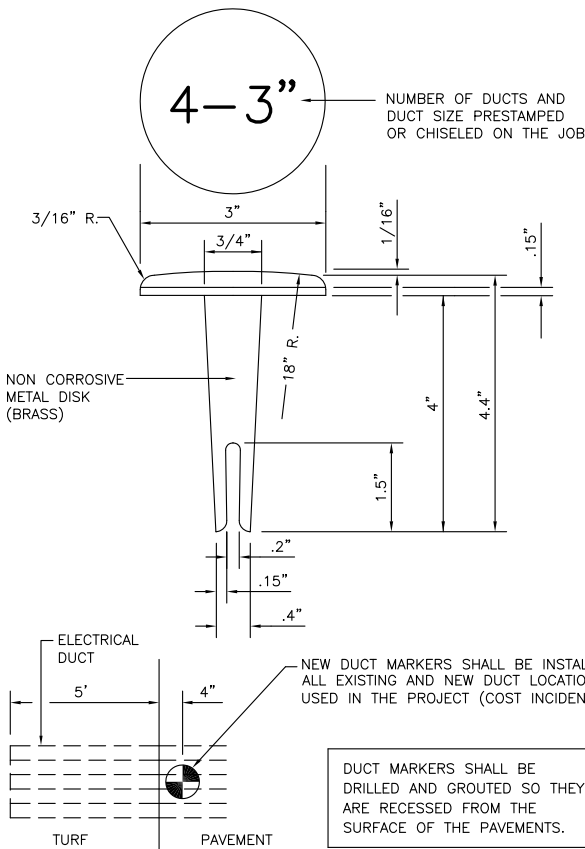
REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B
 NOT TO SCALE

PAVEMENT REMOVAL AND REPLACEMENT NOTES

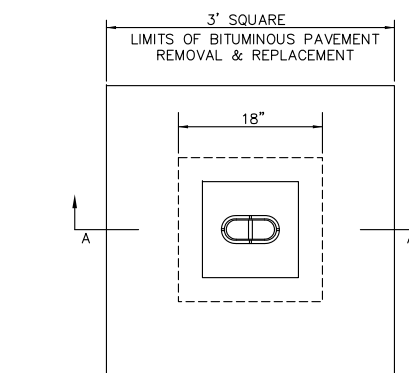
1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT APPROXIMATELY 150' INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.



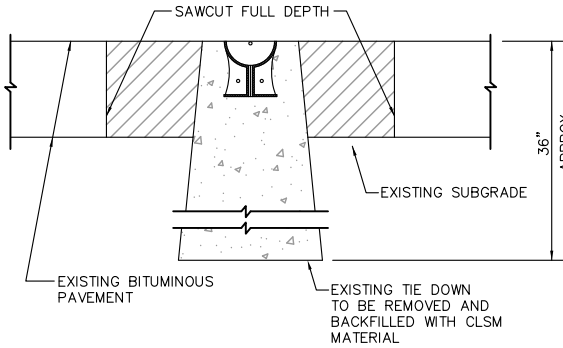
ADJUST EXISTING STAKE MOUNTED MEDIUM INTENSITY LIGHT
 NOT TO SCALE



DUCT MARKER DETAIL
 NOT TO SCALE



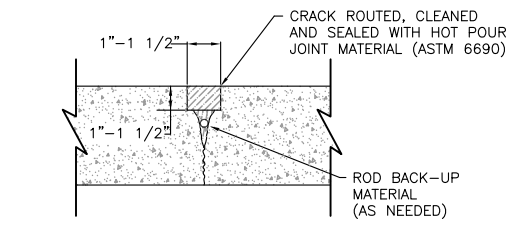
PLAN



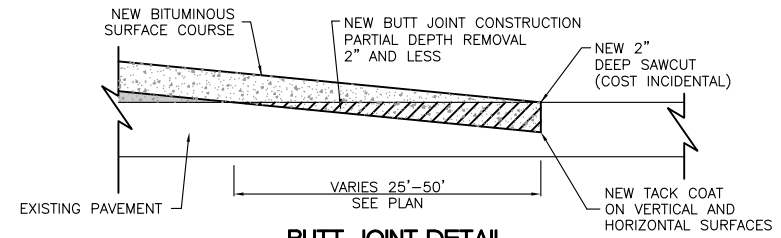
TIE DOWN REMOVAL DETAIL
 NOT TO SCALE

NOTES:

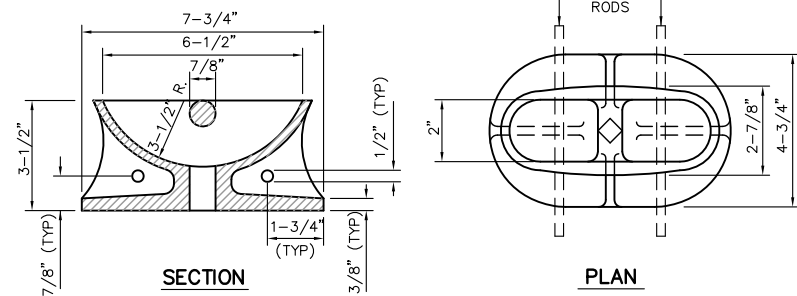
1. THE SAWCUT AND BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE MOORING EYE REMOVAL.
2. THE VOID LEFT BY THE REMOVAL SHALL BE BACKFILLED TO 9" BELOW EXISTING PAVEMENT SURFACE WITH CLSM MATERIAL. THE FINAL 9" SHALL BE FILLED WITH BITUMINOUS SURFACE COURSE TO MATCH THE MILLED SURFACE.



REPAIR METHOD - CRACK ROUTING AND SEALING
 NOT TO SCALE



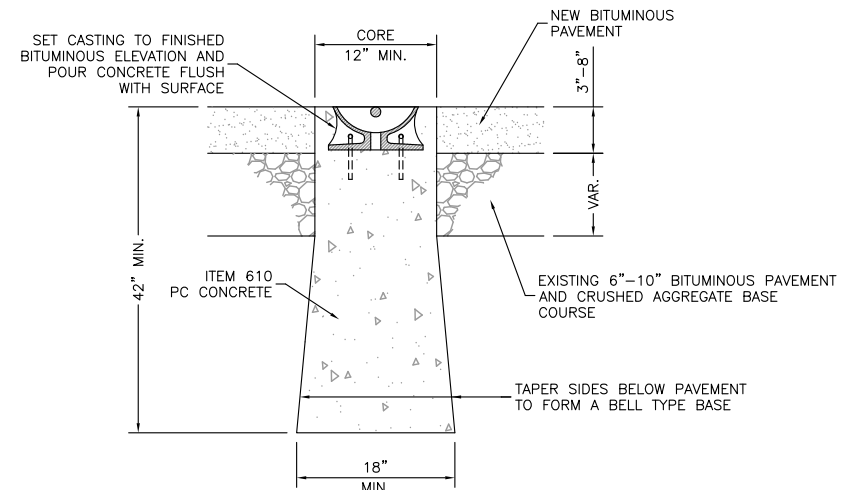
BUTT JOINT DETAIL
 NOT TO SCALE



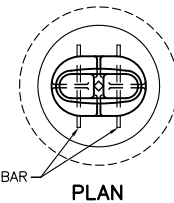
MOORING EYE DETAIL
 NOT TO SCALE

NOTES:

1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS TO BE NO. 3 DEFORMED REINFORCING STEEL 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.



SECTION



PLAN

MOORING EYE INSTALLATION DETAIL
 NOT TO SCALE

NOTE:

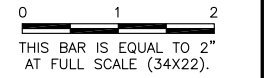
1. THE CONTRACTOR SHALL CORE OR SAW THROUGH BITUMINOUS OVERLAY AND EXISTING BITUMINOUS PAVEMENT AND ENLARGE HOLE TO DIMENSIONS SHOWN.

KA041

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

MISCELLANEOUS DETAILS

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APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20

ILLINOIS PROJECT: IKK-3902
 A.I.P. PROJECT: 3-17-0057-B17
FINAL