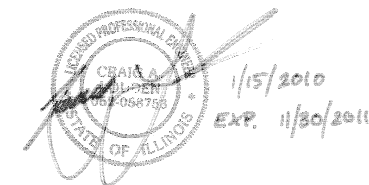


# KANKAKEE VALLEY AIRPORT AUTHORITY KANKAKEE, ILLINOIS


## CONSTRUCTION PLANS FOR GREATER KANKAKEE AIRPORT

### REHABILITATE TERMINAL APRON AND TAXIWAY H PHASE 2

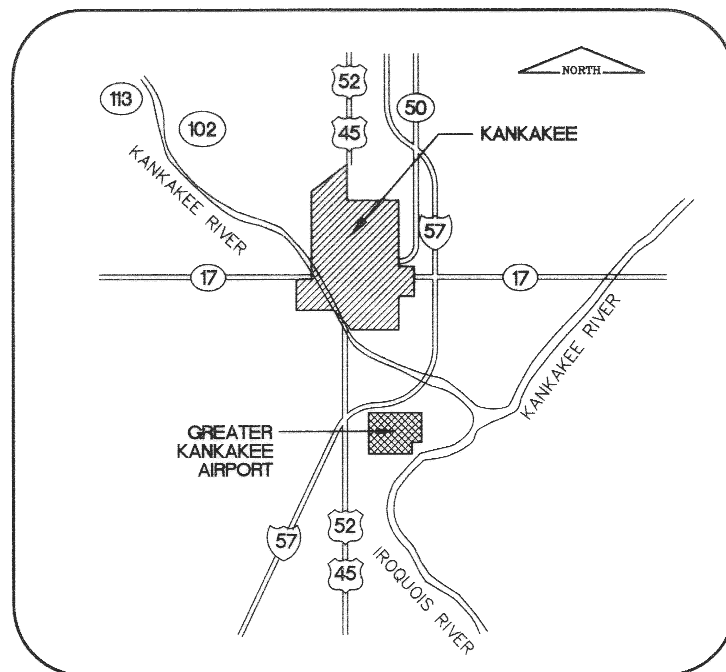
ILLINOIS PROJECT: IKK-3902  
A.I.P. PROJECT: 3-17-0057-B17



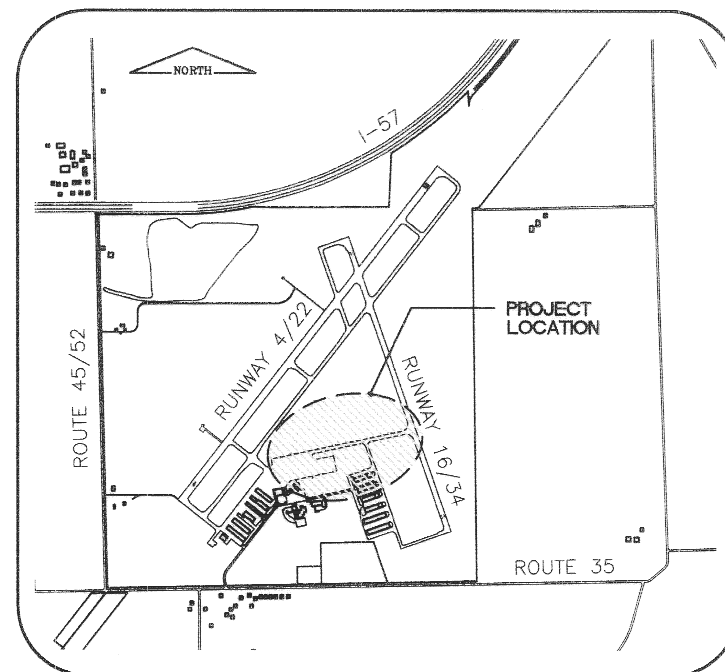
JANUARY 15, 2010

KANKAKEE VALLEY  
AIRPORT AUTHORITY  
APPROVED   
AIRPORT MANAGER  
DATE 1-12-10 2010

CALL J.U.L.I.E.  
BEFORE EXCAVATING  
1-800-892-0123  
KANKAKEE VALLEY AIRPORT AUTHORITY  
GREATER KANKAKEE AIRPORT  
TOWNSHIP: T 30 N  
RANGE: R 12 E  
SECTIONS 20 & 21  
COUNTY: KANKAKEE  
TOWNSHIP: OTTO





SITE PLAN



LOCATION MAP

DESIGN INFORMATION  
DESIGN AIRCRAFT APPROACH CATEGORY B  
DESIGN AIRCRAFT GROUP II

08075-05-20  
 **CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
SUBMITTED BY   
CRAIG LOUDON, P.E.  
DATE 1/15/2010 2010

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
<b>BASE BID</b>				
AR125941	ADJUST STAKE MOUNTED LIGHT	EACH	1	
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR156531	EROSION CONTROL BLANKET	SY	1,600	
AR156540	RIP RAP	SY	17	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	12,000	
AR401610	BITUMINOUS SURFACE COURSE	TON	2,270	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	19,320	
AR401655	BUTT JOINT CONSTRUCTION	SY	705	
AR401915	REM & REP BIT. PAVEMENT - TYPE A	SY	500	
AR401916	REM & REP BIT. PAVEMENT - TYPE B	SY	300	
AR403610	BITUMINOUS BASE COURSE	TON	3,535	
AR403630	BITUMINOUS BASE TEST SECTION	EACH	1	
AR510510	TIE DOWN	EACH	45	
AR510900	REMOVE TIE DOWN	EACH	51	
AR603510	BITUMINOUS TACK COAT	GAL	7,500	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	4,400	
AR625510	TAR EMULSION SEAL COAT	SY	6,615	
AR901510	SEEDING	ACRE	0.4	
AR905530	TOPSOILING	SY	1,600	
<b>ADDITIVE ALTERNATE #1</b>				
AS152410	UNCLASSIFIED EXCAVATION	CY	800	
AS156531	EROSION CONTROL BLANKET	SY	500	
AS209611	CRUSHED AGGREGATE BASE COURSE - 11"	SY	1,435	
AS401610	BITUMINOUS SURFACE COURSE	TON	165	
AS401900	REMOVE BITUMINOUS PAVEMENT	SY	1,380	
AS403610	BITUMINOUS BASE COURSE	TON	330	
AS602510	BITUMINOUS PRIME COAT	GAL	415	
AS603510	BITUMINOUS TACK COAT	GAL	415	
AS620520	PAVEMENT MARKING - WATERBORNE	SF	375	
AS901510	SEEDING	ACRE	0.1	
AS905530	TOPSOILING	SY	500	

INDEX TO SHEETS

1	COVER SHEET
2	INDEX TO SHEETS / SUMMARY OF QUANTITIES
3	SITE PLAN
4	SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
5	SEQUENCE OF CONSTRUCTION PER AC 150/5370-2E (LATEST EDITION)
6	TYPICAL SECTIONS
7	EXISTING CONDITIONS/PROPOSED REMOVALS - SHEET 1
8	EXISTING CONDITIONS/PROPOSED REMOVALS - SHEET 2
9	EXISTING CONDITIONS/PROPOSED REMOVALS - SHEET 3
10	TERMINAL APRON GRADING AND LANDSCAPING PLAN
11	PLAN AND PROFILE - TAXIWAY H - SHEET 1
12	PLAN AND PROFILE - TAXIWAY H - SHEET 2
13	INTERSECTION GRADE DETAIL
14	HANGAR PAVEMENT REHABILITATION PLAN
15	TIEDOWN AND PAVEMENT MARKING PLAN - SHEET 1
16	TIEDOWN AND PAVEMENT MARKING PLAN - SHEET 2
17	TIEDOWN AND PAVEMENT MARKING PLAN - SHEET 3
18	MISCELLANEOUS DETAILS
19	ENGINEERING INFORMATION
20	ENGINEERING INFORMATION - SHEET 2

SURVEY BOOK # BOOK #


REVISIONS

NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2

INDEX TO SHEETS/SUMMARY OF QUANTITIES

© Copyright CMT, Inc.  
  
**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY: CAL

DRAWN BY: JRO

CHECKED BY: CAL

APPROVED BY: DKP

DATE: 01/15/2010

JOB No: 08075-05-20

ILLINOIS PROJECT: IKK-3902  
 A.I.P. PROJECT: 3-17-0057-B17

**FINAL**

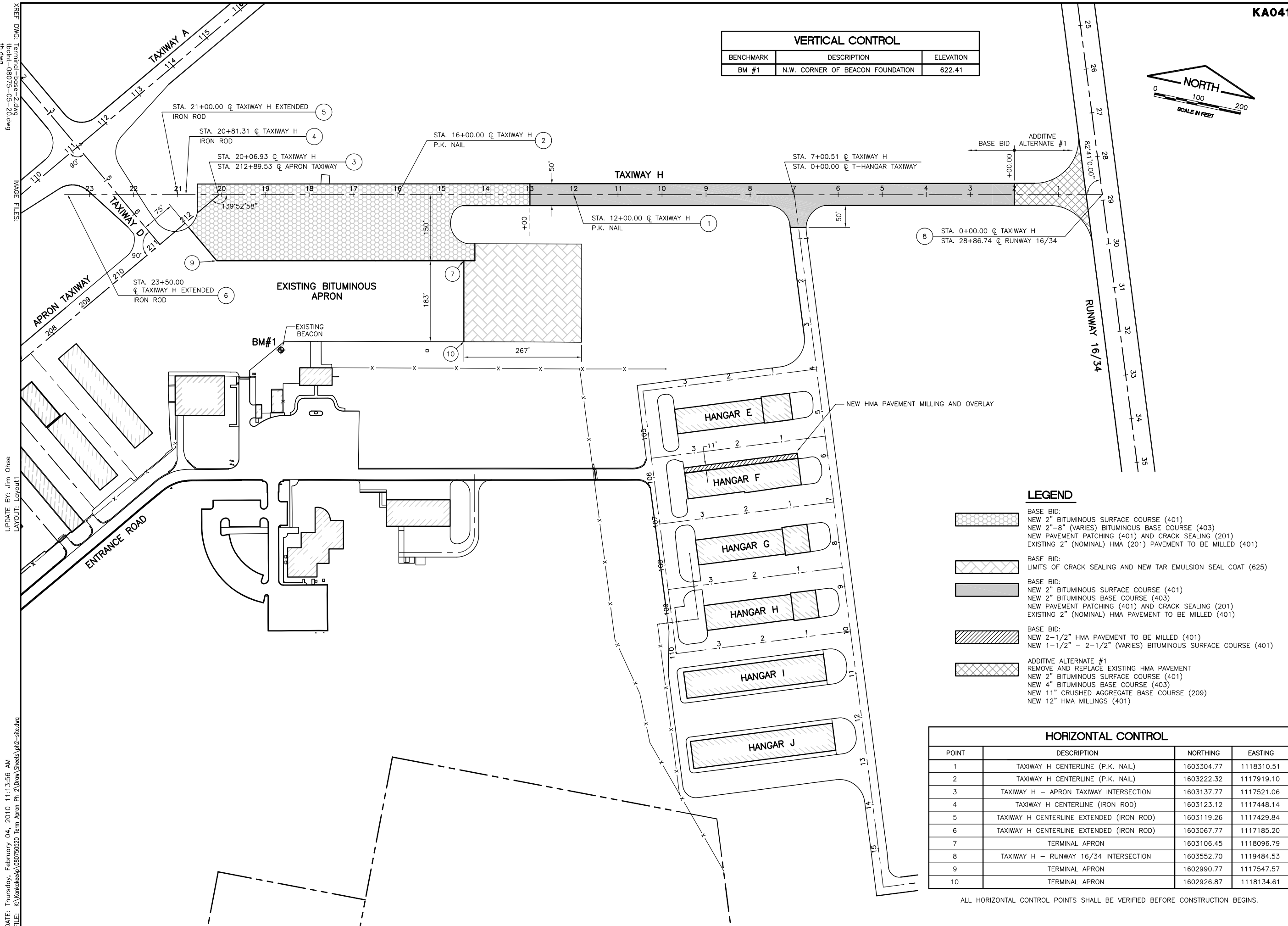
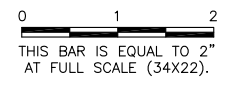
SHEET 2 OF 20 SHEETS

VERTICAL CONTROL		
BENCHMARK	DESCRIPTION	ELEVATION
BM #1	N.W. CORNER OF BEACON FOUNDATION	622.41



SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



- LEGEND**
- BASE BID:
    - NEW 2" BITUMINOUS SURFACE COURSE (401)
    - NEW 2"-8" (VARIES) BITUMINOUS BASE COURSE (403)
    - NEW PAVEMENT PATCHING (401) AND CRACK SEALING (201)
    - EXISTING 2" (NOMINAL) HMA (201) PAVEMENT TO BE MILLED (401)
  - BASE BID:
    - LIMITS OF CRACK SEALING AND NEW TAR EMULSION SEAL COAT (625)
  - BASE BID:
    - NEW 2" BITUMINOUS SURFACE COURSE (401)
    - NEW 2" BITUMINOUS BASE COURSE (403)
    - NEW PAVEMENT PATCHING (401) AND CRACK SEALING (201)
    - EXISTING 2" (NOMINAL) HMA PAVEMENT TO BE MILLED (401)
  - BASE BID:
    - NEW 2-1/2" HMA PAVEMENT TO BE MILLED (401)
    - NEW 1-1/2" - 2-1/2" (VARIES) BITUMINOUS SURFACE COURSE (401)
  - ADDITIVE ALTERNATE #1
    - REMOVE AND REPLACE EXISTING HMA PAVEMENT
    - NEW 2" BITUMINOUS SURFACE COURSE (401)
    - NEW 4" BITUMINOUS BASE COURSE (403)
    - NEW 11" CRUSHED AGGREGATE BASE COURSE (209)
    - NEW 12" HMA MILLINGS (401)

HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
1	TAXIWAY H CENTERLINE (P.K. NAIL)	1603304.77	1118310.51
2	TAXIWAY H CENTERLINE (P.K. NAIL)	1603222.32	1117919.10
3	TAXIWAY H - APRON TAXIWAY INTERSECTION	1603137.77	1117521.06
4	TAXIWAY H CENTERLINE (IRON ROD)	1603123.12	1117448.14
5	TAXIWAY H CENTERLINE EXTENDED (IRON ROD)	1603119.26	1117429.84
6	TAXIWAY H CENTERLINE EXTENDED (IRON ROD)	1603067.77	1117185.20
7	TERMINAL APRON	1603106.45	1118096.79
8	TAXIWAY H - RUNWAY 16/34 INTERSECTION	1603552.70	1119484.53
9	TERMINAL APRON	1602990.77	1117547.57
10	TERMINAL APRON	1602926.87	1118134.61

ALL HORIZONTAL CONTROL POINTS SHALL BE VERIFIED BEFORE CONSTRUCTION BEGINS.

**GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

**SITE PLAN**

© Copyright CMT, Inc.  
**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	

DATE: Thursday, February 04, 2010 11:13:56 AM  
 FILE: K:\Kankakee\180750520 Term Apron Ph. 2\Drawn Streets\p12-site.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 REF: DWG: Terminal - 1118310.51  
 6MP702-CO-CLOD-CO-1118310.51  
 6MP7-esc-2

**GENERAL NOTES**

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2E (LATEST EDITION) SAFETY DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER, IN CONSULTATION WITH THE RESIDENT ENGINEER, SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- FOR WORK ON AIRPORT PROPERTY:** PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES AT 10-FOOT CENTERS WITH ONE ORANGE FLAG (24" x 24") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER OR OWNER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER SEVEN (7) CALENDAR DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION (SEE FLAG DETAIL, THIS SHEET).
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE RESIDENT ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

- COORDINATION MEETINGS – THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN THE AIR OPERATIONS AREA (A.O.A.) FOR ACTIVE TAXIWAYS/RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MAINTENANCE. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

**CONTRACTOR CROSSING RUNWAY AND TAXIWAY AIR OPERATIONS AREA (A.O.A.)**

- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

**LIMITATIONS ON CONSTRUCTION WITHIN AIRPORT OPERATIONS AREA (A.O.A.)**

**RUNWAYS:**

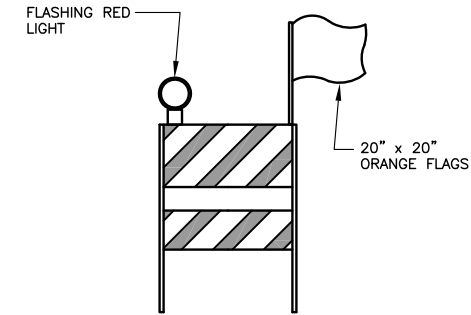
ANY WORK WITHIN THE A.O.A. OF AN ACTIVE RUNWAY SHALL EITHER BE DONE ON WEEKENDS, OFF-PEAK DAYTIME OR NIGHTTIME HOURS, LOCAL TIME AS SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN SHEETS. ON ANY DAY WHEN CONSTRUCTION IS WITHIN THE A.O.A. OF THE RUNWAY, THE RUNWAY SHALL BE CLOSED. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. STEEL PLATES IF NECESSARY SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE A.O.A. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT.

**TAXIWAYS / TAXILANES / APRONS:**

CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

**NOTE – ALL PHASES**

ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLES AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

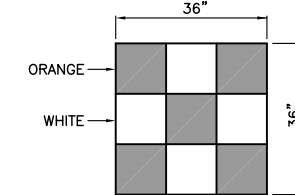


**FLASHER BARRICADE DETAIL**

N.T.S.

**NOTES**

- FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AT TEN-FOOT (10') INTERVALS.
- ONE 20"x20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE.



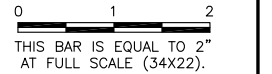
**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**

NOT TO SCALE

SURVEY BOOK # BOOK #

**REVISIONS**

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

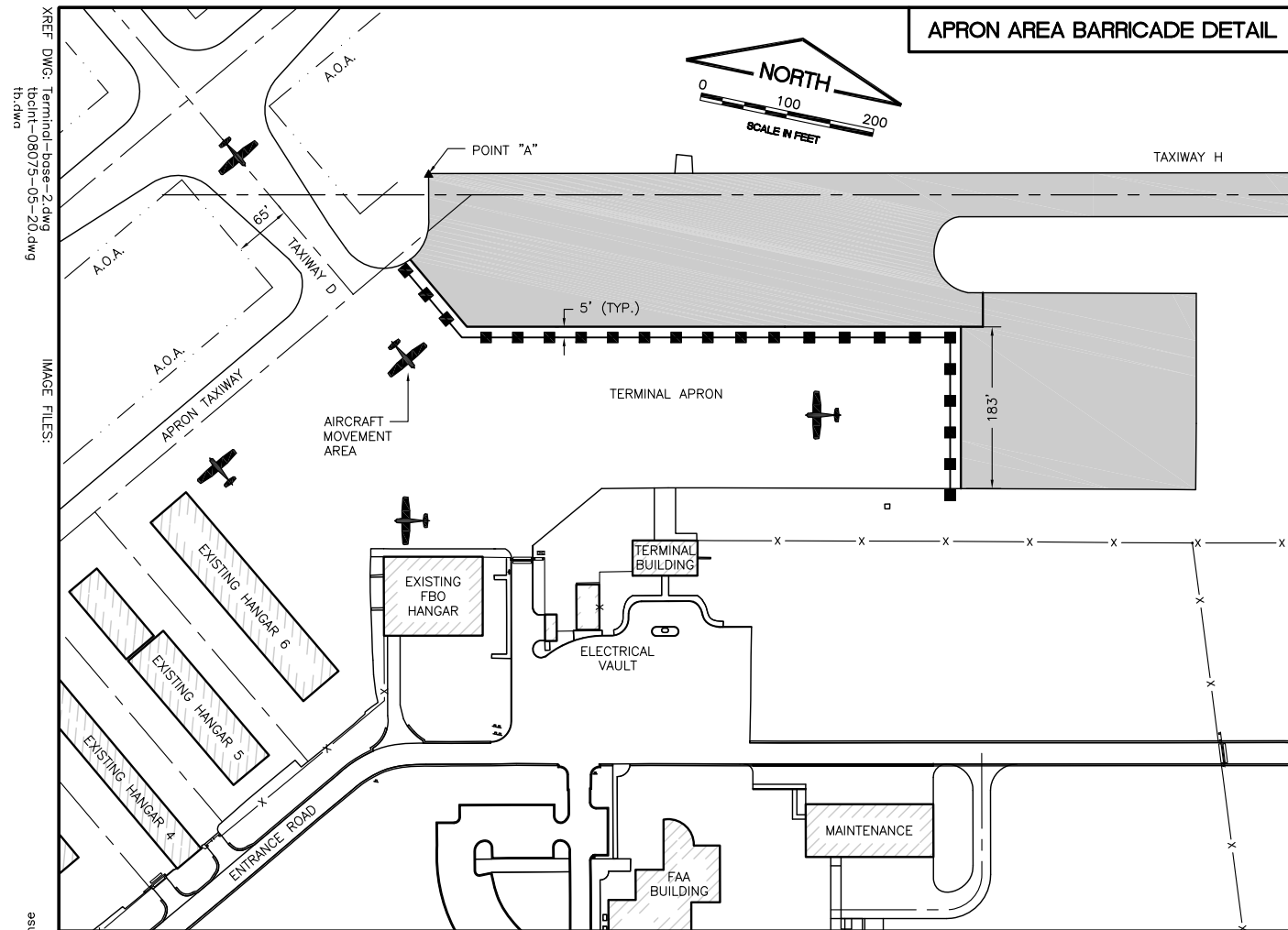
**SEQUENCE OF CONSTRUCTION  
GENERAL NOTES AND DETAILS**

© Copyright CMT, Inc.  
**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613



DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT: IKK-3902 A.I.P. PROJECT: 3-17-0057-B17	
<b>FINAL</b>	
SHEET	4 OF 20 SHEETS

DATE: Thursday, February 04, 2010 11:15:36 AM  
 FILE: K:\Valkenshede\080750520 Term Apron Ph. 2\Drawn Sheets\082-seq-3.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 REF: DWG: 13.dwg  
 Tblch: 08075-05-20.dwg  
 IMAGE FILES:



**APRON AREA BARRICADE DETAIL**



**SUGGESTED SEQUENCE OF CONSTRUCTION**

- COORDINATE RELOCATION OF ANY AIRCRAFT IN WORK AREA (BY OTHERS) WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- PLACE BARRICADES AS SHOWN AND CLOSE AREA TO AIRCRAFT TRAFFIC.
- REMOVE EXISTING AIRCRAFT TIEDOWNS PER PLAN.
- PAVEMENT MILLING PER PLAN.
- PERFORM CRACK SEALING AND ANY NECESSARY PAVEMENT REPAIRS.
- CONSTRUCT BITUMINOUS PAVEMENTS.
- INSTALL AIRCRAFT TIEDOWNS.
- INSTALL PAVEMENT MARKINGS AND PERFORM LANDSCAPING PER PLAN.
- REMOVE BARRICADES AND MISCELLANEOUS DEBRIS FROM CONSTRUCTION AREA AND CLEAN PAVEMENTS.
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER SO THAT DISPLACED AIRCRAFT CAN BE RELOCATED TO THEIR ORIGINAL STORAGE AREAS (BY OTHERS).

**LEGEND**

- PHASE 1 WORK LIMITS (BASE BID)
- PHASE 2 WORK LIMITS (ADDITIVE ALTERNATE #1)
- EXISTING HANGAR / BUILDING
- EXISTING PAVEMENT
- EXISTING AIRPORT PROPERTY LINE
- TEMPORARY BARRICADE PLACEMENT (BASE BID ONLY)
- TEMPORARY BARRICADE PLACEMENT (ADDITIVE ALTERNATE #1 ONLY)
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS / HAUL ROUTE
- PHASE 2 RUNWAY CLOSED MARKER (ADDITIVE ALTERNATE #1 ONLY)
- EXISTING AIR OPERATIONS AREA (A.O.A.)

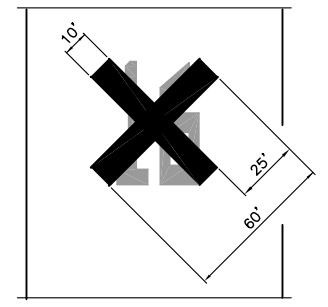
**POINT "A"**  
 NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 4/22  
 • LATITUDE: 41° 04' 02.73" (NAD 83)  
 • LONGITUDE: 87° 51' 00.76" (NAD 83)  
 • EXISTING ELEVATION: 619.2'

**POINT "B"**  
 NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 16/34  
 • LATITUDE: 41° 04' 06.27" (NAD 83)  
 • LONGITUDE: 87° 50' 36.35" (NAD 83)  
 • EXISTING ELEVATION: 617.6'

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20 FEET  
 DESIGN AIRCRAFT APPROACH CATEGORY: B  
 DESIGN AIRPORT GROUP: II

**SEQUENCING NOTES:**

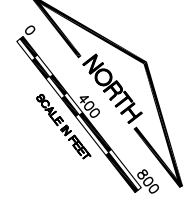
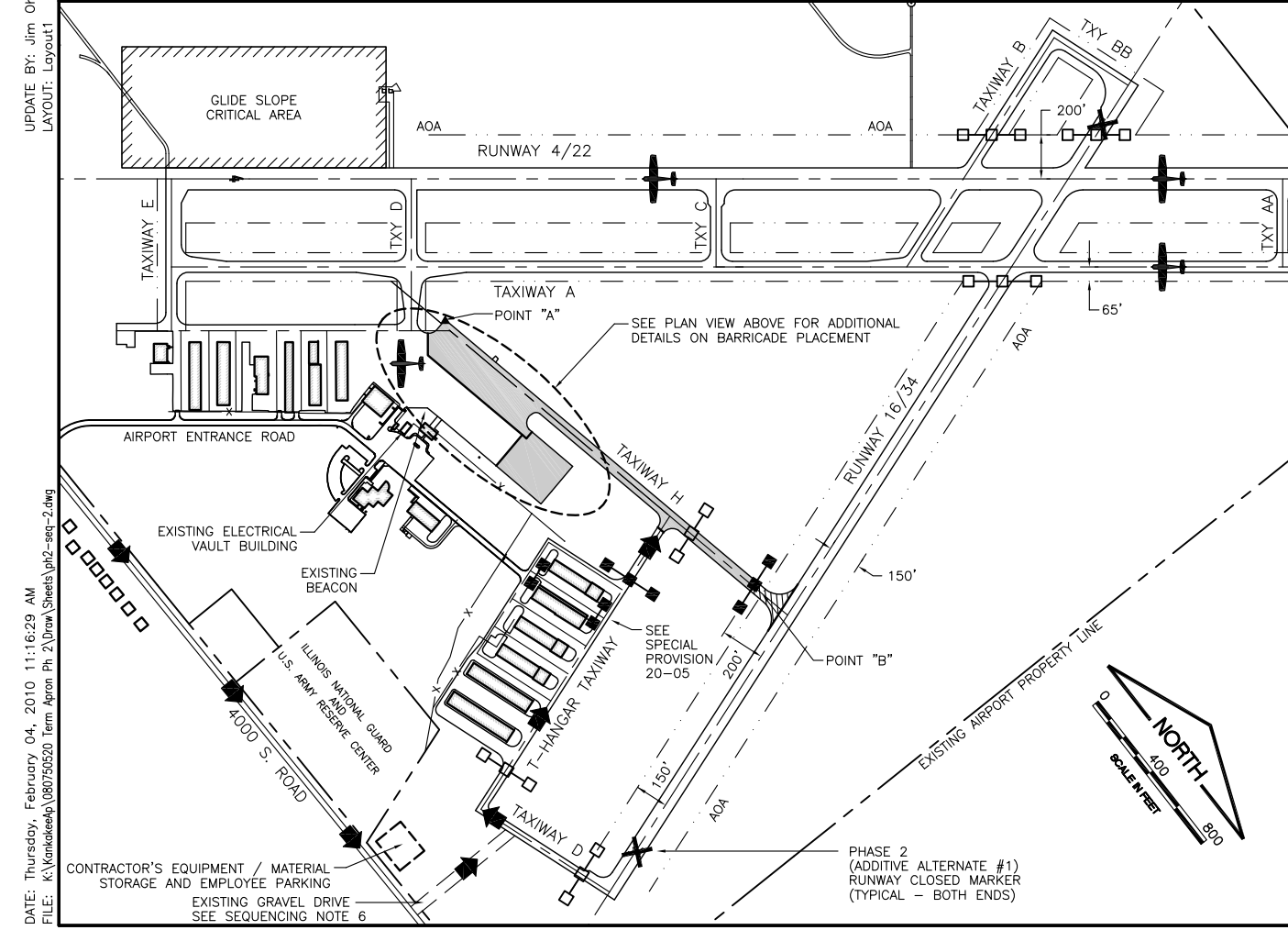
1. BEFORE PHASE 2 (ADDITIVE ALTERNATE #1) WORK CAN BEGIN, THE PHASE 1 IMPROVEMENTS MUST BE COMPLETE AND TAXIWAY H WEST OF THE T-HANGAR TAXIWAY MUST BE OPEN TO TRAFFIC.
2. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON EACH PHASE OR AS DIRECTED BY THE OWNER.
3. THE AIRPORT WILL REQUIRE SEVEN (7) CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
4. FOR ALL PHASES OF WORK, THE CONTRACTOR SHALL RETURN THE EQUIPMENT TO THE STAGING AND STORAGE AREA LOCATED AT THE SOUTH END OF THE AIRPORT OFF OF 4000 S. ROAD UNLESS PERMISSION IS GIVEN BY THE AIRPORT TO DO OTHERWISE.
5. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT MANAGER AND RESIDENT ENGINEER ON THE LOCATION OF ACCESS ROUTES OF AIRPORT-OWNED FUELING TRUCKS TO ALL AIRCRAFT AND TEMPORARY PARKING AREAS.
6. THE CONTRACTOR SHALL PLACE 6" THICK 20' WIDE MAXIMUM HMA MILLINGS ON EXISTING GRAVEL DRIVE. THROUGHOUT CONSTRUCTION THE DRIVE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OF DEBRIS ONTO AIRFIELD PAVEMENTS. THE DRIVE SHALL BE GRADED SMOOTH AT THE END OF THE PROJECT, EXCEPT FOR 50' ADJACENT TO TAXIWAY D, WHERE THE DRIVE SHALL BE REMOVED, TOPSOILED AND LANDSCAPED AS DIRECTED BY THE RESIDENT ENGINEER. THE COST OF MILLING, PLACEMENT, ALL MAINTENANCE DURING CONSTRUCTION AND FINAL GRADING AND REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.



**ON PAVEMENT CLOSED RUNWAY MARKER DETAIL**  
 NO SCALE

**NOTES**

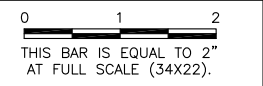
1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND RESIDENT ENGINEER. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



**KA041**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



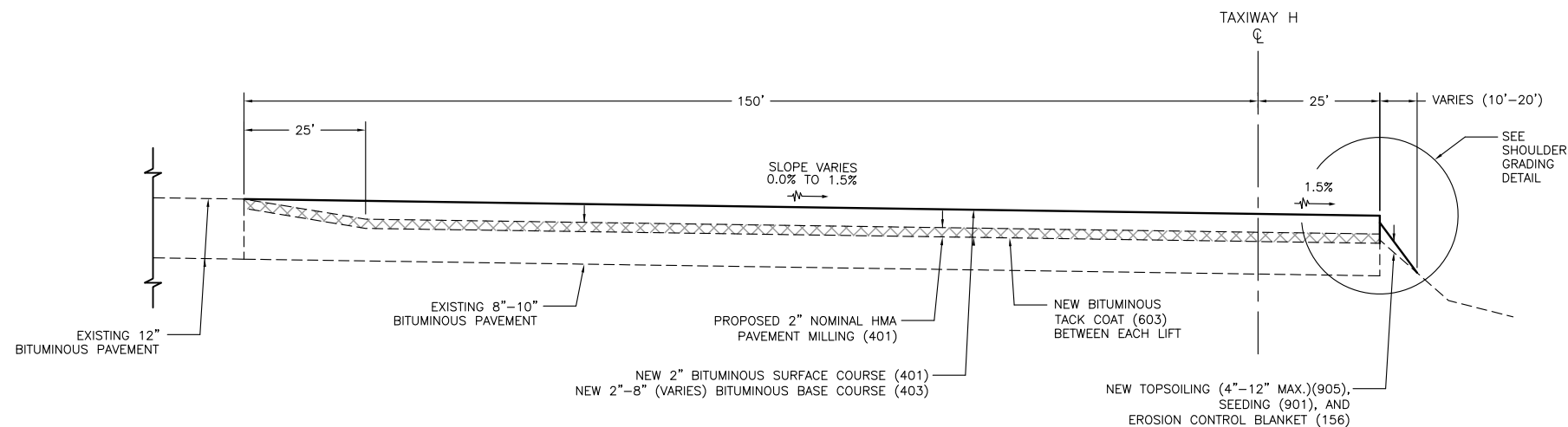
**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

**SEQUENCE OF CONSTRUCTION  
 PER AC 150/5370-2E (LATEST EDITION)**

© Copyright G.M.T. Inc.  
**G.M.T.**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

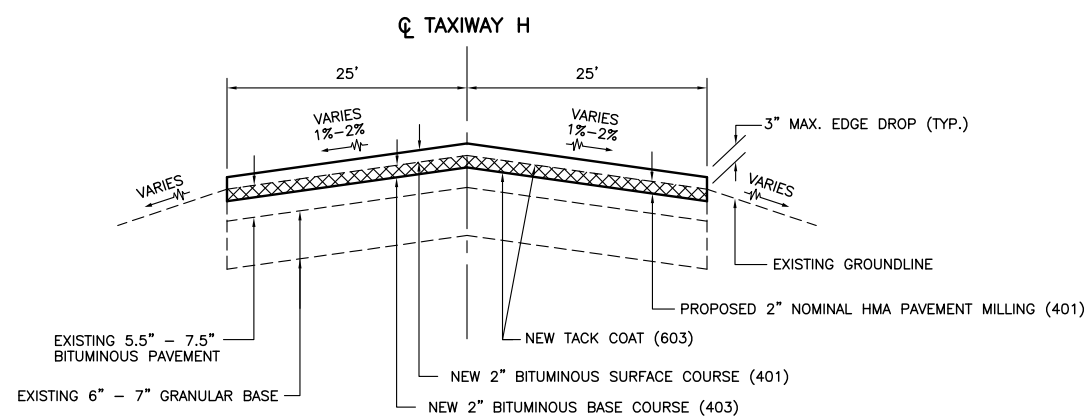
DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	5 OF 20 SHEETS

REF: DWG: Terminal Base - 2.dwg  
 18Jan-08075-05-20.dwg  
 13:41:00  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 DATE: Thursday, February 04, 2010 11:16:29 AM  
 FILE: K:\Vend\kdep\080750520 Term Apron Ph 2\Draw\Sheets\p2-sep-2.dwg



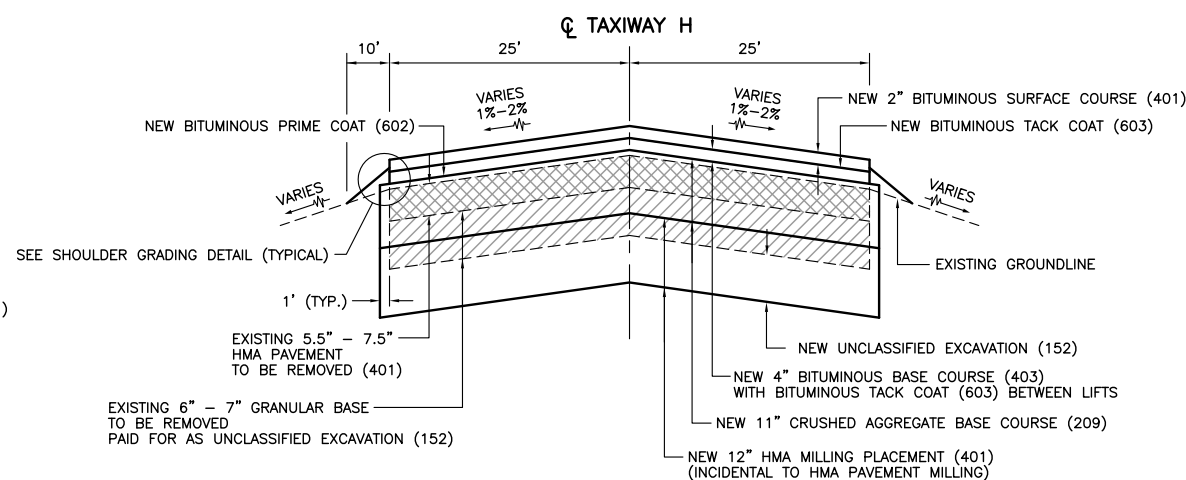
**TYPICAL SECTION A-A**

NOT TO SCALE  
BASE BID



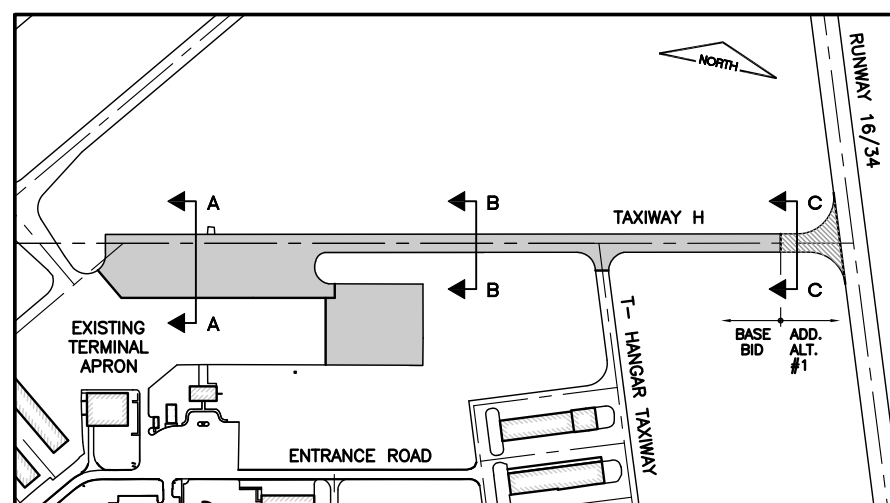
**TYPICAL SECTION B-B**

NOT TO SCALE  
BASE BID

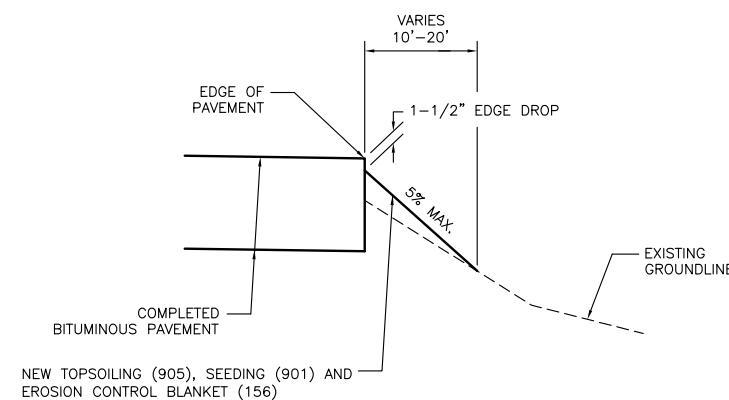


**TYPICAL SECTION C-C**

NOT TO SCALE  
ADDITIVE ALTERNATE #1



**KEY MAP**



**SHOULDER GRADING DETAIL**

NOT TO SCALE

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2

TYPICAL SECTIONS

© Copyright GMT, Inc.  
**GMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

DESIGN BY: CAL

DRAWN BY: JRO

CHECKED BY: CAL

APPROVED BY: DKP

DATE: 01/15/2010

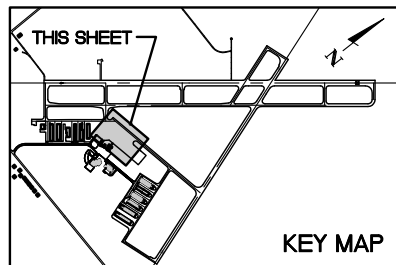
JOB No: 08075-05-20

ILLINOIS PROJECT: IKK-3902

A.I.P. PROJECT: 3-17-0057-B17

**FINAL**

SHEET 6 OF 20 SHEETS



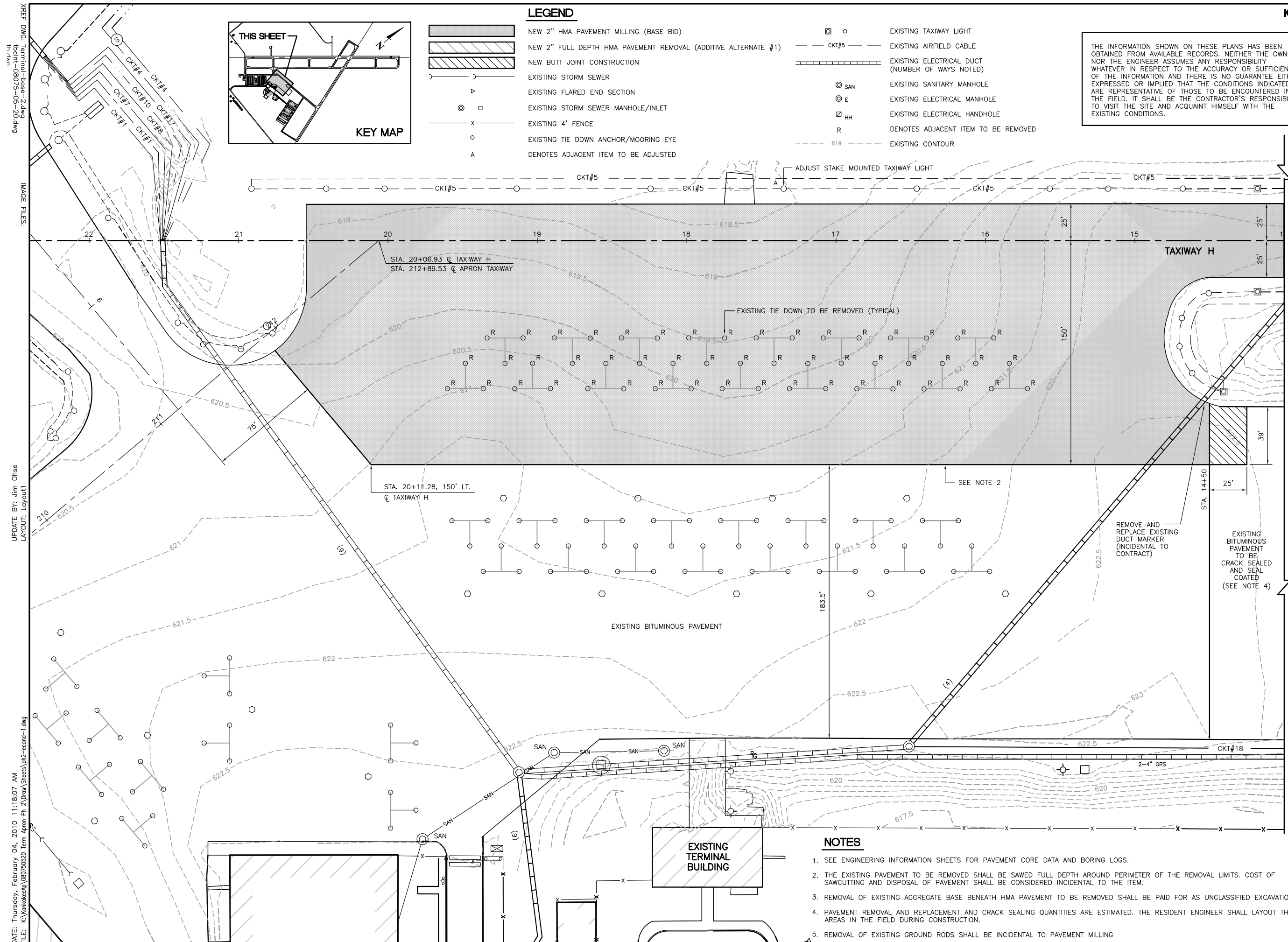
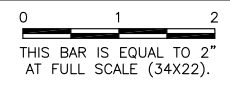
**LEGEND**

	NEW 2" HMA PAVEMENT MILLING (BASE BID)		EXISTING TAXIWAY LIGHT
	NEW 2" FULL DEPTH HMA PAVEMENT REMOVAL (ADDITIVE ALTERNATE #1)		EXISTING AIRFIELD CABLE
	NEW BUTT JOINT CONSTRUCTION		EXISTING ELECTRICAL DUCT (NUMBER OF WAYS NOTED)
	EXISTING STORM SEWER		EXISTING SANITARY MANHOLE
	EXISTING FLARED END SECTION		EXISTING ELECTRICAL MANHOLE
	EXISTING STORM SEWER MANHOLE/INLET		EXISTING ELECTRICAL HANDHOLE
	EXISTING 4' FENCE		DENOTES ADJACENT ITEM TO BE REMOVED
	EXISTING TIE DOWN ANCHOR/MOORING EYE		EXISTING CONTOUR
	DENOTES ADJACENT ITEM TO BE ADJUSTED		

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



MATCHLINE STA. 14+00 TAXIWAY H

**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2  
 EXISTING CONDITIONS/PROPOSED REMOVALS  
 SHEET 1**

© Copyright CMT, Inc.

**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	7 OF 20 SHEETS

**NOTES**

- SEE ENGINEERING INFORMATION SHEETS FOR PAVEMENT CORE DATA AND BORING LOGS.
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- REMOVAL OF EXISTING AGGREGATE BASE BENEATH HMA PAVEMENT TO BE REMOVED SHALL BE PAID FOR AS UNCLASSIFIED EXCAVATION.
- PAVEMENT REMOVAL AND REPLACEMENT AND CRACK SEALING QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAYOUT THESE AREAS IN THE FIELD DURING CONSTRUCTION.
- REMOVAL OF EXISTING GROUND RODS SHALL BE INCIDENTAL TO PAVEMENT MILLING

DATE: Thursday, February 04, 2010 11:18:07 AM  
 FILE: K:\kankakee\080750520 Term Apron Ph 2\Draw\Sheets\p2-ecord-1.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 IMAGE FILES:  
 KRFE-DWG: Terminal-Base-2.dwg  
 Terminal-08075-05-20.dwg  
 h.rdw

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

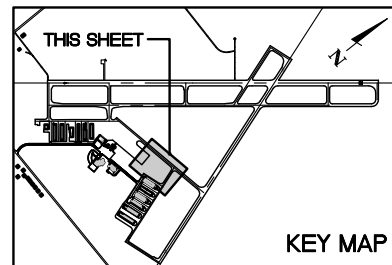
GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2  
EXISTING CONDITIONS/PROPOSED REMOVALS  
SHEET 2

© Copyright, CMT, Inc.  
**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

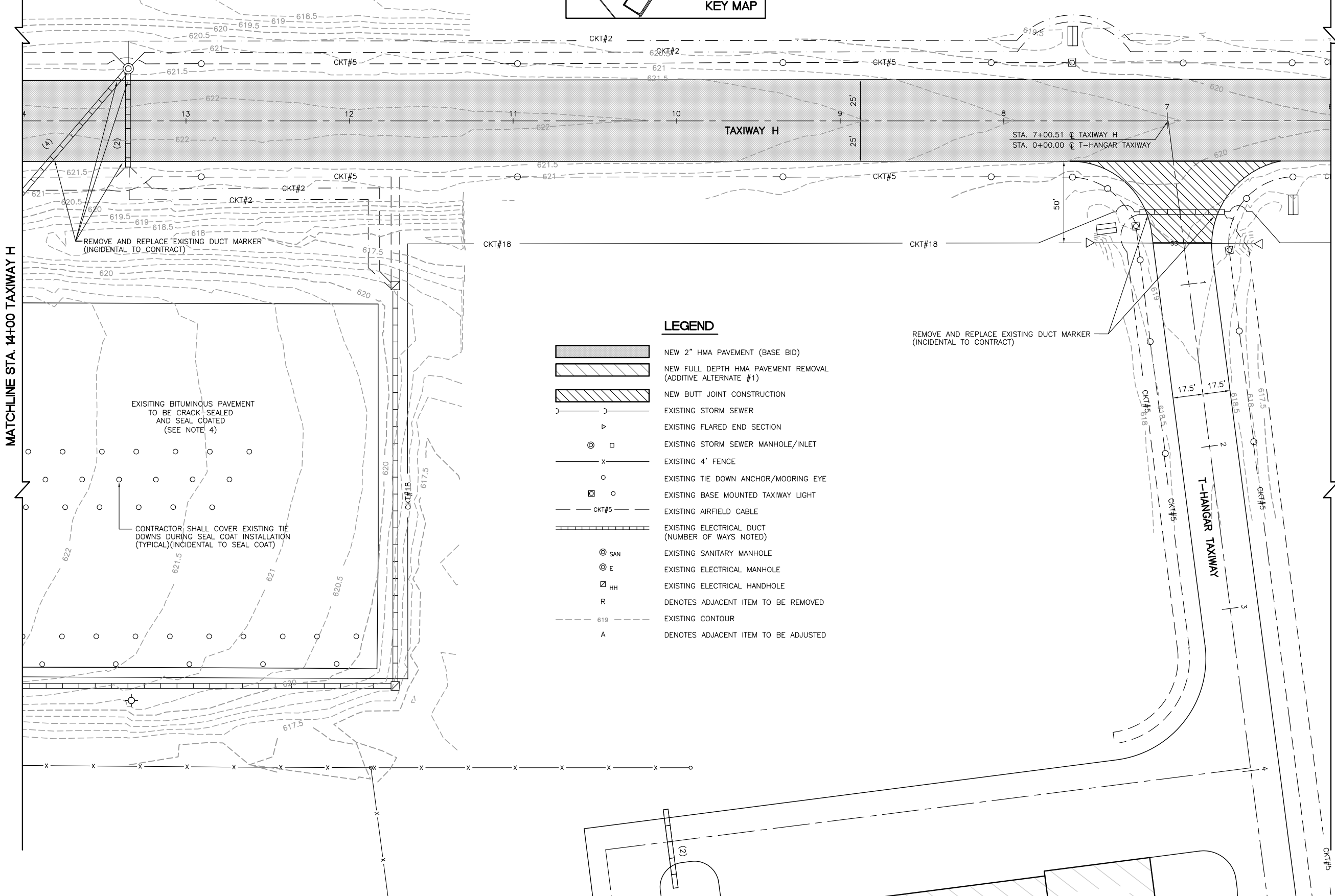
DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	8 OF 20 SHEETS

NOTES

- SEE ENGINEERING INFORMATION SHEETS FOR PAVEMENT CORE DATA AND BORING LOGS.
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- REMOVAL OF EXISTING AGGREGATE BASE BENEATH HMA PAVEMENT TO BE REMOVED SHALL BE PAID FOR AS UNCLASSIFIED EXCAVATION.
- PAVEMENT REMOVAL AND REPLACEMENT AND CRACK SEALING QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAYOUT THESE AREAS IN THE FIELD DURING CONSTRUCTION.



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



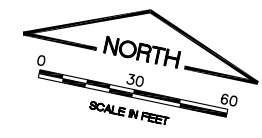
LEGEND

- [Solid Gray Box] NEW 2" HMA PAVEMENT (BASE BID)
- [Diagonal Hatching Box] NEW FULL DEPTH HMA PAVEMENT REMOVAL (ADDITIVE ALTERNATE #1)
- [Diagonal Hatching Box] NEW BUTT JOINT CONSTRUCTION
- [Line with Arrow] EXISTING STORM SEWER
- [Line with Triangle] EXISTING FLARED END SECTION
- [Circle with Square] EXISTING STORM SEWER MANHOLE/INLET
- [Line with X] EXISTING 4' FENCE
- [Circle with Dot] EXISTING TIE DOWN ANCHOR/MOORING EYE
- [Circle with Square] EXISTING BASE MOUNTED TAXIWAY LIGHT
- [Line with Dash] EXISTING AIRFIELD CABLE
- [Line with Dash] EXISTING ELECTRICAL DUCT (NUMBER OF WAYS NOTED)
- [Circle with SAN] EXISTING SANITARY MANHOLE
- [Circle with E] EXISTING ELECTRICAL MANHOLE
- [Square with HH] EXISTING ELECTRICAL HANDHOLE
- [Line with R] DENOTES ADJACENT ITEM TO BE REMOVED
- [Line with A] DENOTES ADJACENT ITEM TO BE ADJUSTED
- [Dashed Line] EXISTING CONTOUR

DATE: Thursday, February 04, 2010 11:19:06 AM  
 FILE: K:\Vandenberg\080750520 Term Apron Ph 2\Drawn Sheets\ph2-second-2.dwg  
 UPDATE BY: Jim Onse  
 LAYOUT: Layout1  
 IMAGE FILES:  
 DWP: 3  
 DWP: 3  
 6MP: 02-CO-C:\JOB\080750520 Term Apron Ph 2-sec2-2.dwg



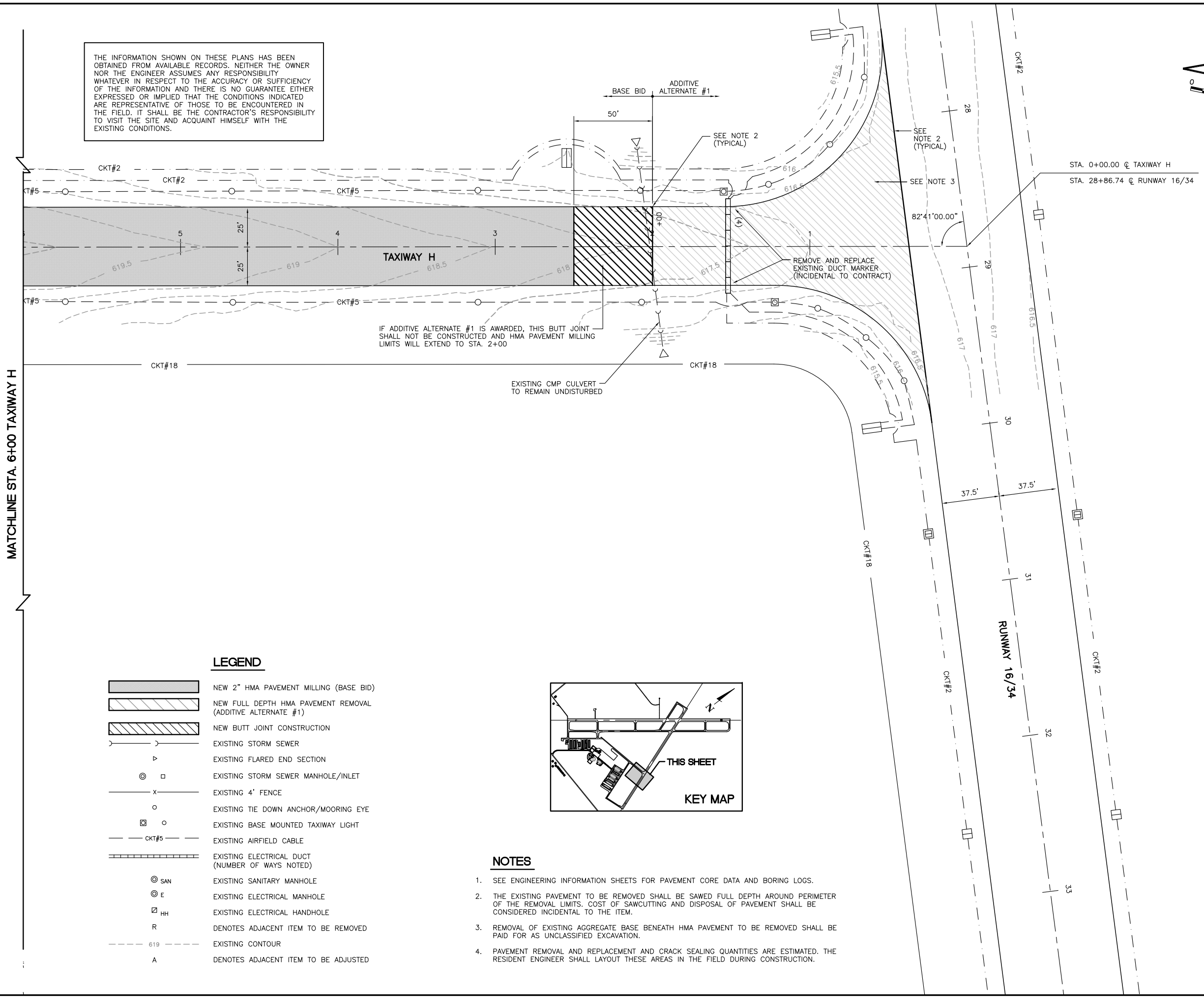
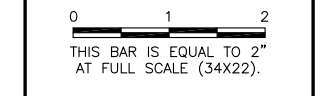
THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



KA041

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



IF ADDITIVE ALTERNATE #1 IS AWARDED, THIS BUTT JOINT SHALL NOT BE CONSTRUCTED AND HMA PAVEMENT MILLING LIMITS WILL EXTEND TO STA. 2+00

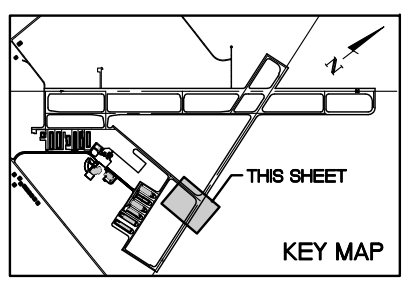
EXISTING CMP CULVERT TO REMAIN UNDISTURBED

REMOVE AND REPLACE EXISTING DUCT MARKER (INCIDENTAL TO CONTRACT)

STA. 0+00.00 @ TAXIWAY H  
STA. 28+86.74 @ RUNWAY 16/34

**LEGEND**

- NEW 2" HMA PAVEMENT MILLING (BASE BID)
- NEW FULL DEPTH HMA PAVEMENT REMOVAL (ADDITIVE ALTERNATE #1)
- NEW BUTT JOINT CONSTRUCTION
- EXISTING STORM SEWER
- EXISTING FLARED END SECTION
- EXISTING STORM SEWER MANHOLE/INLET
- EXISTING 4' FENCE
- EXISTING TIE DOWN ANCHOR/MOORING EYE
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- CKT#5 EXISTING AIRFIELD CABLE
- EXISTING ELECTRICAL DUCT (NUMBER OF WAYS NOTED)
- EXISTING SANITARY MANHOLE
- EXISTING ELECTRICAL MANHOLE
- EXISTING ELECTRICAL HANDHOLE
- DENOTES ADJACENT ITEM TO BE REMOVED
- DENOTES ADJACENT ITEM TO BE ADJUSTED
- EXISTING CONTOUR



**NOTES**

- SEE ENGINEERING INFORMATION SHEETS FOR PAVEMENT CORE DATA AND BORING LOGS.
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- REMOVAL OF EXISTING AGGREGATE BASE BENEATH HMA PAVEMENT TO BE REMOVED SHALL BE PAID FOR AS UNCLASSIFIED EXCAVATION.
- PAVEMENT REMOVAL AND REPLACEMENT AND CRACK SEALING QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAYOUT THESE AREAS IN THE FIELD DURING CONSTRUCTION.

**GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

**EXISTING CONDITIONS/PROPOSED REMOVALS  
SHEET 3**

© Copyright G.M.T., Inc.

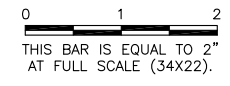
**G.M.T.**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	9 OF 20 SHEETS

DATE: Thursday, February 04, 2010 11:19:51 AM  
 FILE: K:\kankakee\080750520 Term Apron Ph 2\Draw\Sheets\p12-second-3.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 REF: DWG: Terminal-base-2.dwg  
 6WF:20-05-20.dwg  
 IMAGE FILES:  
 :STLH

SURVEY BOOK # BOOK #


REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

**TERMINAL APRON  
GRADING AND LANDSCAPING PLAN**




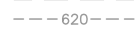
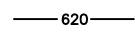
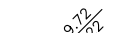
© Copyright CMT, Inc.

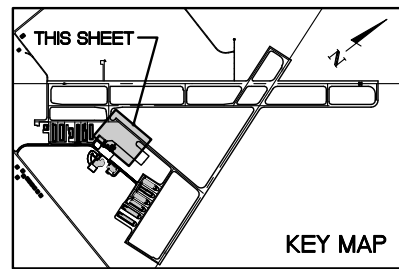


**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

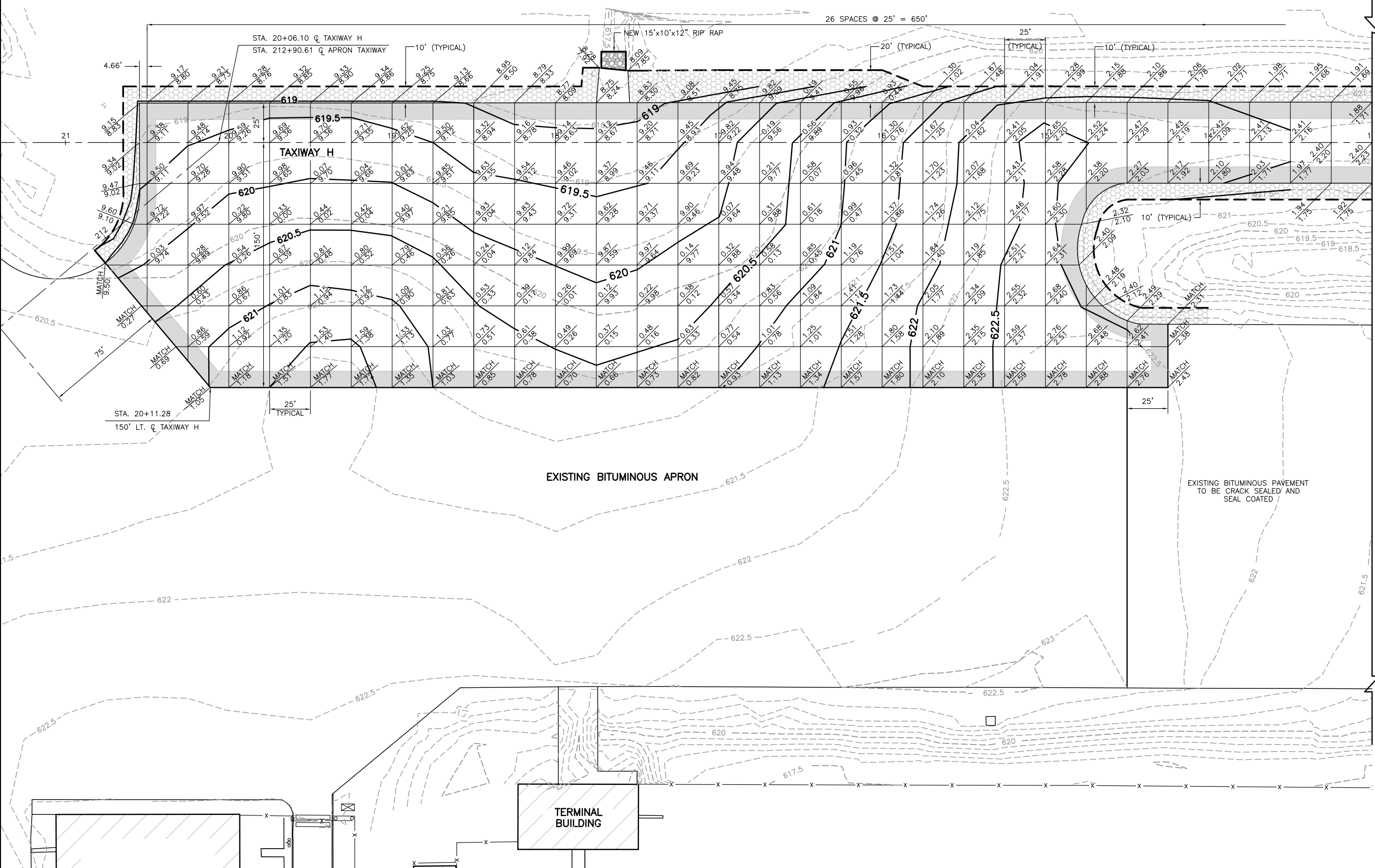
DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	10 OF 20 SHEETS

**LEGEND**

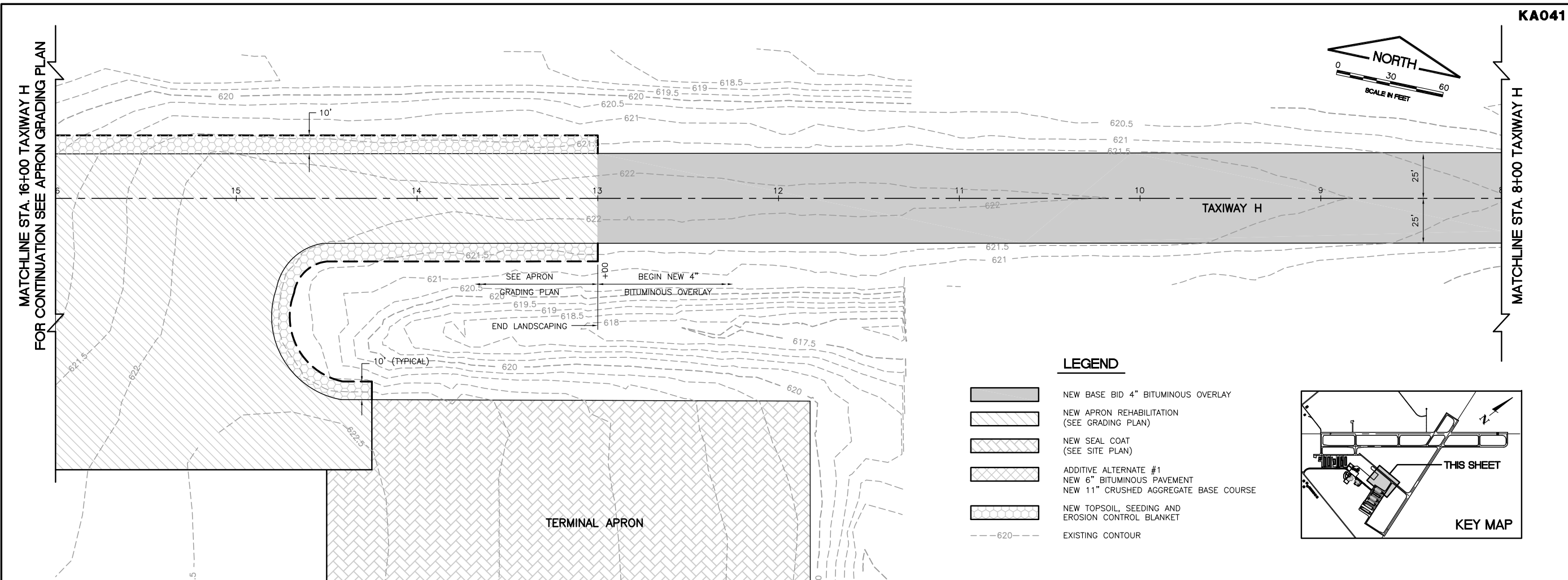
-  NEW BITUMINOUS OVERLAY LIMITS
-  NEW TOPSOILING, SEEDING AND EROSION CONTROL BLANKET
-  NEW RIP RAP
-  EXISTING CONTOUR
-  NEW CONTOUR
-  NEW ELEVATION (9.72)  
EXISTING ELEVATION (9.22)  
(ADD 610.00 FOR ELEVATIONS GREATER THAN 5.00 AND  
620.00 FOR ELEVATIONS LESS THAN 5.00 FOR USGS DATUM)



DATE: Thursday, February 04, 2010 11:20:59 AM  
FILE: K:\Kankakee\08075\0520 Term Apron Ph 2\Drawn\Sheets\g12-grade-1.dwg  
UPDATE BY: Jim Ohse  
LAYOUT: Layout1  
IMAGE FILES:  
1b.dwg  
REF: DWG: Terminal-base-2.dwg  
1b.dwg



DATE: Thursday, February 04, 2010 11:21:44 AM  
 FILE: K:\kankakee\080750520 Term Apron Ph 2\Draw\Sheets\p12-grade-2.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 REF: DWG: 52,080-080750520 Term Apron Ph 2.dwg  
 IMAGE FILES: 52,080-080750520 Term Apron Ph 2.dwg



KA041

MATCHLINE STA. 16+00 TAXIWAY H  
FOR CONTINUATION SEE APRON GRADING PLAN

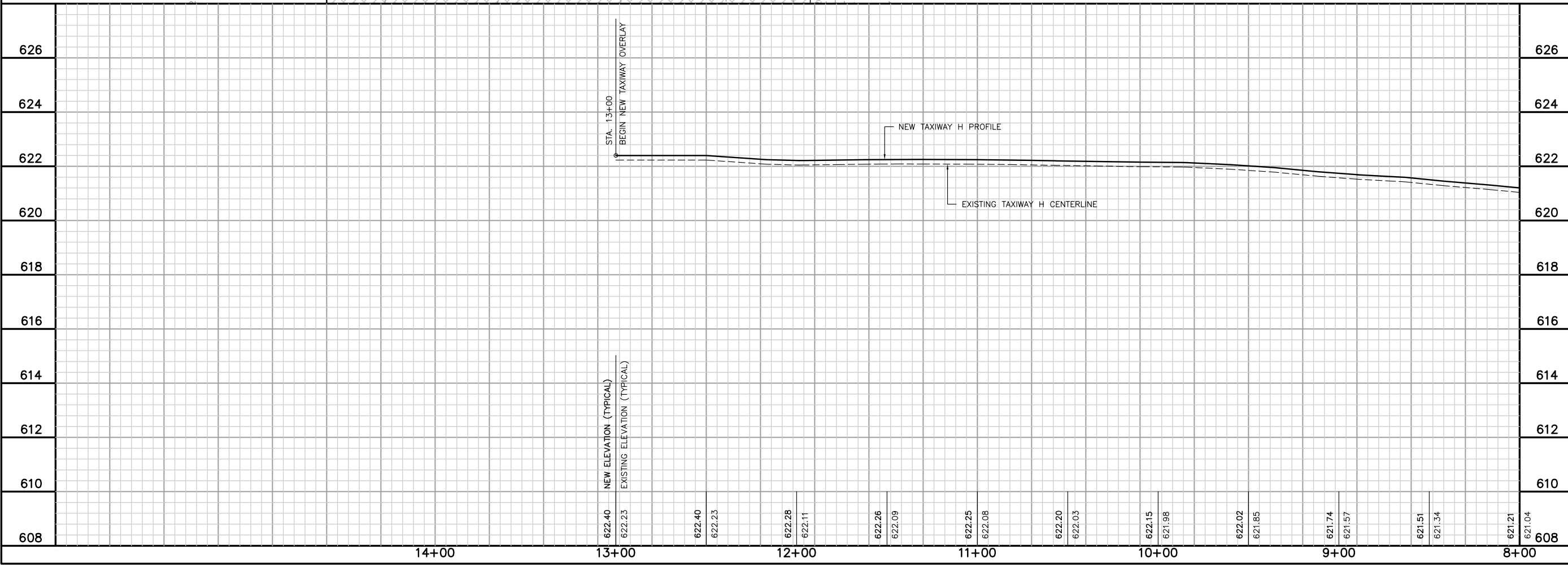
MATCHLINE STA. 8+00 TAXIWAY H

0 30 60  
NORTH  
SCALE IN FEET

0 1 2  
THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2

PLAN AND PROFILE - TAXIWAY H  
 SHEET 1

© Copyright GMT, Inc.

**GMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY: CAL  
 DRAWN BY: JRO  
 CHECKED BY: CAL  
 APPROVED BY: DKP  
 DATE: 01/15/2010  
 JOB No: 08075-05-20  
 ILLINOIS PROJECT: IKK-3902  
 A.I.P. PROJECT: 3-17-0057-B17  
**FINAL**

SHEET 11 OF 20 SHEETS

DATE: Thursday, February 04, 2010 11:22:32 AM  
 FILE: K:\Kankakee\080756520 Term Apron Ph 2\Draw Sheets\12-grade-3.dwg

UPDATE BY: Jim Ohse  
 LAYOUT: Layout1

REF: DWG: 12-grade.dwg  
 DATE: 08-05-05-20.dwg

IMAGE FILES:  
 :STILL\_E3W1

DATE: Thursday, February 04, 2010 11:22:32 AM  
 FILE: K:\Kankakee\080756520 Term Apron Ph 2\Draw Sheets\12-grade-3.dwg

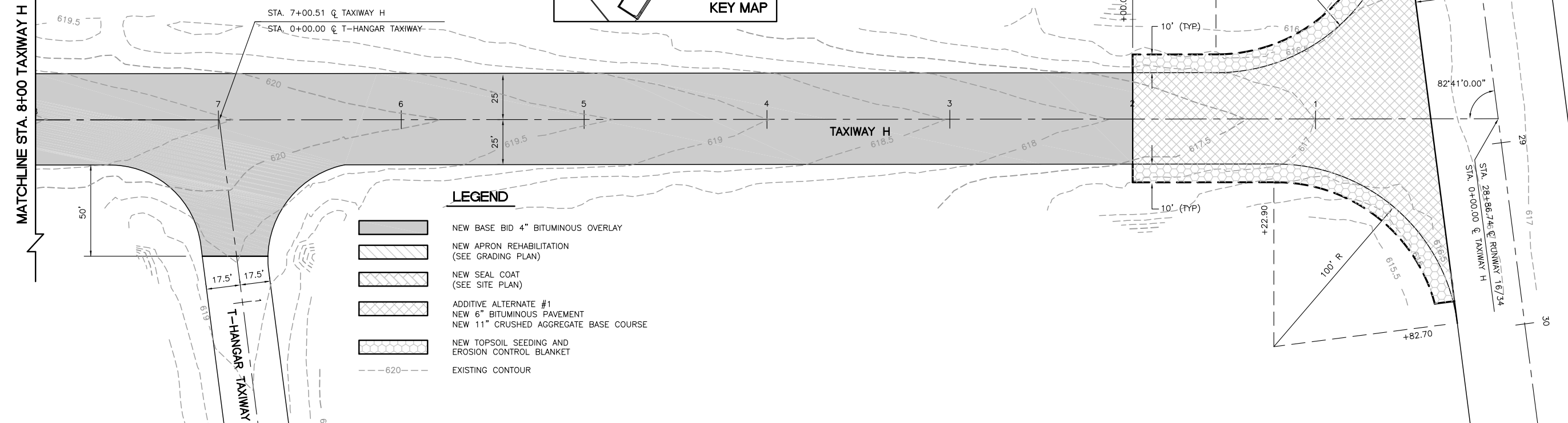
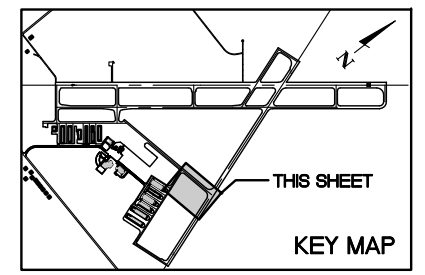
DATE: Thursday, February 04, 2010 11:22:32 AM  
 FILE: K:\Kankakee\080756520 Term Apron Ph 2\Draw Sheets\12-grade-3.dwg

DATE: Thursday, February 04, 2010 11:22:32 AM  
 FILE: K:\Kankakee\080756520 Term Apron Ph 2\Draw Sheets\12-grade-3.dwg

DATE: Thursday, February 04, 2010 11:22:32 AM  
 FILE: K:\Kankakee\080756520 Term Apron Ph 2\Draw Sheets\12-grade-3.dwg

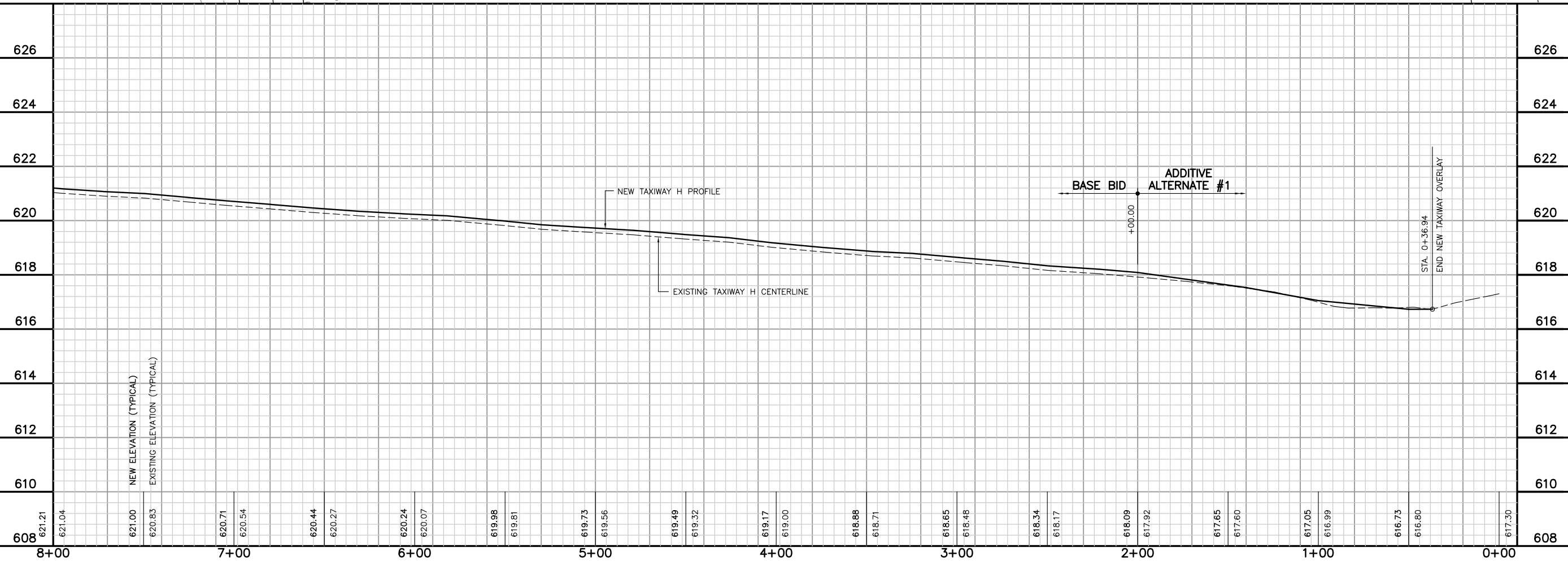
DATE: Thursday, February 04, 2010 11:22:32 AM  
 FILE: K:\Kankakee\080756520 Term Apron Ph 2\Draw Sheets\12-grade-3.dwg

DATE: Thursday, February 04, 2010 11:22:32 AM  
 FILE: K:\Kankakee\080756520 Term Apron Ph 2\Draw Sheets\12-grade-3.dwg



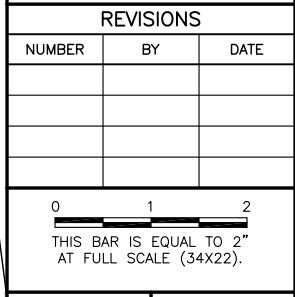
**LEGEND**

	NEW BASE BID 4" BITUMINOUS OVERLAY
	NEW APRON REHABILITATION (SEE GRADING PLAN)
	NEW SEAL COAT (SEE SITE PLAN)
	ADDITIVE ALTERNATE #1 NEW 6" BITUMINOUS PAVEMENT NEW 11" CRUSHED AGGREGATE BASE COURSE
	NEW TOPSOIL SEEDING AND EROSION CONTROL BLANKET
	EXISTING CONTOUR



SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

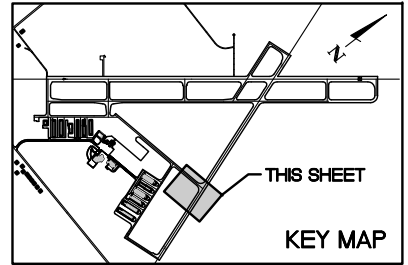
**PLAN AND PROFILE - TAXIWAY H  
 SHEET 2**

© Copyright CMT, Inc.

**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	12 OF 20 SHEETS

DATE: Thursday, February 04, 2010 11:23:08 AM  
 FILE: K:\Vandeeep\080750520 Term Apron Ph. 2\Draw\Sheets\p2-grade-intersection.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 IMAGE FILES:  
 XREF DWG: Terminal-base-2.dwg  
 H:\M\11-08075-0-20.dwg  
 6/14/09



KA041

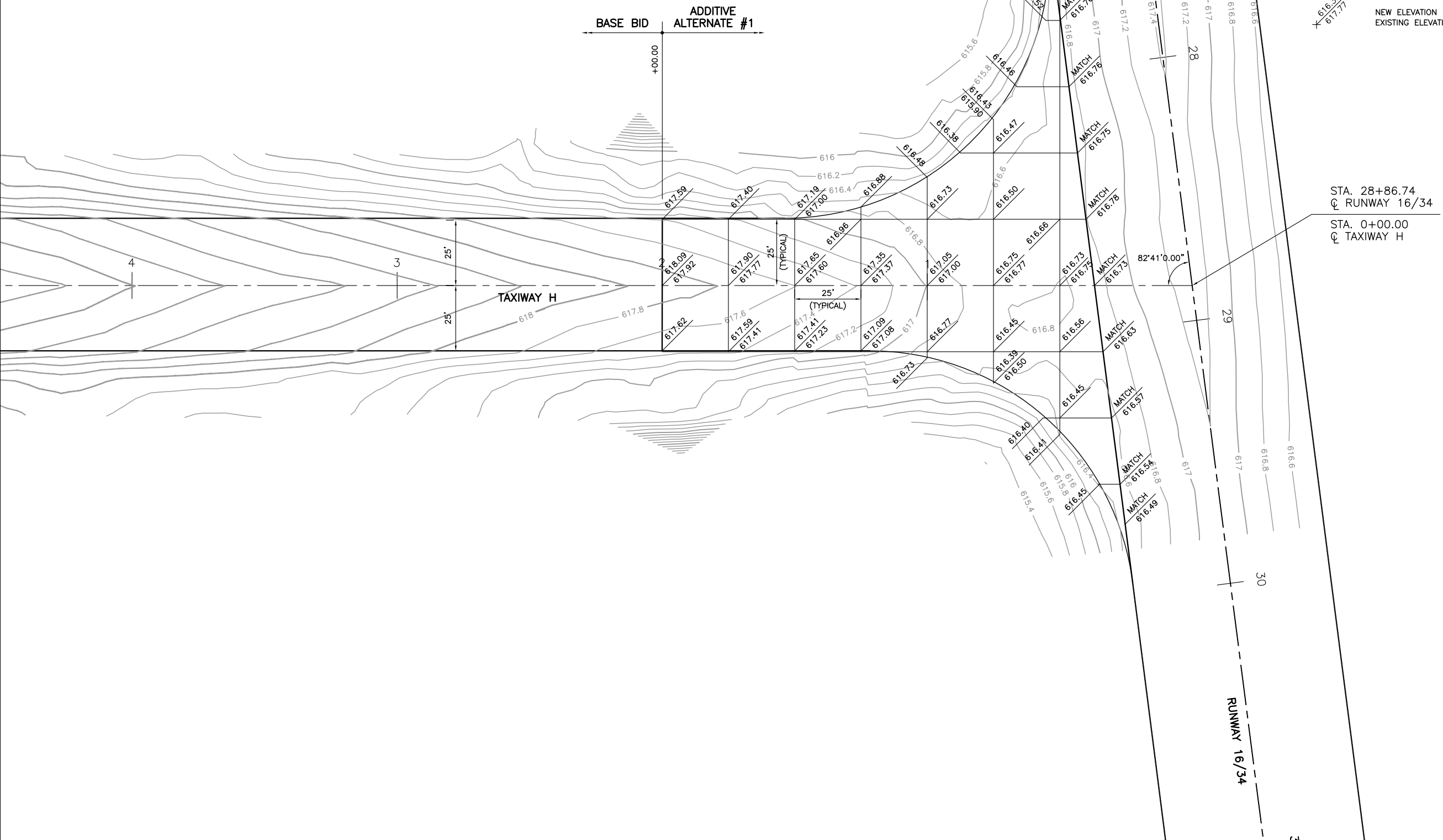
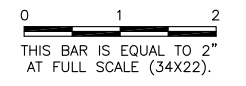


LEGEND

- 616 — EXISTING CONTOUR
- $\frac{616.38}{617.77}$  NEW ELEVATION (616.38)  
EXISTING ELEVATION (617.77)

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



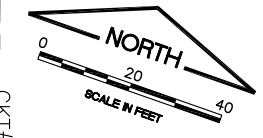
GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2

INTERSECTION GRADE DETAIL

© Copyright G.M.T., Inc.  
**G.M.T.**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

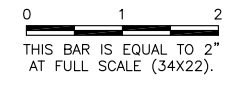
DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT: IKK-3902 A.I.P. PROJECT: 3-17-0057-B17	
<b>FINAL</b>	
SHEET	13 OF 20 SHEETS

KA041



SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

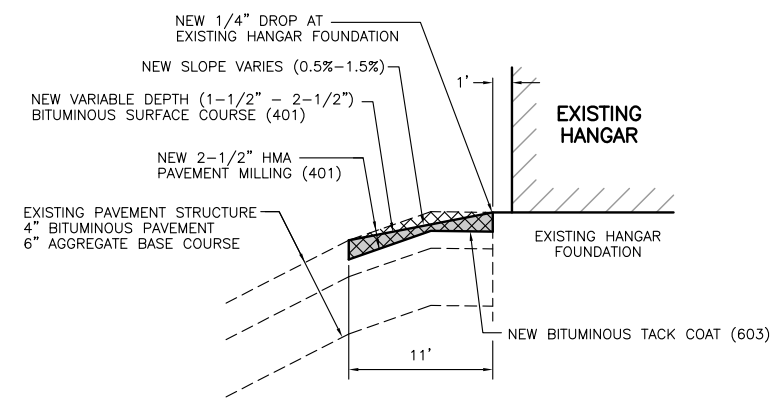
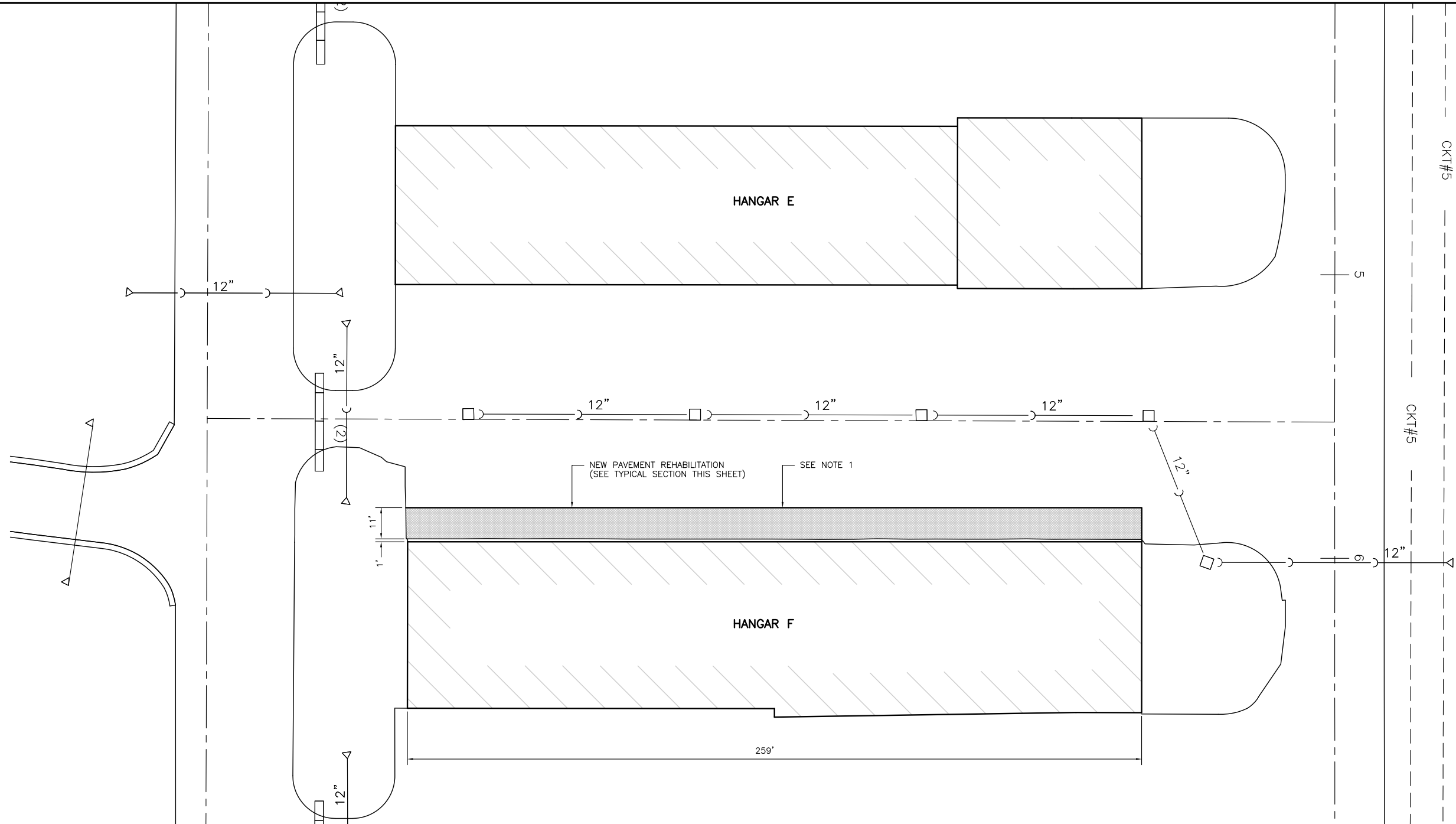


GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2

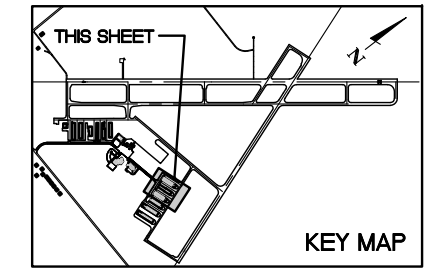
HANGAR PAVEMENT REHABILITATION PLAN

© Copyright CMT, Inc.  
**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	14 OF 20 SHEETS



NOTE: THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.



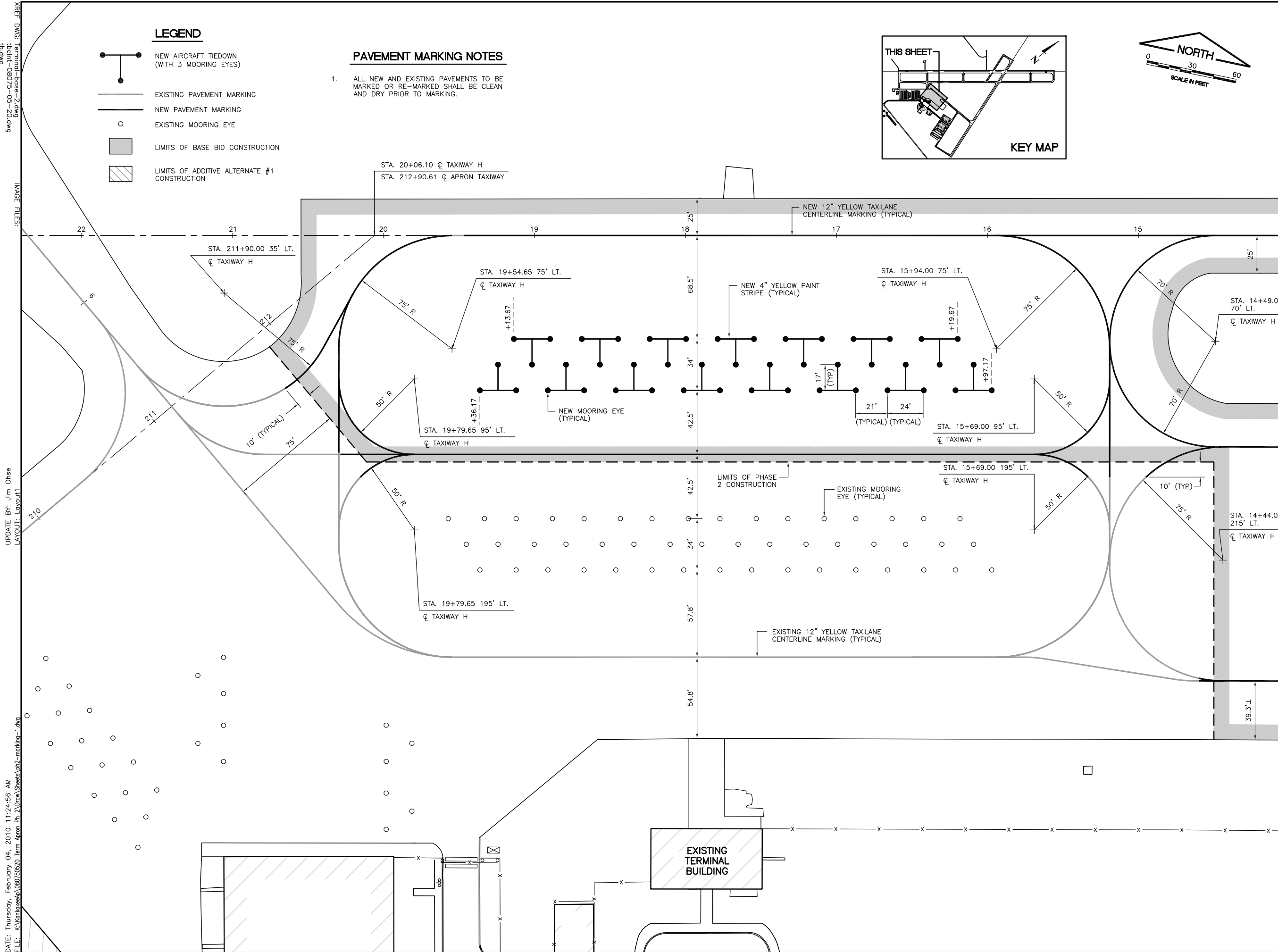
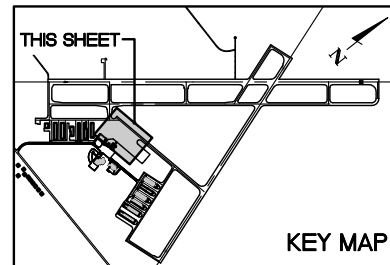
DATE: Thursday, February 04, 2010 11:24:00 AM  
 FILE: K:\Vandenberg\080750520 Term Apron Ph 2\Draw\Sheets\p12-hangar-pvt.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 IMAGE FILES:  
 REF DWG: Terminal-base-2.dwg  
 REF DWG: Terminal-base-2.dwg  
 REF DWG: 18.dwg

**LEGEND**

- NEW AIRCRAFT TIEDOWN (WITH 3 MOORING EYES)
- EXISTING PAVEMENT MARKING
- NEW PAVEMENT MARKING
- EXISTING MOORING EYE
- LIMITS OF BASE BID CONSTRUCTION
- LIMITS OF ADDITIVE ALTERNATE #1 CONSTRUCTION

**PAVEMENT MARKING NOTES**

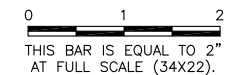
1. ALL NEW AND EXISTING PAVEMENTS TO BE MARKED OR RE-MARKED SHALL BE CLEAN AND DRY PRIOR TO MARKING.



SURVEY BOOK # BOOK #

**REVISIONS**

NUMBER	BY	DATE



GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2

TIEDOWN/PAVEMENT MARKING PLAN  
SHEET 1

© Copyright: CMT, Inc.  
**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

DESIGN BY: CAL

DRAWN BY: JRO

CHECKED BY: CAL

APPROVED BY: DKP

DATE: 01/15/2010

JOB No: 08075-05-20

ILLINOIS PROJECT: IKK-3902  
A.I.P. PROJECT: 3-17-0057-B17

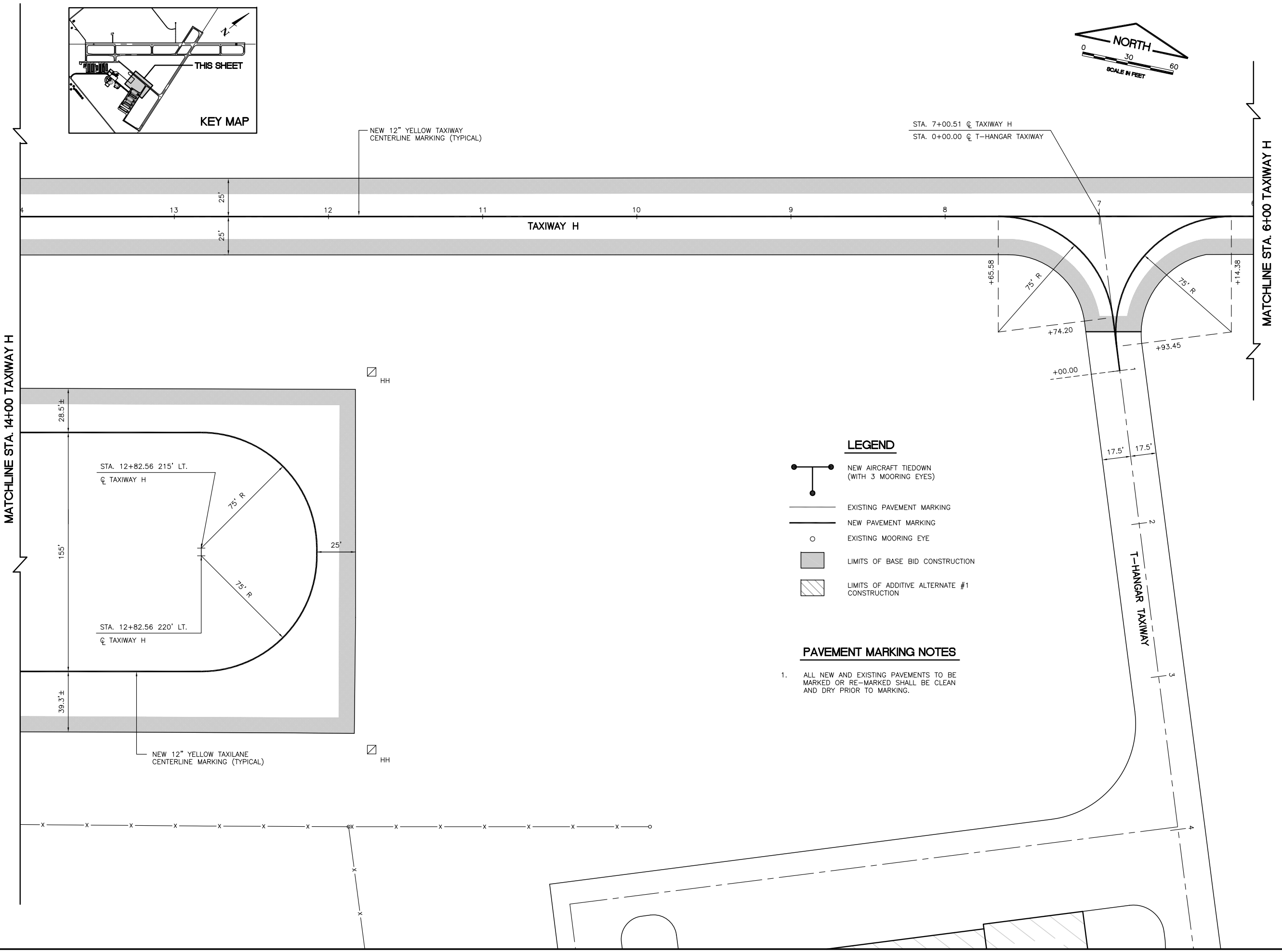
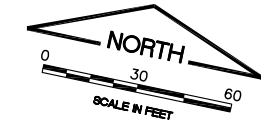
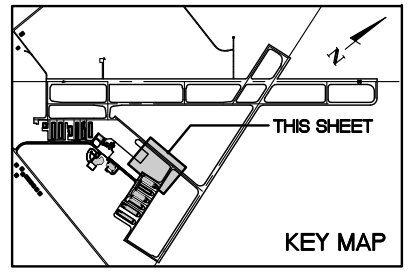
**FINAL**

SHEET 15 OF 20 SHEETS

DATE: Thursday, February 04, 2010 11:24:56 AM  
 FILE: K:\Kankakee\08075020 Term Apron Ph 2\Drawn Sheets\ph2-marking-1.dwg  
 UPDATE BY: Jim Onse  
 LAYOUT: Layout1  
 IMAGE FILES:  
 REF: DMC: Terminal Base 2.dwg  
 6MP: 02-CO-CLOSED-WHICH  
 11:24:56 AM

DATE: Thursday, February 04, 2010 11:25:44 AM  
 FILE: K:\Vandenberg\080750520 Term Apron Ph. 2\Draw\Sheets\p2-marking-2.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 REF: DWG: Terminal\_base-2.dwg  
 Pict: 080750520-2.dwg  
 IMAGE FILES:

KA041



**LEGEND**

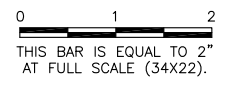
- NEW AIRCRAFT TIEDOWN (WITH 3 MOORING EYES)
- EXISTING PAVEMENT MARKING
- NEW PAVEMENT MARKING
- EXISTING MOORING EYE
- LIMITS OF BASE BID CONSTRUCTION
- LIMITS OF ADDITIVE ALTERNATE #1 CONSTRUCTION

**PAVEMENT MARKING NOTES**

1. ALL NEW AND EXISTING PAVEMENTS TO BE MARKED OR RE-MARKED SHALL BE CLEAN AND DRY PRIOR TO MARKING.

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

**TIEDOWN/PAVEMENT MARKING PLAN  
 SHEET 2**

© Copyright CMT, Inc.

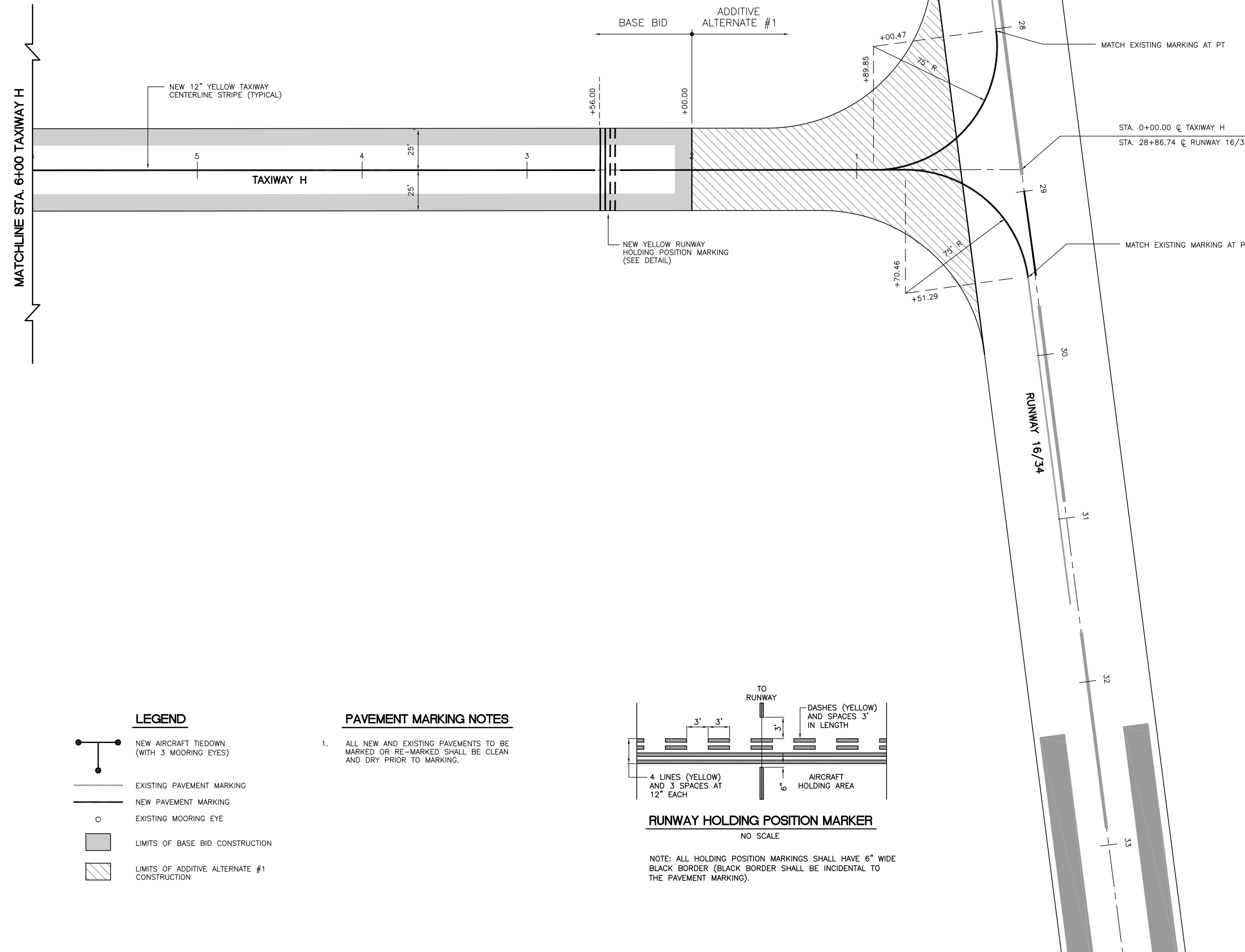
**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT: IKK-3902 A.I.P. PROJECT: 3-17-0057-B17	
<b>FINAL</b>	
SHEET	16 OF 20 SHEETS



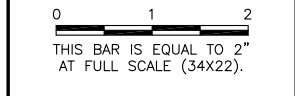
DATE: Thursday, February 04, 2010 11:26:31 AM  
 FILE: R:\Kankakee\08075-05-20\Drawn\Sheets\12-marking-3.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 IMAGE FILES:  
 REF: DWG: Terminal-base-2.dwg  
 6xw

KA041



SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

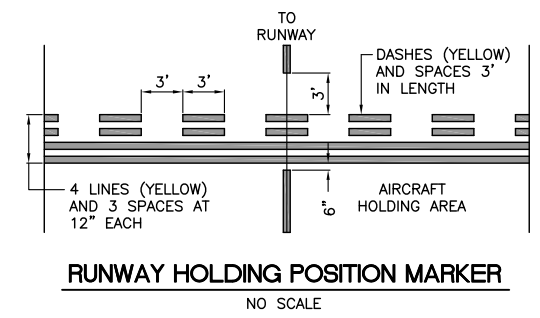
**TIEDOWN/PAVEMENT MARKING PLAN  
 SHEET 3**

© Copyright G.M.T., Inc.  
**G.M.T.**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

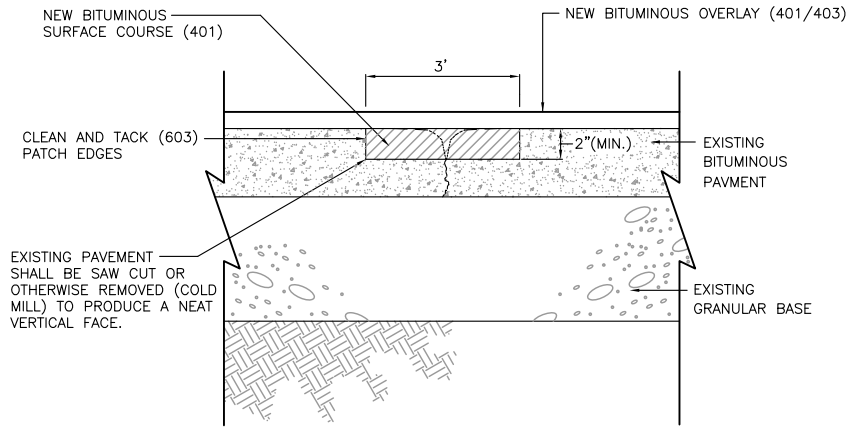
DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	17 OF 20 SHEETS

- LEGEND**
- NEW AIRCRAFT TIEDOWN (WITH 3 MOORING EYES)
  - EXISTING PAVEMENT MARKING
  - NEW PAVEMENT MARKING
  - EXISTING MOORING EYE
  - LIMITS OF BASE BID CONSTRUCTION
  - LIMITS OF ADDITIVE ALTERNATE #1 CONSTRUCTION

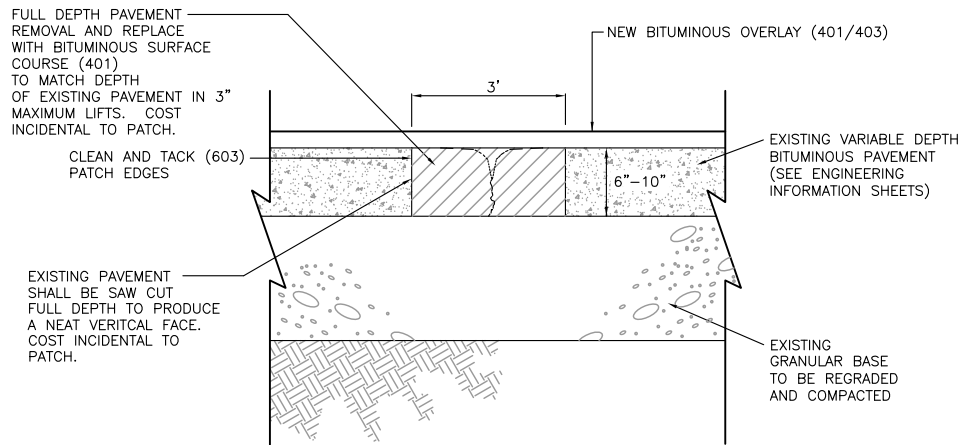
- PAVEMENT MARKING NOTES**
- ALL NEW AND EXISTING PAVEMENTS TO BE MARKED OR RE-MARKED SHALL BE CLEAN AND DRY PRIOR TO MARKING.



NOTE: ALL HOLDING POSITION MARKINGS SHALL HAVE 6" WIDE BLACK BORDER (BLACK BORDER SHALL BE INCIDENTAL TO THE PAVEMENT MARKING).



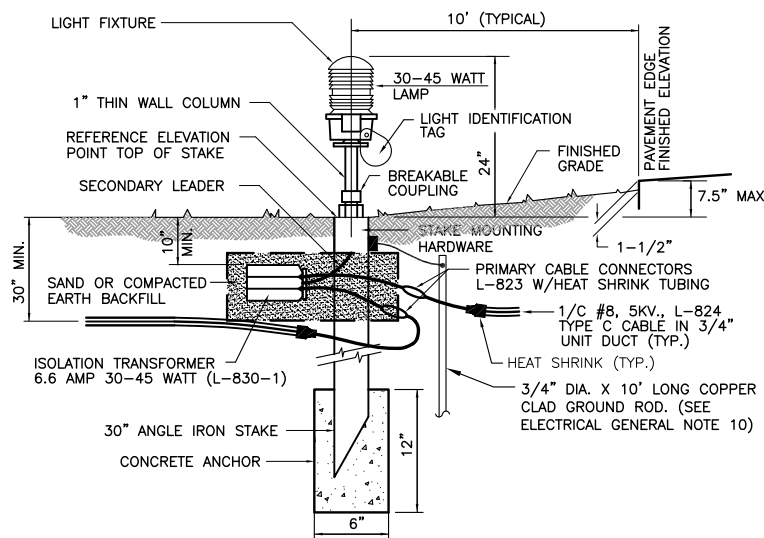
**REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A**  
 NOT TO SCALE



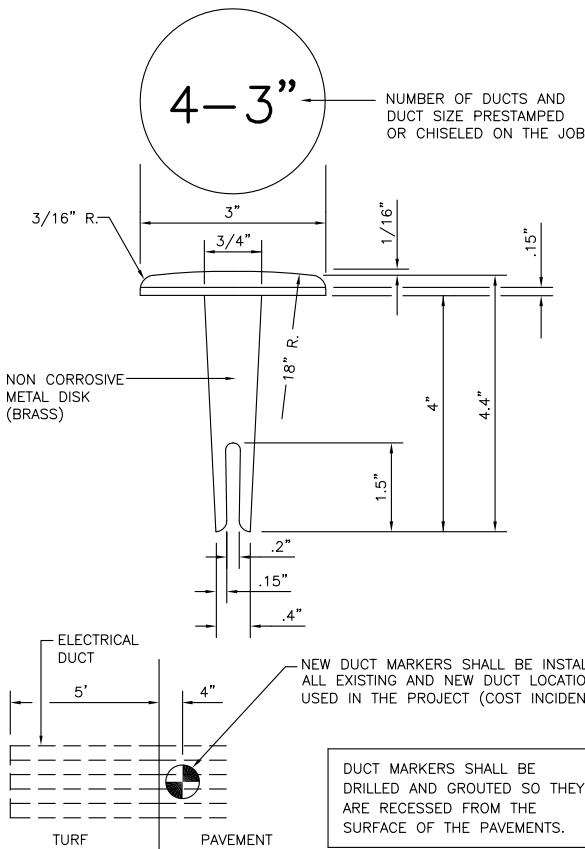
**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B**  
 NOT TO SCALE

**PAVEMENT REMOVAL AND REPLACEMENT NOTES**

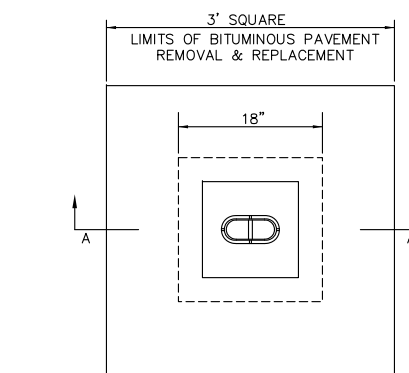
1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT APPROXIMATELY 150' INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.



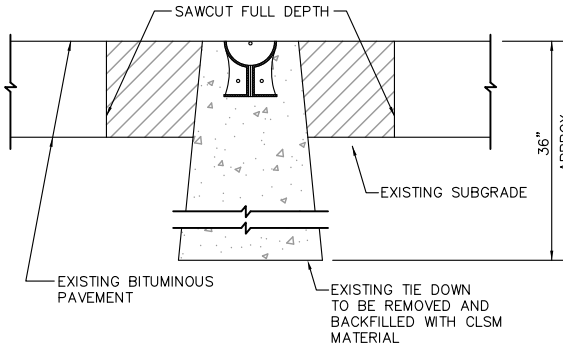
**ADJUST EXISTING STAKE MOUNTED MEDIUM INTENSITY LIGHT**  
 NOT TO SCALE



**DUCT MARKER DETAIL**  
 NOT TO SCALE



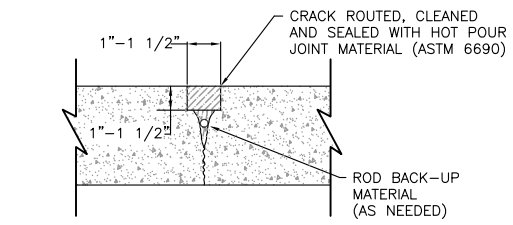
**PLAN**



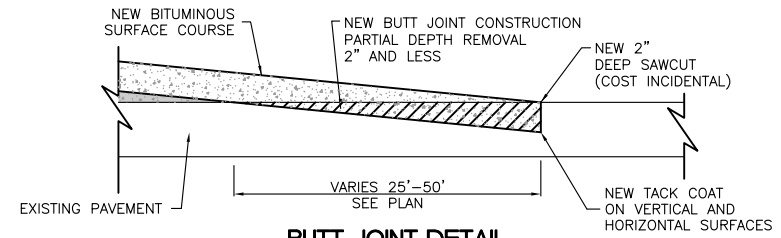
**TIE DOWN REMOVAL DETAIL**  
 NOT TO SCALE

**NOTES:**

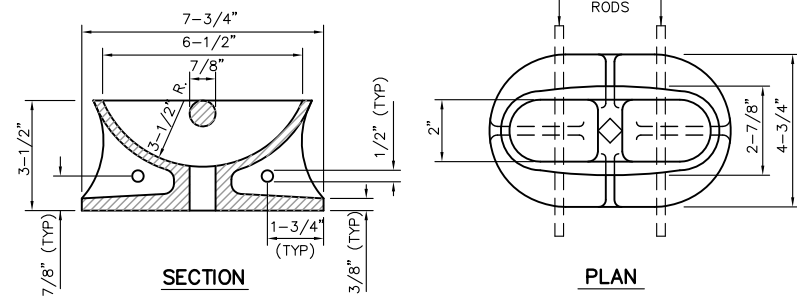
1. THE SAWCUT AND BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE MOORING EYE REMOVAL.
2. THE VOID LEFT BY THE REMOVAL SHALL BE BACKFILLED TO 9" BELOW EXISTING PAVEMENT SURFACE WITH CLSM MATERIAL. THE FINAL 9" SHALL BE FILLED WITH BITUMINOUS SURFACE COURSE TO MATCH THE MILLED SURFACE.



**REPAIR METHOD - CRACK ROUTING AND SEALING**  
 NOT TO SCALE



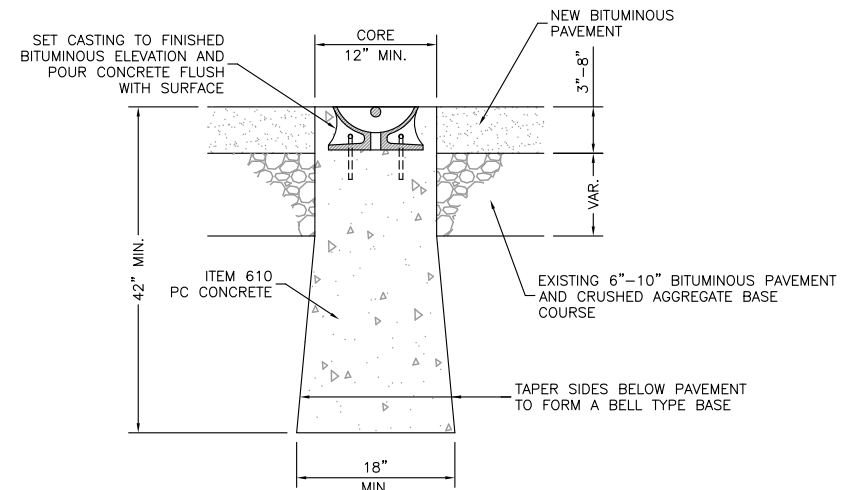
**BUTT JOINT DETAIL**  
 NOT TO SCALE



**MOORING EYE DETAIL**  
 NOT TO SCALE

**NOTES:**

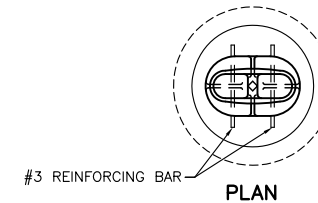
1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS TO BE NO. 3 DEFORMED REINFORCING STEEL 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.



**SECTION**

**NOTE:**

1. THE CONTRACTOR SHALL CORE OR SAW THROUGH BITUMINOUS OVERLAY AND EXISTING BITUMINOUS PAVEMENT AND ENLARGE HOLE TO DIMENSIONS SHOWN.



**PLAN**

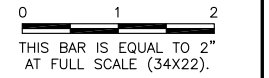
**MOORING EYE INSTALLATION DETAIL**  
 NOT TO SCALE

**KA041**

SURVEY BOOK # BOOK #

**REVISIONS**

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

**MISCELLANEOUS DETAILS**

© Copyright G.M.T. Inc.  
**G.M.T.**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613



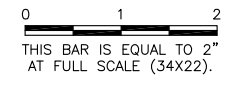
DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20

ILLINOIS PROJECT: IKK-3902  
 A.I.P. PROJECT: 3-17-0057-B17

**FINAL**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

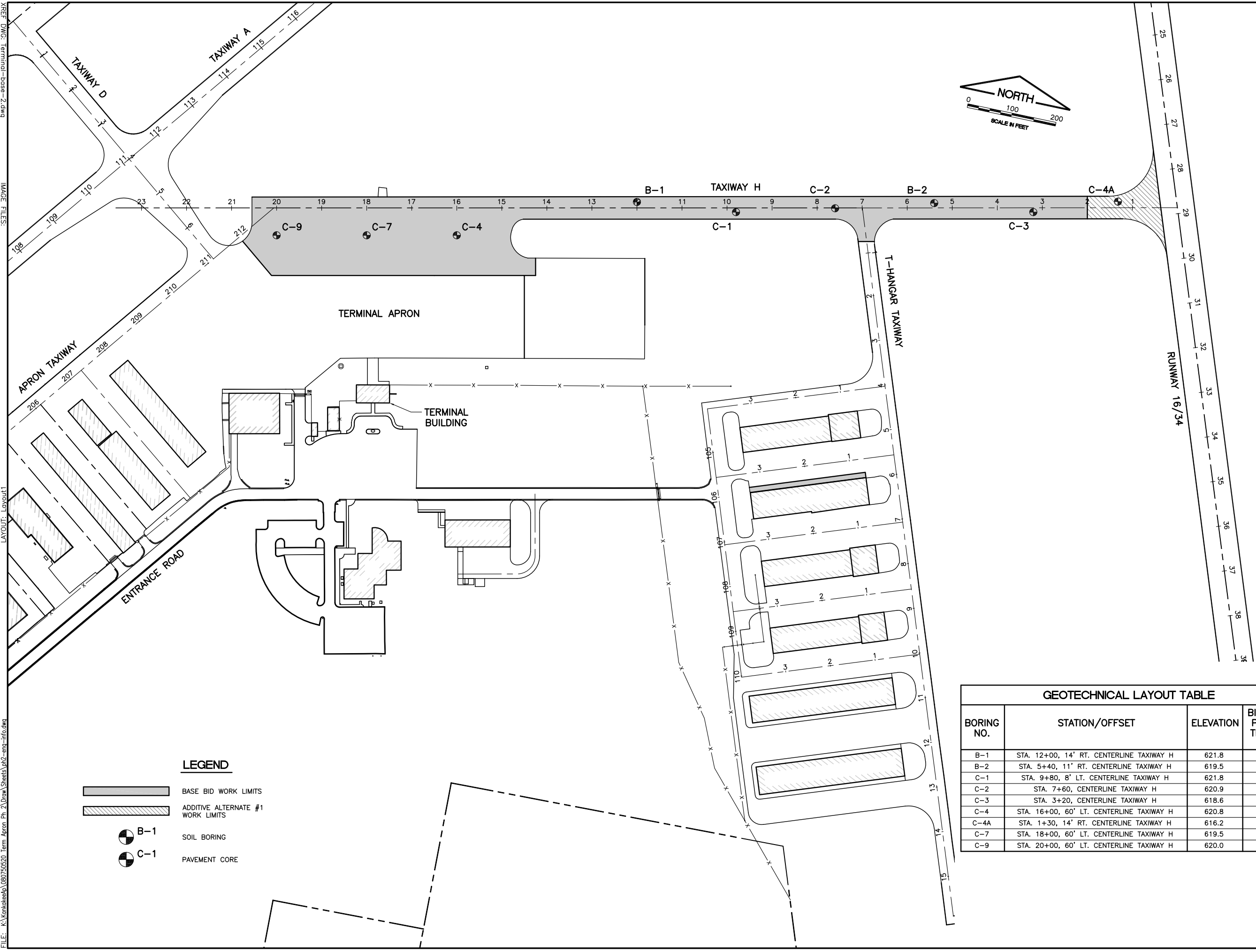
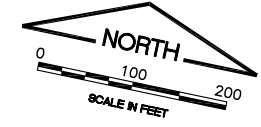


**GREATER KANKAKEE AIRPORT  
KANKAKEE, ILLINOIS  
REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2**

**ENGINEERING INFORMATION**

© Copyright CMT, Inc.  
**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	19 OF 20 SHEETS



BORING NO.	STATION/OFFSET	ELEVATION	BITUMINOUS PAVEMENT THICKNESS (IN)
B-1	STA. 12+00, 14' RT. CENTERLINE TAXIWAY H	621.8	6.0"
B-2	STA. 5+40, 11' RT. CENTERLINE TAXIWAY H	619.5	6.0"
C-1	STA. 9+80, 8' LT. CENTERLINE TAXIWAY H	621.8	7.3"
C-2	STA. 7+60, CENTERLINE TAXIWAY H	620.9	7.4"
C-3	STA. 3+20, CENTERLINE TAXIWAY H	618.6	6.2"
C-4	STA. 16+00, 60' LT. CENTERLINE TAXIWAY H	620.8	8.7"
C-4A	STA. 1+30, 14' RT. CENTERLINE TAXIWAY H	616.2	5.7"
C-7	STA. 18+00, 60' LT. CENTERLINE TAXIWAY H	619.5	9.8"
C-9	STA. 20+00, 60' LT. CENTERLINE TAXIWAY H	620.0	7.9"

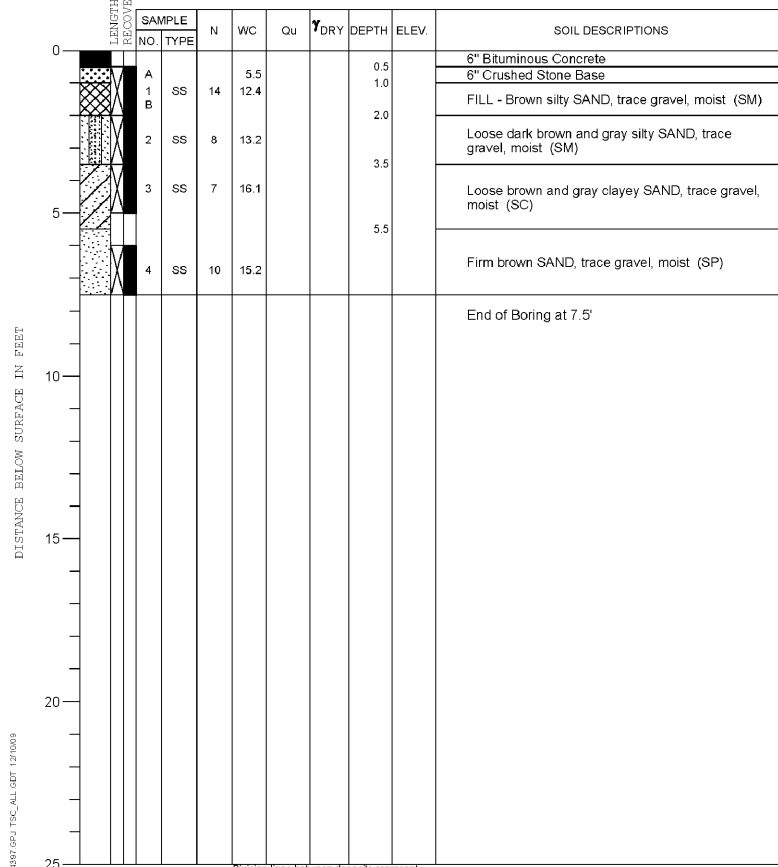
**LEGEND**

- BASE BID WORK LIMITS
- ADDITIVE ALTERNATE #1 WORK LIMITS
- B-1 SOIL BORING
- C-1 PAVEMENT CORE

DATE: Thursday, February 04, 2010 11:28:23 AM  
 FILE: K:\Kankakee\08075\0520 Term Apron Ph 2\Drawn\Sheets\19-ang-info.dwg  
 UPDATE BY: Jim Ohse  
 LAYOUT: Layout1  
 IMAGE FILES:  
 REF: DWG: Terminal-base-2.dwg  
 19.dwg



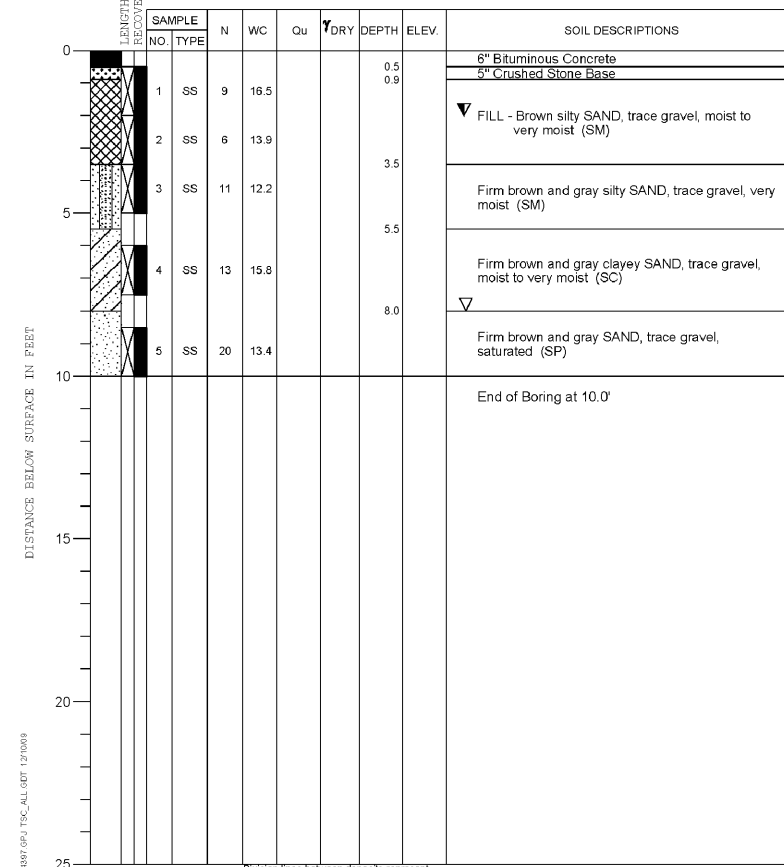
PROJECT **Greater Kankakee Airport, Taxiway H, Kankakee, Illinois**  
 CLIENT **Crawford, Murphy & Tilly, Inc., Aurora, Illinois**  
 BORING **1** DATE STARTED **11-20-09** DATE COMPLETED **11-20-09** JOB **L-74,397**  
 ELEVATIONS WATER LEVEL OBSERVATIONS  
 GROUND SURFACE \_\_\_\_\_ WHILE DRILLING **Dry**  
 END OF BORING \_\_\_\_\_ AT END OF BORING **Dry**  
 \_\_\_\_\_ 24 HOURS \_\_\_\_\_



DRILL RIG NO. **315**  
 Division lines between deposits represent approximate boundaries between soil types; in-situ, the transition may be gradual.



PROJECT **Greater Kankakee Airport, Taxiway H, Kankakee, Illinois**  
 CLIENT **Crawford, Murphy & Tilly, Inc., Aurora, Illinois**  
 BORING **2** DATE STARTED **11-20-09** DATE COMPLETED **11-20-09** JOB **L-74,397**  
 ELEVATIONS WATER LEVEL OBSERVATIONS  
 GROUND SURFACE \_\_\_\_\_ WHILE DRILLING **2.0'**  
 END OF BORING \_\_\_\_\_ AT END OF BORING **8.0'**  
 \_\_\_\_\_ 24 HOURS \_\_\_\_\_

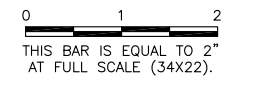


DRILL RIG NO. **315**  
 Division lines between deposits represent approximate boundaries between soil types; in-situ, the transition may be gradual.

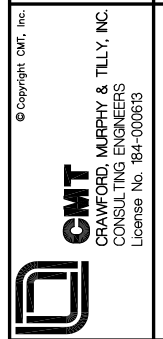
KA041

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON AND TAXIWAY H - PHASE 2  
 ENGINEERING INFORMATION  
 SHEET 2**



DESIGN BY:	CAL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
DATE:	01/15/2010
JOB No:	08075-05-20
ILLINOIS PROJECT:	IKK-3902
A.I.P. PROJECT:	3-17-0057-B17
<b>FINAL</b>	
SHEET	20 OF 20 SHEETS

PAVEMENT CORE SUMMARY (EACH COMPONENT OF PAVEMENT SECTION LISTED FROM TOP DOWN)			
CORE NO.	THICKNESS (IN)	MATERIAL DESCRIPTION	NOTE
C-1	1.4	BITUMINOUS SURFACE COURSE	
	1.1	BITUMINOUS SURFACE COURSE	
	2.5	BITUMINOUS BINDER COURSE	FABRIC BENEATH THIS LAYER
	2.3	EMULSIFIED BITUMINOUS CONCRETE	
	<b>7.3</b>	<b>TOTAL PAVEMENT THICKNESS</b>	
C-2	7.0	CRUSHED STONE	2" TO FINE
	1.3	BITUMINOUS SURFACE COURSE	
	1.3	BITUMINOUS SURFACE COURSE	NOT BONDED
	2.9	BITUMINOUS BINDER COURSE	FABRIC BENEATH THIS LAYER
	1.9	EMULSIFIED BITUMINOUS CONCRETE	
	<b>7.4</b>	<b>TOTAL PAVEMENT THICKNESS</b>	
C-3	7.0	CRUSHED STONE	2" TO FINE
	2.2	BITUMINOUS SURFACE COURSE	
	1.8	BITUMINOUS SURFACE COURSE	FABRIC BENEATH THIS LAYER
	2.2	EMULSIFIED BITUMINOUS CONCRETE	
	<b>6.2</b>	<b>TOTAL PAVEMENT THICKNESS</b>	
C-4	7.0	CRUSHED STONE	2" TO FINE
	1.6	BITUMINOUS SURFACE COURSE	
	1.9	BITUMINOUS SURFACE COURSE	
	0.9	BITUMINOUS SURFACE COURSE	
	2.0	BITUMINOUS SURFACE COURSE	NOT BONDED
	2.3	BITUMINOUS BASE COURSE	
	<b>8.7</b>	<b>TOTAL PAVEMENT THICKNESS</b>	
C-4A	9.3	CRUSHED LIMESTONE	MAX. SIZE 1 INCH TO FINES
	2.1	BITUMINOUS SURFACE COURSE	
	1.8	BITUMINOUS SURFACE COURSE	
	1.8	EMULSIFIED BITUMINOUS CONCRETE	
	<b>5.7</b>	<b>TOTAL PAVEMENT THICKNESS</b>	
	6.0	CRUSHED STONE	2" TO FINE

PAVEMENT CORE SUMMARY (EACH COMPONENT OF PAVEMENT SECTION LISTED FROM TOP DOWN)			
CORE NO.	THICKNESS (IN)	MATERIAL DESCRIPTION	NOTE
C-7	1.3	BITUMINOUS SURFACE COURSE	
	1.5	BITUMINOUS SURFACE COURSE	
	1.9	BITUMINOUS SURFACE COURSE	
	0.6	BITUMINOUS SURFACE COURSE	
	2.0	BITUMINOUS BASE COURSE	NOT BONDED
	2.5	BITUMINOUS BASE COURSE	
	<b>9.8</b>	<b>TOTAL PAVEMENT THICKNESS</b>	
C-9	9.0	CRUSHED LIMESTONE	MAX. SIZE 1 INCH
	1.3	BITUMINOUS SURFACE COURSE	NOT BONDED
	0.7	BITUMINOUS SURFACE COURSE	
	1.0	BITUMINOUS SURFACE COURSE	
	0.9	BITUMINOUS SURFACE COURSE	NOT BONDED
	1.6	BITUMINOUS BASE COURSE	
	2.4	BITUMINOUS BASE COURSE	
	<b>7.9</b>	<b>TOTAL PAVEMENT THICKNESS</b>	
	9.5	CRUSHED LIMESTONE	MAX. SIZE 1 INCH TO FINES