ITEM 4A

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF AERONAUTICS

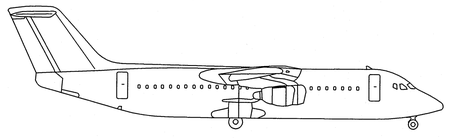
CONSTRUCTION PLANS
FOR

QUAD-CITY INTERNATIONAL AIRPORT

ROCK ISLAND COUNTY, ILLINOIS

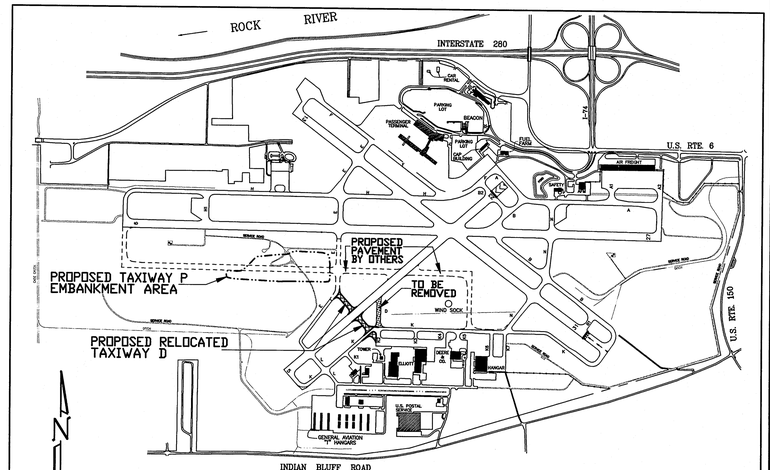
TAXIWAY D RELOCATION

REMOVE EXISTING TAXIWAY D (524' X 75') PAVEMENT AND CONSTRUCT PROPOSED TAXIWAY D (422' X 75' AND 492' X 75') PAVEMENT INCLUDING EARTHWORK, LIGHTING, UNDERDRAINS, MARKING, UTILITY ADJUSTMENTS AND TURFING. ALSO CONSTRUCT TAXIWAY P SAFETY AREA EMBANKMENT.



ILLINOIS PROJECT MLI-3943
A.I.P. PROJECT NO. 3-17-0068-66
AIRPORT CLASSIFICATION – AIR CARRIER

AIRCRAFT APPROACH CATEGORY – C
AIRPLANE DESIGN GROUP – III
LATITUDE 41°26'52.4", LONGITUDE 90°30'33.9", ELEVATION 589'M.S.L.



PROJECT LOCATION MAP

WARNING

CALL J.U.L.I.E. TOLL FREE 800-892-0123

CALL BEFORE YOU DIG

CONSULTING ENGINEERS
MISSMAN, STANLEY & ASSOC.

ENGINEER'S SIGNATURE Dennis R. Martin
SEALED & SIGNED 1/28/10
DATE OF LICENSE EXPIRATION 11/30/11

DENNIS R. MARTIN
62-32026
REGISTERED PROFESSIONAL ENGINEER
OF
ILLINOIS

METROPOLITIAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY, ILLINOIS

1/28/10 *DATE APPROVED*

JANUARY 15, 2010
OFFICIAL DATE OF PLANS

PREPARED BY
MISSMAN, STANLEY & ASSOCIATES
Consulting Civil Engineers
ROCK ISLAND, ILLINOIS

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	4,560	
AR108258	2/C #8 5 KV UG CABLE IN UD	L.F.	1,695	
AR110502	2-WAY CONCRETE ENCASED DUCT	L.F.	260	
AR110710	ELECTRICAL MANHOLE	EACH	1	
AR110901	CONCRETE DUCT REMOVAL	L.F.	115	
AR125415	MITL-BASE MOUNTED	EACH	19	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	4	
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	3	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	2	
AR125448	TAXI GUIDANCE SIGN, 8 CHARACTER	EACH	3	
AR125449	TAXI GUIDANCE SIGN, 9 CHARACTER	EACH	1	
AR125565	SPLICE CAN	EACH	6	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	7	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	34	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	5,650	
AR152442	OFFSITE BORROW EXCAVATION	C.Y.	36,550	
AR156500	TEMPORARY EROSION CONTROL	L.S.	1	
AR156540	RIPRAP	S.Y.	380	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	5,320	
AR209600	GEOTEXTILE FABRIC	S.Y.	10,915	
AR401610	BITUMINOUS SURFACE COURSE	TON	230	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	25	
AR501508	8" PCC PAVEMENT	S.Y.	3,695	
AR501510	10" PCC PAVEMENT	S.Y.	6,030	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	S.Y.	6,140	
AR602510	BITUMINOUS PRIME COAT	GAL.	185	
AR620510	PAVEMENT MARKING	S.F.	8,025	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	670	
AR701318	18" RCP, CLASS II	L.F.	72	
AR701442	42" RCP, CLASS III	L.F.	62	
AR701512	12" RCP, CLASS IV	L.F.	109	
AR701710	RCEP SPAN 23 RISE 14	L.F.	193	
AR705506	6" PERFORATED UNDERDRAIN	L.F.	2,725	
AR751415	INLET-SPECIAL	EACH	2	
AR751570	MANHOLE - SPECIAL	EACH	11	
AR751903	REMOVE MANHOLE	EACH	5	
AR752442	PRECAST REINFORCED CONC. FES 42"	EACH	2	
AR801605	REPLACE TAXI GUIDANCE SIGN PANEL	EACH	7	
AR801614	SUPPLY TAXI GUIDANCE SIGN PANEL	EACH	20	
AR801629	REMOVE RETROREFLECTIVE MARKER	EACH	1	
AR901510	SEEDING	ACRE	23	
AR908513	MULCHING - METHOD 3	ACRE	23	
AR908520	EXCELSIOR BLANKET	S.Y.	1,500	

GENERAL NOTES:

1. MAXIMUM PAY WIDTH FOR 209510 CRUSHED AGGREGATE BASE COURSE SHALL BE 12 INCHES BEYOND THE EDGE OF PAVEMENT. IF THE CONTRACTOR REQUIRES ADDITIONAL WIDTH FOR PAVEMENT INSTALLATION, THE ADDITIONAL MATERIALS SHALL MEET THE SAME SPECIFICATIONS, BUT WILL BE CONSIDERED INCIDENTAL.
2. THE CONTRACTOR SHALL SALVAGE EXISTING AIRFIELD LIGHTING EQUIPMENT AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS PRIOR TO THE START OF EARTHWORK AND/OR PAVING ACTIVITIES. SALVAGED EQUIPMENT SHALL BE CLEANED AND REUSED OR DELIVERED TO THE METROPOLITAN AIRPORT AUTHORITY.
3. CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS APPROVED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
4. THE CONTRACTOR SHALL EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALES, AS REQUIRED BY THE RESIDENT ENGINEER, TO CONTROL STORM WATER RUN-OFF.
5. THE CONTRACT AR152410-UNCLASSIFIED EXCAVATION / AR152442-OFFSITE BORROW EXCAVATION ITEMS SHALL INCLUDE ALL COSTS ASSOCIATED WITH EXCAVATION OF SOILS, HAULING OF SOILS, STOCKPILING SOILS, INSTALLATION OF SOILS, COMPACTING OF SOILS, GRADING OF SOILS, INSTALLATION AND REMOVAL OF HAUL ROADS OR ROUTES, RESTORATION OF HAUL ROADS OR ROUTES, DISPOSAL OF WASTE SOILS, CLEANING OF PAVEMENTS, AND ALL OTHER ITEMS THAT ARE REQUIRED TO COMPLETE THE EARTHWORK. THESE ITEMS SHALL BE PAID FOR BASED UPON THE CUBIC YARDS OF MATERIALS REMOVED AS ACCEPTED BY THE RESIDENT ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
6. AFTER CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEED AND HYDRAULIC MULCH ALL DISTURBED AREAS PER SPECIAL PROVISIONS 901 AND 908. ONLY SEED AND MULCHING AREAS WITHIN THE LIMITS OF CONSTRUCTION/SEEDING WILL BE ELIGIBLE FOR PAYMENT UNDER THESE CONTRACT PAY ITEMS. AREAS OUTSIDE OF THE LIMITS OF CONSTRUCTION/SEEDING SHALL BE SEEDED AND MULCHED BY THE CONTRACTOR PER SPECIAL PROVISION 901/908, BUT SHALL NOT BE MEASURED FOR PAYMENT.
7. ITEM 908513 MULCHING - METHOD 3 SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE IDOT-DOA SUPPLEMENTAL SPECIFICATIONS FOR HYDRAULIC MULCHING AND IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, METHOD 3 (HYDRAULIC MULCH).
8. ITEM 908520 EXCELSIOR BLANKET SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, EROSION CONTROL BLANKET (EXCELSIOR BLANKET). THE LOCATION OF THE PROPOSED EXCELSIOR BLANKET SHALL BE DETERMINED BY THE RESIDENT ENGINEER, IN THE FIELD, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI–3943, QU013
SHEET 3 OF 69

NUMBERED LEGEND:

1. PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
2. DURING STAGE 2: CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES. KEEP PAVEMENT CLEAR OF FOD (FOREIGN OBJECT DEBRIS) AT ALL TIMES.
3. SEE SHEET 11 FOR PROPOSED MINIMUN UTILITY PROTECTION ALONG HAUL ROUTES.

LEGEND:

- = PROPOSED LIMITS OF EXCAVATION, EMBANKMENT, AND/OR CONSTRUCTION
- = EXISTING ROAD TO BE USED FOR HAUL ROUTE OR CONSTRUCTION ACCESS
- = PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD. SUPPLY & INSTALL TEMPORARY CULVERTS AS REQUIRED.
- = PROPOSED PAVEMENT BY OTHERS
- = PROPOSED BARRICADES, SEE NOTE 8 ON SHEET 5 AND NOTE 2 ON THIS SHEET
- = EXISTING RUNWAY / TAXIWAY SAFETY AREA
- = EXISTING RUNWAY RPZ
- = STAGE 1 AREA, T–P EMBANKMENT AREA (STA. 612+00 TO STA. 628+00) AND RELOCATED T–D NORTH OF R5–23 (STA. 761+00 TO STA. 766+90).
- = STAGE 2 AREA, RELOCATED T–D SOUTH OF R5–23 (STA. 766+90 TO STA. 774+00) INCLUDING REMOVAL OF OLD T–D PAVEMENT.
- = EARTHWORK BORROW AREAS
- = PROPOSED LATH LINES

ROCK RIVER
INTERSTATE 280
AIRPORT RD.
HOME ST.
69th AVE
RPZ
MTI
REILS
CAR RENTAL
PARKING LOT
BEACON
PASSENGER TERMINAL
NORTH APRON
CAP BUILDING
FUEL FARM
AIR FREIGHT
POND
SAFETY
AFM
INTERSTATE 74
U.S. RTE. 6
DEERE & CO.
RSA
RUNWAY 23 CLOSED
RUNWAY MARKING.
DME
27 LOCALIZER
9 MALSR
9 GLIDE SLOPE AND RVR
GS CRITICAL AREA
PROPOSED PAVEMENT BY OTHERS
PROPOSED T–P EMBANKMENT AREA CONSTRUCTION, SEEDING, AND GRADING LIMTS
PROPOSED T–D AREA CONSTRUCTION, SEEDING, AND GRADING LIMTS
PROPOSED PAVEMENT (BY OTHERS) UNDER CONSTRUCTION
STAGE 1 BARRICADES
STAGE 2 BARRICADES
WIND SOCK
L–880 PAPI
TOWER
ELLIOTT
HANGAR
U.S. POSTAL SERVICE
GENERAL AVIATION "T" HANGARS
SERVICE ROAD
9 LOCALIZER
27 MALSR
27 GLIDE SLOPE
REIL
U.S. RTE. 150
CASE CREEK
DITCH
PROPOSED TEMPORARY CONTRACTOR ACCESS ROUTE TO PROJECT SITE.
PROPOSED CONSTRUCTION BATCH PLANT AREA.
CRITICAL POINT # 3
RUNWAY 5 CLOSED RUNWAY MARKING.
AIRPORT OWNED INDUSTRIAL PARK
73rd AVE
77th AVE
47th ST
50th ST
68th ST
PROPOSED CONSTRUCTION ACCESS ROUTE.
PROPOSED TEMPORARY CONTRACTOR ACCESS ROUTE TO SOUTH BORROW AREA. SEE SHT 5 FOR CONTINUATION.
INDIAN BLUFF ROAD
SCALE 0 400 800 FEET

GENERAL NOTES:

1. SEE SHEETS 4 & 5 FOR SAFETY PLAN NOTES, CLOSED RUNWAY MARKING, AND STAGING DETAILS.
2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'c/c ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 5 FOR ADDITIONAL INFORMATION.
3. PROPOSED TEMPORARY CONSTRUCTION STAGING / BATCH PLANT AREA EQUIPMENT HEIGHT LIMIT = 50 FEET. AT ALL OTHER LOCATIONS: EQUIPMENT HEIGHT LIMIT = 25 FEET.
4. SEE SHEET 5 FOR CRITICAL POINT DATA TABLE.
5. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

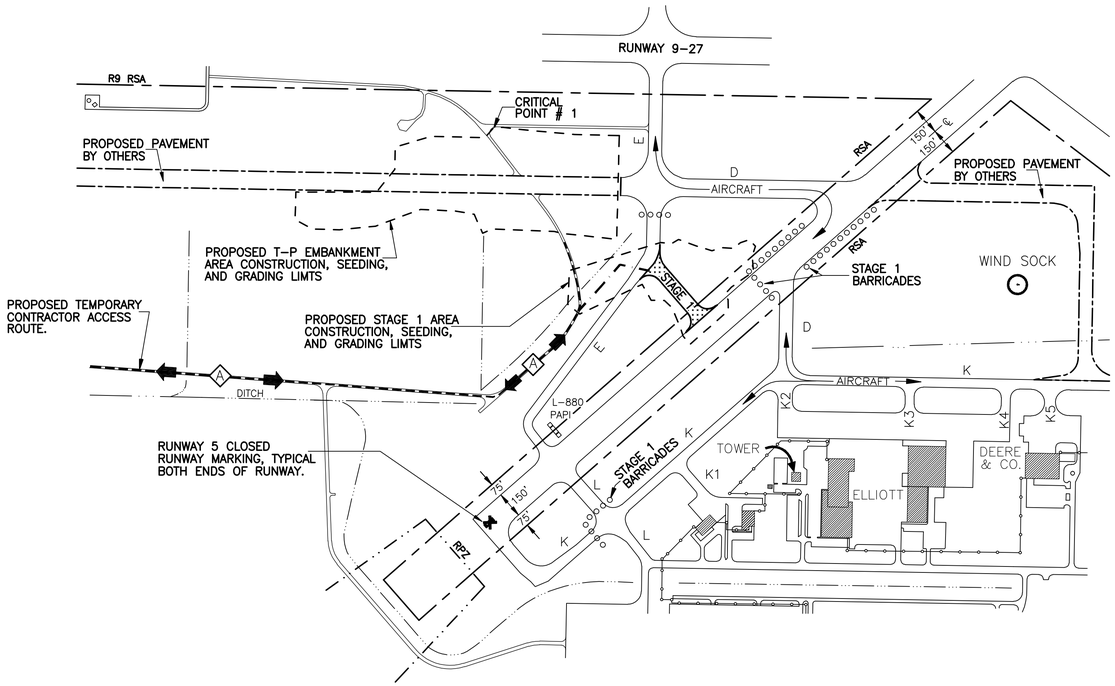
PROPOSED SAFETY PLAN

STAGE 1 AIRFIELD STATUS

1. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC.
2. TAXIWAY K CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY L.
3. TAXIWAY L CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY K.
4. TAXIWAY E CLOSED BETWEEN TAXIWAY D AND RUNWAY 5-23.
5. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.

STAGE 1 NOTES:

1. STAGE 1 SHALL NOT START UNTIL TAXIWAY D BETWEEN RUNWAY 5-23 AND TAXIWAY E IS CONSTRUCTED (BY OTHERS) AND OPENED TO AIRCRAFT TRAFFIC.
2. STAGE 1 SHALL NOT START UNTIL TAXIWAY E BETWEEN RUNWAY 9-27 AND TAXIWAY D IS RELOCATED (BY OTHERS) AND OPENED TO AIRCRAFT TRAFFIC.
3. COMPLETE STAGE 1 AND OPEN ALL OF TAXIWAY E AND TAXIWAY L TO AIRCRAFT TRAFFIC PRIOR TO THE START OF STAGE 2.
4. UPON COMPLETION OF STAGE 1, CLOSED TAXIWAY D BETWEEN TAXIWAY E AND RUNWAY 5-23 TO AIRCRAFT TRAFFIC.



STAGE 1 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

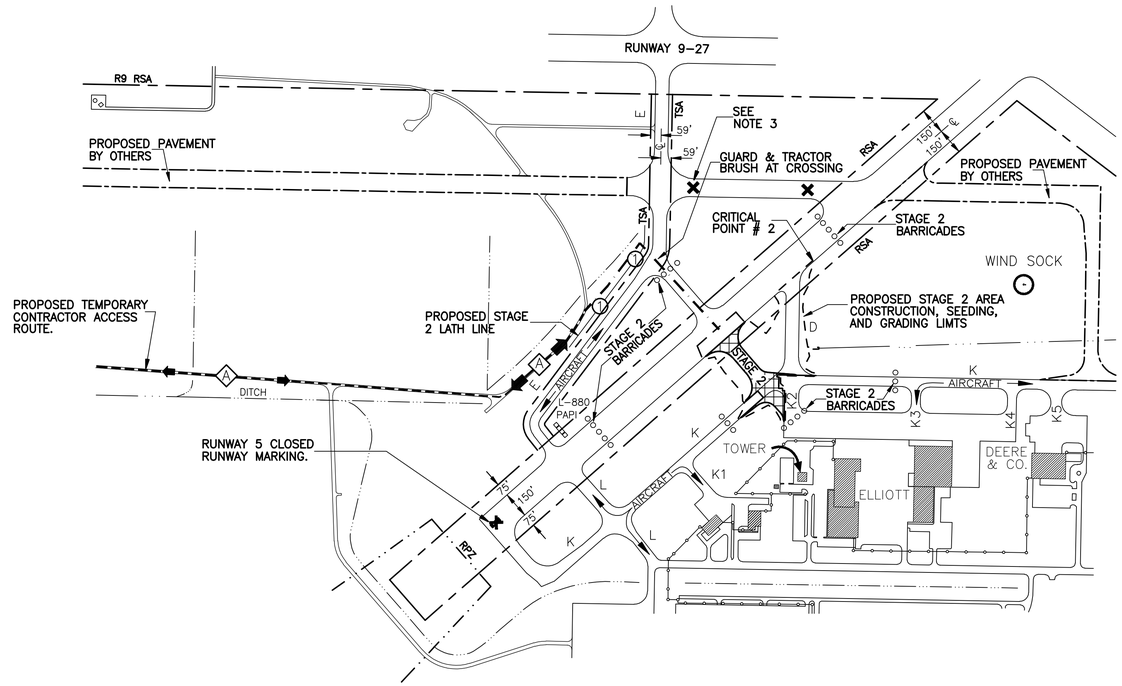
0 400 800
SCALE FEET

STAGE 2 AIRFIELD STATUS

1. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC.
2. TAXIWAY D CLOSED BETWEEN TAXIWAY E AND TAXIWAY K.
3. TAXIWAY K CLOSED BETWEEN TAXIWAY K1 AND TAXIWAY K3.
4. TAXIWAY K2 CLOSED.
5. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.

STAGE 2 NOTES:

1. STAGE 2 SHALL NOT START UNTIL TAXIWAY E BETWEEN RUNWAY 5-23 AND RUNWAY 9-27 IS RELOCATED (BY OTHERS) AND OPENED TO AIRCRAFT TRAFFIC.
2. COMPLETE STAGE 1 AND OPEN ALL OF TAXIWAY E AND TAXIWAY L TO AIRCRAFT TRAFFIC PRIOR TO THE START OF STAGE 2.
3. KEEP TAXIWAY E PAVEMENT CROSSING CLEAR OF FOD (FOREIGN OBJECT DEBRIS) AT ALL TIMES.
4. CONTRACTOR TO SUPPLY, INSTALL, AND MAINTAIN CLOSED RUNWAY MARKERS AT TWO EACH TAXIWAY LOCATIONS AS SHOWN BELOW. THESE TWO MARKERS SHALL REMAIN IN-PLACE AFTER THIS CONSTRUCTION PROJECT (MLI-3XXX) HAS BEEN COMPLETED. UPON COMPLETION OF THIS PROJECT, THE MARKERS (TWO EACH) SHALL BECOME THE PROPERTY OF THE MAA.



STAGE 2 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

0 400 800
SCALE FEET

SAFETY PLAN NOTES:

1. THE METROPOLITAN AIRPORT AUTHORITY (MAA) SHALL DETERMINE WHEN AND WHERE THE CONTRACTOR WILL BE ALLOWED TO WORK. THE CONTRACTOR SHALL BE PREPARED TO EXIT THE RESTRICTED AREAS, SAFETY AREAS AND/OR AIR FIELD AT ALL TIMES WHEN DIRECTED TO DO SO BY THE MAA. ALL PAVEMENTS SHALL BE OPEN TO AIRCRAFT TRAFFIC UNLESS "NOTAMED" OTHERWISE DURING THE CONSTRUCTION OF THIS PROJECT. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION ON WORKING RESTRICTIONS AND CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ALL BARRICADES AND LATH LINES AS SHOWN, PRIOR TO ANY EQUIPMENT ENTERING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION STAGING AREA.

2. SOLID CLOSED RUNWAY MARKING CROSSES ARE REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. SEE SPECIAL PROVISIONS FOR DETAILS OF CROSSES.

3. THE CONTRACTOR SHALL NOT TRAVEL IN OR THROUGH THE RESTRICTED AREAS AND/OR SAFETY AREAS UNLESS PERMISSION IS RECEIVED AND CONTACT HAS BEEN MADE WITH THE FAA CONTROL TOWER.

4. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PROPOSED CONSTRUCTION STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR'S MARKED VEHICLES AND EQUIPMENT SHALL BE ALLOWED ON THE AIRFIELD. ALL CONSTRUCTION VEHICLES AND EQUIPMENT, EXCEPT THE PAVING TRAIN, SHALL BE PARKED IN THE CONSTRUCTION STAGING AREA DURING ALL NON–WORKING HOUR. THE PAVING TRAIN MAY BE PARKED ON THE AIRFIELD OUTSIDE ALL RESTRICTED AREAS IN A LOCATION AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL LOCATE HIS TRAILER, THE ENGINEERS FIELD OFFICE, AND ALL OTHER NECESSARY FACILITIES AND MATERIALS IN THE PROPOSED CONSTRUCTION STAGING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT WHATEVER ACCESS ROAD HE DEEMS NECESSARY BETWEEN THE EXISTING ROADS AND THE CONSTRUCTION AREAS. OVERNIGHT PARKING OF EMPLOYEE VEHICLES IN THE CONSTRUCTION STAGING AREA WILL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.

5. THE CONTRACTOR SHALL PROCURE ENOUGH QUAD CITY AIRPORT SECURITY/IDENTIFICATION BADGES FOR HIS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES FROM THE AIRPORT AUTHORITY TO GUARANTEE AT LEAST ONE MEMBER OF EACH CONSTRUCTION CREW WILL HAVE A BADGE. ANY CONSTRUCTION CREW WITHOUT A BADGED MEMBER SHALL NOT BE ALLOWED ON THE AIRFIELD SITE. ALL INDIVIDUALS WHO ARE ISSUED SECURITY BADGES MUST CORRECTLY WEAR THEIR OWN BADGE WHILE ON THE AIRFIELD. BADGES MAY BE OBTAINED, AFTER MEETING SECURITY REQUIREMENTS, FROM THE SECURITY OFFICE AT THE QCI AIRPORT. A FIFTY DOLLAR ($50.00) REFUNDABLE DEPOSIT IS REQUIRED FOR EACH BADGE.

6. THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTES, CONSTRUCTION STAGING AREA, AND ENTRANCE TO THE AIRFIELD AS SHOWN ON THE SAFETY PLAN SHEET. NO CONSTRUCTION TRAFFIC SHALL BE ALLOWED ON THE AIRFIELD OUTSIDE THE HAUL ROUTES AND WORK AREAS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUPPLY AND INSTALL TEMPORARY LOCKS ON EXISTING GATES AT THE PROPOSED CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE CONSTRUCTION ENTRANCE GATE CLOSED AT ALL TIMES. VIOLATIONS ARE SUBJECT TO FINES/PENALITIES AND THE CONTRACTOR SHALL PAY ANY FINES INCURRED, INCLUDING FINES INCURRED BY THE RESIDENT ENGINEER AND/OR MAA DUE TO THE CONTRACTOR'S NEGLIGENCE. ALL VEHICULAR TRAFFIC SHALL BE KEPT TO A MINIMUM. ALL VEHICLES ON THE APRONS, RAMPS, TAXIWAYS, OR RUNWAYS REQUIRE THE APPROVAL OF THE RESIDENT ENGINEER / MAA.

7. THE CONTRACTOR AND EACH SUBCONTRACTOR SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WORK OR EQUIPMENT. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE ON A 24–HOUR / 7 DAYS PER WEEK BASIS.

8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL TYPE II BARRICADES EQUIPPED WITH FLASHING RED LIGHTS AND 20" X 20" ORANGE FLAGS AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR WILL FURNISH, MAINTAIN, AND MOVE THE BARRICADES AS REQUIRED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE SUFFICIENTLY WEIGHTED WITH SANDBAGS OR OTHER APPROPRIATE METHOD TO WITHSTAND HIGH WINDS AND/OR JET BLAST WITHOUT DISLOCATION. BARRICADES SHALL BE CHECKED DAILY BY THE CONTRACTOR FOR PROPER PLACEMENT, ADEQUATE BALLAST, PROPER LIGHTING, PROPER FLAGGING AND WORKING BATTERIES. ALL DISCREPANCIES SHALL BE CORRECTED IMMEDIATELY. BARRICADES SHALL BE RELOCATED UPON THE COMPLETION OF EACH STAGE OF CONSTRUCTION. COST TO BE INCLUDED IN MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

9. WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRFIELD, THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE FOOT (3') SQUARE FLAG WITH A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT (1') ON EACH SIDE, DISPLAYED IN FULL VIEW ABOVE THE VEHICLE OR EQUIPMENT. EACH VEHICLE SHALL HAVE A FLASHING YELLOW LIGHT MOUNTED ON TOP OF THE ROOF.

10. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE THE CONSTRUCTION STAGING AREA, SERVICE ROADS, ACCESS ROADS, AND HAUL ROADS TO THEIR ORIGINAL CONDITIONS FOLLOWING COMPLETION OF CONSTRUCTION. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REGRADING, FERTILIZING, SEEDING AND MULCHING OF EARTH SURFACES AND/OR REGRADING, GRAVELING & SEAL COATING OF TREATED SURFACES, AS REQUIRED, TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL GROUND SURFACES, GRAVEL ROADS, PAVEMENTS, AND OTHER FACILITIES DAMAGED BY THE CONTRACTOR WHILE COMPLETING THE PROPOSED WORK SHALL BE REPAIRED OR RETURNED TO ITS ORIGINAL STATE. COST TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

11. THE CONTRACTOR SHALL IMMEDIATELY SWEEP OR PICK UP ANY SOIL, DEBRIS, AGGREGATE CHIPS OR ROCK, OR LOOSE MATERIALS WHICH HAS BEEN DROPPED ONTO AIRPORT ROADS, RUNWAYS, TAXIWAYS, OR SODDED AREAS.

12. THE DISPOSAL OF ALL MATERIALS NOT TO BE INCORPORATED IN EMBANKMENTS ON THE PROJECT SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT A LOCATION OFF AIRPORT PROPERTY.

13. THE SEQUENCE OF CONSTRUCTION OPERATIONS AND DESCRIPTION OF CONDITIONS ARE OUTLINED IN THE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL GIVE THE MAA A 96 HOUR NOTICE PRIOR TO THE START OF ANY WORK REQUIRING THE CLOSING OF ANY PAVEMENTS TO AIRCRAFT TRAFFIC SO THAT A NOTAM CAN BE ISSUED.

14. WORKING HOURS OF THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL CONFORM TO ALL APPLICABLE LOCAL LAWS, INCLUDING ANY NOISE CONTROL.

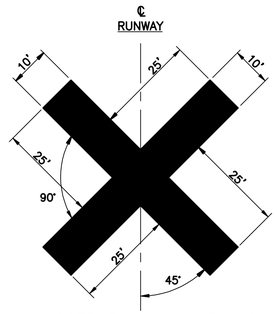
15. NO MOUNDS OF DIRT OR IRREGULARITIES GREATER THAN 3" WHICH, IN THE OPINION OF THE RESIDENT ENGINEER, COULD INTERFERE WITH ANY AIRFIELD OPERATIONS WILL BE PERMITTED ON THE AIRFIELD. NO EXPOSED FACES IN EXCESS OF ONE AND ONE–HALF (1–1/2) INCHES IN HEIGHT AND 2:1 SLOPES ON ANY EXCAVATION WILL BE PERMITTED WITHIN THE RESTRICTED AREAS.

16. DUST ABATEMENT MEASURES WILL BE REQUIRED, WHEN IN THE OPINION OF THE RESIDENT ENGINEER, A HAZARD TO AIR TRAFFIC, LOCAL RESIDENCES, OR CONSTRUCTION PROJECT PERSONNEL EXISTS. PREVENTIVE MEASURES TO BE ACCOMPLISHED BY THE CONTRACTOR SHALL INCLUDE, BUT NOT BE LIMITED TO, WATERING AND TREATMENT WITH CALCIUM CHLORIDE.

17. BY THE END OF EACH WORK DAY AND PRIOR TO LEAVING THE AIRFIELD, THE CONTRACTOR SHALL HAVE THOROUGHLY SWEPT THE AIR TRAFFIC CORRIDORS ADJACENT TO THE WORK AREAS TO REMOVE DUST AND DEBRIS. IN ADDITION, ALL AIR TRAFFIC AREAS USED BY CONSTRUCTION PERSONNEL AND EQUIPMENT MUST BE CONTINUOUSLY SWEPT AND MAINTAINED FREE OF DEBRIS. SWEEPERS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE ENTIRE LENGTH OF THE CONTRACT AND SHALL BE OF A TYPE CAPABLE OF REMOVING ALL DUST AND DEBRIS TO THE SATISFACTION OF THE MAA. SWEEPERS MUST BE COMMERCIAL QUALITY AND APPROVED BY THE RESIDENT ENGINEER AND MAA PRIOR TO THE START OF CONSTRUCTION.

18. THE CONTRACTOR SHALL INSTALL AND MAINTAIN LATH LINES DURING THE LENGTH OF THE PROJECT AS SHOWN OR DIRECTED BY THE RESIDENT ENGINEER. SEE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

19. NO CHANGES SHALL BE MADE IN ANY PROVISIONS OF THIS SAFETY PLAN UNLESS APPROVED IN WRITING BY THE METROPOLITAN AIRPORT AUTHORITY, THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, AND THE FEDERAL AVIATION ADMINISTRATION. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

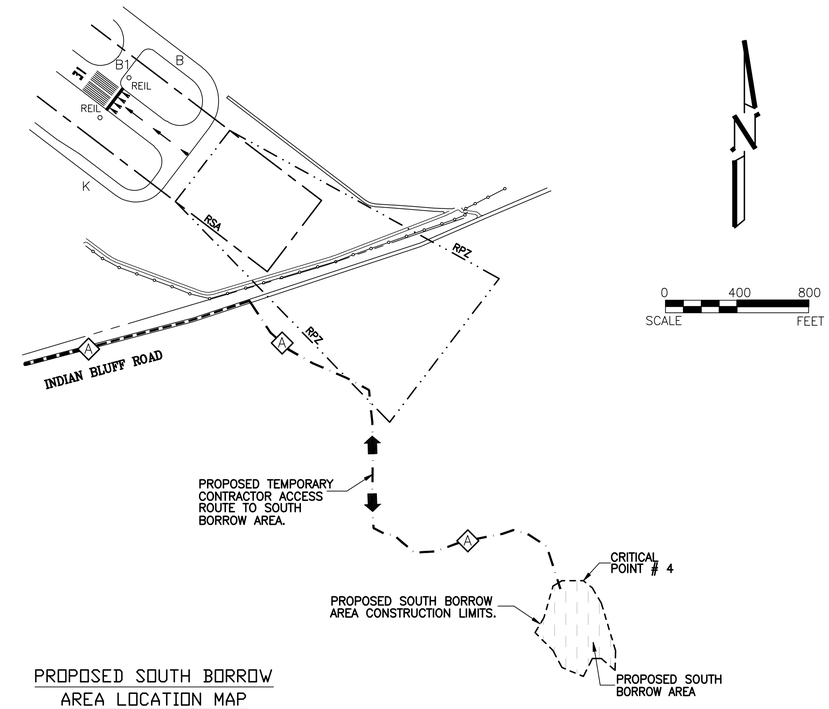


CLOSED RUNWAY MARKING DETAIL

CLOSED RUNWAY MARKINGS NOTES:

1. SOLID CROSS FOR CLOSED RUNWAY IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES AND SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
2. THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
3. COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
4. SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.

CRITICAL POINT DATA					
NUMBER	LOCATION	LATITUDE	LONGITUDE	GROUND ELEVATION	
				EXISTING	PROPOSED
1	NORTH EDGE OF STAGE 1	41D 26' 51.93"	90D 30' 56.02"	577.0'	577.0'
2	NORTH EDGE OF STAGE 2	41D 26' 44.55"	90D 30' 33.22"	580.7'	580.7'
3	NE CORNER OF BATCH PLANT STAGING AREA	41D 26' 33.24"	90D 31' 41.54"	580.0'	580.0'
4	NE CORNER OF SOUTH BORROW AREA	41D 26' 11.43"	90D 29' 07.75"	631.0'	631.0'



PROPOSED SOUTH BORROW AREA LOCATION MAP

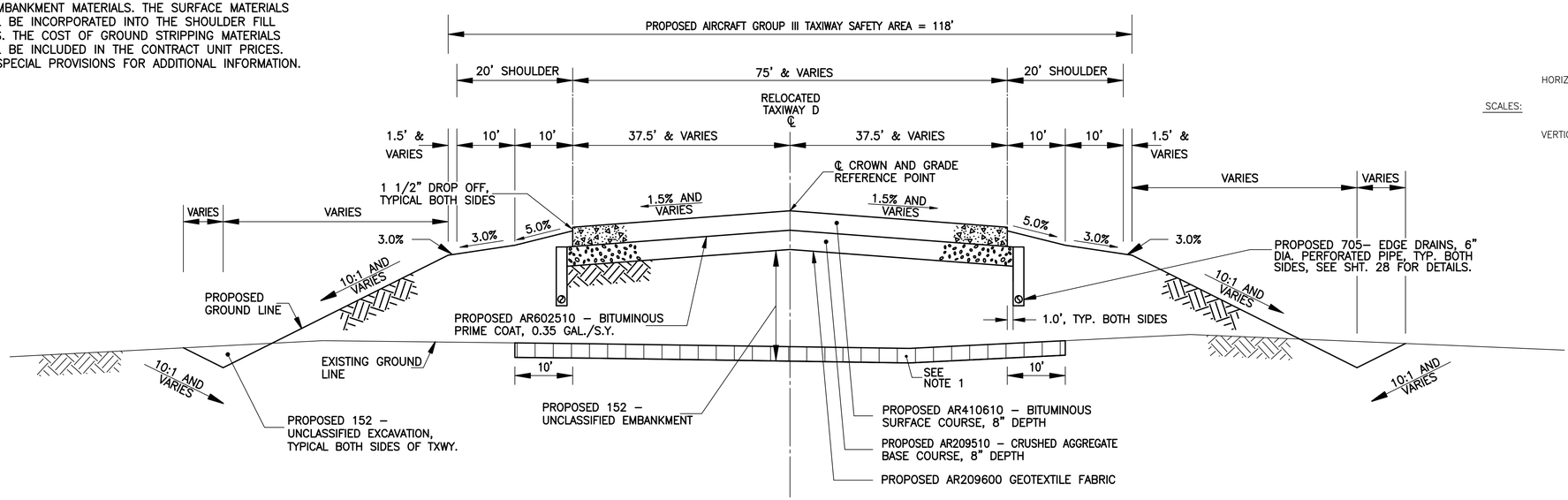
NOTE:

1. THE EXISTING GROUND MATERIALS UNDER THE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

PROPOSED TYPICAL SECTION – BITUMINOUS PAVEMENT

RELOCATED TAXIWAY D: STA. 761+92.99 TO STA. 762+18.19

AIRPLANE DESIGN GROUP III



SCALES:

HORIZONTAL SCALE FEET 0 5 10 15 20

VERTICAL SCALE FEET 0 1 2 3 4

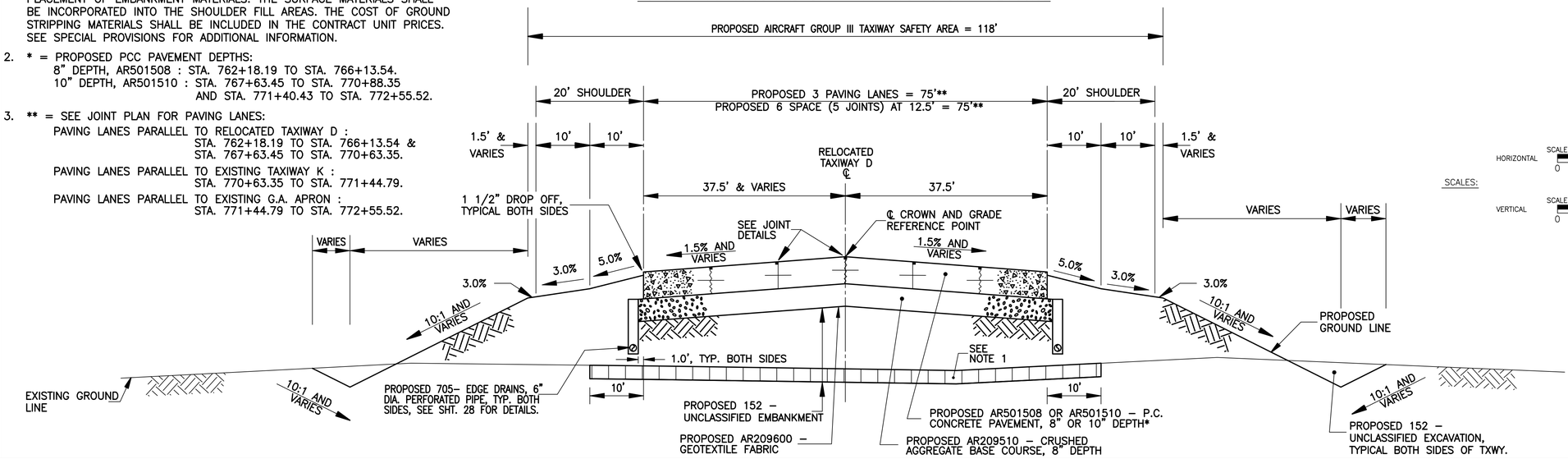
NOTES:

1. THE EXISTING GROUND MATERIALS UNDER THE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. * = PROPOSED PCC PAVEMENT DEPTHS:
   8" DEPTH, AR501508 : STA. 762+18.19 TO STA. 766+13.54.
   10" DEPTH, AR501510 : STA. 767+63.45 TO STA. 770+88.35 AND STA. 771+40.43 TO STA. 772+55.52.
3. ** = SEE JOINT PLAN FOR PAVING LANES:
   PAVING LANES PARALLEL TO RELOCATED TAXIWAY D : STA. 762+18.19 TO STA. 766+13.54 & STA. 767+63.45 TO STA. 770+63.35.
   PAVING LANES PARALLEL TO EXISTING TAXIWAY K : STA. 770+63.35 TO STA. 771+44.79.
   PAVING LANES PARALLEL TO EXISTING G.A. APRON : STA. 771+44.79 TO STA. 772+55.52.

PROPOSED TYPICAL SECTION – PCC PAVEMENT

RELOCATED TAXIWAY D: STA. 762+18.19 TO STA. 766+13.54, STA. 767+63.45 TO STA. 770+63.35 AND STA. 771+44.79 TO STA. 772+55.52

AIRPLANE DESIGN GROUP III



SCALES:

HORIZONTAL SCALE FEET 0 5 10 15 20

VERTICAL SCALE FEET 0 1 2 3 4

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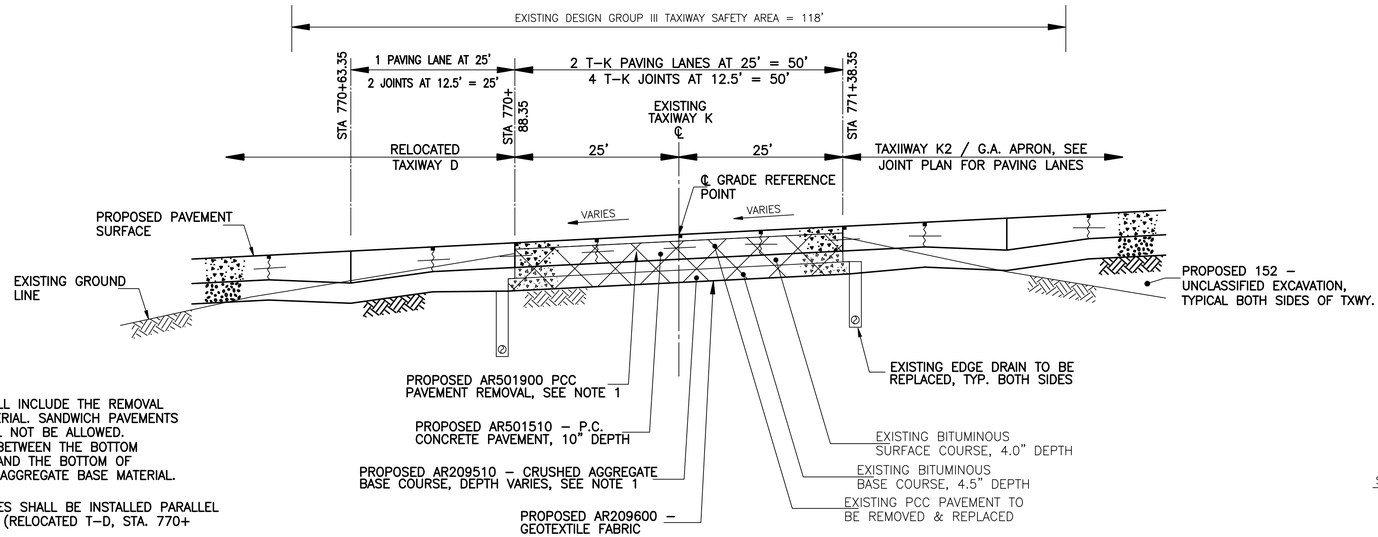
AIRPLANE DESIGN GROUP III

PROPOSED TYPICAL SECTION – REMOVE & REPLACE EXISTING TAXIWAY K PCC PAVEMENT

RELOCATED TAXIWAY D STATIONING: STA. 770+63.35 TO STA. 771+44.79, LT. & RT.



NOTES:

1. THE PROPOSED PAVEMENT REMOVAL SHALL INCLUDE THE REMOVAL OF THE EXISTING BITUMINOUS BASE MATERIAL. SANDWICH PAVEMENTS (PAVEMENT – ROCK – PAVEMENT) SHALL NOT BE ALLOWED. THE CONTRACTOR SHALL FILL THE VOID BETWEEN THE BOTTOM OF THE PROPOSED 10" PCC PAVEMENT AND THE BOTTOM OF THE PAVEMENT REMOVAL WITH AR209 – AGGREGATE BASE MATERIAL.
2. SEE JOINT PLAN, TAXIWAY K PAVING LANES SHALL BE INSTALLED PARALLEL TO THE EXISTING TAXIWAY K CENTERLINE (RELOCATED T-D, STA. 770+63.35 TO STA. 771+44.79).

SCALES:

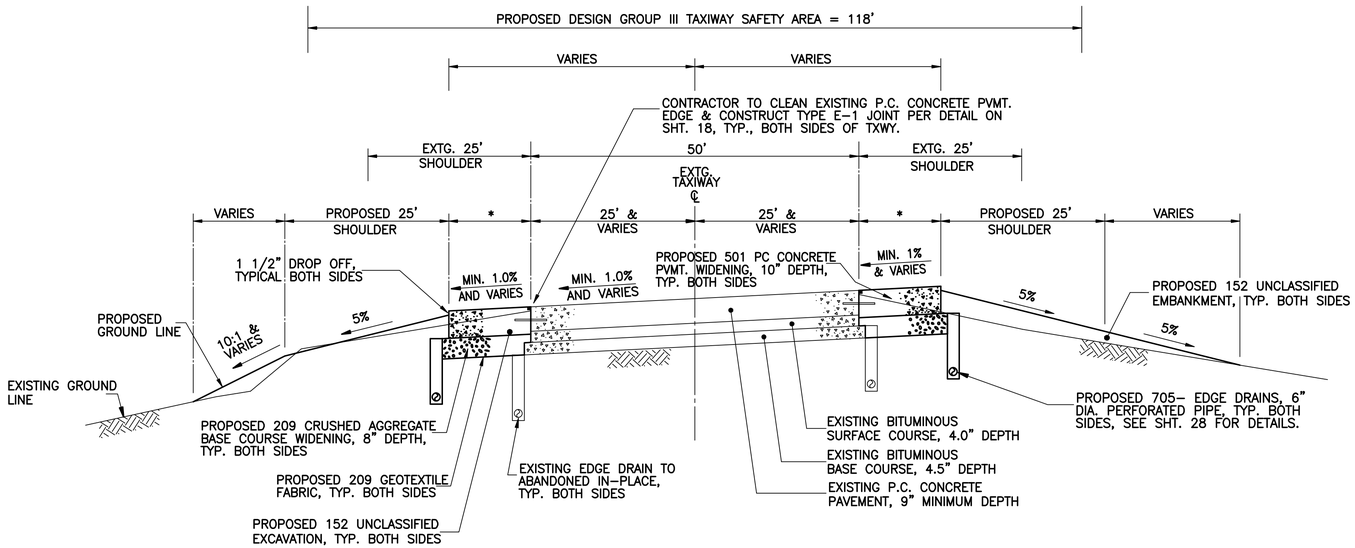
HORIZONTAL SCALE FEET 0 5 10 15 20

VERTICAL SCALE FEET 0 1 2 3 4

PROPOSED TYPICAL SECTION – WIDENING

TAXIWAY K WIDENING, SEE NOTE 1 FOR LOCATION

AIRPLANE DESIGN GROUP III



NOTES:

1. * = PROPOSED PCC PVMT WIDENING:
   A. NORTH SIDE OF EXTG. TAXIWAY K:
      1. RT. 37.5' FROM T-D ℄ TO CLOSEST EXTG. JOINT AT RIGHT 137.5'∓ FROM T-D ℄.
      2. CLOSEST EXTG. JOINT AT LEFT 67' FROM T-D ℄ TO CLOSEST EXTG. JOINT AT STA. 771+63.7, LT. 160'.
   B. SOUTH SIDE OF EXTG. TAXIWAY K:
      1. RT. 37.5' FROM T-D ℄ TO CLOSEST EXTG. JOINT AT RIGHT 212.4'∓ FROM T-D ℄.

SCALES:

HORIZONTAL SCALE FEET 0 5 10 15 20

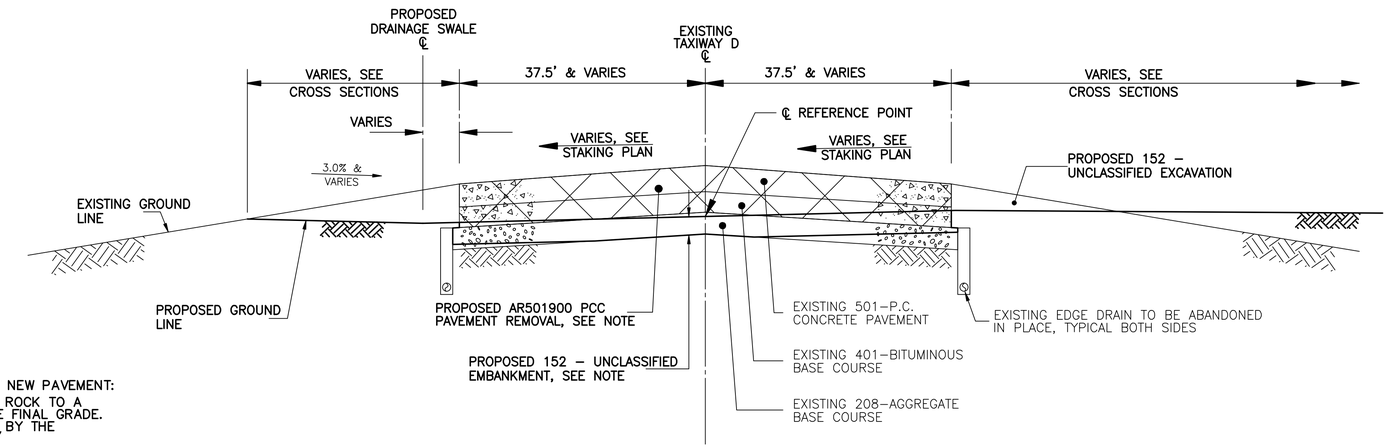
VERTICAL SCALE FEET 0 1 2 3 4

AIRPLANE DESIGN GROUP III

PROPOSED TYPICAL SECTION – REMOVE EXISTING PCC PAVEMENT

EXISTING TAXIWAY D

EXISTING TAXIWAY D STATIONING: STA. 11+01.74 TO STA. 16+25.26



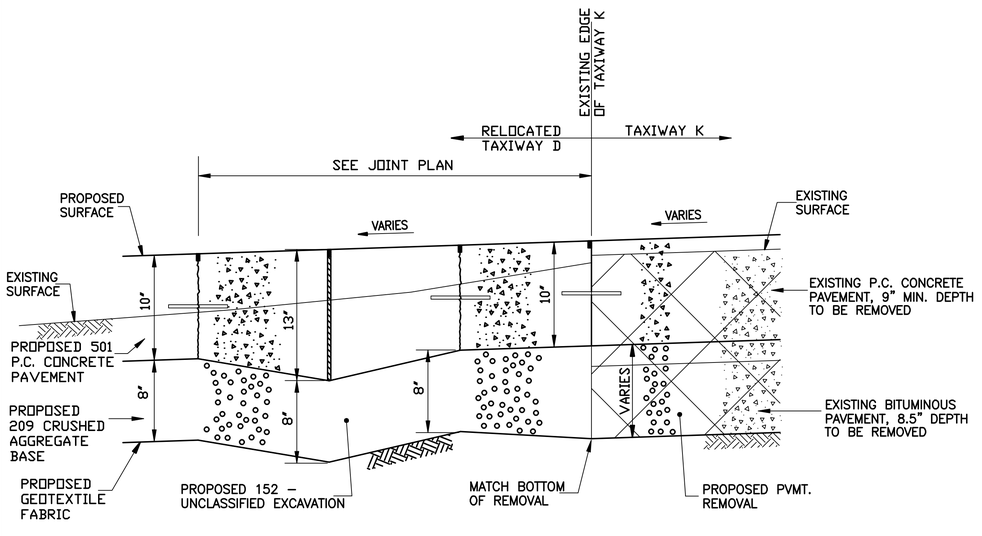
NOTE:

IN PAVEMENT REMOVAL AREAS WITHOUT NEW PAVEMENT: REMOVE EXISTING PAVEMENT AND BASE ROCK TO A POINT THAT IS AT LEAST 8" BELOW THE FINAL GRADE. INSTALL EARTH SOILS IN VOID CREATED BY THE REMOVAL. INCLUDE COST IN REMOVAL / EARTHWORK CONTRACT UNIT RATES.

SCALES:

HORIZONTAL SCALE FEET 0 10 20

VERTICAL SCALE FEET 0 2 4

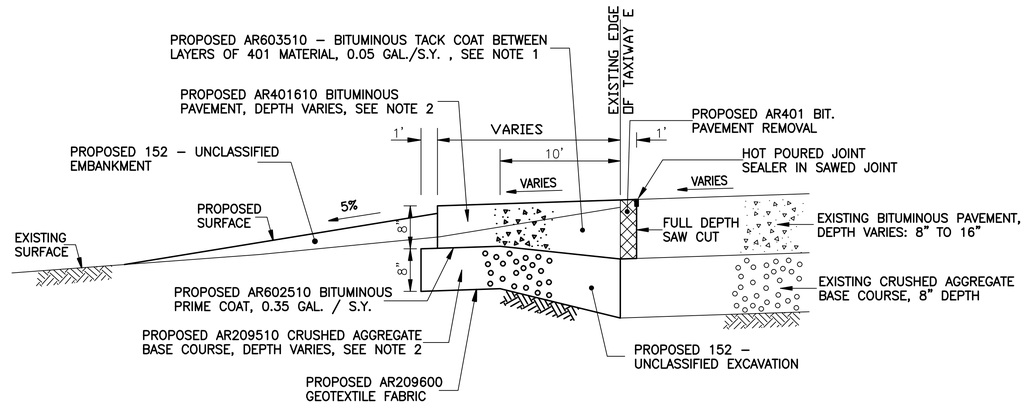


TYPICAL SECTION, NORTH EDGE OF TXWY. K

TAXIWAY D, STA. 771+40.4, LT. 67.4' ~~T~~ TO RT. 37.5'

NOTES:

1. CONTRACTOR SHALL SPRAY A LIGHT COAT OF 603–BITUMINOUS TACK COAT MATERIALS BETWEEN ALL LAYERS OF BITUMINOUS AS DIRECTED BY THE RESIDENT ENGINEER.
2. MATCH DEPTH OF EXISTING MATERIAL AT THE EDGE OF THE TAXIWAY. VARY DEPTH OF THE PROPOSED MATERIAL TO A POINT 10' OFF OF THE EXISTING TAXIWAY EDGE. INSTALL A CONSTANT 8" DEPTH OF MATERIAL BEYOND 10' OFF OF THE EXISTING TAXIWAY EDGE.

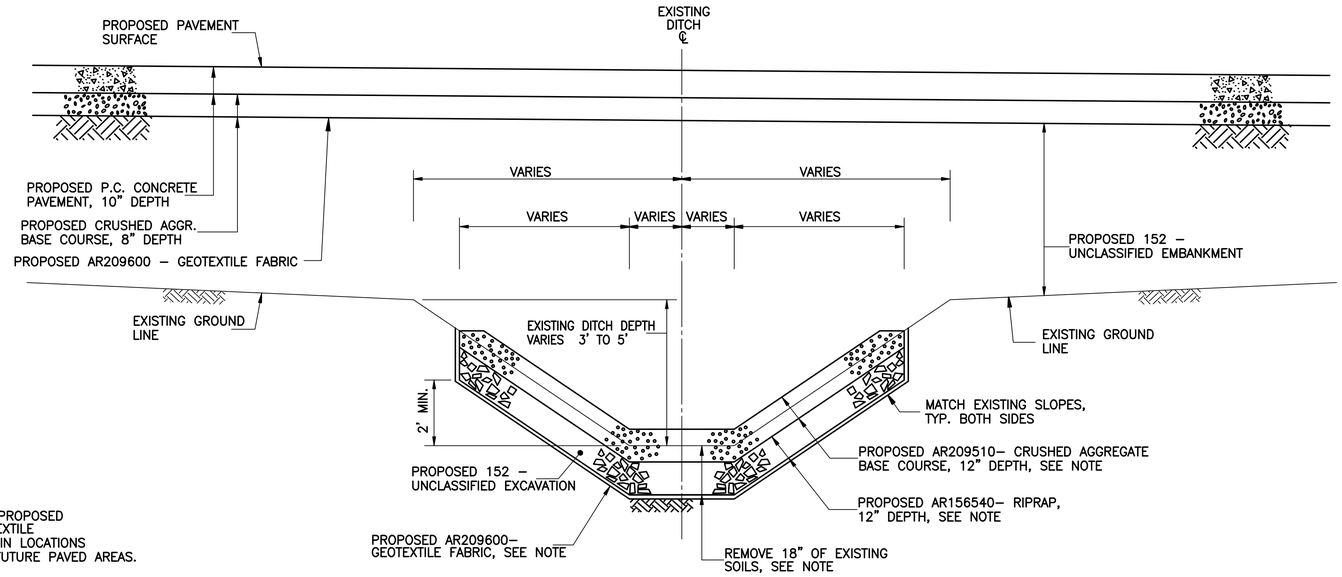


TYPICAL SECTION, BITUMINOUS PAVEMENT WIDENING

TAXIWAY E, STA. 861+07.37 TO STA 962+86.91, LEFT SIDE ONLY

AIRPLANE DESIGN GROUP III

PROPOSED TYPICAL SECTION
EXISTING DITCH TO BE ABANDONED IN PLACE
STA. 770+08 LT. & RT.



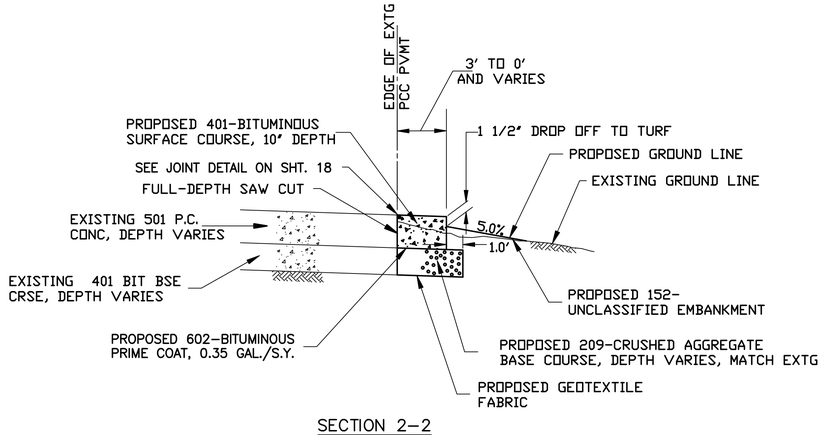
NOTE:

REMOVE EXISTING SOIL MATERIALS AND INSTALL PROPOSED CRUSHED AGGREGATE BASE, RIP RAP AND GEOTEXTILE FABRIC IN BOTTOM OF ABANDONED DITCH ONLY IN LOCATIONS UNDER (OR WITHIN 10' OF) THE PROPOSED / FUTURE PAVED AREAS.

SCALES:

HORIZONTAL SCALE FEET 0 5 10

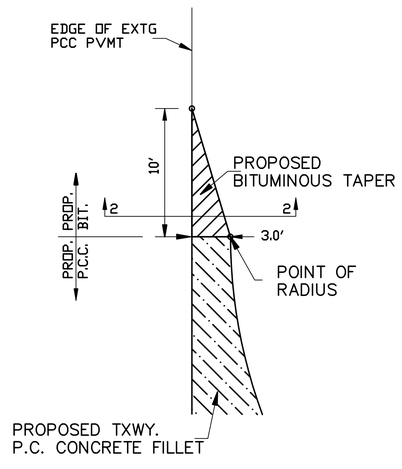
VERTICAL SCALE FEET 0 2 4



SECTION 2–2

TYPICAL SECTION

BITUMINOUS TAPER AT EDGE OF PCC PAVEMENT :
STATIONS: 766+13.54, LT & RT; 767+63.45, LT & RT; 770+88.35, RT; 771+40.43, RT; 771+66, LT; AND 773+11, RT



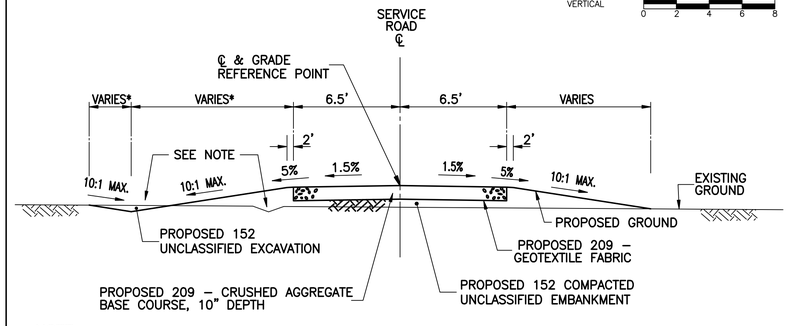
PLAN VIEW
BITUMINOUS TAPER

PROPOSED TYPICAL SECTION
FLUSH CRUSHED AGGREGATE SERVICE ROAD
RIGHT, STA. 864+88 TO STA. 865+84

HORIZONTAL SCALE FEET 0 2 4 6 8

SCALES:

VERTICAL SCALE FEET 0 2 4 6 8



NOTE:

* = RELOCATE EXISTING DRAINAGE DITCH ON NORTH SIDE OF SERVICE ROAD AS REQUIRED TO FLOW INTO PROPOSED F.E.S.

TYPICAL SECTIONS

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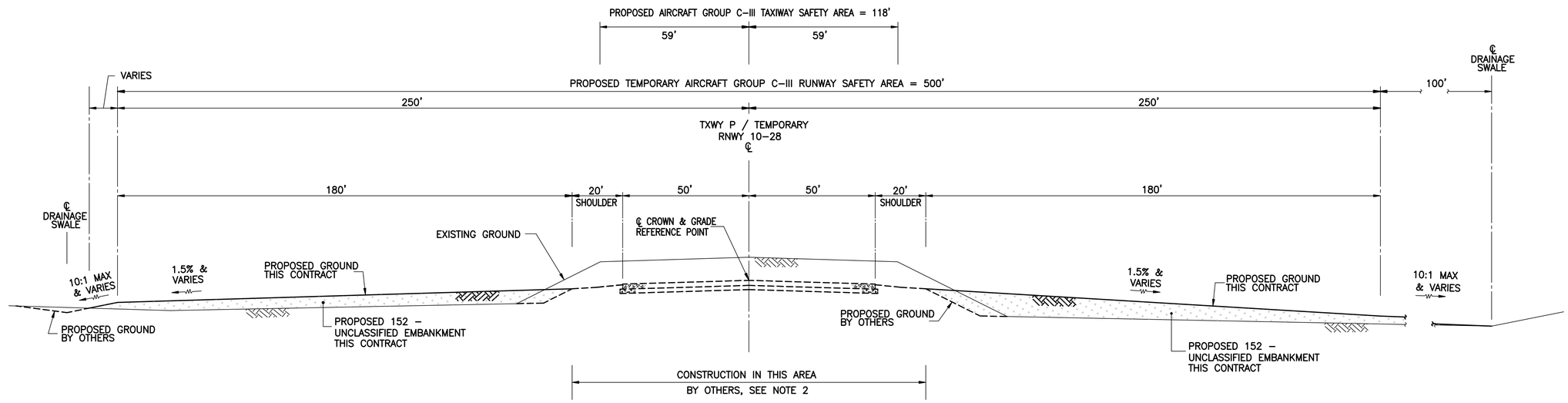
TEMPORARY RUNWAY 10-28:

AIRCRAFT APPROACH CATEGORY C
AIRPLANE DESIGN GROUP III
NON-PRECISION RUNWAY

PROPOSED TYPICAL SECTION – EARTHWORK & TURFING ONLY

TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 609+50 TO STA. 627+50



NOTES:

1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. PAVEMENT, BASE ROCK, FINAL GRADING, ETC. IN THIS AREA BY OTHERS (T-P, PHASE III – WEST PAVING, MLI-3855).

SOIL EXCAVATION LEGEND:

SOIL EMBANKMENT BY THIS CONTRACTOR.

PROPOSED CONSTRUCTION BY OTHERS (MLI-3855).

SCALES:

HORIZONTAL SCALE FEET 0 10 20 30 40

VERTICAL SCALE FEET 0 5 10 15 20

TYPICAL SECTIONS

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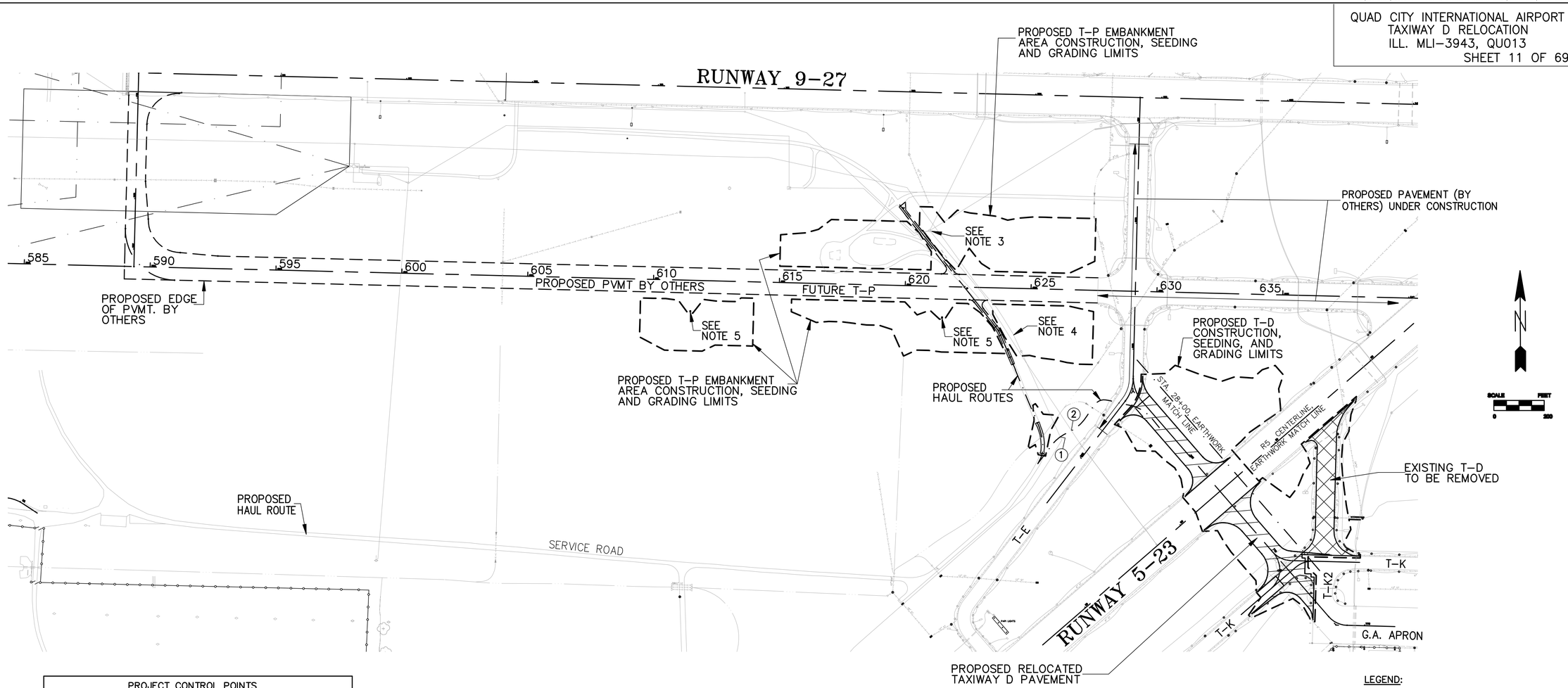

PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57' N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70' N1741496.8955, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69' N1741518.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46' N1740729.9858, E2203194.7470	574.40	1/2" REBAR
2000	STA. 107+69.21, LT. 736.44' N1742717.7720, E2199686.1370	587.72	SPK

TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199823.7550, STATION 107+06.74, LT. 739.58'
TBM K	578.34	TOP OF RCP, S SIDE OF SERVICE RD, N1741508.6290, E2201629.7690, STATION 127+42.16, RT. 424.08'
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9–27, BETWEEN SIGNS [2] – [7] N1741569.4966, E2200420.1701 STATION 115+31.40, RT. 393.12'
TBM T	576.35	CHIS. □ , SE COR CONC PAD W SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03'
TBM U	583.18	CHIS. □ , HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359 STATION 144+70.44, RT. 113.99'

PROPOSED MINIMUM UTILITY PROTECTION*		
LOCATION	UTILITY DESCRIPTION	MINIMUM PROTECTION
①	R9 ILS CONTROL CABLE	STEEL PLATES W/ 3' SOIL COVER
②	SCAN CABLE	STEEL PLATES W/ 3' SOIL COVER

* = MINIMUM THICKNESS OF ALL STEEL PLATES = 1" (ONE INCH), ALL STEEL PLATES TO HAVE A MINIMIM OF 3" SOIL COVER TO HOLD PLATES IN PLACE.

NOTES:

1. SEE PLAN & PROFILE SHEETS FOR LOCATION OF EXISTING UNDERGROUND ELECTRICAL AND FIELD TILES.
2. CONTRACTOR SHALL REPAIR OR REPLACE ANY IMPROVEMENTS DAMAGED DURING CONSTRUCTION AT HIS OWN EXPENSE.
3. EXISTING SCAN SYSTEM REMOTE PROCESSING UNIT TO BE RELOCATED BY OTHERS. THIS CONTRACTOR SHALL PROTECT THIS EXISTING ELECTRICAL SYSTEM FROM DAMAGE UNTIL IT IS RELOCATED BY OTHERS. CONTRACTOR SHALL DELAY EARTHWORK AROUND UNIT UNTIL EQUIPMENT IS RELOCATED.
4. DO NOT DISTURB EXISTING FAA R9 ILS CONTROL CABLE.
5. PROPOSED GUIDANCE SIGNS BY OTHERS. DO NOT DISTURB.

LEGEND:

- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- PROPOSED PAVEMENT BY OTHERS
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- PROPOSED HAUL ROUTE
- ① PROPOSED UTILITY PROTECTION LOCATION NUMBER.
- PROPOSED PAVEMENT AREA
- PROPOSED PAVEMENT REMOVAL AREA

PROJECT AREA PLAN

11/69

G:\AIRPORT\A09T021- TWY D PHASE I NORTH END\PP.DWG\ 08-18-09

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TAXIWAY E ℄

762+18.19

PROP. BIT. PVMT. | PROP. PCC PVMT.

EDGE OF PROPOSED PAVMENT (BY OTHERS) UNDER CONSTRUCTION

R.P. STA. 762+18.19, LT. 150.0'

112.5' R

PROPOSED PAVEMENT (BY OTHERS) UNDER CONSTRUCTION

STA. 760+00, RELOCATED TXWY D = STA. 860+00, TXWY E

RELOCATE EXISTING EDGE LIGHTS, TYP., SEE LIGHTING PLAN

PROPOSED INLET

PROPOSED EDGE OF PAVMENT

PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

R.P. STA. 765+40.43, LT. 107.5'

RSA

10'

70' R

RUNWAY 5-23

29 CONC. 28 27 26

100' R

PROPOSED EDGE OF PAVMENT

MEET EXTG. EDGE OF RNWY.

RELOCATE EXISTING EDGE LIGHTS, TYP., SEE LIGHTING PLAN

PROPOSED RELOCATED T-D

37.5'

37.5'

760 761 762 763 764 765 766 767 768

90D

PROPOSED STM. SEW.

STA. 761+91.68, START RELOCATED T-D PVMT., MEET EXTG, EL 579.75'

862

MEET EXTG. EDGE OF RNWY.

STA. 766+88.43, RELOCATED TXWY D = STA. 27+07.07, RNWY 5-23

SAW CUT AND REMOVE 12" OF EXISTING PAVEMENT IN INTERSECTION AREA, STA. 861+07.37 TO STA. 862+86.91, SEE SHT. 8 FOR

EXISTING INLET

RELOCATED TAXIWAY D ℄

863

24" STM SEW

R.P. STA. 762+85.85, RT. 107.5'

R.P. STA. 765+40.43, RT. 107.5'

PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

EXISTING EDGE OF PAVMENT

TAXIWAY E ℄

STA. 761+91.68, START RELOCATED T-D PVMT., MEET EXTG, EL 579.75'

℄ CURVE DATA:

I = 42D 19' 48"
D = 38D 11' 50"
T = 58.08'
L = 110.82'
R = 150.00'
E = 10.85'
P.C. = 761+07.37
P.I. = 761+65.45
P.T. = 762+18.19

SCALE FEET HORZ 0 40
SCALE FEET VERT 0 4

LEGEND:
- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- PROPOSED PAVEMENT MILLING
- PROPOSED PAVEMENT REMOVAL
- PROPOSED AR401 BITUMINOUS PAVEMENT

PROPOSED BITUMINOUS PAVEMENT BY OTHERS, 8" DEPTH

PROPOSED AGGREGATE BASE BY OTHERS, 8" DEPTH

EXISTING GRADE LINE ALONG RELOCATED T-D CENTERLINE

VPI STA=762+65
VPI EL=578.73'
CURVE LEN=100'
E=0.15'

-1.389%

PROPOSED PAVEMENT CENTERLINE PROFILE

-0.224%

VPI STA=765+60
VPI EL=578.07'
CURVE LEN=100'
E=0.10'

0.595%

MEET EXTG. EDGE OF RNWY.

-1.472%

PROPOSED AR50110 - P.C. CONCRETE PAVEMENT, 10" DEPTH

PROPOSED PVMT. REMOVAL

PROPOSED AR602 - BITUMINOUS PRIME COAT, 0.35 GAL. / S.Y., STA. 761+91.68 TO STA 762+18.19

PROPOSED AR401610 - BITUMINOUS SURFACE COURSE, 8" DEPTH, STA. 761+91.68 TO STA 762+18.19

PROPOSED AR209600 - GEOTEXTILE FABRIC

MATCH EXTG DEPTH, TYPICAL, 3 PLACES

PROPOSED AR209600 - GEOTEXTILE FABRIC

PROPOSED AR501508 - P.C. CONCRETE PAVEMENT, 8" DEPTH, STA. 762+18.19 TO STA 766+13.54

PROPOSED AR209510 - CRUSHED AGGREGATE BASE COURSE, 8" DEPTH

PROPOSED AR209510 - CRUSHED AGGREGATE BASE COURSE, 8" DEPTH

RELOCATED T-E, STA 860 +00 = RELOCATED T-D, STA 760+00, EL 581.65

SE EOP, RELOCATED T-D, STA. 761+92.99, EL 579.73

N EDGE R 5-23, STA. 766+13.54, EXTG. EL 578.39

RELOCATED T-D, STA. 766+88.43 = R5-23, STA. 27+07.07, EL 579.40

S EDGE R 5-23, STA. 767+63.45, EXTG EL 578.43

580 576 572 568 564 560

CENTERLINE ELEVATIONS

581.96 | 581.65 | 581.34 | 581.02 | 580.67 | 580.21 | 579.28 | 579.63 | 577.95 | 579.11 | 577.36 | 578.77 | 576.78 | 578.61 | 576.19 | 578.52 | 575.88 | 578.43 | 576.16 | 578.34 | 576.45 | 578.25 | 576.74 | 578.16 | 577.02 | 578.17 | 577.95 | 578.31 | 578.76 | 579.30 | 578.97 | 578.47 | 577.02 | 577.89 | 575.83 | 577.30 | 574.93 | 576.71 | 574.27 | 576.16 | 573.71 | 575.75

759+20 760+00 760+80 761+60 762+40 763+20 764+00 764+80 765+60 766+40 767+20 768+00 768+80

RELOCATED TAXIWAY D PLAN & PROFILE, STA 759+40 TO STA 768+20

12/69

G:\Airport\A09T021-Twy D Phase I North End\PP.dwg, 1/30/2010 2:03:20 PM, jeffm

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 13 OF 69

PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

PROPOSED PVMT. REMOVAL

RELOCATE EXISTING EDGE LIGHTS, TYP., SEE LIGHTING PLAN

R.P. STA. 768+66.43, LT. 137.5'

R.P. STA. 770+07.13, LT. 100'

PROPOSED EDGE OF PAVMENT

RELOCATED TAXIWAY D ℄

PROPOSED RELOCATED T-D

PROPOSED ROCK BACKFILL AREA

REMOVE MANHOLE, SEE DRAINAGE PLAN

EXTEND EXISTING 18" DIA. CONC. PIPE CASING, TYP. BOTH SIDES OF TXWY. SEE DRAINAGE PLAN FOR DETAILS.

EXTG. DITCH

T-K ℄

FULL DEPTH SAW CUT ALL AROUND, TYP.

PROPOSED PVMT. REMOVAL

STA. 772+55.52, MEET EXISTING PAVEMENT

PROPOSED EDGE OF PAVMENT

R.P. STA. 772+72.57, RT. 87.5'

R.P. STA. 772+18.48, RT. 107.5'

REMOVE SIGN, TYP., SEE SIGNAGE PLAN

PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

R.P. STA. 769+85.48, RT. 137.5'

R.P. STA. 768+66.43, RT. 137.5'

T-K

T-K2

G.A. APRON

℄ CURVE DATA:

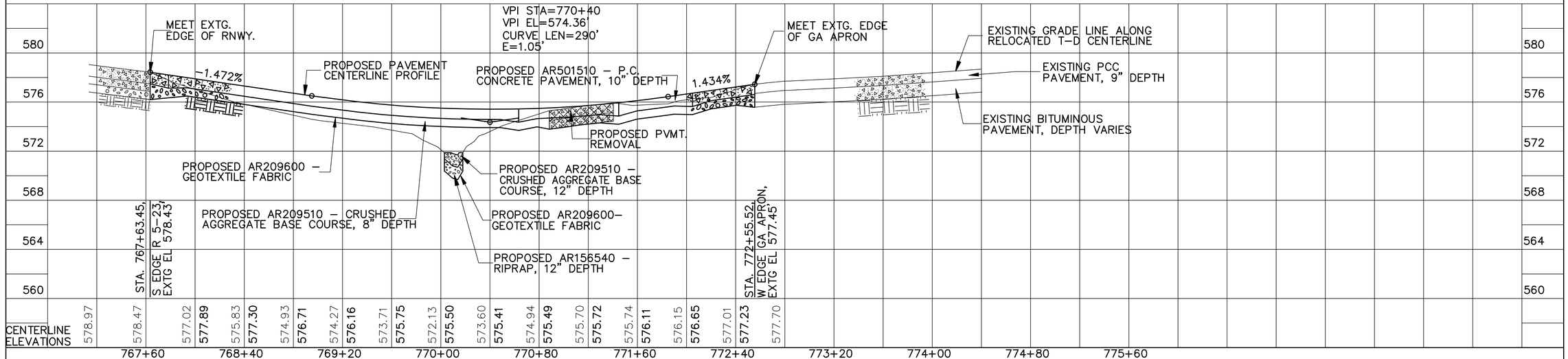
I = 47D 39' 34.6"
D = 57D 17' 45"
T = 44.17'
L = 83.18'
R = 100.00'
E = 9.32'
P.C. = 773+47.53
P.I. = 773+91.70
P.T. = 774+30.71

N

LEGEND:

- RETROREFLECTIVE MARKERS
- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- PROPOSED PAVEMENT MILLING
- PROPOSED PAVEMENT REMOVAL
- PROPOSED ROCK BACKFILL AREA

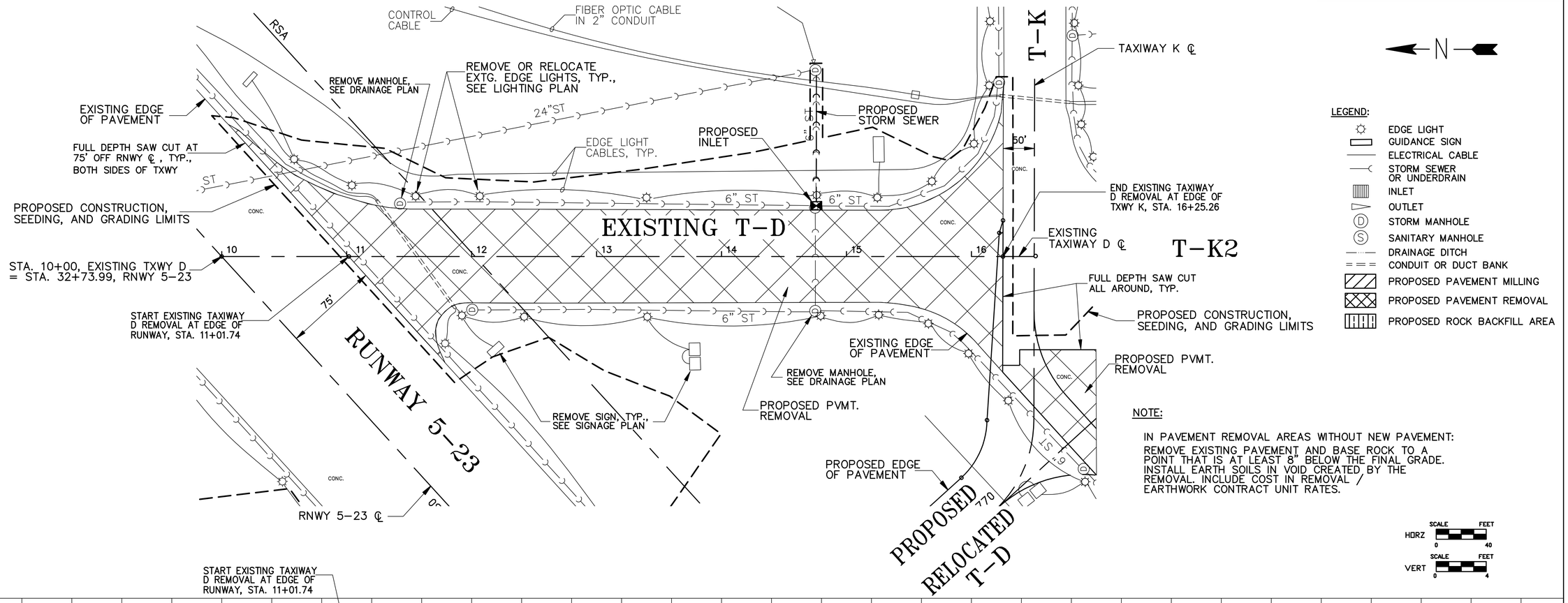
SCALE FEET HORZ 0 40
SCALE FEET VERT 0 4

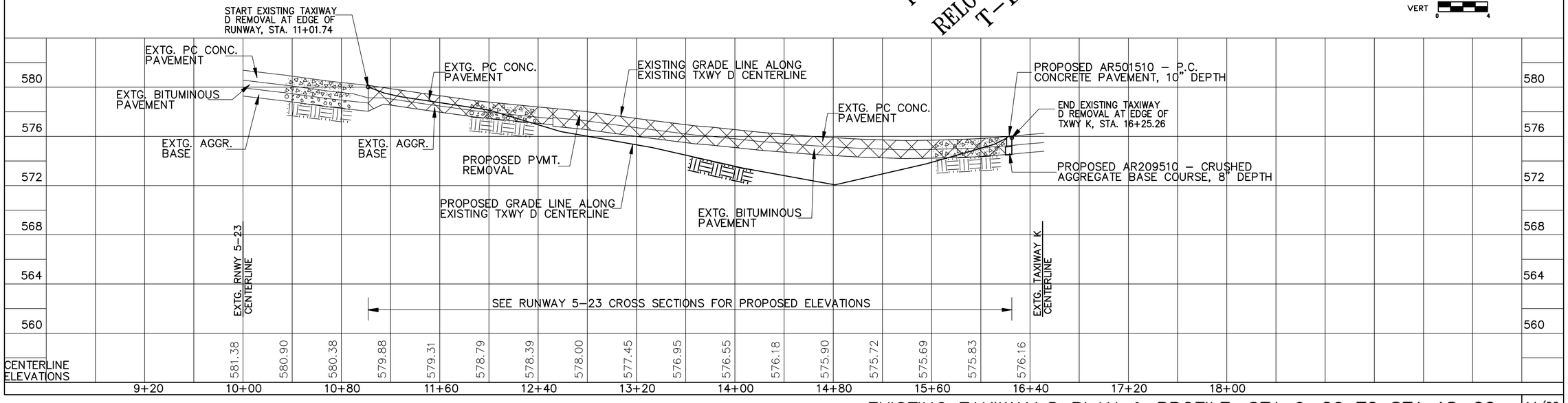


RELOCATED TAXIWAY D PLAN & PROFILE, STA 768+20 TO STA 775+00

13/69

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 14 OF 69





EXISTING TAXIWAY D PLAN & PROFILE, STA 9+20 TO STA 18+00

14/69

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
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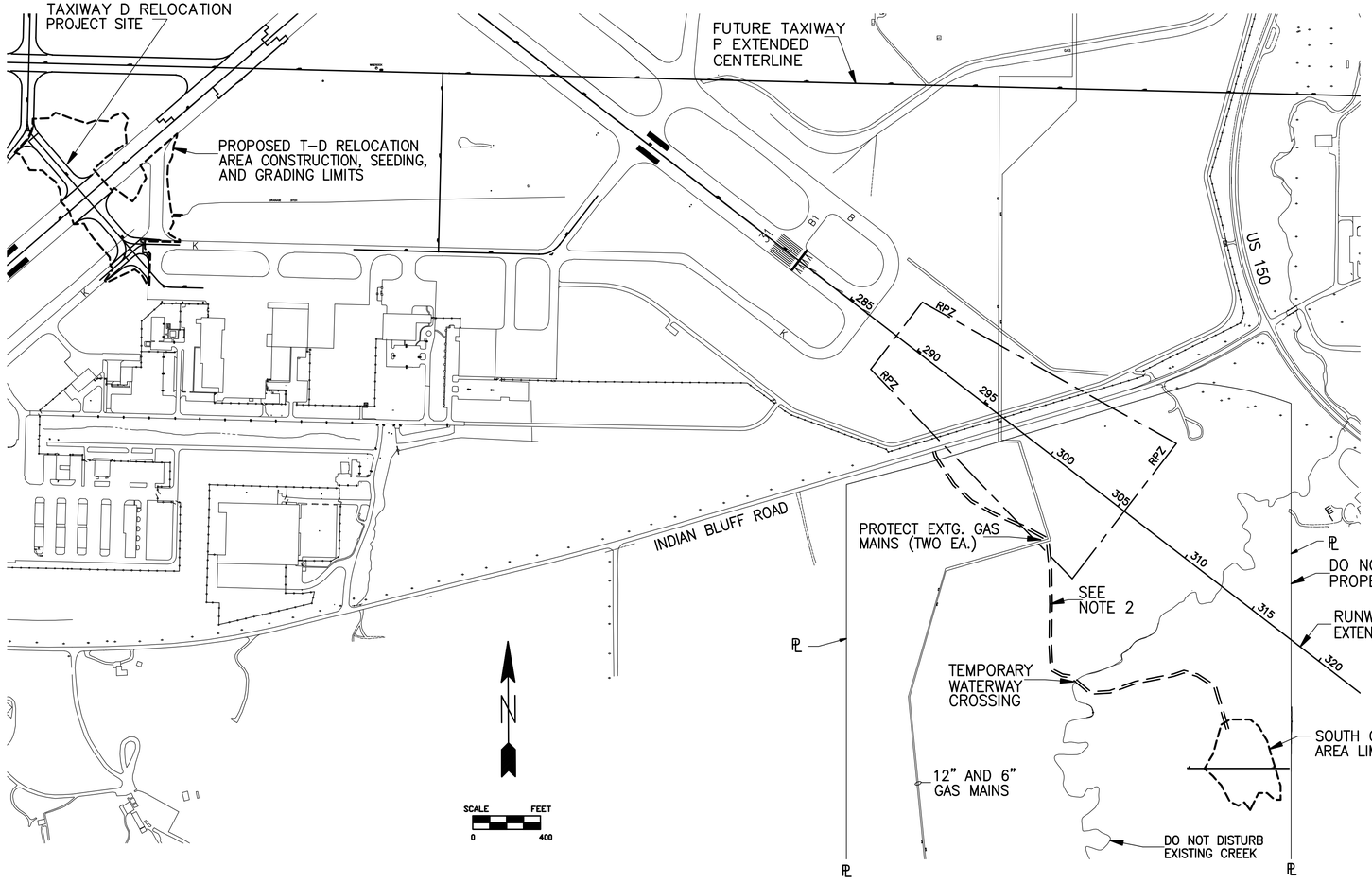
SHEET 15 OF 69



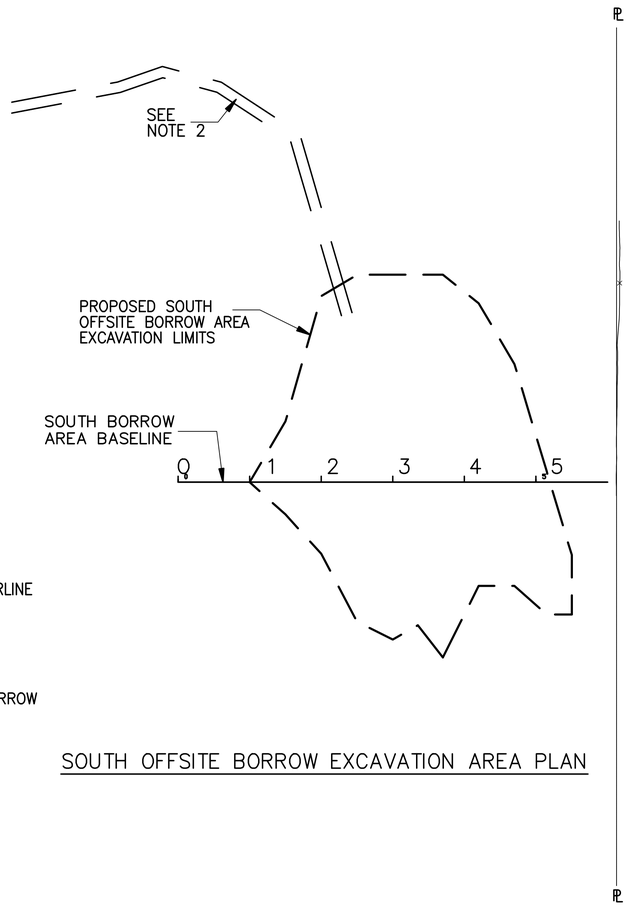
SOUTH OFFSITE BORROW EXCAVATION AREA LOCATION MAP

LEGEND:

- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- FUTURE PAVEMENT
- PROPOSED GRADING LIMITS
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

NOTES:

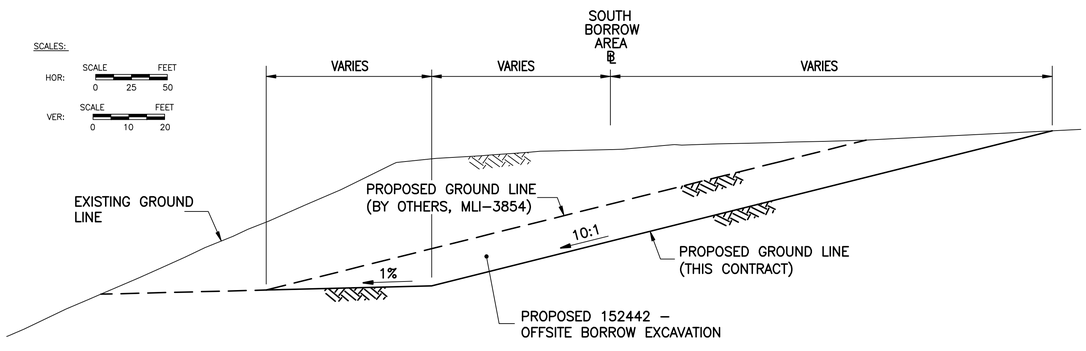
1. SOILS HAULED FROM THE SOUTH OFFSITE BORROW AREA TO THE TAXIWAY D/P CONSTRUCTION SITE AREA SHALL BE PAID FOR BY CUBIC YARDS REMOVED UNDER CONTRACT ITEM 152442, OFFSITE BORROW EXCAVATION .
2. PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD. SUPPLY & INSTALL TEMPORARY CULVERTS AS REQUIRED. EXACT LOCATION TO BE DETERMINE IN THE FIELD AT THE TIME OF CONSTRUCTION. LOCATION OF HAUL ROAD SUBJECT TO THE APPROVAL OF THE RESIDENT ENGINEER AND THE MAA. REMOVE HAUL ROAD AFTER BORROW ACTIVITIES ARE DONE.



SOUTH OFFSITE BORROW EXCAVATION AREA PLAN

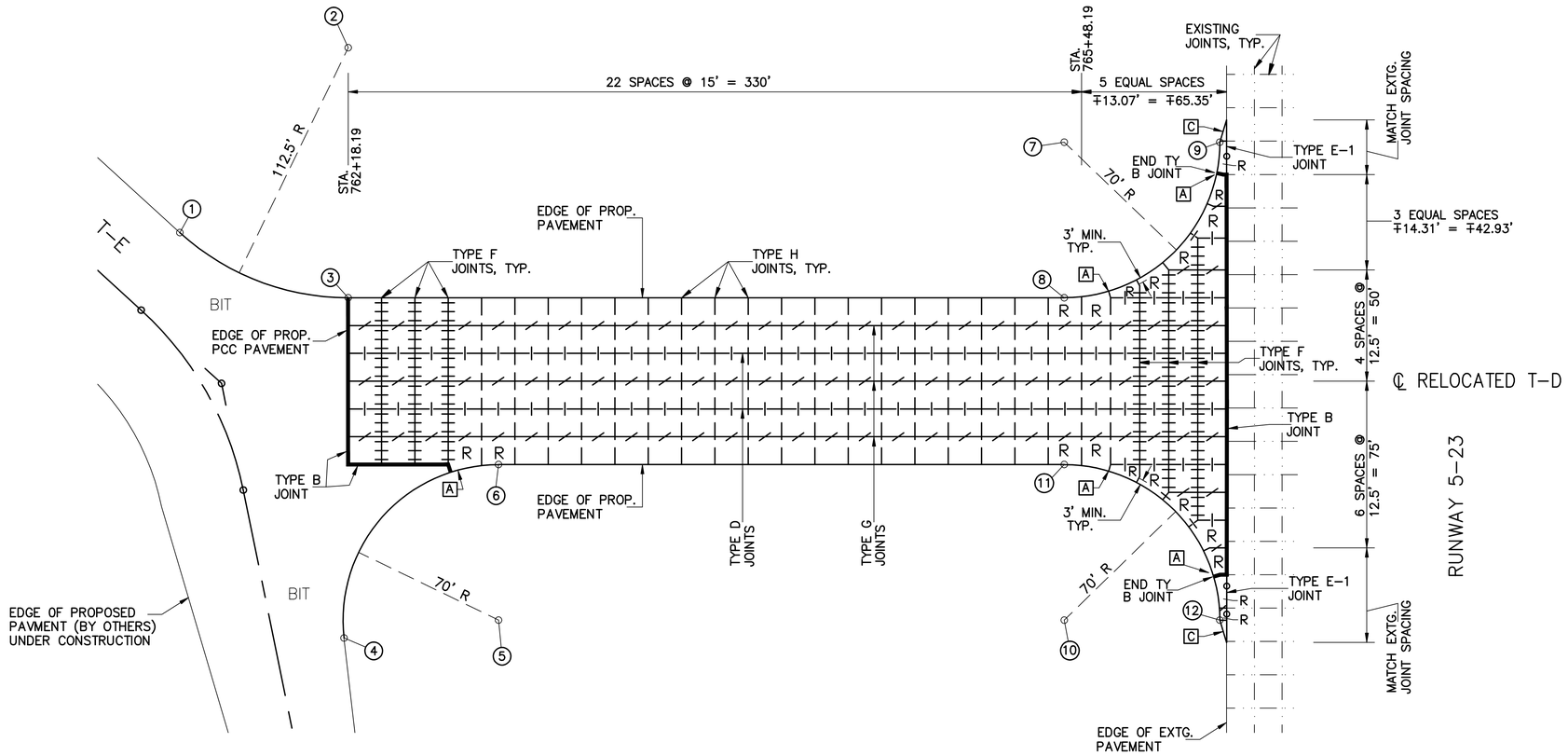
PROPOSED TYPICAL SECTION – SOUTH OFFSITE BORROW EXCAVATION AREA

SOUTH BORROW AREA LEFT, STA. 0+00 TO STA. 6+00



PROPOSED SOUTH OFFSITE BORROW EXCAVATION AREA PLAN

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 16 OF 69



PAVEMENT LAYOUT POINTS			
NUMBER	STATION	OUT	REMARK
①	761+07.37	LT. 37.50'	RAD. PT.
②	762+18.19	LT. 150.00'	PT. OF RAD.
③	762+18.19	LT. 37.50'	RAD. PT.
④	762+17.13	RT. 115.55'	RAD. PT.
⑤	762+85.85	RT. 107.50'	PT. OF RAD.
⑥	762+85.85	RT. 37.50'	RAD. PT.
⑦	765+40.43	LT. 107.50'	PT. OF RAD.
⑧	765+40.43	LT. 37.50'	RAD. PT.
⑨	766+10.43	LT. 107.50'	RAD. PT.
⑩	765+40.43	RT. 107.50'	PT. OF RAD.
⑪	765+40.43	RT. 37.50'	RAD. PT.
⑫	766+10.43	RT. 107.50'	RAD. PT.

NORTHWEST RELOCATED TAXIWAY D PLAN

JOINT SYMBOL LEGEND

- TYPE B–THICKENED EDGE EXPANSION JOINT
- TYPE D–DOWELED CONSTRUCTION JOINT
- TYPE E–1–HINGED CONSTRUCTION JOINT WITHOUT KEY
- TYPE F–DOWELED CONTRACTION JOINT
- TYPE G–HINGED CONTRACTION JOINT
- TYPE H–DUMMY
- R — ODD SHAPE PANEL REINFORCEMENT
- EXISTING PAVEMENT JOINT

JOINT NOTES:

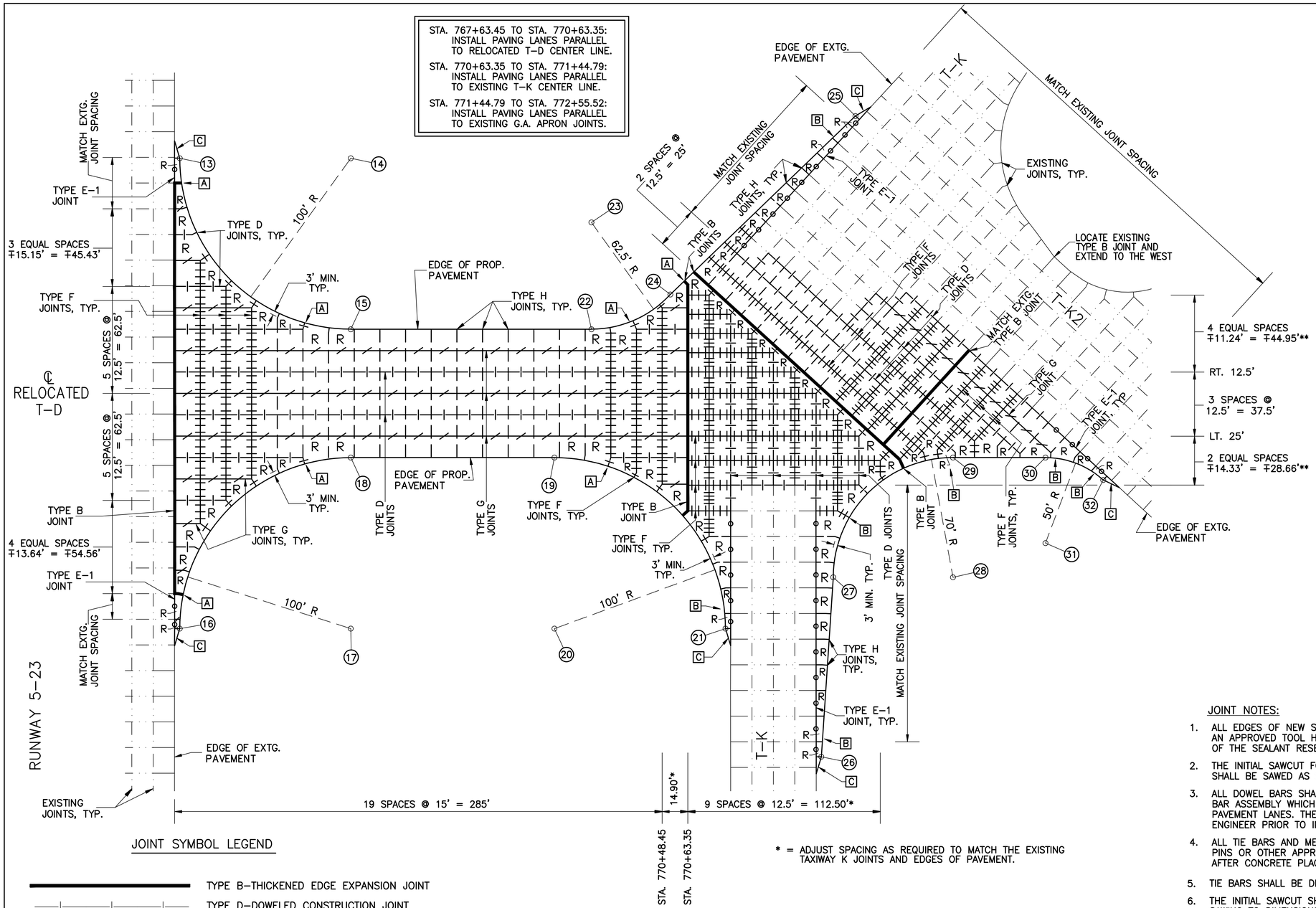
1. ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
2. THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
4. ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
5. TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH AASHTO M137.
6. THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO DIMENSIONS OF THE SECOND SAWCUT WILL BE ALLOWED.
7. SEE SHEET 18 FOR JOINT DETAILS, FILLET STUB DETAILS, FILLET TAPER DETAILS, AND ODD SHAPE PANEL REINFORCEMENT DETAILS.
8. RUNWAY 5–23 / TAXIWAY K / TAXIWAY K–2 EXISTING JOINT LOCATIONS AND SPACING BASED ON ORIGINAL PLANS AND/OR AERIAL PHOTOS. ACTUAL EXISTING JOINT LOCATIONS AND SPACING MAY VARY.
9. [A] = STUB, SEE FILLET STUB DETAIL.
10. [B] = TAPER, SEE FILLET TAPER DETAIL.
11. [C] = BITUMINOUS TAPER, SEE TYPICAL ON SHEET 9.

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013

STA. 767+63.45 TO STA. 770+63.35: INSTALL PAVING LANES PARALLEL TO RELOCATED T-D CENTER LINE.

STA. 770+63.35 TO STA. 771+44.79: INSTALL PAVING LANES PARALLEL TO EXISTING T-K CENTER LINE.

STA. 771+44.79 TO STA. 772+55.52: INSTALL PAVING LANES PARALLEL TO EXISTING G.A. APRON JOINTS.



PAVEMENT LAYOUT POINTS			
NUMBER	STATION	OUT	REMARK
13	767+16.43	LT. 137.50'	RAD. PT.
14	768+66.43	LT. 137.50'	PT. OF RAD.
15	768+66.43	LT. 37.50'	RAD. PT.
16	767+16.43	RT. 137.50'	RAD. PT.
17	768+66.43	RT. 137.50'	PT. OF RAD.
18	768+66.43	RT. 37.50'	RAD. PT.
19	769+85.48	RT. 37.50'	RAD. PT.
20	769+85.48	RT. 137.50'	PT. OF RAD.
21	770+85.48	RT. 137.50'	RAD. PT.
22	770+07.13	LT. 37.50'	RAD. PT.
23	770+07.13	LT. 100.00'	PT. OF RAD.
24	770+53.27	LT. 57.84'	RAD. PT.
25	771+61.46	LT. 162.16'	P.I.
26	771+41.49	RT. 212.42'	P.I.
27	771+48.48	RT. 107.50'	RAD. PT.
28	772+18.48	RT. 107.50'	PT. OF RAD.
29	772+18.48	RT. 37.50'	RAD. PT.
30	772+72.57	RT. 37.50'	RAD. PT.
31	772+72.57	RT. 87.50'	PT. OF RAD.
32	773+06.32	RT. 50.61'	RAD. PT.

** = ADJUST SPACING AS REQUIRED TO MATCH THE PROPOSED RELOCATED TAXIWAY D JOINTS AT LT. 25', LT. 12.5', CENTERLINE AND RT. 12.5'.

* = ADJUST SPACING AS REQUIRED TO MATCH THE EXISTING TAXIWAY K JOINTS AND EDGES OF PAVEMENT.

JOINT SYMBOL LEGEND

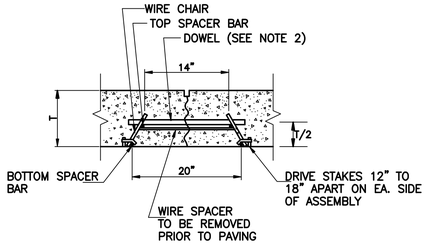
- TYPE B–THICKENED EDGE EXPANSION JOINT
- TYPE D–DOWELED CONSTRUCTION JOINT
- TYPE E–1–HINGED CONSTRUCTION JOINT WITHOUT KEY
- TYPE F–DOWELED CONTRACTION JOINT
- TYPE G–HINGED CONTRACTION JOINT
- TYPE H–DUMMY
- R — ODD SHAPE PANEL REINFORCEMENT
- EXISTING PAVEMENT JOINT

SOUTHEAST RELOCATED TAXIWAY D PLAN

JOINT NOTES:

1. ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
2. THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
4. ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
5. TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH AASHTO M137.
6. THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO DIMENSIONS OF THE SECOND SAWCUT WILL BE ALLOWED.
7. SEE SHEET 18 FOR JOINT DETAILS, FILLET STUB DETAILS, FILLET TAPER DETAILS, AND ODD SHAPE PANEL REINFORCEMENT DETAILS.
8. RUNWAY 5-23 / TAXIWAY K / TAXIWAY K-2 EXISTING JOINT LOCATIONS AND SPACING BASED ON ORIGINAL PLANS AND/OR AERIAL PHOTOS. ACTUAL EXISTING JOINT LOCATIONS AND SPACING MAY VARY.
9. [A] = STUB, SEE FILLET STUB DETAIL.
10. [B] = TAPER, SEE FILLET TAPER DETAIL.
11. [C] = BITUMINOUS TAPER, SEE TYPICAL ON SHEET 9.

RELOCATED TAXIWAY D JOINT PLAN



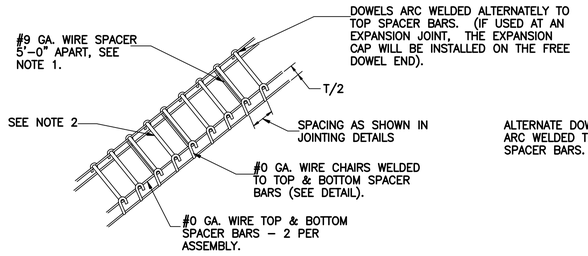
INSTALLATION DETAIL

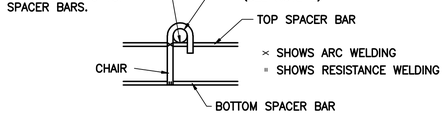
BASKET ASSEMBLY NOTES:

1.) #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.

2.) DOWELS SHALL BE EPOXY COATED IN ACCORDANCE TO THE REQUIREMENTS OF AASHTO M254. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE GREASED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.

3.) ADJUST HEIGHT OF THE BASKET ASSEMBLIES (IN THE FIELD AT THE TIME OF CONSTRUCTION) SO THAT THE DOWELS ARE IN THE MIDDLE OF THE PROPOSED CONCRETE SLAB.

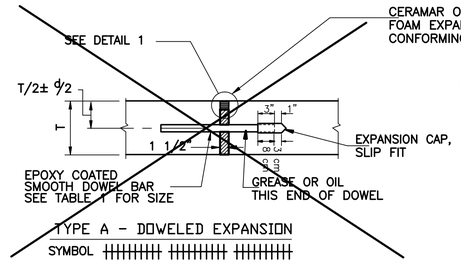




TYPICAL ELEVATION DETAIL SHOWING CHAIR

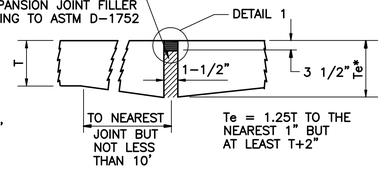
DOWEL BASKET ASSEMBLY DETAILS AND NOTES

* = IN LOCATIONS WHERE PROPOSED PAVEMENTS MEET EXISTING PAVEMENTS INCREASE DEPTH OF PROPOSED TO MATCH THE EXISTING PAVEMENT DEPTH.



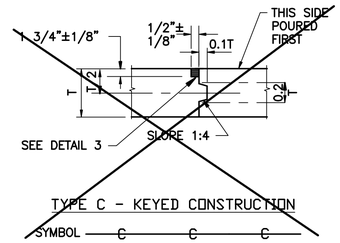
TYPE A - DOWELED EXPANSION

SYMBOL



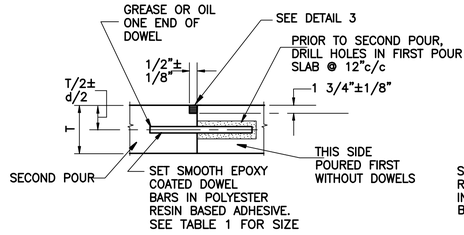
TYPE B - THICKENED EDGE EXPANSION

SYMBOL



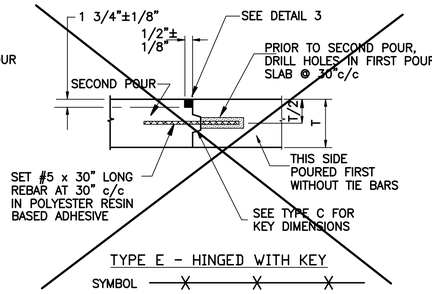
TYPE C - KEYED CONSTRUCTION

SYMBOL C C C



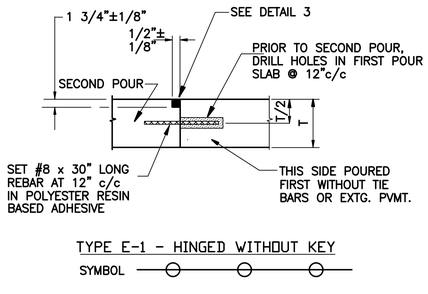
TYPE D - DOWELED CONSTRUCTION

SYMBOL



TYPE E - HINGED WITH KEY

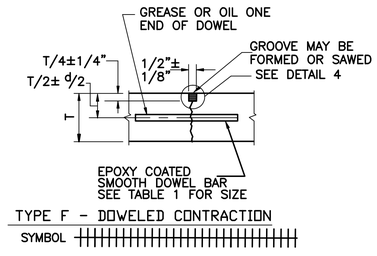
SYMBOL X X X



TYPE E-1 - HINGED WITHOUT KEY

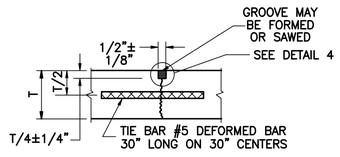
SYMBOL

TYPE E-1 JOINT NOTE: THE TYPE E-1 JOINT SHALL BE A CONSTRUCTION BUTT JOINT WITH TIE BARS. THE TIE BARS WILL BE USED TO TRANSFER THE LOAD ACROSS THE JOINT.



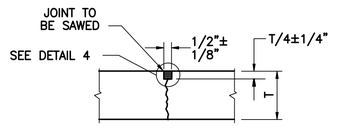
TYPE F - DOWELED CONTRACTION

SYMBOL



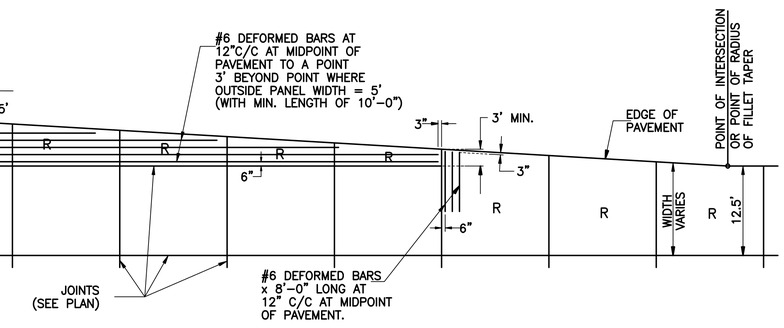
TYPE G - HINGED CONTRACTION

SYMBOL

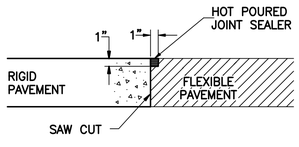


TYPE H - DUMMY CONTRACTION

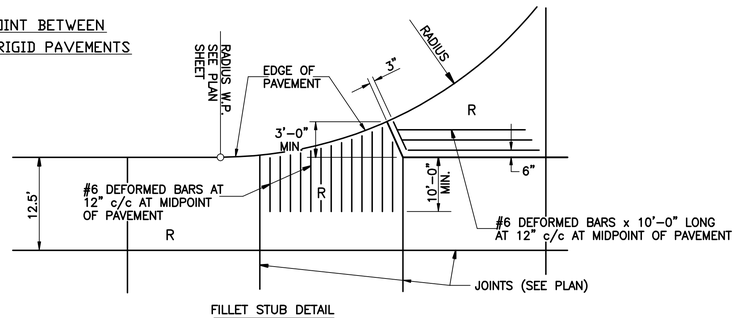
SYMBOL



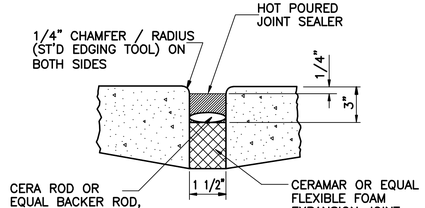
FILLET TAPER DETAIL



DETAIL - JOINT BETWEEN FLEXIBLE AND RIGID PAVEMENTS

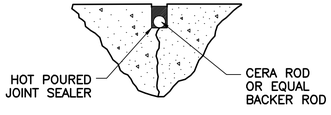


FILLET STUB DETAIL

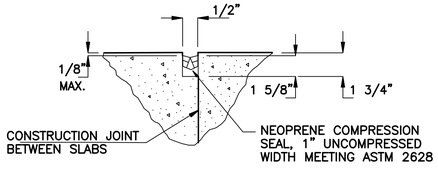


DETAIL 1

JOINT TYPE A & B

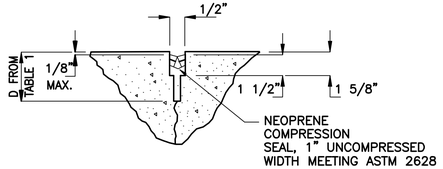


DETAIL 2



DETAIL 3

JOINT TYPES C, D, AND E

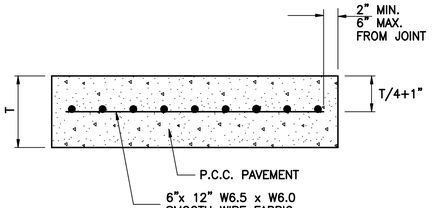


DETAIL 4

JOINT TYPES F, G, AND H

TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT D, INCHES D=(T/4)±1/4"	STEEL DOWELS		
		DIAMETER (INCHES)	LENGTH (INCHES)	SPACING (INCHES)
8	2.00	1.00	19	12
9	2.25	1.00	19	12
10	2.50	1.00	19	12
12	3.00	1.00	19	12
15	3.75	1.25	20	15
17	4.25	1.50	20	18



ODD SHAPED PANEL REINFORCEMENT

SYMBOL R

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SCALE 40 0 40 80 FEET

T-P

N

76.80 GRADE REFERENCE POINT

PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS.

THE EXISTING TAXIWAY E EDGE OF PAVEMENT GRADES SHOWN ON THIS SHEET ARE BASED ON PROPOSED PAVEMENT CONSTRUCTION (SPRING 2010). PRIOR TO CONSTRUCTION OF THIS PROJECT, THE ACTUAL T-E EOP ELEVATIONS SHALL BE SURVEYED AND ALL TAXIWAY D PROPOSED GRADES NORTHWEST OF STATION 763+25 SHALL BE ADJUSTED TO MATCH THE ACTUAL SURVEYED ELEVATIONS.

RUNWAY 5-23

EDGE OF EXTG. PVMT.

EDGE OF PROPOSED PAVMENT (BY OTHERS) UNDER CONSTRUCTION

EDGE OF PROP. PVMT.

25' SPACING (TYPICAL)

℄ CROWN

SHOULDER EMBANKMENT AREA, SEE NOTES 2 & 3.

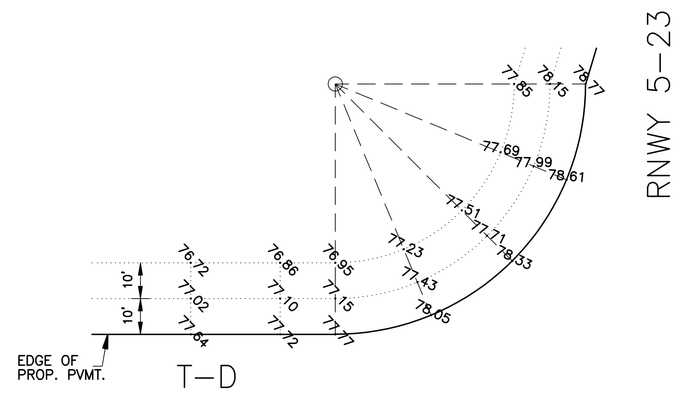
PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS.

LT. 62.5'
LT. 37.5'
LT. 12.5'
℄ RELOCATED T-D
RT. 12.5'
RT. 37.5'
RT. 62.5'

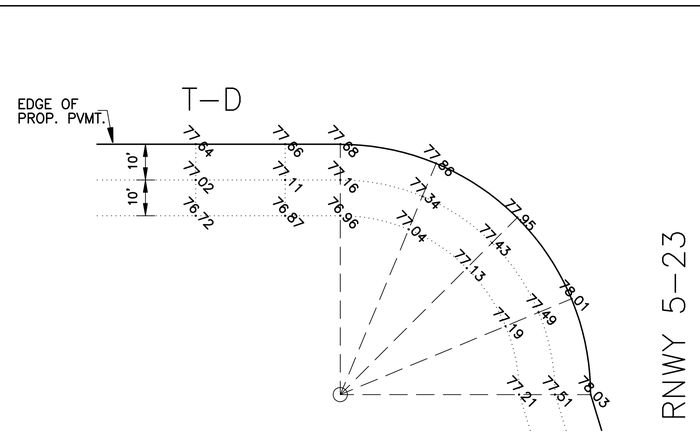
STA. 763+00
STA. 764+00
STA. 765+00
STA. 766+00

T-E

NORTHWEST RELOCATED TAXIWAY D PLAN



LEFT (NORTH) FILLET SHOULDER GRADES



RIGHT (SOUTH) FILLET SHOULDER GRADES

NOTES:

1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.

2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.

3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.

4.) 76.80 = PROPOSED ELEVATION 576.80.

5.) 76.60 = EXISTING ELEVATION 576.60.

RELOCATED TAXIWAY D STAKING PLAN

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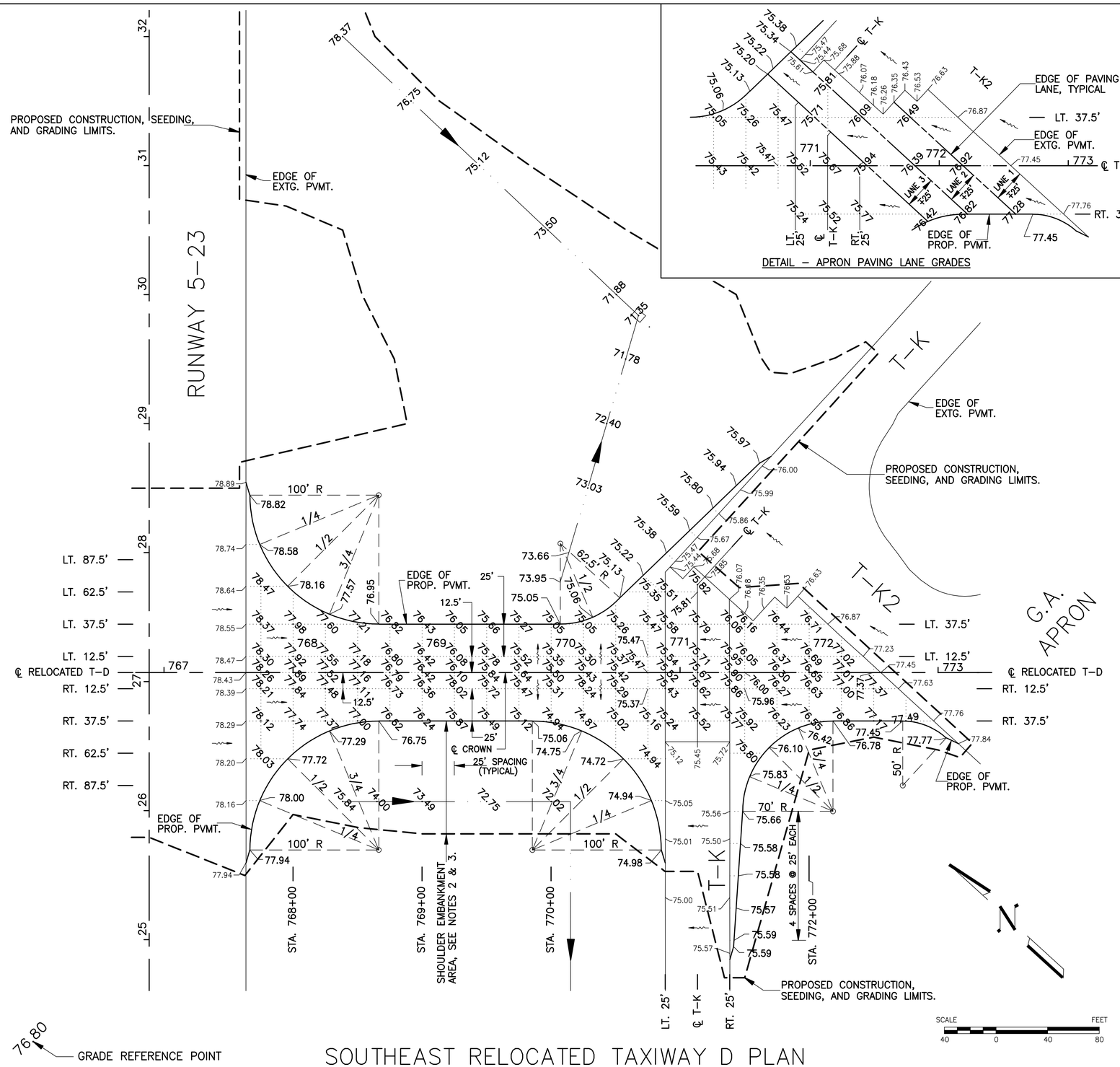
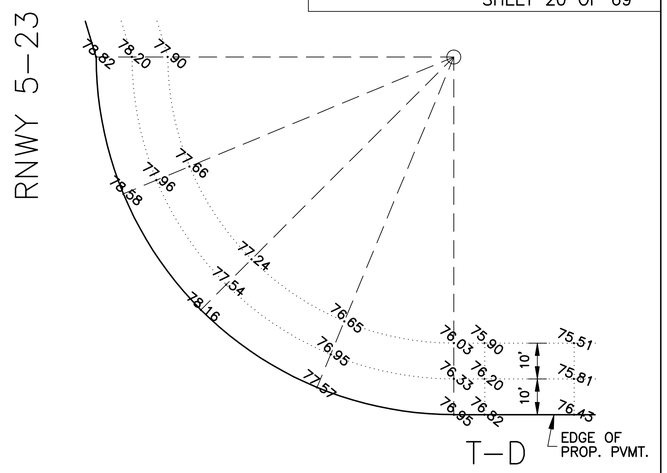
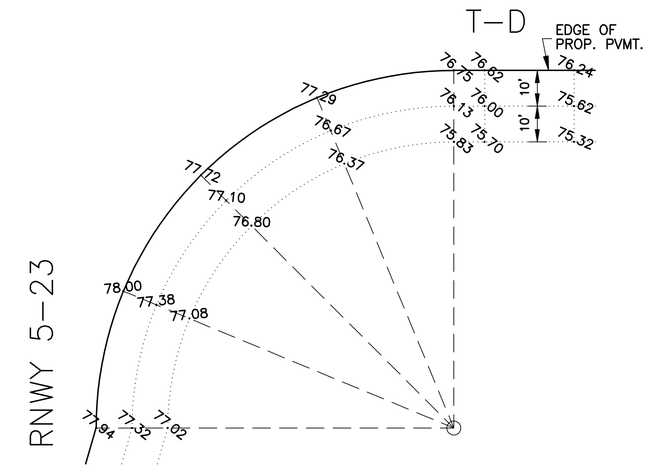


DETAIL – APRON PAVING LANE GRADES



LEFT (NORTH) FILLET SHOULDER GRADES



RIGHT (SOUTH) FILLET SHOULDER GRADES

NOTES:

1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.

2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.

3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.

4.) 76.80 = PROPOSED ELEVATION 576.80.

5.) 76.60 = EXISTING ELEVATION 576.60.

76.80 GRADE REFERENCE POINT

SOUTHEAST RELOCATED TAXIWAY D PLAN

RELOCATED TAXIWAY D STAKING PLAN

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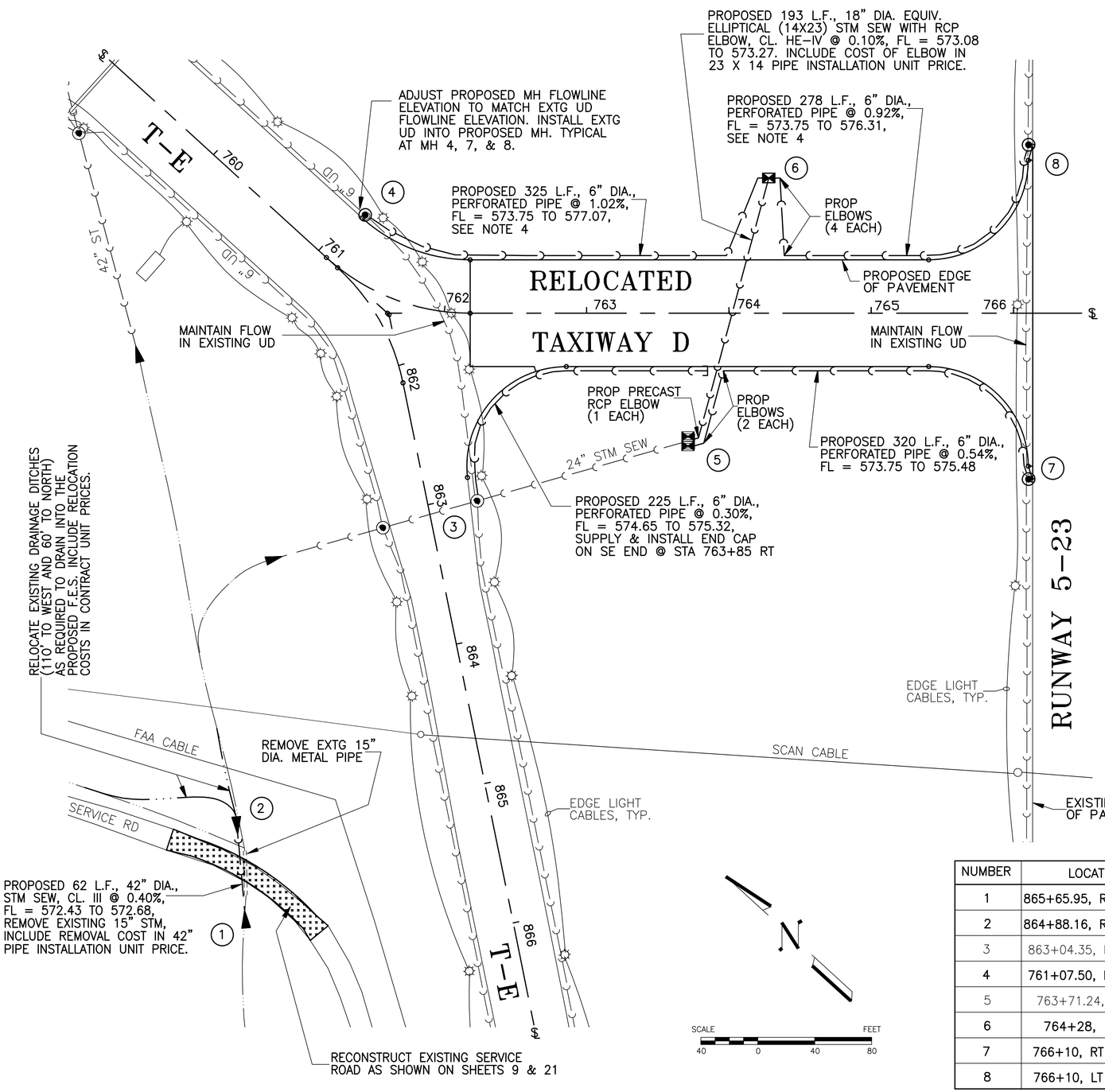

GENERAL DRAINAGE NOTES:

1. SEE PLAN & PROFILE SHEETS FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL HAND DIG AROUND ALL EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR COST OF REPAIRING ALL DAMAGE UTILITIES.
2. OUTLET PROPOSED UNDER DRAIN INTO EXISTING / PROPOSED STM. SEW. PER DETAIL.
3. CONTRACTOR TO EXCAVATE AND EXPOSE EXISTING STORM PIPES, ELEC. CABLES, AND ELEC. DUCT BANKS AT PROPOSED MANHOLE, INLET, AND PIPE CROSSING LOCATIONS TO FIELD VERIFY VERTICAL LOCATIONS OF EXISTING UTILITIES. ENGINEER MAY ADJUST SLOPES AND TIE-INS AT THE TIME OF CONSTRUCTION AS REQUIRED.
4. THE LOCATIONS OF THE EXISTING UNDERDRAINS SHOWN ON THESE DRAWINGS WERE TAKEN FROM AIRPORT RECORDS AND ARE APPROXIMATE. PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL EXCAVATE, EXPOSE, AND FIELD VERIFY THE HORIZONTAL LOCATION, VERTICAL LOCATION, AND SIZE OF THE EXISTING CLAY UNDERDRAINS. ELEVATIONS, SLOPES, AND PIPE SIZES SHALL BE ADJUSTED BY THE RESIDENT ENGINEER IN THE FIELD AT THE TIME OF CONSTRUCTION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT RATES.
5. PLUG EXISTING PIPES TO BE ABANDONED IN PLACE. THE PLUGGING OF EXISTING PIPES TO BE ABANDONED IN PLACE SHALL BE ACCOMPLISHED WITH BRICKS AND GROUT (3,500 LBS. @ 28 DAY MIX) TO THE SATISFACTION OF THE RESIDENT ENGINEER. COST FOR PLUGGING EXISTING PIPES SHALL BE INCLUDED IN THE CONTRACT 701 UNIT PRICES. NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK.

LEGEND:

- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- (1) PROPOSED / EXISTING STRUCTURE NUMBER
- RECONSTRUCT EXISTING 13' WIDE FLUSH SERVICE ROAD (10" CRUSHED AGGREGATE ON GEOTEX)

STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	℄ ELEV.	LID OR GRATE ELEV.	REMARK
1	865+65.95, RT 184.58'	PRC FLARED END SECTION, 42" DIA.	572.40'	---	AR752442
2	864+88.16, RT 175.79'	PRC FLARED END SECTION, 42" DIA.	572.71'	---	AR752442
3	863+04.35, LT 34.60'	EXISTING MANHOLE	PROP EAST UD =574.65' EXTG FLOWLINE, ∓572.97'	578.72'	EXISTING, TO REMAIN
4	761+07.50, LT 40.50'	MANHOLE, SPL., 2' DIA.	PROP SOUTH UD =577.07'* MATCH EXTG UD, ∓577.38'*	579.98'*	AR751570
5	763+71.24, RT 90'	EXISTING DOUBLE INLET	EXTG FLOWLINE, ∓573.08' PROP E UD =573.75'	575.32'	EXISTING, TO REMAIN
6	764+28, LT 95'	INLET SPECIAL	573.27' PROP N/S UD =573.75'	575.30'	NEENAH R-3475, AR751415
7	766+10, RT 117.50'	MANHOLE, SPL., 2' DIA.	PROP NORTH UD =575.48' MATCH EXTG UD, ∓572.50'*	577.80'	AR751570
8	766+10, LT 117.50'	MANHOLE, SPL., 2' DIA.	PROP NORTH UD =576.31' MATCH EXTG UD, ∓572.90'*	578.56'	AR751570

* = CONFIRM ELEVATION IN FIELD AT TIME OF CONSTRUCTION PRIOR TO ORDERING MATERIALS.

WEST DRAINAGE PLAN, STA 760+00 TO STA 767+00

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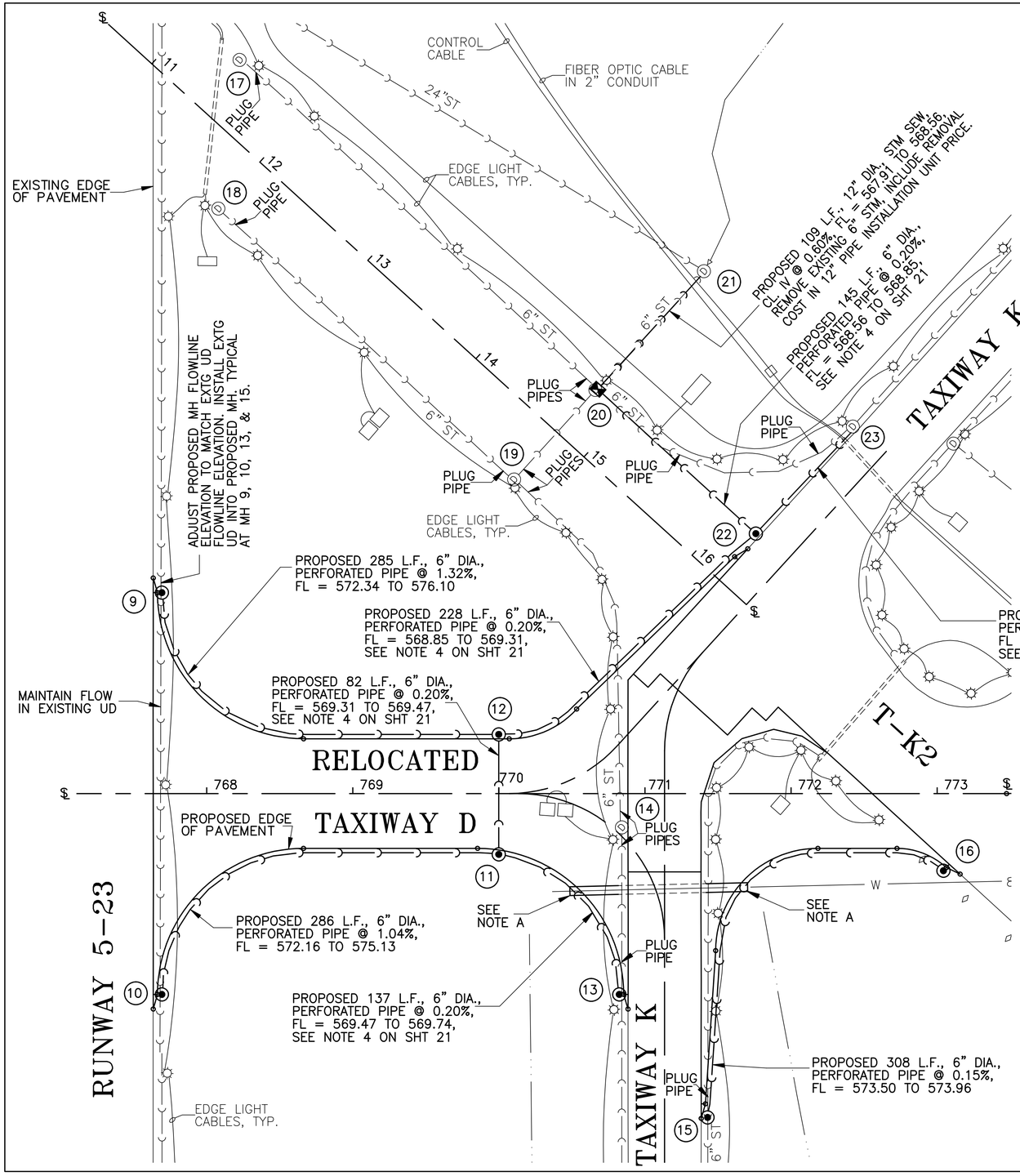
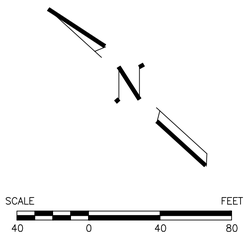


NOTES:

1. SEE SHEET 21 FOR GENERAL DRAINAGE.

A. = EXISTING 8" DIA. WATERMAIN IN 18" DIA. CONCRETE PIPE CASING CULVERT. UN-PLUG THE EXIST ENDS OF THE CASING PIPE. USE 18" DIA., CL II, PRECAST CONCRETE PIPE HALF FLUMES WITH VERTICAL PIPE COUPLERS PER DETAIL ON SHEET 25 TO EXTEND EXISTING CONCRETE PIPE CASING CULVERT TO A POINT THAT IS 3' OUTSIDE OF THE PROPOSED EDGE OF PAVEMENT. PAYMENT BY LINEAR FOOT OF EXTENSION UNDER CONTRACT ITEM AR701318 – 18" RCP, CL II. INSTALL PIPE SPACERS BETWEEN 18" CASING PIPE AND 8" WATERMAIN. SEAL ENDS OF CASING PIPE EXTENSIONS. PROPOSED 18" DIA. RCP CASING EXTENSION LENGTHS: NORTH SIDE = 40 L.F. AND SOUTH SIDE = 32 L.F.



LEGEND:

- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- (1) PROPOSED / EXISTING STRUCTURE NUMBER

STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	℄ ELEV.	LID OR GRATE ELEV.	REMARK
9	767+69.43, LT 137.50'	MANHOLE, SPL., 2' DIA.	PROP SOUTH = 576.10'* MATCH EXTG UD, ∓573.91'*	578.53'	AR751570
10	767+69.43, RT 137.50'	MANHOLE, SPL., 2' DIA.	PROP SOUTH = 575.13'* MATCH EXTG UD, ∓573.50'*	577.67'	AR751570
11	770+00, RT 41.56'	MANHOLE, SPL., 2' DIA.	PROP NORTH = 572.16' PROP E/W = 569.47'	574.67'	AR751570
12	770+00, LT 40.50'	MANHOLE, SPL., 2' DIA.	PROP NORTH = 572.34' PROP E/W = 569.31'	574.78'	AR751570
13	770+82.50, RT 137.50'	MANHOLE, SPL., 2' DIA.	PROP NORTH = 569.74'* MATCH EXTG UD, ∓570.45'*	574.71'	AR751570
14	770+85.25, RT 23.57'	EXISTING MANHOLE	570.05'	575.00'	REMOVE EXISTING, AR751903
15	771+42.89, RT 221.75'	MANHOLE, SPL., 2' DIA.	PROP EAST = 573.50'* MATCH EXTG UD, ∓573.25'*	575.32'	AR751570
16	773+04.30, RT 52.83'	MANHOLE, SPL., 2' DIA.	573.96'	577.54'	AR751570
17	11+42.4, LT 40.75'	EXISTING MANHOLE	574.76'	578.98'	REMOVE EXISTING, AR751903
18	768+04.42, LT 406.25'	EXISTING MANHOLE	573.36'	578.76'	REMOVE EXISTING, AR751903
19	14+76, RT 40.5'	EXISTING MANHOLE	569.17'	575.38'	REMOVE EXISTING, AR751903
20	770+67.75, LT 276.74'	PROPOSED INLET SPECIAL AND EXISTING MANHOLE	PROP = 568.56' EXTG = 568.74'	PROP = 571.35' EXTG = 575.52'	NEENAH R-3475, AR751415 & AR751903
21	771+40.21, LT 358.23'	EXISTING MANHOLE	PROP WEST = 567.91' EXTG = 566.60'	572.50'	EXISTING, TO REMAIN
22	771+75.90, LT 177.92'	MANHOLE, SPL., 2' DIA.	568.85'	575.81'	AR751570
23	772+41.71, LT 248'	EXISTING MANHOLE	PROP WEST = ∓569.54'* MATCH EXTG UD, ∓569.54'*	576.26'	EXISTING, TO REMAIN

* = CONFIRM ELEVATION IN FIELD AT TIME OF CONSTRUCTION PRIOR TO ORDERING MATERIALS.

EAST DRAINAGE PLAN, STA 767+00 TO STA 773+00

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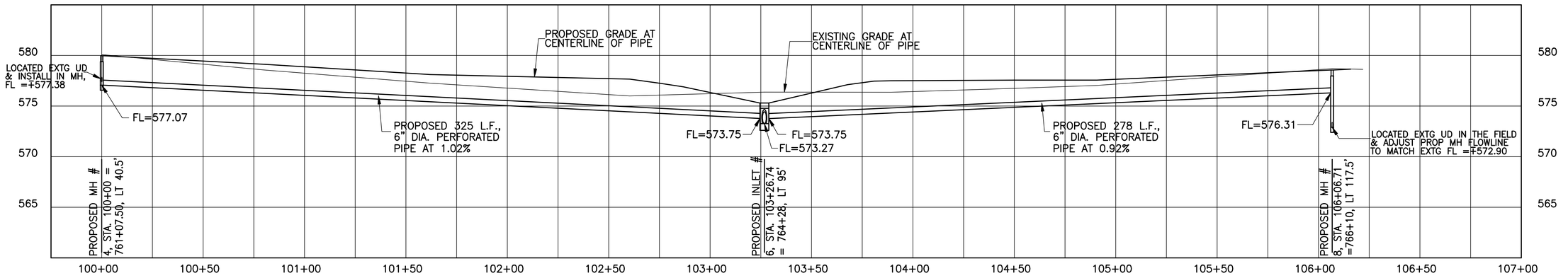
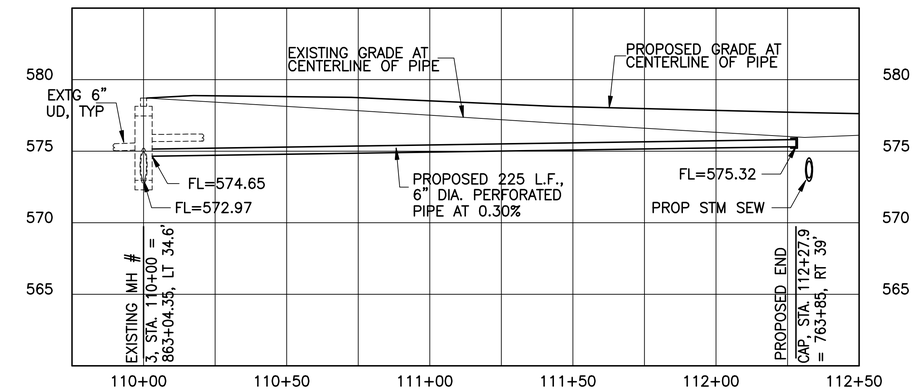
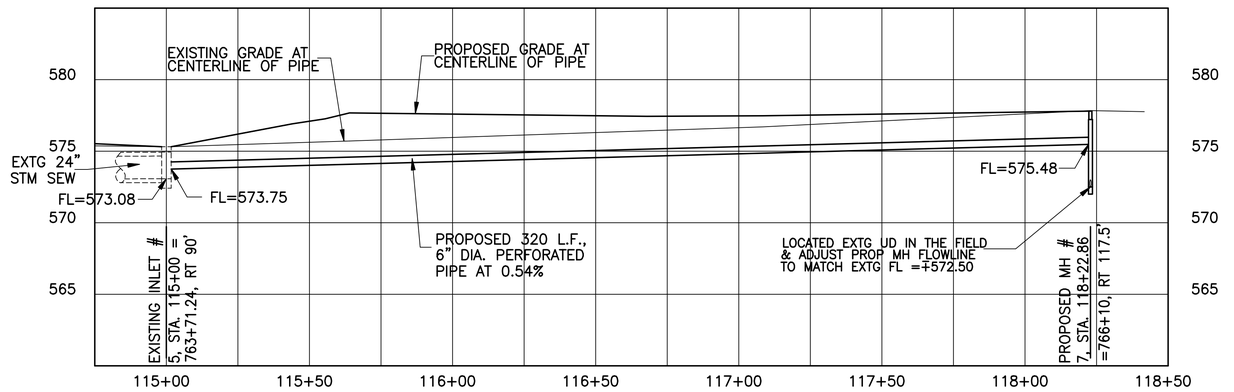
RELOCATED TAXIWAY D: LEFT, STA. 761+07.50 TO STA. 766+10; UNDERDRAIN CENTERLINE PROFILE



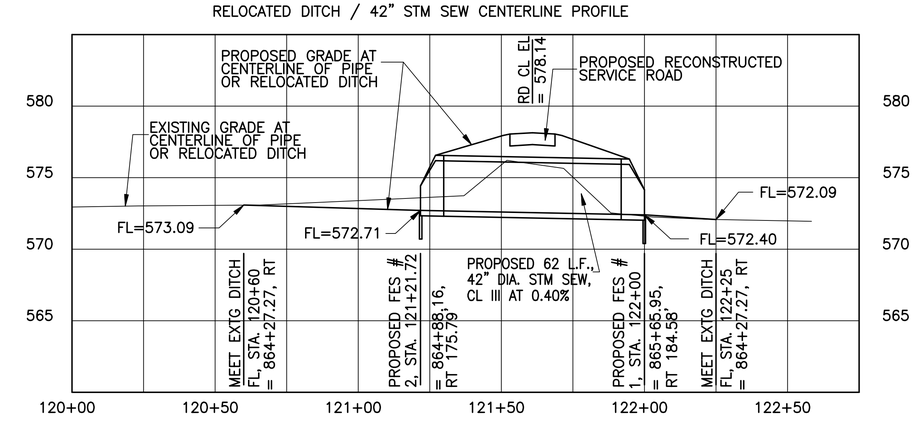
RELOCATED TAXIWAY D: RIGHT, STA. 863+04.35 TO STA. 766+10; UNDERDRAIN CENTERLINE PROFILE

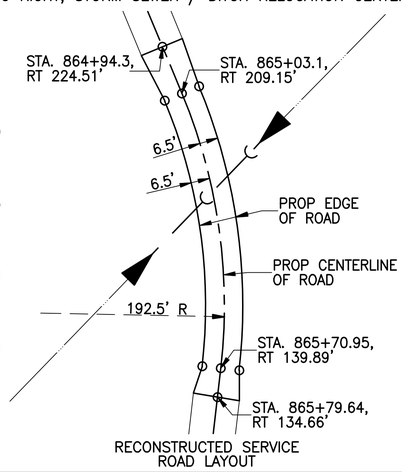




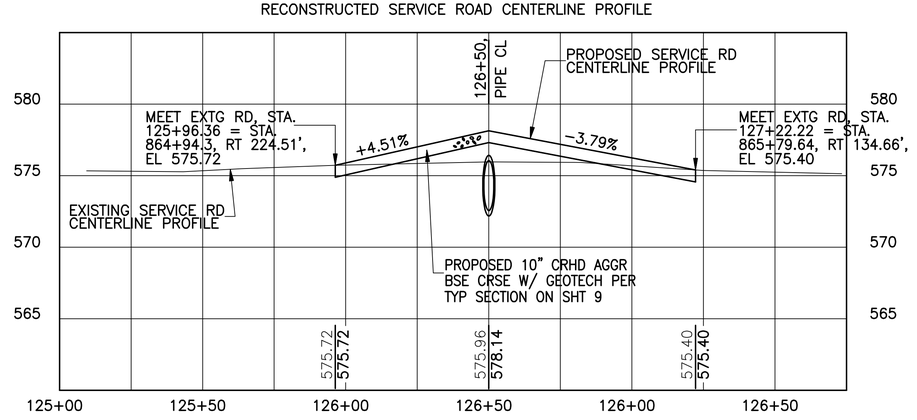
TAXIWAY E, STA. 864+25 TO STA. 865+70 RIGHT, STORM SEWER / DITCH RELOCATION CENTERLINE PROFILE

RELOCATED DITCH / 42" STM SEW CENTERLINE PROFILE





RECONSTRUCTED SERVICE ROAD CENTERLINE PROFILE



DRAINAGE – PIPE PROFILES

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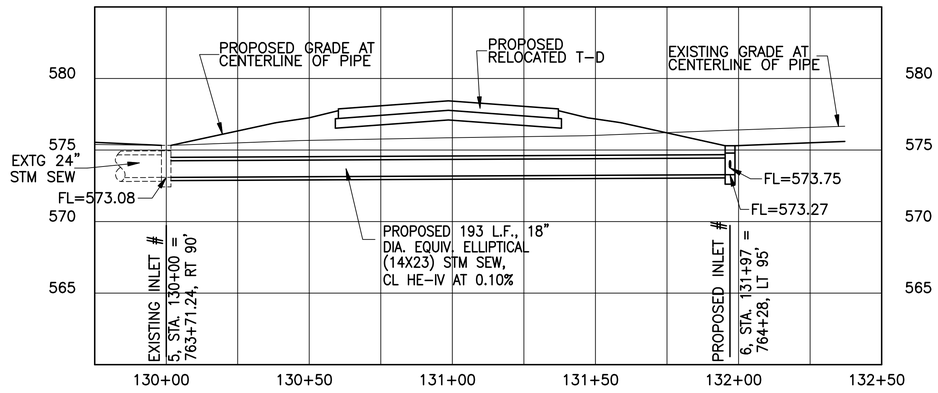
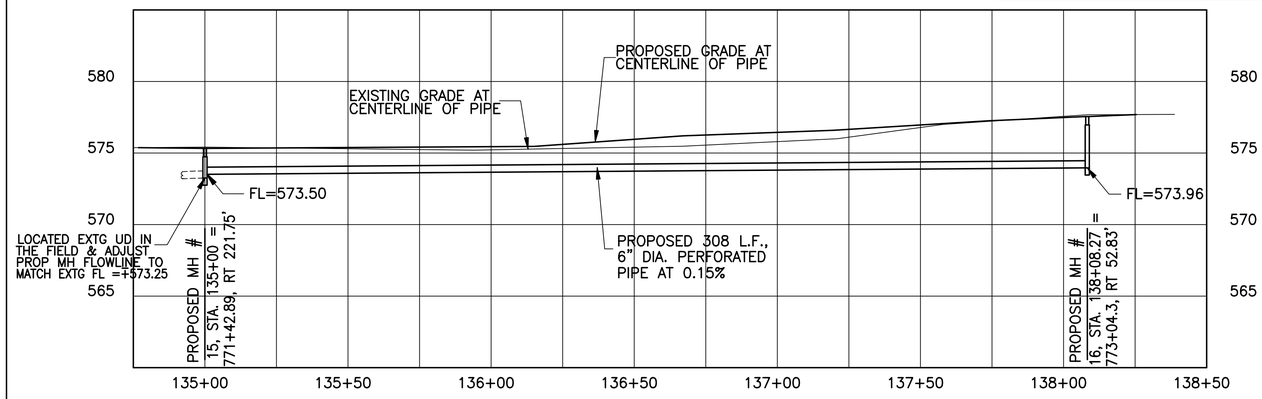
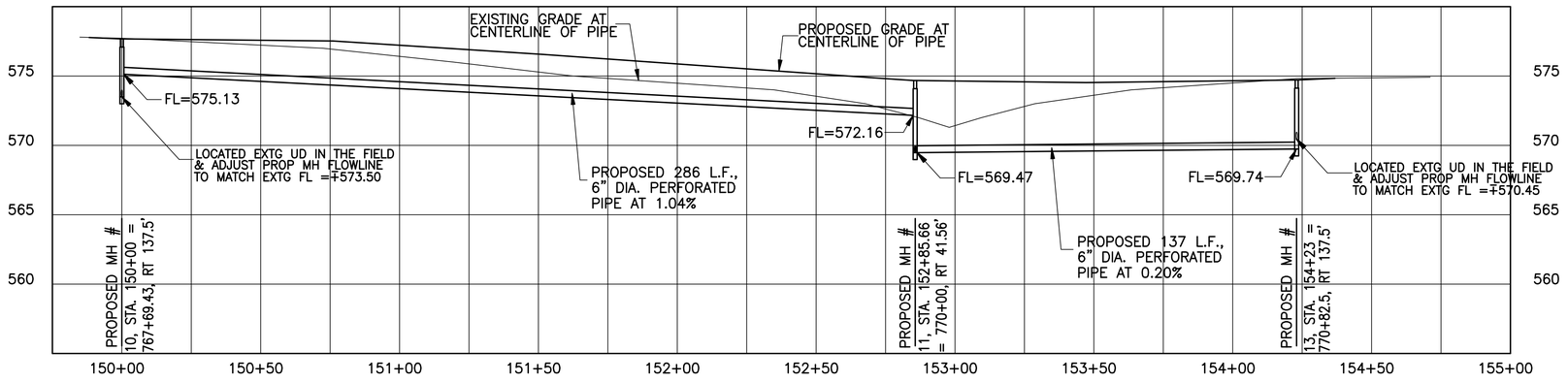
RELOCATED TAXIWAY D, STA. 763+71.24, RIGHT TO STA. 764+28 LEFT, STORM SEWER CENTERLINE PROFILE



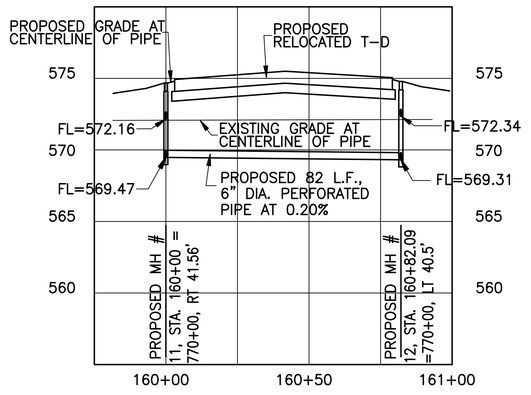
RELOCATED TAXIWAY D: RIGHT, STA. 771+42.89 TO STA. 773+04.30; UNDERDRAIN CENTERLINE PROFILE



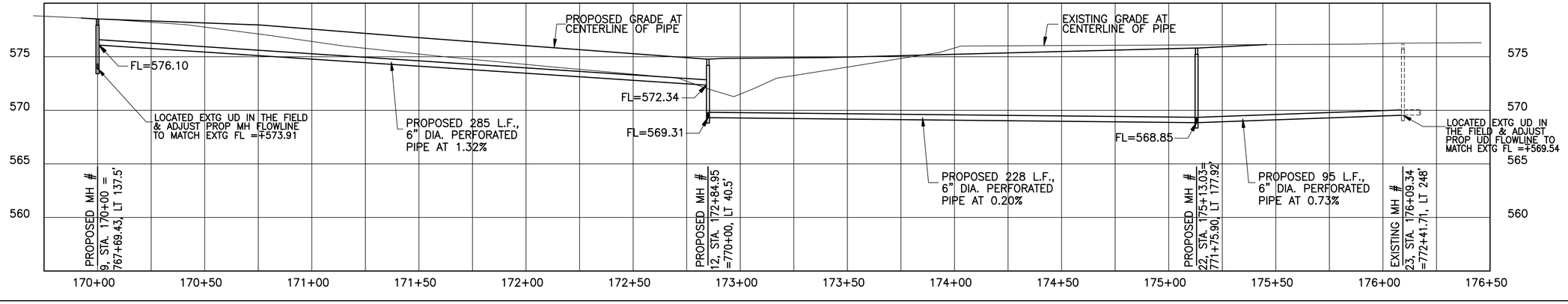
RELOCATED TAXIWAY D: RIGHT, STA. 767+69.43 TO STA. 770+82.50; UNDERDRAIN CENTERLINE PROFILE



RELOCATED TAXIWAY D: LEFT & RIGHT STA. 770+00; UNDERDRAIN CENTERLINE PROFILE

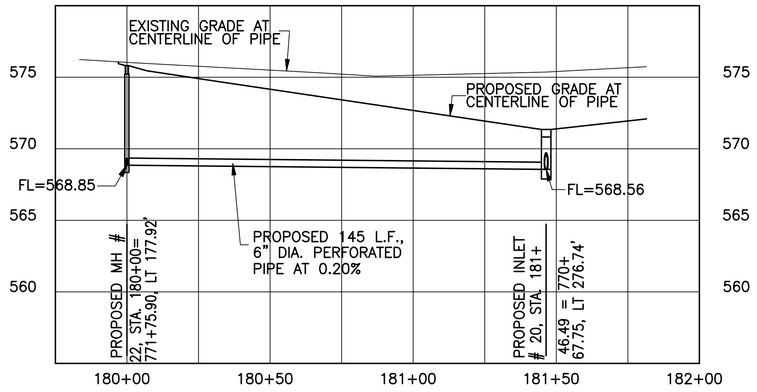


RELOCATED TAXIWAY D: LEFT, STA. 767+69.43 TO STA. 772+41.71; UNDERDRAIN CENTERLINE PROFILE

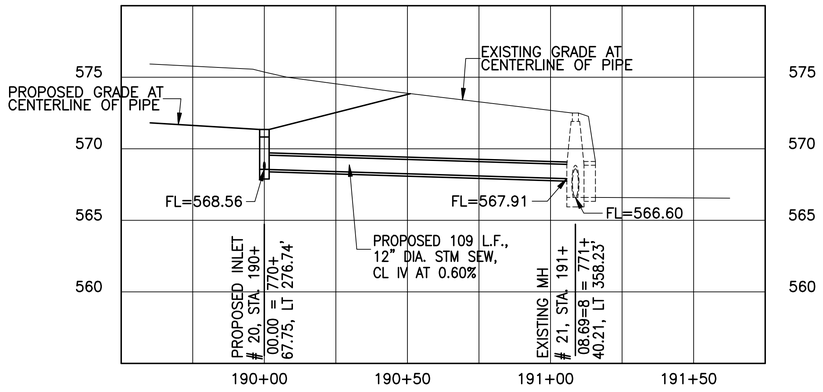


DRAINAGE – PIPE PROFILES

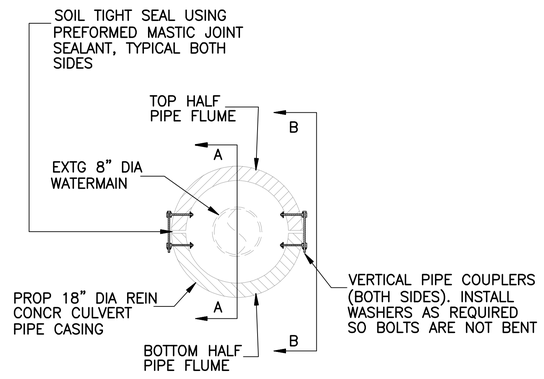
EXISTING TAXIWAY K: UNDERDRAIN OUTLET CENTERLINE PROFILE



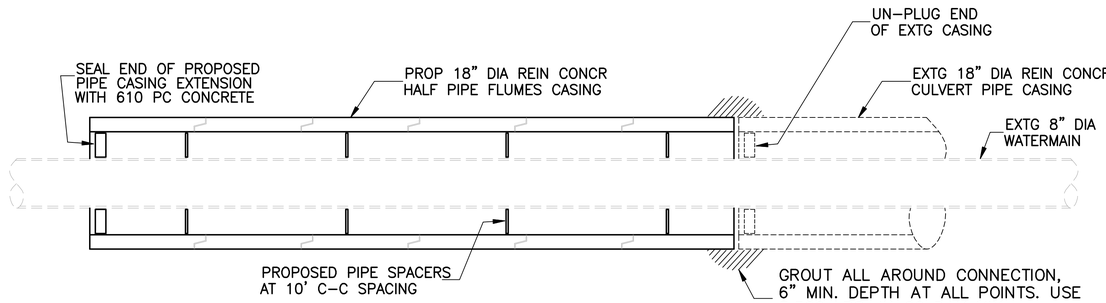
TAXIWAY K LEFT: 12" DIA. STORM SEWER CENTERLINE PROFILE



EXISTING TAXIWAY K 18" DIA. CONCRETE PIPE CASING CULVERT EXTENSION DETAILS
(TYPICAL, BOTH SIDES OF TAXIWAY K)



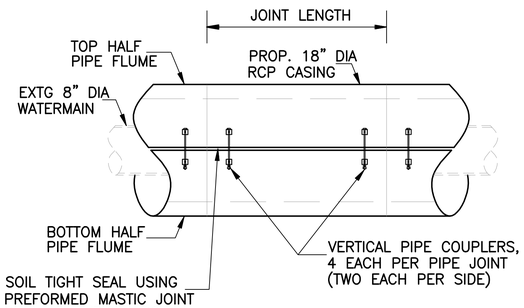
CASING CROSS SECTION



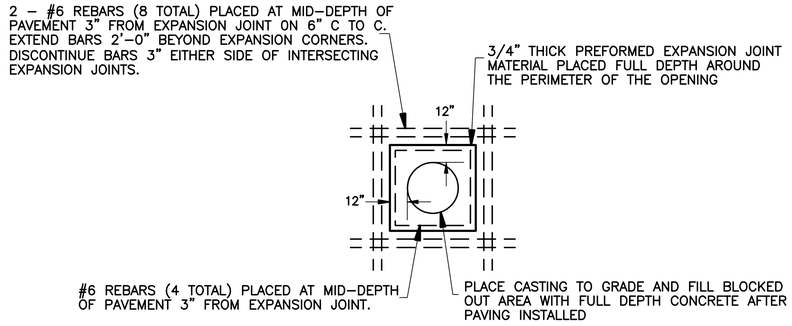
BACKFILL PER TYPICAL PIPE TRENCH DETAIL

SECTION A-A

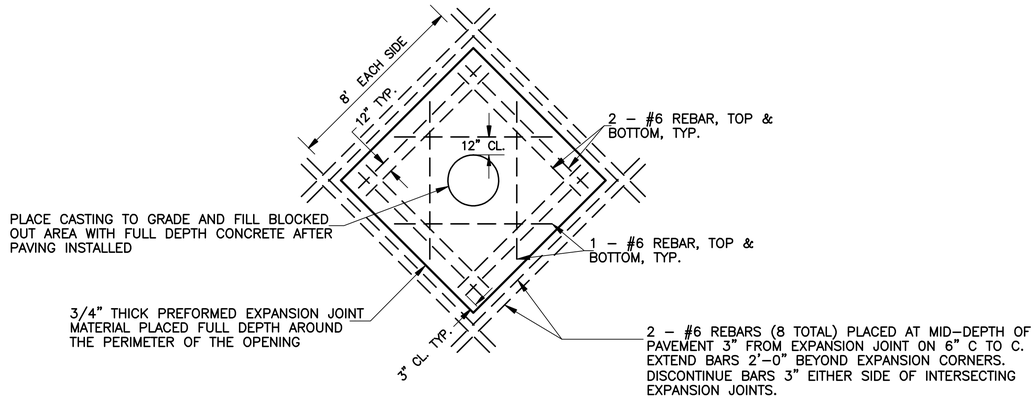
INCLUDE COST FOR ALL ITEMS SHOWN IN THESE DETAILS IN THE AR701318 – 18" RCP CL II CONTRACT UNIT PRICE.



SECTION B-B

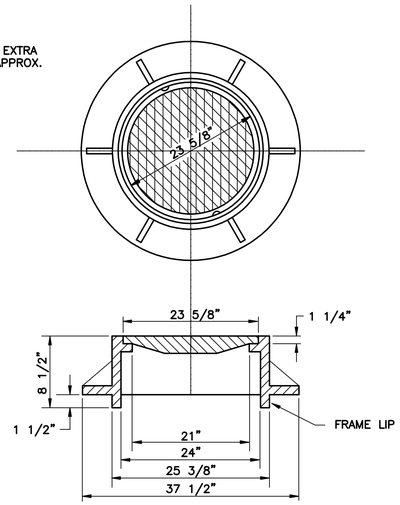


PAVEMENT SQUARE BLOCKOUT
FOR CIRCULAR CASTING
NO SCALE



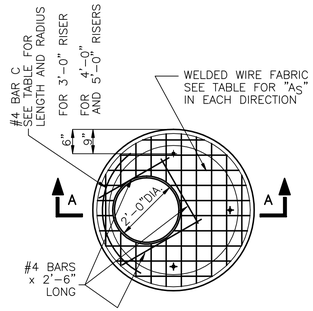
DIAMOND BLOCKOUT
FOR CIRCULAR CASTING
NO SCALE

NEENAH R-3493-A OR EQUAL EXTRA HEAVY DUTY FRAME AND LID. APPROX. WEIGHT 440 POUNDS.

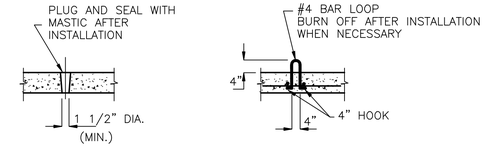


NOTES:
1. USE FOR ALL MANHOLES (UNLESS NOTED OTHERWISE ON THE PLAN SHEETS).
2. REMOVE BOLTS THAT HOLD THE GRATE TO TO THE FRAME AND DO NOT REINSTALL.

MANHOLE FRAME AND LID



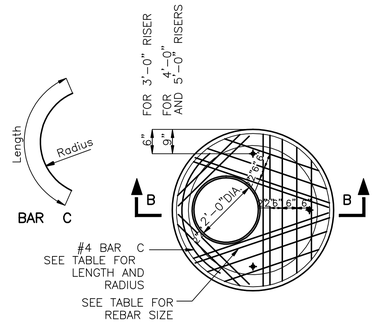
PLAN
SHOWING WELDED WIRE FABRIC



TYPICAL
(3 REQUIRED PER SLAB)

TABLE

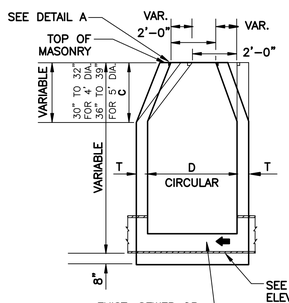
D	T	D_O (MIN)	f	REINFORCEMENT "AS" W.W.F. EACH DIRECTION	OR BAR SIZE	#4 BAR C LENGTH	#4 BAR C RADIUS
3'-0"	See Standards 1514, 1526, 1527, and 1886	D + 2T	6"	.20 sq.in./lin.ft.	#4	4'-0"	1'-7"
4'-0"			6"	.35 sq.in./lin.ft.	#5	4'-6"	2'-2"
5'-0"			8"	.35 sq.in./lin.ft.	#5	5'-0"	2'-8"



PLAN
SHOWING REBAR REIFORCEMENT WITH TYPICAL SPACING

ALT. MATERIAL FOR WALLS	D	C	T
PRECAST REINFORCED CONCRETE RINGS	4'	2'-6"	5"
	5'	3'-0"	6"
MONOLITHIC CONCRETE	4'	2'-6"	6"
	5'	3'-0"	8"

FOR 6' DIAMETER MANHOLES, SEE IDOT STANDARD 602406. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602406.



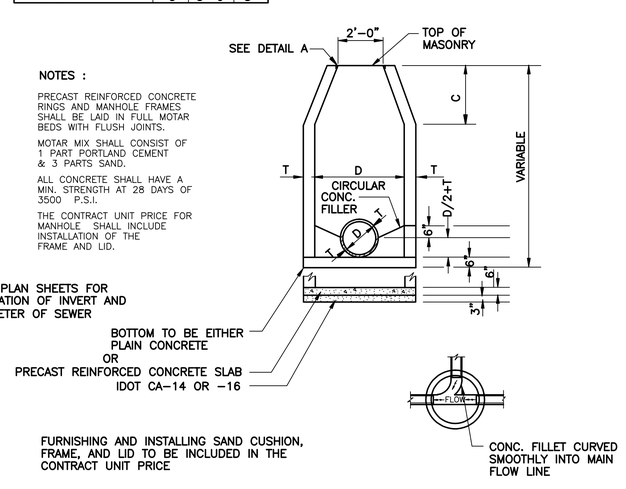
NOTES :

PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MOTAR BEDS WITH FLUSH JOINTS.

MOTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT & 3 PARTS SAND.

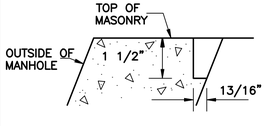
ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.

THE CONTRACT UNIT PRICE FOR MANHOLE SHALL INCLUDE INSTALLATION OF THE FRAME AND LID.



DIAMETER OF MAIN SEWER	D
30 INCHES AND UNDER	4'-0"
36 TO 60 INCHES INCLUSIVE	5'-0"

FURNISHING AND INSTALLING SAND CUSHION, FRAME, AND LID TO BE INCLUDED IN THE CONTRACT UNIT PRICE

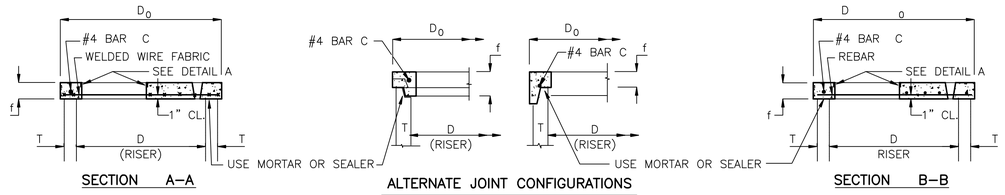


DETAIL A
NOTCH OUT FOR FRAME LIP

NOTE :
THE CONE OF THE MANHOLE SHALL BE CONSTRUCTED AS SHOWN BY THE DOTTED LINES ONLY WHEN THERE IS INTERFERENCE WITH UNDERGROUND CONDITIONS AND THESE CONDITIONS CAN NOT BE ALTERED.

MODIFIED IDOT
STANDARD 602401

DETAIL OF STORM MANHOLE
ITEMS AR751540, AR751550, & AR751560

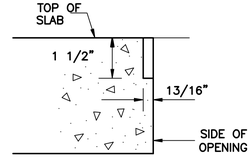


ALTERNATE JOINT CONFIGURATIONS

PRECAST REINFORCED CONCRETE FLAT SLAB TOP FOR MANHOLES, CATCH BASINS AND VALVE VAULTS

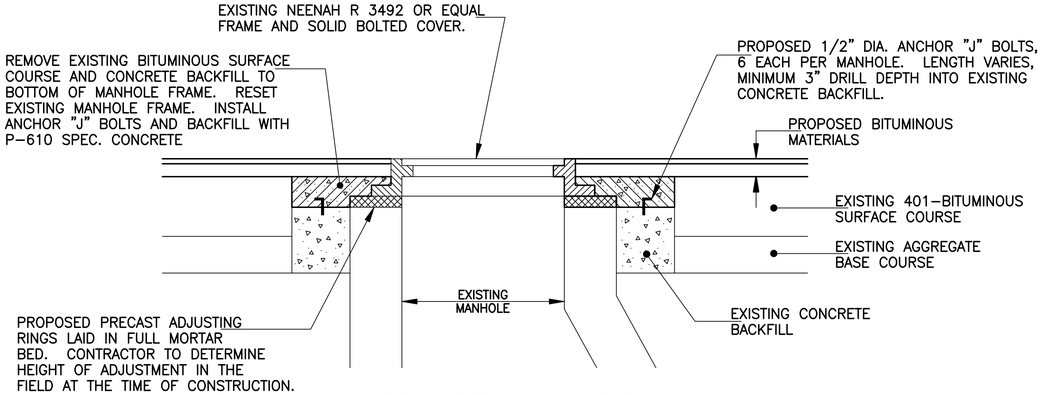
MODIFIED I.D.O.T.
STANDARD 602601

NOTES:
1. PRECAST FLAT SLAB TOPS SHALL CONFORM TO SECTION 602 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
2. REINFORCEMENT BARS OR WELDED WIRE FABRIC SHALL BE IN ACCORDANCE WITH ARTICLE 1006.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
3. JOINT CONFIGURATION AND DIMENSIONS SHALL MATCH AND FIT THE RISER JOINT DETAIL.
4. LIFTING DEVICES OTHER THAN SHOWN MAY BE USED SUBJECT TO APPROVAL BY THE ENGINEER.
5. THE FLAT SLAB TOP MAY BE USED IN LIEU OF THE TAPERED TOPS SHOWN ON STANDARDS 602001, 602011,602306, 602401, OR 602501 AT THE OPTION OF THE CONTRACTOR OR WHEN FIELD CONDITIONS PROHIBIT THE USE OF TAPERED TOPS.
6. THE COST OF FURNISHING AND INSTALLING THE FLAT SLAB TOP SHALL BE INCLUDED IN THE UNIT PRICE FOR CATCH BASINS, MANHOLES, OR VALVE VAULTS.

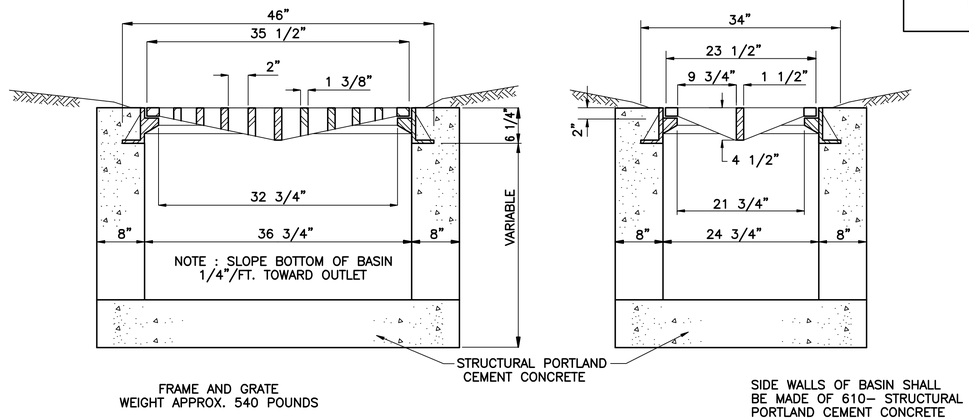


DETAIL A

FOR 6' DIAMETER MANHOLES, SEE IDOT STANDARD 602406. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602406.



TYPICAL DETAIL – MANHOLE
ADJUSTMENT IN BITUMINOUS PAVEMENT
NO SCALE



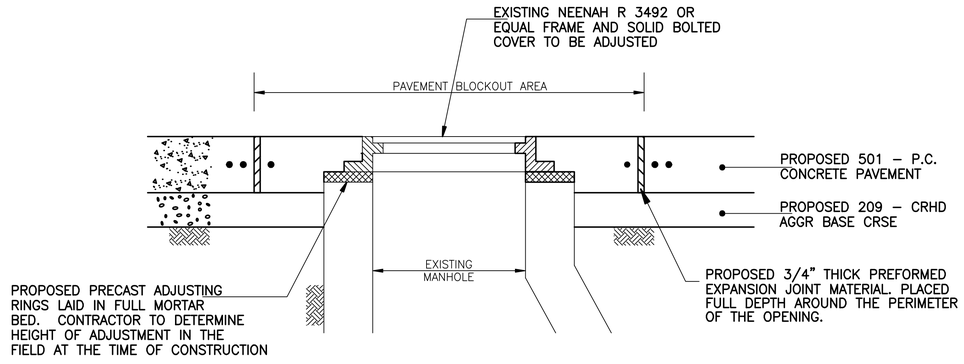
FRAME AND GRATE
WEIGHT APPROX. 540 POUNDS

SIDE WALLS OF BASIN SHALL BE MADE OF 610- STRUCTURAL PORTLAND CEMENT CONCRETE

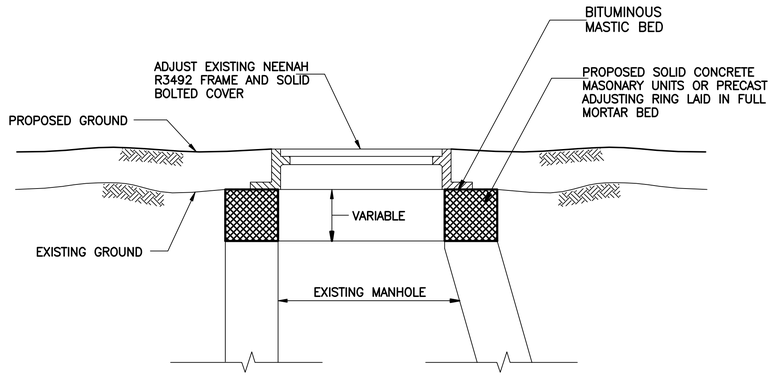
INLET SPECIAL, ITEM 751415

NOTES :

1. INLET SHALL INCLUDE FRAME WITH SINGLE GRATE OF TYPE SIMILAR AND EQUAL TO NO. R 3475 AS SHOWN BY CATALOG "R" ELEVENTH EDITION OF NEENAH FOUNDRY CO.
2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.



TYPICAL DETAIL – MANHOLE
ADJUSTMENT IN P.C.C. PAVEMENT
NO SCALE

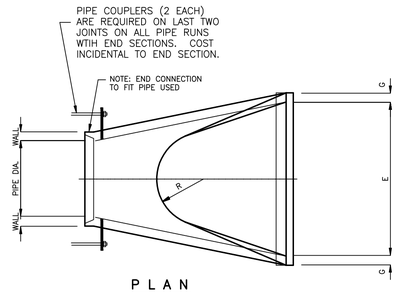


TYPICAL DETAIL – MANHOLE
ADJUST – IN TURF

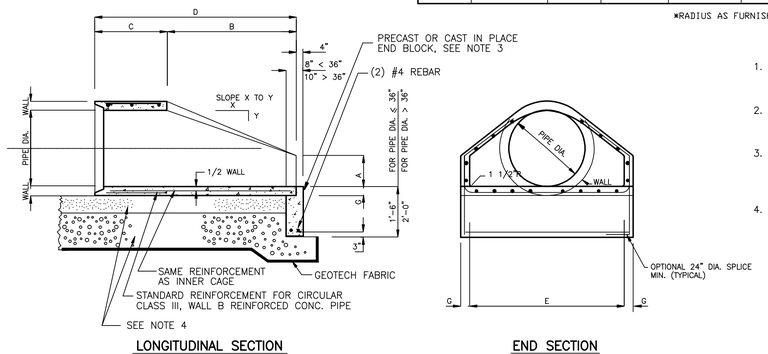
PRECAST REINFORCED CONCRETE FLARED END SECTION

PIPE DIA.	APPROX. WT. (LBS.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0-7/8"	6'-0-7/8"	2'-0"	2"	9"	3:1
15"	740	2-1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2-1/4"	11"	3:1
18"	990	2-1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2-1/2"	12"	3:1
21"	1280	2-3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2-3/4"	13"	3:1
24"	1520	3"	9-1/2"	3'-7-1/2"	2'-6"	6'-1-1/2"	4'-0"	3"	14"	3:1
27"	1930	3-1/4"	10-1/2"	4'-0"	2'-1-1/2"	6'-1-1/2"	4'-6"	3-1/4"	14-1/2"	3:1
30"	2190	3-1/2"	1'-0"	4'-6"	1'-7-3/4"	6'-1-3/4"	5'-0"	3-1/2"	15"	3:1
33"	3200	3-3/4"	1'-1-1/2"	4'-10-1/2"	3'-3-1/4"	8'-1-3/4"	5'-6"	3-3/4"	17-1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10-3/4"	8'-1-3/4"	6'-0"	4"	20"	3:1
42"	5380	4-1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4-1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5-1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5-1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6-1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5-1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7-1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6-1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6-1/2"	1'-9"	9-3-1/2"	10'-0"	6-1/2"	*	1.5:1

*RADIUS AS FURNISHED BY MANUFACTURER



PLAN



LONGITUDINAL SECTION

END SECTION

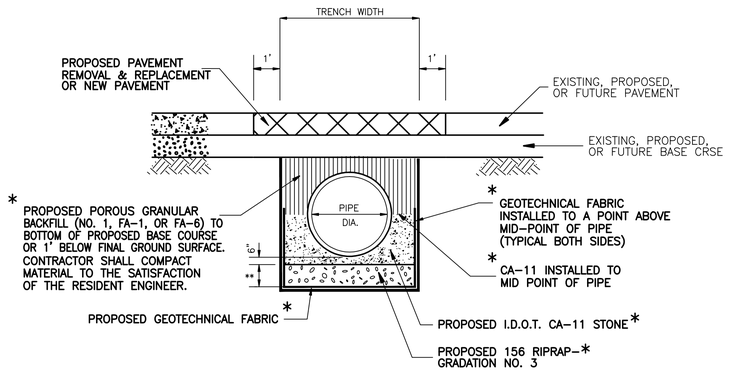
NOTES:

1. PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
2. PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
3. THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ART. 701-3.7 OF THE STANDARD SPECIFICATIONS, COST INCIDENTAL TO END SECTION.
4. BEDDING MATERIALS AND GEOTECH FABRIC PER 701 PIPE SPECIFICATION IN THE SPECIAL PROVISIONS AS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE 752 PIPE END SECTION CONTRACT UNIT PRICE.

MODIFIED I.D.O.T.
STANDARD 542301

FOR EQUIV ELLIPTICAL PRC FES, SEE IDOT STANDARD 542306. ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 542306.

PROPOSED TYPICAL SECTION – PIPE TRENCH
(ALL PROPOSED PIPES)



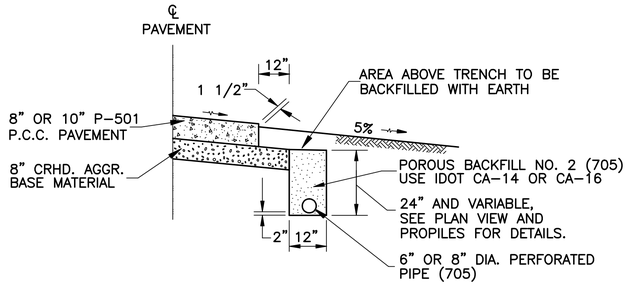
*= COST OF THESE ITEMS SHALL BE INCLUDED IN THE 701 PIPE CONTRACT UNIT PRICES

GENERAL PIPE NOTES:

1. GROUND WATER IS EXPECTED. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEWATERING, TO THE SATISFACTION OF THE ENGINEER, TO INSURE PROPER INSTALLATION OF PIPES. NO ADDITIONAL COMPENSATION BEYOND THE CONTRACT UNIT PRICES WILL BE ALLOWED FOR DEWATERING COSTS.

2. COMPACTED BEDDING AND BACKFILL MATERIALS SHALL BE REQUIRED AS SHOWN IN DETAIL. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

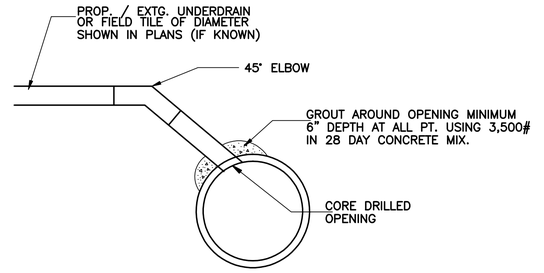
PIPE DIA.	TRENCH WIDTH	PVMT. RMVL. WIDTH	** FOUNDATION DEPTH
8"	3.75'	6'	10"
12"	4.17'	6.5'	10"
18"	4.75'	7'	10"
ELLIPTIAL, 18" EQUIV	5.21'	7.5'	10"
24'	5.33'	7.5'	18"
30"	5.92'	8'	18"
42"	7.08'	9.5'	18"



NOTES :

1) PIPE UNDERDRAIN MATERIAL SHALL MEET THE MATERIAL REQUIREMENTS OF ITEM 705 AND THE SPECIAL PROVISIONS.
2) PIPE UNDERDRAIN TO BE INSTALLED BEFORE PLACEMENT OF PAVEMENT & CRUSHED AGGR.
3) COST OF POROUS BACKFILL NO. 2 (CA-14 OR CA-16), BENDS AND FITTINGS TO BE INCLUDED IN THE UNIT PRICE FOR UNDERDRAINS.
4) NO ADDITIONAL COMPENSATION SHALL BE MADE FOR CHANGES IN ELEVATIONS MADE BY THE RESIDENT ENGINEER.
5) PIPE UNDERDRAIN TO BE INSTALLED ON BOTH SIDES OF PAVEMENT.

PERFORATED PIPE UNDERDRAIN DETAIL

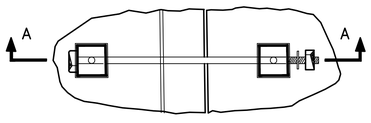


DETAIL OF PIPE UNDERDRAIN / FIELD TILE OUTLET INTO STORM SEWER OR PIPE CULVERT

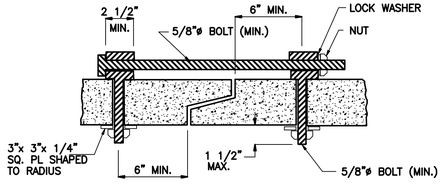
NOTES :

COST OF NEW PIPE, ELBOW AND GROUTING TO BE INCLUDED IN CONTRACT UNIT PRICES. ANY DAMAGE TO EXISTING STORM SEWER, TILE, OR PIPE CULVERT SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AT THE CONTRACTOR'S EXPENSE.

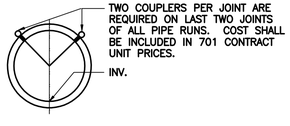
PROPOSED PIPE COUPLERS

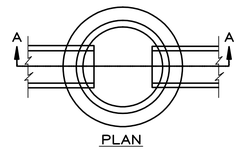


PLAN OF ONE COUPLER

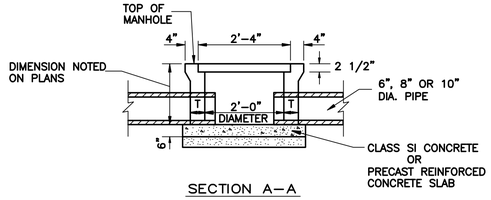


SECTION A-A

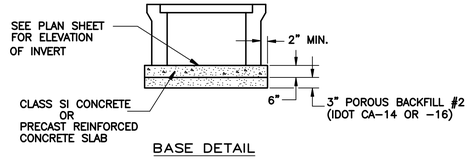




PLAN



SECTION A-A



BASE DETAIL

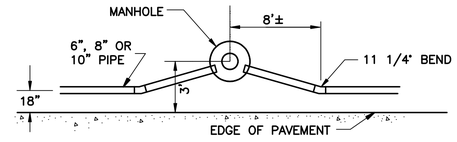
DETAIL OF MANHOLE SPECIAL
ITEM 751570

ALTERNATE MATERIALS FOR WALLS	T
PRECAST REINFORCED CONCRETE RINGS	5"
CAST IN PLACE CONCRETE	6"

NOTES :

1) PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MORTAR BEDS WITH FLUSH JOINTS.
2) MORTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT AND 3 PARTS SAND.
3) ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.
4) THE CONTRACT UNIT PRICE FOR MANHOLE SPECIAL SHALL INCLUDE FURNISHING AND INSTALLING THE FRAME AND LID, THE SAND CUSHION, AND COMPACTING THE BACKFILL MATERIAL. SEE PLAN SHEETS FOR FRAME AND LID TYPE.

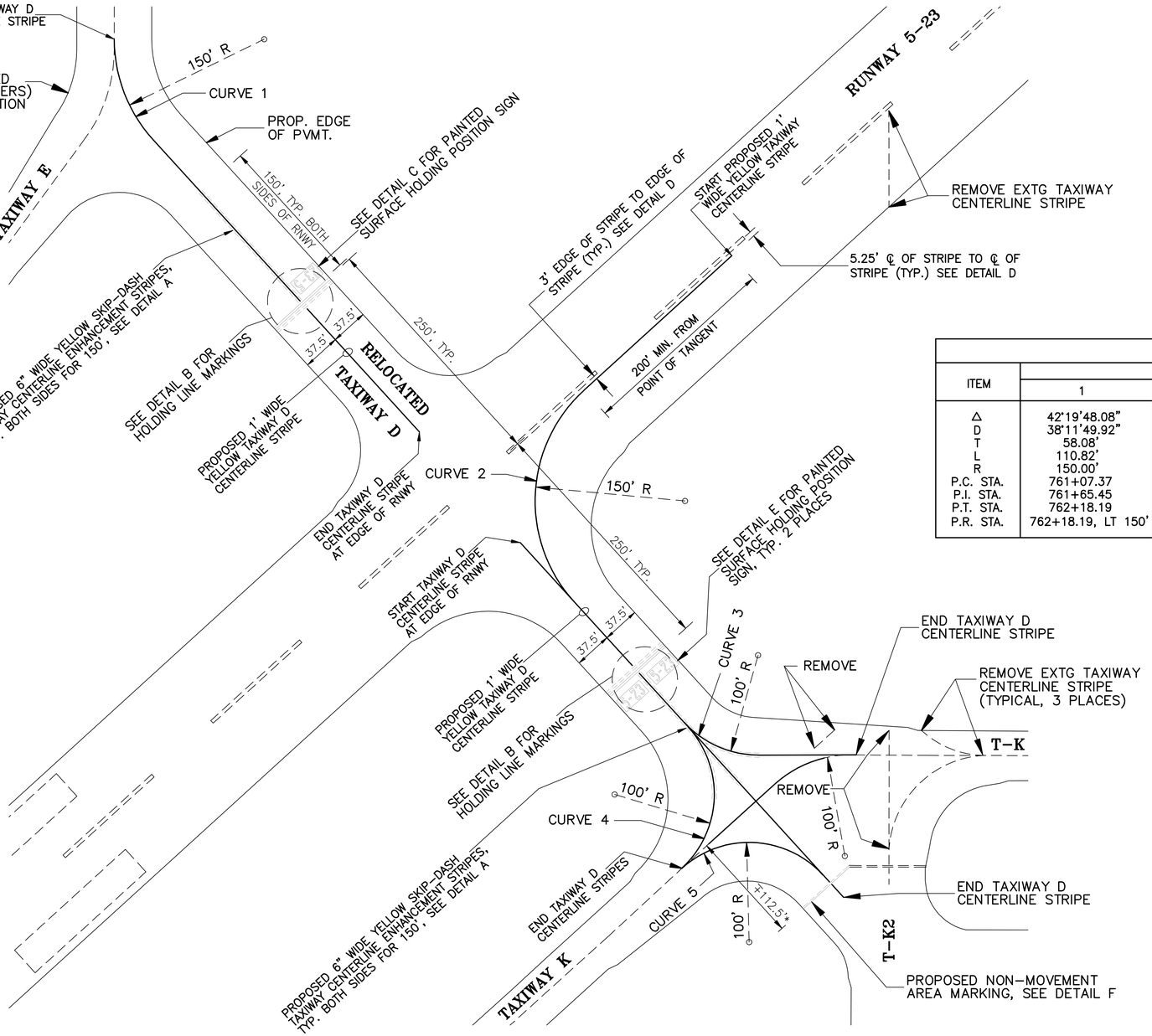
MODIFIED IDOT
STANDARD 602301



COST OF FITTINGS TO BE INCLUDED IN THE UNIT PRICE FOR UNDERDRAINS.

PLAN VIEW OF MANHOLE SPECIAL ADJACENT TO EDGE OF PVMT.

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI–3943, QU013
SHEET 29 OF 69



TAXIWAY CENTERLINE CURVE DATA

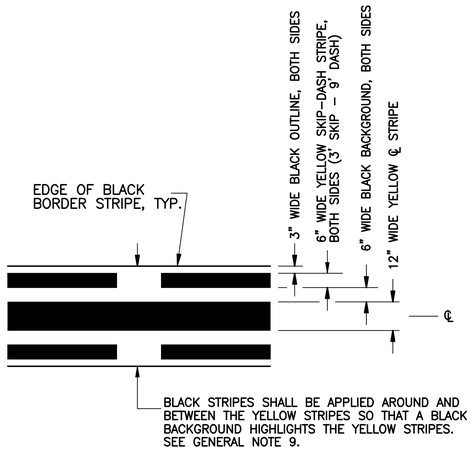
ITEM	CURVE NUMBER				
	1	2	3	4	5
Δ	42°19'48.08"	90°00'00.00"	47°34'47.74"	89°59'39.18"	90°00'20.83"
D	38°11'49.92"	38°11'49.92"	57°17'44.88"	57°17'44.88"	57°17'44.88"
T	58.08'	150.00'	44.08'	99.99'	100.01'
L	110.82'	235.62'	83.04'	157.07'	157.09'
R	150.00'	150.00'	100.00'	100.00'	100.00'
P.C. STA.	761+07.37	766+93.68, LT 150'	770+07.13	770+13.48	771+13.48, RT 100.01'
P.I. STA.	761+65.45	766+93.68	770+51.21	771+13.47	771+13.47
P.T. STA.	762+18.19	768+43.68	770+80.95, LT 32.54'	771+13.48, RT 99.99'	772+13.48
P.R. STA.	762+18.19, LT 150'	768+43.68, LT 150'	770+07.13, LT 100'	770+13.48, RT 100'	772+13.48, RT 100'

* = MATCH EXISTING TAXIWAY K2 NON–MOVEMENT AREA MARKING SET–BACK DISTANCE, BUT NOT LESS THAN AIRPLANE DESIGN GROUP C–III TAXIWAY OFA = 93'.

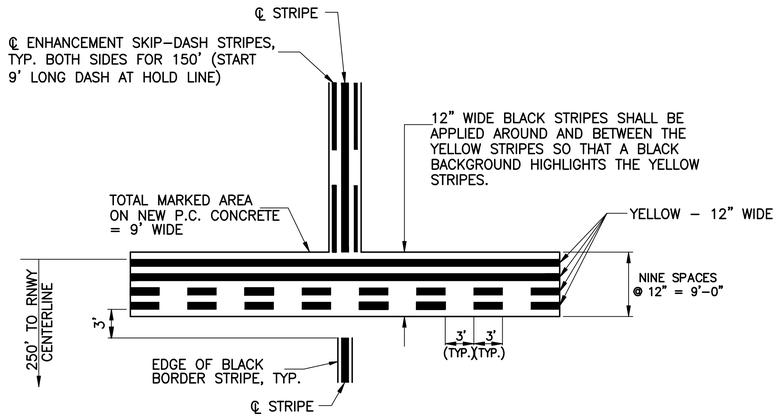
GENERAL NOTES – PAINTING:

1. ALL RUNWAY MARKINGS SHALL BE WHITE UNLESS NOTED OTHERWISE.
2. ALL TAXIWAY MARKINGS SHALL BE YELLOW UNLESS NOTED OTHERWISE.
3. RUNWAY MARKINGS 3' FEET WIDE AND OVER SHALL CONSIST OF A SERIES OF LONGITUDINAL STRIPES 6" WIDE WITH EQUAL WIDTH SPACING BETWEEN THE STRIPES (STRIATED MARKINGS.) TAXIWAY PAINTED SURFACE HOLDING POSITION SIGNS ARE NOT STRIATED.
4. ALL MARKINGS SHALL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
5. TAXIWAY EDGE STRIPES AND TAXIWAY CENTERLINE STRIPES SHALL BE SOLID TO MATCH THE EXISTING TAXIWAY MARKINGS. RUNWAY EDGE STRIPES SHALL BE STRIATED TO MATCH THE EXISTING RUNWAY MARKINGS.
6. EXISTING TAXIWAY AND RUNWAY MARKINGS DAMAGED BY CONSTRUCTION OF THE PROJECT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
7. MARKING LAYOUT BY CONTRACTOR, SEE SPECIAL PROVISIONS.
8. ALL NEW PAVEMENT MARKINGS SHALL BE OUTLINED WITH A SIX INCH (6") WIDE BLACK BORDER. FOR HOLD LINES, THE BLACK BORDER WIDTH SHALL BE INCREASE TO TWELVE INCHES (12").
9. BLACK BACKGROUND STRIPES SHALL BE APPLIED BETWEEN THE YELLOW / WHITE PAVEMENT STRIPES ON STRIATED MARKINGS.
10. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO DELETE THE PAVEMENT MARKING PAY ITEMS FROM THE CONTRACT WORK. THE SPONSOR MAY OPT TO COMPLETE THIS WORK WITH THEIR OWN WORK FORCE.
11. SEE SHEET 30 FOR PAVEMENT MARKING DETAILS.

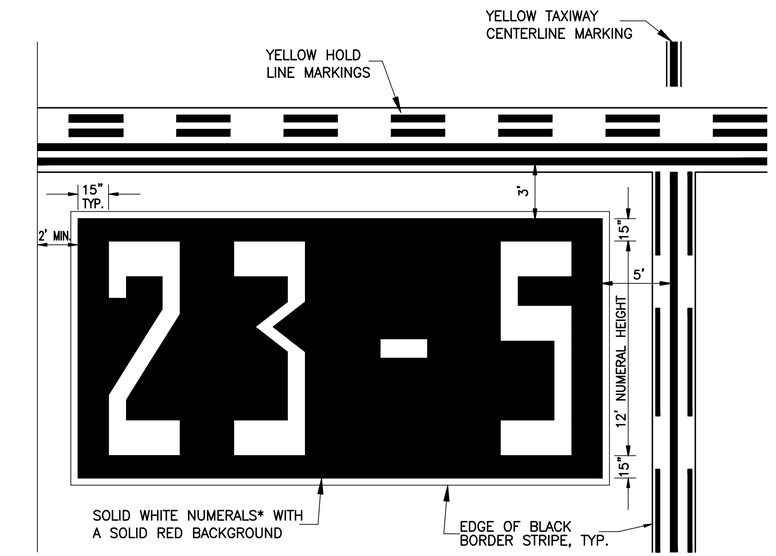
PROPOSED PAVEMENT MARKINGS



DETAIL A
TAXIWAY CENTERLINE ENHANCEMENT STRIPES
STRIATED

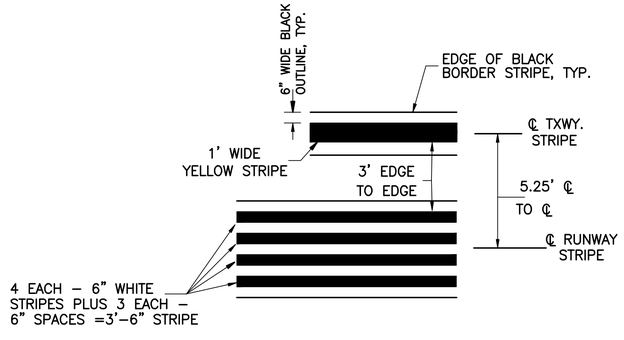


DETAIL B
HOLDING LINE MARKINGS

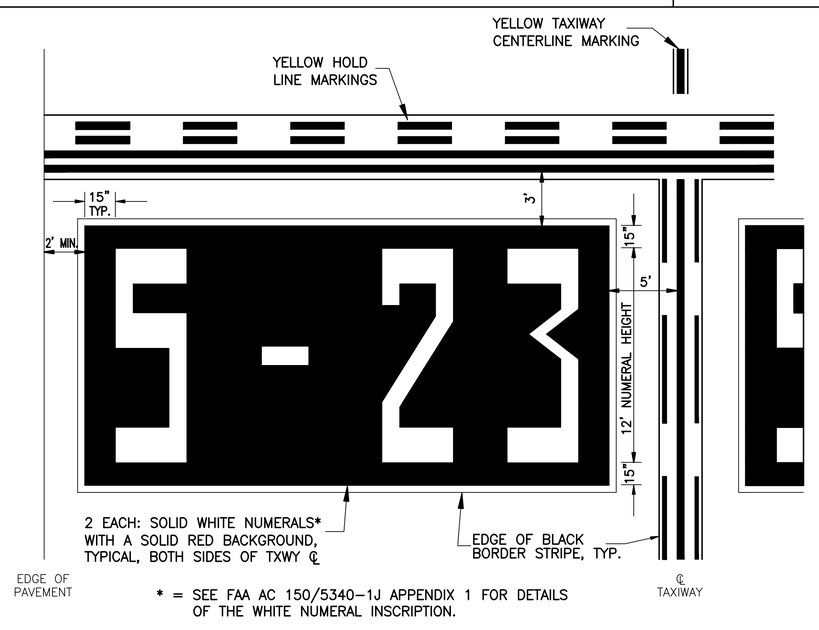


* = SEE FAA AC 150/5340–1J APPENDIX 1 FOR DETAILS OF THE WHITE NUMERAL INSCRIPTION.

DETAIL C
PAINTED SURFACE HOLDING POSITION SIGN

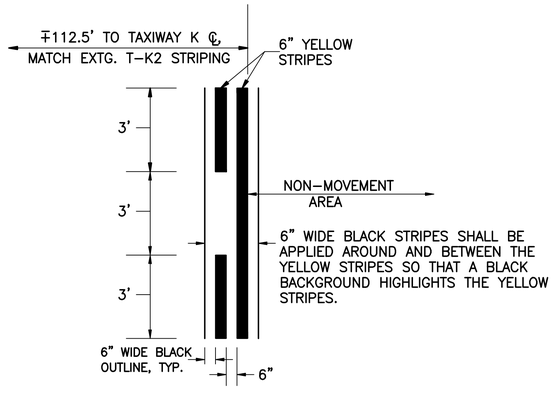


DETAIL D
RUNWAY ℄ / TAXIWAY ℄ STRIPES



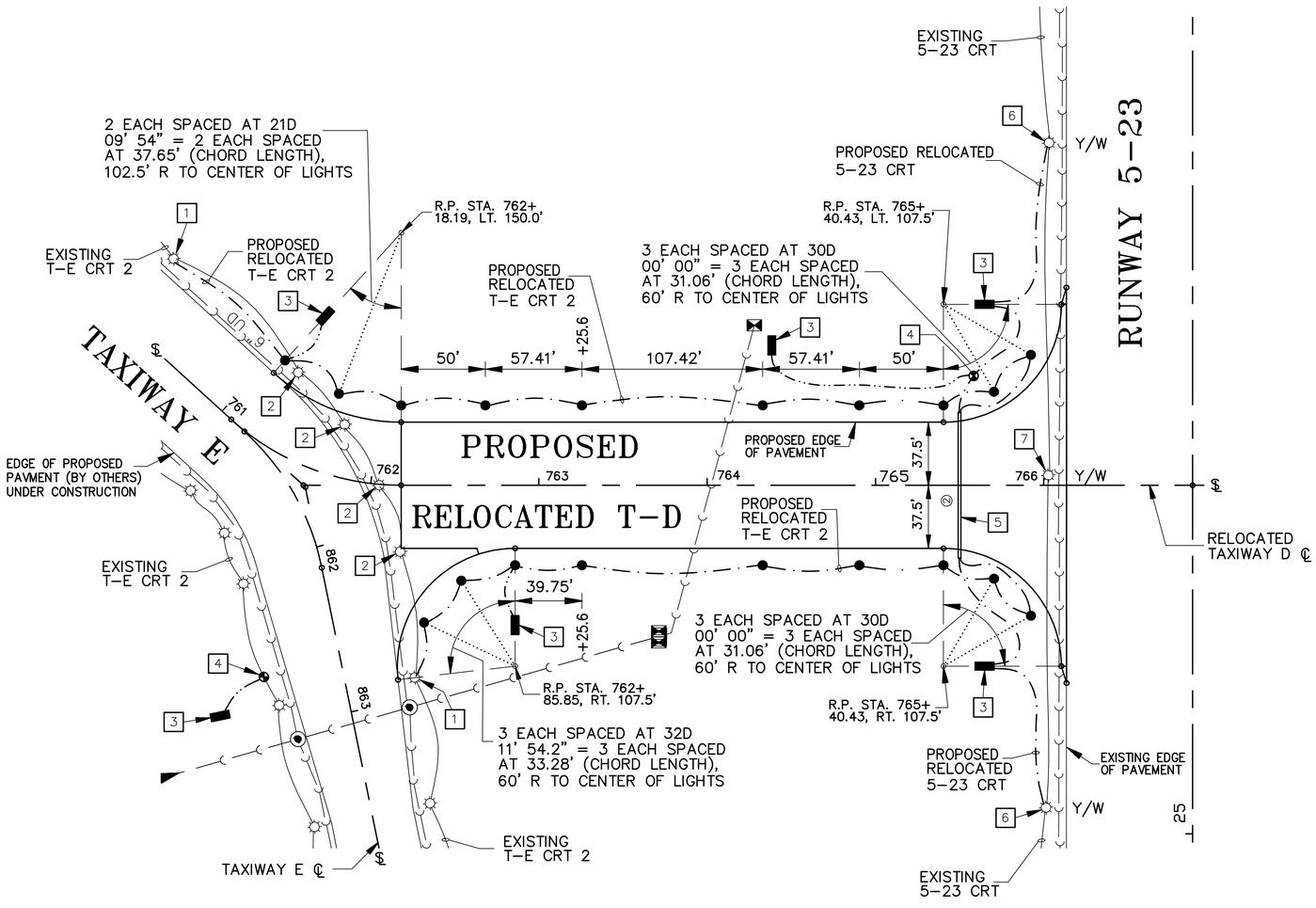
* = SEE FAA AC 150/5340–1J APPENDIX 1 FOR DETAILS OF THE WHITE NUMERAL INSCRIPTION.

DETAIL E
PAINTED SURFACE HOLDING POSITION SIGN

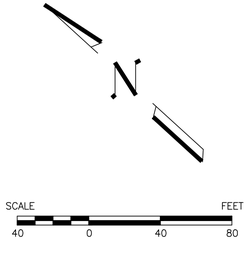


DETAIL F
NON–MOVEMENT AREA MARKINGS

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI–3943, QU013
SHEET 31 OF 69



RELOCATED T–D AREA – NORTHWEST OF RUNWAY 5



GENERAL NOTES:

1. SEE SHEET 36 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 36–39 FOR ELECTRICAL DETAILS
3. SEE SHEET 37 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

NUMBERED LEGEND

1. DISCONNECT EXISTING T–E CRT 2 CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED T–E CRT 2 RELOCATION AT THESE LOCATIONS.
2. RELOCATE OR REMOVE EXISTING L–861T MITL INCANDESCENT UNITS. CONVERT EXISTING INCANDESCENT UNITS (THAT ARE TO BE RELOCATED) TO PROPOSED LED UNITS. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER DETAIL. INCLUDE GROUNDING ROD, WIRE, & LED CONVERSION COSTS IN RELOCATION UNIT PRICE.
3. PROPOSED LOCATION FOR NEW GUIDANCE SIGN. SEE SHEETS 34 AND 35 FOR MORE INFO.
4. PROPOSED LOCATION FOR NEW L–867 SPLICE CAN.
5. PROPOSED 2–WAY DUCT BANK (4" ID CONDUITS) TO A POINT 5' BEYOND THE EDGE OF THE PROPOSED PAVEMENT.
6. DISCONNECT EXISTING 5–23 CRT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED 5–23 CRT RELOCATION AT THESE LOCATIONS.
7. RELOCATE EXISTING L–861 MIRL INCANDESCENT UNIT. CONVERT EXISTING INCANDESCENT UNIT TO PROPOSED LED UNIT. SUPPLY & INSTALL NEW BLUE LENSES TO CONVERT EXTG MIRL UNIT TO PROPOSED MITL UNIT. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER DETAIL. INCLUDE GROUNDING ROD, WIRE, LENSE, & LED CONVERSION COSTS IN RELOCATION UNIT PRICE.

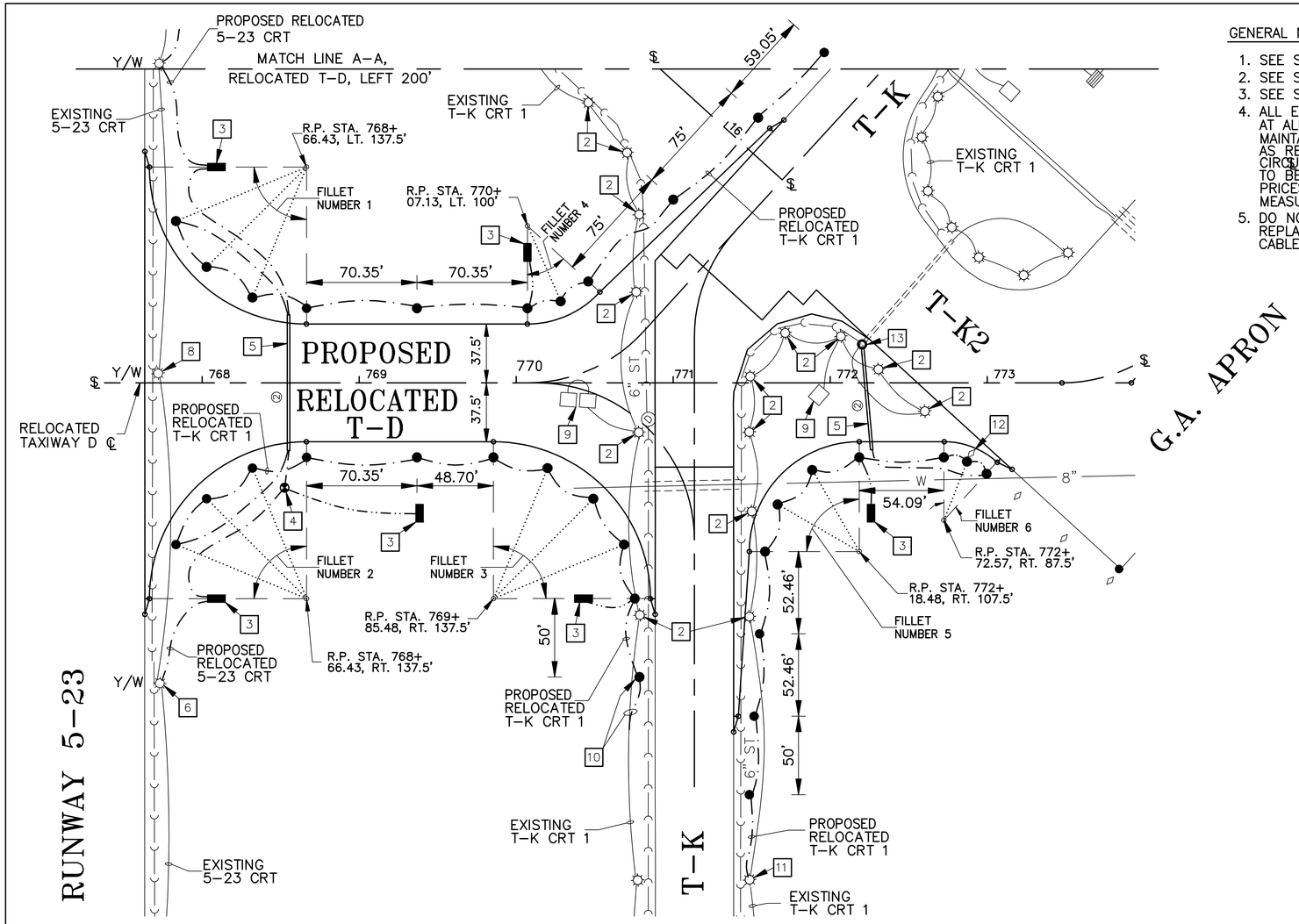
SYMBOL LEGEND

- EXISTING L–861T MITL OR L–861 MIRL
- EXISTING L–862 HIRLQ
- EXISTING L–867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW LED GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW L–861T LED MITL (BLUE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L–861 MIRL INCANDESCENT (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L–867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1–1/2" HDPE UNIT DUCT

NORTH LIGHTING PLAN

31/69

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 32 OF 69



RELOCATED T-D AREA - SOUTHEAST OF RUNWAY 5

GENERAL NOTES:

1. SEE SHEET 36 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 36-39 FOR ELECTRICAL DETAILS
3. SEE SHEET 37 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

NUMBERED LEGEND

2 RELOCATE OR REMOVE EXISTING L-861T MITL INCANDESCENT UNITS. CONVERT EXISTING INCANDESCENT UNITS (THAT ARE TO BE RELOCATED) TO PROPOSED LED UNITS. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER DETAIL. INCLUDE GROUNDING ROD, WIRE, & LED CONVERSION COSTS IN RELOCATION UNIT PRICE.

3 PROPOSED LOCATION FOR NEW GUIDANCE SIGN. SEE SHEETS 34 AND 35 FOR MORE INFO.

4 PROPOSED LOCATION FOR NEW L-867 SPLICE CAN.

5 PROPOSED 2-WAY DUCT BANK (4" ID CONDUITS) TO A POINT 5' BEYOND THE EDGE OF THE PROPOSED PAVEMENT.

6 DISCONNECT EXISTING 5-23 CRT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED 5-23 CRT RELOCATION AT THESE LOCATIONS.

8 RELOCATE EXISTING L-861 MIRL INCANDESCENT UNIT TO RNWY 5-23 STA. 32+58.52 RIGHT. L-861 MIRL UNITS TO REMAIN INCANDESCENT. SUPPLY & INSTALL NEW WHITE LENSES. SUPPLY & INSTALL NEW GROUNDING ROD PER DETAIL. INCLUDE GROUNDING ROD, WIRE, & LENSES COSTS IN RELOCATION UNIT PRICE.

9 EXISTING GUIDANCE SIGN TO BE REMOVED. SEE SHEET 34 AND 35 FOR MORE INFO.

10 LOCATE EXISTING T-K CRT 1 CABLE IN UNIT DUCT AND ROUTE EXISTING CABLE INTO PROPOSED LIGHT CAN AT THIS LOCATION. START / END PROPOSED T-K CRT 1 RELOCATION AT THIS LOCATION.

11 DISCONNECT EXISTING T-K CRT 1 CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED T-K CRT 1 RELOCATION AT THESE LOCATIONS.

12 REMOVE EXISTING RETROREFLECTIVE MARKER.

13 EXISTING T-K CRT 1 CABLE IN DUCT BANK AND PROPOSED ELECTRICAL MANHOLE. CONSTRUCT PROPOSED ELECTRICAL MANHOLE AROUND EXISTING DUCT BANK. START / END PROPOSED T-K CRT 1 RELOCATION AT THIS LOCATION.

SYMBOL LEGEND

- EXISTING L-861T MITL OR L-861 MIRL
- EXISTING L-862 HIRLQ
- EXISTING RETROREFLECTIVE MARKERS
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW LED GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T LED MITL (BLUE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL INCANDESCENT (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT

N

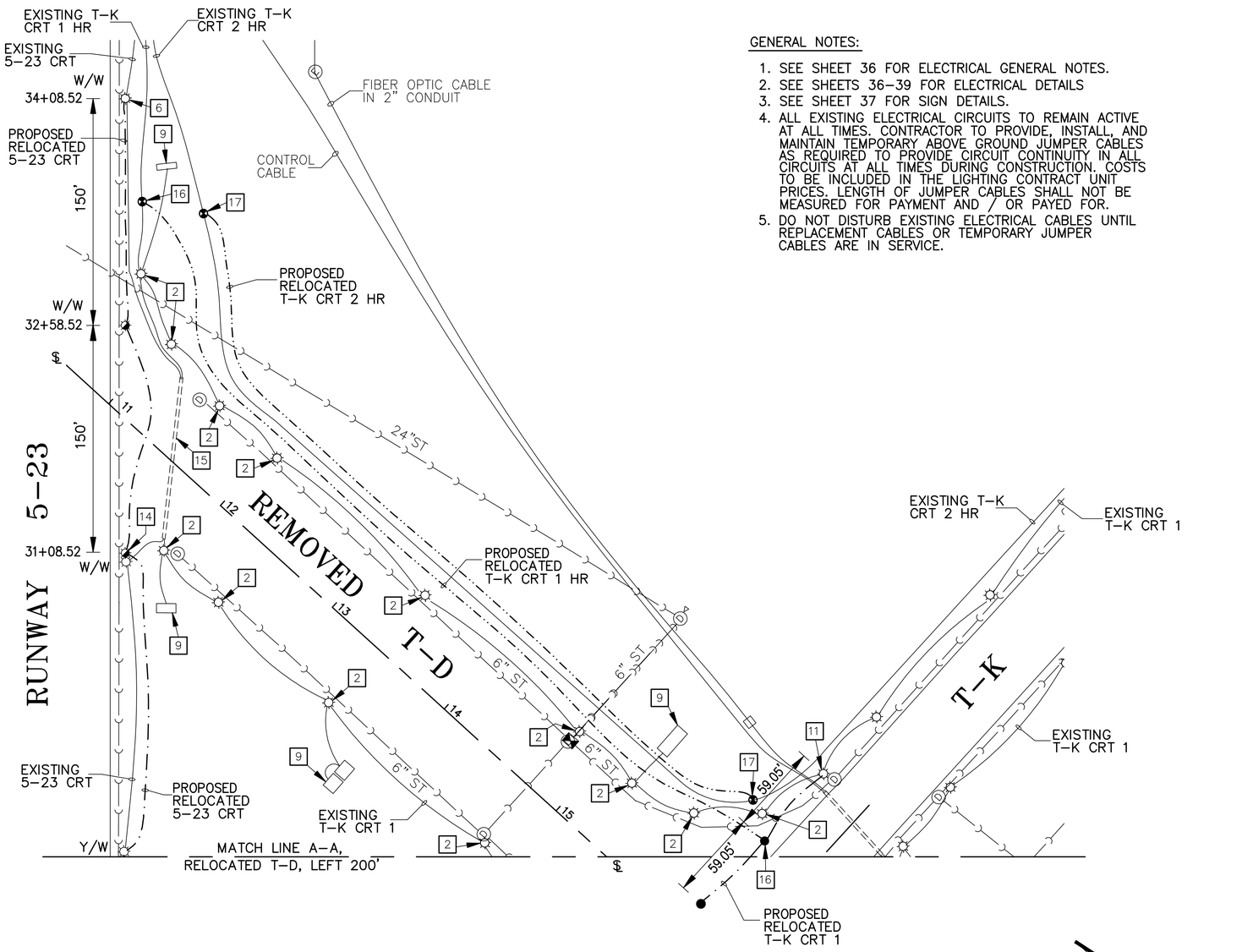
SCALE FEET
40 0 40 80

PROPOSED LOCATIONS FOR TAXIWAY EDGE LIGHTS IN FILLETS

FILLET REFERENCE NUMBER	NUMBER OF SPACES IN FILLET	SPACING OF LIGHTS		RADIUS TO CENTER OF LIGHTS
		ANGLE	CHORD LENGTH	
1	4	22D 30' 00"	35.12'	90'
2	4	22D 30' 00"	35.12'	90'
3	4	22D 29' 54.8"	35.11'	90'
4	2	23D 47' 23.9"	21.64'	52.5'
5	3	30D 00' 06.9"	31.06'	60'
6	2	21D 13' 51.9"	14.74'	40'

SOUTH LIGHTING PLAN

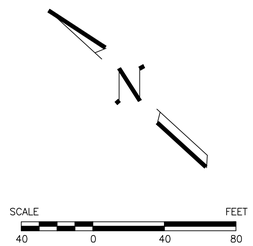
QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 33 OF 69



EXISTING T-D AREA - SOUTHEAST OF RUNWAY 5

GENERAL NOTES:

1. SEE SHEET 36 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 36-39 FOR ELECTRICAL DETAILS
3. SEE SHEET 37 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.



NUMBERED LEGEND

[2] RELOCATE OR REMOVE EXISTING L-861T MITL INCANDESCENT UNITS. CONVERT EXISTING INCANDESCENT UNITS (THAT ARE TO BE RELOCATED) TO PROPOSED LED UNITS. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER DETAIL. INCLUDE GROUNDING ROD, WIRE, & LED CONVERSION COSTS IN RELOCATION UNIT PRICE.

[6] DISCONNECT EXISTING 5-23 CRT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED 5-23 CRT RELOCATION AT THESE LOCATIONS.

[9] EXISTING GUIDANCE SIGN TO BE REMOVED. SEE SHEET 34 AND 35 FOR MORE INFO.

[11] DISCONNECT EXISTING T-K CRT 1 CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED T-K CRT 1 RELOCATION AT THESE LOCATIONS.

[14] RELOCATE EXISTING L-861 MIRL UNIT TO RNWY 5-23 STA. 31+08.52 RIGHT. SUPPLY & INSTALL NEW GROUNDING ROD PER DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE. MIRL UNIT TO REMAIN INCANDESCENT.

[15] EXISTING DUCT BANK TO BE REMOVED.

[16] LOCATE EXISTING T-K CRT 1 HR CABLE IN UNIT DUCT AND ROUTE EXISTING CABLE INTO PROPOSED SPLICE CANS AT THESE LOCATIONS. START / END PROPOSED T-K CRT 1 HR CABLE RELOCATION AT THESE LOCATIONS. THE ACTUAL LOCATIONS TO START / END CABLE RELOCATION SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION. INSTALL TEMPORARY JUMPER CABLES AS REQUIRED TO KEEP CIRCUIT ACTIVATED DURING CONSTRUCTION.

[17] LOCATE EXISTING T-K CRT 2 HR CABLE IN UNIT DUCT AND ROUTE EXISTING CABLE INTO PROPOSED SPLICE CANS AT THESE LOCATIONS. START / END PROPOSED T-K CRT 2 HR CABLE RELOCATION AT THESE LOCATIONS. THE ACTUAL LOCATIONS TO START / END CABLE RELOCATION SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION. INSTALL TEMPORARY JUMPER CABLES AS REQUIRED TO KEEP CIRCUIT ACTIVATED DURING CONSTRUCTION.

SYMBOL LEGEND

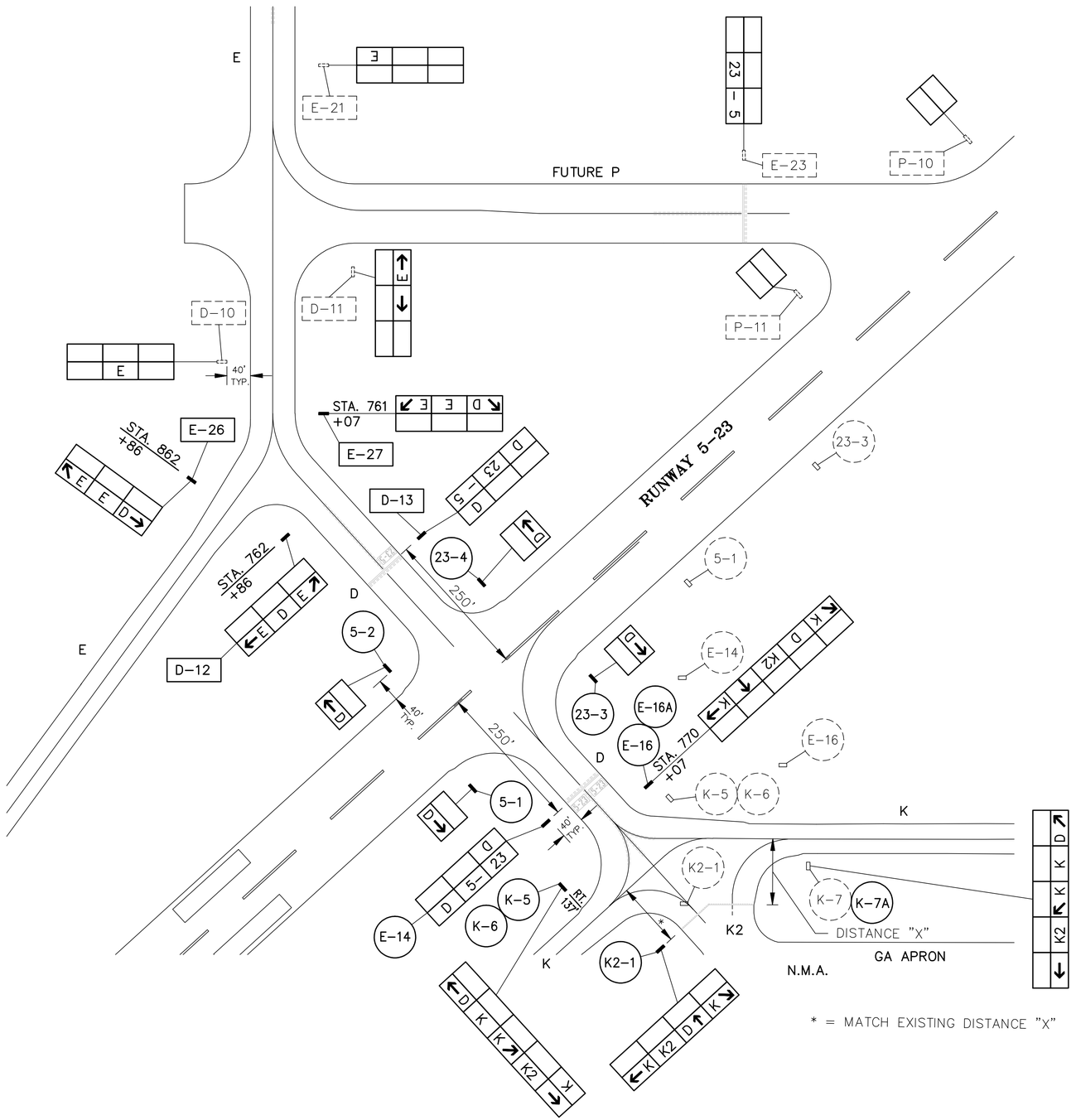
- EXISTING L-861T MITL OR L-861 MIRL
- EXISTING L-862 HIRLQ
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- (D) (S) EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW LED GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T LED MITL (BLUE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL INCANDESCENT (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT

EAST LIGHTING PLAN

33/69

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 34 OF 69

DO NOT CHANGE GUIDANCE SIGN D-10, D-11, E-21, E-23, P-10, AND P-11 PANELS UNTIL STAGE 1 OF THE PROPOSED SAFETY PLAN (SHEETS X THROUGH X) IS COMPLETE.



N

SCALE FEET
0 100

LEGEND:

- EXISTING GUIDANCE SIGN
- 23-4 — EXISTING SIGN NUMBER AT EXISTING SIGN LOCATION
- 23-4 — EXISTING SIGN NUMBER AT NEW SIGN LOCATION
- PROPOSED LOCATION FOR NEW GUIDANCE SIGN
- H-20 — PROPOSED NEW SIGN NUMBER
- E-20 — PROPOSED (BY OTHERS) / FUTURE (BY OTHERS) NEW SIGN NUMBER
- PROPOSED (BY OTHERS) / FUTURE (BY OTHERS) GUIDANCE SIGN

GUIDANCE SIGN GENERAL NOTES:

1) "LEGEND" COLUMN IN TABLE (FOUND ON SHEET 35) INDICATES NUMBER OF PANELS (MODULES), SPECIFIC CHARACTERS PER PANEL AND BLANK PANELS. SIGNS ARE TO BE MANUFACTURED AND SUPPLIED AS SHOWN IN THE LEGEND COLUMN UNLESS OTHERWISE APPROVED BY THE MAA.
2) ALL SIGNS SHALL COMPLY WITH FAA ADVISORY CIRCULAR 150/5345-44E.
3) CONTRACTOR SHALL VERIFY ALL BASE DIMENSIONS WITH MANUFACTURER PRIOR TO INSTALLATION OF ANCHOR BOLTS.
4) THE CONTRACTOR SHALL DISASSEMBLE EXISTING GUIDANCE SIGNS AS REQUIRED TO INSTALL PROPOSED NEW SIGN PANELS AND REUSE EXISTING PANELS AS INDICATED IN TABLE. EXISTING PANELS REMOVED MAY ONLY BE REUSED, IF THE CONDITION OF THE PANEL IS ACCEPTABLE TO THE MAA AND THE RESIDENT ENGINEER. THE CONTRACTOR SHALL REPLACE ALL DAMAGED EXISTING PANELS AND EQUIPMENT AS REQUIRED BY THE RESIDENT ENGINEER. ONCE THE SIGNS HAVE BEEN REBUILT, THE CONTRACTOR SHALL SEAL SIGNS AND RETURN THE EXISTING SIGNS TO THEIR ORIGINAL CONDITION. EXISTING PANELS REMOVED AND NOT REUSED SHALL BE DELIVERED BY THE CONTRACTOR TO THE OWNER (MAA).
5) SIGNS SHALL BE DOUBLE FACED AS INDICATED IN TABLE (TYPE L-858Y, L-858R, L-858L, OR L-858B). SIGNS AND REPLACEMENT PANELS SHALL BE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND WITH THE EXISTING SIGNS CURRENTLY IN PLACE AT THE QUAD CITY INTERNATIONAL AIRPORT. SIGNS AND REPLACEMENT PANELS SHALL BE AGM / LUMACURVE, OR APPROVED EQUAL.
6) "FACE" COLUMN INDICATES DIRECTION OF SIGN FACE.
7) SEE SHEET 36 - 39 FOR ELECTRICAL GENERAL NOTES AND ELECTRICAL DETAILS.
8) SEE SHEET 37 FOR SIGN DETAILS.
9) WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.

RELOCATED TAXIWAY D GUIDANCE SIGN PLAN

34/69

RELOCATED TAXIWAY D GUIDANCE SIGN SCHEDULE

SIZE 3, STYLE 2 OR 3 (WITH LED LIGHTING SYSTEM INCLUDING LED LAMPS), CLASS 2

SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS / BACKGROUND	PROPOSED LEGEND (BY OTHERS)	LETTERS / BACKGROUND	PROPOSED LEGEND (THIS CONTRACT)	LETTERS / BACKGROUND	FAA STYLE	CIRCUIT	PAY ITEMS	REMARK
D-10	N					NO CHANGE	NO CHANGE	2	T-E-2	AR801605	REPLACE 1 PANEL
	S			E D→		E					
D-11	W					NO CHANGE	NO CHANGE	2	T-E-2	AR801605	REPLACE 1 PANEL
	E			D ← E→		← E→					
D-12	NW							2	T-E-2	AR125445	PROPOSED NEW 5 CHARACTER SIGN
	SE					←E D E↗					
D-13	NW					D 23 – 5		3	R5-23	AR125446	PROPOSED NEW 6 CHARACTER SIGN
	SE					D					
E-14	NW	E		D		D		3	R5-23	AR125904 & AR125446	REMOVE EXISTING 1 & 2 MODULE SIGNS & PROPOSED NEW 6 CHARACTER SIGN (SEE NOTE 1)
	SE	E 5– 23		D 5– 23		D 5– 23					
E-16 / E-16A	NW	←K E K↗		←K D K↗		↖K D K2 ↑ K→		2	T-K-1	AR125904 & AR125448	REMOVE EXISTING 3 MODULE SIGN & PROPOSED NEW 8 CHARACTER SIGN (SEE NOTE 2)
	SE										
E-21	N			←D E		E		2	T-E-1	AR801605	REPLACE 1 PANEL
	S					NO CHANGE	NO CHANGE				
E-23	W			D 23 – 5		23 – 5		3	R5-23	AR801605	REPLACE 2 PANELS
	E			D							
E-26	NE							2	T-E-2	AR125445	PROPOSED NEW 5 CHARACTER SIGN
	SW					↖E E D→					
E-27	N					↖D E E↗		2	T-E-2	AR125445	PROPOSED NEW 5 CHARACTER SIGN
	S										
K-5 / K-6	NE	K		K		K		2	T-K-1	AR125904 & AR125449	REMOVE EXISTING 2 & 3 MODULE SIGNS & PROPOSED NEW 9 CHARACTER SIGN (SEE NOTE 3)
	SW	↖E K K↗ K2 ↘		↖D K K↗ K2 ↘		←D K K↗ K2 →					
K-7 / K-7A	W							2	T-K-1	AR125904 & AR125448	REMOVE EXISTING 3 MODULE SIGN & PROPOSED NEW 8 CHARACTER SIGN (SEE NOTE 2)
	E	↖K K E→		↖K K D→		← K2 ↖K K D↗					
K2-1	NW							2	T-K-1	AR125904 & AR125448	REMOVE EXISTING 4 MODULE SIGN & PROPOSED NEW 8 CHARACTER SIGN
	SE	↙K K2 E↑K→		↙K K2 D↑K→		←K K2 D↑K↘					
P-10	NE			D↗	B / Y		B / B	3	R5-23	AR801605	REPLACE 1 PANEL
	SW				B / B	NO CHANGE	NO CHANGE				
P-11	NE				B / B	NO CHANGE	NO CHANGE	3	R5-23	AR801605	REPLACE 1 PANEL
	SW			↙D	B / Y		B / B				
5-1	NE		B / B		B / B		B / B	3	R5-23	AR125904 & AR125442	REMOVE EXISTING 1 MODULE SIGN & PROPOSED NEW 2 CHARACTER SIGN
	SW	E↘	B / Y	D↘	B / Y	D→	B / Y				
5-2	NE	SIGN REMOVED BY OTHERS					B / B	3	R5-23	AR125442	PROPOSED NEW 2 CHARACTER SIGN
	SW					←D	B / Y				
23-3	NE	↖E	B / Y	↖D	B / Y	←D	B / Y	3	R5-23	AR125904 & AR125442	REMOVE EXISTING 1 MODULE SIGN & PROPOSED NEW 2 CHARACTER SIGN
	SW		B / B		B / B		B / B				
23-4	NE	SIGN REMOVED BY OTHERS				D→	B / Y	3	R5-23	AR125442	PROPOSED NEW 2 CHARACTER SIGN
	SW						B / B				

N = NORTH
S = SOUTH
E = EAST
W = WEST
NW = NORTHWEST
SE = SOUTHEAST
NE = NORTHEAST
SW = SOUTHWEST

B/Y = BLACK LETTERS ON YELLOW BACKGROUND (TYPE L-858Y)
Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)
B/B = BLACK BLANK PANEL
Y/Y = YELLOW BLANK PANEL
W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)
W/B = WHITE LETTERS ON BLACK BACKGROUND (TYPE L-858B)

NOTES:

1. THE SIGN E-14 REMOVAL CONTRACT UNIT PRICE SHALL INCLUDE THE COST FOR REMOVING ALL EXISTING MODULES AS REQUIRED (INCLUDING ONE EACH 1 MODULE SIGN AND ONE EACH 2 MODULE SIGN AS SHOWN).
2. PROPOSED NEW 8 CHARACTER SIGN (E-16/-16A AND K-7/-7A) CONTRACT UNIT PRICE SHALL INCLUDE THE COST FOR SUPPLYING AND INSTALL ALL PROPOSED MODULES AS REQUIRED (INCLUDING ONE EACH 2 MODULE SIGN AND ONE EACH 3 MODULE SIGN AS SHOWN).
3. PROPOSED NEW 9 CHARACTER SIGN K-5 / K-6 CONTRACT UNIT PRICE SHALL INCLUDE THE COST FOR SUPPLYING AND INSTALL ALL PROPOSED MODULES AS REQUIRED (INCLUDING ONE EACH 3 MODULE SIGN AND ONE EACH 2 MODULE SIGN AS SHOWN). THE SIGN K-5 / K-6 REMOVAL CONTRACT UNIT PRICE SHALL INCLUDE THE COST FOR REMOVING ALL EXISTING MODULES AS REQUIRED (INCLUDING ONE EACH 2 MODULE SIGN AND ONE EACH 3 MODULE SIGN AS SHOWN).
4. FAA STYLE 2 = 4.8A-6.6A, 3 STEP BRIGHTNESS CIRCUIT.
5. FAA STYLE 3 = 2.8A-6.6A, 5 STEP BRIGHTNESS CIRCUIT.

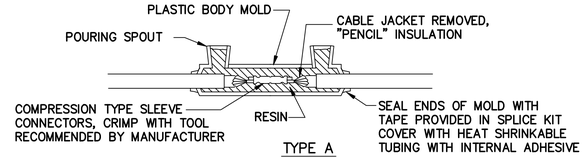
SIZE 3 TAXI GUIDANCE / SIZE 4 DIST. REMAIN. SIGNS ISOLATION TRANSFORMER DATA*

NUMBER OF MODULES	TRANSFORMER WATTAGE			
	STYLE 2, 4.8A-6.6A		STYLE 3, 2.8A-6.6A	
	TRADITIONAL	LED SYSTEM	TRADITIONAL	LED SYSTEM
1	100	200	200	200
2	300	200	300	300
3	500	300	500	500
4	500	300	500	500
DISTANCE REMAINING	300	200	300	300

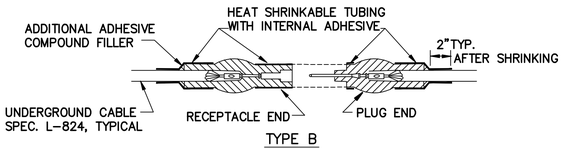
TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* = OR AS REQUIRED BY SIGN MANUFACTURER.
** = WITH A SIAMESE PIGTAIL ADAPTER AND TWO TRANSFORMERS.

GENERAL ELECTRICAL NOTES:

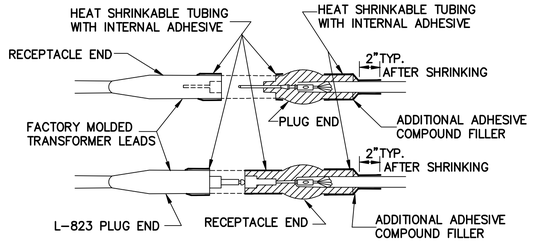
1. ALL EXCAVATION SHALL BE DONE VERY CAREFULLY. EXCAVATION BY HAND DIGGING SHALL BE REQUIRED AROUND ALL EXISTING DUCT BANKS, SPLICE CANS, MANHOLES, AND EXISTING CABLES. MANY EXISTING ACTIVE UNDERGROUND CABLES, WHOSE EXACT LOCATIONS CANNOT BE DETERMINED, ARE FOUND IN THE PROJECT AREA. IN ORDER TO AVOID EXISTING UNDERGROUND CABLES, THE CONTRACTOR SHALL CONNECT A THUMPER TO ALL EXISTING CIRCUITS AFTER WHICH THEY SHALL BE STAKED IN ALL AREAS REQUIRING TRENCHING OR EXCAVATION. CONTRACTOR SHALL ALSO NOTE THAT LOW VOLTAGE, FAA CABLES ALSO RUN UNDERGROUND THROUGHOUT THESE AREAS. ANY CABLE DAMAGED SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
2. A MINIMUM OF 3 FEET OF SLACK SHALL BE PROVIDED IN THE CABLES AT EACH TRANSFORMER, CONNECTOR, OR SPLICE POINT. ALL CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS OR SPLICE CANS, UNLESS NOTED OTHERWISE.
3. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
4. ALL MANUFACTURERS FOR SUPPLYING AIRPORT LIGHTING EQUIPMENT SHALL APPEAR ON THE CURRENT FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-53C. THE EQUIPMENT SHALL COMPLY WITH THE APPLICABLE CURRENT FAA ADVISORY CIRCULAR LISTED IN THE FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-2 (AIRPORTS ELECTRONIC BULLETIN BOARD NUMBER 14).
5. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
6. IF THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT WHICH REQUIRES ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., BEYOND THAT SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, THEN THE COST FOR THE ADDITIONAL ITEMS SHALL BE INCLUDED TO THE CONTRACT UNIT PRICES.
7. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
8. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
9. ALL CONCRETE FOR ELECTRICAL EQUIPMENT SHALL COMPLY WITH SPECIFICATION 610-STRUCTURAL PC CONCRETE 3500 PSI AT 28 DAYS, AIR ENTRAINED CONCRETE MIX SHALL BE USED.
10. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL HAVE A 1/4" DIAMETER OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE CONNECTOR INTO THE BASE.
11. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1 1/2" ABOVE THE EDGE OF THE COVER IN THE CASE OF A BASE MOUNTED COUPLING.
12. ALL PERMANENT CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS, OR SPLICE CANS, UNLESS NOTED OTHERWISE.
13. MIMIC PANEL COLORS : RUNWAY 5-23 CIRCUIT = WHITE, TAXIWAY E-1 CIRCUIT = YELLOW, TAXIWAY E-2 CIRCUIT = ORANGE, TAXIWAY K-1 CIRCUIT = RED, AND TAXIWAY K-2 CIRCUIT = RED.



TYPE A
FOR SPLICES IN HOMERUNS FOR EXTENSIONS TO EXISTING CABLES ONLY



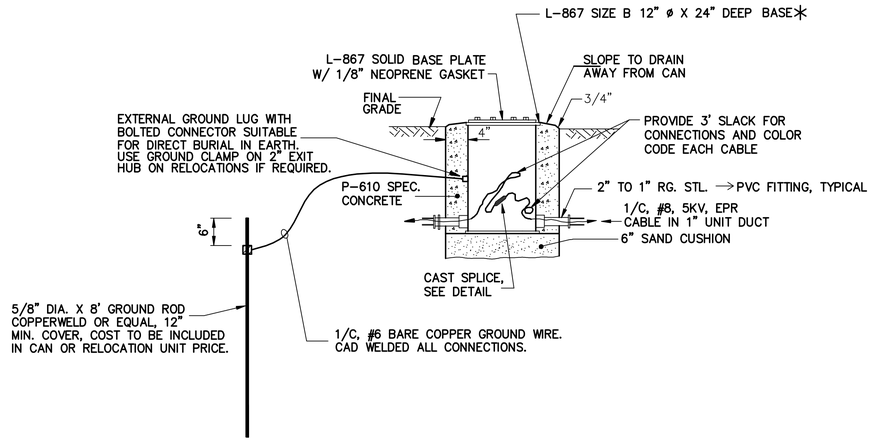
TYPE B
FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT



TYPE C
FOR SPLICES AT RUNWAY LIGHTS

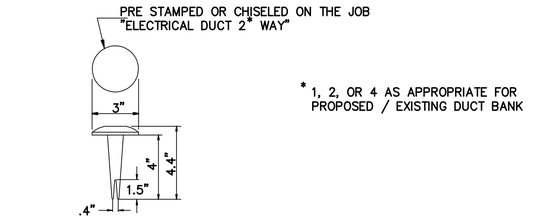
NOTES :
1. SEE LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE
2. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE

CABLE SPLICES
(NOT TO SCALE)

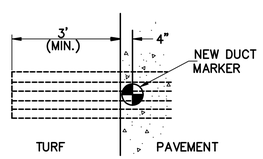


L-867 SPLICE CAN DETAIL
(NOT TO SCALE)

NOTES :
1. * ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90° FROM MAIN ENTRANCE HUB WHERE SHOWN ON PLAN SHEETS.
2. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.

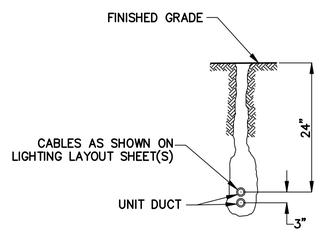


* 1, 2, OR 4 AS APPROPRIATE FOR PROPOSED / EXISTING DUCT BANK

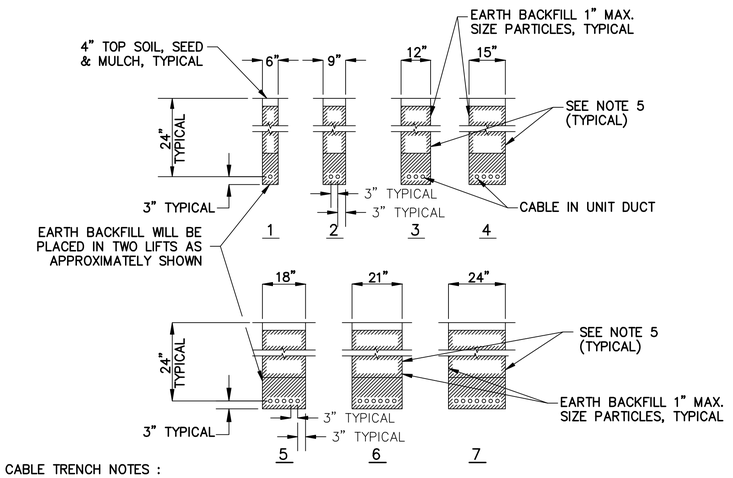


IN-PAVEMENT BRASS DUCT MARKER DETAIL

NOTE: PAVING CONTRACTOR SHALL INSTALL NEW BRASS DUCT MARKERS IN THE PROPOSED PAVEMENT AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT CROSSES EXISTING OR PROPOSED ELECTRICAL DUCTS. COST OF DUCT MARKERS SHALL BE INCLUDED IN THE 401 AND / OR 501 CONTRACT UNIT PRICES.

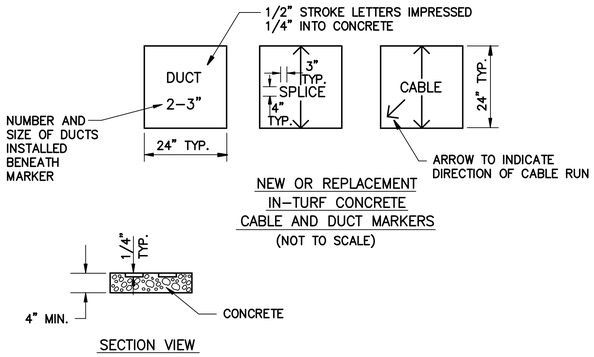


PLOWED CABLE
(NOT TO SCALE)



CABLE TRENCH NOTES :
1. DETAIL NUMBERS INDICATE NO. OF CABLES.
2. TRENCHES WITH MORE THAN 7 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
3. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH. RETURFING MATERIALS AND RATES MAY BE SHOWN ON THE PLANS.
5. INSTALL YELLOW PLASTIC WARNING RIBBON IN TRENCH 9" ABOVE CABLES (TYPICAL ALL TRENCHES).

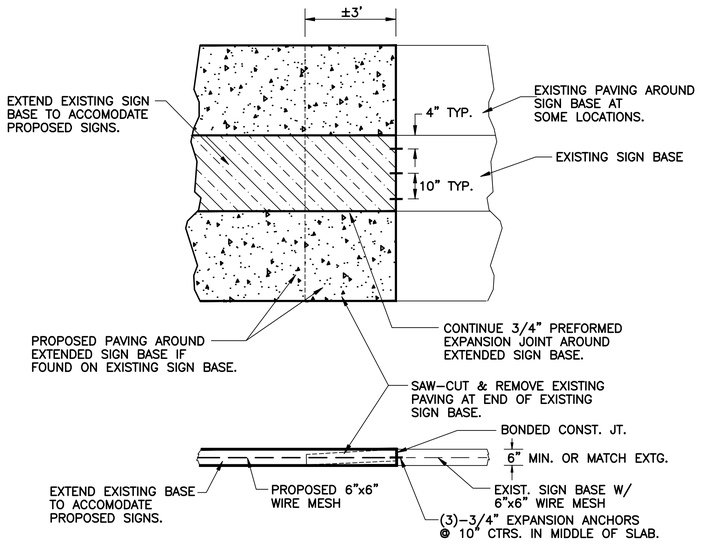
CABLE TRENCHES
(NOT TO SCALE)



NEW OR REPLACEMENT
IN-TURF CONCRETE
CABLE AND DUCT MARKERS
(NOT TO SCALE)

NOTES:

1. NEW MARKERS ARE REQUIRED ON ALL FAA CABLES. CABLE MARKERS SHALL BE INSTALLED BY THE CONTRACTOR AT ALL LOCATIONS SELECTED BY THE RESIDENT ENGINEER AND / OR THE FAA. THE CONTRACTOR SHALL BE REQUIRED TO ADJUST THE ELEVATION OF EXISTING MARKERS AND / OR REPLACE EXISTING MARKERS DAMAGED DURING CONSTRUCTION.
2. COST OF CONCRETE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.
3. EDGE EXPOSED CONCRETE WITH A 1/4" RADIUS TOOL.
4. WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED, SOME OF THE FOLLOWING METHODS SHALL BE EMPLOYED.
   A. REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
   B. INCREASE THE MARKER SIZE TO 30" X 30" MAX.
   C. PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.

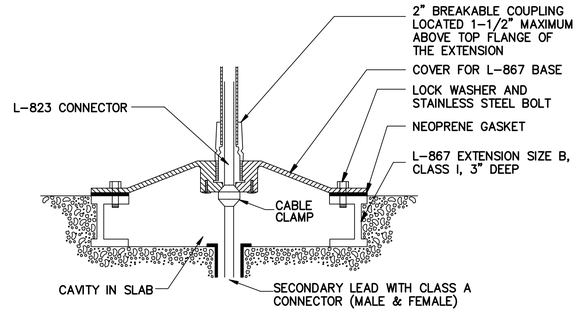


EXISTING SIGN BASE
EXTENSION DETAIL

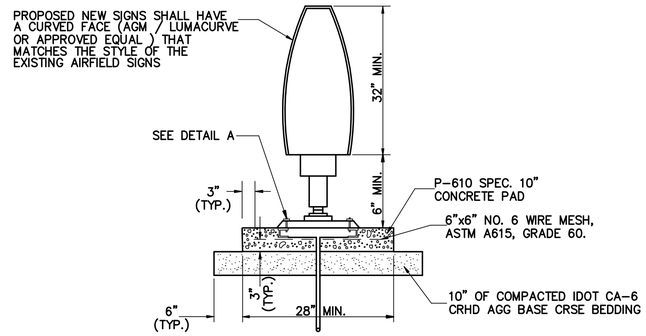
SIZE 3 TAXI GUIDANCE SIGNS ISOLATION TRANSFORMER DATA*				
NUMBER OF MODULES	TRANSFORMER WATTAGE			
	STYLE 2, 4.8A-6.6A		STYLE 3, 2.8A-6.6A	
	TRADITIONAL	LED SYSTEM	TRADITIONAL	LED SYSTEM
1	100	200	200	200
2	300	200	300	300
3	500	300	500	500
4	500	300	500	500
DISTANCE REMAINING	300	200	300	300

TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* = OR AS REQUIRED BY SIGN MANUFACTURER.
** = WITH A SIAMESE PIGTAIL ADAPTER AND TWO TRANSFORMERS.

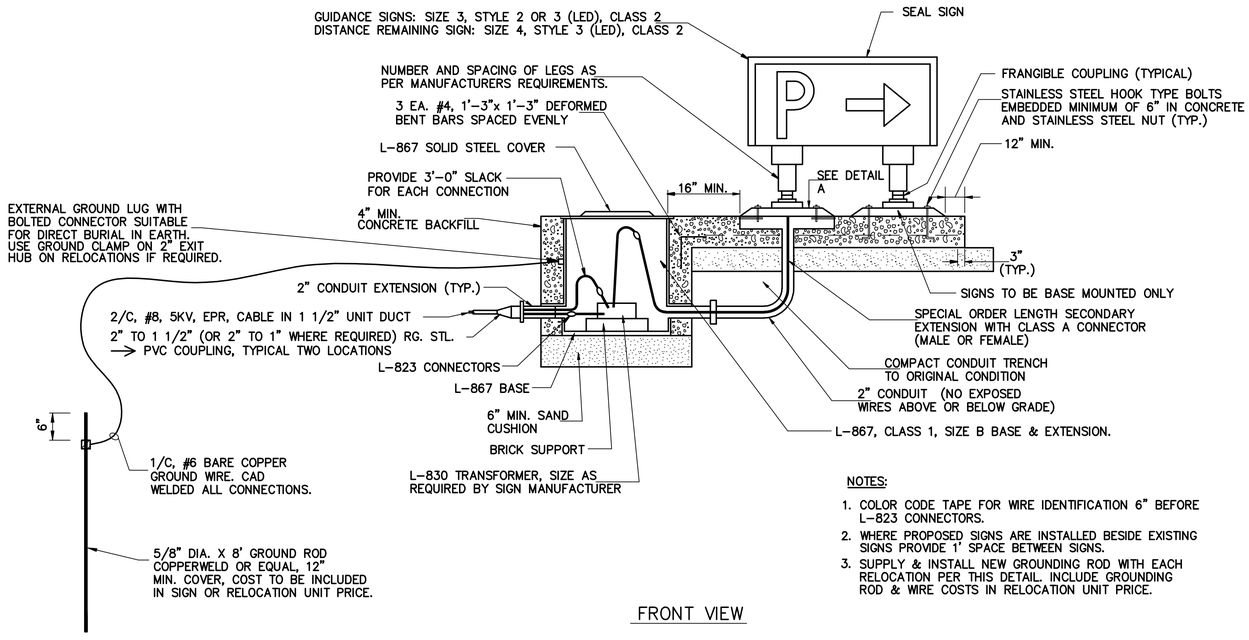
PROPOSED GUIDANCE SIGNS



DETAIL A



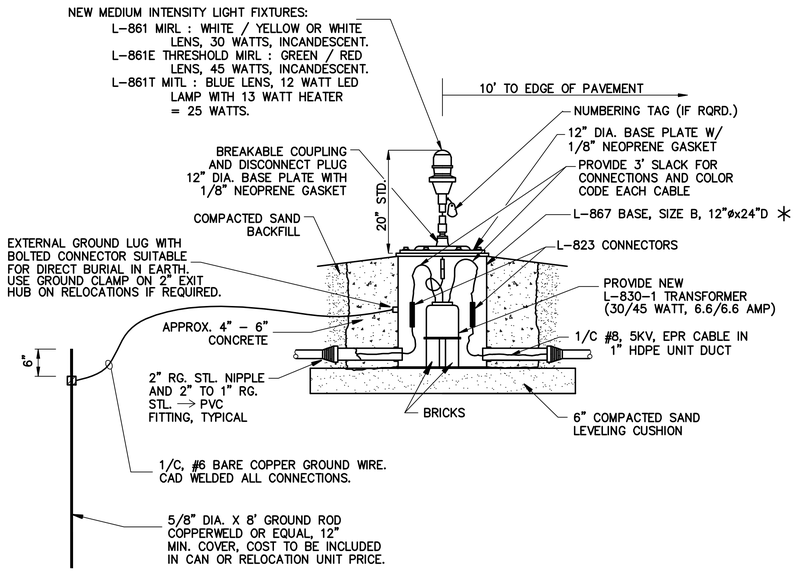
SIDE VIEW



NOTES:

1. COLOR CODE TAPE FOR WIRE IDENTIFICATION 6" BEFORE L-823 CONNECTORS.
2. WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.
3. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.

FRONT VIEW

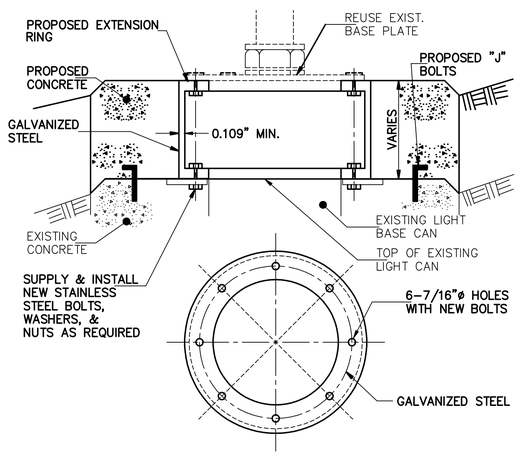


L-861 / L-861T EDGE LIGHT INSTALLATION DETAILS
MEDIUM INTENSITY RUNWAY LIGHT (MIRL) / MEDIUM INTENSITY TAXIWAY LIGHT (MITL)
BASE MOUNTED, 6.6 AMP SERIES CIRCUIT

NOT TO SCALE

NOTES :

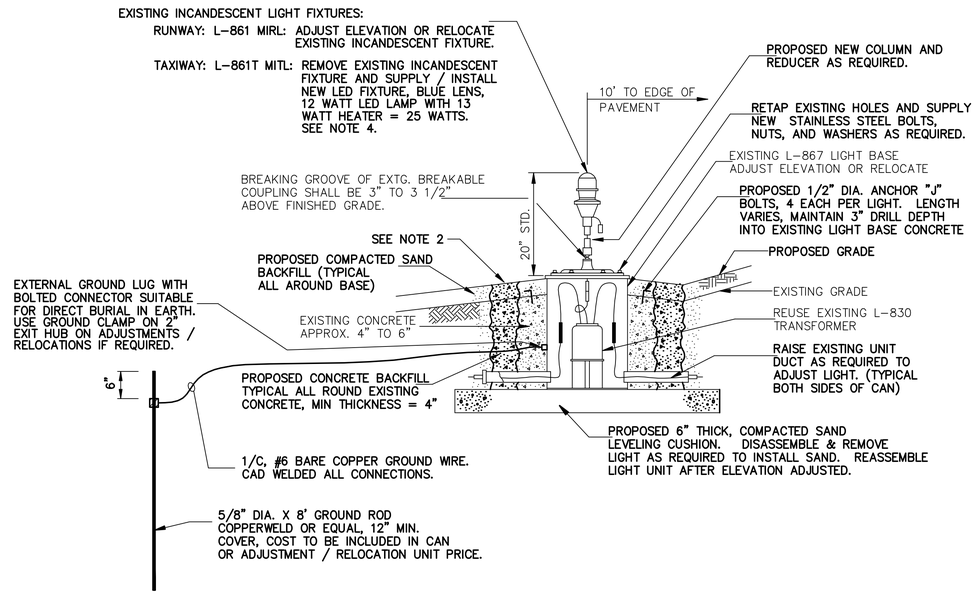
1. BREAKING GROOVE OF BREAKABLE COUPLING SHALL BE 3" TO 3 1/2" ABOVE FINISHED GRADE.
2. * ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90° FROM MAIN ENTRANCE HUB WHERE INDICATED ON PLAN SHEETS.
3. PROPOSED NEW L-861 MIRL UNITS SHALL BE INCANDESCENT.
4. PROPOSED NEW L-861T MITL UNITS SHALL BE LED.
5. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH UNIT PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN CONTRACT UNIT PRICE.



NOTE: THE CONTRACTOR SHALL FIELD VERIFY EXISTING LIGHT BASE TYPES (MOST ARE 12" L-867) AND REQUIRED HEIGHT ADJUSTMENT DIMENSIONS. NO ADDITIONAL PAYMENT WILL BE MADE FOR ALTERNATE LIGHT CAN TYPES. REDRILL AND RETAP EXISTING HOLES AS REQUIRED. COSTS SHALL BE INCLUDED IN THE UNIT PRICE FOR LIGHT ADJUSTMENT.

OPTIONAL EXTENSION RING DETAIL

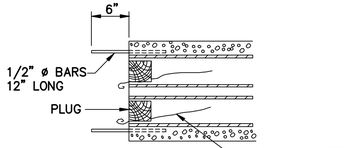
N.T.S.



TYPICAL DETAIL L-861 / L-861T
EDGE LIGHTS ADJUSTMENT OR RELOCATION - IN TURF

NOTES:

1. AT THE CONTRACTOR'S OPTION, LIGHT BASE CAN EXTENSION RINGS MAY BE USED TO ADJUST LIGHT FIXTURE. CONTRACTOR TO EXTEND EXISTING CONCRETE APRON AROUND LIGHT BASE TO PROPOSED GRADE ELEVATION IF BASE CAN EXTENSION RINGS ARE USED. CONTRACTOR TO DETERMINE HEIGHT OF BASE CAN EXTENSION RINGS IN THE FIELD AT THE TIME OF CONSTRUCTION.
2. GROUND SURFACE DRAINAGE TO FLOW AWAY FROM EDGE OF PAVEMENT AND LIGHT BASE CANS. NO WATER PONDING OR LOW SPOTS SHALL BE PERMITTED AROUND LIGHT BASE CANS.
3. BREAKING GROOVE OF BREAKABLE COUPLING SHALL BE 3" TO 3 1/2" ABOVE FINISHED GRADE.
4. IF THE CONDITION OF THE EXISTING LIGHTING EQUIPMENT TO BE REMOVED OR ADJUSTED IS ACCEPTABLE TO THE RESIDENT ENGINEER, THE CONTRACTOR MAY REUSE THE REMOVED LIGHT FIXTURES (MIRL ONLY), BASE PLATES, CANS, AND TRANSFORMERS AT THE PROPOSED LIGHT LOCATIONS. INCANDESCENT MITL UNITS (TO BE RELOCATED OR ADJUSTED) SHALL BE CONVERTED TO LED UNITS BY SUPPLYING & INSTALLING NEW LED FIXTURES, ELECTRICAL LEADS, LED LAMPS, GLASS GLOBES, COLUMNS, REDUCERS (IF REQUIRED) AND THERMOSTATICALLY CONTROLLED ARTIC HEATER KITS. IF THE EXISTING EDGE LIGHT EQUIPMENT IS REUSED, THEN THE CONTRACTOR SHALL SUPPLY AND INSTALL PROPOSED 6" COMPACTED SAND LEVELING CUSHION, EXTEND THE EXISTING STEEL CONDUIT TO A POINT OUTSIDE OF THE PROPOSED CONCRETE BACKFILL, ADDITIONAL P.C. CONCRETE BACKFILL ALL AROUND (MIN. 4" THICKNESS), COMPACTED SAND BACKFILL, NEW 1/8" NEOPRENE GASKETS, RETAP BASE HOLES AS REQUIRED, NEW STAINLESS STEEL BOLTS, NUTS, & WASHERS, AND NEW HEATERS, LENS & LAMPS AS REQUIRED. CONTRACTOR SHALL REPLACE ANY EQUIPMENT DAMAGED DURING RELOCATION / ADJUSTMENT AS DIRECTED BY THE RESIDENT ENGINEER. CONTRACTOR SHALL SUPPLY AND INSTALL ALL NEW EDGE LIGHT EQUIPMENT AS REQUIRED. INCLUDE COSTS FOR ALL OF THE ABOVE ITEMS IN THE EDGE LIGHT RELOCATION / ADJUSTMENT CONTRACT UNIT PRICES.
5. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.
6. PROPOSED RELOCATED L-861 MIRL UNITS SHALL BE INCANDESCENT.
7. PROPOSED RELOCATED L-861T MITL UNITS SHALL BE LED.
8. EXISTING USEABLE LIGHTING EQUIPMENT THAT IS REMOVED AND NOT REINSTALLED SHALL BE TURNED OVER TO THE MAA.



TYPICAL SECTION
(NOT TO SCALE)

NOTES :

A. DIMENSIONS SHOWN ARE MINIMUM.

B. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE.

C. DUCT CONCRETE SHALL BE 610 STRUCTURAL P.C. CONC.

D. PLASTIC DUCT (PVC) SHALL BE TYPE 1 CONFORMING TO FEDERAL SPEC. W-C-1094.

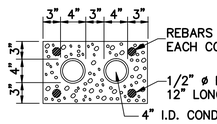
E. ALL DUCT SHALL BE 4" INSIDE DIA.

F. WHERE EDGE DRAINS ARE USED, THE LENGTH OF THE DUCT SHALL BE SUCH THAT THE ENDS OF THE DUCTS WILL NOT BE LESS THAN TWO FEET FROM THE OUTSIDE EDGE OF ANY POROUS GRANULAR BACKFILL MATERIAL.

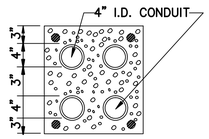
G. WHERE EDGE DRAINS ARE NOT USED, THE LENGTH OF THE DUCT SHALL BE SUCH THAT THE ENDS OF THE DUCTS WILL NOT BE LESS THAN THREE FEET FROM THE EDGE OF ANY PAVED SURFACE.

H. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN (TURF CABLE MARKER)

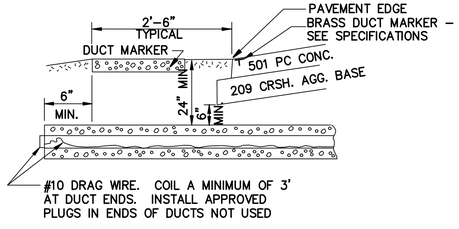
I. USE SPLIT DUCT IN DUCT BANKS AS REQUIRED WHERE EXISTING CABLES ARE PRESENT. COST OF SPLIT DUCT TO BE INCLUDED IN THE UNIT PRICE FOR DUCT BANK.



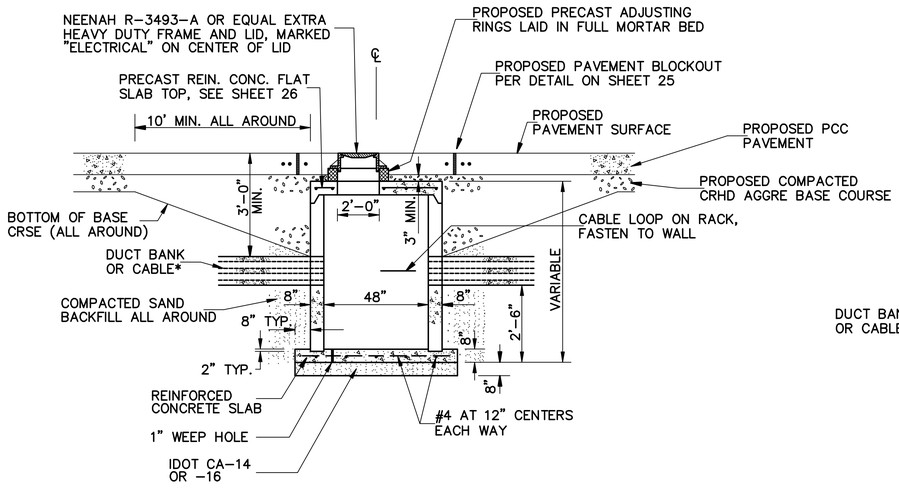
2-DUCT BANK
(NOT TO SCALE)



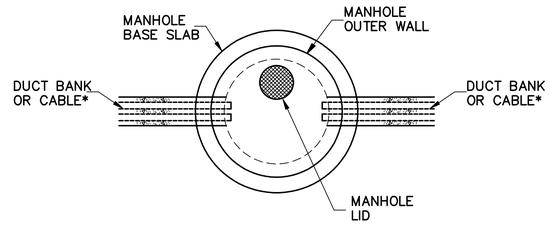
4-DUCT BANK
(NOT TO SCALE)



UNDERGROUND ELECTRICAL DUCT
(NOT TO SCALE)



PROFILE VIEW



PLAN VIEW

* = FOR CABLE: INSTALL 2" RG. STL. CONDUIT SLEEVES THROUGH MANHOLE WALL. INSTALL WATERPROOF BUSHINGS ON ENDS OF CONDUITS.

FURNISHING AND INSTALLING SAND CUSHION, CONCR. BASE SLAB, SAND BACKFILL, FRAME & LID, CABLE RACK AND FLAT SLAB TOP TO BE INCLUDED IN THE CONTRACT UNIT PRICE .

DETAIL OF ELECTRICAL MANHOLE

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 40 OF 69

PROPOSED SEDIMENT TRAP PER DETAIL ON SHEET 41 AROUND INLET (TYP. 6 PLACES)

SERVICE ROAD

R9 RSA

TXWY E

PROPOSED PAVEMENT (BY OTHERS) UNDER CONSTRUCTION

FUTURE TXWY P

PROPOSED T-P AREA AR901510 - SEEDING AND CONSTRUCTION LIMITS

PROPOSED PAVEMENT (BY OTHERS) UNDER CONSTRUCTION

PROPOSED T-D AREA AR901510 - SEEDING AND CONSTRUCTION LIMITS

R5 RSA

PROPOSED SILT FENCE

PROPOSED DITCH CHECKS ACROSS PROPOSED OR EXISTING DRAINAGE SWALES, EXACT LOCATION AND NUMBER TO BE DETERMINED IN THE FIELD (TYP. 6 PLACES)

NOTE:

SEE SHEET 42 FOR STORM WATER MANAGERS SIGNATURE CHART, EROSION / SEDIMENT CONTROL MEASURES LIST, STORM WATER GENERAL NOTES AND CONSTRUCTION ACTIVITIES SCHEDULE.

EXISTING PAVEMENT TO BE REMOVED

T-E

PROPOSED SEDIMENT TRAP PER DETAIL ON SHEET 41 AROUND INLET (TYP. 6 PLACES)

RELOCATED T-D

PROPOSED SEDIMENT TRAP PER DETAIL ON SHEET 41 AROUND INLET (TYP. 6 PLACES)

T-K

T-K2

SERVICE ROAD

RNWY 5-23

PAPI LIGHTS

R5 RSA

PROPOSED DITCH CHECKS ACROSS PROPOSED OR EXISTING DRAINAGE SWALES, EXACT LOCATION AND NUMBER TO BE DETERMINED IN THE FIELD (TYP. 6 PLACES)

T-K

PROPOSED SILT FENCE

N

SCALE FEET
0 100

LEGEND:

- EXTG. EDGE LIGHT
- EXTG. GUIDANCE SIGN
- EXTG. ELECTRICAL CABLE
- EXTG. STORM SEWER OR UNDERDRAIN
- EXTG. INLET
- EXTG. OUTLET
- EXTG. MANHOLE
- EXTG. DRAINAGE DITCH
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- EXISTING FENCE
- EXISTING OR PROPOSED DRAINAGE SWALE
- PROPOSED SILT FENCE
- PROPOSED TEMPORARY STRAW BALE DITCH CHECK
- PROPOSED SEDIMENT TRAP

TAXIWAY P EMBANKMENT AREA AND RELOCATED TAXIWAY D AREA PLAN

PROPOSED STORM WATER POLLUTION PREVENTION PLAN

QUAD CITY INTERNATIONAL AIRPORT
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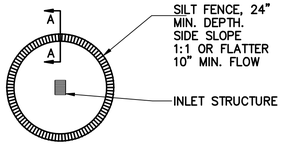
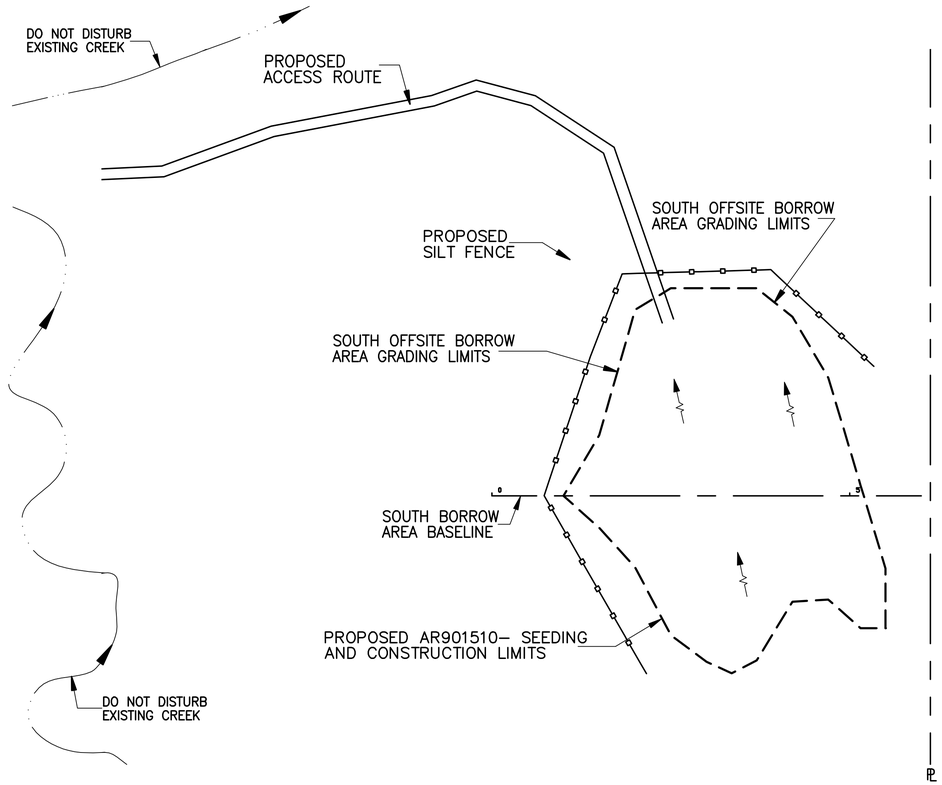
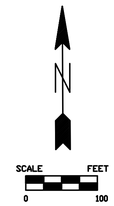
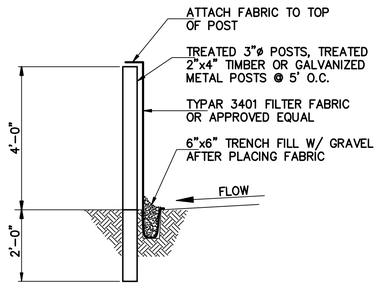


TEMPORARY SEDIMENT TRAP
AT ALL INLET STRUCTURES



SOUTH OFFSITE BORROW AREA PLAN





SECTION A-A
SILT FENCE DETAIL
NOT TO SCALE

LEGEND:

- EXTG. EDGE LIGHT
- EXTG. GUIDANCE SIGN
- EXTG. ELECTRICAL CABLE
- EXTG. STORM SEWER OR UNDERDRAIN
- EXTG. INLET
- EXTG. OUTLET
- EXTG. MANHOLE
- EXTG. DRAINAGE DITCH
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- EXISTING FENCE
- EXISTING OR PROPOSED DRAINAGE SWALE
- PROPOSED SILT FENCE
- PROPOSED TEMPORARY STRAW BALE DITCH CHECK
- PROPOSED SEDIMENT TRAP

NOTE:

SEE SHEET 42 FOR STORM WATER MANAGERS SIGNATURE CHART, EROSION / SEDIMENT CONTROL MEASURES LIST, STORM WATER GENERAL NOTES AND CONSTRUCTION ACTIVITIES SCHEDULE.

GENERAL NOTES:

1. TEMPORARY DITCH CHECKS – TWO BALES HIGH WITH SILT FENCING SHALL BE REQUIRED PER IDOT STANDARD 280001 – TEMPORARY EROSION SITE CONTROL SYSTEM DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING, MAINTAINING, AND REMOVING DITCH CHECKS, SILT FENCE, AND SEDIMENT TRAPS TO THE SATISFACTION OF THE RESIDENT ENGINEER. THIS INCLUDES, BUT IS NOT LIMITED TO, CLEANING EROSION SOILS AS REQUIRED.
2. LOCATION OF THE DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHOWN ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
3. ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 21 DAYS (SUCH AS A WINTER SHUTDOWN), THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED IN ACCORDANCE WITH ITEM 901510 WITHIN 14 DAYS OF THE LAST DISTURBANCE. DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR AND EACH SUBCONTRACTOR RESPONSIBLE FOR WATER POLLUTION CONTROL SHALL DESIGNATED, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WATER POLLUTION CONTROL ITEMS. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE TO REPAIR AND MAINTAIN WATER POLLUTION CONTROL DEVICES ON A 24–HOUR / 7 DAYS PER WEEK BASIS.
5. CONTRACTOR TO EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALE AS REQUIRED TO PREVENT RAIN WATER PONDING AND TO CONTROL STORM WATER RUN–OFF.
6. CONTRACTOR SHALL ADHERE TO THE CITY OF MOLINE'S EROSION AND SEDIMENT CONTROL REGULATIONS AND THE ILLINOIS MANUAL ON EROSION AND SEDIMENT CONTROL.
7. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION THAT DISTURBS EXISTING STORM WATER RUN–OFF CONDITIONS AND/OR GROUND VEGETATION.
8. EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPLACED OR REPAIRED AS NECESSARY.
9. THE CONTRACTOR SHALL MONITOR AND MAINTAIN ALL SITE EROSION INCLUDING ALL LOCATIONS WHERE VEHICLES / CONSTRUCTION EQUIPMENT TRAVEL AROUND THE SITE, STORAGE / PARKING AREAS, BATCH PLANT AREAS AND ALL OTHER AREAS SUBJECT TO EROSION. THESE AREAS SHALL BE CONTROLLED AND INSPECTED BY THE CONTRACTOR AT LEAST ONCE EVERY SEVEN DAYS AND WITHIN 24 HOURS AFTER THE END OF EACH 0.5 INCHES OR GREATER PRECIPITATION EVENT.
10. RESIDENT ENGINEER SHALL CHECK THAT ALL FILL AREAS ARE TO A MINIMUM COMPACTION OF 95% OF THE MATERIALS STANDARD PROCTOR MAXIMUM DRY DENSITY.
11. SILT FENCE, SEDIMENT TRAPS, AND HAY BALES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE OR HAY BALES.
12. THE CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF AND CREATE / KEEP ALL RECORDS AND REPORTS REQUIRED BY THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THIS CONSTRUCTION PROJECT.
13. THE RESIDENT ENGINEER WILL:
    A. PREPARE AND SUBMIT THE NOTICE OF INTENT (NOI) DOCUMENT.
    B. CONDUCT SITE INSPECTIONS AND COMPLETE / FILE INSPECTION REPORTS.
    C. IF REQUIRED, SUBMIT INCIDENCE OF NON–COMPLIANCE (ION) FORMS.
    D. SUBMIT NOTICE OF TERMINATION (NOT) FORM.
14. ADDITIONAL EROSION CONTROL DEVICES SHALL BE USED AS REQUIRED. THE COSTS OF ALL MEASURES NECESSARY TO COMPLY WITH THIS STORM WATER POLLUTION PREVENTION PLAN AND THE NPDES PERMIT SHALL BE INCLUDED IN THE ITEM 156500 – TEMPORARY EROSION CONTROL LUMP SUM PRICES.

EROSION AND SEDIMENT CONTROL MEASURES

ITEM	LOCATION	SPECIAL PROVISION/ PAY ITEM
TEMPORARY DITCH CHECKS	TAXIWAY P, STA. 620+00, RT. 480'; TAXIWAY P, STA. 627+30, LT. 300'; TAXIWAY P, STA. 624+80, RT. 365'; TAXIWAY P, STA. 624+85, RT. 650'; RELOCATED TAXIWAY D, STA. 765+10, RT. 175'; AND RELOCATED TAXIWAY D, STA. 770+20, RT. 190'	AR156500
TEMPORARY SILT FENCE	AT ALL SEDIMENT TRAP & DITCH CHECK LOCATIONS AND TAXIWAY P, RIGHT, STA. 625+00 TO STA. 627+70; RELOCATED TAXIWAY D, RIGHT STA. 771+55 TO STA. 773+14; AND RUNWAY 13–31 STA. 317+25 RT. TO STA. 320+95 RT. (AROUND SOUTH BORROW AREA).	AR156500
TEMPORARY SEDIMENT TRAPS	TAXIWAY P, STA. 616+50, LT. 270'; TAXIWAY P, STA. 620+83, LT. 332'; RELOCATED TAXIWAY D, STA. 763+71, RT. 90'; RELOCATED TAXIWAY D, STA. 764+28, LT. 95'; RELOCATED TAXIWAY D, STA. 764+88, RT. 787'; AND RELOCATED TAXIWAY D, STA. 770+68 LT. 277'.	AR156500

ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE

NO.	ITEM DESCRIPTION	WEEK 1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
1	INSTALL SEDIMENT/EROSION CONTROLS	■															
2	CLEARING AND GRADING		■														
3	EXCAVATION AND EMBANKMENT			■	■	■	■	■	■								
4	TURFING														■	■	
5	MAINTAIN SEDIMENT/EROSION CONTROL		■	■	■	■	■	■	■	■	■	■	■	■	■	■	■
6	PAVING						■	■	■	■	■	■	■	■	■		
7	CLEAN–UP															■	■

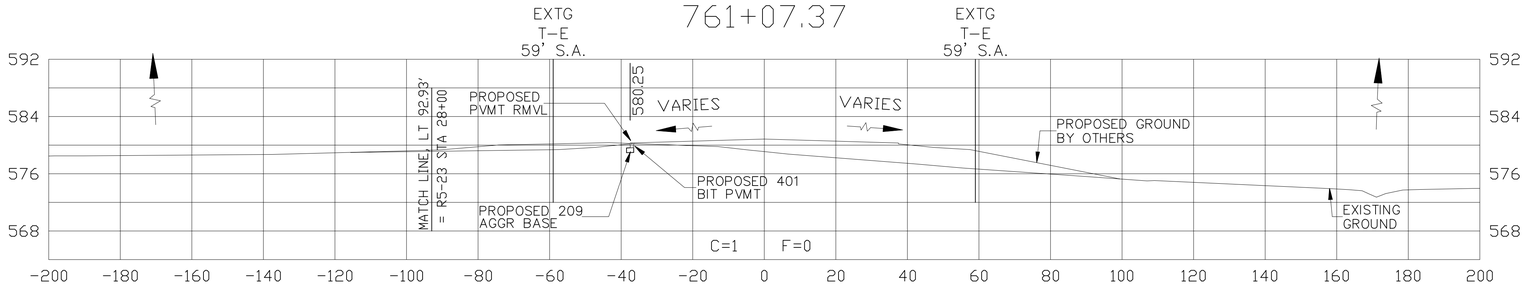
CONTRACTOR AND SUBCONTRACTOR'S CERTIFICATION STATEMENT:

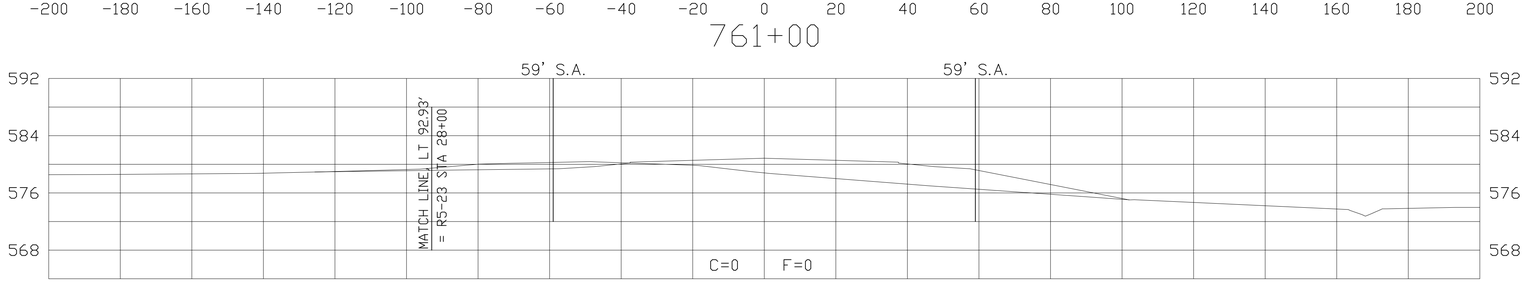
BY THE ACTION OF SIGNING AND AGREEING TO THE TERMS AND CONDITIONS OF THE CONSTRUCTION CONTRACT FOR THIS PROJECT, I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION AND DETAILED IN THESE PLANS.

STORM WATER MANAGERS

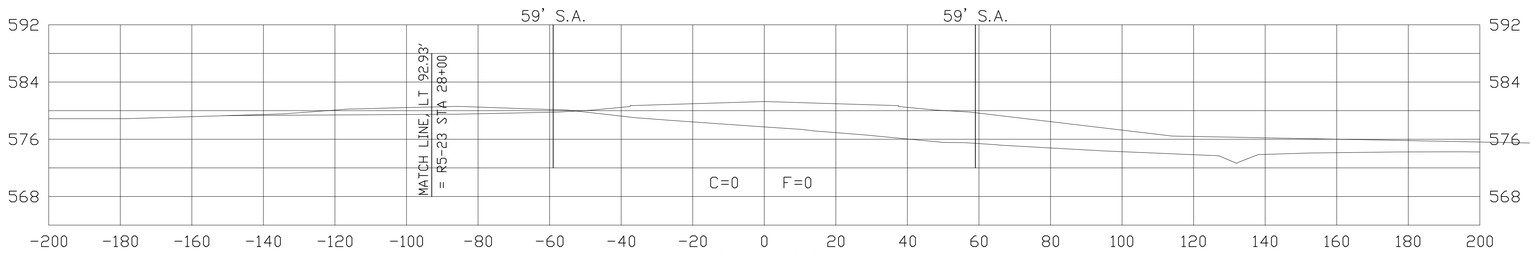
	NAME	ADDRESS	TELEPHONE NUMBER WORK	TELEPHONE NUMBER HOME	SIGNATURE
CONTRACTOR					
SUBCONTRACTOR					
SUBCONTRACTOR					

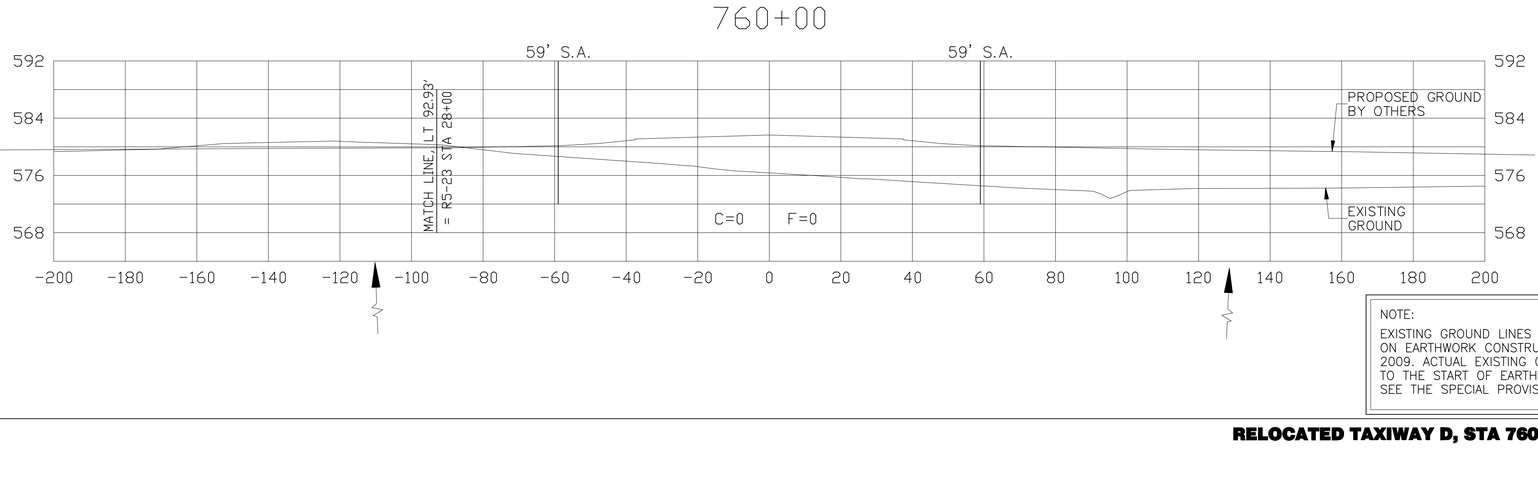
START PROP
BIT PVMT





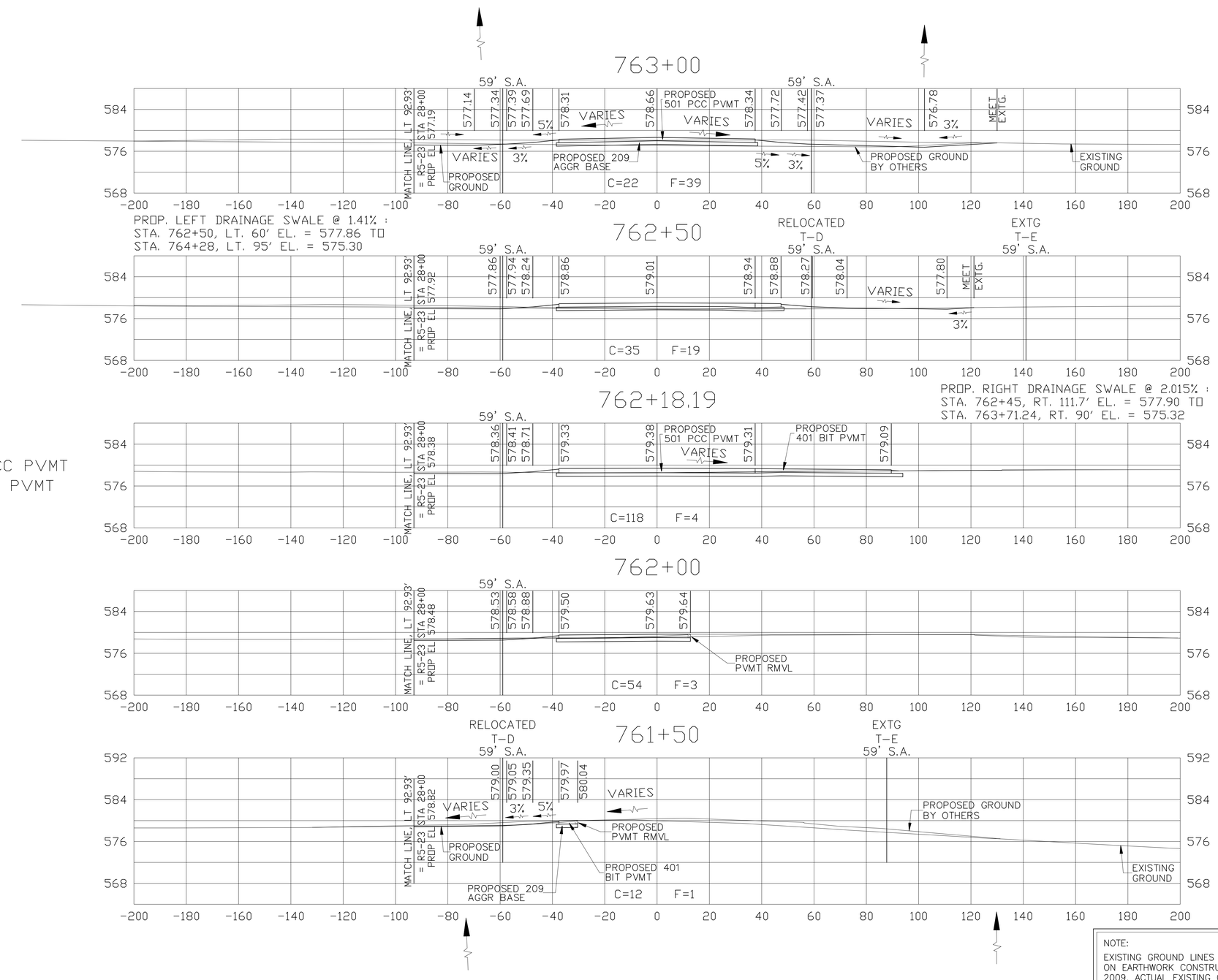
START RELOCATED T-D EARTHWORK AT STA 761+00





T-D 760+00 =
T-E 860+00

NOTE:
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EARTHWORK CONSTRUCTION COMPLETED IN THE FALL OF 2009. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



AHEAD: PCC PVMT
BACK: BIT PVMT

NOTE:
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RELOCATED TAXIWAY D, STA 761+50 TO STA 763+00

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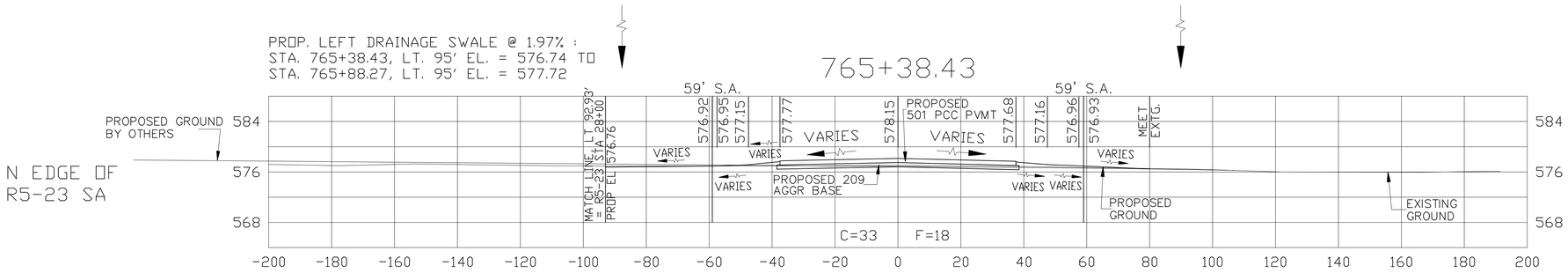
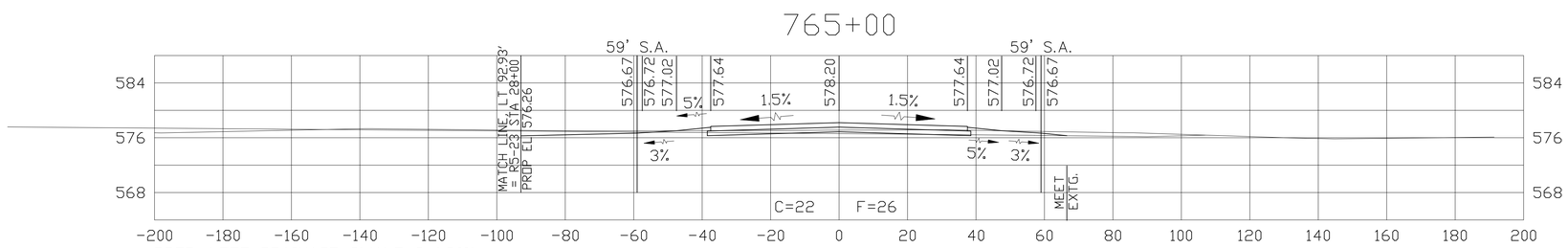
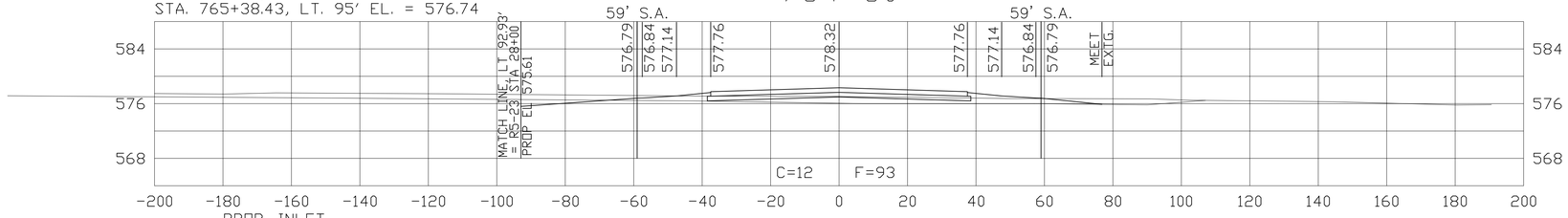
PROP. LEFT DRAINAGE SWALE @ 1.97% :
STA. 765+38.43, LT. 95' EL. = 576.74 TO
STA. 765+88.27, LT. 95' EL. = 577.72

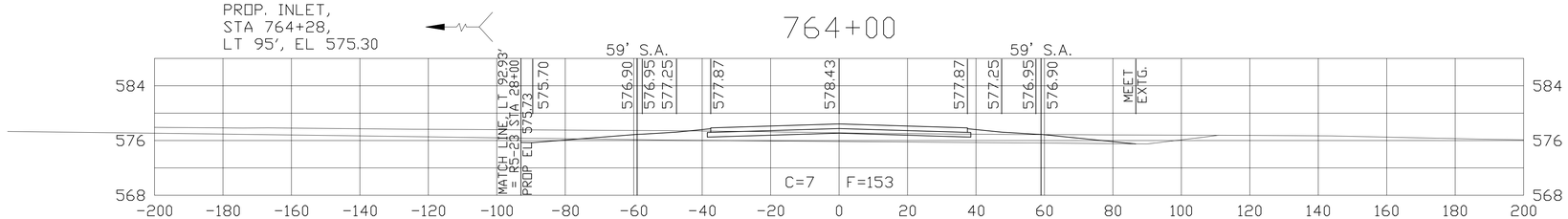




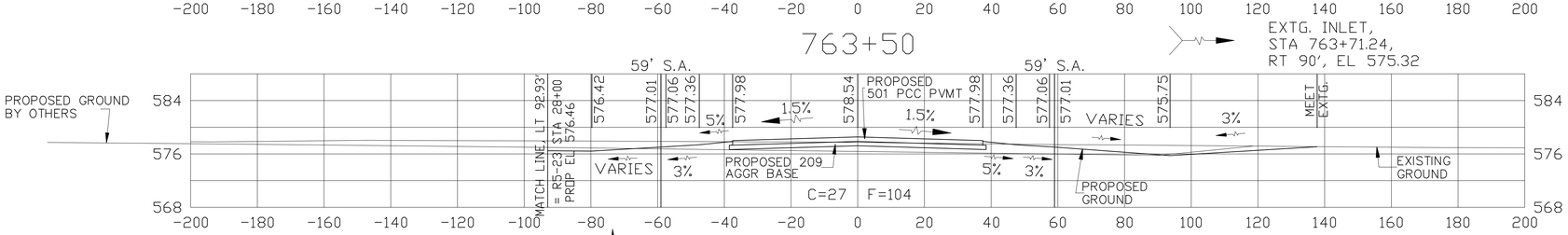
PROP. LEFT DRAINAGE SWALE @ 1.30% :
STA. 764+28, LT. 95' EL. = 575.30 TO
STA. 765+38.43, LT. 95' EL. = 576.74



PROP. INLET,
STA 764+28,
LT 95', EL 575.30



EXTG. INLET,
STA 763+71.24,
RT 90', EL 575.32



NOTE:
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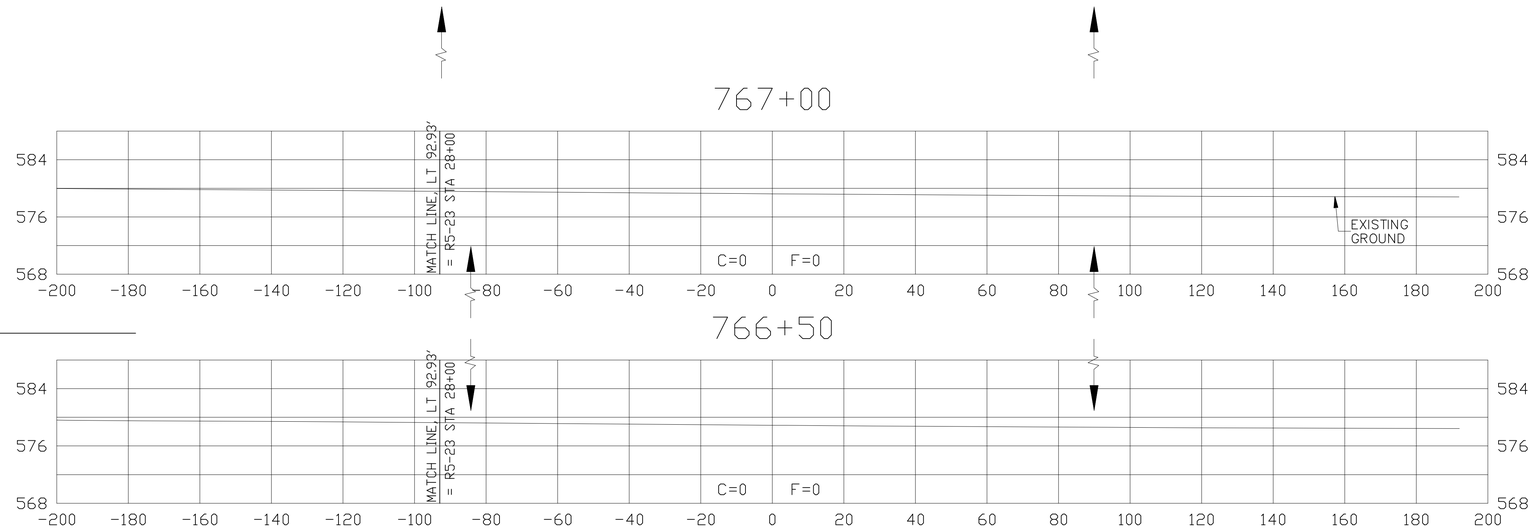
RELOCATED TAXIWAY D, STA 763+50 TO STA 765+38.43

766+88.43,
CENTERLINE R5-23

N EDGE OF
R5-23 PVMT



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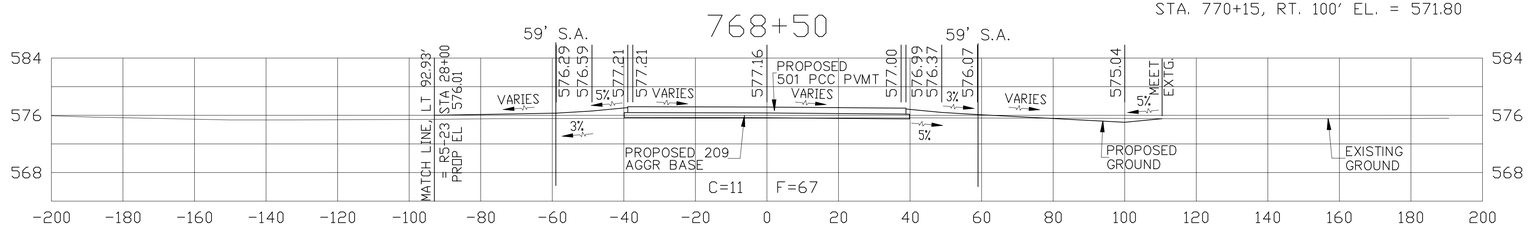
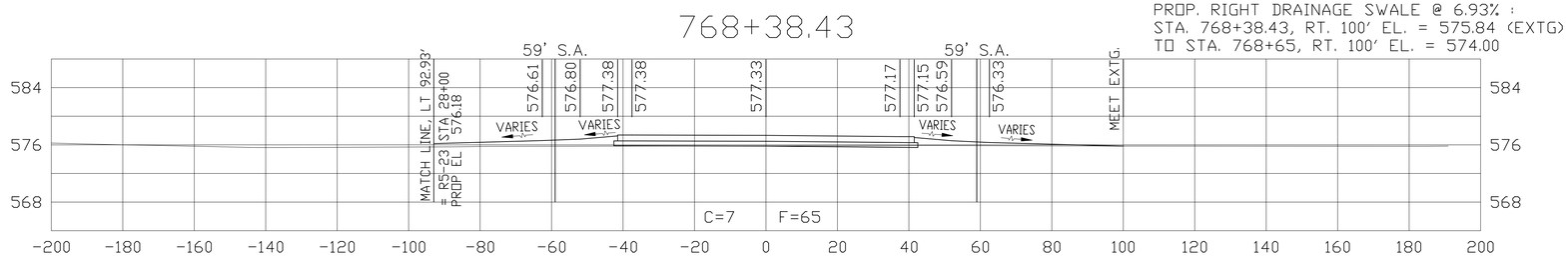
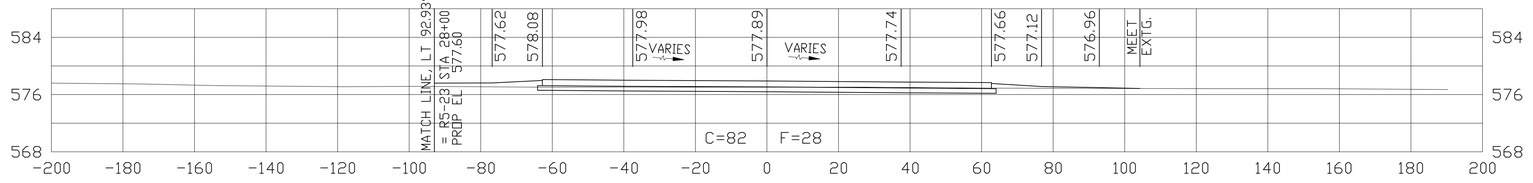
PROP. RIGHT DRAINAGE SWALE @ 1.47% :
STA. 768+65, RT. 100' EL. = 574.00 TO
STA. 770+15, RT. 100' EL. = 571.80

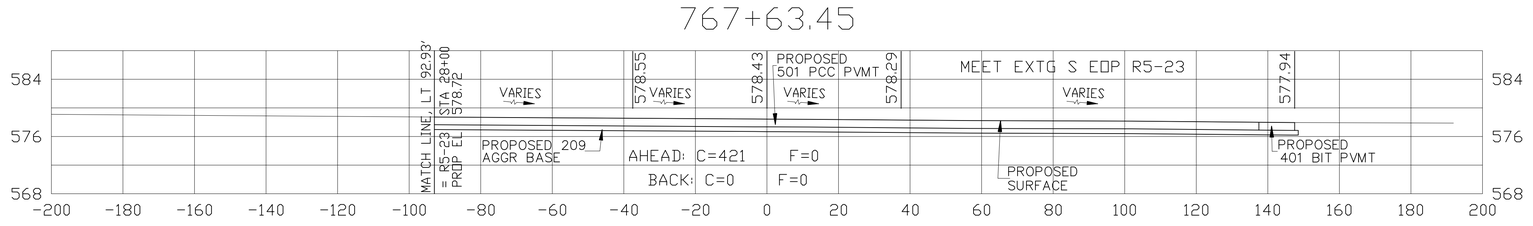


PROP. RIGHT DRAINAGE SWALE @ 6.93% :
STA. 768+38.43, RT. 100' EL. = 575.84 (EXTG)
TO STA. 768+65, RT. 100' EL. = 574.00

S EDGE OF
R5-23 SA







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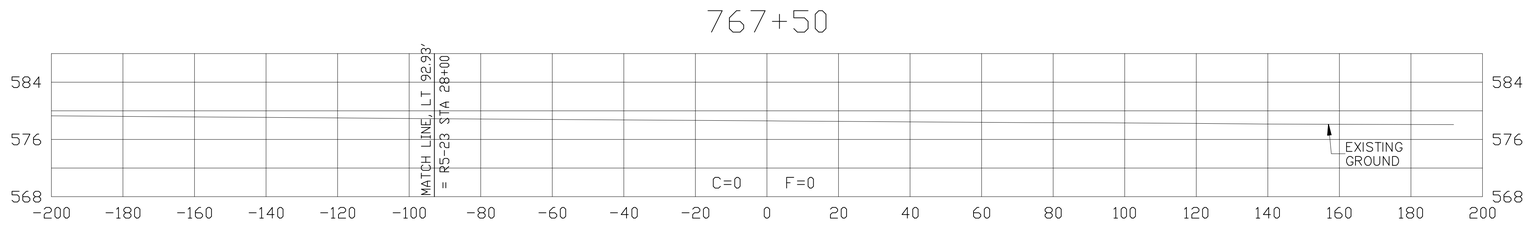
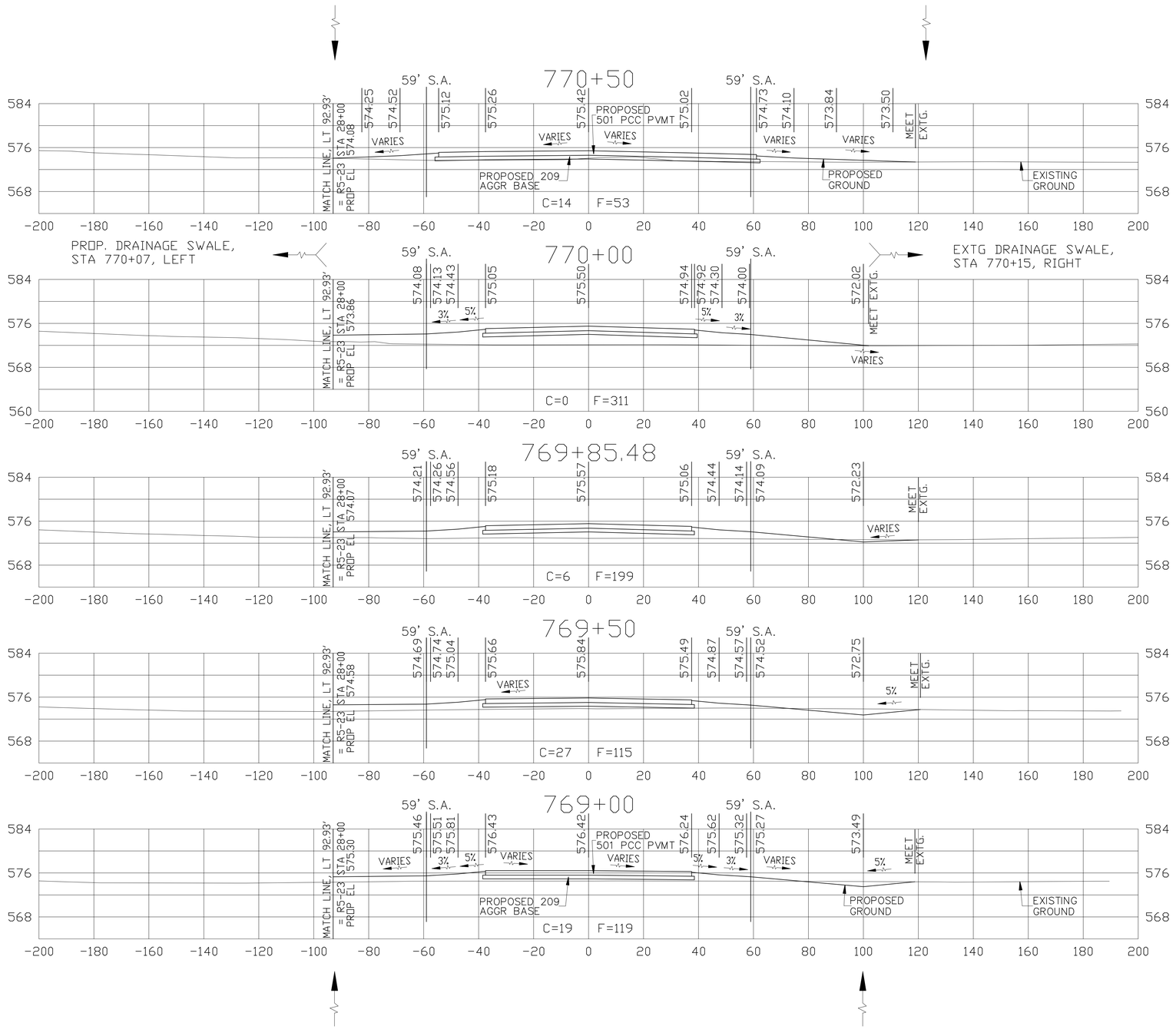
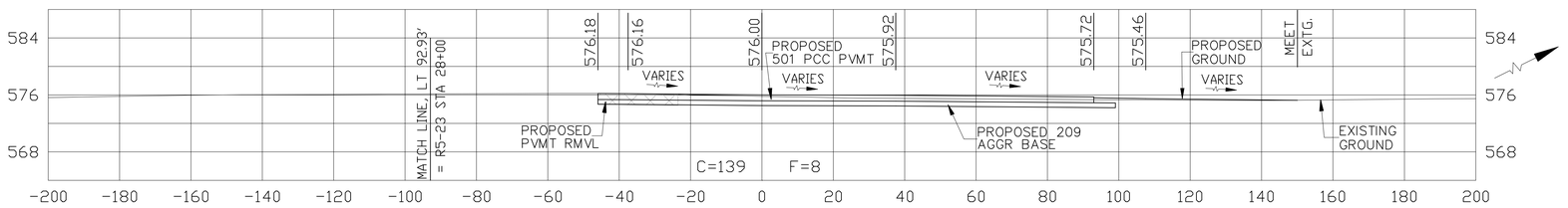
R5-23 PVMT



EARTHWORK NOTE:
SOUTH TOE OF SLOPE OF EXTG SWALE @ STA. 770+20. USE STA 770+00 CUT / FILL AREAS AT STA. 770+20.

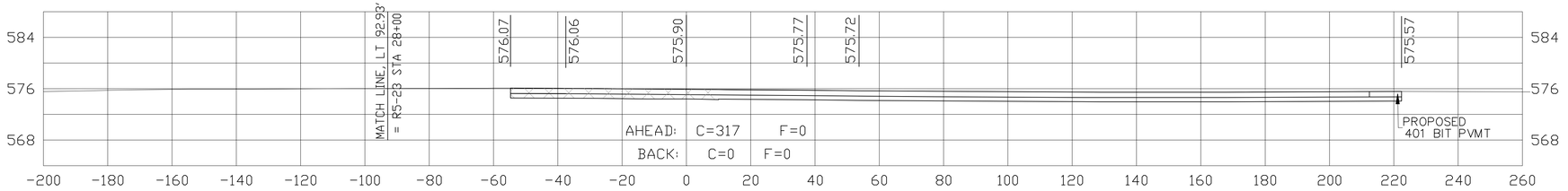


771+50



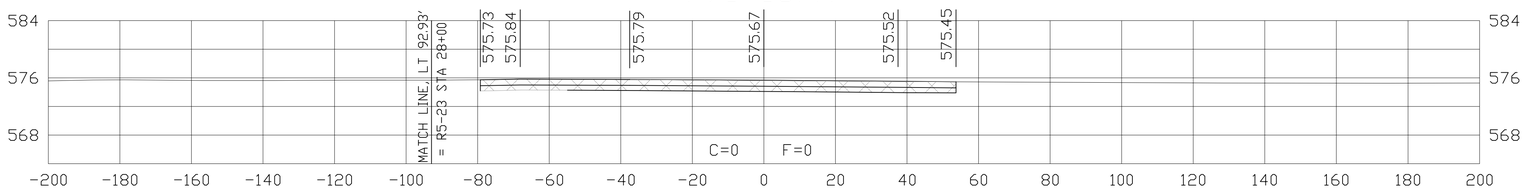
771+40.43

S EDGE OF
T-K PVMT

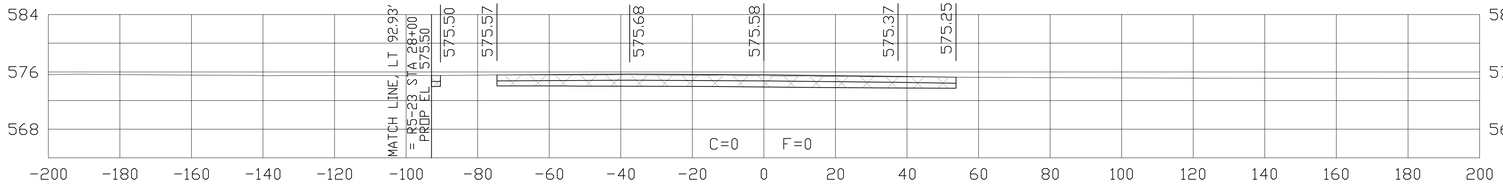


771+13.47

T-K PVMT
CENTERLINE



771+00



770+88.35

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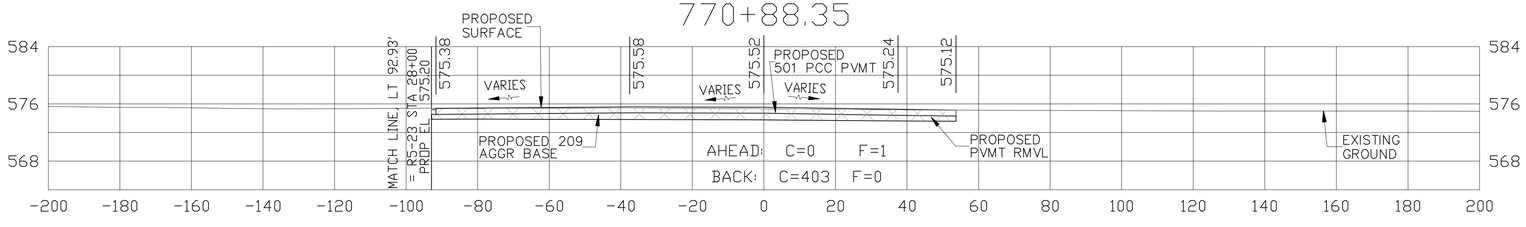
T-K PVMT



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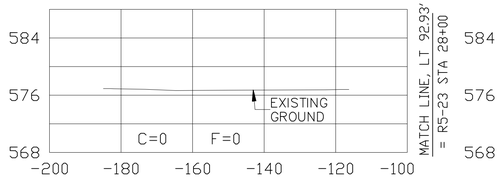
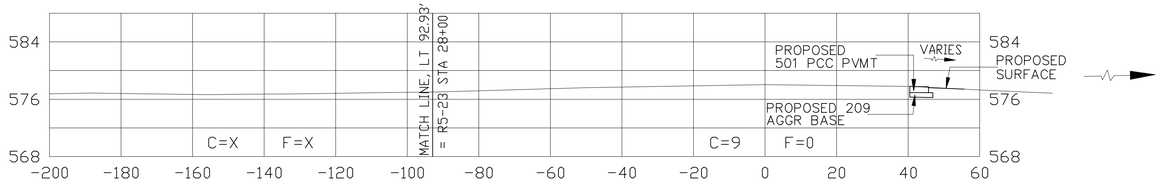
END RELOCATED T-D EARTHWORK AT STA 773+15.72

773+15.72

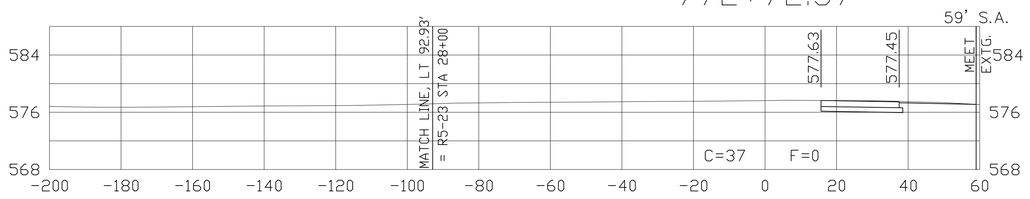


773+00

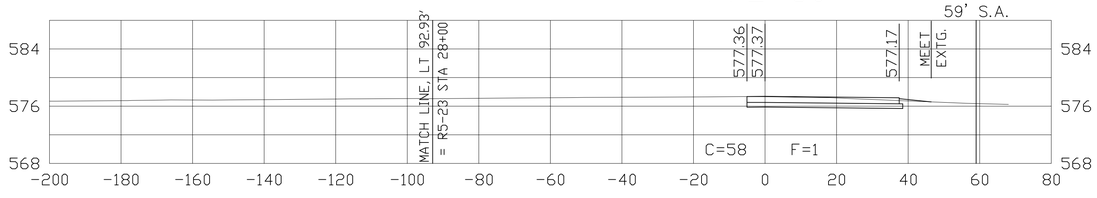


772+72.57

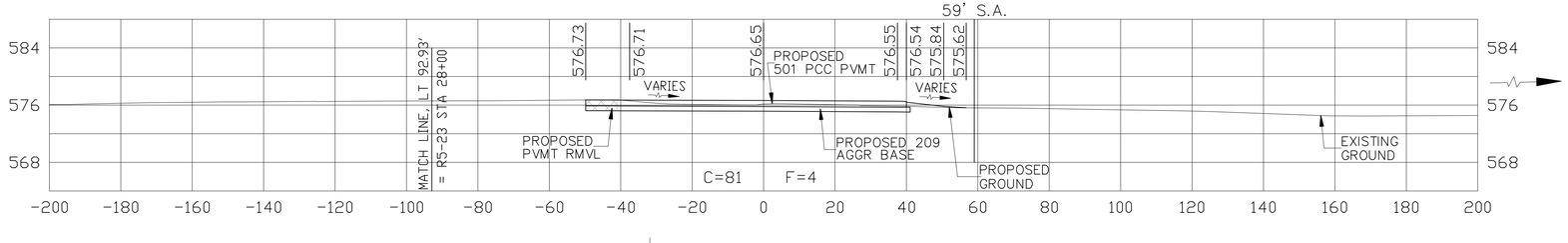
W EDGE OF G.A. APRON PVMT



772+50



772+00



NOTE:
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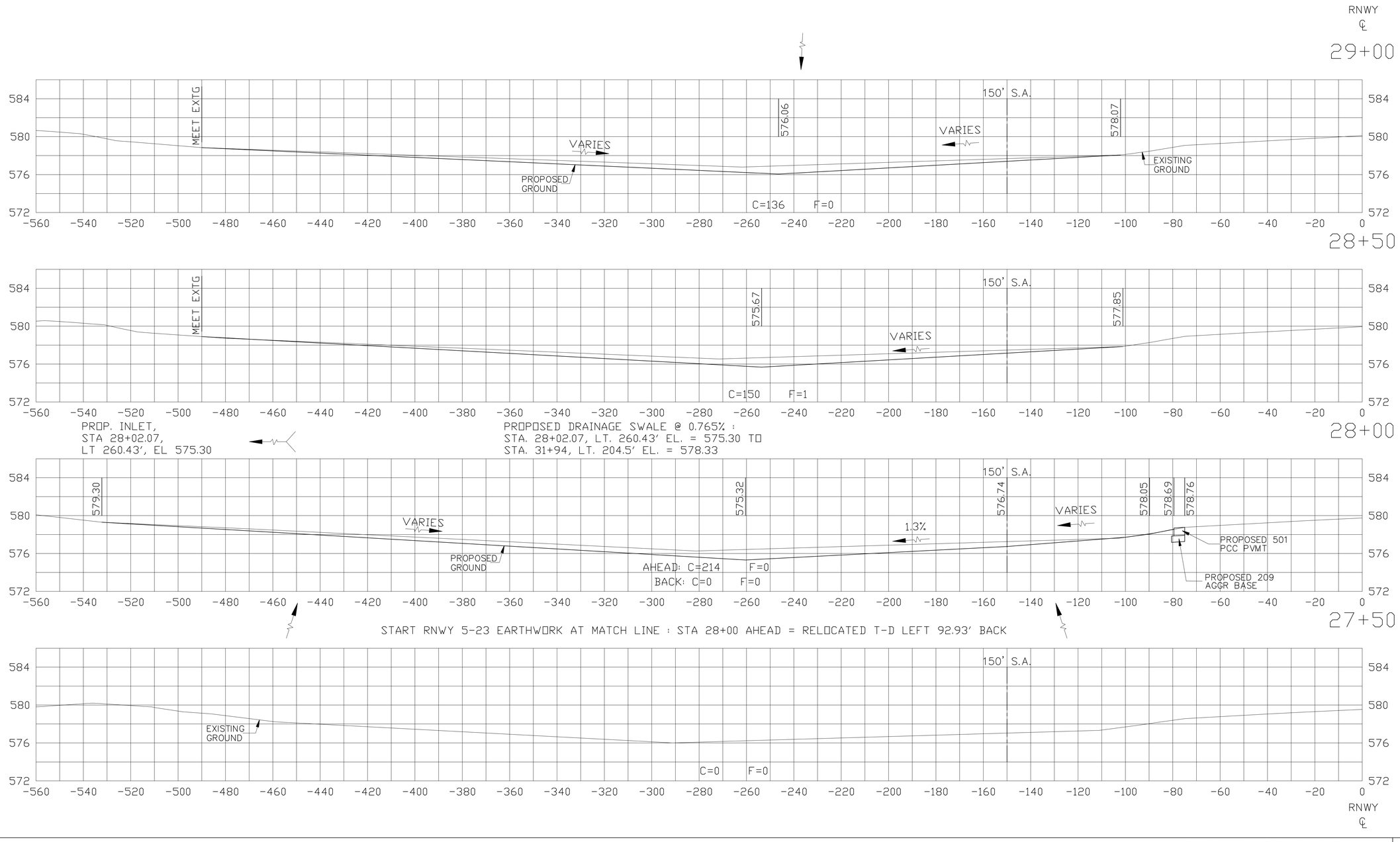

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TAXIWAY D RELOCATION
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NOTE:
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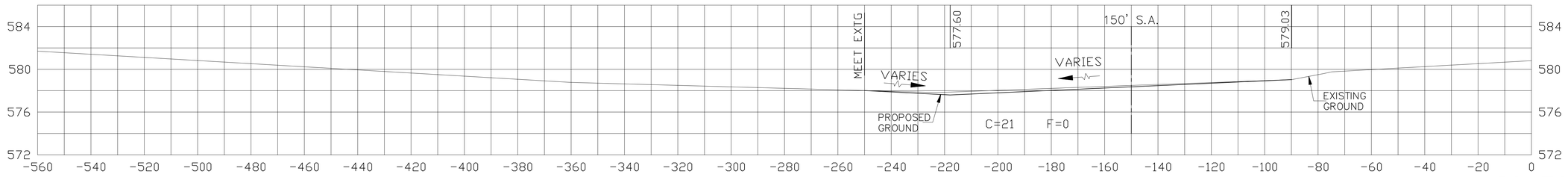
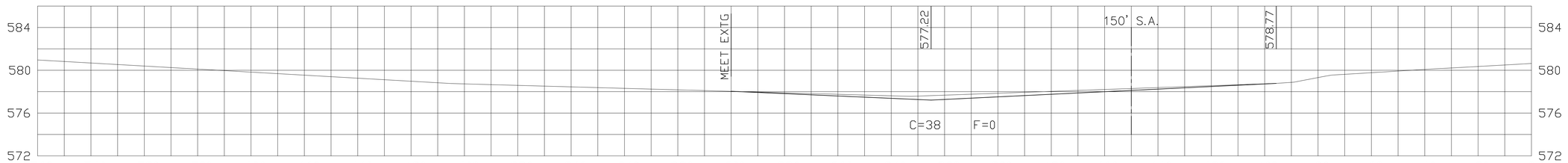
RNWY ℄

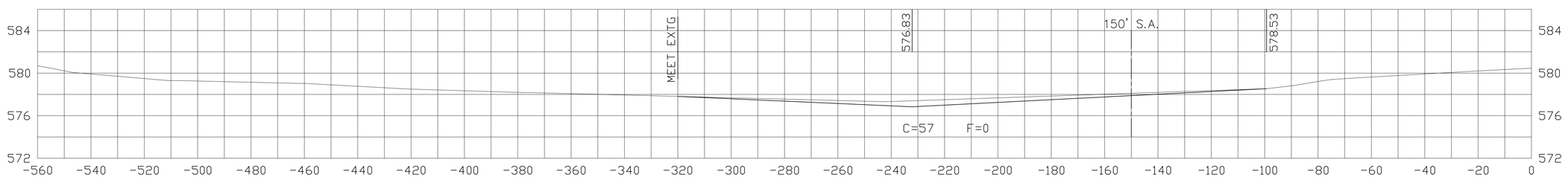
31+00



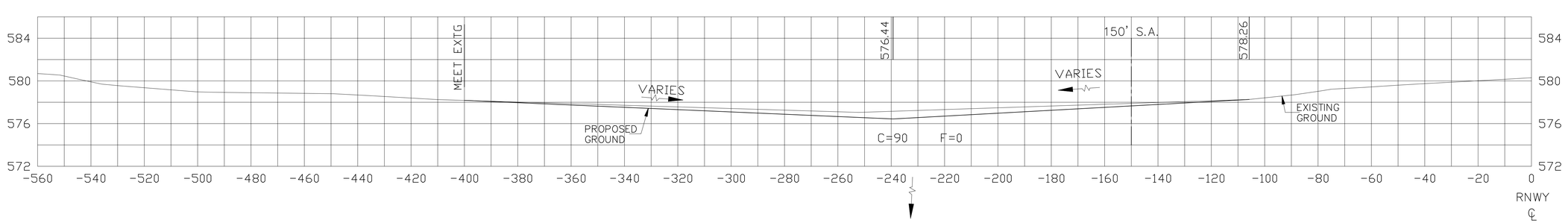
30+50



30+00



29+50



RNWY ℄

RUNWAY 5-23 LEFT, STA 29+50 TO STA 31+00 **52/69**

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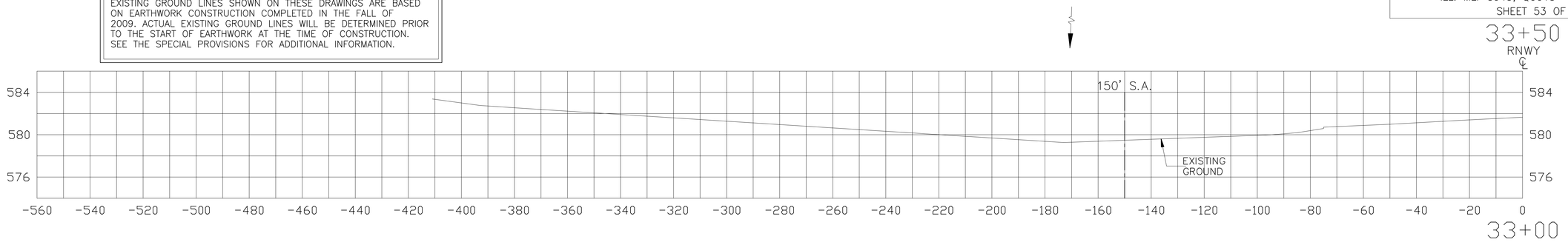
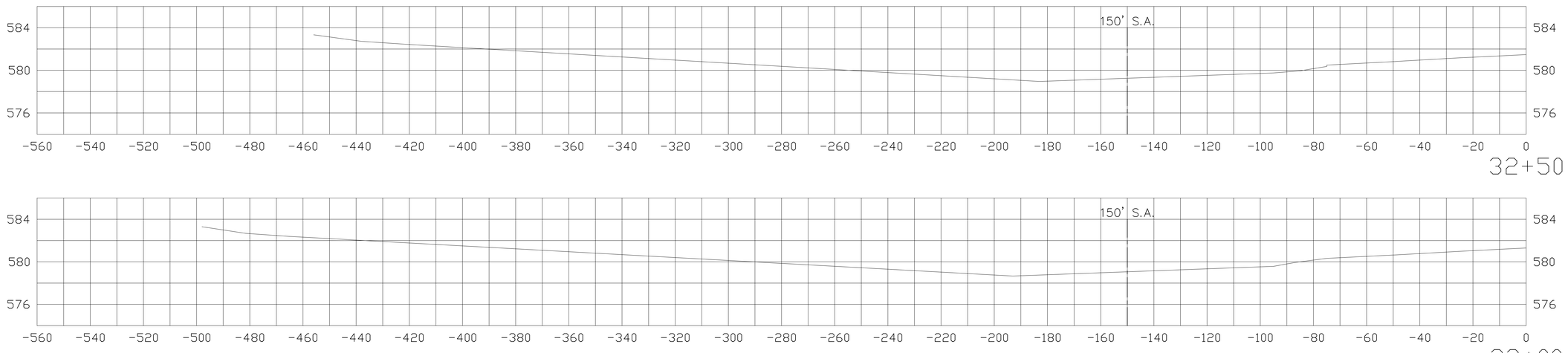
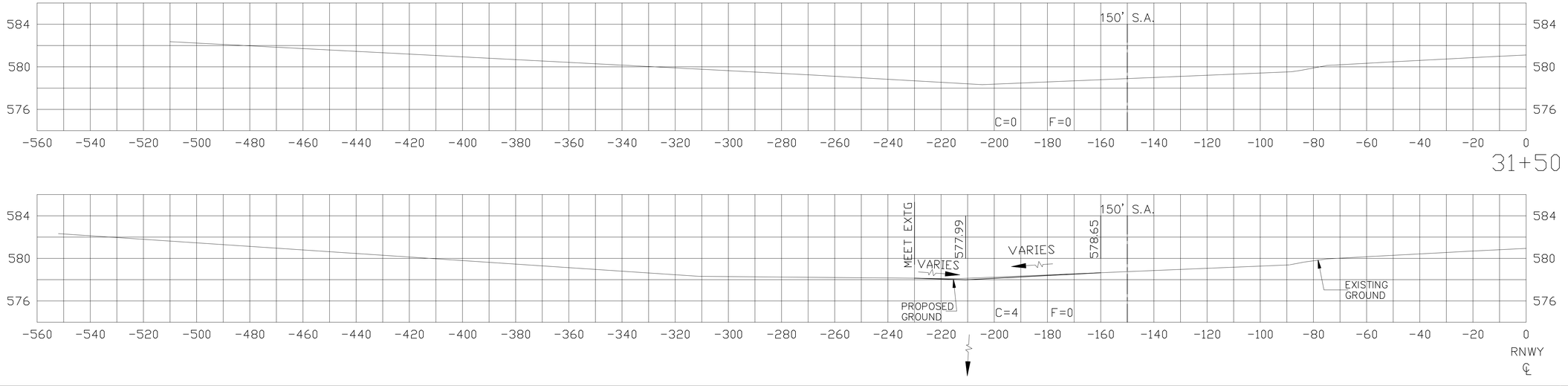
NOTE:
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EARTHWORK CONSTRUCTION COMPLETED IN THE FALL OF 2009. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.





END LEFT SIDE RNWY 5-23 EARTHWORK AT STA. 32+00

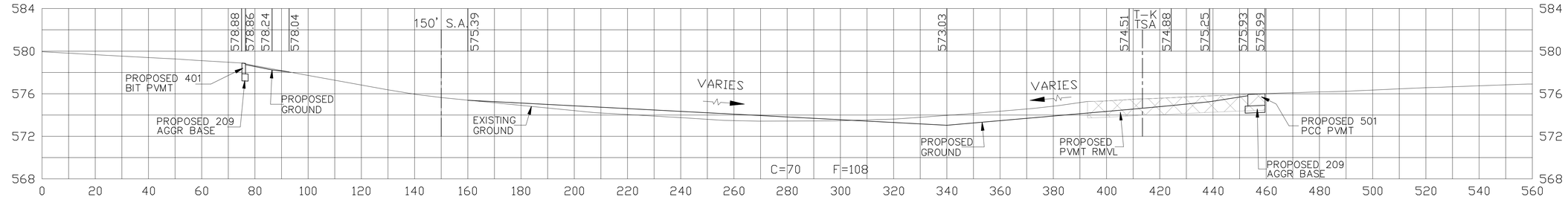


RUNWAY 5-23 LEFT, STA 31+50 TO STA 33+50 **53/69**

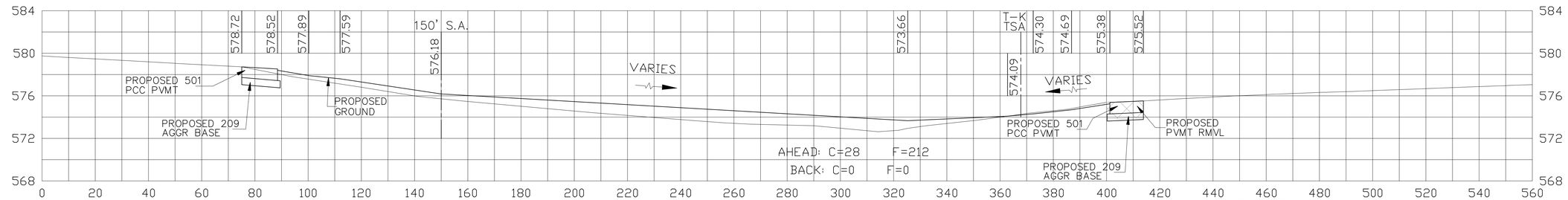
QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 54 OF 69

RNWY ℄

28+50



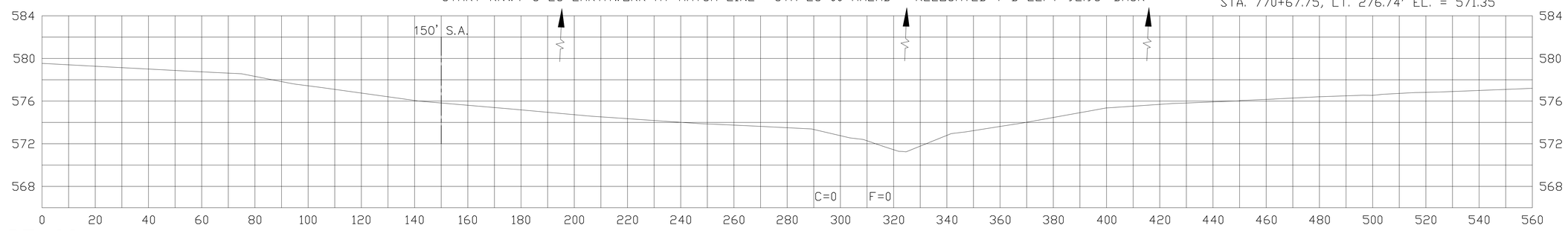
28+00



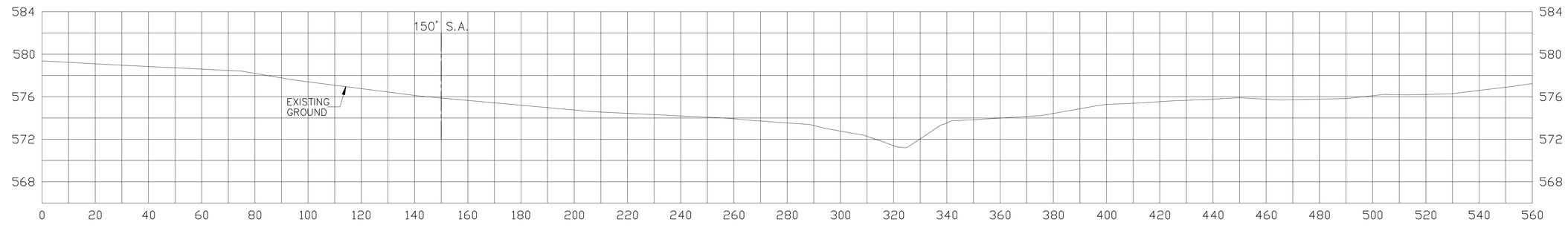
27+50

START RNWY 5-23 EARTHWORK AT MATCH LINE : STA 28+00 AHEAD = RELOCATED T-D LEFT 92.93' BACK

AHEAD: DRAINAGE SWALE @ -1.206% :
STA. 770+07, LT. 70' EL. = 573.95 TO
STA. 770+67.75, LT. 276.74' EL. = 571.35



27+00



RNWY ℄

RUNWAY 5-23 RIGHT, STA 27+00 TO STA 28+50 **54/69**

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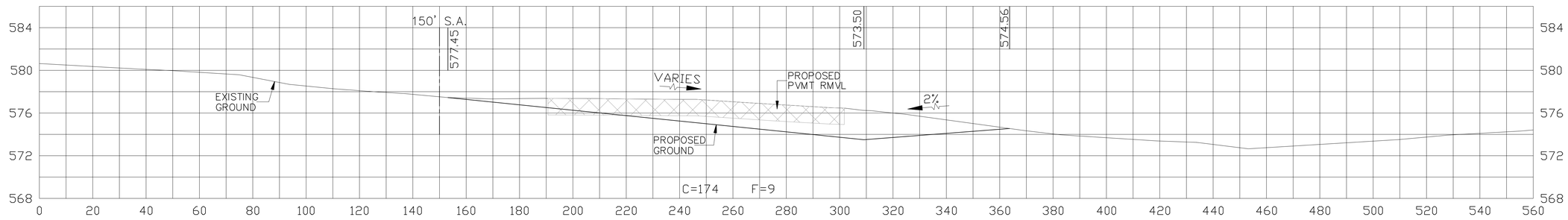
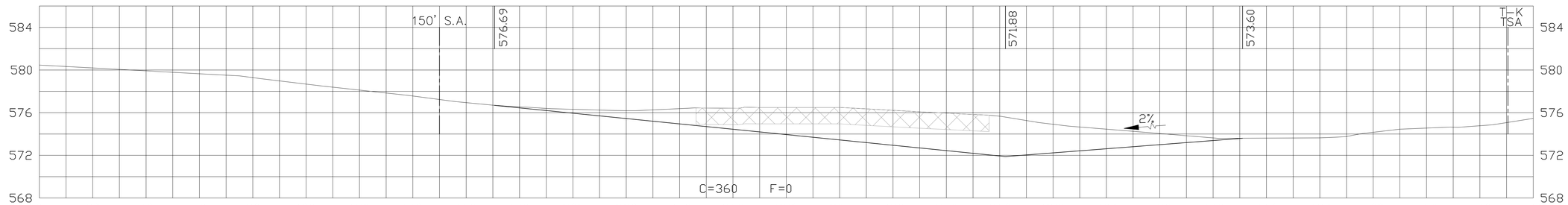
RNWY ℄

30+50



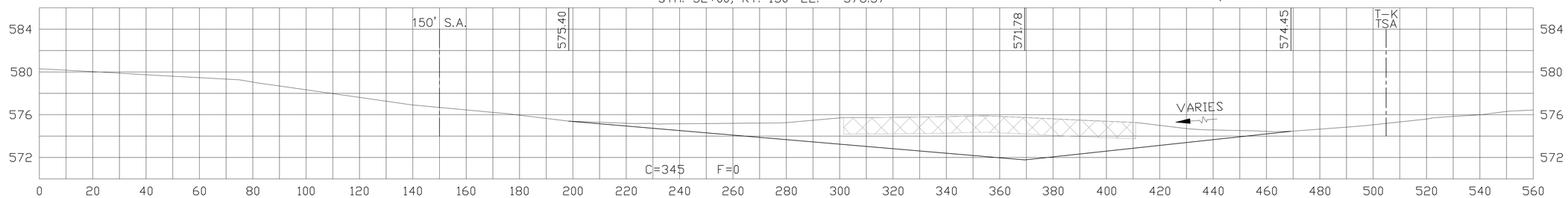
30+00



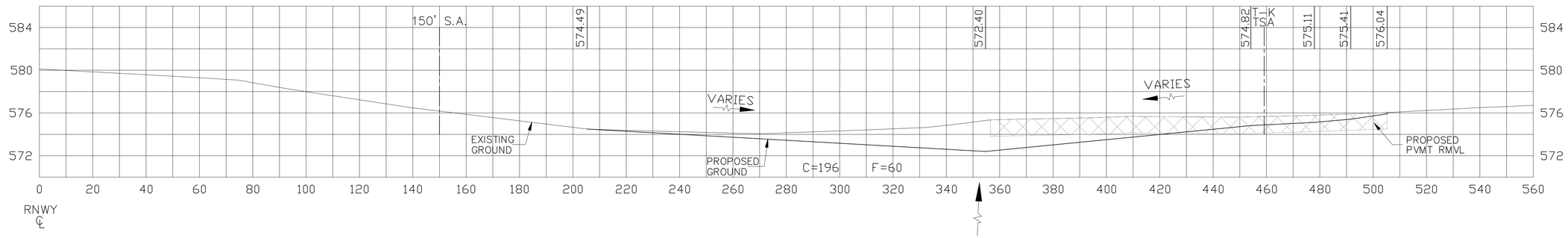
29+50

AHEAD: DRAINAGE SWALE @ 2.23% :
STA. 29+83.80, RT. 379.33' EL. = 571.35 TO
STA. 32+00, RT. 150' EL. = 578.37

PROP. INLET,
STA 29+83.80,
RT 379.33', EL 571.35



29+00



RNWY ℄

RUNWAY 5-23 RIGHT, STA 29+00 TO STA 30+50

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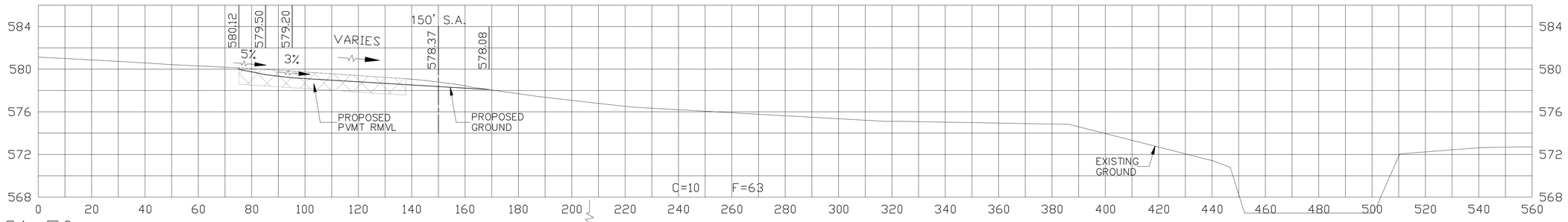
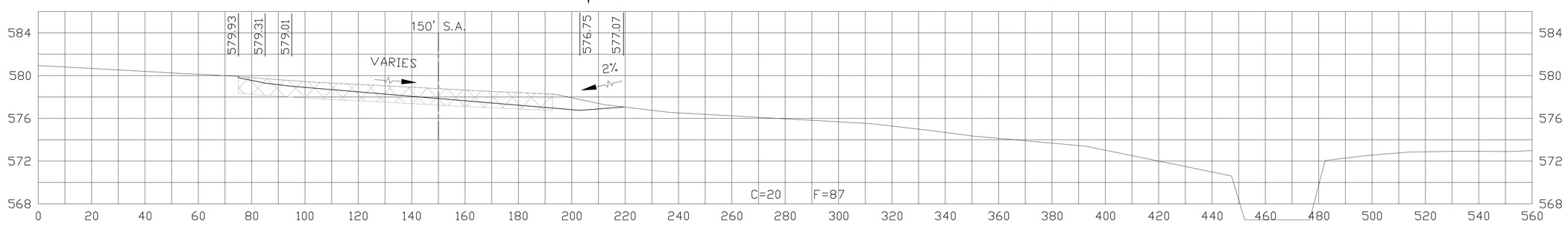
RNWY ℄

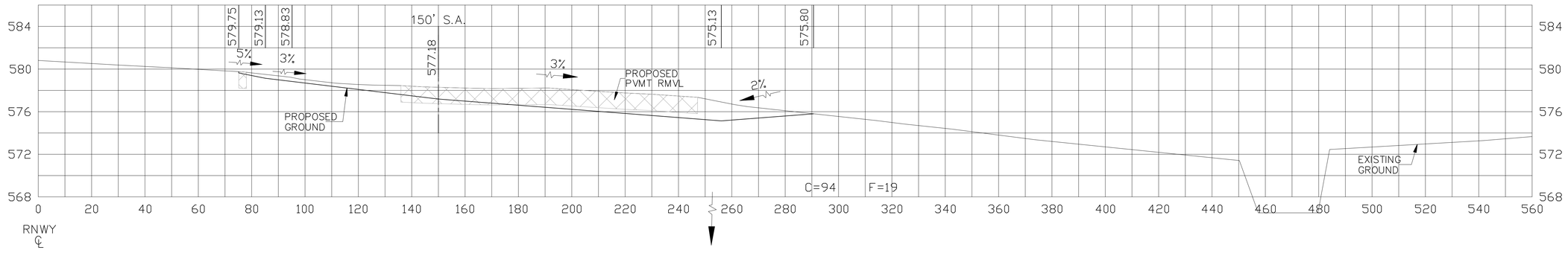
32+00



31+50



31+00



RNWY ℄

RUNWAY 5-23 RIGHT, STA 31+00 TO STA 32+00

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TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
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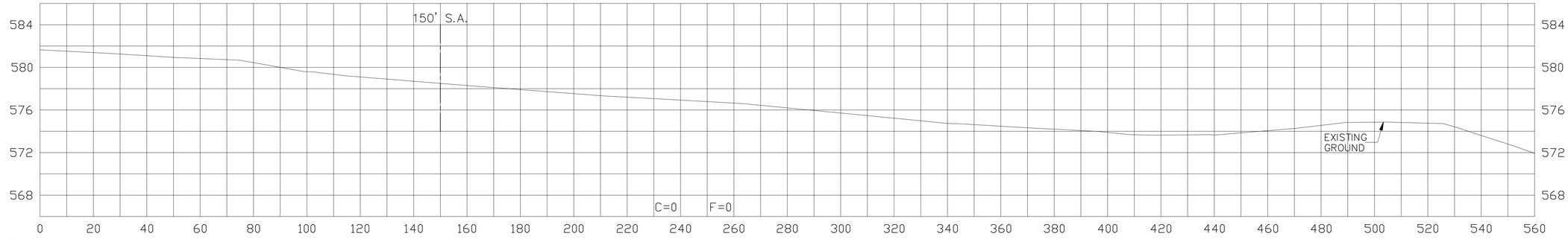
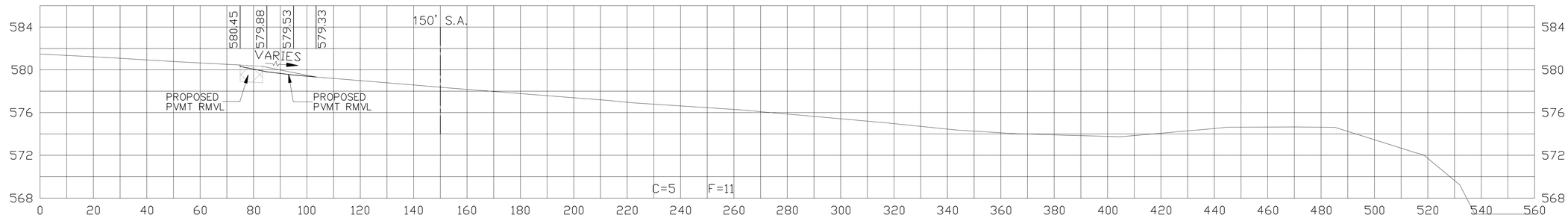
RNWY
℄

END RIGHT SIDE RNWY 5-23 EARTHWORK AT STA. 33+50

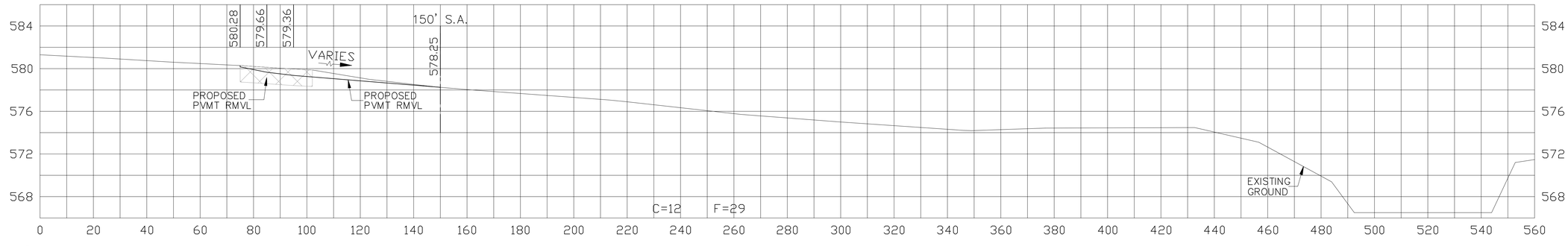
33+50



33+00



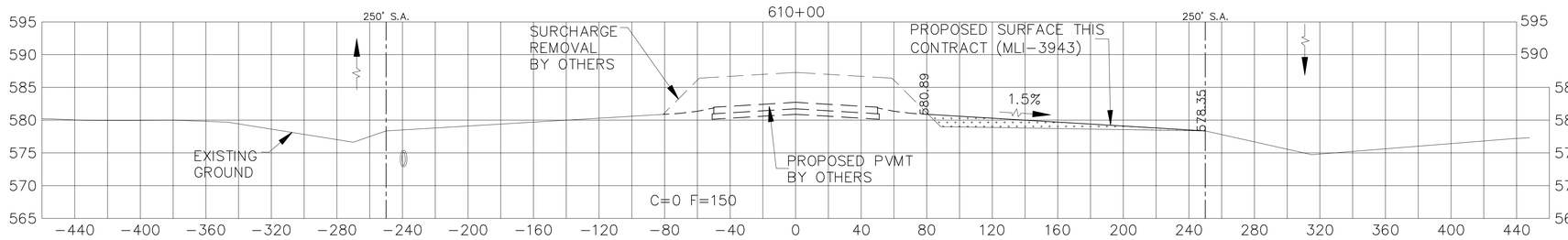
32+50



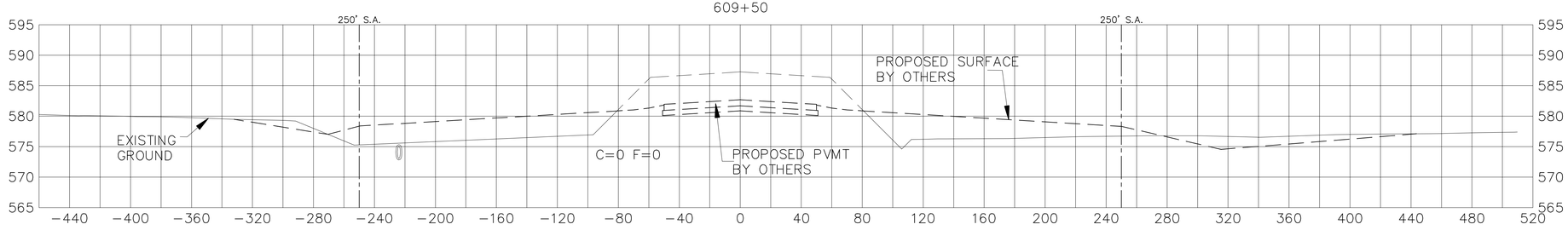
RNWY
℄

RUNWAY 5-23 RIGHT, STA 32+50 TO STA 33+50

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 58 OF 69



PROPOSED MLI-3943 AR152410-
UNCLASSIFIED EMBANKMENT
(THIS CONTRACT)

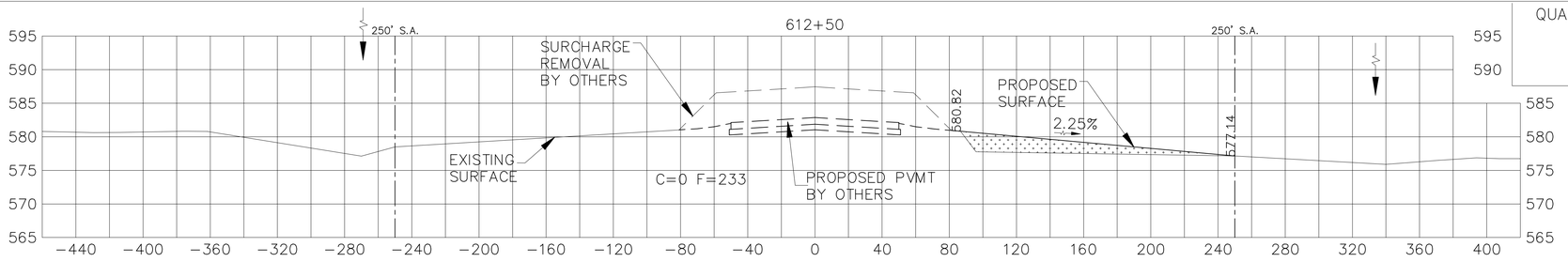


START MLI-3943 EARTHWORK AT
STA 609+50, MEET PROPOSED
EARTHWORK BY OTHERS

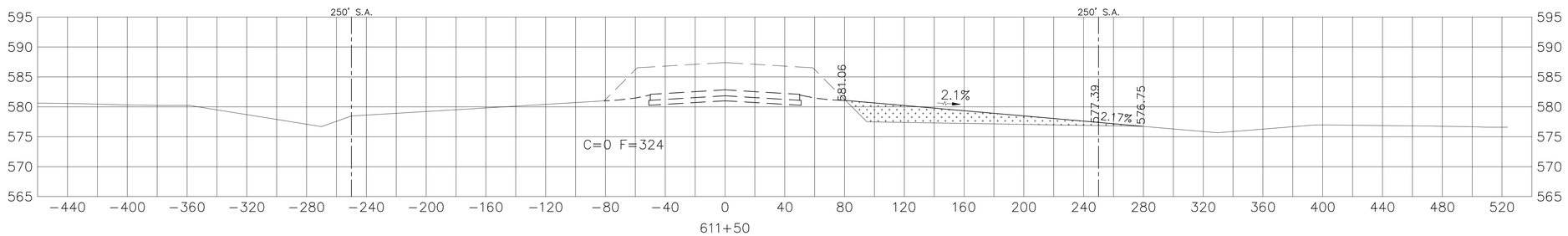
SCALE
V: 1" = 10'
H: 1" = 40'

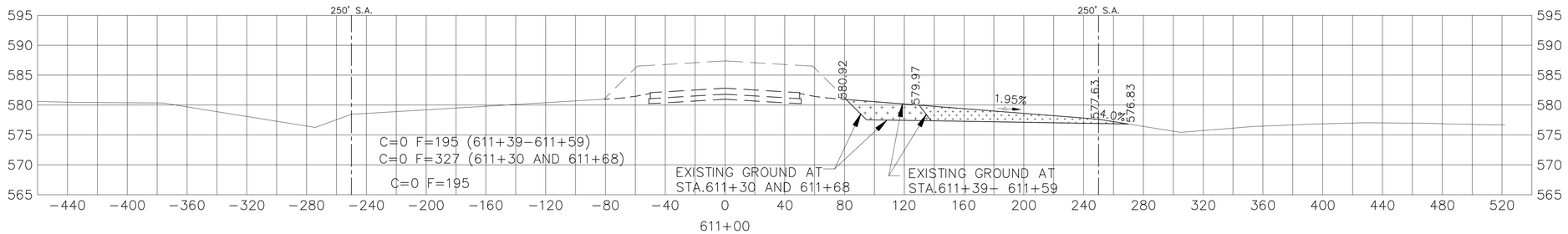
TAXIWAY P, STA 608+00 TO STA 610+00 **58/69**

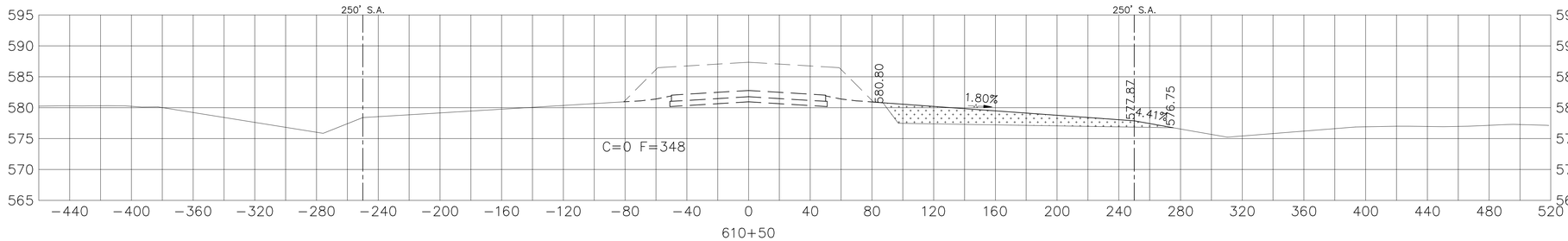
QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 59 OF 69

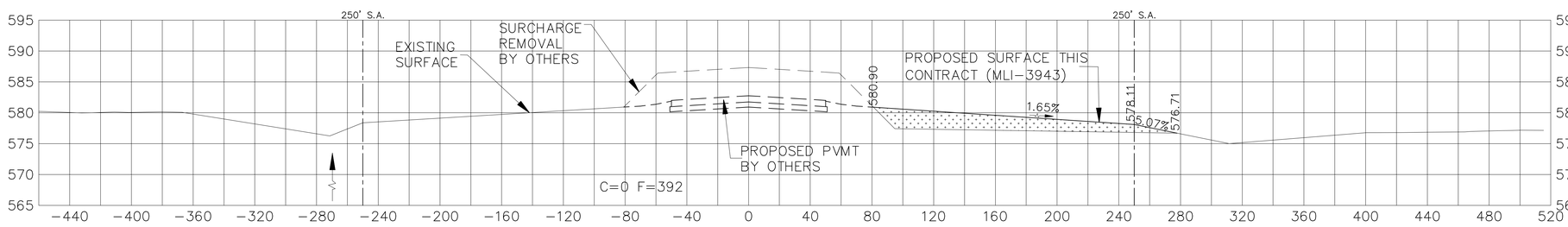


PROPOSED MLI-3943 AR152410-
UNCLASSIFIED EMBANKMENT
(THIS CONTRACT)









SCALE
V: 1" = 10'
H: 1" = 40'

TAXIWAY P, STA 610+50 TO STA 612+50

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
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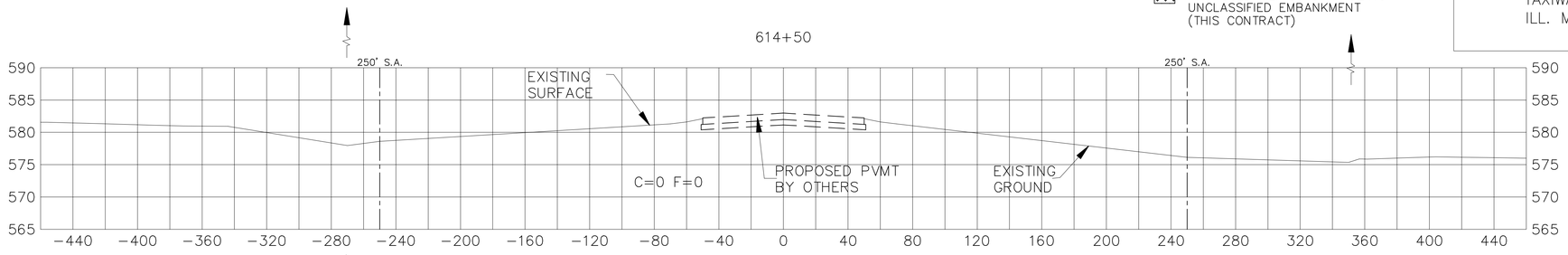
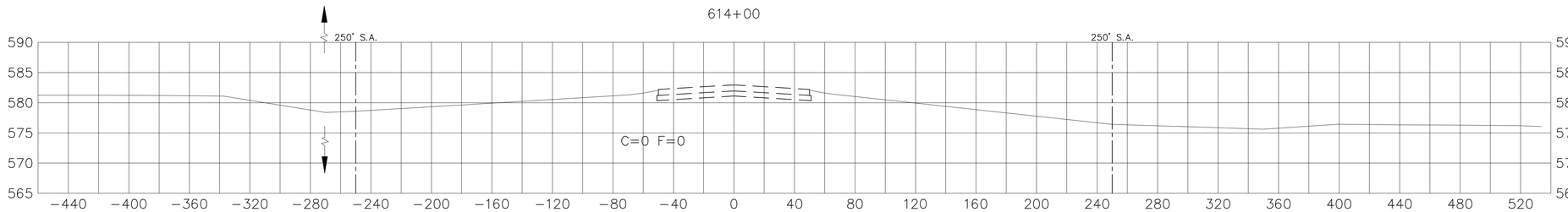
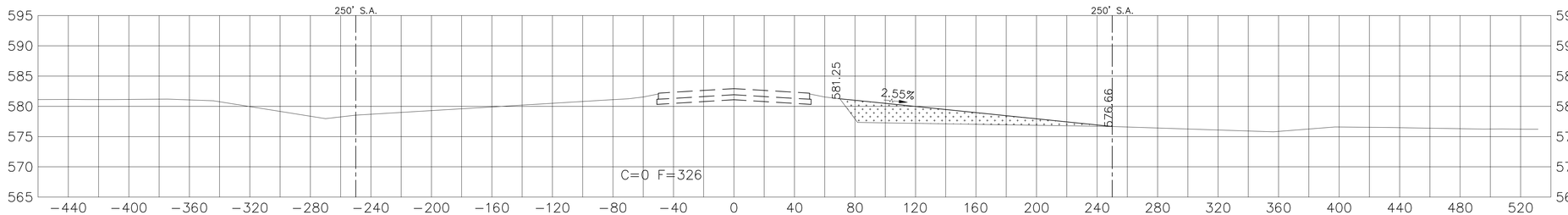
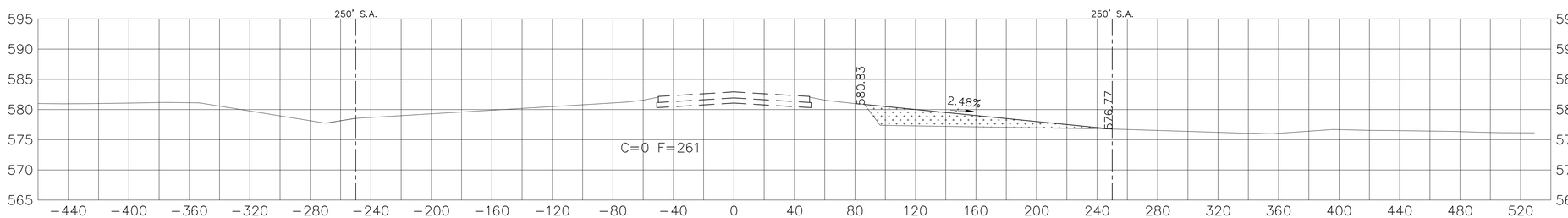
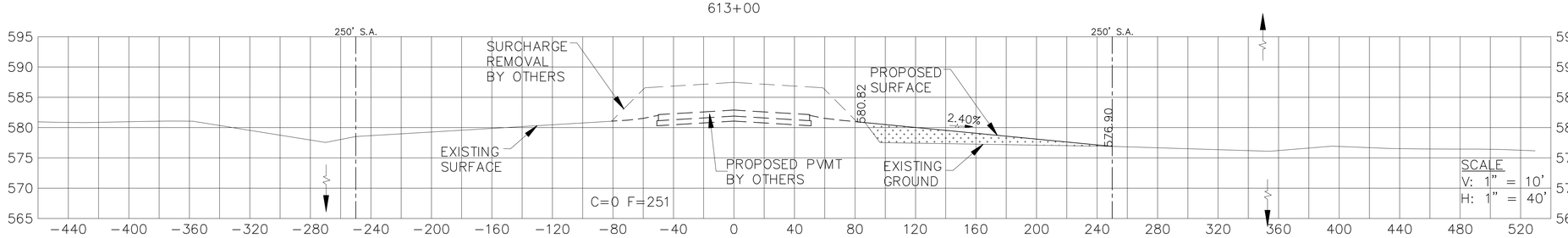
PROPOSED MLI-3943 AR152410-
UNCLASSIFIED EMBANKMENT
(THIS CONTRACT)











QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
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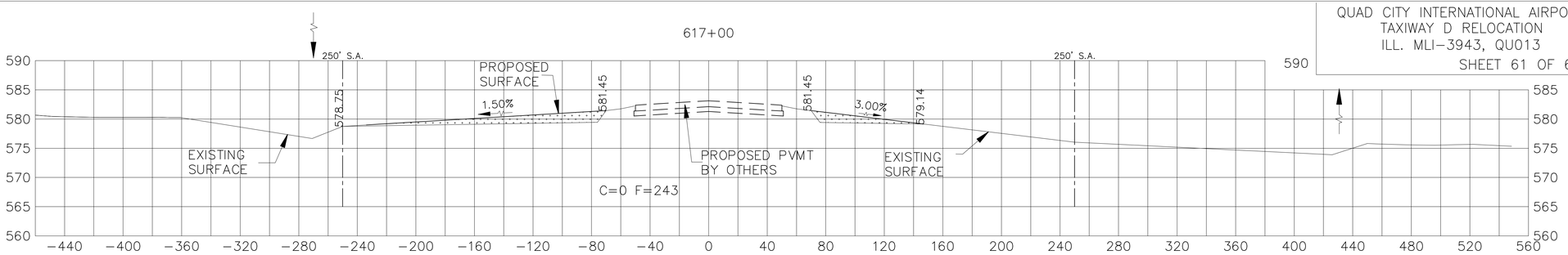
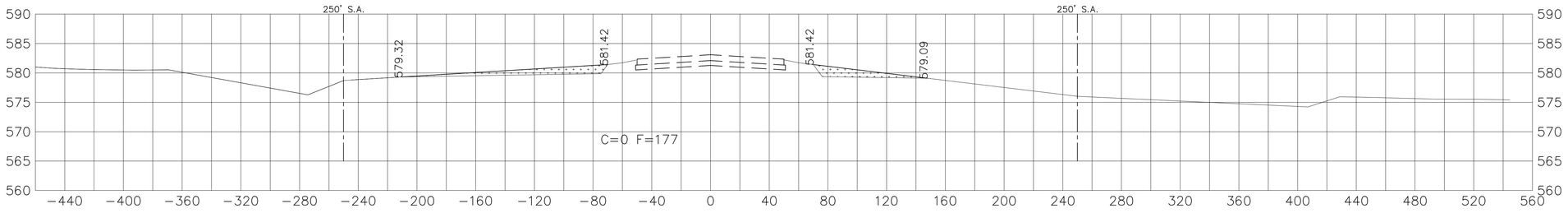
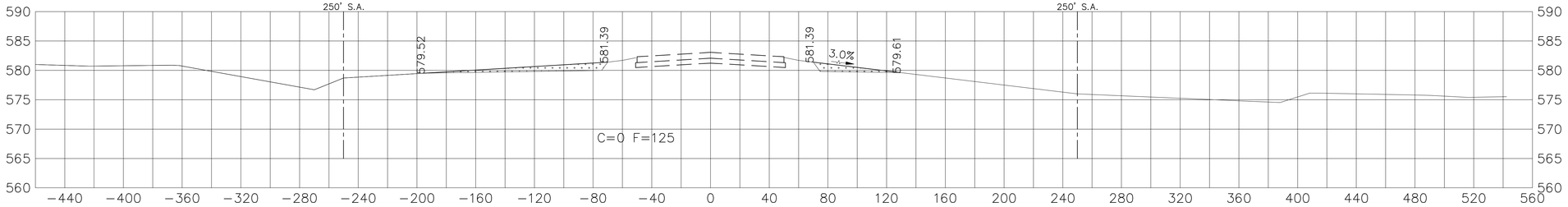
617+00



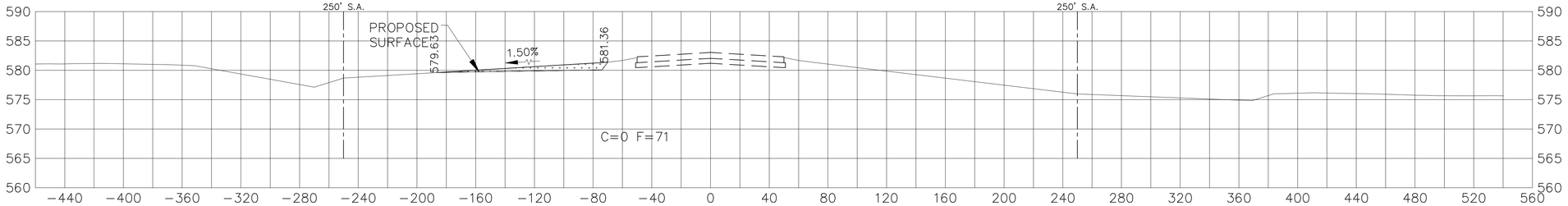
616+50



616+00

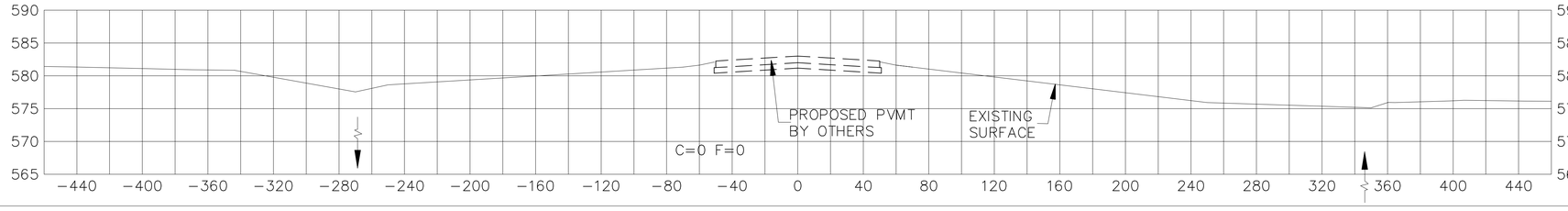


615+50



PROPOSED MLI-3943 AR152410-
UNCLASSIFIED EMBANKMENT
(THIS CONTRACT)

615+00



SCALE
V: 1" = 10'
H: 1" = 40'

TAXIWAY P, STA 615+00 TO STA 617+00

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
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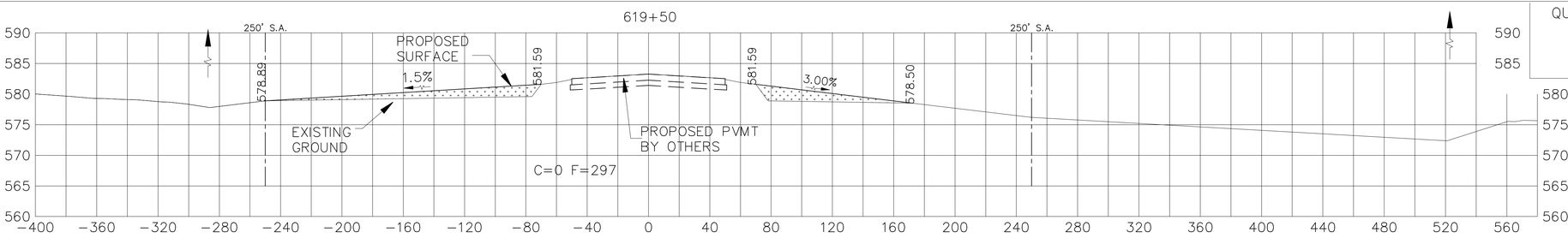
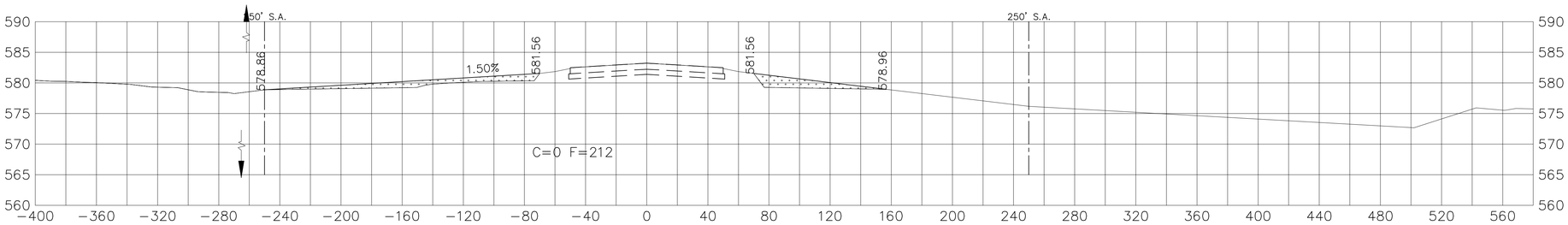
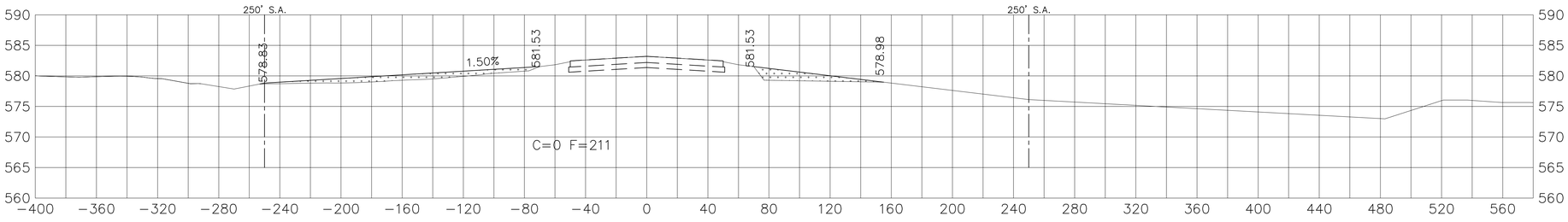
619+50



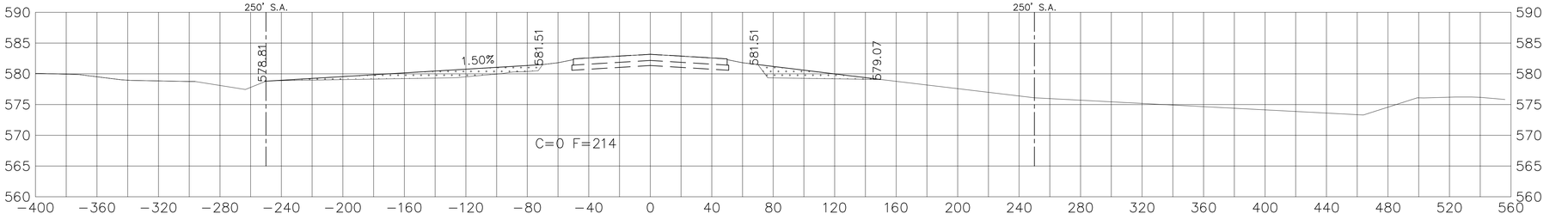
619+00



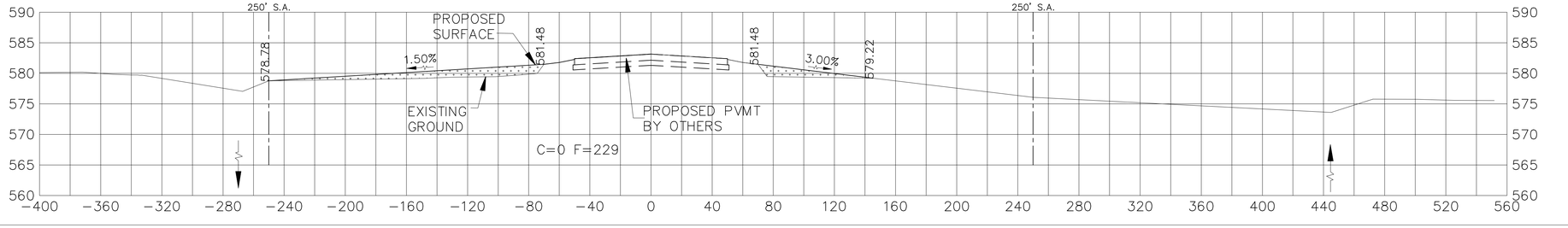
618+50



618+00



617+50



SCALE
V: 1" = 10'
H: 1" = 40'

PROPOSED MLI-3943 AR152410-
UNCLASSIFIED EMBANKMENT
(THIS CONTRACT)

TAXIWAY P, STA 617+50 TO STA 619+50 **62/69**

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TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
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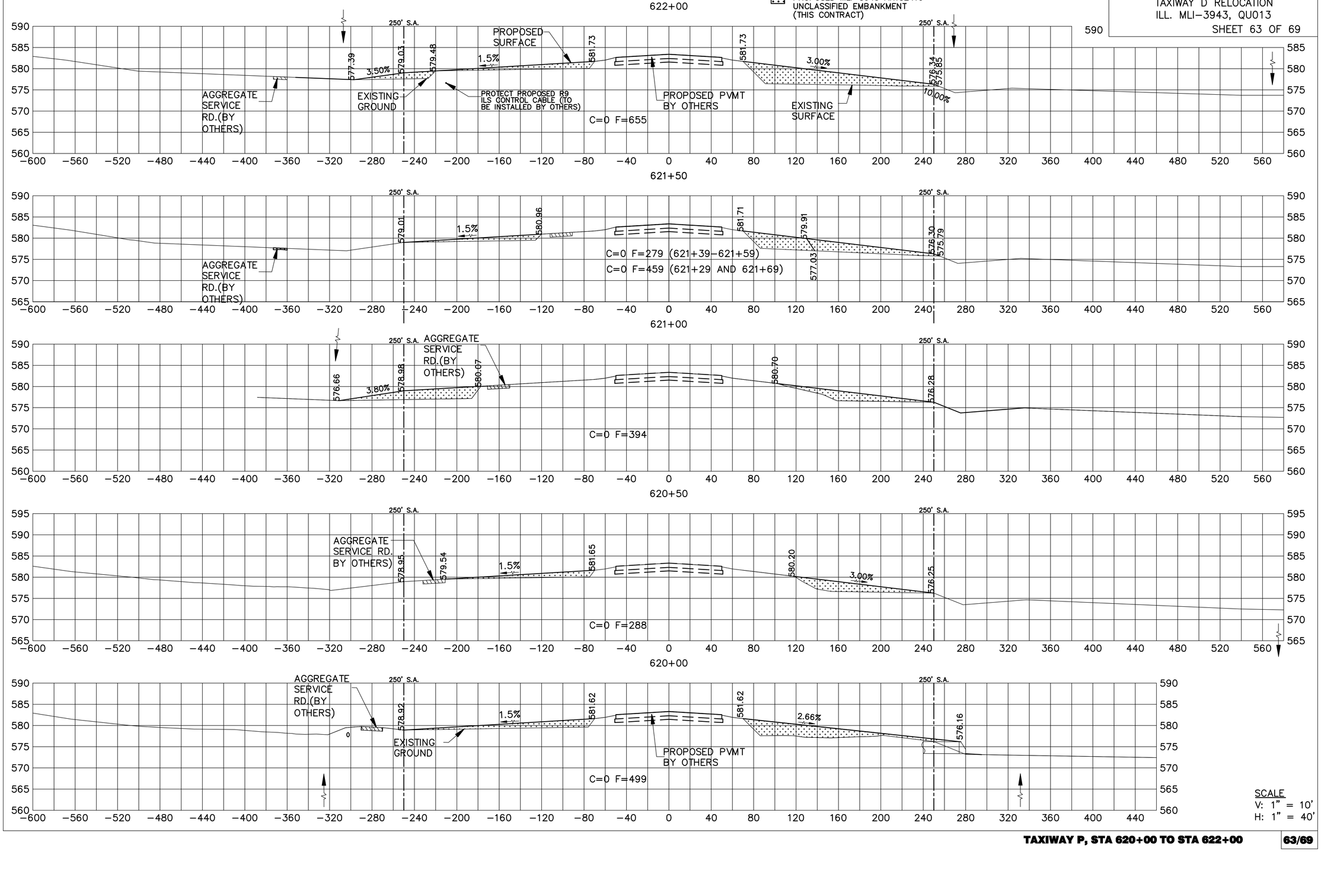
PROPOSED MLI-3943 AR152410-
UNCLASSIFIED EMBANKMENT
(THIS CONTRACT)

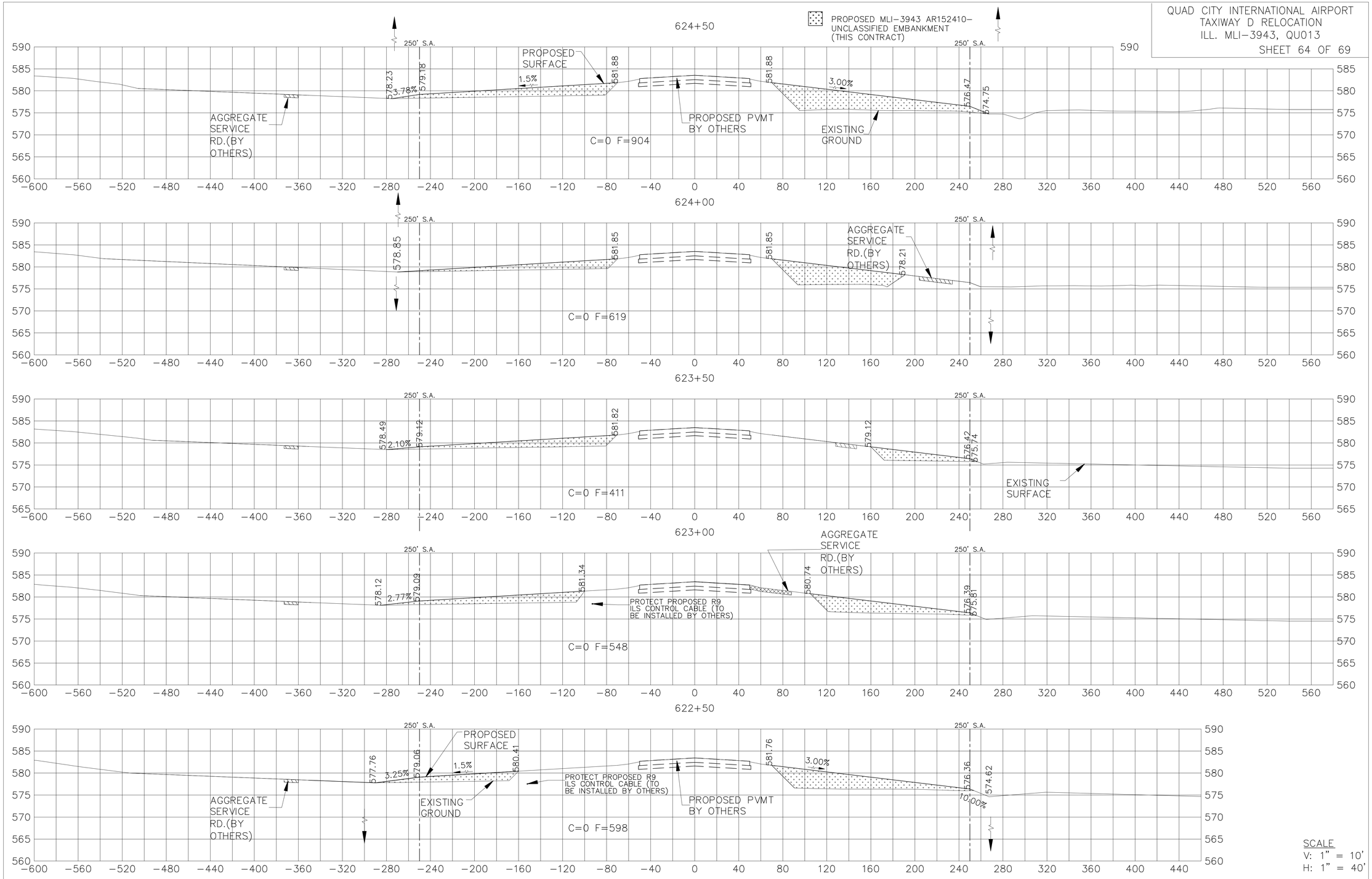


SCALE
V: 1" = 10'
H: 1" = 40'

TAXIWAY P, STA 620+00 TO STA 622+00

63/69

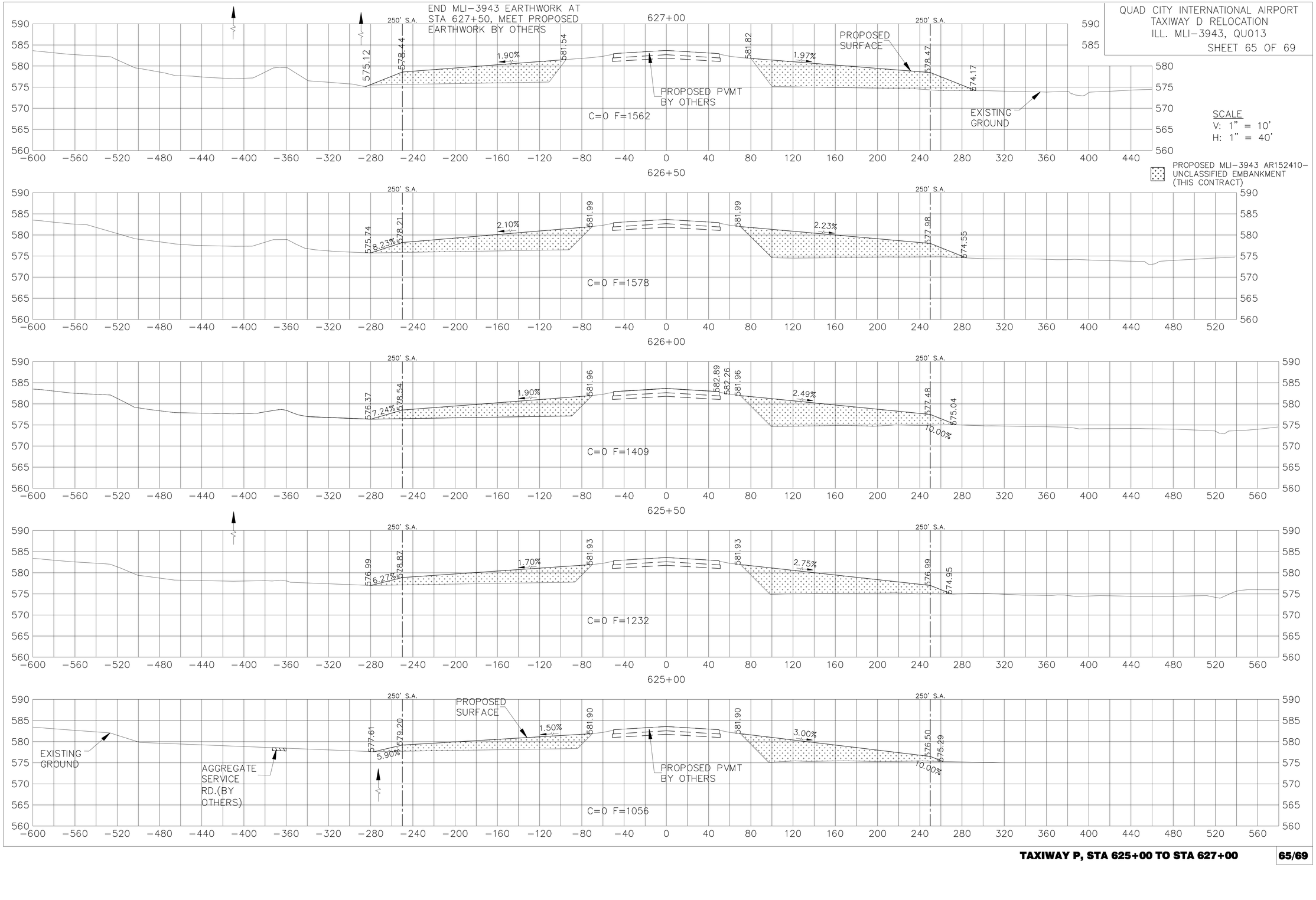
QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 64 OF 69



SCALE
V: 1" = 10'
H: 1" = 40'

TAXIWAY P, STA 622+50 TO STA 624+50 64/69

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI-3943, QU013
SHEET 65 OF 69



SCALE
V: 1" = 10'
H: 1" = 40'

PROPOSED MLI-3943 AR152410-UNCLASSIFIED EMBANKMENT (THIS CONTRACT)

TAXIWAY P, STA 625+00 TO STA 627+00

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY D RELOCATION
ILL. MLI–3943, QU013
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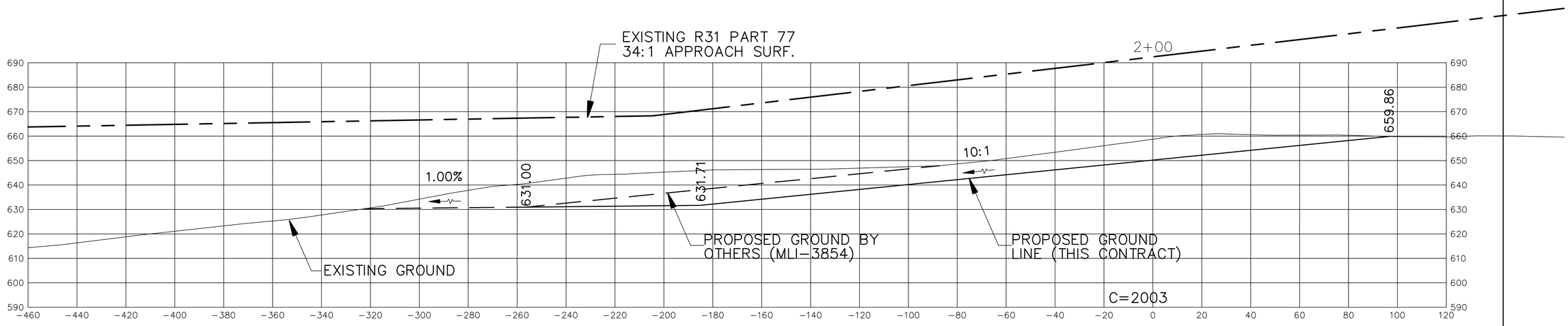
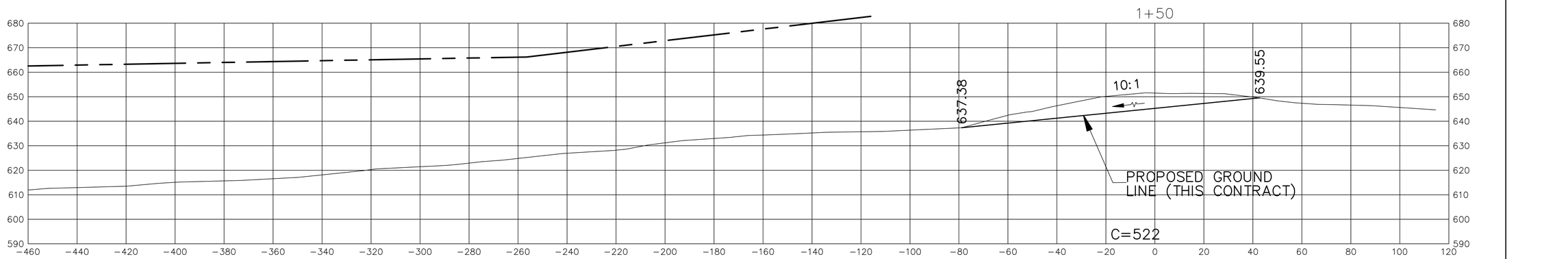
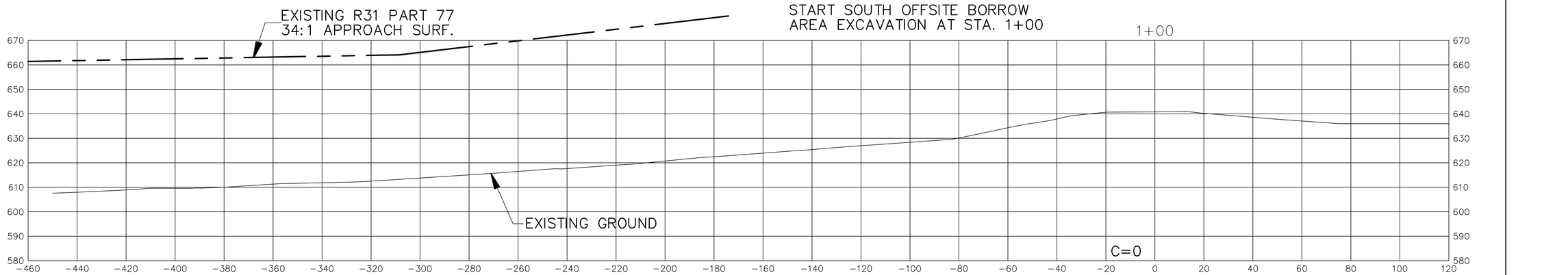
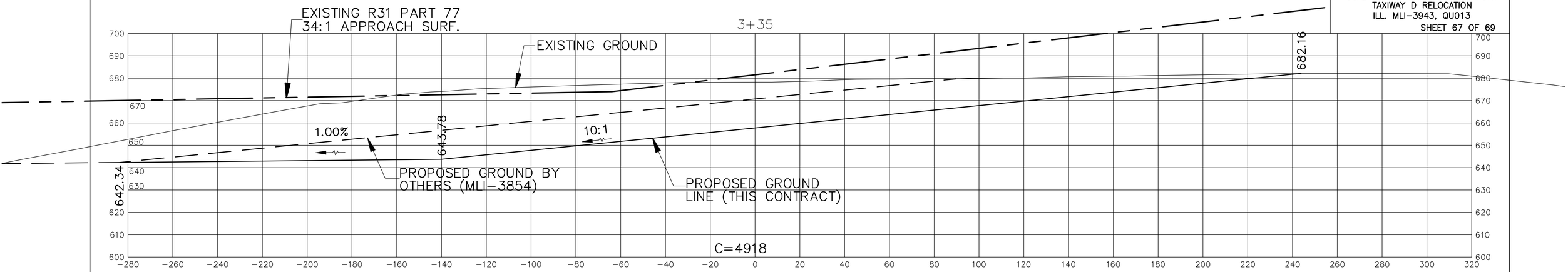
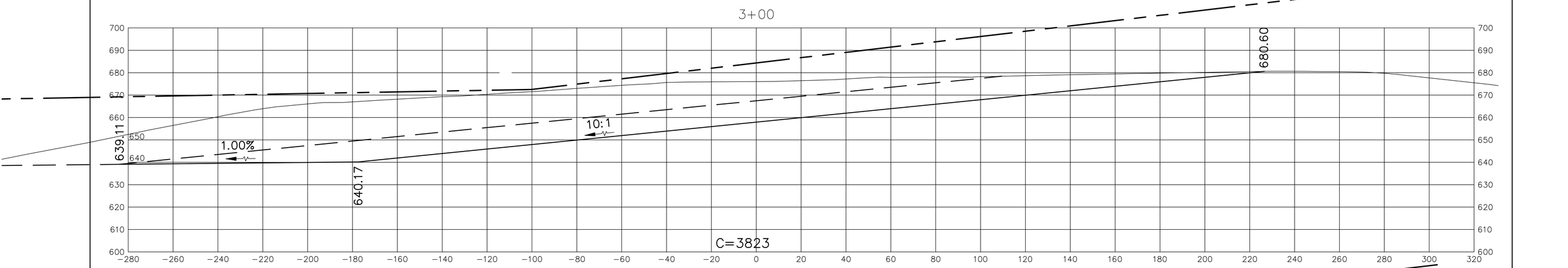


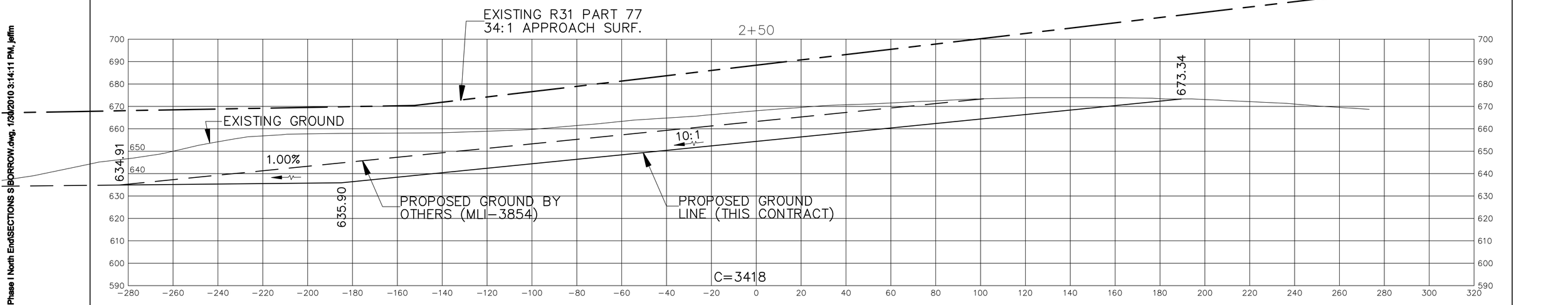


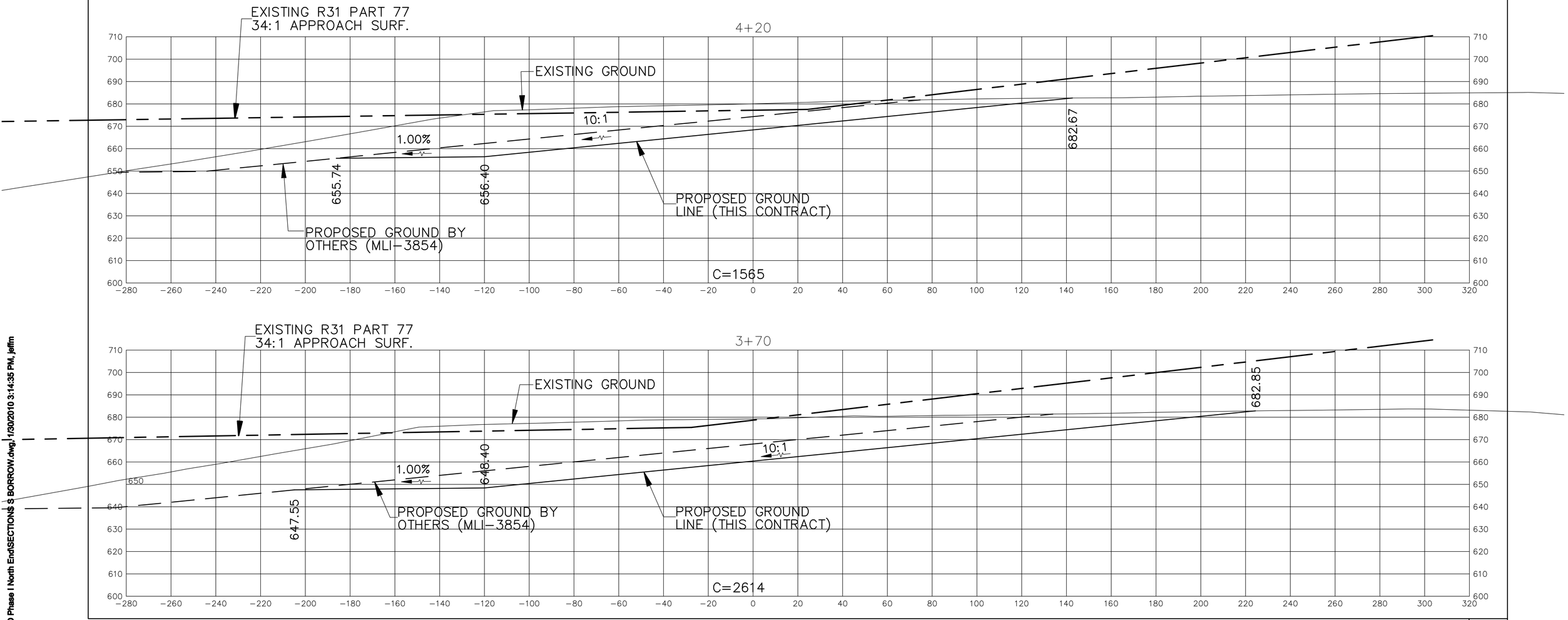


SOUTH OFFSITE BORROW EXCAVATION AREA, STA 0+00 TO STA 2+00 66/69









SOUTH OFFSITE BORROW EXCAVATION AREA, STA 3+70 TO STA 4+20 68/69

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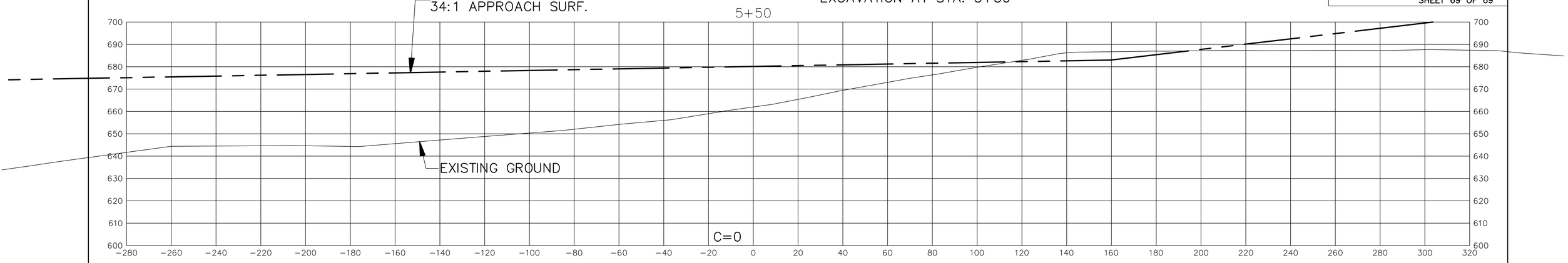
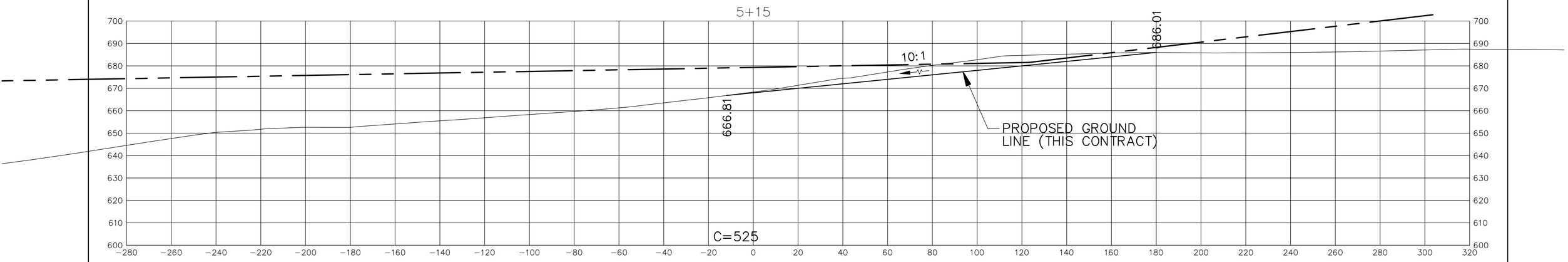
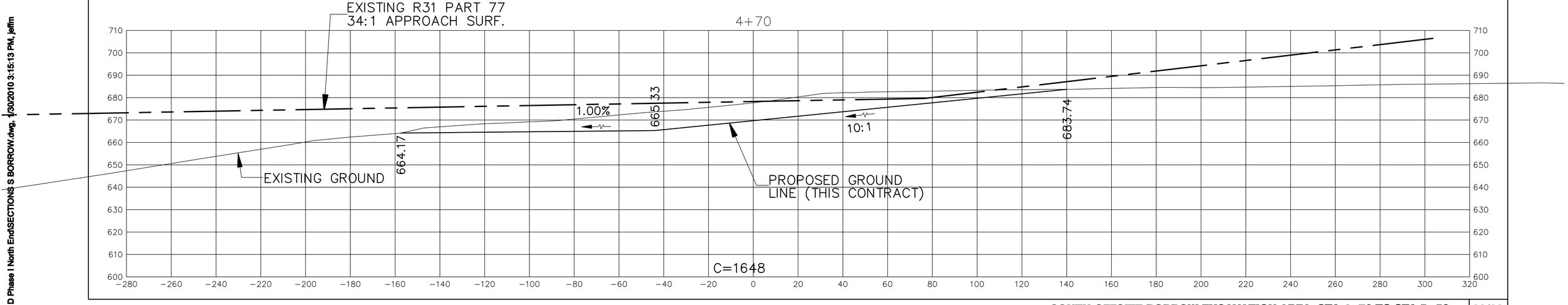
END SOUTH OFFSITE BORROW AREA EXCAVATION AT STA. 5+50







SOUTH OFFSITE BORROW EXCAVATION AREA, STA 4+70 TO STA 5+50 **69/69**