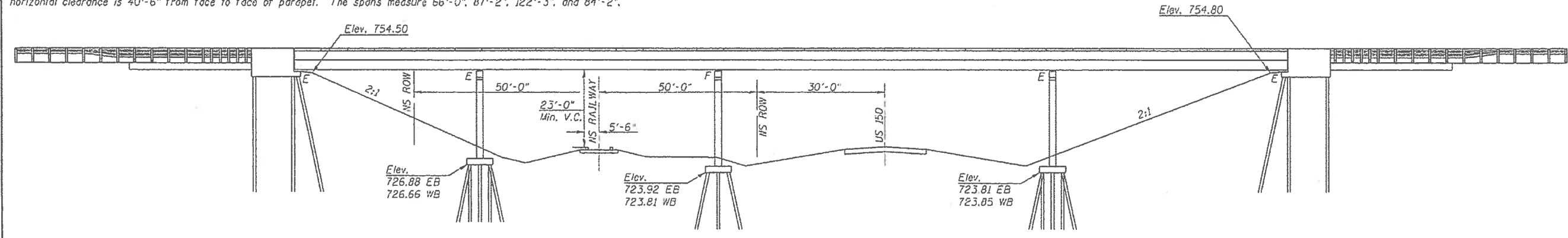


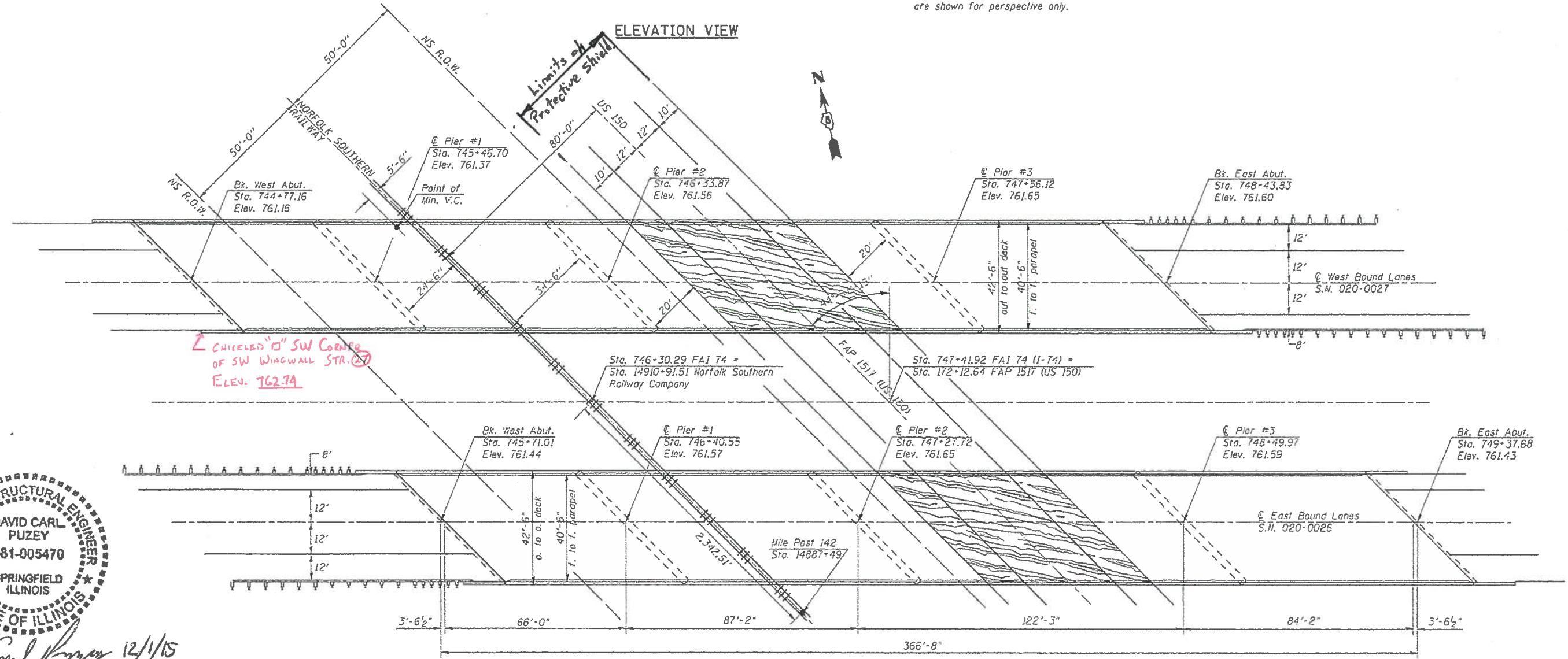
The existing structures were built in 1970 as Route FAI-74, Section 20-1HVB at Sta. 746+30.29 in Dewitt County. The structures carry I-74 over US 150 and Norfolk Southern Railway northwest of Farmer City. In 1975 a waterproofing membrane and HMA wearing surface was placed under Section D5 Bridge Deck Waterproofing 1975-3, Contract 29686. In 2004 the HMA wearing surface was removed and replaced with Section 7413RS-2, 4-1-RS-1, Contract 70027.

The superstructure consists of seven 44" steel plate girders supporting a 7 1/2" R.C. deck and 1 3/4" HMA wearing surface with waterproofing membrane system. The substructure consists of concrete pile supported stub abutments and timber pile supported piers. The structures measure 366'-8" bk. to bk. abutments and 42'-6" out to out of deck. The horizontal clearance is 40'-6" from face to face of parapet. The spans measure 66'-0", 87'-2", 122'-3", and 84'-2".

NOTE:
See Next Sheet for the Proposed Work, Bill of Material, and General Notes.



Elevations taken from as-built plans and are shown for perspective only.



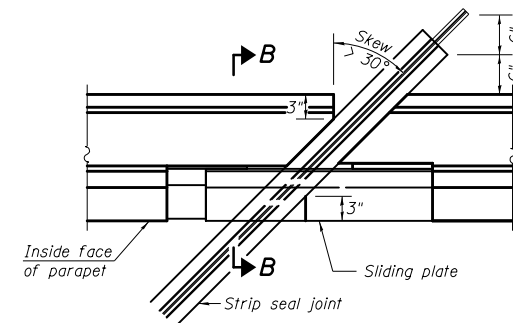
CHICKENED SW CORNER OF SW WINGWALL STR. ELEV. 762.74



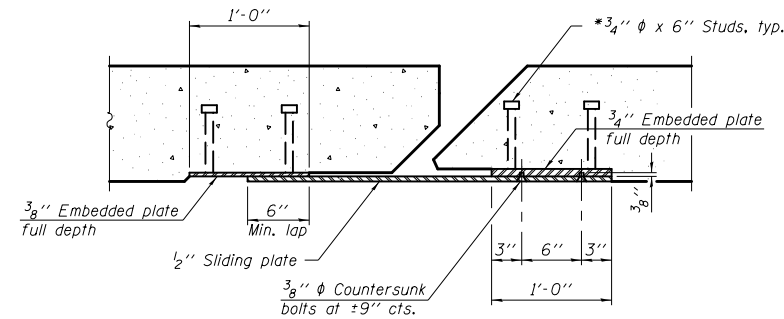
David Carl Puzey 12/1/15
Expires 11/30/16

FILE NAME =	USER NAME = brendenburgtj	DESIGNED RTC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN AND ELEVATION			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PROJECT =	PROJECT =	PROJECT =	PROJECT =		74	-	DEWITT	166	13			
PLOT SCALE = 1/8" = 1'-0"	CHECKED TJB	REVISED - TJB	REVISED - TJB		S.N. 020-0026 (EB) & S.N. 020-0027 (WB)			CONTRACT NO. 90876				
PLOT DATE = 10/13/2015	DATE 11/29/2011	REVISED - 8/31/2015	REVISED - 8/31/2015		SCALE:	SHEET 1	OF 26 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

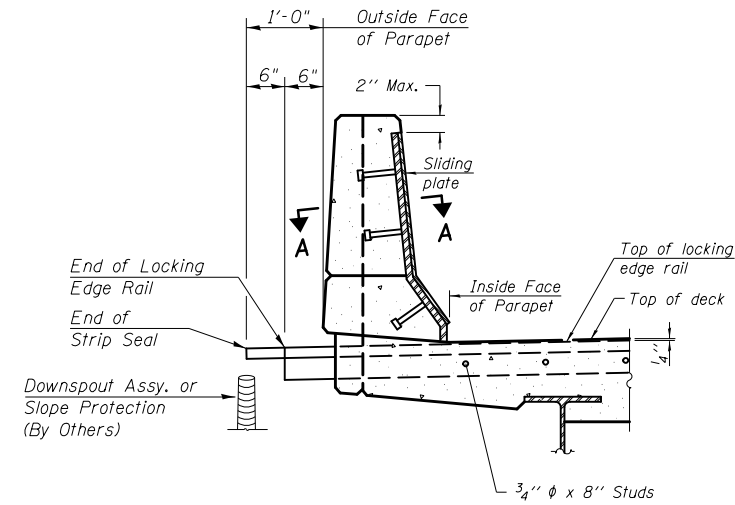
PREFORMED JOINT STRIP SEAL DETAILS S.N. 020-0026 (EB) & S.N. 020-0027 (WB)



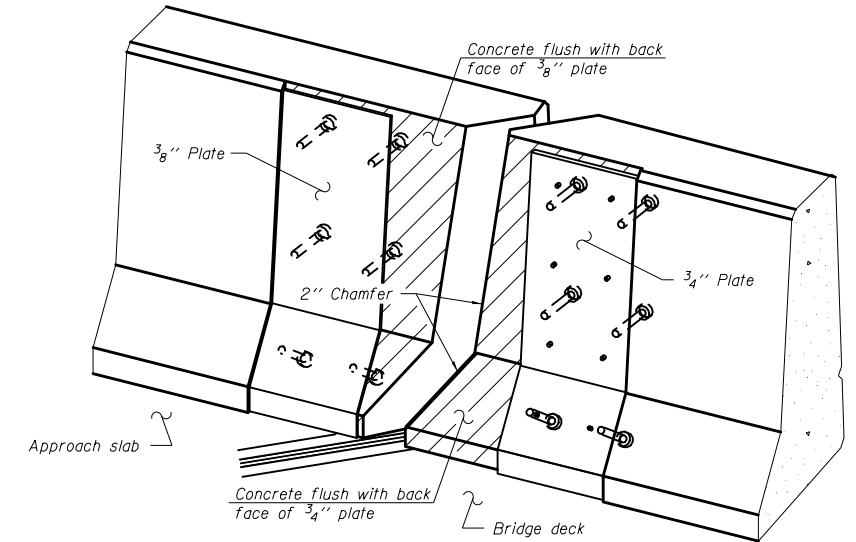
PLAN
(For skews > 30°)
Showing point block



SECTION A-A

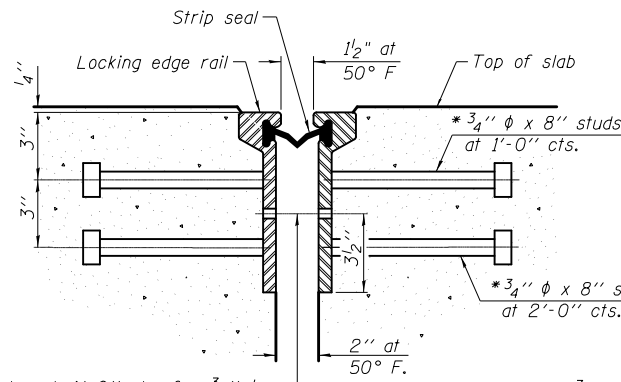


SECTION B-B

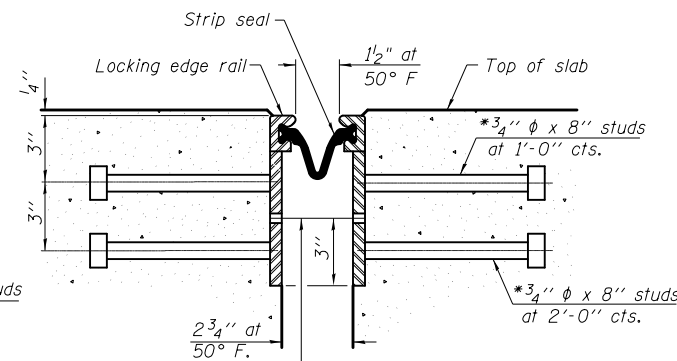


TRIMETRIC VIEW
(Showing back plates only)

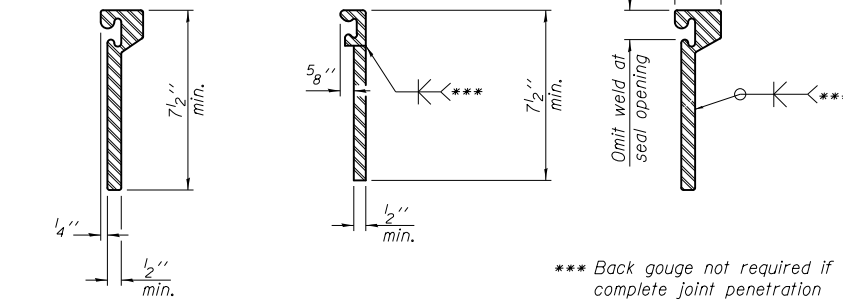
SLIDING PLATE IS NOT REQUIRED SINCE EXISTING PARAPET DOES NOT EXTEND ALONG APPROACH SLAB;
I.E. PROPOSED PARAPET AND WINGWALL DIMENSIONS MATCH EXISTING CONDITIONS



**SECTION THRU
ROLLED RAIL JOINT**



**SECTION THRU
WELDED RAIL JOINT**



**ROLLED
EXTRUDED RAIL WELDED RAIL**

**LOCKING EDGE
RAIL SPLICE**

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

*** Back gouge not required if complete joint penetration is verified by mock-up.

The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The strip seal shall be inserted into the Locking Edge Rails before the parapet ends are formed and poured to allow for proper placement of the strip seal.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.

BILL OF MATERIAL

Structure	Item	Unit	Total
020-0026	Preformed Joint Strip Seal	Foot	126.0
020-0027	Preformed Joint Strip Seal	Foot	126.0

•20-1(HVB,HB-1,VB-1,B,B-1)1