

F.A.I. ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	-	**	64	1
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
		* BRIDGE DECK REPAIRS 2007-1 ** ROCK ISLAND & WHITESIDE		

CONTRACT NO. 64D24

D-92-064-07



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
CONTRACT MAINTENANCE**

ROUTE FAI 88 (I-88)
BRIDGE REHABILITATIONS OVER I-88
SECTION: BRIDGE DECK REPAIRS 2007-1
PROJECT NO. IM-088-1(015)006
ROCK ISLAND AND WHITESIDE COUNTIES
JOB NO. C-92-085-07

FOR INDEX OF SHEETS
SEE SHEET 2
FOR A LISTING OF
HIGHWAY STANDARDS
SEE SHEET 2

TRAFFIC DATA
I-88 SPEED LIMIT 65 MPH

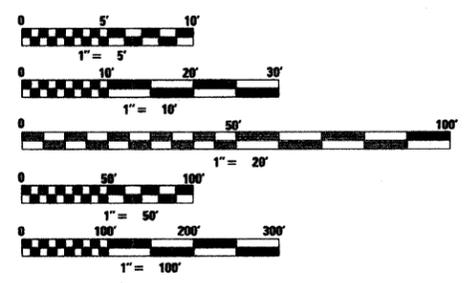
EXISTING ADT
I-88 10,800 (2005)
286TH ST.. 450 (2005)
317TH ST. 375 (2005)
WILMOT RD. 500 (2005)
LYNDON RD 1050 (2005)

PROJECT LOCATED IN ROCK ISLAND
AND WHITESIDE COUNTIES



SQUAD LEADER: FAITH DUNCAN 815-284-5364

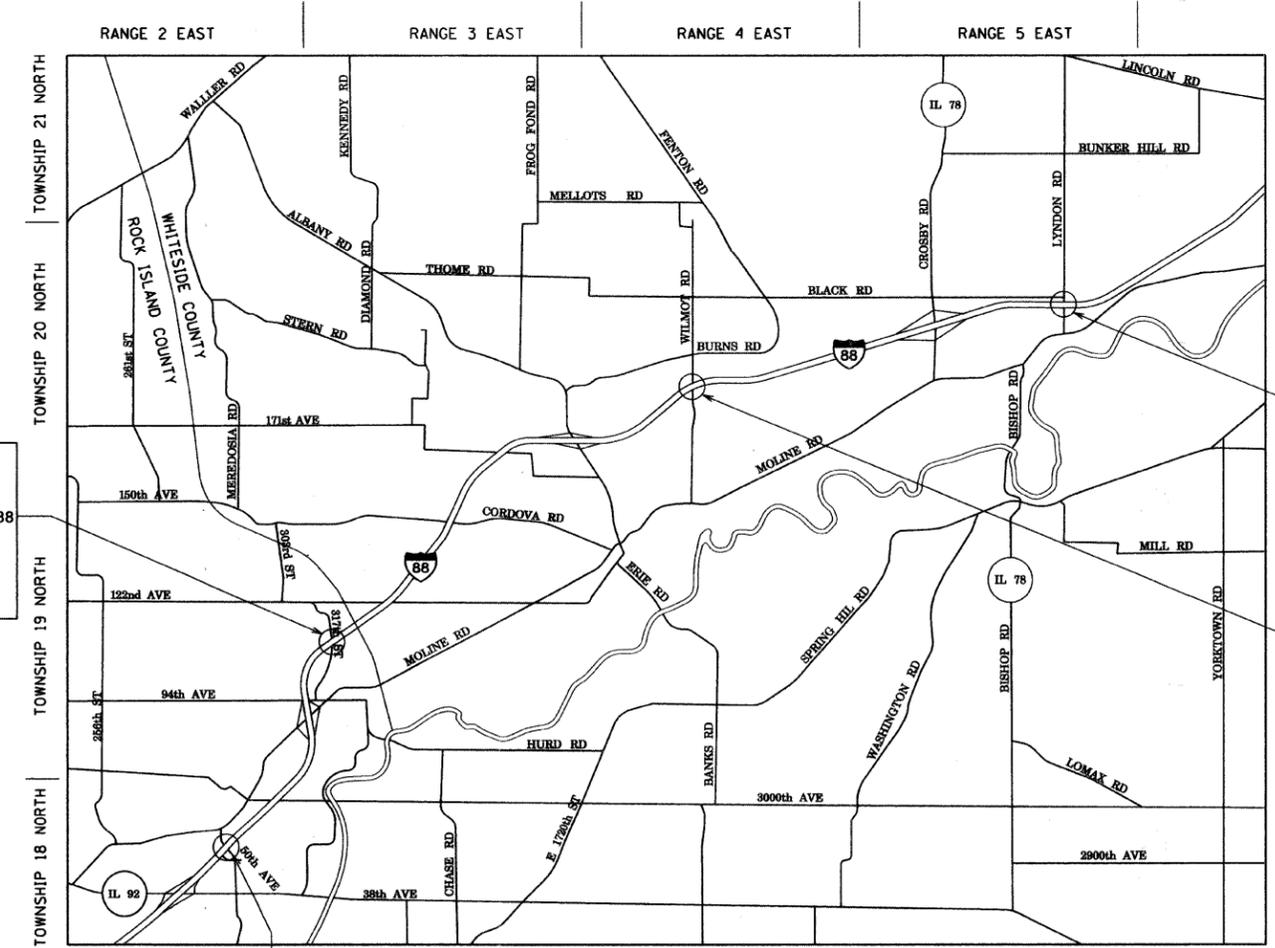
CONSULTANT CONTACT: JOHN ZERFAS, S.E. 773-399-5456
PROJECT ENGINEER: MAHMOUD ETEMADI 815-284-5393



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. JOINT UTILITY LOCATION INFORMATION
FOR EXCAVATION 1-800-892-0123

CONTRACT NO. 64D24



PROJECT LOCATION STRUCTURE NO: 081-0132 STA. 633+63.08, I-88 CH 49 (317TH ST) SECTION 17 CANOE CREEK TOWNSHIP

PROJECT LOCATION STRUCTURE NO: 081-0126 STA. 371+14.85, I-88 STA 2008+40.54 CH 25 (286TH ST.) SECTION 12 ZUMA TOWNSHIP.

NOT TO SCALE

PROJECT LOCATION STRUCTURE NO: 098-0066 STA. 1489+04.50, I-88 STA 3529+69.36 CH 37 (LYNDON RD) SECTION 15-16 LYNDON TOWNSHIP

PROJECT LOCATION STRUCTURE NO: 098-0058 STA. 1009+07.00, I-88 STA 3014+40.25 T.R.J.O. (WILMOT RD) SECTION 20-21 FENTON TOWNSHIP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED December 9 20 08
John F. Ryan
DEPUTY DIRECTOR OF HIGHWAYS, DISTRICT 2 ENGINEER

January 30, 20 09
Charles G. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT

January 30, 20 09
Christine M. Reed
DIRECTOR, DIVISION OF HIGHWAYS

DISTRICT 2, DIXON, IL

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(773) 399-0112

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	2
STA.		TO STA.		
FED. AID PROJECT	ILLINOIS	FED. ROAD DIST. NO.		

- * BRIDGE DECK REPAIRS 2007-1
- ** ROCK ISLAND & WHITESIDE

INDEX OF SHEETS

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4	SUMMARY OF QUANTITIES		
5	SUMMARY OF QUANTITIES		
6	SCHEDULE OF QUANTITIES		
7	SCHEDULE OF QUANTITIES DELETED		
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9	PROPOSED ROADWAY PLAN		
10	MOT PLAN		
11	GENERAL PLAN & ELEVATION		
12	DECK PLAN & CROSS SECTION		
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14	PARAPET & DECK SECTIONS & DETAILS		
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	CH 49 (317TH STREET) OVER I-88, S.N. 081-0132		
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20	PROPOSED ROADWAY PLAN		
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22	GENERAL PLAN & ELEVATION		
23	DECK PLAN & CROSS SECTION		
24	JOINT DETAILS		
25	PARAPET & DECK SECTIONS & DETAILS		
26	PREFORMED JOINT STRIP SEAL		
27	TYPE I ELASTOMERIC BEARINGS		
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STANDARD NUMBER	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
483001-04	PORTLAND CEMENT CONCRETE SHOULDER
701006-03	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701101-02	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701301-03	LANE CLOSURE, 2L, 2W, SHORT TERM OPERATIONS
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701400-03	APPROACH TO LANE CLOSURE - FREEWAY/EXPRESSWAY
701402-07	LANE CLOSURE FREEWAY/EXPRESSWAY WITH BARRIER
701406-05	LANE CLOSURE FREEWAY/EXPRESSWAY DAY OPERATIONS ONLY
701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS > 45 MPH
701901-01	TRAFFIC CONTROL DEVICES
704001-05	TEMPORARY CONCRETE BARRIER
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORTS
729001-01	APPLICATIONS OF TYPE A & B POSTS
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 BRIDGE REHABILITATION
 OVER I-88
 ROCK ISLAND AND
 WHITESIDE COUNTIES
**INDEX OF SHEETS
 AND HIGHWAY STANDARDS**
 SCALE: NTS
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	3
STA.		TO STA.		
FED. AID PROJECT	ILLINOIS	FED. ROAD DIST. NO.		
• BRIDGE DECK REPAIRS 2007-1 ** ROCK ISLAND & WHITESIDE				

GENERAL NOTES

1. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

MIXTURE USES(S):	SURFACE COURSE	
PG:	PG 64-22	
DESIGN AIR VOIDS	4% @ 50 GYR	
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL 9.5mm or IL 12.5mm	
FRICTION AGGREGATE	Mixture 'C'	
20 YEAR ESAL		

THE UNIT WEIGHT USED FOR CALCULATING ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 POUNDS PER SQUARE YARD PER INCH.

2. BITUMINOUS AND AGGREGATE PRIME COAT SHALL BE PLACED IN ACCORDANCE WITH SECTION 406 OF THE STANDARD SPECIFICATIONS. THE COST OF THE PRIME COATS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER TON FOR HOT-MIX ASPHALT SURFACE COURSE OF THE TYPE SPECIFIED.

3. THESE STRUCTURES WILL RETAIN THE SAME NUMBERS.

4. THE CONTRACTOR SHALL SUBMIT FOUR COPIES OF THE REQUIRED SHOP DRAWINGS FOR REVIEW AND APPROVAL TO THE BUREAU OF BRIDGES AND STRUCTURES, 2300 SOUTH DIRKSEN PARKWAY, SPRINGFIELD, IL 62764. AFTER APPROVAL OF INITIAL SUBMITTAL, THE CONTRACTOR SHALL SUBMIT ONE SET OF SHOP DRAWINGS TO DAVE LIPPERT, ENGINEER OF MATERIALS, 126 EAST ASH STREET, SPRINGFIELD, IL 62706, AND EIGHT (8) SETS OF SHOP DRAWINGS TO BE DISTRIBUTED TO:

- DISTRICT 2 DISTRICT ENGINEER (1)
- FABRICATOR (1)
- CONTRACTOR (2)
- RESIDENT ENGINEER (2)
- DISTRICT 2 BUREAU OF MATERIALS (2)

5. REFLECTOR MARKERS TYPE B SHALL BE INSTALLED ON THE TOP OF BRIDGE PARAPET WALLS. THE MARKERS SHALL BE ACCORDING TO STANDARD 635011 AND THE COLOR AND SPACING ACCORDING TO STANDARD 635006, EXCEPT THE MINIMUM IS 2 PER SIDE.

6 X PAVEMENT MARKING SHALL BE DONE ACCORDING TO STANDARD 780001, EXCEPT AS FOLLOWS:

1. ALL WORDS, SUCH AS ONLY, SHALL BE 2.4 M (8 FEET) HIGH.
2. ALL NON-FREEWAY ARROWS SHALL BE THE LARGE SIZE.
3. THE DISTANCE BETWEEN YELLOW NO-PASSING LINES SHALL BE 200 MM (8"), NOT 180 MM (7") AS SHOWN IN THE DETAIL OF TYPICAL LANE AND EDGE LINES.

7 X THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK. THE JULIE NUMBER IS 800-892-0123. THE FOLLOWING LISTED UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS ARE MEMBERS OF JULIE:

FRONTIER/CITIZENS	TELEPHONE
MAGELLAN MIDSTREAM PARTNERS	PETROLEUM
MCI WORLD COM	TELEPHONE
ALLIANCE PIPELINE COMPANY	PETROLEUM
DOME PIPELINE CORPORATION	PETROLEUM
KINDER-MORGAN ENERGIES PARTNERS	GAS
A T & T COMMUNICATIONS, INC.	TELEPHONE
MEDIACOM	CATV
COMMONWEALTH EDISON COMPANY	ELECTRIC
ALLIANT ENERGY	ELECTRIC
MIDAMERICAN ENERGY COMPANY	GAS
JO-CARROLL ENERGY	GAS
MID-AMERICA PIPELINE CO.	PETROLEUM
NORTHERN BORDER PIPELINE CO.	PETROLEUM
LIGHTCORE	COMMUNICATIONS

FOLLOWING ARE THE KNOWN UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS WHICH ARE NOT MEMBERS OF JULIE AND SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR:
 IDOT

8 X THE TEMPORARY CONCRETE BARRIER SHALL BE ANCHORED TO THE PAVEMENT AT THE FOLLOWING LOCATIONS FOR BRIDGE PAINTING OPERATIONS:
 I-88 UNDER 286TH STREET
 I-88 UNDER 317TH STREET
 I-88 UNDER WILMOT ROAD
 I-88 UNDER LYNDON ROAD

9 X CADD DATA WILL BE AVAILABLE TO CONTRACTORS AND CONSULTANTS WORKING ON THIS PROJECT. THIS INFORMATION WILL BE PROVIDED UPON REQUEST AS MICROSTATION CADD FILES AND GEOPAK COORDINATE GEOMETRY FILES ONLY. IF DATA IS REQUIRED IN OTHER FORMATS IT WILL BE YOUR RESPONSIBILITY TO MAKE THESE CONVERSIONS. IF ANY DISCREPANCY OR INCONSISTENCY ARISES BETWEEN THE ELECTRONIC DATA AND THE INFORMATION ON THE HARD COPY, THE INFORMATION ON THE HARD COPY SHOULD BE USED. CONTACT THE DISTRICT'S PROJECT ENGINEER TO REQUEST THESE FILES.

10 X DURING THE COURSE OF CONSTRUCTION, ONLY TWO OF THE BRIDGES SHALL BE CLOSED TO TRAFFIC AT ANY TIME.

11 X RAISED REFLECTIVE PAVEMENT MARKERS QUANTITIES HAVE BEEN INCLUDED IN THE SUMMARY OF QUANTITIES AS A CONTINGENCY ITEM; ONLY TO BE USED IF NEEDED.

12 X ALL REFERENCE TO, QUANTITIES AND NOTES ASSOCIATED WITH GUARDRAIL IMPROVEMENTS SHALL BE ELIMINATED FROM THIS PROJECT.

13 X THE COST OF CONCRETE CURB SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PORTLAND CEMENT CONCRETE SHOULDERS 9".

14 X ALL REFERNECE TO CH 17 SHALL BE SWITCHED TO TR 104.

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 BRIDGE REHABILITATION
 OVER I-88
 ROCK ISLAND AND
 WHITESIDE COUNTIES

GENERAL NOTES

SCALE: NTS
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS

CONTRACT NO. 64D24

SUMMARY OF QUANTITIES

← ROCK ISLAND → ← WHITESIDE →

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	5
STA.		TO STA.		
FED. AID PROJECT ILLINOIS		FED. ROAD DIST. NO.		
* BRIDGE DECK REPAIRS 2007-1				
** ROCK ISLAND & WHITESIDE				

PAY ITEM NUMBER	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	S.N. 081-0126		S.N. 081-0132		S.N. 098-0058		S.N. 098-0066	
				90% FEDERAL 10% STATE X2 31-2A 286TH ST	100% ROCK ISLAND COUNTY X2 31-2A 286TH ST	90% FEDERAL 10% STATE X2 31-2A 317TH ST	100% ROCK ISLAND COUNTY X2 31-2A 317TH ST	90% FEDERAL 10% STATE X2 31-2A WILMOT RD	100% FENTON TOWNSHIP X2 31-2A WILMOT RD	90% FEDERAL 10% STATE X2 31-2A LYNDON RD	100% WHITESIDE COUNTY X2 31-2A LYNDON RD
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	10206	1,584	1,972	1,920	466	1,872	398	1,716	278
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	24332	6,068		6,088		6,084		6,092	
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	1952	488		488		488		488	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	16	4		4		4		4	
* 78200520	BARRIER WALL MARKERS, TYPE B	EACH	94	4		4		4		82	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	8,761	2,185		2,192		2,191		2,193	
X0321468	PLUG EXISTING DECK DRAINS	EACH	68	12		8		36		12	
X0321743	SILICONE JOINT SEALER, 1"	FOOT	65				19		18		28
X0323082	DRAINAGE SCUPPERS, DS-33	EACH	16	4		4		4		4	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	819	150		295		198		176	
X5067501	BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 1	L SUM	1	1							
X5067502	BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 2	L SUM	1			1					
X5067503	BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 3	L SUM	1					1			
X5067504	BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 4	L SUM	1								1
X7013015	TRAFFIC CONTROL FOR ROAD CLOSURE	L SUM	1	0.25		0.25		0.25		0.25	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.25		0.25		0.25		0.25	
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	32	3		6		21		2	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	103	9		12		27		55	
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	349	32		128		63		126	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	8	2		2		2		2	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	8	2		2		2		2	

* DENOTES SPECIALTY ITEM

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 BRIDGE REHABILITATION
 OVER I-88
 ROCK ISLAND AND
 WHITESIDE COUNTIES
SUMMARY OF QUANTITIES
 SCALE: NTS
 DATE 10/08/08
 DRAWN BY EF
 CHECKED BY RS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	6
STA.		TO STA.		
FED. AID PROJECT		ILLINOIS	FED. ROAD DIST. NO.	

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE

STATION		OFFSET	48300400	42001300	60260100	44004250
FROM	TO		PCC SHOULDER, 9"	PROTECTIVE COAT	INLETS TO BE ADJUSTED	PAVED SHOULDER REMOVAL
			SQ YD	SQ YD	EACH	EACH
317TH ST CH 49						
2129+07.3	2129+22.3	LEFT	6.1	6.1	1	7
2129+23.7	2129+38.4	RIGHT	6	6	1	6
2132+17.9	2132+31.9	LEFT	5.7	5.7	1	6
2132+33.9	2132+48.9	RIGHT	6.1	6.1	1	6
SUBTOTAL 317TH ST CH 49			24	24	4	25
WILMOT RD CH 17						
3012+70.9	3012+92.9	LEFT	9.2	9.2	1	9.1
3012+83.1	3013+06.2	RIGHT	8.4	8.4	1	7.5
3015+74.7	3015+94.7	LEFT	8.4	8.4	1	7.3
3015+87.9	3016+09.9	RIGHT	9.3	9.3	1	6.4
SUBTOTAL WILMOT RD CH 17			35	35	4	30.3
LYNDON RD CH 37						
3528+33.6	3528+44.6	LEFT	8.6	8.6	1	9
3528+33.6	3528+44.6	RIGHT	8.6	8.6	1	9
3530+94.6	3531+05.2	LEFT	8.6	8.6	1	9
3530+94.6	3531+05.2	RIGHT	8.6	8.6	1	9
SUBTOTAL LYNDON RD CH 37			34	34	4	36
TOTALS			93	93	12	91

STATION		OFFSET	78001110	
FROM	TO		PAVEMENT MARKING LINE 4" WHITE EDGE LINE	PAVEMENT MARKING LINE 4" YELLOW CENTERLINE
			FOOT	FOOT
286TH ST				
2007+16.97	2009+63.57	C/L		1972
TOTAL 286TH ST				1,972
317th ST				
2131+97.9	2132+27.7	LT/RT & C/L	120	120
2129+57.9	2131+97.9	LT/RT & C/L	960	960
2129+29.7	2129+57.9	LT/RT & C/L	113	113
TOTAL 317th ST				2,386
WILMOT RD				
3015+57.3	3015+82.4	LT/RT & C/L	101	101
3013+23.3	3015+57.3	LT/RT & C/L	936	936
3012+98.9	3013+23.3	LT/RT & C/L	98	98
TOTAL WILMOT RD				2,270
LYNDON RD				
3530+76.7	3530+94.1	LT/RT & C/L	70	70
3528+62.1	3530+76.7	LT/RT & C/L	859	859
3528+44.9	3528+62.1	LT/RT & C/L	69	69
TOTAL LYNDON RD				1,996
TOTALS				8,624

LOCATION	OFFSET	70300100	70300520		70301000	78008210		78008230	78300100
		SHORT TERM PAVEMENT MARKING	YELLOW PAVEMENT MARKING TAPE TYPE III 4"	WHITE PAVEMENT MARKING TAPE TYPE III 4"	WORK ZONE PAVEMENT MARKING REMOVAL	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" WHITE EDGE LINE	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" YELLOW EDGE LINE	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 6" WHITE LANE SKIP-DASH	PAVEMENT MARKING REMOVAL
		FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	SQ FT
I-88 UNDER 286TH ST CH 25		355	6,068	6,068	4,045	3,034	3,034	488	2,185
I-88 UNDER 317TH ST CH 49		355	6,088	6,088	4,059	3,044	3,044	488	2,192
I-88 UNDER WILMOT RD CH 17		355	6,084	6,084	4,056	3,042	3,042	488	2,191
I-88 UNDER LYNDON RD CH 37		355	6,092	6,092	4,061	3,046	3,046	488	2,193
TOTALS		1,420	48,664	48,664	16,221	24,332	24,332	1,952	8,761

LOCATION	70400100		78200520	Z0030250	70400200		Z0030350
	TEMPORARY CONCRETE BARRIER (WB)	TEMPORARY CONCRETE BARRIER (EB)	BARRIER WALL MARKERS TYPE B	IMPACT ATTENUATOR NON-RD	RELOCATE CONCRETE BARRIER (WB)	RELOCATE CONCRETE BARRIER (EB)	IMPACT ATTENUATOR NON-RD
	FOOT	FOOT	EACH	EACH	FOOT	FOOT	EACH
I-88 UNDER 286TH ST CH 25	0	0	0	0	474	474	4
I-88 UNDER 317TH ST CH 49	0	0	0	0	484	484	4
I-88 UNDER WILMOT RD CH 17	0	0	0	0	482	482	4
I-88 UNDER LYNDON RD CH 37	243	243	78	2	243	243	2
TOTALS	243	243	78	2	1683	1683	14

\$FILE\$ Sat Jan 10 12:55:00 2009

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
BRIDGE REHABILITATION
OVER I-88
ROCK ISLAND AND
WHITESIDE COUNTIES
SCHEDULE OF QUANTITIES

SCALE: NTS
DATE 12/09/08

DRAWN BY EF
CHECKED BY RS

CONTRACT NO. 64D24

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	7
STA.		TO STA.		
FED. AID PROJECT		ILLINOIS FED. ROAD DIST. NO.		

* BRIDGE DECK REPAIRS 2007-1
** ROCK ISLAND & WHITESIDE

STATION	OFFSET	63000000	63100167	63100085	63200310	63500105	78200410	78200520	78201000
		STEEL PLATE BEAM GUARDRAIL TYPE A FOOT	TRAFFIC BARRIER TERMINAL TYPE 1 (SP) TANGENT EACH	TRAFFIC BARRIER TERMINAL TYPE 6 EACH	GUARDRAIL REMOVAL FOOT	DELINEATORS EACH	GUARDRAIL MARKERS TYPE A EACH	BARRIER WALL MARKERS TYPE B EACH	TERMINAL MARKERS DIRECT APPLIED EACH
286TH ST CH 25									
2002+17.9	2002+67.9	LEFT		1		50	1		1
2002+48.1	2002+98.1	RIGHT		1		50	1		1
2006+23.8	2006+36.3	LEFT	12.5			12.5			
2006+36.3	2006+67.0	LEFT				30.5		4	
2006+23.8	2006+36.3	RIGHT	12.5			12.5			
2006+36.3	2006+67.0	RIGHT				30.5		4	
2006+67.0	2009+63.6	RT & LT						4	
2009+63.6	2009+94.2	LEFT				30.5		4	
2009+94.2	2010+06.7	LEFT	12.5			12.5			
2009+63.6	2009+94.2	RIGHT				30.5		4	
2009+94.2	2010+06.7	RIGHT	12.5			12.5			
2013+77.8	2014+27.8	RIGHT		1		50	1		1
2014+14.3	2014+64.3	LEFT		1		50	1		1
SUBTOTAL 286TH ST CH 25			50	4	4	372	4	16	4
317TH ST CH 49									
2128+08.0	2128+58.0	LEFT		1		46	1		1
2128+58.0	2128+70.5	LEFT	12.5			12.5			
2128+70.5	2129+01.2	LEFT			1	43		4	
2128+25.6	2128+75.6	RIGHT		1		45	1		1
2128+75.6	2128+88.1	RIGHT	12.5			12.5			
2128+88.1	2129+18.8	RIGHT			1	43		4	
2129+31.3	2132+25.5	RT & LT						4	
2132+37.0	2132+67.6	LEFT			1	43		4	
2132+67.6	2132+80.1	LEFT	12.5			12.5			
2132+80.1	2133+30.1	LEFT		1		46	1		1
2132+29.0	2132+72.7	RIGHT			1	43		4	
2132+72.7	2132+97.7	RIGHT	12.5			12.5			
2132+97.7	2133+47.7	RIGHT		1		46	1		1
SUBTOTAL 317TH ST CH 49			50	4	4	405	4	16	4
WILMOT RD CH 17									
3008+22.7	3008+72.7	LEFT		1		50	1		1
3008+37.7	3008+87.7	RIGHT		1		50	1		1
3012+42.6	3012+55.1	LEFT	12.5			12.5			
3012+55.1	3012+85.7	LEFT			1	30.5		4	
3012+56.3	3012+68.8	RIGHT	12.5			12.5			
3012+68.8	3012+98.4	RIGHT			1	30.5		4	
3012+99.4	3015+80.1	RT & LT						4	
3015+80.1	3016+10.8	LEFT			1	30.5		4	
3016+10.8	3016+23.3	LEFT	12.5			12.5			
3015+94.9	3016+25.6	RIGHT			1	30.5		4	
3016+25.6	3016+38.1	RIGHT	12.5			12.5			
3019+93.4	3020+43.4	LEFT		1		50	1		1
3020+08.1	3020+58.1	RIGHT		1		50	1		1
SUBTOTAL WILMOT RD CH 17			50	4	4	372	4	16	4
LYNDON RD CH 37									
3527+34.5	3527+84.5	LEFT		1		45	1		1
3527+84.5	3527+97.0	LEFT	12.5			12.5			
3527+97.0	3528+40.1	LEFT			1	43		4	
3527+34.5	3527+84.5	RIGHT		1		46	1		1
3527+84.5	3527+97.0	RIGHT	12.5			12.5			
3527+97.0	3528+40.1	RIGHT			1	43		4	
3528+40.1	3530+98.7	RT & LT						4	
3530+98.7	3531+41.9	LEFT			1	43		4	
3531+41.9	3531+54.4	LEFT	12.5			12.5			
3531+54.4	3532+04.4	LEFT		1		46	1		1
3530+98.7	3531+41.9	RIGHT			1	43		4	
3531+41.9	3531+54.4	RIGHT	12.5			12.5			
3531+54.4	3532+04.4	RIGHT		1		45	1		1
SUBTOTAL LYNDON RD CH 37			50	4	4	404	4	16	4
TOTALS			200	16	16	1,553	16	64	16

STATION	OFFSET	70300100	78001110	78100100	78100105
		SHORT TERM PAVEMENT MARKING FOOT	PAVEMENT MARKING LINE 4" WHITE EDGE LINE FOOT	PAVEMENT MARKING LINE 4" YELLOW CENTERLINE FOOT	RAISED REFLECTIVE PAVEMENT MARKERS EACH
286TH ST					
2001+83.9	2002+67.9	LEFT		168	
2002+14.1	2002+98.1	RIGHT		168	
2006+67.0	2007+41.5	C/L	54	298	298
2007+41.5	2009+39.5	C/L	144	792	792
2009+39.5	2010+13.6	C/L	54	296	296
2013+77.8	2014+81.8	LEFT		168	
2014+14.3	2014+88.3	RIGHT		168	
TOTAL 286TH ST			54	3,444	4
317TH ST					
2127+74.0	2129+57.9	LEFT		368	
2129+57.9	2131+97.9	LEFT		480	
2131+97.9	2133+64.1	LEFT		333	
2127+91.6	2129+57.9	RIGHT		333	
2129+57.9	2131+97.9	RIGHT		480	
2131+97.9	2133+81.7	RIGHT		368	
2128+78.9	2129+57.9	C/L	57		316
2129+57.9	2131+97.9	C/L	175		960
2131+97.9	2132+76.9	C/L	57		316
TOTAL 317TH ST			289	3,954	4
WILMOT RD					
3007+89.0	3008+73.0	LEFT		168	
3008+03.7	3008+87.7	RIGHT		168	
3012+40.3	3013+23.3	RT & LT		332	
3013+23.3	3015+57.3	RT & LT		936	
3015+57.3	3016+40.3	RT & LT		332	
3012+48.3	3013+23.3	C/L	55		300
3013+23.3	3015+57.3	C/L	170		936
3015+57.3	3016+32.3	C/L	55		300
TOTAL WILMOT RD			280	3,472	4
LYNDON RD					
3527+00.5	3528+62.1	RT & LT		646	
3528+62.1	3530+76.7	RT & LT		858	
3530+76.7	3532+38.4	RT & LT		647	
3527+94.6	3528+62.1	C/L	49		270
3528+62.1	3530+76.7	C/L	156		858
3530+76.7	3531+44.2	C/L	49		270
TOTAL LYNDON RD			254	3,549	4
TOTALS			877	14,419	16

LOCATION	70400100	78200520	Z0030250	70400200	Z0030350		
	TEMPORARY CONCRETE BARRIER (WB) FOOT	TEMPORARY CONCRETE BARRIER (EB) FOOT	BARRIER WALL MARKERS TYPE B EACH	IMPACT ATTENUATOR NON-RD EACH	RELOCATE CONCRETE BARRIER (WB) FOOT	RELOCATE CONCRETE BARRIER (EB) FOOT	IMPACT ATTENUATOR NON-RD EACH
I-88 UNDER 286TH ST CH 25	0	0	0	0	474	474	4
I-88 UNDER 317TH ST CH 49	0	0	0	0	484	484	4
I-88 UNDER WILMOT RD CH 17	0	0	0	0	482	482	4
I-88 UNDER LYNDON RD CH 37	243	243	78	2	243	243	2
TOTALS	243	243	78	2	1683	1683	14

LOCATION	OFFSET	70300100	70300520	70301000	78008210	78008230	78300100		
		SHORT TERM PAVEMENT MARKING FOOT	YELLOW PAVEMENT MARKING TAPE TYPE III 4" FOOT	WHITE PAVEMENT MARKING TAPE TYPE III 4" FOOT	WORK ZONE PAVEMENT MARKING REMOVAL SQ FT	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" WHITE EDGE LINE FOOT	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" YELLOW EDGE LINE FOOT	POLYUREA PAVEMENT MARKING TYPE 1 - LINE 6" WHITE LANE SKIP-DASH FOOT	PAVEMENT MARKING REMOVAL SQ FT
I-88 UNDER 286TH ST CH 25		355	6,068	6,068	4,045	3,034	3,034	488	2,185
I-88 UNDER 317TH ST CH 49		355	6,088	6,088	4,059	3,044	3,044	488	2,192
I-88 UNDER WILMOT RD CH 17		355	6,084	6,084	4,056	3,042	3,042	488	2,191
I-88 UNDER LYNDON RD CH 37		355	6,092	6,092	4,061	3,046	3,046	488	2,193
TOTALS		1,420	48,664	48,664	16,221	24,332	24,332	1,952	8,761

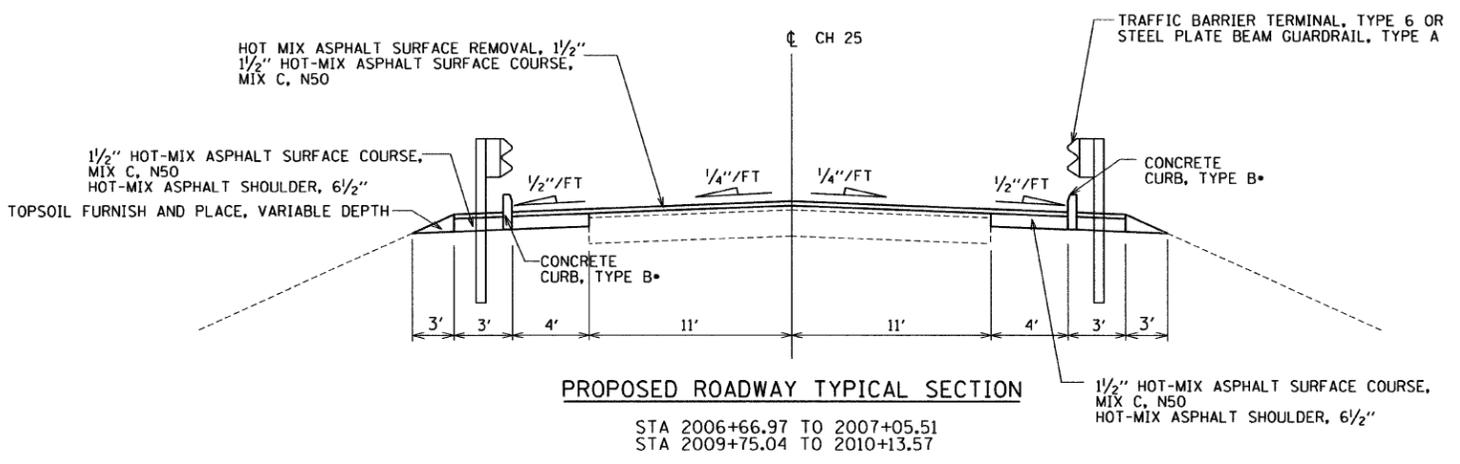
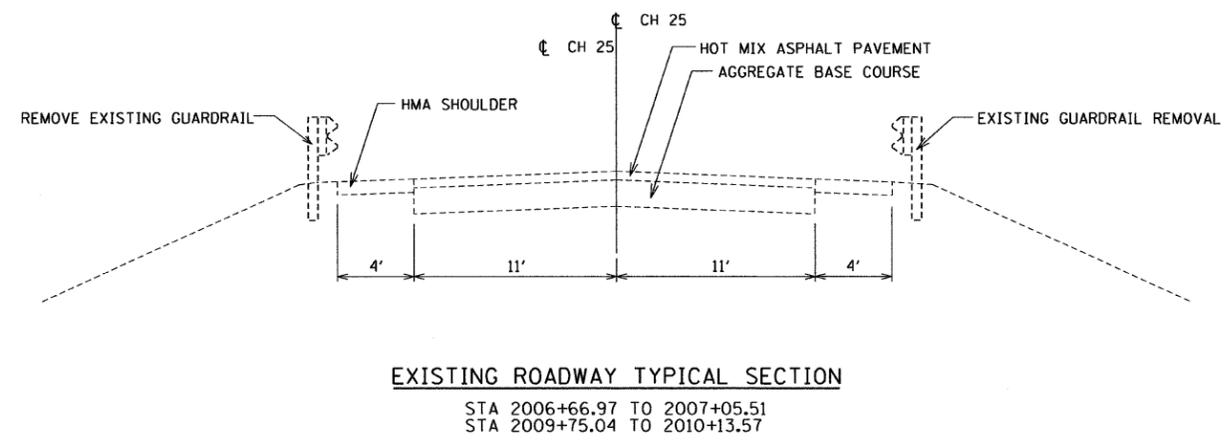
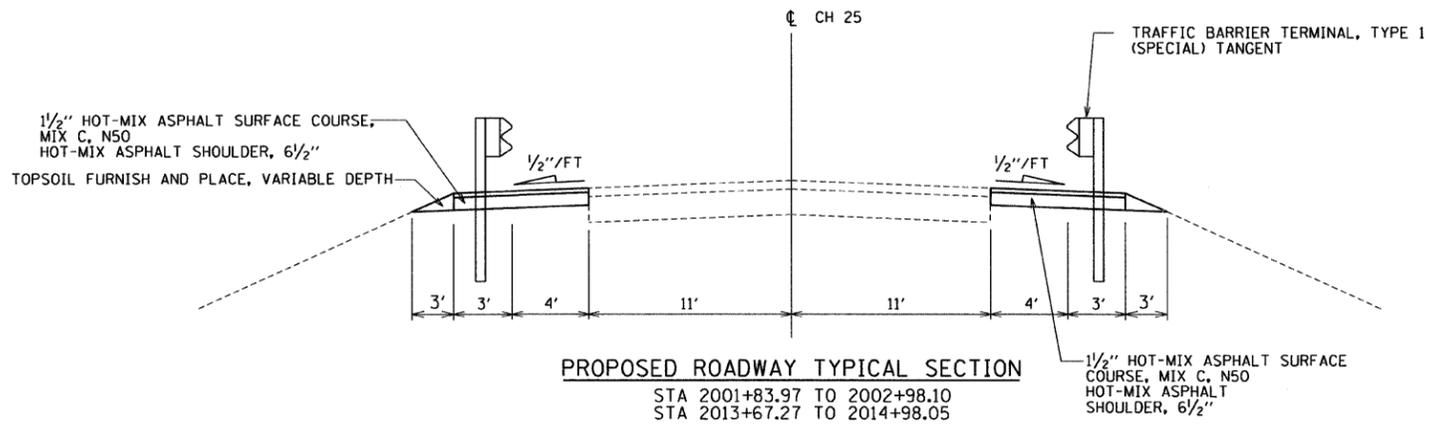
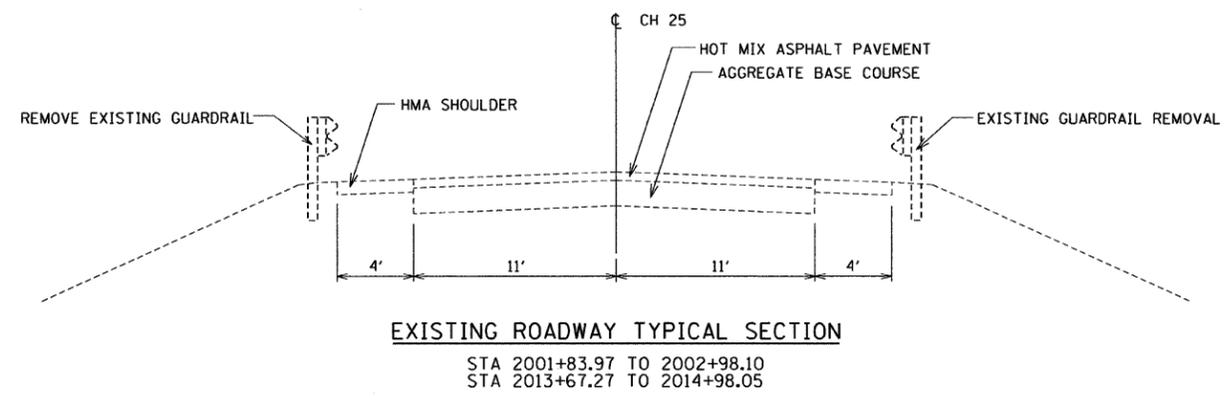
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
BRIDGE REHABILITATION OVER I-88
ROCK ISLAND AND WHITESIDE COUNTIES
SCHEDULE OF QUANTITIES
SCALE: NTS
DATE 10/08/08
DRAWN BY
CHECKED BY RS

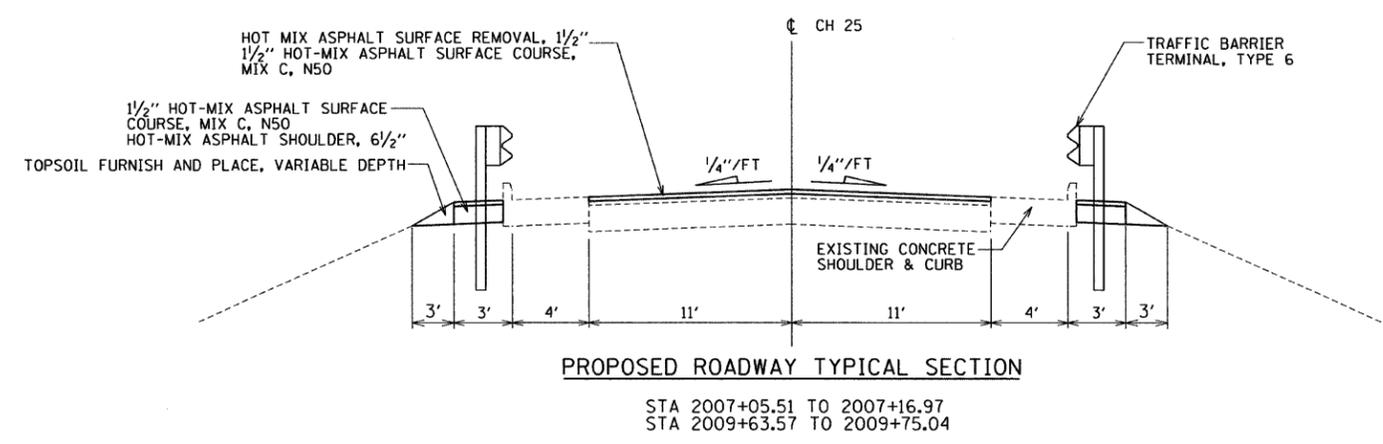
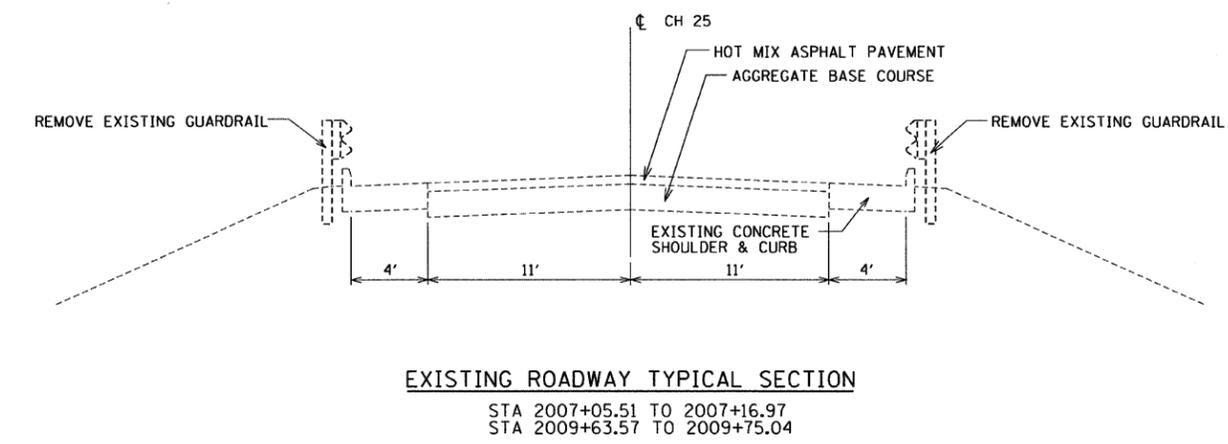
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	8
STA. 2002+17.81		TO STA. 2014+63.54		
FED. AID PROJECT	ILLINOIS	FED. ROAD DIST. NO.		

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE



- STA 2006+82.20 TO 2007+05.51
- STA 2009+75.04 TO 2009+99.20



- HOT-MIX ASPHALT SURFACE COURSE 112 LBS/IN THICK/SQ YD
- HOT-MIX ASPHALT SHOULDER 112 LBS/IN THICK/SQ YD
- TEMPORARY EROSION CONTROL SEEDING 100 LB/ACRE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 25 (286th STREET) BRIDGE OVER I-88
 STRUCTURE NO.: 081-0126
 BRIDGE REHABILITATION

TYPICAL SECTIONS

SCALE: NTS
 DATE 12/09/08

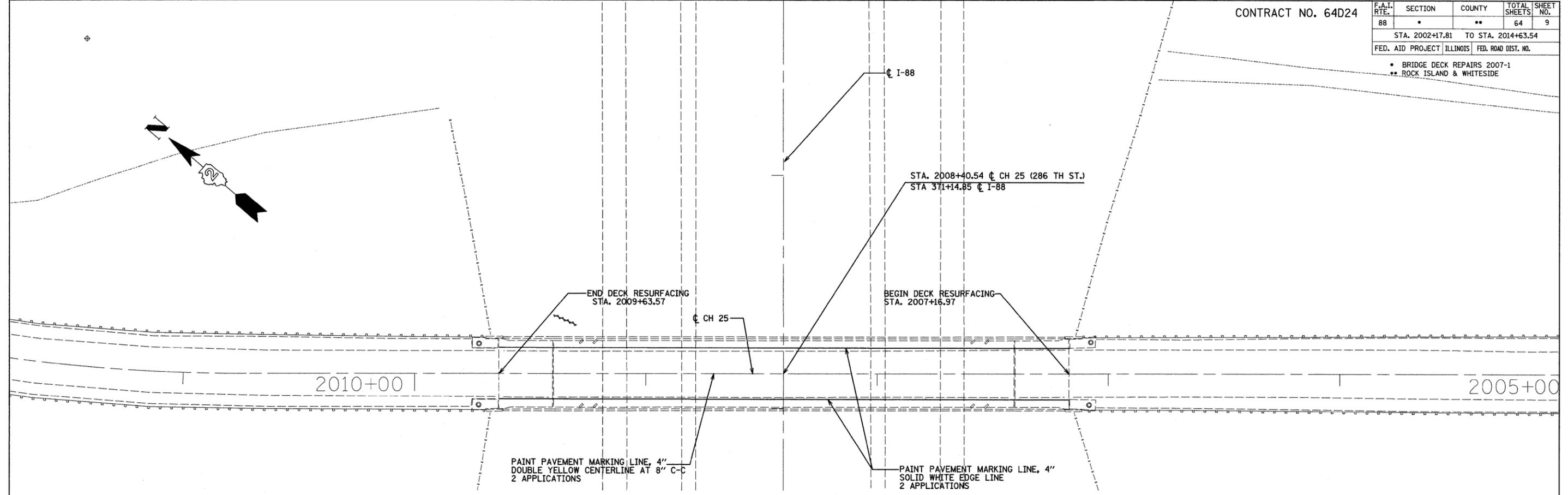
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CONTRACT NO. 64D24

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	9
STA. 2002+17.81		TO STA. 2014+63.54		
FED. AID PROJECT ILLINOIS		FED. ROAD DIST. NO.		

- BRIDGE DECK REPAIRS 2007-1
- ** ROCK ISLAND & WHITESIDE



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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 25 (286th STREET) BRIDGE OVER I-88
 STRUCTURE NO. 081-0126
 BRIDGE REHABILITATION

PROPOSED ROADWAY PLAN

SCALE: 1"=20'
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS

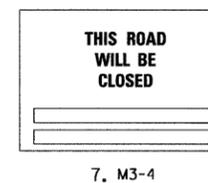
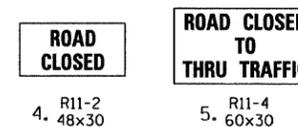
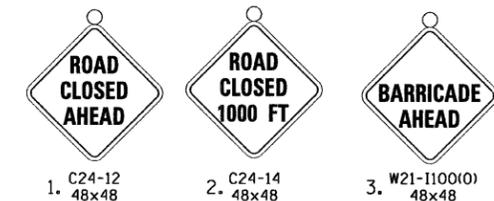
CONTRACT NO. 64D24

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	10
STA. 2002+17.81		TO STA. 2014+63.54		
FED. AID PROJECT		ILLINOIS	FED. ROAD DIST. NO.	

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE

MAINTENANCE OF TRAFFIC

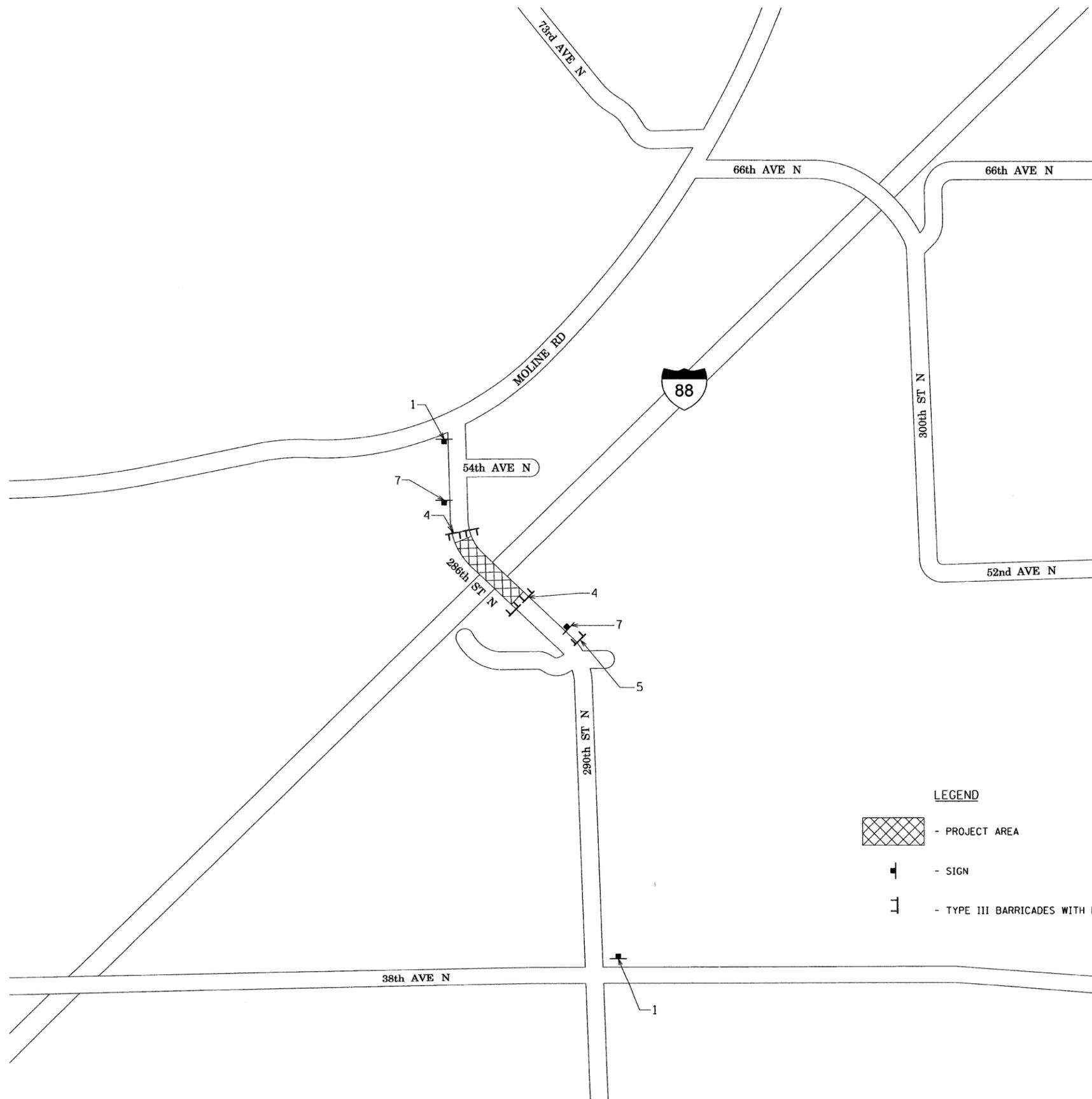
1. THE DEPARTMENT SHALL BE NOTIFIED 72 HOURS PRIOR TO ROAD CLOSURE AND IMMEDIATELY AFTER THE ROAD HAS BEEN REOPENED.
2. ROCK ISLAND COUNTY SHALL BE NOTIFIED AT LEAST TWO WEEKS PRIOR TO CLOSING THE ROAD.
3. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL TEMPORARY SIGN SUPPORTS. AFTER REMOVING THE SUPPORTS, THE CONTRACTOR SHALL FILL HOLES (IF ANY) AND RESTORE THE GROUND TO ITS ORIGINAL CONDITION AND ELEVATION. PLACING OF GRAVEL, SOIL, OR SEED SHALL BE INCLUDED IN THE UNIT BID PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE".
4. ALL WARNING SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND ON AN ORANGE BACKGROUND, UNLESS OTHERWISE NOTED.
5. REFER TO DISTRICT STANDARD 40.1 FIR SIGNING REQUIREMENTS
6. EACH TYPE III BARRICADE SHALL BE EQUIPPED WITH TWO FLASHING AMBER LIGHTS.
7. THE CONTRACTOR SHALL ERECT SIGN NO. 7 AT LEAST 2 WEEKS IN ADVANCE OF THE CLOSURE.
8. ALL ENTRANCES ON THE CLOSED PORTION OF 50TH AVENUE SHALL BE OPEN FOR TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD.
9. THE WORK SHALL BE PAID FOR AT THE LUMP SUM PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE"



7. M3-4

LEGEND

- PROJECT AREA
- SIGN
- TYPE III BARRICADES WITH FLASHERS



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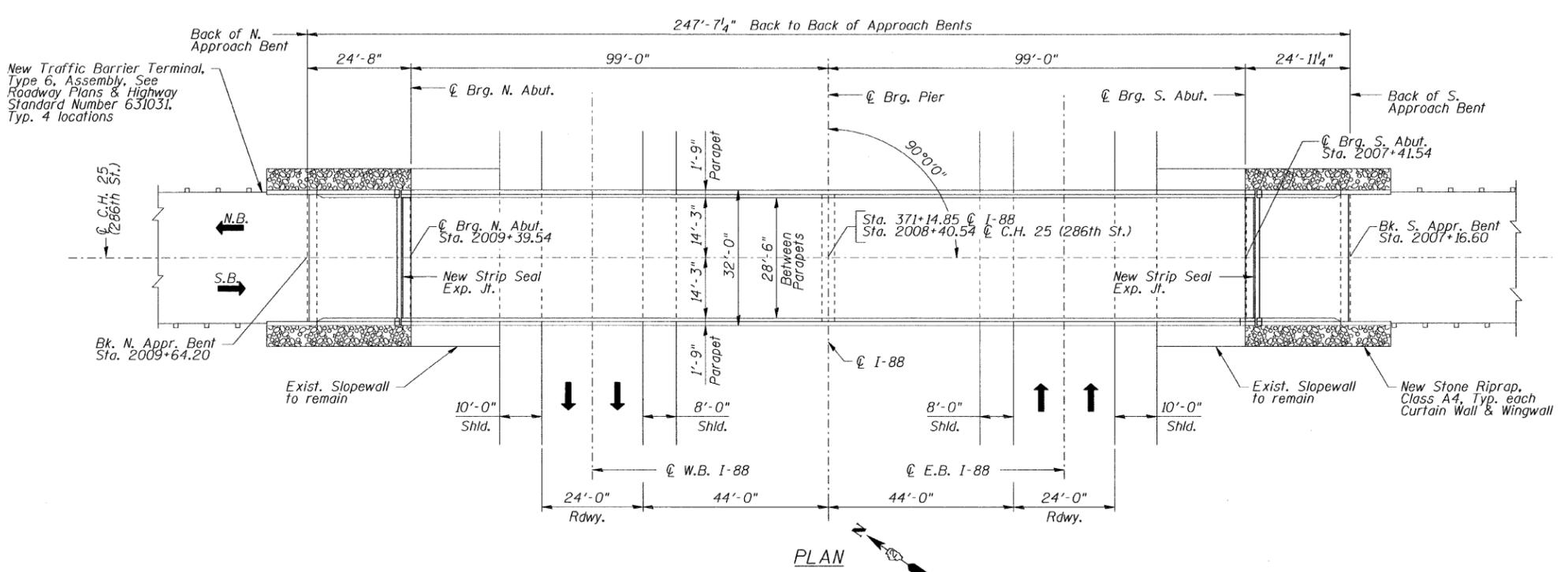
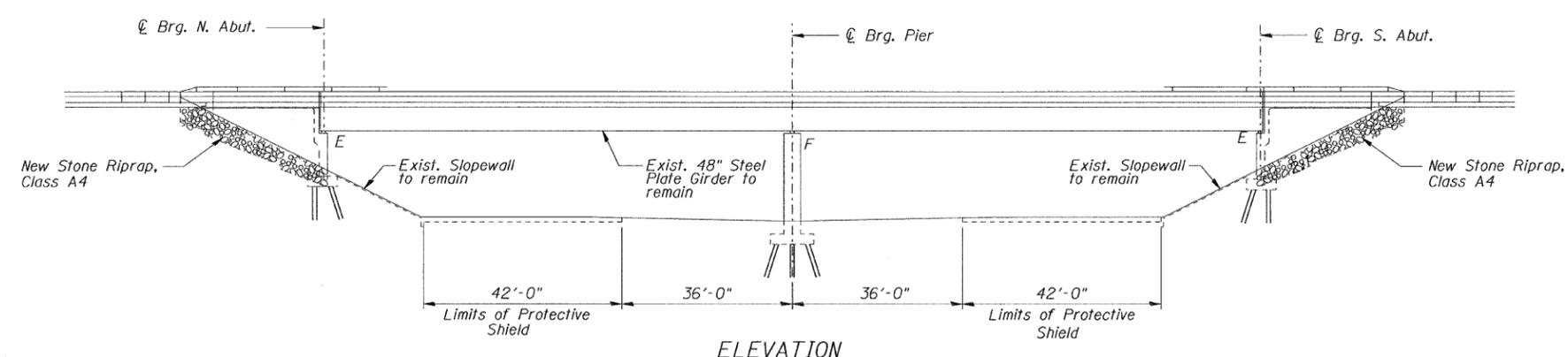
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 25 (286th STREET) BRIDGE OVER I-88
 STRUCTURE NO.: 081-0126
 BRIDGE REHABILITATION

MOT PLAN

SCALE: NTS
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS



TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
STONE RIPRAP, CLASS A4	SQ. YD.	96
FILTER FABRIC	SQ. YD.	96
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	65
HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ. YD.	781
CONCRETE REMOVAL	CU. YD.	6.6
PROTECTIVE SHIELD	SQ. YD.	243
CONCRETE SUPERSTRUCTURE	CU. YD.	7.0
PROTECTIVE COAT	SQ. YD.	16
CLEANING AND PAINTING STEEL BRIDGE NO. 1	L. SUM	1
CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 1	L. SUM	1
REINFORCEMENT BARS, EPOXY COATED	POUND	1,040
PREFORMED JOINT STRIP SEAL	FOOT	61
JACK AND REMOVE EXISTING BEARINGS	EACH	8

ITEM	UNIT	QUANTITY
ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	8
ANCHOR BOLTS, 1"	EACH	32
FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1520
PLUG EXISTING DECK DRAINS	EACH	12
WATERPROOFING MEMBRANE SYSTEM	SQ. YD.	769
DRAINAGE SCUPPERS, DS-33	EACH	4
STRUCTURAL REPAIR OF CONCRETE (DEPTH<5")	SQ. FT.	150
BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 1	L. SUM	1
DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	3
DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	9
DECK SLAB REPAIR (PARTIAL)	SQ YD	32

GENERAL NOTES

All structural steel shall be AASHTO M270 Grade 50.

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck at the expansion joint, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that cannot be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for work.

Protective Coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

All structural steel at expansion joints shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type I. Cost included with Preformed Joint Strip Seal.

Quantities for deck slab repair are approximate. Quantity of Deck Slab Repair (Full Depth, Type I) and Deck Slab Repair (Full Depth, Type II) are each assumed to be equal to 0.40% of the deck area. Quantity of Deck Slab Repair (Partial) is assumed to be equal to 5.0% of the deck area. Locations will be determined by the Resident Engineer following removal of the Hot-Mix Asphalt Surface Course. Actual repair locations shall be shown on the as-built plans. Contractor will be paid for the quantity furnished.

Cleaning and painting of the existing structural steel shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All existing steel shall be cleaned per Near White Blast Cleaning - SSPC-SPI10. All existing steel shall be painted according to the requirements of Paint System 1 -0Z/E/U. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Blue, Munsell No. 10B 3/6.

A minimum of 2 air monitors will be required to monitor abrasive blasting operations at this site, see special provision for "Containment and Disposal of Lead Paint Cleaning Residues."

The SSPC QP1 & QP2 certifications will be required for this contract.

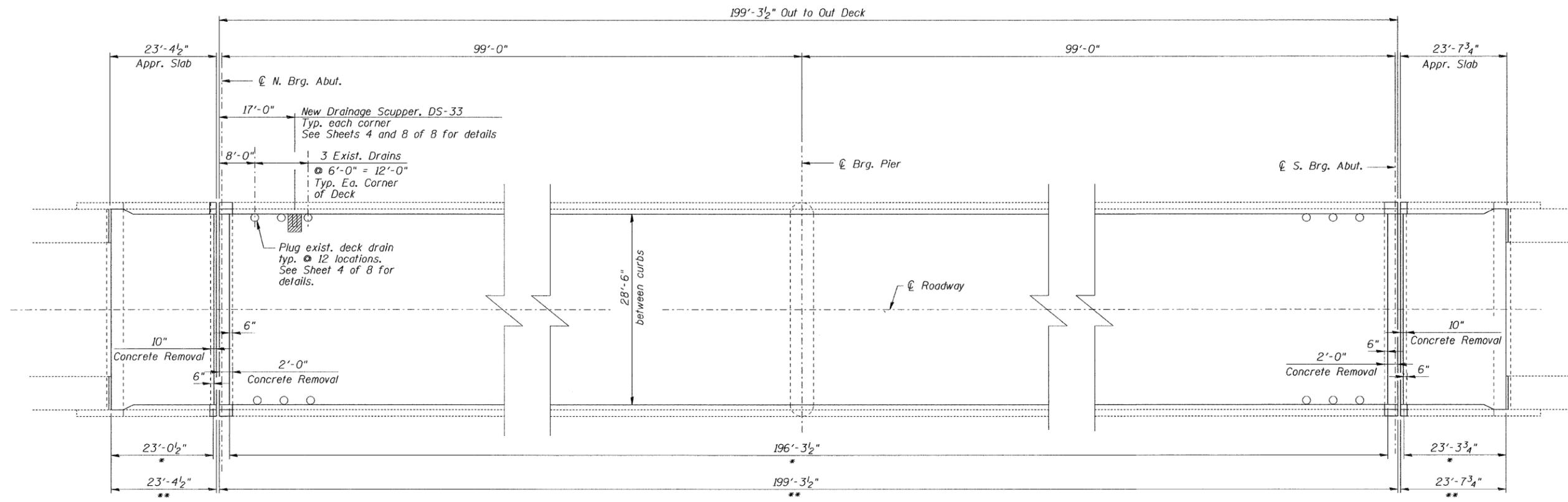
Existing plans are available from the District to determine painting quantities.

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

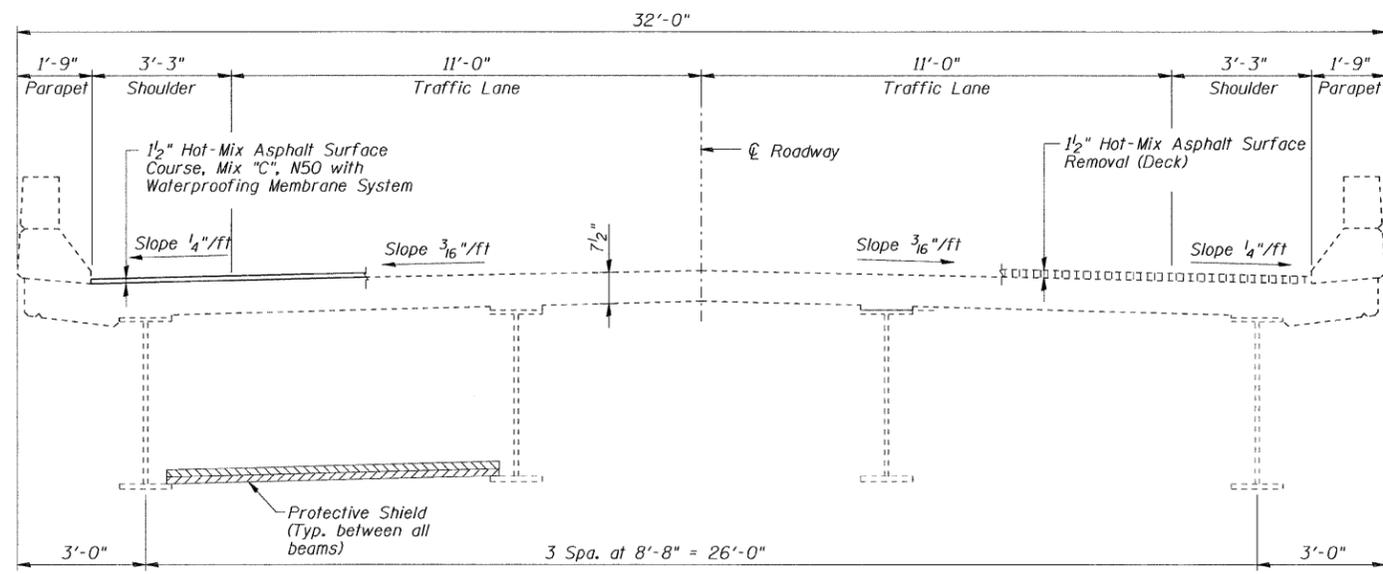


GENERAL PLAN & ELEVATION
 C.H. 25 (286th STREET) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 ROCK ISLAND COUNTY
 STATION 2008+40.54
 S.N. 081-0126
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

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 12/8/2008



PLAN



DECK CROSS SECTION

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Hot Mix Asphalt Surface Course, Mix "C", N50	Ton	65
Hot Mix Asphalt Surface Removal (Deck)	Sq. Yd.	781
Protective Shield	Sq. Yd.	243
Waterproofing Membrane System	Sq. Yd.	769

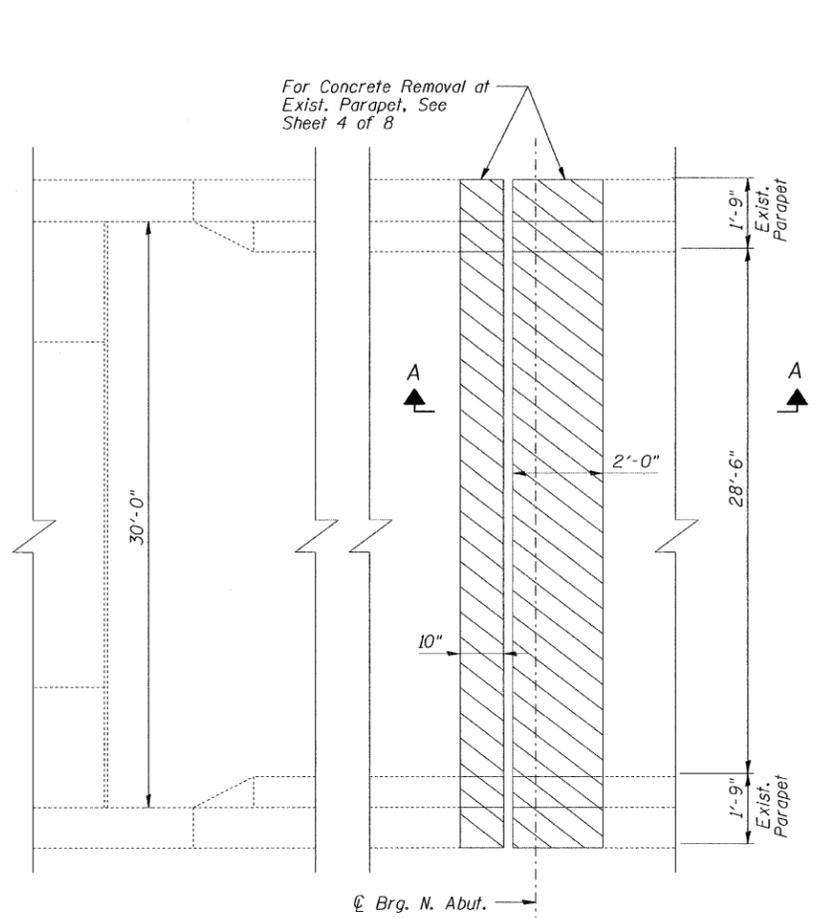
Notes:

- * - Limits of Hot-Mix Asphalt Surface Course, Mix "C", N50 and Waterproofing Membrane System
- ** - Limits of Hot-Mix Asphalt Surface Removal (Deck)

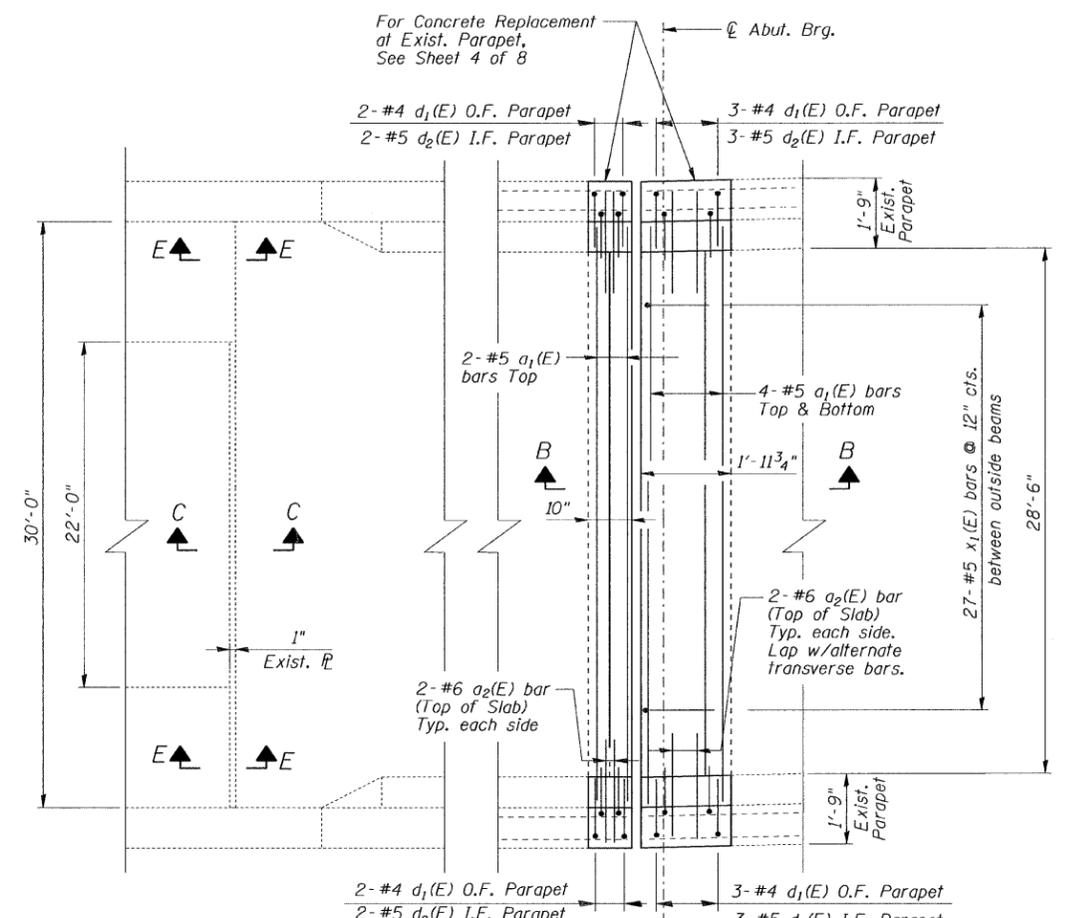
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

DECK PLAN & CROSS SECTION
 C.H. 25 (286th STREET) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 ROCK ISLAND COUNTY
 STATION 2008+40.54
 S.N. 081-0126
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

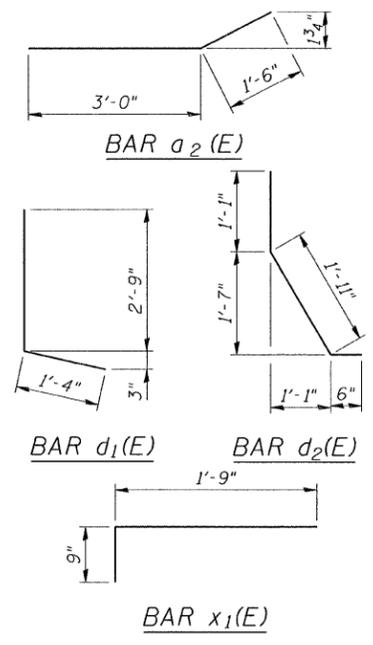
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CONCRETE REMOVAL @ EXIST. NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN



CONCRETE REMOVAL @ EXIST. NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN



BILL OF MATERIAL

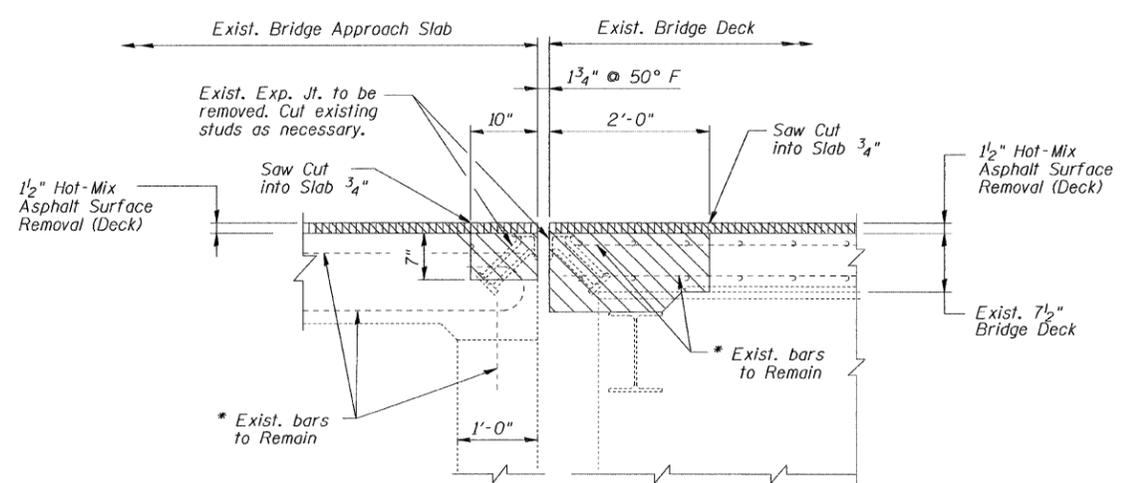
Bar	No.	Size	Length	Shape
a ₁ (E)	20	#5	30'-0"	—
a ₂ (E)	16	#6	4'-6"	—
a ₁₀ (E)	16	#5	2'-0"	—
d ₁ (E)	20	#4	4'-1"	L
d ₂ (E)	20	#5	3'-6"	L
x ₁ (E)	54	#5	2'-6"	L
Concrete Removal			Cu. Yd.	6.6
Concrete Superstructure			Cu. Yd.	7.0
Protective Coat			Sq. Yd.	16
Reinforcement Bars, Epoxy Coated			Pound	1,040

LEGEND

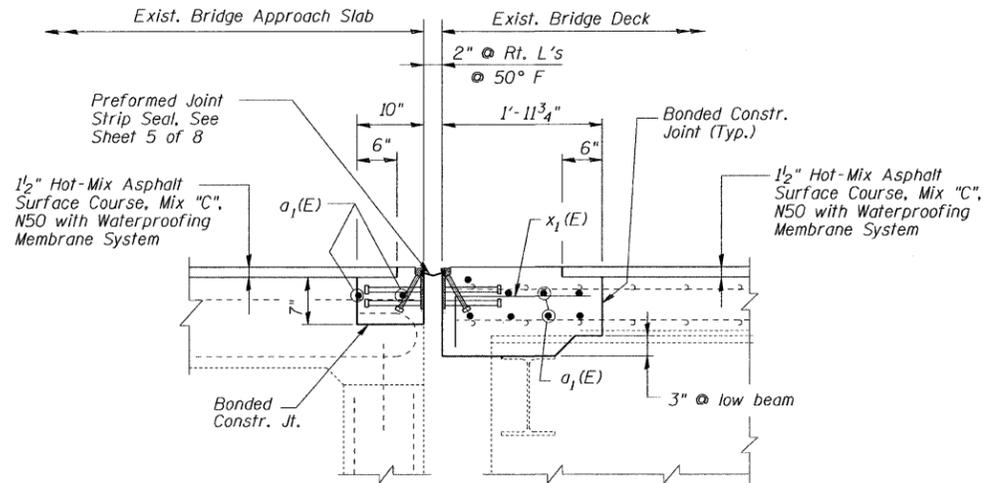
- Concrete Removal

NOTES:

- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal.
- For Sections C-C and E-E, See Sheet 4 of 8.



SECTION A-A



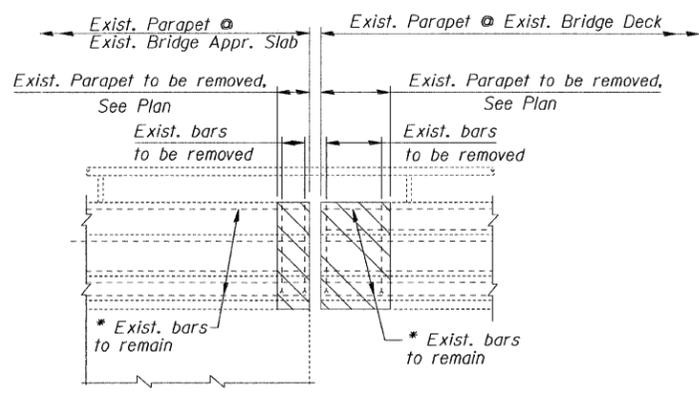
SECTION B-B

Note:
Dimensions are based on a Rolled Rail Strip Seal Joint.
If the Contractor elects to use the Welded Rail Strip Seal Joint, deck dimensions may require adjustments to satisfy the details on Base Sheet EJ-SSJ as shown on Sheet 5 of 8.

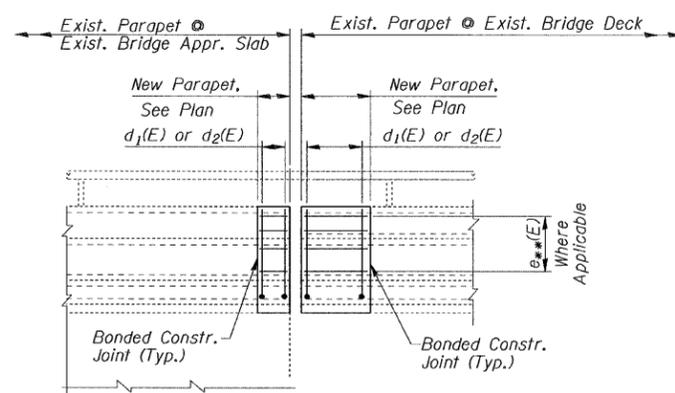
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

JOINT DETAILS
C.H. 25 (286th STREET) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
ROCK ISLAND COUNTY
STATION 2008+40.54
S.N. 081-0126
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

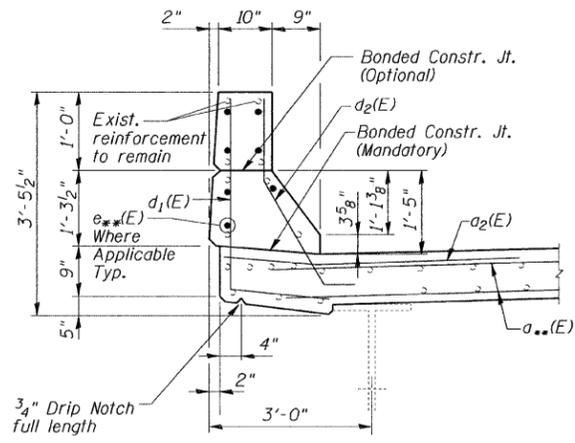
n:\jobs\2008\pilot\2001-4014-09\081-0126\cn26.50\th ave.\00\081-0126-S13.dgn 12/8/2008



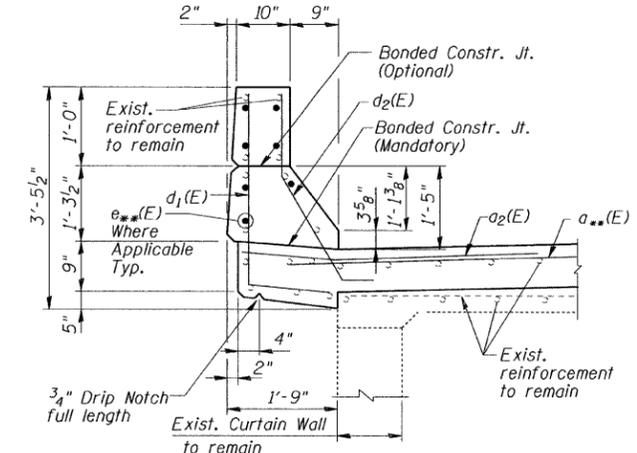
CONCRETE REMOVAL @
EXISTING PARAPET ELEVATION



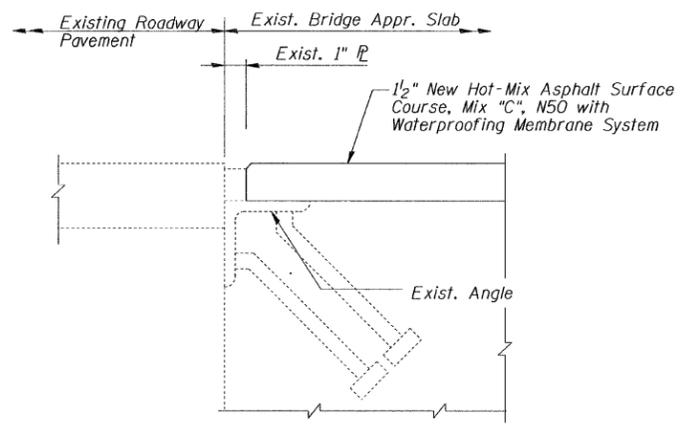
CONCRETE REPLACEMENT @
EXISTING PARAPET ELEVATION



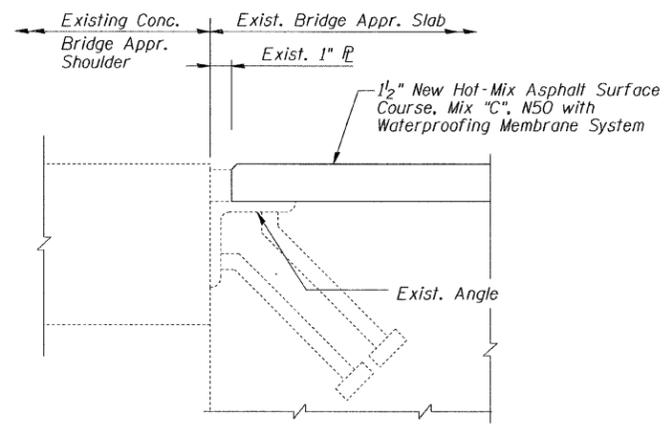
SECTION THRU NEW PARAPET
AT EXISTING BRIDGE DECK



SECTION THRU NEW PARAPET AT
EXISTING APPROACH SLAB



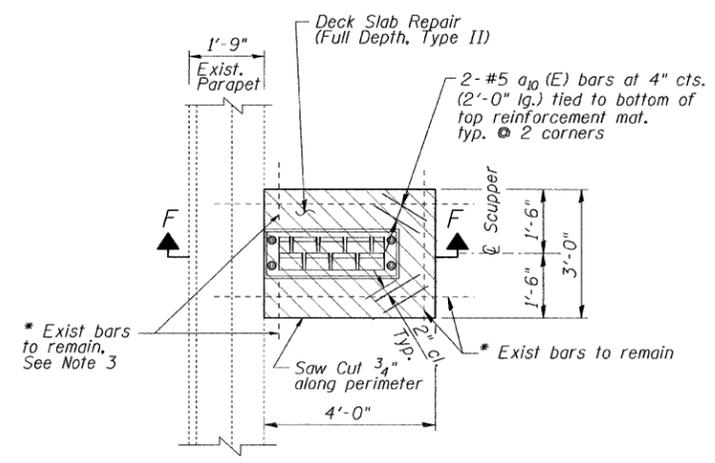
SECTION C-C



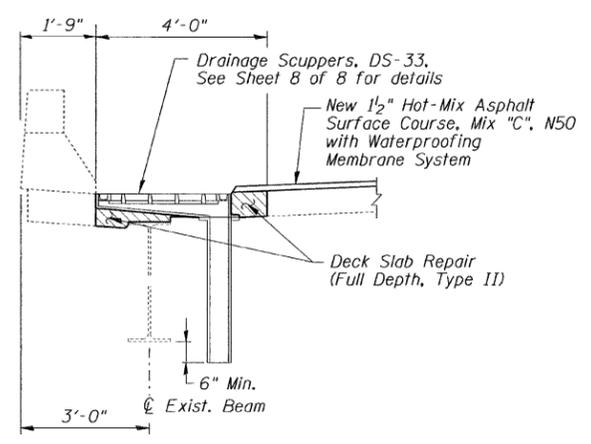
SECTION E-E

NOTES:

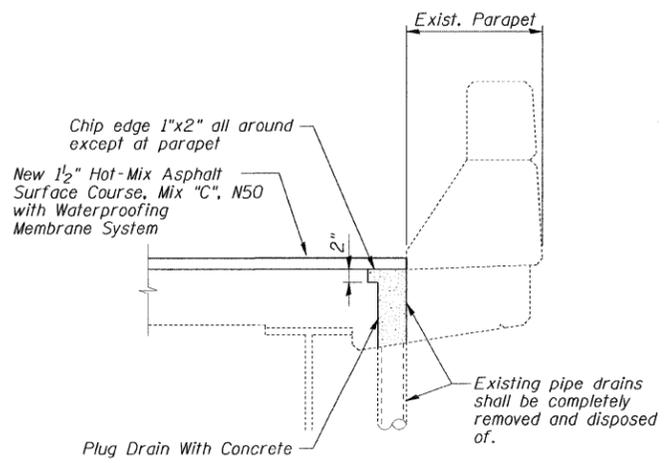
- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Cut to fit as required. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal or Deck Slab Repair (Full Depth, Type II).
- ** See Plan on "Joint Details" sheet for bar designation.
- Cut exist. longitudinal reinforcement to clear drainage scupper.
- Hatched areas indicate concrete sections to be removed and replaced. Perimeters of concrete removal areas shall be saw cut 3/4 inch prior to the removal of concrete.



NEW SCUPPER PLAN



SECTION F-F



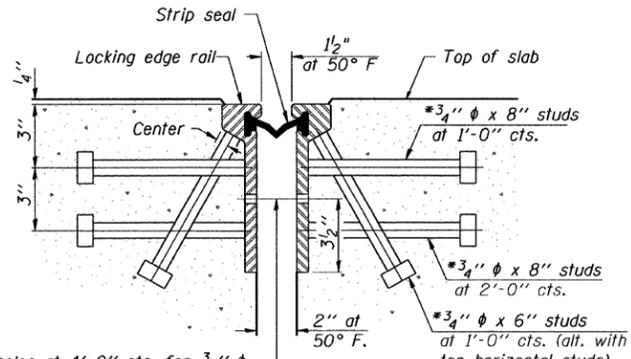
DRAIN ELIMINATION DETAIL

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

PARAPET & DECK SECTIONS & DETAILS
 C.H. 25 (286th STREET) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 ROCK ISLAND COUNTY
 STATION 2008+40.54
 S.N. 081-0126
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

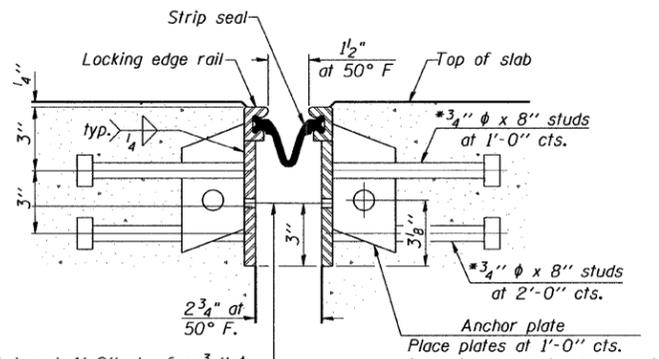
n:\jobs\2008\pilot\2001-4014\09\081-0126\ch26_50th_ave.\00\081-0126-S14.dgn
 12/8/2008

*Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT

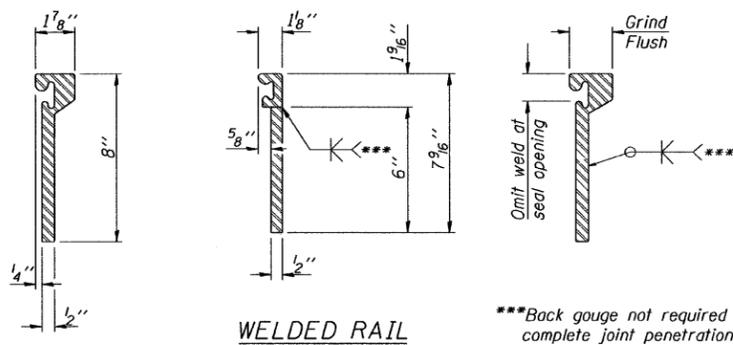


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU WELDED RAIL JOINT

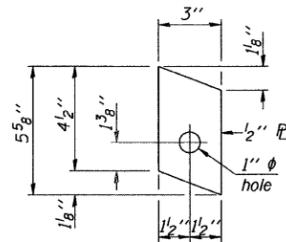
Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

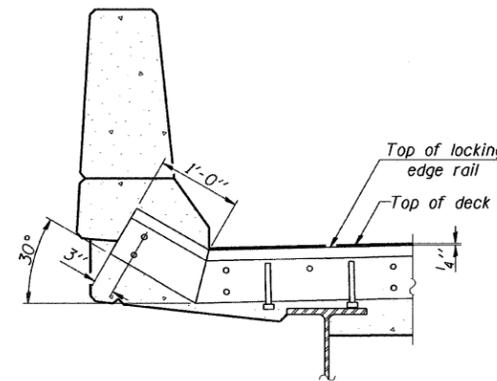


WELDED RAIL

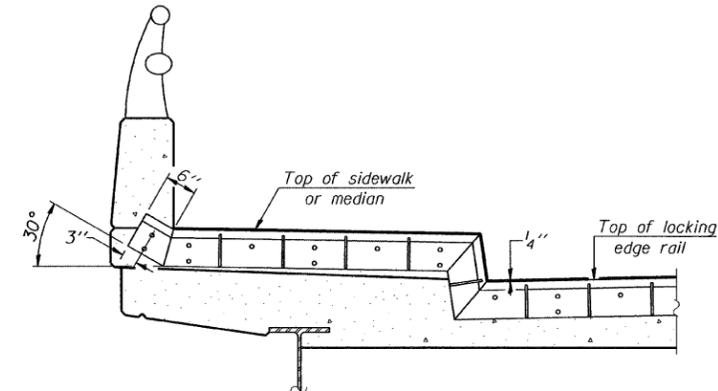
***Back gouge not required if complete joint penetration is verified by mock-up.



ANCHOR PLATE (for welded rail)



AT PARAPET



AT SIDEWALK OR MEDIAN

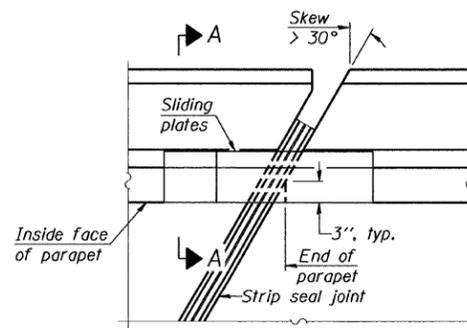
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

ROLLED EXTRUDED RAIL

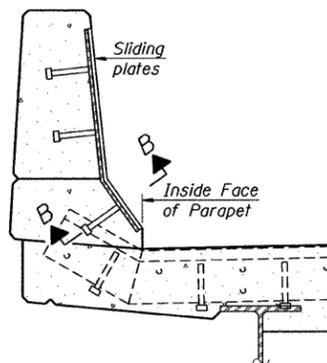
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

LOCKING EDGE RAILS



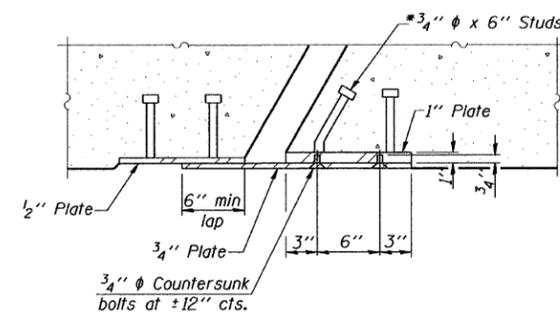
PLAN



SECTION A-A

POINT BLOCK DETAILS (for skews > 30°)

TYPICAL END TREATMENTS



SECTION B-B

BILL OF MATERIAL

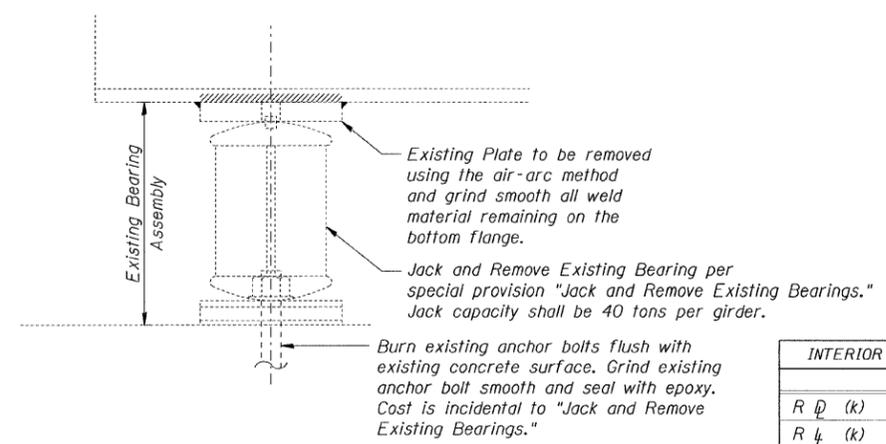
Item	Unit	Total
Preformed Joint Strip Seal	Foot	61

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

EJ-SSJ 5-16-08

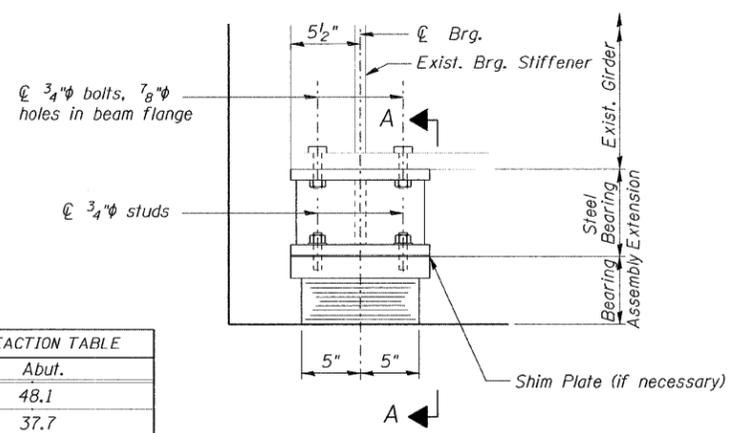
PREFORMED JOINT STRIP SEAL
C.H. 25 (286th STREET) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
ROCK ISLAND COUNTY
STATION 2008+40.54
S.N. 081-0126

DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

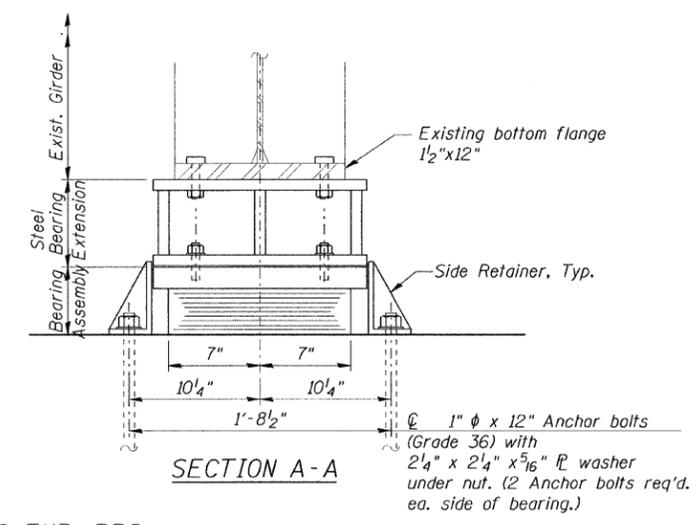


EXISTING BEARING REMOVAL DETAIL

INTERIOR GIRDER REACTION TABLE	
	Abut.
R _D (k)	48.1
R _L (k)	37.7
Imp. (k)	8.4
R _{Total} (k)	94.2

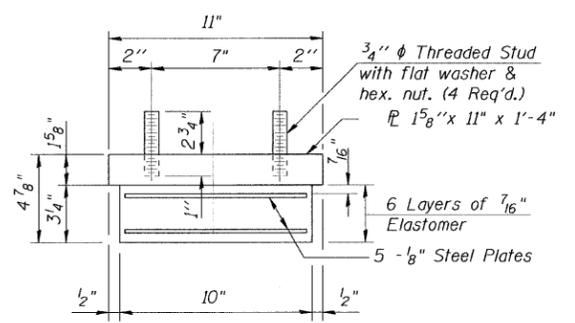


ELEVATION AT ABUT.



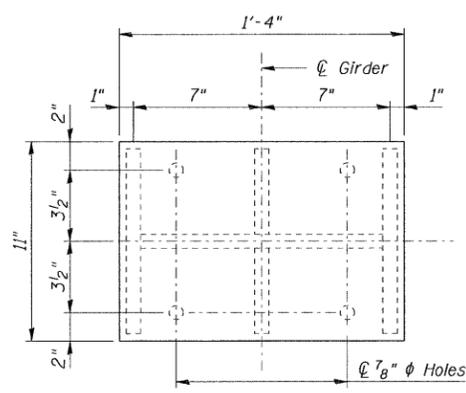
SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.

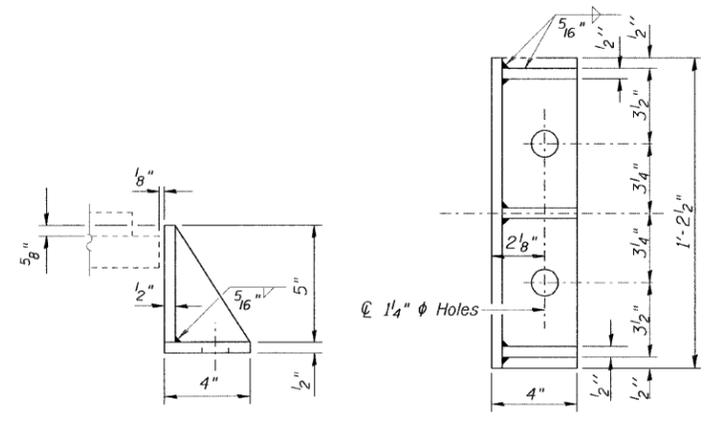


BEARING ASSEMBLY

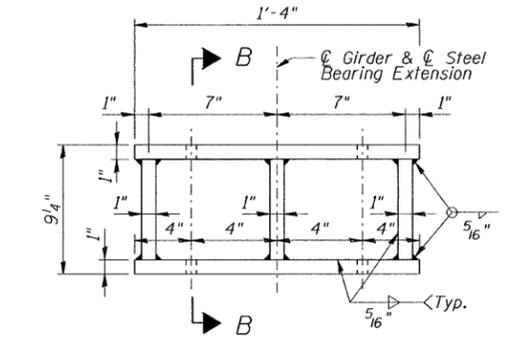
Note: Shim plates shall not be placed under Bearing Assembly.



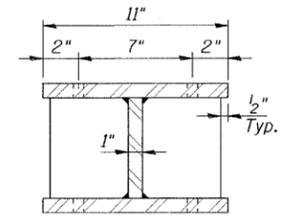
PLAN STEEL BEARING EXTENSION



SIDE RETAINER
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



ELEVATION STEEL BEARING EXTENSION



SECTION B-B

STEEL BEARING EXTENSION

Notes:
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers, shall be included in the cost of Elastomeric Bearing Assembly, Type I.
The structural steel plates of the Bearing Assembly and the Steel Bearing Extension shall conform to the requirements of AASHTO M 270 Grade 36.
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.
Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
New Steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

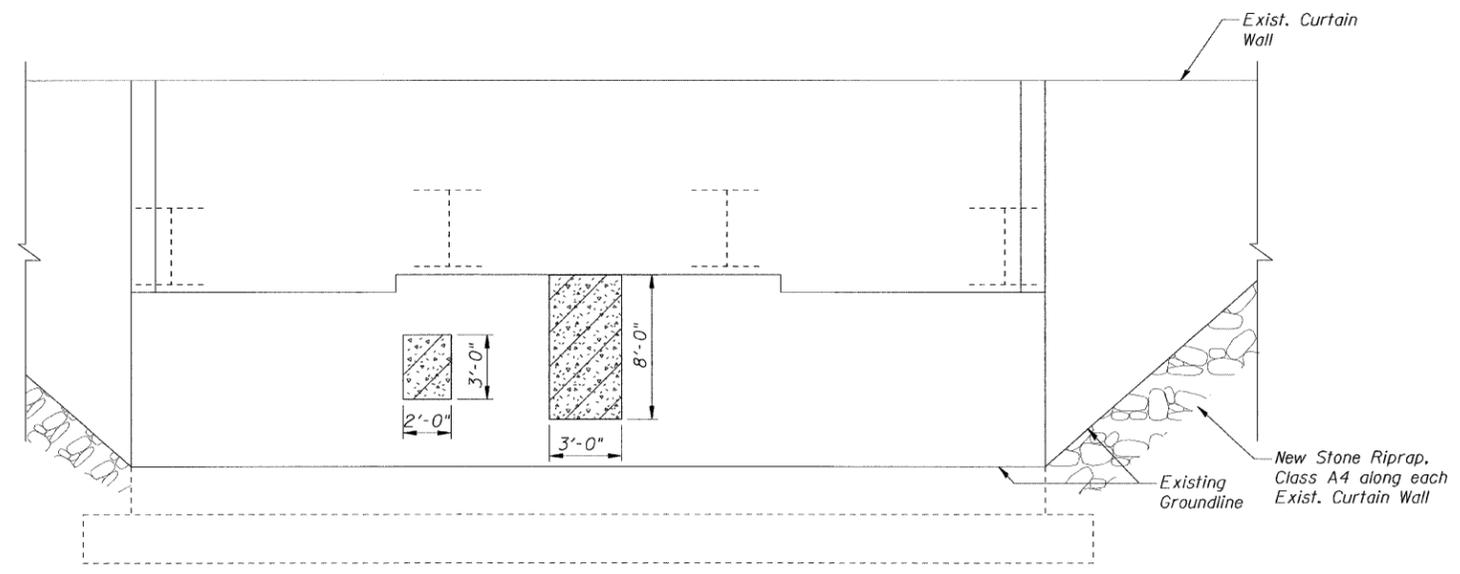
BILL OF MATERIAL

Item	Unit	Total
Jack and Remove Existing Bearings	Each	8
Elastomeric Bearing Assembly, Type I	Each	8
Anchor Bolts, 1"	Each	32
Furnishing and Erecting Structural Steel	Pound	1,520

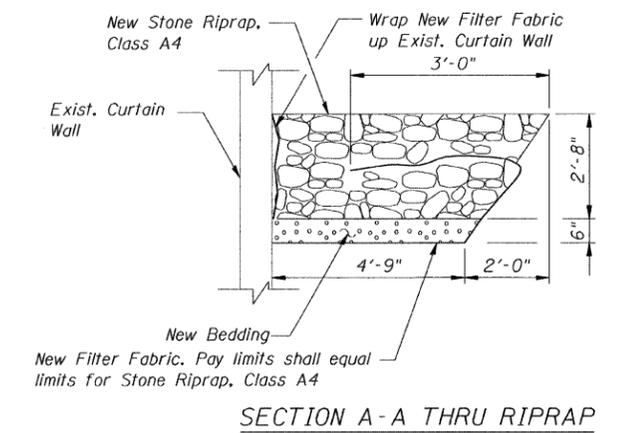
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

TYPE I ELASTOMERIC BEARING
C.H. 25 (286th STREET) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
ROCK ISLAND COUNTY
STATION 2008+40.54
S.N. 081-0126
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

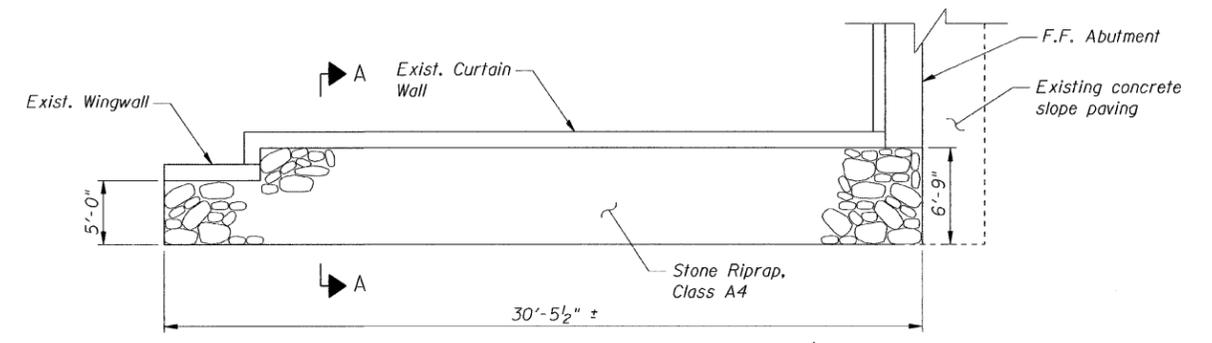
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12/8/2008



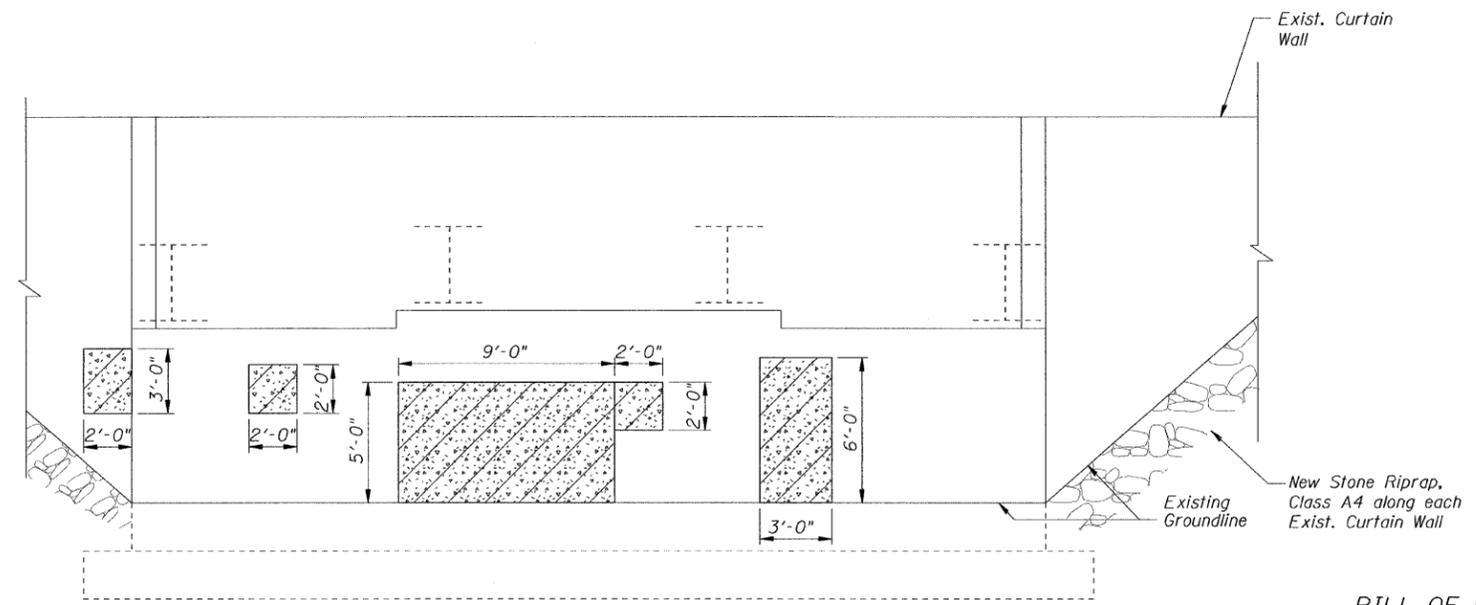
EXIST. SOUTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking South)



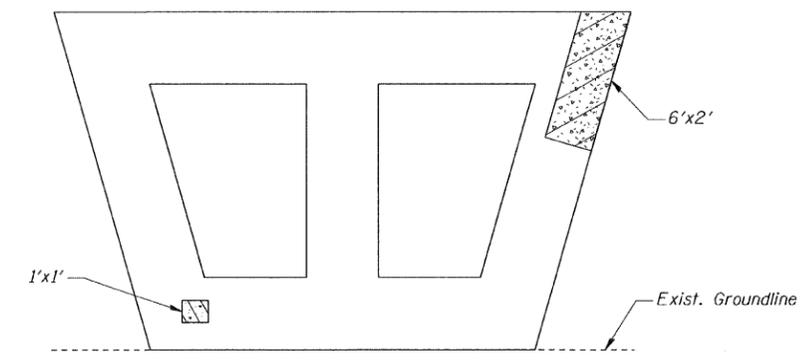
SECTION A-A THRU RIPRAP



RIPRAP PLAN AT ABUTMENTS
(Exist. North Abutment, west side shown. Other locations similar)



EXIST. NORTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking North)



EXISTING PIER ELEVATION
(Looking South)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Stone Riprap, Class A4	Sq. Yd.	96
Filter Fabric	Sq. Yd.	96
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	150

LEGEND

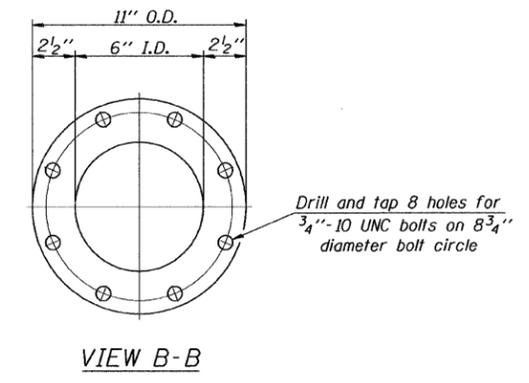
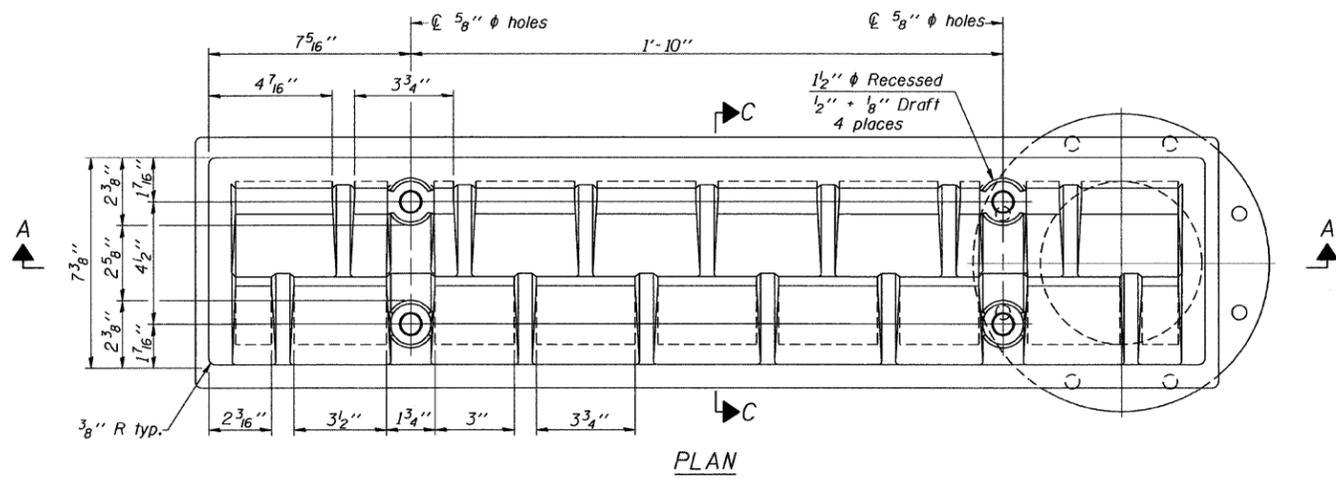
Structural Repair of Concrete (Depth Equal to or Less than 5")

Note:
Quantity for Structural Repair of Concrete has been increased to include additional repair areas not apparent at the time of the inspection. The actual repair areas will be determined by the Resident Engineer. The Contractor will be paid for the quantity furnished.

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

SUBSTRUCTURE REPAIR & RIPRAP DETAILS
C.H. 25 (286th STREET) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
ROCK ISLAND COUNTY
STATION 2008+40.54
S.N. 081-0126
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

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12/8/2008



Notes:

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.

Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.

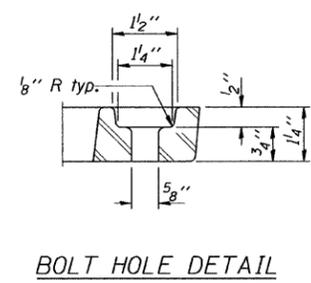
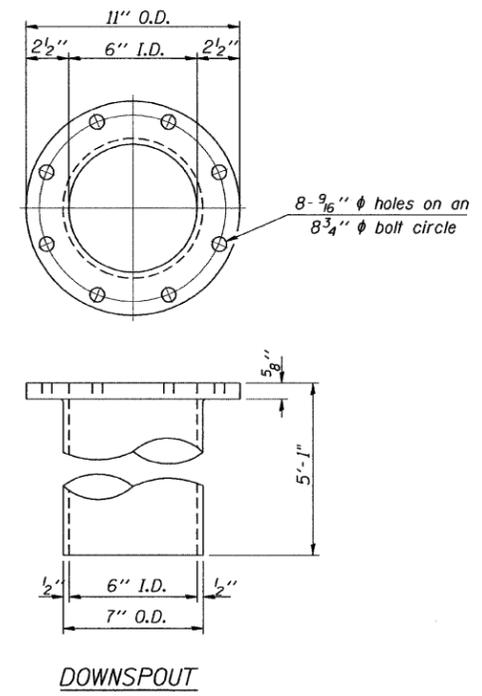
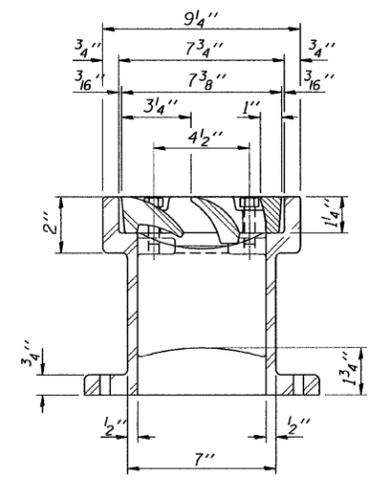
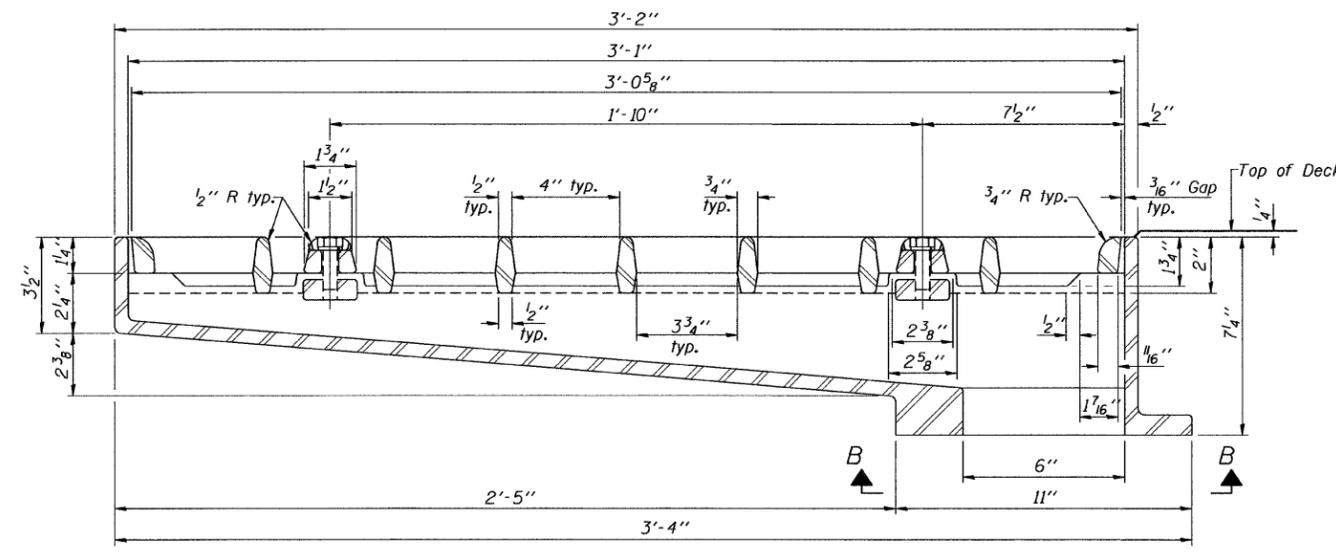
Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.

As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.

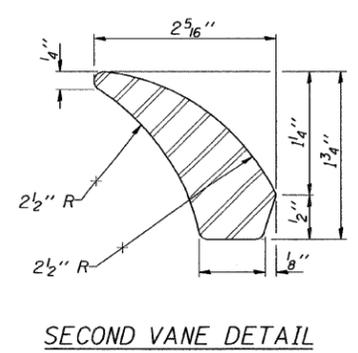
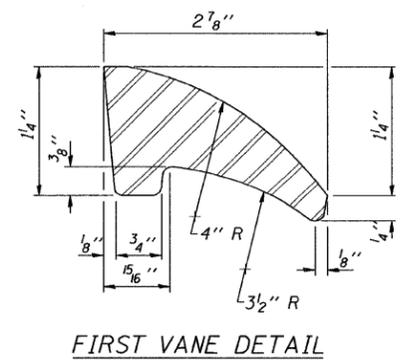
Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M 111.

The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-33.



See sheet of for scupper location relative to parapet.



BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-33	Each	4

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

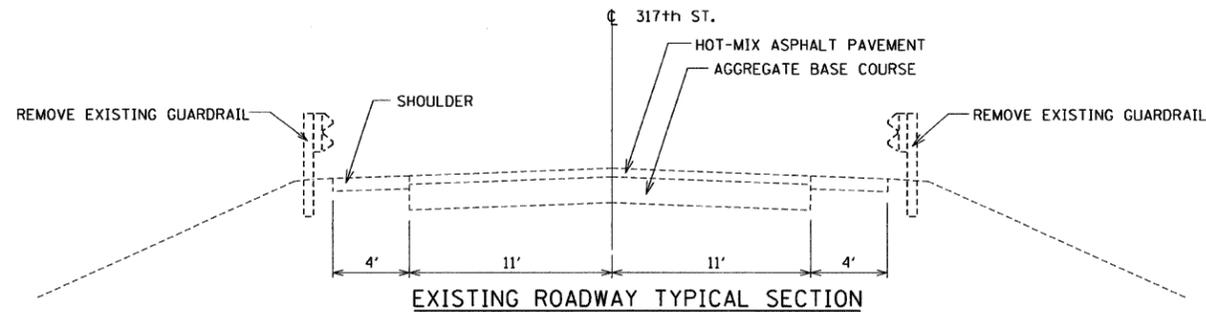
DS-33 5-16-08

DRAINAGE SCUPPER, DS-33
 C.H. 25 (286th STREET) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 ROCK ISLAND COUNTY
 STATION 2008+40.54
 S.N. 081-0126
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

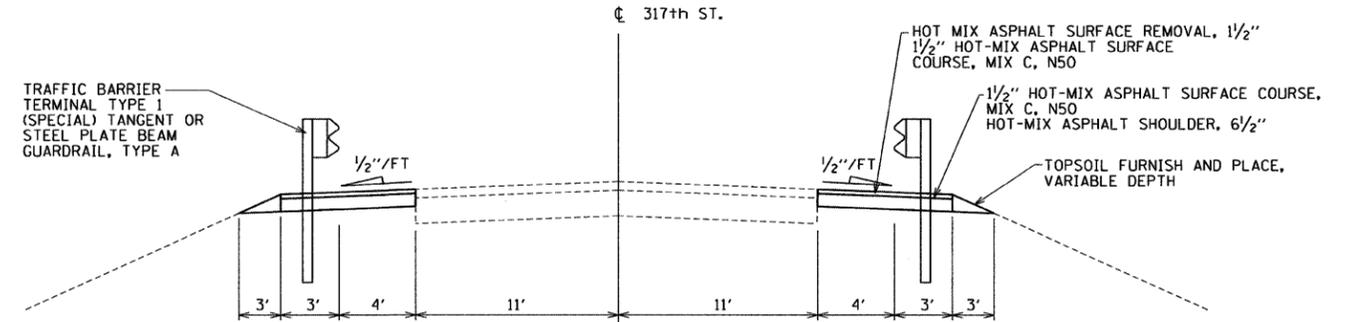
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F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	19
STA. 2127+74.02		TO STA. 2133+81.69		
FED. AID PROJECT	ILLINOIS	FED. ROAD DIST. NO.		

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE

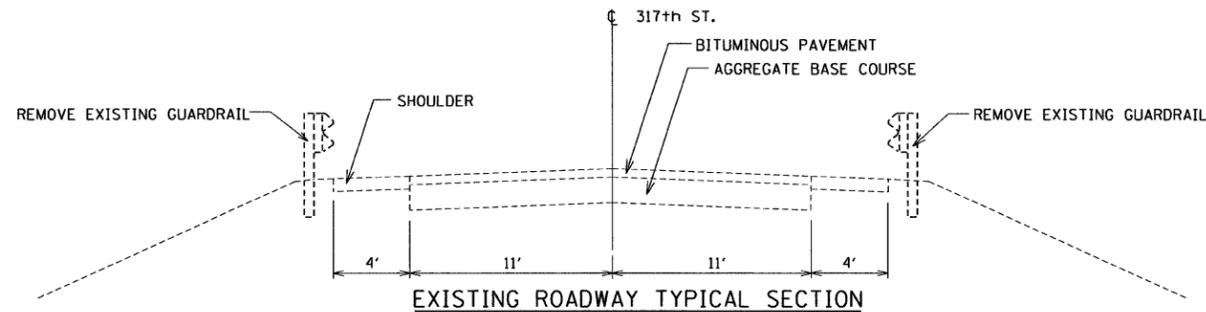


EXISTING ROADWAY TYPICAL SECTION
 STA. 2127+74.02 TO 2128+78.86
 STA. 2132+76.86 TO 2133+81.69

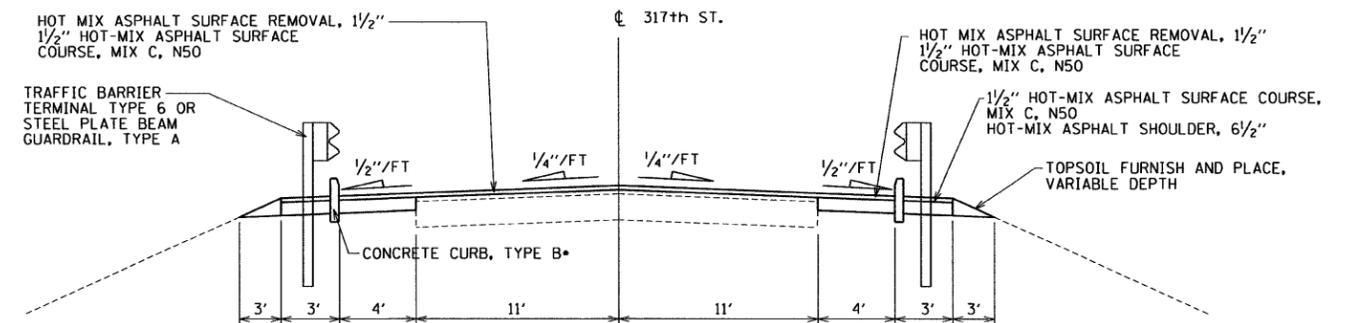


PROPOSED ROADWAY TYPICAL SECTION

STA. 2127+74.02 TO 2128+78.86
 STA. 2132+76.86 TO 2133+81.69
 (LOOKING SOUTH)



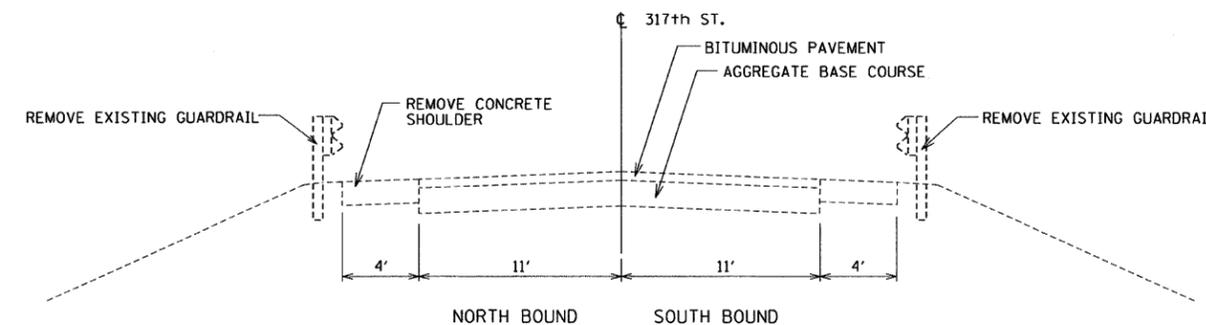
EXISTING ROADWAY TYPICAL SECTION
 STA 2128+78.86 TO 2129+07.10 LT
 STA 2128+78.86 TO 2129+23.44 RT
 STA 2132+31.60 TO 2132+76.86 LT
 STA 2132+48.61 TO 2132+76.86 RT



PROPOSED ROADWAY TYPICAL SECTION

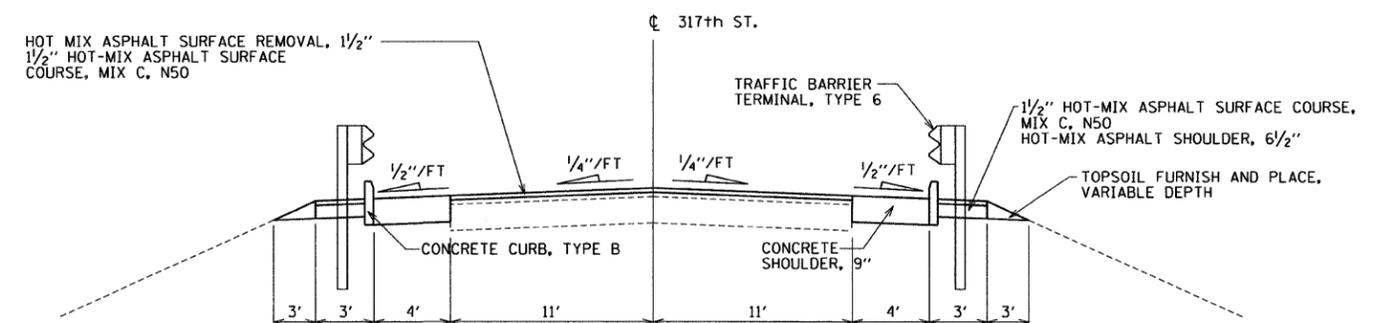
STA 2128+78.86 TO 2129+07.10 LT
 STA 2128+78.86 TO 2129+23.44 RT
 STA 2132+31.60 TO 2132+76.86 LT
 STA 2132+48.61 TO 2132+76.86 RT

• STA 2128+83.67 TO 2129+07.10 LT
 STA 2129+01.26 TO 2129+23.44 RT
 STA 2132+31.60 TO 2132+54.46 LT
 STA 2132+48.61 TO 2132+72.04 RT



EXISTING ROADWAY TYPICAL SECTION

NORTH BOUND SOUTH BOUND
 STA 2129+07.10 TO 2129+28.86 LT
 STA 2129+23.44 TO 2129+28.86 RT
 STA 2132+26.86 TO 2132+31.60 LT
 STA 2132+26.86 TO 2132+48.61 RT



PROPOSED ROADWAY TYPICAL SECTION

STA 2129+07.10 TO 2129+28.86 LT
 STA 2129+23.44 TO 2129+28.86 RT
 STA 2132+26.86 TO 2132+31.60 LT
 STA 2132+26.86 TO 2132+48.61 RT

HOT-MIX ASPHALT SURFACE COURSE 112 LBS/IN THICK/SO YD
 HOT-MIX ASPHALT SHOULDER 112 LBS/IN THICK/SO YD
 TEMPORARY EROSION CONTROL SEEDING 100 LB/ACRE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 49 (317th STREET) BRIDGE OVER I-88
 STRUCTURE NO.: 081-0132
 BRIDGE REHABILITATION

TYPICAL SECTIONS

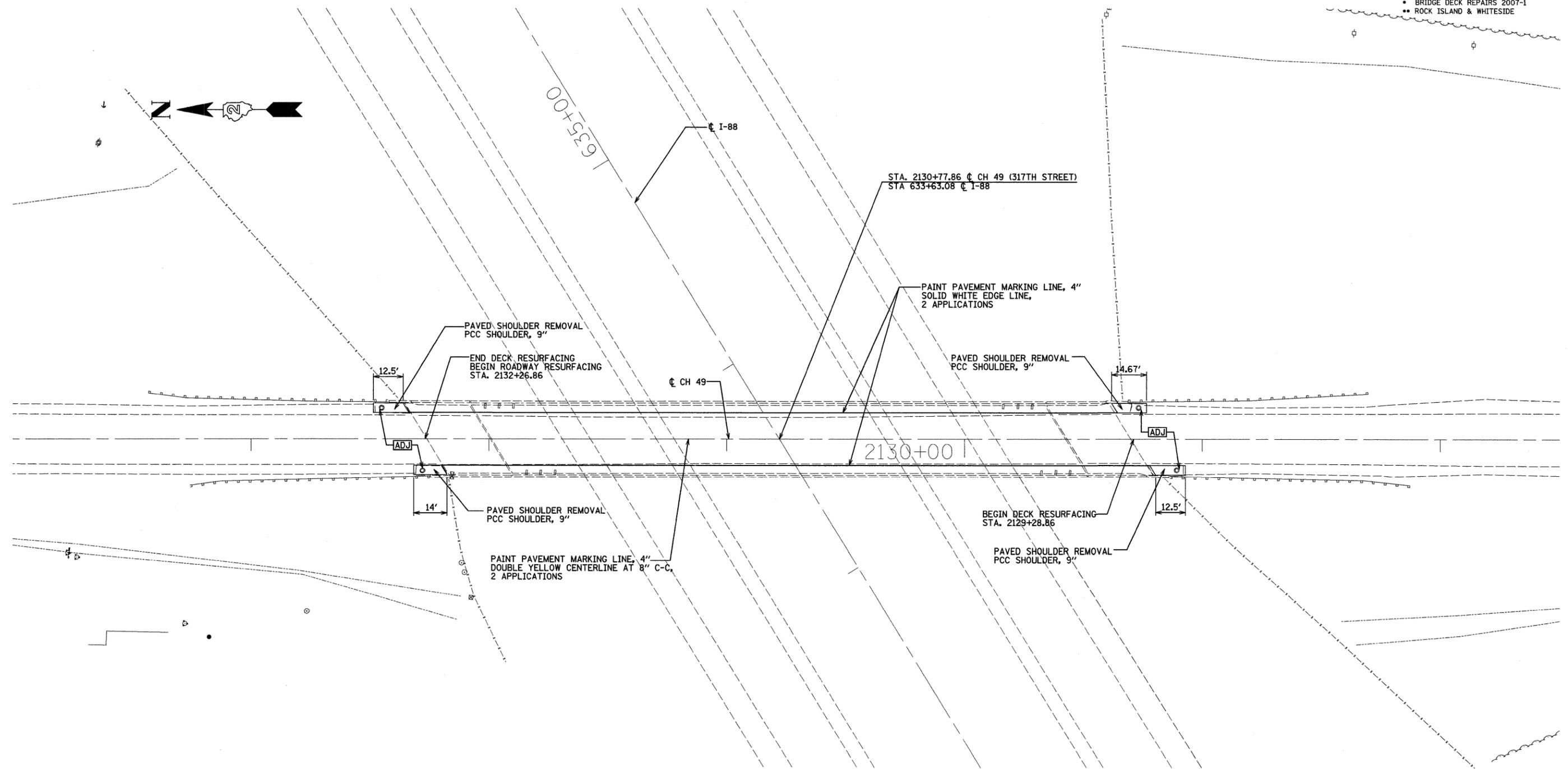
SCALE: NTS
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS

CONTRACT NO. 64D24

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	20
STA. 2127+74.02 TO STA. 2133+81.69				
FED. AID PROJECT ILLINOIS FED. ROAD DIST. NO.				

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE



\$FILE\$ SOT JOM ID D3262M09

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CH 49 (317th STREET) BRIDGE OVER I-88
 STRUCTURE NO.: 081-0132
 BRIDGE REHABILITATION

PROPOSED ROADWAY PLAN

SCALE: 1"=20'
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS

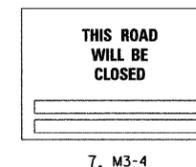
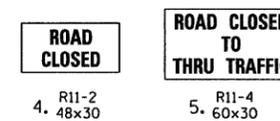
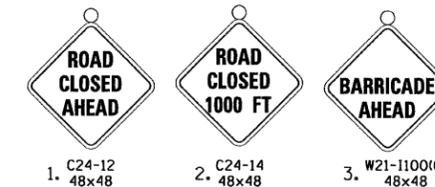
CONTRACT NO. 64D24

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	21
STA. 2128+78.86		TO STA. 2132+76.86		
FED. AID PROJECT		ILLINOIS	FED. ROAD DIST. NO.	

- * BRIDGE DECK REPAIRS 2007-1
- ** ROCK ISLAND & WHITESIDE

MAINTENANCE OF TRAFFIC

1. THE DEPARTMENT SHALL BE NOTIFIED 72 HOURS PRIOR TO ROAD CLOSURE AND IMMEDIATELY AFTER THE ROAD HAS BEEN REOPENED.
2. ROCK ISLAND COUNTY SHALL BE NOTIFIED AT LEAST TWO WEEKS PRIOR TO CLOSING THE ROAD.
3. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL TEMPORARY SIGN SUPPORTS. AFTER REMOVING THE SUPPORTS, THE CONTRACTOR SHALL FILL HOLES (IF ANY) AND RESTORE THE GROUND TO ITS ORIGINAL CONDITION AND ELEVATION. PLACING OF GRAVEL, SOD, OR SEED SHALL BE INCLUDED IN THE UNIT BID PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE".
4. ALL WARNING SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND ON AN ORANGE BACKGROUND, UNLESS OTHERWISE NOTED.
5. REFER TO DISTRICT STANDARD 40.1 FIR SIGNING REQUIREMENTS
6. EACH TYPE III BARRICADE SHALL BE EQUIPPED WITH TWO FLASHING AMBER LIGHTS.
7. THE CONTRACTOR SHALL ERECT SIGN NO. 7 AT LEAST 2 WEEKS IN ADVANCE OF THE CLOSURE.
8. ALL ENTRANCES ON THE CLOSED PORTION OF 50TH AVENUE SHALL BE OPEN FOR TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD.
9. THE WORK SHALL BE PAID FOR AT THE LUMP SUM PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE"

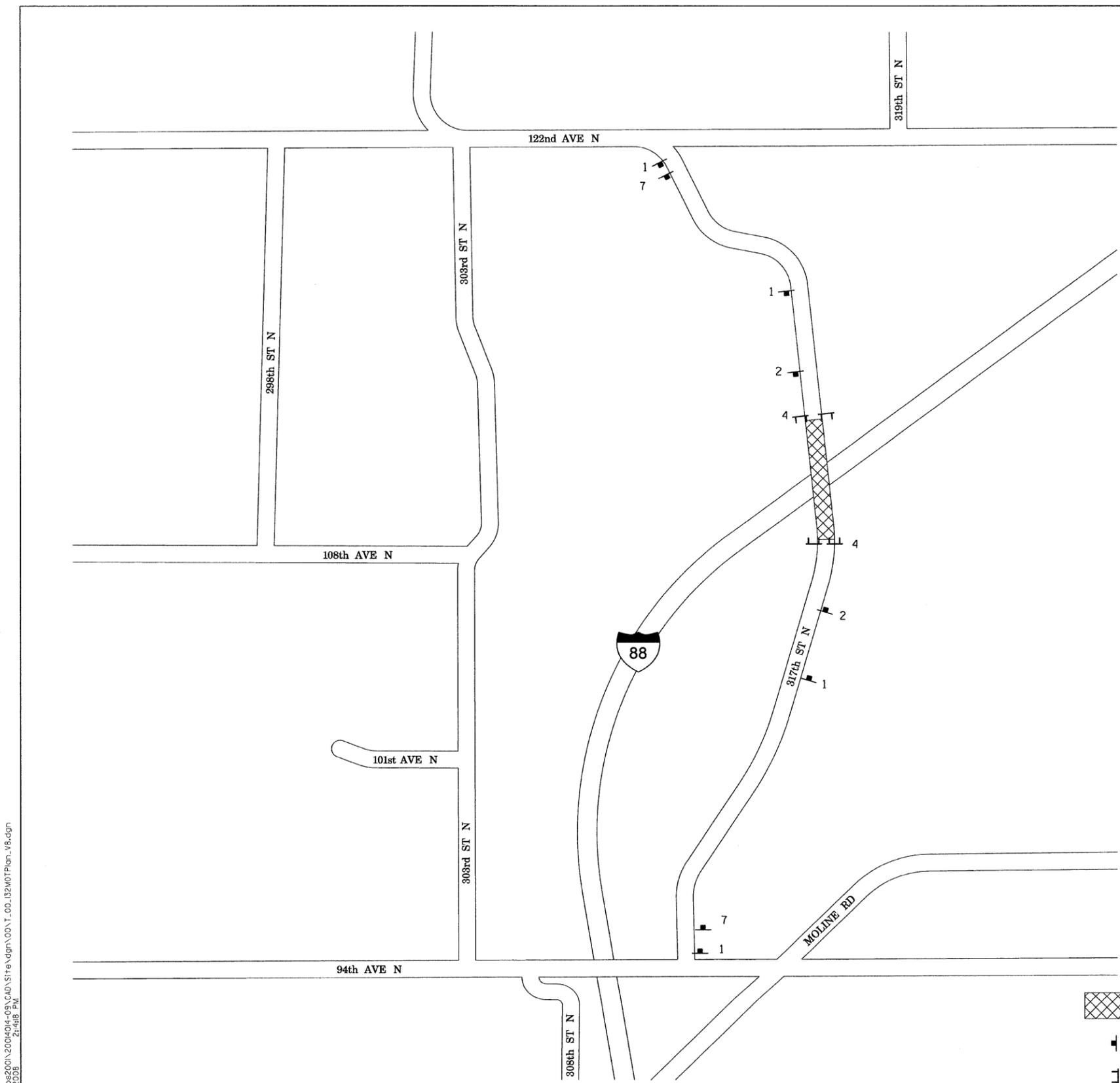


LEGEND

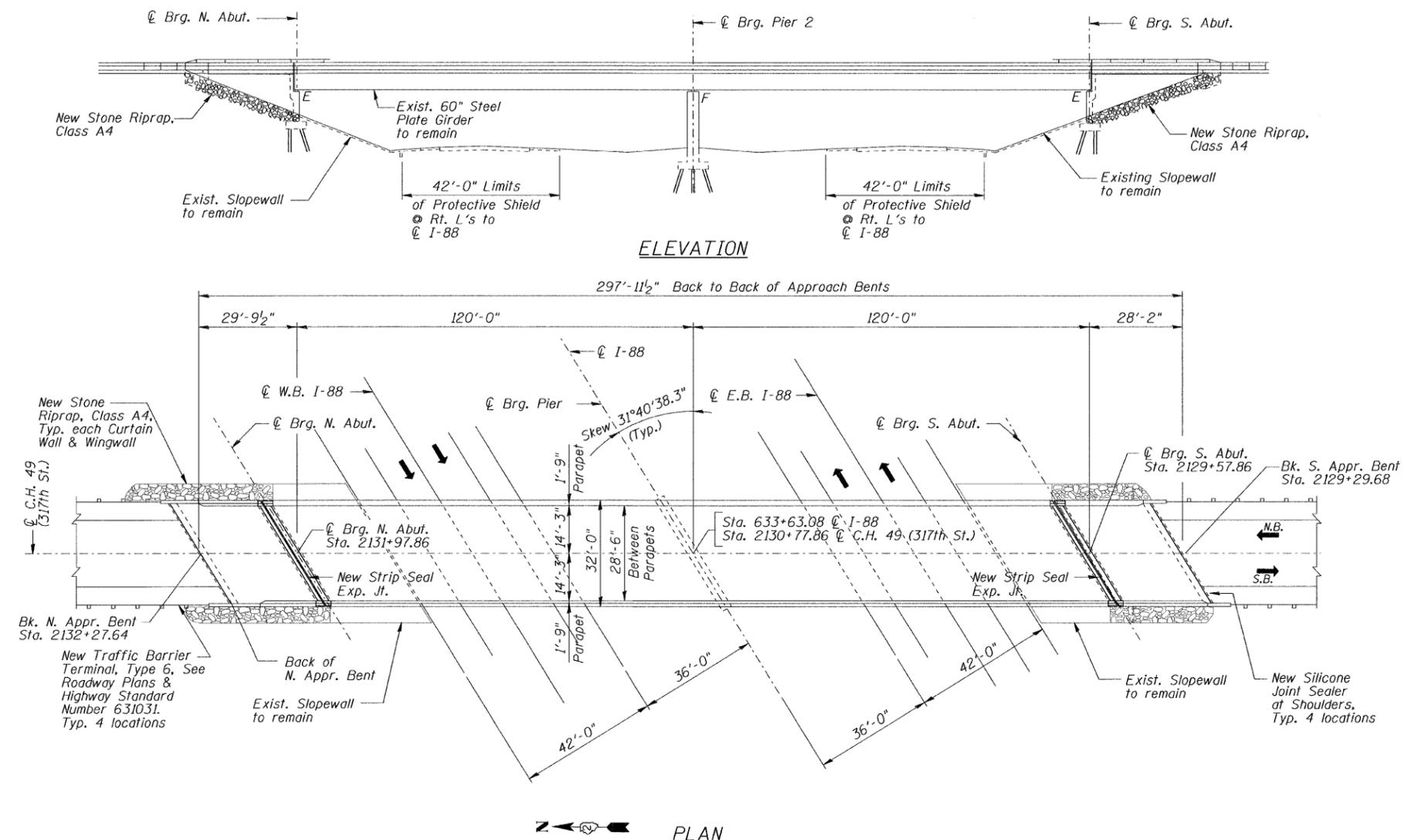
- PROJECT AREA
- SIGN
- TYPE III BARRICADES WITH FLASHERS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 49 (317th STREET) BRIDGE OVER I-88
 STRUCTURE NO.: 081-0132
 BRIDGE REHABILITATION
MOT PLAN
 SCALE: NTS
 DATE 12/09/08
 DRAWN BY EF
 CHECKED BY RS



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GENERAL NOTES

All structural steel shall be AASHTO M270 Grade 50.
 No field welding is permitted except as specified in the contract documents.
 Reinforcement bars shall conform to the requirements of ASTM A706 Gr 60. See Special Provisions.
 Reinforcement bars designated (E) shall be epoxy coated.
 Prior to pouring the new concrete deck at the expansion joint, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
 As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that cannot be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for work.
 Protective Coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
 Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.
 All structural steel at expansion joints shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Preformed Joint Strip Seal.

Quantities for deck slab repair are approximate. Quantity of Deck Slab Repair (Full Depth, Type I) and Deck Slab Repair (Full Depth, Type II) are each assumed to be equal to 0.65% of the deck area. Quantity of Deck Slab Repair (Partial) is assumed to be equal to 15.0% of the deck area. Locations will be determined by the Resident Engineer following removal of the Hot-Mix Asphalt Surface Course. Actual repair locations shall be shown on the as-built plans. Contractor will be paid for the quantity furnished.

Cleaning and painting of the existing structural steel shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All existing steel shall be cleaned per Near White Blast Cleaning - SSPC-SP10. All existing steel shall be painted according to the requirements of Paint System 1 - OZ/E/U. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Blue, Munsell No. 10B 3/6.

A minimum of 2 air monitors will be required to monitor abrasive blasting operations at this site, see special provision for "Containment and Disposal of Lead Paint Cleaning Residues."

The SSPC QP1 & QP2 certifications will be required for this contract.
 Existing plans are available from the District to determine painting quantities.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
STONE RIPRAP, CLASS A4	SQ. YD.	115
FILTER FABRIC	SQ. YD.	115
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	79
HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ. YD.	944
CONCRETE REMOVAL	CU. YD.	9.1
PROTECTIVE SHIELD	SQ. YD.	286
CONCRETE SUPERSTRUCTURE	CU. YD.	9.9
PROTECTIVE COAT	SQ. YD.	21
CLEANING AND PAINTING STEEL BRIDGE NO. 2	L. SUM	1
CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 2	L. SUM	1
REINFORCEMENT BARS, EPOXY COATED	POUND	1,360
PREFORMED JOINT STRIP SEAL	FOOT	72
JACK AND REMOVE EXISTING BEARINGS	EACH	8

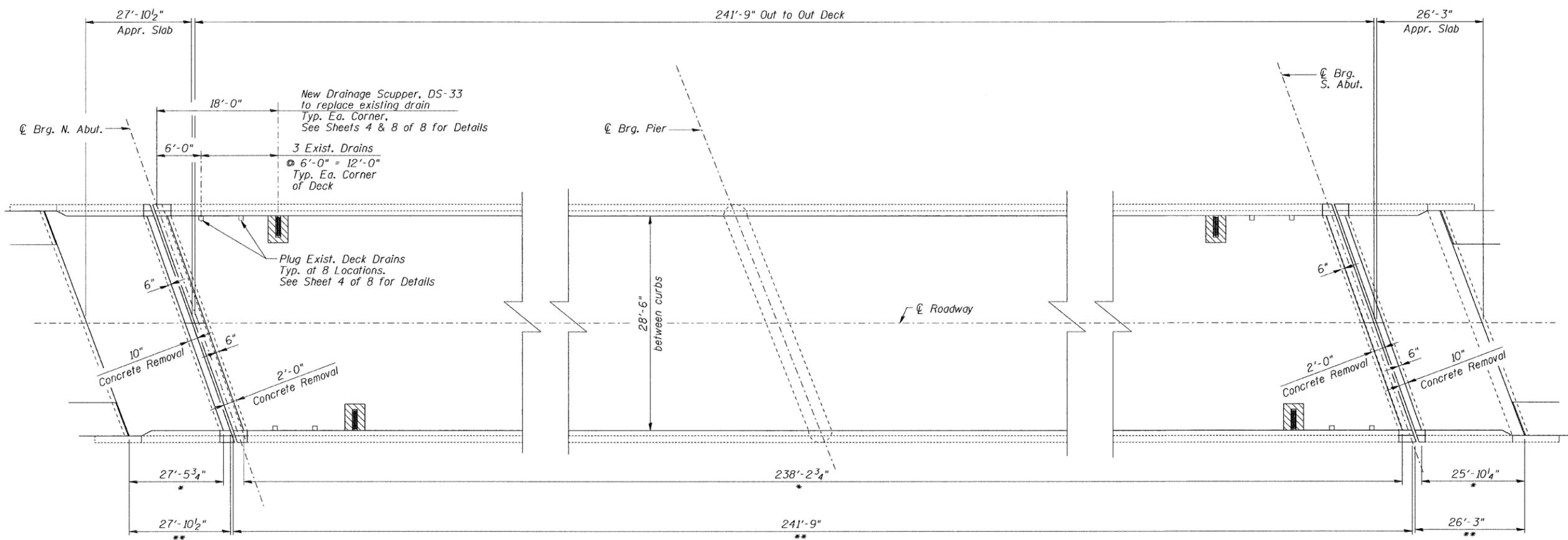
ITEM	UNIT	QUANTITY
ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	8
ANCHOR BOLTS, 1"	EACH	32
FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1,690
PLUG EXISTING DECK DRAINS	EACH	8
SILICONE JOINT SEALER, 1"	FOOT	19
WATERPROOFING MEMBRANE SYSTEM	SQ. YD.	930
DRAINAGE SCUPPERS, DS-33	EACH	4
STRUCTURAL REPAIR OF CONCRETE (DEPTH<5")	SQ. FT.	295
BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 2	L. SUM	1
DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	6
DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	12
DECK SLAB REPAIR (PARTIAL)	SQ YD	128

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

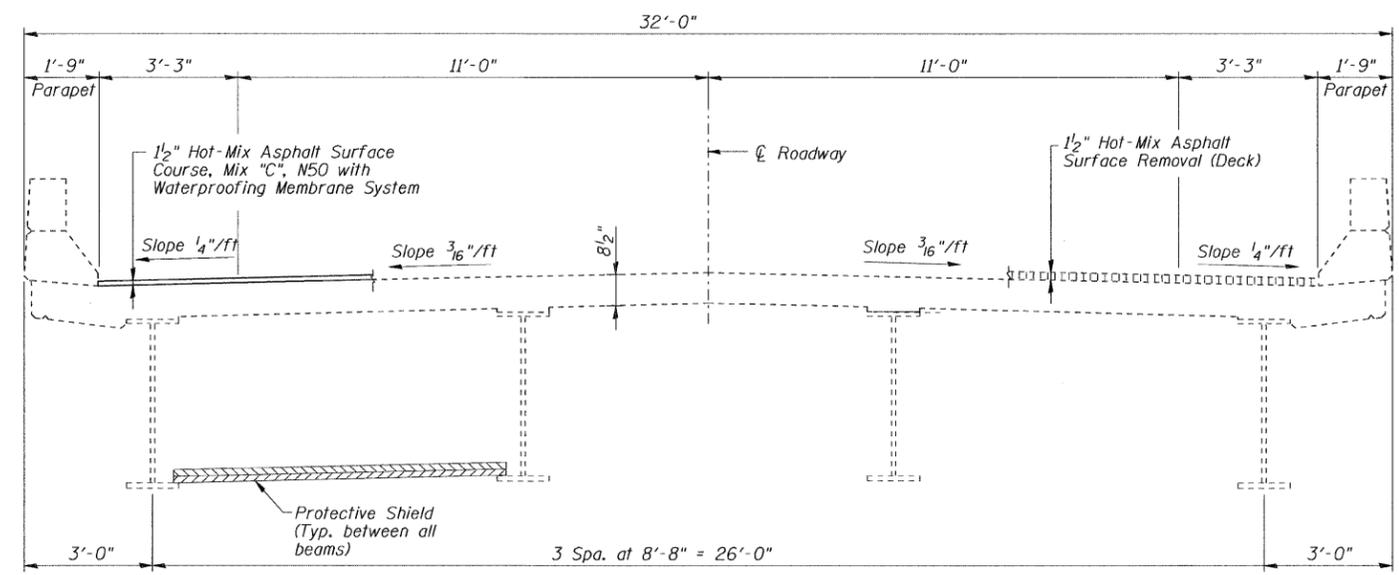


GENERAL PLAN & ELEVATION
 C.H. 49 (317th STREET) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 ROCK ISLAND COUNTY
 STATION 2130+77.86
 S.N. 081-0132
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

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 12/8/2008



PLAN



DECK CROSS SECTION

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Hot Mix Asphalt Surface Course, Mix "C", N50	Ton	79
Hot Mix Asphalt Surface Removal (Deck)	Sq. Yd.	944
Protective Shield	Sq. Yd.	286
Plug Existing Deck Drains	Each	8
Waterproofing Membrane System	Sq. Yd.	930
Drainage Scuppers, DS-33	Each	4

Notes:

- * - Limits of Hot-Mix Asphalt Surface Course, Mix "C", N50 with Waterproofing Membrane System
- ** - Limits of Hot-Mix Asphalt Surface Removal (Deck)

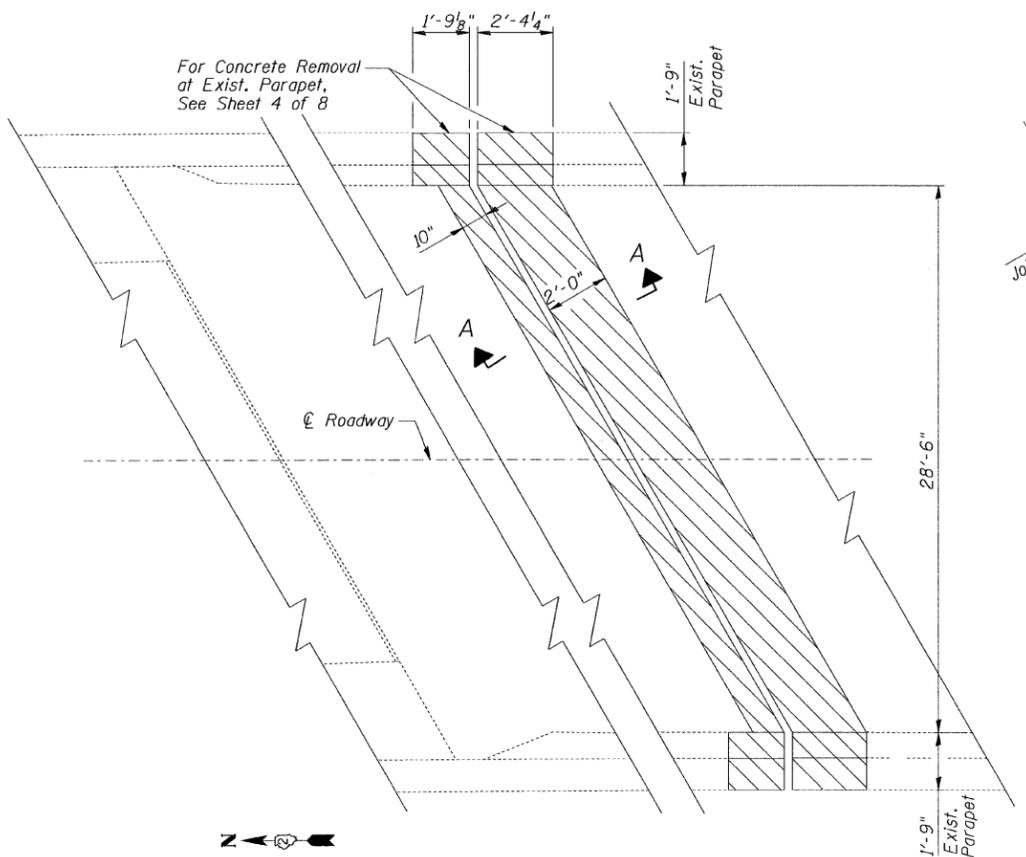
LEGEND

- Deck Slab Repair (Full Depth, Type II)

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

DECK PLAN & CROSS SECTION
 C.H. 49 (317th STREET) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 ROCK ISLAND COUNTY
 STATION 2130+77.86
 S.N. 081-0132
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

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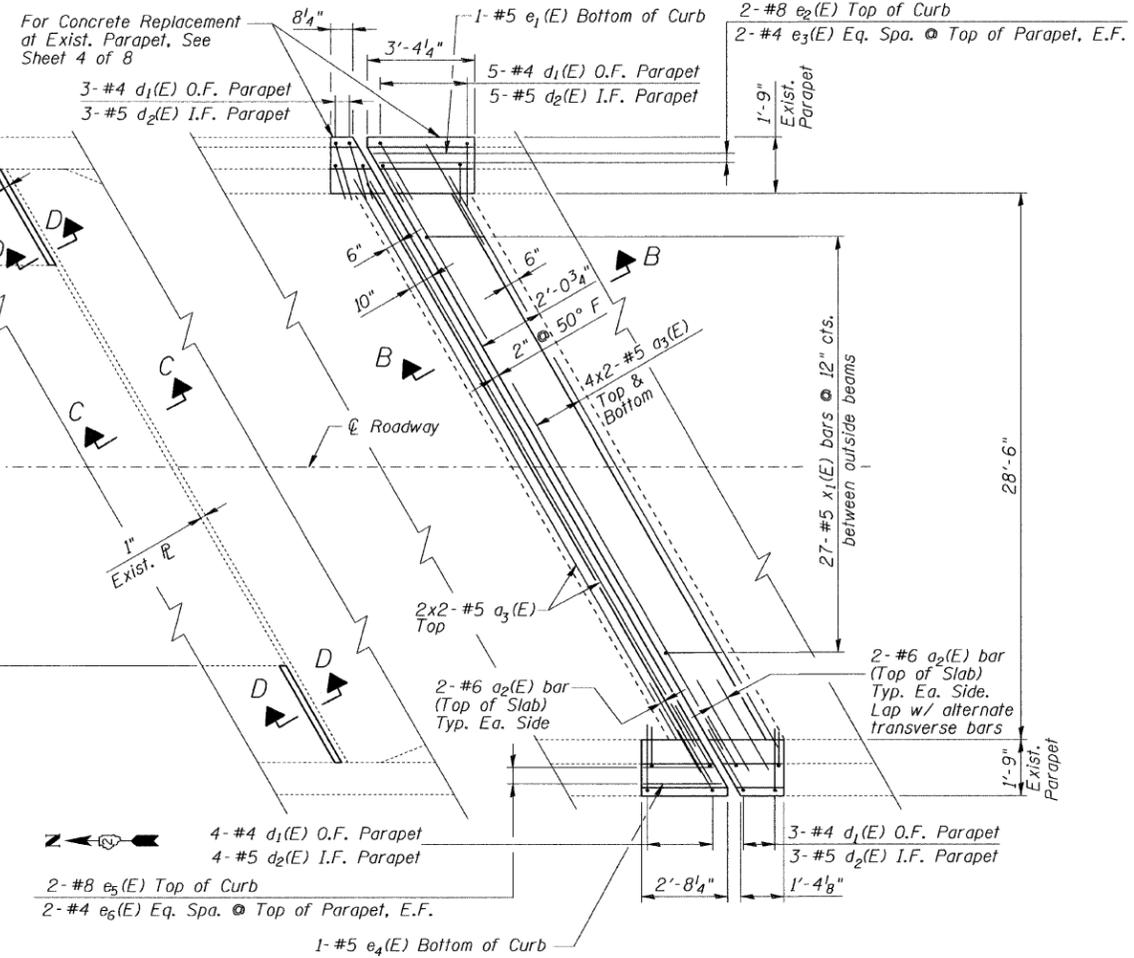
CONCRETE REMOVAL @ EXIST. NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN

NOTES:

- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal.
- For Sections C-C & D-D see Sheet 4 of 8.
- Bars indicated thus 4x2-#5 etc. indicates 4 lines of bars with 2 lengths per line.
- Lap #5 bars 2'-2" min.

LEGEND

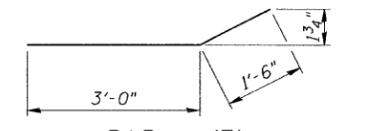
- Concrete Removal



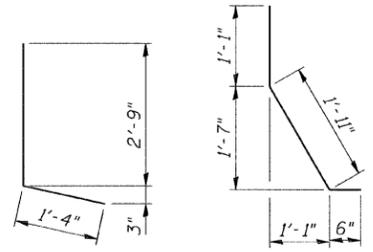
CONCRETE REPLACEMENT @ EXIST. NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₂ (E)	16	#6	4'-6"	—
a ₃ (E)	40	#5	19'-0"	—
a ₁₀ (E)	16	#5	2'-0"	—
d ₁ (E)	30	#4	4'-1"	⌒
d ₂ (E)	30	#5	3'-6"	⌒
e ₁ (E)	2	#5	2'-8"	—
e ₂ (E)	4	#8	2'-8"	—
e ₃ (E)	8	#4	2'-8"	—
e ₄ (E)	2	#5	2'-0"	—
e ₅ (E)	4	#8	2'-0"	—
e ₆ (E)	8	#4	2'-0"	—
x ₁ (E)	54	#5	2'-6"	⌒
Concrete Removal	Cu. Yd.		9.1	
Concrete Superstructure	Cu. Yd.		9.9	
Protective Coat	Sq. Yd.		21	
Reinforcement Bars, Epoxy Coated	Pound		1,360	
Silicone Joint Sealer, 1"	Foot		19	

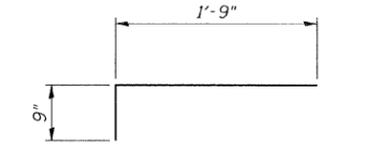


BAR a₂(E)

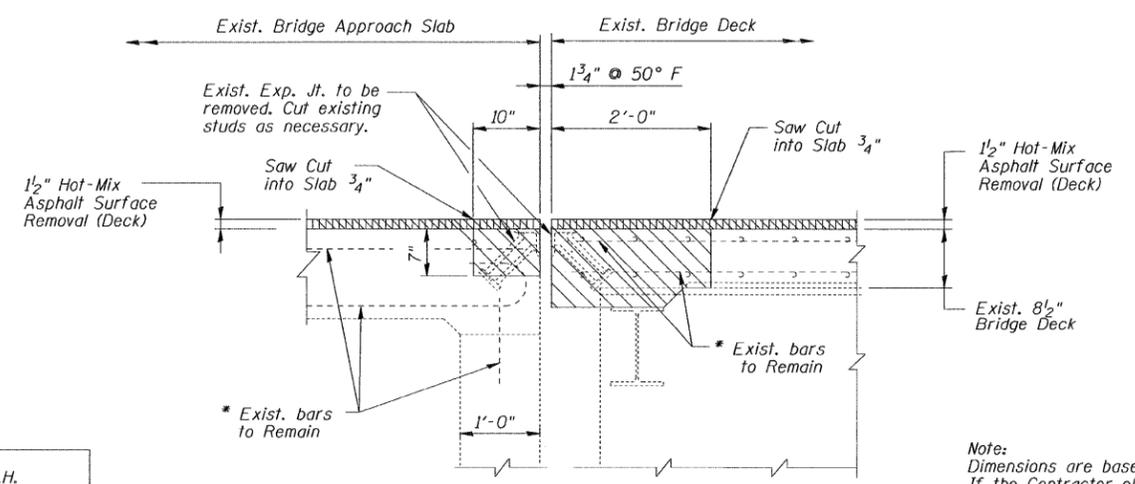


BAR d₁(E)

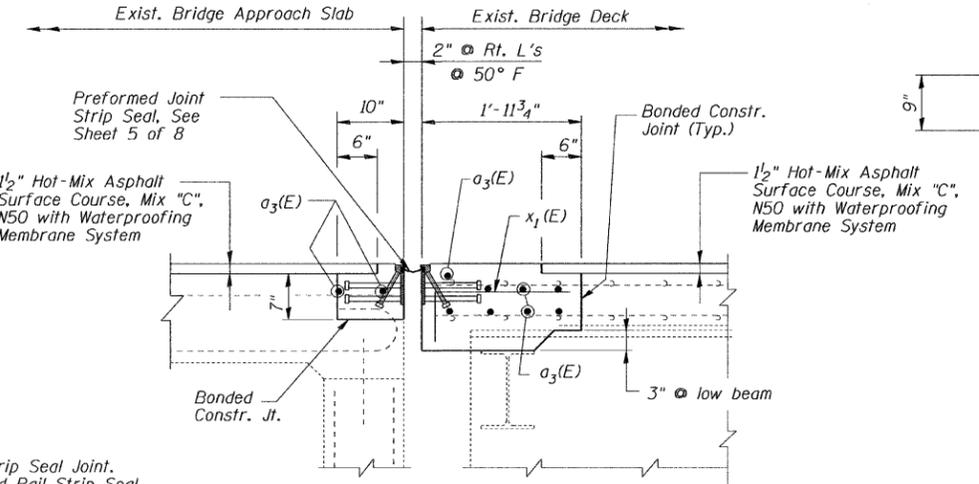
BAR d₂(E)



BAR x₁(E)



SECTION A-A



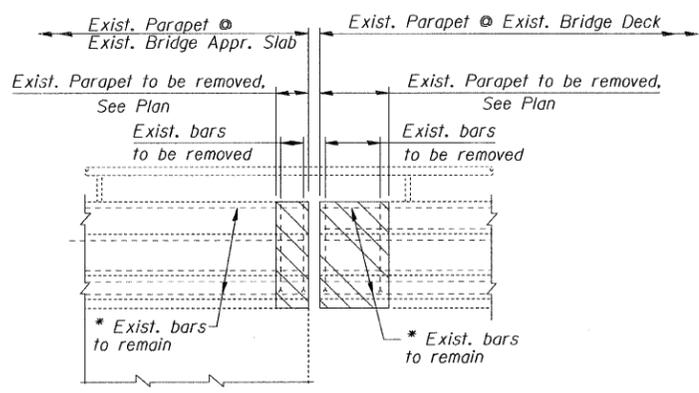
SECTION B-B

Note:
Dimensions are based on a Rolled Rail Strip Seal Joint.
If the Contractor elects to use the Welded Rail Strip Seal Joint, deck dimensions may require adjustments to satisfy the details on Base Sheef EJ-SSJ as shown on Sheet 5 of 8.

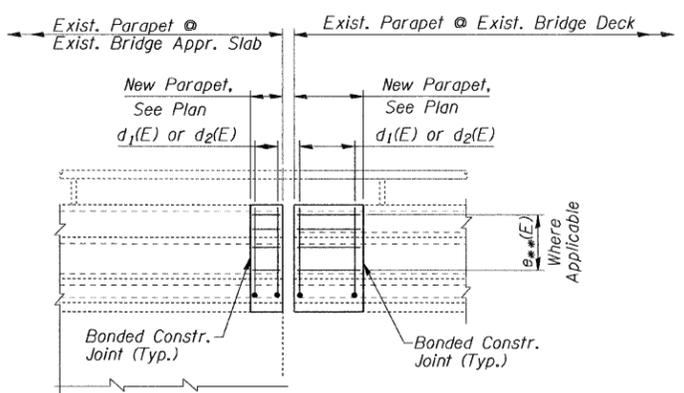
JOINT DETAILS
C.H. 49 (317th STREET) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
ROCK ISLAND COUNTY
STATION 2130+77.86
S.N. 081-0132
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

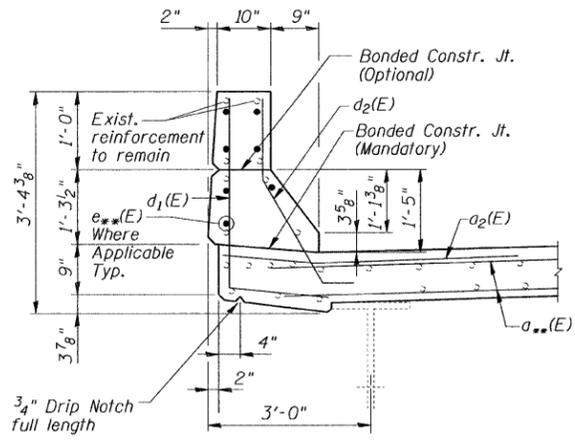
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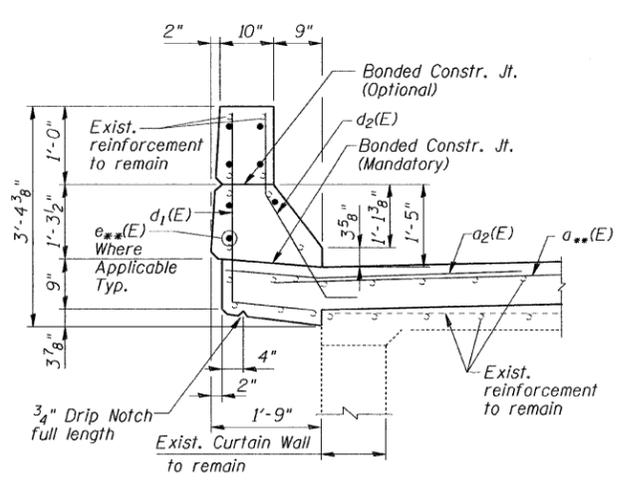
CONCRETE REMOVAL @
EXISTING PARAPET ELEVATION



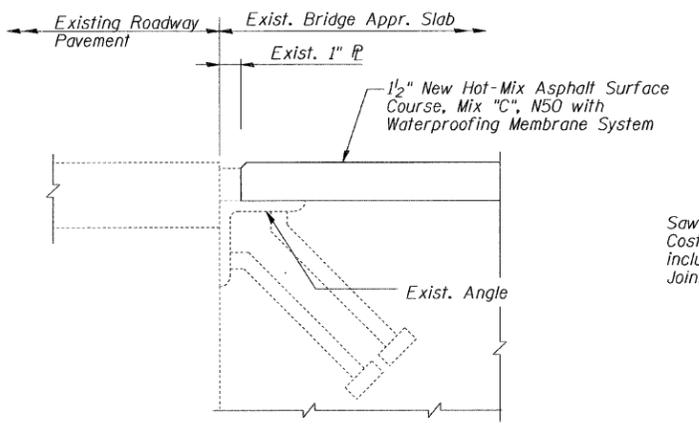
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EXISTING PARAPET ELEVATION



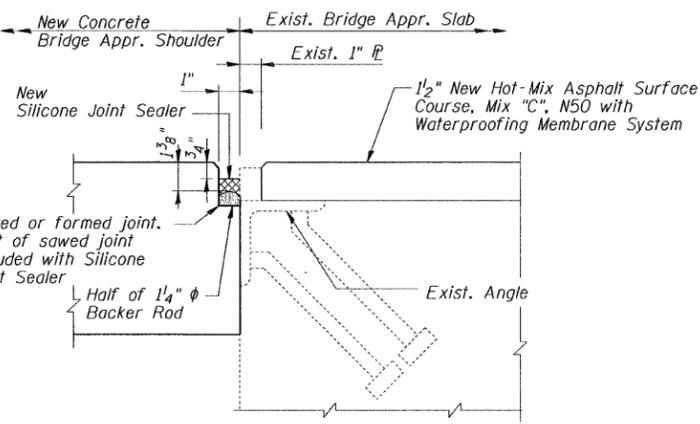
SECTION THRU NEW PARAPET
AT EXISTING BRIDGE DECK



SECTION THRU NEW PARAPET AT
EXISTING APPROACH SLAB



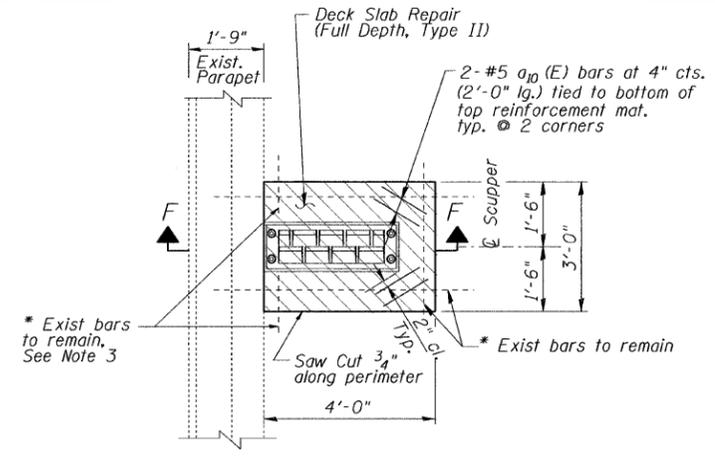
SECTION C-C



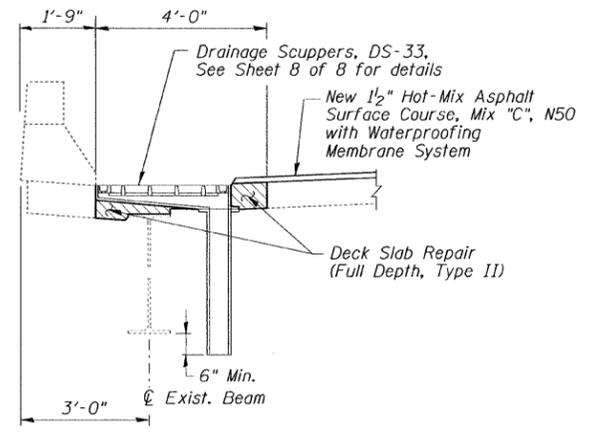
SECTION D-D

NOTES:

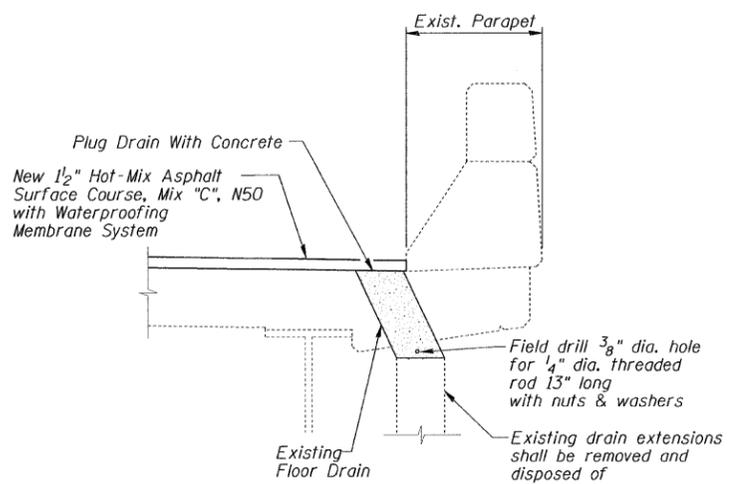
- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Cut to fit as required. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal or Deck Slab Repair (Full Depth, Type II).
- ** See Plan on "Joint Details" sheet for bar designation.
- Cut exist. longitudinal reinforcement to clear drainage scupper.
- Hatched areas indicate concrete sections to be removed and replaced. Perimeters of concrete removal areas shall be saw cut 3/4" prior to the removal of concrete.



NEW SCUPPER PLAN



SECTION F-F



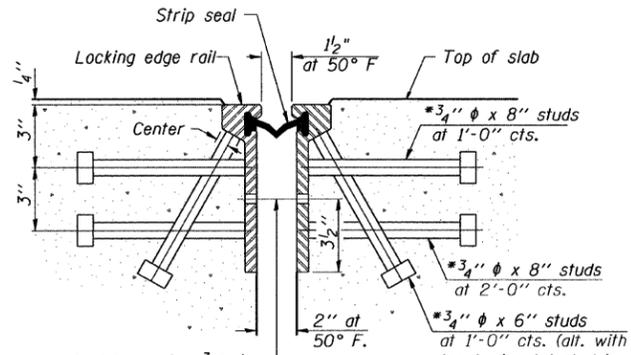
DRAIN ELIMINATION DETAIL

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

PARAPET & DECK SECTIONS & DETAILS
C.H. 49 (317th STREET) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
ROCK ISLAND COUNTY
STATION 2130+77.86
S.N. 081-0132
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

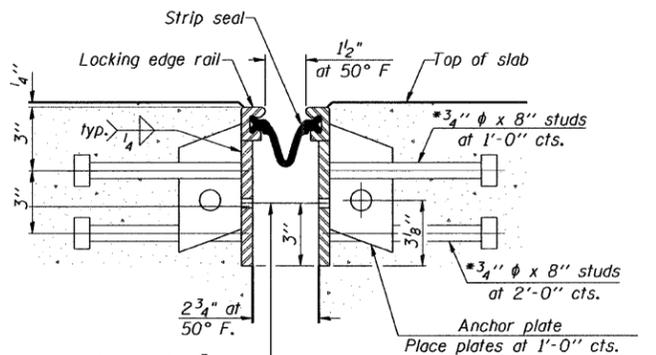
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*Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16 inch diameter holes at 4 foot 0 inch centers for 3/8 inch diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT

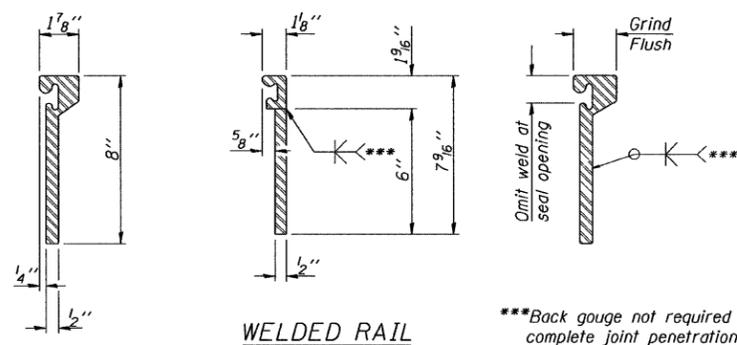


7/16 inch diameter holes at 4 foot 0 inch centers for 3/8 inch diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU WELDED RAIL JOINT

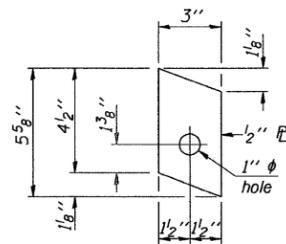
Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4 inch. The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

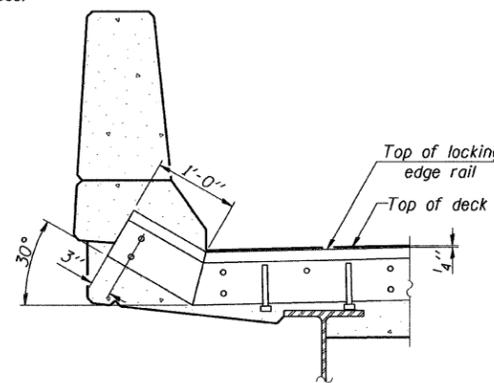


WELDED RAIL

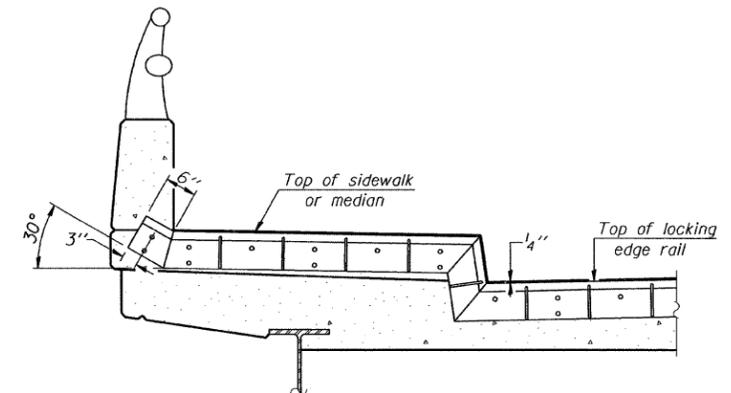
ROLLED EXTRUDED RAIL



ANCHOR PLATE (for welded rail)



AT PARAPET



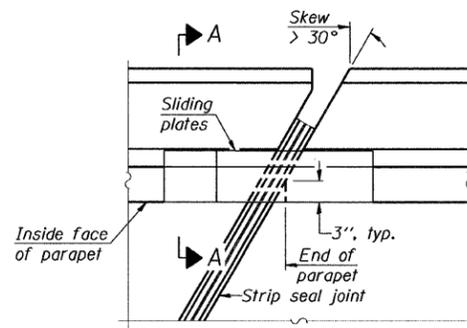
AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12 inch centers may be necessary on medians which are shallower than 9 inches. See manufacturer's recommendation.

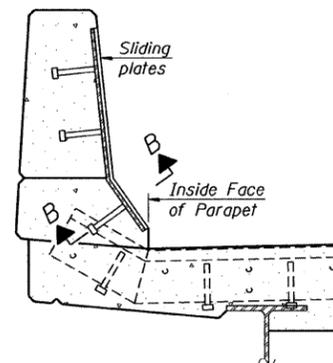
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

LOCKING EDGE RAILS



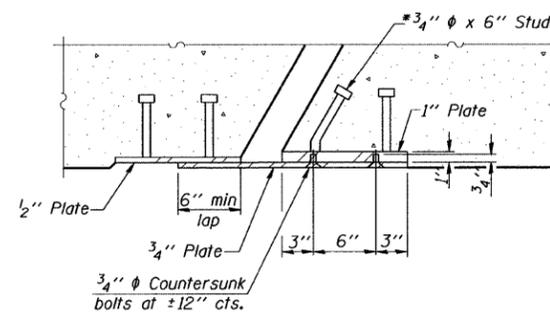
PLAN



SECTION A-A

POINT BLOCK DETAILS (for skews > 30 degrees)

TYPICAL END TREATMENTS



SECTION B-B

BILL OF MATERIAL

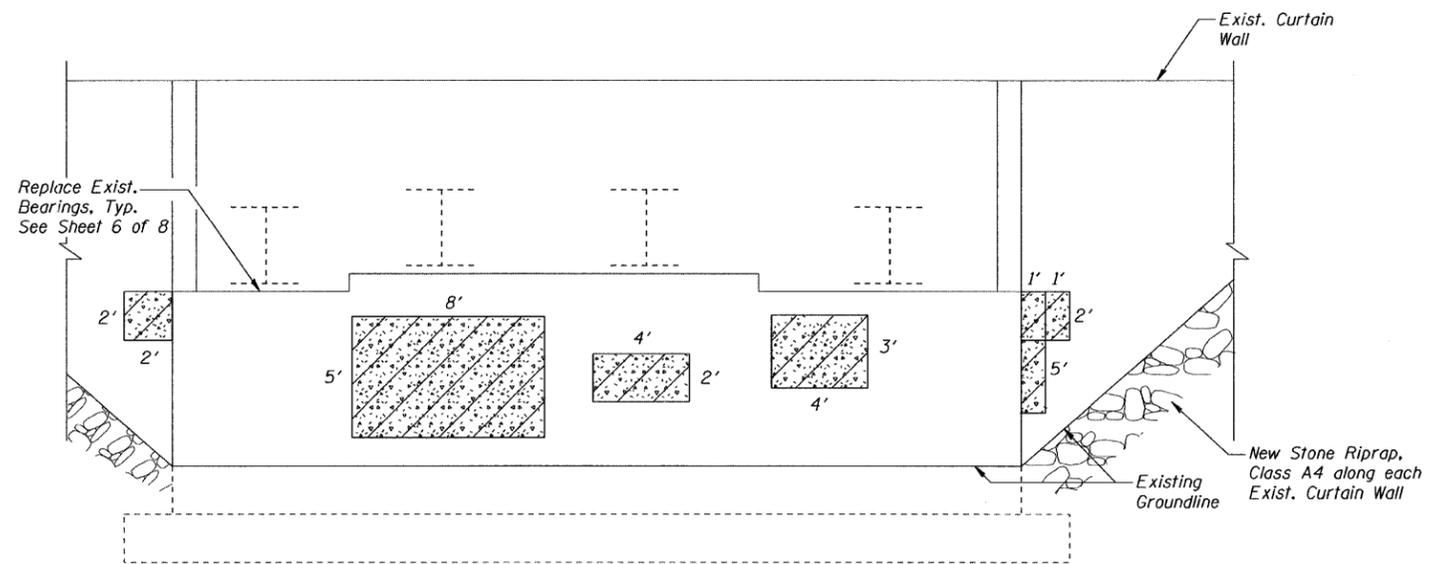
Item	Unit	Total
Preformed Joint Strip Seal	Foot	72

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

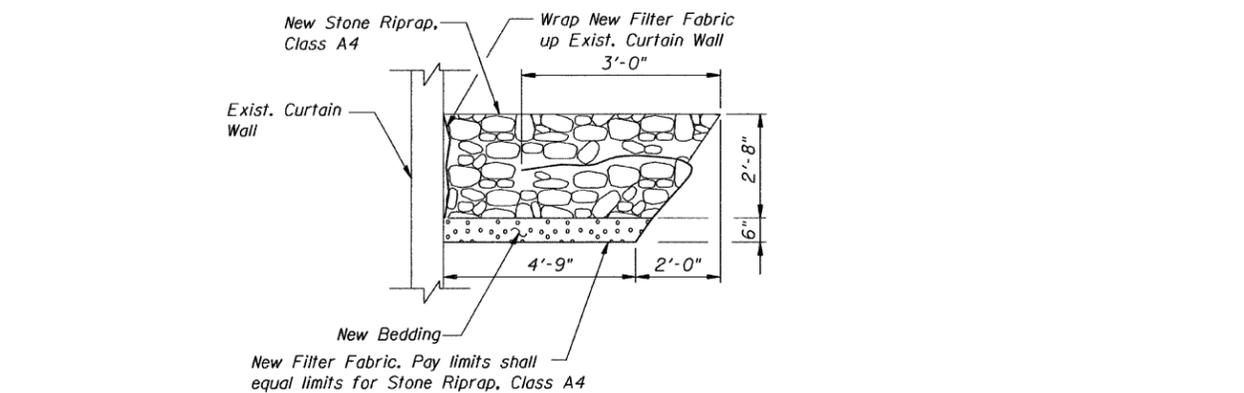
EJ-SSJ 5-16-08

PREFORMED JOINT STRIP SEAL
 C.H. 49 (317th STREET) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 ROCK ISLAND COUNTY
 STATION 2130+77.86
 S.N. 081-0132
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

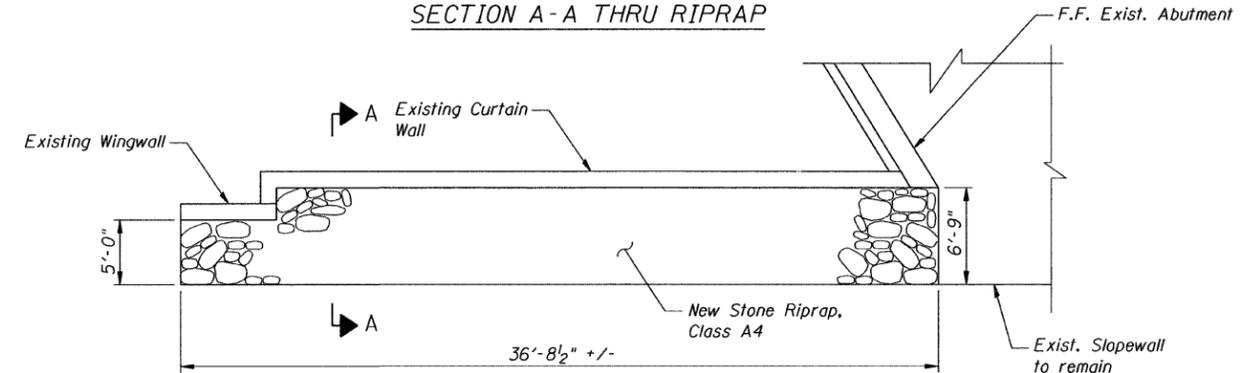
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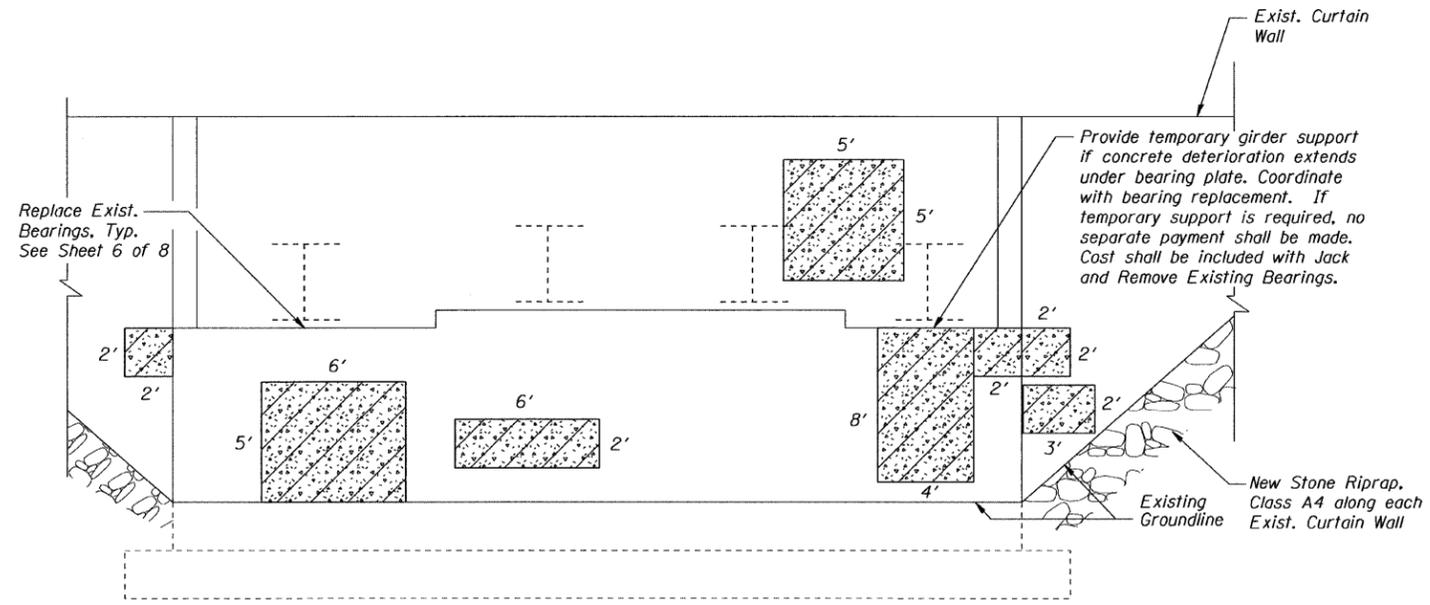
EXIST. SOUTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking South)



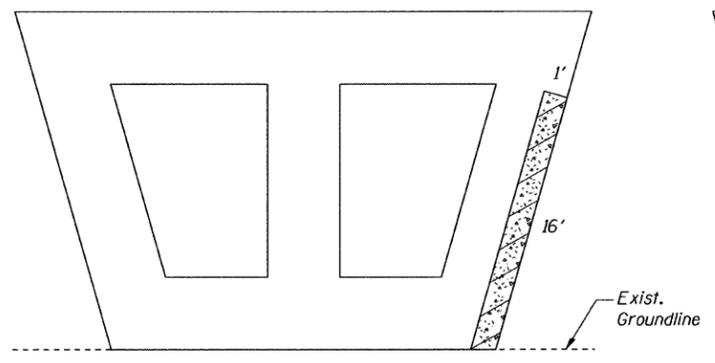
SECTION A-A THRU RIPRAP



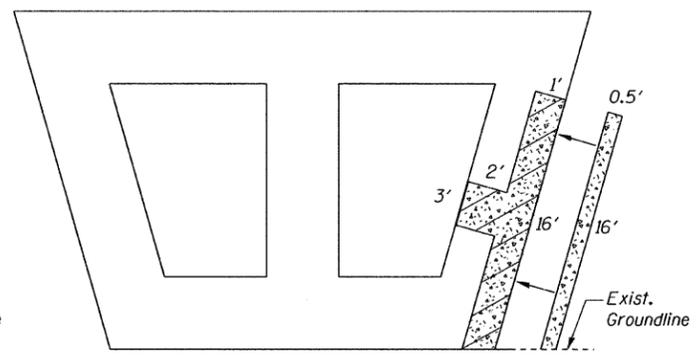
RIPRAP PLAN AT ABUTMENTS
(Exist. North Abutment, west side shown. Other locations similar)



EXIST. NORTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking North)



EXISTING PIER ELEVATION
(Looking North)



EXISTING PIER ELEVATION
(Looking South)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Stone Riprap, Class A4	Sq. Yd.	115
Filter Fabric	Sq. Yd.	115
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.	295

LEGEND

Structural Repair of Concrete (Depth Equal to or Less than 5")

Note:
Quantity for Structural Repair of Concrete has been increased to include additional repair areas not apparent at the time of the inspection. The actual repair areas will be determined by the Resident Engineer. The Contractor will be paid for the quantity furnished.

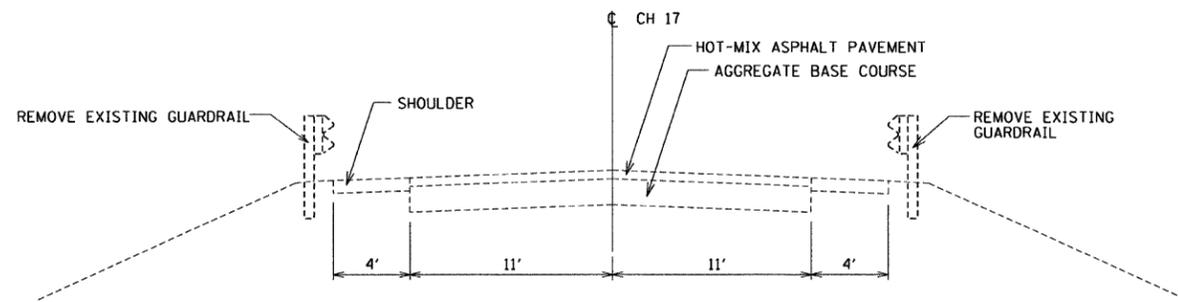
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

SUBSTRUCTURE REPAIR & RIPRAP DETAILS
C.H. 49 (317th STREET) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
ROCK ISLAND COUNTY
STATION 2130+77.86
S.N. 081-0132
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

n:\jobs\2008\plot\2001-4014-09\081-0132\317th st.\00\081-0132-S28.dgn 12/8/2008

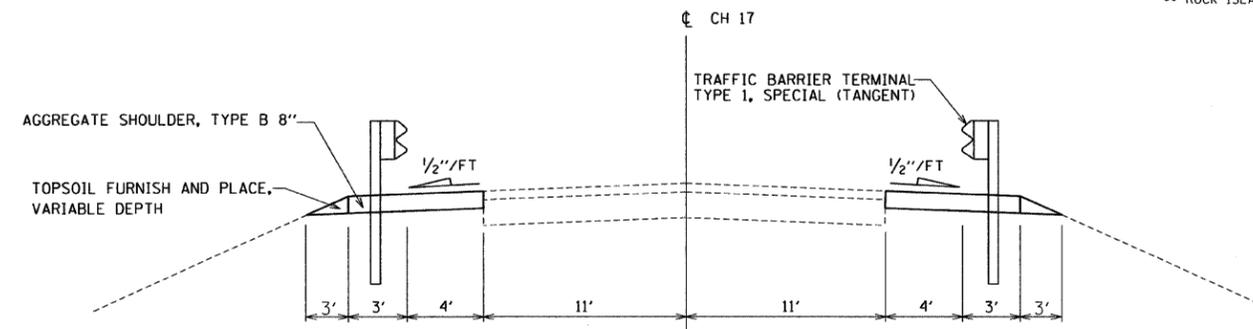
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	30
STA. 3007+89.03		TO STA. 3020+92.38		
FED. AID PROJECT	ILLINOIS	FED. ROAD DIST. NO.		

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE



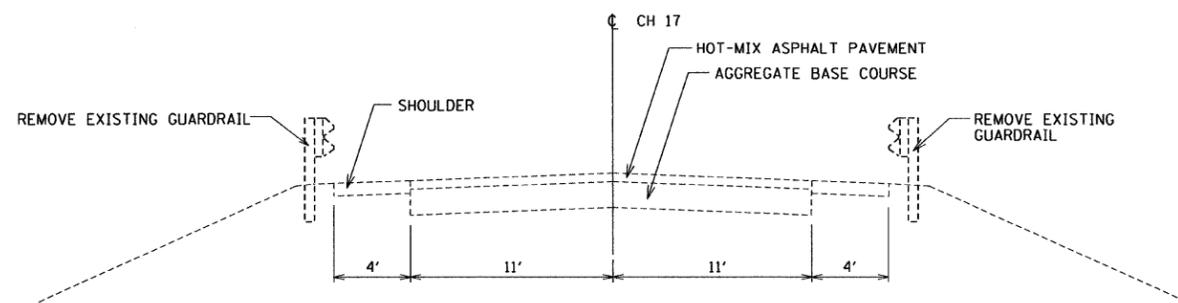
EXISTING ROADWAY TYPICAL SECTION

STA 3007+89.03 TO 3008+87.72
STA 3019+93.41 TO 3020+92.38



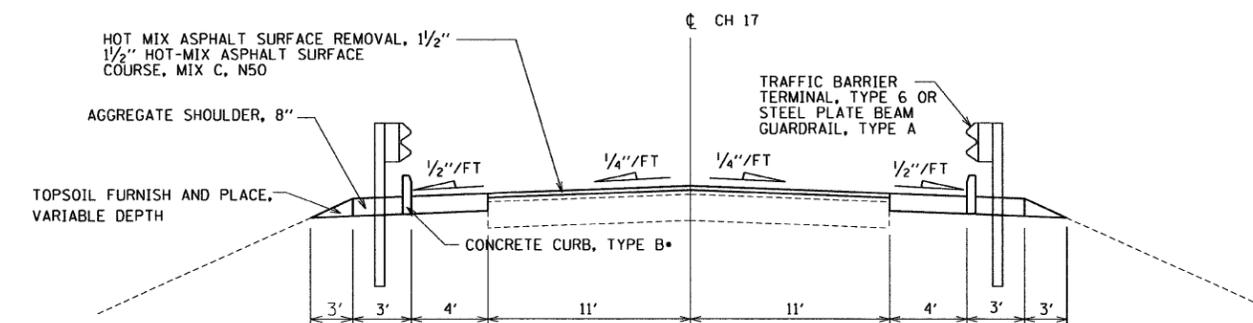
PROPOSED ROADWAY TYPICAL SECTION

STA 3007+89.03 TO 3008+87.72
STA 3019+93.41 TO 3020+92.38



EXISTING ROADWAY TYPICAL SECTION

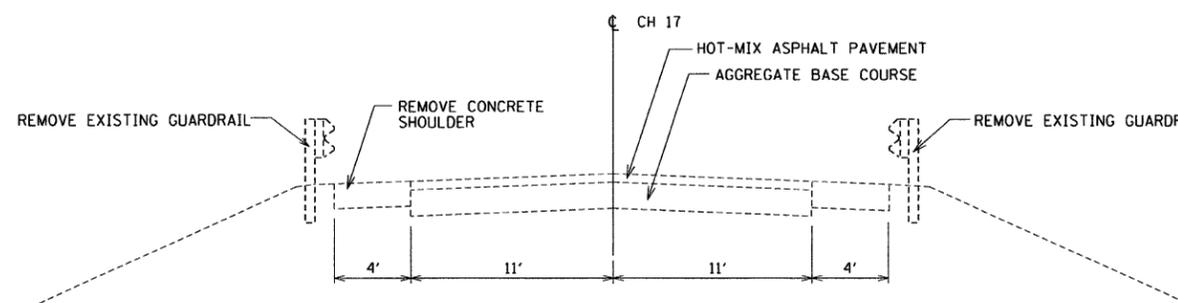
STA 3012+48.25 TO 3012+70.36 LT
STA 3012+48.25 TO 3012+85.62 RT
STA 3015+94.40 TO 3016+40.25 LT
STA 3016+09.65 TO 3016+40.25 RT



PROPOSED ROADWAY TYPICAL SECTION

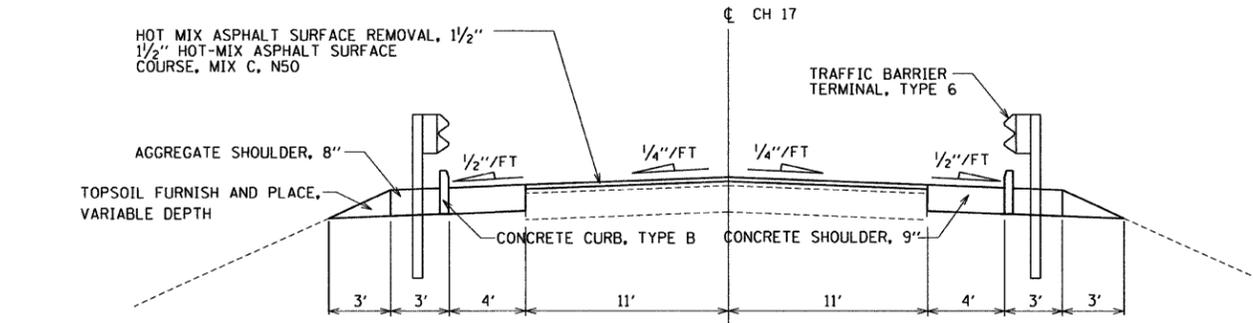
STA 3012+48.25 TO 3012+70.36 LT
STA 3012+48.25 TO 3012+85.62 RT
STA 3015+94.40 TO 3016+40.25 LT
STA 3016+09.65 TO 3016+40.25 RT

- STA 3012+55.51 TO 3012+70.36 LT
- STA 3012+70.09 TO 3012+85.62 RT
- STA 3015+94.40 TO 3016+09.93 LT
- STA 3016+09.65 TO 3016+24.50 RT



EXISTING ROADWAY TYPICAL SECTION

STA 3012+70.36 TO 3012+98.25 LT
STA 3012+85.62 TO 3012+98.25 RT
STA 3015+82.25 TO 3015+94.40 LT
STA 3015+82.25 TO 3016+40.25 RT



PROPOSED ROADWAY TYPICAL SECTION

STA 3012+70.36 TO 3012+98.25 LT
STA 3012+85.62 TO 3012+98.25 RT
STA 3015+82.25 TO 3015+94.40 LT
STA 3015+82.25 TO 3016+40.25 RT

- HOT-MIX ASPHALT SURFACE COURSE 112 LBS/IN THICK/SQ YD
- HOT-MIX ASPHALT SHOULDER 112 LBS/IN THICK/SQ YD
- TEMPORARY EROSION CONTROL SEEDING 100 LB/ACRE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CH 17 (WILMOT ROAD) BRIDGE OVER I-88
STRUCTURE NO.: 098-0058
BRIDGE REHABILITATION

TYPICAL SECTIONS

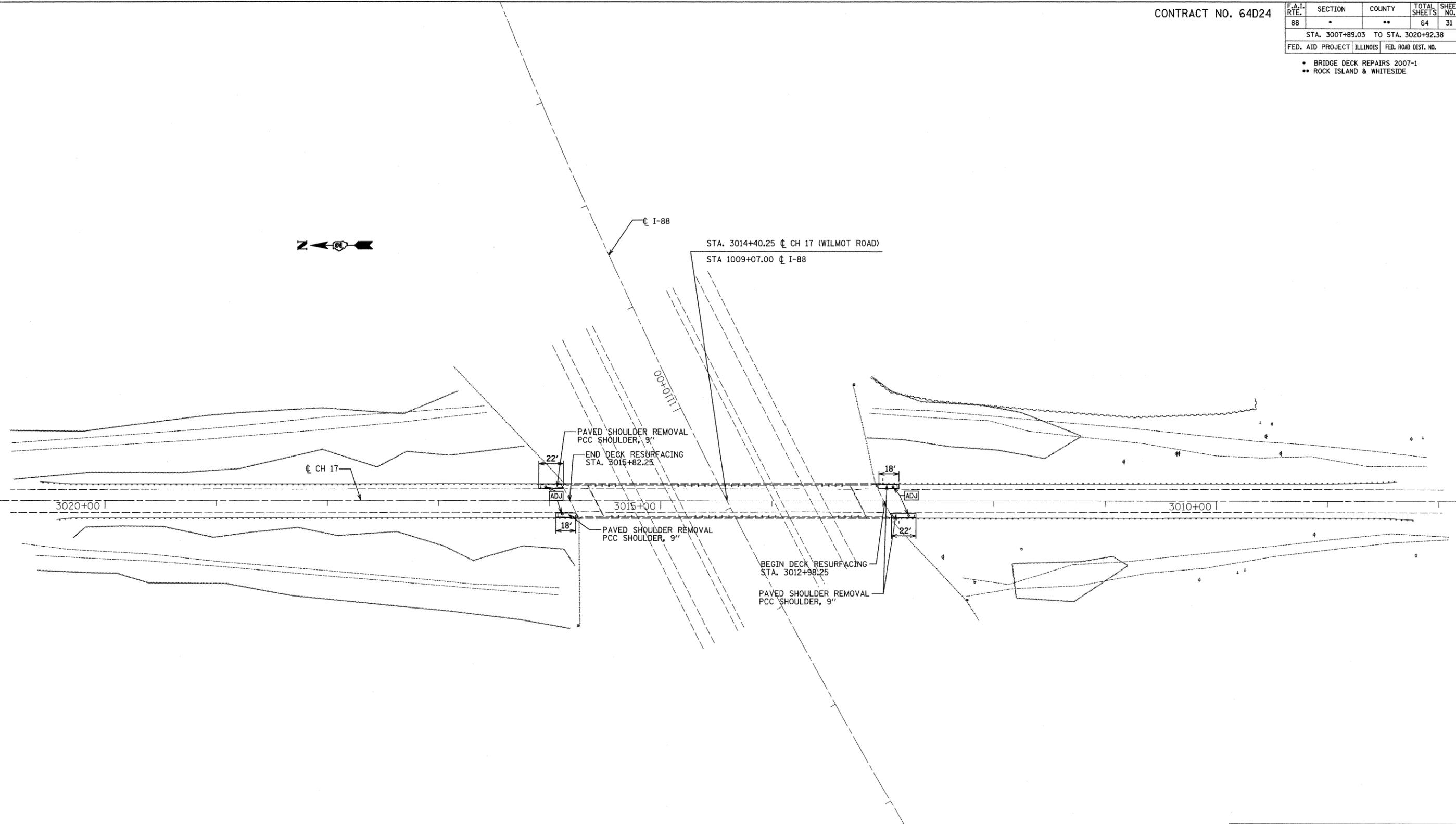
SCALE: NTS
DATE 12/09/08

DRAWN BY EF
CHECKED BY RS

CONTRACT NO. 64D24

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	.	**	64	31
STA. 3007+89.03 TO STA. 3020+92.38				
FED. AID PROJECT ILLINOIS FED. ROAD DIST. NO.				

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE



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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 17 (WILMOT ROAD) BRIDGE OVER I-88
 STRUCTURE NO. 098-0058
 BRIDGE REHABILITATION

PROPOSED ROADWAY PLAN

SCALE: 1"=40'
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS

CONTRACT NO. 64D24

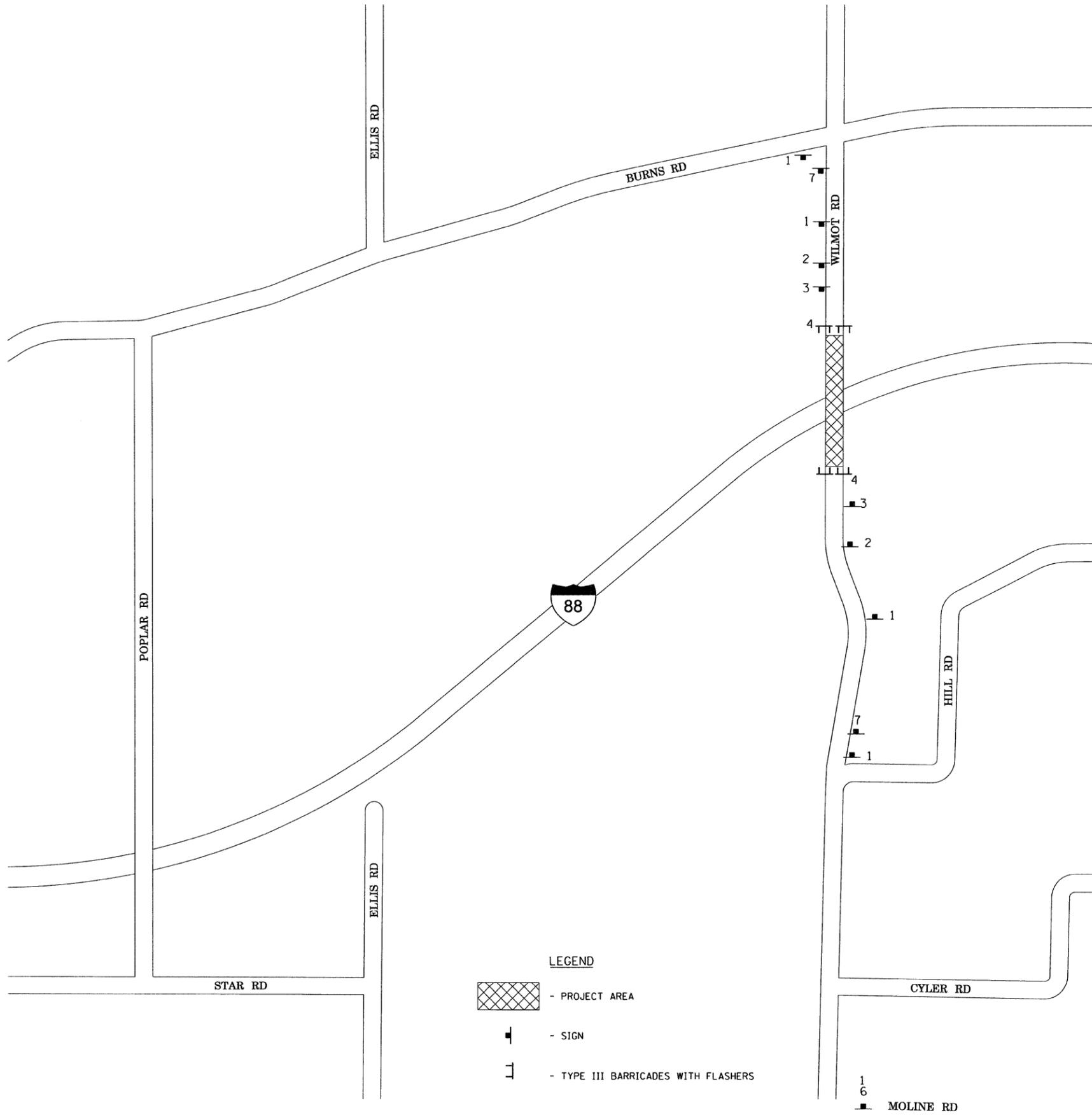
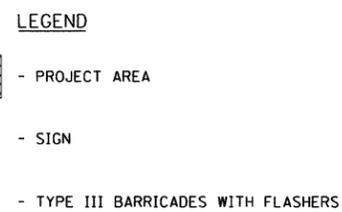
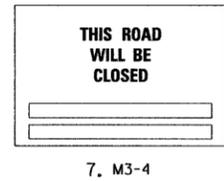
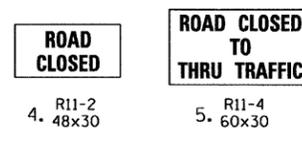
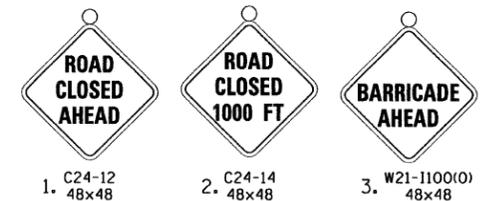
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	32
STA. 3007+89.03		TO STA. 3020+92.38		
FED. AID PROJECT	ILLINOIS	FED. ROAD DIST. NO.		

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE



MAINTENANCE OF TRAFFIC

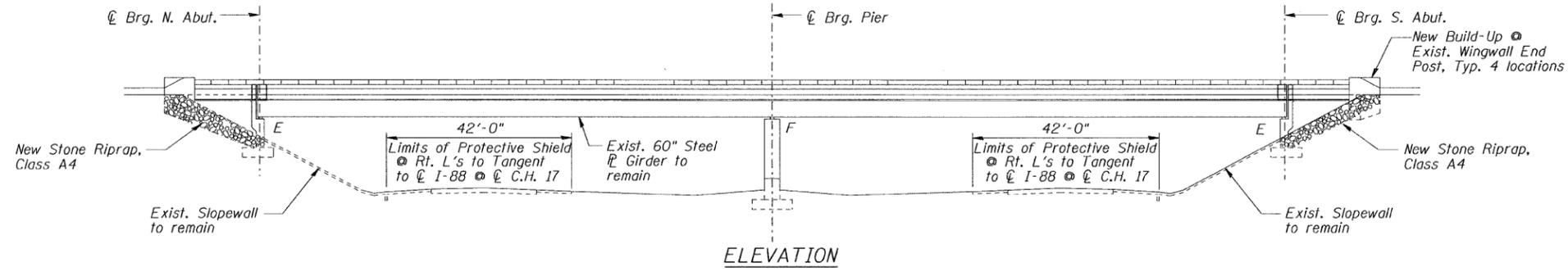
1. THE DEPARTMENT SHALL BE NOTIFIED 72 HOURS PRIOR TO ROAD CLOSURE AND IMMEDIATELY AFTER THE ROAD HAS BEEN REOPENED.
2. ROCK ISLAND COUNTY SHALL BE NOTIFIED AT LEAST TWO WEEKS PRIOR TO CLOSING THE ROAD.
3. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL TEMPORARY SIGN SUPPORTS. AFTER REMOVING THE SUPPORTS, THE CONTRACTOR SHALL FILL HOLES (IF ANY) AND RESTORE THE GROUND TO ITS ORIGINAL CONDITION AND ELEVATION. PLACING OF GRAVEL, SOD, OR SEED SHALL BE INCLUDED IN THE UNIT BID PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE".
4. ALL WARNING SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND ON AN ORANGE BACKGROUND, UNLESS OTHERWISE NOTED.
5. REFER TO DISTRICT STANDARD 40.1 FIR SIGNING REQUIREMENTS
6. EACH TYPE III BARRICADE SHALL BE EQUIPPED WITH TWO FLASHING AMBER LIGHTS.
7. THE CONTRACTOR SHALL ERECT SIGN NO. 7 AT LEAST 2 WEEKS IN ADVANCE OF THE CLOSURE.
8. ALL ENTRANCES ON THE CLOSED PORTION OF 50TH AVENUE SHALL BE OPEN FOR TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD.
9. THE WORK SHALL BE PAID FOR AT THE LUMP SUM PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE"



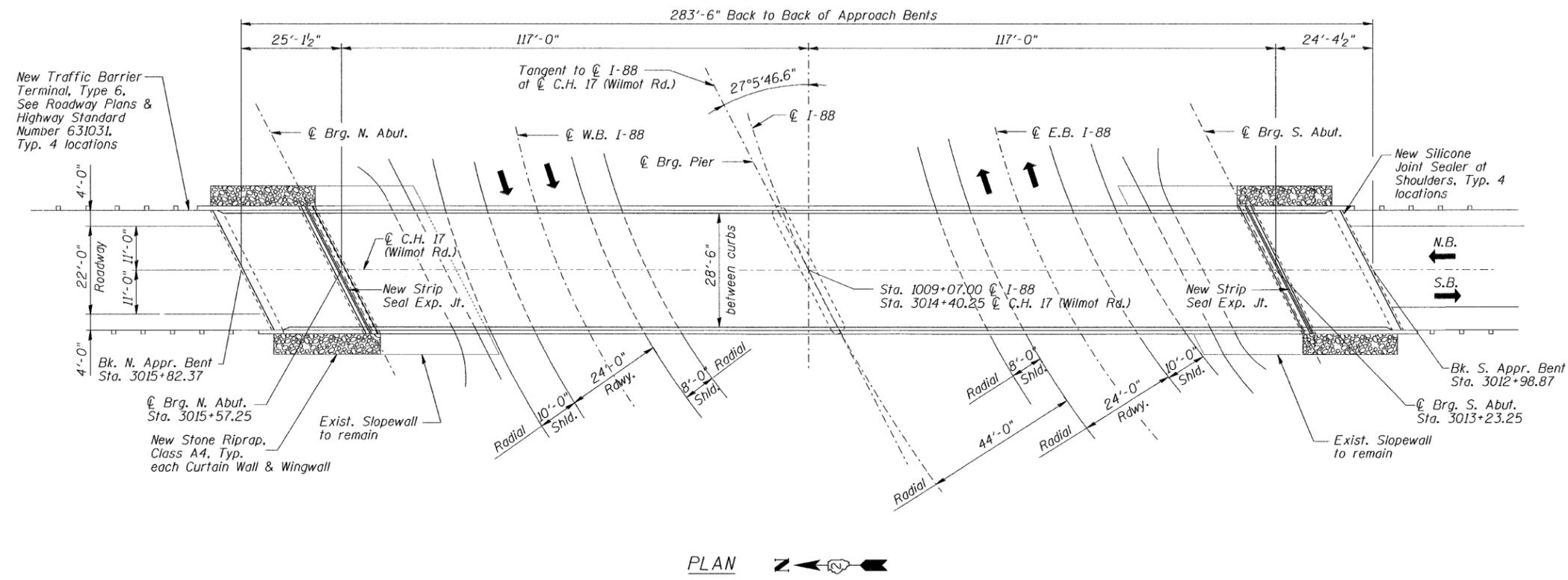
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 17 (WILMOT ROAD) BRIDGE OVER I-88
 STRUCTURE NO.: 098-0058
 BRIDGE REHABILITATION
MOT PLAN
 SCALE: NTS
 DATE 12/09/08
 DRAWN BY EF
 CHECKED BY RS

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ELEVATION



PLAN

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
STONE RIPRAP, CLASS A4	SQ. YD.	98
FILTER FABRIC	SQ. YD.	98
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	75
HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ. YD.	897
CONCRETE REMOVAL	CU. YD.	8.3
PROTECTIVE SHIELD	SQ. YD.	273
CONCRETE SUPERSTRUCTURE	CU. YD.	9.6
PROTECTIVE COAT	SQ. YD.	24
CLEANING AND PAINTING STEEL BRIDGE NO. 3	L. SUM	1
CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 3	L. SUM	1
REINFORCEMENT BARS, EPOXY COATED	POUND	1,420
PREFORMED JOINT STRIP SEAL	FOOT	68
JACK AND REMOVE EXISTING BEARINGS	EACH	8

ITEM	UNIT	QUANTITY
ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	8
ANCHOR BOLTS, 1"	EACH	32
FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1570
PLUG EXISTING DECK DRAINS	EACH	36
SILICONE JOINT SEALER, 1"	FOOT	18
WATERPROOFING MEMBRANE SYSTEM	SQ. YD.	884
DRAINAGE SCUPPERS, DS-33	EACH	4
STRUCTURAL REPAIR OF CONCRETE (DEPTH-<5")	SQ. FT.	198
BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 3	L. SUM	1
DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	21
DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	27
DECK SLAB REPAIR (PARTIAL)	SQ YD	63

GENERAL NOTES

All structural steel shall be AASHTO M 270 Grade 50.

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck at the expansion joint, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that cannot be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans and are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Protective Coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

All structural steel at expansion joints shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Preformed Joint Strip Seal.

Quantities for deck slab repair are approximate. Quantity of Deck Slab Repair (Full Depth, Type I) and Deck Slab Repair (Full Depth, Type II) are each assumed to be equal to 2.50% of the deck area. Quantity of Deck Slab Repair (Partial) is assumed to be equal to 7.5% of the deck area. Locations will be determined by the Resident Engineer following removal of the Hot-Mix Asphalt Surface Course. Actual repair locations shall be shown on the as-built plans. Contractor will be paid for the quantity furnished.

Cleaning and painting of the existing structural steel shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All existing steel shall be cleaned per Near White Blast Cleaning - SSPC-SP10. All existing steel shall be painted according to the requirements of Paint System 1 - OZ/E/U. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Blue, Munsell No. 10B 3/6.

A minimum of 2 air monitors will be required to monitor abrasive blasting operations at this site, see special provision for "Containment and Disposal of Lead Paint Cleaning Residues."

The SSPC QP1 & QP2 certifications will be required for this contract.

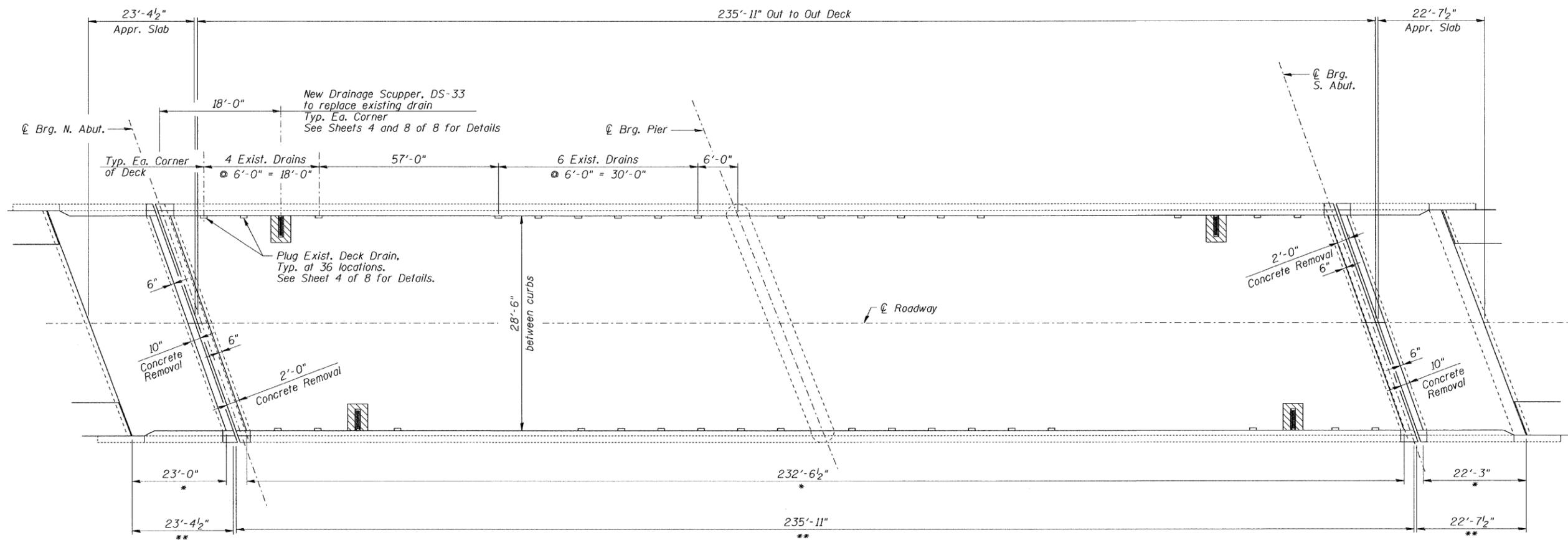
Existing plans are available from the District to determine painting quantities.



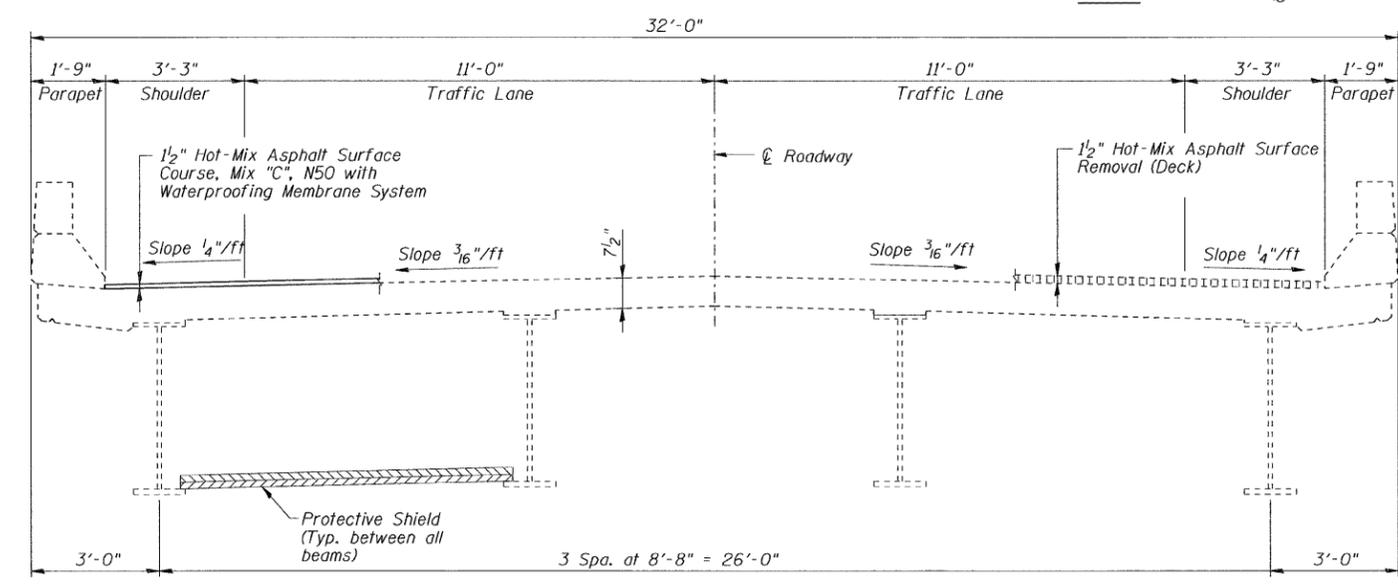
GENERAL PLAN & ELEVATION
 C.H. 17 (WILMOT RD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3014+40.25
 S.N. 098-0058
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

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 12/8/2008

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.



PLAN



DECK CROSS SECTION

- Notes:
- * - Limits of Hot-Mix Asphalt Surface Course, Mix "C", N50 with Waterproofing Membrane System
 - ** - Limits of Hot-Mix Asphalt Surface Removal (Deck)

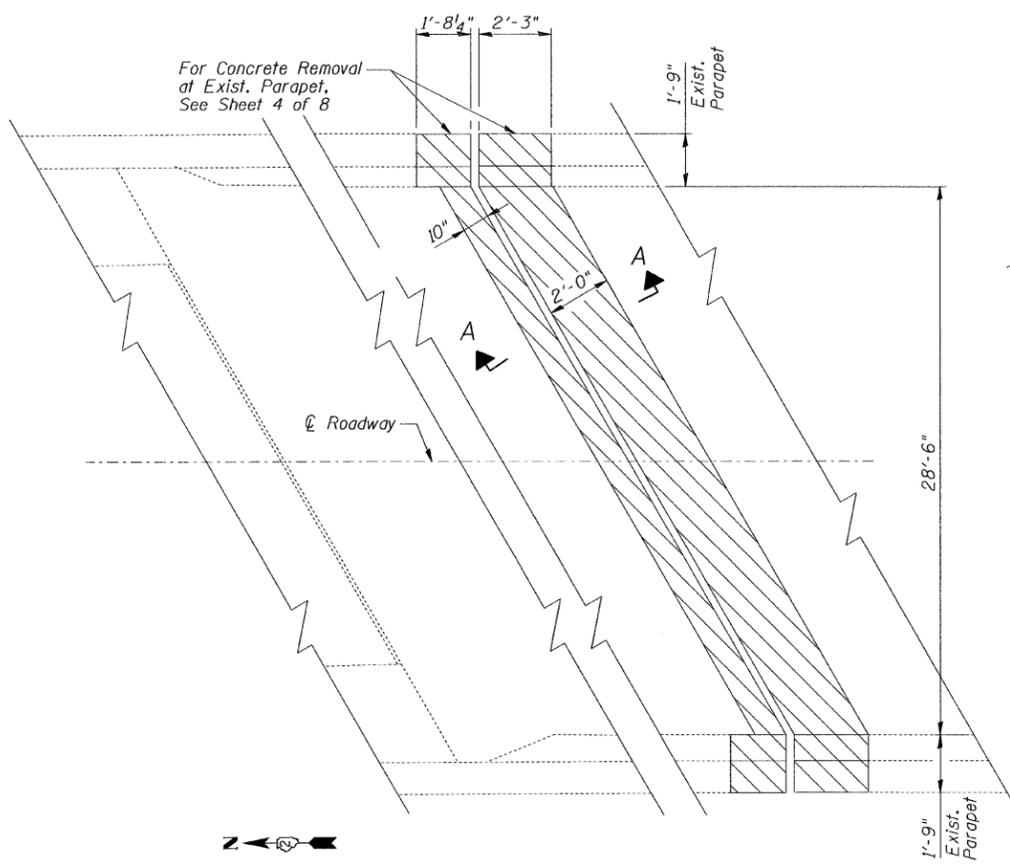
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Course, Mix "C", N50	Ton	75
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	897
Protective Shield	Sq. Yd.	273
Plug Existing Deck Drains	Each	36
Waterproofing Membrane System	Sq. Yd.	884
Drainage Scuppers, DS-33	Each	4

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

DECK PLAN & CROSS SECTION
 C.H. 17 (WILMOT ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3014+40.25
 S.N. 098-0058
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

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 12/8/2008



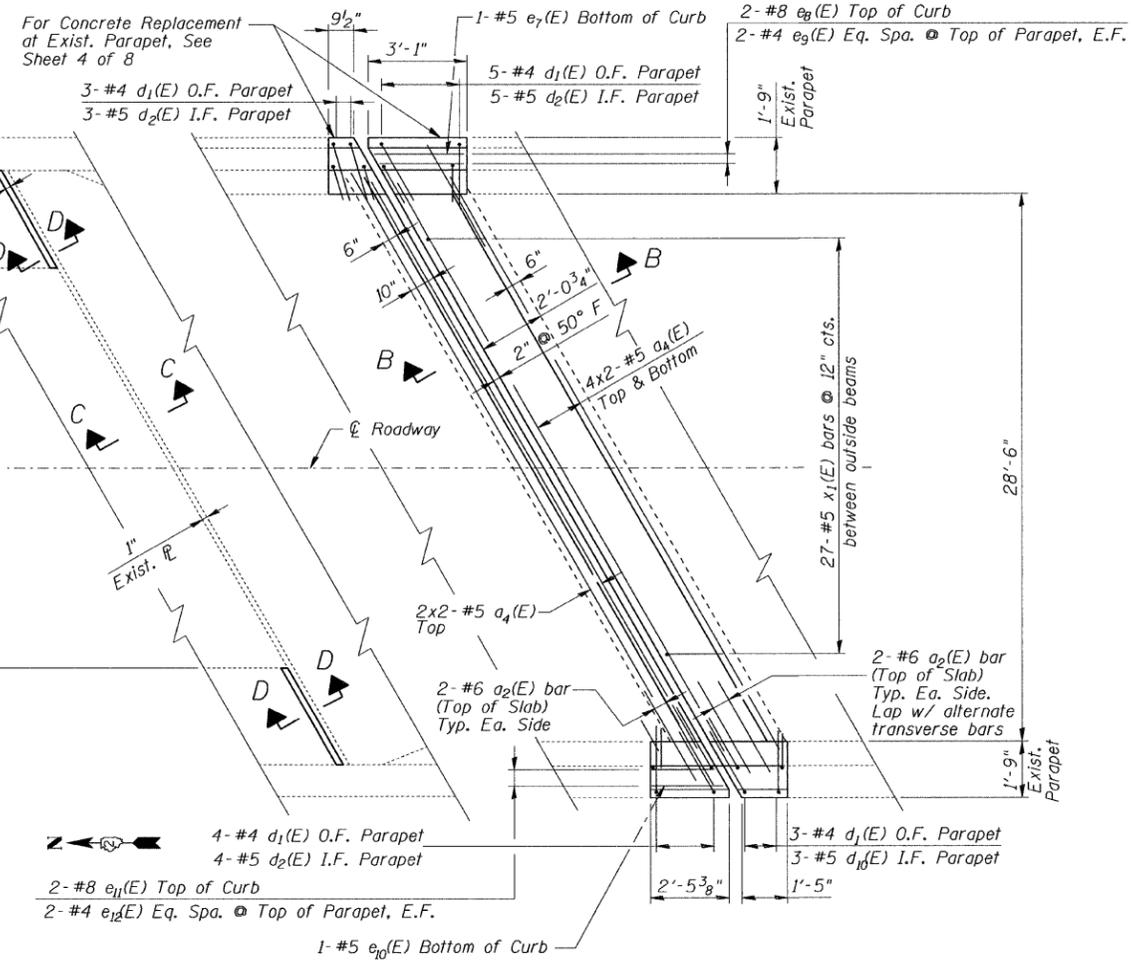
CONCRETE REMOVAL @ EXIST. NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN

NOTES:

- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal.
- For Sections C-C & D-D see Sheet 4 of 8.
- Bars indicated thus 4x2-#5 etc. indicates 4 lines of bars with 2 lengths per line.
- Lap #5 bars 2'-2" min.

LEGEND

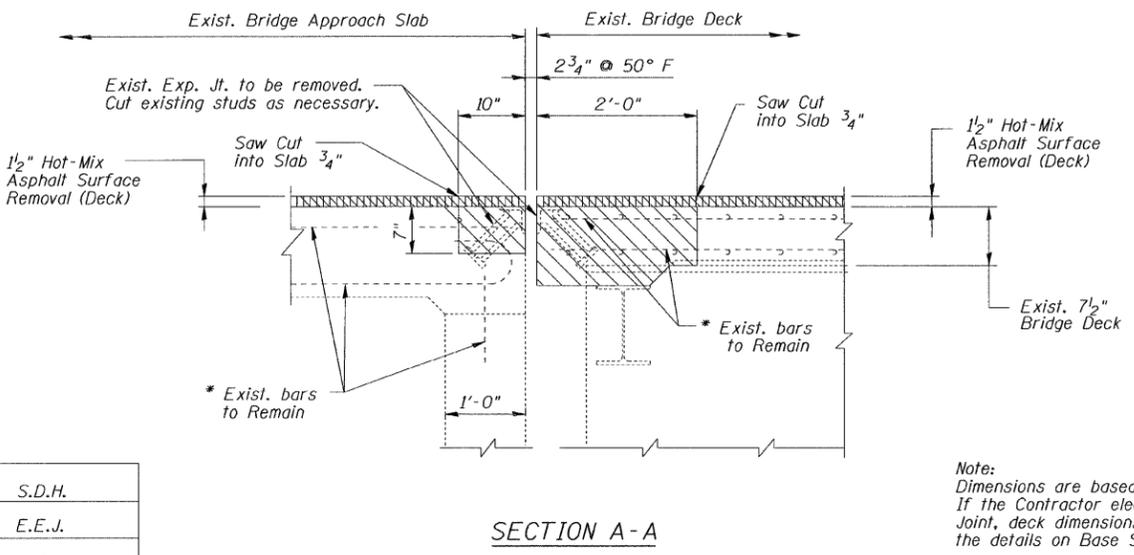
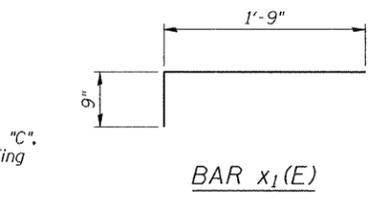
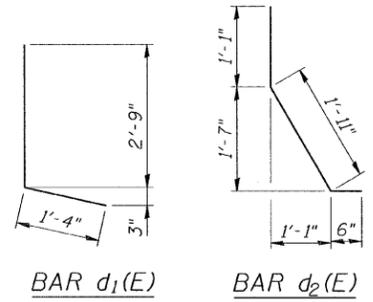
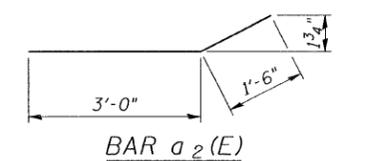
- Concrete Removal



CONCRETE REPLACEMENT @ EXIST. NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN

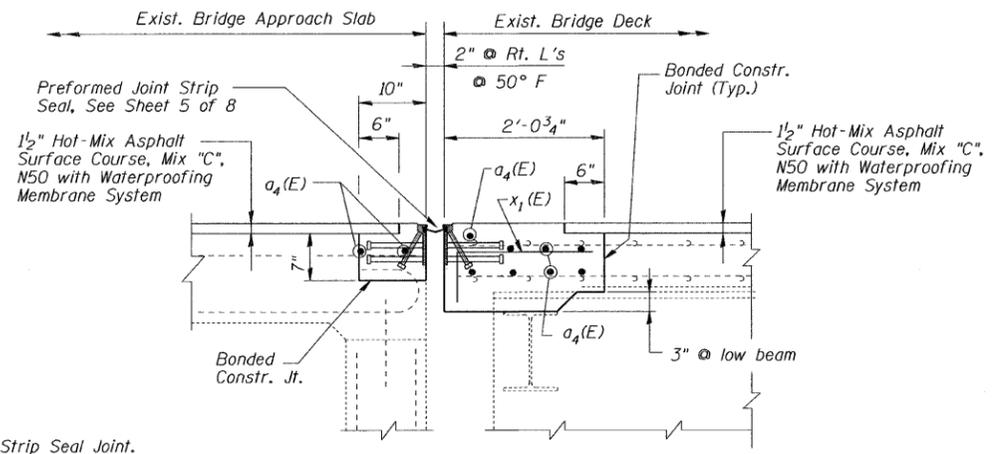
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₂ (E)	16	#6	4'-6"	
a ₄ (E)	40	#5	18'-2"	
a ₁₀ (E)	16	#5	2'-0"	
d ₁ (E)	24	#4	4'-1"	
d ₂ (E)	24	#5	3'-6"	
e ₇ (E)	2	#5	2'-6"	
e ₈ (E)	4	#8	2'-6"	
e ₉ (E)	8	#4	2'-6"	
e ₁₀ (E)	2	#5	1'-10"	
e ₁₁ (E)	4	#8	1'-10"	
e ₁₂ (E)	8	#4	1'-10"	
x ₁ (E)	54	#5	2'-6"	
Concrete Removal		Cu. Yd.	8.2	
Concrete Superstructure		Cu. Yd.	9.0	
Protective Coat		Sq. Yd.	20	
Reinforcement Bars, Epoxy Coated		Pound	1,280	
Silicone Joint Sealer, 1"		Foot	18	



SECTION A-A

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

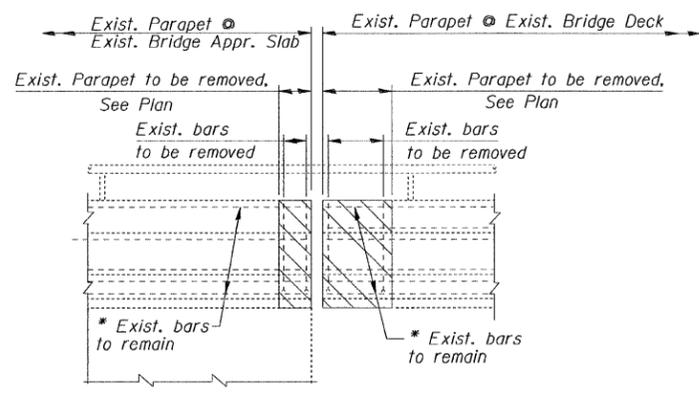


SECTION B-B

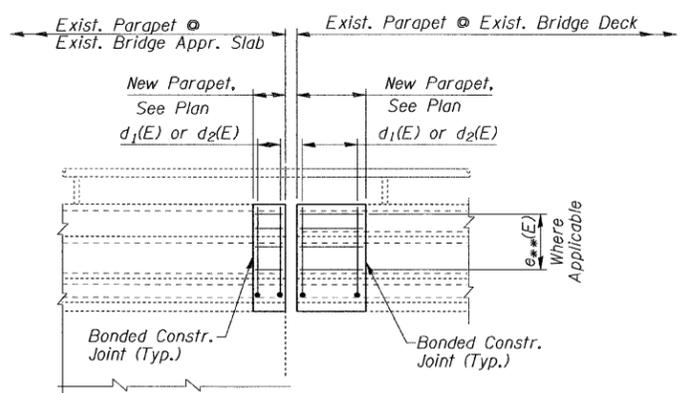
Note:
Dimensions are based on a Rolled Rail Strip Seal Joint.
If the Contractor elects to use the Welded Rail Strip Seal Joint, deck dimensions may require adjustments to satisfy the details on Base Sheet E.J-SSJ as shown on Sheet 5 of 8.

JOINT DETAILS
C.H. 17 (WILMOT ROAD) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
WHITESIDE COUNTY
STATION 3014+40.25
S.N. 098-0058
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

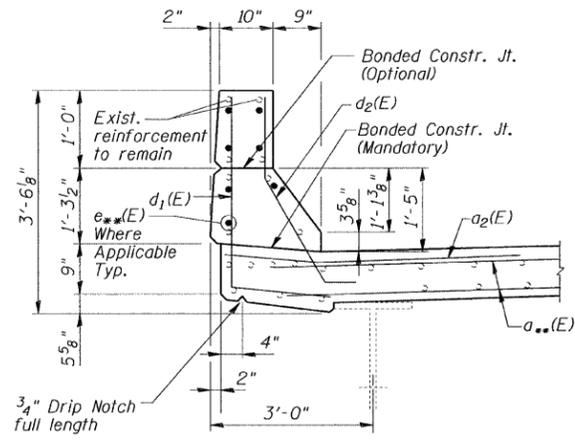
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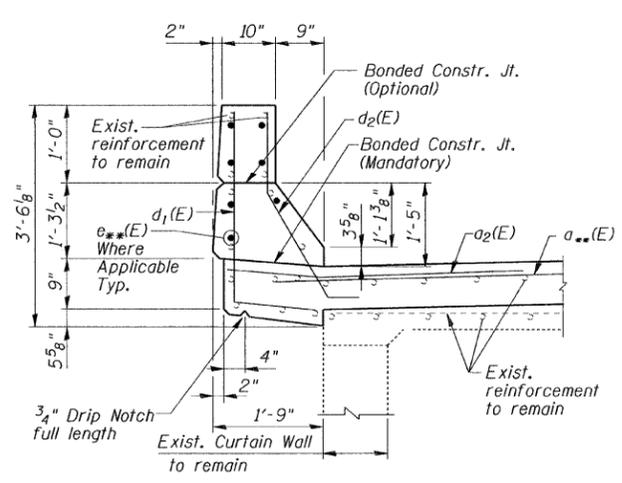
CONCRETE REMOVAL @
EXISTING PARAPET ELEVATION



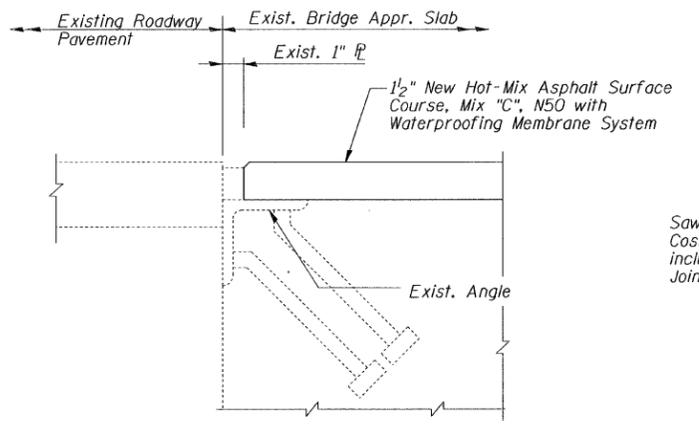
CONCRETE REPLACEMENT @
EXISTING PARAPET ELEVATION



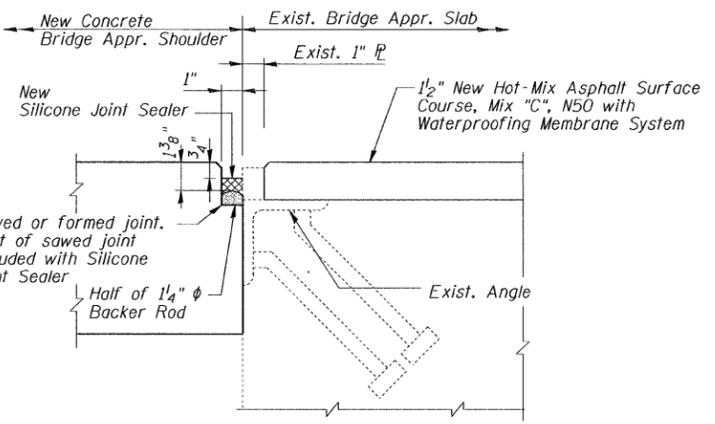
SECTION THRU NEW PARAPET
AT EXISTING BRIDGE DECK



SECTION THRU NEW PARAPET AT
EXISTING APPROACH SLAB



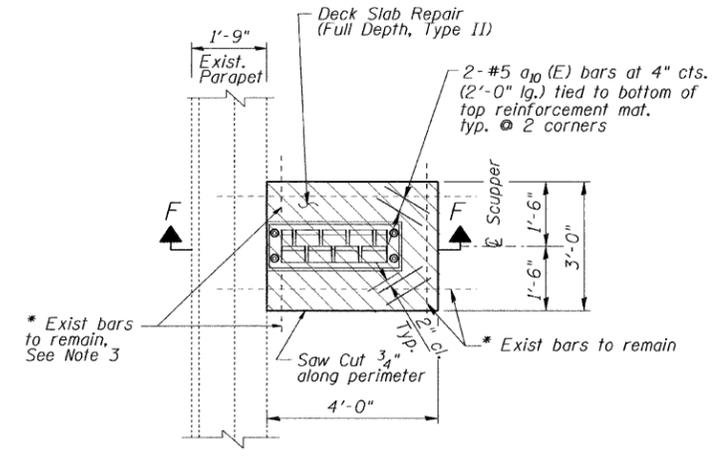
SECTION C-C



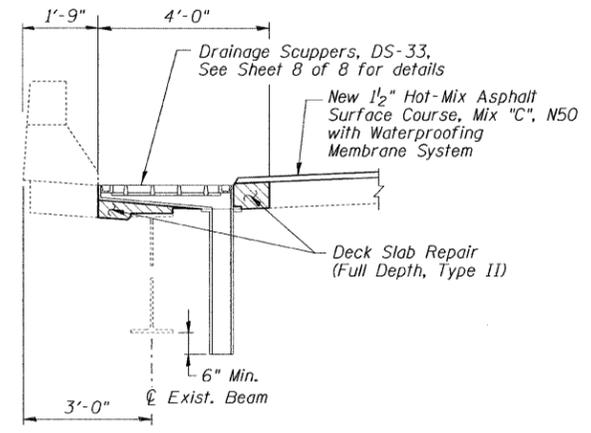
SECTION D-D

NOTES:

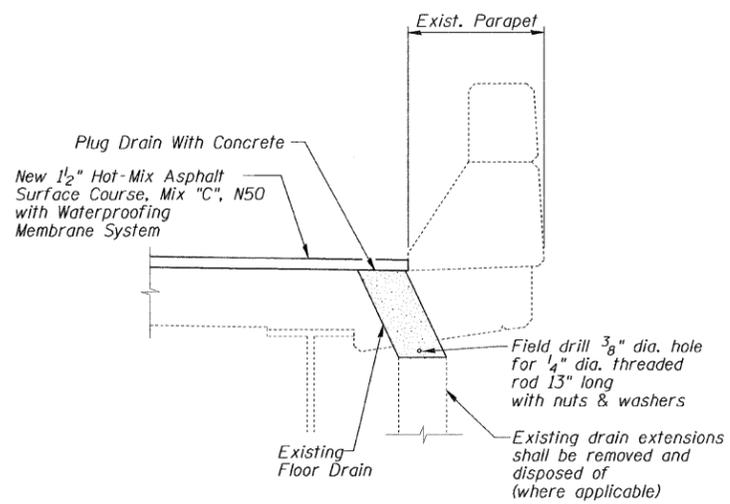
- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Cut to fit as required. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal or Deck Slab Repair (Full Depth, Type II).
- ** See Plan on "Joint Details" sheet for bar designation.
- Cut exist. longitudinal reinforcement to clear drainage scupper.
- Hatched areas indicate concrete sections to be removed and replaced. Perimeters of concrete removal areas shall be saw cut 3/4" prior to the removal of concrete.



NEW SCUPPER PLAN



SECTION F-F



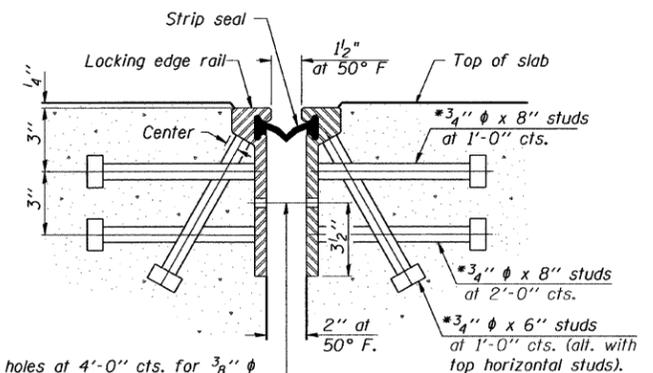
DRAIN ELIMINATION DETAIL

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

PARAPET & DECK SECTIONS & DETAILS
 C.H. 17 (WILMOT ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3014+40.25
 S.N. 098-0058
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

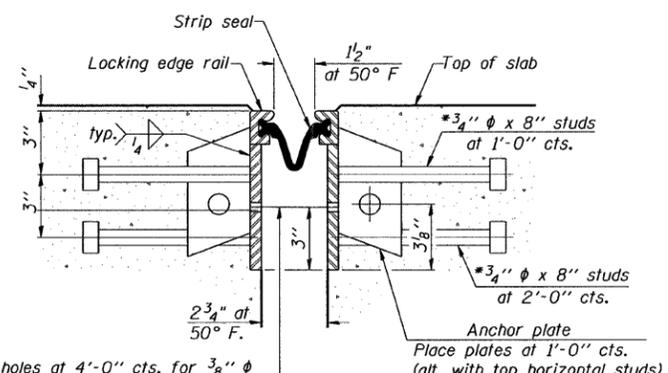
n:\jobs\2008\plot\2001-4014.09\098-0058\Wlmo1.Rg.\00\098-0058-S36.dgn
 12/8/2008

*Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

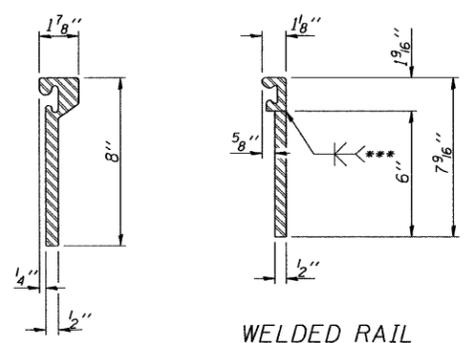
SECTION THRU ROLLED RAIL JOINT



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

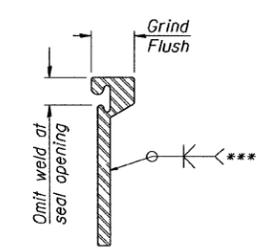
SECTION THRU WELDED RAIL JOINT

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



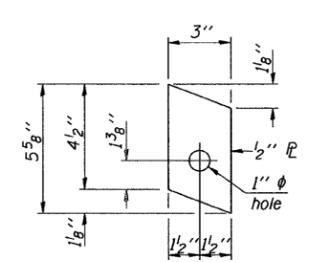
ROLLED EXTRUDED RAIL

WELDED RAIL

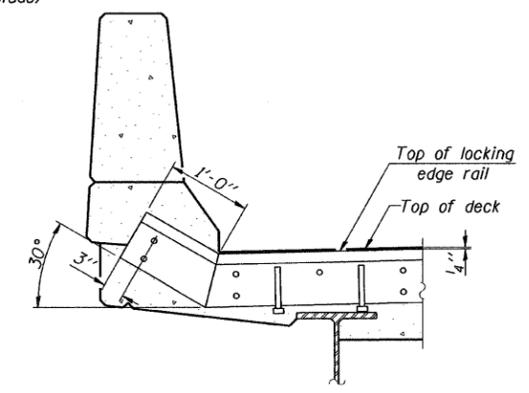


LOCKING EDGE RAIL SPLICE

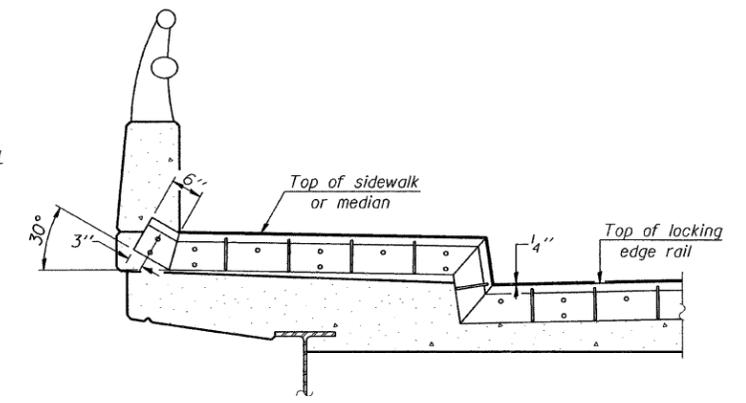
The inside of the locking edge rail groove shall be free of weld residue.



ANCHOR PLATE (for welded rail)



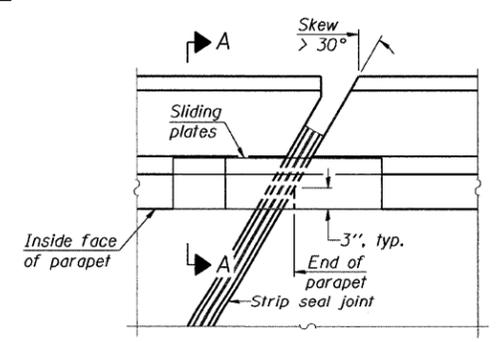
AT PARAPET



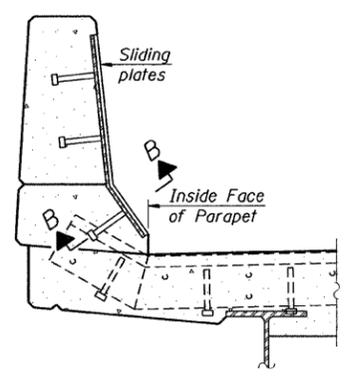
AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

LOCKING EDGE RAILS

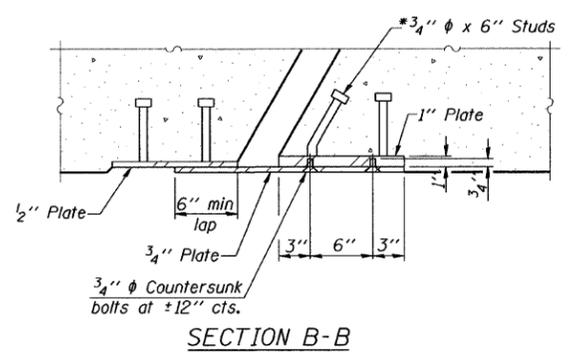


PLAN



SECTION A-A

POINT BLOCK DETAILS (for skews > 30 degrees)



SECTION B-B

Note: Skew is 27 degrees 05' - 46.6"

TYPICAL END TREATMENTS

BILL OF MATERIAL

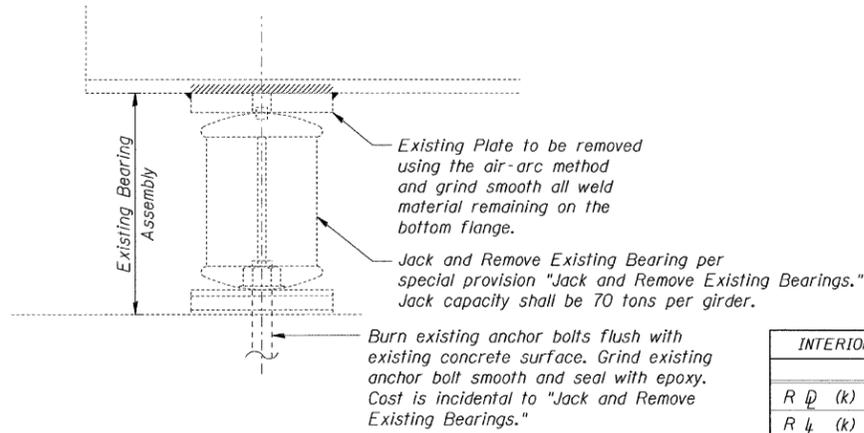
Item	Unit	Total
Preformed Joint Strip Seal	Foot	68

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

EJ-SSJ 5-16-08

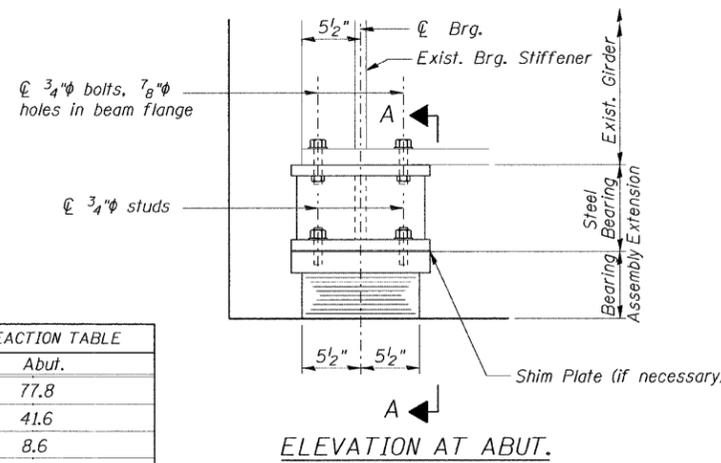
PREFORMED JOINT STRIP SEAL
 C.H. 17 (WILMOT ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3014+40.25
 S.N. 098-0058
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

n:\jobs\2008\plot\2001-4014.09\098-0058(Wilomat Rd.)\00 098-0058-S37.dgn
 12/8/2008

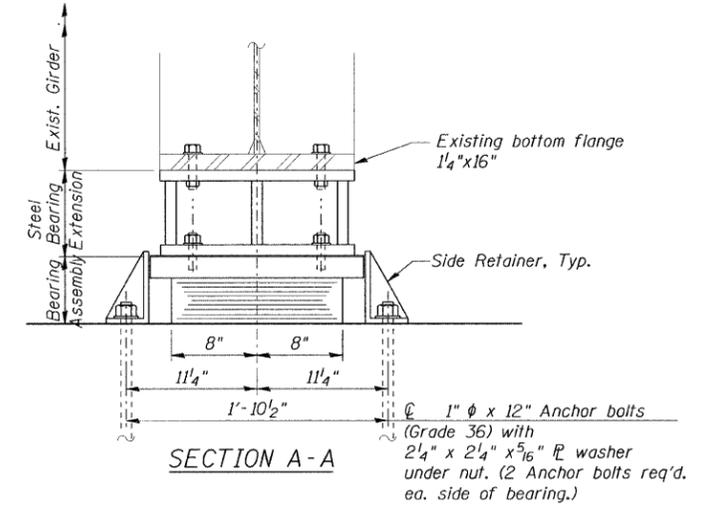


EXISTING BEARING REMOVAL DETAIL

INTERIOR GIRDER REACTION TABLE	
	Abut.
R _D (k)	77.8
R _L (k)	41.6
Imp. (k)	8.6
R _{Total} (k)	128.0

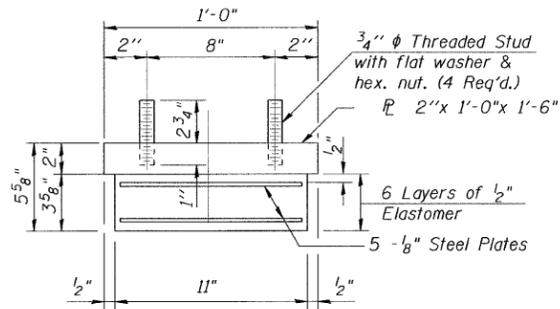


ELEVATION AT ABUT.



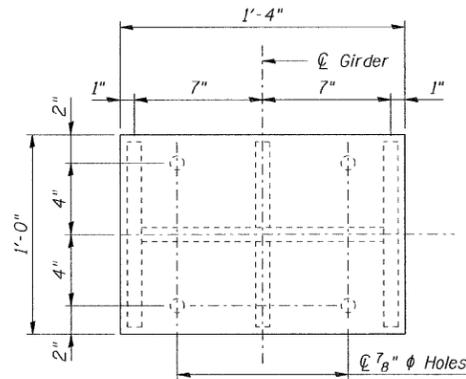
SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.



BEARING ASSEMBLY

Note: Shim plates shall not be placed under Bearing Assembly.



PLAN STEEL BEARING EXTENSION

Notes:

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type I.

The structural steel plates of the Bearing Assembly and the Steel Bearing Extension shall conform to the requirements of AASHTO M 270 Grade 36.

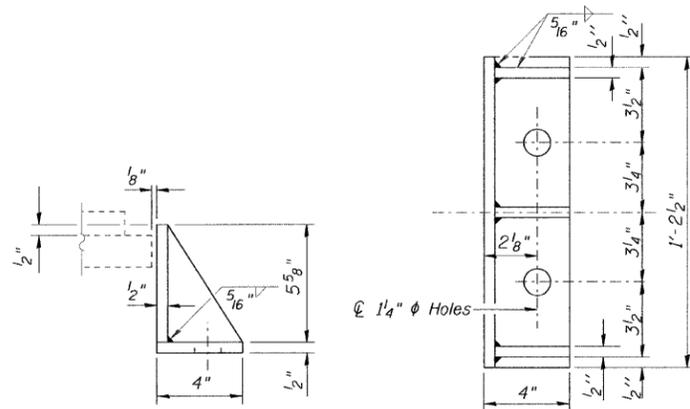
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.

Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.

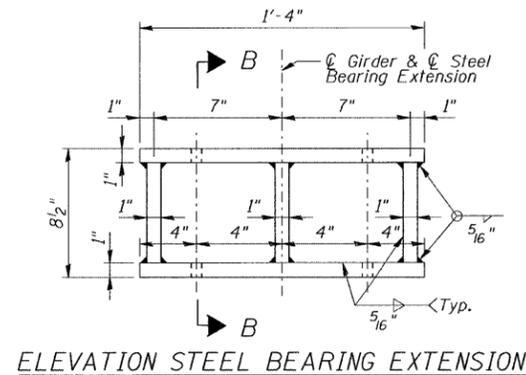
New Steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

BILL OF MATERIAL

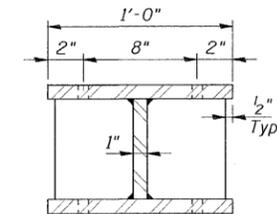
Item	Unit	Total
Jack and Remove Existing Bearings	Each	8
Elastomeric Bearing Assembly, Type I	Each	8
Anchor Bolts, 1"	Each	32
Furnishing and Erecting Structural Steel	Pound	1,570



SIDE RETAINER
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



ELEVATION STEEL BEARING EXTENSION



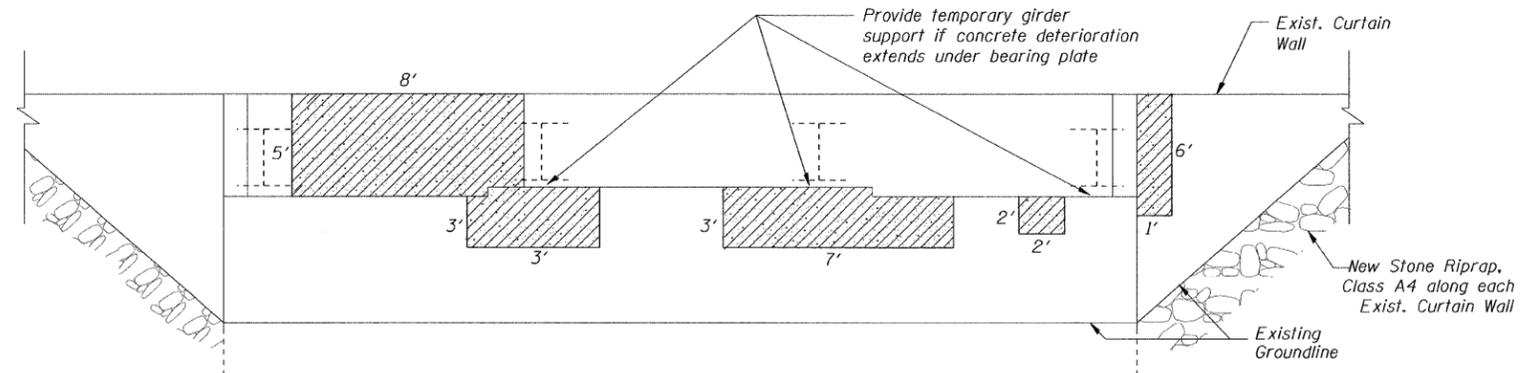
SECTION B-B

STEEL BEARING EXTENSION

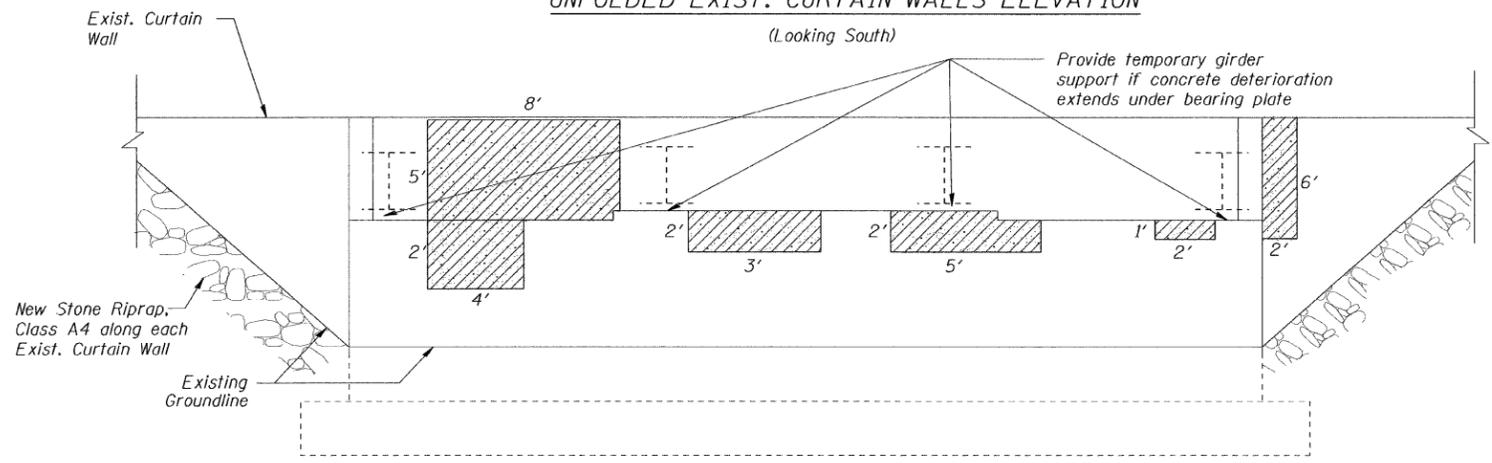
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

TYPE I ELASTOMERIC BEARING
C.H. 17 (WILMOT ROAD) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
WHITESIDE COUNTY
STATION 3014+40.25
S.N. 098-0058

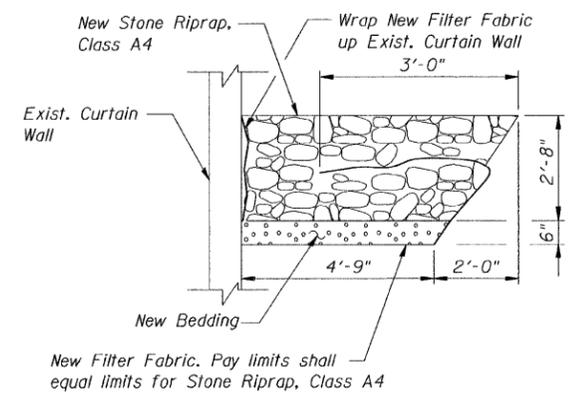
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS



EXIST. SOUTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking South)



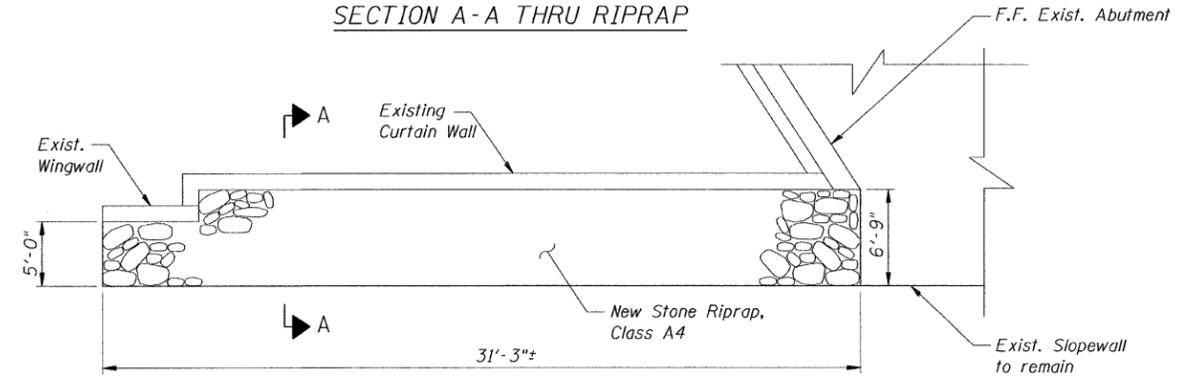
EXIST. NORTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking North)



SECTION A-A THRU RIPRAP

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less than 5")
- Concrete Removal



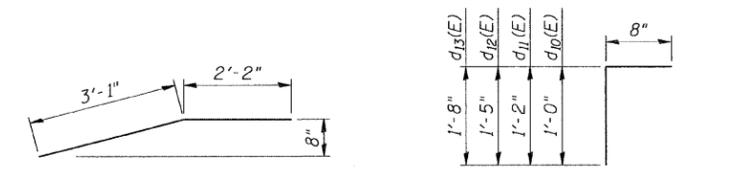
RIPRAP PLAN AT EXIST. ABUTMENTS

(Exist. North Abutment, west side shown. Other locations similar)

BILL OF MATERIAL

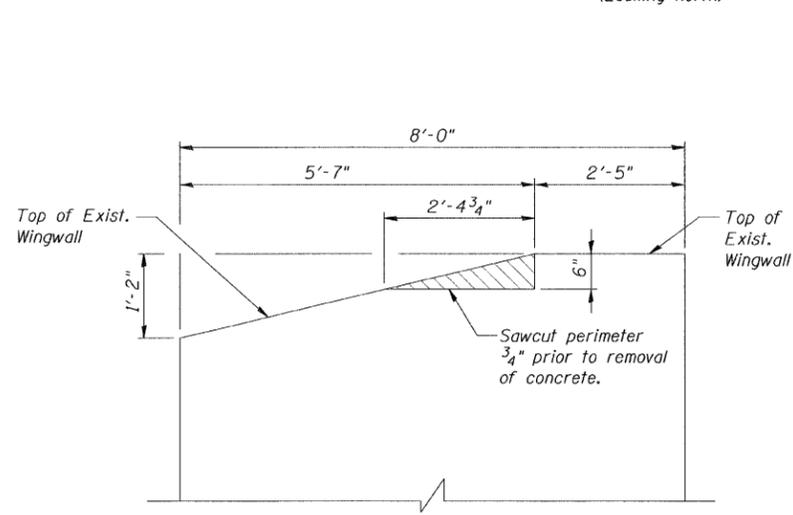
Bar	No.	Size	Length	Shape
d ₁₀ (E)	12	#5	1'-8"	□
d ₁₁ (E)	4	#5	1'-10"	□
d ₁₂ (E)	4	#5	2'-1"	□
d ₁₃ (E)	4	#5	2'-4"	□
e ₁₂ (E)	8	#5	5'-3"	—
e ₁₃ (E)	8	#5	5'-3"	—
Stone Riprap, Class A4		Sq. Yd.	98	
Filter Fabric		Sq. Yd.	98	
Concrete Removal		Cu. Yd.	0.1	
Concrete Superstructure		Cu. Yd.	0.6	
Protective Coat		Sq. Yd.	4	
Reinforcement Bars, Epoxy Coated		Pound	140	
Structural Repair of Concrete (Depth Equal to or Less than 5")		Sq. Ft.	198	

Note: Quantity for Structural Repair of Concrete has been increased to include additional repair areas not apparent at the time of the inspection. The actual repair areas will be determined by the Resident Engineer. The Contractor will be paid for the quantity furnished.



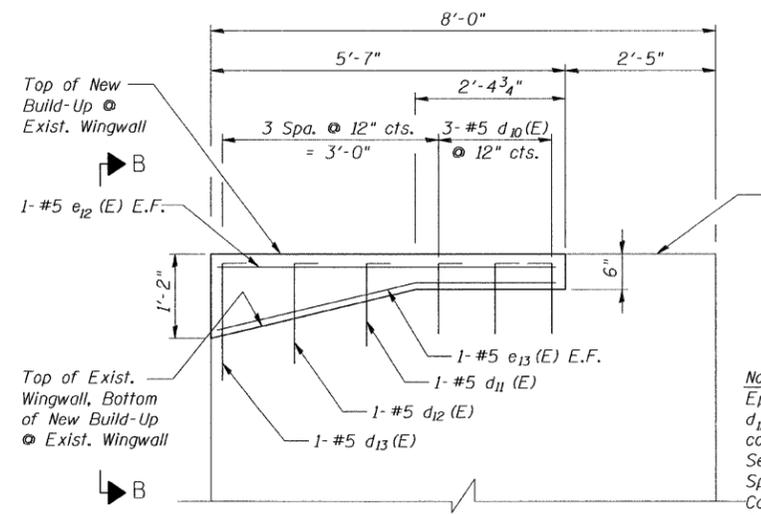
BAR e₁₃(E)

BARS d₁₀(E), d₁₁(E), d₁₂(E), d₁₃(E)



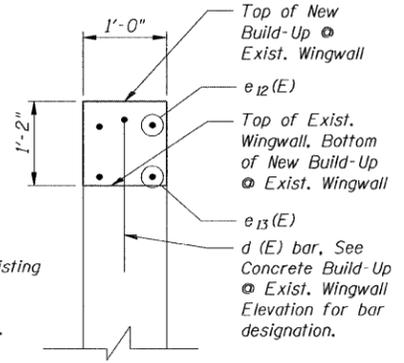
CONCRETE REMOVAL @ EXIST. WINGWALL ELEVATION

(O.F. of Northwest Exist. Wingwall shown, looking east. Other locations similar)



CONCRETE BUILD-UP @ EXIST. WINGWALL ELEVATION

(O.F. of Northwest Exist. Wingwall shown, looking east. Other locations similar)



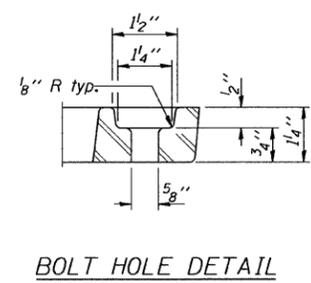
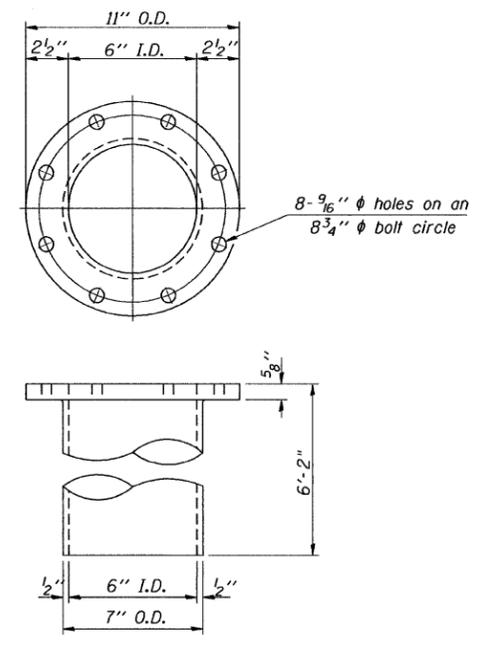
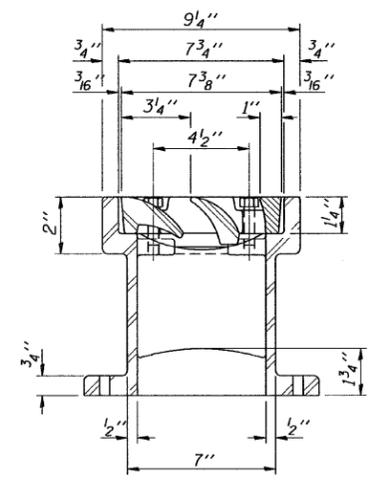
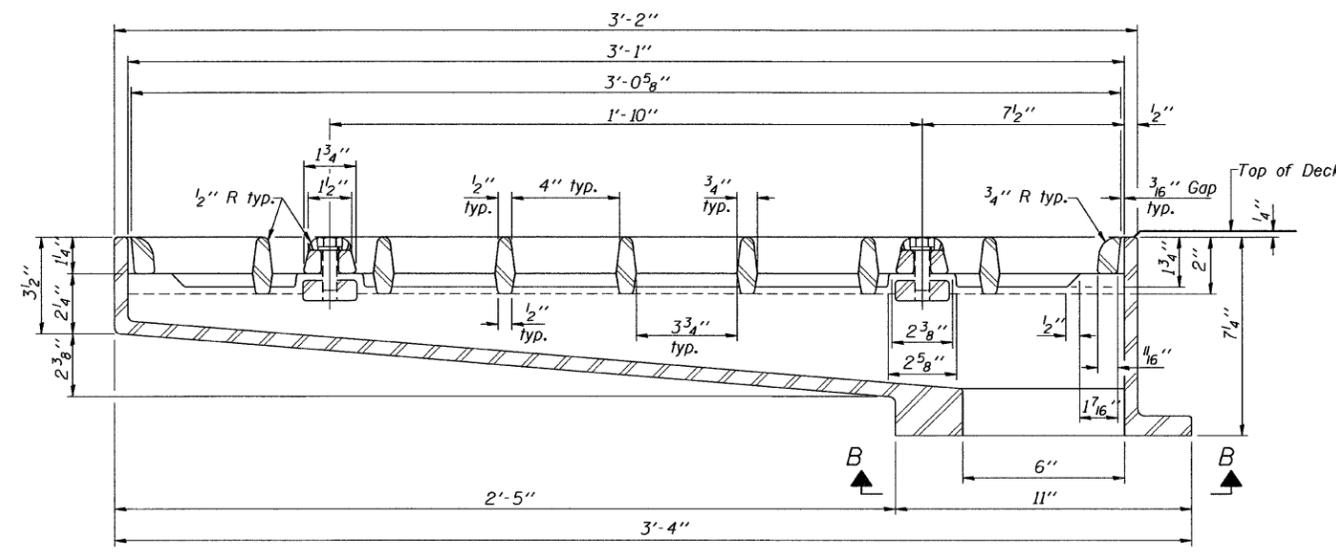
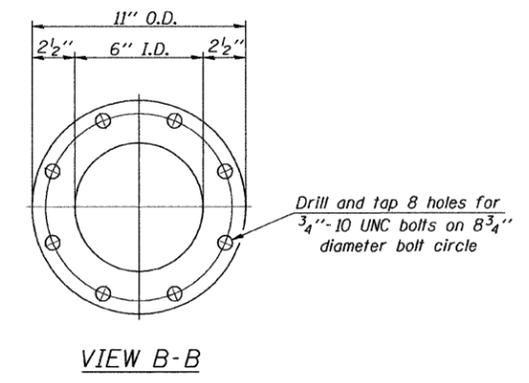
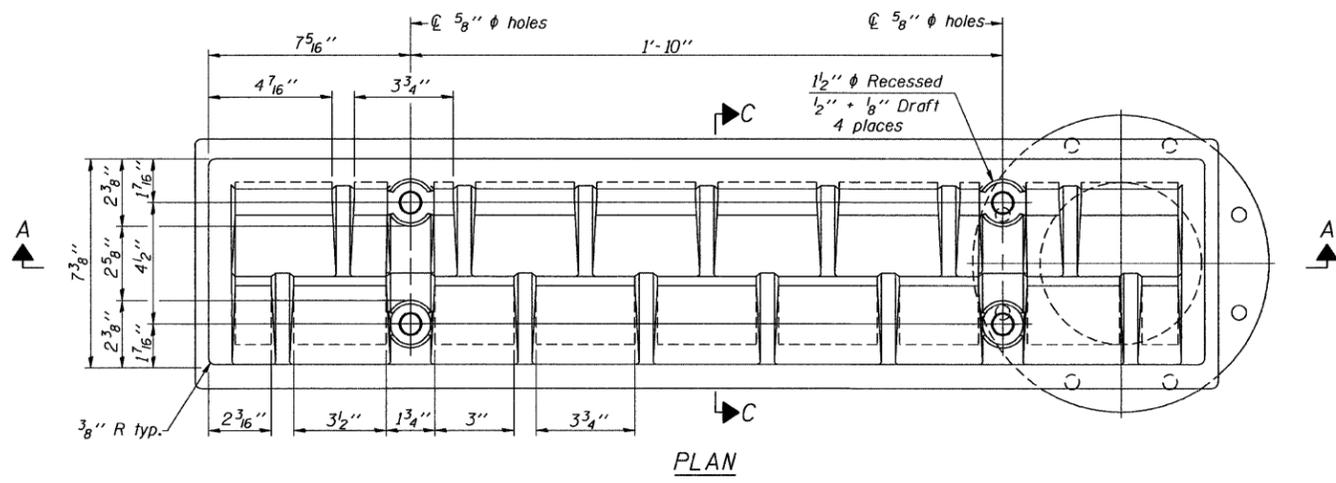
VIEW B-B

Note: Epoxy grout d₁₀(E), d₁₁(E), d₁₂(E) & d₁₃(E) bars into existing concrete in accordance with Section 584 of the Standard Specifications, min. 8" embed. Cost included with Concrete Superstructure.

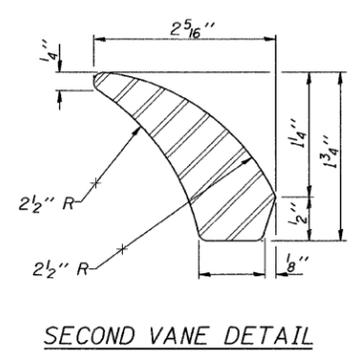
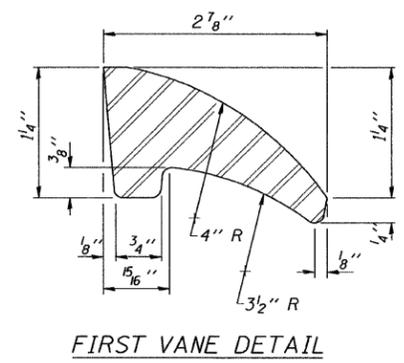
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

SUBSTRUCTURE REPAIR & RIPRAP DETAILS
C.H. 17 (WILMOT ROAD) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
WHITESIDE COUNTY
STATION 3014+40.25
S.N. 098-0058
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

n:\jobs\2008\p101\2001-4014.09\098-0058\Wilmot Rd.\00-098-0058-S39.dgn
12/8/2008



See sheet of for scupper location relative to parapet.



BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-33	Each	4

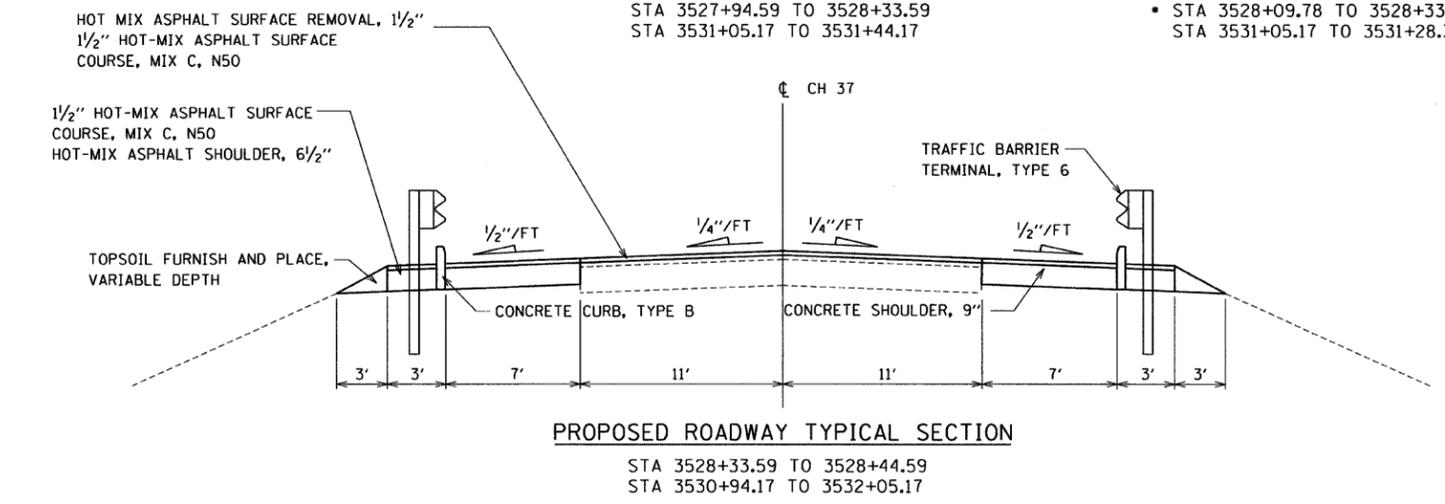
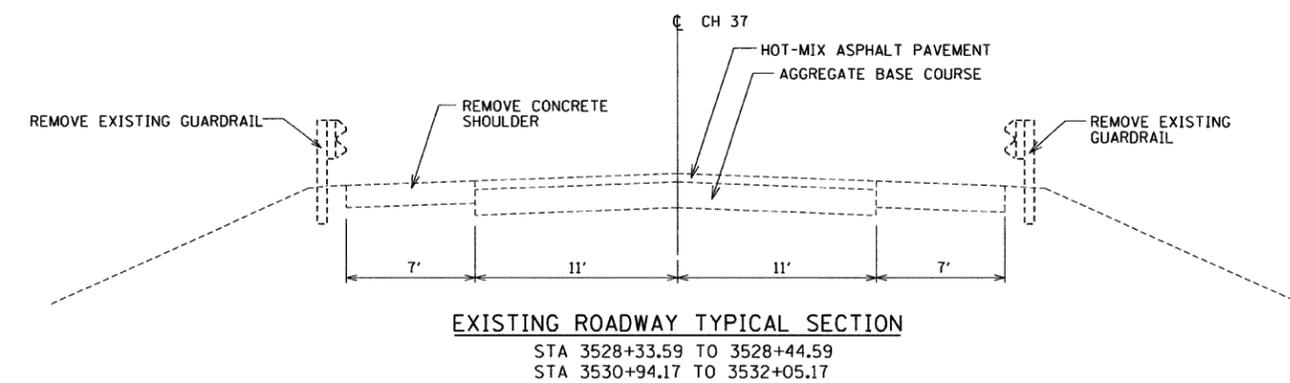
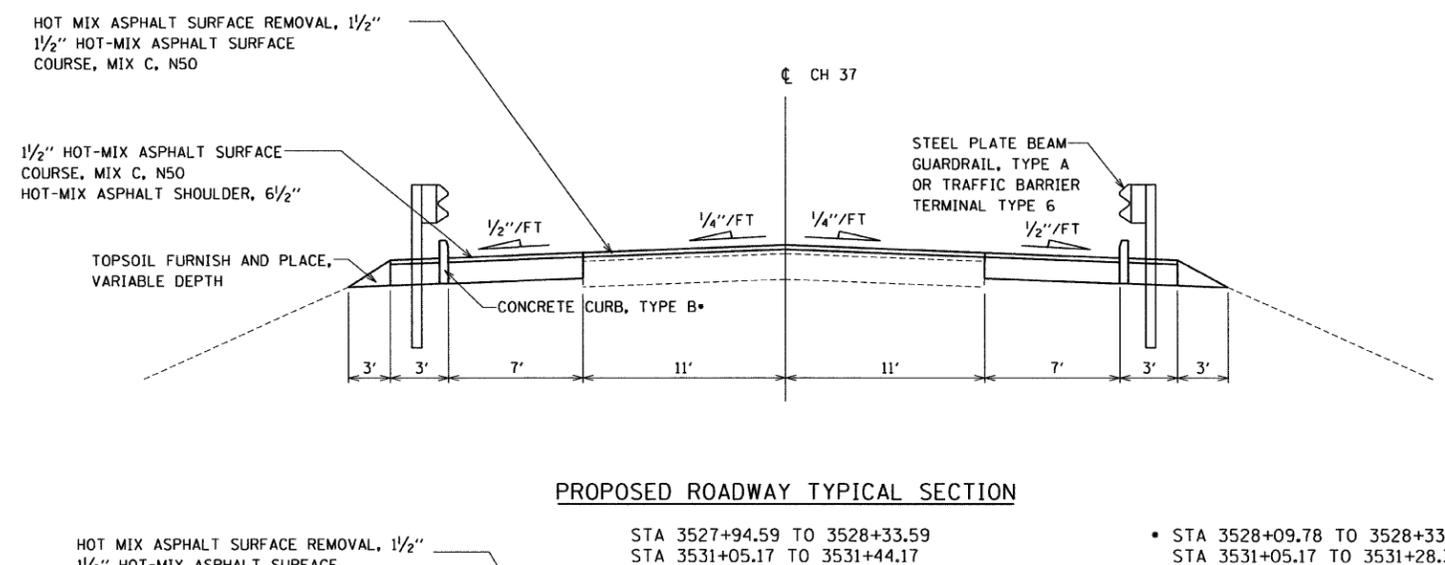
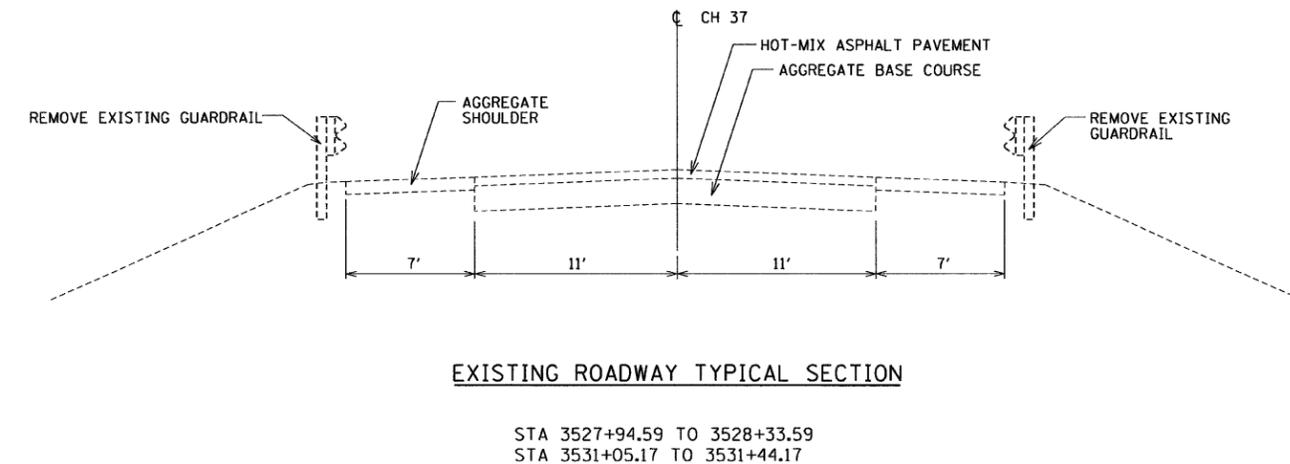
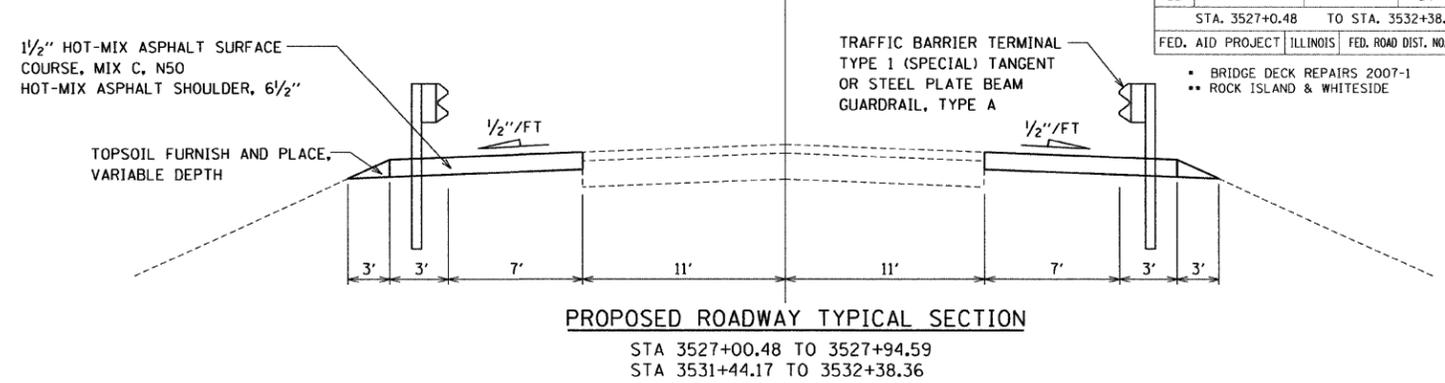
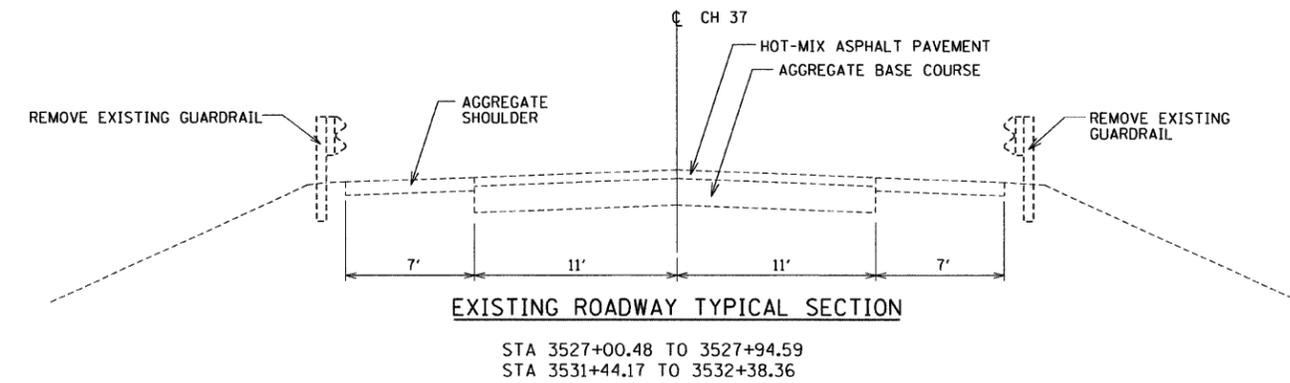
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

DS-33 5-16-08

DRAINAGE SCUPPER, DS-33
 C.H. 17 (WILMOT ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3014+40.25
 S.N. 098-0058
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

n:\jobs\2008\plot\2001-4014.09\098-0058\Wimot Rd.\00\098-0058-S40.dgn 12/8/2008

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	41
STA. 3527+00.48		TO STA. 3532+38.36		
FED. AID PROJECT		ILLINOIS	FED. ROAD DIST. NO.	
<ul style="list-style-type: none"> • BRIDGE DECK REPAIRS 2007-1 •• ROCK ISLAND & WHITESIDE 				



HOT-MIX ASPHALT SURFACE COURSE 112 LBS/IN THICK/SO YD
 HOT-MIX ASPHALT SHOULDER 112 LBS/IN THICK/SO YD
 TEMPORARY EROSION CONTROL SEEDING 100 LB/ACRE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 37 (LYNDON ROAD) BRIDGE OVER I-88
 STRUCTURE NO.: 098-0066
 BRIDGE REHABILITATION

TYPICAL SECTIONS

SCALE: NTS
 DATE 12/09/08

DRAWN BY EF
 CHECKED BY RS

H:_jobs\001\2009\04\09\CH37\Site\Drawings\001_T_00_66Sect_V3.dgn 2:06:55 PM 2/8/2009

CONTRACT NO. 64D24

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	42
STA. 3527+00.48		TO STA. 3532+38.36		
FED. AID PROJECT	ILLINOIS	FED. ROAD DIST. NO.		

- BRIDGE DECK REPAIRS 2007-1
- ROCK ISLAND & WHITESIDE



1490+00
 I-88

STA. 3529+69.36 C CH 37 (LYNDON ROAD)
 STA 1489+04.50 C I-88

END DECK RESURFACING
 STA. 3530+94.17

PAVED SHOULDER REMOVAL
 PCC SHOULDER, 9"

3530+00
 CH 37

GUARDRAIL REMOVAL
 TRAFFIC BARRIER TERMINAL, TYPE 1
 (SPECIAL) TANGENT
 STEEL PLATE BEAM GUARDRAIL, TYPE A
 & TRAFFIC BARRIER TERMINAL TYPE 6

BEGIN DECK RESURFACING
 STA. 3528+44.59

PAVED SHOULDER REMOVAL
 PCC SHOULDER, 9"

SFILES SGT Jun 10 13:24:56M09

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 37 (LYNDON ROAD) BRIDGE OVER I-88
 STRUCTURE NO.: 098-0066
 BRIDGE REHABILITATION
PROPOSED ROADWAY PLAN
 SCALE: 1"=20'
 DATE 12/09/08
 DRAWN BY EF
 CHECKED BY RS

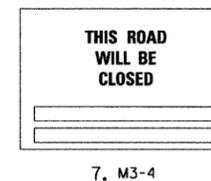
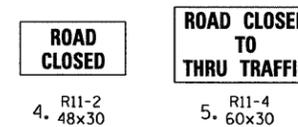
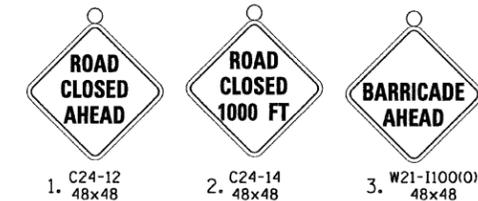
CONTRACT NO. 64D24

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	*	**	64	43
STA. 3527+00.48 TO STA. 3532+38.36				
FED. AID PROJECT		ILLINOIS	FED. ROAD DIST. NO.	

- BRIDGE DECK REPAIRS 2007-1
- ** ROCK ISLAND & WHITESIDE

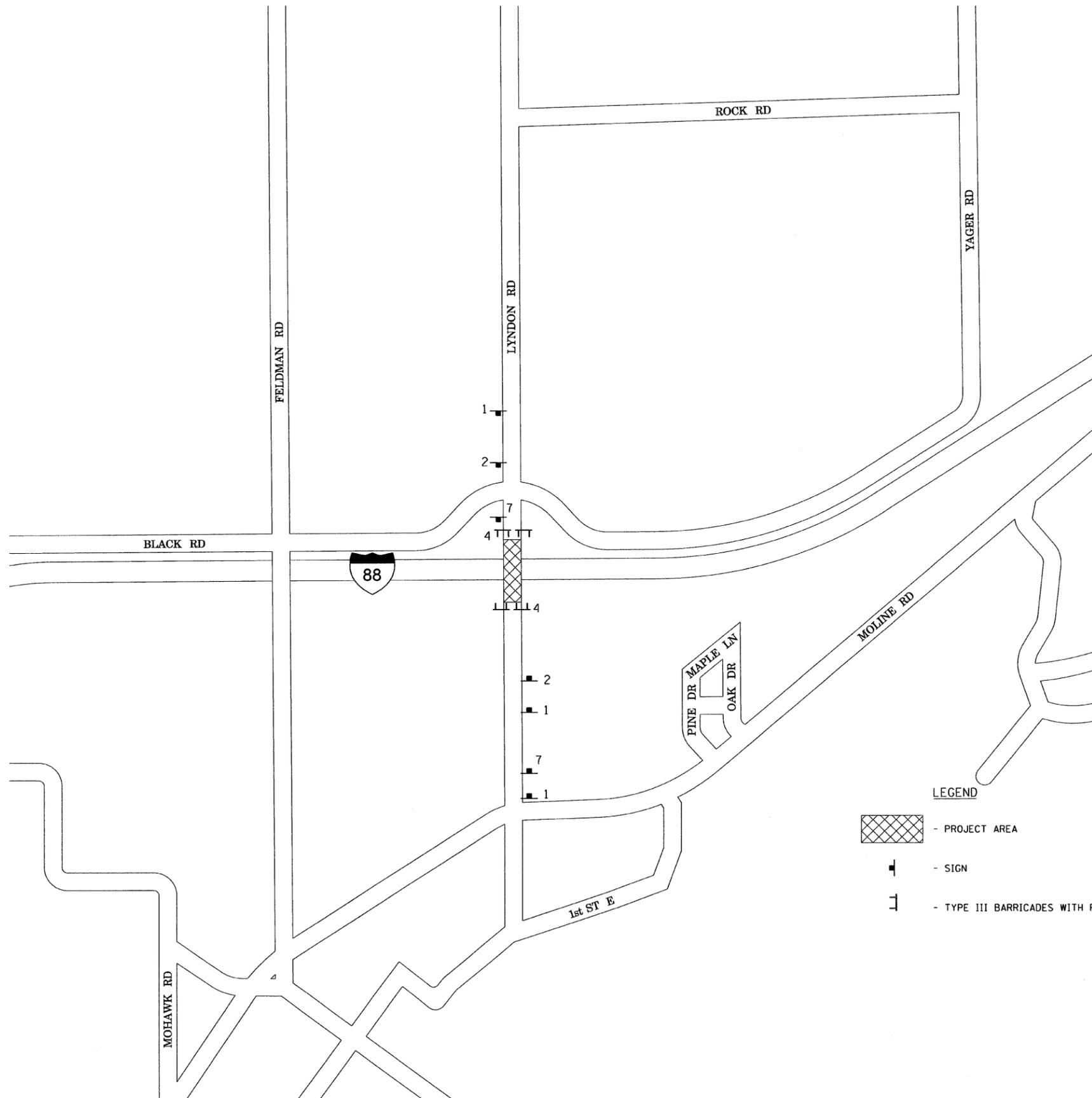
MAINTENANCE OF TRAFFIC

1. THE DEPARTMENT SHALL BE NOTIFIED 72 HOURS PRIOR TO ROAD CLOSURE AND IMMEDIATELY AFTER THE ROAD HAS BEEN REOPENED.
2. ROCK ISLAND COUNTY SHALL BE NOTIFIED AT LEAST TWO WEEKS PRIOR TO CLOSING THE ROAD.
3. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL TEMPORARY SIGN SUPPORTS. AFTER REMOVING THE SUPPORTS, THE CONTRACTOR SHALL FILL HOLES (IF ANY) AND RESTORE THE GROUND TO ITS ORIGINAL CONDITION AND ELEVATION. PLACING OF GRAVEL, SOD, OR SEED SHALL BE INCLUDED IN THE UNIT BID PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE".
4. ALL WARNING SIGNS IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND ON AN ORANGE BACKGROUND, UNLESS OTHERWISE NOTED.
5. REFER TO DISTRICT STANDARD 40.1 FIR SIGNING REQUIREMENTS
6. EACH TYPE III BARRICADE SHALL BE EQUIPPED WITH TWO FLASHING AMBER LIGHTS.
7. THE CONTRACTOR SHALL ERECT SIGN NO. 7 AT LEAST 2 WEEKS IN ADVANCE OF THE CLOSURE.
8. ALL ENTRANCES ON THE CLOSED PORTION OF 50TH AVENUE SHALL BE OPEN FOR TRAFFIC DURING THE ENTIRE CONSTRUCTION PERIOD.
9. THE WORK SHALL BE PAID FOR AT THE LUMP SUM PRICE FOR "TRAFFIC CONTROL FOR ROAD CLOSURE"



LEGEND

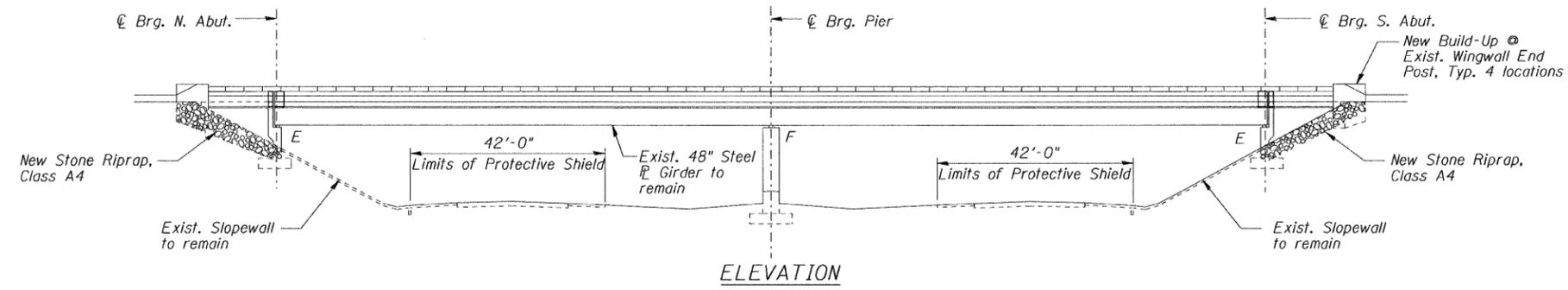
- PROJECT AREA
- SIGN
- TYPE III BARRICADES WITH FLASHERS



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 2/8/2008 2:17:56 PM

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CH 37 (LYNDON ROAD) BRIDGE OVER I-88
 STRUCTURE NO.: 098-0066
 BRIDGE REHABILITATION
MOT PLAN
 SCALE: NTS
 DATE 12/09/08
 DRAWN BY EF
 CHECKED BY RS



GENERAL NOTES

All structural steel shall be AASHTO M 270 Grade 50.

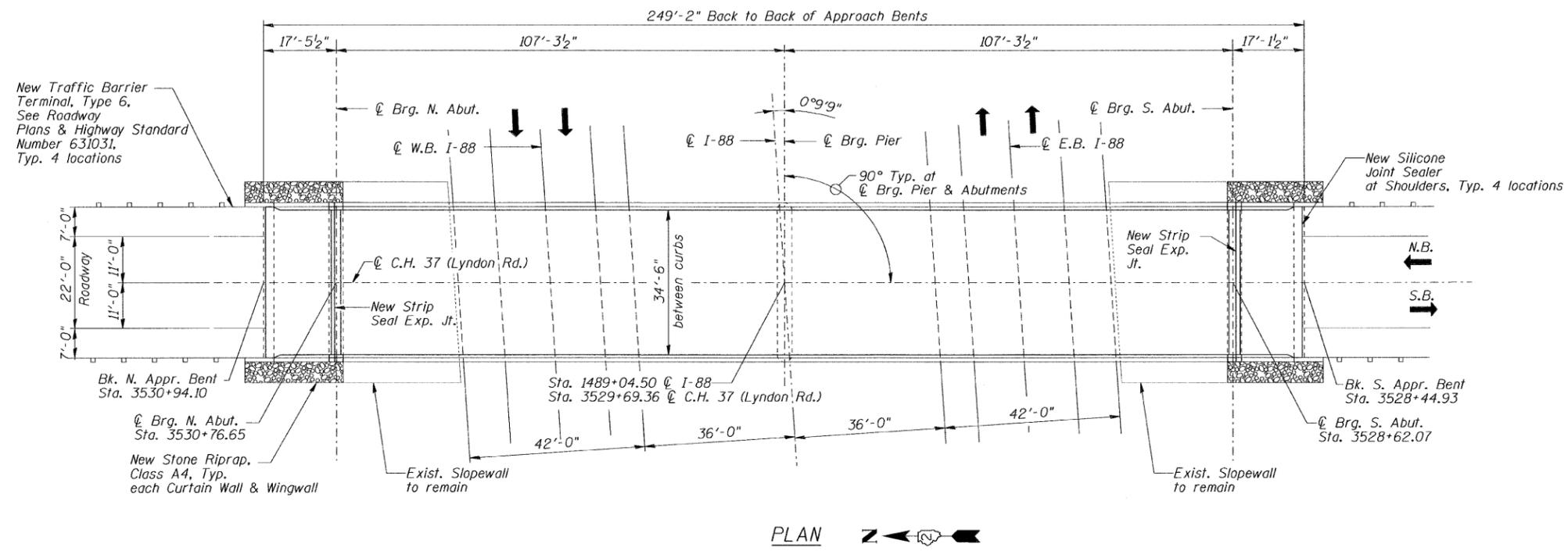
No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck at the expansion joint, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that cannot be removed by grinding 4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.



Plan dimensions and details relative to existing plans and are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Protective Coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

All structural steel at expansion joints shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Preformed Joint Strip Seal.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
STONE RIPRAP, CLASS A4	SQ. YD.	71
FILTER FABRIC	SQ. YD.	71
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	79
HOT-MIX ASPHALT SURFACE REMOVAL (ASBESTOS)	SQ. YD.	951
CONCRETE REMOVAL	CU. YD.	8.1
PROTECTIVE SHIELD	SQ. YD.	299
CONCRETE SUPERSTRUCTURE	CU. YD.	9.3
PROTECTIVE COAT	SQ. YD.	23
CLEANING AND PAINTING STEEL BRIDGE NO. 4	L. SUM	1
CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 4	L. SUM	1
REINFORCEMENT BARS, EPOXY COATED	POUND	1,390
PREFORMED JOINT STRIP SEAL	FOOT	73
JACK AND REMOVE EXISTING BEARINGS	EACH	10

ITEM	UNIT	QUANTITY
ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	10
ANCHOR BOLTS, 1"	EACH	40
FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1960
PLUG EXISTING DECK DRAINS	EACH	12
SILICONE JOINT SEALER, 1"	FOOT	28
WATERPROOFING MEMBRANE SYSTEM	SQ. YD.	937
DRAINAGE SCUPPERS, DS-33	EACH	4
STRUCTURAL REPAIR OF CONCRETE (DEPTH<5")	SQ. FT.	176
BRIDGE CLEANING AND PAINTING WARRANTY NUMBER 4	L. SUM	1
DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	2
DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	55
DECK SLAB REPAIR (PARTIAL)	SQ YD	126

Quantities for deck slab repair are approximate. Locations will be determined by the Resident Engineer following removal of the existing Hot-Mix Asphalt Surface Course. Actual repair locations shall be shown on the as-built plans. Contractor will be paid for the quantity furnished.

The existing Hot-Mix Asphalt Surface Course has tested positive for asbestos. Removal and disposal of the existing Hot-Mix Asphalt Surface Course shall be in accordance with the Special Provision for "Asbestos Waterproofing Membrane or Asbestos Hot-Mix Asphalt Surface Removal".

Cleaning and painting of the existing structural steel shall be as specified in the special provision for "Cleaning and Painting Existing Steel Structures". All existing steel shall be cleaned per Near White Blast Cleaning - SSPC-SP10. All existing steel shall be painted according to the requirements of Paint System 1 - OZ/E/U. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Blue, Munsell No. 10B 3/6.

A minimum of 2 air monitors will be required to monitor abrasive blasting operations at this site, see special provision for "Containment and Disposal of Lead Paint Cleaning Residues."

The SSPC QP1 & QP2 certifications will be required for this contract.

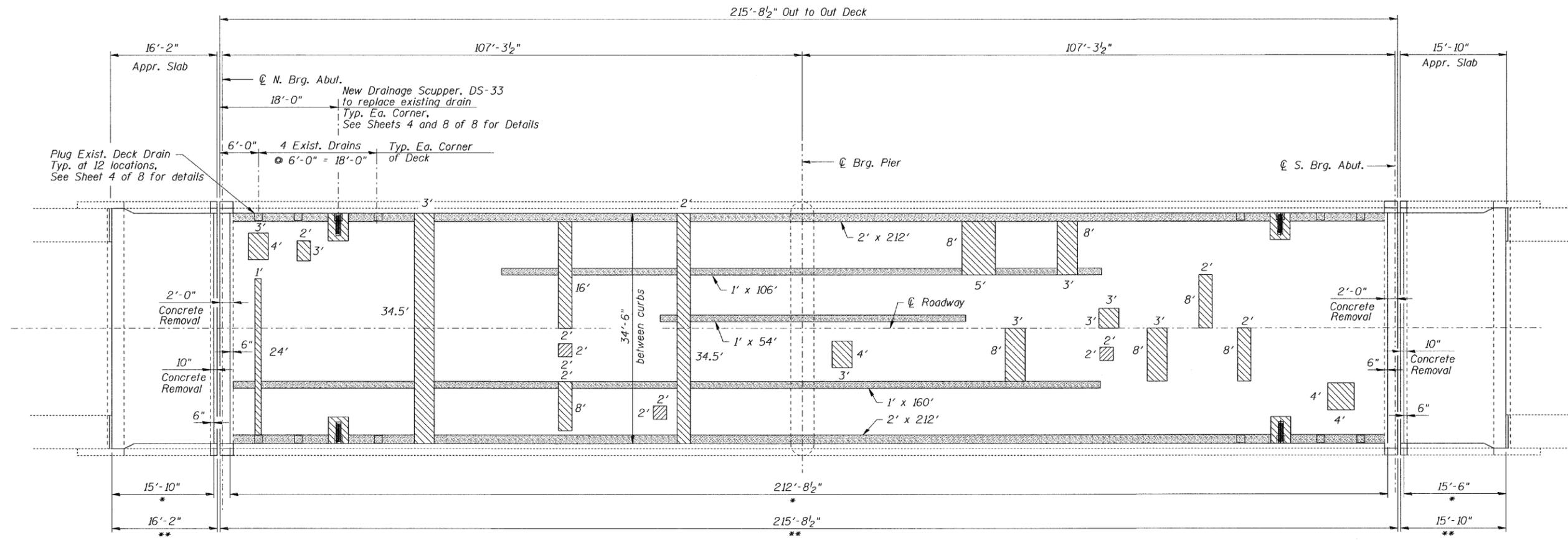
Existing plans are available from the District to determine painting quantities.



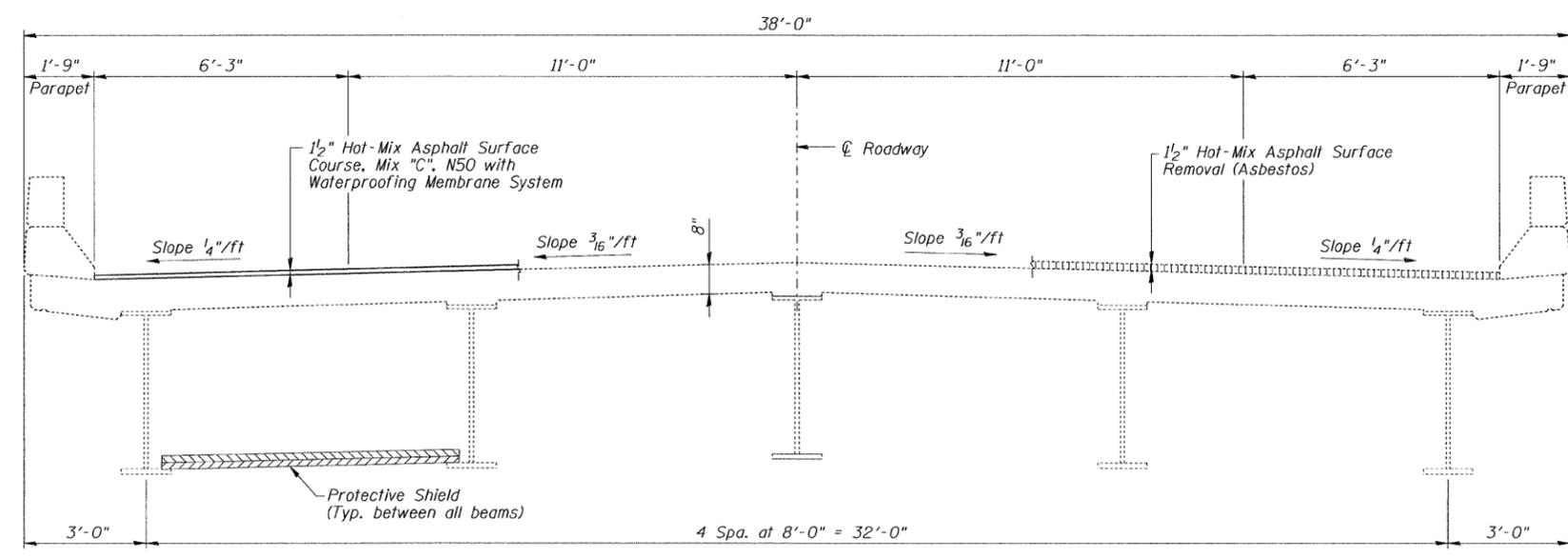
GENERAL PLAN & ELEVATION
 C.H. 37 (LYNDON ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3529+69.36
 S.N. 098-0066
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

n:\jobs\2008\plot\2001-4014-09\098 0066(Lyndon Rd.)\00\098-0066-S44.dgn
 12/8/2008

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.



PLAN



DECK CROSS SECTION

Notes:

- * - Limits of Hot-Mix Asphalt Surface Course, Mix "C", N50 with Waterproofing Membrane System
- ** - Limits of Hot-Mix Asphalt Surface Removal (Asbestos)

Lengths of Deck Slab Repair (Partial) identified on the Plan include areas of overlap with Deck Slab Repair (Full Depth, Type II). Where overlap occurs, Deck Slab Repair (Full Depth, Type II) shall be used.

- LEGEND**
- Deck Slab Repair (Full Depth, Type I)
 - Deck Slab Repair (Full Depth, Type II)
 - Deck Slab Repair (Partial)

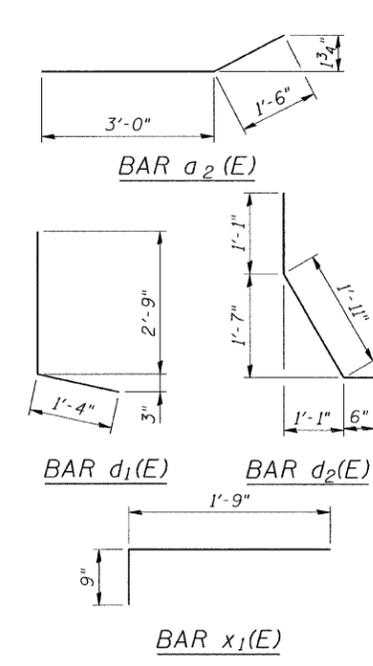
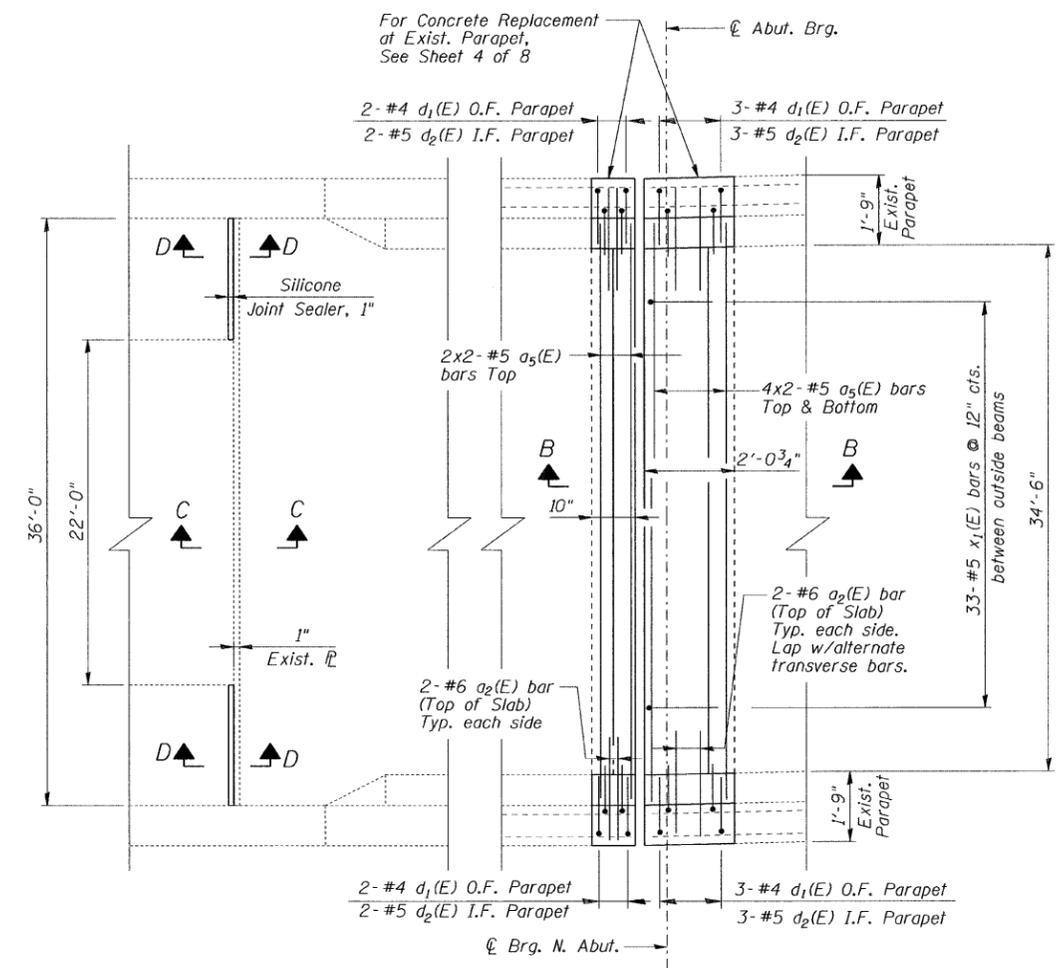
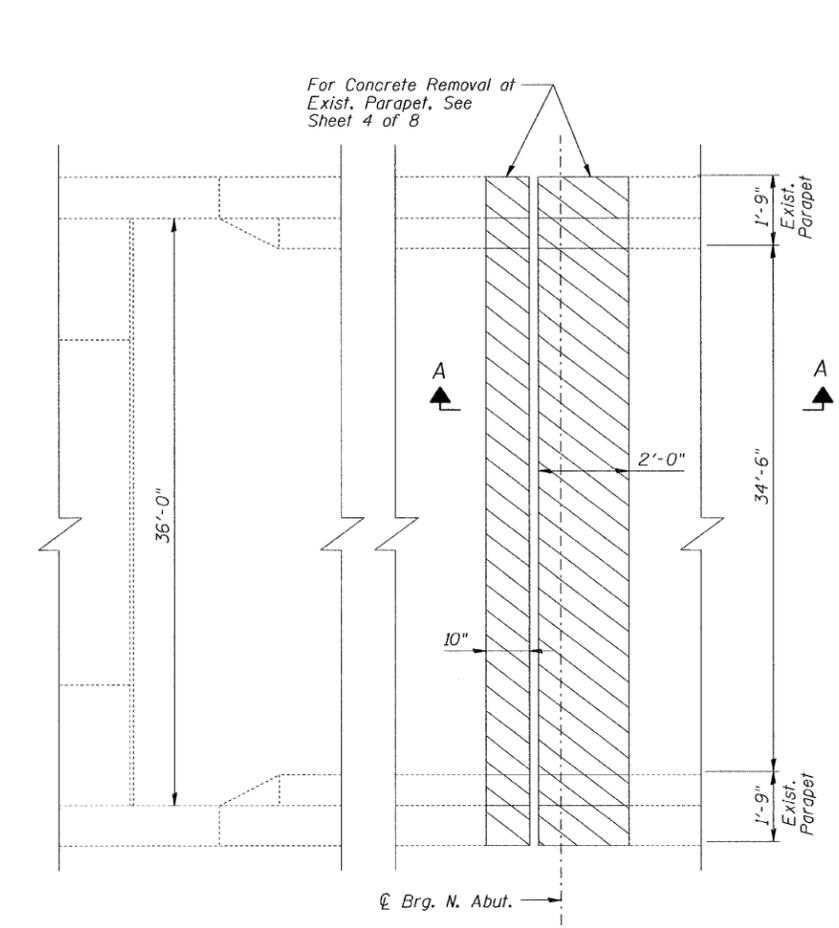
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Course, Mix "C", N50	Ton	79
Hot-Mix Asphalt Surface Removal (Asbestos)	Sq. Yd.	951
Protective Shield	Sq. Yd.	299
Plug Existing Deck Drains	Each	12
Waterproofing Membrane System	Sq. Yd.	937
Drainage Scuppers, DS-33	Each	4
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	2
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	55
Deck Slab Repair (Partial)	Sq. Yd.	126

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

DECK PLAN & CROSS SECTION
 C.H. 37 (LYNDON ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3529+69.36
 S.N. 098-0066
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

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 12/8/2008



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₂ (E)	16	#6	4'-6"	—
a ₅ (E)	40	#5	19'-6"	—
a ₁₀ (E)	16	#5	2'-0"	—
d ₁ (E)	20	#4	4'-1"	L
d ₂ (E)	20	#5	3'-6"	L
x ₁ (E)	66	#5	2'-6"	—
Concrete Removal			Cu. Yd.	8.0
Concrete Superstructure			Cu. Yd.	8.8
Protective Coat			Sq. Yd.	19
Reinforcement Bars, Epoxy Coated			Pound	1,260
Silicone Joint Sealer, 1"			Foot	28

LEGEND

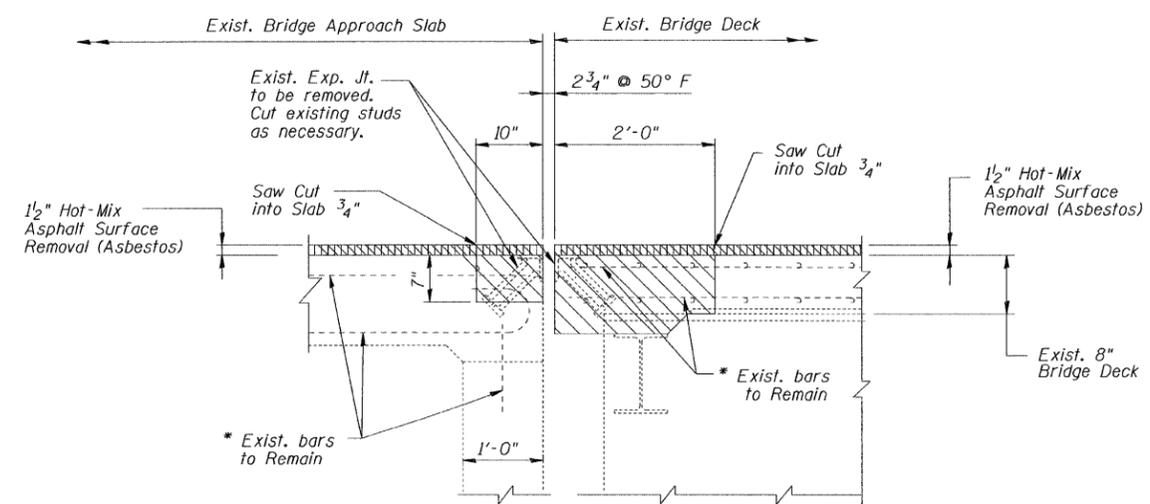
- Concrete Removal

NOTES:

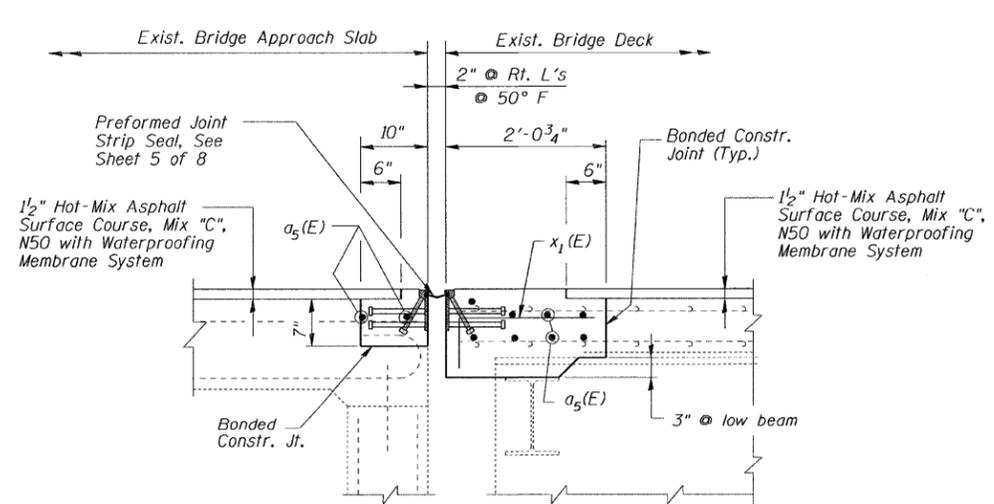
- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal.
- For Sections C-C and D-D, See Sheet 4 of 8.
- Bars indicated thus 4x2-#5 etc. indicates 4 lines of bars with 2 lengths per line.
- Lap #5 bars 2'-2" min.

CONCRETE REMOVAL @ EXIST. NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN

CONCRETE REPLACEMENT @ NORTH ABUTMENT EXP. JOINT PLAN
EXIST. SOUTH ABUTMENT SIM. OPP. HAND PLAN



SECTION A-A



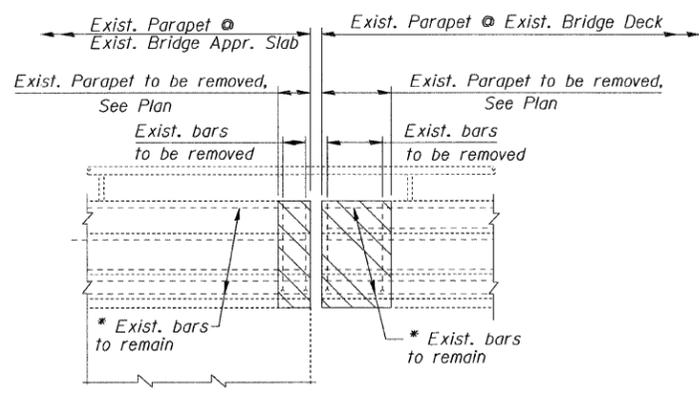
SECTION B-B

Note:
Dimensions are based on a Rolled Rail Strip Seal Joint.
If the Contractor elects to use the Welded Rail Strip Seal Joint, deck dimensions may require adjustments to satisfy the details on Base Sheet EJ-SSJ as shown on Sheet 5 of 8.

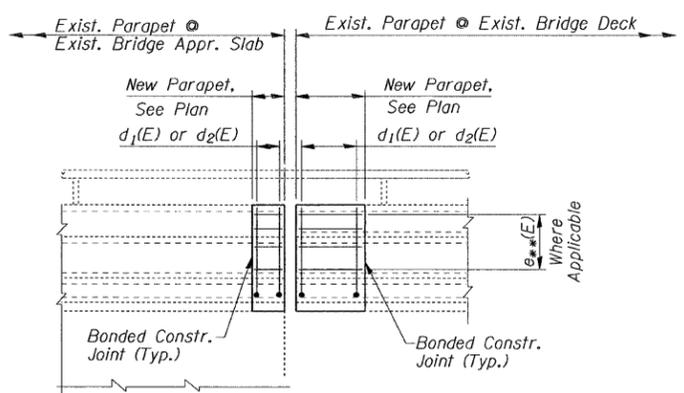
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

JOINT DETAILS
C.H. 37 (LYNDON ROUTE) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
WHITESIDE COUNTY
STATION 3529+69.36
S.N. 098-0066
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

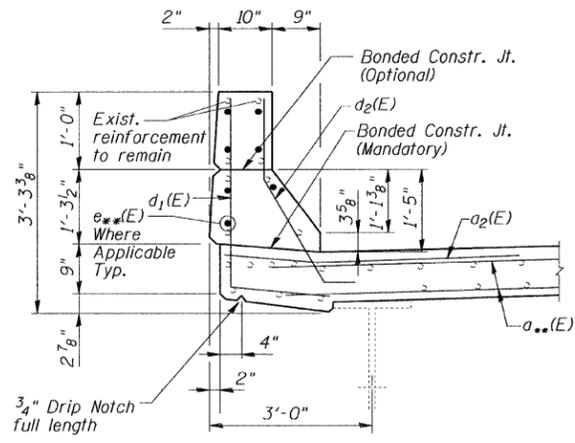
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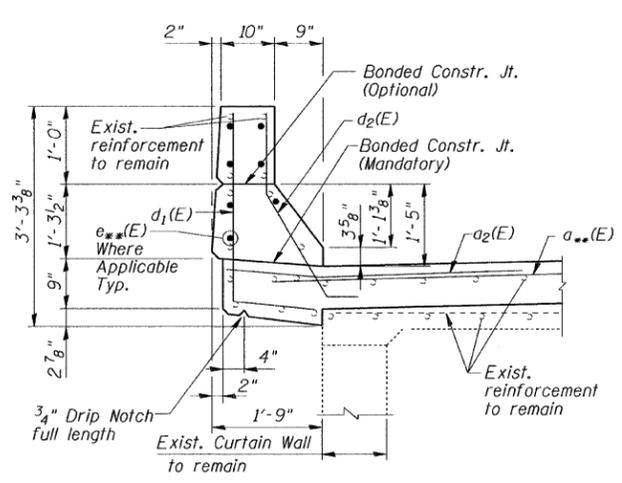
CONCRETE REMOVAL @
EXISTING PARAPET ELEVATION



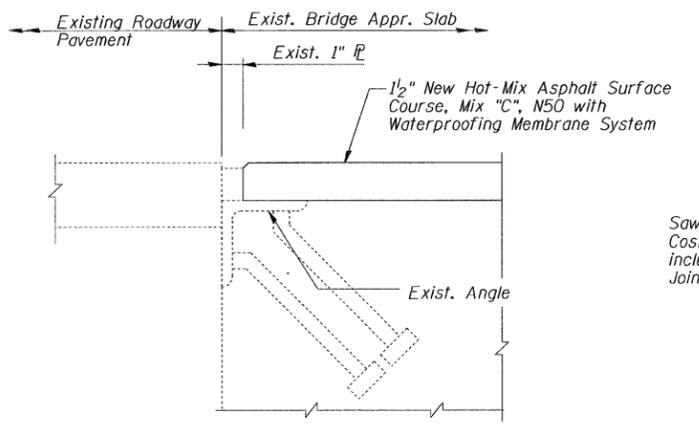
CONCRETE REPLACEMENT @
EXISTING PARAPET ELEVATION



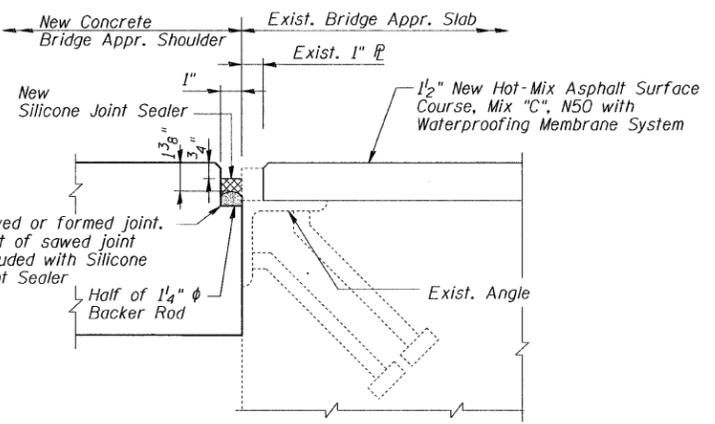
SECTION THRU NEW PARAPET
AT EXISTING BRIDGE DECK



SECTION THRU NEW PARAPET AT
EXISTING APPROACH SLAB



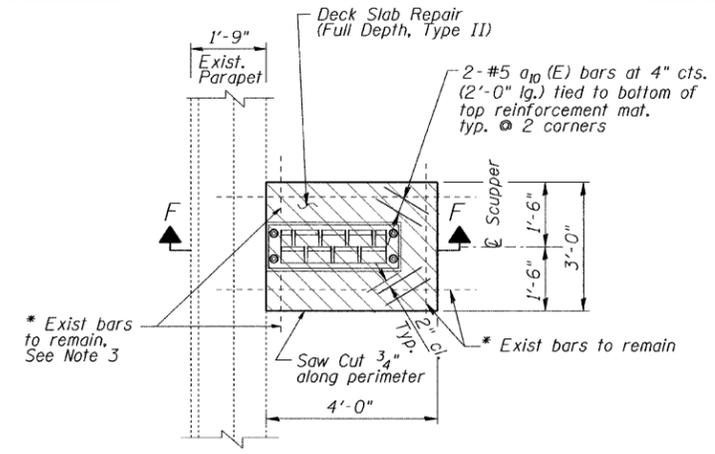
SECTION C-C



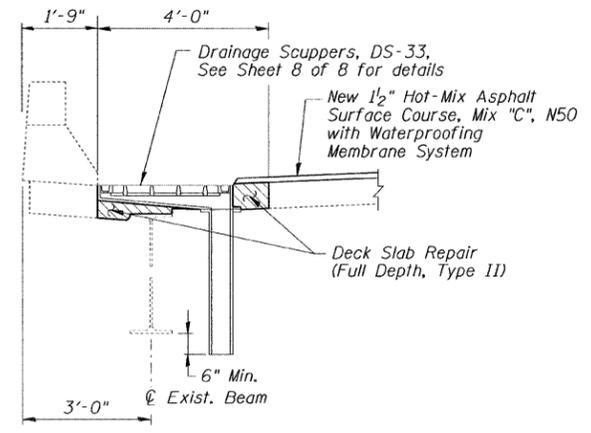
SECTION D-D

NOTES:

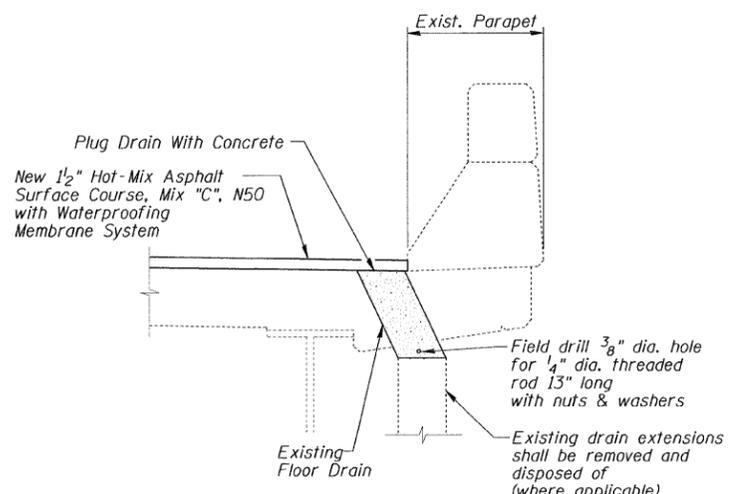
- * Exist. reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction. Cut to fit as required. Any reinforcement bars that are damaged during concrete removal shall be replaced with approved bar splicer or anchorage system. Cost included with Concrete Removal or Deck Slab Repair (Full Depth, Type II).
- ** See Plan on "Joint Details" sheet for bar designation.
- Cut exist. longitudinal reinforcement to clear drainage scupper.
- ▨ Hatched areas indicate concrete sections to be removed and replaced. Perimeters of concrete removal areas shall be saw cut 3/4" prior to the removal of concrete.



NEW SCUPPER PLAN



SECTION F-F



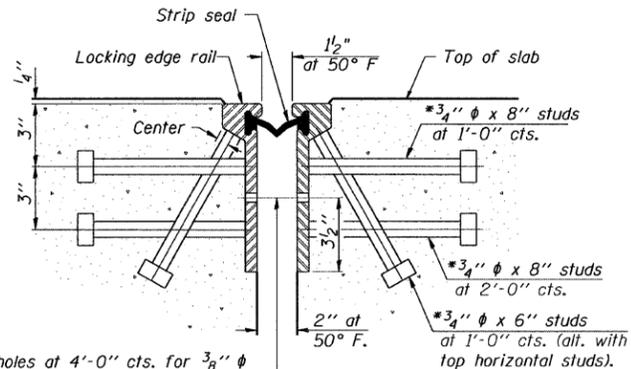
DRAIN ELIMINATION DETAIL

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

PARAPET & DECK SECTIONS & DETAILS
 C.H. 37 (LYNDON ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3529+69.36
 S.N. 098-0066
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

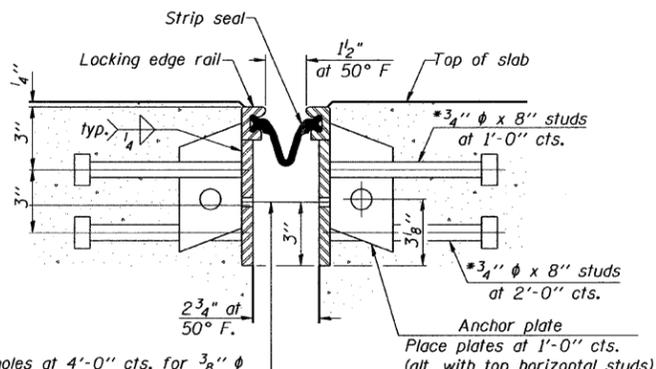
n:\jobs\2008\plot\2001-4014.09\098 0066(Lyndon Rd.)\00\098-0066-S47.dgn
 12/8/2008

*Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU WELDED RAIL JOINT

Notes:

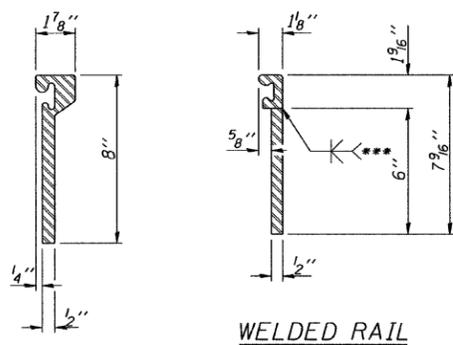
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed.

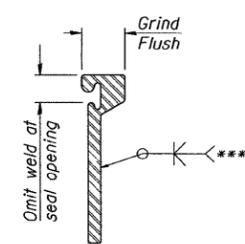
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



ROLLED EXTRUDED RAIL

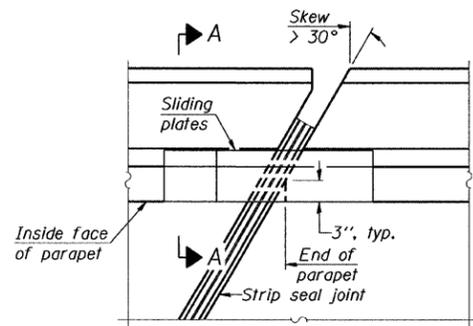
WELDED RAIL



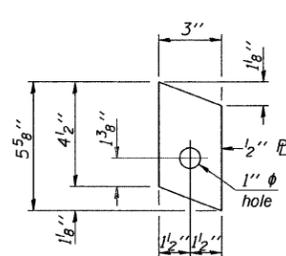
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

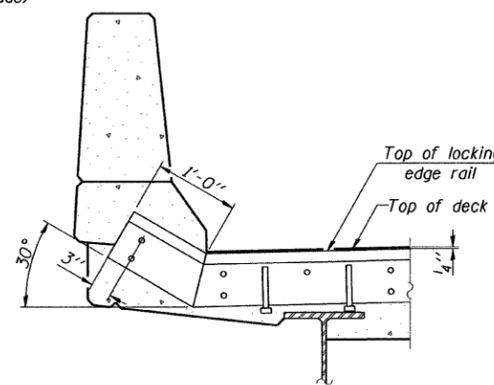
LOCKING EDGE RAILS



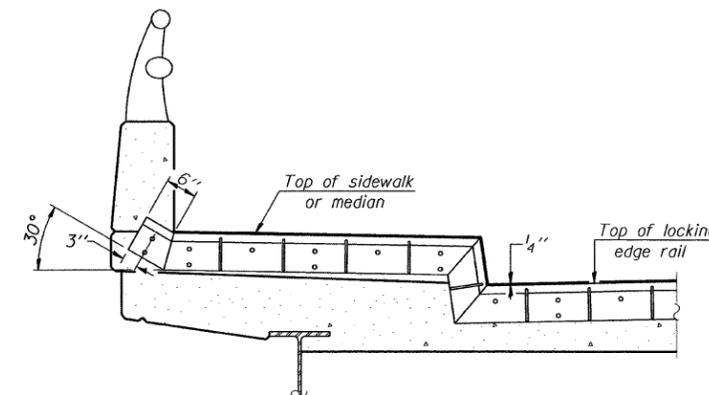
PLAN



ANCHOR PLATE (for welded rail)



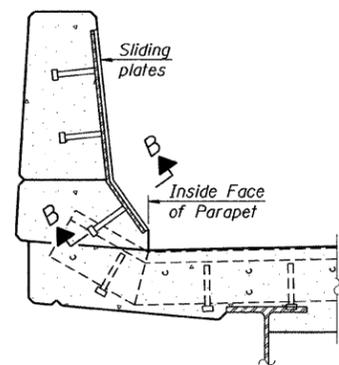
AT PARAPET



AT SIDEWALK OR MEDIAN

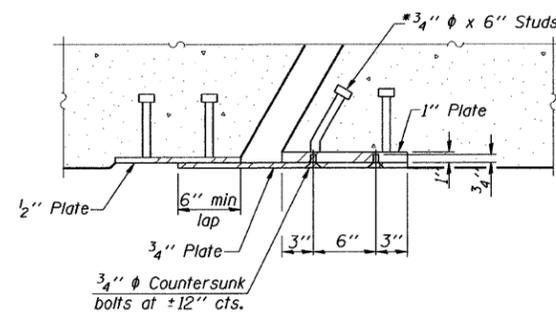
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

TYPICAL END TREATMENTS



SECTION A-A

POINT BLOCK DETAILS (for skews > 30°)



SECTION B-B

BILL OF MATERIAL

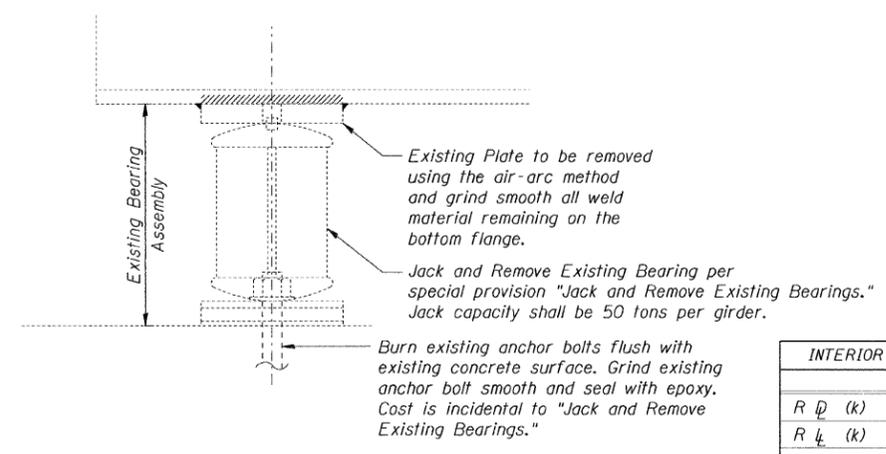
Item	Unit	Total
Preformed Joint Strip Seal	Foot	73

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

EJ-SSJ 5-16-08

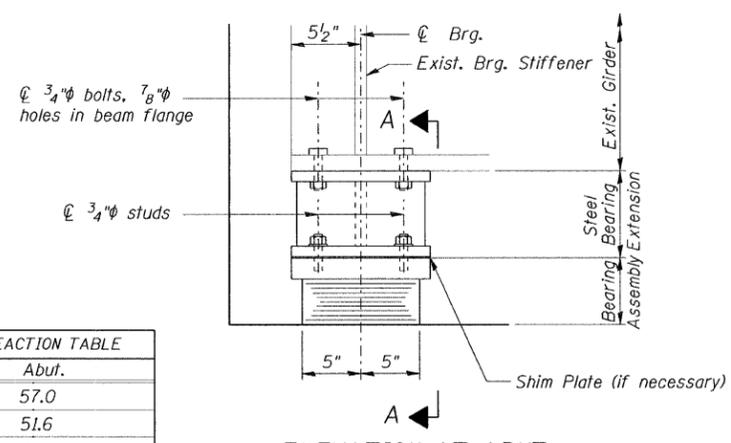
PREFORMED JOINT STRIP SEAL
C.H. 37 (LYNDON ROAD) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
WHITESIDE COUNTY
STATION 3529+69.36
S.N. 098-0066

DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

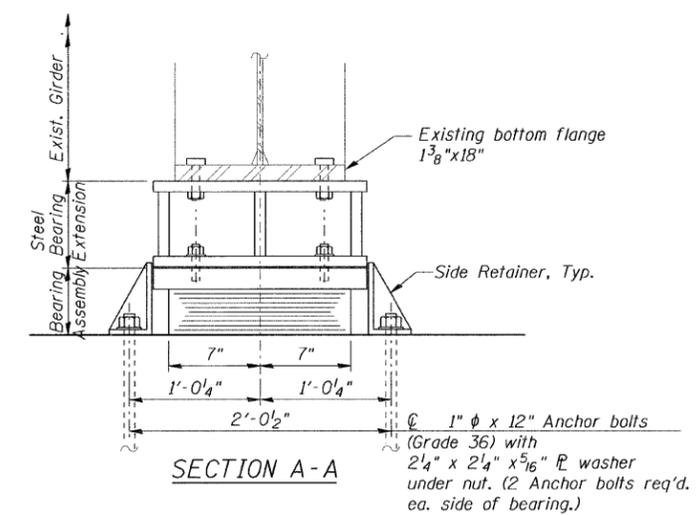


EXISTING BEARING REMOVAL DETAIL

INTERIOR GIRDER REACTION TABLE	
	Abut.
R _D (k)	57.0
R _L (k)	51.6
Imp. (k)	11.1
R _{Total} (k)	119.7

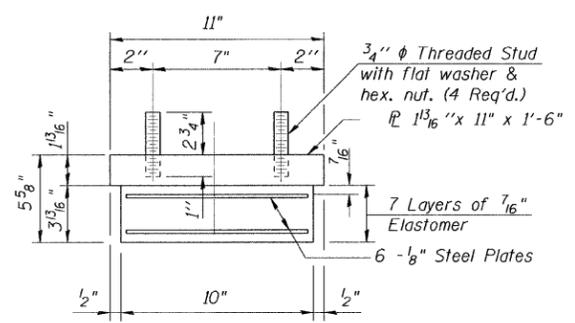


ELEVATION AT ABUT.



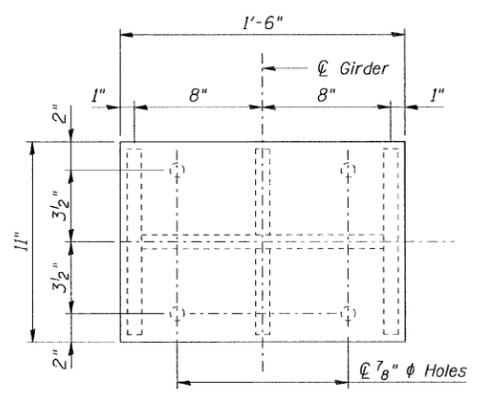
SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.



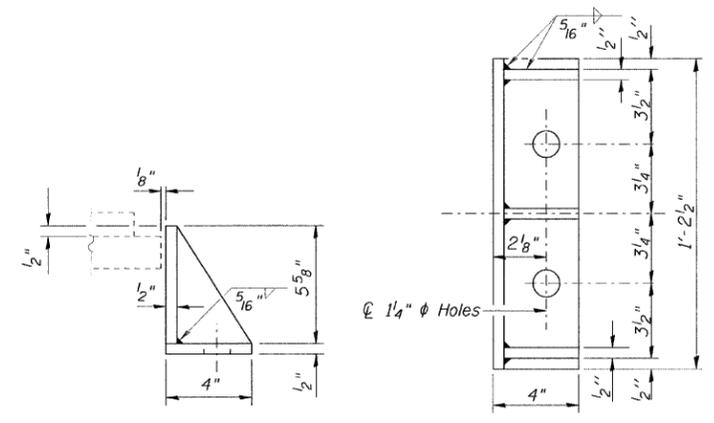
BEARING ASSEMBLY

Note: Shim plates shall not be placed under Bearing Assembly.

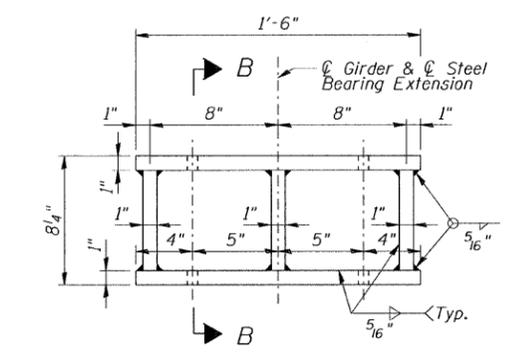


PLAN STEEL BEARING EXTENSION

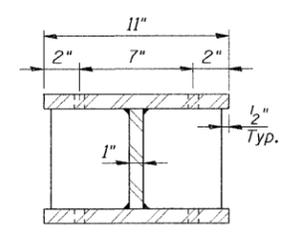
Notes:
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type I.
 The structural steel plates of the Bearing Assembly and the Steel Bearing Extension shall conform to the requirements of AASHTO M 270 Grade 36.
 Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New Steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.



SIDE RETAINER
 Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



ELEVATION STEEL BEARING EXTENSION



SECTION B-B

STEEL BEARING EXTENSION

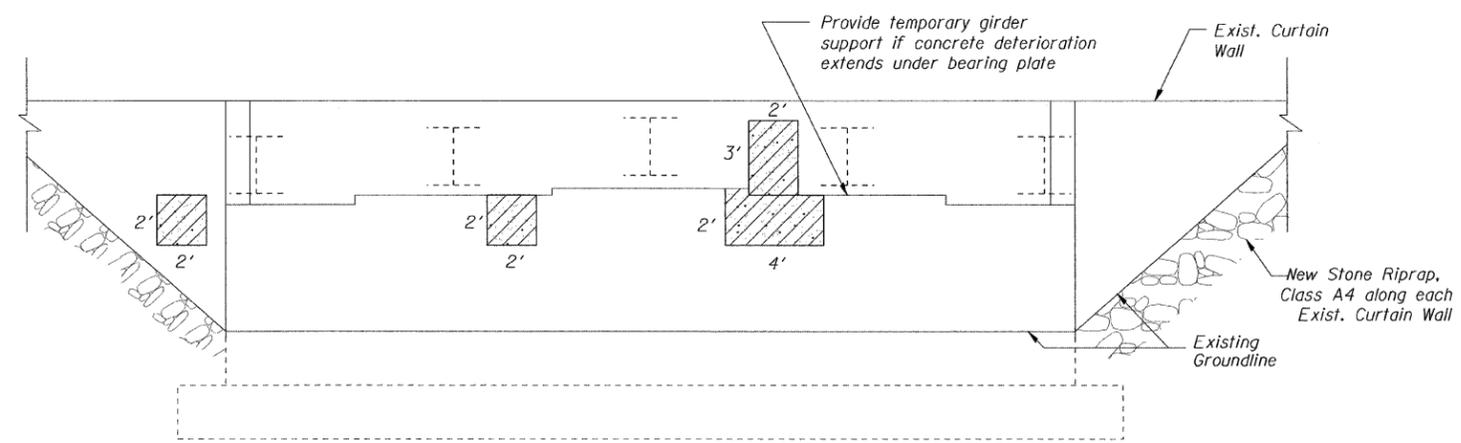
BILL OF MATERIAL

Item	Unit	Total
Jack and Remove Existing Bearings	Each	10
Elastomeric Bearing Assembly, Type I	Each	10
Anchor Bolts, 1"	Each	40
Furnishing and Erecting Structural Steel	Pound	1,960

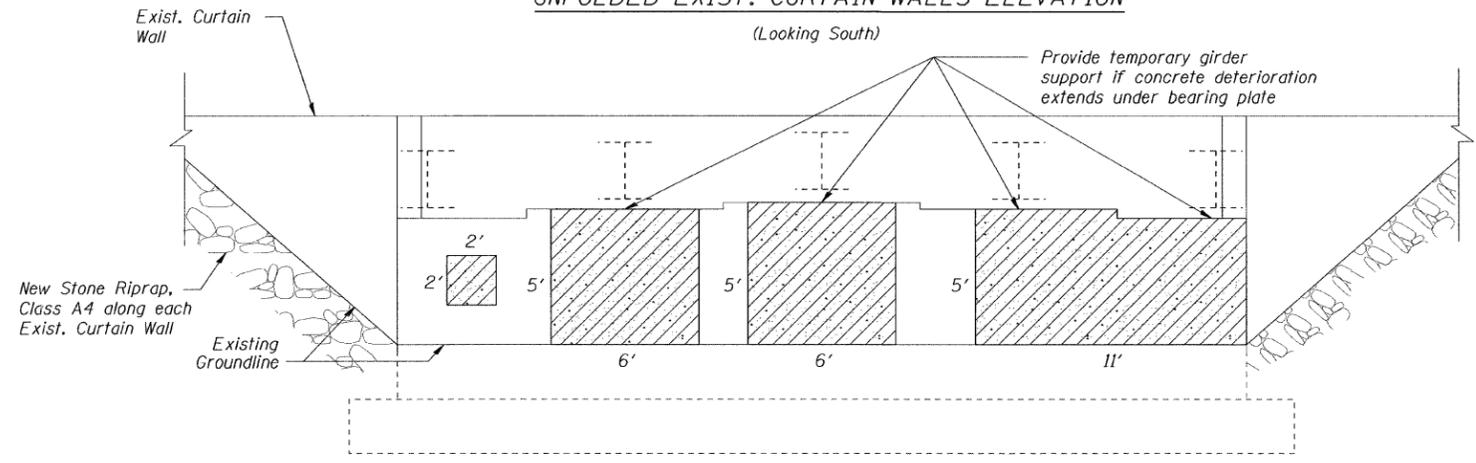
DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

TYPE I ELASTOMERIC BEARING
 C.H. 37 (LYNDON ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3529+69.36
 S.N. 098-0066
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

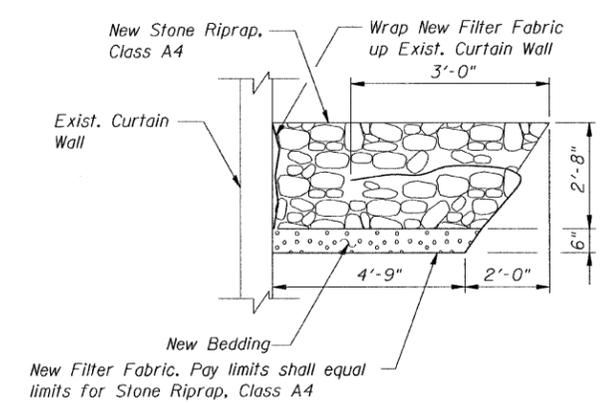
n:\jobs\2008\plot\2001-4014-09\098 0066(Lyndon Rd.)\00\098-0066-549.dgn
 12/8/2008



EXIST. SOUTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking South)



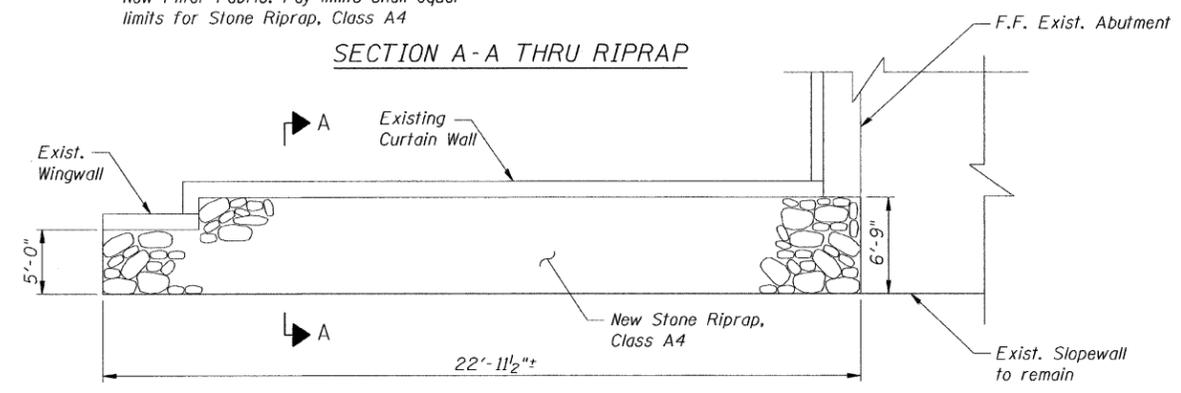
EXIST. NORTH ABUTMENT & UNFOLDED EXIST. CURTAIN WALLS ELEVATION
(Looking North)



SECTION A-A THRU RIPRAP

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less than 5")
- Concrete Removal



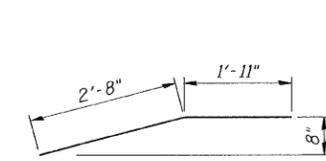
RIPRAP PLAN AT EXIST. ABUTMENTS

(Exist. North Abutment, west side shown. Other locations similar)

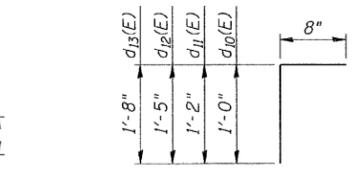
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d ₁₀ (E)	12	#5	1'-8"	┌
d ₁₁ (E)	4	#5	1'-10"	┌
d ₁₂ (E)	4	#5	2'-1"	┌
d ₁₃ (E)	4	#5	2'-4"	┌
e ₁₀ (E)	8	#5	4'-7"	—
e ₁₁ (E)	8	#5	4'-7"	└
Stone Riprap, Class A4		Sq. Yd.	71	
Filter Fabric		Sq. Yd.	71	
Concrete Removal		Cu. Yd.	0.1	
Concrete Superstructure		Cu. Yd.	0.5	
Protective Coat		Sq. Yd.	4	
Reinforcement Bars, Epoxy Coated		Pound	130	
Structural Repair of Concrete (Depth Equal to or Less than 5")		Sq. Ft.	176	

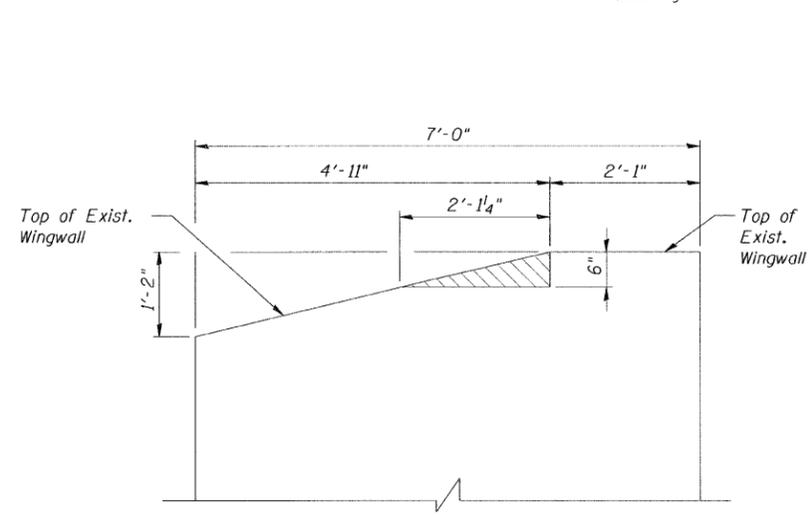
Note: Quantity for Structural Repair of Concrete has been increased to include additional repair areas not apparent at the time of the inspection. The actual repair areas will be determined by the Resident Engineer. The Contractor will be paid for the quantity furnished.



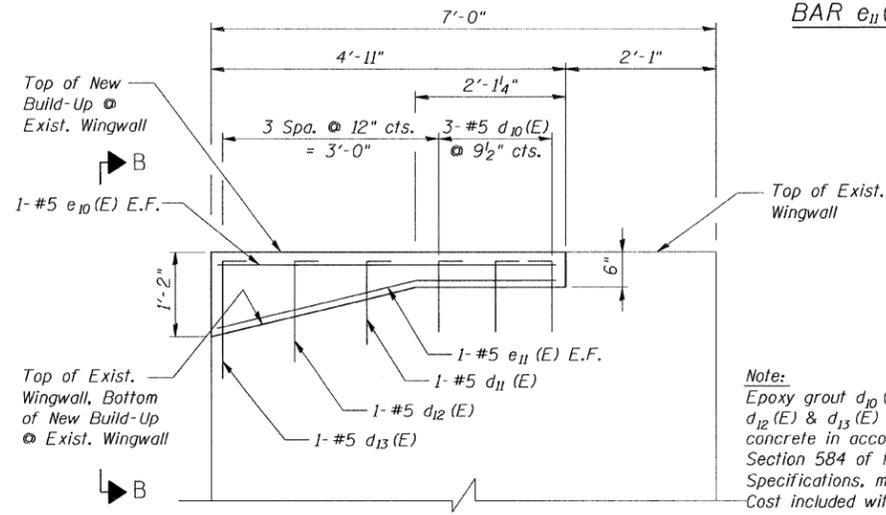
BAR e₁₁(E)



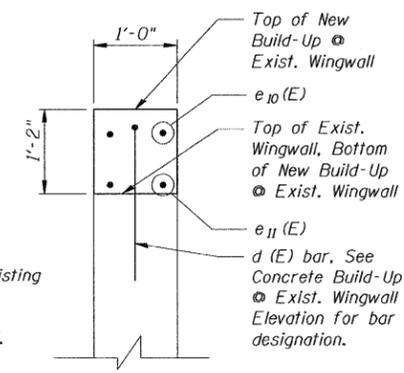
BARS d₁₀(E), d₁₁(E), d₁₂(E), d₁₃(E)



CONCRETE REMOVAL @ EXIST. WINGWALL ELEVATION
(O.F. of Northwest Exist. Wingwall shown, looking east. Other locations similar)



CONCRETE BUILD-UP @ EXIST. WINGWALL ELEVATION
(O.F. of Northwest Exist. Wingwall shown, looking east. Other locations similar)



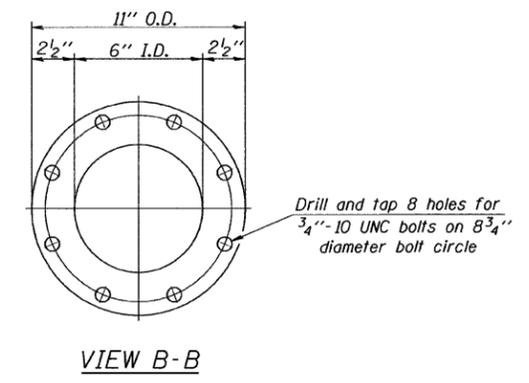
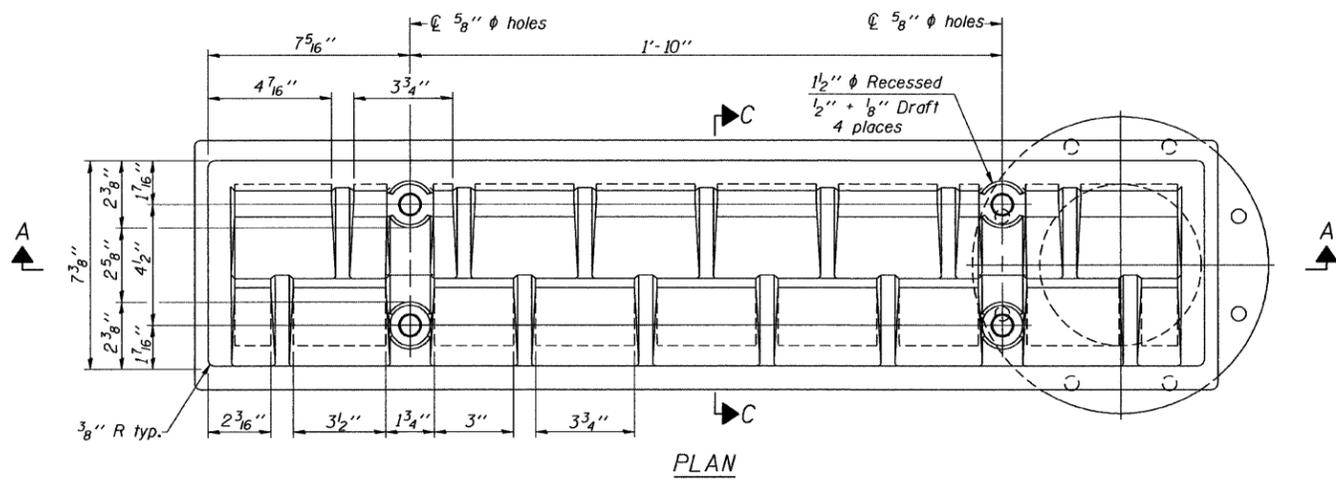
VIEW B-B

Note: Epoxy grout d₁₀(E), d₁₁(E), d₁₂(E) & d₁₃(E) bars into existing concrete in accordance with Section 584 of the Standard Specifications, min. 8" embed. Cost included with Concrete Superstructure.

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

SUBSTRUCTURE REPAIR & RIPRAP DETAILS
C.H. 37 (LYNDON ROAD) OVER I-88
F.A.I. ROUTE 88
SECTION: BRIDGE DECK REPAIRS 2007-1
WHITESIDE COUNTY
STATION 3529+69.36
S.N. 098-0066
DATE: 12-09-08
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
CHICAGO ILLINOIS

n:\jobs\2008\p10\2001-4014.09\098 0066(Lyndon Rd.)\00\098-0066-S50.dgn 12/8/2008



Notes:

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.

Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.

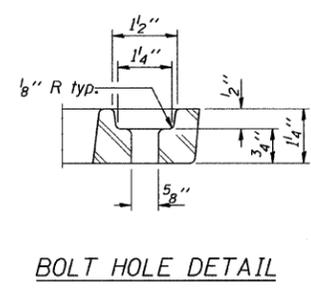
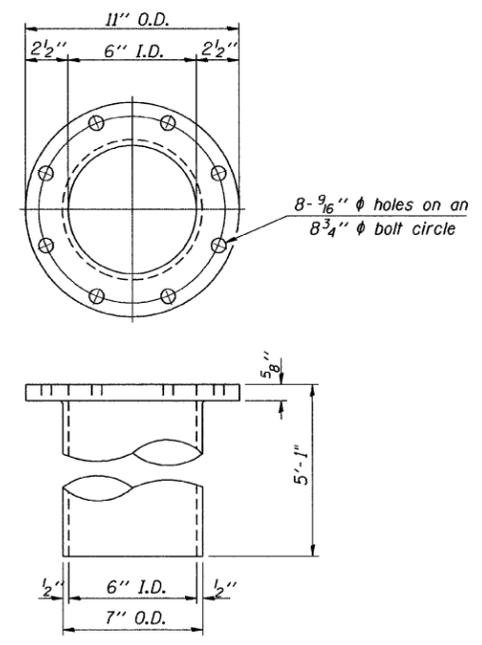
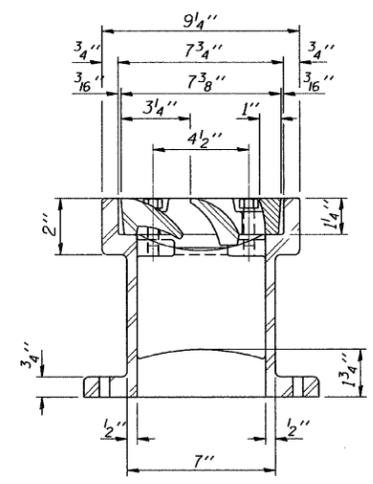
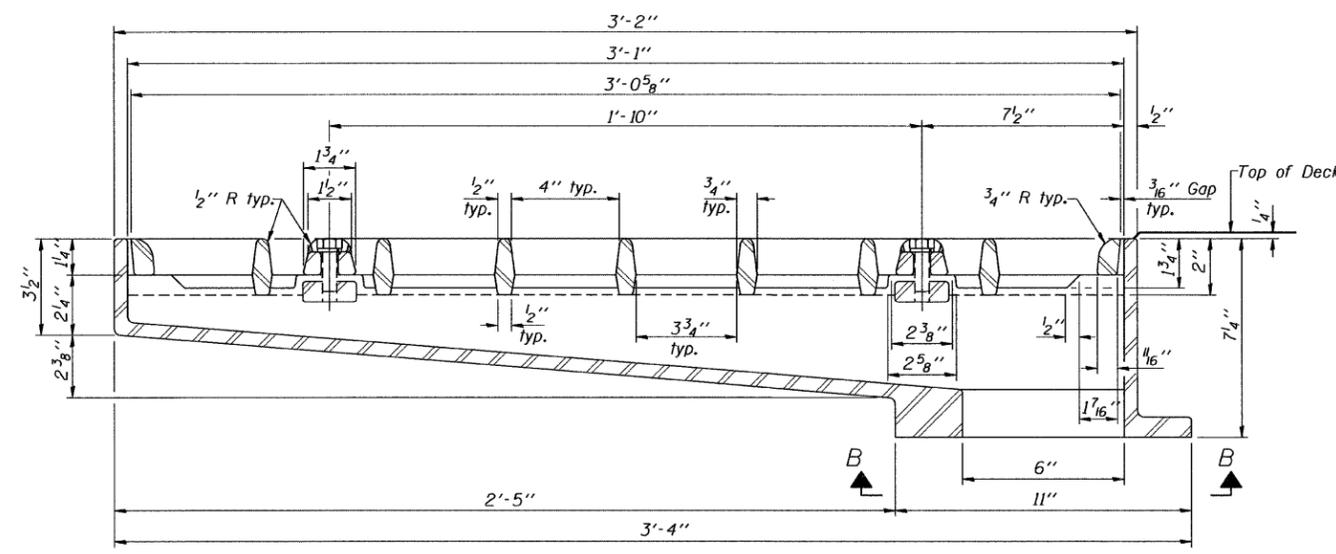
Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.

As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.

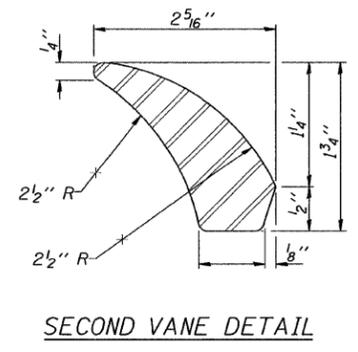
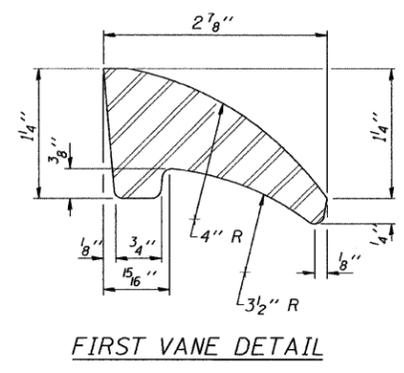
Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.

The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-33.



See sheet of for scupper location relative to parapet.



BILL OF MATERIAL

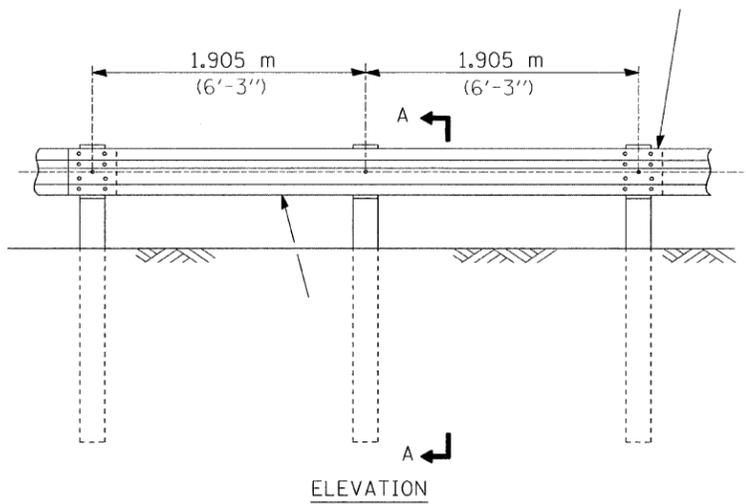
ITEM	UNIT	QUANTITY
Drainage Scupper, DS-33	Each	4

DESIGNED	S.D.H.
CHECKED	E.E.J.
DRAWN	M.S.M.
CHECKED	S.D.H.

DS-33 5-16-08

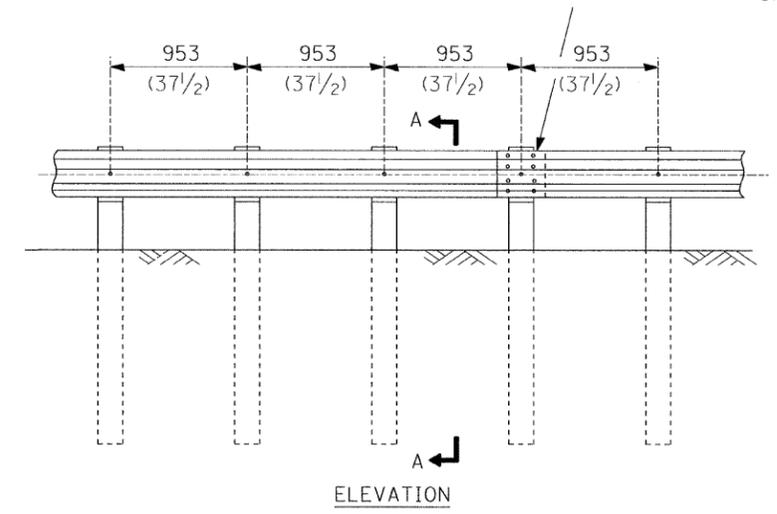
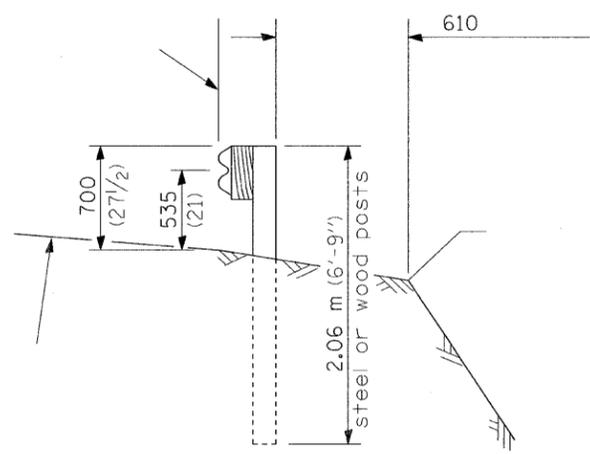
DRAINAGE SCUPPER, DS-33
 C.H. 37 (LYNDON ROAD) OVER I-88
 F.A.I. ROUTE 88
 SECTION: BRIDGE DECK REPAIRS 2007-1
 WHITESIDE COUNTY
 STATION 3529+69.36
 S.N. 098-0066
 DATE: 12-09-08
 GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC
 CHICAGO ILLINOIS

n:\jobs\2008\plot\2001-4014-09\098 00666(Lyndon Rd.)\00\098-0066-SEI.dgn 12/8/2008



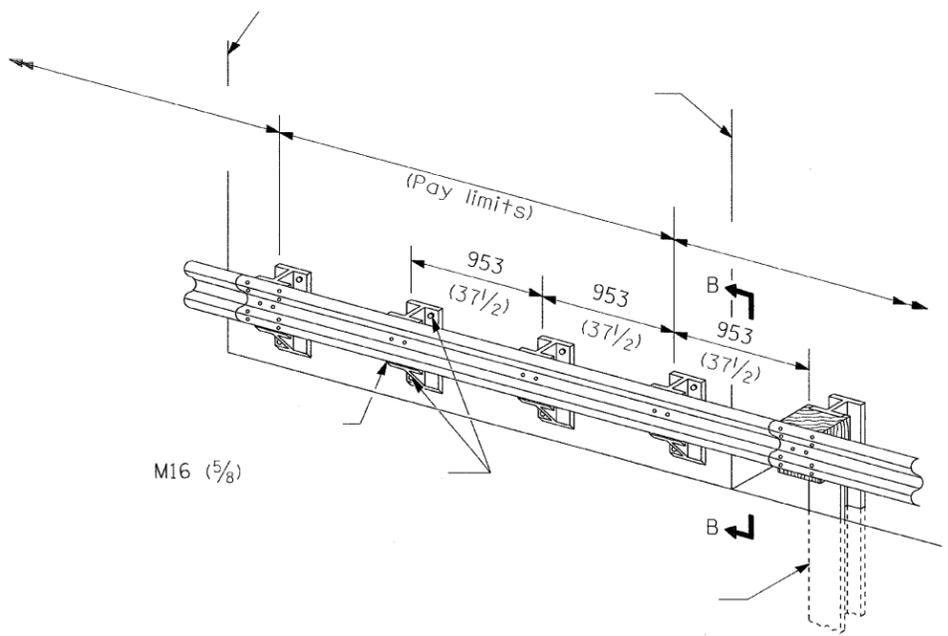
TYPE A

1.905 m (6'-3'')



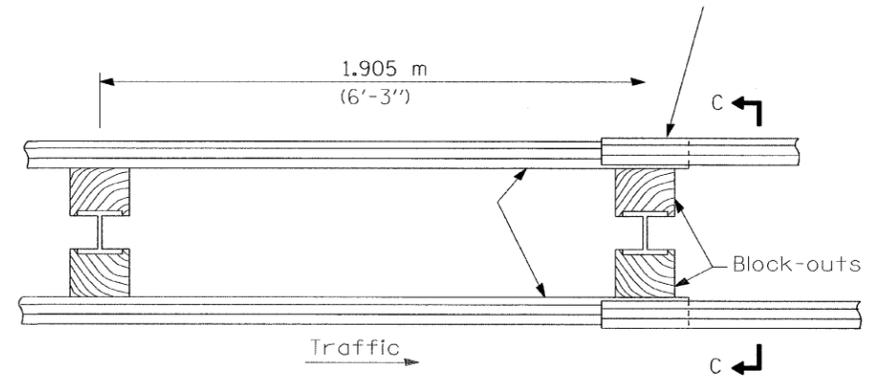
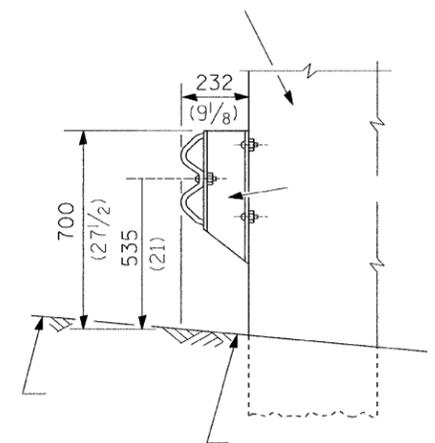
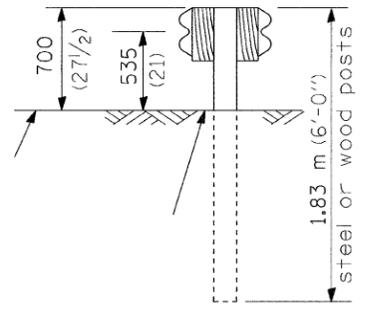
TYPE B

953 (37 1/2) Closed post spacing



TYPE C

953 (37 1/2) Block-out spacing



TYPE D

1.905 m (6'-3'')

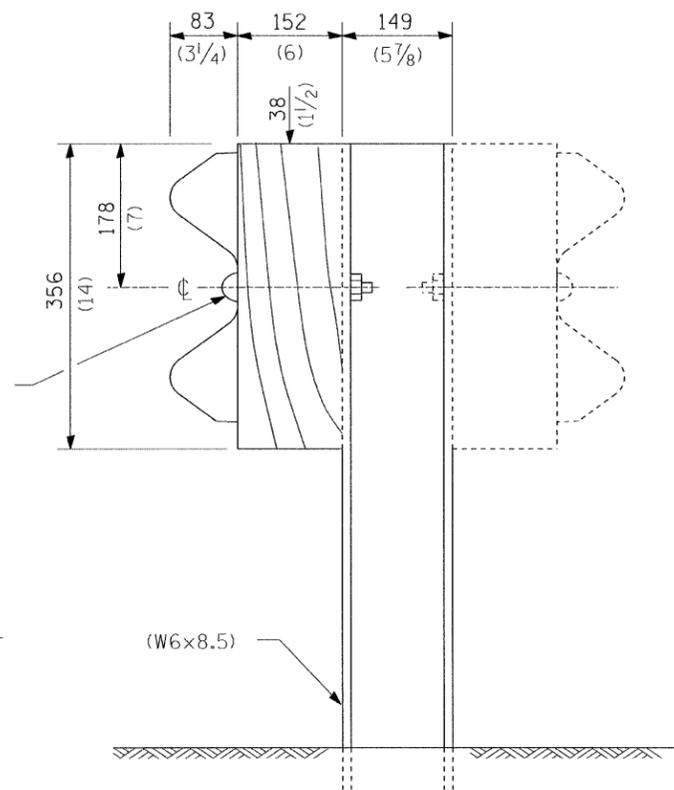
GENERAL NOTES

DATE	REVISIONS
4-1-06	Deleted plate washer F.
1-1-05	Rev. post in impervious material on sheet 4.

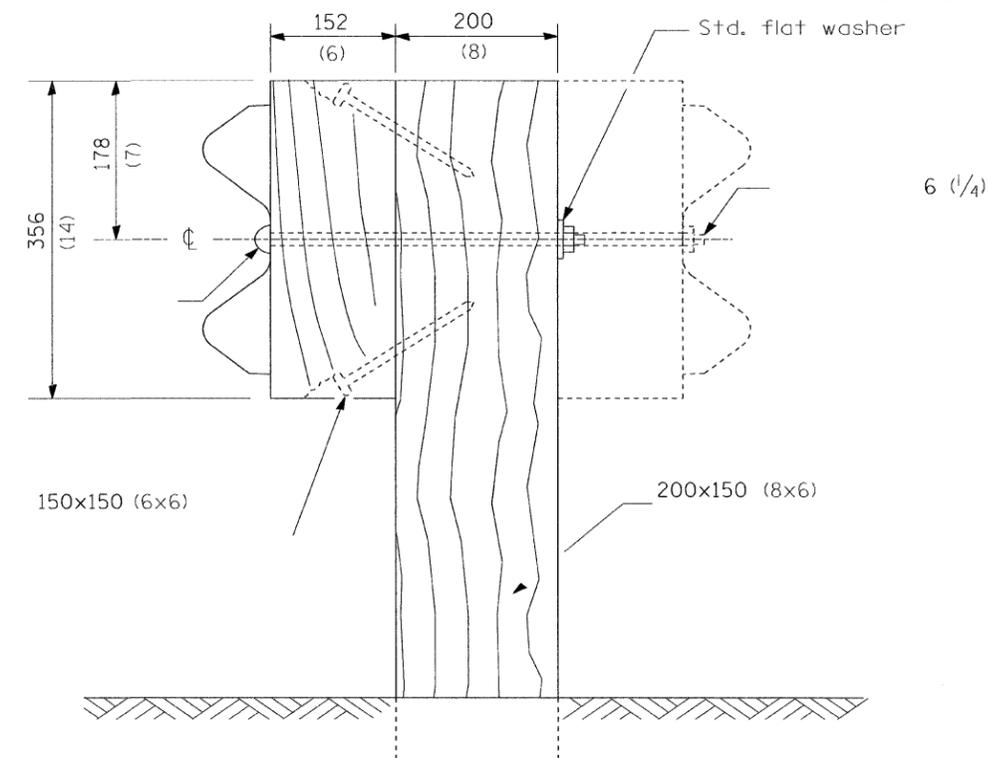
STEEL PLATE BEAM
GUARDRAIL DETAILS

(Sheet 1 of 4)

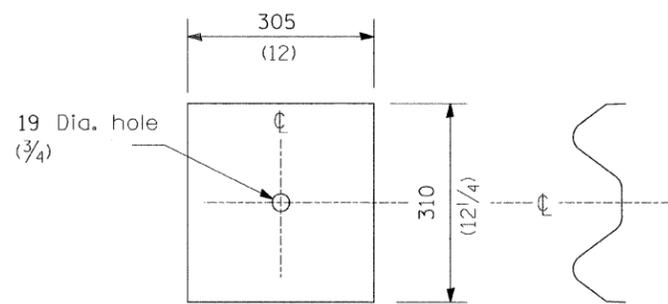
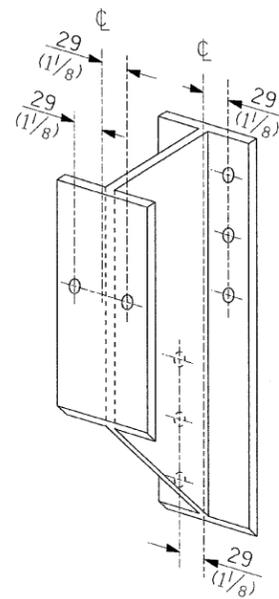
STANDARD 630001-06



STEEL POST CONSTRUCTION

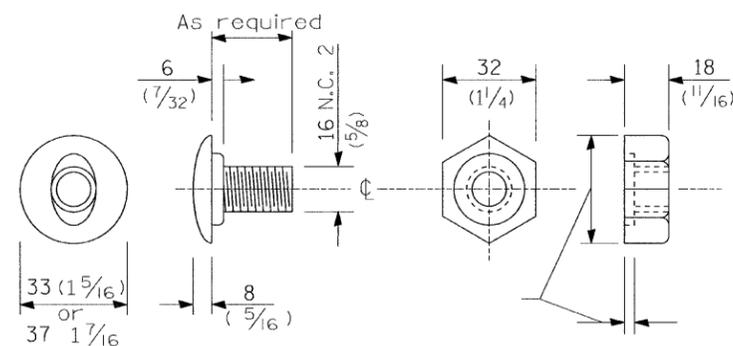


WOOD POST CONSTRUCTION



NOTE

PLATE A

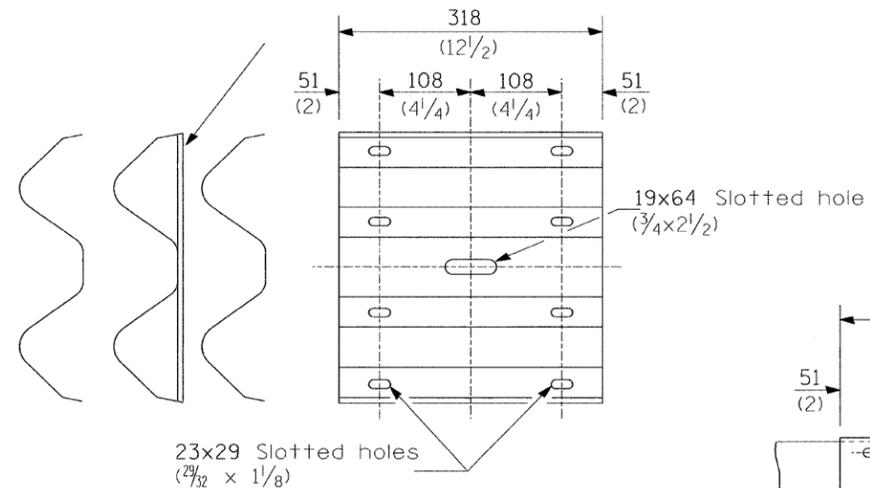


POST OR SPLICE BOLT & NUT

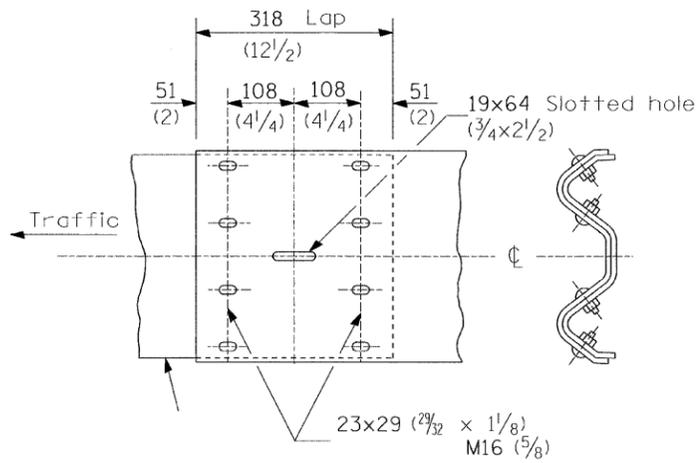
STEEL PLATE BEAM
GUARDRAIL DETAILS

(Sheet 2 of 4)

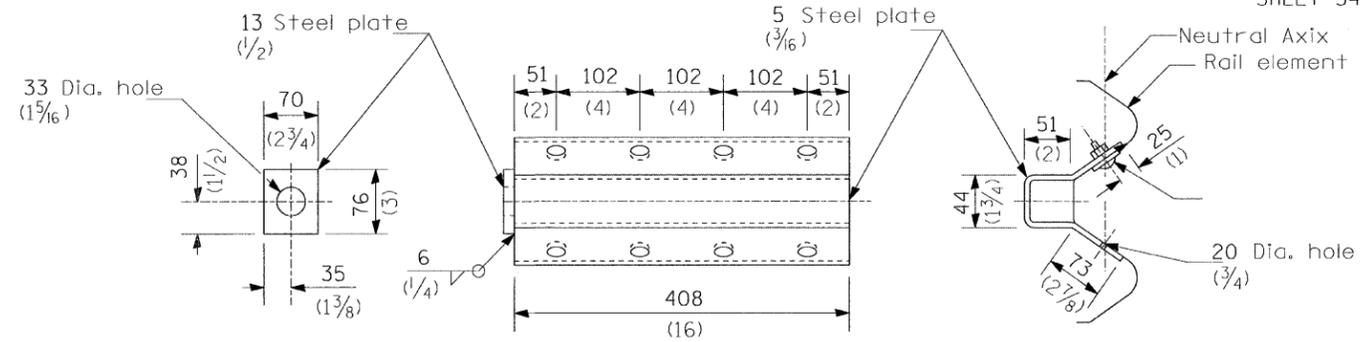
STANDARD 630001-06



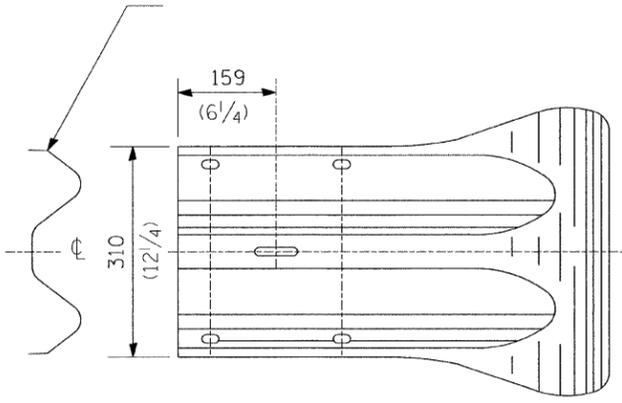
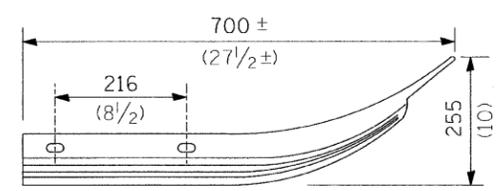
SPLICE PLATE



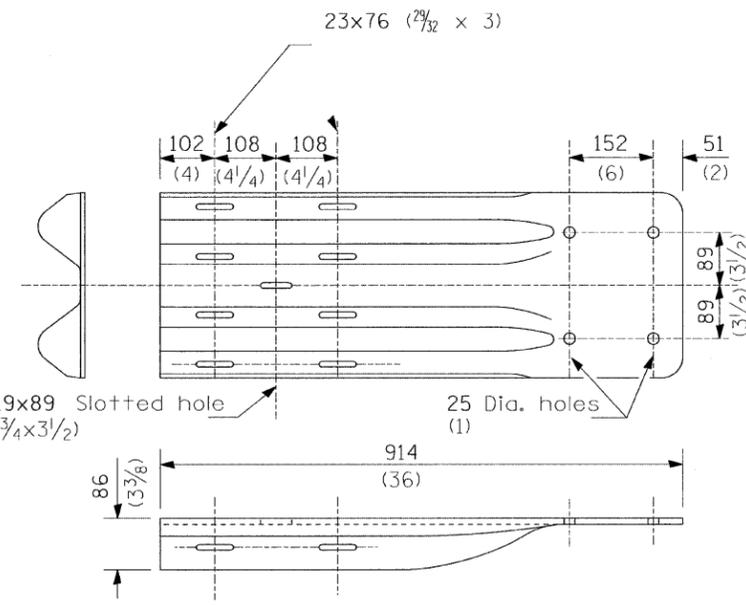
RAIL ELEMENT SPLICE



ANCHOR PLATE T DETAILS

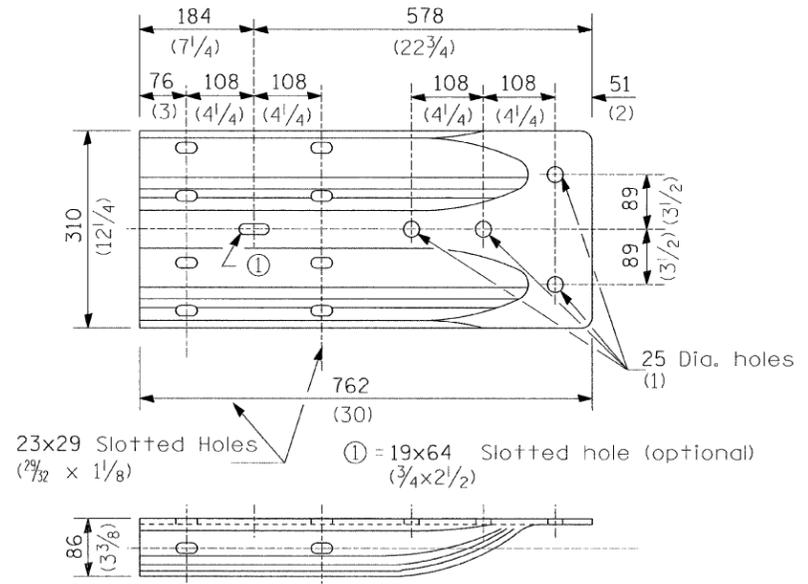


END SECTION



NOTE

END SHOE

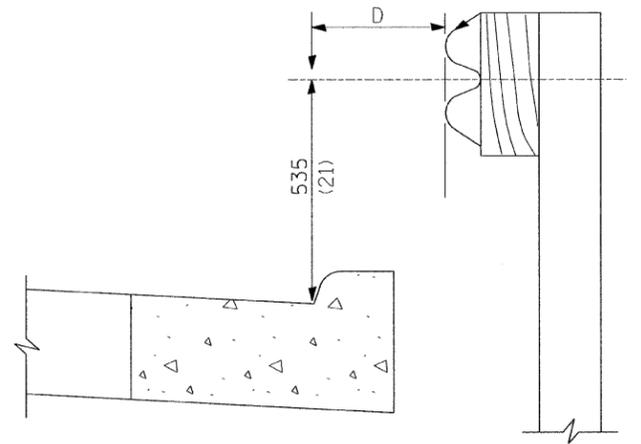
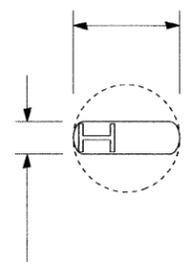


ALTERNATE END SHOE

STEEL PLATE BEAM
GUARDRAIL DETAILS

(Sheet 3 of 4)

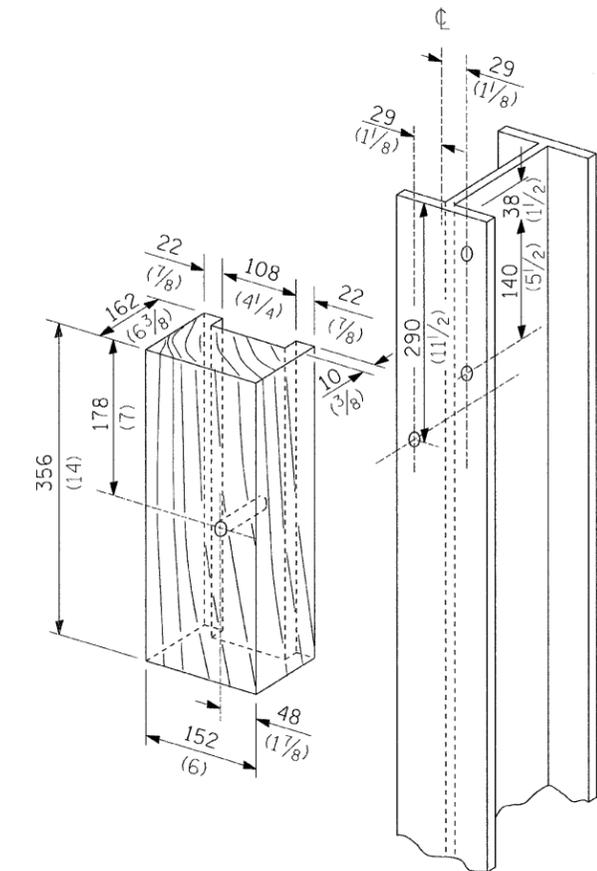
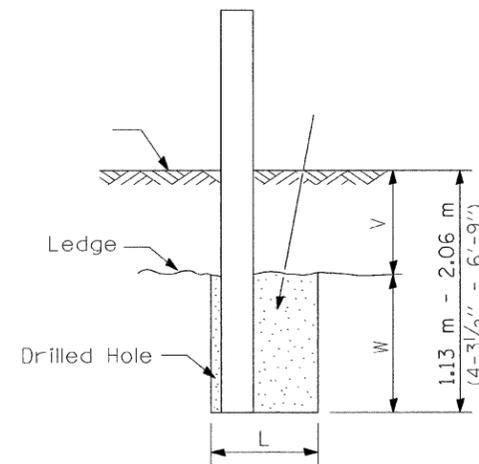
STANDARD 630001-06



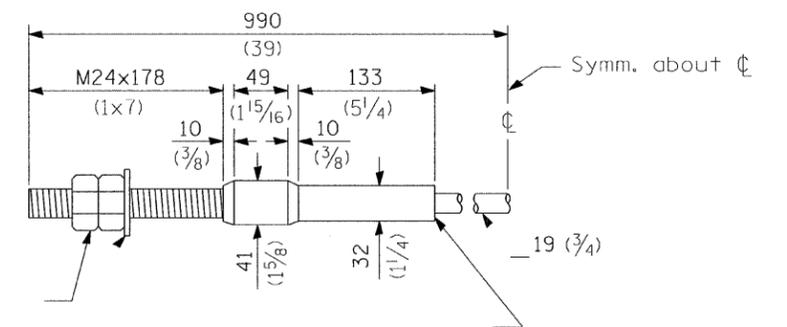
3.0 m (10'-0") M-5 (M-2) 300 (12)

GUARDRAIL PLACED BEHIND CURB

(D = 0 desirable to 300 (12) maximum)



M24 (1)



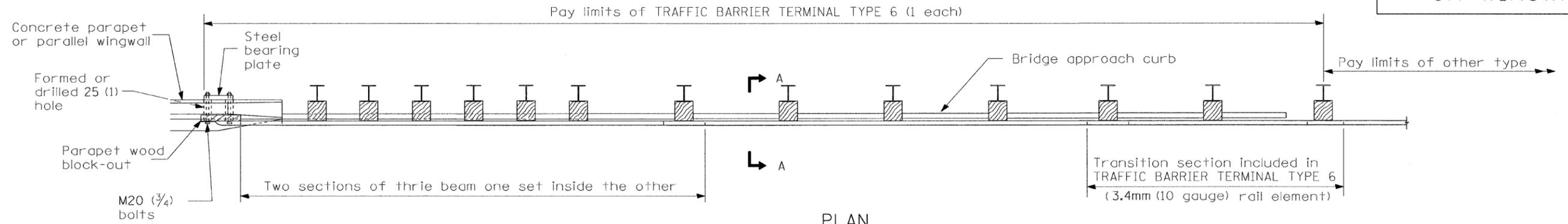
CABLE ASSEMBLY

18,100 kg
Tighten to taut tension.

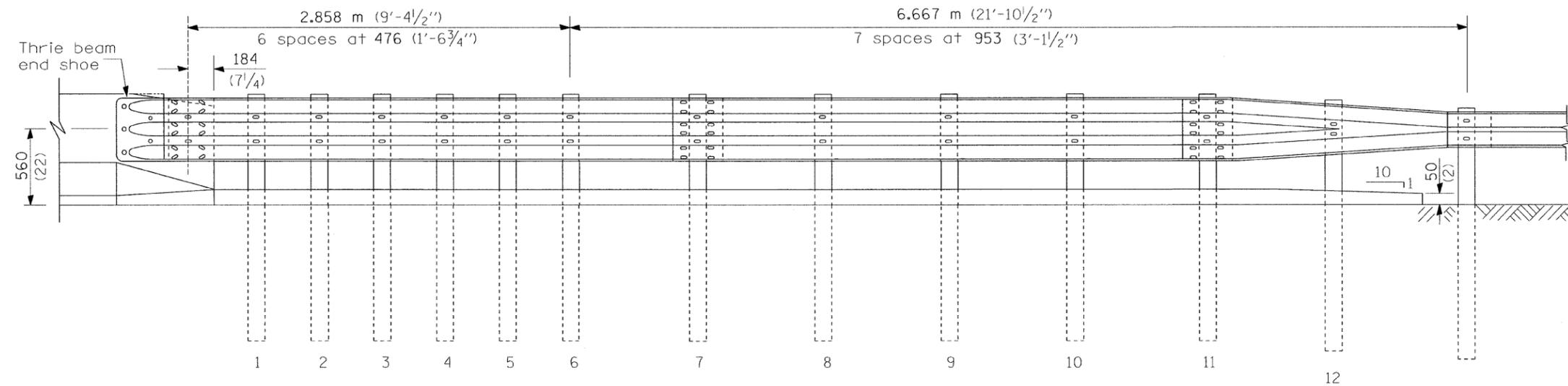
STEEL PLATE BEAM
GUARDRAIL DETAILS

(Sheet 4 of 4)

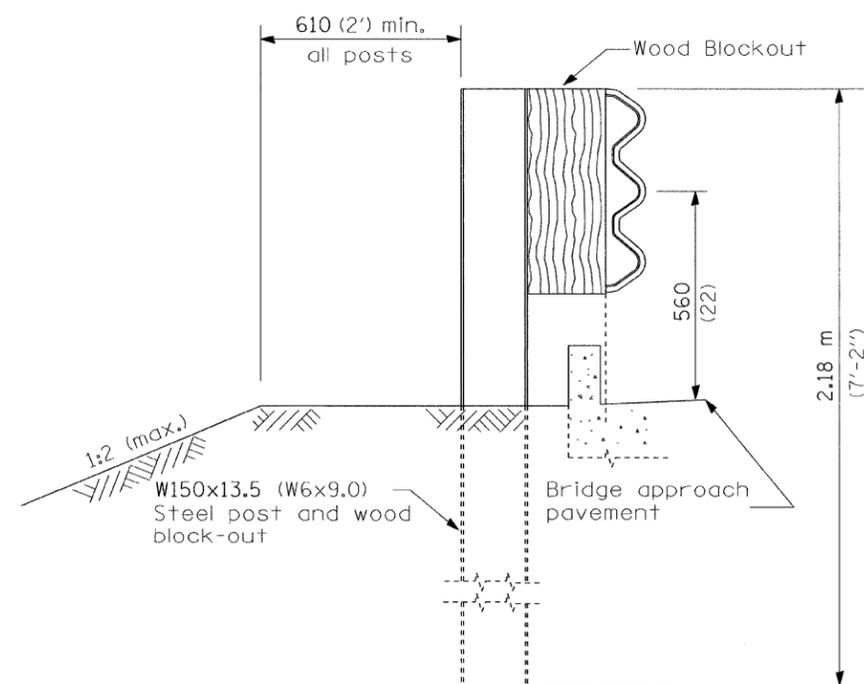
STANDARD 630001-06



PLAN



ELEVATION



SECTION A-A

GENERAL NOTES

See Standard 630001 for details of guardrail not shown.

Thrie beam rail shall be bolted to block-out at all posts.

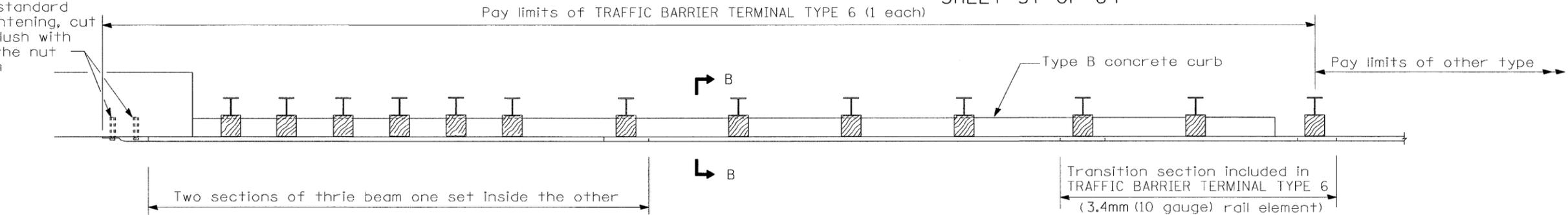
See Standard 420401 for details of bridge approach pavement.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

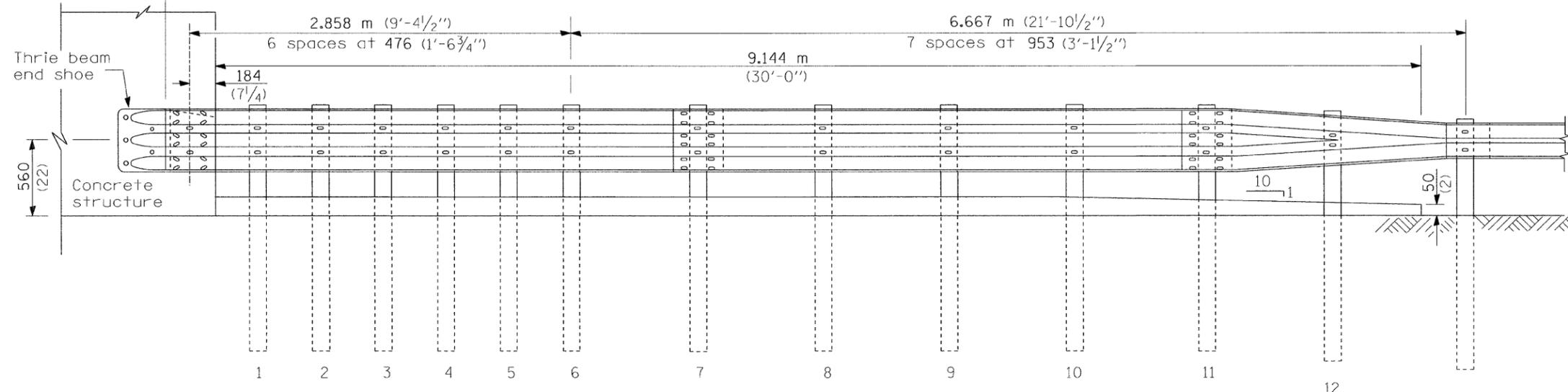
All dimensions are in millimeters (inches) unless otherwise shown.

TRAFFIC BARRIER TERMINAL,
TYPE 6 DETAILS

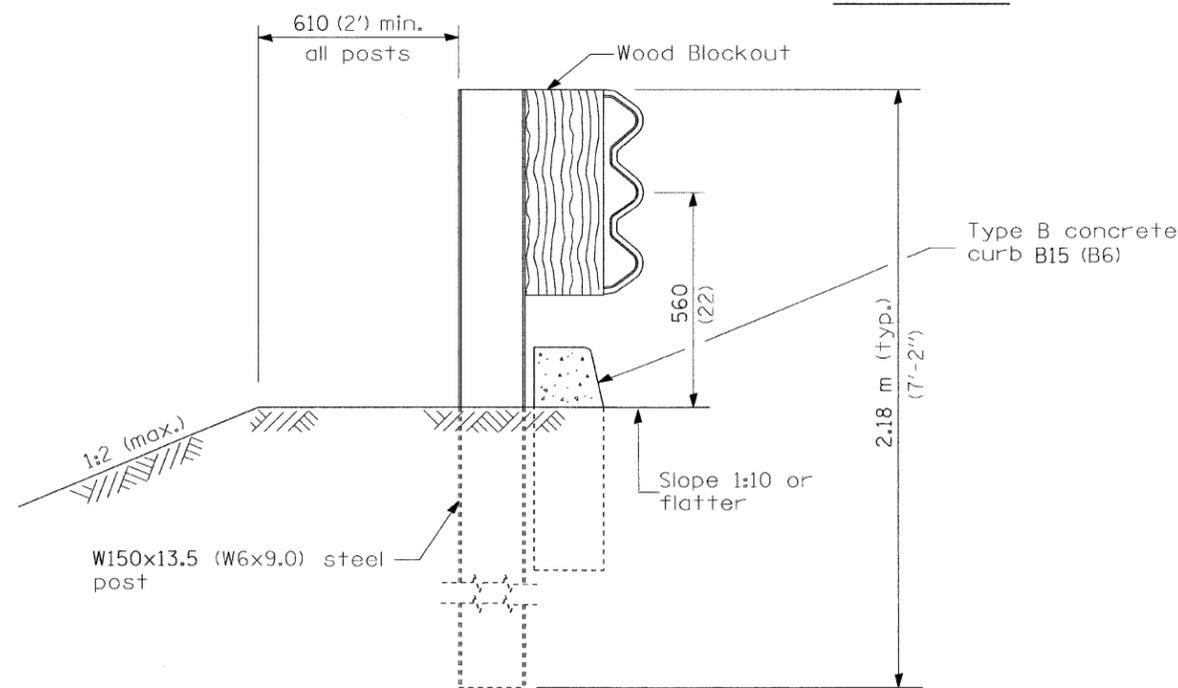
5 Epoxy grouted M20 (3/4") anchor bolts with standard washers. After tightening, cut the anchor bolts flush with nuts, and damage the nut to prevent it from loosening.



PLAN

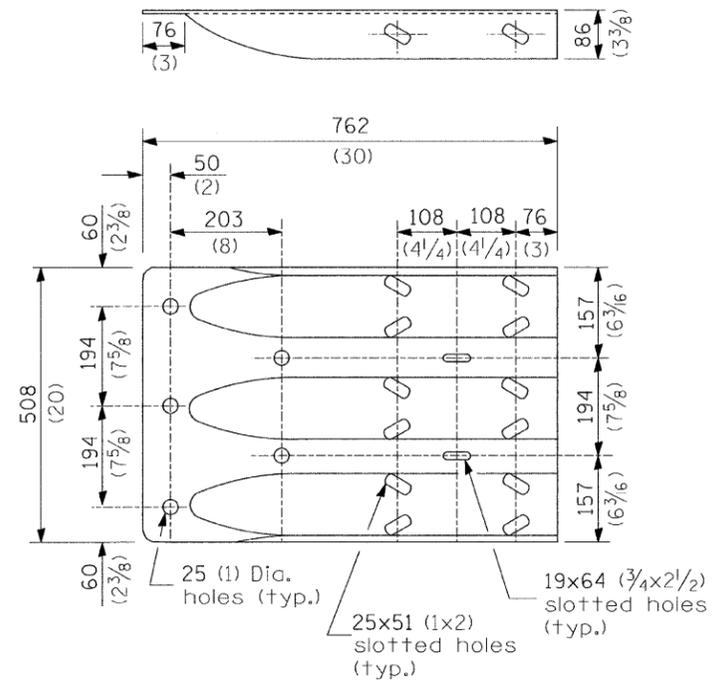


ELEVATION

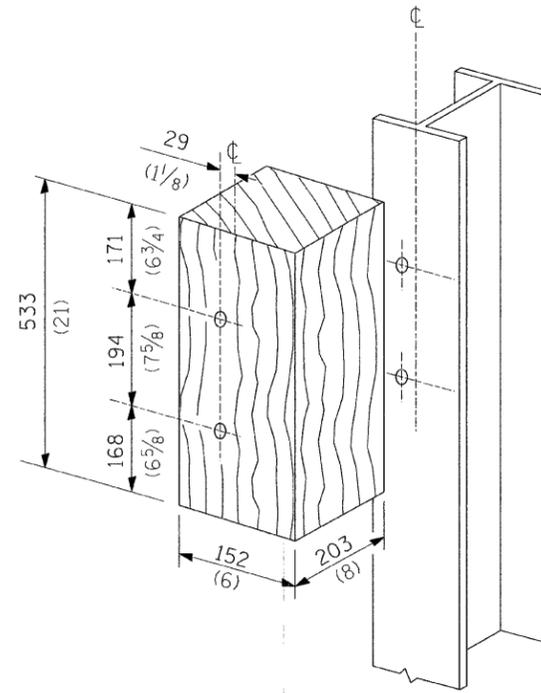


SECTION B-B

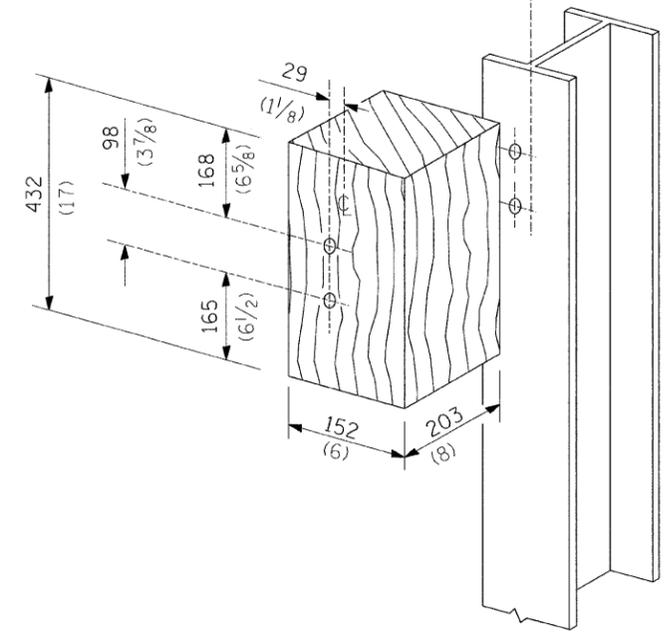
TRAFFIC BARRIER TERMINAL,
TYPE 6 DETAILS



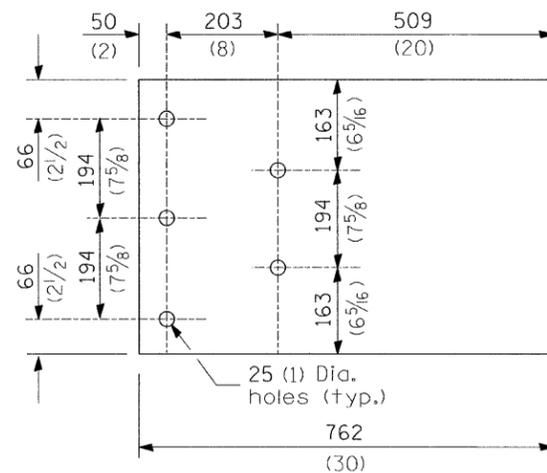
THRIE BEAM END SHOE DETAIL



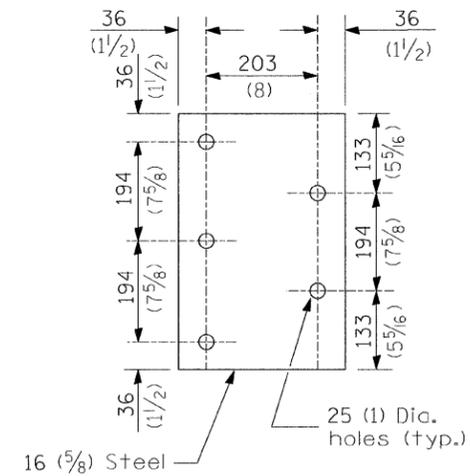
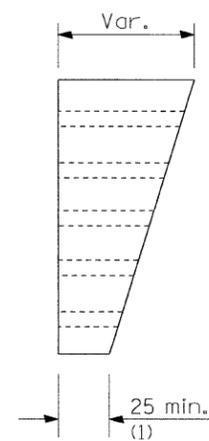
POSTS 1-11 WOOD BLOCKOUT DETAIL



POST 12 WOOD BLOCKOUT DETAIL



PARAPET WOOD BLOCK-OUT DETAIL

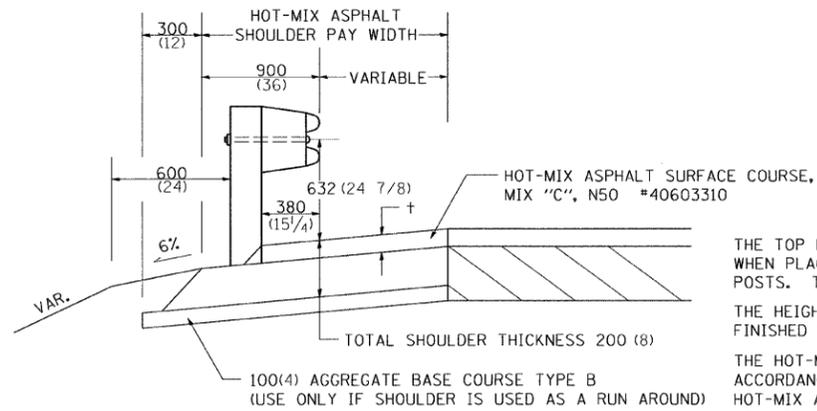


PARAPET STEEL BEARING PLATE DETAIL

(5 each individual 125x125x16 (5x5x5/8) steel plates with centered 25 (1) holes may be substituted for the plate shown.)

TRAFFIC BARRIER TERMINAL,
TYPE 6 DETAILS

DETAIL OF HOT-MIX ASPHALT SHOULDER AT GUARD RAIL



† = SEE TYPICAL SECTIONS FOR THICKNESS

GENERAL NOTES

THE TOP LIFT SHALL NOT BE PLACED BEHIND THE GUARDRAIL POSTS. WHEN PLACING THE TOP LIFT THE RAIL MUST BE REMOVED FROM THE POSTS. THE POST SHALL NOT BE REMOVED.

THE HEIGHT OF THE GUARD RAIL SHALL BE SET **632** (24 7/8) FROM THE FINISHED SURFACE.

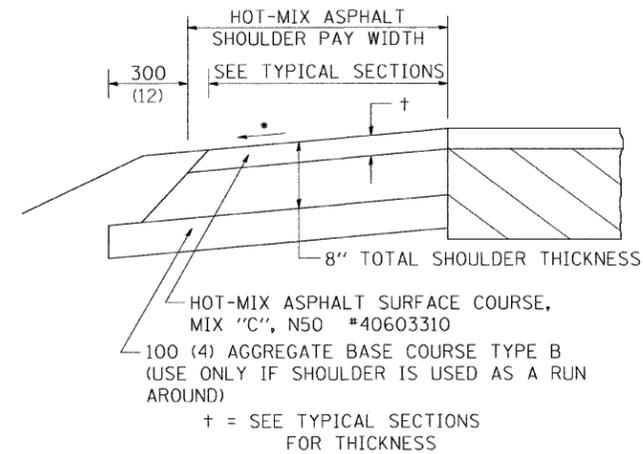
THE HOT-MIX ASPHALT SHOULDER SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 482 EXCEPT THE TOP LIFT SHALL BE HOT-MIX ASPHALT SURFACE COURSE, MIXTURE C, N50. THE WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR HOT-MIX ASPHALT SURFACE COURSE, MIXTURE "C", N50 AND SQUARE METER (SQUARE YARD) FOR HOT-MIX ASPHALT SHOULDERS OF THE THICKNESS SPECIFIED. THE REMOVAL & REINSTALLATION OF THE GUARDRAIL WILL BE INCLUDED IN THE COST OF THE HOT-MIX ASPHALT SURFACE COURSE, MIXTURE C, N50.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 11-01-07

DETAIL OF HOT-MIX ASPHALT SHOULDER AT GUARD RAIL 23.4

HOT-MIX ASPHALT SHOULDER



GENERAL NOTES

THE HOT-MIX ASPHALT SHOULDER SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 482 EXCEPT THE TOP LIFT SHALL BE HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 #40603310. THE WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 #40603310 AND SQUARE YARD FOR HOT-MIX ASPHALT SHOULDERS OF THE THICKNESS SPECIFIED.

USE HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 #40603310. WHEN RESURFACING EXISTING HOT-MIX ASPHALT SHOULDERS. THE THICKNESS IS SHOWN ON THE TYPICAL SECTIONS. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 #40603310.

REMOVAL OF MATERIAL FOR PLACEMENT OF THE HOT-MIX ASPHALT SHOULDER TO BE PAID FOR IN UNITS FOR EXCAVATING AND GRADING EXISTING SHOULDERS OR IN CUBIC YARDS FOR EARTH EXCAVATION OR EARTH EXCAVATION WIDENING.

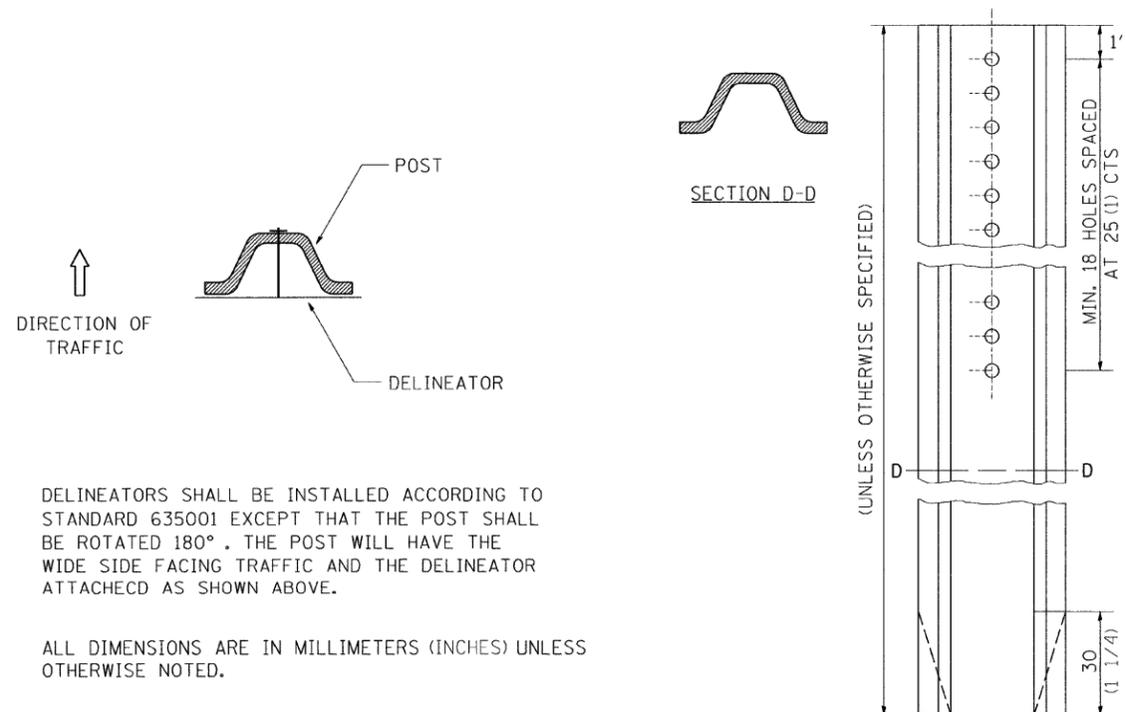
* 4% WHEN MAINLINE IS ON TANGENT. FOR CROSS SLOPE ON SUPERELEVATION SECTION, SEE HIGHWAY STANDARD 482001 OR 482006.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 11-01-07

HOT-MIX ASPHALT SHOULDER 23.4a

DELINEATOR AND POST ORIENTATION



DELINEATORS SHALL BE INSTALLED ACCORDING TO STANDARD 635001 EXCEPT THAT THE POST SHALL BE ROTATED 180°. THE POST WILL HAVE THE WIDE SIDE FACING TRAFFIC AND THE DELINEATOR ATTACHED AS SHOWN ABOVE.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 11-01-07

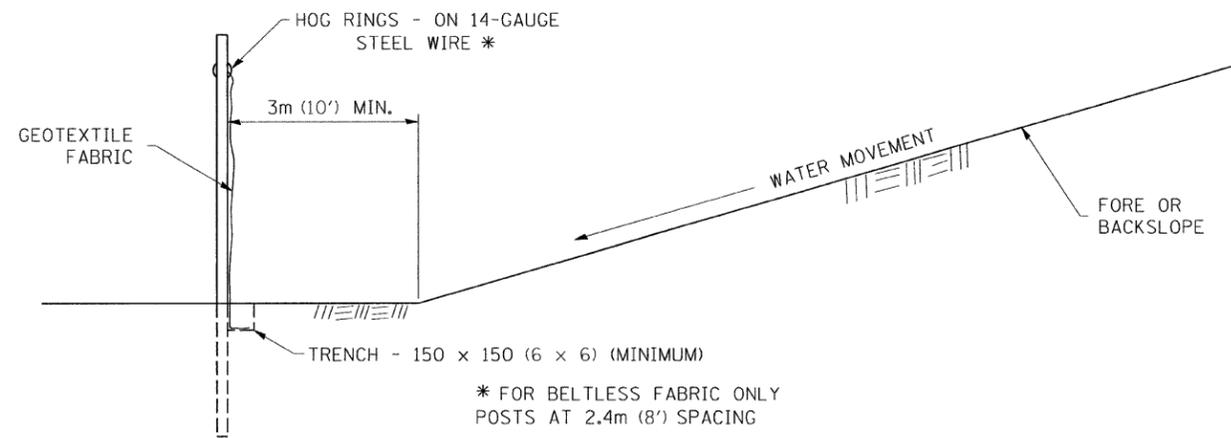
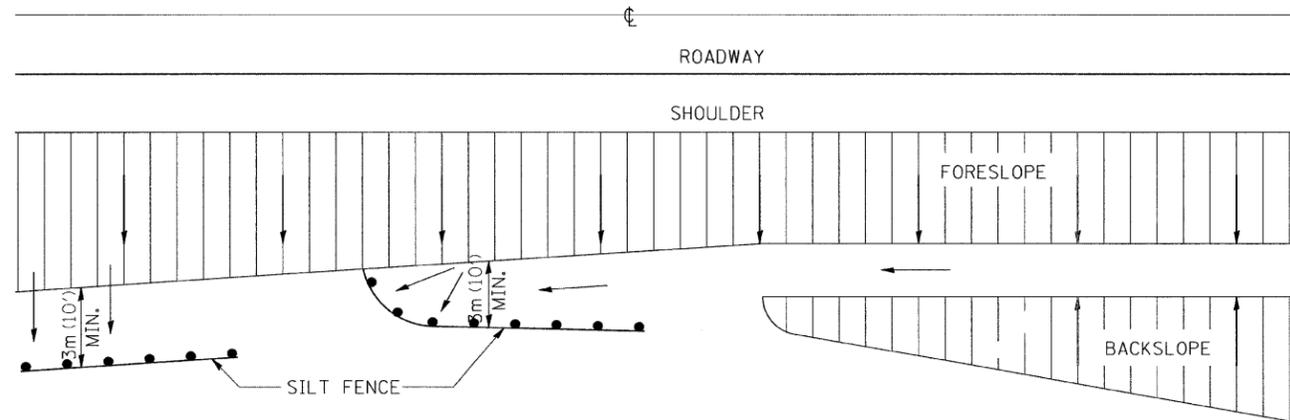
DELINEATOR AND POST ORIENTATION 37.4

• BRIDGE DECK REPAIRS 2007-1
•• ROCK ISLAND & WHITESIDE

REVISED - ---	REGION 2 / DISTRICT 2 STANDARD	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED - ---		88	.	••	64	59
REVISED - ---		CONTRACT NO. 64024				
REVISED - ---		SCALE: 47.5298' / 1" SHEET NO. ___ OF ___ SHEETS	STA. _____ TO STA. _____	FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT		

PLOT DATE = 12/8/2008

EROSION CONTROL DETAILS FOR SILT FENCE



DETAILS OF SILT FENCE

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

REVISED - 10-22-01

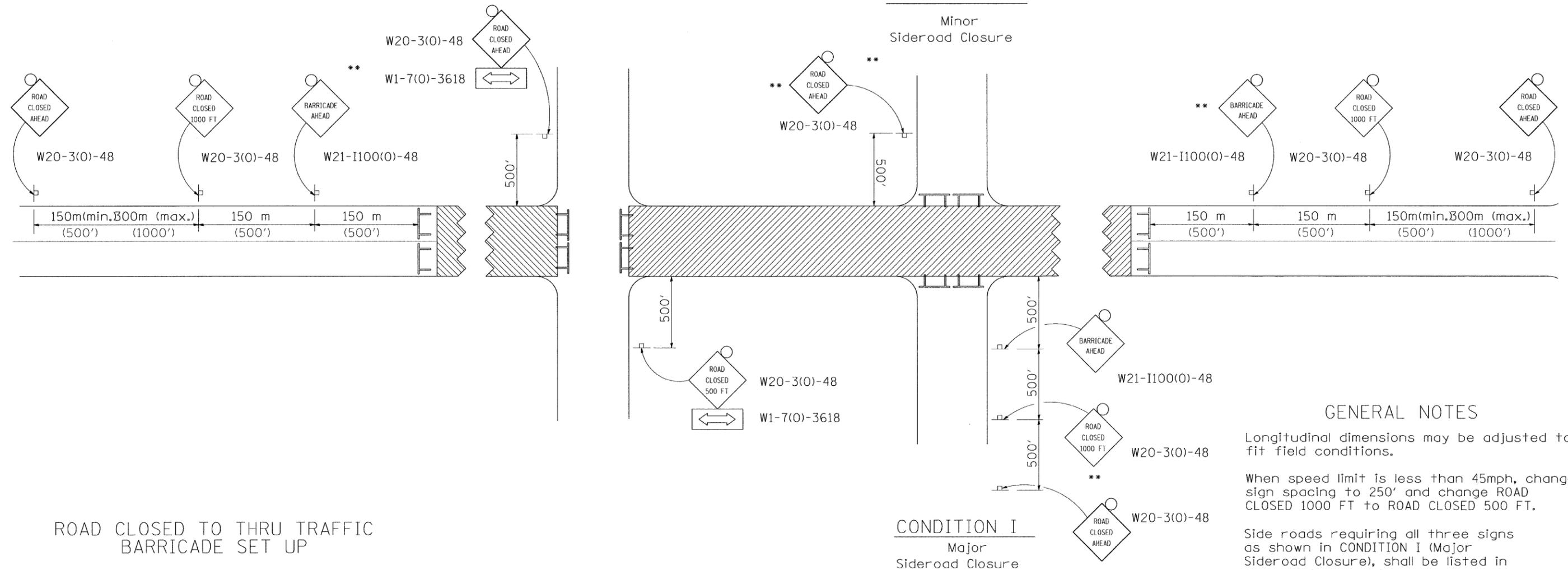
• BRIDGE DECK REPAIRS 2007-1
•• ROCK ISLAND & WHITESIDE

REVISED -	REGION 2 / DISTRICT 2 STANDARD	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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REVISED -		CONTRACT NO. 64D24				
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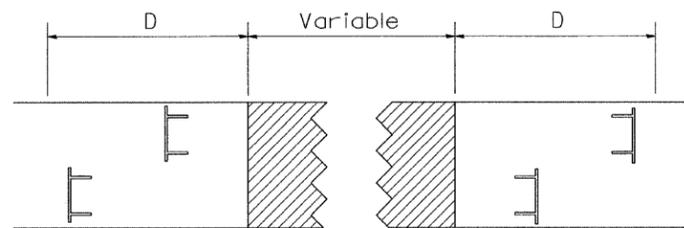
TRAFFIC CONTROL FOR ROAD CLOSURE

CONDITION II

Minor Sideroad Closure



ROAD CLOSED TO THRU TRAFFIC BARRICADE SET UP



Type III Barricades and R11-4-4830 signs shall be as shown in "Road Closed To All Thru Traffic" detail on Highway Standard 701901. If the distance "D" exceeds 600 m (2000') an additional set of barricades and R11-4-4830 shall be placed at each end of the work area.

SYMBOLS

-  Work area
-  Type III Barricade with Flashers
-  Sign with flashing light

GENERAL NOTES

Longitudinal dimensions may be adjusted to fit field conditions.

When speed limit is less than 45mph, change sign spacing to 250' and change ROAD CLOSED 1000 FT to ROAD CLOSED 500 FT.

Side roads requiring all three signs as shown in CONDITION I (Major Sideroad Closure), shall be listed in the special provision.

** Where local access is to be maintained, barricades are to be set up as shown in Road Closed to thru traffic. Type III Barricades and R11-2-4830 signs shall be as shown in "Road Closed To All Traffic" detail on Highway Standard 701901.

All dimensions are in millimeters (inches) unless otherwise shown.

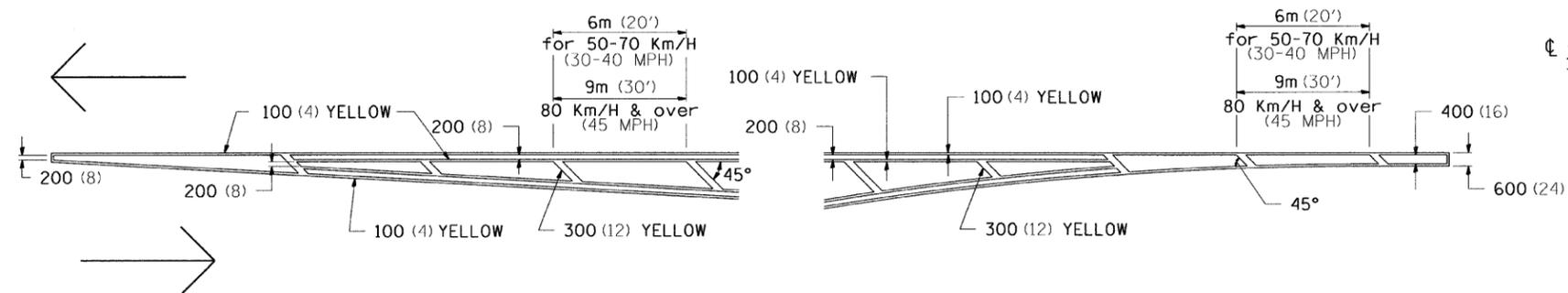
TYPICAL APPLICATION FOR ROAD CLOSURE

• BRIDGE DECK REPAIRS 2007-1
** ROCK ISLAND & WHITESIDE

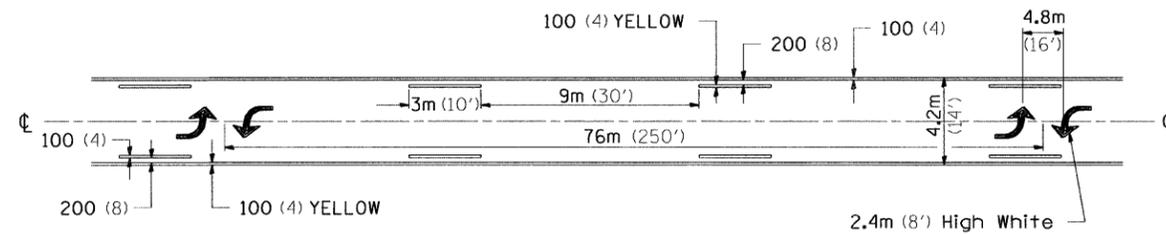
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TYPICAL PAVEMENT MARKINGS

TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN AT LEFT TURN LANE

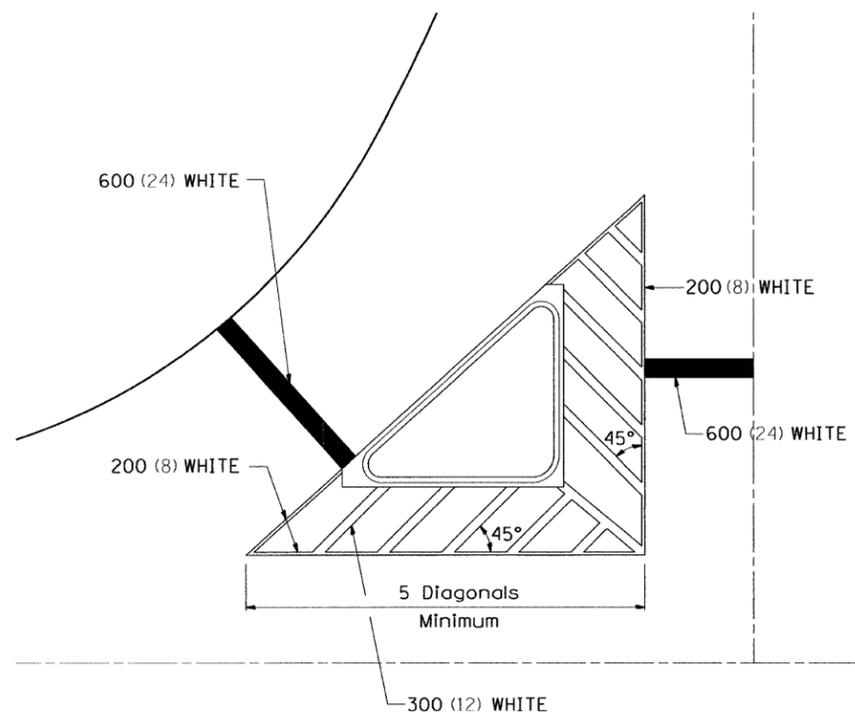


MEDIAN PAVEMENT MARKING

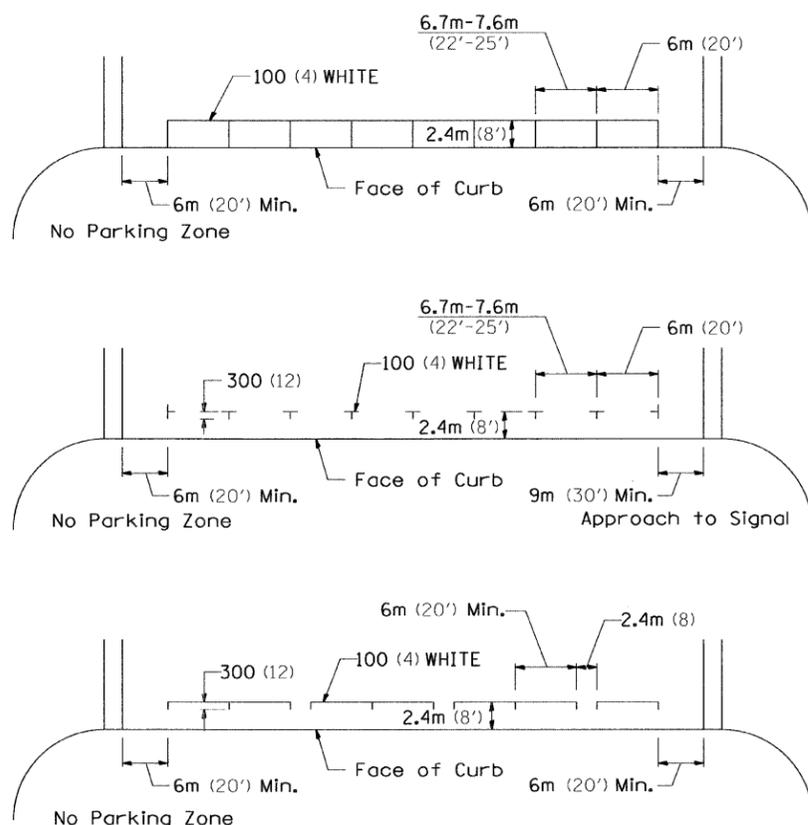


•• ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

TYPICAL ISLAND OFFSET SHOULDER WIDTH

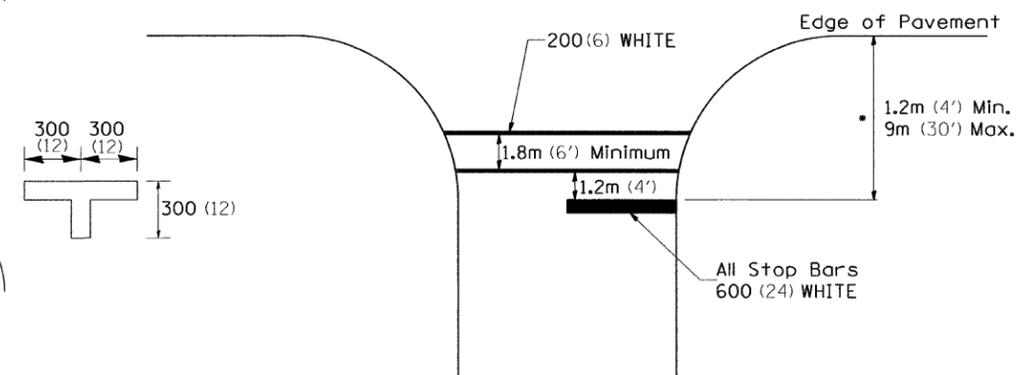


TYPICAL PARKING SPACING



STANDARD CROSSWALK MARKING

See Schedules for Locations



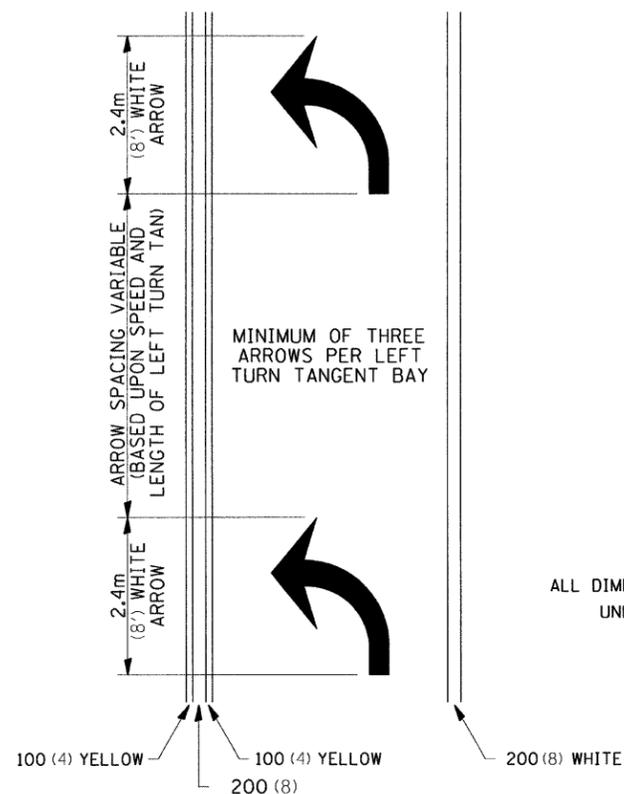
• Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

• BRIDGE DECK REPAIRS 2007-1
•• ROCK ISLAND & WHITESIDE

FILE NAME =	USER NAME = .USER.	DESIGNED -	REVISED - 10-21-08	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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						TYPICAL PAVEMENT MARKINGS SHEET 1 OF 3 41.1					

TYPICAL PAVEMENT MARKINGS

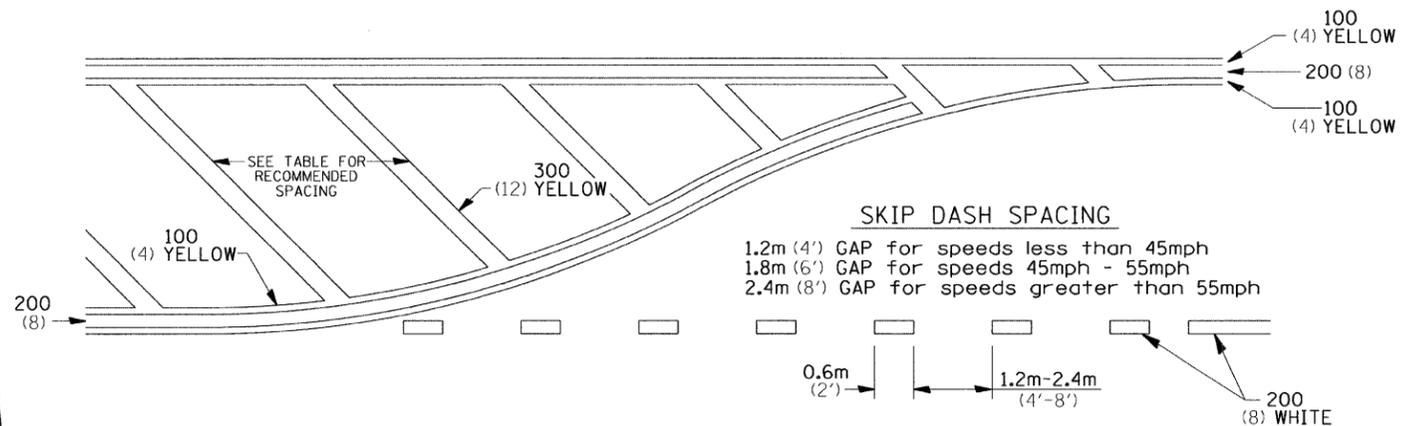
ARROW LAYOUT



- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER
- ◆ TWO-WAY AMBER MARKER

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

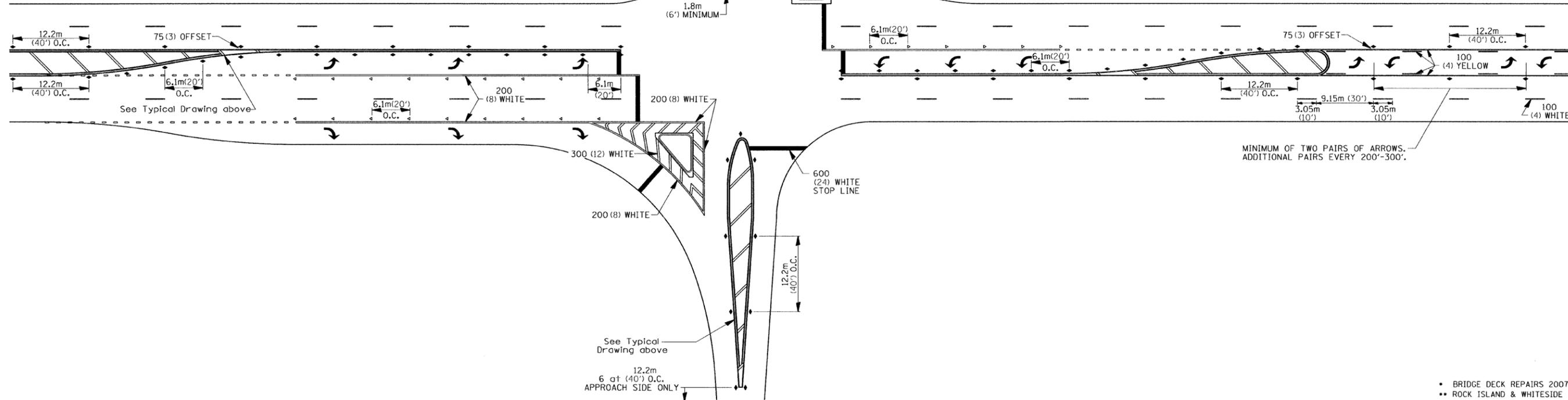
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

Speed Limit Range	Continuous Median Area	Intersection Channelization	Objects (Islands)
less than 50km/H (30MPH)	15.3m (50')	4.53m (15')	3.05m (10')
50-60km/H (30-40MPH)	22.9m (75')	6.1m (20')	4.53m (15')
70km/H (45MPH) & over	22.9m (75')	9.05m (30')	6.1m (20')

NOTE: If the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.



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*FILE#		DRAWN -	REVISED -
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	PLOT DATE = 12/8/2008	DATE -	REVISED -

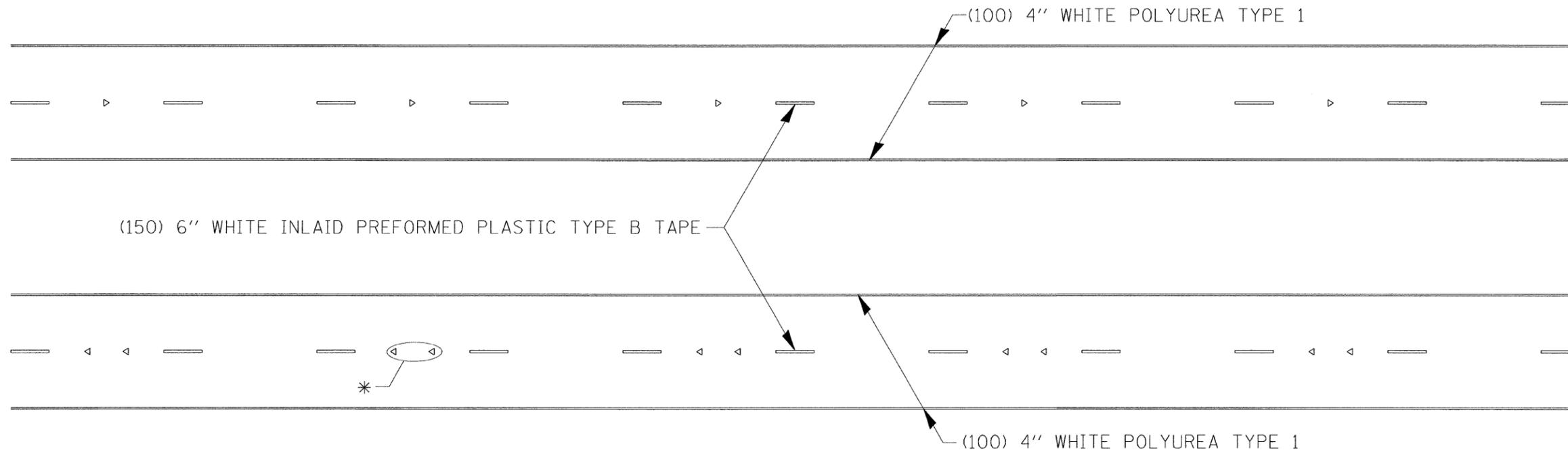
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: _____ SHEET NO. _____ OF _____ SHEETS STA. _____ TO STA. _____

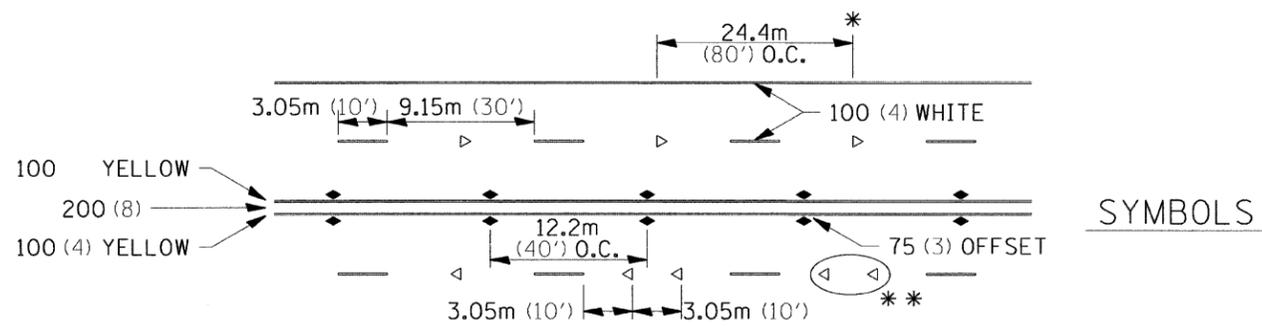
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
88	.	..	64	63
CONTRACT NO. 64D24				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

TYPICAL PAVEMENT MARKINGS



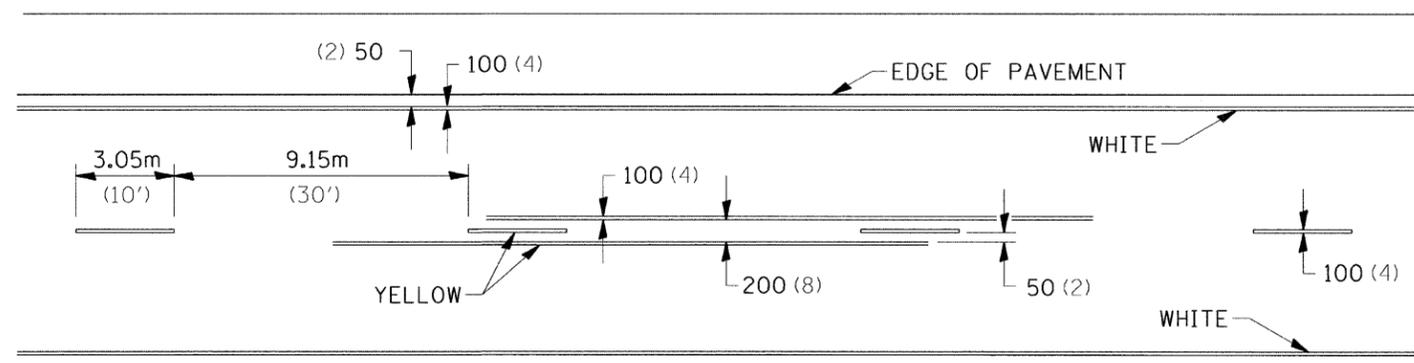
* SEE HIGHWAY STANDARD 781001 FOR SPACING DETAILS.
USE DOUBLE MARKERS WHEN ADT \geq 25,000.

MULTI-LANE / DIVIDED



* REDUCE TO 12.2m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 15Km/H (10MPH) LOWER THAN POSTED SPEEDS.
** USE DOUBLE MARKERS WHEN ADT \geq 25,000

MULTI-LANE / UNDIVIDED



TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES

SYMBOLS

FILE NAME =	USER NAME = .USER.	DESIGNED -	REVISED - 10-21-08	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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						• BRIDGE DECK REPAIRS 2007-1 ** ROCK ISLAND & WHITESIDE TYPICAL PAVEMENT MARKINGS SHEET 3 OF 3 41.1					