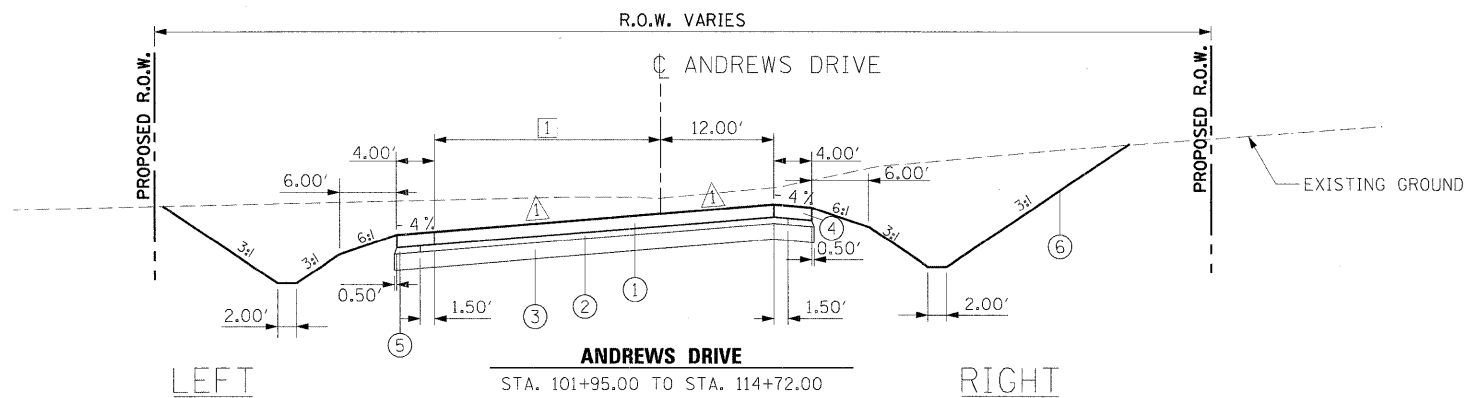
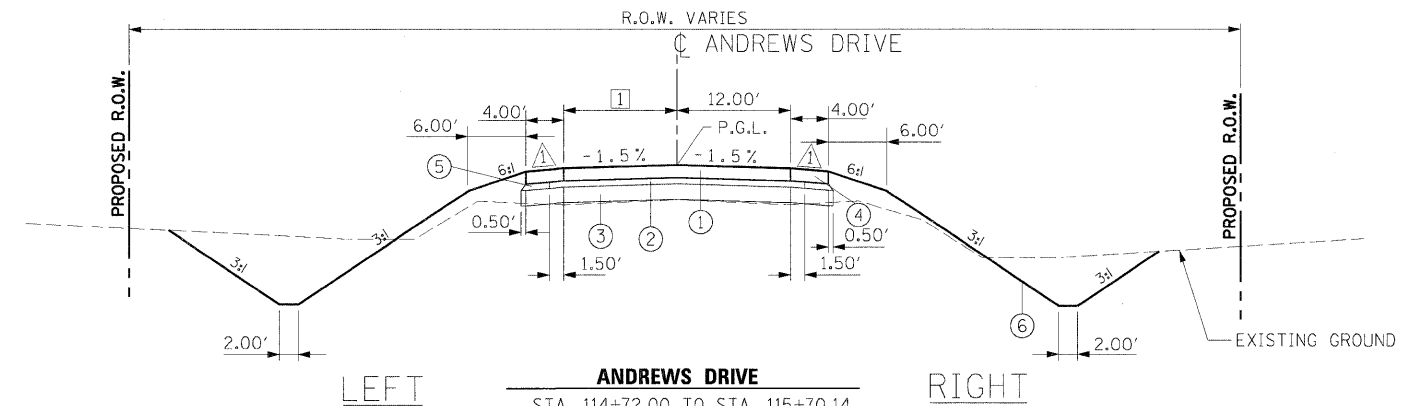


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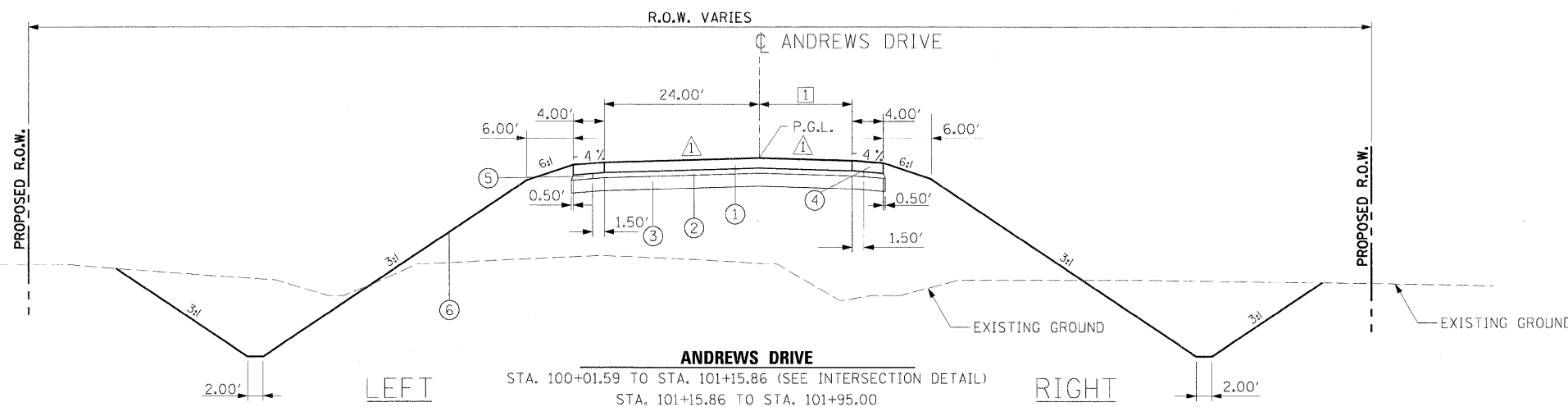
- ▲ STATION 101+95.00 TO STATION 113+74.00 = -4.00%
 STATION 113+74.00 TO STATION 114+19.00
 TRANSITION FROM -4.00% TO -1.50%
 STATION 114+19.00 TO STATION 114+72.00 = -1.50%
- ① STATION 101+95.00 TO STATION 101+99.12 = 24.00'
 STATION 101+99.12 TO STATION 103+55.12 =
 TRANSITION FROM 24.00' TO 12.00'
 STATION 103+55.12 TO STATION 114+72.00 = 12.00'

- ▲ STATION 101+95.00 TO STATION 113+74.00 = 4.00%
 STATION 113+74.00 TO STATION 114+72.00
 TRANSITION FROM 4.00% TO -1.50%



- ▲ STATION 114+72.00 TO STATION 115+14.10 = -4.00%
 STATION 115+14.10 TO STATION 115+57.89
 TRANSITION FROM -4.00% TO -2.00%
 STATION 115+57.89 TO STATION 115+70.14 = -2.00%
 STATION 119+75.86 TO STATION 119+78.42 = -2.00%
 STATION 119+78.42 TO STATION 120+20.86
 TRANSITION FROM -2.00% TO -4.00%
 STATION 120+20.86 TO STATION 129+21.53 = -4.00%
- ① STATION 114+72.00 TO STATION 115+70.14 = 12.00'
 STATION 119+75.86 TO STATION 122+37.17 = 12.00'
 STATION 122+37.17 TO STATION 125+97.23 (SEE INTERSECTION DETAIL)
 STATION 125+97.23 TO STATION 128+21.53 = 12.00'
 STATION 128+65.70 TO STATION 129+21.53 =
 TRANSITION FROM 12.00' TO 14.94'

- ▲ STATION 114+72.00 TO STATION 115+24.10 = -4.00%
 STATION 115+24.10 TO STATION 115+67.06
 TRANSITION FROM -4.00% TO -2.00%
 STATION 115+67.06 TO STATION 115+70.14 = -2.00%
 STATION 119+75.86 TO STATION 119+87.60 = -2.00%
 STATION 119+87.60 TO STATION 120+30.83
 TRANSITION FROM -2.00% TO -4.00%
 STATION 120+30.83 TO STATION 129+21.53 = -4.00%



- ▲ STATION 101+15.86 TO STATION 101+50.00 = -1.50%
 STATION 101+50.00 TO STATION 101+95.00
 TRANSITION FROM -1.50% TO -4.00%

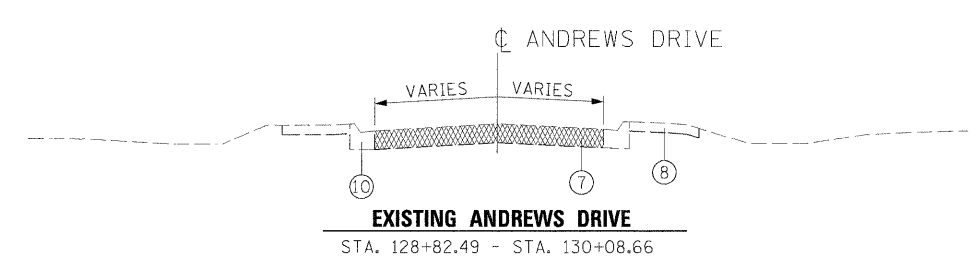
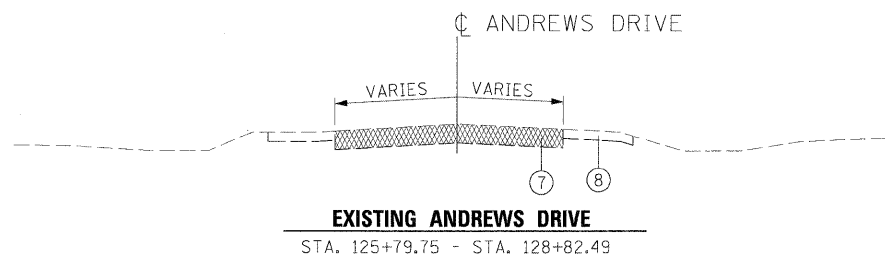
- ▲ STATION 101+15.86 TO STATION 101+95.00
 TRANSITION FROM -0.39% TO 4.00%

- ① STATION 101+15.86 TO STATION 101+76.42
 TRANSITION AROUND RADIUS FROM 18.38' TO 12.00'
 STATION 101+76.42 TO STATION 101+95.00 = 12.00'

PCC PAVEMENT (8")

STRUCTURAL DESIGN TRAFFIC	YEAR 2017	
PV = 2,120	SU = 159	MU = 371
ROAD/STREET CLASSIFICATION:	CLASS II	
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P = 80%	S = 6%	M = 14%
TRAFFIC FACTOR:	ACTUAL TF = 2.32	AC TYPE = N/A
	MINIMUM TF = N/A	
PG GRADE:	BINDER = N/A	SURFACE = N/A
SUBGRADE SUPPORT RATING:	SSR = POOR	

*SEE U.S. ROUTE 40 TYPICAL SECTIONS FOR HMA SHOULDER MIX DESIGN



- ITEM
- ① PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED)
 - ② SUB-BASE GRANULAR MATERIAL, TYPE A 4"
 - ③ PROCESSING MODIFIED SOIL 10"
 - ④ HOT-MIX ASPHALT SHOULDERS, 8"
 - ⑤ SUB-BASE GRANULAR MATERIAL, TYPE C 4"
 - ⑥ 4" TOPSOIL, SEEDING, CLASS 2, MULCH METHOD 2
 - ⑦ EXISTING AGGREGATE BASE WITH HOT-MIX ASPHALT SURFACE
 - ⑧ EXISTING HOT-MIX ASPHALT SHOULDER
 - ⑨ EXISTING HOT-MIX ASPHALT SURFACE
 - ⑩ EXISTING B-6.18 CURB AND GUTTER
 - ⑪ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70-1 1/4"
 - ⑫ HOT-MIX ASPHALT BINDER COURSE, IL-12.5, N50-2"
 - ⑬ HOT-MIX ASPHALT PAVEMENT, (FULL DEPTH), 13 1/2"
 - ⑭ STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ⑮ BITUMINOUS CONCRETE SURFACE COURSE, MIXTURE D, CLASS I: 1 1/2"
 - ⑯ EXISTING BITUMINOUS SURFACE
 - ⑰ LEVELING BINDER (MACHINE METHOD) 1/2"
 - ⑱ 1/2" DEFORMED BAR
 - ⑲ EXISTING PAVEMENT PCC 7"
 - ⑳ PAVEMENT FABRIC
 - ㉑ AGGREGATE SHOULDERS, TYPE B

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS - ANDREWS DRIVE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	99-00036-00-BR	BOND	99	11
CONTRACT NO. 97366				

SCALE: 50 SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT