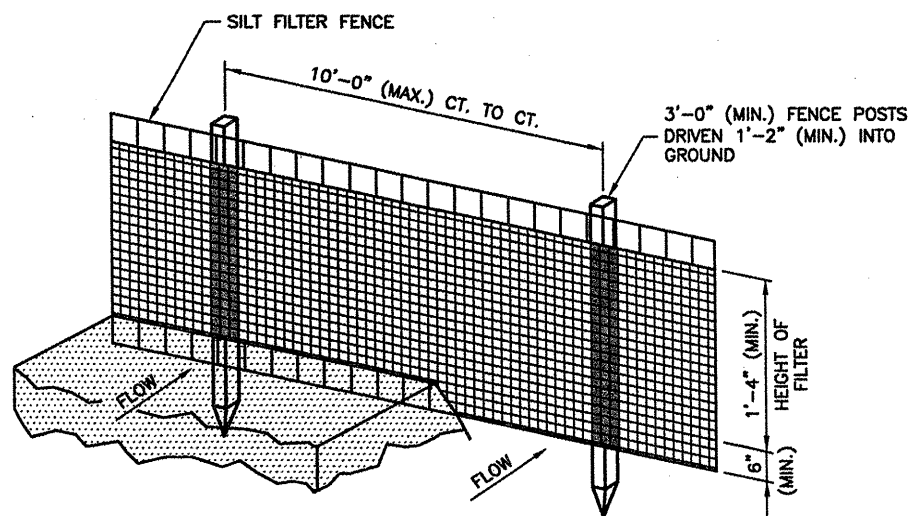


SECTION



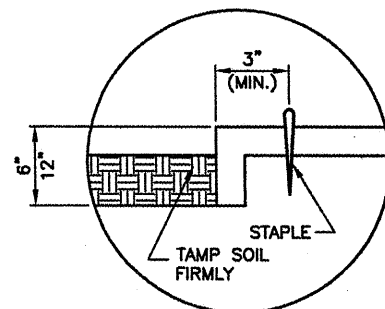
PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS

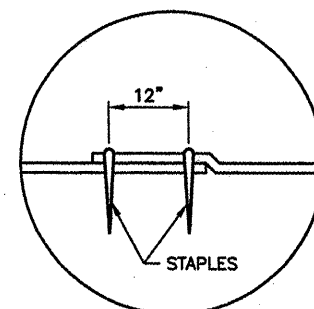
N.T.S.

EROSION CONTROL FABRIC FENCE NOTES

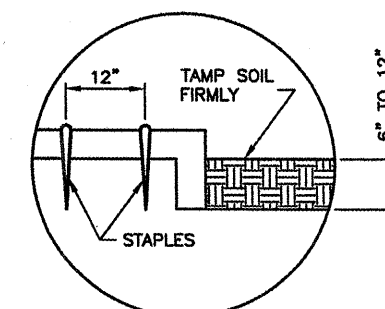
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



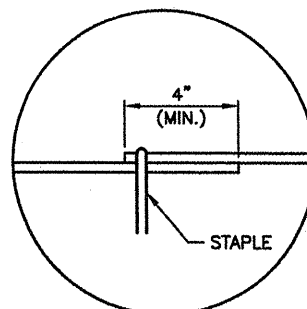
DETAIL 1 - TERMINAL FOLD



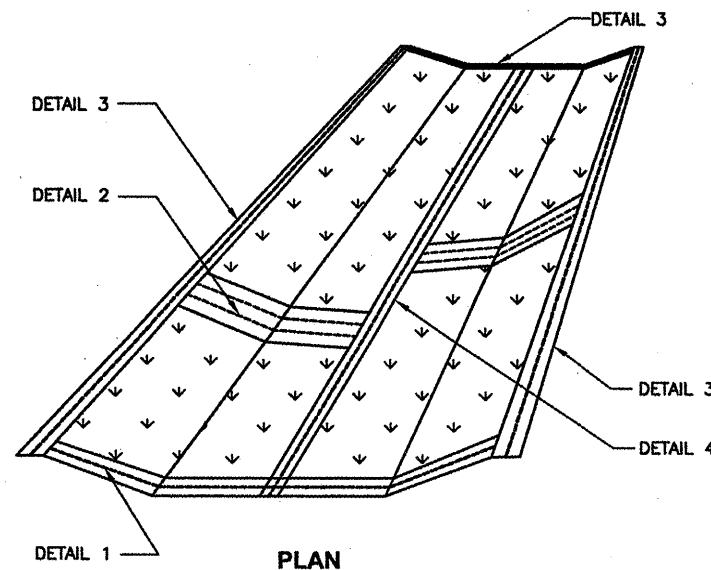
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



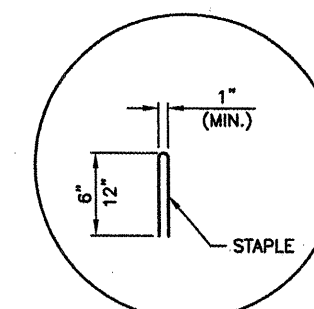
PLAN

EXCELSIOR BLANKET DETAILS

N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



DETAIL 5 - STAPLE DETAIL

K:\Springfield\p\0703509\Draw\Sheets\Phase 2 Full
FILE: EROSDETL.dwg
UPDATE BY: Kristy Brod
PLOT DATE: 1/21/2009 11:07 AM

CA007

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2'
AT FULL SCALE (34x22).



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

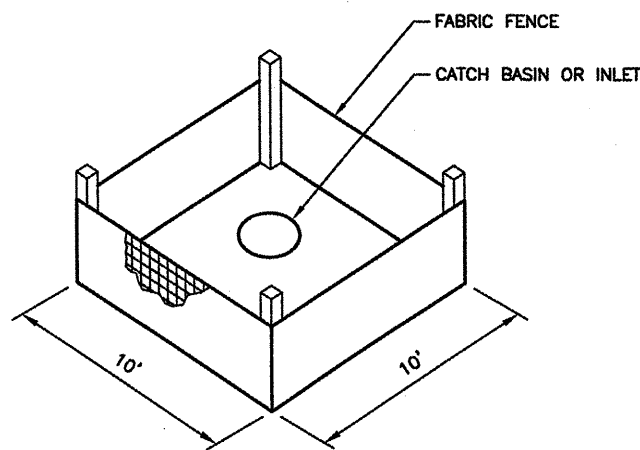
WIDEN TAXIWAY G - PHASE 2

EROSION CONTROL DETAILS

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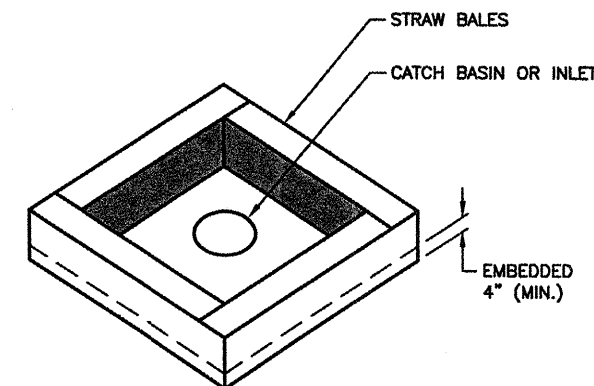


DESIGN BY:	CBG
DRAWN BY:	CMT
CHECKED BY:	REN
APPROVED BY:	REN
DATE:	01/30/2009
JOB No:	07035-09-00
IL PROJ. NO.	SPI-3860
AIP PROJ. NO.	3-17-0096-XX
SHEET	57 OF 65 SHEETS



INLET PROTECTION WITH FABRIC

N.T.S.



INLET PROTECTION WITH STRAW BALES

N.T.S.