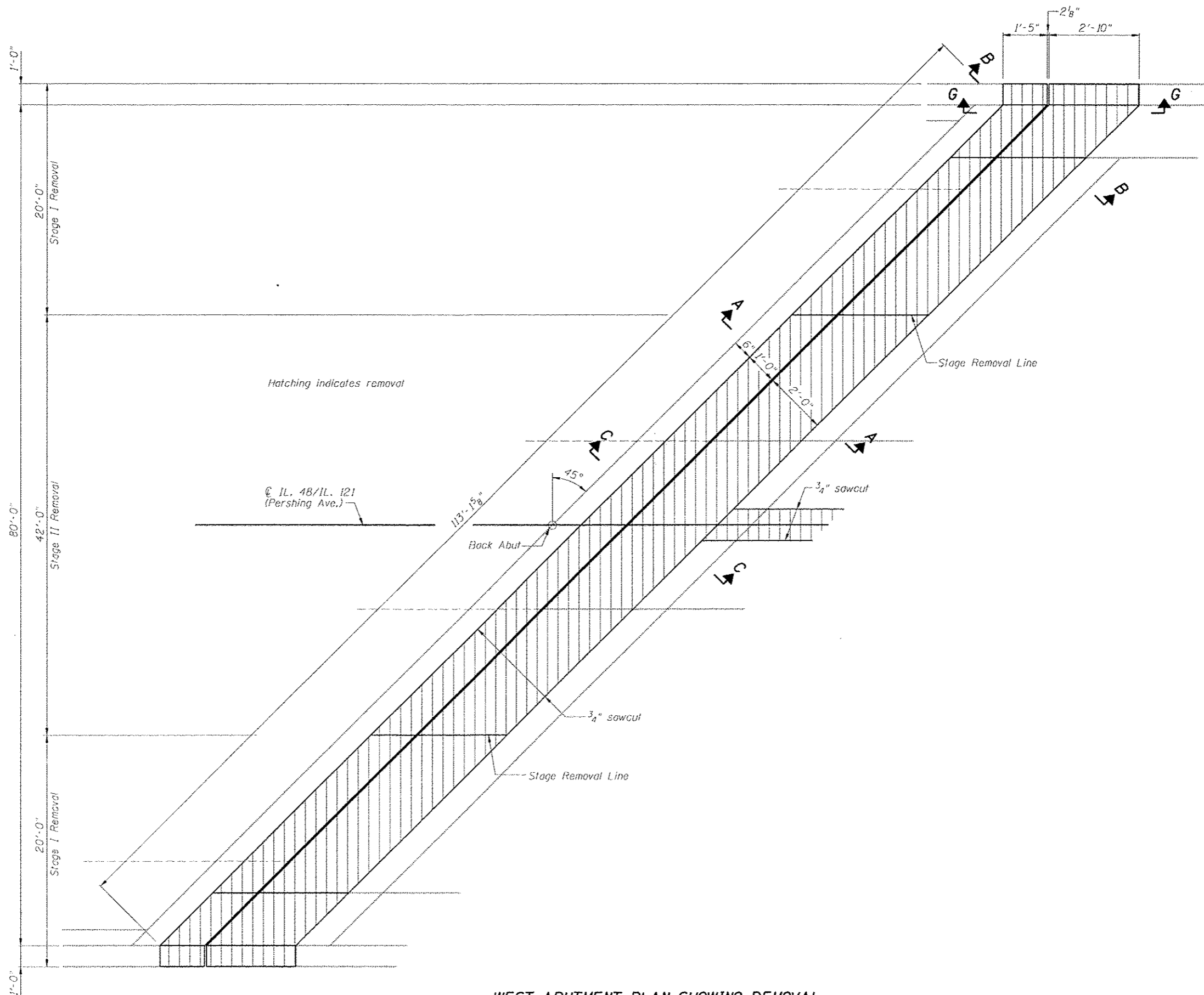


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WEST ABUTMENT PLAN SHOWING REMOVAL
(West abutment shown East abutment similar)

CHASTAIN & ASSOCIATES LLC
 CONSULTING ENGINEERS
 184-001397

USER NAME : abullock
 PLOT TIME : 4:44:58 PM
 PLOT SCALE : 1/8" = 1'-0"
 PLOT DATE : 8/19/2014

DESIGNED - JMB
 CHECKED - ACB
 DRAWN - RLK
 CHECKED - JMB

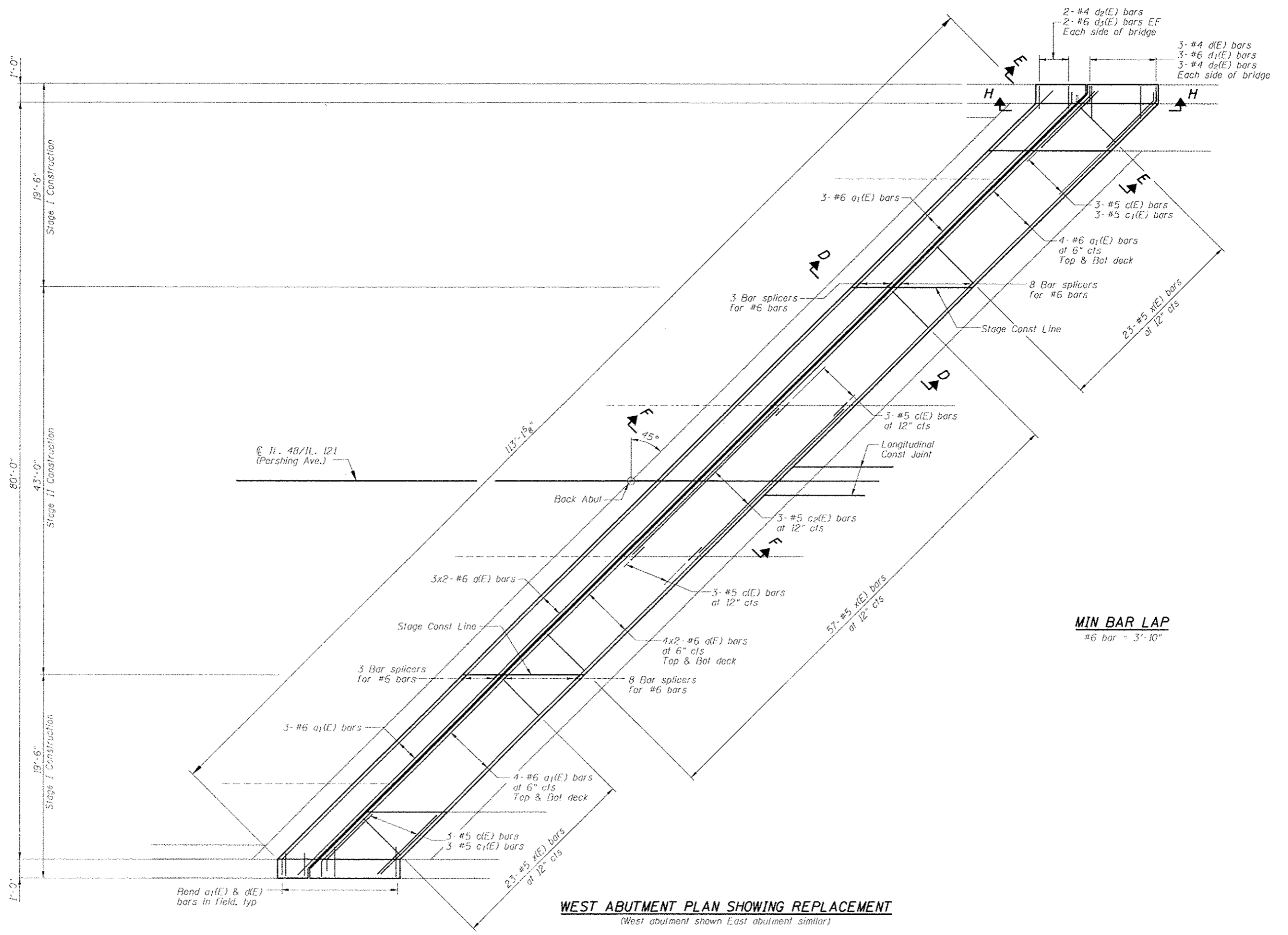
REVISED -
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 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXPANSION JOINT REPLACEMENT DETAILS
 STRUCTURE NO. 058-0050**

SHEET NO. 3 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
320	1126X1RS-2.135RS-1	MACON	119	101
STA.	SN. 058-0050	CONTRACT NO. 74624		
ILLINOIS FED. AID PROJECT				



WEST ABUTMENT PLAN SHOWING REPLACEMENT
 (West abutment shown East abutment similar)

FILE NAME: I:\Users\ASB23\d7\work\work order 3 - 1121\work-facility\520.Structure\plan-replace.dgn

CHASTAIN & ASSOCIATES LLC
 CONSULTING ENGINEERS
 184-001397

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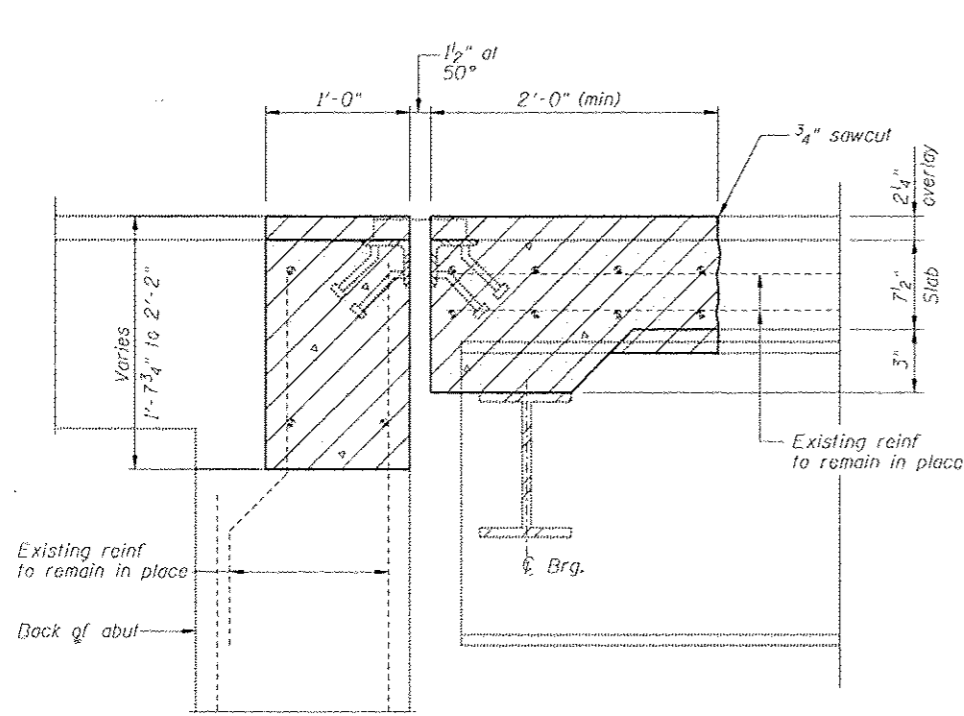
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REVISED: -
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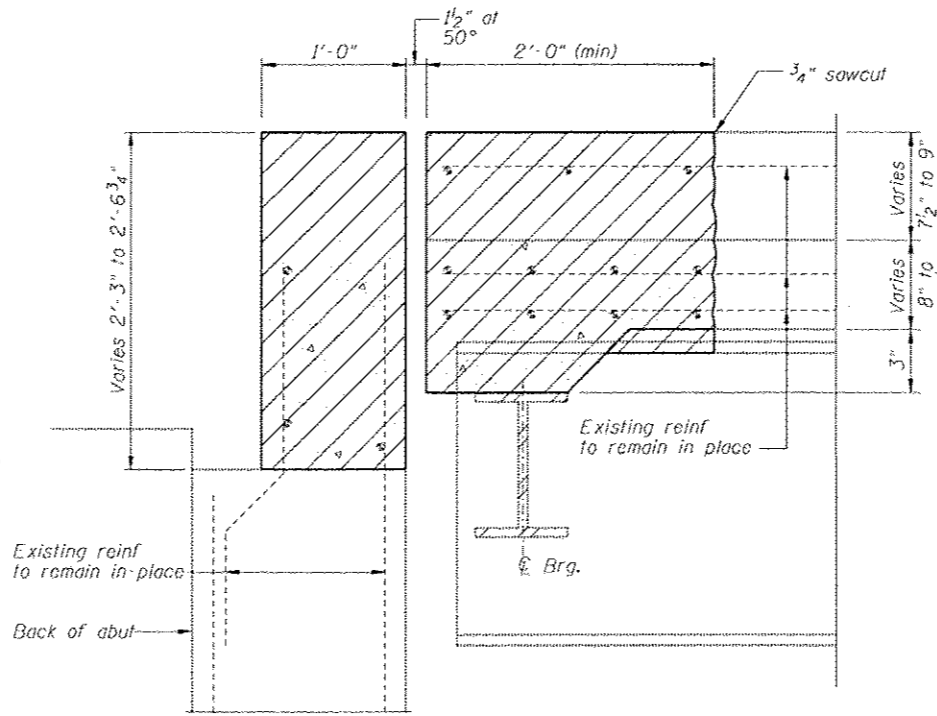
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPLACEMENT DETAILS
STRUCTURE NO. 058-0050
 SHEET NO. 4 OF 13 SHEETS

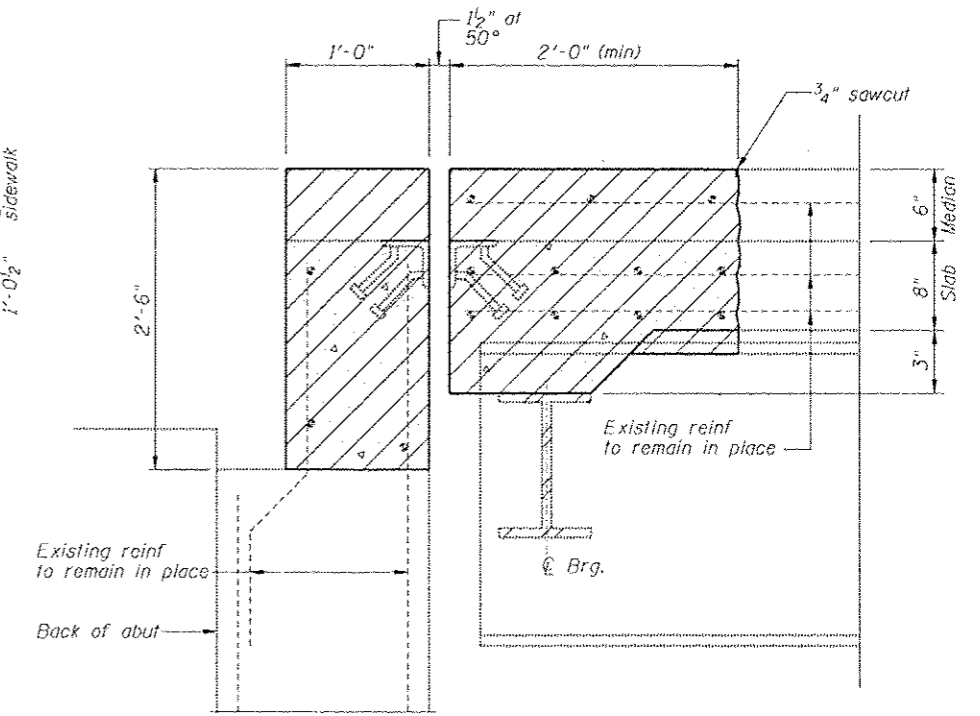
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
320	126XRS-2,135RS-1	MACON	119	102
	SN. 058-0050	CONTRACT NO.	74624	
STA.	ILLINOIS FED. AID PROJECT			



SECTION A-A

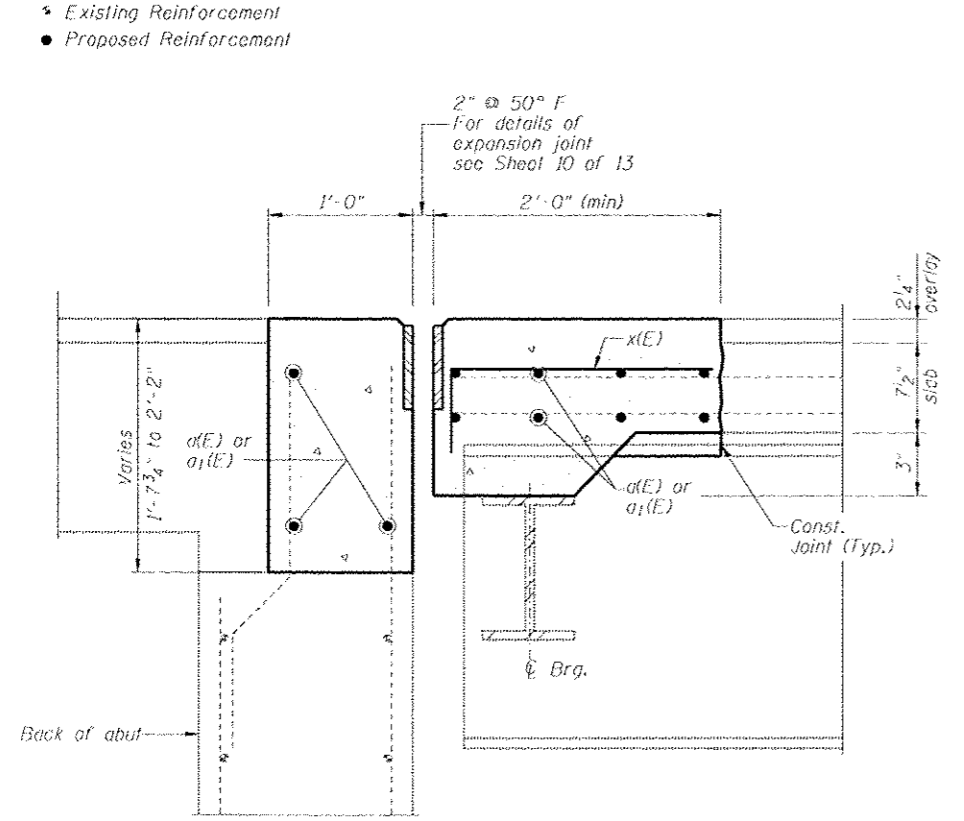


SECTION B-B

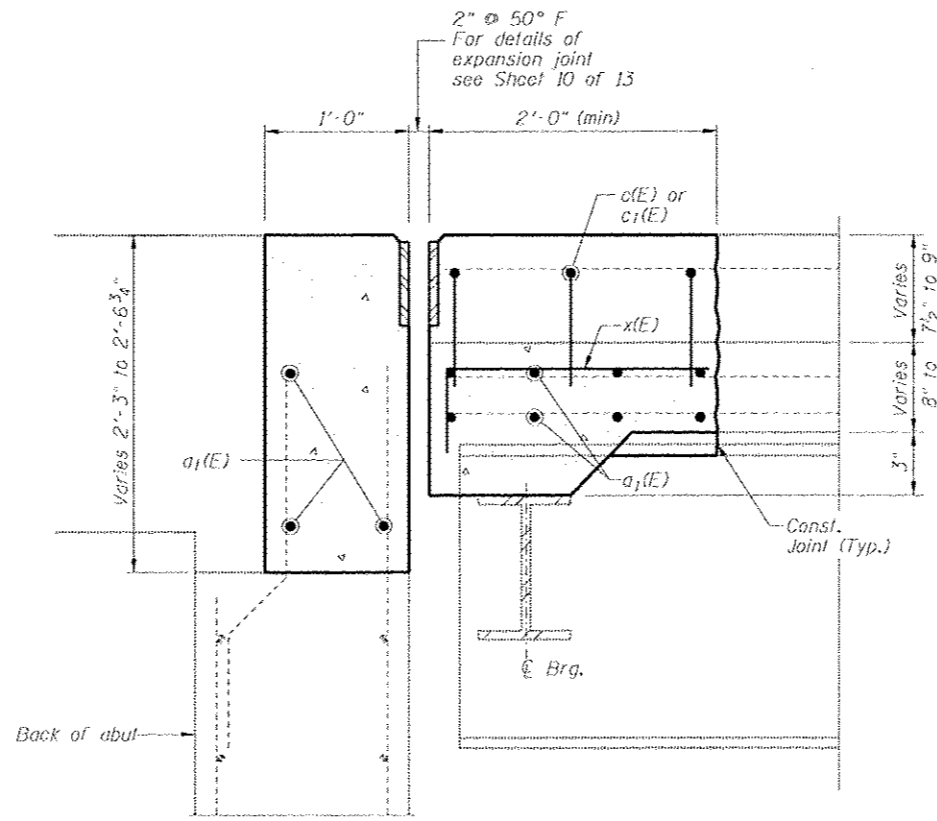


SECTION C-C

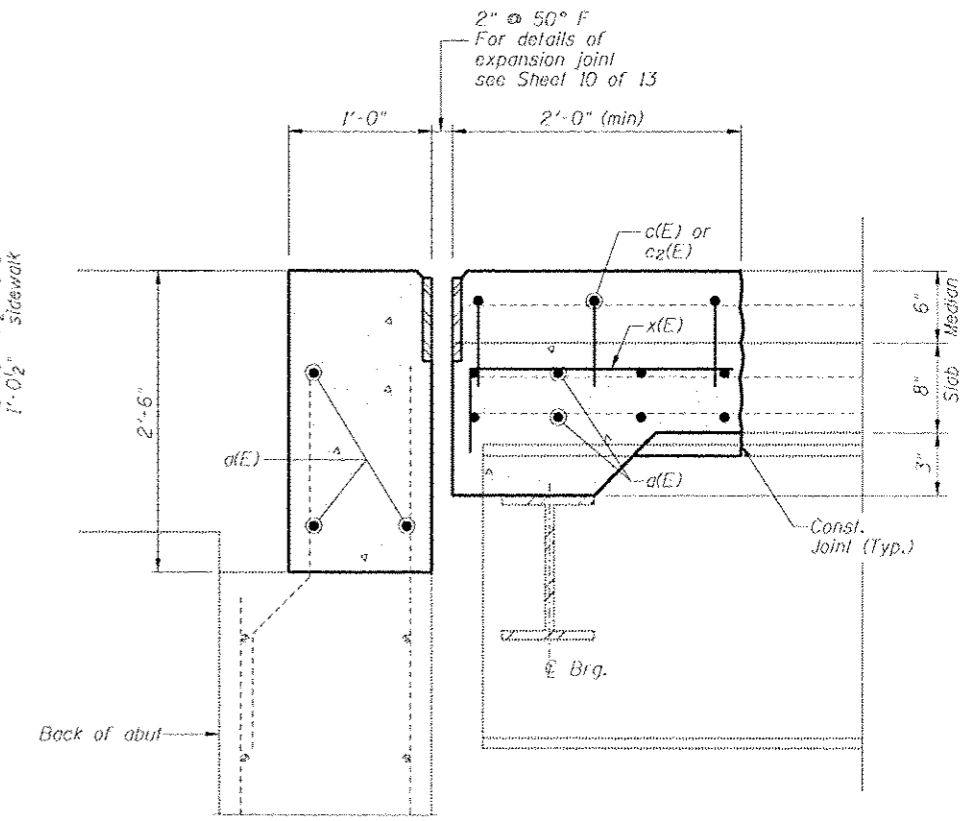
- Existing Reinforcement
- Proposed Reinforcement



SECTION D-D



SECTION E-E



SECTION F-F

FILE NAME: I:\projects\058-0050\058-0050-1\121\msh\fac\058-0050-1\058-0050-1.dwg
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CHASTAIN & ASSOCIATES LLC
 CONSULTING ENGINEERS
 184-001397

USER NAME: aboiback
 PLOT TIME: 4:47:50 PM
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 PLOT DATE: 07/19/2014

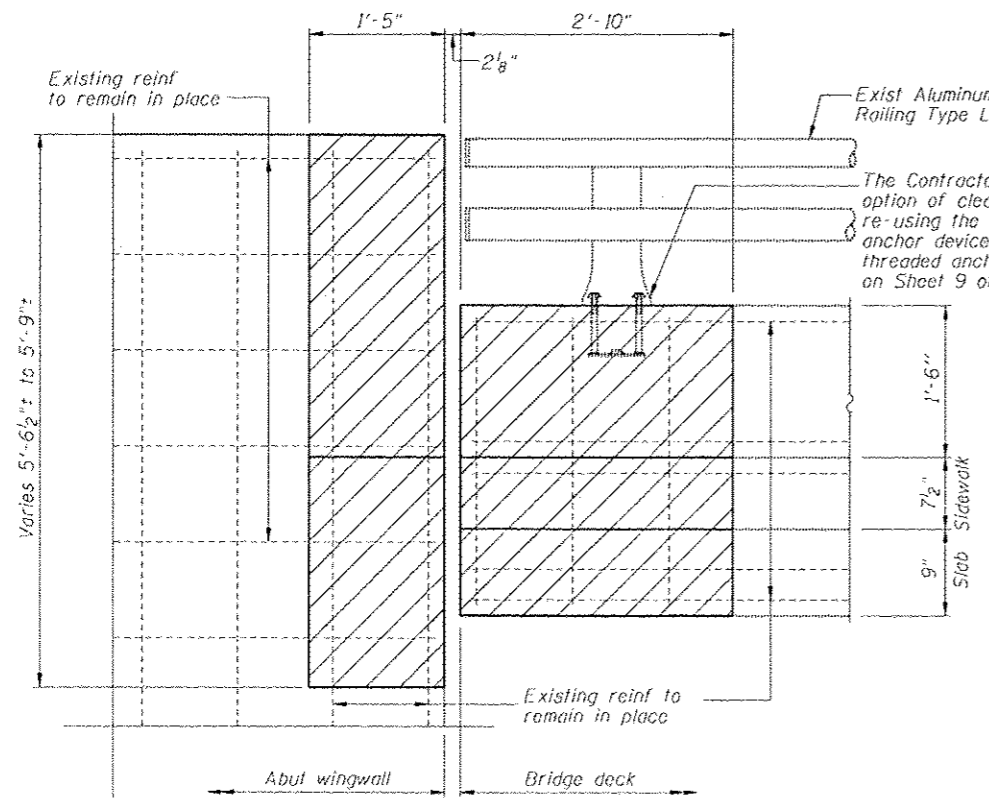
DESIGNED - JMB
 CHECKED - ACB
 DRAWN - RLK
 CHECKED - JMB

REVISED -
 REVISED -
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 REVISED -

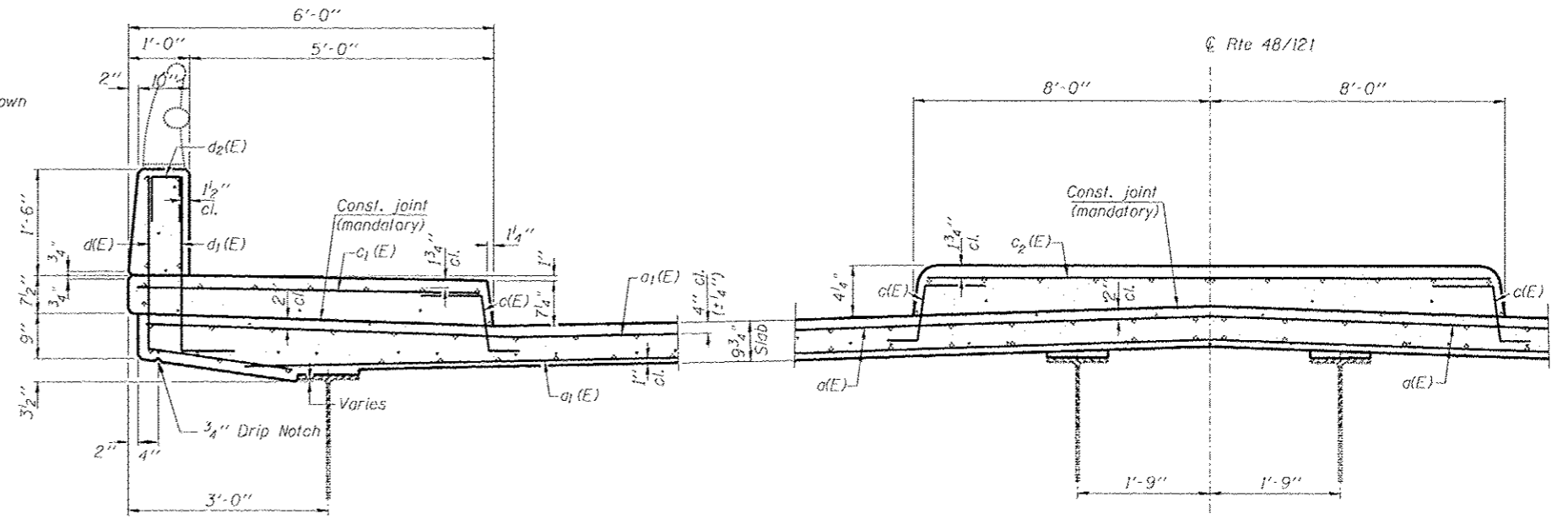
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPLACEMENT DETAILS
STRUCTURE NO. 058-0050
 SHEET NO. 5 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.	ILLINOIS FED. AID PROJECT			

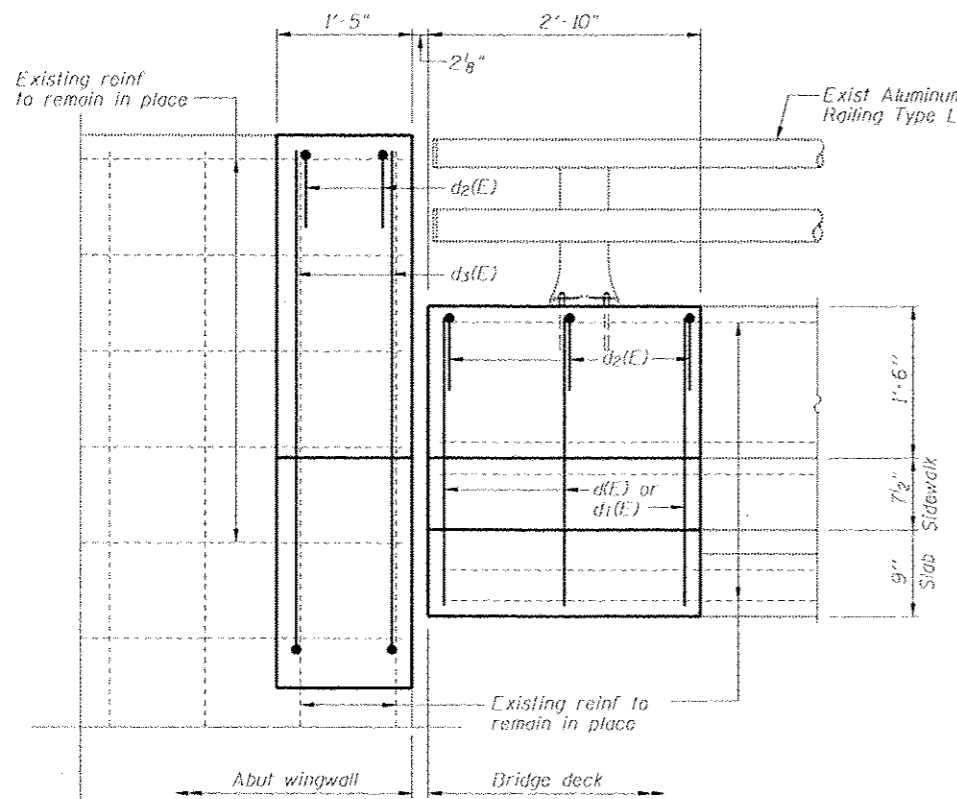


SECTION G-G

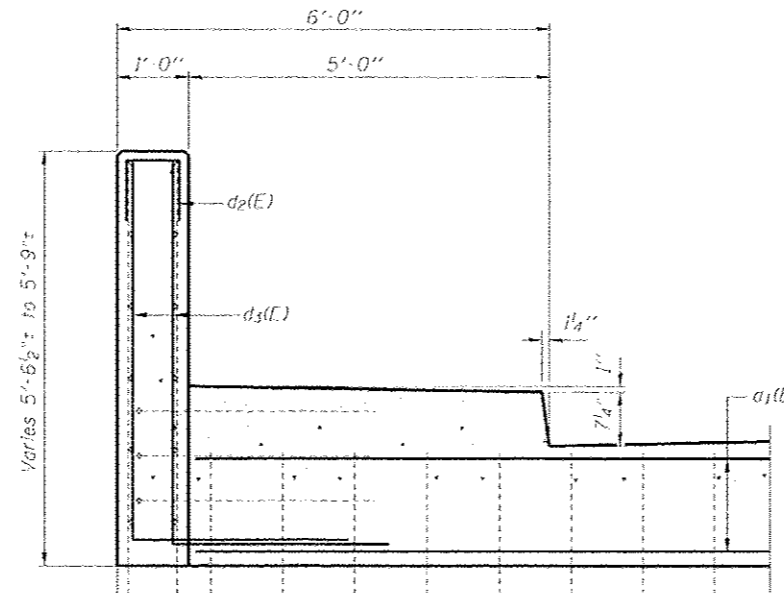


SECTION THRU SIDEWALK

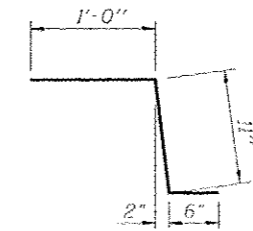
SECTION THRU MEDIAN



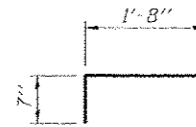
SECTION H-H



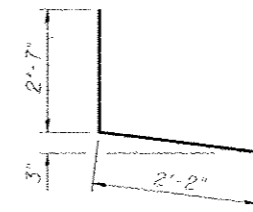
SECTION THRU APPROACH SIDEWALK



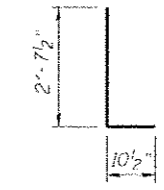
BAR c(E)



BAR x(E)



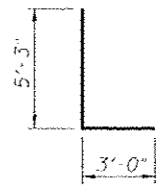
BAR d(E)



BAR d1(E)



BAR d2(E)



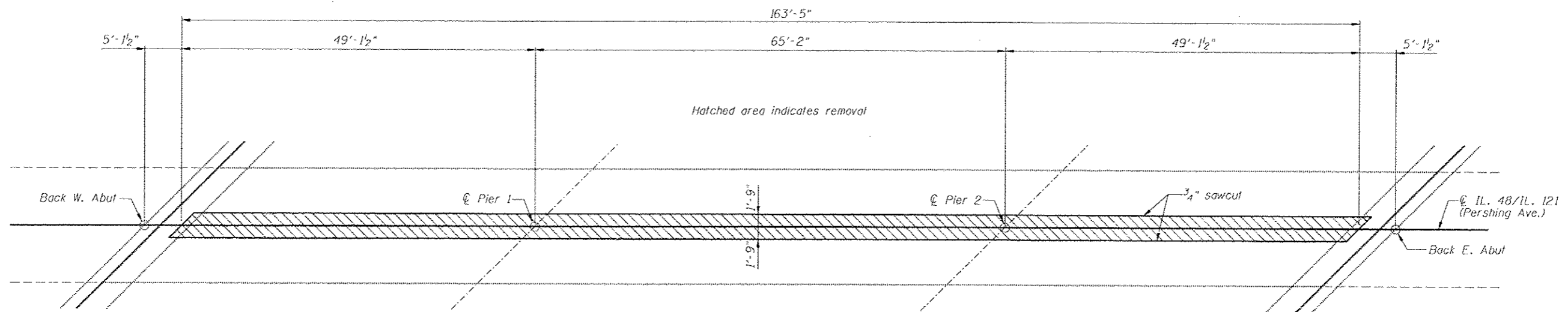
BAR d3(E)

BILL OF MATERIAL (BOTH ABUTMENTS)

Bar	No.	Size	Length	Shape
d(E)	44	#6	32'-2"	┌
a1(E)	44	#6	28'-8"	┌
c(E)	24	#5	2'-5"	┌
c1(E)	12	#5	8'-0"	┌
c2(E)	6	#5	21'-8"	┌
d1(E)	12	#4	4'-9"	┌
d1(E)	12	#6	3'-6"	┌
d2(E)	20	#4	2'-1"	┌
d3(E)	16	#6	8'-3"	┌
x(E)	206	#5	2'-3"	┌
Concrete Removal			Cu. Yd.	37.7
Concrete Superstructure			Cu. Yd.	37.7
Reinforcement Bars, Epoxy Coated			Pound	5130

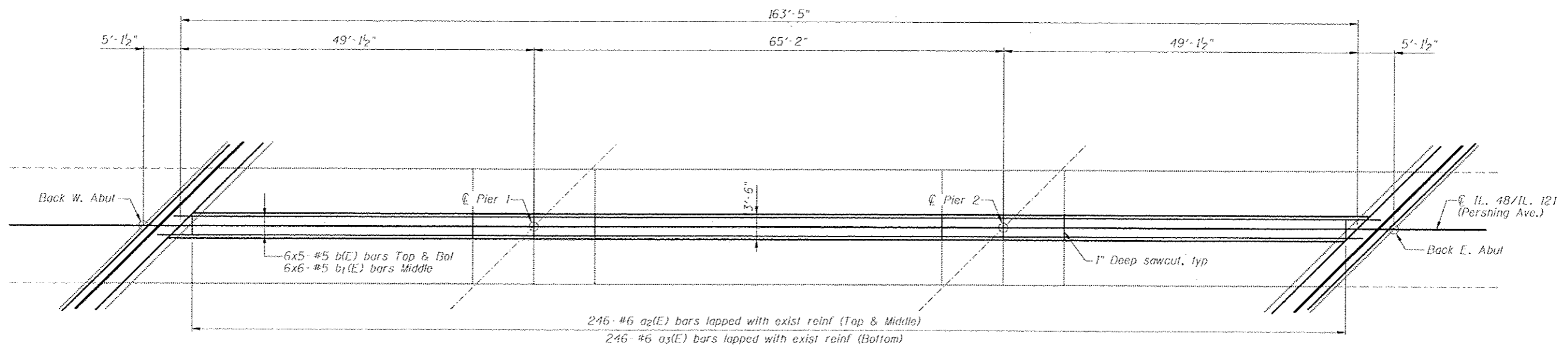
Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

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DECK PLAN SHOWING LONGITUDINAL JOINT REMOVAL - STAGE II

MIN BAR LAP
#5 bar = 2'-6"

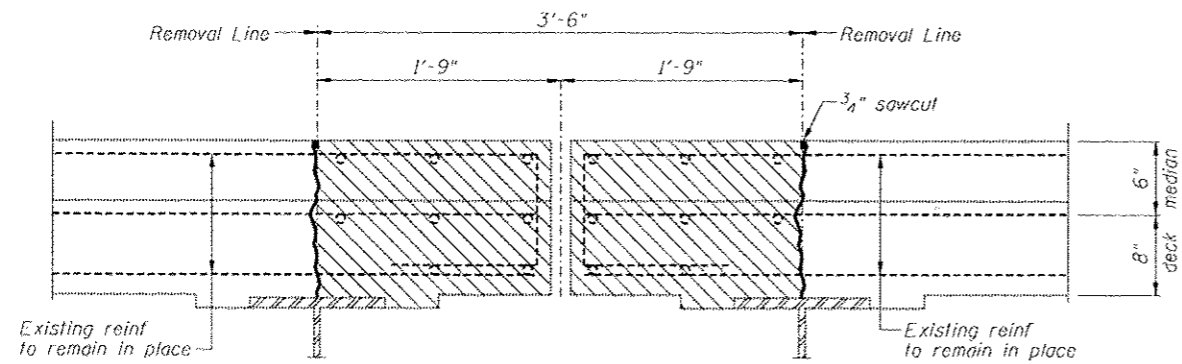


DECK PLAN SHOWING LONGITUDINAL JOINT RECONSTRUCTION - STAGE II

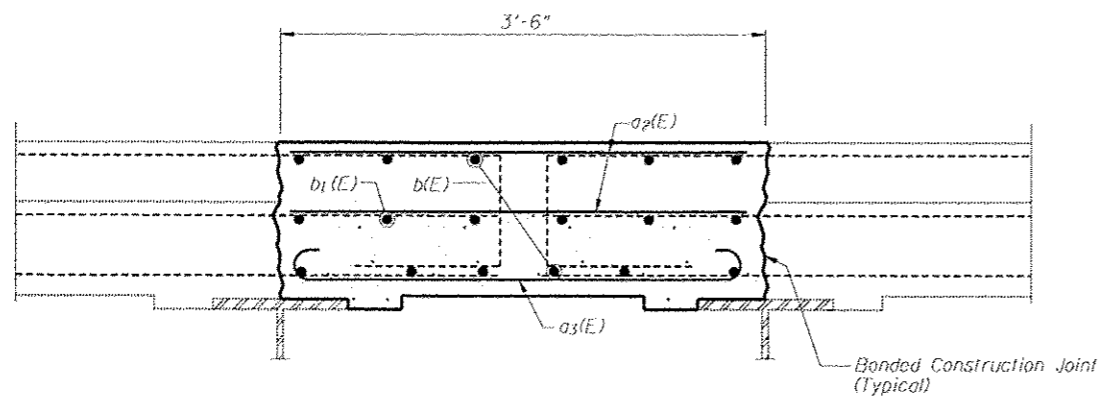
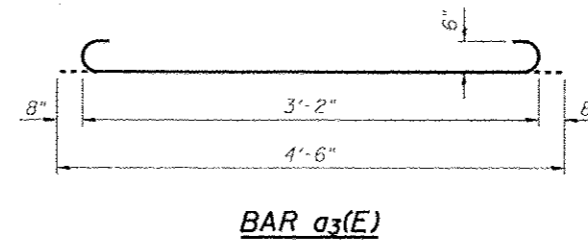
Note:
Hatched areas indicate concrete sections to be saw cut 3/4" prior to the removal of the concrete.
Existing reinforcement shall be cleaned and incorporated into the new construction.

FILE NAME: I:\projects\058-0050\Structure\LongJoint\stage2.dwg

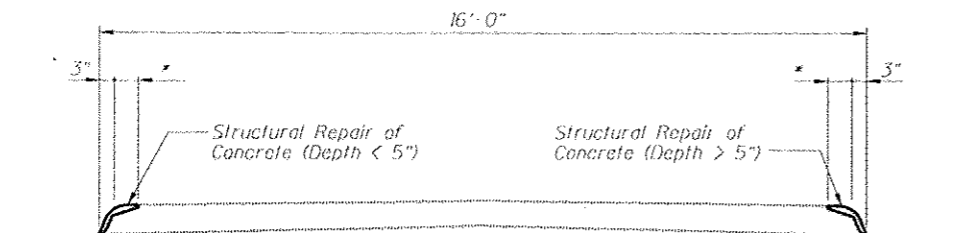
CHASTAIN & ASSOCIATES LLC CONSULTING ENGINEERS 184-001397	USER NAME: jbullock PLOT TIME: 4/4/18 PM PLOT SCALE: 1/8" = 1'-0" PLOT DATE: 6/19/2014	DESIGNED: JMB CHECKED: ACB DRAWN: RLK CHECKED: JMB	REVISED: - REVISED: - REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LONGITUDINAL JOINT REPLACEMENT DETAILS STRUCTURE NO. 058-0050 SHEET NO. 7 OF 13 SHEETS	F.A.P. RTE.: 320 SECTION: (126)RS-2,13SR5-1 COUNTY: MACON STA.:	TOTAL SHEETS: 119 SHEET NO.: 105 CONTRACT NO. 74624 ILLINOIS FED. AID PROJECT



EXISTING MEDIAN LONGITUDINAL JT. CROSS SECTION



RECONSTRUCTED MEDIAN LONGITUDINAL JOINT



Note:
Quantity of repairs are estimated based on a visual survey and does not limit the amount of repair required as directed by the Engineer.

* Width as directed by the Engineer.

BRIDGE MEDIAN REPAIR
(Looking West)

BILL OF MATERIAL

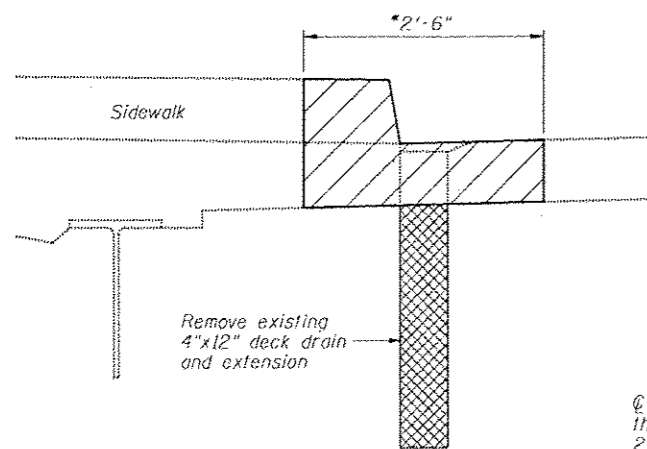
Bar	No.	Size	Length	Shape
a2(E)	492	#6	3'-2"	—
a3(E)	246	#6	4'-6"	—
b(E)	60	#5	35'-0"	—
b1(E)	36	#5	29'-8"	—
Concrete Removal			Cu. Yd.	24.8
Concrete Superstructure			Cu. Yd.	24.8
Reinforcement Bars, Epoxy Coated			Pound	7310
Structural Repair of Concrete (Depth > 5")			Sq. Ft.	338
Structural Repair of Concrete (Depth < 5")			Sq. Ft.	150

Bars indicated thus 1 x 2 - #5 etc. indicates 1 line of bars with 2 lengths per line.

Note:
Hatched areas indicate concrete sections to be saw cut 3/4" prior to the removal of the concrete. Existing reinforcement shall be cleaned and incorporated into the new construction.

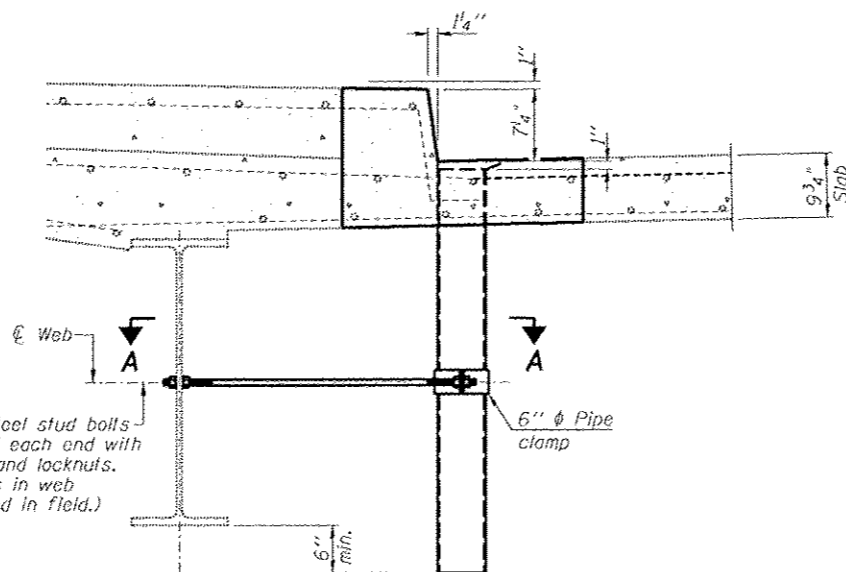
Saw cuts in existing median near piers shall be extended through new median section.

C:\E:\New\1\1121\resu\Facing\002_Structural\LongJoint.dwg



Hatched area indicates concrete removal at floor drain replacement.

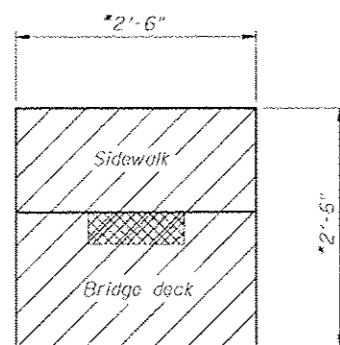
SECTION THRU EXISTING FLOOR DRAIN



3/4" ϕ steel stud bolts threaded 6" each and with 2 washers and locknuts. 1/8" ϕ holes in web (To be drilled in field.)

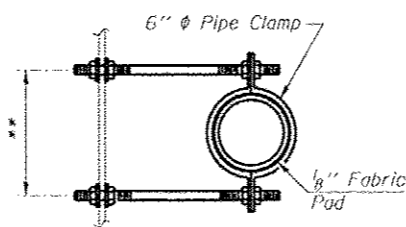
6" ϕ Pipe clamp

DRAIN REPLACEMENT DETAIL



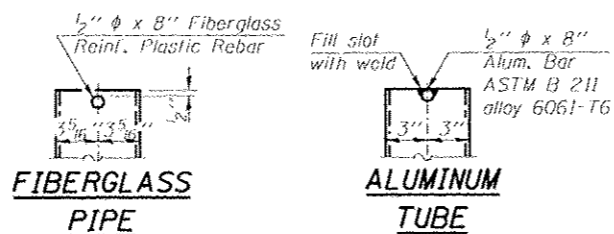
PLAN AT DECK DRAIN REMOVAL

* Concrete removal and replacement dimensions shall be determined by the Engineer based on the extent of deck deterioration of each drain.



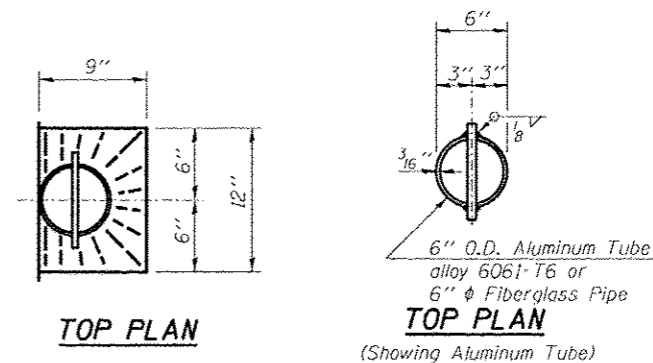
SECTION A-A

** Dimension as required by Pipe Clamp



FIBERGLASS PIPE

ALUMINUM TUBE



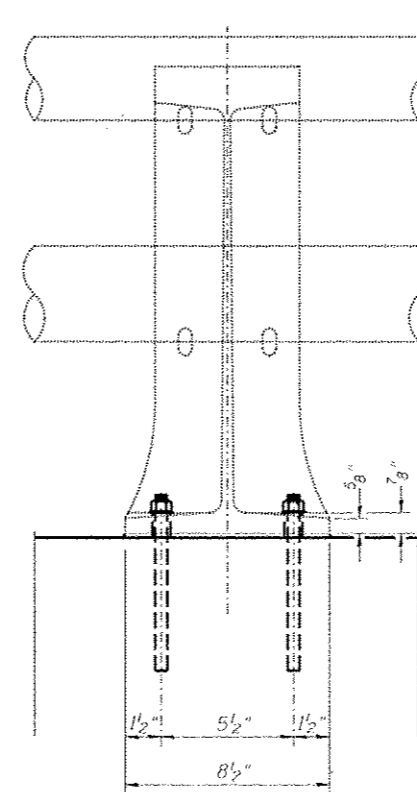
TOP PLAN

TOP PLAN

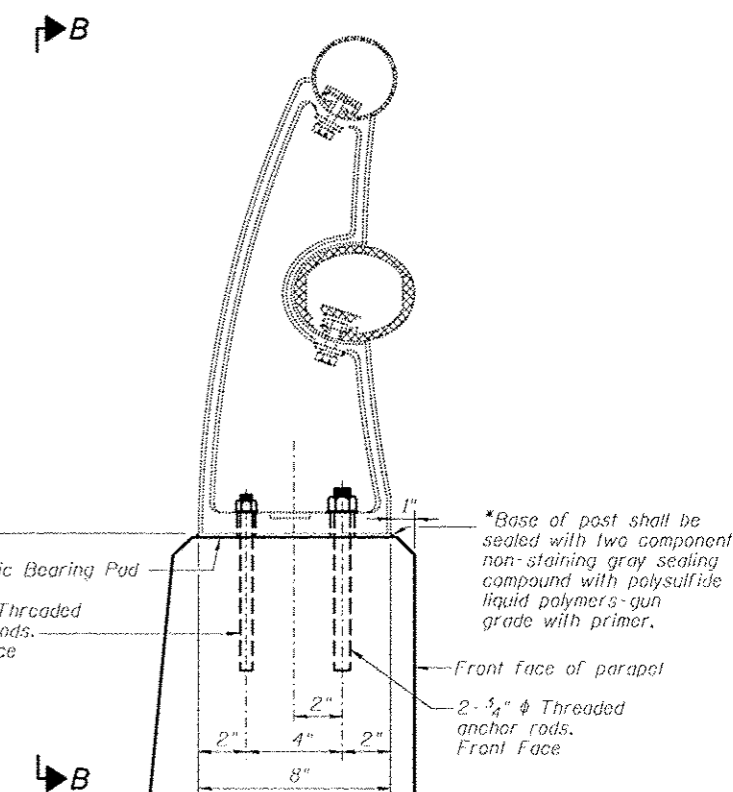
(Showing Aluminum Tube)

BILL OF MATERIAL

Item	Unit	Total
Floor Drains	Each	22
Deck Slab Repair (Full Depth Type II)	Sq. Yd.	15.4



VIEW B-B



SECTION A-A

RAIL POST DETAILS

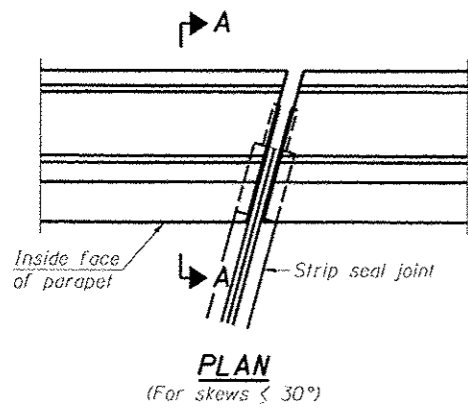
* New Rail Post anchorage devices will be required at each location where posts are connected to new construction when existing anchorage devices cannot be salvaged. Cost shall be included with Concrete Superstructure.

Notes:
The Contractor shall drill and set stainless steel anchor rods (ASTM A193, B7 or equivalent), according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturers specifications.
All Posts shall be normal to parapet.
Provide 1-1/8" and 2-1/16" Aluminum Shims for the Posts.
Horizontal rail element & rail posts shown are for information only.

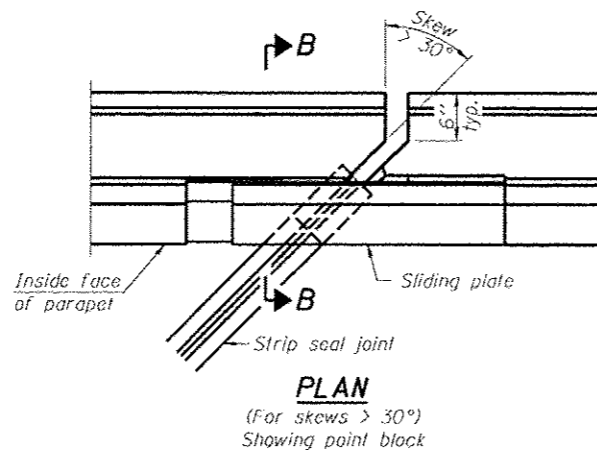
Notes:

The exterior surfaces of the floor drains shall be cleaned and painted as specified in Section 506 of the Standard Specifications.
Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
Galvanize clamping device according to AASHTO M232. Cost of clamping device and inserts is included with Floor Drains.
See Sheet 1 of 13 for floor drain replacement locations.
Concrete removal and replacement quantities for floor drains are included in Deck Slab Repair (Full Depth Type II).
Existing reinforcement in the removal area shall be cleaned and straightened and incorporated into the new construction.

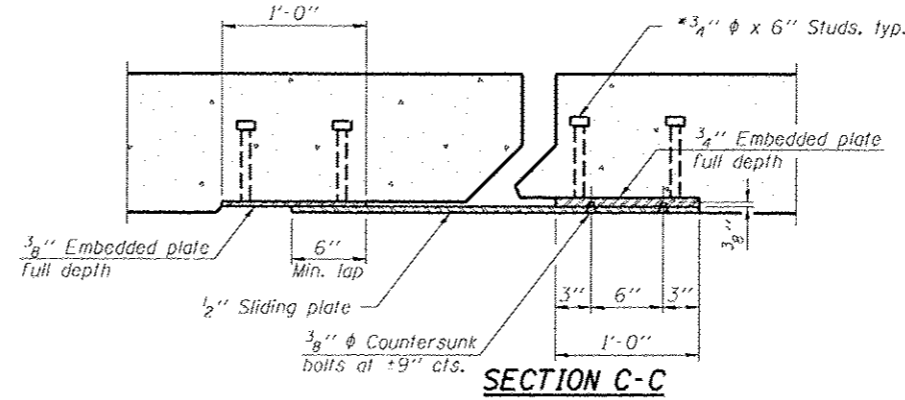
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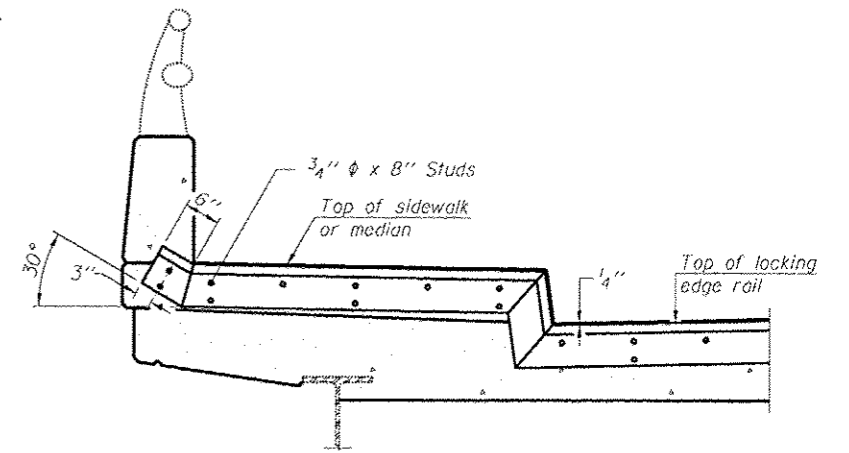
PLAN
(For skews $\le 30^\circ$)



PLAN
(For skews $> 30^\circ$)
Showing point block

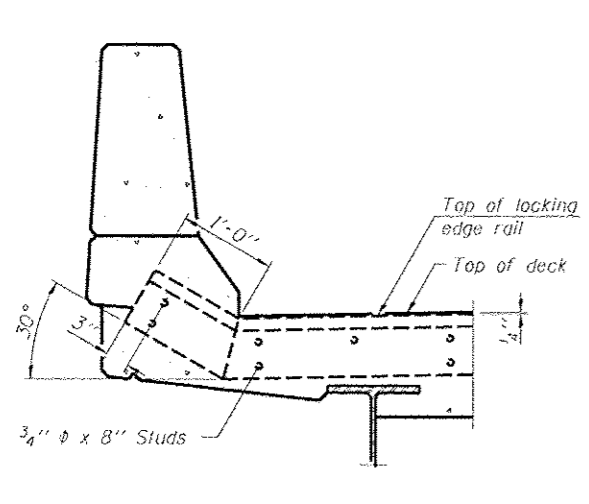


SECTION C-C

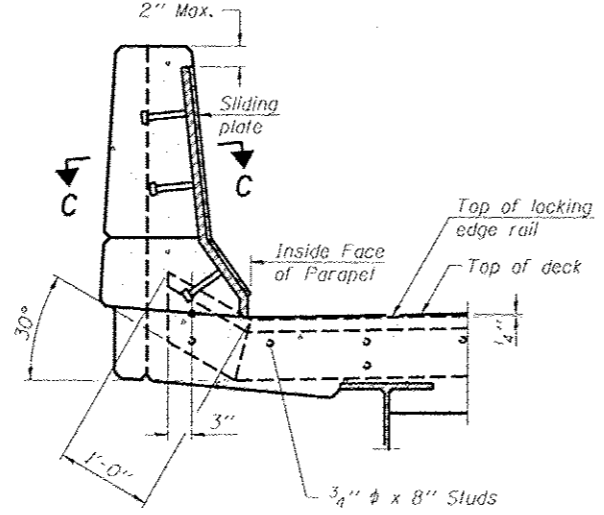


TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

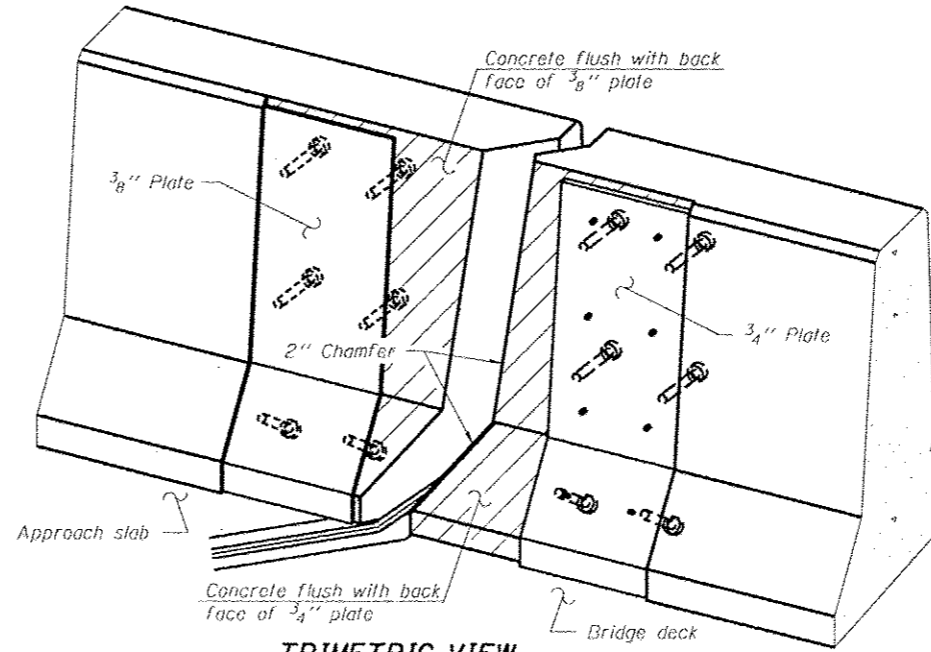
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



SECTION A-A



SECTION B-B



TRIMETRIC VIEW
(Showing back plates only)

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

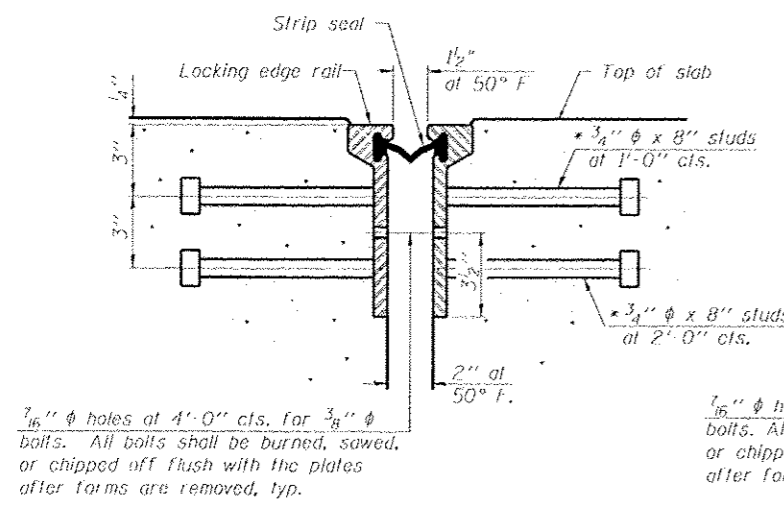
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

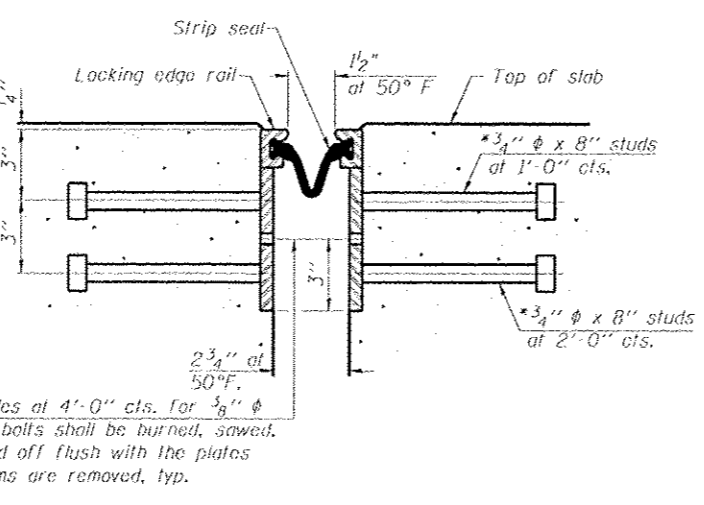
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments shall be $\frac{3}{16}$ ", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews $> 30^\circ$ included in the cost of Preformed Joint Strip Seal.



SECTION THRU ROLLED RAIL JOINT



SECTION THRU WELDED RAIL JOINT

$\frac{7}{16}$ " ϕ holes at 4'-0" cts. for $\frac{3}{8}$ " ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

$\frac{1}{16}$ " ϕ holes at 4'-0" cts. for $\frac{3}{8}$ " ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

ROLLED EXTRUDED RAIL WELDED RAIL

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

*** Back gauge not required if complete joint penetration is verified by mock-up.

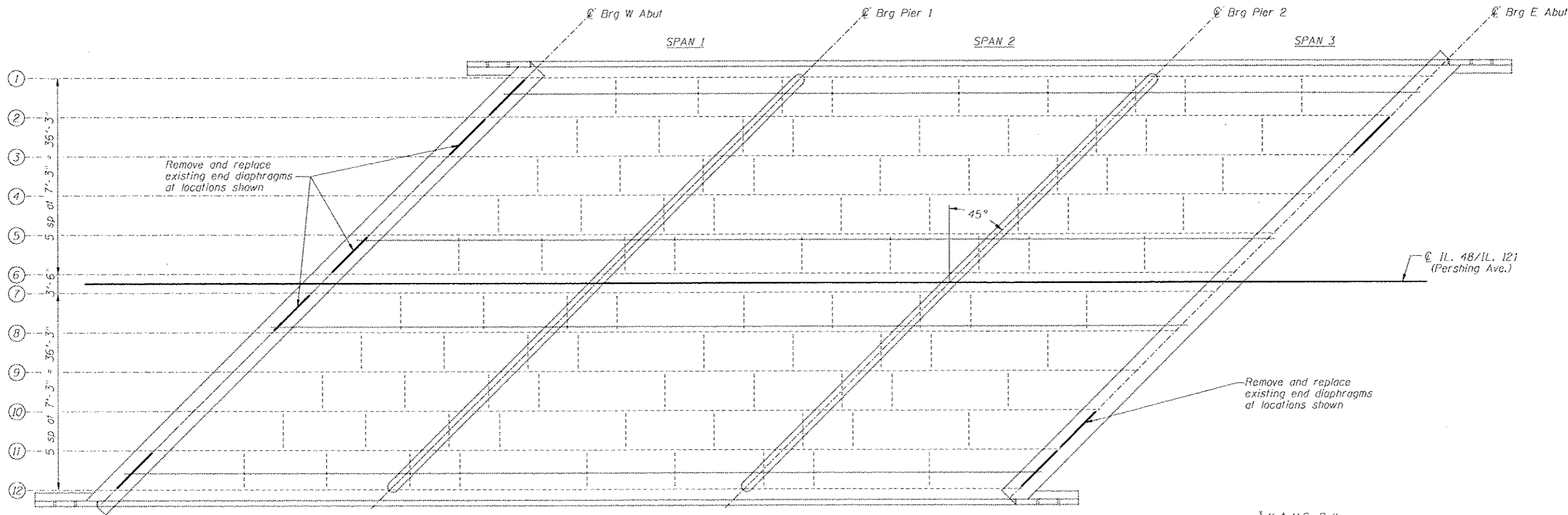
LOCKING EDGE RAILS

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

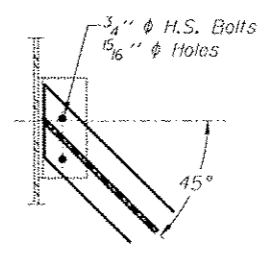
BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	235

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FRAMING PLAN

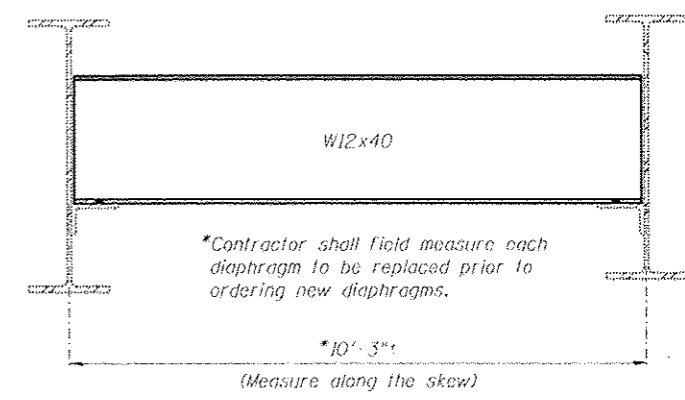


PLAN

Note:
Two hardened washers shall be required over all oversize holes for diaphragms.
The Contractor shall field drill holes in the new diaphragm using existing connection angles as a template. Cost included with Furnishing and Erecting Structural Steel.

BILL OF MATERIAL

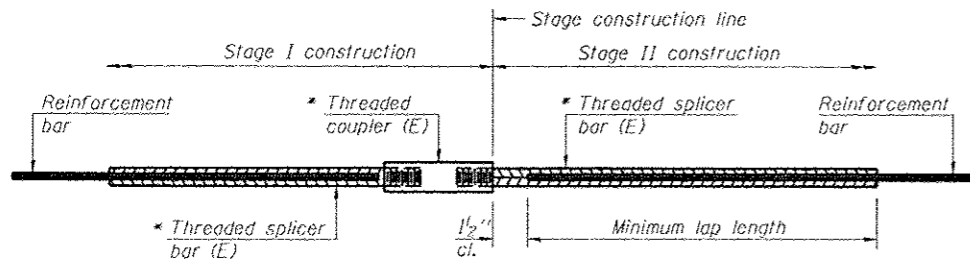
Item	Unit	Total
Furnishing & Erecting Structural Steel	Pound	3220
Structural Steel Removal	Pound	3220



DIAPHRAGM REPLACEMENT DETAIL

(3 Required)

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STANDARD BAR SPLICER ASSEMBLY

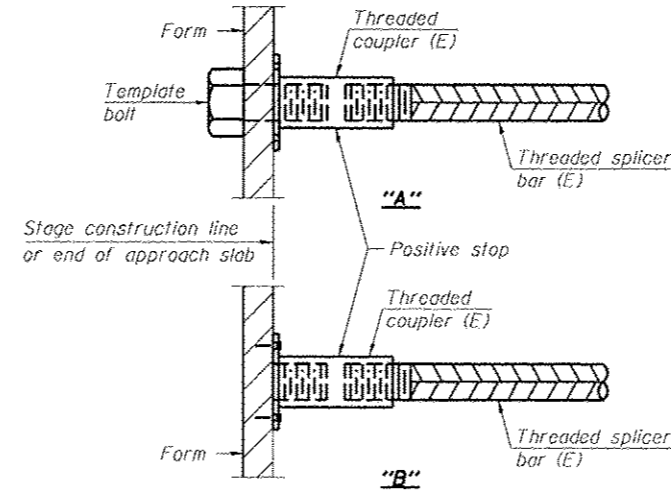
Minimum Lap Lengths						
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar lap, Class C

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

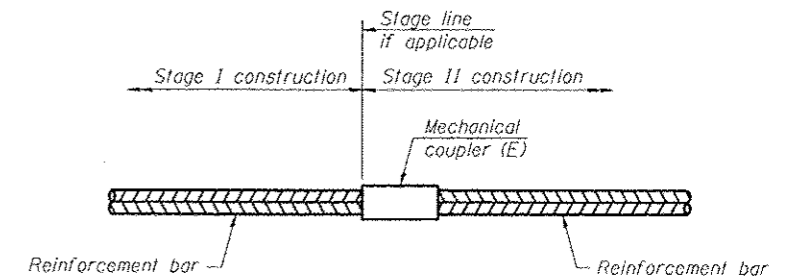
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
West Abutment	#6	22	Table 3
East Abutment	#6	22	Table 3



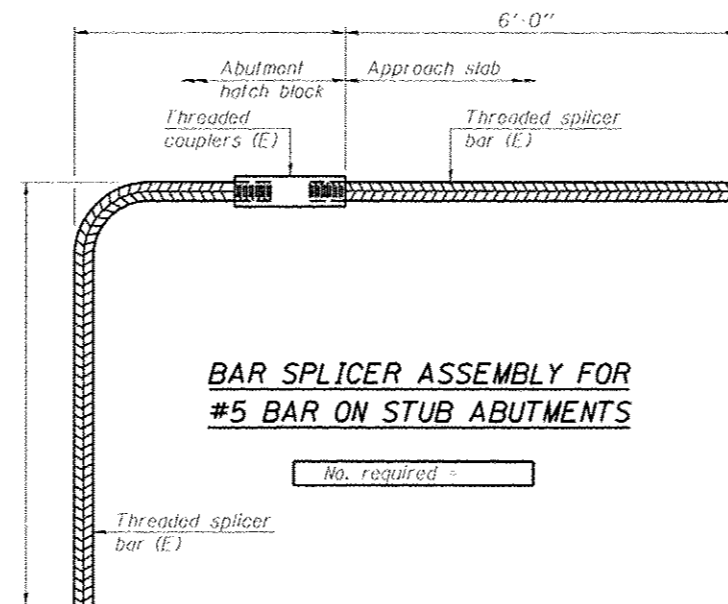
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
 "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E): Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

FILE NAME: I:\notes\B222 - d7 var-va-veck order 3 - 11 121 resur facing\0000 Structural\bar-splicer.dwg

BSD-1

8-31-12

CHASTAIN & ASSOCIATES LLC
 CONSULTING ENGINEERS
 184-001397

USER NAME: abullock
 PLOT TIME: 4:47:15 PM
 PLOT SCALE: 2/8" = 1'-0"
 PLOT DATE: 8/19/2014

DESIGNED: JMB
 CHECKED: ACB
 DRAWN: RLK
 CHECKED: JMB

REVISED: -
 REVISED: -
 REVISED: -
 REVISED: -

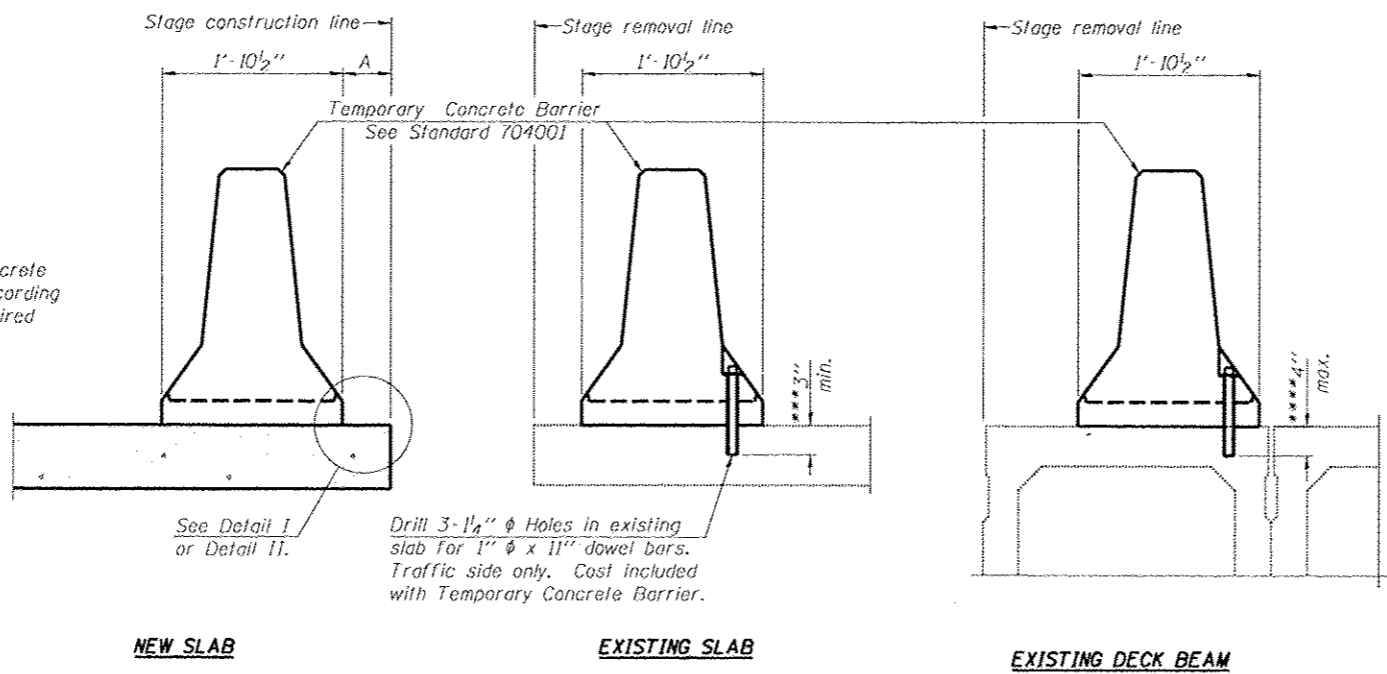
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 058-0050

SHEET NO. 12 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
320	(126X)RS-2,135RS-1	MACON	119	110
STA.	SN. 058-0050	CONTRACT NO. 74624		
ILLINOIS FED. AID PROJECT				

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

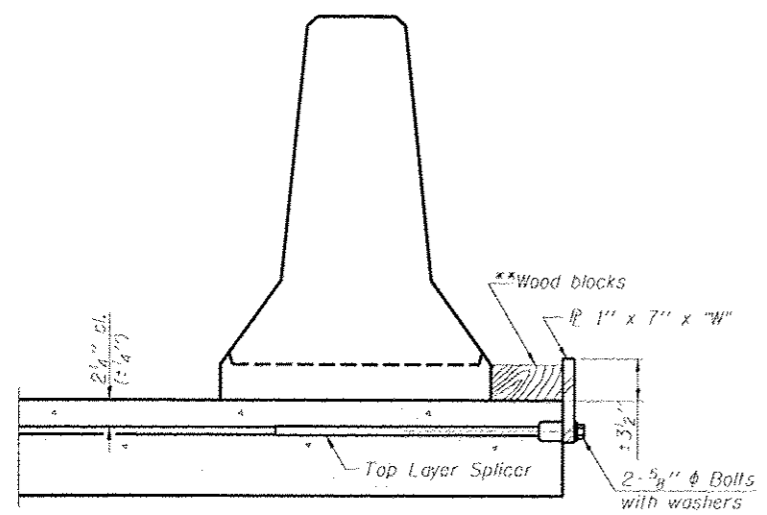
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel \bar{r} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{c} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel \bar{r} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{c} of each barrier panel.

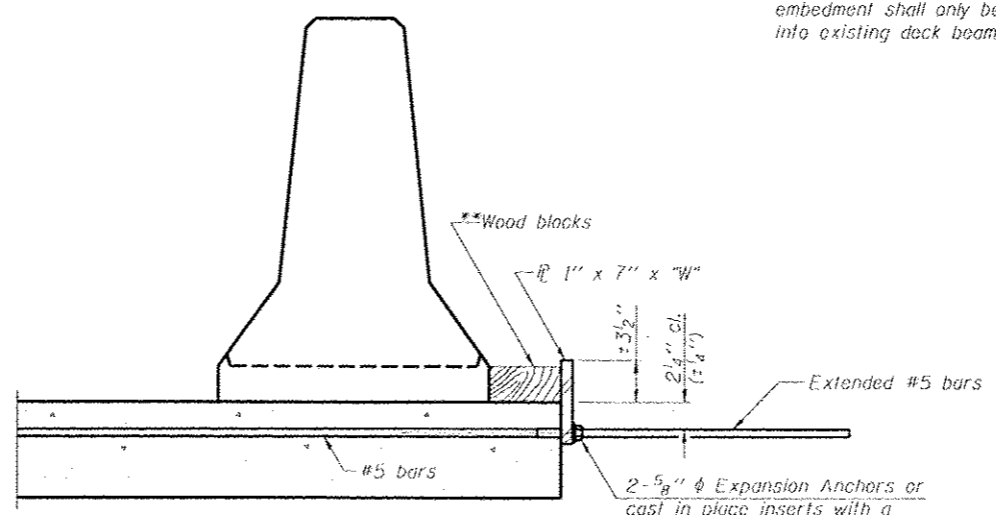
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

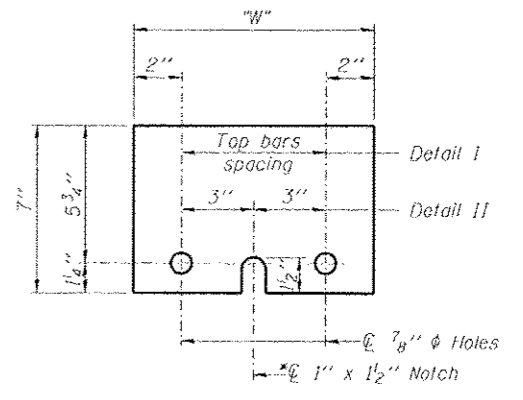
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{r} 1" x 7" x "W"

* Required only with Detail II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

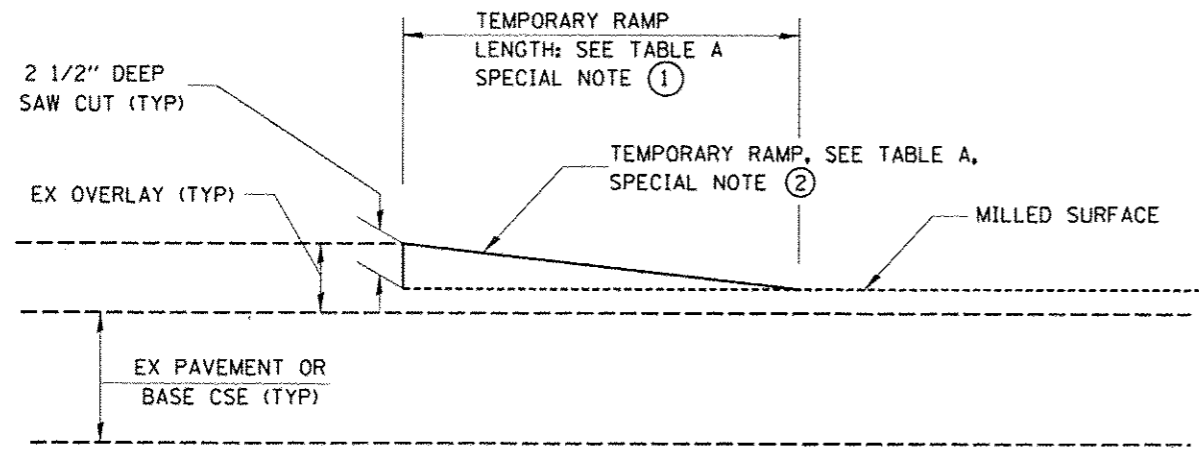
"W" = Top bars spacing + 4"

FILE NAME: 1
c:\paw\6220 - 07 rev\work order 3 - 11-21-resurfacing\2003-Structural\user\bar{r}barrier.dwg

R-27

7-1-10

CHASTAIN & ASSOCIATES LLC CONSULTING ENGINEERS 184-001397	USER NAME = abullock PLOT TIME = 4:49:06 PM PLOT SCALE = 2/8"=1'-0" PLOT DATE = 8/17/2014	DESIGNED - JMB CHECKED - ACB DRAWN - RLK CHECKED - JMB	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 058-0050	F.A.P. RTE. 320 SECTION (126X)RS-2,135RS-1 SN. 058-0050 STA.	COUNTY MACON	TOTAL SHEETS 119 SHEET NO. 111	CONTRACT NO. 74624 ILLINOIS FED. AID PROJECT
	SHEET NO. 13 OF 13 SHEETS					ILLINOIS FED. AID PROJECT			



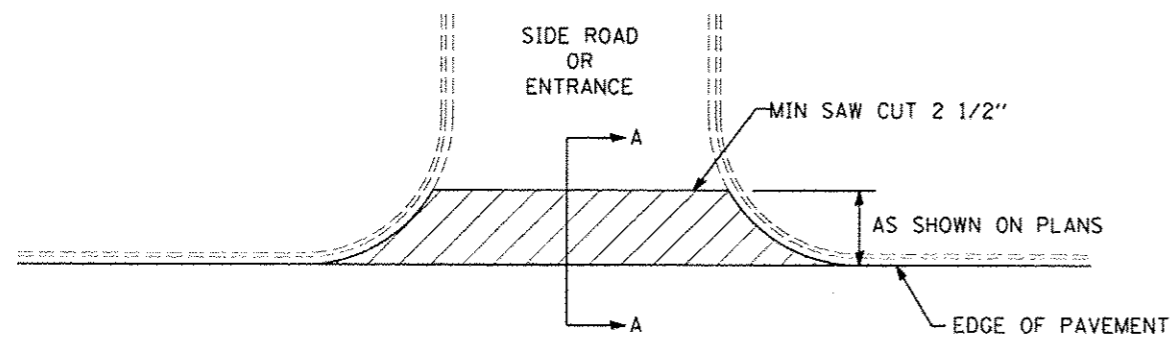
DETAIL TEMPORARY RAMP

TABLE A
(LENGTHS AND TAPER RATES)

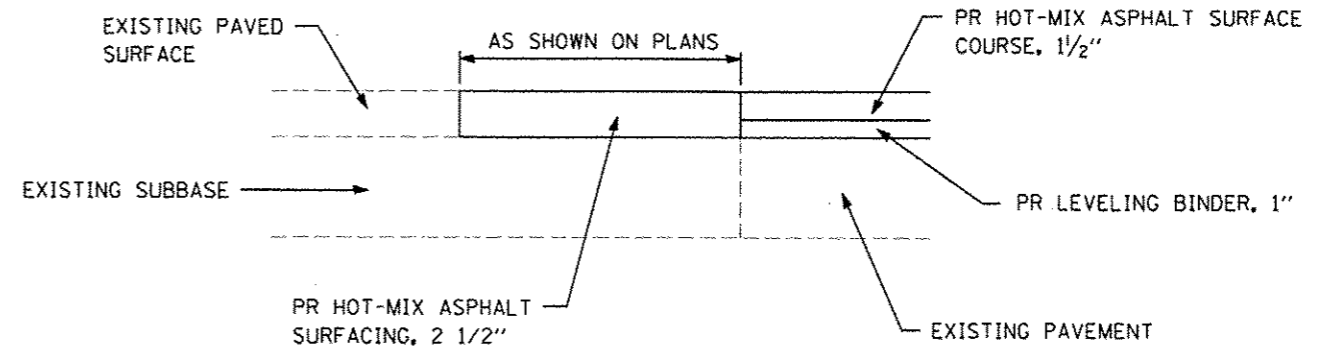
SPECIAL NOTE NUMBER	ELEMENT	RATE/LENGTH
①	TEMPORARY RAMP TAPER RATE	1:40
②	TEMPORARY RAMP LENGTH	8.33'

GENERAL NOTES

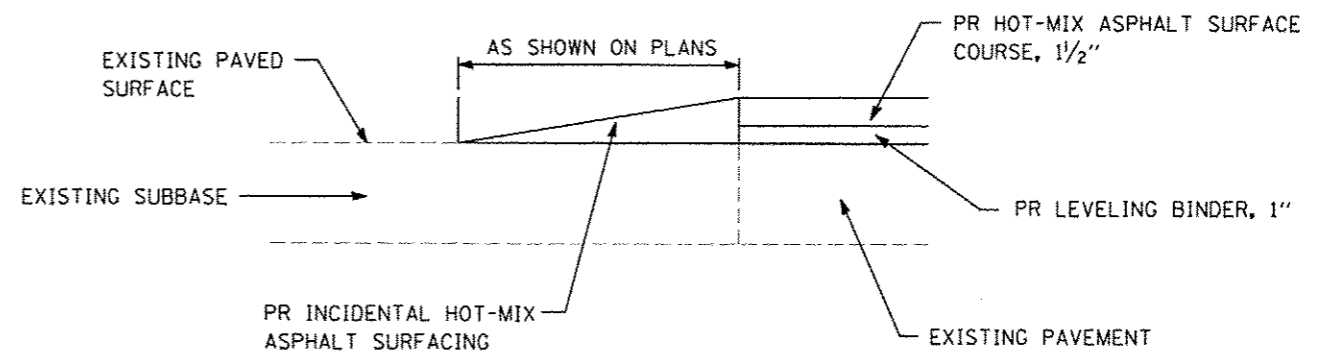
1. THIS WORK SHALL BE DONE IN ACCORDANCE WITH ARTICLE 406.08, AND THE DETAILS IN THE PLANS.
2. THE PAVEMENT SURFACE TO BE REMOVED MAY BE EITHER HOT-MIX ASPHALT OR P.C. CONCRETE. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH ARTICLE 440.04 AND THE DETAILS IN THE PLANS.
3. THE SAW CUT JOINTS SHALL BE PRIMED JUST PRIOR TO THE PLACING OF BITUMINOUS MATERIAL. THIS WORK WILL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF ARTICLE 406.05.



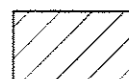
PLAN - SIDE ROAD AND ENTRANCE IMPROVEMENTS



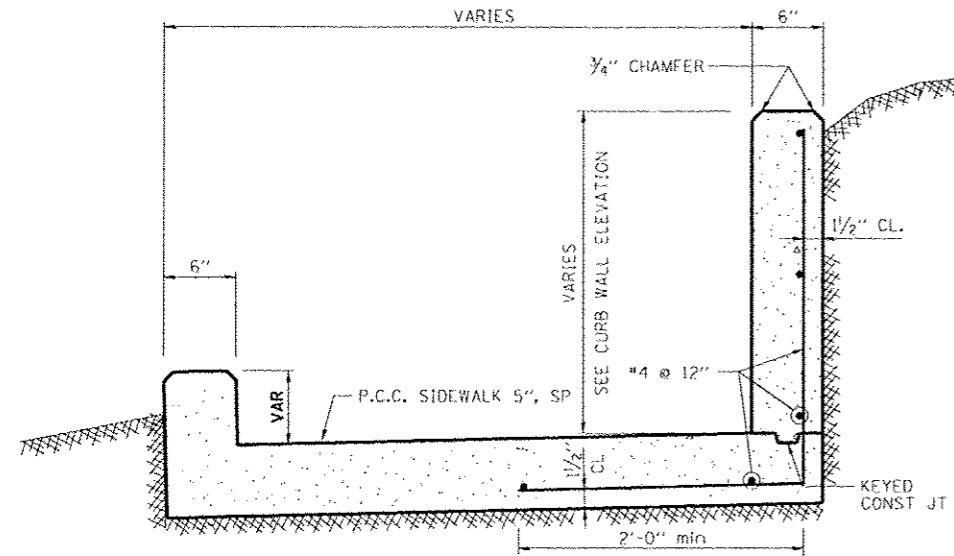
SECTION A-A (SIDE ROAD)



SECTION A-A (ENTRANCE)



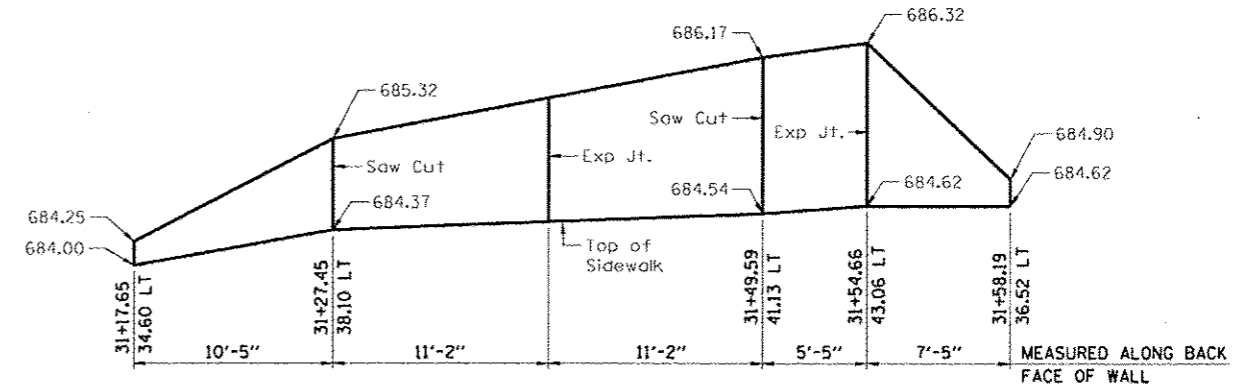
HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"



CONCRETE CURB WALL

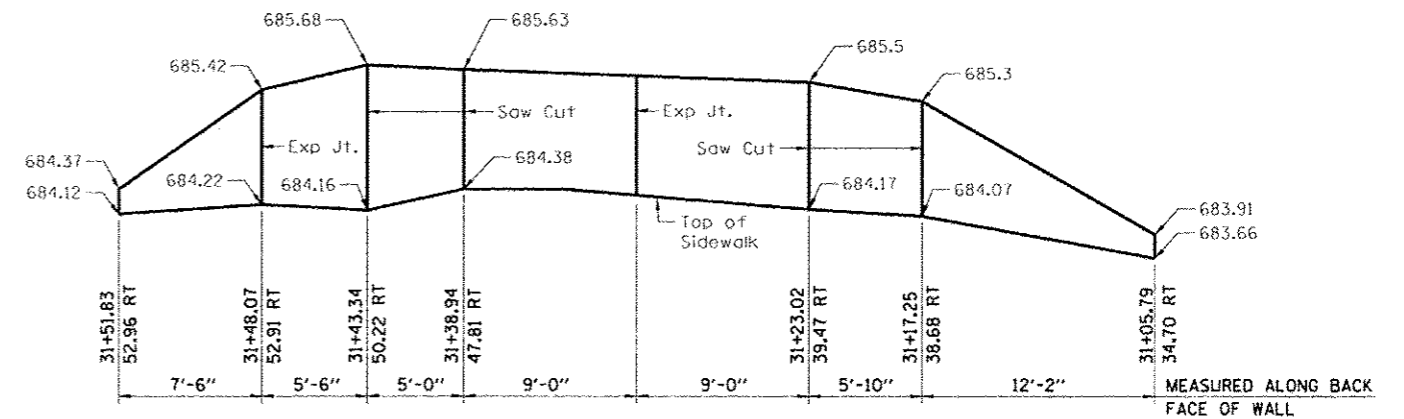
NOT TO SCALE

1. REINFORCING BARS SHALL BE EPOXY COATED AND SHALL BE FURNISHED IN CONFORMANCE WITH SECTION 508. COST TO FURNISH AND PLACE REINFORCING BARS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT OF PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL.
2. THE CURB WALL AND SIDEWALK PORTIONS OF PCC SIDEWALK 5 INCH SPECIAL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 503 (FOR WALL AND REINFORCING BAR PLACEMENT) AND SECTION 424 FOR THE SIDEWALK SLAB PORTION.
3. EXPANSION JOINTS CONSISTING OF 1/2" PJF SHALL BE USED ON THE WALLS AT THE DESIGNATED LOCATIONS ON WALL ELEVATION DETAILS. LONGITUDINAL REINFORCEMENT BARS SHALL STOP 2" PRIOR TO THE EXPANSION JOINTS.
4. CURB WALLS SHALL BE SAW CUT AT A MINIMUM DEPTH OF 1" AT THE DESIGNATION LOCATIONS ON WALL ELEVATION DETAILS.



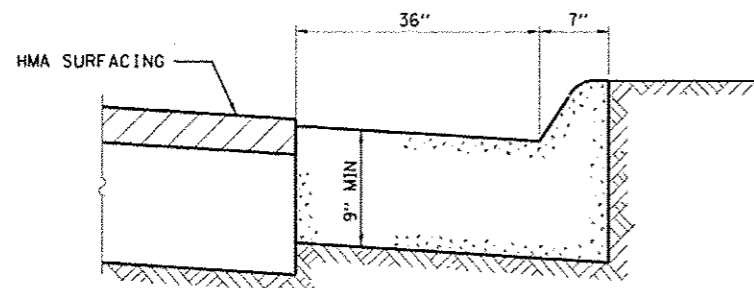
CURB WALL ELEVATION

(VICKI DR. - NW QUADRANT)



CURB WALL ELEVATION

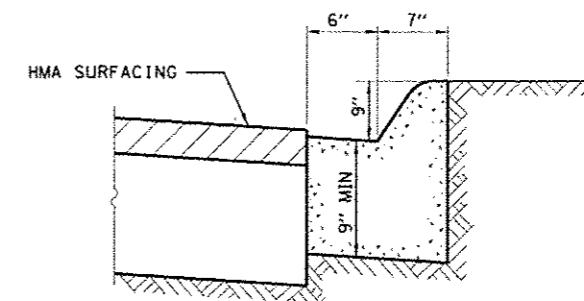
(VICKI DR. - SW QUADRANT)



COMBINATION CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)

NOT TO SCALE

1. CURB AND GUTTER SHALL CONFORM TO HIGHWAY STANDARD 606001 EXCEPT AS NOTED ABOVE.

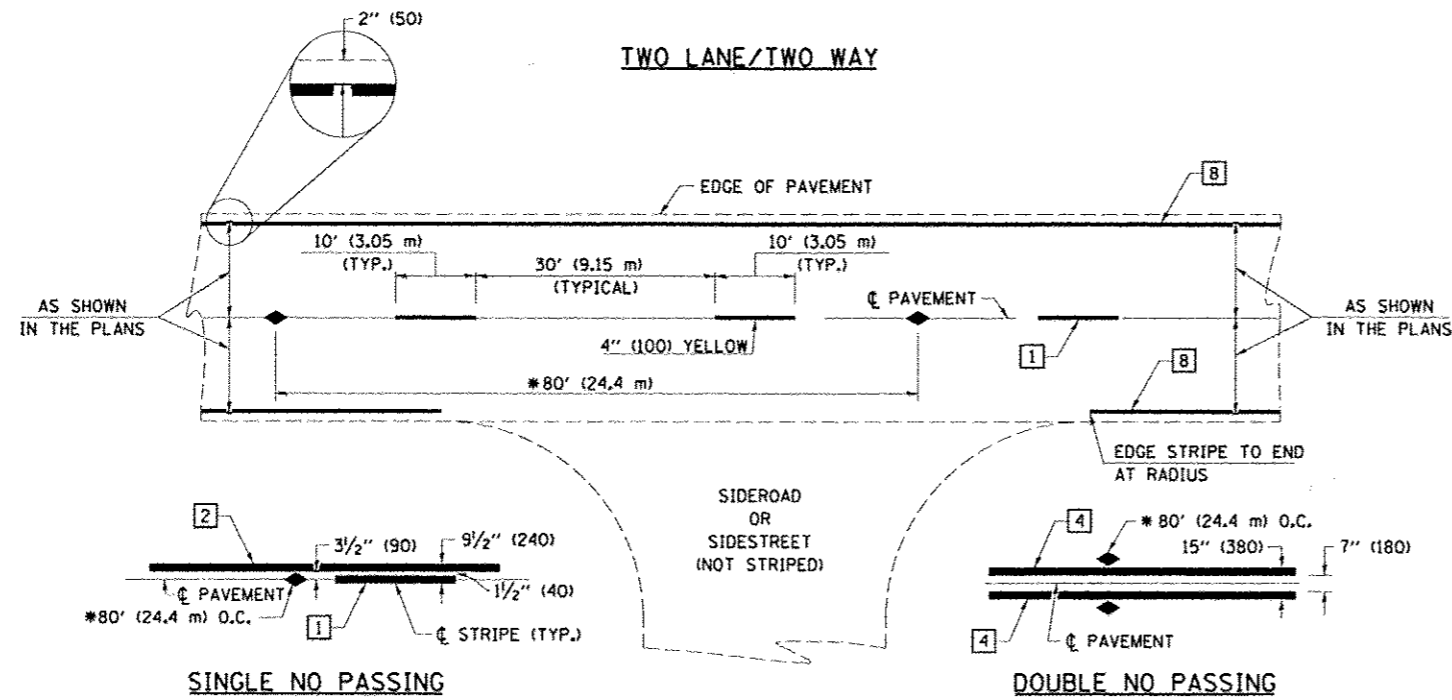


COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.06 (SPECIAL)

NOT TO SCALE

1. CURB AND GUTTER SHALL CONFORM TO HIGHWAY STANDARD 606001 EXCEPT AS NOTED ABOVE.

FILE NAME: J:\1001\6088 - 07 Var Var Work Order 3	USER NAME: j_burns	DESIGNED: CAS	REVISED:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONC. CURB WALL & COMB. CONCRETE CURB AND GUTTER (SP) DETAILS FAP 320 (IL 121/L 48)	F.A.P. RTE.:	SECTION:	COUNTY:	TOTAL SHEETS:	SHEET NO.:	
	IL 121 Resurfacing\CA000_Ext\10774624_sht.d	DRAWN: DLB	REVISED:			320	(126)RS-2,135RS-1	MACON	119	113	
	PLOT SCALE: 40:8000 1/2 in.	CHECKED: JMB	REVISED:			CONTRACT NO. 74624					
	PLOT DATE: 8/29/2014	DATE: 8/13/14	REVISED:			ILLINOIS FED. AID PROJECT					



* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

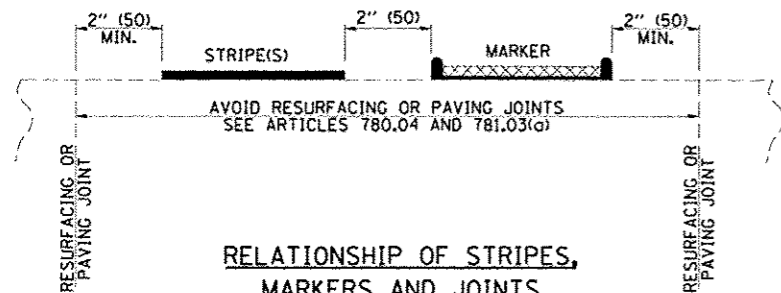
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID WHITE
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING WHITE

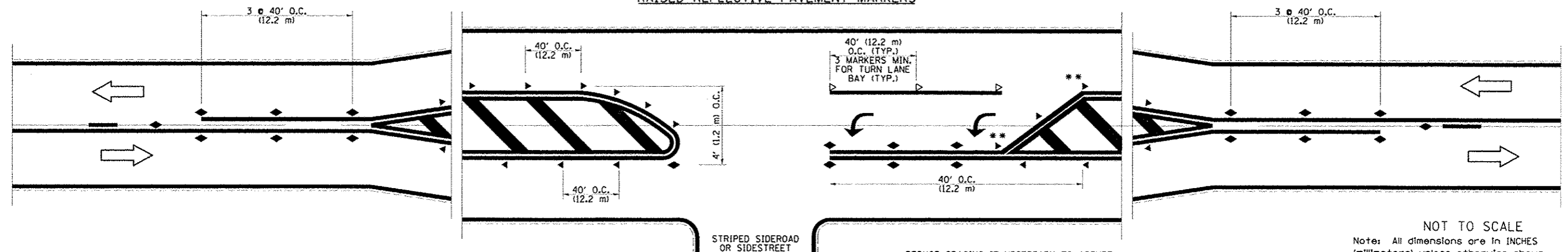
TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RELATIONSHIP OF STRIPES, MARKERS AND JOINTS



RAISED REFLECTIVE PAVEMENT MARKERS



NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)

FILE NAME: J:\dot\6828 - d7 van work order 3	USER NAME: mbullck	DESIGNED: CAS	REVISED:
PLT SCALE: 48x9000 / in.	PLT DATE: 8/19/2014	DRAWN: DLB	REVISED:
		CHECKED: JMB	REVISED:
		DATE: 8/13/14	REVISED:

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PARKING MARKING DETAILS
FAP 320 (IL 121/L 48)

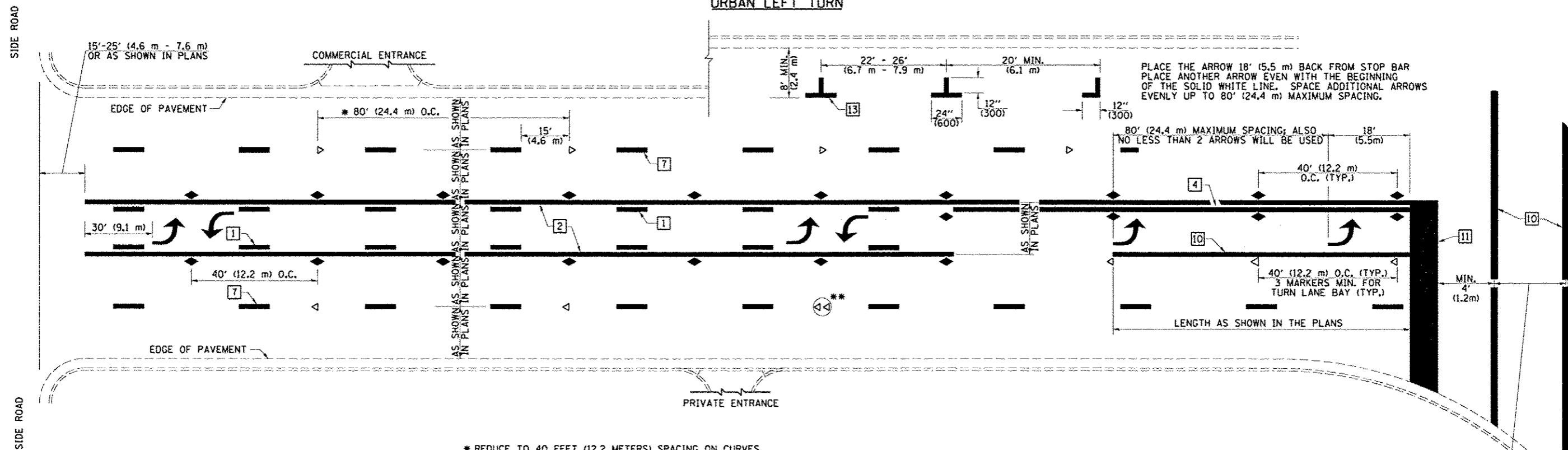
SCALE: SHEET OF SHEETS STA. TO STA.

DISTRICT 7 DETAIL NO. 7800001

F.A.P. RTE. 320	SECTION 1126X1RS-2,135RS-1	COUNTY MACON	TOTAL SHEETS 119	SHEET NO. 114
CONTRACT NO. 74624				

ILLINOIS FED. AID PROJECT

URBAN LEFT TURN

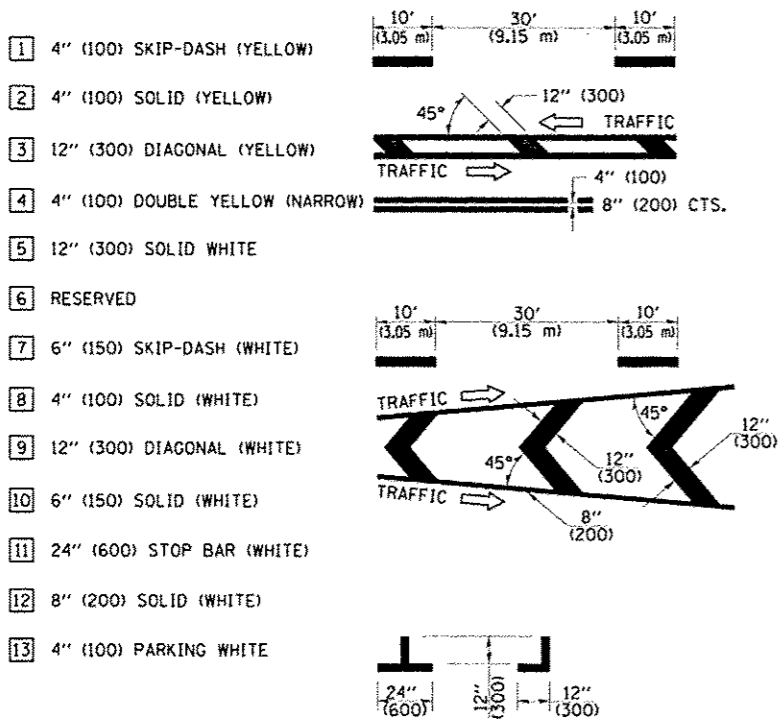


PLACE THE ARROW 18' (5.5 m) BACK FROM STOP BAR
PLACE ANOTHER ARROW EVEN WITH THE BEGINNING
OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS
EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING.

* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES
WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER
THAN POSTED SPEEDS.

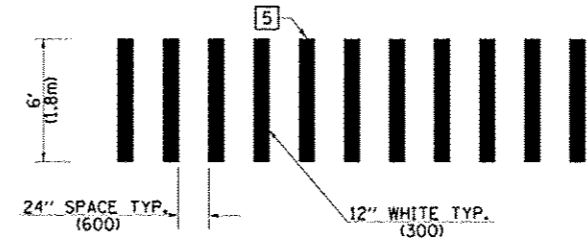
** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED
AND SPACED AS SHOWN IN HIGHWAY STANDARD
T81001 FOR MULTI-LANE DIVIDED AND UNDIVIDED
HIGHWAYS.

PAVEMENT MARKING LEGEND



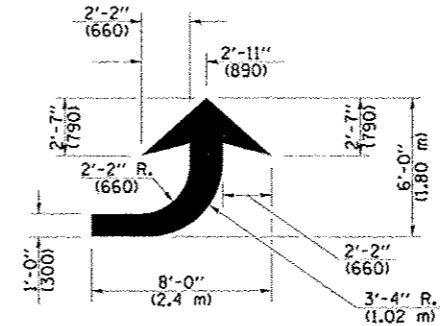
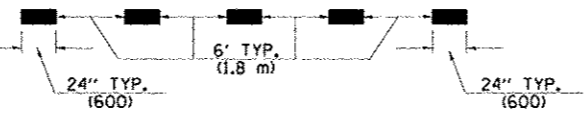
GENERAL NOTES

1. TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
2. THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
3. THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
4. USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)
5. LANE LINE EXTENSIONS SHALL BE THE SAME COLOR AND WIDTH AS THE LANE LINE BEING EXTENDED.

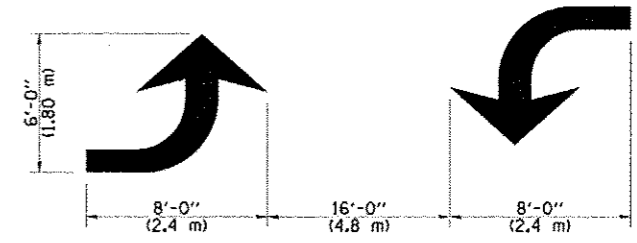


**CROSSWALK DETAIL
(DECATUR CITY LIMITS ONLY)**

LANE LINE EXTENSIONS



LEFT ARROW
REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m²)
(WHITE)



**TYPICAL DOUBLE
TURN ARROWS (WHITE)**

NOT TO SCALE

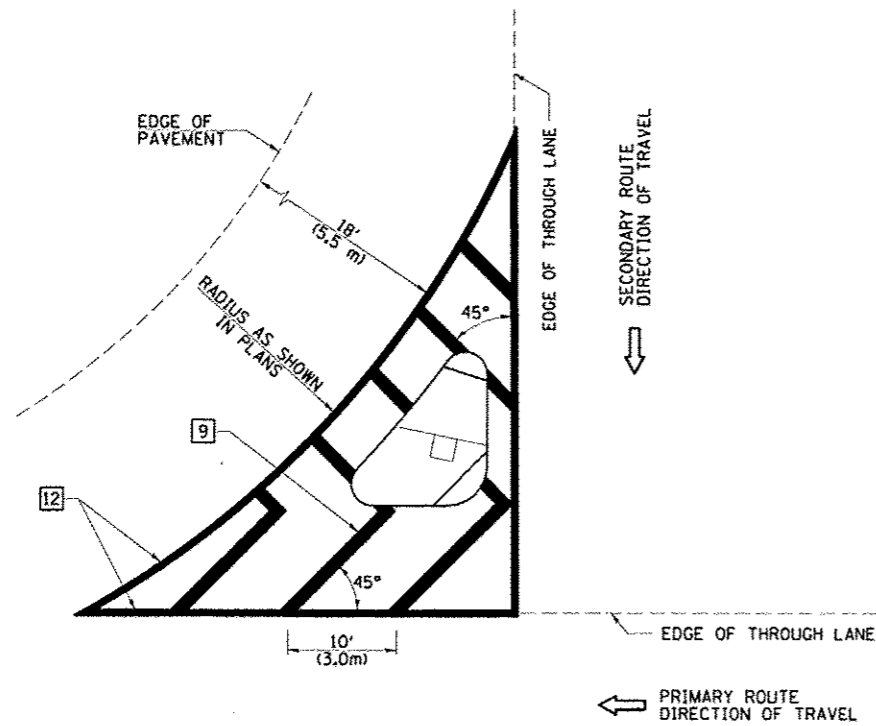
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
(RURAL & URBAN APPLICATIONS)**

DISTRICT 7 DETAIL NO. 78000001

FILE NAME: m:\dot\6288 - df var var\work order 3	USER NAME: cdbullock	DESIGNED: CAS	REVISED:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING DETAILS FAP 320 (IL 121/L 48)	F.A.P. NO. 320	SECTION (126X)RS-2,135RS-1	COUNTY MACON	TOTAL SHEETS 119	SHEET NO. 115		
PLT SCALE: 1/8" = 1'-0"	PLT DATE: 8/13/2014	DRAWN: DLB	REVISED:			SCALE:	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 74624			
CHECKED: JMB	DATE: 8/13/14	REVISED:	REVISED:			ILLINOIS FED. AID PROJECT						
PLT DATE: 8/13/2014	DATE: 8/13/14	REVISED:	REVISED:									

ISLAND

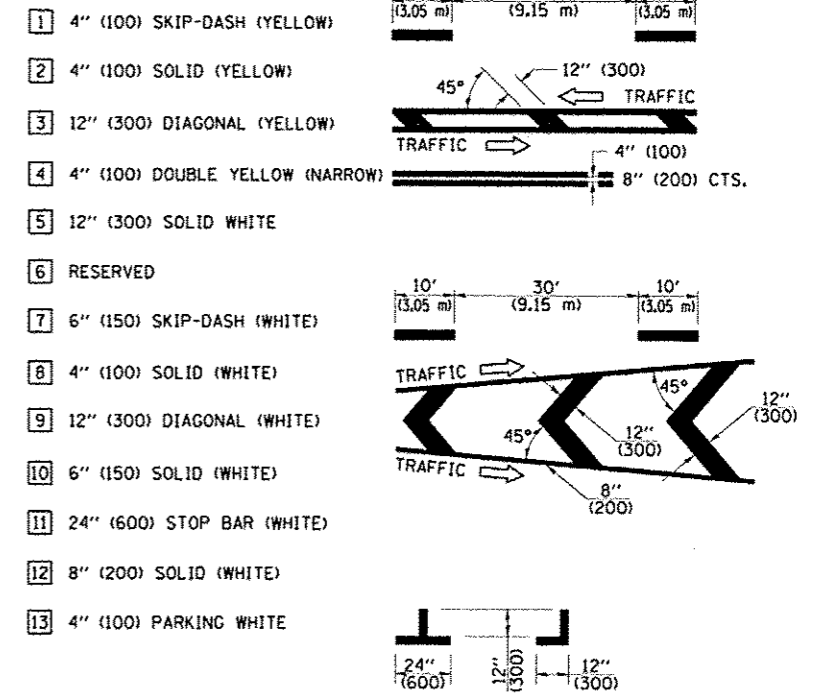


GENERAL NOTES

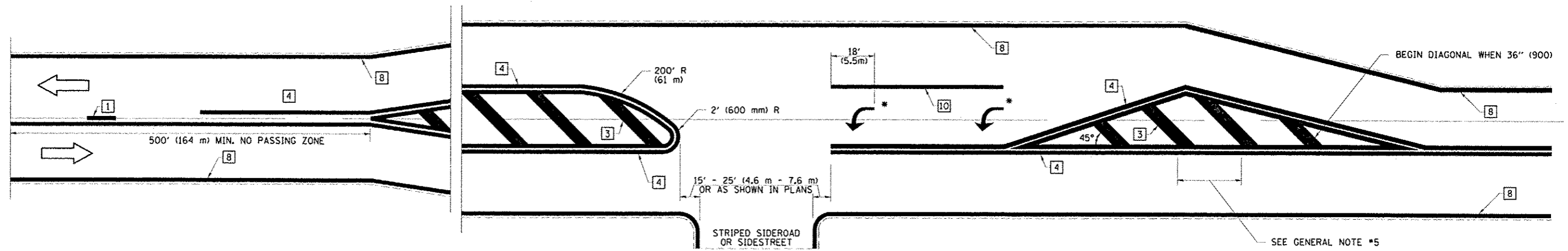
1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

< 30 MPH (< 50 km/h)	15' (4.5 m)
30-45 MPH (50-75 km/h)	20' (6.0 m)
> 45 MPH (> 75 km/h)	30' (9.0 m)

PAVEMENT MARKING LEGEND



RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)

DISTRICT 7 DETAIL NO. 78000001

FILE NAME : d:\dot\NR66 - d7 var\work order 3	USER NAME : cballack	DESIGNED - CAS	REVISED -
PL01 SCALE : 48.00000 1/2 in.	PL01 DATE : 8/13/2014	DRAWN - DLB	REVISED -
		CHECKED - JMB	REVISED -
		DATE - 8/13/14	REVISED -

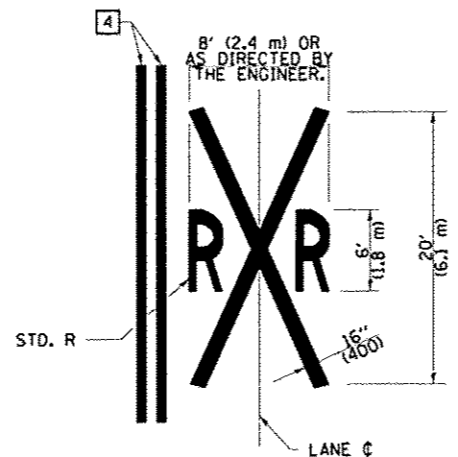
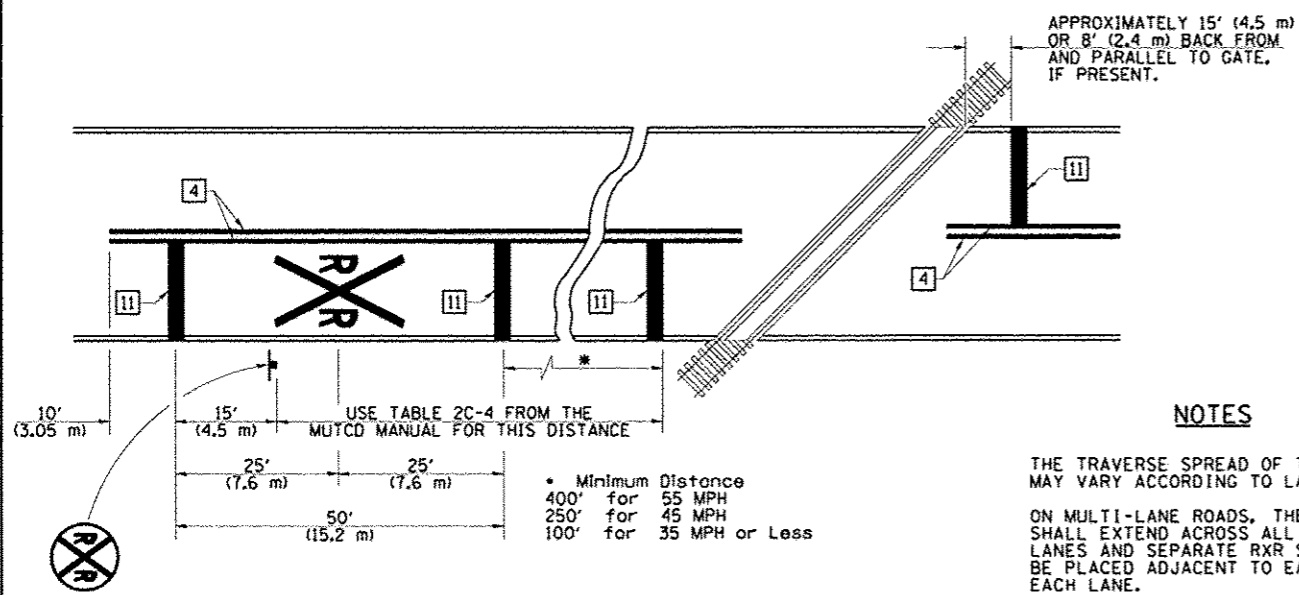
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING DETAILS
FAP 320 (IL 121/L 48)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
320	(126X)RS-2,135RS-1	MACON	119	116
CONTRACT NO. 74624				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING



NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

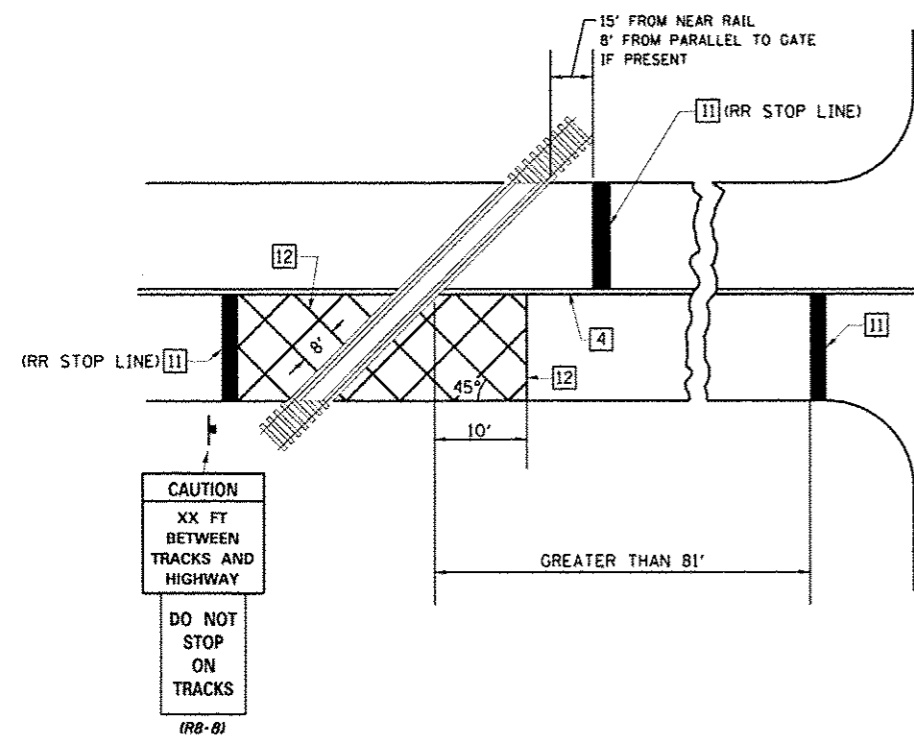
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

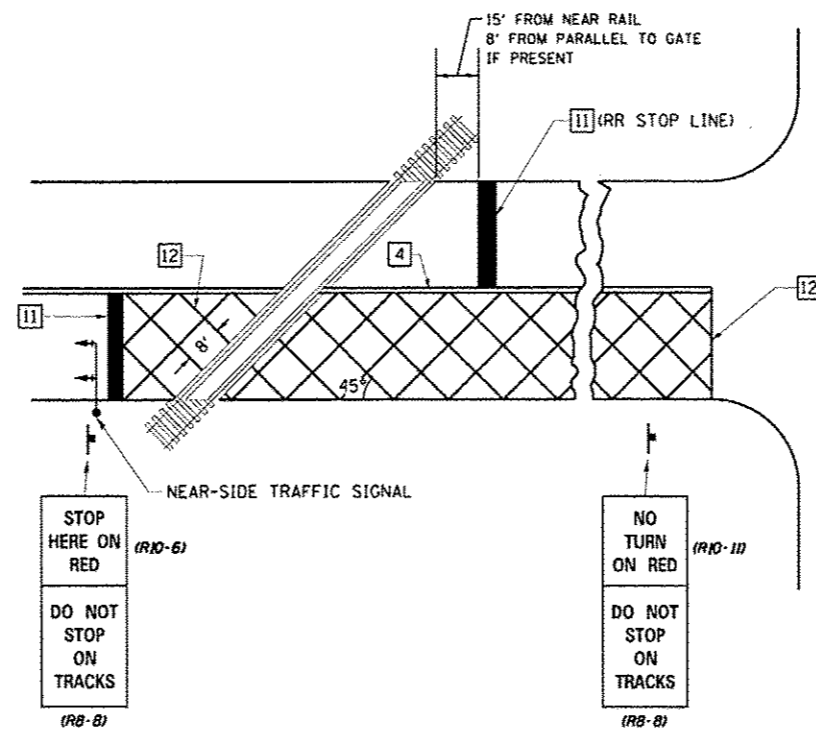
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID WHITE
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING WHITE

RAILROAD CROSSING WITH INTERCONNECT ONLY



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

1. SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.

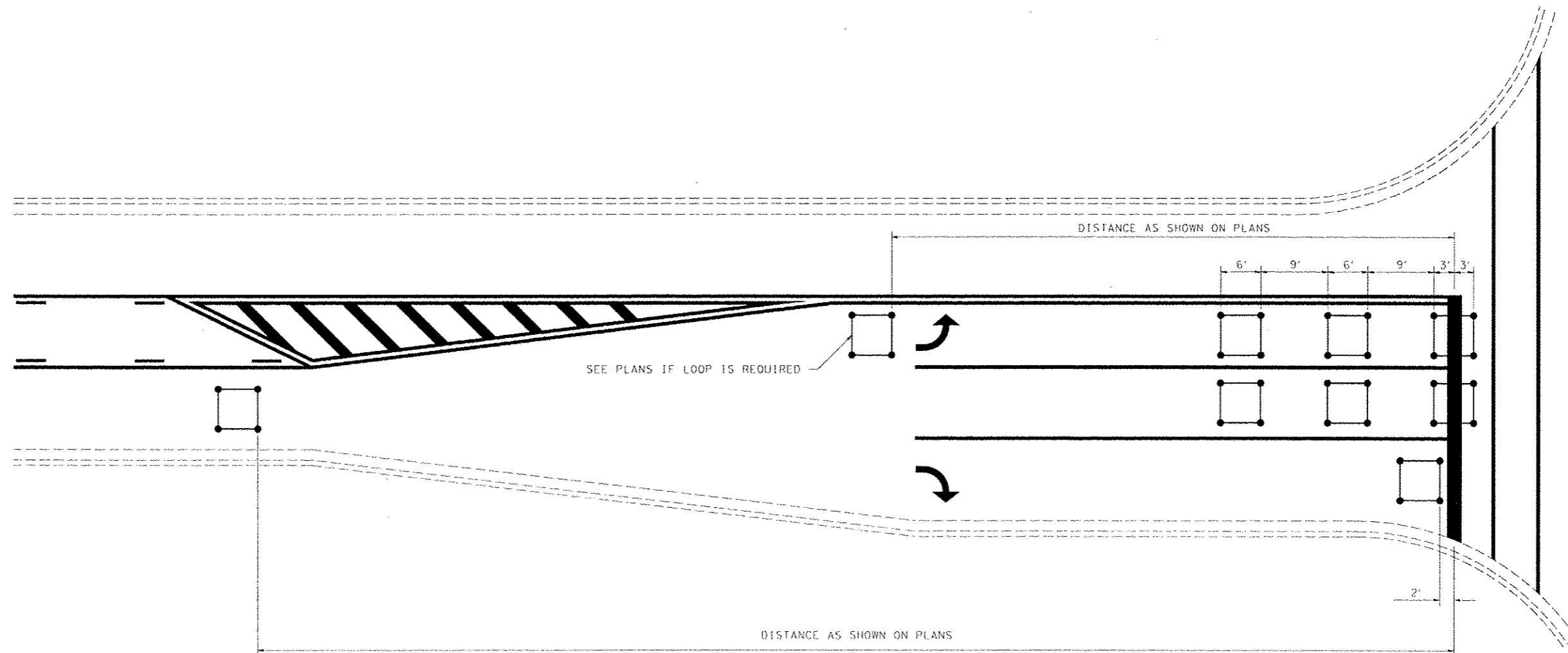
SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)

NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME: d7 ver work order 3	USER NAME: jdblock	DESIGNED: CAS	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING DETAILS FAP 320 (IL 121/L 48)			
	PL01 SCALE = 48.0000 1/8" = 1'	DRAWN: DLB	REVISED: -		SCALE:	SHEET	OF SHEETS	STA. TO STA.
	PL01 DATE = 8/14/2014	CHECKED: JMB	REVISED: -		DISTRICT 7 DETAIL NO. 7800001			
		DATE: 8/13/14	REVISED: -		F.A.P. RTE. 320	SECTION (126XJRS-2,135RS-1)	COUNTY MACON	TOTAL SHEETS 119

TYPICAL APPLICATION OF DETECTOR LOOPS



NOTE:

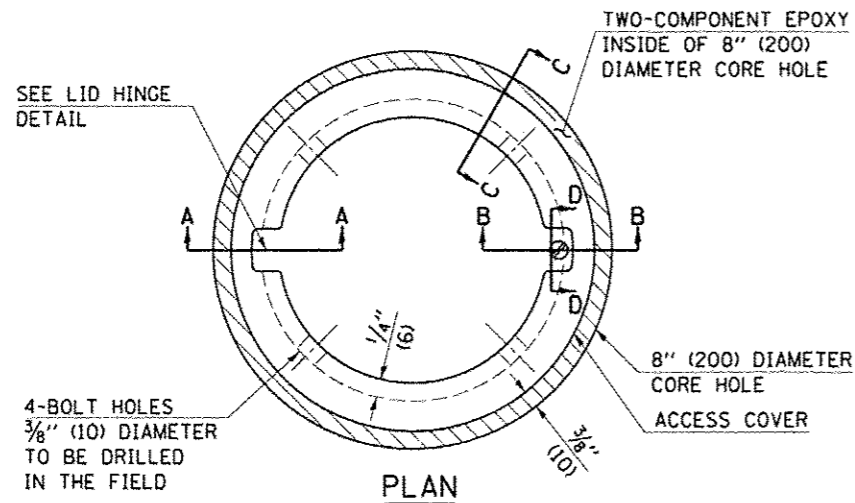
1. EACH LOOP SHALL HAVE ITS OWN HOMERUN TO THE GULFBOX/HANDHOLE.
2. LOOPS SHOULD BE CENTERED IN EACH LANE.
3. USE 6' BY 6' LOOPS FOR LANE WIDTHS OF 11' OR GREATER.
4. USE 5' BY 5' LOOPS IF LANE WIDTH IS LESS THAN 11'.

NOT TO SCALE

TYPICAL APPLICATIONS OF DETECTOR LOOPS

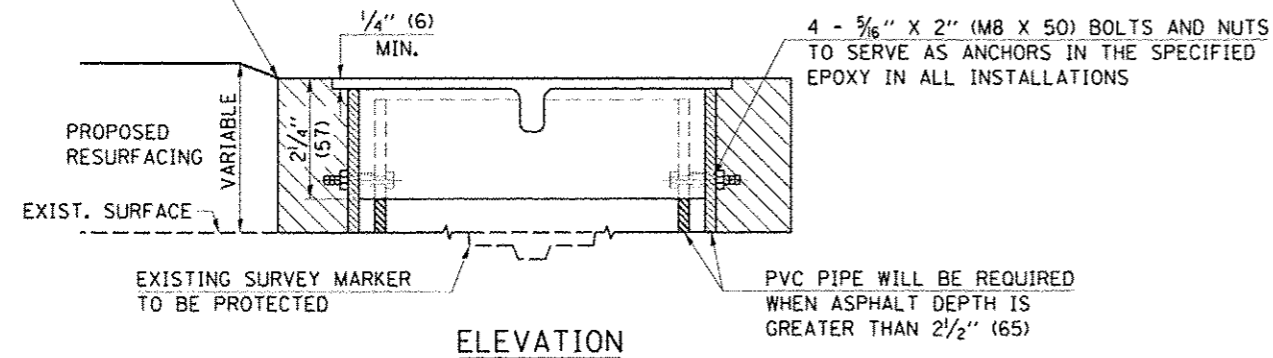
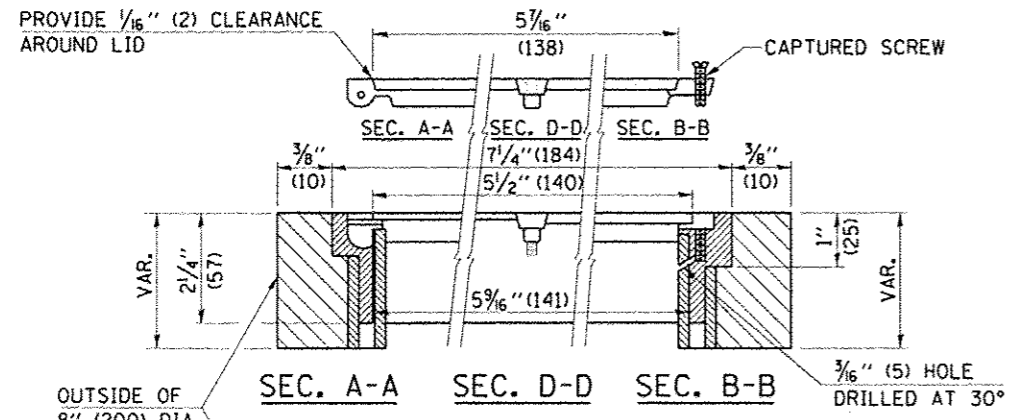
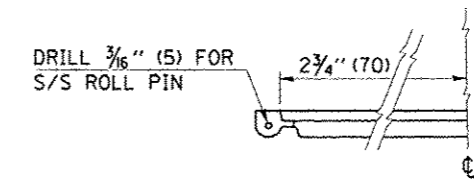
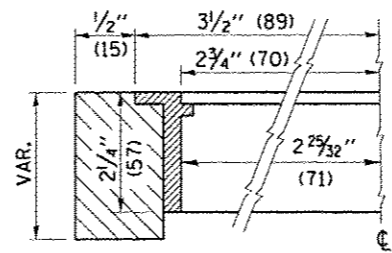
DISTRICT 7 DETAIL NO. 88600100

FILE NAME : J:\Vint\6288 - 47 var var\work order 3	USCH NAME : dbnlloop	DESIGNED - CAS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP DETAILS FAP 320 (IL 121/L 48)	F.A.P. RTE. 320	SECTION 1126XRS-2,135RS-1	COUNTY MACON	TOTAL SHEETS 119	SHEET NO. 118
	PLOT SCALE : 48,0000 1/4 in.	CHECKED - JMB	REVISED -			SCALE:	SHEET OF SHEETS STA. TO STA.	CONTRACT NO. 74624		ILLINOIS FED. AID PROJECT
PLOT DATE : 8/19/2014	DATE : 8/13/14	REVISED -	REVISED -							

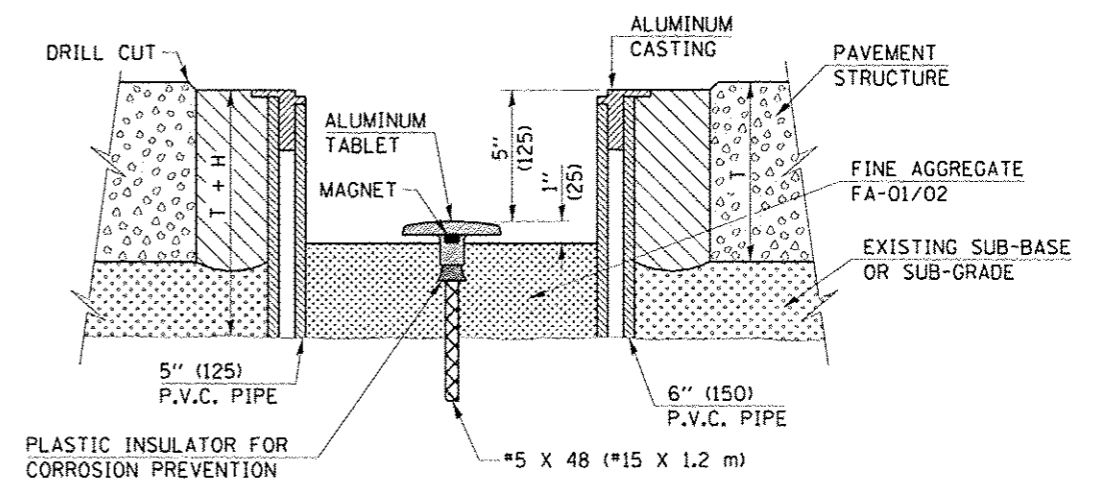


- LEGEND**
- ALUMINUM CASTING
 - 5" (125) OR 6" (150) P.V.C. PIPE
 - TWO-COMPONENT EPOXY
- T = THICKNESS OF PAVEMENT STRUCTURE
- H = THE THICKNESS OF THE SUB-BASE GRANULAR + 1" (25)

BILL OF MATERIAL	
ALUMINUM CASTING OF THE DIMENSIONS AND SPECIFICATIONS SHOWN OR OTHER SUBJECT TO ENGINEER'S APPROVAL OF SHOP DRAWINGS,	
4 EACH - 5/16" X 2" (M8 X 50) BOLTS WITH NUTS, EPOXY,	
5" OR 6" (125 mm OR 150 mm) DIAMETER P.V.C. PIPE, SCHEDULE 40 (WHEN REQUIRED).	



EXISTING SURVEY MARKER



PROPOSED SURVEY MARKER

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. Z0070202

FILE NAME : d:\data\6220 - d7 var var\work order 3	USER NAME : abullock	DESIGNED - CAS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SURVEY VAULT DETAILS FAP 320 (IL 121/L 48)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLT SCALE : 48.0000 "/td> <td>PLT DATE : 8/13/2014</td> <td>CHECKED - JMB</td> <td>REVISED -</td> <td>320</td> <td>(126X)RS-2.135RS-1</td> <td>MACON</td> <td>119</td> <td>119</td>	PLT DATE : 8/13/2014	CHECKED - JMB	REVISED -			320	(126X)RS-2.135RS-1	MACON	119	119	
DATE - 8/13/14	DATE - 8/13/14	DATE - 8/13/14	DATE - 8/13/14			CONTRACT NO. 74624		ILLINOIS FED. AID PROJECT			
SCALE:	SHEET	OF	SHEETS			STA.	TO	STA.			