

03-06-2015 LETTING ITEM 139

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

# PROPOSED HIGHWAY PLANS

FAP 331 (IL 13)  
SECTION D9 CM PAVEMENT REPAIR 2015-2

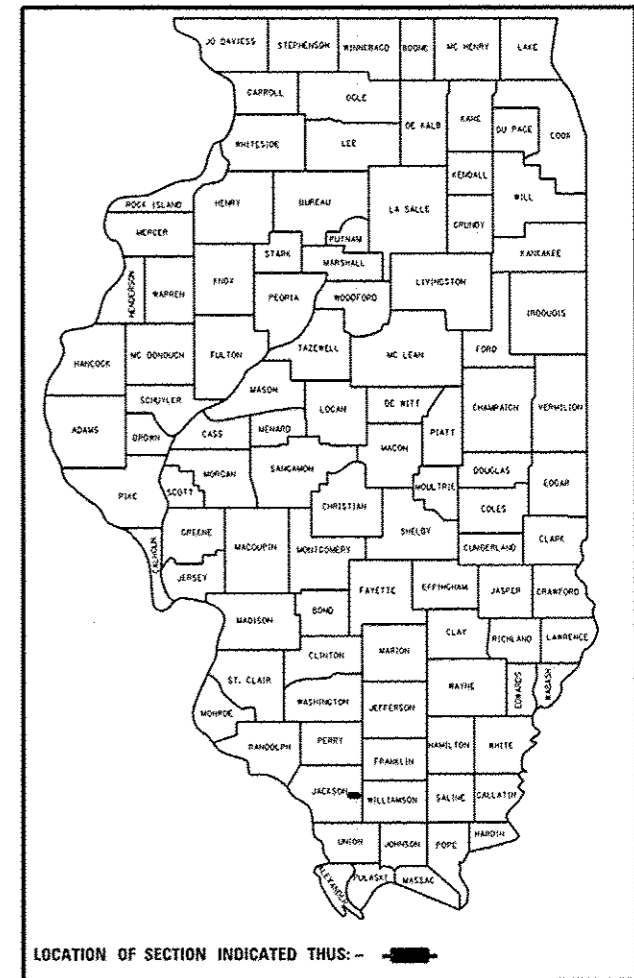
PATCHING & PAVEMENT REPLACEMENT  
JACKSON COUNTY

C-99-049-14

F.A.E. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331		JACKSON	23	1
		ILLINOIS	CONTRACT NO. 78421	

\*D9 CM PAVEMENT REPAIR 2015-2

D-99-049-14



FOR SIGNATURE SHEET, SEE SHEET NO. 2  
FOR INDEX OF SHEETS, SEE SHEET NO. 3  
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 5

OMISSIONS: NONE

EQUATION STATIONS: NONE

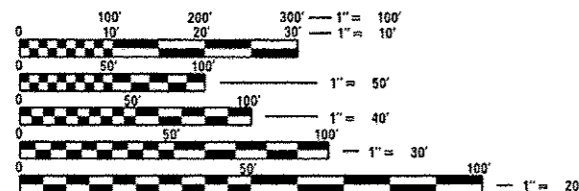
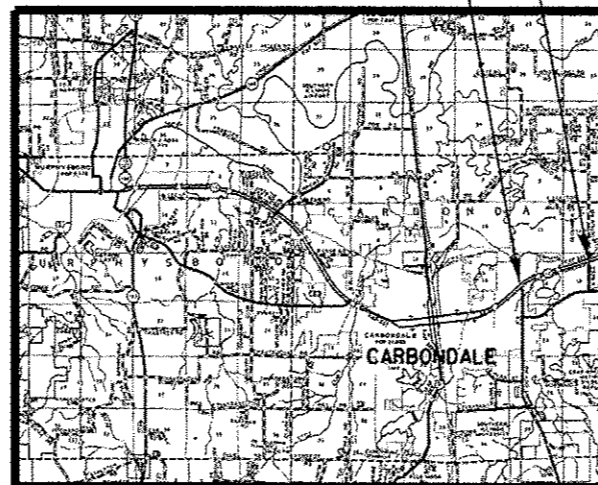
TRAFFIC DATA:

LOC #1, REED STATION: YEAR 2014  
EB ADT = 15,380 TRUCKS = 7.2%

LOC #2, GIANT CITY: YEAR 2014  
EB ADT = 12,080 TRUCKS = 8.4%

LOCATION #1  
IL 13 EASTBOUND @  
REED STATION ROAD

LOCATION #2  
IL 13 EASTBOUND @  
GIANT CITY ROAD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DARRYL LEFTWICH (618) 351-5291  
PROJECT MANAGER: CHARLES STEIN (618) 351-5210

CONTRACT NO. 78421

LOCATION 1: GROSS LENGTH = 400.00 FT. = 0.076 MILE  
LOCATION 2: GROSS LENGTH = 334.00 FT. = 0.063 MILE  
NET LENGTH = 734.00 FT. = 0.139 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED Dec 15 20 14

Jeffrey S. Keenan  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Jan 30 20 15  
John D. Baranzelli, PE  
ENGINEER OF DESIGN AND ENVIRONMENT

Jan 30 20 15  
Omer Osman, PE  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

Prepared By: *Joe Zdaniewicz*  
DISTRICT STUDIES & PLANS ENGINEER

Examined By: *[Signature]*  
DISTRICT LAND ACQUISITION ENGINEER

Examined By: *Carrie Nelson*  
DISTRICT PROGRAM DEVELOPMENT ENGINEER

Examined By: *Keel Nely*  
DISTRICT OPERATIONS ENGINEER

Examined By: *[Signature]*  
DISTRICT PROJECT IMPLEMENTATION ENGINEER

Examined By: \_\_\_\_\_  
DISTRICT CONSTRUCTION ENGINEER

Examined By: *[Signature]*  
DISTRICT MATERIALS ENGINEER

FILE NAME	USER NAME * USER *	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SIGNATURE SHEET			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
...	...	DRAWN -	REVISED -		331	*	JACKSON	23	2			
...	...	CHECKED -	REVISED -		CONTRACT NO. 78421			ILLINOIS FED. AID PROJECT				
...	...	DATE	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	*09 CM PAVEMENT REPAIR 2015-2	

# INDEX OF SHEETS

SHEET #	DESCRIPTION
1	COVER SHEET
2	SIGNATURE SHEET
3	INDEX OF SHEETS & STANDARDS
4	GENERAL NOTES
4	MIXTURE REQUIREMENTS
5-6	SUMMARY OF QUANTITIES
7	TYPICAL SECTIONS: REED STATION ROAD
8	TYPICAL SECTIONS: GIANT CITY ROAD
	SCHEDULES
9	PAVEMENT & SHOULDERS
10	PAVEMENT MARKING & MISC
	PLAN SHEET - REED STATION ROAD
11	PLAN SHEET - REED STATION ROAD
12	PLAN SHEET - GIANT CITY ROAD
	STAGING PLANS
13-15	REED STATION ROAD - PLAN SHEETS
16-18	REED STATION ROAD - TYPICALS
19-20	GIANT CITY ROAD - PLAN SHEET
21-22	GIANT CITY ROAD - TYPICALS
23	DROP-OFF PROTECTION POLICY

# HIGHWAY STANDARDS

000001-06	Standard Symbols Abbreviations & Patterns
420001-08	Pavement Joints
420101-05	24ft Jointed PCC Pvmt
442001-04	Class A Patches
442101-07	Class B Patches
442201-03	Class C & D Patches
701101-04	Off Road Op-Multi Ln-Less Than 15ft To EOP
701106-02	Off Road Op-Multi Ln-More Than 15ft Away
701427-03	Ln Closure Multi Ln-Intermittant Or Moving Op LT or EQ 40 MPH
701701-09	Urban Ln Closure Multi Ln Intersection
701901-04	Traf Cntrl Devices
780001-05	Typical Pvmt Mrkings
781001-03	Typical App Raised Reflc Pvmt Mrkrs

FILE NAME #	USER NAME # USER	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS &amp; STANDARDS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
or\p\work\p\widet\ref\std\1\23\98565\7\421-ah-1-plan.dgn		DRAWN -	REVISED -			331	*	JACKSON	23	3	
#MODELNAME#	PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED -			CONTRACT NO. 78421					
	PLOT DATE = 12/11/2014	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO

# GENERAL NOTES

THE QUANTITY OF CLASS C PATCHING SHOWN IN THE PLANS IS TO ADDRESS ISSUES AT THE REED'S STATION ROAD INTERSECTION. THE ACTUAL LOCATION AND QUANTITY OF PATCHING SHALL BE DETERMINED BY THE ENGINEER. ALL PATCHING SHALL BE COMPLETED BEFORE COMMENCING ON THE STAGED PAVEMENT REPLACEMENT. ANY PATCHING ADJACENT TO PAVEMENT REPLACEMENT REQUIRES TIE BARS. TIE BARS SHALL BE INCLUDED IN THE CONSTRUCTION OF THE CLASS C PATCHING

ALL PAVEMENT MARKINGS SHALL BE LOCATED AND RECORDED BEFORE PAVEMENT REMOVAL AND REPLACED ACCORDINGLY.

THE DISTRICT BUREAU OF OPERATIONS SHALL BE NOTIFIED AT LEAST 10 DAYS PRIOR TO THE PLACEMENT OF THE FINAL PAVEMENT MARKINGS. THE BUREAU OF OPERATIONS WILL THEN INSPECT AND/OR APPROVE THE FINAL LAYOUT

FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

ALL HOT MIX ASPHALT	2.016 TONS/CU. YD.
ALL AGGREGATE	2.05 TONS/CU. YD.

SUBBASE COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER

CONCRETE FORMS SHALL BE OF METAL ONLY, EXCEPT THAT WOOD FORMS MAY BE USED ON SHORT RADIUS CURVES

PROTECTIVE COAT SHALL BE APPLIED TO ALL CONCRETE SURFACES ACCORDING TO THE SEASONAL REQUIREMENTS OF ARTICLE 420.18

THE CONTRACTOR SHALL REPLACE STAMPED STATIONING THAT FALLS WITHIN THE REPAIR LIMITS. THE STAMPED STATIONING SHALL BE PLACED AT 300 FT INTERVALS OF THE PAVEMENT AND AS DIRECTED BY THE ENGINEER. IF STATIONING FALLS WITHIN THE LIMITS OF MULTIPLE LANES, THE STATIONING SHALL BE STAMPED ON BOTH SIDES OF THE PAVEMENT. THE STATION SYMBOL STAMPS USED SHALL BE FURNISHED BY THE CONTRACTOR. THEY SHALL BE 5 1/2" IN TALL, OF A DESIGN APPROVED BY THE ENGINEER, AND SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

THE LOCATION OF DETECTOR LOOPS, AS SHOWN ON THE PLANS, MAY BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER OF TRAFFIC OPERATIONS.

DETECTOR LOOP WIRE SHALL BE PLACED IN THE EXISTING CONDUIT FROM THE LOOP SAWCUT TO THE SPLICE POINT AT THE EXISTING HANDHOLE.

TRAVERSE EXPANSION JOINTS (FOR PAVEMENTS WITH UNEQUAL THICKNESS) SHALL BE CONSTRUCTED AT ALL LOCATIONS WHERE TYING TO EXISTING CRC PAVEMENT. THE ACTUAL TIE TO EXISTING PAVEMENT SHALL BE ACCOMPLISHED USING EITHER A CLASS A "STYLE" PATCH OR CLASS B "STYLE" PATCH AT THE DIRECTION OF THE ENGINEER. THE PATCH SHALL USE #6 EPOXY COATED TIE BARS AND SHALL BE ANCHORED INTO THE EXISTING PAVEMENT WITH EPOXY BONDING COMPOUND. THE COST OF THESE EXPANSION JOINTS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10" (SEE STANDARD 420001 FOR JOINT DETAIL)

COMMITMENTS: NONE  
STRUCTURES WITHIN PROJECT LIMITS: NONE

## MIXTURE REQUIREMENTS

Locations	HMA Shoulders
Mixture Use(s):	Hot-Mix Asphalt Binder Course, N70, IL-19.0 mm
AC/PG:	PG64-22
ABR % (Max):	See Special Provision
Design Air Voids:	4.0 %, 70 Gyration Design
Mixture Composition: (Gradation Mixture)	IL-19.0 mm
Friction Aggregate:	None
Quality Management Program:	OCQA

FILE NAME :	USER NAME : #USER#	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES &amp; MIXTURE REQUIREMENTS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
dr\p\work\pav\doc\1\ef\wchd\1\0998565\78421-shc\plan.dgn	421-shc\plan.dgn	DRAWN -	REVISED -			331	*	JACKSON	23	4	
PLOT SCALE = 1/8"=1'-0"		CHECKED -	REVISED -			CONTRACT NO. 78421					
PLOT DATE = 12/11/2014		DATE -	REVISED -			ILLINOIS FED. AID PROJECT *09 CM PAVEMENT REPAIR 2015-2					

# SUMMARY OF QUANTITIES

JACKSON (077)  
 FAP 331, IL 13  
 100% STATE  
 URBAN  
 ROADWAY  
 0005

CODE NUMBER	ITEM DESCRIPTION	UNIT	QUANTITY
31100700	SUBBASE GRANULAR MATERIAL, TYPE A 8"	SQ YD	2,275
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	22
42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	SQ YD	2,275
42101300	PROTECTIVE COAT	SQ YD	2,339
44000100	PAVEMENT REMOVAL	SQ YD	2,275
44004250	PAVED SHOULDER REMOVAL	SQ YD	98
44201353	CLASS C PATCHES, TYPE II, 10 INCH	SQ YD	24
44201357	CLASS C PATCHES, TYPE III, 10 INCH	SQ YD	40
44213200	SAW CUTS	FOOT	576
44213204	TIE BARS 3/4"	EACH	788
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	98
67100100	MOBILIZATION	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	10

# SUMMARY OF QUANTITIES - CONT

JACKSON (077)  
 FAP 331, IL 13  
 100% STATE  
 URBAN  
 ROADWAY  
 0005

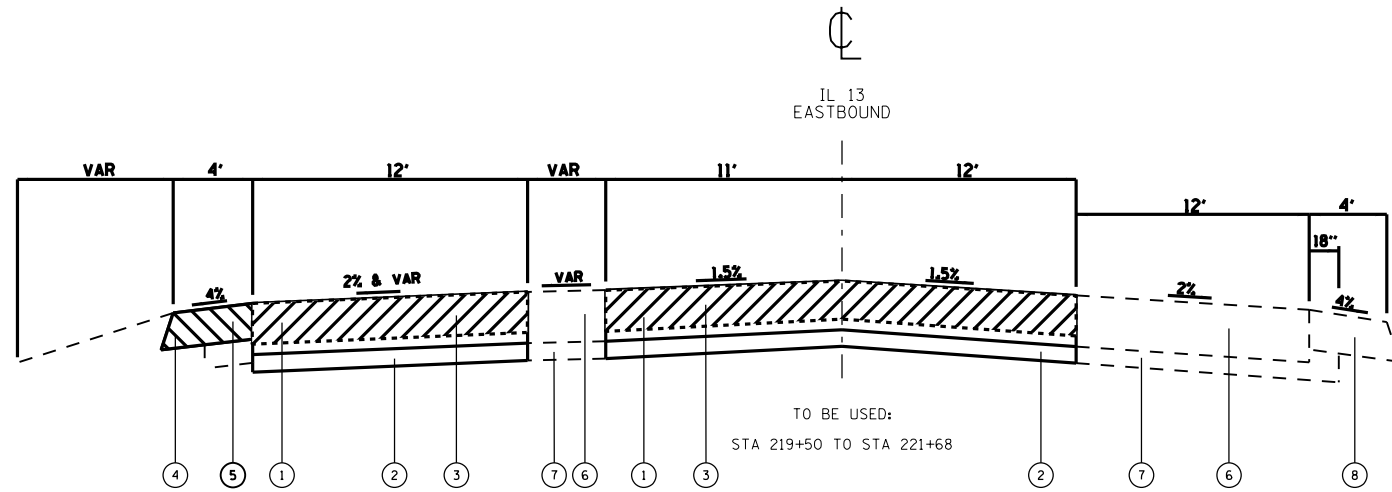
CODE NUMBER	ITEM DESCRIPTION	UNIT	QUANTITY
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	73
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	1,519
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	60
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	651
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,519
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	60
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	45
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	25
* 88600100	DETECTOR LOOP, TYPE I	FOOT	350
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	1,519
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	60

26

\* SPECIALTY ITEM

# TYPICAL SECTION

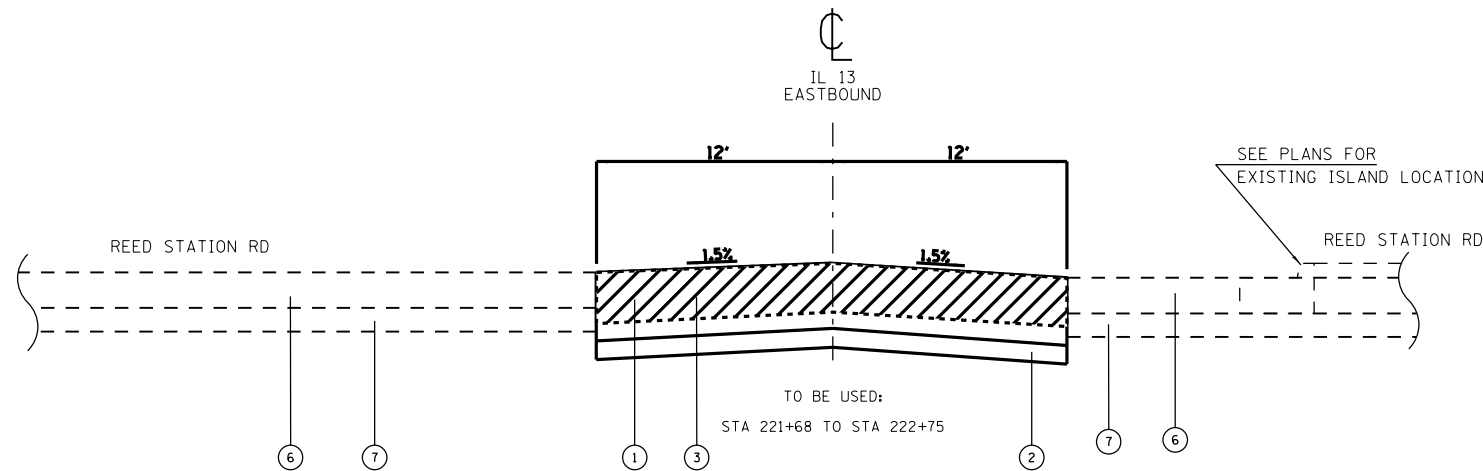
EASTBOUND IL 13 NEAR REED STATION ROAD (FAP 331)  
(NOT TO SCALE)



- ① PROPOSED PCC PAVEMENT JOINTED, 10"
- ② PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 8"
- ③ PROPOSED PAVEMENT REMOVAL
- ④ PROPOSED PAVED SHOULDER REMOVAL
- ⑤ PROPOSED HMA SHOULDER, 8"
- ⑥ EXISTING PCC PAVEMENT
- ⑦ EXISTING SUBBASE GRANULAR MATERIAL
- ⑧ EXISTING BITUMINOUS SHOULDER
- ⑩ TIE BARS AT 30" CTRS, TO BE DRILLED AND EPOXY GROUTED INTO THE EXISTING CONCRETE PAVEMENT

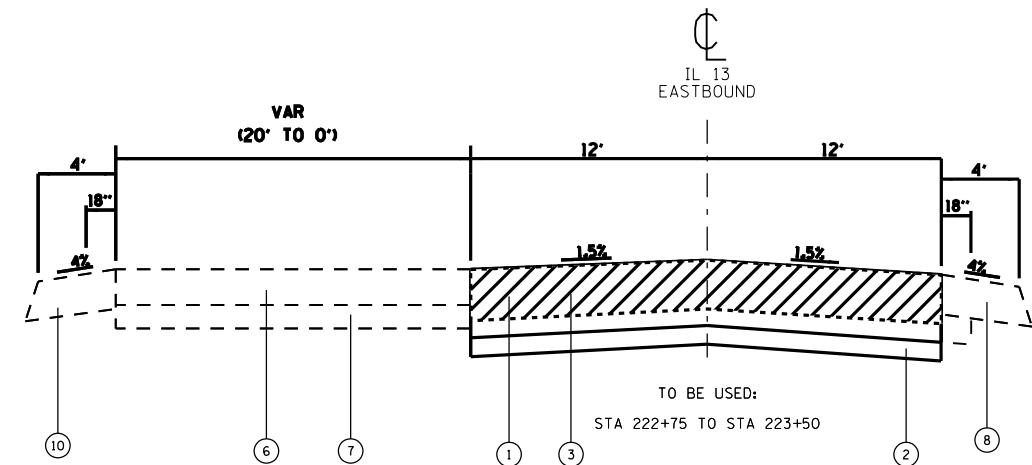
# TYPICAL SECTION

EASTBOUND IL 13 NEAR REED STATION ROAD (FAP 331)  
(NOT TO SCALE)



# TYPICAL SECTION

EASTBOUND IL 13 NEAR REED STATION ROAD (FAP 331)  
(NOT TO SCALE)



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

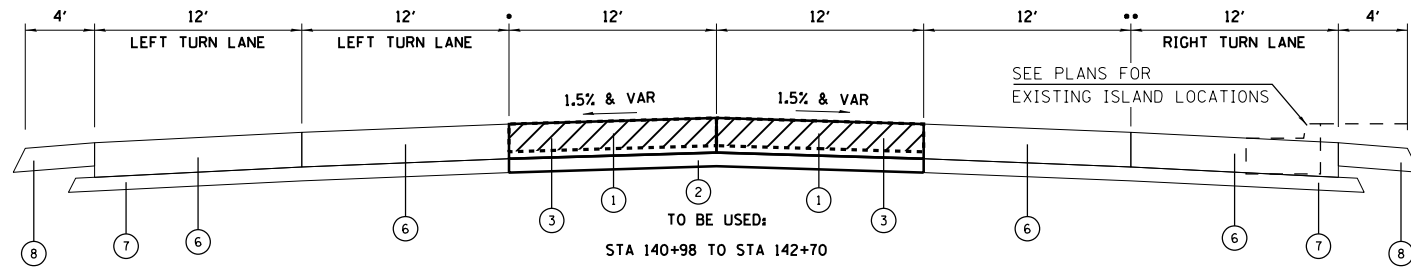
TYPICAL SECTIONS: REED STATION ROAD

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	7
			CONTRACT NO. 78421	

# TYPICAL SECTION

EASTBOUND IL 13 NEAR GIANT CITY ROAD (FAP 331)  
(NOT TO SCALE)

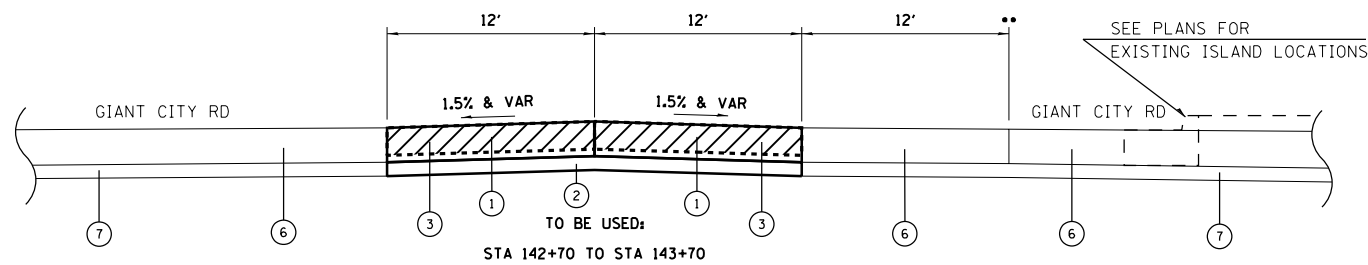


- SEE PLAN VIEW FOR LOCATION OF PAINTED MEDIAN BETWEEN THE PASSING LANE AND THE LEFT TURN LANES
- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE

- ① PROPOSED PCC PAVEMENT JOINTED, 10"
- ② PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 8"
- ③ PROPOSED PAVEMENT REMOVAL
- ④ PROPOSED PAVED SHOULDER REMOVAL
- ⑤ PROPOSED HMA SHOULDER, 8"
- ⑥ EXISTING PCC PAVEMENT
- ⑦ EXISTING SUBBASE GRANULAR MATERIAL
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- ⑩ TIE BARS AT 30" CTRS, TO BE DRILLED AND EPOXY GROUTED INTO THE EXISTING CONCRETE PAVEMENT

# TYPICAL SECTION

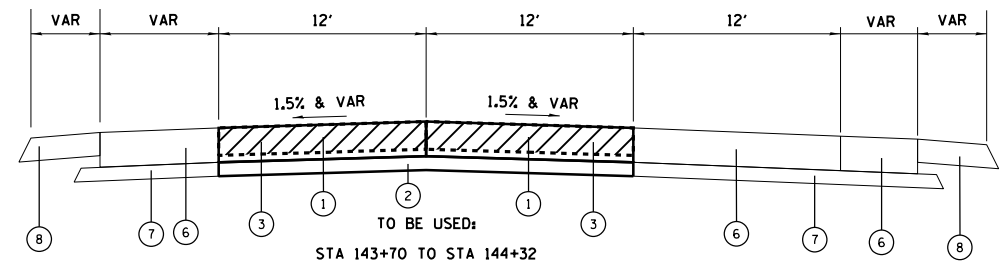
EASTBOUND IL 13 NEAR GIANT CITY ROAD (FAP 331)  
(NOT TO SCALE)



- SEE PLAN VIEW FOR LOCATION OF PAINTED MEDIAN BETWEEN THE PASSING LANE AND THE LEFT TURN LANES
- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE

# TYPICAL SECTION

EASTBOUND IL 13 NEAR GIANT CITY ROAD (FAP 331)  
(NOT TO SCALE)



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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS: GIANT CITY ROAD			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	8
CONTRACT NO.			78421	



## PAVEMENT SCHEDULE

EB IL 13 CARBONDALE LOCATION STATION TO STATION			LENGTH ( INFO ONLY) FEET	SUB GRAN MATERIAL TYPE A 8" SQ YD	PCC PVMT 10" SQ YD	CLASS C PATCH TYPE II 10" SQ YD	CLASS C PATCH TYPE III 10" SQ YD	SAW CUTS FOOT	TIE BARS 3/4"		HMA SHLD 8" SQ YD	PROTECTIVE COAT SQ YD	BIT MATLS PR CT LB	PVMT REMOVAL SQ YD	PAVED SHLD REMOVAL SQ YD
STA	TO	STA							PVMT	PATCH					
STA	TO	STA	FEET	SQ YD	SQ YD	SQ YD	SQ YD	FOOT	EACH	EACH	SQ YD	SQ YD	LB	SQ YD	SQ YD
<u>IL 13 &amp; REED STATION RD</u>															
STA 219+50.00	TO STA	221+70.00	220.00	904.4	904.4				264		97.8	904.4	22.0	904.4	97.8
STA 221+70.00	TO STA	222+90.00	120.00	320.0	320.0	12.0	22.0	306	96	60		354.0		320.0	
STA 222+90.00	TO STA	223+15.00	25.00	66.7	66.7				20			66.7		66.7	
STA 223+15.00	TO STA	223+50.00	35.00	93.3	93.3				28			93.3		93.3	
REED STATION RD TOTALS			400.0	1384.4	1384.4	12.0	22.0	306	408	60	97.8	1418.4	22.0	1384.4	97.8
<u>IL 13 &amp; GIANT CITY RD</u>															
STA 140+98.00	TO STA	144+32.00	334.00	890.7	890.7	12.0	18.0	270	268	52		920.6		890.7	
GIANT CITY RD TOTALS			334.00	890.7	890.7	12.0	18.0	270	268	52	0.0	920.6	0.0	890.7	0.0
PROJECT TOTALS			734.00	2275.1	2275.1	24.0	40.0	576	676	112	97.8	2339.1	22.0	2275.1	97.8

NOTE: TIE BARS FOR PVMT USED IN OUTSIDE LONGITUDINAL JOINTS WHERE ADJACENT TO EXISTING PCC.

788

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT &amp; SHOULDER SCHEDULE</b>	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw\work\p\dot\leftwich\d0398565\78421-sht-plen.dgn		DRAWN -	REVISED -			331	*	JACKSON	23	9	
\$MODELNAME\$		CHECKED -	REVISED -			CONTRACT NO. 78421					
	PLOT DATE = 12/11/2014	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO

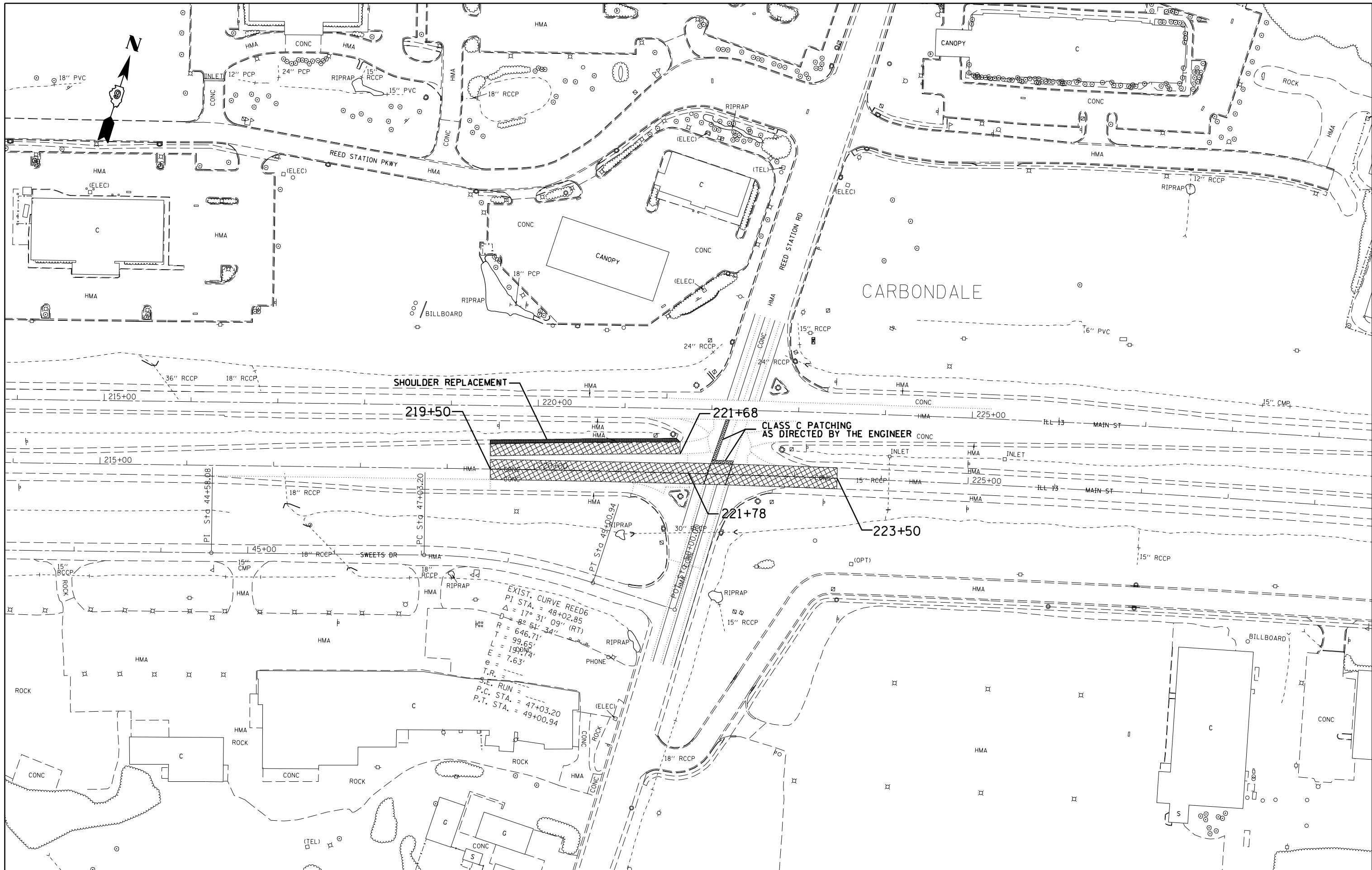
## PAVEMENT MARKING SCHEDULE

EB IL 13 CARBONDALE  LOCATION STATION TO STATION  STA TO STA	MODIFIED URETHANE PAVEMENT MARKING					SHORT TERM PVMT MRK FT	TEMP PVMT MRK 4" FT	TEMP PVMT MRK 24" FT	WORK ZONE PAVT MK REM SQ FT	RAISED REF PVMT MKR EACH	RAISED REF PVMT MKR REMOVAL EACH	DETECTOR LOOP TYPE 1 FEET	DESCRIPTION
	SKIP DASH WHITE 4"	SOLID WHITE 4"	SOLID YELLOW 4"	SOLID WHITE 24"	LETTERS AND SYMBOLS SQ FT								
	FT	FT	FT	FT	SQ FT								
<u>IL 13 &amp; REED STATION RD</u>													
STA 219+50.00 TO STA 221+70.00	55.0	660.0	220.0	12.0		22.0	935.0	12.0	343.0	24	18	258	
STA 221+70.00 TO STA 221+86.00	4.0	32.0		24.0		1.6	36.0	24.0	60.5				
STA 221+86.00 TO STA 222+90.00	26.0	0.0				10.4	26.0		12.1	2	2		
STA 222+90.00 TO STA 223+15.00	6.3	25.0				2.5	31.3		11.3				
STA 223+15.00 TO STA 223+50.00	8.8	70.0	35.0			3.5	113.8		39.1				
<b>REED'S STATION RD TOTALS</b>	100.0	787.0	255.0	36.0	NA	40.0	1142.0	36.0	466.0	26	20	258	
<u>IL 13 &amp; GIANT CITY RD</u>													
STA 141+00.00 TO STA 142+85.00	92.5	185.0		24.0		18.5	277.5	24.0	146.7	15	5	92	
STA 142+85.00 TO STA 144+06.00	60.5					12.1	60.5		24.2	2	0		
STA 144+06.00 TO STA 144+32.00	13.0		26.0			2.6	39.0		13.9	2	0		
<b>GIANT CITY RD TOTALS</b>	166.0	185.0	26.0	24.0	NA	33.2	377.0	24.0	184.7	19	5	92	
<b>SHEET TOTALS</b>	266	972	281	60	NA	73	1519	60	651	45	25	350	
1519													

**NOTES**

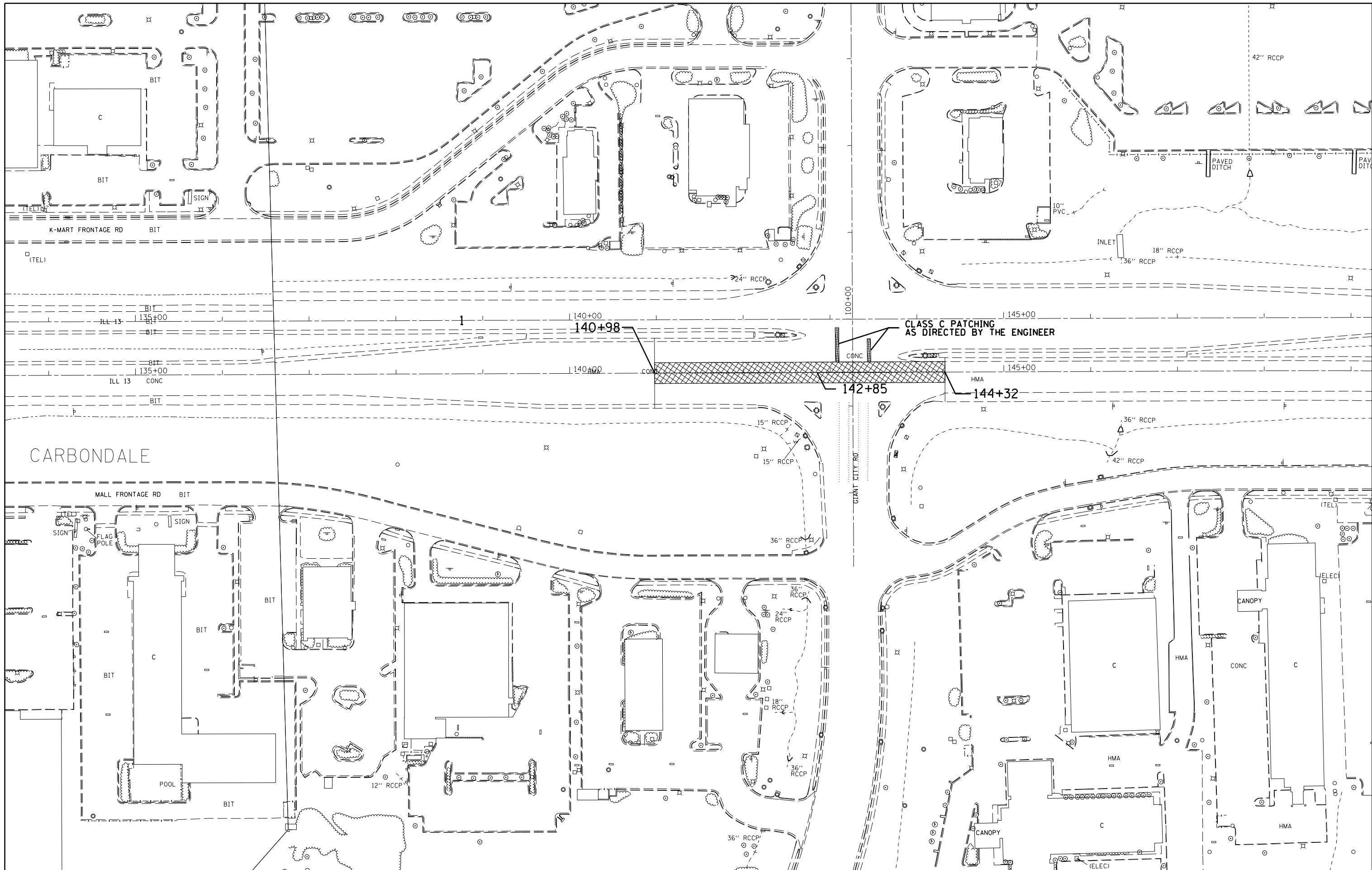
- \* QUANTITY IS ALSO FOR GROOVING FOR RECESSED PAVEMENT MARKING 5"
- \*\* QUANTITY IS ALSO FOR GROOVING FOR RECESSED PAVEMENT MARKING 25"

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT MARKING SCHEDULE (ALSO RAISED REF PVMT MARKERS &amp; DETECTOR LOOPS)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw\work\p\dot\left\lchd\ld0398565\78421-sht-plen.dgn		DRAWN -	REVISED -			331	*	JACKSON	23	10	
\$MODELNAME\$		CHECKED -	REVISED -			CONTRACT NO. 78421					
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FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET: REED STATION ROAD</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pwork\pwork\dot\left\ltd\0398565\78421-sht-plan.dgn		DRAWN -	REVISED -		331	.	JACKSON	23	11				
\$MODELNAME\$	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 78421				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/11/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		

\*D9 CM PAVEMENT REPAIR 2015-2



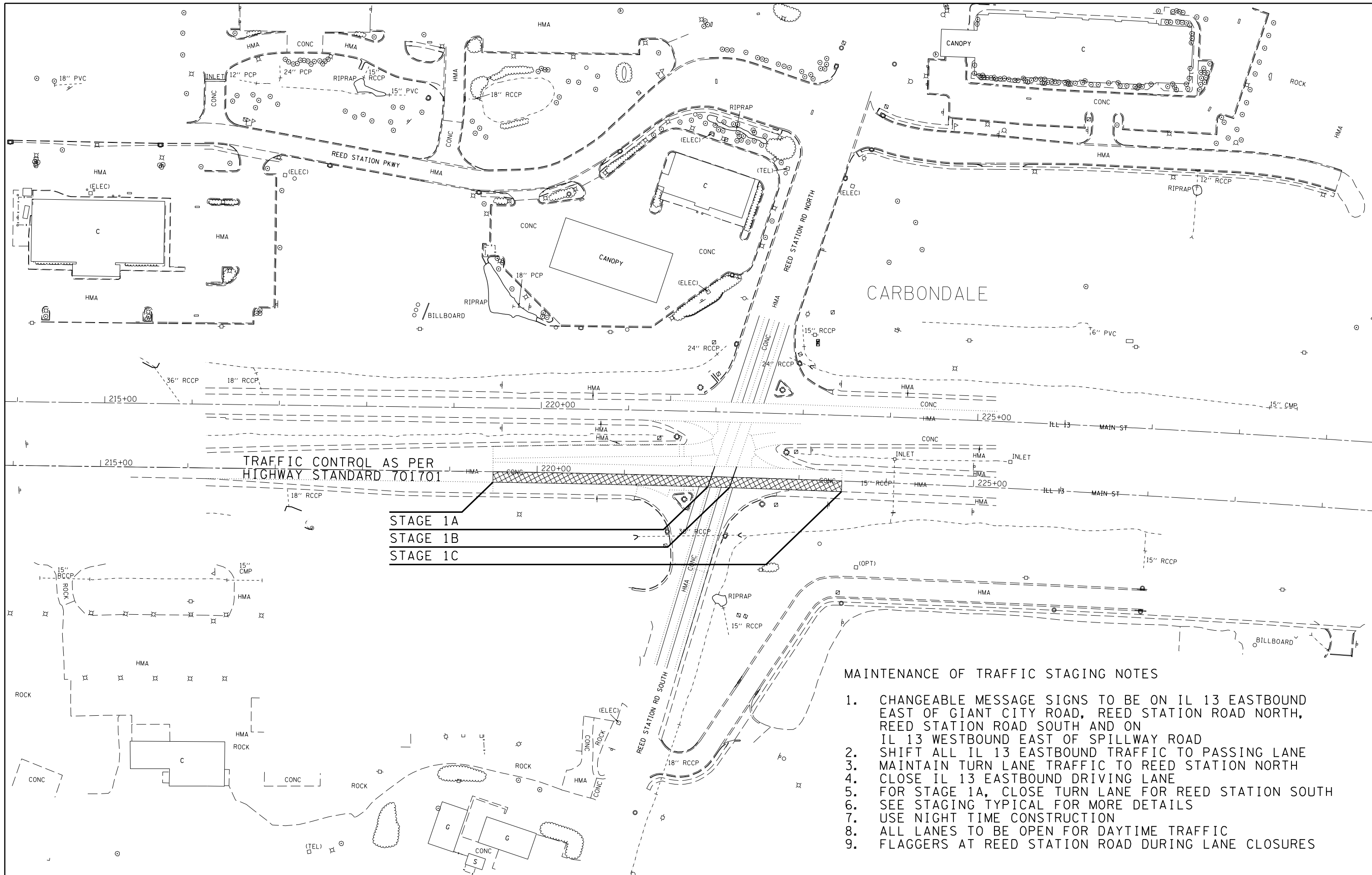
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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>PLAN SHEET: GIANT CITY ROAD</b>				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 78421	

\*D9 CM PAVEMENT REPAIR 2015-2



TRAFFIC CONTROL AS PER  
HIGHWAY STANDARD 701701

STAGE 1A  
STAGE 1B  
STAGE 1C

MAINTENANCE OF TRAFFIC STAGING NOTES

1. CHANGEABLE MESSAGE SIGNS TO BE ON ILL 13 EASTBOUND EAST OF GIANT CITY ROAD, REED STATION ROAD NORTH, REED STATION ROAD SOUTH AND ON ILL 13 WESTBOUND EAST OF SPILLWAY ROAD
2. SHIFT ALL ILL 13 EASTBOUND TRAFFIC TO PASSING LANE
3. MAINTAIN TURN LANE TRAFFIC TO REED STATION NORTH
4. CLOSE ILL 13 EASTBOUND DRIVING LANE
5. FOR STAGE 1A, CLOSE TURN LANE FOR REED STATION SOUTH
6. SEE STAGING TYPICAL FOR MORE DETAILS
7. USE NIGHT TIME CONSTRUCTION
8. ALL LANES TO BE OPEN FOR DAYTIME TRAFFIC
9. FLAGGERS AT REED STATION ROAD DURING LANE CLOSURES

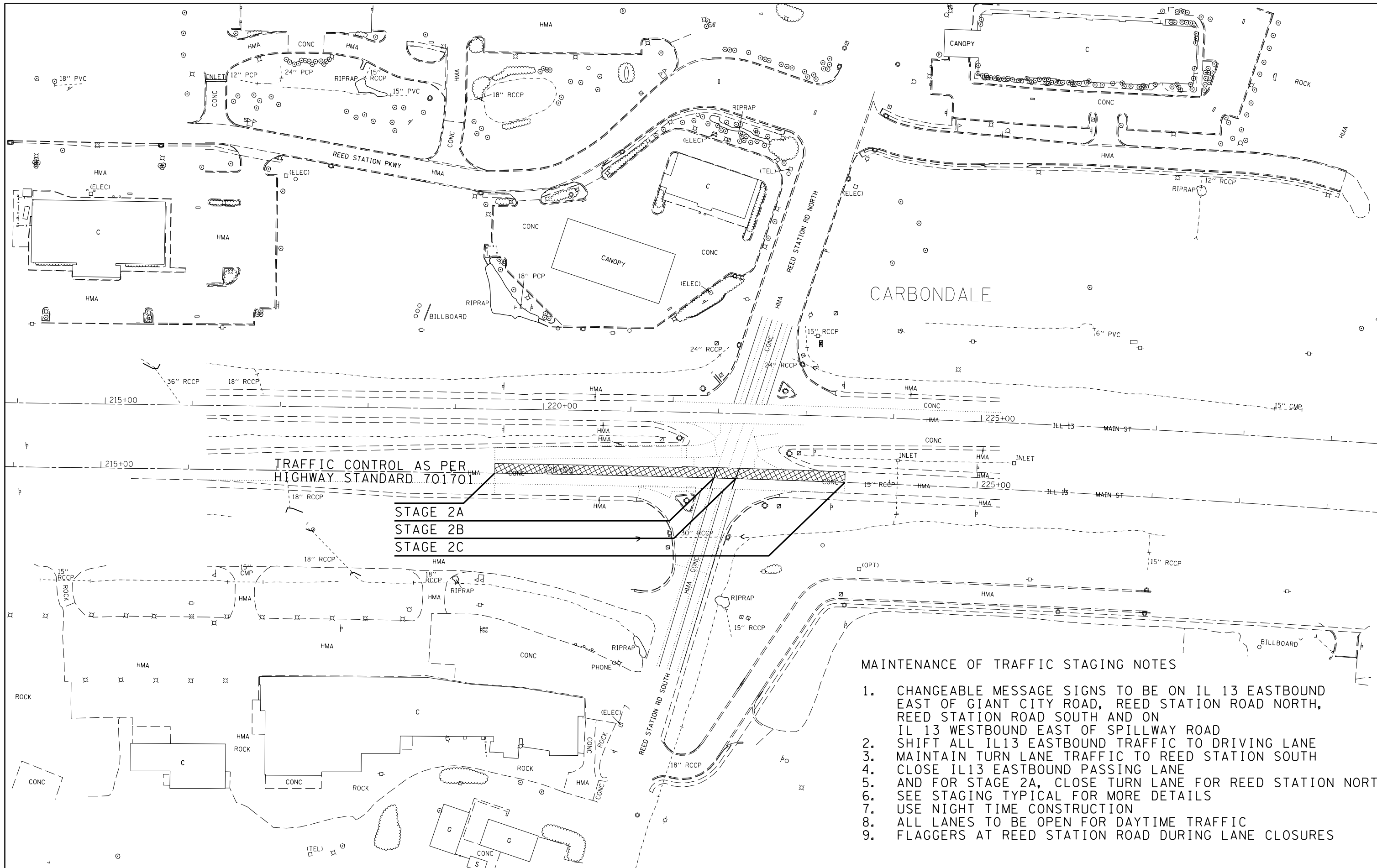
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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REED STATION ROAD STAGING  
PLAN SHEET 1 OF 3

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	13
			CONTRACT NO. 78421	



TRAFFIC CONTROL AS PER  
HIGHWAY STANDARD 701701

STAGE 2A  
STAGE 2B  
STAGE 2C

MAINTENANCE OF TRAFFIC STAGING NOTES

1. CHANGEABLE MESSAGE SIGNS TO BE ON IL 13 EASTBOUND EAST OF GIANT CITY ROAD, REED STATION ROAD NORTH, REED STATION ROAD SOUTH AND ON IL 13 WESTBOUND EAST OF SPILLWAY ROAD
2. SHIFT ALL IL13 EASTBOUND TRAFFIC TO DRIVING LANE
3. MAINTAIN TURN LANE TRAFFIC TO REED STATION SOUTH
4. CLOSE IL13 EASTBOUND PASSING LANE
5. AND FOR STAGE 2A, CLOSE TURN LANE FOR REED STATION NORTH
6. SEE STAGING TYPICAL FOR MORE DETAILS
7. USE NIGHT TIME CONSTRUCTION
8. ALL LANES TO BE OPEN FOR DAYTIME TRAFFIC
9. FLAGGERS AT REED STATION ROAD DURING LANE CLOSURES

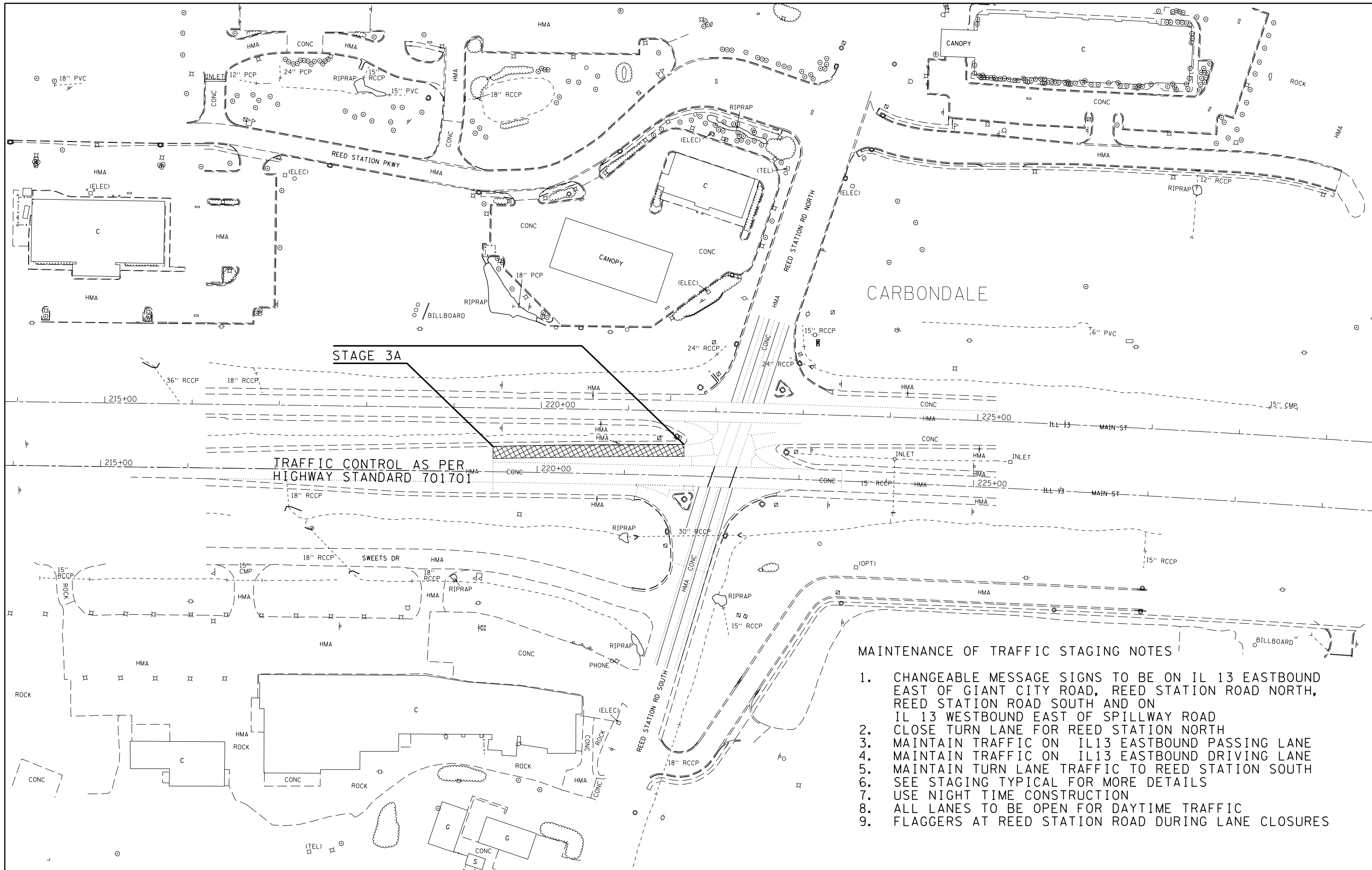
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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REED STATION ROAD STAGING  
PLAN SHEET 2 OF 3

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	14
CONTRACT NO. 78421				



- MAINTENANCE OF TRAFFIC STAGING NOTES**
1. CHANGEABLE MESSAGE SIGNS TO BE ON IL 13 EASTBOUND EAST OF GIANT CITY ROAD, REED STATION ROAD NORTH, REED STATION ROAD SOUTH AND ON IL 13 WESTBOUND EAST OF SPILLWAY ROAD
  2. CLOSE TURN LANE FOR REED STATION NORTH
  3. MAINTAIN TRAFFIC ON IL13 EASTBOUND PASSING LANE
  4. MAINTAIN TRAFFIC ON IL13 EASTBOUND DRIVING LANE
  5. MAINTAIN TURN LANE TRAFFIC TO REED STATION SOUTH
  6. SEE STAGING TYPICAL FOR MORE DETAILS
  7. USE NIGHT TIME CONSTRUCTION
  8. ALL LANES TO BE OPEN FOR DAYTIME TRAFFIC
  9. FLAGGERS AT REED STATION ROAD DURING LANE CLOSURES

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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

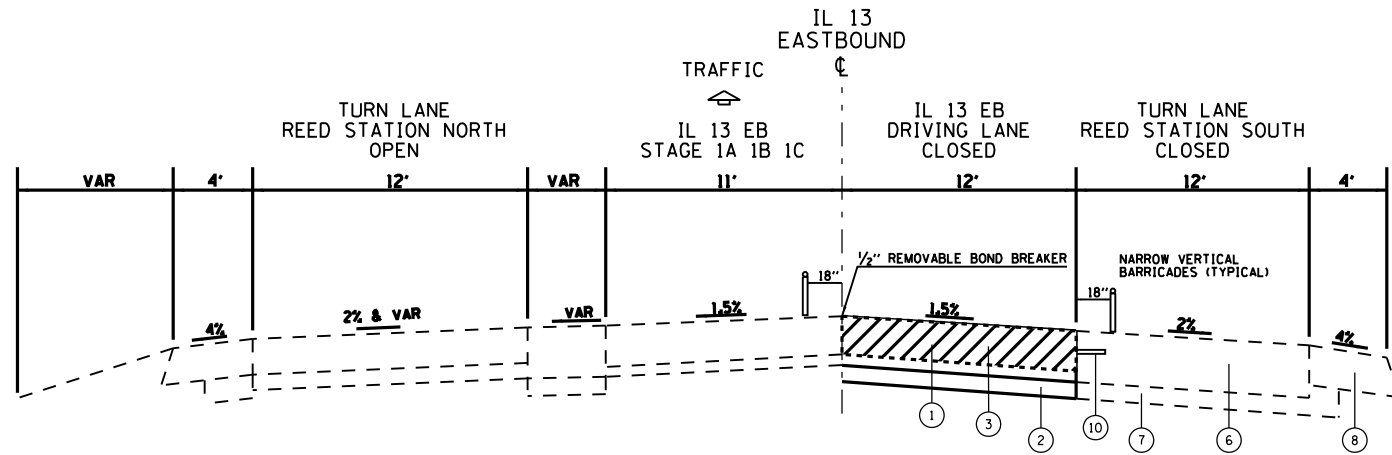
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>REED STATION ROAD STAGING PLAN SHEET 3 OF 3</b>				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	15
ILLINOIS FED. AID PROJECT			CONTRACT NO. 78421	

# STAGE 1A TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)

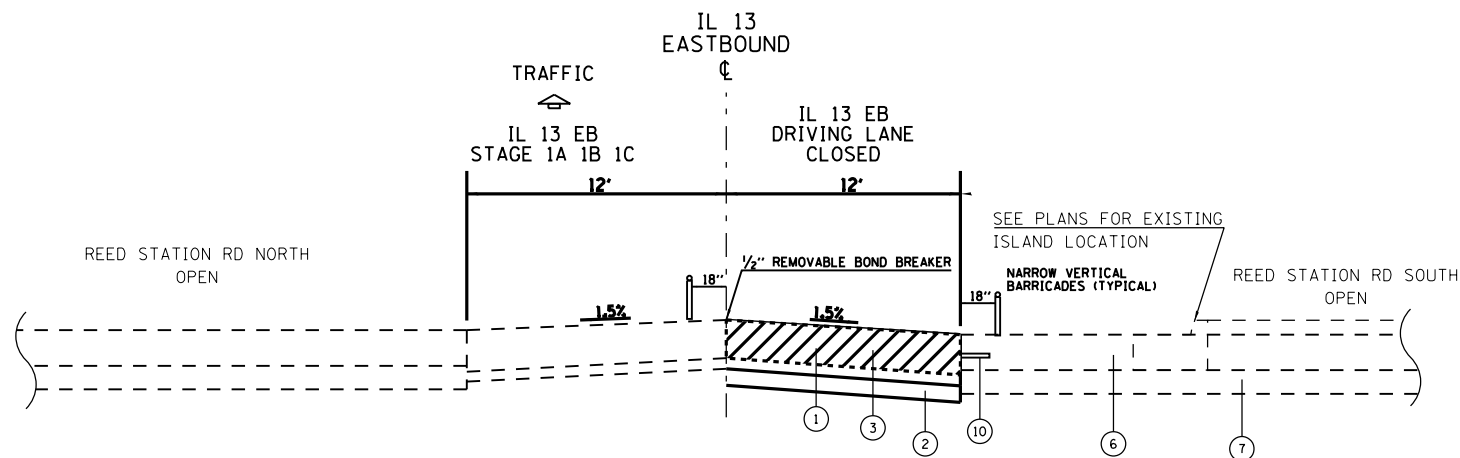


TO BE USED:  
STA 219+50 TO STA 221+68

- |   |   |   |   |
|---|---|---|---|
| ① | PROPOSED PCC PAVEMENT JOINTED, 10"            | ⑥ | EXISTING PCC PAVEMENT   |
| ② | PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 8" | ⑦ | EXISTING SUBBASE GRANULAR MATERIAL  |
| ③ | PROPOSED PAVEMENT REMOVAL                     | ⑧ | EXISTING BITUMINOUS SHOULDER  |
| ④ | PROPOSED PAVED SHOULDER REMOVAL               | ⑩ | TIE BARS AT 30" CTRS. TO BE DRILLED AND EPOXY GROUTED INTO THE EXISTING CONCRETE PAVEMENT |
| ⑤ | PROPOSED HMA SHOULDER, 8"                     |   |   |

# STAGE 1A 1B 1C TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)



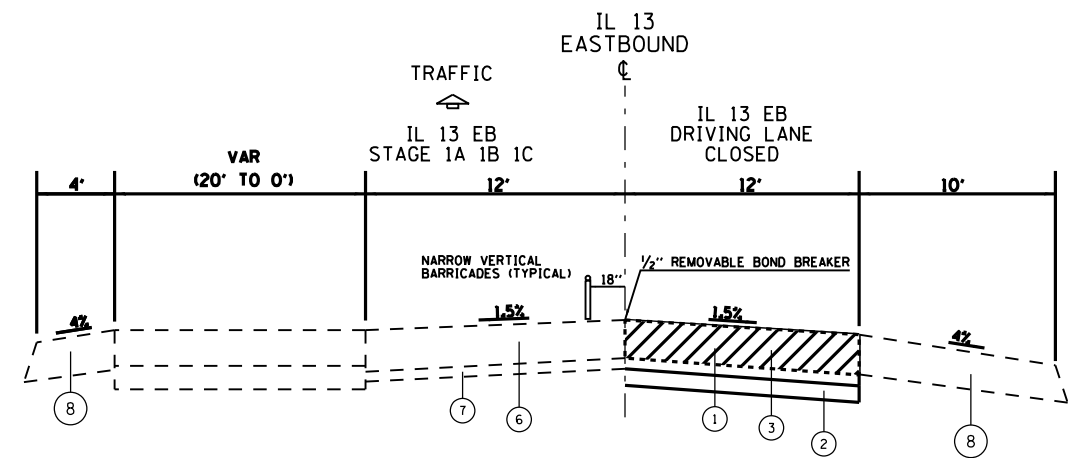
TO BE USED:  
1A: STA 221+68.0 TO STA 221+99.5  
1B: STA 221+99.5 TO STA 222+24.5  
1C: STA 221+24.5 TO STA 222+75.0

TURN LANE  
REED STATION NORTH  
OPEN TO TRAFFIC

TURN LANE  
REED STATION SOUTH  
OPEN TO TRAFFIC  
STAGE 1B 1C

# STAGE 1C TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)



TO BE USED:  
222+75 TO STA 223+50

TURN LANE  
REED STATION NORTH  
OPEN TO TRAFFIC

TURN LANE  
REED STATION SOUTH  
OPEN TO TRAFFIC

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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REED STATION ROAD STAGING  
TYPICALS: SHEET 1 OF 3

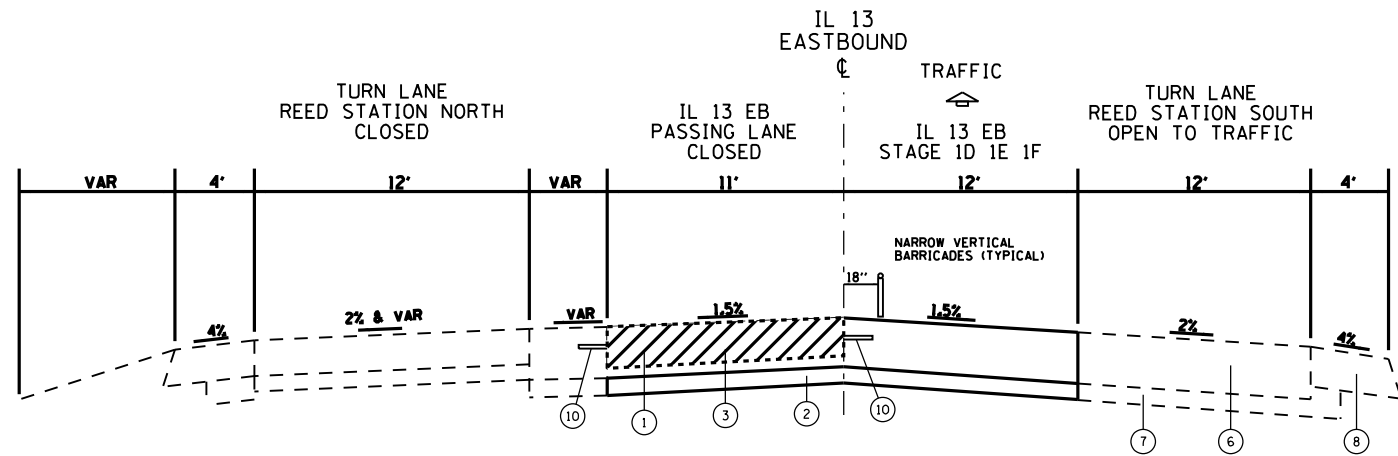
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	16
CONTRACT NO. 78421				



# STAGE 2A TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)

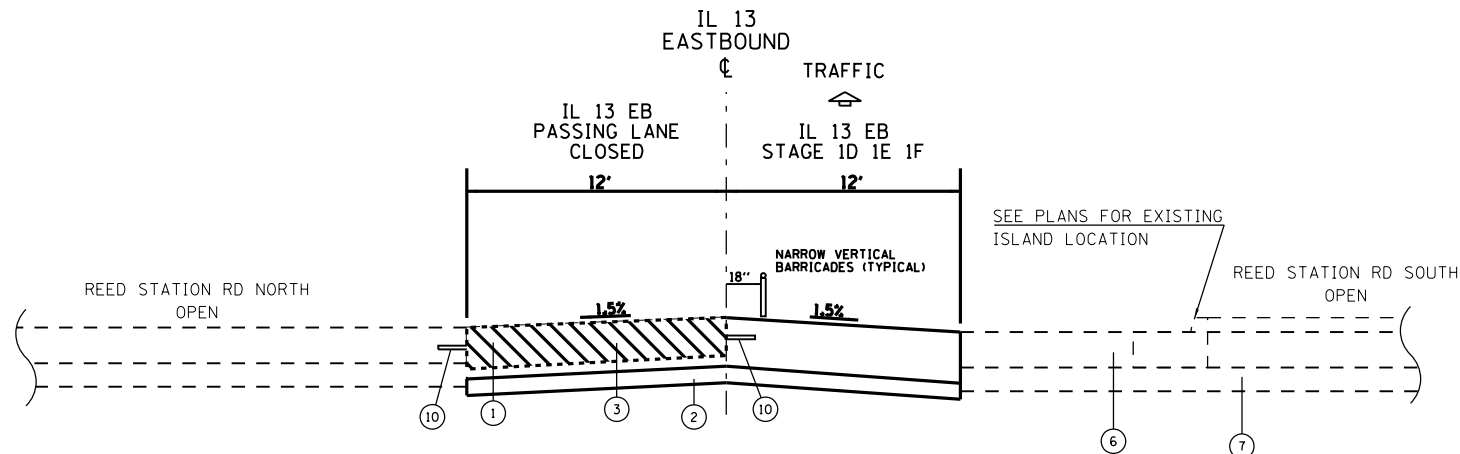


TO BE USED:  
STA 219+50 TO STA 221+68

- ① PROPOSED PCC PAVEMENT JOINTED, 10"
- ② PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 8"
- ③ PROPOSED PAVEMENT REMOVAL
- ④ PROPOSED PAVED SHOULDER REMOVAL
- ⑤ PROPOSED HMA SHOULDER, 8"
- ⑥ EXISTING PCC PAVEMENT
- ⑦ EXISTING SUBBASE GRANULAR MATERIAL
- ⑧ EXISTING BITUMINOUS SHOULDER
- ⑩ TIE BARS AT 30" CTRS. TO BE DRILLED AND EPOXY GROUTED INTO THE EXISTING CONCRETE PAVEMENT

# STAGE 2A 2B 2C TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)



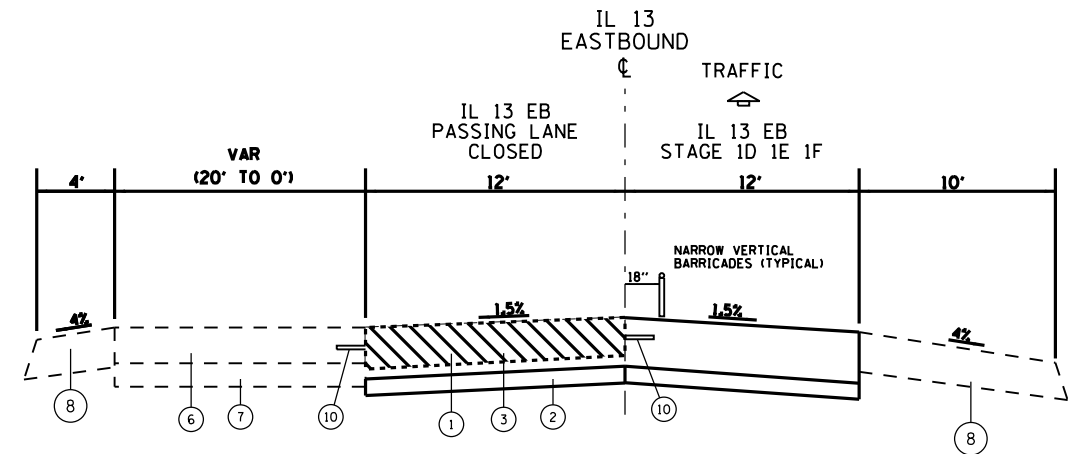
TO BE USED:  
1D: STA 221+68.0 TO STA 221+99.5 (31.5')  
1E: STA 221+99.5 TO STA 222+24.5 (25.0')  
1F: STA 222+24.5 TO STA 222+75.0 (50.5')

TURN LANE  
REED STATION NORTH  
OPEN TO TRAFFIC  
STAGE 2B 2C

TURN LANE  
REED STATION SOUTH  
OPEN TO TRAFFIC

# STAGE 2C TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)



TO BE USED:  
222+75 TO STA 223+50

TURN LANE  
REED STATION NORTH  
OPEN TO TRAFFIC

TURN LANE  
REED STATION SOUTH  
OPEN TO TRAFFIC

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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REED STATION ROAD STAGING  
TYPICALS: SHEET 2 OF 3

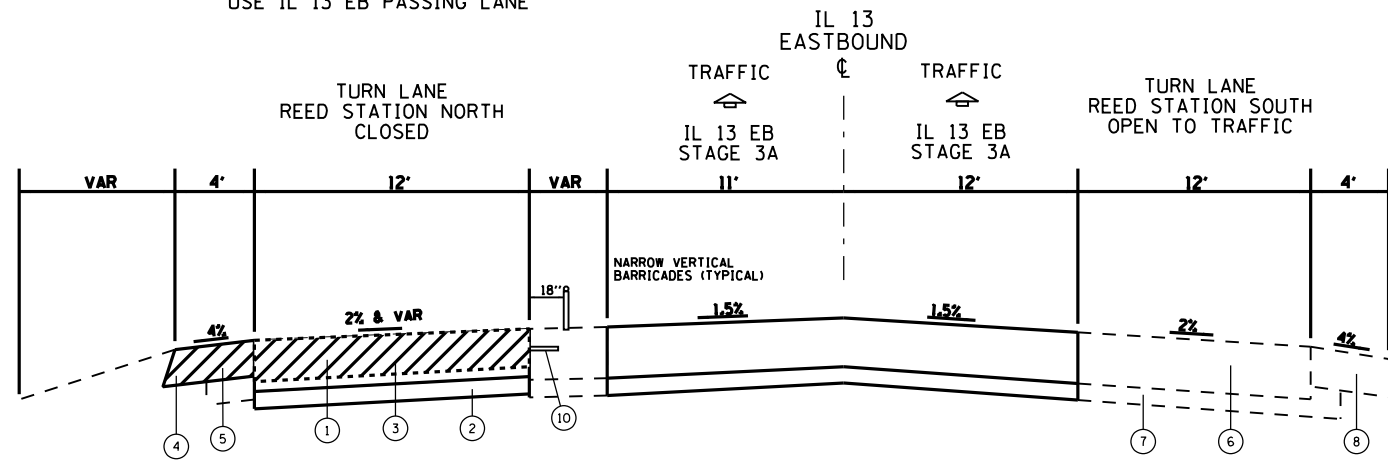
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	17
CONTRACT NO. 78421				

# STAGE 3A TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)

CHANGEABLE MESSAGE SIGN  
TO READ  
REED STATION ROAD NORTH  
USE IL 13 EB PASSING LANE



TO BE USED:  
STA 219+50 TO STA 221+68

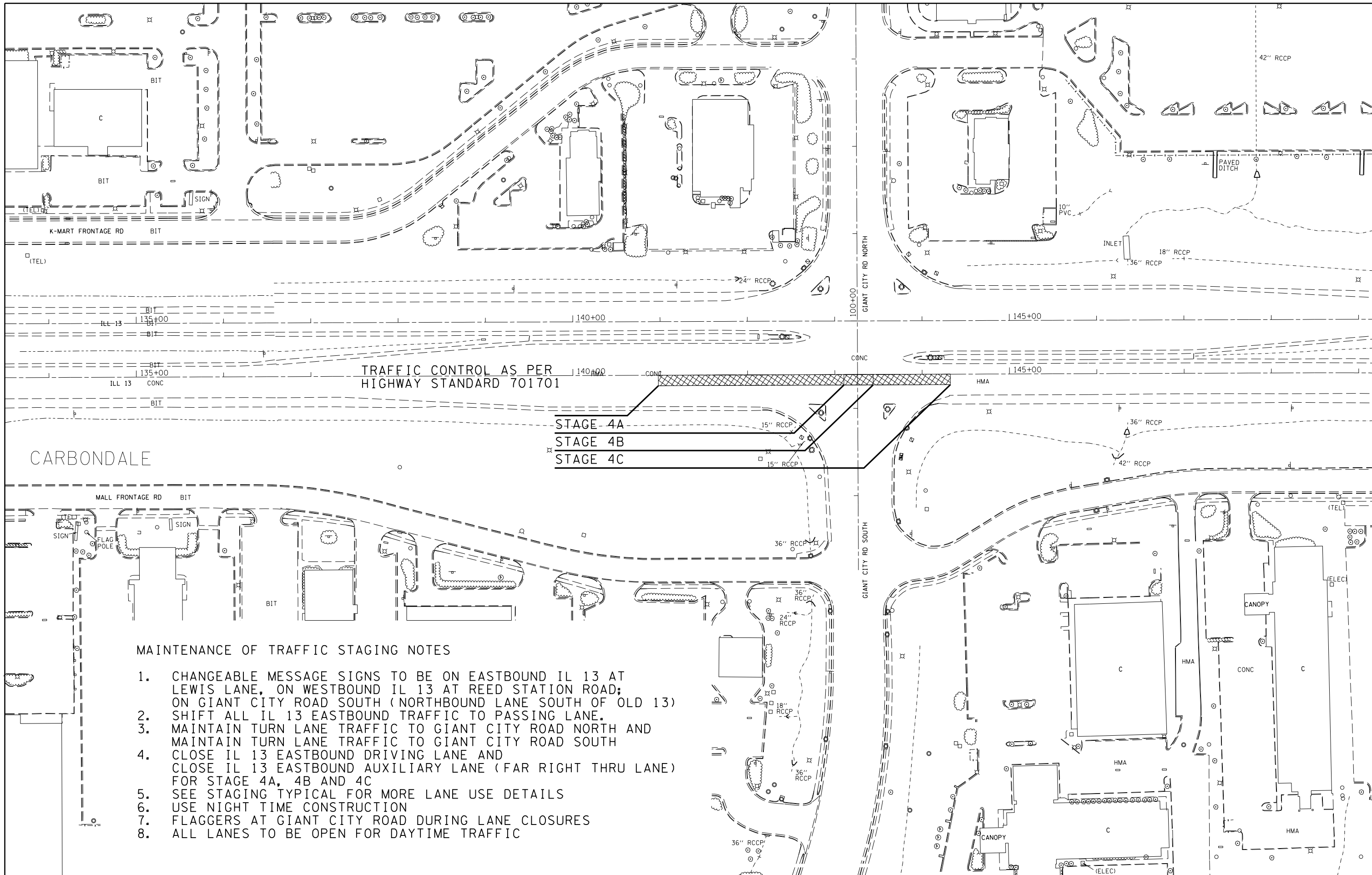
- |   |   |   |   |
|---|---|---|---|
| ① | PROPOSED PCC PAVEMENT JOINTED, 10"            | ⑥ | EXISTING PCC PAVEMENT   |
| ② | PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 8" | ⑦ | EXISTING SUBBASE GRANULAR MATERIAL  |
| ③ | PROPOSED PAVEMENT REMOVAL                     | ⑧ | EXISTING BITUMINOUS SHOULDER  |
| ④ | PROPOSED PAVED SHOULDER REMOVAL               | ⑩ | TIE BARS AT 30" CTRS. TO BE DRILLED AND EPOXY GROUTED INTO THE EXISTING CONCRETE PAVEMENT |
| ⑤ | PROPOSED HMA SHOULDER, 8"                     |   |   |

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\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REED STATION ROAD STAGING			
TYPICALS: SHEET 3 OF 3			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	18
CONTRACT NO.			78421	
ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AS PER  
HIGHWAY STANDARD 701701

STAGE 4A  
STAGE 4B  
STAGE 4C

CARBONDALE

**MAINTENANCE OF TRAFFIC STAGING NOTES**

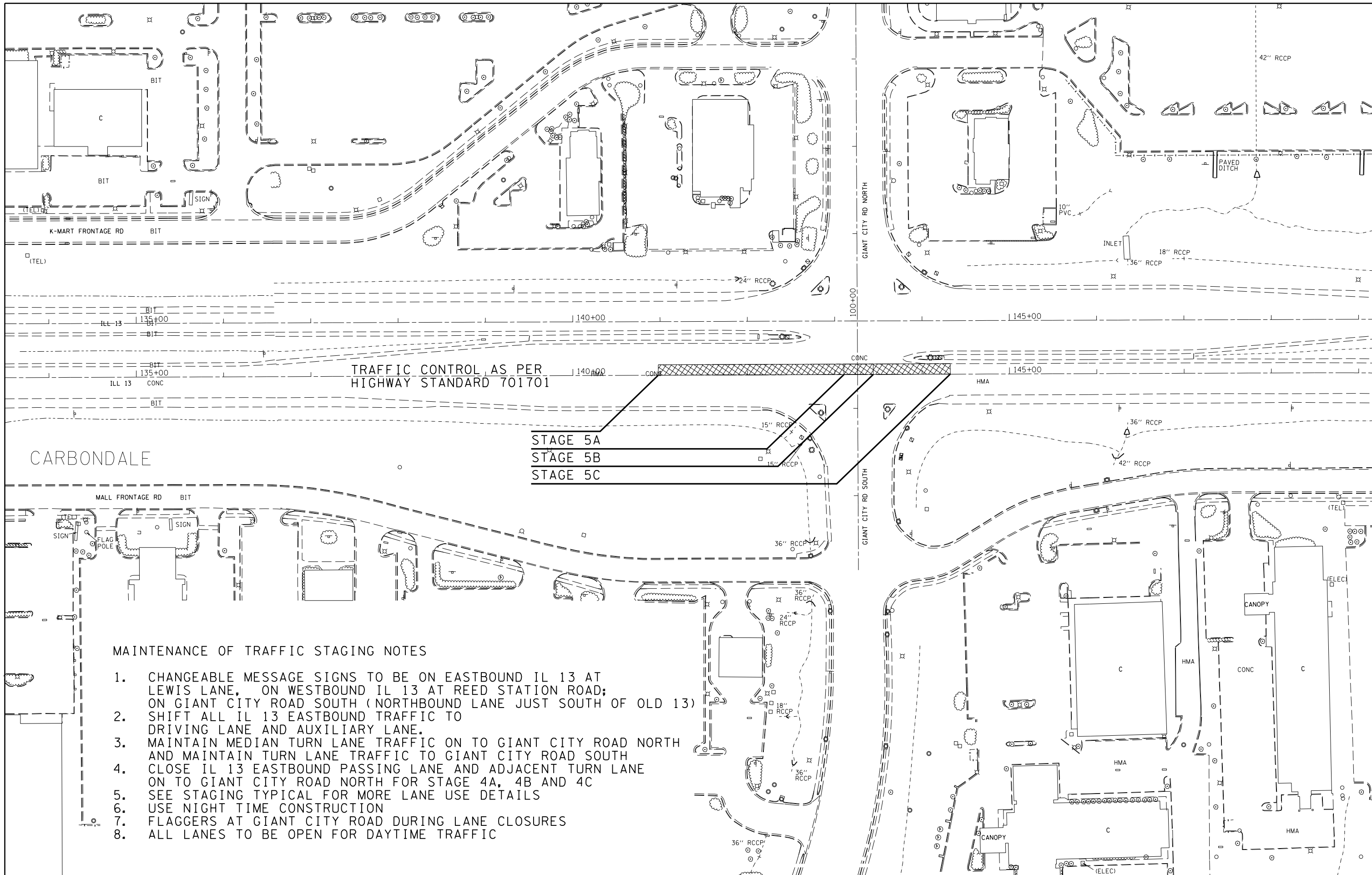
1. CHANGEABLE MESSAGE SIGNS TO BE ON EASTBOUND IL 13 AT LEWIS LANE, ON WESTBOUND IL 13 AT REED STATION ROAD; ON GIANT CITY ROAD SOUTH (NORTHBOUND LANE SOUTH OF OLD 13)
2. SHIFT ALL IL 13 EASTBOUND TRAFFIC TO PASSING LANE.
3. MAINTAIN TURN LANE TRAFFIC TO GIANT CITY ROAD NORTH AND MAINTAIN TURN LANE TRAFFIC TO GIANT CITY ROAD SOUTH
4. CLOSE IL 13 EASTBOUND DRIVING LANE AND CLOSE IL 13 EASTBOUND AUXILIARY LANE (FAR RIGHT THRU LANE) FOR STAGE 4A, 4B AND 4C
5. SEE STAGING TYPICAL FOR MORE LANE USE DETAILS
6. USE NIGHT TIME CONSTRUCTION
7. FLAGGERS AT GIANT CITY ROAD DURING LANE CLOSURES
8. ALL LANES TO BE OPEN FOR DAYTIME TRAFFIC

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>GIANT CITY ROAD STAGING PLAN SHEET 1 OF 2</b>	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	19
CONTRACT NO. 78421				



TRAFFIC CONTROL AS PER  
HIGHWAY STANDARD 701701

STAGE 5A  
STAGE 5B  
STAGE 5C

CARBONDALE

MAINTENANCE OF TRAFFIC STAGING NOTES

1. CHANGEABLE MESSAGE SIGNS TO BE ON EASTBOUND ILL 13 AT LEWIS LANE, ON WESTBOUND ILL 13 AT REED STATION ROAD; ON GIANT CITY ROAD SOUTH (NORTHBOUND LANE JUST SOUTH OF OLD 13)
2. SHIFT ALL ILL 13 EASTBOUND TRAFFIC TO DRIVING LANE AND AUXILIARY LANE.
3. MAINTAIN MEDIAN TURN LANE TRAFFIC ON TO GIANT CITY ROAD NORTH AND MAINTAIN TURN LANE TRAFFIC TO GIANT CITY ROAD SOUTH
4. CLOSE ILL 13 EASTBOUND PASSING LANE AND ADJACENT TURN LANE ON TO GIANT CITY ROAD NORTH FOR STAGE 4A, 4B AND 4C
5. SEE STAGING TYPICAL FOR MORE LANE USE DETAILS
6. USE NIGHT TIME CONSTRUCTION
7. FLAGGERS AT GIANT CITY ROAD DURING LANE CLOSURES
8. ALL LANES TO BE OPEN FOR DAYTIME TRAFFIC

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -
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	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

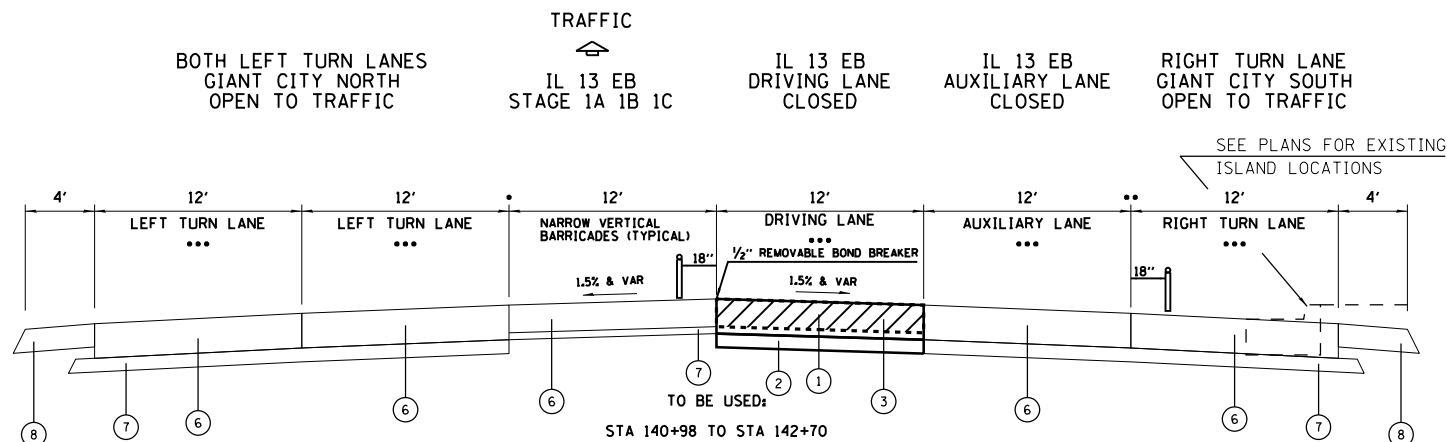
SCALE:		SHEET	OF	SHEETS	STA.	TO	STA.
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GIANT CITY ROAD STAGING  
PLAN SHEET 2 OF 2

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	20
CONTRACT NO. 78421				

# STAGE 4A TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)

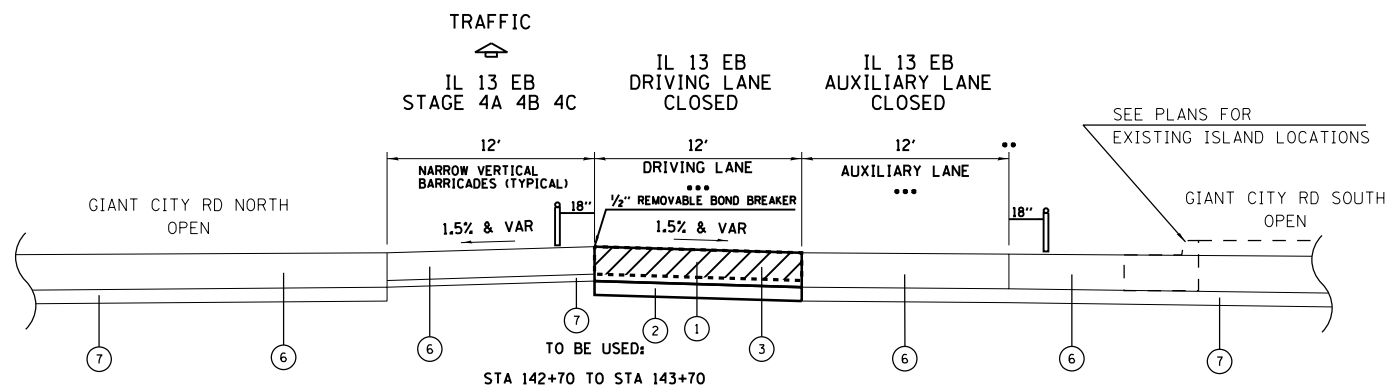


- SEE PLAN VIEW FOR LOCATION OF PAINTED MEDIAN BETWEEN THE PASSING LANE AND THE LEFT TURN LANES
- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE
- PAVEMENT STRUCTURE IS THE SAME FOR GIANT CITY ROAD THRU THE INTERSECTION, SEE PLAN VIEW. ALL CONSTRUCTION IS TO MATCH EXISTING PAVEMENT JOINTS AS DIRECTED BY THE ENGINEER.

- ① PROPOSED PCC PAVEMENT JOINTED, 10"
- ② PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 8"
- ③ PROPOSED PAVEMENT REMOVAL
- ④ PROPOSED PAVED SHOULDER REMOVAL
- ⑤ PROPOSED HMA SHOULDER, 8"
- ⑥ EXISTING PCC PAVEMENT
- ⑦ EXISTING SUBBASE GRANULAR MATERIAL
- ⑧ EXISTING BITUMINOUS SHOULDER
- ⑩ TIE BARS AT 30" CTRS, TO BE DRILLED AND EPOXY GROUTED INTO THE EXISTING CONCRETE PAVEMENT

# STAGE 4A 4B 4C TYPICAL SECTION

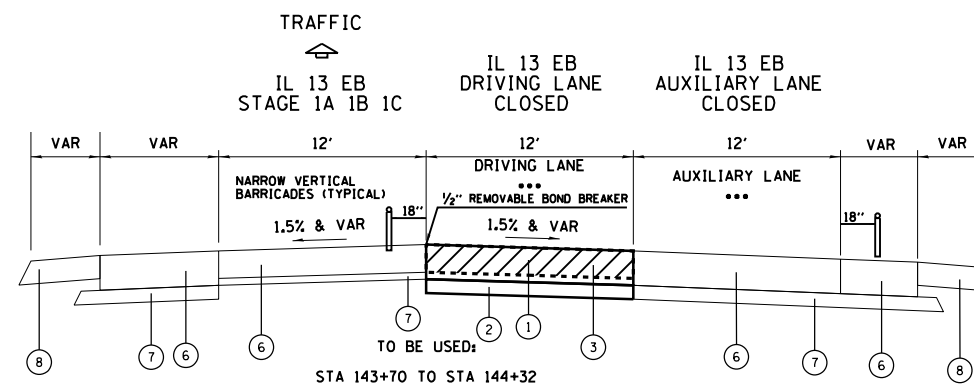
EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)



- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE
- PAVEMENT STRUCTURE IS THE SAME FOR GIANT CITY ROAD THRU THE INTERSECTION, SEE PLAN VIEW. ALL CONSTRUCTION IS TO MATCH EXISTING PAVEMENT JOINTS AS DIRECTED BY THE ENGINEER.

# STAGE 4C TYPICAL SECTION

EASTBOUND IL 13 NEAR REED'S STATION ROAD (FAP 331)  
(NOT TO SCALE)



- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE
- PAVEMENT STRUCTURE IS THE SAME FOR GIANT CITY ROAD THRU THE INTERSECTION, SEE PLAN VIEW. ALL CONSTRUCTION IS TO MATCH EXISTING PAVEMENT JOINTS AS DIRECTED BY THE ENGINEER.

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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

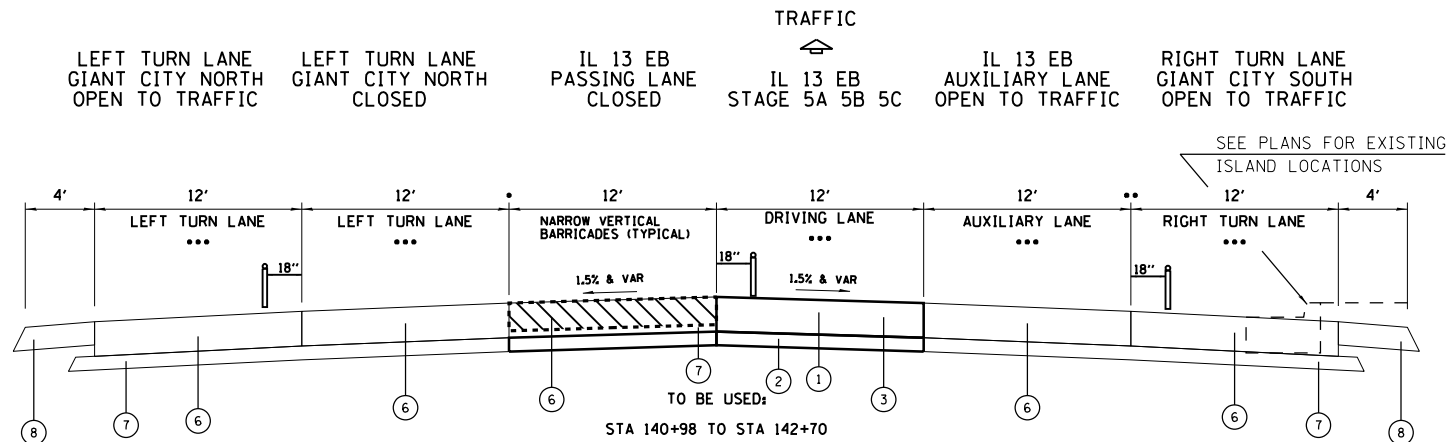
GIANT CITY ROAD STAGING  
TYPICALS: SHEET 1 OF 2

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	.	JACKSON	23	21
CONTRACT NO. 78421				

# STAGE 5A TYPICAL SECTION

EASTBOUND IL 13 NEAR GIANT CITY ROAD (FAP 331)  
(NOT TO SCALE)

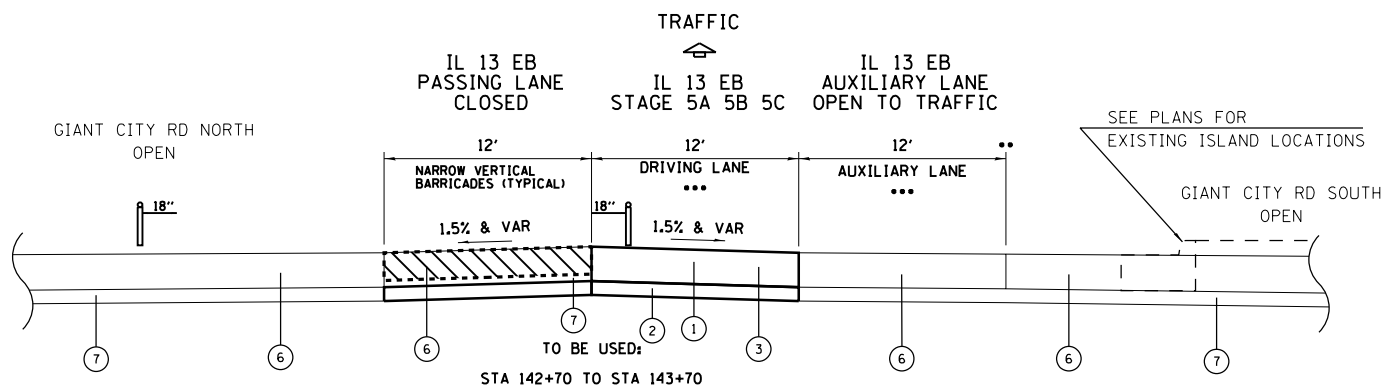


- SEE PLAN VIEW FOR LOCATION OF PAINTED MEDIAN BETWEEN THE PASSING LANE AND THE LEFT TURN LANES
- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE
- PAVEMENT STRUCTURE IS THE SAME FOR GIANT CITY ROAD THRU THE INTERSECTION, SEE PLAN VIEW. ALL CONSTRUCTION IS TO MATCH EXISTING PAVEMENT JOINTS AS DIRECTED BY THE ENGINEER.

- ① PROPOSED PCC PAVEMENT JOINTED, 10"
- ② PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 8"
- ③ PROPOSED PAVEMENT REMOVAL
- ④ PROPOSED PAVED SHOULDER REMOVAL
- ⑤ PROPOSED HMA SHOULDER, 8"
- ⑥ EXISTING PCC PAVEMENT
- ⑦ EXISTING SUBBASE GRANULAR MATERIAL
- ⑧ EXISTING BITUMINOUS SHOULDER
- ⑩ TIE BARS AT 30" CTRS, TO BE DRILLED AND EPOXY GROUTED INTO THE EXISTING CONCRETE PAVEMENT

# STAGE 5A 5B 5C TYPICAL SECTION

EASTBOUND IL 13 NEAR GIANT CITY ROAD (FAP 331)  
(NOT TO SCALE)



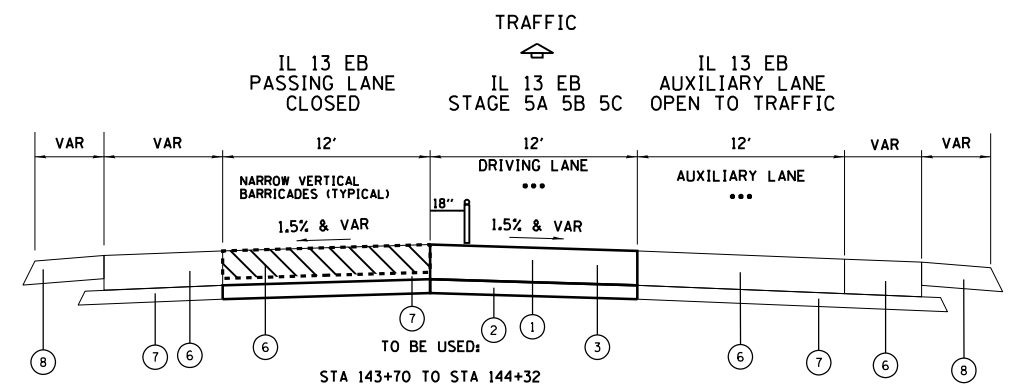
BOTH LEFT TURN LANES  
GIANT CITY NORTH  
OPEN TO TRAFFIC

- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE
- PAVEMENT STRUCTURE IS THE SAME FOR GIANT CITY ROAD THRU THE INTERSECTION, SEE PLAN VIEW. ALL CONSTRUCTION IS TO MATCH EXISTING PAVEMENT JOINTS AS DIRECTED BY THE ENGINEER.

RIGHT TURN LANE  
GIANT CITY SOUTH  
OPEN TO TRAFFIC

# STAGE 5C TYPICAL SECTION

EASTBOUND IL 13 NEAR GIANT CITY ROAD (FAP 331)  
(NOT TO SCALE)



BOTH LEFT TURN LANES  
GIANT CITY NORTH  
OPEN TO TRAFFIC

- SEE PLAN VIEW FOR PAINTED & CONCRETE MEDIAN LOCATIONS BETWEEN THE TRAVEL LANES AND THE RIGHT TURN LANE
- PAVEMENT STRUCTURE IS THE SAME FOR GIANT CITY ROAD THRU THE INTERSECTION, SEE PLAN VIEW. ALL CONSTRUCTION IS TO MATCH EXISTING PAVEMENT JOINTS AS DIRECTED BY THE ENGINEER.

RIGHT TURN LANE  
GIANT CITY SOUTH  
OPEN TO TRAFFIC

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 12/11/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GIANT CITY ROAD STAGING  
TYPICALS: SHEET 2 OF 2

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	*	JACKSON	23	22
CONTRACT NO. 78421				

Drop-off Location	Normal Posted Speed	Drop-off Height (x) and Type	Physical Treatment <sup>(1)(2)</sup>	Additional Requirements
Between Lanes	≥ 45 mph	≤ 1 in lift difference or ≤ 1 in vertical milled face	None	None
		1 in < x ≤ 2 in lift difference or 1 in < x ≤ 1.5 in vertical milled face	None	Uneven lane signs (2 mile spacing on Interstate & Expressway) (1 mile spacing on rural highway) (Spacing as per the TCP on urban sections)
		2 in < x ≤ 4 in lift difference	Notched longitudinal wedge <sup>(4)</sup>	As per lane closure standard
		1.5 in < x ≤ 4 in vertical milled face	Temporary wedge or milled sloped edge min 1:3 <sup>(4)</sup>	
		4 in < x ≤ 12 in <sup>(3)</sup>	Lane closure using channelizing devices	
		> 12 in	Lane closure using temporary traffic barrier	
Between Lanes	< 45 mph	≤ 1 in lift difference or ≤ 1.5 in vertical milled face	None	None
		1 in < x ≤ 2.5 in lift difference	None	Uneven lane signs
		2.5 in < x ≤ 4 in lift difference	Notched longitudinal wedge <sup>(4)</sup>	
		1.5 in < x ≤ 4 in vertical milled face	Temporary or milled sloped edge min of 1:3 <sup>(4)</sup>	As per lane closure standard
		4 in < x ≤ 12 in <sup>(3)</sup>	Lane closure using channelizing devices	
		> 12 in	Lane closure using temporary traffic barrier	

Notes:

- <sup>(1)</sup> Place channelizing devices and/or temporary barrier at the same level as the traveling lane.
- <sup>(2)</sup> Channelizing devices may be placed at the drop-off elevation to preserve lane width. Raise the reflective area and warning light (if required) to the required elevation above the traveling lane as per Highway Standard 701901.
- <sup>(3)</sup> Drop-off 4 in < x ≤ 12 in is permitted for less than 0.5 mile length of drop-off exposure in work zone or less than 48-hour closure time. Length and duration of drop-off in excess of limits require temporary traffic barrier. Adjacent work spaces that are essentially continuous in drop-off exposure should be considered as one work zone.
- <sup>(4)</sup> Or the same physical treatment and sign requirements as 4 in < x ≤ 12 in.

**CONDITION 1 — DROP-OFF BETWEEN TRAVELED LANES**  
(Excludes Pavement Patching) (US Customary)

Figure 55-2.A

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55-2.7

Drop-off Location	Normal Posted Speed	Drop-off Height (x) and Type	Treatment Required
≤ 3 ft <sup>(1)(2)</sup>	all	≤ 1 in	None
		1 in < x ≤ 3 in	Low Shoulder signs (2 mile spacing)
	< 45 mph	3 in < x ≤ 18 in	Place channelizing devices a. 50-ft spacing
	≥ 45 mph	3 in < x ≤ 12 in	Place channelizing devices at 100-ft spacing
	< 45 mph	18 in < x ≤ 24 in for < 0.5 mile or < 48 hours <sup>(4)</sup>	Place channelizing devices a. 50-ft spacing
	≥ 45 mph	12 in < x ≤ 18 in for < 0.5 mile or < 48 hours	Place channelizing devices at 100-ft spacing
	≥ 45 mph	12 in < x ≤ 24 in for > 0.5 mile or > 48 hours	Closure using temporary traffic barrier
	< 45 mph	18 in < x ≤ 24 in <sup>(5)</sup>	Closure using temporary traffic barrier
	≥ 45 mph	18 in < x ≤ 24 in for < 0.5 mile or < 48 hours <sup>(5)</sup>	Closure using temporary traffic barrier
	all	> 24 in <sup>(5)</sup>	Closure using temporary traffic barrier
3 ft < x ≤ 8 ft <sup>(3)</sup>	all	≤ 1 in	none
		1 in < x ≤ 3 in	Low Shoulder signs (2-mile spacing)
	< 45 mph	3 in < x ≤ 24 in <sup>(4)</sup>	Place channelizing devices at 50-ft spacing
	≥ 45 mph	3 in < x ≤ 24 in	Place channelizing devices a. 100-ft spacing
all	24 in <sup>(5)</sup>	Closure using temporary traffic barrier	
> 8 ft to the work zone clear zone <sup>(3)</sup>	< 45 mph	12 in < x ≤ 24 in <sup>(4)</sup>	Place channelizing devices at 50-ft spacing
	≥ 45 mph	12 in < x ≤ 24 in	Place channelizing devices at 100-ft spacing
	all	> 24 in <sup>(5)</sup>	Closure using temporary traffic barrier

Notes:

- <sup>(1)</sup> Place channelizing devices and/or temporary barrier at the same level as the traveling lane or shoulder profile.
- <sup>(2)</sup> Channelizing devices may be placed at the drop-off elevation to preserve lane width. Raise the reflective area and warning light (if required) to the elevation above the traveling lane or shoulder profile as per Highway Standard 701901.
- <sup>(3)</sup> Place channelizing devices or temporary barrier at same level as the side slope profile to be fully visible.
- <sup>(4)</sup> Length and duration may be exceeded for urban areas when engineering judgment indicates sight distance will be adversely affected by temporary barrier.
- <sup>(5)</sup> Temporary traffic barrier may be eliminated for stationary operations of less than 24 hours for multi-lane, and may be eliminated for stationary operations of less than 96 hours per stage for two lanes, based on engineering judgment.

**CONDITION 2 — DROP-OFF BETWEEN TRAVEL LANE AND SHOULDER/EDGE OF PAVEMENT (US Customary)**

Figure 55-2.B

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55-2.9

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GIANT CITY ROAD STAGING PLAN SHEET 1 OF 2</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\left\lft\chd\ld0398565\78421-sht-plen.dgn		DRAWN -	REVISED -			331	.	JACKSON	23	23
PLOT SCALE = 100.0000' / 1 in.		CHECKED -	REVISED -			CONTRACT NO. 78421				
PLOT DATE = 12/11/2014		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				