

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.

NOTE:
PRIOR TO INSTALLATION OF THE TEMPORARY "WIRELESS" RADIO INTERCONNECT, THE CONTRACTOR SHALL TEST BETWEEN THE INTERSECTIONS SHOWN ON THE PLANS FOR WIRELESS COMMUNICATIONS AND BE FOUND TO BE ACCEPTABLE BEFORE DECIDING ON THE TYPE OF INTERCONNECT TO BE USED.

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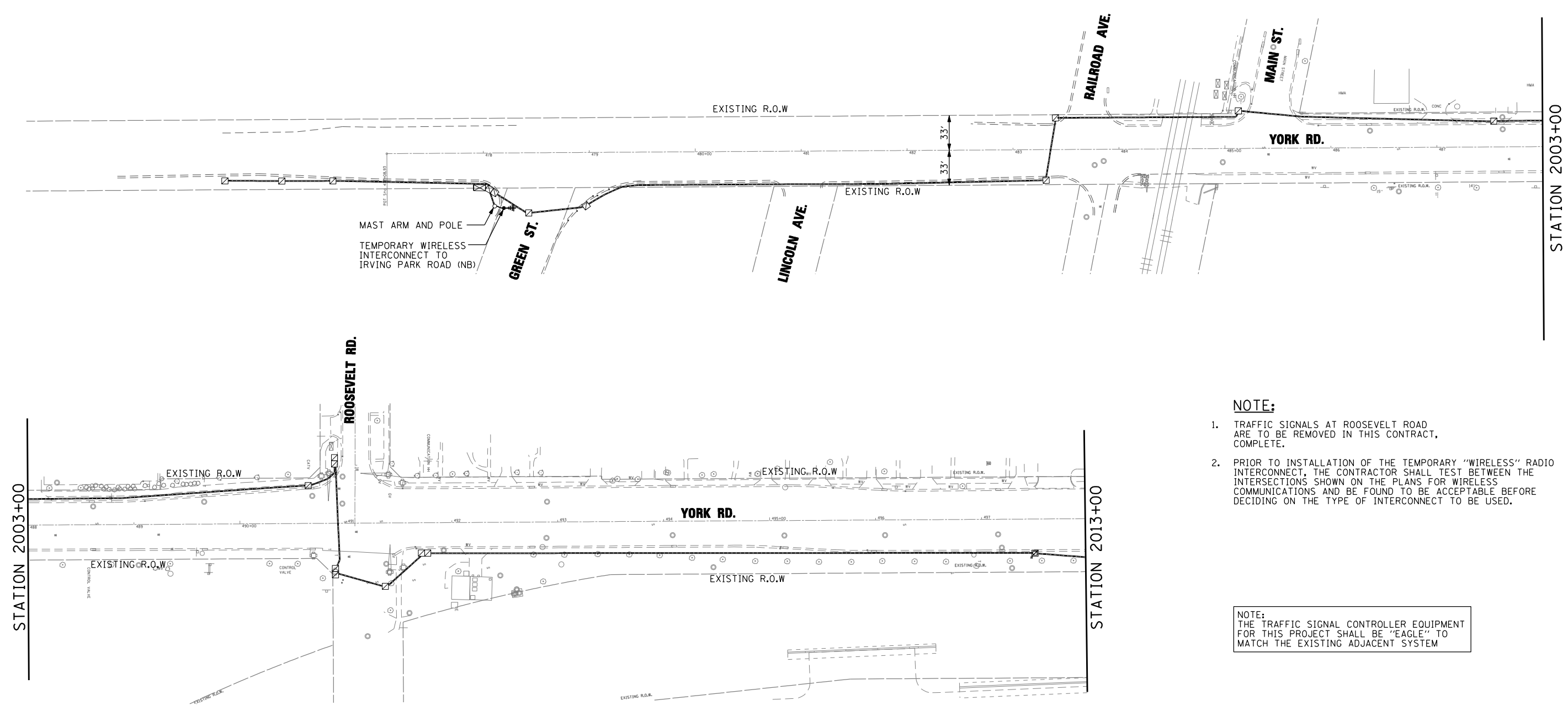
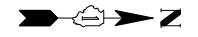
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD**

SCALE: 1"=50' SHEET NO. 20 OF 36 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	301
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



NOTE:

1. TRAFFIC SIGNALS AT ROOSEVELT ROAD ARE TO BE REMOVED IN THIS CONTRACT, COMPLETE.
2. PRIOR TO INSTALLATION OF THE TEMPORARY "WIRELESS" RADIO INTERCONNECT, THE CONTRACTOR SHALL TEST BETWEEN THE INTERSECTIONS SHOWN ON THE PLANS FOR WIRELESS COMMUNICATIONS AND BE FOUND TO BE ACCEPTABLE BEFORE DECIDING ON THE TYPE OF INTERCONNECT TO BE USED.

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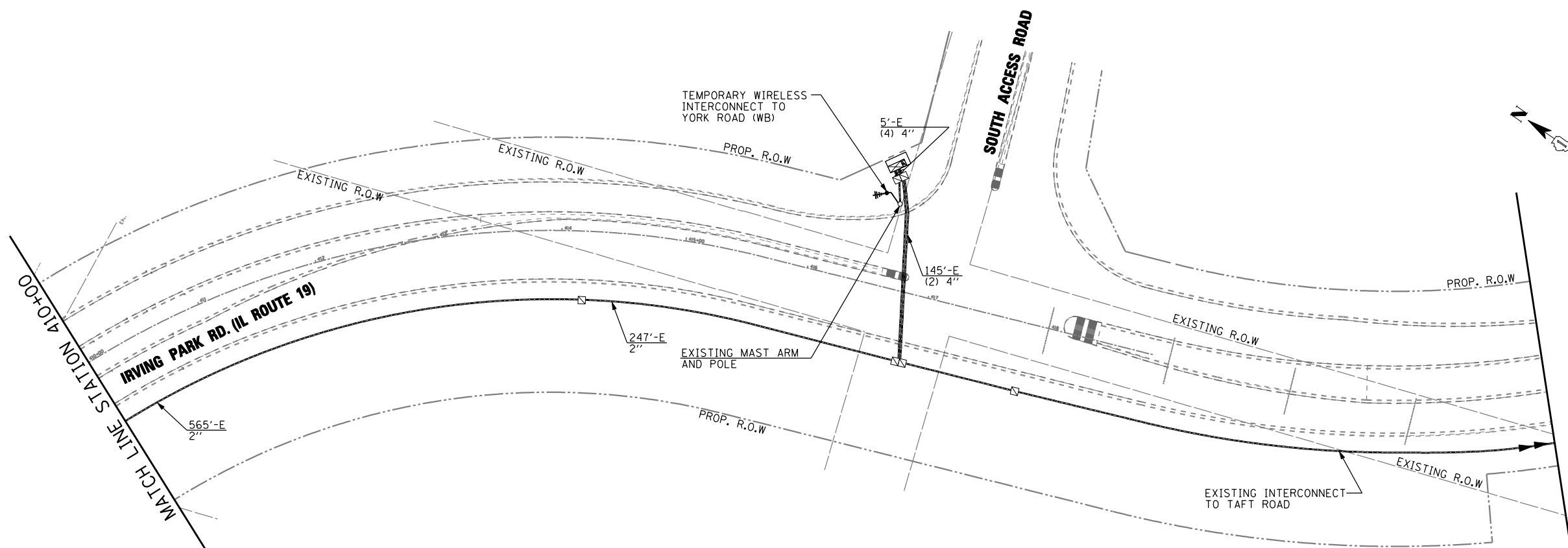
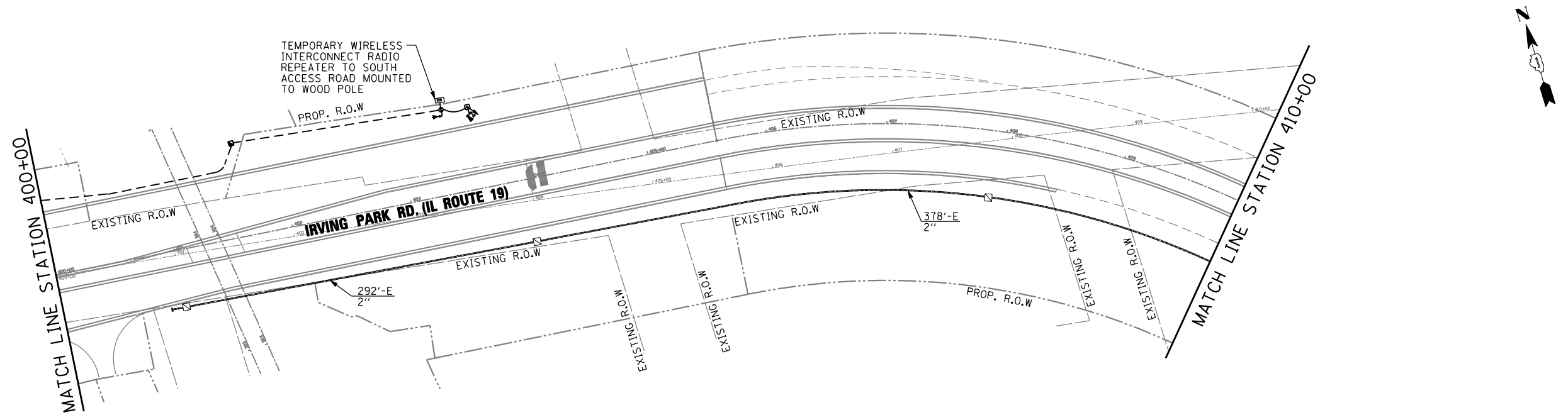
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT PLAN
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD

SCALE: SHEET NO. 21 OF 36 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	302
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

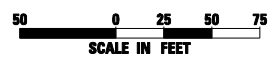


NOTE:

PRIOR TO INSTALLATION OF THE TEMPORARY "WIRELESS" RADIO INTERCONNECT, THE CONTRACTOR SHALL TEST BETWEEN THE INTERSECTIONS SHOWN ON THE PLANS FOR WIRELESS COMMUNICATIONS AND BE FOUND TO BE ACCEPTABLE BEFORE DECIDING ON THE TYPE OF INTERCONNECT TO BE USED.

NOTE:

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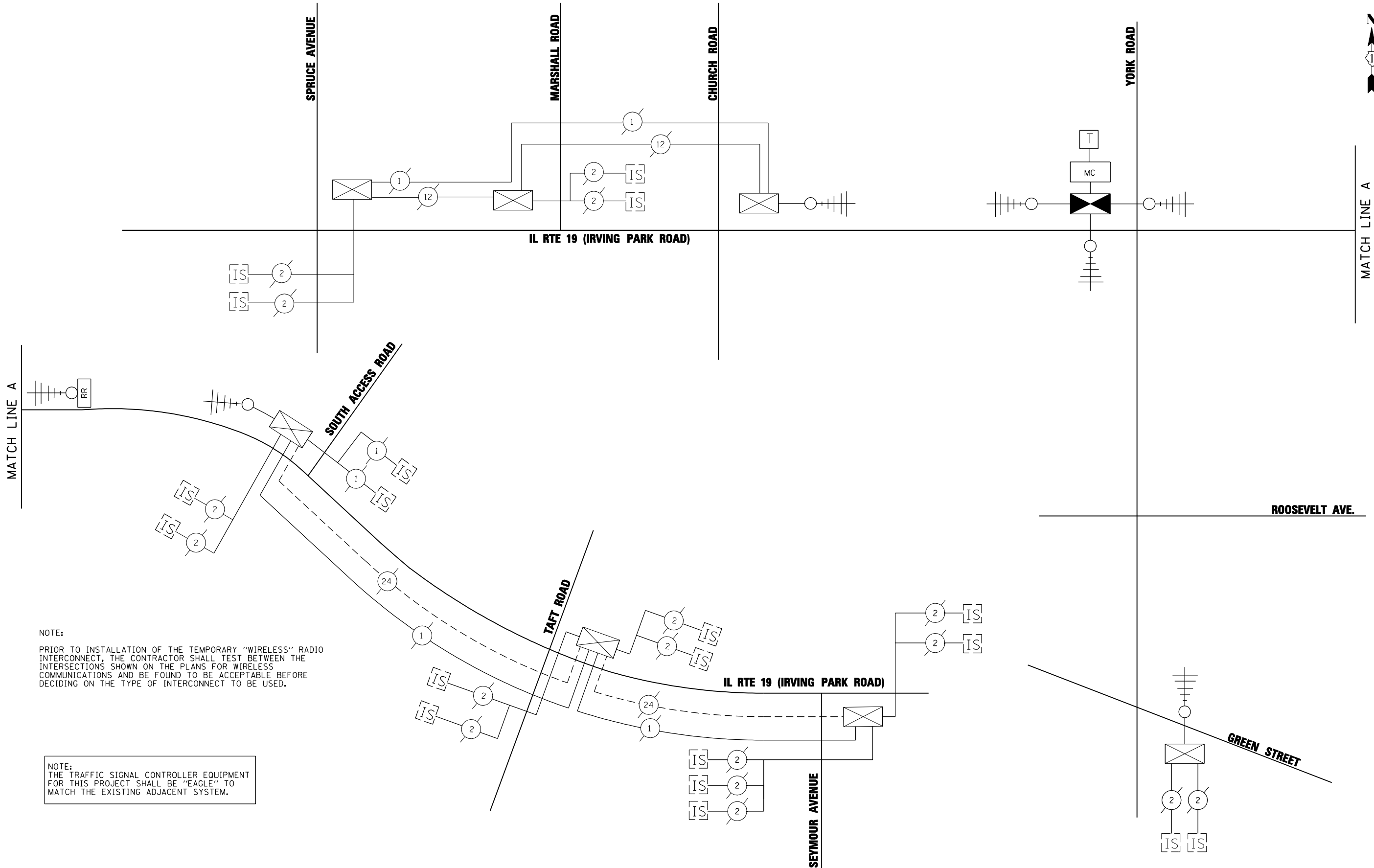
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	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD**

SCALE: 1"=50' SHEET NO. 22 OF 36 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	303
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



NOTE:
 PRIOR TO INSTALLATION OF THE TEMPORARY "WIRELESS" RADIO INTERCONNECT, THE CONTRACTOR SHALL TEST BETWEEN THE INTERSECTIONS SHOWN ON THE PLANS FOR WIRELESS COMMUNICATIONS AND BE FOUND TO BE ACCEPTABLE BEFORE DECIDING ON THE TYPE OF INTERCONNECT TO BE USED.

NOTE:
 THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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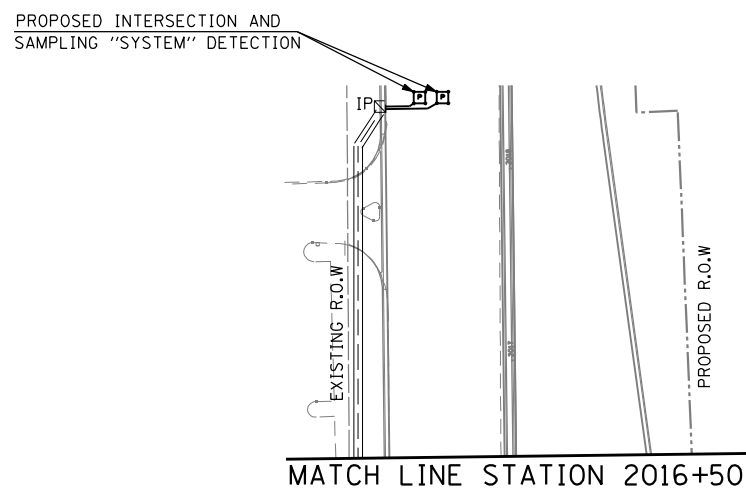
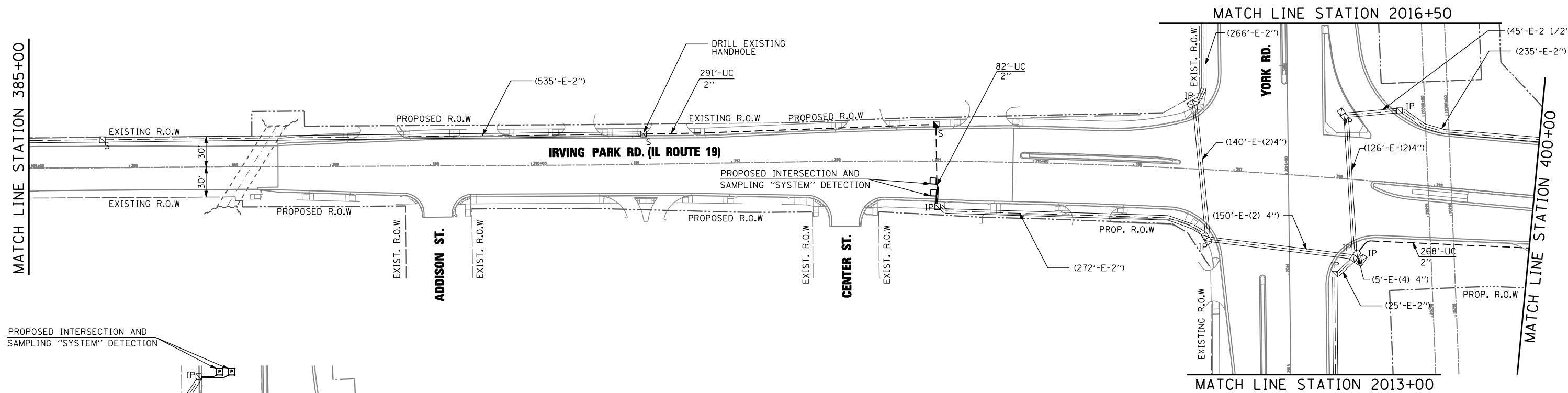
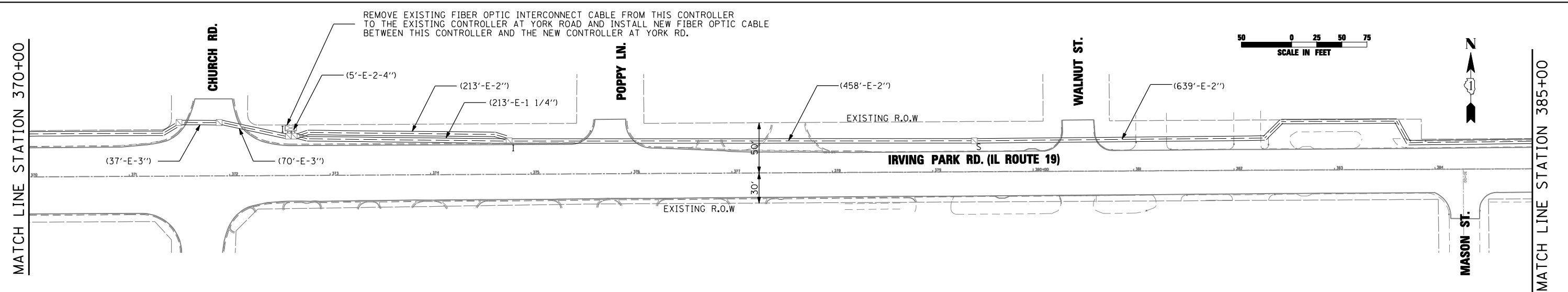
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT SCHEMATIC
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD

SCALE: NTS SHEET NO. 23 OF 36 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	304
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



NOTE:
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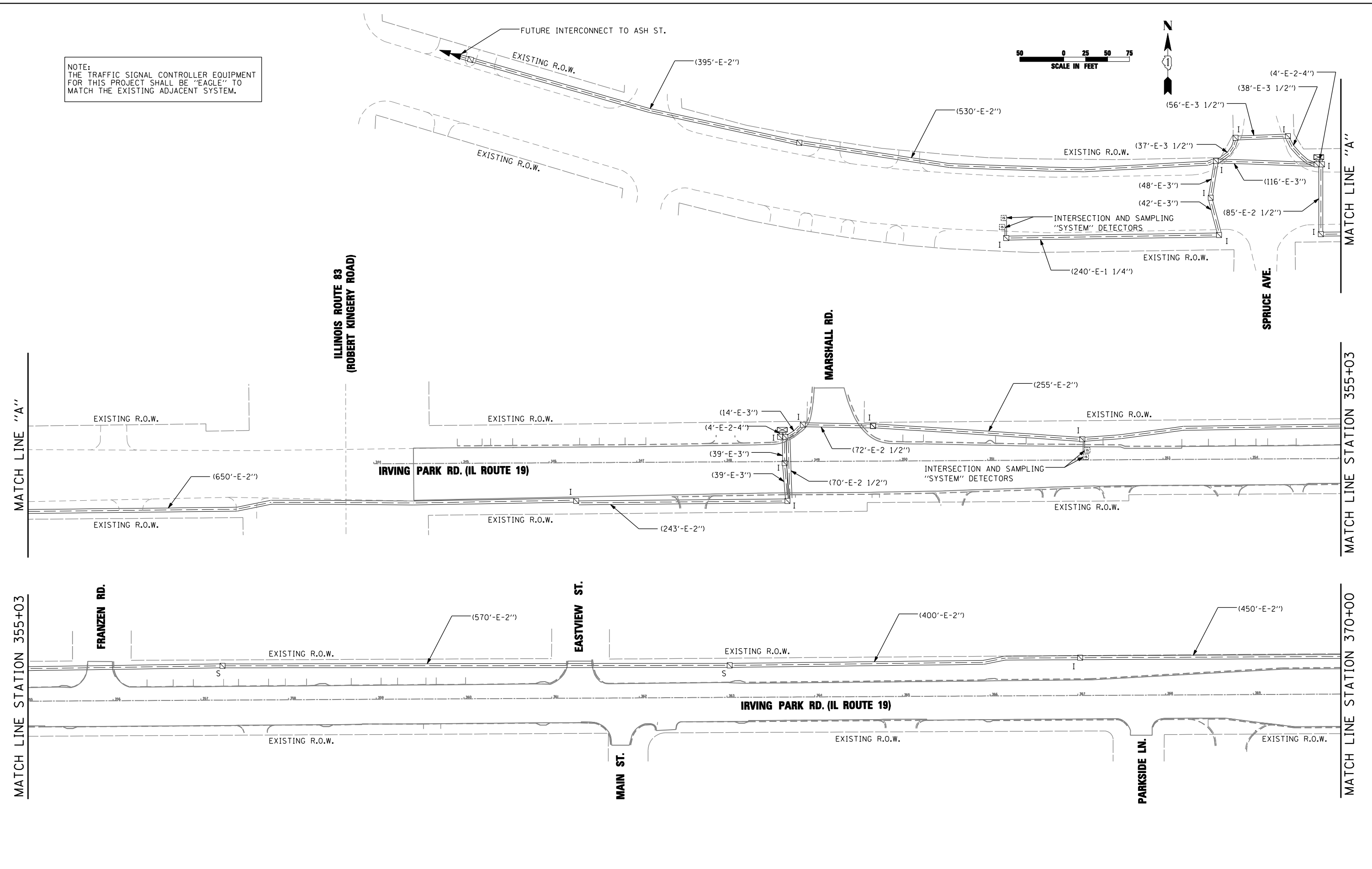
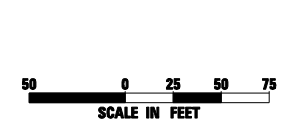
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERCONNECT PLAN	
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD	
SCALE: 1"=50'	SHEET NO. 24 OF 36 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	305
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT
FOR THIS PROJECT SHALL BE "EAGLE" TO
MATCH THE EXISTING ADJACENT SYSTEM.



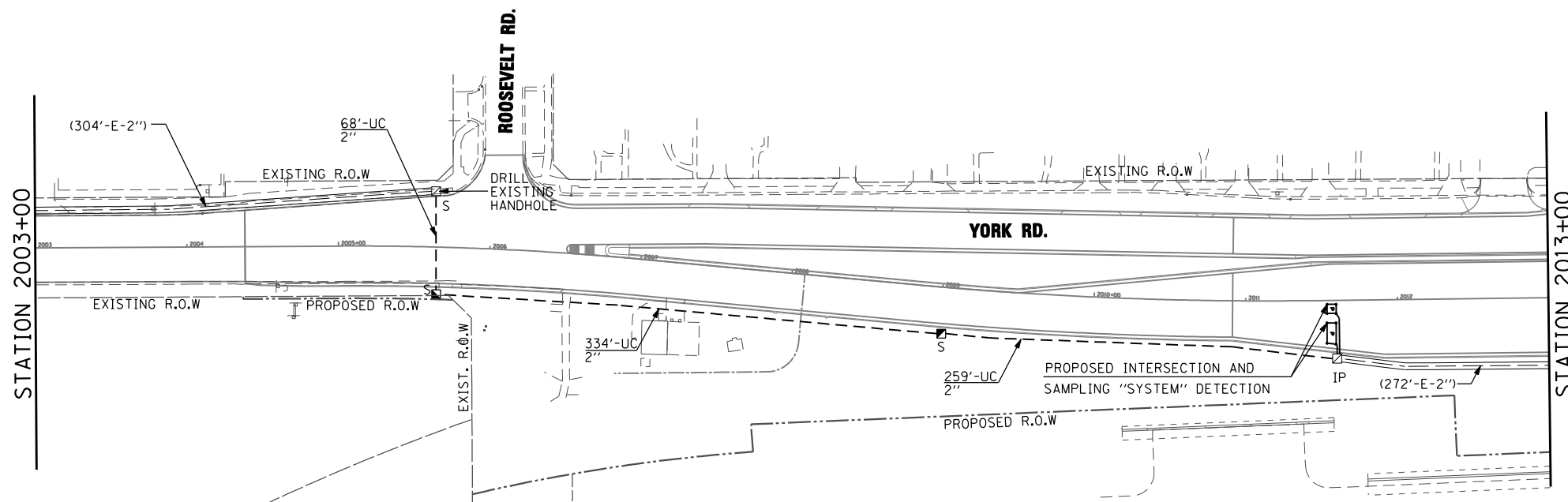
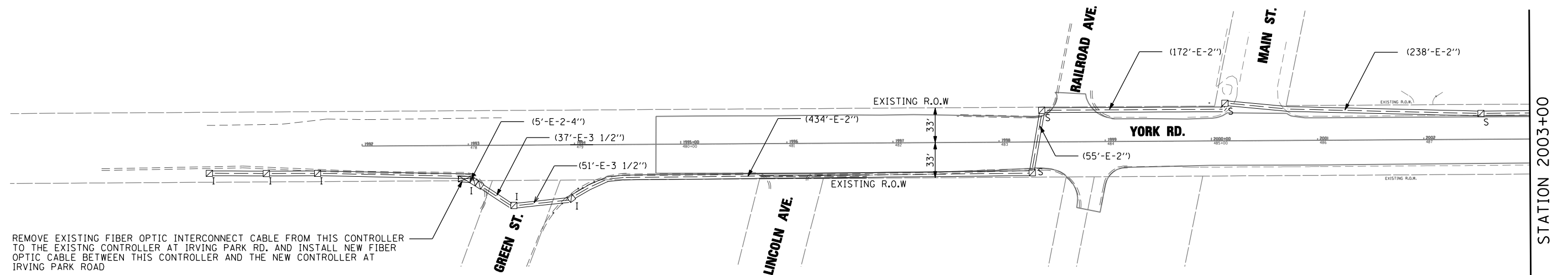
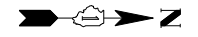
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PLOT SCALE = 100.0000' / IN.	CHECKED - RP	REVISED -
PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT PLAN
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD**
SCALE: 1"=50' SHEET NO. 25 OF 36 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	306
CONTRACT NO. 60B42			ILLINOIS FED. AID PROJECT	



NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM

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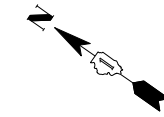
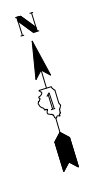
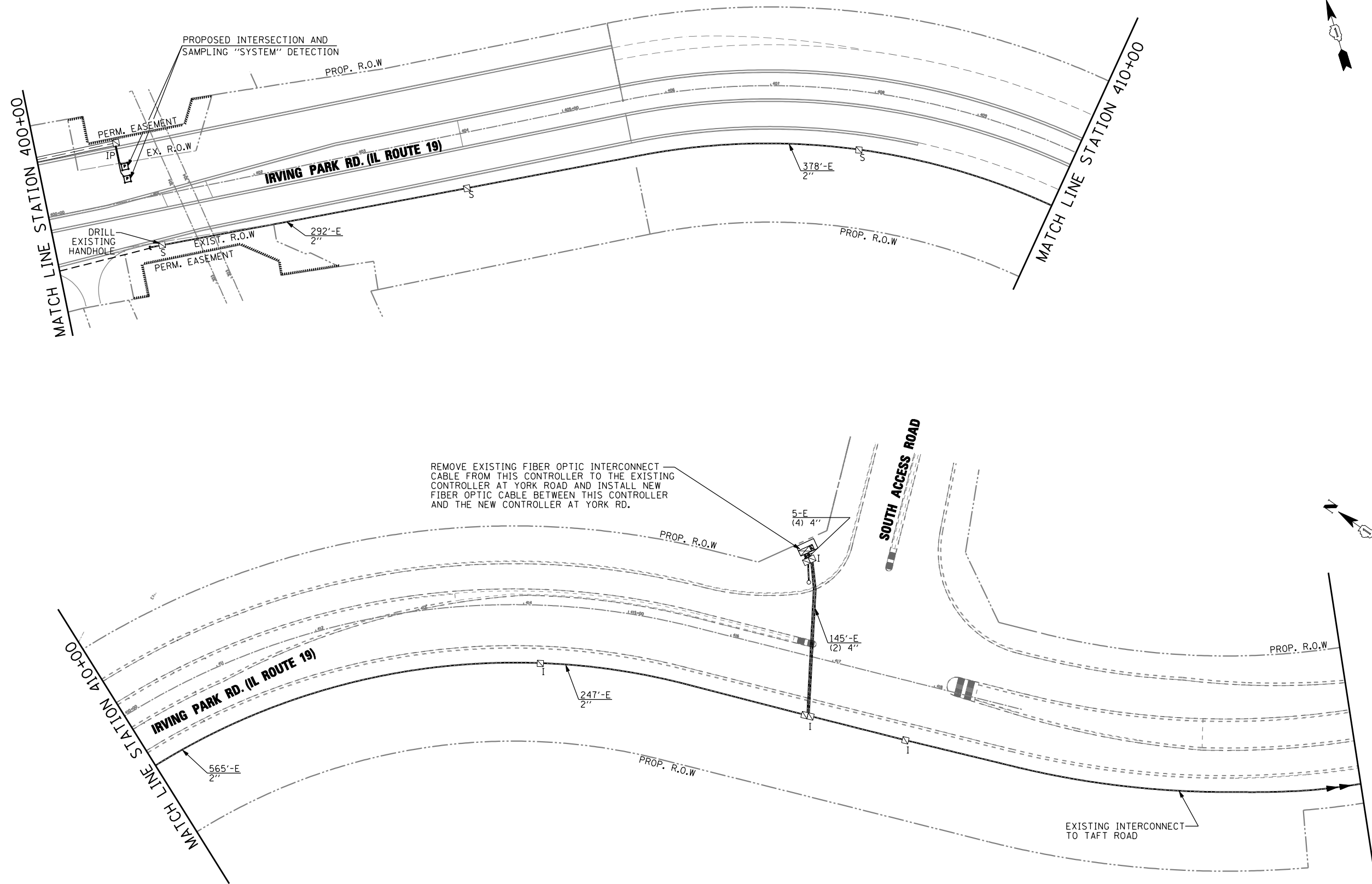
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERCONNECT PLAN
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD

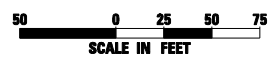
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	307
CONTRACT NO. 60B42			ILLINOIS FED. AID PROJECT	



REMOVE EXISTING FIBER OPTIC INTERCONNECT CABLE FROM THIS CONTROLLER TO THE EXISTING CONTROLLER AT YORK ROAD AND INSTALL NEW FIBER OPTIC CABLE BETWEEN THIS CONTROLLER AND THE NEW CONTROLLER AT YORK RD.

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.



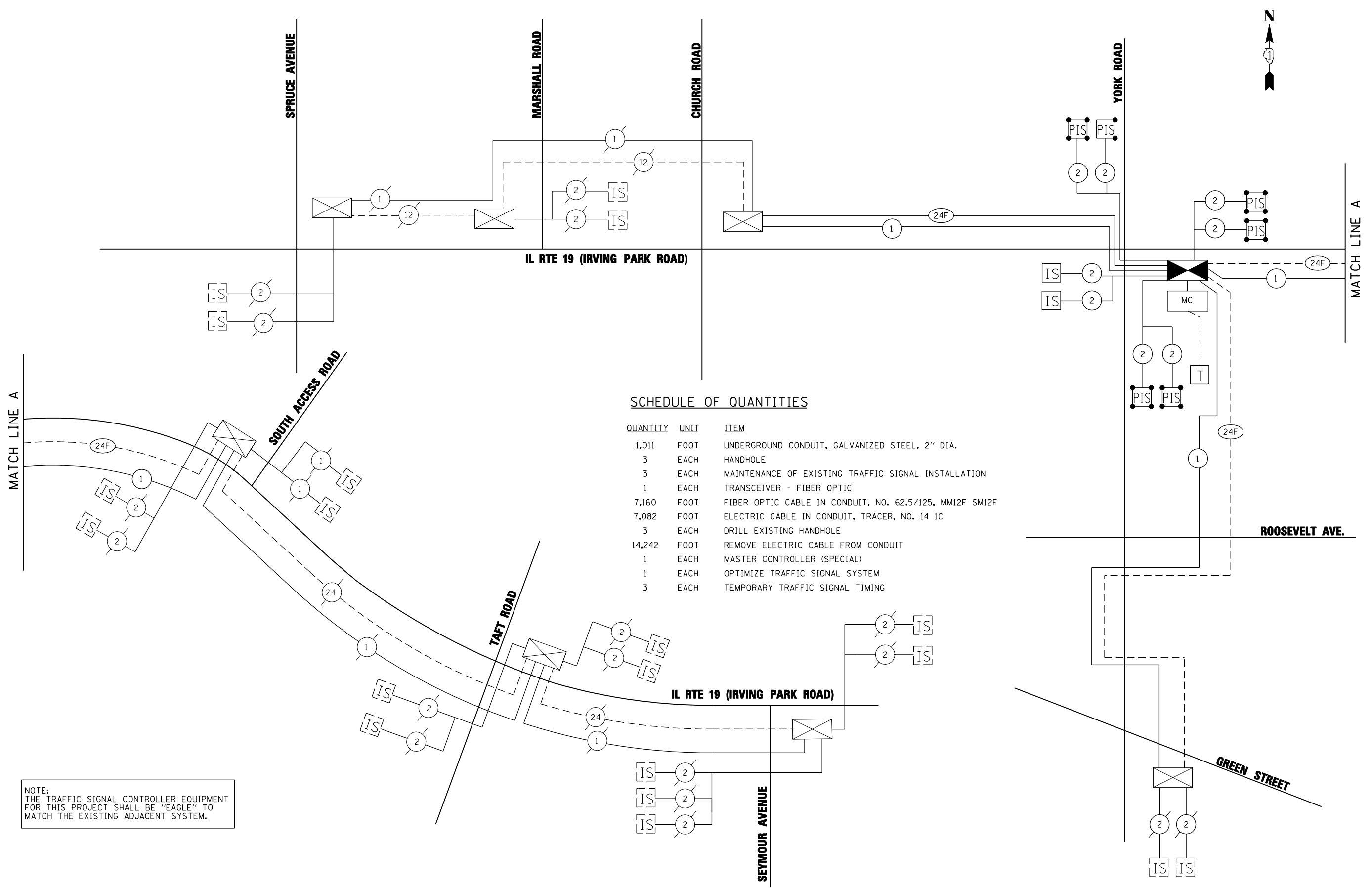
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERCONNECT PLAN
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD
SCALE: 1"=50' SHEET NO. 27 OF 36 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	308
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



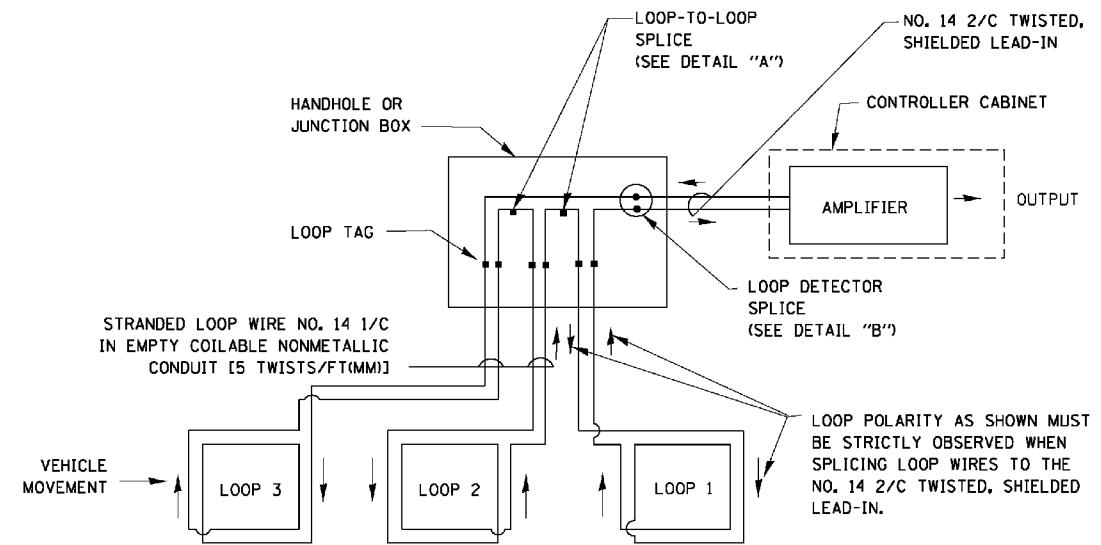
SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
1,011	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
3	EACH	HANDHOLE
3	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	TRANSCEIVER - FIBER OPTIC
7,160	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F
7,082	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C
3	EACH	DRILL EXISTING HANDHOLE
14,242	FOOT	REMOVE ELECTRIC CABLE FROM CONDUIT
1	EACH	MASTER CONTROLLER (SPECIAL)
1	EACH	OPTIMIZE TRAFFIC SIGNAL SYSTEM
3	EACH	TEMPORARY TRAFFIC SIGNAL TIMING

NOTE:
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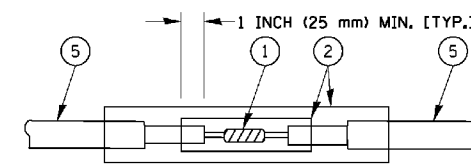
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

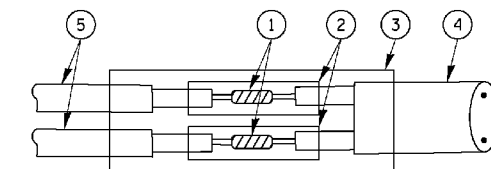


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

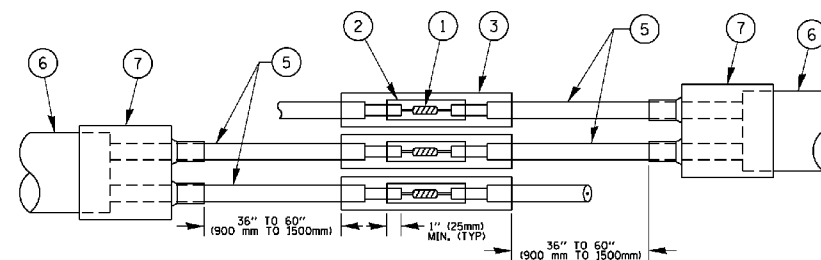


DETAIL "A"
LOOP-TO-LOOP SPLICE

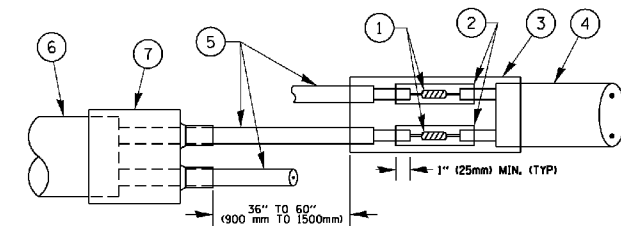


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



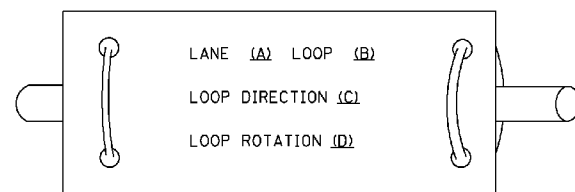
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

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	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STANDARD TRAFFIC SIGNAL DESIGN DETAILS						
SCALE: NONE		TS-05		CONTRACT NO.		
SHEET NO. 2 OF 7 SHEETS		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

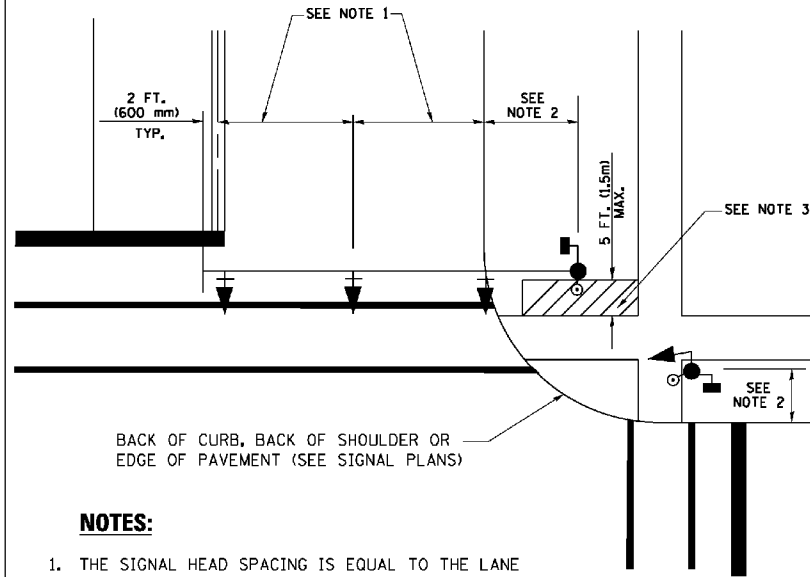
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STANDARD TRAFFIC SIGNAL DESIGN DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD						
SCALE:		32RS&N (12)		DUPAGE 495 310		
SHEET NO. 29 OF 36 SHEETS		CONTRACT NO. 60B42				
		ILLINOIS FED. AID PROJECT				

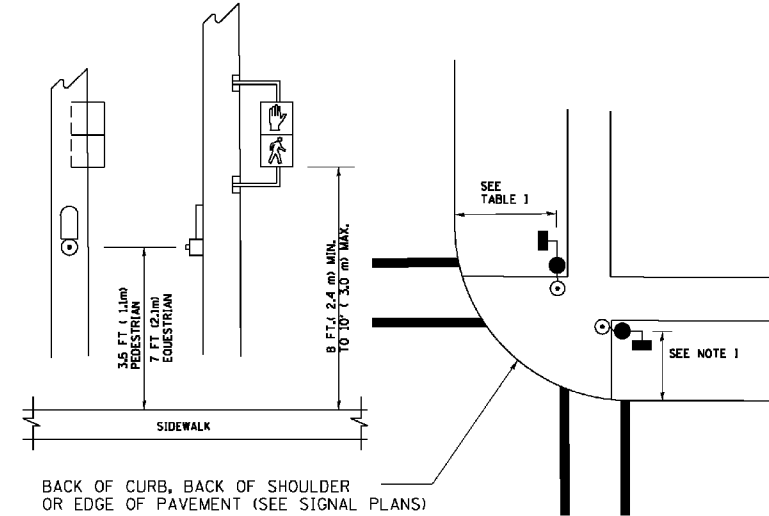
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

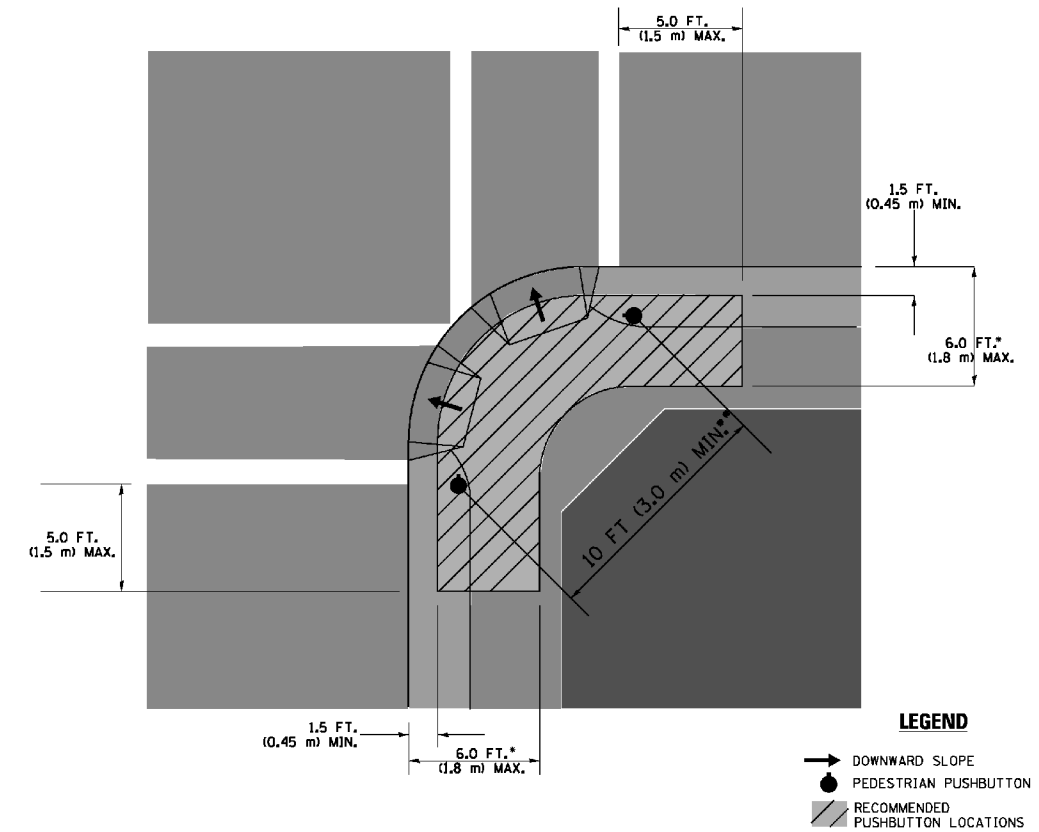
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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PLOT DATE = 1/13/2014		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

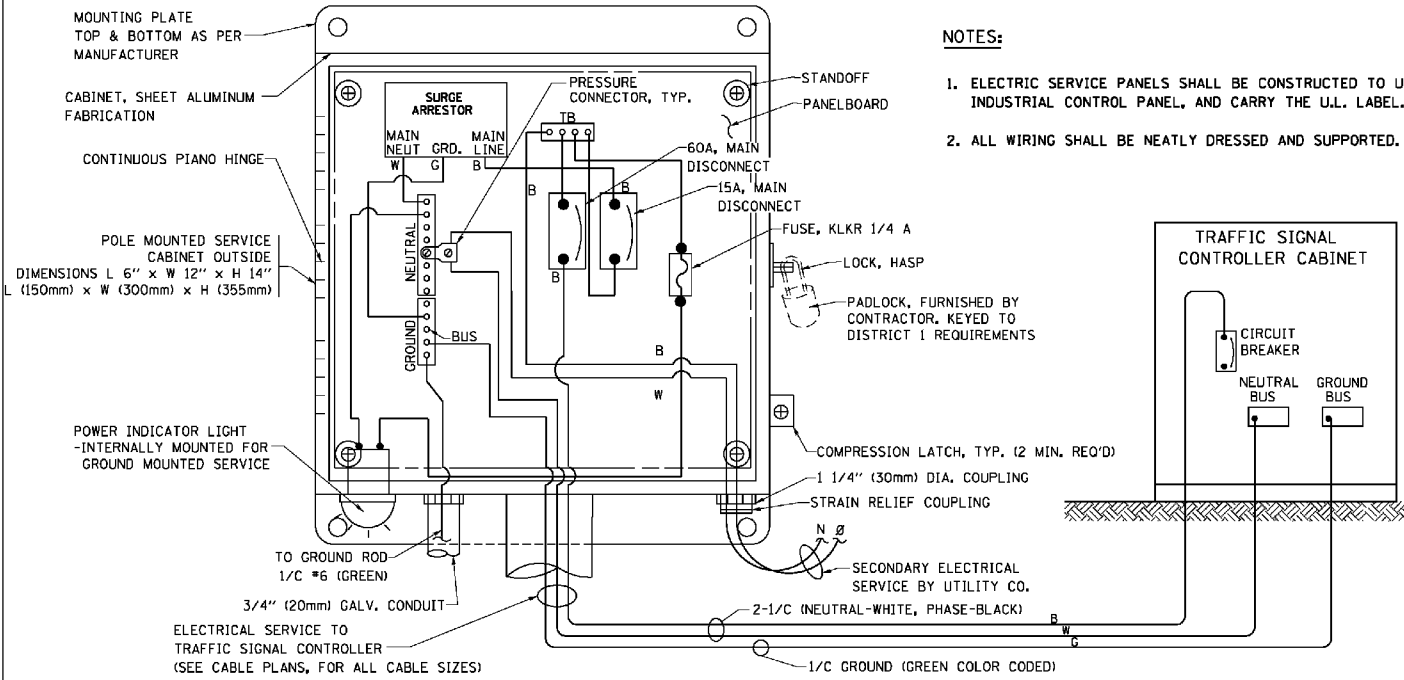
DISTRICT ONE				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STANDARD TRAFFIC SIGNAL DESIGN DETAILS								
SCALE: NONE				SHEET NO. 3 OF 7 SHEETS		STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
				TS-05		CONTRACT NO.		

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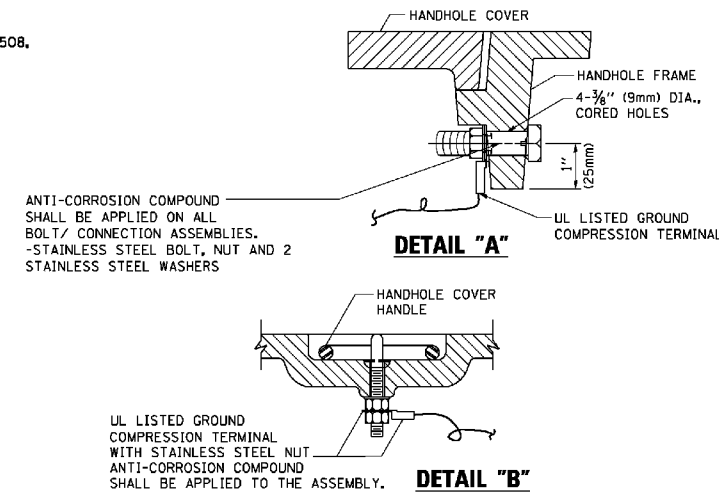
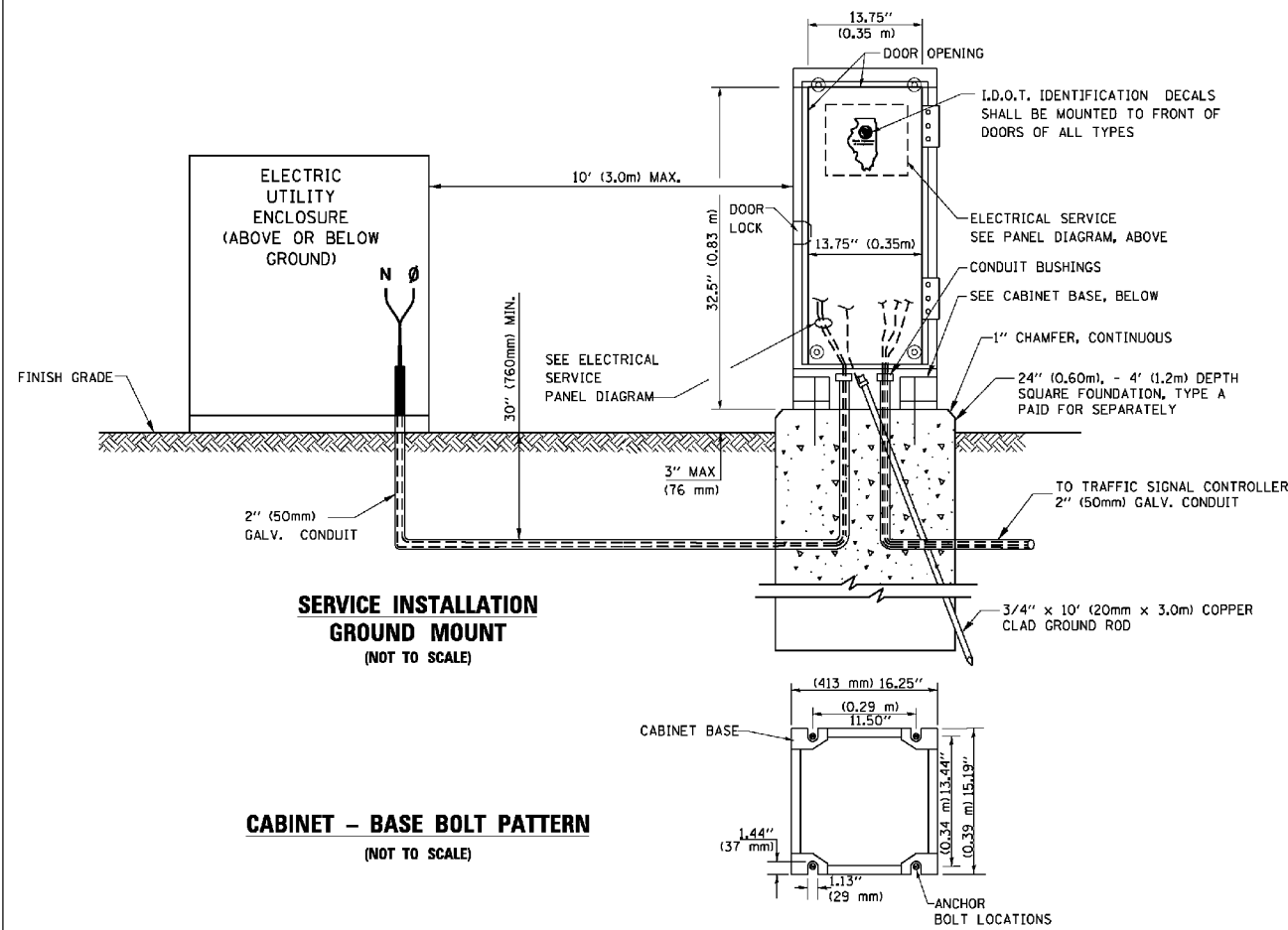
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STANDARD TRAFFIC SIGNAL DESIGN DETAILS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD								
SCALE:				SHEET NO. 30 OF 36 SHEETS		STA. TO STA.	ILLINOIS FED. AID PROJECT	
				1321		32RS&N (12)	DUPAGE	495 311
						CONTRACT NO. 60B42		

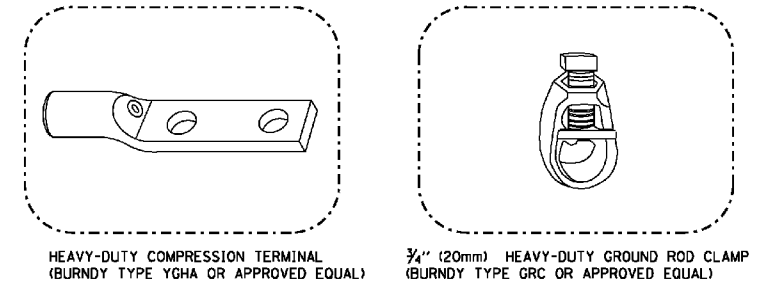


**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)**

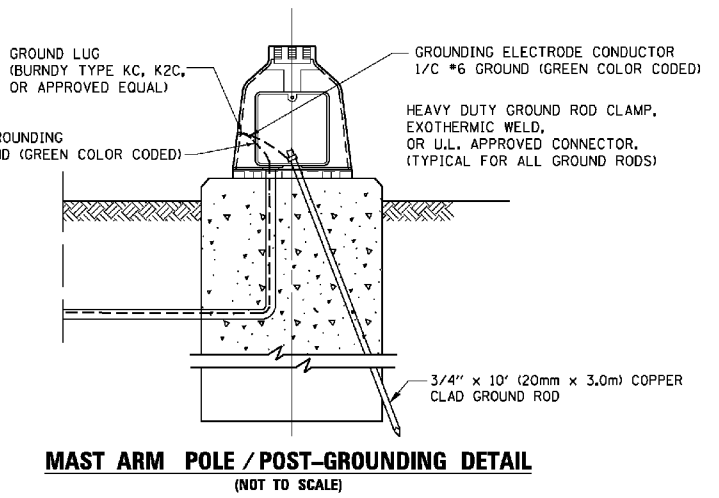
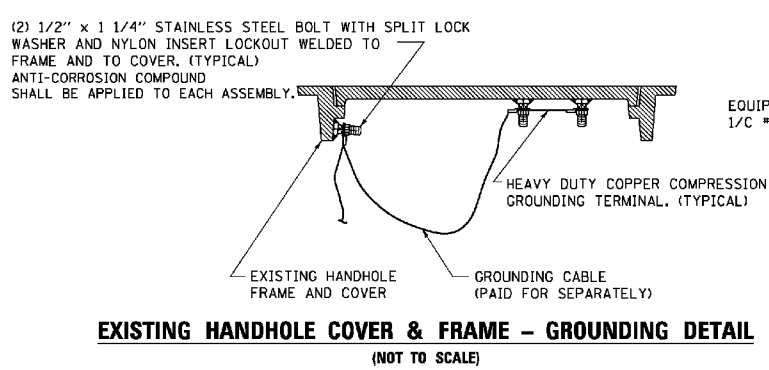
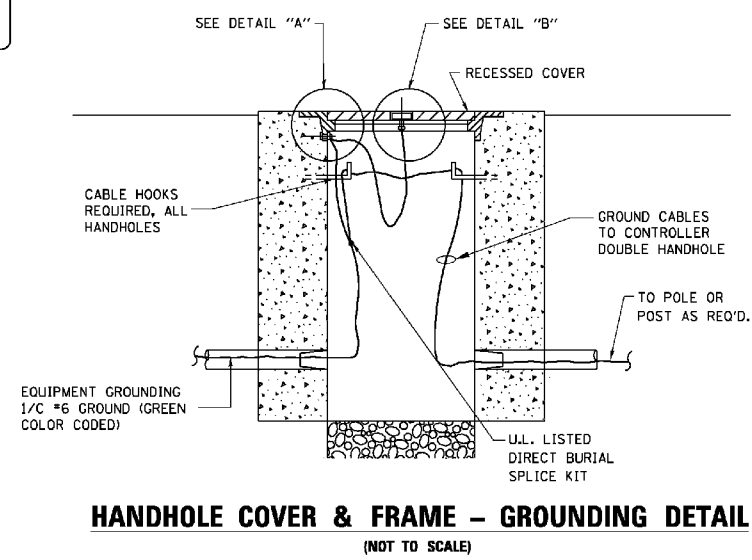


**NOTES:
GROUNDING SYSTEM**

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



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	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 4 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TS-05			CONTRACT NO.	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

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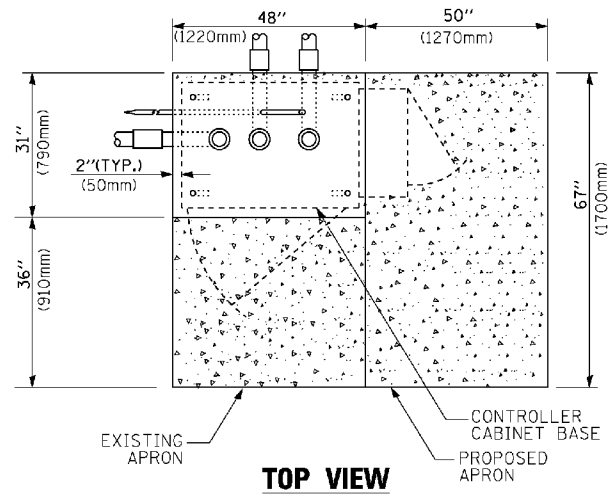
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

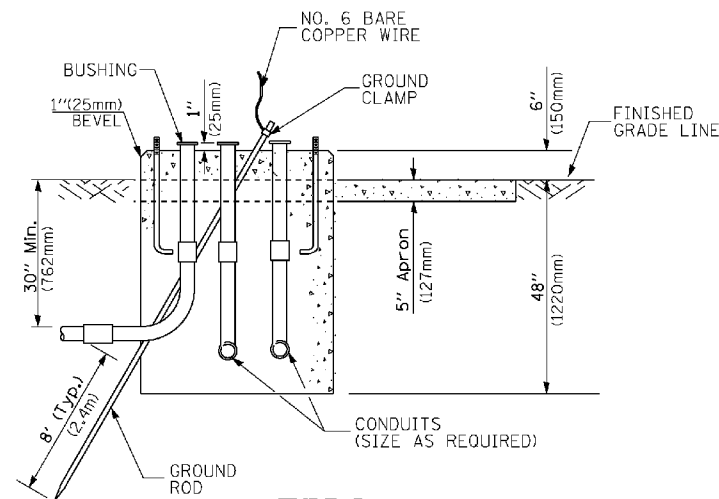
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD**

SCALE: SHEET NO. 31 OF 36 SHEETS STA. TO STA.

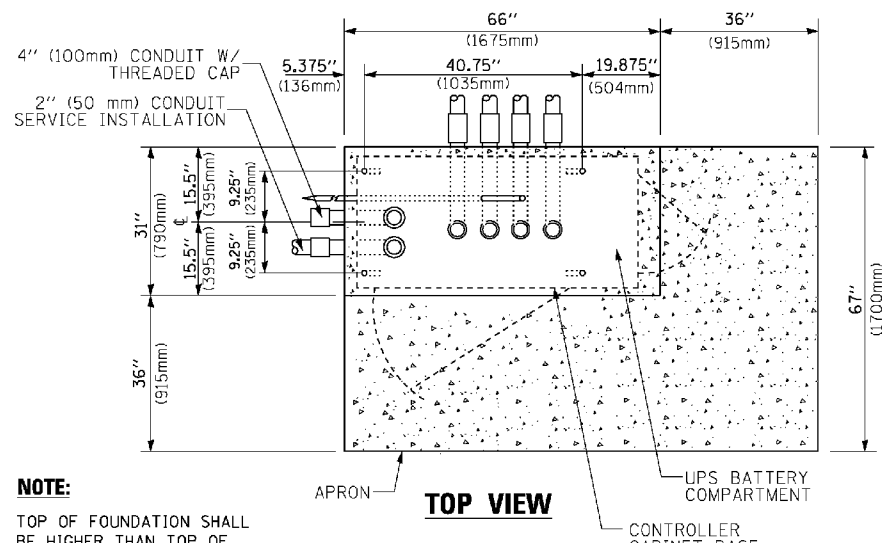
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	312
CONTRACT NO. 60B42			ILLINOIS FED. AID PROJECT	



TOP VIEW

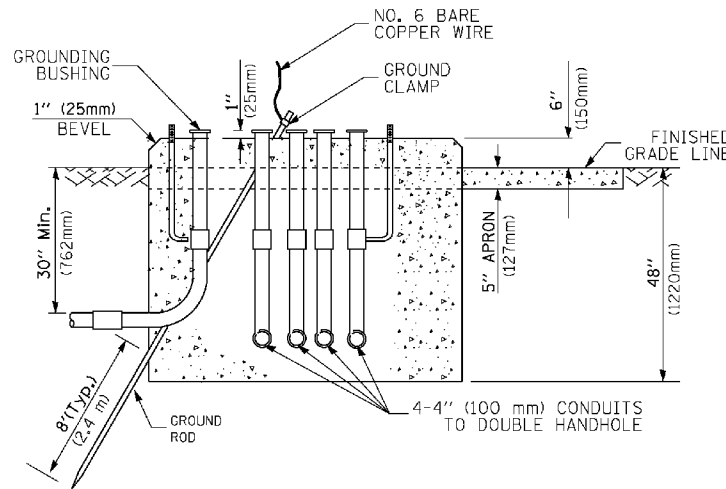


**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

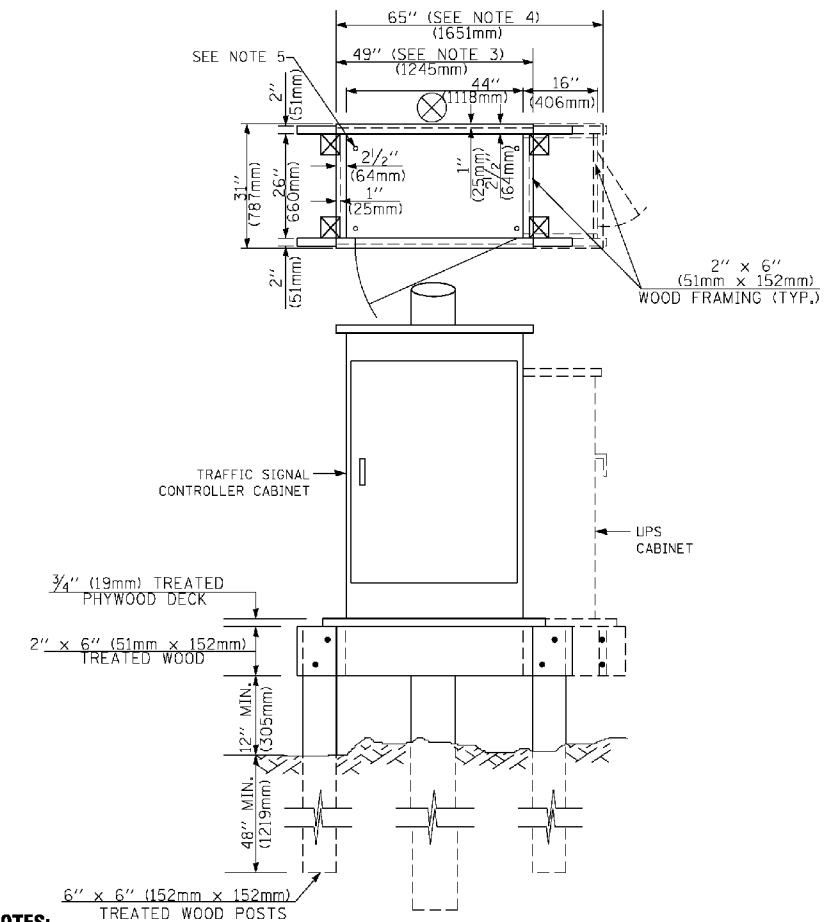


TOP VIEW

NOTE:
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

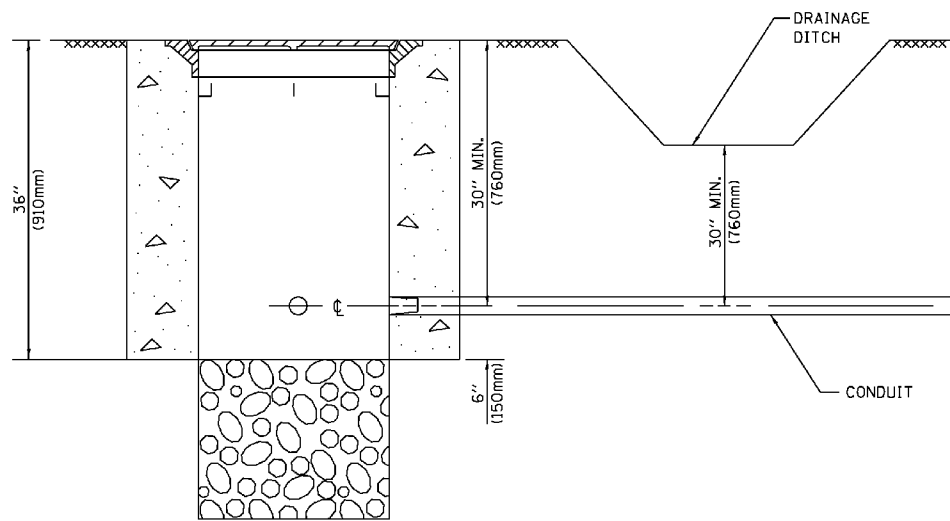
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	DATE - 12-19-2014	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STANDARD TRAFFIC SIGNAL DESIGN DETAILS		TS-05		CONTRACT NO.		
SCALE: NONE	SHEET NO. 5 OF 7 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD		1321	32RS&N (12)	DUPAGE	495	313
SCALE:	SHEET NO. 32 OF 36 SHEETS	STA.	TO STA.		CONTRACT NO. 60B42	
ILLINOIS FED. AID PROJECT						

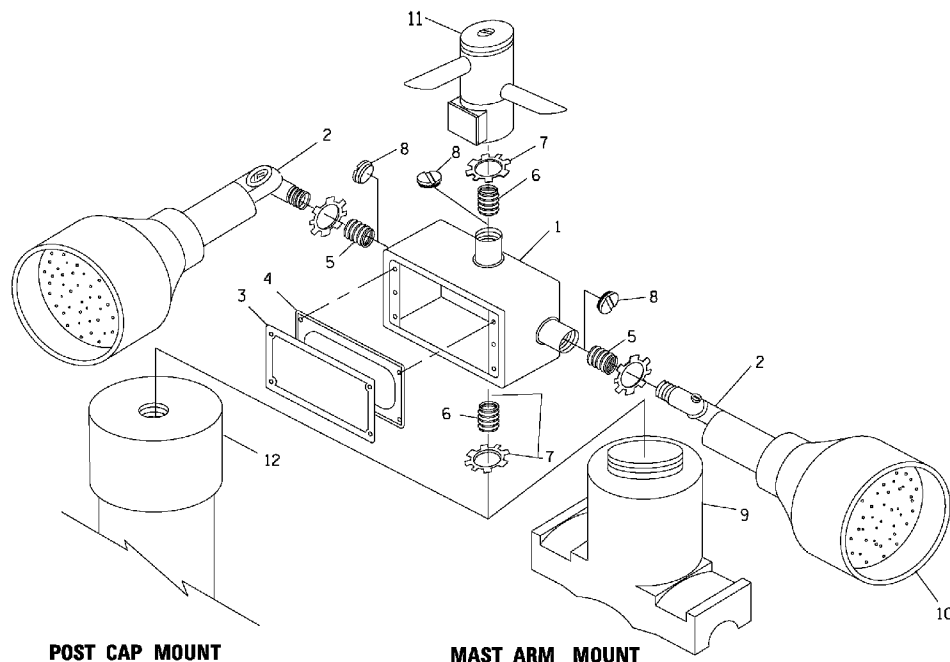
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NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

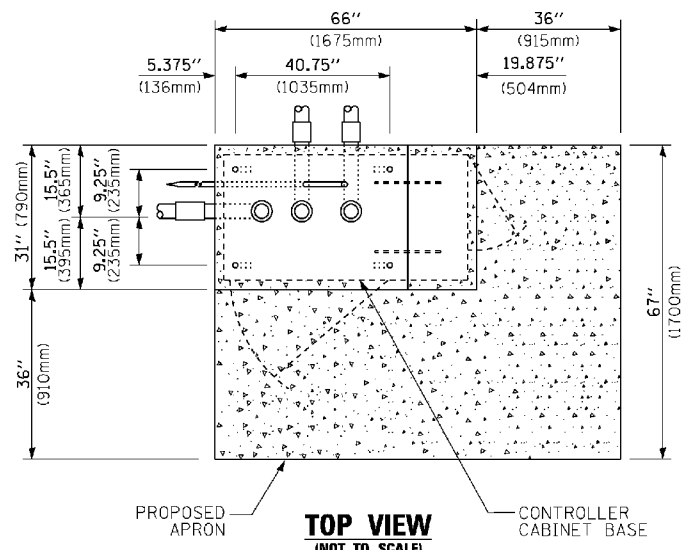
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



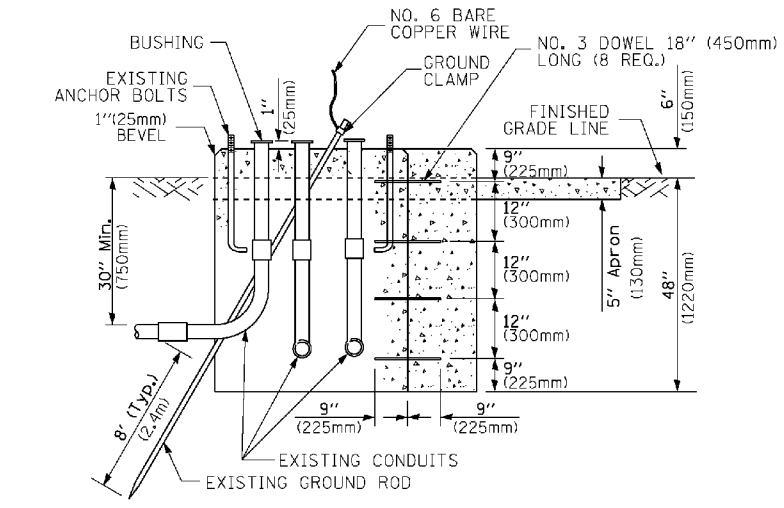
POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW
(NOT TO SCALE)

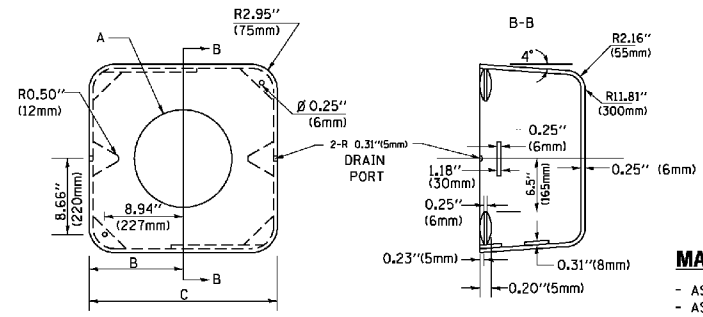


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

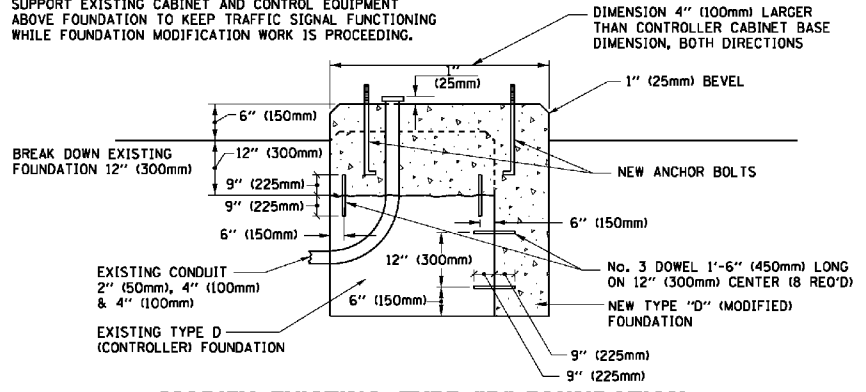
SHROUD

NOTES:

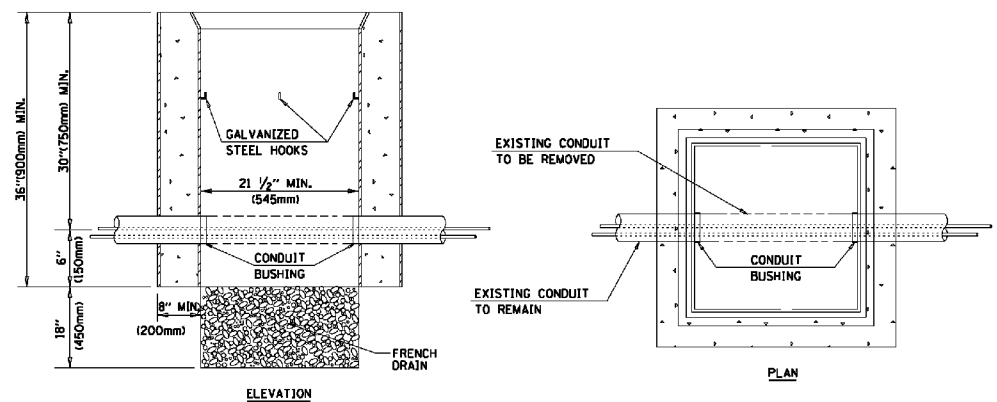
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS
SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TS-05			CONTRACT NO.	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

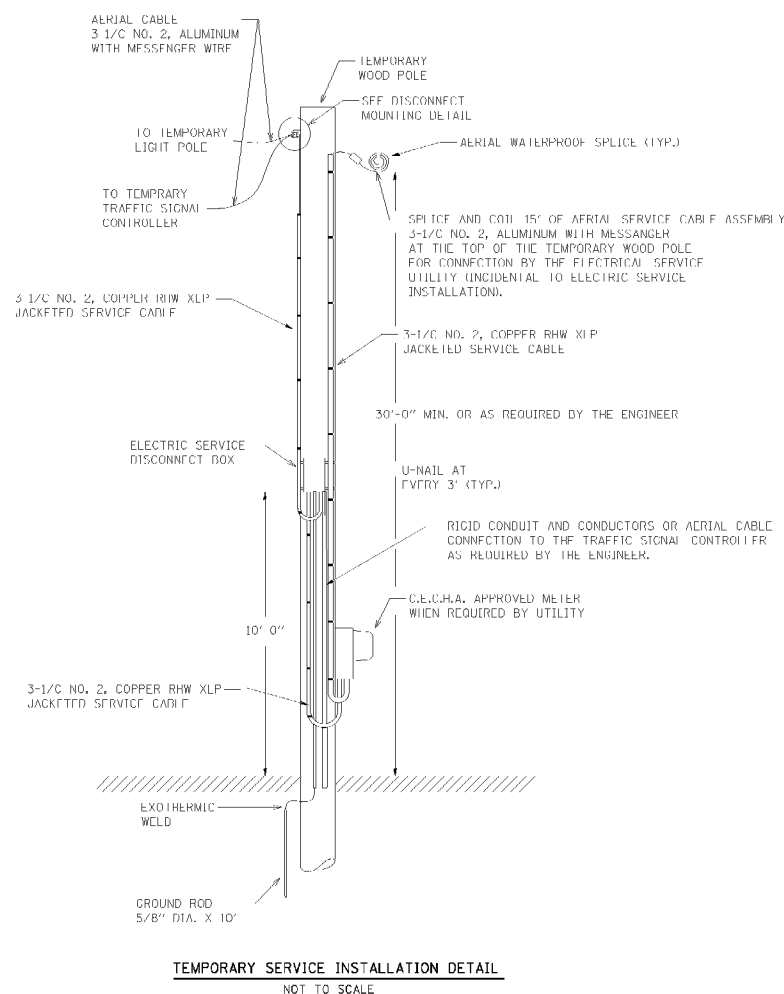
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PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

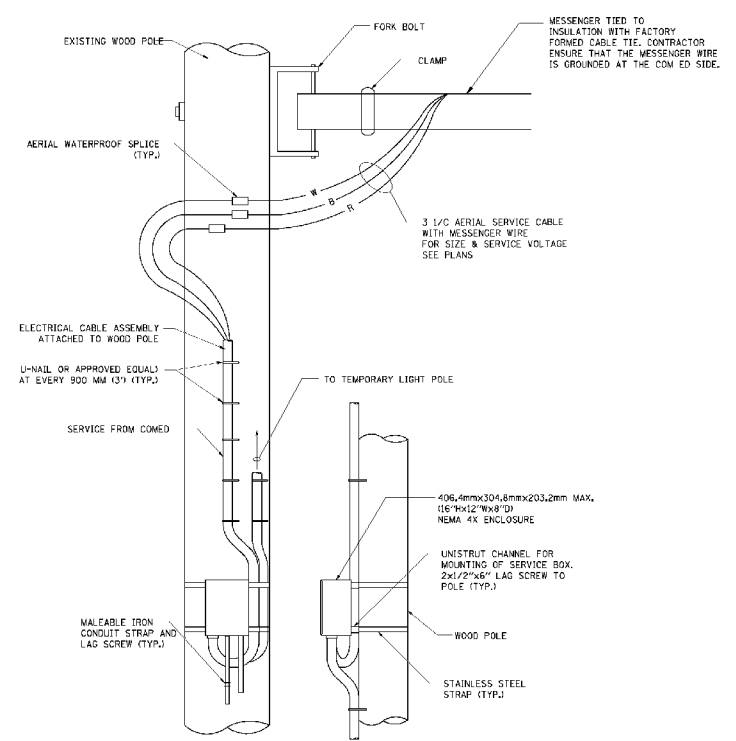
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STANDARD TRAFFIC SIGNAL DESIGN DETAILS
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD
SCALE: SHEET NO. 33 OF 36 SHEETS STA. TO STA.

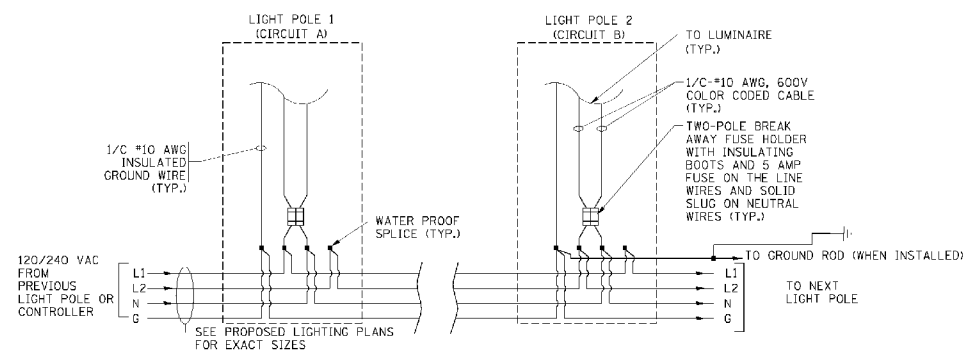
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32R5&N (12)	DUPAGE	495	314
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B42	



TEMPORARY SERVICE INSTALLATION DETAIL
NOT TO SCALE



DISCONNECT MOUNTING DETAIL
NOT TO SCALE



LIGHT POLE WIRING DETAIL
NOT TO SCALE

FILE NAME -	USER NAME - bsum-dl	DESIGNED - MP	REVISED -
c:\pwwork\VPWIDOT\BAUERDL\d010315\be803.dgn		DRAWN - YJ	REVISED -
PLOT SCALE - 50.0000 / / IN.		CHECKED - RP	REVISED -
PLOT DATE - 1/14/2010		DATE - 01/14/10	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TEMPORARY LIGHTING AND TRAFFIC SIGNALS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FOR SINGLE LANE STAGING			BE-805			
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS	STA.	CONTRACT NO.			
		TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

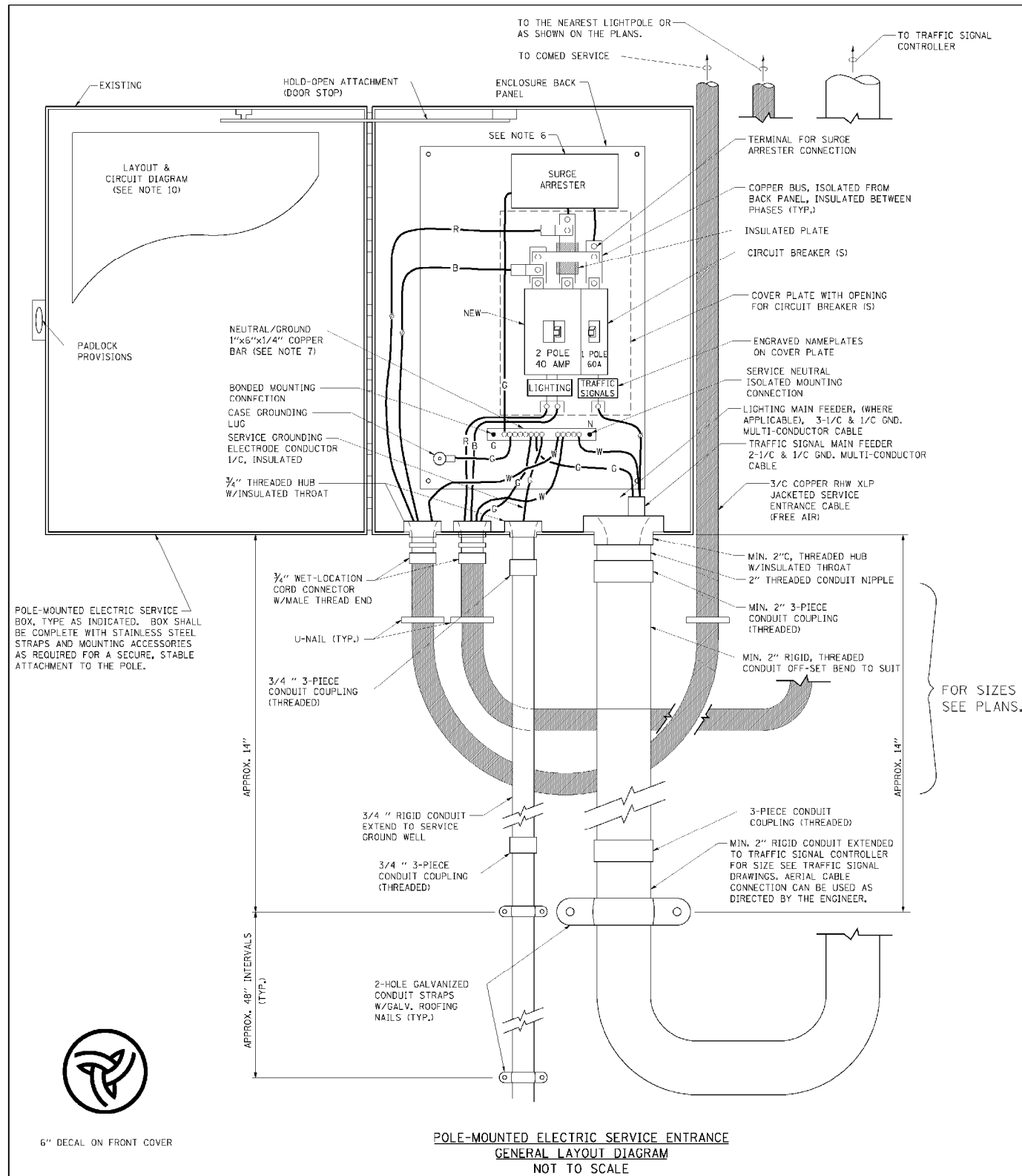


USER NAME - mkucinas	DESIGNED - VP	REVISED -
	DRAWN - YJ	REVISED -
PLOT SCALE - 40.0000 / / IN.	CHECKED - RP	REVISED -
PLOT DATE - 12/17/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

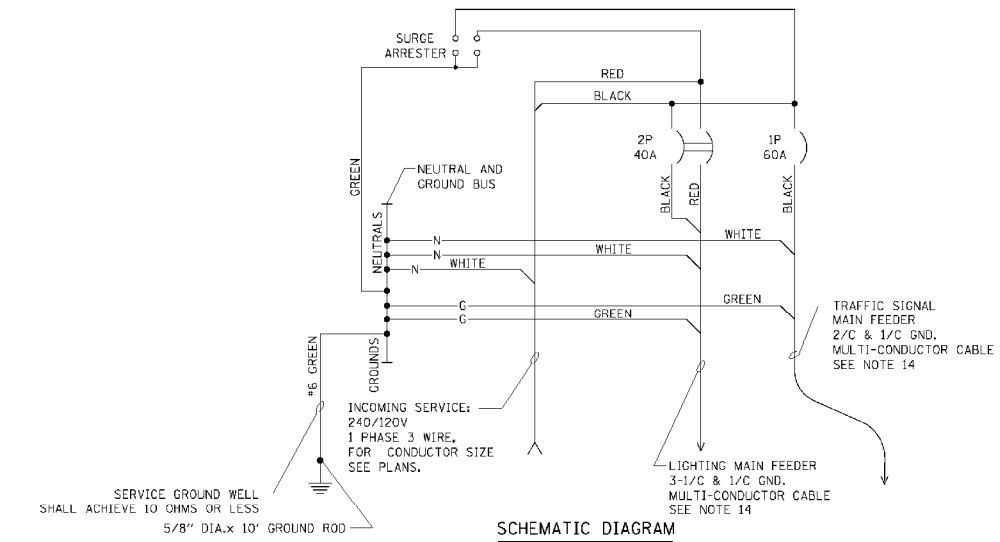
STANDARD TEMPORARY LIGHTING DESIGN DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD			1321	32RS&N (12)	DUPAGE	495 315
SCALE:	SHEET NO. 34 OF 36 SHEETS	STA.	CONTRACT NO. 60B42			
		TO STA.	ILLINOIS FED. AID PROJECT			

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	315
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



NOTES:

- ELECTRIC SERVICE SHALL BE OF THE VOLTAGE INDICATED OR DESIGNATED BY THE ENGINEER, AND SERVICE DROP CABLE SHALL BE COMPATIBLE WITH THE SERVICE ACCORDINGLY. SOME INSTALLATIONS MAY CALL FOR SERVICE ENTRANCE EQUIPMENT SUITABLE FOR 3-WIRE SERVICE EVEN THOUGH INITIALLY WIRED FOR 2-WIRE SERVICE.
- THE POLE-MOUNTED ELECTRIC SERVICE BOX SHALL BE CONFIGURED AND FULLY EQUIPPED FOR 240/120V 3W SERVICE, COMPLETE WITH LIGHTING MAIN BREAKER AND TRAFFIC SIGNALS MAIN BREAKER AS REQUIRED.
- THE ELECTRIC SERVICE EQUIPMENT ASSEMBLY SHALL BE UL LISTED AS SUITABLE FOR USE AS SERVICE ENTRANCE EQUIPMENT.
- THE ELECTRIC SERVICE EQUIPMENT ENCLOSURE SHALL BE NEMA 4X STAINLESS STEEL, NOMINALLY 12" W X 16" H X 8" D, WITH A PIANO-HINGED DOOR, STEEL BACK PANEL, FAST-ACTING STAINLESS STEEL ENCLOSURE CLAMPS, PADLOCK PROVISIONS AND DOOR STOP, HOFFMAN CATALOG NO. A-16H120856LP/A-16 P12/A-DSTOPK/C-PM12, OR APPROVED EQUAL.
- CIRCUIT BREAKERS SHALL BE THERMAL MAGNETIC BOLT-ON TYPE WITH A MINIMUM INTERRUPTING CAPACITY OF 25,000 SYMMETRICAL AMPERES AT 240 VOLTS. THEY SHALL BE LOCKABLE IN THE "OFF" POSITION FOR COMPLIANCE WITH OSHA LOCK-OUT/TAG-OUT REQUIREMENTS. HANDLES SHALL BE TRIP FREE.
- THE SURGE PROTECTOR SHALL BE SUITABLE FOR THE SERVICE VOLTAGE SINGLE PHASE 60HZ AC, WITH A SURGE ENERGY CAPABILITY OF 2160 JOULES OR BETTER AT 8/20 MICRO-SECONDS, RATED -40 TO 60 DEGREES C., WITH LED OPERATING INDICATORS, AND SHALL BE UL LISTED PER UL 1449, CUTLER-HAMMER CMQV230L06XST OR APPROVED EQUAL.
- BUS BARS, CONNECTORS, AND LUGS SHALL BE COPPER, INSULATED AND ISOLATED, AND CONFIGURED TO PREVENT SHORTED CONDITIONS FROM TIGHTENING TERMINATIONS, ETC. THE OVERALL BUS SECTION SHALL BE CONFIGURED BEHIND AN INSULATING BARRIER SHIELD WHICH IS REMOVABLE FOR ACCESS TO CONNECTIONS, OR THE ASSEMBLY SHALL BE A MANUFACTURED SPECIALTY PANELBOARD, CUTLER-HAMMER PRL2A OR APPROVED EQUAL.
- THE COMBINATION GROUND AND NEUTRAL SECTIONS SHALL BE CONFIGURED WITH SEPARATE GROUND AND NEUTRAL SECTIONS AND SPARE TERMINALS AS INDICATED. THE HEADS OF GROUND SCREWS SHALL BE PAINTED GREEN. THE HEADS OF NEUTRAL SCREWS SHALL BE PAINTED WHITE. THE SERVICE NEUTRAL AND SERVICE GROUNDING ELECTRODE CONDUCTOR SHALL BE TERMINATED ADJACENT TO EACH OTHER AT THE DIVIDE BETWEEN THE SECTIONS AND WIRING SHALL BE TERMINATED ONLY UPON THE APPROPRIATE SECTION.
- THE WIRING TERMINALS, INCLUDING THE GROUND/NEUTRAL BAR SHALL BE ARRANGED TO PROVIDE ADEQUATE ROOM FOR PERFORMING FIELD TERMINATIONS.
- A PLASTIC LAMINATED LAYOUT AND CIRCUIT DIAGRAM SHALL BE MECHANICALLY SECURED TO THE INTERIOR SIDE OF THE ENCLOSURE DOOR.
- A 2-COLOR ENGRAVED PLASTIC NAMEPLATE, ATTACHED WITH SCREWS, AND ENGRAVED AS INDICATED, SHALL BE PROVIDED FOR EACH MAIN BREAKER.
- LUGS AND CONNECTORS SHALL BE RATED FOR 75 C CONDUCTOR.
- THE EXACT MOUNTING HEIGHT OF THE BOX SHALL BE FIELD DETERMINED TO AVOID OBSTRUCTIONS AND PUBLIC ACCESS. TYPICAL HEIGHT SHALL BE APPROXIMATELY 10 FEET ABOVE GRADE.



FILE NAME =	USER NAME = bauerd	DESIGNED - MP	REVISED -
PILOT SCALE = 50,000' / IN.	PLU1 DATE = 1/14/2010	CHECKED	REVISED -
		DATE 01/14/10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS -FOR SINGLE-LANE STAGING-		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	BE-005	CONTRACT NO.		
		TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT		

SINGH
CONSULTING ENGINEERS
www.singhinc.com

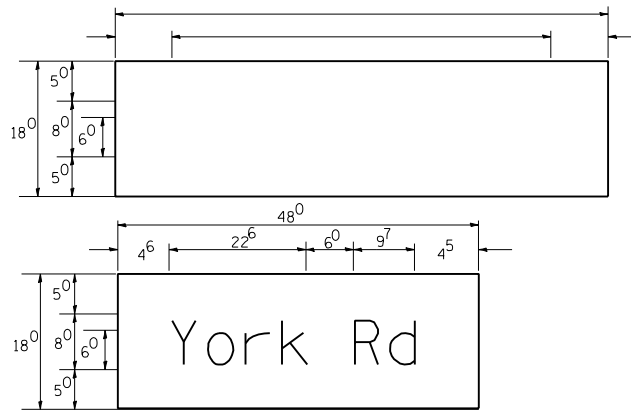
USER NAME = mkucinas	DESIGNED - VP	REVISED -
PILOT SCALE = 40,000' / IN.	DATE = 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STANDARD TEMPORARY LIGHTING DESIGN DETAILS IL ROUTE 19 (IRVING PARK RD.) AT YORK ROAD		F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE:	SHEET NO. 35 OF 36 SHEETS	STA.	1321	32RS&N (12)	DUPAGE	495 316
		TO STA.				CONTRACT NO. 60B42
						ILLINOIS FED. AID PROJECT

EXAMPLE, 2³ DENOTES 3"

PANEL SIGN DESIGN TYPE 1

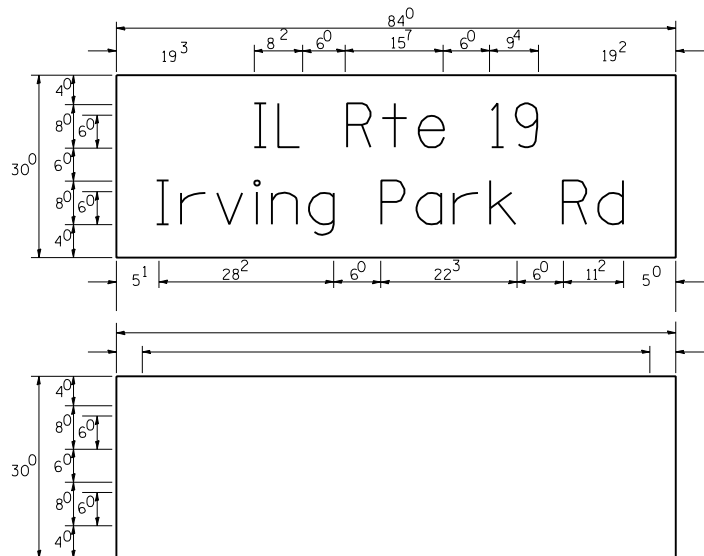


— Sq. M. each
— Sq. Ft. each
— Required
Design Series —

— Sq. M. each
— 6.0 Sq. Ft. each
— 2 Required
Design Series D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

PANEL SIGN DESIGN TYPE 2



— Sq. M. each
— 17.5 Sq. Ft. each
— 2 Required
Design Series D

— Sq. M. each
— Sq. Ft. each
— Required
Design Series —

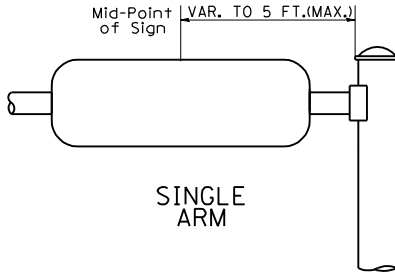
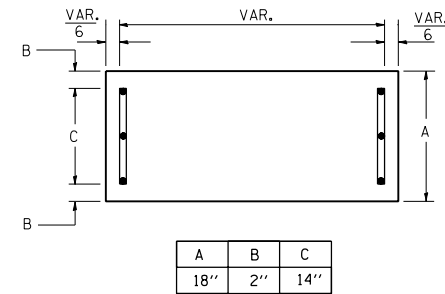
GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

* A.K.T. CORPORATION SCHLAUBURG, IL	* AMERICAN FABRICATION CO. CHICAGO HEIGHTS, IL
* TUCKER COMPANY, INC. WAUWATOSA, WI	* WESTERN TRAFFIC CONTROL INC. CICERO, IL

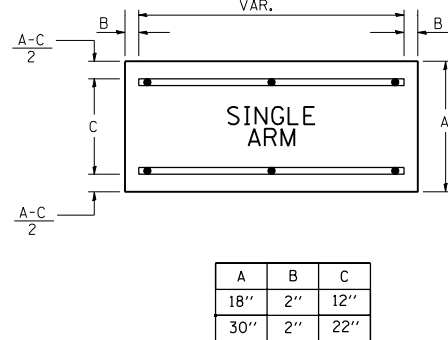
PARTS LISTING:
SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
BRACKETS SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING
OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

SUPPORTING CHANNELS

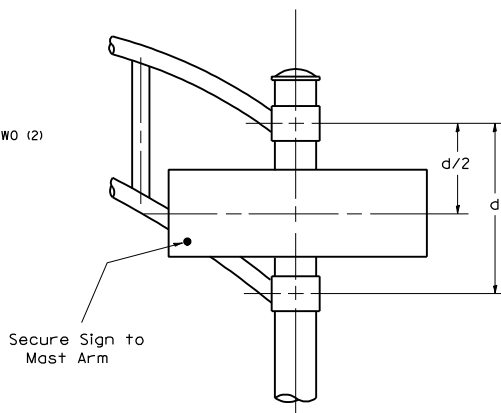


SINGLE ARM

SUPPORTING CHANNELS



SINGLE ARM



DUAL ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM Shall be used. See Note #5.

Upper Case To Lower Case Spacing Chart 8-6 Inch Series "C & D"

SERIES	SECOND LETTER																																								
	a		c		e		g		o		q		b		h		k		i		l		m		n		p		r		u		f	w	j	s	t	v	y	x	z
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D					
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	14	06	10	11	14	12	14	12	14	16	17																	
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	14	15	14	15	14	15	14	15	16	17																	
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15	14	15	14	15	14	15	16	17																	
D O O R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15	14	15	14	15	14	15	16	17																	
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12	11	12	11	12	11	12	11	12																	
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21	20	21	20	21	20	21	20	21																	
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	16	17	16	17	16	17	20	21	20	21																	
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	11	12	11	12	11	12	12	14	15																		
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14	12	14	12	14	12	14	12	14																	
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14	12	14	12	14	12	14	12	14																	
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	11	12	11	12	11	12	12	14	15																		
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14	12	14	12	14	12	14	12	14																	
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12	11	12	11	12	11	12	11	12																	
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	16	17	16	17	20	21	20	21	20	21																	

Lower Case To Lower Case Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER																																								
	a		c		e		g		o		q		b		h		k		i		l		m		n		p		r		u		f	w	j	s	t	v	y	x	z
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	
ad h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17	16	17	16	17	16	17	16	17																	
l m n q u																																									
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14	12	14	12	14	12	14	12	14																	
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14	12	14	12	14	12	14	12	14																	
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10	06	10	06	10	06	10	06	10																	
t z	12	14	16	17	12	14	06	10	11	12	11	12	11	12	12	14	12	14	12	14	12	14	12	14																	
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12	11	12	11	12	11	12	11	12																	
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	11	12	11	12	11	12	11	12	11	12																	
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	11	12	11	12	11	12	11	12	11	12																	

Number To Number Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																																		
	0		1		2		3		4		5		6		7		8		9																
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17	16	17	16	17											
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21	20	21	20	21											
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15	14	15	14	15											
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15	14	15	14	15											
6	16	17	14	15	14	15	12	14	14	15	14	15	14	15	11	12	14	15	14	15	14	15	14	15											
7	12	14	12	14	14	15	12	14	15	12	14	14	15	11	12	14	15	14	15	14	15	14	15												
8	16	17	16	17	14	15	12	14	14	15	16	17	12	14	16	17	14	15	14	15	14	15	14	15											

LETTER S	6 INCH UPPER CASE LETTERS				8 INCH UPPER CASE LETTERS				LETTER S	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES		SERIES		SERIES			SERIES	
	C	D	C	D	C	D	C	D		C	D
A	36	50	50	65	a	35	42				
B	32	40	43	53	b	35	42				
C	32	40	43	53	c	35	41				
D	32	40	43	53	d	35	42				
E	30	35	40	47	e	35	42				
F	30	35	40	47	f	23	26				
G	32	40	43	53	g	35	42				
H	32	40	43	53	h	35	42				
I	07	07	11	12	i	11	11				
J	30	36	40	50	j	20	22				
K	32	41	43	54	k	35	42				
L	30	35	40	47	l	11	11				
M	37	45	51	61	m	60	70				
N	32	40	43	53	n	35	42				
O	34	42	45	55	o	36	43				
P	32	40	43	53	p	35	42				
Q	34	42	45	55	q	35	42				
R	32	40	43	53	r	26	32				
S	32	40	43	53	s	36	42				
T	30	35	40	47	t	27	32				
U	32	40	43	53	u	35	42				
V	35	44	47	60	v	42	47				
W	44	52	60	70	w	55	64				
X	34	40	45	53	x	44	51				
Y	36	50	50	66	y	46	53				
Z	32	40	43	53	z	36	43				

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	12	14	15	20
2	32	40	43	53
3	32	40	43	53
4	35	43	47	57
5	32	40	43	53
6	32	40	43	53
7	32	40	43	53
8	32	40	43	53
9	32	40	43	53
0	34	42	45	55

REVISIONS	
NAME	DATE
D.A.Z./D.A.G.	11/90
CADD	6/98
	10/00

Illinois Department of Transportation DISTRICT 1

MAST ARM MOUNTED STREET NAME SIGNS

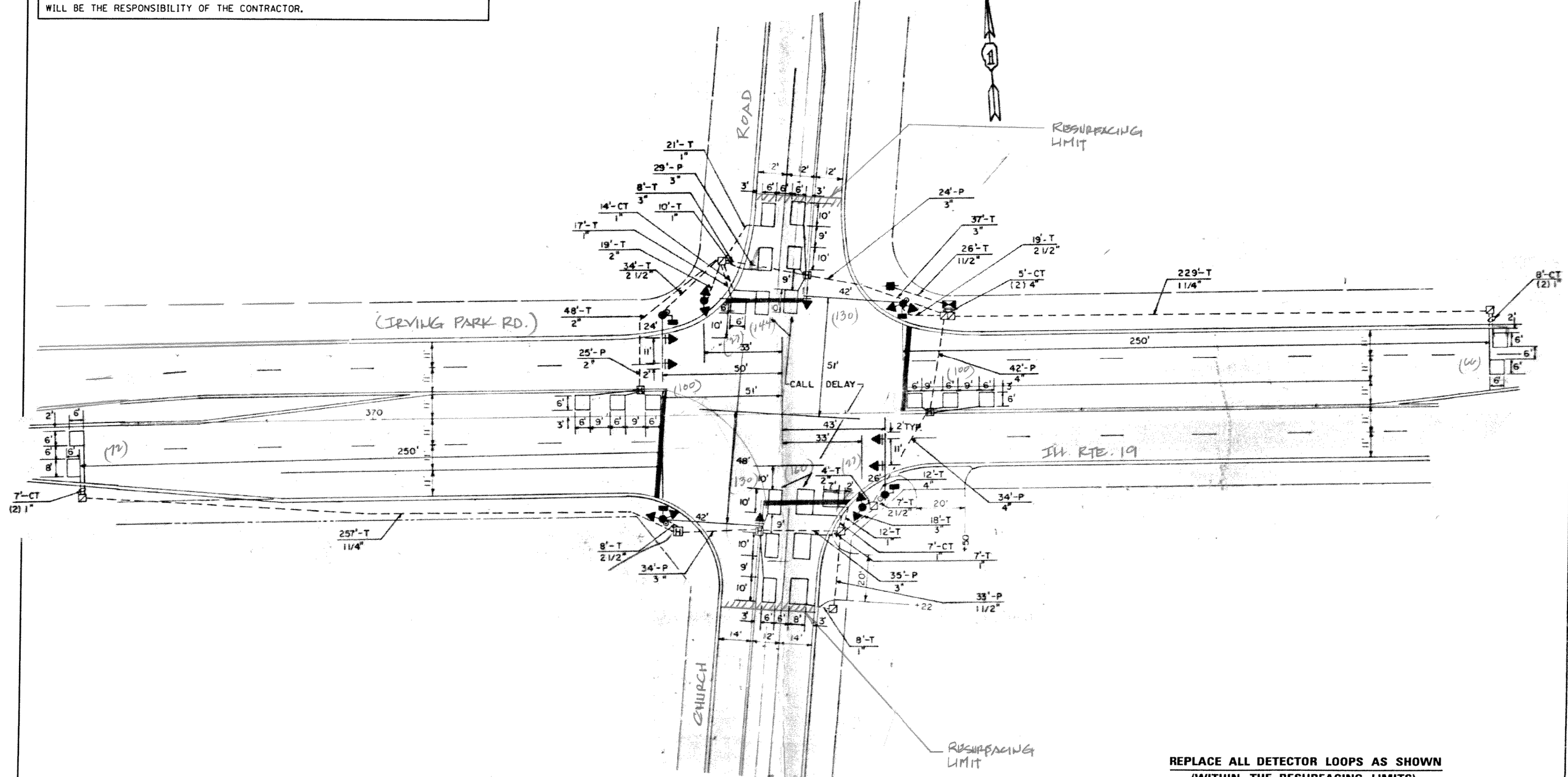
SCALE: NONE DATE: **DATE**

DRAWN BY: ROB DESIGNED BY: JHE CHECKED BY: DAD

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	→	→
SIGNAL HEAD	→	→
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	—	—
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	—	—



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	956	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = pascencia	DESIGNED -	REVISED -
at:\pwork\pwork\pascencia\00112618\T	sher.dgn	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -
PLOT DATE = 6/22/2011		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. RTE. 19 (IRVING RD.) @ CHURCH RD.**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DUPAGE	
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	CONTRACT NO.



USER NAME = mikelly	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

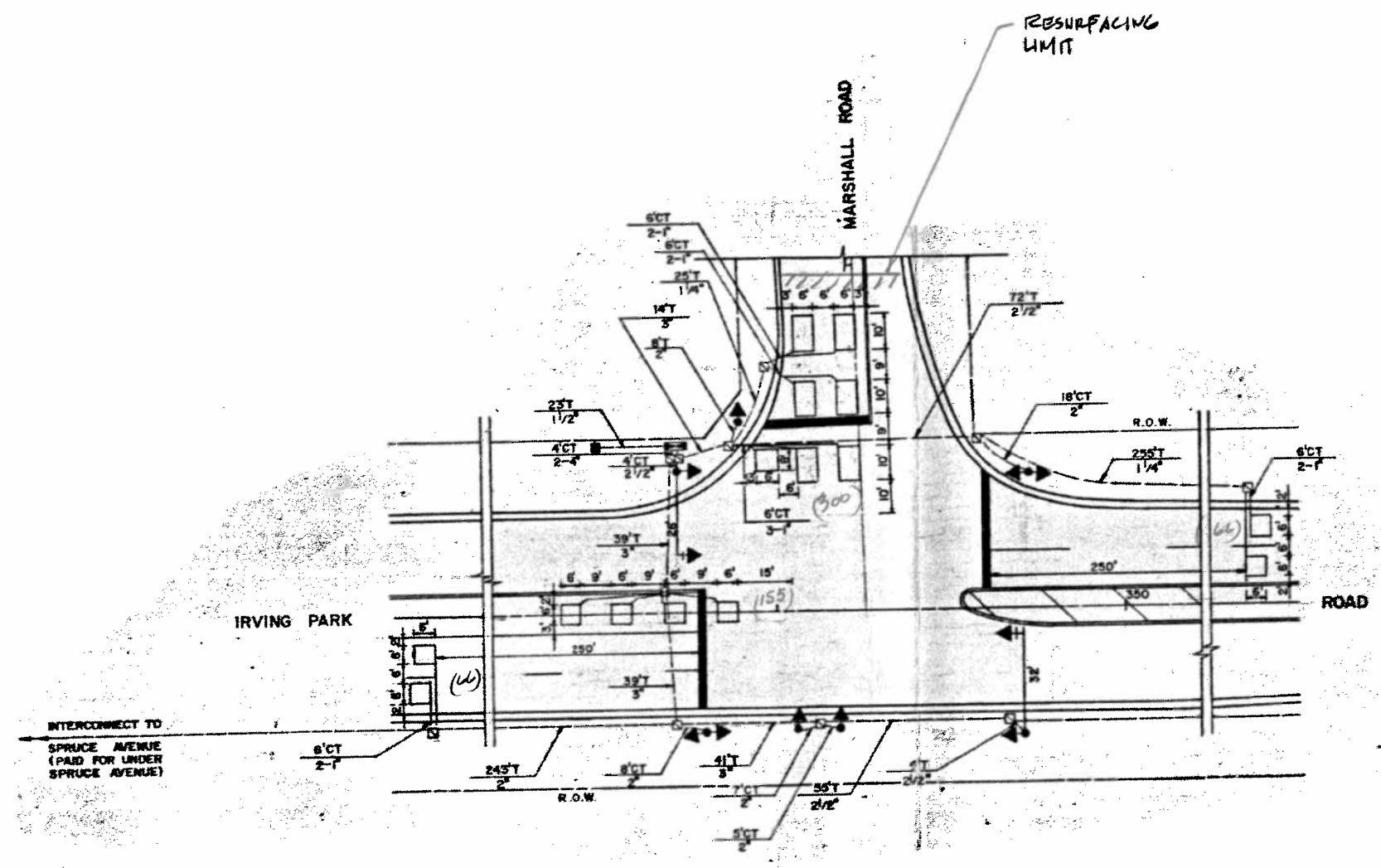
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
IL-19 AT CHURCH RD.**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	318
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	CONTRACT NO. 60B42

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	+	+
SIGNAL HEAD	+	+
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	□	□
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	—	—



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	587	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME *	USER NAME * pleoencuat	DESIGNED -	REVISED -
c:\pwork\pudat\pleoencuat\112618\1\shar.dgn		DRAWN -	REVISED -
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -
PLOT DATE = 6/22/2011		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
IL RTE. 19 (IRVING PK.) @ MARSHALL RD.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				



USER NAME = mikelly	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 12/17/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
IL-19 AT MARSHALL RD.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	319
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".

UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER OR VILLAGE.

THE CONTRACTOR SHALL NOTIFY THE VILLAGE PUBLIC WORKS ADMINISTRATOR AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.

MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING CRUSHED STONE OR CRUSHED GRAVEL.

EXISTING STREET LIGHTING UNITS (NOTED IN PLANS) TO REMAIN IN OPERATION AND TO BE MAINTAINED BY THE CONTRACTOR UNTIL NEW STREET LIGHTS ARE OPERATIONAL AND ACCEPTED BY THE VILLAGE.

CARE IS TO BE TAKEN AS NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUITS, DETECTORS AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED THE CONTRACTOR SHALL REPAIR AND/OR REPLACE THE CONDUIT AND/OR EQUIPMENT AT NO COST TO THE COUNTY OR THE VILLAGE.

THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENT FOR BURIED WARNING TAPE, SPECIFIED AS PART OF "TRENCH AND BACKFILL FOR ELECTRICAL WORK". THE INSTALLATION OF THE TAPE SHALL BE INSPECTED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO BACKFILLING OR DURING PLOWING OPERATIONS, AS APPLICABLE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ESTABLISHMENT OF FINISHED GRADE. THE RESIDENT ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATION WITH THE TOP OF FOUNDATION HEIGHTS AND THE LIGHT SHALL REMAIN WITH THE CONTRACTOR.

THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR WIRE MARKERS AND SHALL TAG ALL WIRE ACCORDINGLY.

EQUIPMENT GROUND CONDUCTORS SHALL BE SPLICED AND/OR BONDED AT EACH LIGHT POLE OR OTHER PIECE OF EQUIPMENT.

CONDUIT AND UNIT DUCT MUST BE POSITIONED IN THE FIELD TO AVOID CONFLICT WITH TREES, BUSHES, DRAINS AND OTHER UTILITIES AND LANDSCAPING.

ALL DISTURBED AREAS WHERE RESTORATION IS NOT COVERED BY APPLICABLE SECTIONS OF THE SPECIAL PROVISIONS MUST BE RESTORED TO THE SATISFACTION OF THE ENGINEER. THE WORK MUST BE CONSIDERED INCIDENTAL TO THE CONTRACT. SEPARATE PAYMENT WILL NOT BE MADE.

THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE LIGHTING SYSTEM. FOR THE LOCATIONS OF THE UTILITIES, CALL JULIE TOLL FREE AT 1-800-892-0123. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE EXISTING TRAFFIC SIGNAL CABLES AND CONDUITS.

ELECTRIC HANDHOLES ARE FOR THE PURPOSE OF PULLING CABLES ONLY. NO UNDERGROUND SPLICES OR SPLICES IN HANDHOLES ARE PERMITTED.

LIGHTING UNIT SETBACKS ARE FROM FACE OF CURB TO FACE OF POLE AS CALLED OUT ON DRAWINGS.

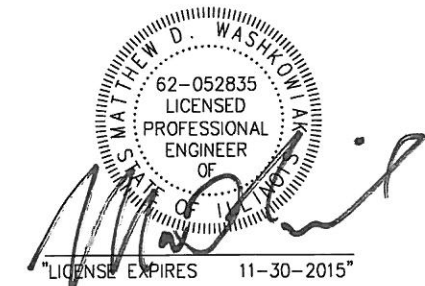
CONSTRUCTION SEQUENCING NOTES

CONTRACTOR IS RESPONSIBLE FOR COORDINATING ELECTRICAL/LIGHTING WORK WITH OTHER TRADES AND ANY CHANGES IN CONSTRUCTION STAGING SEQUENCING.

- A. PRE-STAGE:
 - 1. INSTALL TEMPORARY LIGHTING
 - A. NORTHWEST CORNER OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.
 - B. SOUTHWEST CORNER OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.
 - C. IRVING PARK ROAD (IL19) STATION 394+10.
 - 2. REMOVE EXISTING LIGHTING IN AFFECTED AREA NOTED FOR REMOVAL ON LIGHTING PLANS.
- B. STAGE 1:
 - 1. NO CHANGE.
- C. STAGE 1A:
 - 1. INSTALL LIGHTING CONDUITS, FOUNDATIONS AND HANDHOLES ON EAST SIDE OF YORK ROAD, NORTH OF IRVING PARK ROAD (IL19).
 - 2. PUSH CONDUITS UNDER IRVING PARK ROAD (IL19), EAST SIDE OF INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.
 - 3. PUSH CONDUITS UNDER YORK ROAD, NORTH SIDE OF INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.
- D. STAGE 2:
 - 1. INSTALL TEMPORARY LIGHTING ON EAST SIDE OF YORK ROAD, SOUTH OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.
 - 2. REMOVE DESIGNATED EXISTING LIGHTING ON EAST SIDE OF YORK ROAD, SOUTH OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.
 - 3. INSTALL PERMANENT LIGHTING CONTROLLER, CONDUITS AND FOUNDATIONS ON EAST SIDE OF YORK ROAD, SOUTH OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD. ERECT LIGHT POLES/LUMINAIRES ON EAST SIDE OF YORK ROAD, SOUTH OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD. ENERGIZE INSTALLED PERMANENT LIGHTING.
- E. STAGE 3:
 - 1. NO CHANGE.
- F. STAGE 4:
 - 1. INSTALL/ENERGIZE PERMANENT LIGHTING ALONG IRVING PARK ROAD (IL19) AND ON WEST SIDE OF YORK ROAD, SOUTH OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.
 - 2. REMOVE TEMPORARY LIGHTING.
- G. STAGE 5:
 - 1. NO CHANGE.
- H. STAGE 6:
 - 1. INSTALL/ENERGIZE PERMANENT LIGHTING ON WEST SIDE OF YORK ROAD, NORTH OF THE INTERSECTION OF IRVING PARK ROAD (IL19) AND YORK ROAD.

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
80400100	ELECTRIC SERVICE INSTALLATION	EACH	1
80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1
81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	275
81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	702
81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	645
81400730	HANDHOLE, COMPOSITE CONCRETE	EACH	7
81603158	UNIT DUCT, 600V, 5-1C NO.8, 1/C NO.8 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	5320
82102400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT	EACH	6
83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	225
84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	33
84200804	REMOVAL OF POLE FOUNDATION	EACH	33
X8250505	LIGHTING CONTROLLER, SPECIAL	EACH	1
X8410102	TEMPORARY LIGHTING SYSTEM	LSUM	1
X8410103	REMOVE TEMPORARY LIGHTING SYSTEM	L SUM	1
XX008367	DECORATIVE LIGHTING UNIT, TYPE "D1"	EACH	9
XX008368	DECORATIVE LIGHTING UNIT, TYPE "D2"	EACH	16
Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	24



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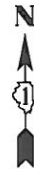
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**VILLAGE OF BENSENVILLE, ILLINOIS
IL ROUTE 19 (IRVING PARK ROAD) AND YORK ROAD
LIGHTING IMPROVEMENTS**

GENERAL NOTES & SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

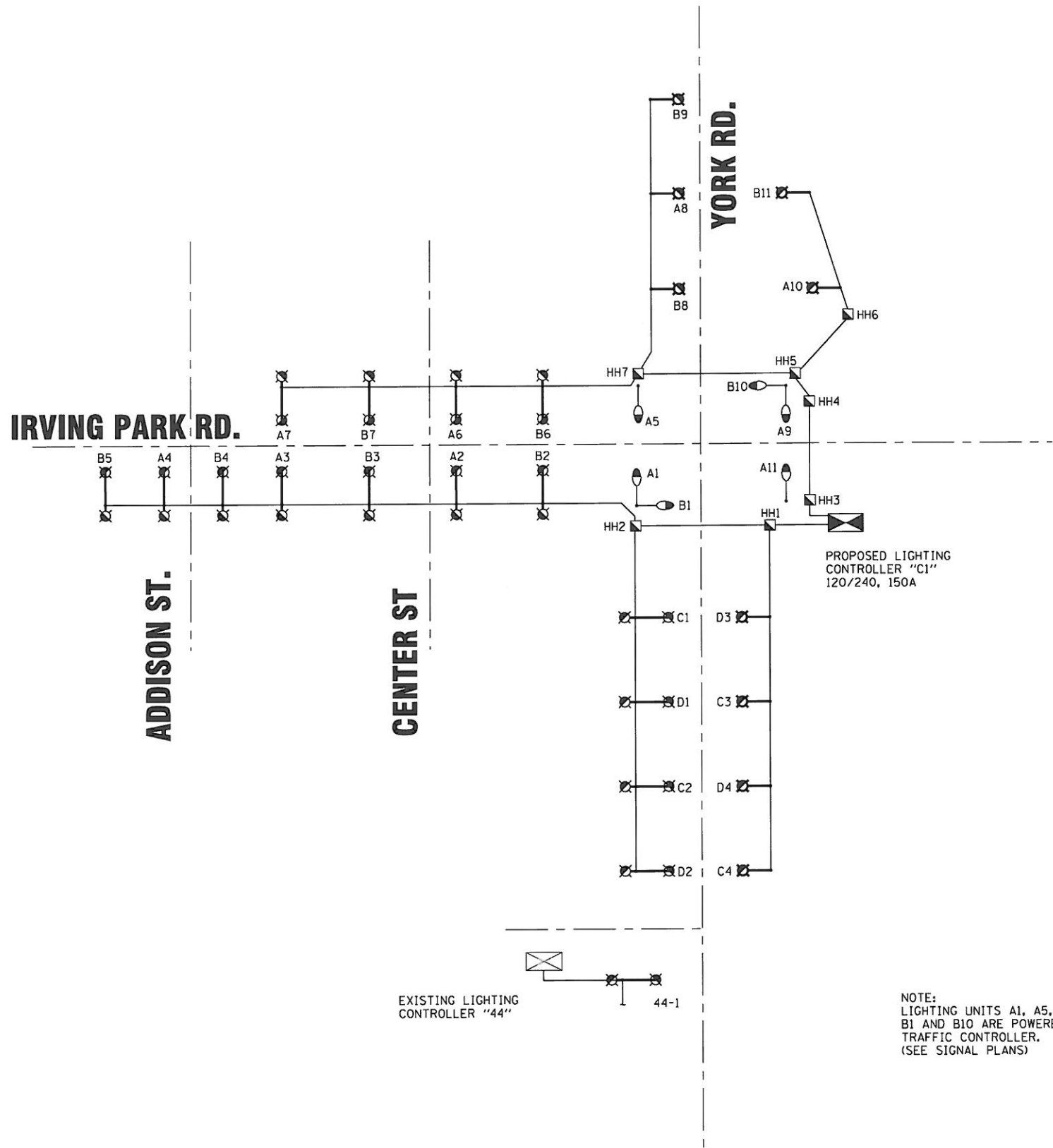
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32 WRS-5	DUPAGE	495	320
CONTRACT NO. 60B42				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



LOAD TABULATION FOR LIGHTING CONTROLLER "C1"			
CIRCUIT	96W	96W & 32W	AMPS
A	(2) 0.5	(5) 0.7	4.5A
B	(3) 0.5	(6) 0.7	5.7A
C	(2) 0.5	(2) 0.7	2.4A
D	(2) 0.5	(2) 0.7	2.4A
TOTAL			18.7A

LEGEND

- EXISTING COM ED POWER POLE
- 2 1/2" C, P-8' W/1 1/4" UD - UD- 1 1/4" UNIT DUCT 5*8 & 1*8 GROUND OR AS INDICATED
- 2" C, T-4' W/1 1/4" UD - PC- 2" CONDUIT, PUSHED 8' LONG WITH 1 1/4" UNIT DUCT OR AS INDICATED
- 2" C, T-4' W/1 1/4" UD - U- 2" CONDUIT, TRENCHED 4' LONG WITH 1 1/4" UD OR AS INDICATED
- PROPOSED COBRAHEAD STREET LIGHT 40' MOUNTING HEIGHT, 400 WATT HPS, W/8' ARM, 120 VOLTS, MOUNT ON COMBO POLE
- ⊗ PROPOSED DECORATIVE LIGHTING UNIT, TYPE "D1", 40' MOUNTING HEIGHT, 160 WATT LED, W/15' ARM, 240 VOLTS
- ⊗ PROPOSED DECORATIVE LIGHTING UNIT TYPE "D2", 40' MOUNTING HEIGHT, 160 WATT LED, W/15' ARM, 240 VOLTS AND 20' MOUNTING HEIGHT, 32 WATT LED, W/2' ARM, 240 VOLTS
- EX - - EXISTING STREET LIGHTING WIRING TO REMAIN
- ⊠ PROPOSED ELECTRIC HANDHOLE
- EX EXISTING LIGHTING UNIT TO REMAIN
- ⊠ EX EXISTING LIGHTING UNIT, REMOVE AND SALVAGE
- ⊠ EXISTING LIGHTING CONTROLLER
- ⊠ PROPOSED LIGHTING CONTROLLER
- ⊙ TEMPORARY WOOD POLE
- TEMPORARY LIGHTING UNIT
- ▲— TEMPORARY AERIAL CABLE
- S— STORM SEWER
- PROPOSED R.O.W.



PROPOSED LIGHTING CONTROLLER "C1"
120/240, 150A

EXISTING LIGHTING CONTROLLER "44"
44-1

NOTE:
LIGHTING UNITS A1, A5, A9, A11,
B1 AND B10 ARE POWERED FROM
TRAFFIC CONTROLLER.
(SEE SIGNAL PLANS)

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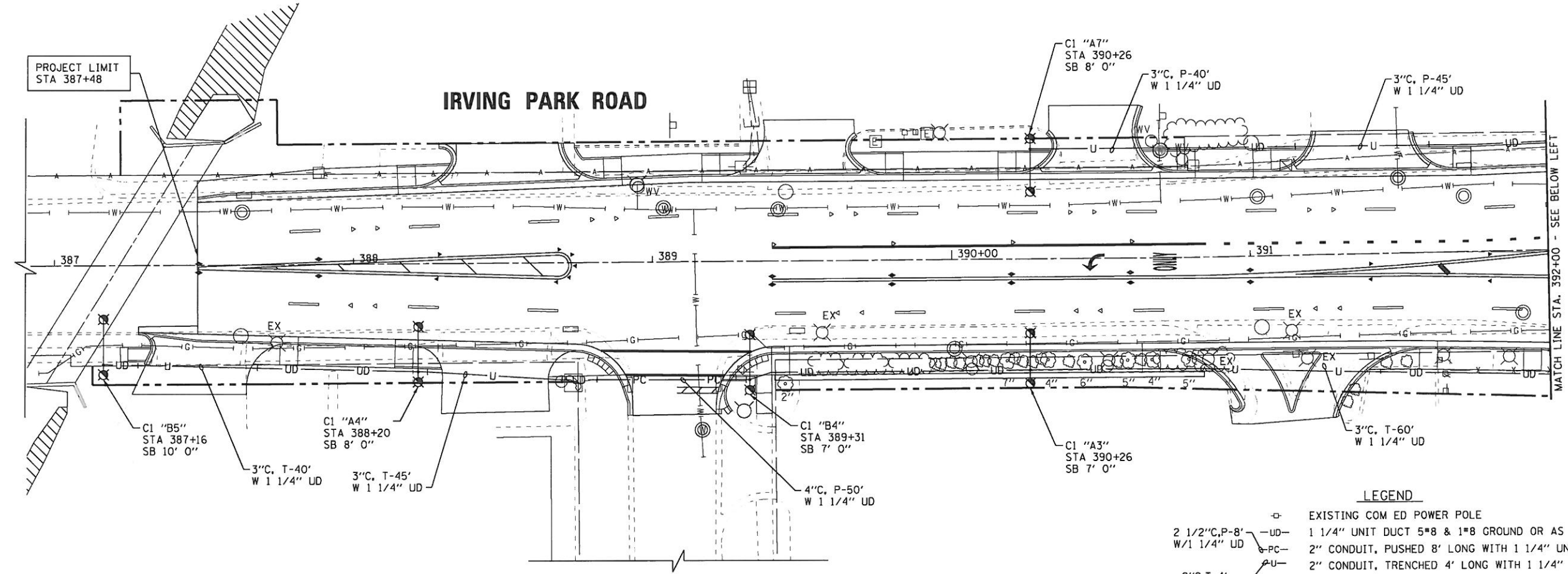
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DATE - 06/10/11	FILE - 100943-SLD.shx

**VILLAGE OF BENSENVILLE, ILLINOIS
IL ROUTE 19 (IRVING PARK ROAD) AND YORK ROAD
LIGHTING IMPROVEMENTS**

**LIGHTING PLAN
SINGLE LINE DIAGRAM**

SCALE: NONE STA. TO STA.

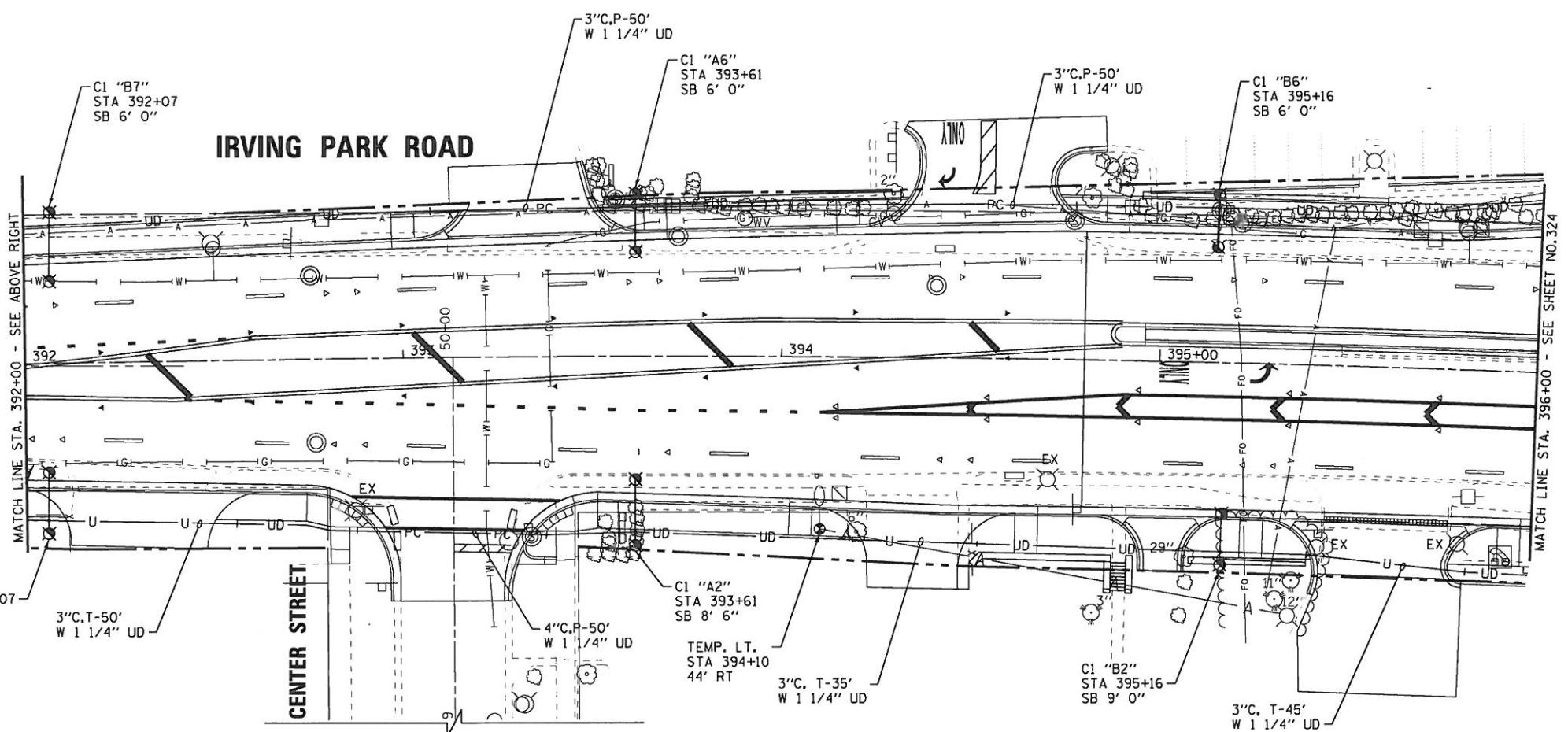
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FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 60B42				



MATCH LINE STA. 392+00 - SEE BELOW LEFT

LEGEND

- EXISTING COM ED POWER POLE
- UD— 1 1/4" UNIT DUCT 5*8 & 1*8 GROUND OR AS INDICATED
- PC— 2" CONDUIT, PUSHED 8' LONG WITH 1 1/4" UD OR AS INDICATED
- U— 2" CONDUIT, TRENCHED 4' LONG WITH 1 1/4" UD OR AS INDICATED
- ⊙ PROPOSED COBRAHEAD STREET LIGHT 40' MOUNTING HEIGHT, 400 WATT HPS, W/8' ARM, 120 VOLTS, MOUNT ON COMBO POLE
- ⊙ PROPOSED DECORATIVE LIGHTING UNIT, TYPE "D1", 40' MOUNTING HEIGHT, 160 WATT LED, W/15' ARM, 240 VOLTS
- ⊙ PROPOSED DECORATIVE LIGHTING UNIT TYPE "D2", 40' MOUNTING HEIGHT, 160 WATT LED, W/15' ARM, 240 VOLTS AND 20' MOUNTING HEIGHT, 32 WATT LED, W/2' ARM, 240 VOLTS
- EX -- EXISTING STREET LIGHTING WIRING TO REMAIN
- ⊠ PROPOSED ELECTRIC HANDHOLE
- ⊠ EX EXISTING LIGHTING UNIT TO REMAIN
- ⊠ EX EXISTING LIGHTING UNIT, REMOVE AND SALVAGE
- ⊠ EX EXISTING LIGHTING CONTROLLER
- ⊠ PROPOSED LIGHTING CONTROLLER
- ⊙ TEMPORARY WOOD POLE
- ⊙ TEMPORARY LIGHTING UNIT
- A— TEMPORARY AERIAL CABLE
- S— STORM SEWER
- - - PROPOSED R.O.W.



MATCH LINE STA. 392+00 - SEE ABOVE RIGHT

MATCH LINE STA. 396+00 - SEE SHEET NO. 324

NOTES:

1. EXISTING STREET LIGHTING UNITS TO REMAIN IN OPERATION UNTIL NEW STREET LIGHTS ARE OPERATIONAL AND ACCEPTED BY THE VILLAGE.
2. THE SANITARY AND STORM SEWERS IN THE VICINITY OF THE LIGHT POLE FOUNDATIONS SHALL BE TELEVIEWED IMMEDIATELY AFTER THE FOUNDATIONS HAVE BEEN INSTALLED.
3. THE POSITIONING OF THE LIGHT POLES SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO PLACEMENT OF FOUNDATIONS.

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DATE - 06/10/11	FILE - 100943-SL01.sht

**VILLAGE OF BENSENVILLE, ILLINOIS
IL ROUTE 19 (IRVING PARK ROAD) AND YORK ROAD
LIGHTING IMPROVEMENTS**

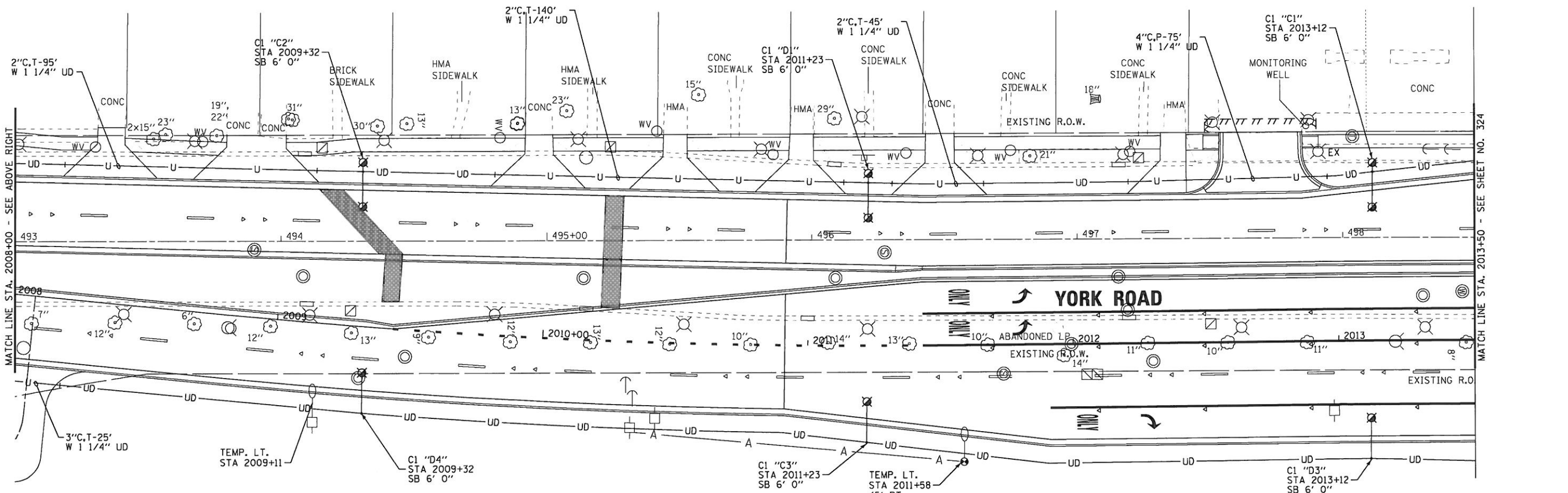
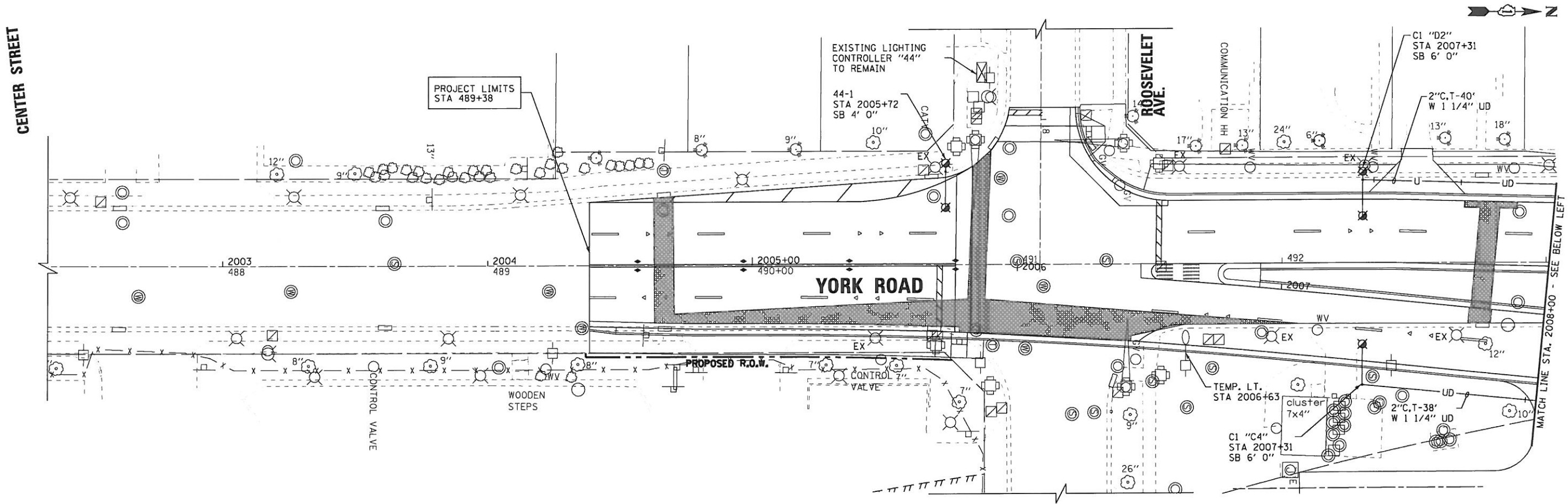
**STREET LIGHTING
IRVING PARK ROAD**

SCALE: 1"=20'

STA. 387+00 TO STA. 396+00

F.A. RTE. 1321	SECTION 32 WRS-5	COUNTY DUPAGE	TOTAL SHEETS 495	SHEET NO. 322
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B42	

CENTER STREET



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 PROJECT NO. - 100943-SL03-SHT
 DATE - 06/10/11

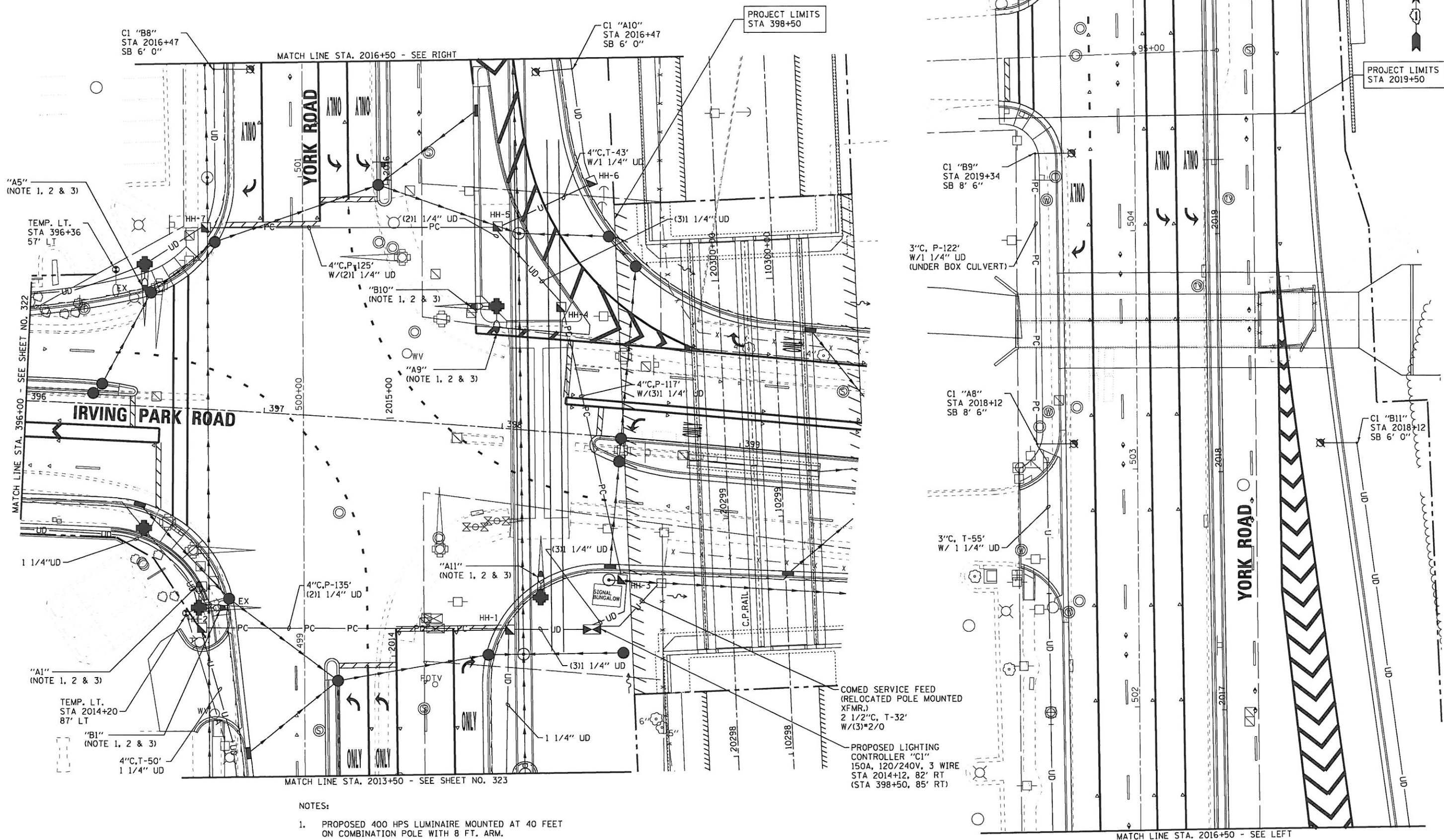


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DATE - 06/10/11	FILE - 100943-SL03.sht

VILLAGE OF BENSenville, ILLINOIS
IL ROUTE 19 (IRVING PARK ROAD) AND YORK ROAD
LIGHTING IMPROVEMENTS

STREET LIGHTING	
YORK ROAD	
SCALE: 1"=20'	STA. 2004+ TO STA. 2013+50

F.A. RTE. 1321	SECTION 32 WRS-5	COUNTY DUPAGE	TOTAL SHEETS 495	SHEET NO. 323
CONTRACT NO. 60B42				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



- NOTES:
1. PROPOSED 400 HPS LUMINAIRE MOUNTED AT 40 FEET ON COMBINATION POLE WITH 8 FT. ARM.
 2. LUMINAIRE POWERED FROM TRAFFIC SIGNAL CABINET, SEE SIGNAL PLANS AND LIGHTING DETAILS.
 3. 2*10 AND 1*10 GROUND, XLP - TYPE USE CABLES IN SIGNAL CONDUIT.

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 LICENSE NO. - 181-0217200R
 PROJECT NO. - 100943-SL02-SHT



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CHECKED - GSW	REVISED -
DATE - 06/10/11	FILE - 100943-SL02.sht

VILLAGE OF BENSenville, ILLINOIS
IL ROUTE 19 (IRVING PARK ROAD) AND YORK ROAD
LIGHTING IMPROVEMENTS

STREET LIGHTING	
IRVING PARK ROAD & YORK ROAD	
SCALE: 1"=20'	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32 WRS-5	DUPAGE	495	324
CONTRACT NO. 60B42				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

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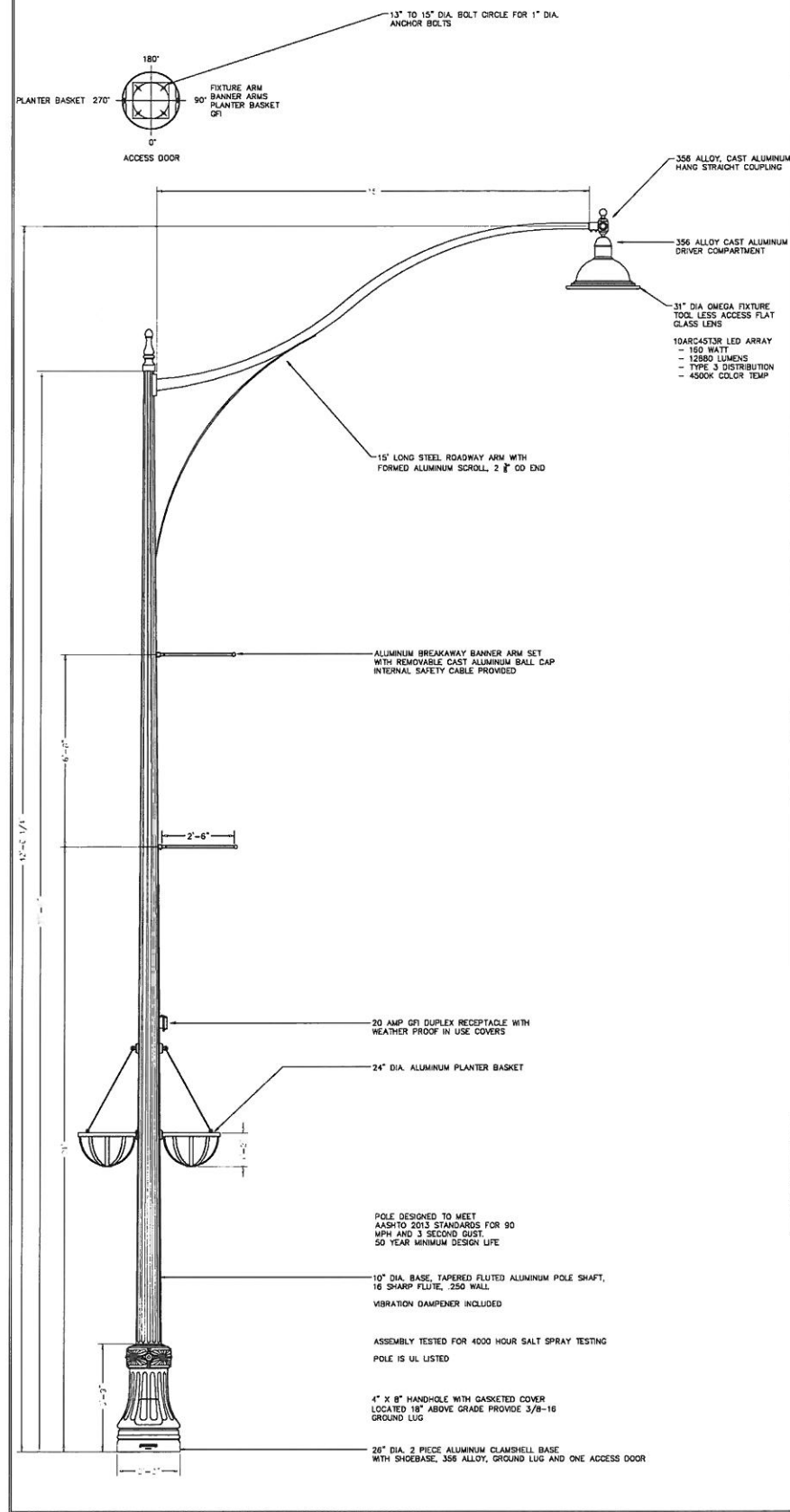
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VILLAGE OF BENSenville, ILLINOIS
IL ROUTE 19 (IRVING PARK ROAD) AND YORK ROAD
LIGHTING IMPROVEMENTS

SCALE: NONE

STA. TO STA.

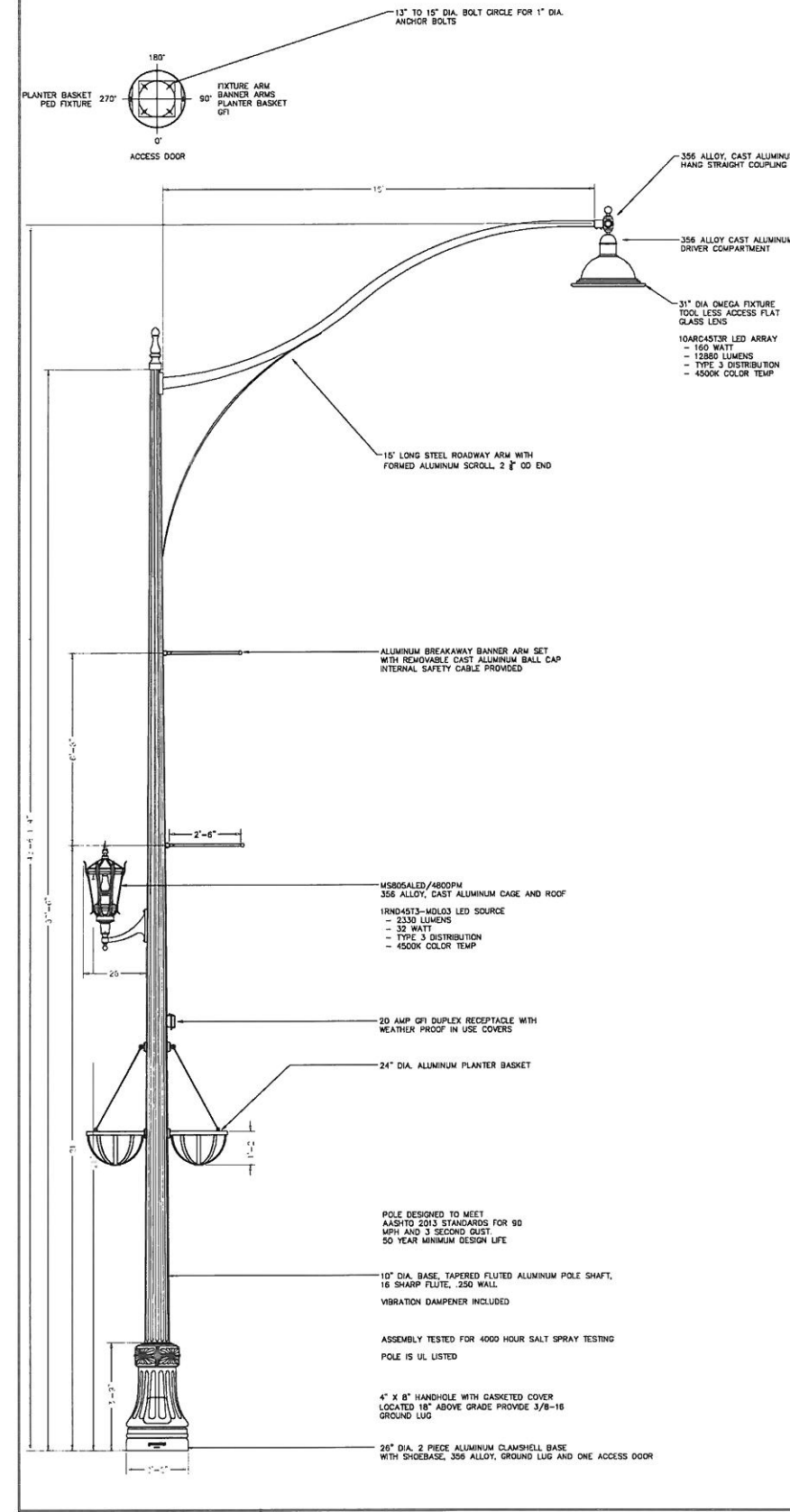
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1321	32 WRS-5	DUPAGE	495	326
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 60B42				



DECORATIVE LIGHTING UNIT, TYPE "D1"

REV	REVISIONS	DATE	RVS	BY	DATE	SCALE
A	ORIGINAL	12/9/14				1:40

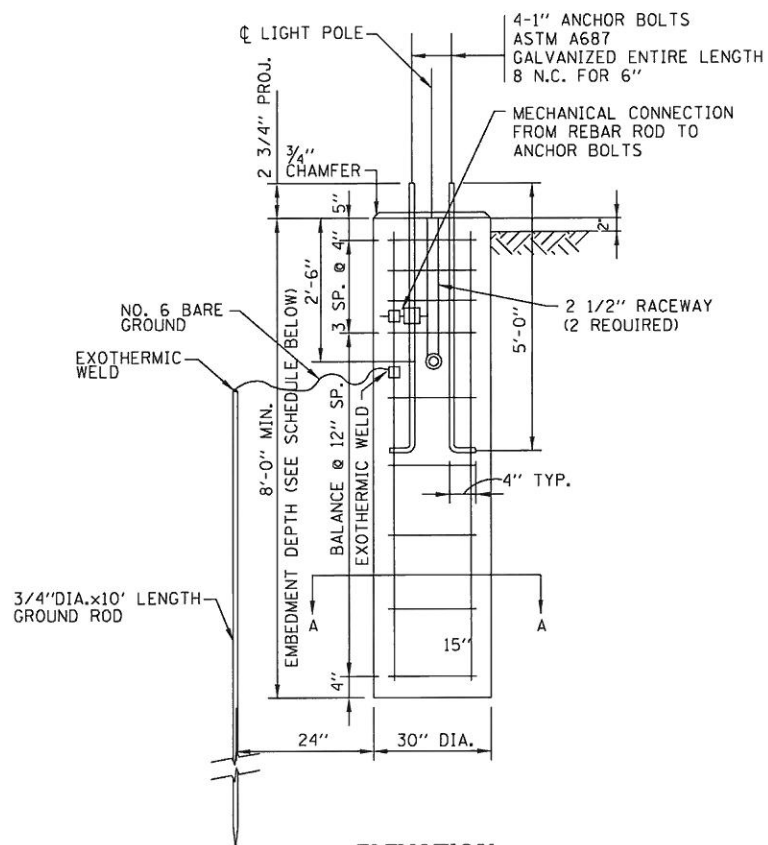
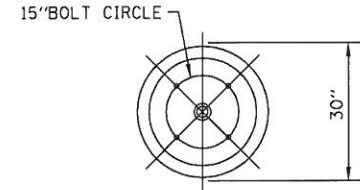
POLE HEIGHTS HAVE A TOLERANCE OF + OR - 2"
 BENSENVILLE, IL
 IRVING PARK ROAD - YORK AND IRVING
 1-1531RLED/FG/CAS15/10ARC45T3R-MDL03
 /93376"ARTF-16SF/HDBA/2-LPB/GFI-IUC/RPBP/BK
STERMBERG
 SINCE 1923
 DRAWING NUMBER
 SC22806B



DECORATIVE LIGHTING UNIT, TYPE "D2"

REV	REVISIONS	DATE	RVS	BY	DATE	SCALE
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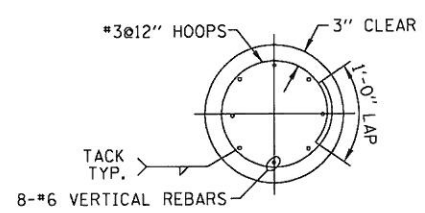
POLE HEIGHTS HAVE A TOLERANCE OF + OR - 2"
 BENSENVILLE, IL
 IRVING PARK ROAD - YORK AND IRVING
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STERMBERG
 SINCE 1923
 DRAWING NUMBER
 SC22806A



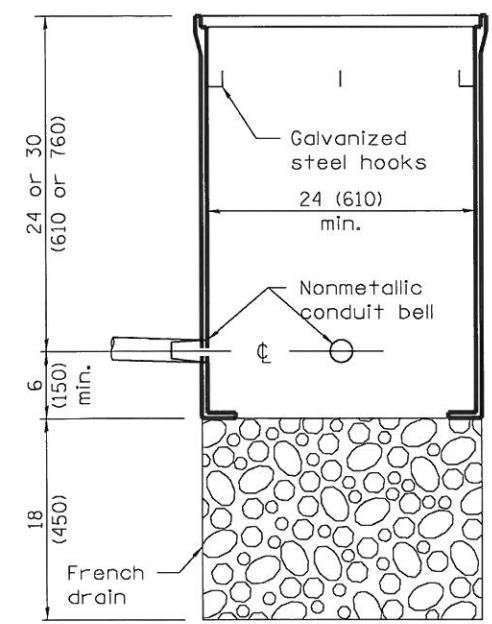
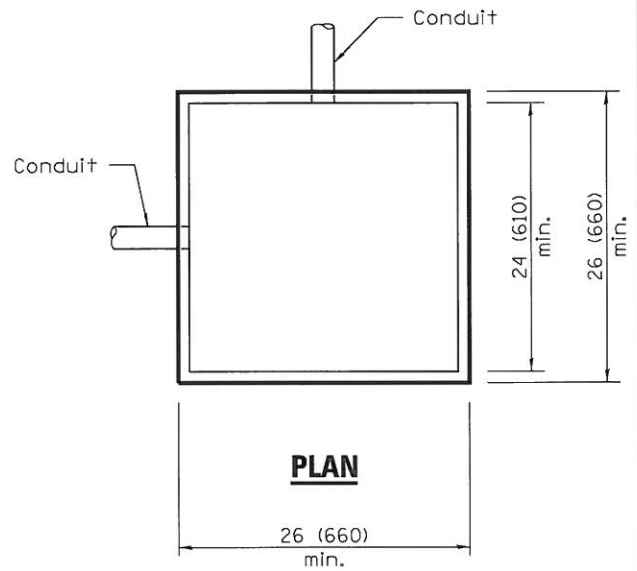
ELEVATION
DECORATIVE LIGHTING UNIT, TYPE "D1 & D2"
POLE FOUNDATION (TYPICAL)
NO SCALE

FOUNDATION SCHEDULE		
TYPE OF SOILS		EMBEDMENT
DESCRIPTION	STANDARDS	
SOFT CLAY	QU = .25-.50 T/FT ²	13'-0"
MEDIUM CLAY	QU = .25-.50 T/FT ²	10'-0"
DENSE CLAY	QU = .25-.50 T/FT ²	8'-0"
LOOSE SAND	N = 4-10	10'-6"
MEDIUM SAND	N = 10-30	9'-0"
DENSE SAND	N = 30-50	8'-6"

- NOTE:**
1. CONCRETE SHALL BE 3500 PSI AT 14 DAYS. (CLASS SI)
 2. REINFORCING BARS SHALL BE OF NEW BILLET STEEL AND DEFORMED ASTM M42M (M42).
 3. HOLE FOR THE FOUNDATION SHALL BE AUGERED.
 4. CONTRACTOR MUST VERIFY BOLT CIRCLE AND ANCHOR ROD DIMENSIONS FOR MATERIAL ORDERED BEFORE SETTING FOUNDATION.



SECTION A-A



ELEVATION
COMPOSITE CONCRETE

All dimensions are in inches (millimeters) unless otherwise shown.

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 067-0017-EXPRES 4/20/2013
 15400 S. WOODLAND AVE. SUITE 200
 BLOOMINGTON, IL 61820
 TEL: 312.234.1234 FAX: 312.234.1235



DESIGNED - MWH	REVISED -
DRAWN - BCD	REVISED -
CHECKED - GSW	REVISED -
DATE - 06/10/11	FILE - 100943-SL-De+2.sht

VILLAGE OF BENSENVILLE, ILLINOIS
IL ROUTE 19 (IRVING PARK ROAD) AND YORK ROAD
LIGHTING IMPROVEMENTS

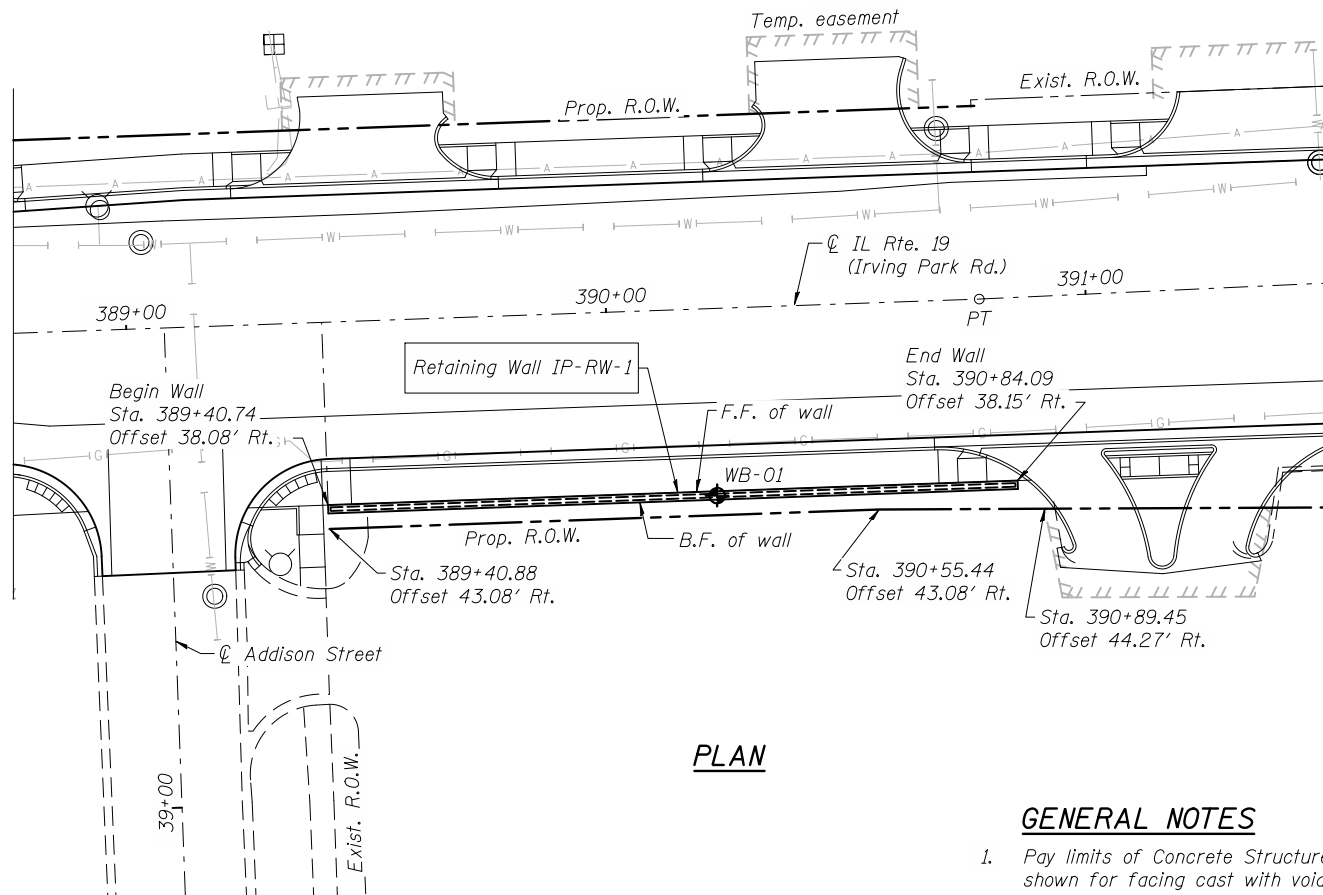
STREET LIGHTING DETAILS

SCALE: NONE STA. TO STA.

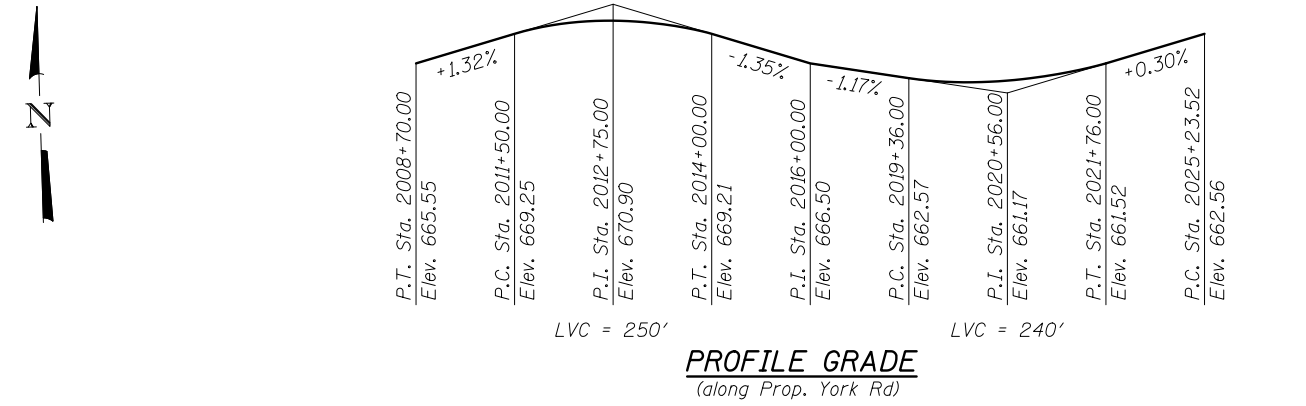
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32 WRS-5	DUPAGE	495	327
CONTRACT NO. 60B42				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

Bench Mark: TBM 4 - Chiseled "□" in southeast portion of cantilever traffic signal concrete foundation at northeast corner of York Rd. & IL 19; approx Sta. 397+64.78, offset 53.82' Lt., Elev. 668.85 (NAVD 88).
 TBM 5 - Arrow bolt of FH at north side of IL 19 at Addison Street; Elev. 666.34 (NAVD 88).

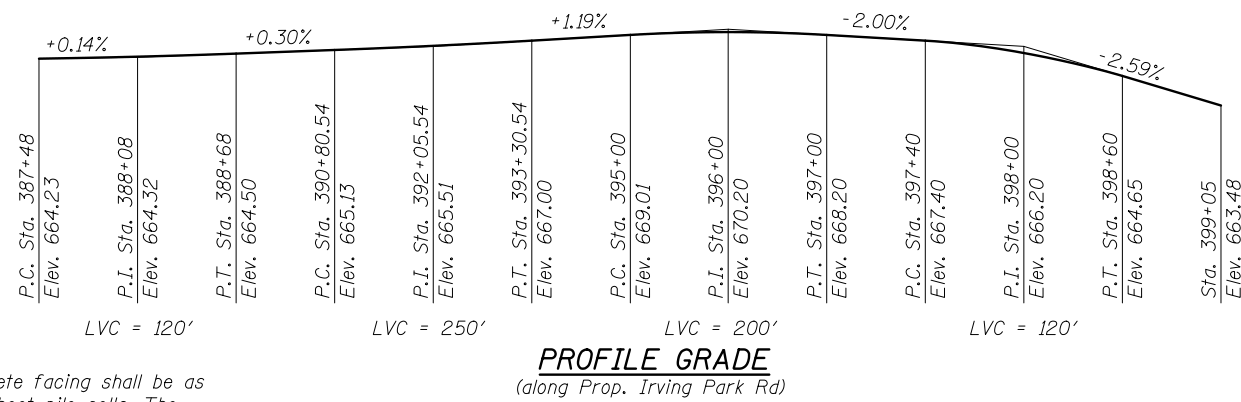
Existing Structure: none.



PLAN



PROFILE GRADE
(along Prop. York Rd)



PROFILE GRADE
(along Prop. Irving Park Rd)

GENERAL NOTES

1. Pay limits of Concrete Structures for concrete facing shall be as shown for facing cast with voids between sheet pile cells. The Contractor is allowed the option to cast the facing as solid sections, but the additional concrete shall not be measured for payment.
2. The Contractor shall choose a sheet pile section that has an effective section modulus of at least 3.5 in³. The section chosen must also satisfy all geometric constraints.
3. PZ 22 is shown in the plans for detailing purposes only. If a different section is chosen by the Contractor, the location of the front face shall be maintained for walls IP-RW-1, IP-RW-2a, IP-RW-2b and IP-RW-3, and the position of the back face shall be maintained for walls IP-RW-4 and IP-RW-5. At walls with adjacent sidewalks, a minimum 7'-0" sidewalk width must be maintained.
4. Quantities dependent on the sheet pile section, such as Concrete Structures and Stud Shear Connectors, may vary slightly depending on the actual sheet pile section chosen. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
5. Concrete Sealer shall be applied to all exposed surfaces of the concrete facing at each retaining wall.
6. Incidental excavation required to install the concrete facing that is not otherwise included in the cost of Earth Excavation shall be included in the cost of Concrete Structures.

INDEX OF SHEETS

1. General Plan
2. General Plan
3. Developed Elevations
4. Typical Sections
5. IP-RW-1 Plan and Elevation
6. IP-RW-2a & IP-RW-2b Plan and Elevation
7. IP-RW-2a & IP-RW-2b Details
8. IP-RW-3 Plan and Elevation
9. IP-RW-4 Plan and Elevation
10. IP-RW-5 Plan and Elevation
11. Handrail Details
12. Soil Boring Logs
13. Soil Boring Logs

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Permanent Steel Sheet Piling	Sq. Ft.	3,093
Concrete Structures	Cu. Yd.	46.6
Stud Shear Connectors	Each	1,145
Reinforcement Bars, Epoxy Coated	Pound	2,620
Pipe Handrail	Foot	245.0
Concrete Sealer	Sq. Ft.	2,019

LOADING HS20-44

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
 Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.089g
 Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.152g
 Soil Site Class = D

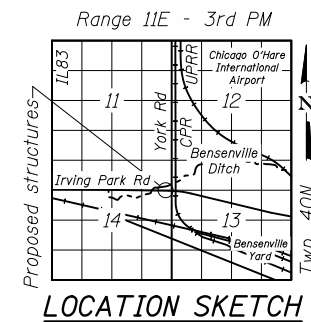
DESIGN SPECIFICATIONS

2002 AASHTO Standard Specification for Highway Bridges

DESIGN STRESSES

FIELD UNITS

f'_c = 3,500 psi
 f_y = 50,000 psi (sheet pile)
 f_y = 60,000 psi (reinforcement)



LOCATION SKETCH

GENERAL PLAN
IRVING PARK RD RETAINING WALLS
F.A.P. 345A - SEC. 32-WRS-5
DU PAGE COUNTY
STATION 389+40.74 TO 390+84.09
STATION 394+50.32 TO 395+35.00
STATION 395+98.44 TO 396+80.92
STATION 394+94.60 TO 395+59.26
STATION 393+54.28 TO 394+31.20

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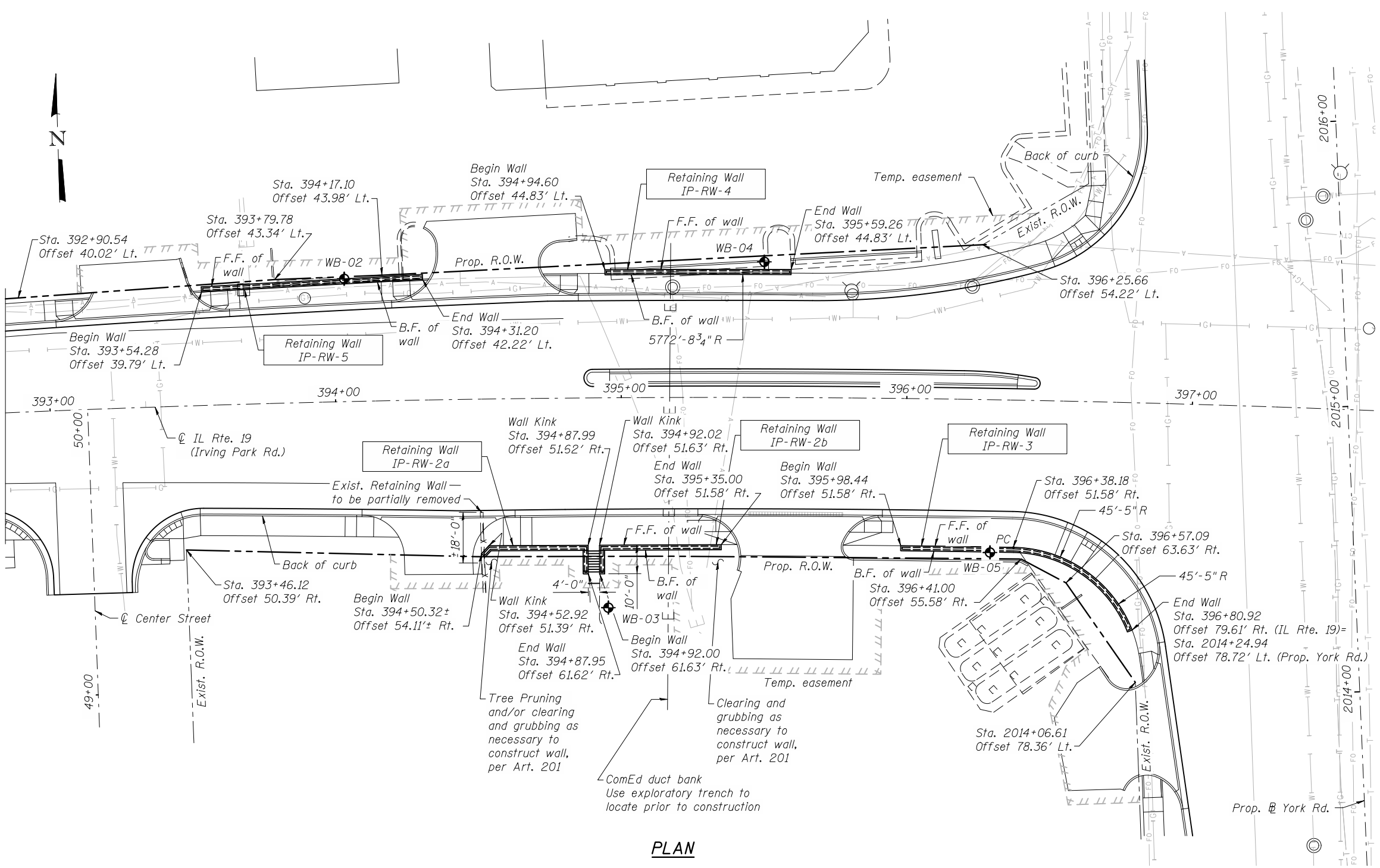


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PLOT DATE = 12-19-2014	CHECKED - VEVS	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN
IRVING PARK RD RETAINING WALLS
 SHEET NO. 1 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	329
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



PLAN

GENERAL PLAN
IRVING PARK RD RETAINING WALLS
F.A.P. 345A - SEC. 32-WRS-5
DU PAGE COUNTY
STATION 389+40.74 TO 390+84.09
STATION 394+50.32 TO 395+35.00
STATION 395+98.44 TO 396+80.92
STATION 394+94.60 TO 395+59.26
STATION 393+54.28 TO 394+31.20

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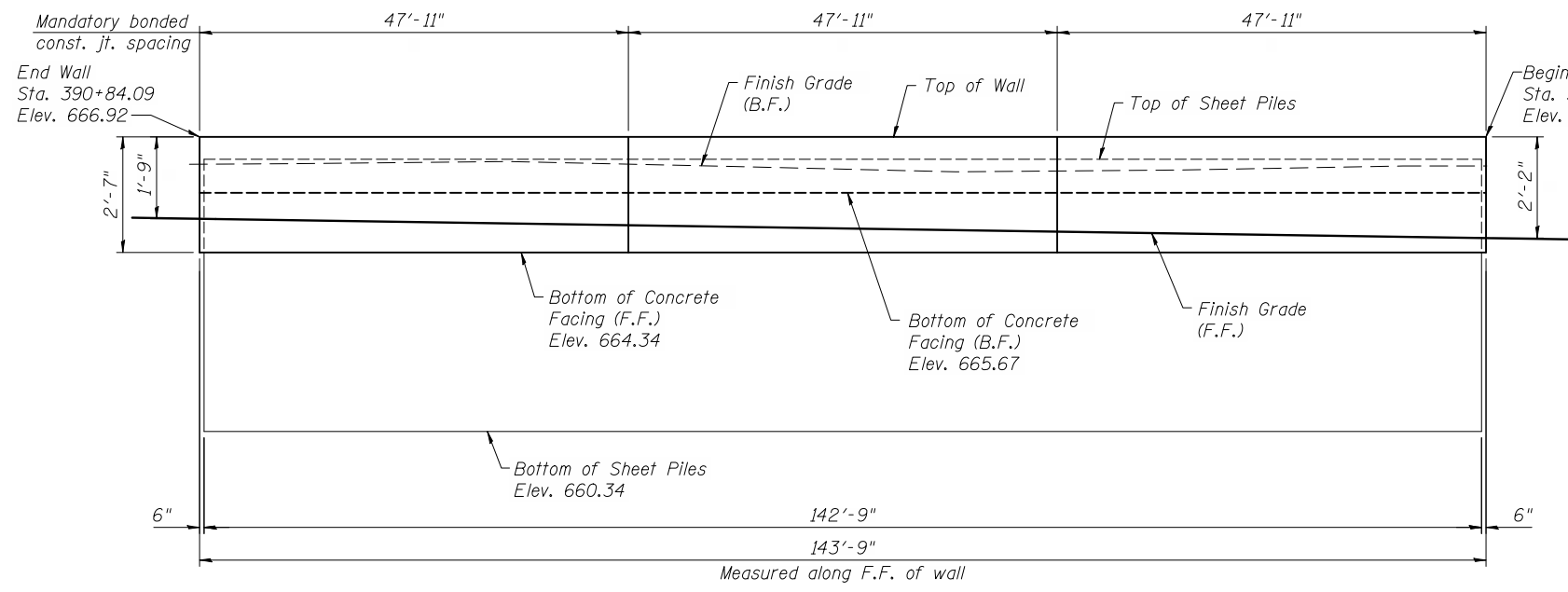


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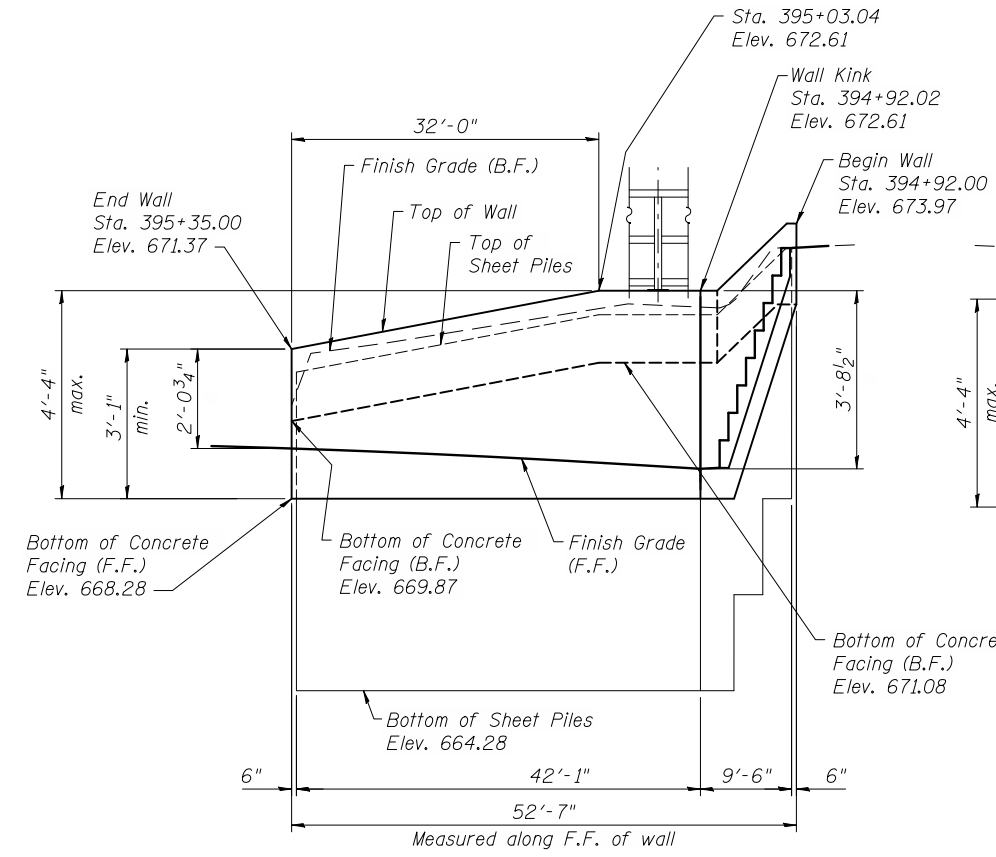
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN
IRVING PARK RD RETAINING WALLS
 SHEET NO. 2 OF 13 SHEETS

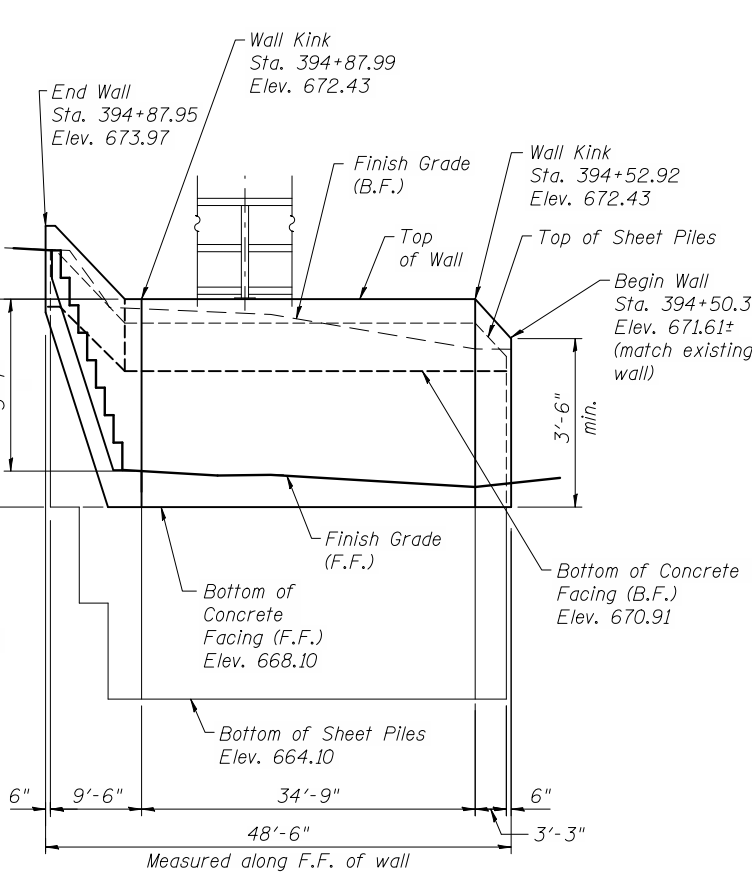
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	330
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



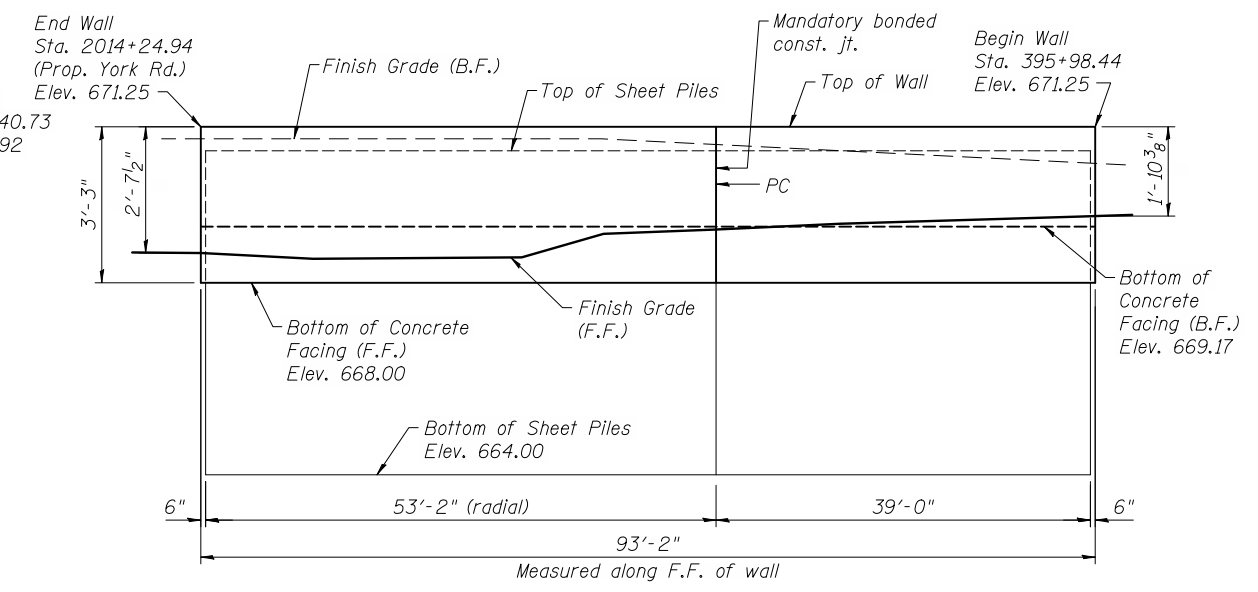
DEVELOPED ELEVATION - IP-RW-1
(Front Face Looking South)



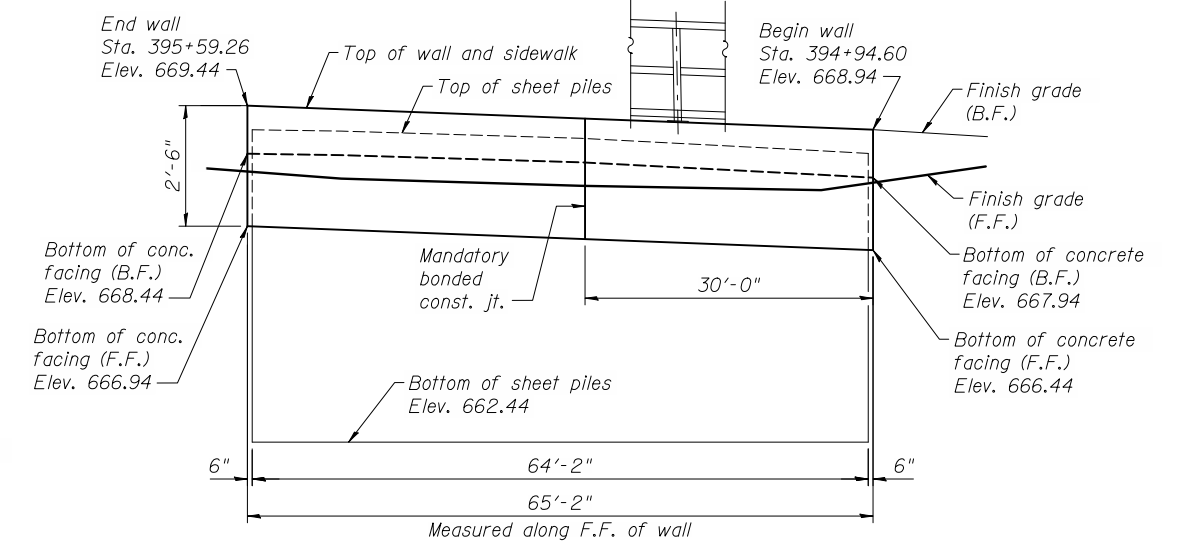
DEVELOPED ELEVATION - IP-RW-2b
(Front Face Looking South)



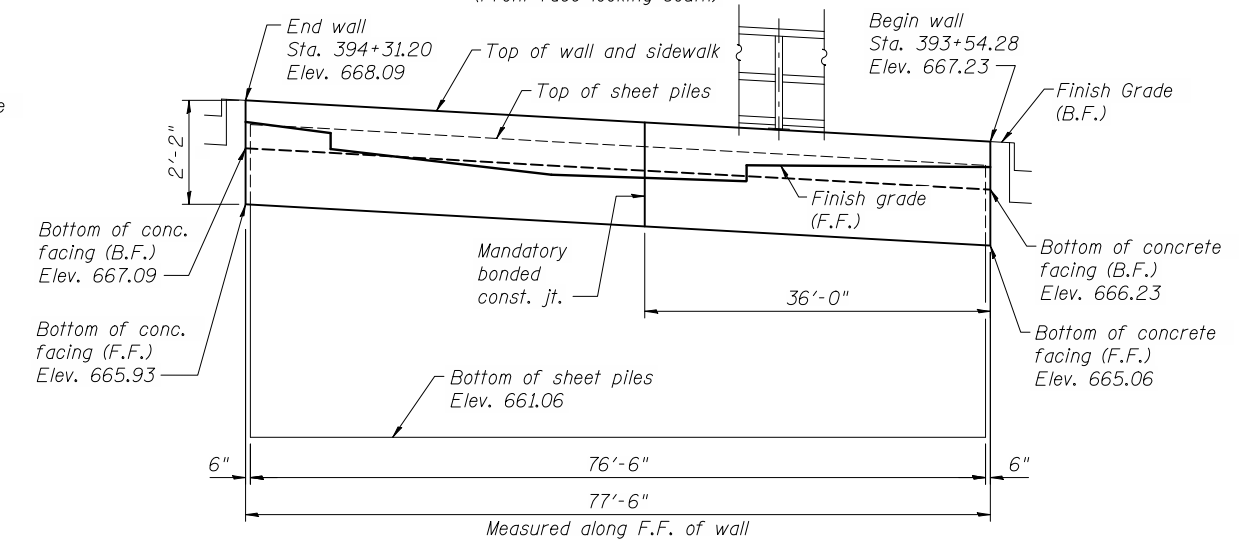
DEVELOPED ELEVATION - IP-RW-2a
(Front Face Looking South)



DEVELOPED ELEVATION - IP-RW-3
(Front Face Looking South)



DEVELOPED ELEVATION - IP-RW-4
(Front face looking south)



DEVELOPED ELEVATION - IP-RW-5
(Front face looking south)

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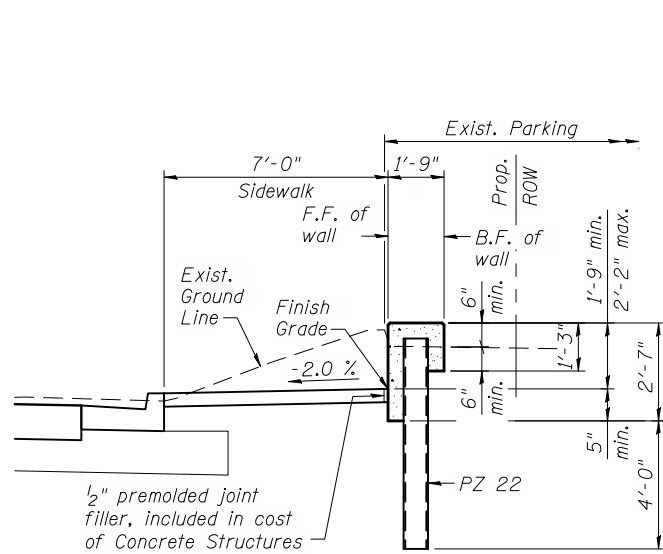
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DEVELOPED ELEVATIONS
IRVING PARK RD RETAINING WALLS**

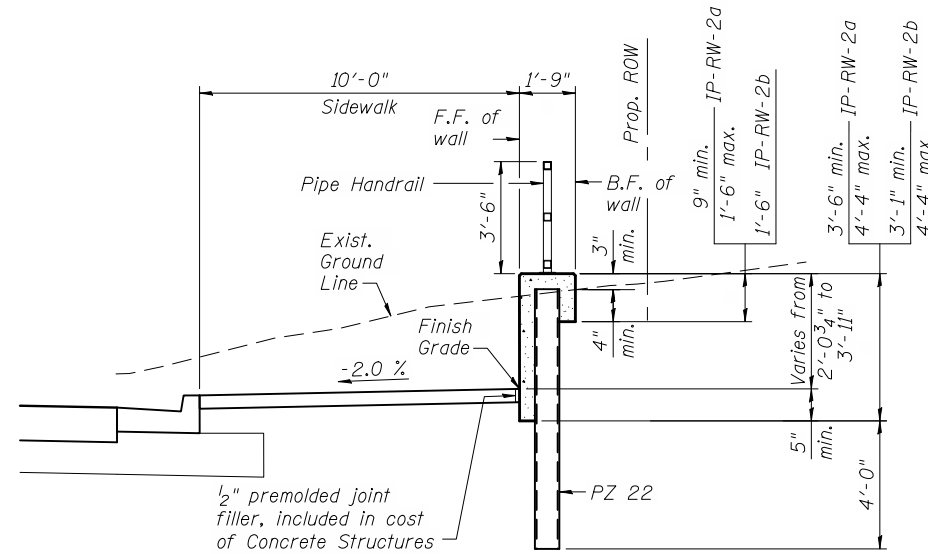
SHEET NO. 3 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



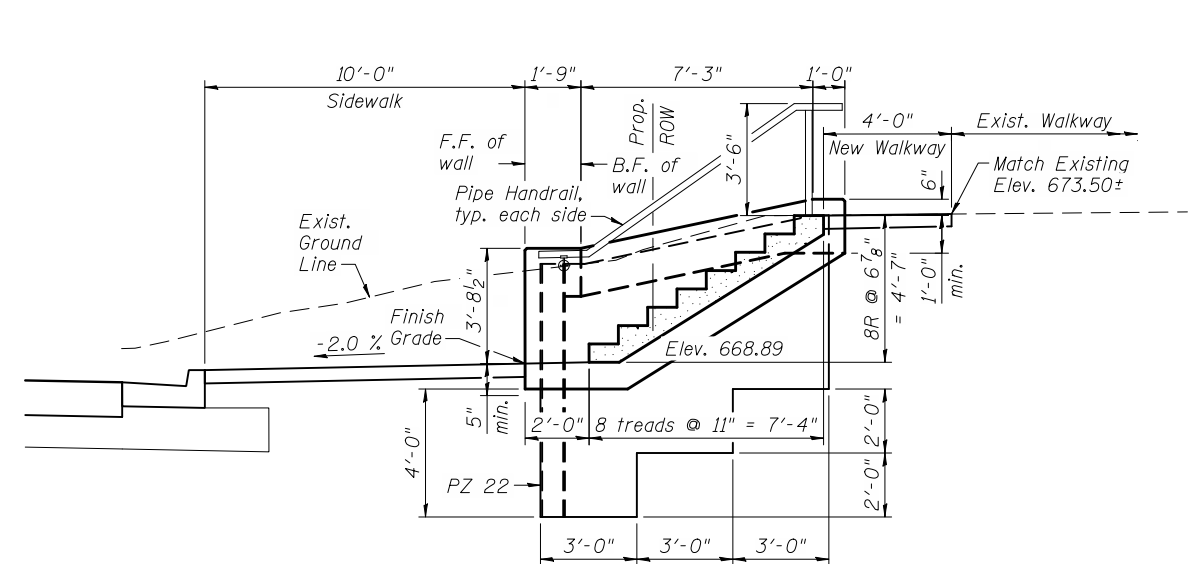
TYPICAL SECTION

Wall IP-RW-1



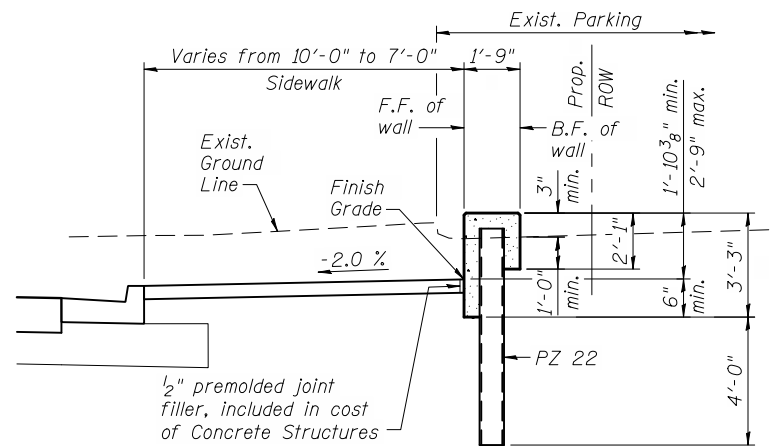
TYPICAL SECTION

Walls IP-RW-2a and IP-RW-2b



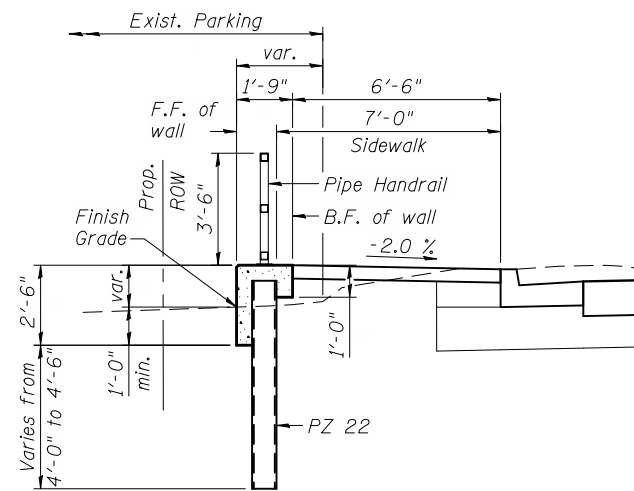
TYPICAL SECTION

Wall IP-RW-2b at stairs,
Wall IP-RW-2a similar



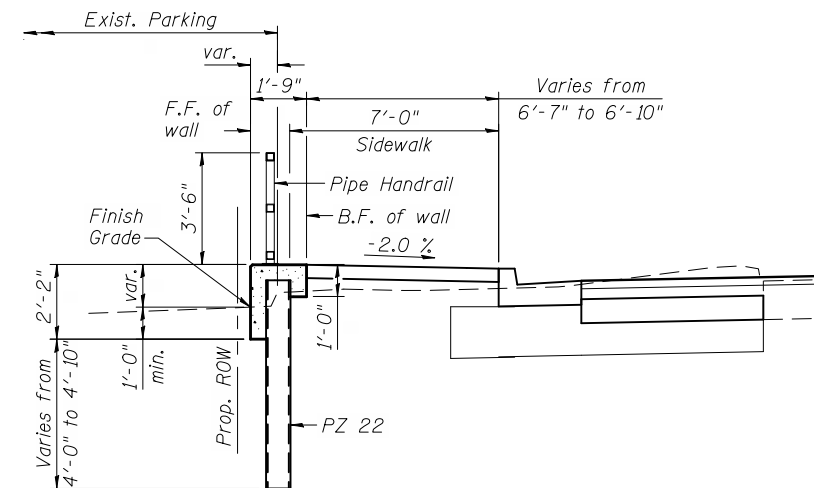
TYPICAL SECTION

Wall IP-RW-3



TYPICAL SECTION

Wall IP-RW-4



TYPICAL SECTION

Wall IP-RW-5

NOTES:

- For handrail details, see Sheet 11.

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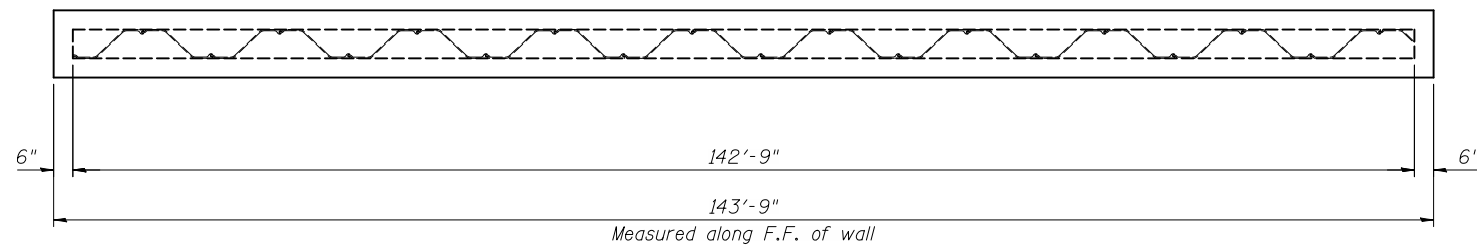
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

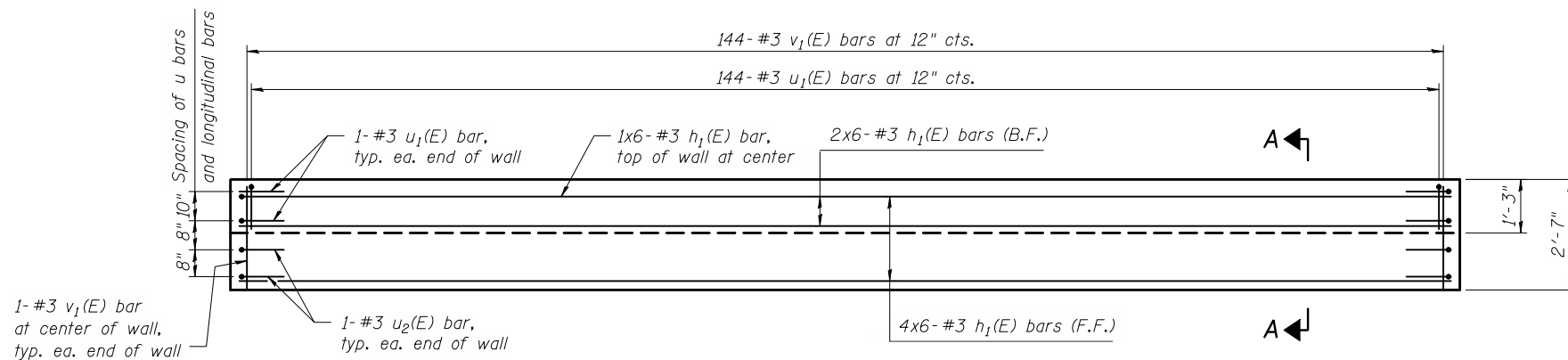
**TYPICAL SECTIONS
IRVING PARK RD RETAINING WALLS**

SHEET NO. 4 OF 13 SHEETS

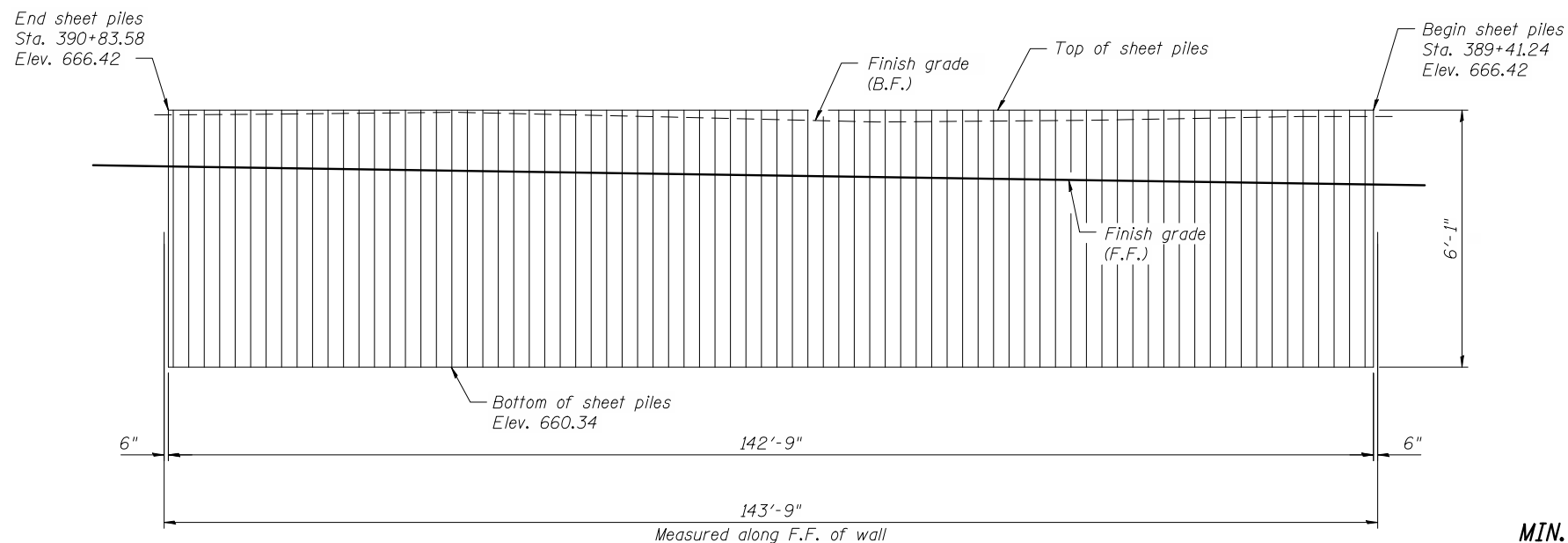
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1321	32-WRS-5	DU PAGE	495	332
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



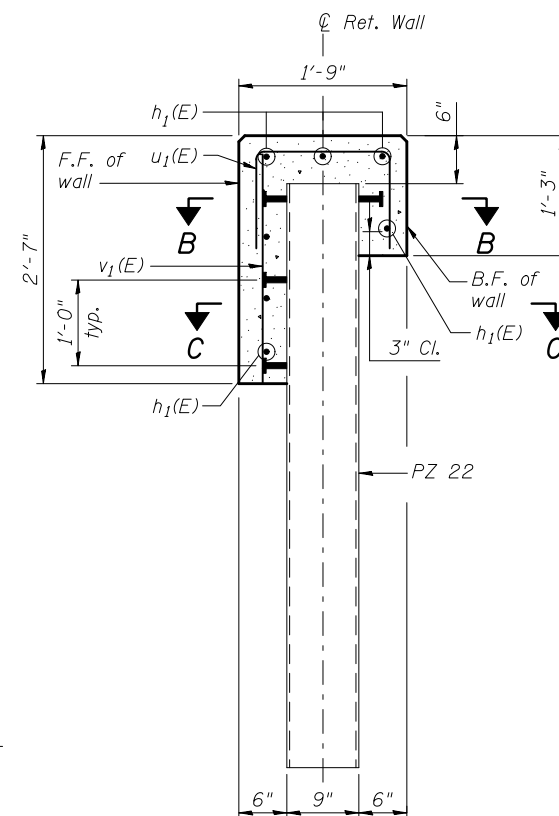
PLAN



CONCRETE FACING ELEVATION
(Front face looking south)



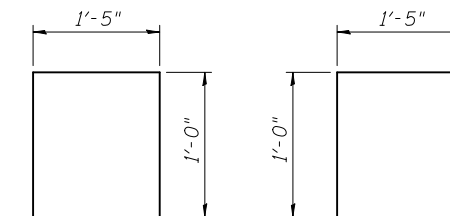
SHEET PILE ELEVATION
(Front face looking south)



SECTION A-A

BILL OF BARS

Bar	No.	Size	Length	Shape
h ₁ (E)	42	#3	24'-9"	—
u ₁ (E)	148	#3	3'-5"	□
u ₂ (E)	4	#3	2'-5"	□
v ₁ (E)	146	#3	2'-5"	—

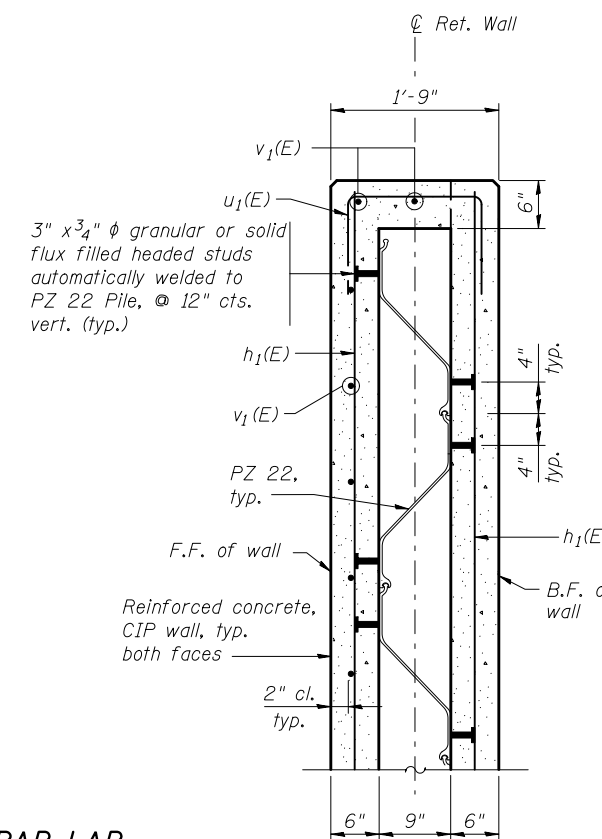


BAR u₁(E)

BAR u₂(E)

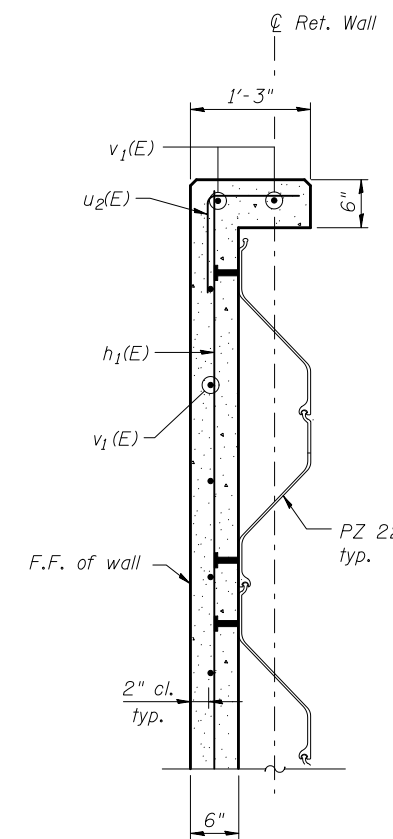
BILL OF MATERIAL

ITEM	UNIT	TOTAL
Permanent Steel Sheet Piling	Sq. Ft.	869
Concrete Structures	Cu. Yd.	12.3
Stud Shear Connectors	Each	312
Reinforcement Bars, Epoxy Coated	Pound	720
Concrete Sealer	Sq. Ft.	644



SECTION B-B

MIN. BAR LAP
#3 bar = 1'-0"



SECTION C-C

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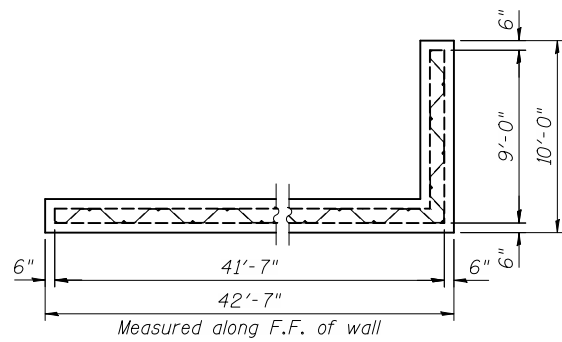
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

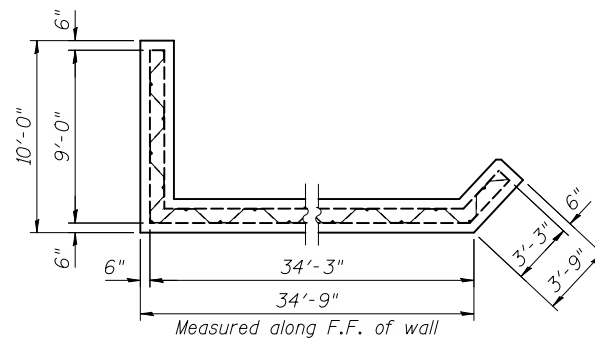
**IP-RW-1 PLAN AND ELEVATION
IRVING PARK RD RETAINING WALLS**

SHEET NO. 5 OF 13 SHEETS

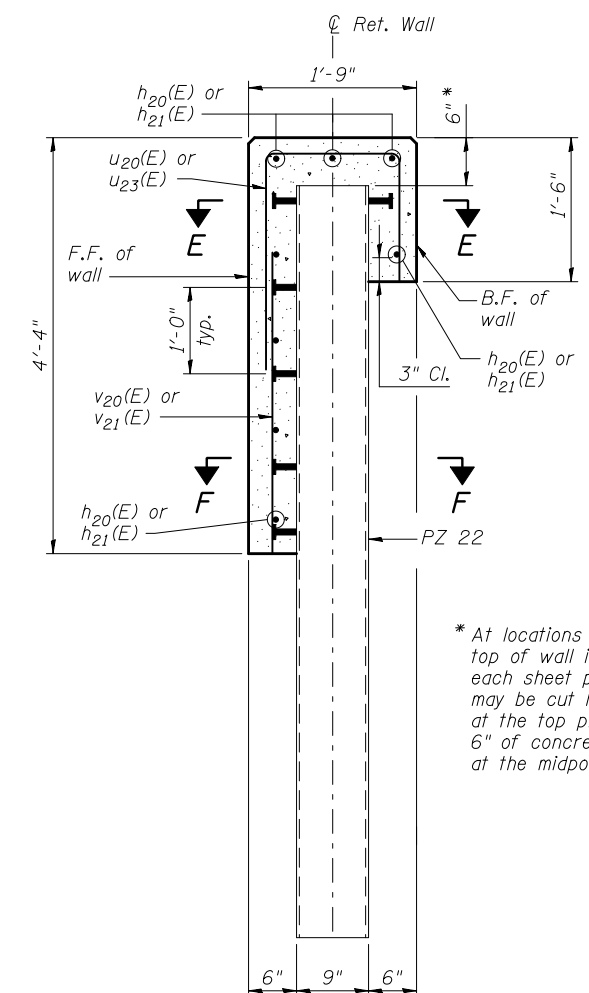
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CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



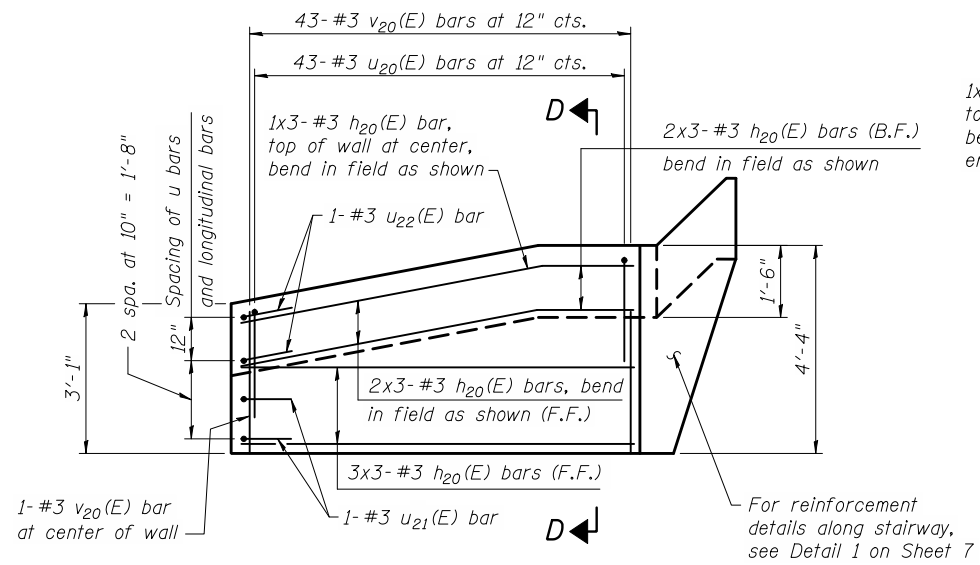
PLAN
IP-RW-2b



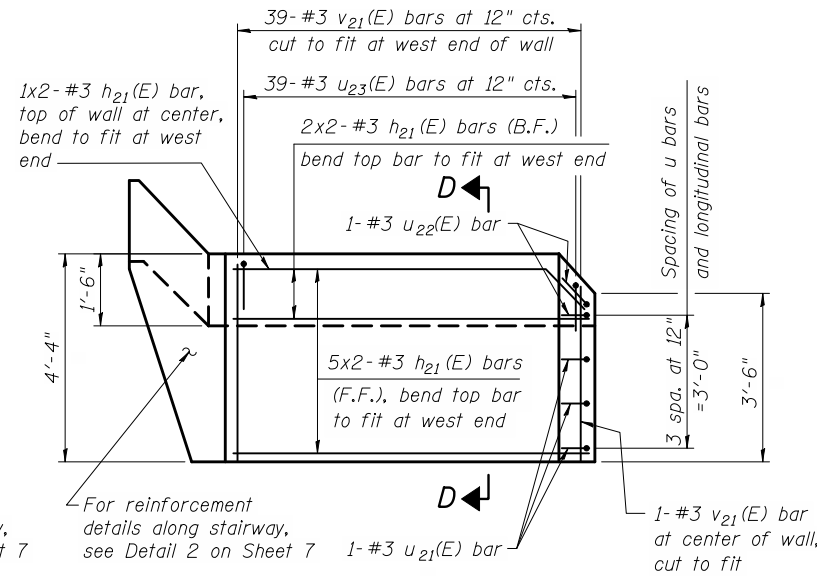
PLAN
IP-RW-2a



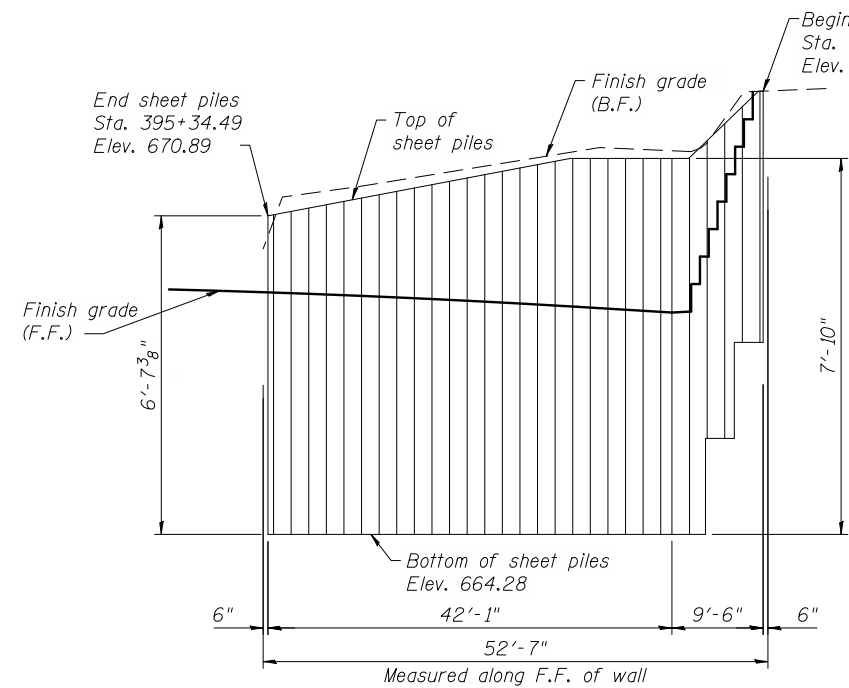
* At locations where the top of wall is sloped, each sheet pile section may be cut horizontally at the top provided that 6" of concrete is maintained at the midpoint of the section.



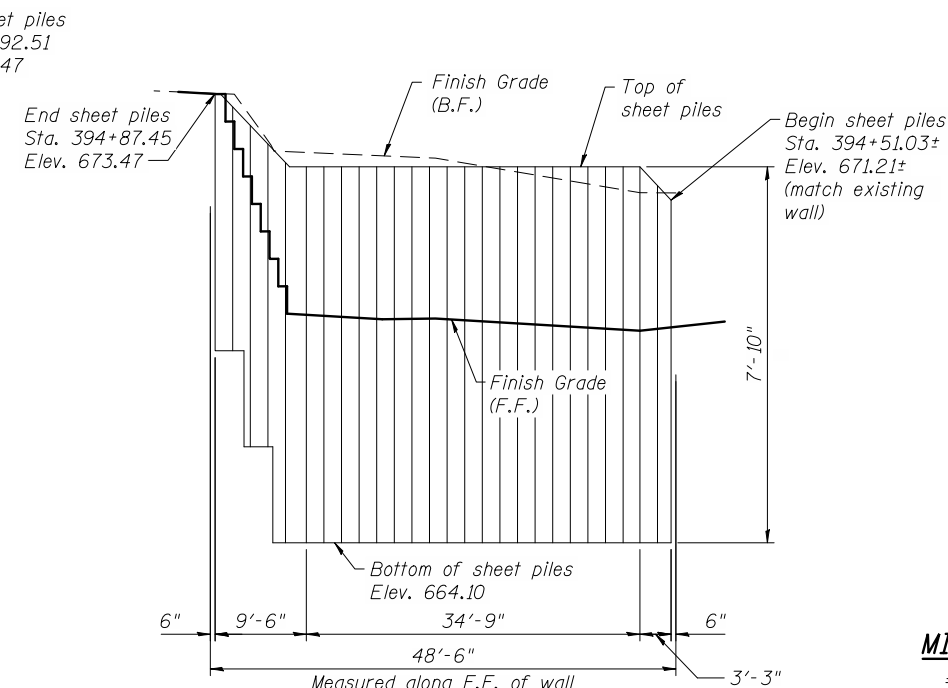
CONCRETE FACING DEVELOPED ELEVATION
IP-RW-2b, Front face looking south



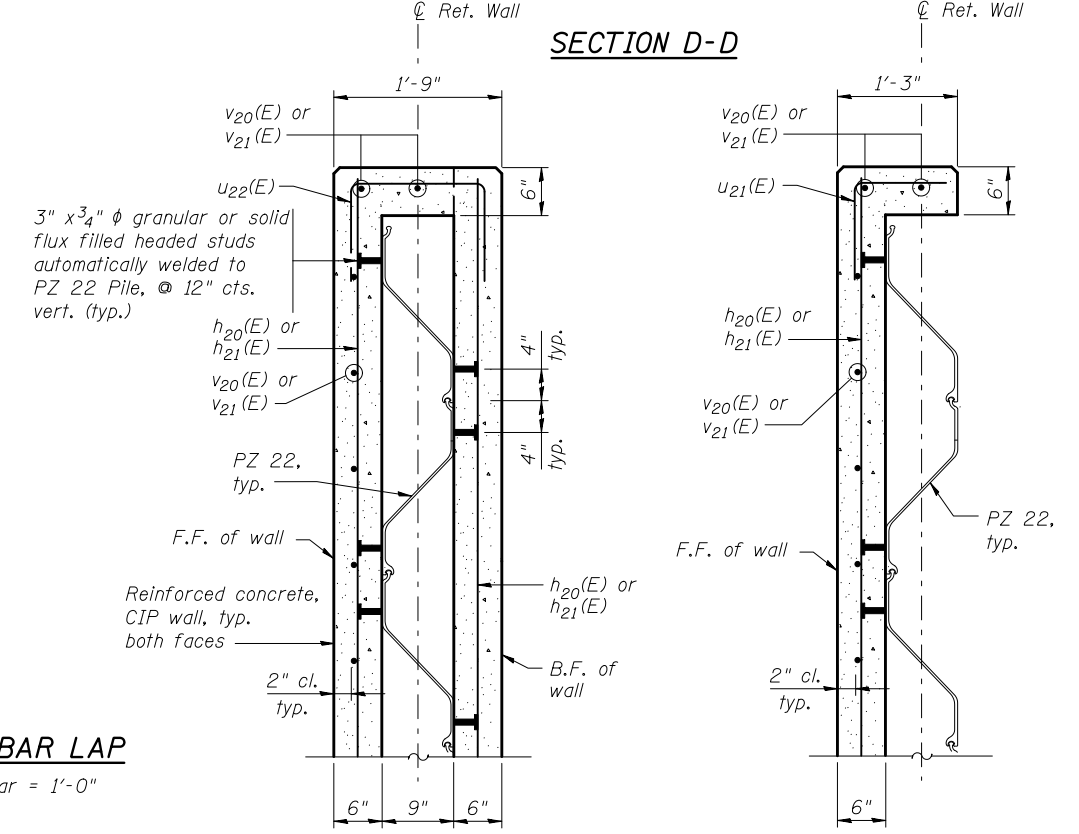
CONCRETE FACING DEVELOPED ELEVATION
IP-RW-2a, Front face looking south



SHEET PILE DEVELOPED ELEVATION
IP-RW-2b, Front face looking south



SHEET PILE DEVELOPED ELEVATION
IP-RW-2a, Front face looking south



MIN. BAR LAP
#3 bar = 1'-0"

SECTION E-E

SECTION F-F

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IP-RW-2a & IP-RW-2b PLAN AND ELEVATION
IRVING PARK RD RETAINING WALLS

SHEET NO. 6 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	334
CONTRACT NO. 60B42				

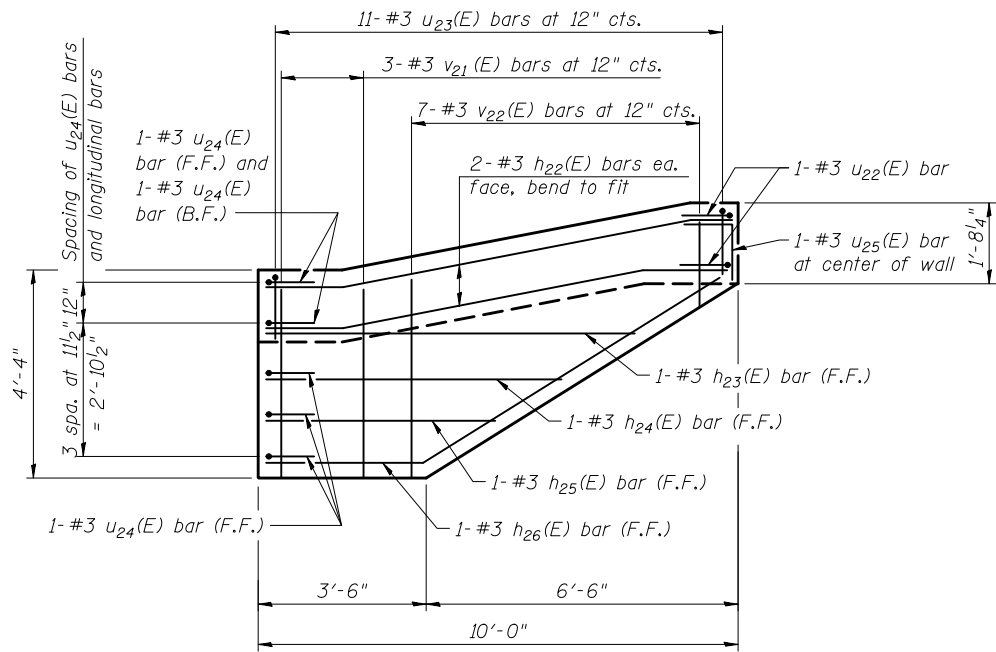
ILLINOIS FED. AID PROJECT

BILL OF BARS

Bar	No.	Size	Length	Shape
h ₂₀ (E)	24	#3	14'-9"	—
h ₂₁ (E)	16	#3	19'-8"	—
h ₂₂ (E)	8	#3	9'-9"	—
h ₂₃ (E)	2	#3	7'-9"	—
h ₂₄ (E)	2	#3	6'-4"	—
h ₂₅ (E)	2	#3	4'-9"	—
h ₂₆ (E)	2	#3	10'-9"	—
h ₂₇ (E)	5	#3	8'-1"	—
h ₂₈ (E)	8	#3	3'-9"	—
u ₂₀ (E)	43	#3	5'-0"	┌
u ₂₁ (E)	6	#3	2'-5"	┌
u ₂₂ (E)	8	#3	3'-5"	┌
u ₂₃ (E)	61	#3	4'-1"	┌
u ₂₄ (E)	14	#3	2'-0"	┌
u ₂₅ (E)	2	#3	2'-4"	┌
v ₂₀ (E)	44	#3	2'-11"	—
v ₂₁ (E)	46	#3	4'-2"	—
v ₂₂ (E)	7	#3	6'-2"	—

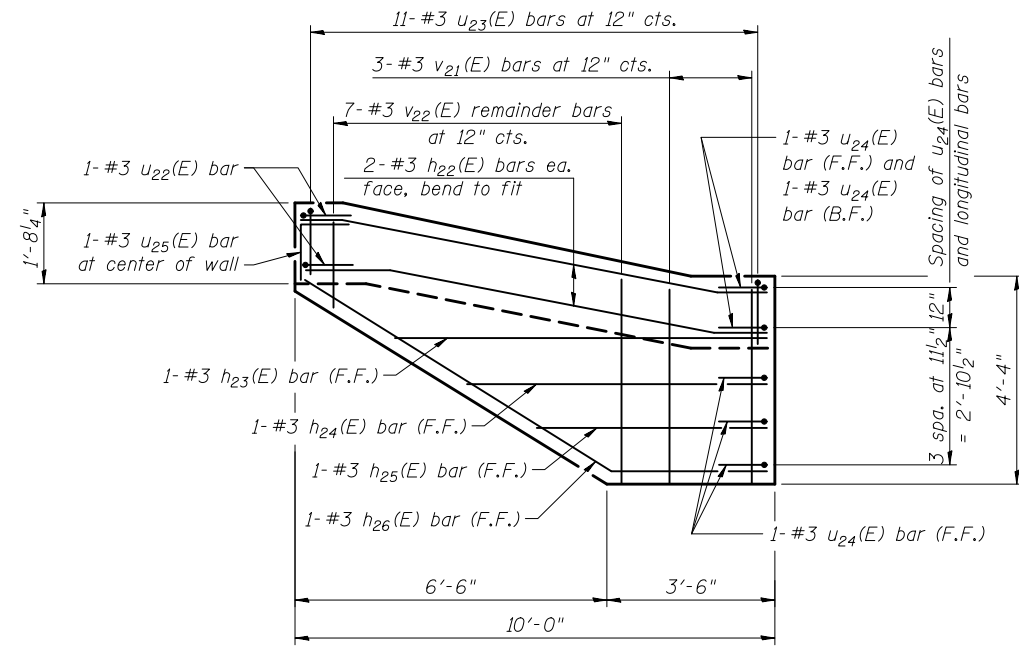
BILL OF MATERIAL

ITEM	UNIT	TOTAL
Permanent Steel Sheet Piling	Sq. Ft.	733
Concrete Structures	Cu. Yd.	12.9
Stud Shear Connectors	Each	317
Reinforcement Bars, Epoxy Coated	Pound	670
Pipe Handrail	Foot	102.5
Concrete Sealer	Sq. Ft.	534



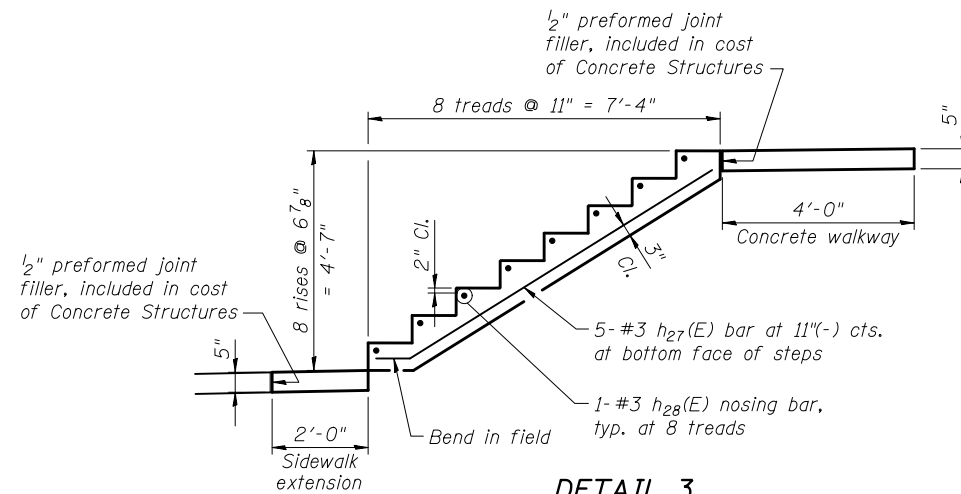
DETAIL 1

IP-RW-2b, along stairway



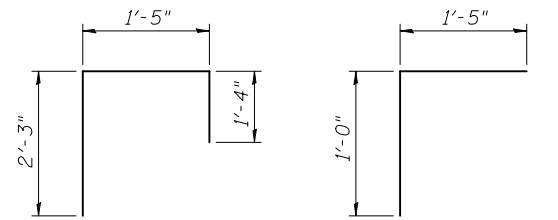
DETAIL 2

IP-RW-2a, along stairway



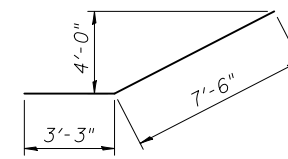
DETAIL 3

New concrete at stairway
(paid for as Concrete Structures)

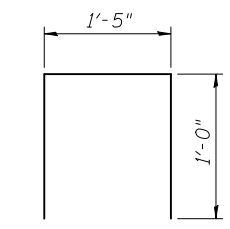


BAR u₂₀(E)

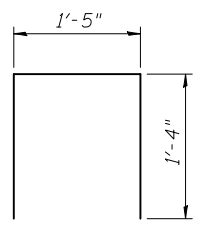
BAR u₂₁(E)



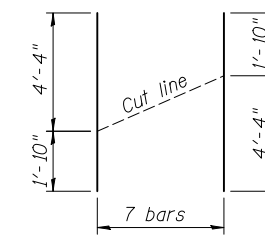
BAR h₂₆(E)



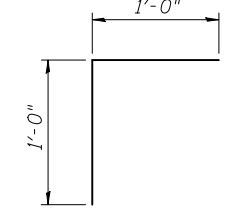
BAR u₂₂(E)



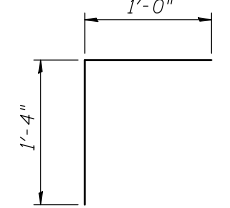
BAR u₂₃(E)



BAR v₂₂(E)



BAR u₂₄(E)



BAR u₂₅(E)

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jbabushr

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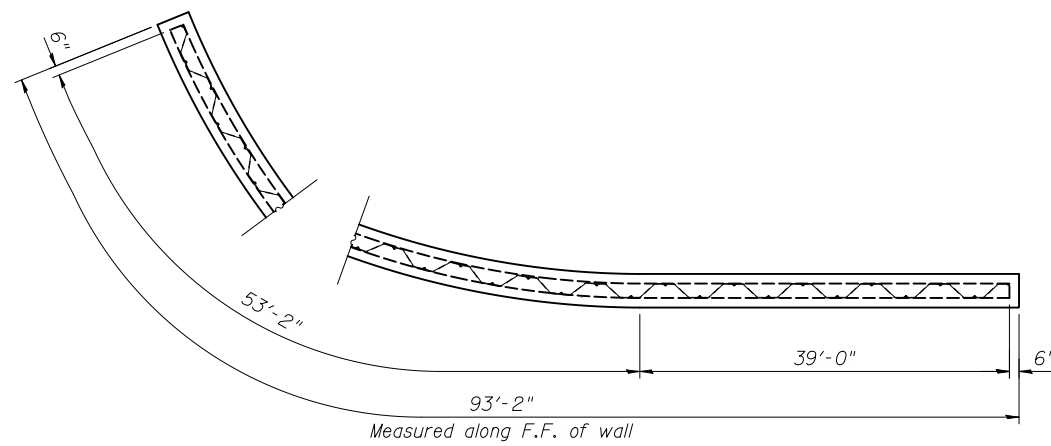
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FILE NAME = WYYY-60B42-007-WEL.DGN	CHECKED - VEVS	REVISED -
PLOT SCALE = NONE	DRAWN - JAR	REVISED -
PLOT DATE = 12-19-2014	CHECKED - VEVS	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

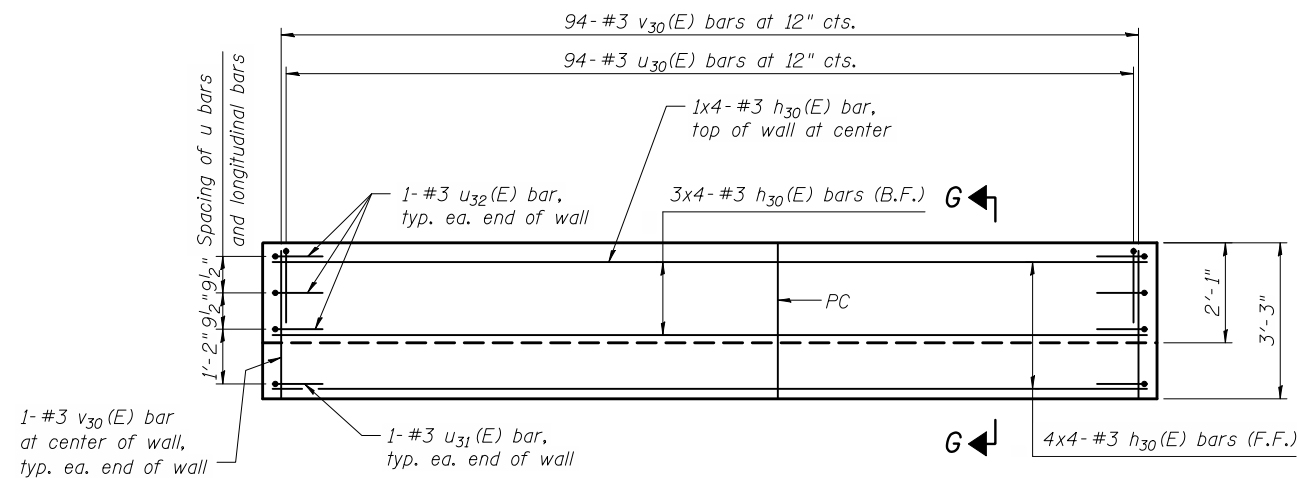
**IP-RW-2a & IP-RW-2b DETAILS
IRVING PARK RD RETAINING WALLS**

SHEET NO. 7 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	335
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

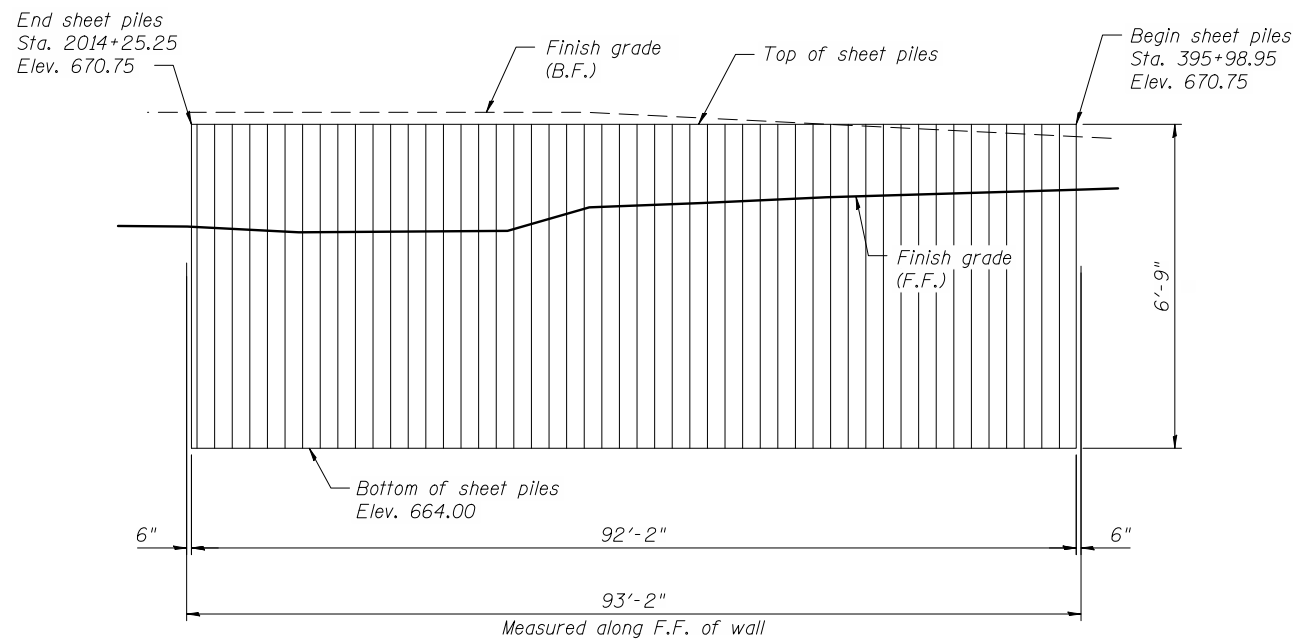


PLAN



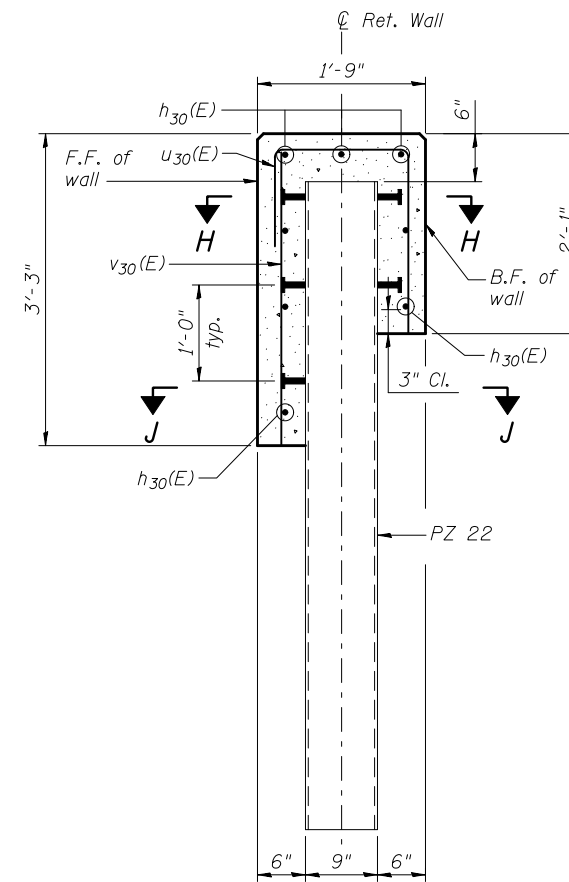
CONCRETE FACING DEVELOPED ELEVATION

(Front face looking south)

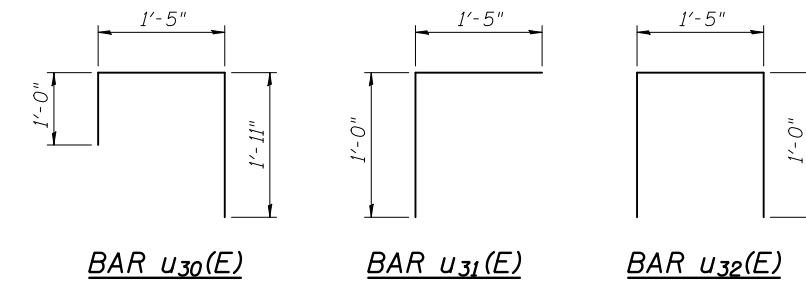


SHEET PILE DEVELOPED ELEVATION

(Front face looking south)



SECTION G-G

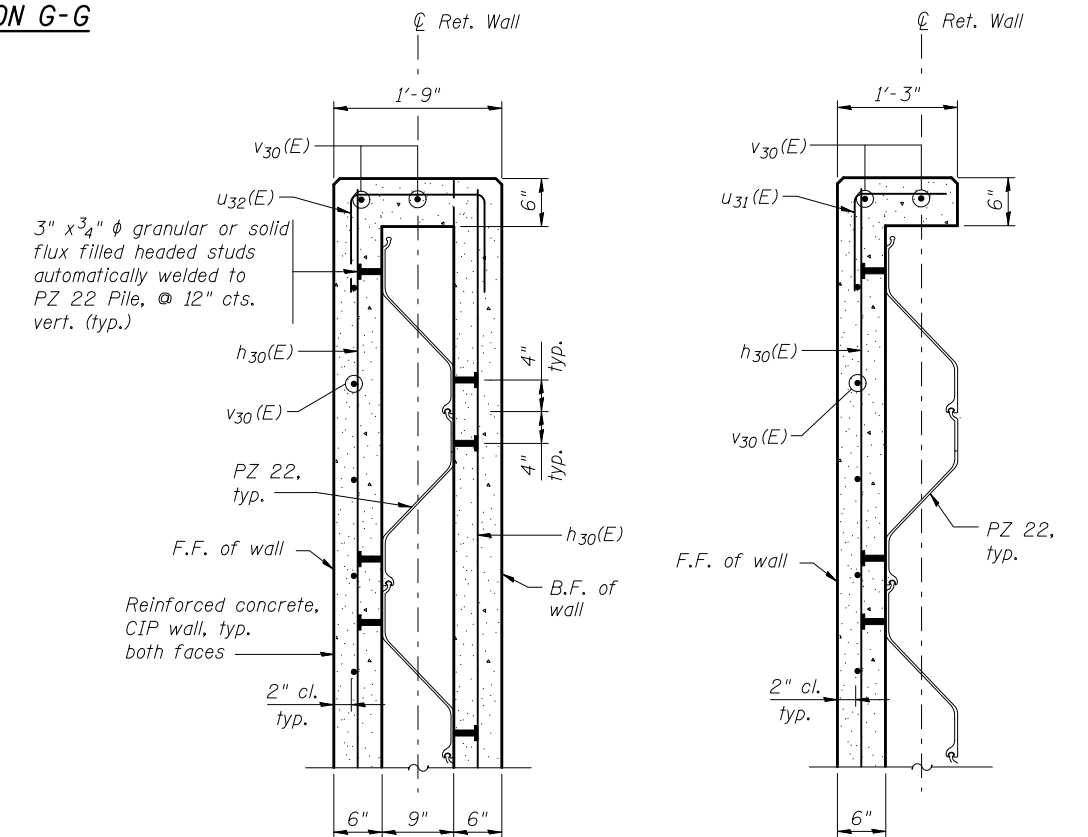


BILL OF BARS

Bar	No.	Size	Length	Shape
h ₃₀ (E)	32	#3	24'-0"	—
u ₃₀ (E)	94	#3	4'-4"	┌
u ₃₁ (E)	2	#3	2'-5"	┌
u ₃₂ (E)	6	#3	3'-5"	┌
v ₃₀ (E)	96	#3	3'-1"	—

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Permanent Steel Sheet Piling	Sq. Ft.	623
Concrete Structures	Cu. Yd.	10.5
Stud Shear Connectors	Each	250
Reinforcement Bars, Epoxy Coated	Pound	570
Concrete Sealer	Sq. Ft.	423



SECTION H-H

SECTION J-J

MIN. BAR LAP

#3 bar = 1'-0"

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jbbushr

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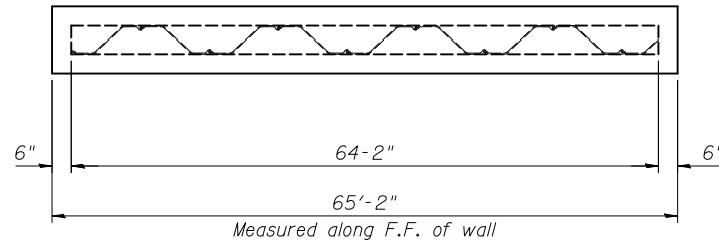
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PLOT SCALE = NONE	DRAWN - JAR	REVISED -
PLOT DATE = 12-19-2014	CHECKED - VEVS	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

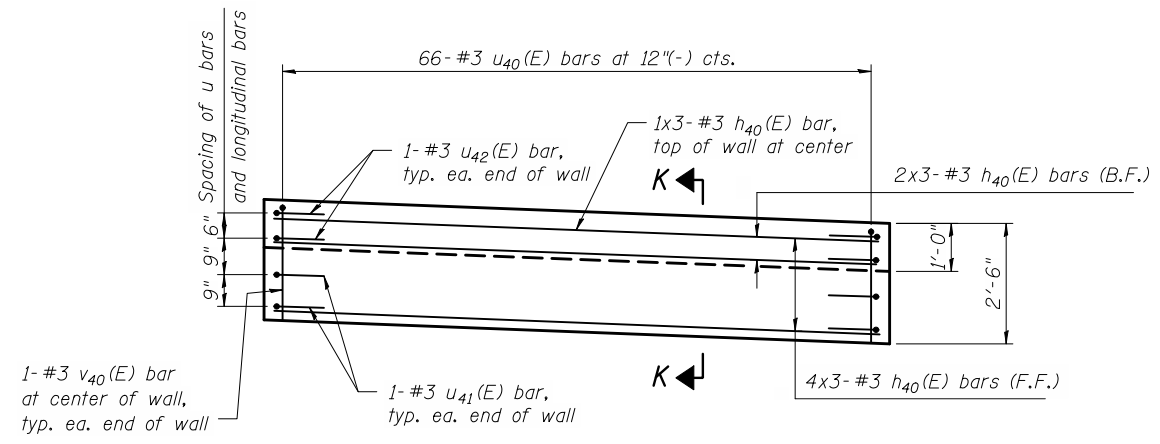
**IP-RW-3 PLAN AND ELEVATION
IRVING PARK RD RETAINING WALLS**

SHEET NO. 8 OF 13 SHEETS

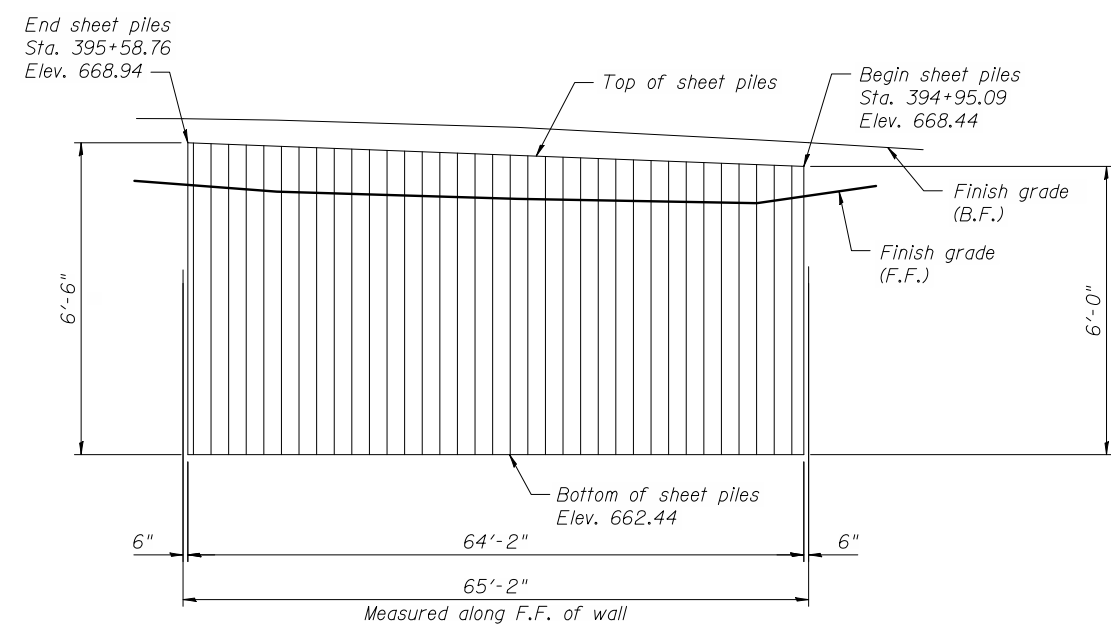
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	336
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



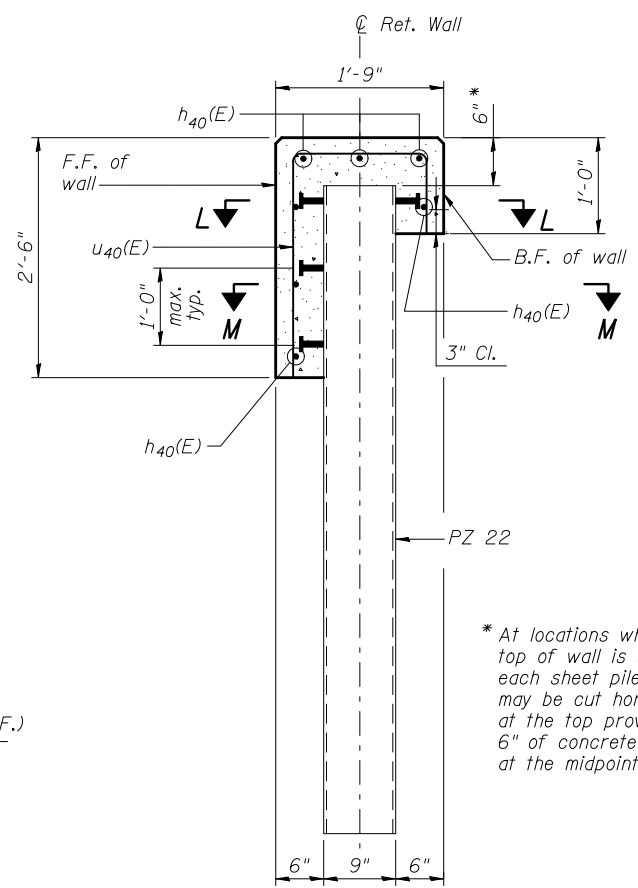
PLAN



CONCRETE FACING ELEVATION
(Front face looking south)

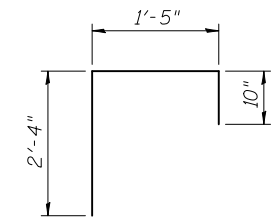


SHEET PILE ELEVATION
(Front face looking south)

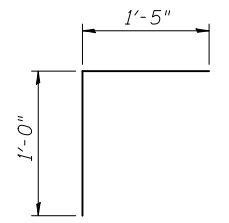


SECTION K-K

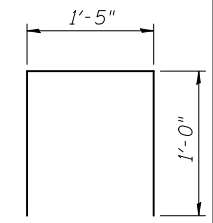
* At locations where the top of wall is sloped, each sheet pile section may be cut horizontally at the top provided that 6" of concrete is maintained at the midpoint of the section.



BAR u40(E)



BAR u41(E)



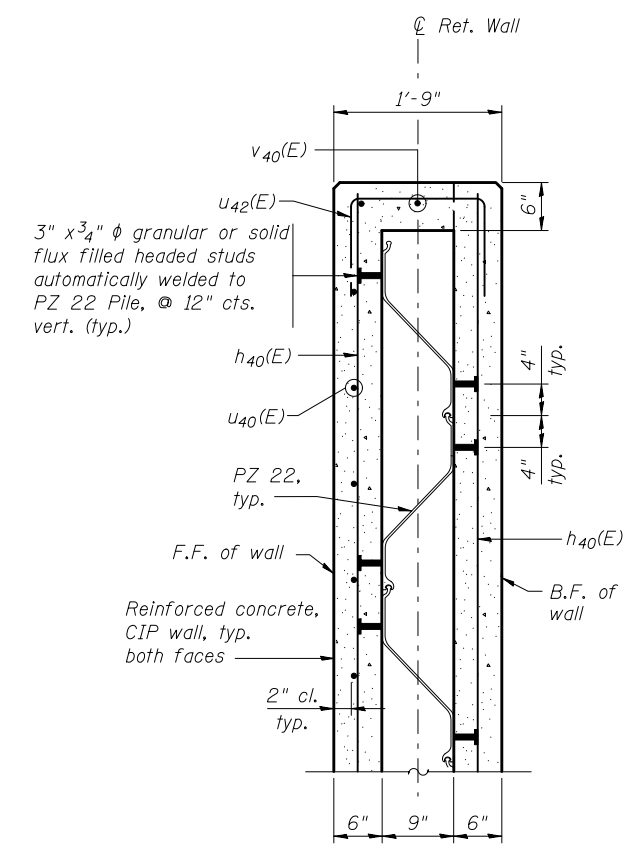
BAR u42(E)

BILL OF BARS

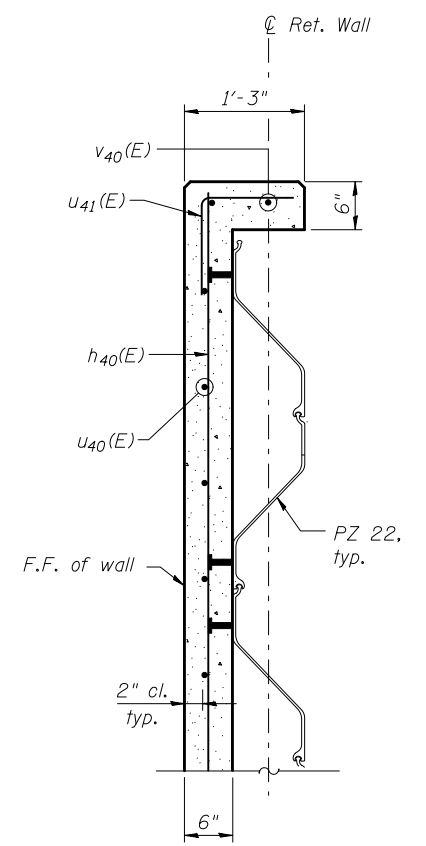
Bar	No.	Size	Length	Shape
h40(E)	21	#3	22'-4"	—
u40(E)	66	#3	4'-7"	┌
u41(E)	4	#3	2'-5"	┌
u42(E)	4	#3	3'-5"	┌
v40(E)	2	#3	2'-4"	—

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Permanent Steel Sheet Piling	Sq. Ft.	402
Concrete Structures	Cu. Yd.	5.2
Stud Shear Connectors	Each	140
Reinforcement Bars, Epoxy Coated	Pound	310
Pipe Handrail	Foot	65.0
Concrete Sealer	Sq. Ft.	209



SECTION L-L



SECTION M-M

MIN. BAR LAP
#3 bar = 1'-0"

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USER NAME = jbabushr	DESIGNED - JAR	REVISED -
FILE NAME = WYYY-60842-009-WEL.DGN	CHECKED - VEVS	REVISED -
PLOT SCALE = NONE	DRAWN - JAR	REVISED -
PLOT DATE = 12-19-2014	CHECKED - VEVS	REVISED -

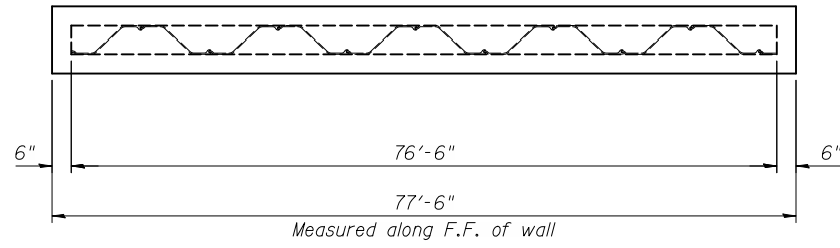
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IP-RW-4 PLAN AND ELEVATION
IRVING PARK RD RETAINING WALLS

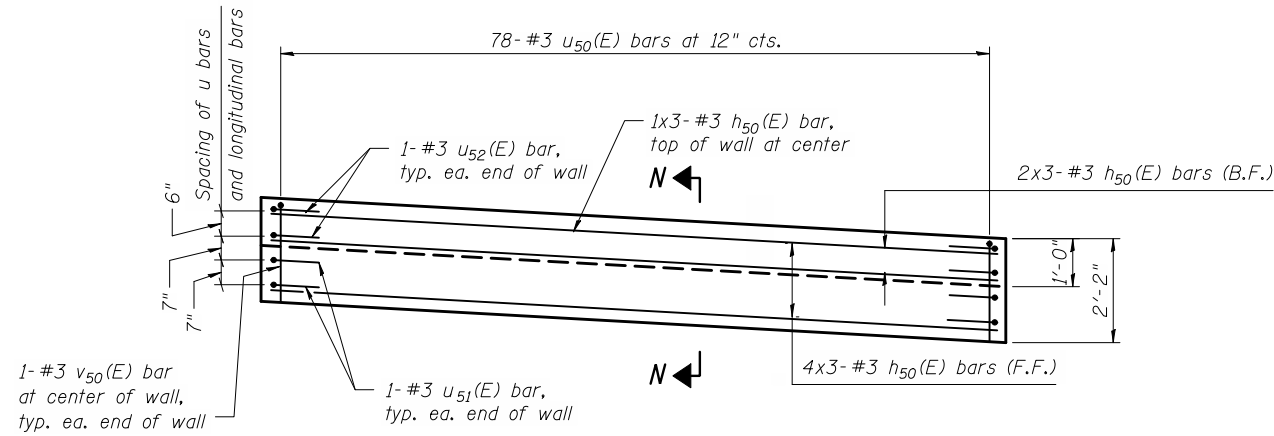
SHEET NO. 9 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	337
CONTRACT NO. 60842				

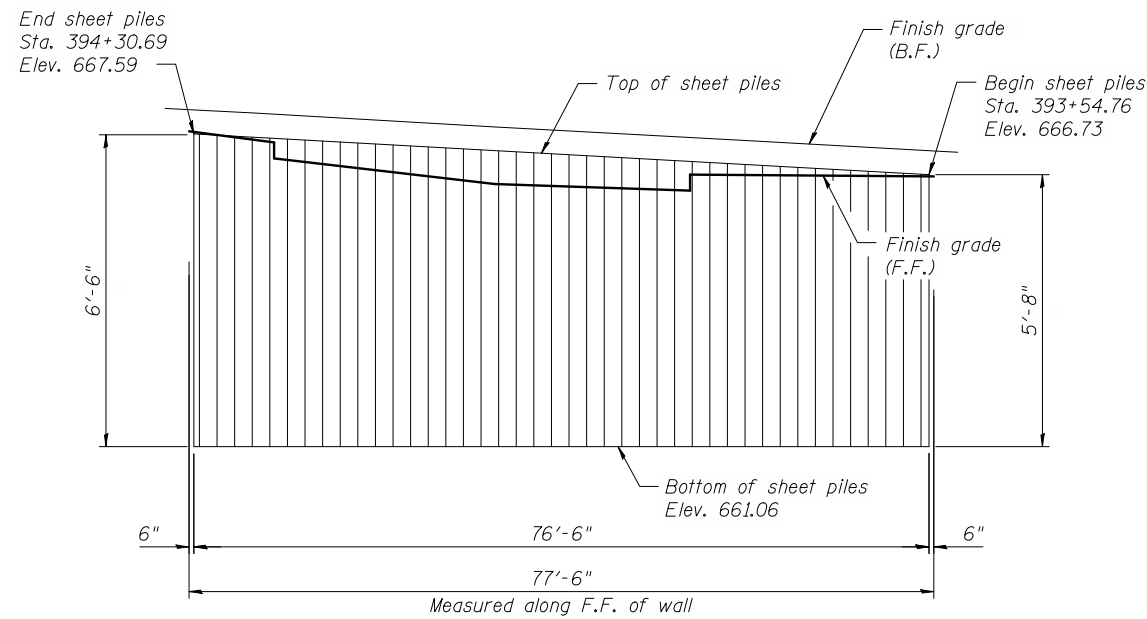
ILLINOIS FED. AID PROJECT



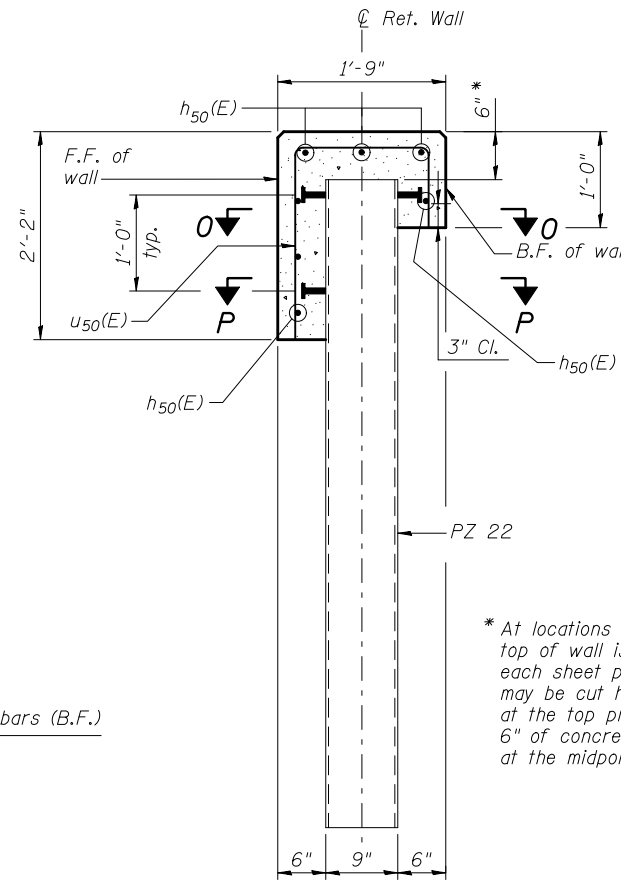
PLAN



CONCRETE FACING ELEVATION
(Front face looking south)

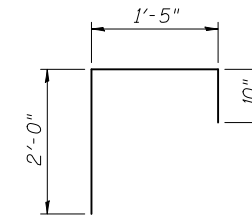


SHEET PILE ELEVATION
(Front face looking south)

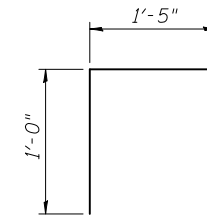


SECTION N-N

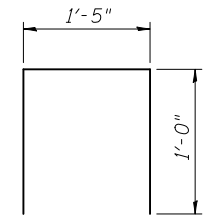
* At locations where the top of wall is sloped, each sheet pile section may be cut horizontally at the top provided that 6" of concrete is maintained at the midpoint of the section.



BAR u50(E)



BAR u51(E)



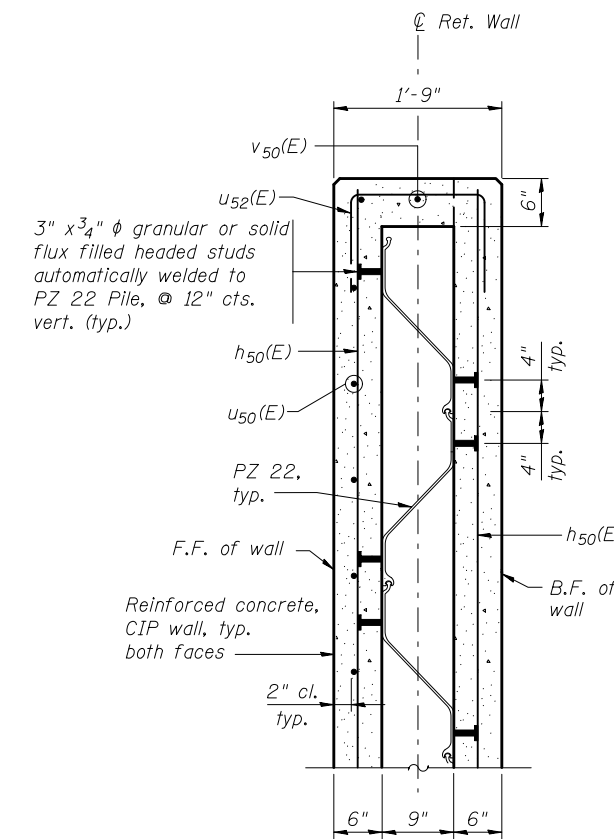
BAR u52(E)

BILL OF BARS

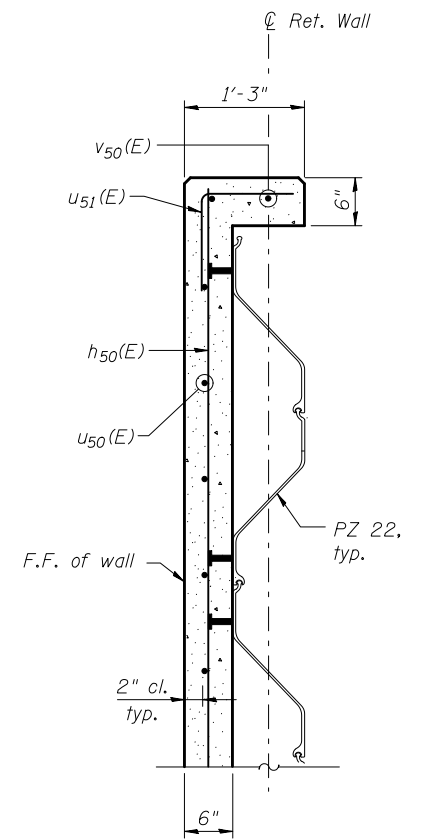
Bar	No.	Size	Length	Shape
h50(E)	21	#3	26'-5"	—
u50(E)	78	#3	4'-3"	┌
u51(E)	4	#3	2'-5"	┌
u52(E)	4	#3	3'-5"	┌
v50(E)	2	#3	2'-0"	—

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Permanent Steel Sheet Piling	Sq. Ft.	466
Concrete Structures	Cu. Yd.	5.7
Stud Shear Connectors	Each	126
Reinforcement Bars, Epoxy Coated	Pound	350
Pipe Handrail	Foot	77.5
Concrete Sealer	Sq. Ft.	209



SECTION O-O



SECTION P-P

MIN. BAR LAP
#3 bar = 1'-0"

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USER NAME = jbbushr	DESIGNED - JAR	REVISED -
FILE NAME = WYYY-60B42-010-WEL.DGN	CHECKED - VEVS	REVISED -
PLOT SCALE = NONE	DRAWN - JAR	REVISED -
PLOT DATE = 12-19-2014	CHECKED - VEVS	REVISED -

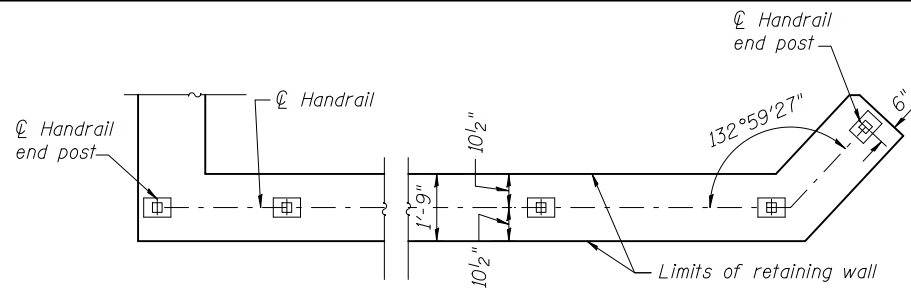
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IP-RW-5 PLAN AND ELEVATION
IRVING PARK RD RETAINING WALLS**

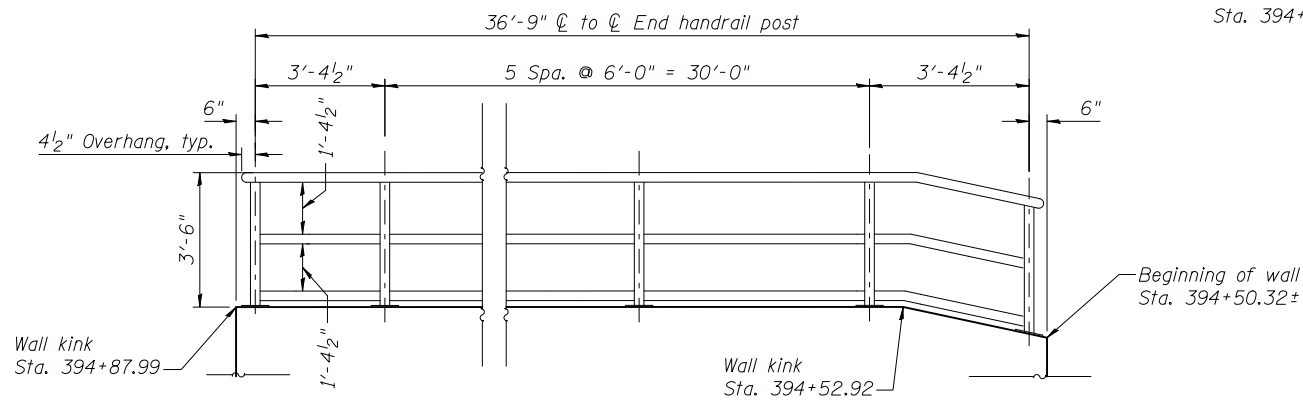
SHEET NO. 10 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	338
CONTRACT NO. 60B42				

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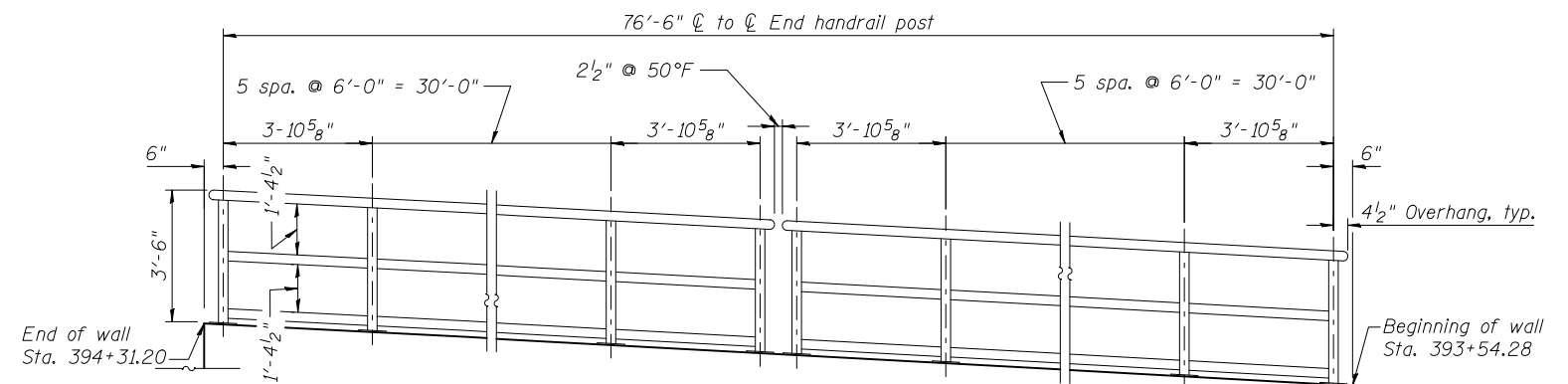


IP-RW-2a HANDRAIL PLAN VIEW



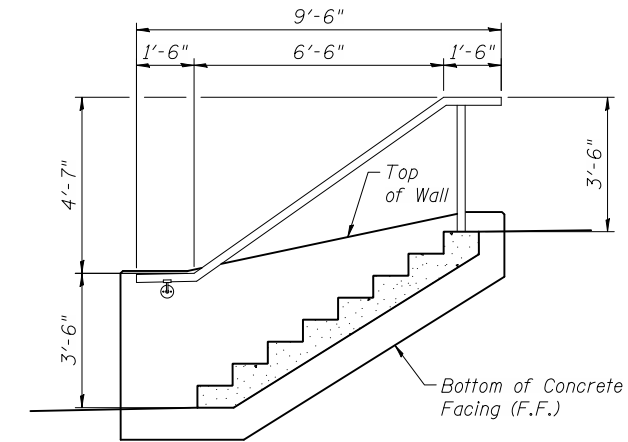
IP-RW-2a HANDRAIL DEVELOPED ELEVATION

(Front face looking south)
Note: Horizontal dimensions are along centerline of handrail



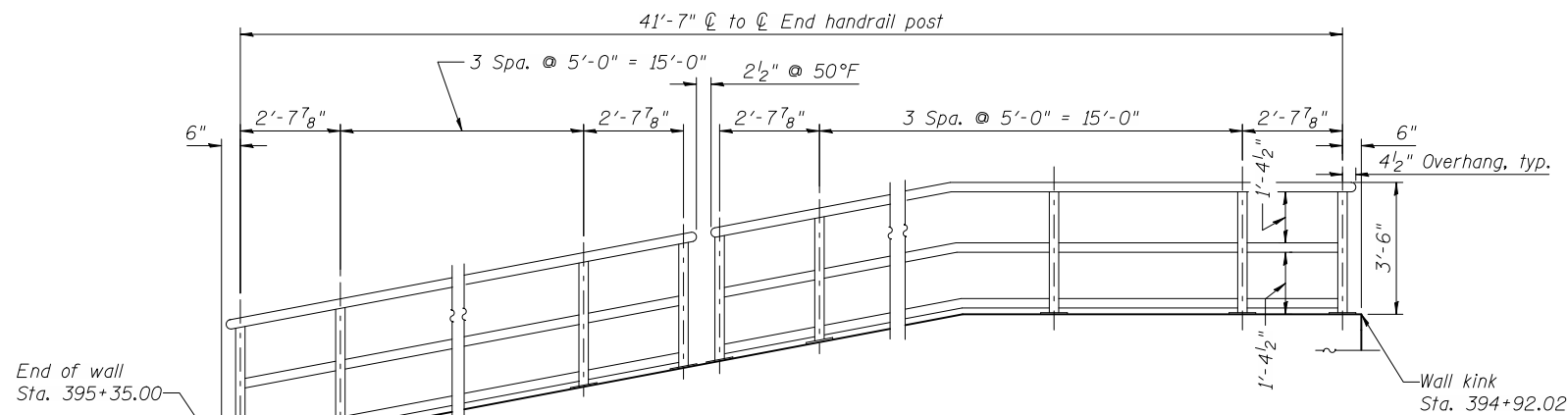
IP-RW-5 HANDRAIL ELEVATION

(Front face looking south)



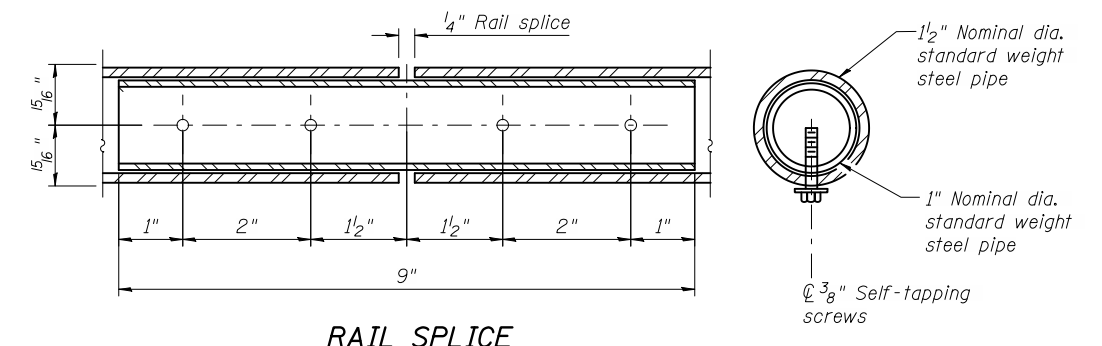
HANDRAIL ELEVATION AT STAIRS

(At IP-RW-2b looking east; IP-RW-2a similar)

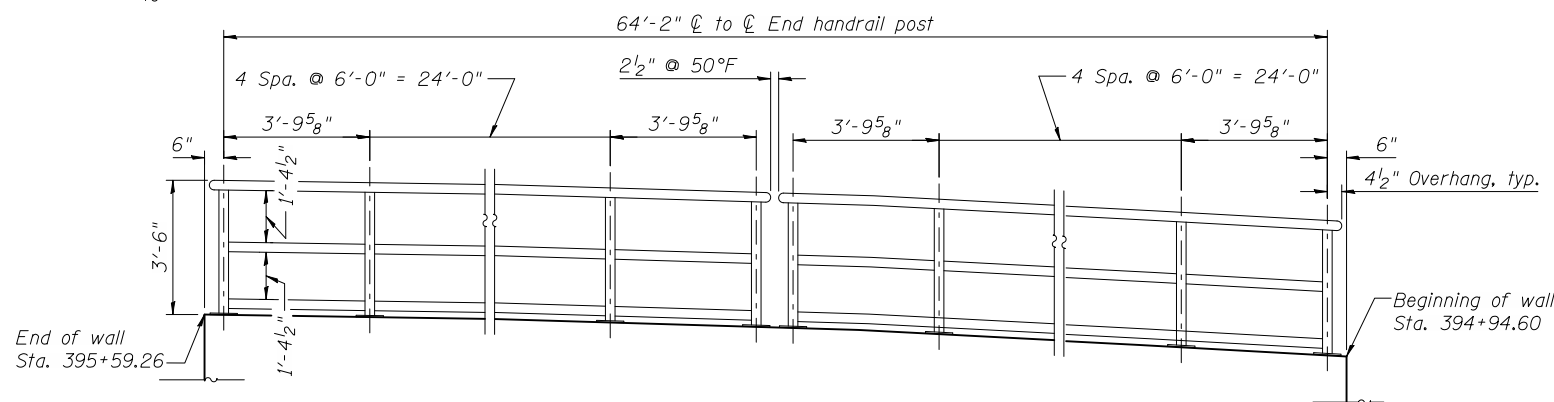


IP-RW-2b HANDRAIL ELEVATION

(Front face looking south)

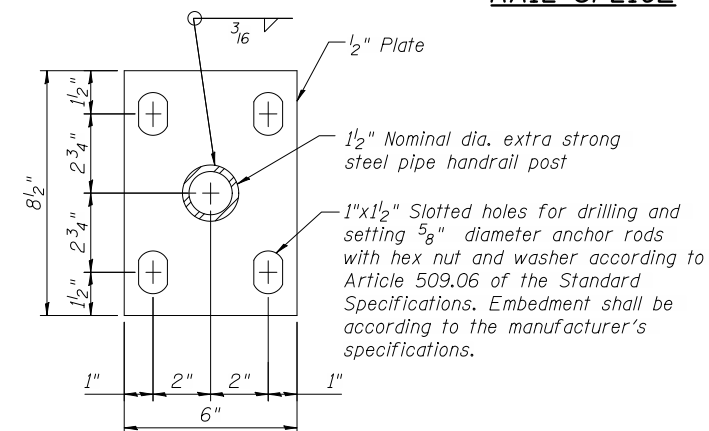


RAIL SPLICE



IP-RW-4 HANDRAIL ELEVATION

(Front face looking south)



BASE PLATE

NOTES:

1. Railings shall be 1/2" dia. standard (Schedule 40) pipe and the posts shall be 1/2" dia. extra strong (Schedule 80) pipe. See Article 509 of the Standard Specifications for additional information regarding Pipe Handrail.
2. All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.
3. Handrail shall be centered on wall. If wall width differs from plan dimension due to sheet pile section selected by the Contractor, the handrail position shall be adjusted to maintain the sidewalk width shown.

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USER NAME = jbabushr	DESIGNED - JAH	REVISD -
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PLOT SCALE = NONE	DRAWN - JAH	REVISD -
PLOT DATE = 12-19-2014	CHECKED - VEVS	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HANDRAIL DETAILS
IRVING PARK RD RETAINING WALLS**

SHEET NO. 11 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	339
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

SOIL BORING LOG

Geo Services, Inc. PAGE 1 of 1
 Geotechnical, Environmental & Civil Engineering
 805 Amberst Court, Suite 204
 Naperville, Illinois 60565
 (630) 355-2838

DATE 5/4/2010
 LOGGED BY DR
 GSI JOB No. 09165

ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri D-91-332-06
 SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. -
 Station -

BORING NO. **WB-01**
 Station: 390+22
 Offset: 39.0' Right
 Ground Surface Elev. 666.4

DEPTH (ft)	BLOW COUNT (blows/6")	UNSATURATED MOISTURE (%)	SPT (blows)	Soil Description		DEPTH (ft)	BLOW COUNT (blows/6")	UNSATURATED MOISTURE (%)	SPT (blows)
				Soil Type	Moisture				
0				Surface Water Elev. n/a		0			
				Stream Bed Elev. n/a					
				Groundwater Elevation:					
				First Encounter 655.4					
				Upon Completion 652.1					
				After Hrs. -					
0				3.0" ASPHALT, 9.0" CRUSHED STONE	665.4				
6				CLAY LOAM-dark gray-loose (Fill)	663.4				
4									
4									
3				TOPSOIL-black	660.9				
3									
5									
2				CLAY-brown & gray-stiff to very stiff (A-6) Wet					
3									
4									
2									
2									
10									
4				CLAY-gray-very stiff to hard (A-6)					
11									
15									
5									
8									
14									
6									
9				End Of Boring @ -20.0'					
				Hollow Stem Augers					
				CME Automatic Hammer	646.1-20	10	3.1B	19	

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-S Shelby Tube Sample VS-Vane Shear Test
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
 NR-No Recovery

SOIL BORING LOG

Geo Services, Inc. PAGE 1 of 1
 Geotechnical, Environmental & Civil Engineering
 805 Amberst Court, Suite 204
 Naperville, Illinois 60565
 (630) 355-2838

DATE 5/11/2010
 LOGGED BY DR
 GSI JOB No. 09165

ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri D-91-332-06
 SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. -
 Station -

BORING NO. **WB-02**
 Station: 394+06
 Offset: 43.0' Left
 Ground Surface Elev. 666.6

DEPTH (ft)	BLOW COUNT (blows/6")	UNSATURATED MOISTURE (%)	SPT (blows)	Soil Description		DEPTH (ft)	BLOW COUNT (blows/6")	UNSATURATED MOISTURE (%)	SPT (blows)
				Soil Type	Moisture				
0				Surface Water Elev. n/a		0			
				Stream Bed Elev. n/a					
				Groundwater Elevation:					
				First Encounter 648.1					
				Upon Completion 651.6					
				After Hrs. -					
0				4.0" ASPHALT, 8.0" CRUSHED STONE	665.6				
3				TOPSOIL-black	663.6				
3									
5									
3				CLAY-brown & gray-hard (A-6)					
4									
5									
6									
9									
4				CLAY-gray-stiff to very stiff (A-6)					
5									
6									
7									
3									
3									
15									
3									
4									
3									
4									
4									
4				End Of Boring @ -20.0'					
				Hollow Stem Augers					
				CME Automatic Hammer	646.6-20	4	1.3B	21	

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-S Shelby Tube Sample VS-Vane Shear Test
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
 NR-No Recovery

SOIL BORING LOG

Geo Services, Inc. PAGE 1 of 1
 Geotechnical, Environmental & Civil Engineering
 805 Amberst Court, Suite 204
 Naperville, Illinois 60565
 (630) 355-2838

DATE 5/4/2010
 LOGGED BY DR
 GSI JOB No. 09165

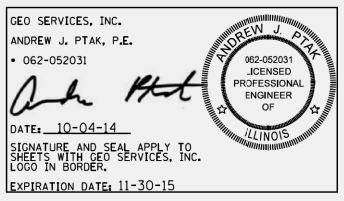
ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri D-91-332-06
 SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. -
 Station -

BORING NO. **WB-03**
 Station: 394+95
 Offset: 73.5' Right
 Ground Surface Elev. 673.3

DEPTH (ft)	BLOW COUNT (blows/6")	UNSATURATED MOISTURE (%)	SPT (blows)	Soil Description		DEPTH (ft)	BLOW COUNT (blows/6")	UNSATURATED MOISTURE (%)	SPT (blows)
				Soil Type	Moisture				
0				Surface Water Elev. n/a		0			
				Stream Bed Elev. n/a					
				Groundwater Elevation:					
				First Encounter 662.3					
				Upon Completion 664.3					
				After Hrs. -					
0				3.0" TOPSOIL with Stone	673.0				
5									
5									
7									
4.5				CLAY-brown & gray-hard (A-6)					
5									
7									
8									
12									
15									
5				SILTY LOAM-brown-medium dense (A-4)					
7									
10									
6									
8									
10									
4				SANDY CLAY LOAM-brown-medium dense (A-2)					
9									
15									
2				CLAY-gray-medium stiff (A-6)					
3									
4									
2									
4				End Of Boring @ -20.0'					
				Hollow Stem Augers					
				CME Automatic Hammer	653.3-20	4	0.8B	22	

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-S Shelby Tube Sample VS-Vane Shear Test
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
 NR-No Recovery



c:\pwworking\oma\d0593506\WYYY-60842-012-SBL.DGN User

12-19-2014 12:03:52 PM



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PLOT DATE = 12-19-2014	CHECKED - DP	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SOIL BORING LOGS
 IRVING PARK RD RETAINING WALLS**

SHEET NO. 12 OF 13 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	340
CONTRACT NO. 60842				
ILLINOIS FED. AID PROJECT				

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User

12-19-2014
12:02:37 PM

PAGE 1 of 1

Geo Services, Inc. SOIL BORING LOG DATE 5/11/2010
 Geotechnical, Environmental & Civil Engineering 805 Amberst Court, Suite 204 Naperville, Illinois 60563 (630) 355-2838
 LOGGED BY DR GSI JOB No. 09165

ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri D-91-332-06
 SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. --
 Station --
 BORING NO. **WB-04**
 Station: 395+50
 Offset: 47.5' Left
 Ground Surface Elev. 667.9

DEPTH (ft)	BLOW COUNT (/6")	UNCONSOLIDATED SOIL TESTS (tsf) (%)	MOISTURE (%)	Surface Water Elev.		DEPTH (ft)	BLOW COUNT (/6")	UNCONSOLIDATED SOIL TESTS (tsf) (%)	MOISTURE (%)
				Stream Bed Elev.	Groundwater Elevation:				
				669.6	n/a				
				n/a	n/a				
				659.4	659.4				
				651.9	651.9				
3									
5									
8	4.5P	18							
4		105							
6									
9	3.4B	19							
6		111							
8									
9	3.9B	19							
5		104							
8									
10	11	3.5B	23						
7		104							
9									
9	2.5B	20							
654.9									
3		107							
5									
15	7	1.1B	21						
3		108							
4									
6	1.7B	21							
3		106							
5									
647.9-20.9		1.8B	22						

2.0" ASPHALT, 10.0" CRUSHED STONE 666.9

CLAY-brown & gray-very stiff to hard (A-6)

CLAY-gray-stiff (A-6)

End Of Boring @ -20.0'
 Hollow Stem Augers
 CME Automatic Hammer 647.9-20.9 1.8B 22

PAGE 1 of 1

Geo Services, Inc. SOIL BORING LOG DATE 5/3/2010
 Geotechnical, Environmental & Civil Engineering 805 Amberst Court, Suite 204 Naperville, Illinois 60563 (630) 355-2838
 LOGGED BY DR GSI JOB No. 09165

ROUTE IL RTE 19 DESCRIPTION Irving Park Rd, York Rd, CNRR Grade Separation Pri D-91-332-06
 SECTION 32 WRS-5 LOCATION Addison Township T40 N, R11 E, NW 1/4 Section 13
 COUNTY DuPage DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO. --
 Station --
 BORING NO. **WB-05**
 Station: 396+30
 Offset: 53.5' Right
 Ground Surface Elev. 670.6

DEPTH (ft)	BLOW COUNT (/6")	UNCONSOLIDATED SOIL TESTS (tsf) (%)	MOISTURE (%)	Surface Water Elev.		DEPTH (ft)	BLOW COUNT (/6")	UNCONSOLIDATED SOIL TESTS (tsf) (%)	MOISTURE (%)
				Stream Bed Elev.	Groundwater Elevation:				
				669.6	n/a				
				n/a	n/a				
				657.1	657.1				
				650.6	650.6				
3									
10									
2									
2	1.75P	15							
667.6									
3									
3									
4	2.25P	19							
6		110							
7									
8	4.2B	19							
5		108							
9									
10	6.0B	19							
4		108							
7									
10	4.0B	19							
657.1									
3									
5									
15	6	4.0P	18						
3		107							
5									
8	2.4B	20							
3		108							
6									
650.6-20.7		2.5B	20						

TOPSOIL-dark brown to black 669.6

SILTY CLAY-dark brown, gray & black-stiff (A-6) Fill 667.6


CLAY-brown & gray-very stiff to hard (A-6)

CLAY-gray-very stiff to hard (A-6)

End Of Boring @ -20.0'
 Hollow Stem Augers
 CME Automatic Hammer 650.6-20.7 2.5B 20

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (S-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
 NR-No Recovery

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (S-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%)
 NR-No Recovery

GEO SERVICES, INC.
 ANDREW J. PTAK, P.E.
 062-052031

 DATE: 10-04-14
 SIGNATURE AND SEAL APPLY TO SHEETS WITH GEO SERVICES, INC. LOGO IN BORDER.
 EXPIRATION DATE: 11-30-15

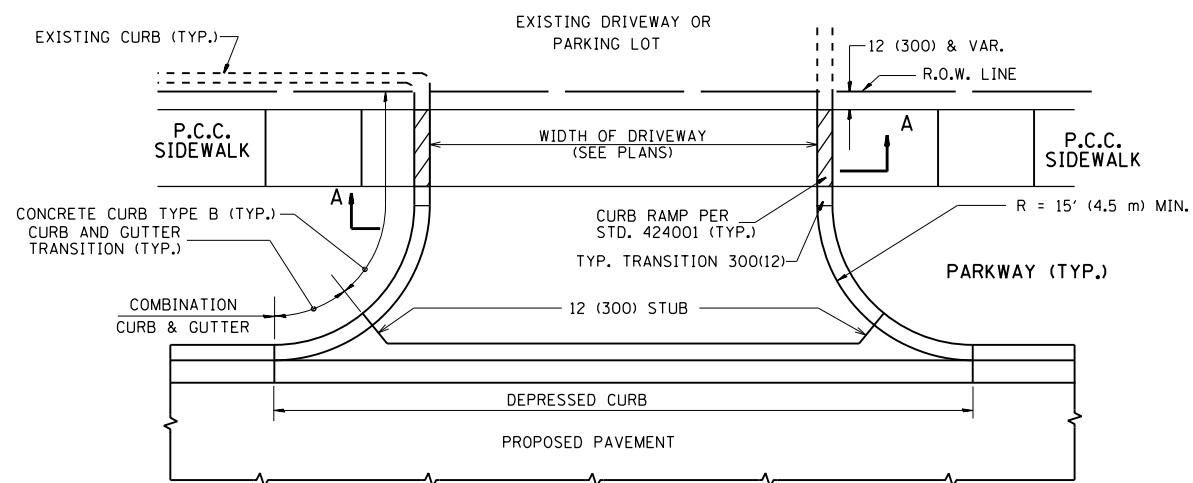
Geo Services, Inc.
 Geotechnical, Environmental & Civil Engineering 805 Amberst Court, Suite 204 Naperville, Illinois 60563 (630) 355-2838

USER NAME = user	DESIGNED - RR	REVISED -
FILE NAME = WYYY-60842-013-SBL.DGN	CHECKED - DP	REVISED -
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PLOT DATE = 12-19-2014	CHECKED - DP	REVISED -

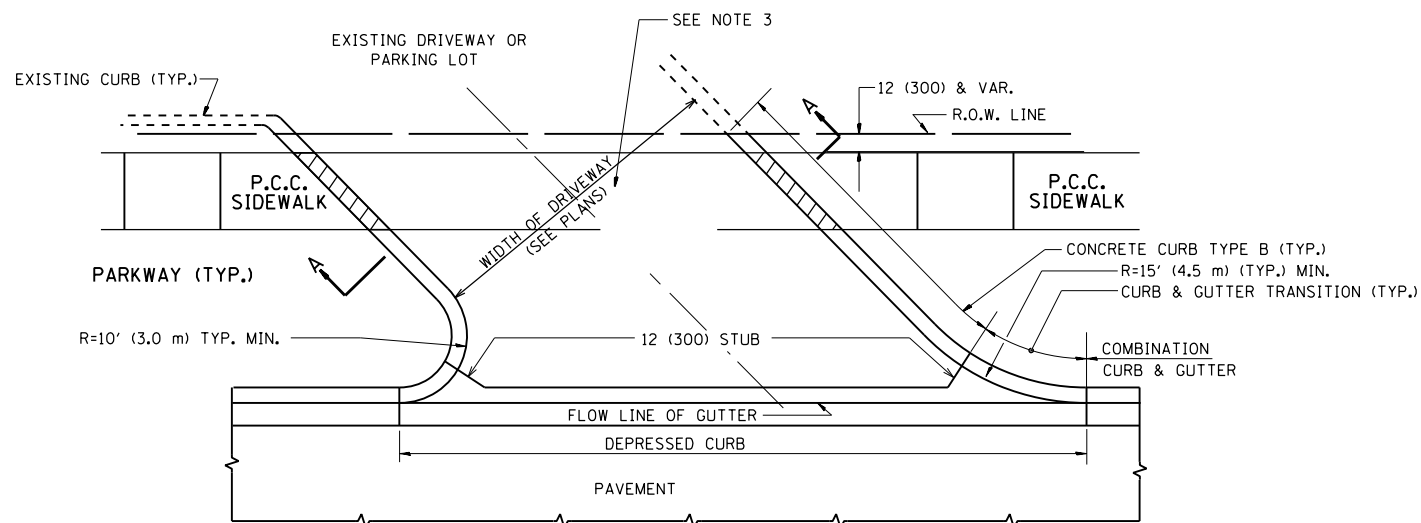
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS
 IRVING PARK RD RETAINING WALLS
 SHEET NO. 13 OF 13 SHEETS

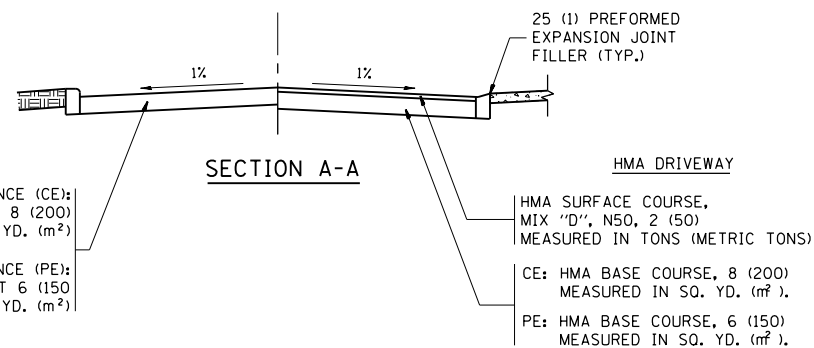
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32-WRS-5	DU PAGE	495	341
CONTRACT NO. 60842				
ILLINOIS FED. AID PROJECT				



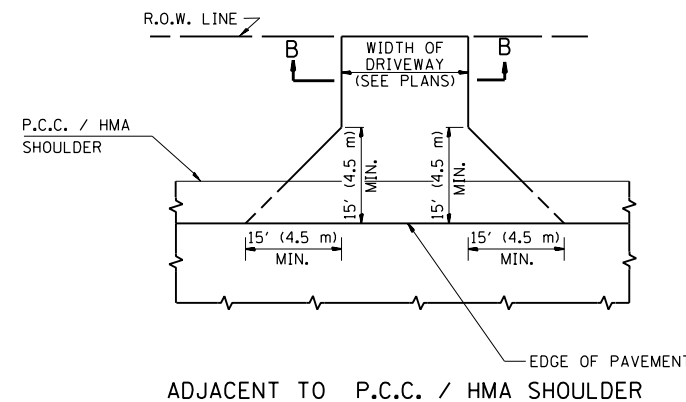
WITH CONCRETE CURB, TYPE B



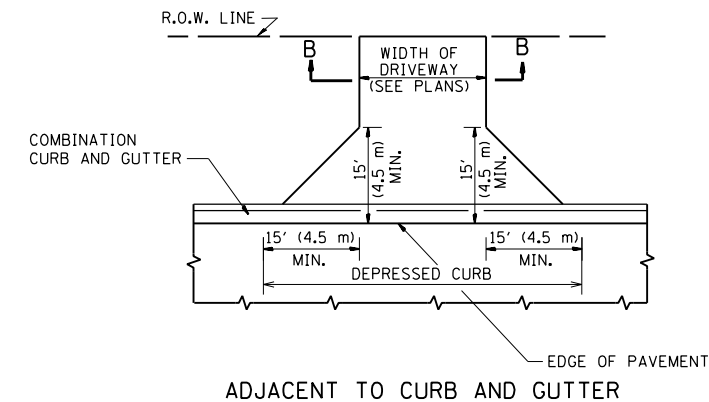
WITH CONCRETE CURB, TYPE B



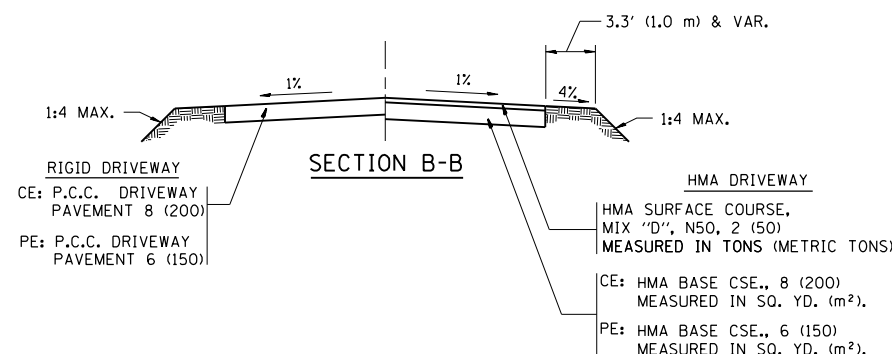
SECTION A-A



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



SECTION B-B

RURAL FIELD ENTRANCE (FE)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

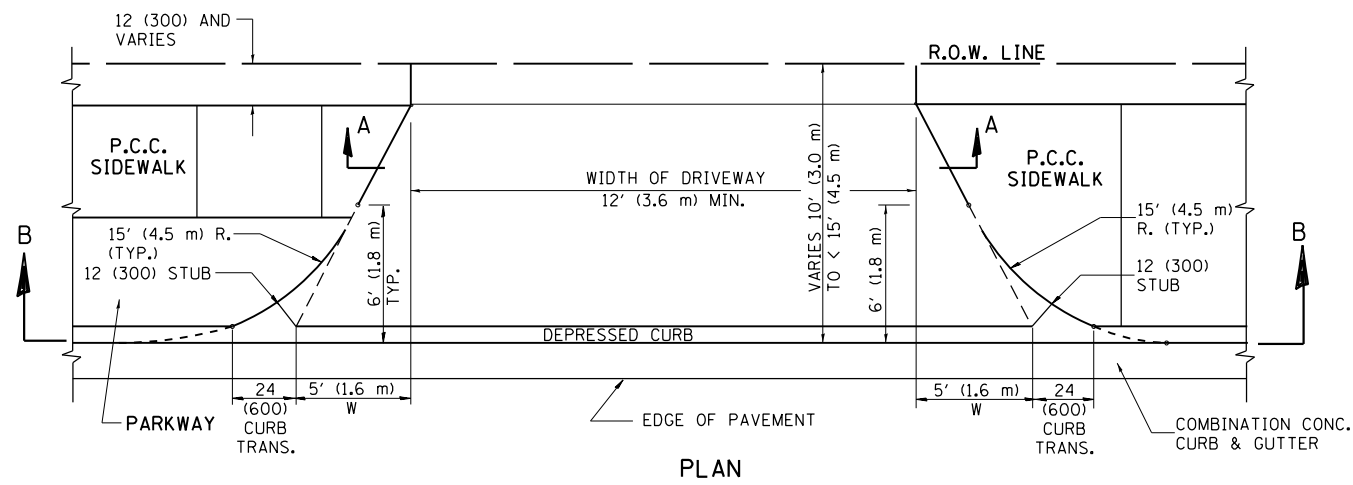
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME = bd01.dgn	USER NAME = joburke	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 99.9998' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 12/19/2014	DATE - 11-04-95	REVISED - R. BORO 09-06-11

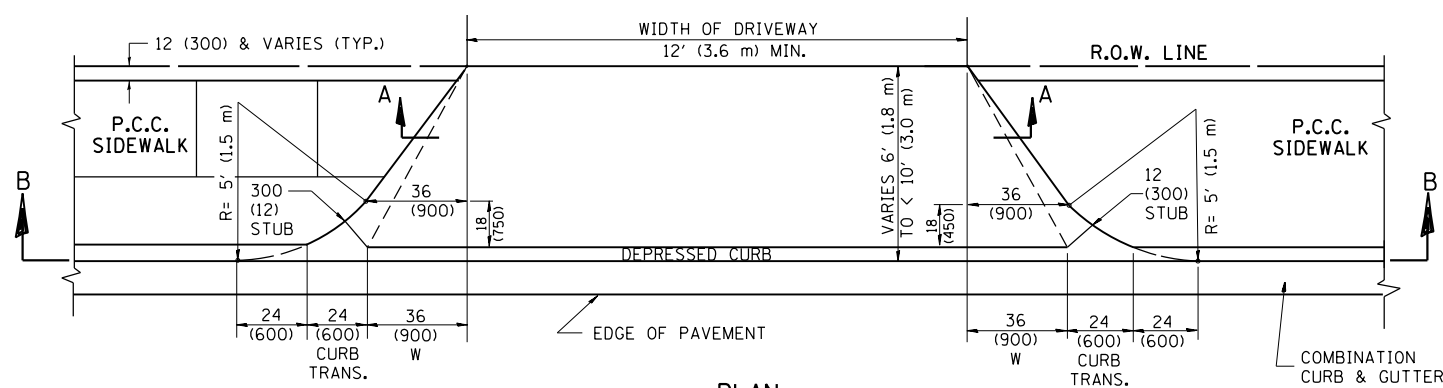
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

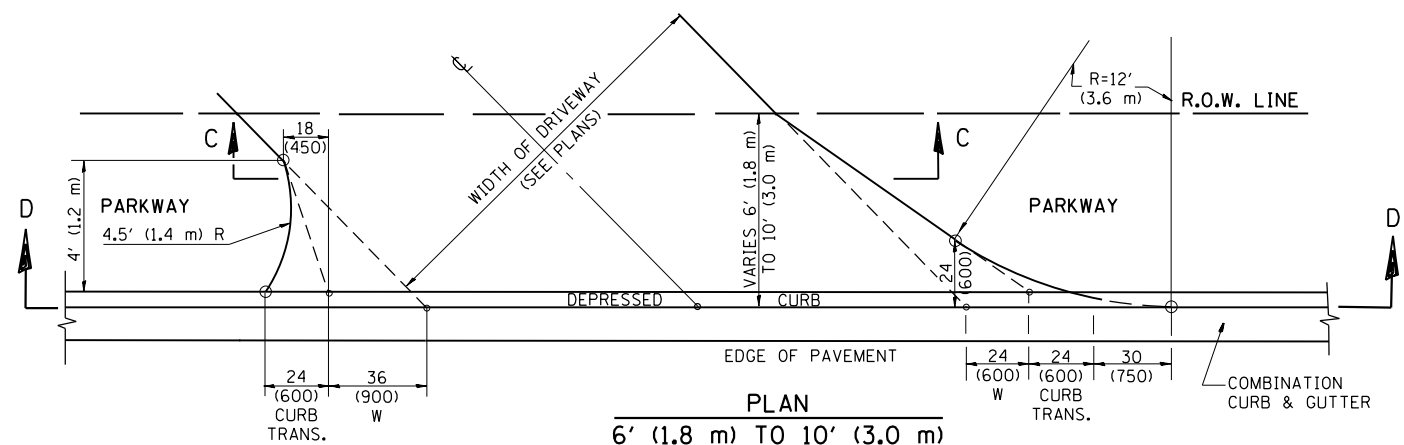
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD0156-07 (BD-01)			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



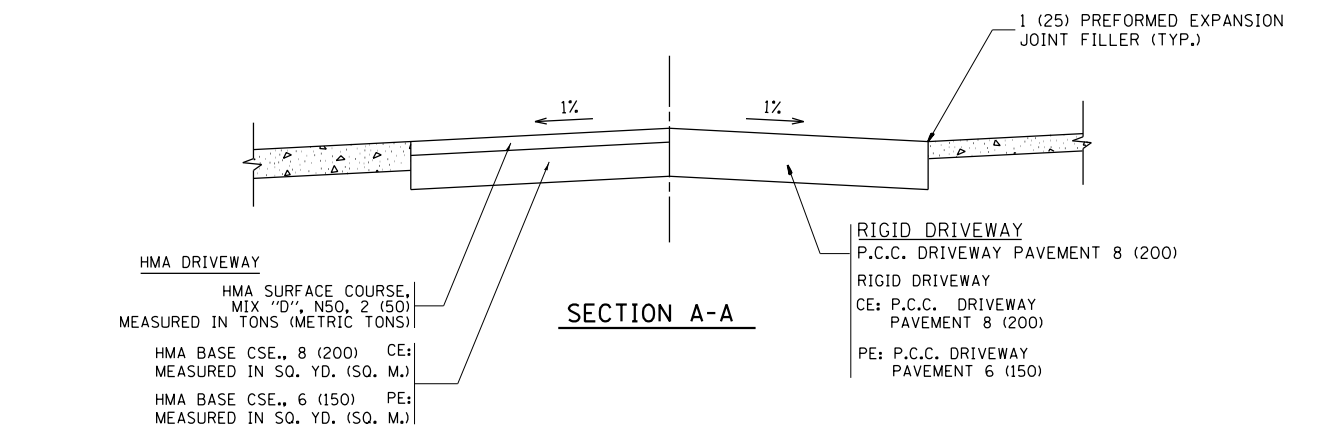
PLAN
10' (3.0 m) TO < 15' (4.5 m)



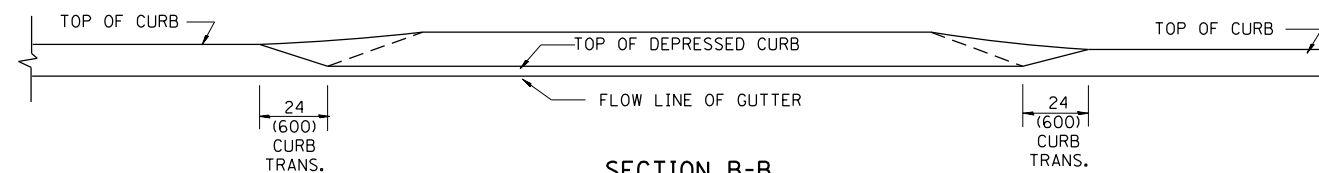
PLAN
6' (1.8 m) TO < 10' (3.0 m)



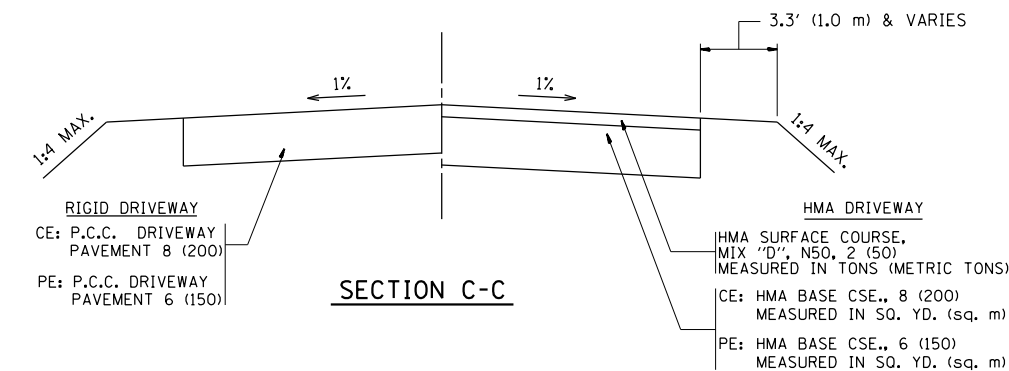
PLAN
6' (1.8 m) TO 10' (3.0 m)



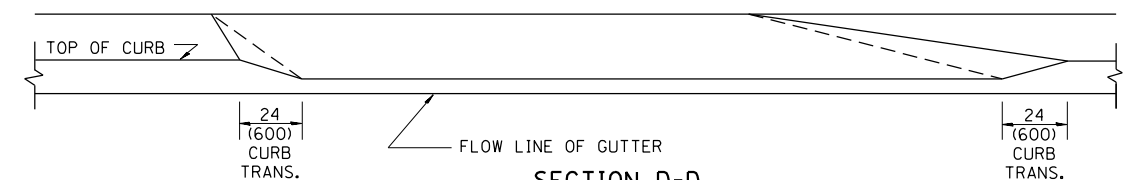
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

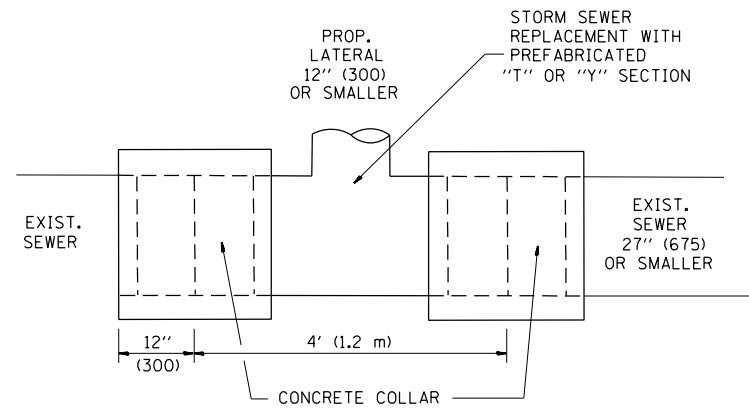
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = bd02.dgn	USER NAME = joburke	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
		DRAWN -	REVISED - P. LaFLEUR 04-15-03
	PLOT SCALE = 99.9998' / in.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 12/19/2014	DATE - 11-06-95	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

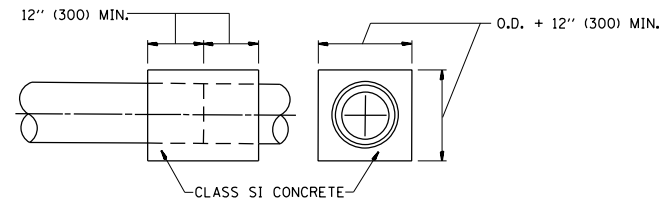
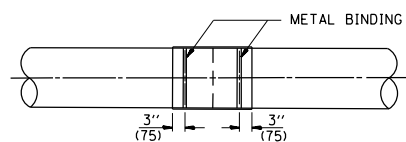
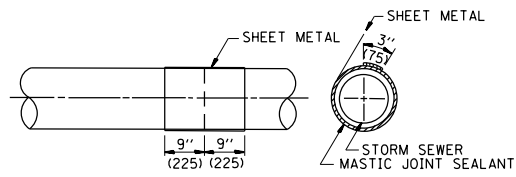
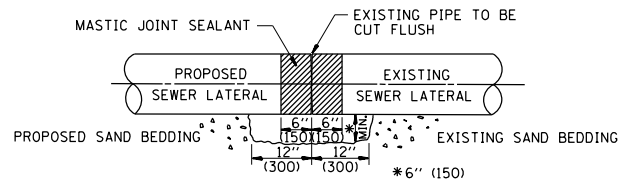
DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	343
BD400-02 (BD-02)			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



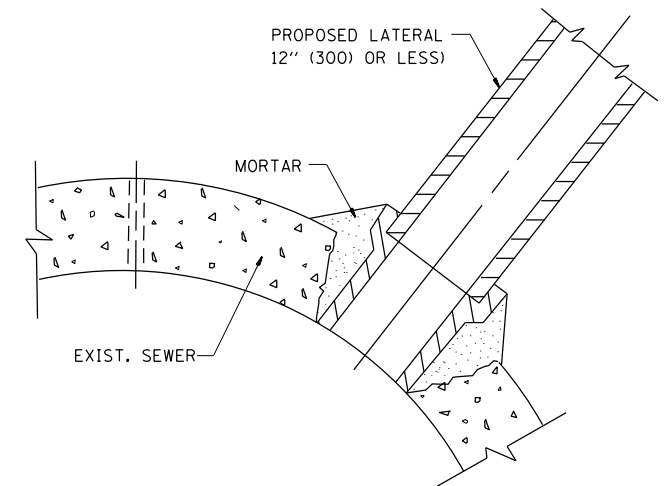
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

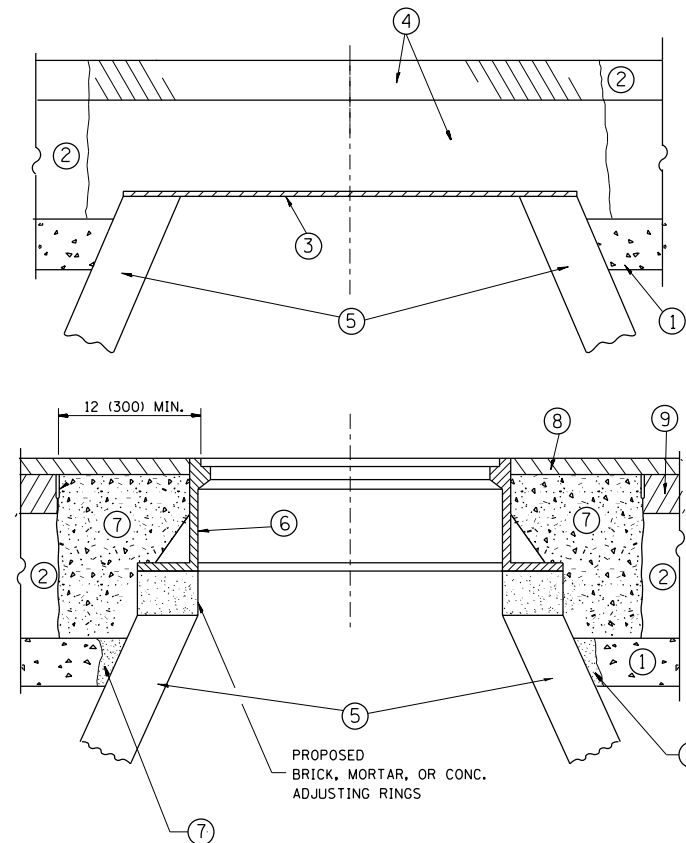
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		DRAWN -	REVISED - R. SHAH 09-09-94
	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED - R. SHAH 10-25-94
	PLOT DATE = 12/19/2014	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	344
BD500-01 (BD-7)			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

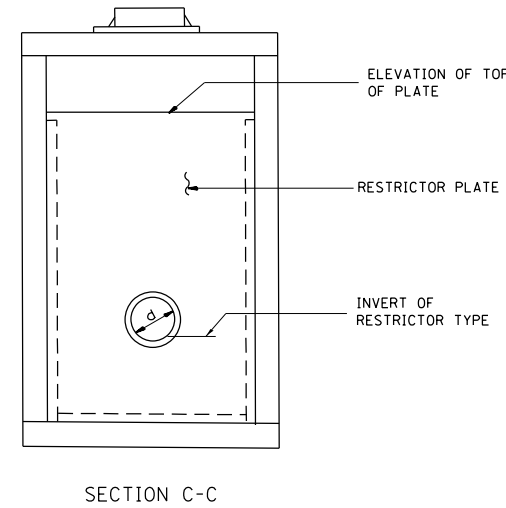
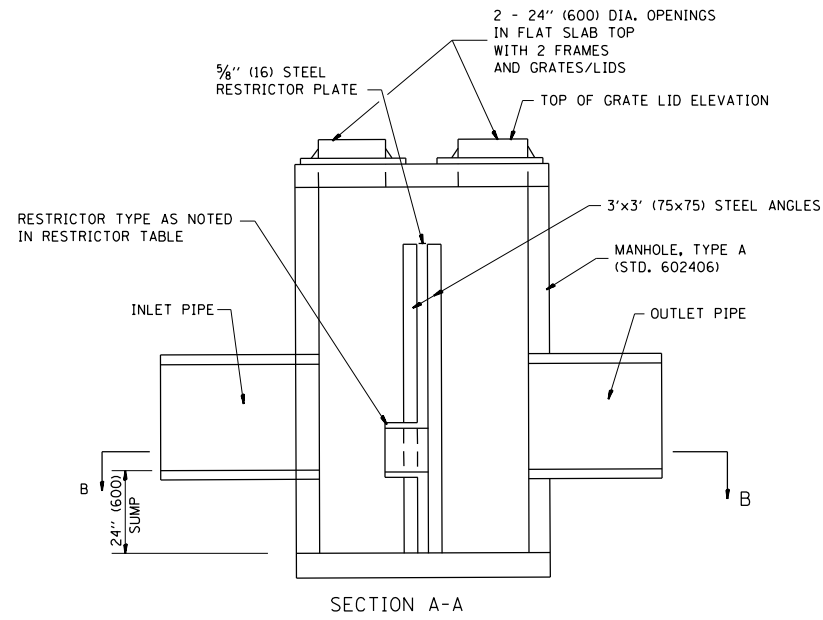
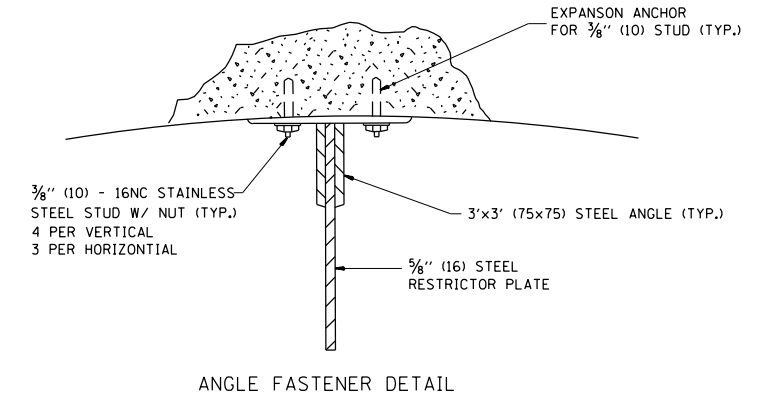
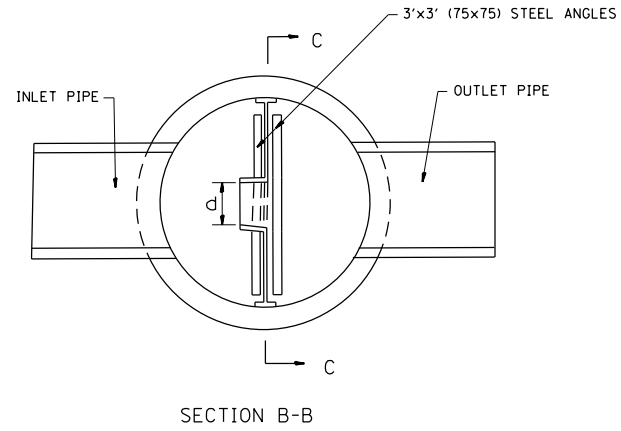
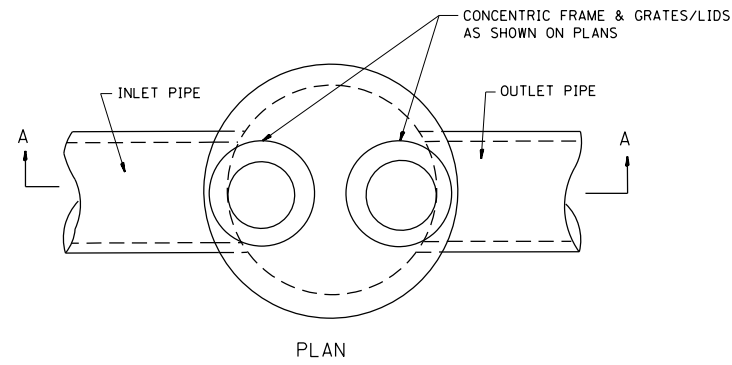
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = bd08.dgn	USER NAME = joburke	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 99.9998' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/19/2014	DATE - 10-25-94	REVISED - R. BORO 12-06-11

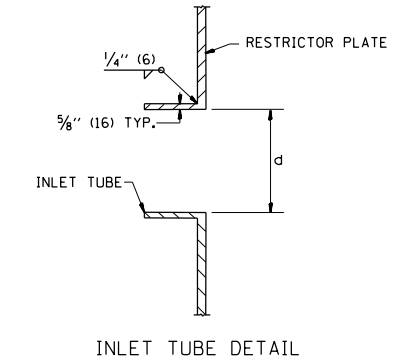
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

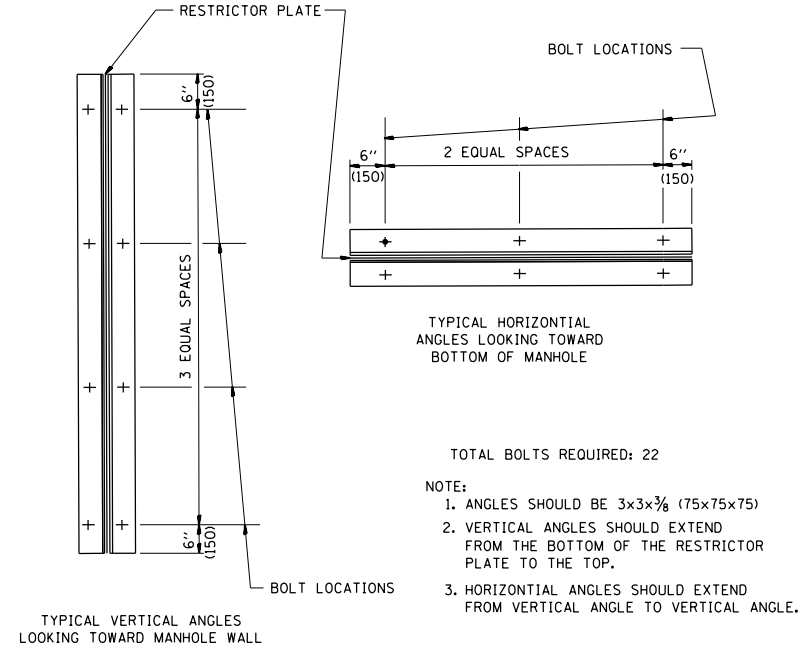
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	345
BD600-03 (BD-8)		CONTRACT NO. 60B42		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



- NOTES:
- ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
 - ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
 - BASIS OF PAYMENT: "MANHOLES, TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER in. (mm) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW

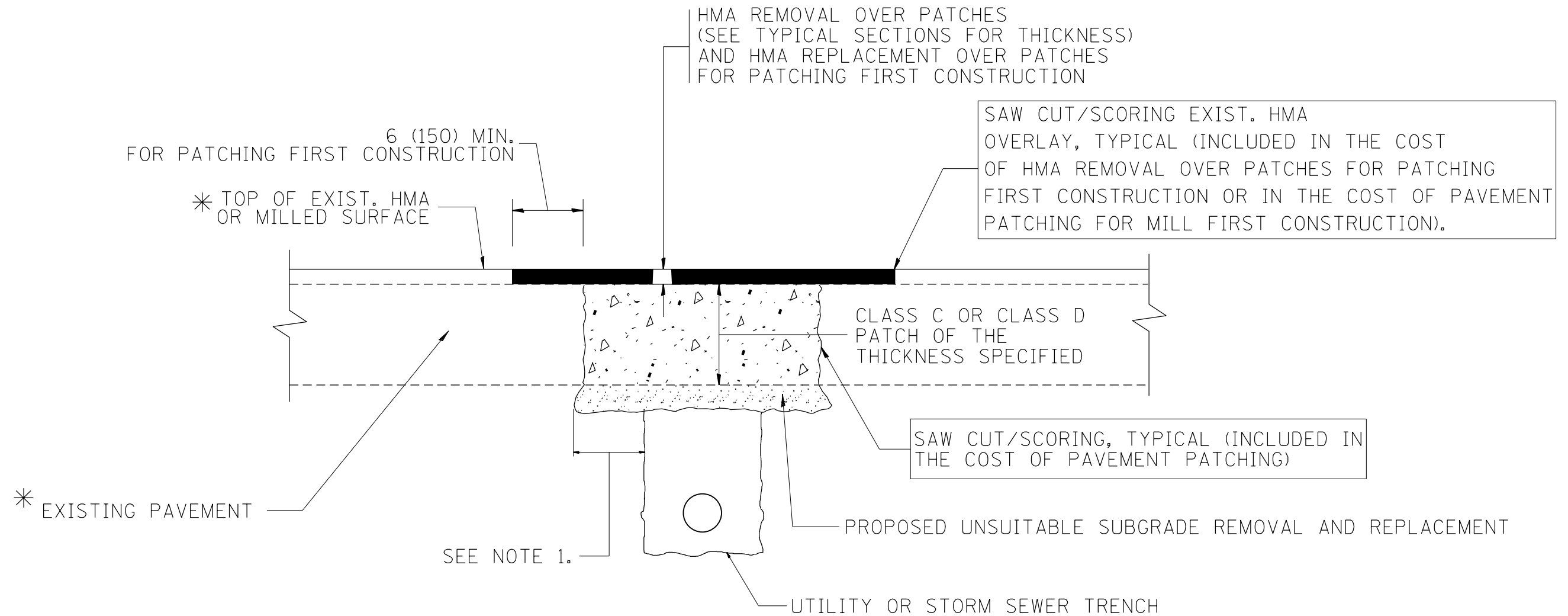


STEEL ANGLE BOLTING DETAILS

RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = bd22.dgn	USER NAME = joburke	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	1321	32RS&N (12)	DUPAGE	495	347
		PLOT SCALE = 100.000' / 1" =	REVISED - R. BORO 09-04-07												
		PLOT DATE = 12/19/2014	REVISED - K. ENG 10-27-08												
											BD400-04 (BD-22)			CONTRACT NO. 60B42	
											<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

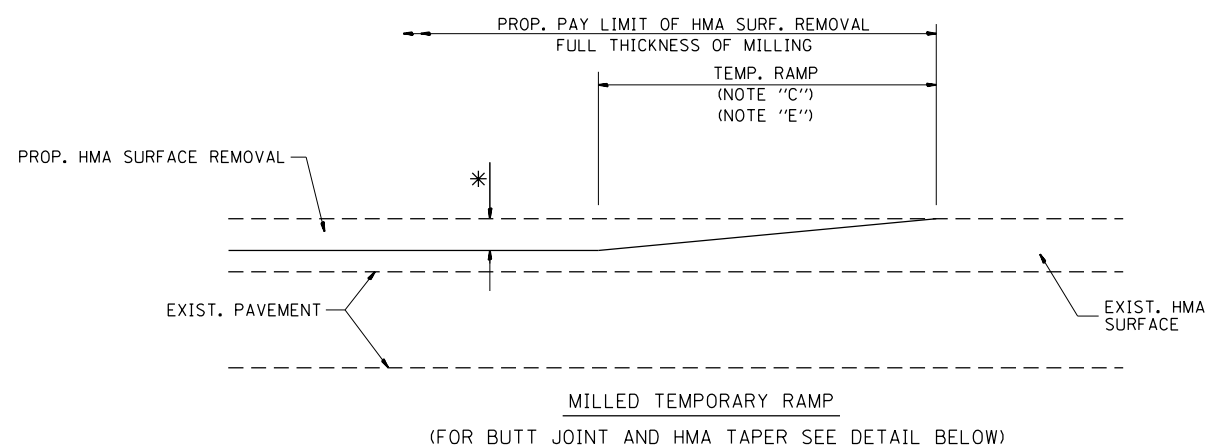
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

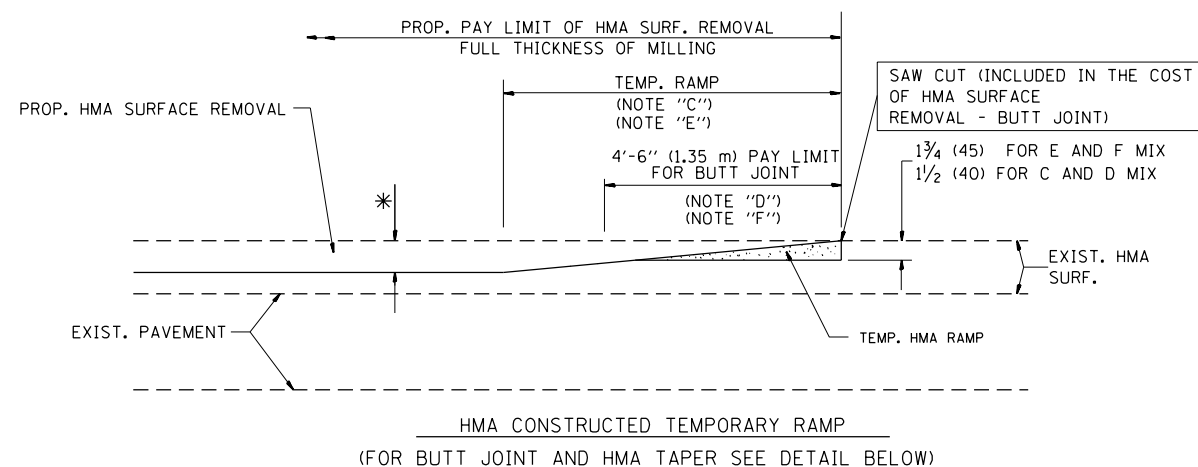
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = bd24.dgn	USER NAME = joburke	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A. RTE. 1321	SECTION 32RS&N (12)	COUNTY DUPAGE	TOTAL SHEETS 495	SHEET NO. 348
	PLOT SCALE = 100.000 / in.	CHECKED - M. GOMEZ 01-22-01	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 60B42	
	PLOT DATE = 12/19/2014	DATE - 03-11-94	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)							
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												

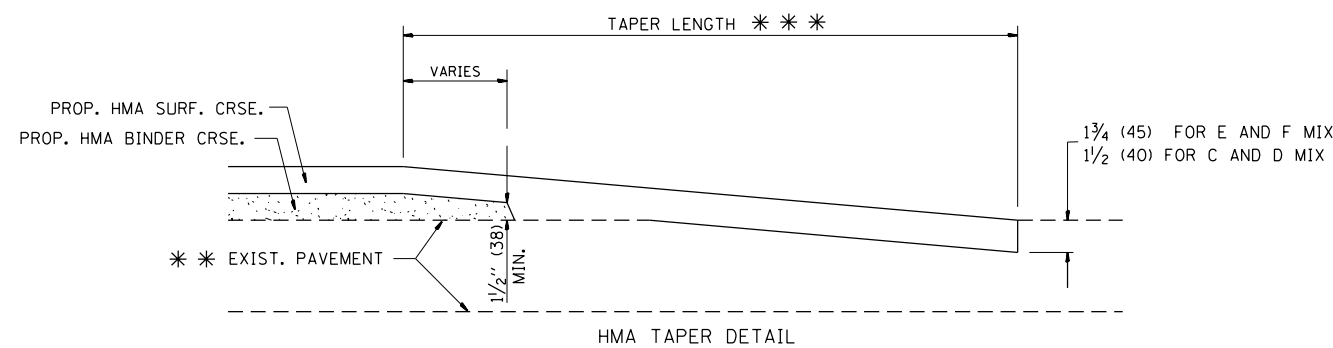
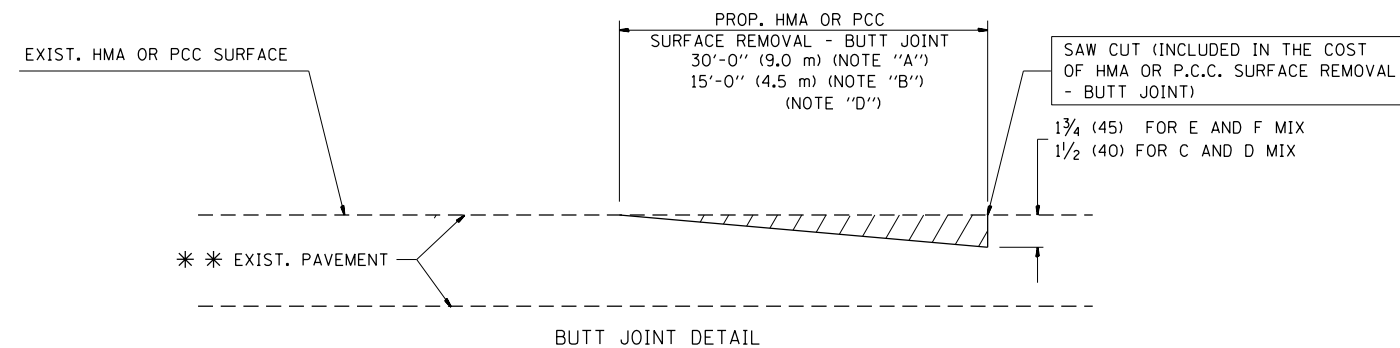


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

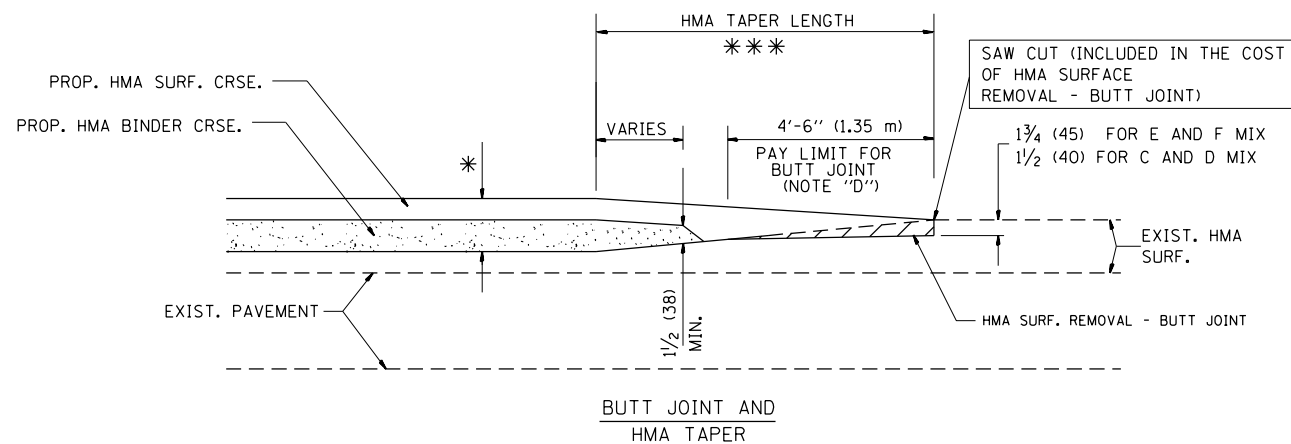
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

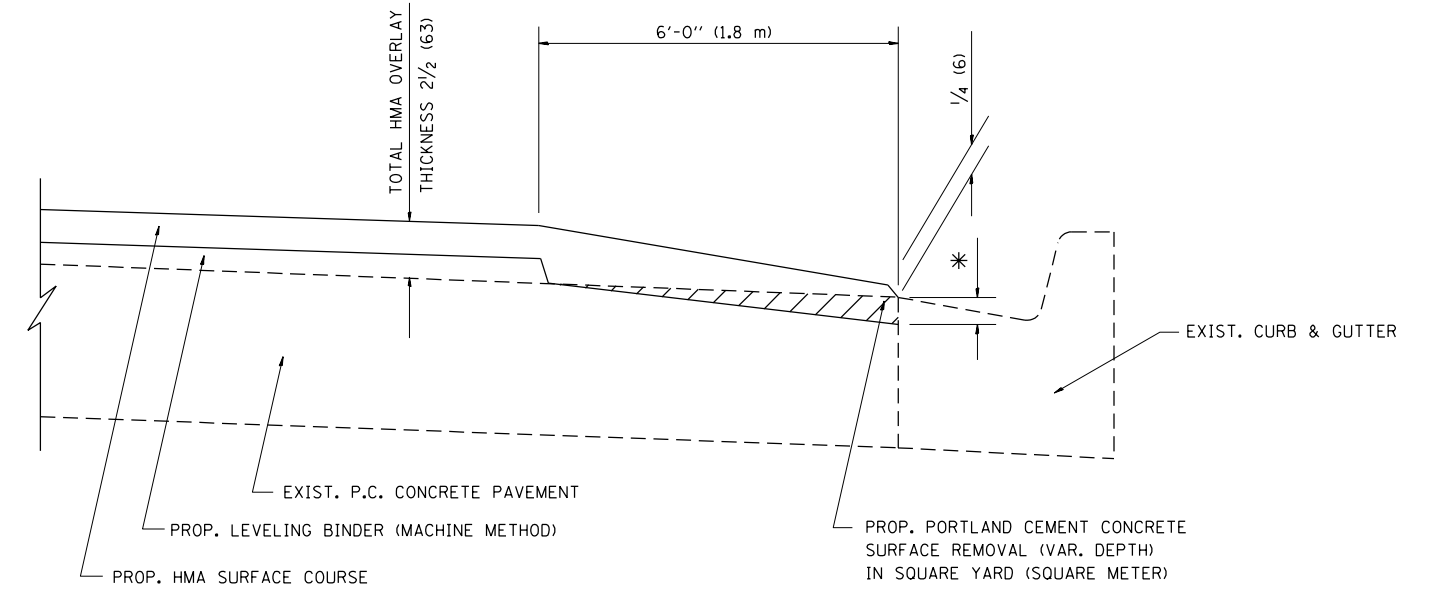
FILE NAME = bd32.dgn	USER NAME = joburke	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 99.9998' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 12/19/2014	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	349
BD400-05 BD32		CONTRACT NO. 60B42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C PAVEMENT

HMA SURFACE	THICKNESS	LEVELING BINDER	THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1½ (38)	1 (25)		1¼ (33)
F	1¾ (44)	¾ (19)		1½ (38)

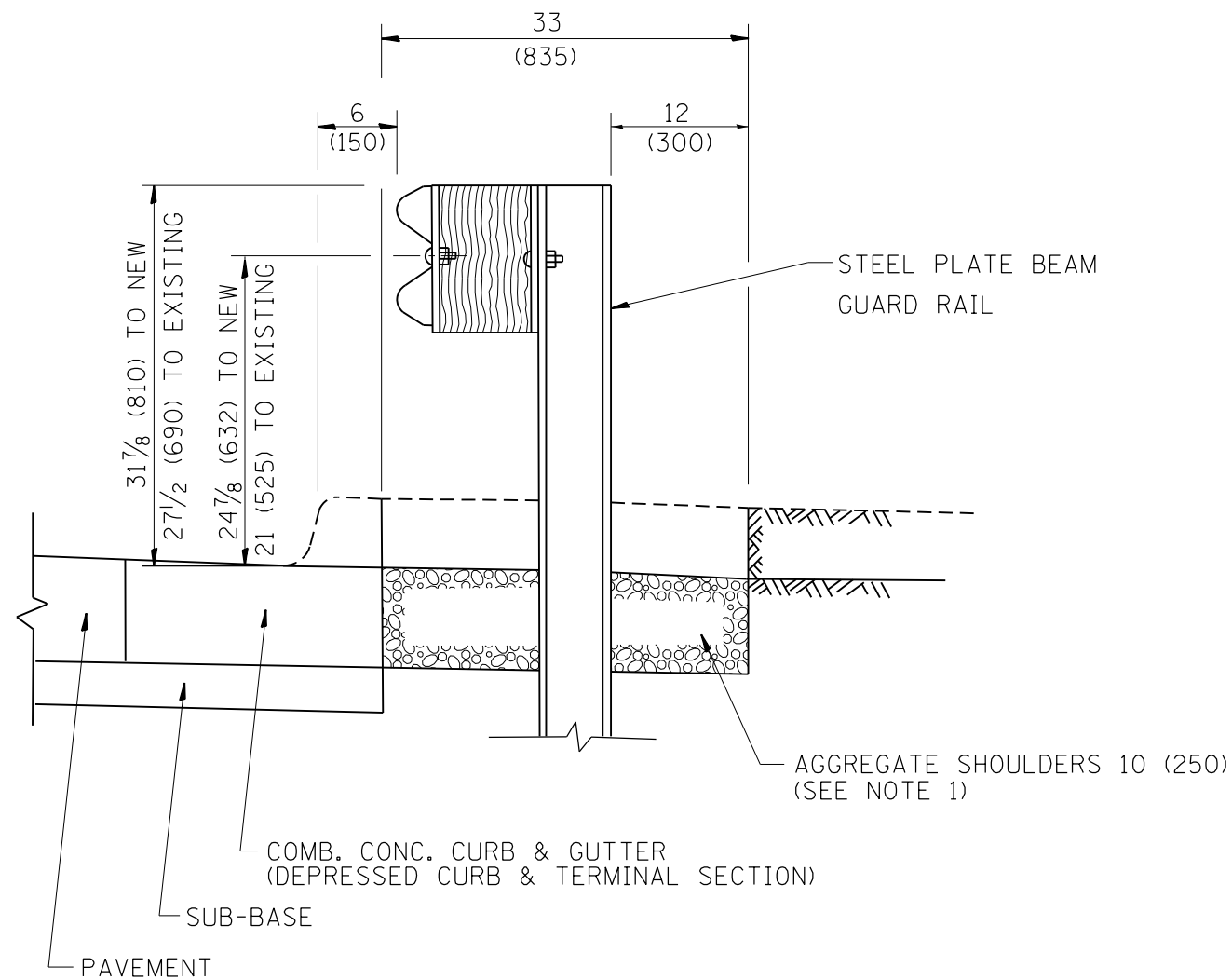
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = bd33.dgn	USER NAME = joburke	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94
		DRAWN - JIS	REVISED - A. ABBAS 05-05-99
	PLOT SCALE = 99.9998' / in.	CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
	PLOT DATE = 12/19/2014	DATE - 09-10-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C.C PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

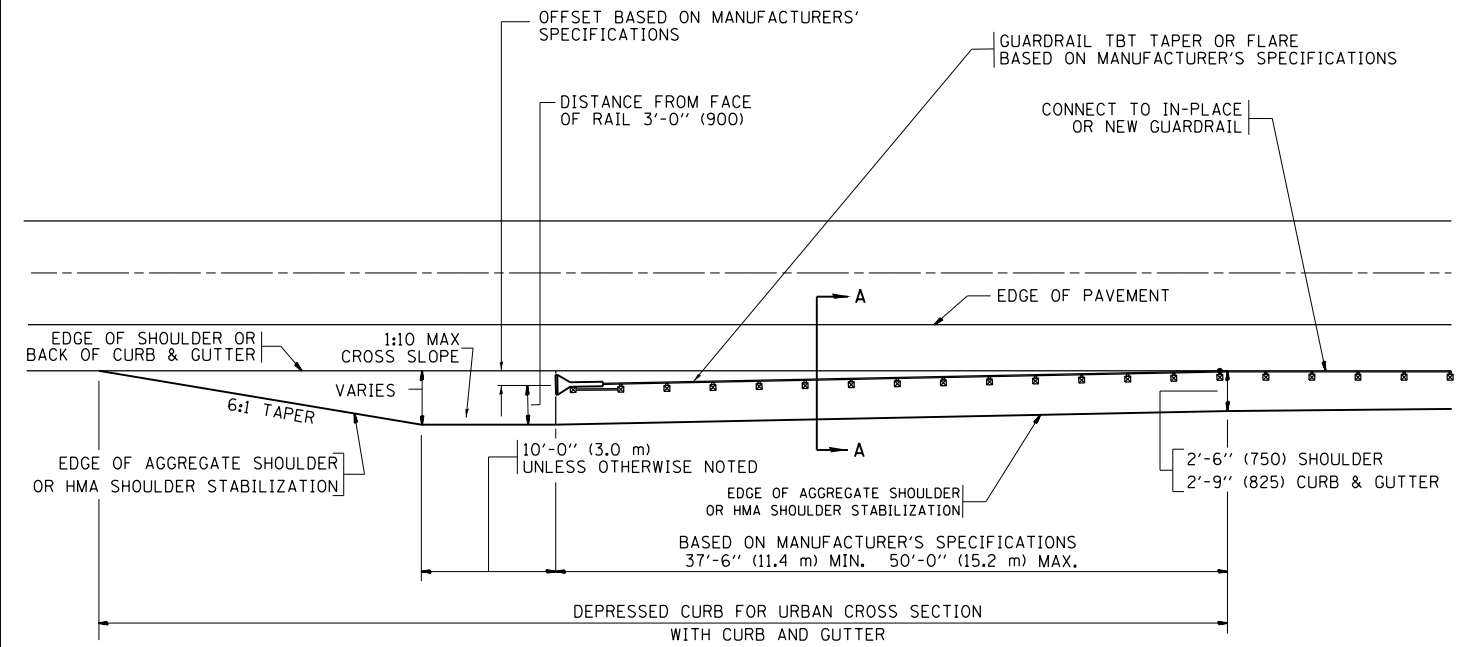
F.A. RTE. 1321	SECTION 32RS&N (12)	COUNTY DUPAGE	TOTAL SHEETS 495	SHEET NO. 350
BD400-06 (BD33)		CONTRACT NO. 60B42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
 2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
 3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

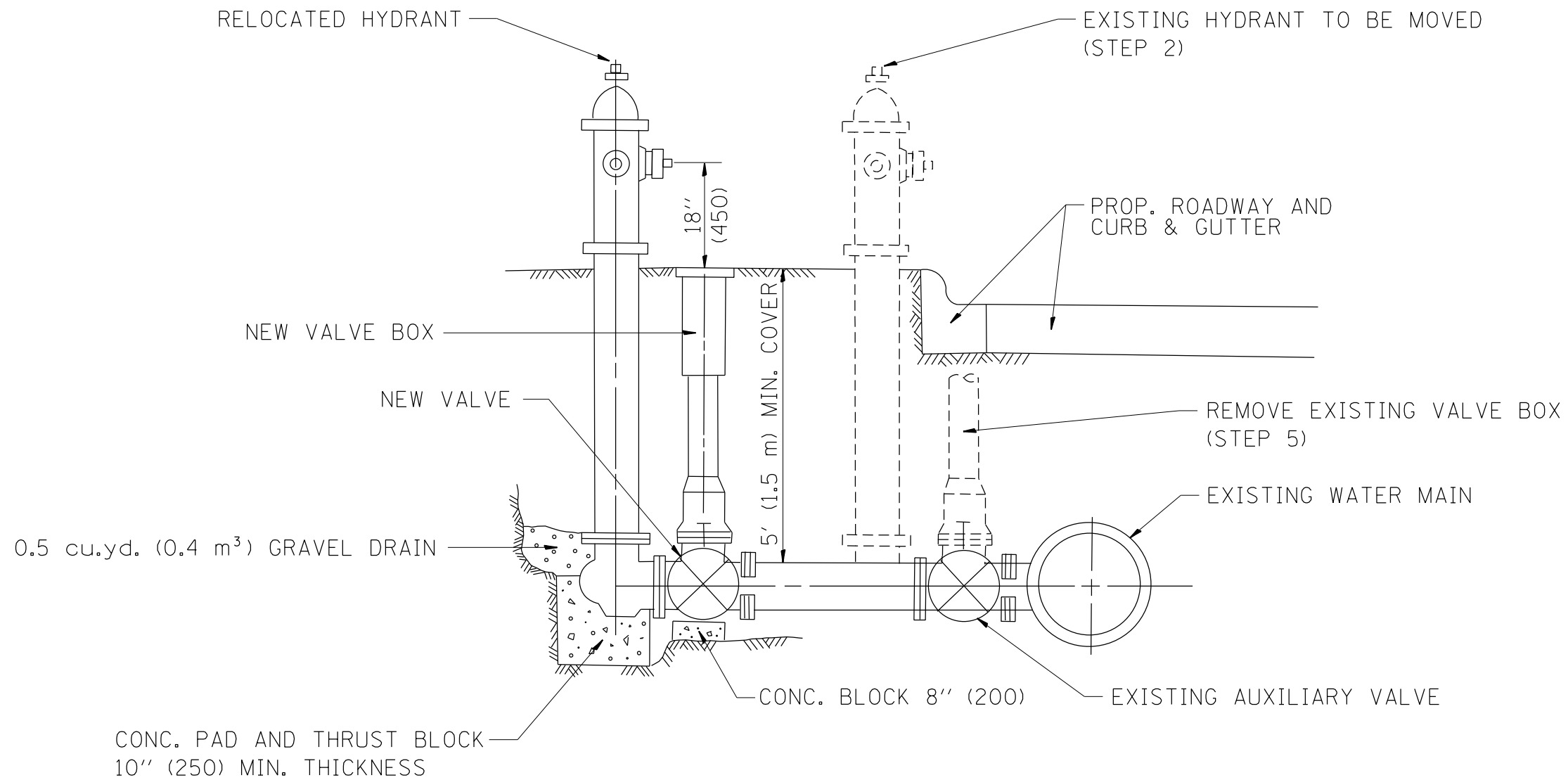
FILE NAME = bd34.dgn	USER NAME = joburke	DESIGNED - M. DE YONG	REVISED - E. GOMEZ 08-28-00
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 99.9998' / in.	CHECKED -	REVISED - R. BORO 12-08-2008
	PLOT DATE = 12/19/2014	DATE - 09-22-90	REVISED - R. BORO 09-14-2009

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY 1 SPL.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 1321	SECTION 32RS&N (12)	COUNTY	TOTAL SHEETS 495	SHEET NO. 351
BD600-10 (BD 34)		CONTRACT NO. 60B42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

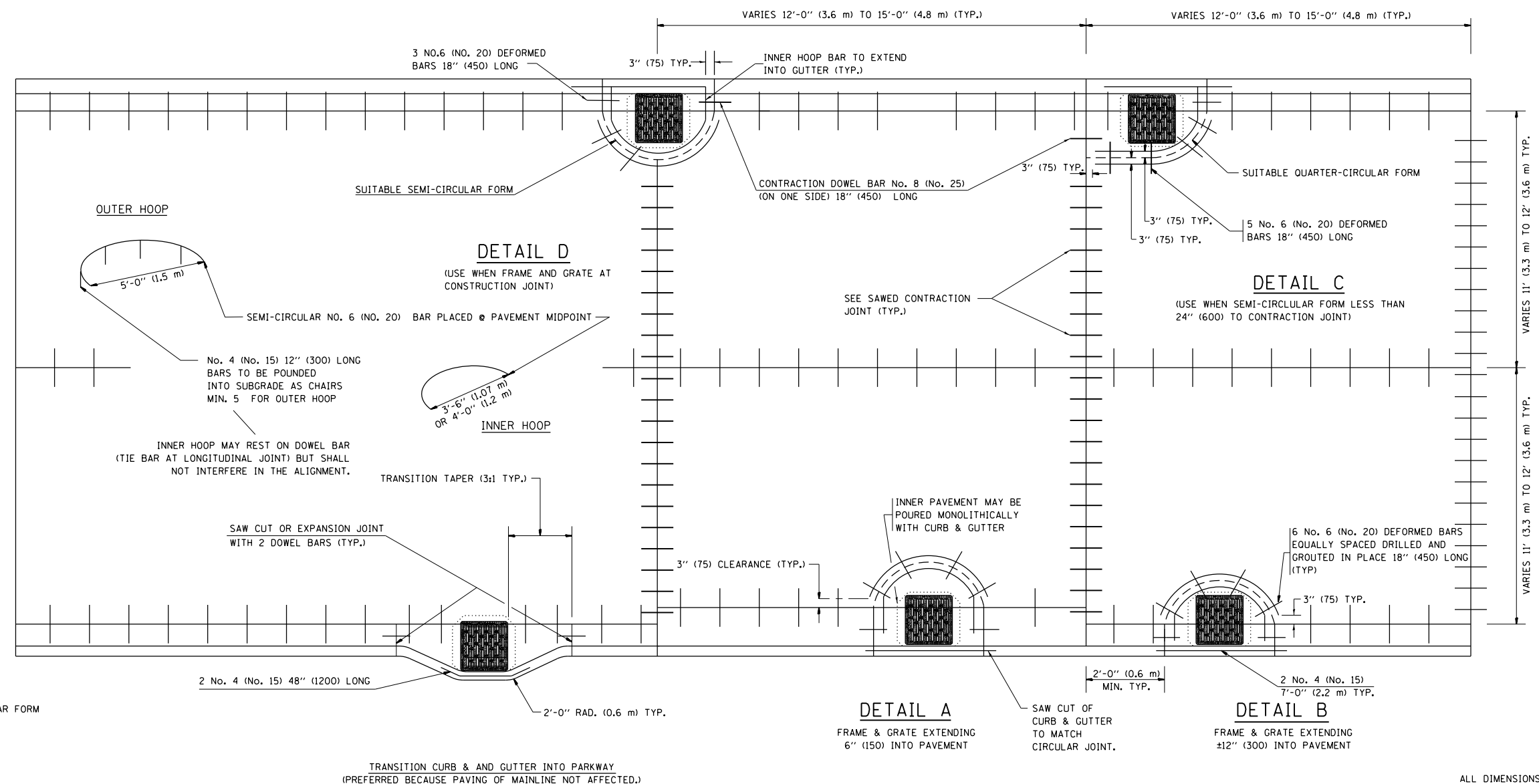
FILE NAME = bd36.dgn	USER NAME = joburke	DESIGNED -	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 99.9998' / in.	DRAWN -	REVISED - R. SHAH 10-25-94					1321	32RS&N (12)	DUPAGE	495	352
PLOT DATE = 12/19/2014	CHECKED -	REVISIED -	REVISIED -	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD-36		CONTRACT NO. 60B42		
	DATE -	REVISIED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								

FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 8" (200)	3'-6" (1.1 m)	4'-0" (1.2 m)	5'-0" (1.5 m)
> 8" (200) TO 14" (360)	4'-0" (1.2 m)	4'-6" (1.4 m)	5'-0" (1.5 m)

DESIGNER NOTE:
THIS DETAIL IS TO BE USED
WHEN THE GUTTER FLAG IS
LESS THAN 24"

NOTES :

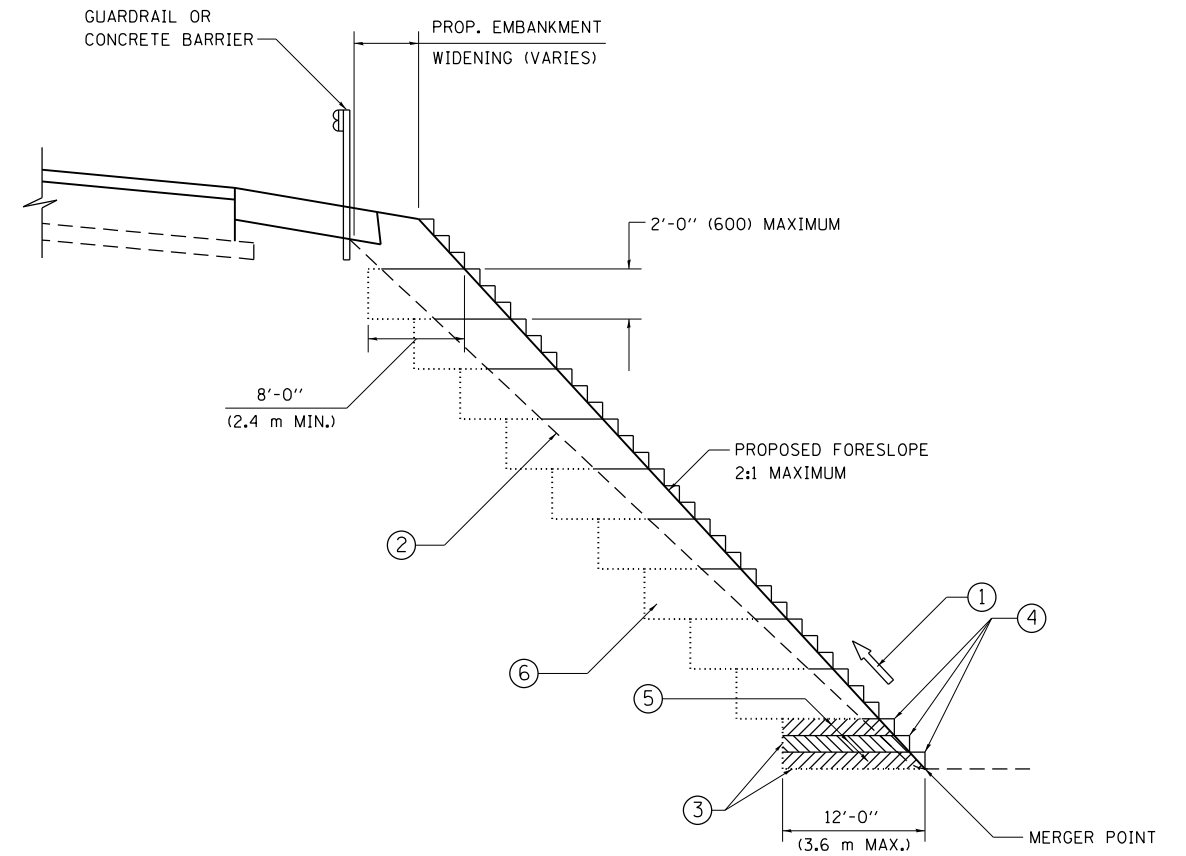
1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
2. TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT. EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 12" (300) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 2" (50) CLEARANCE.
6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



LEGEND:
..... CASTING
- - - - - SUITABLE SEMI-CIRCULAR FORM

ALL DIMENSIONS ARE IN INCHES
(MILLIMETERS) UNLESS OTHERWISE NOTED

FILE NAME = bd48.dgn	USER NAME = joburke	DESIGNED - A. ABBAS	REVISED - T. MATOUSEK 08-28-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PCC PAVEMENT ROUNDOUTS AT CURB AND GUTTER			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN - TOM MATOUSEK	REVISED - T. MATOUSEK 10-02-00		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	1321	32RS&N (12)	495	353
		CHECKED - A. ABBAS	REVISED - T. MATOUSEK 04-25-02								BD-48			CONTRACT NO. 60B42
		DATE - 01-04-99	REVISED - P. LAFLEUR 08-27-02								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = bd51.dgn	USER NAME = joburke	DESIGNED -	REVISED -
		DRAWN - CADD	REVISED -
		CHECKED - S.E.B.	REVISED -
		DATE - 06-16-04	REVISED -

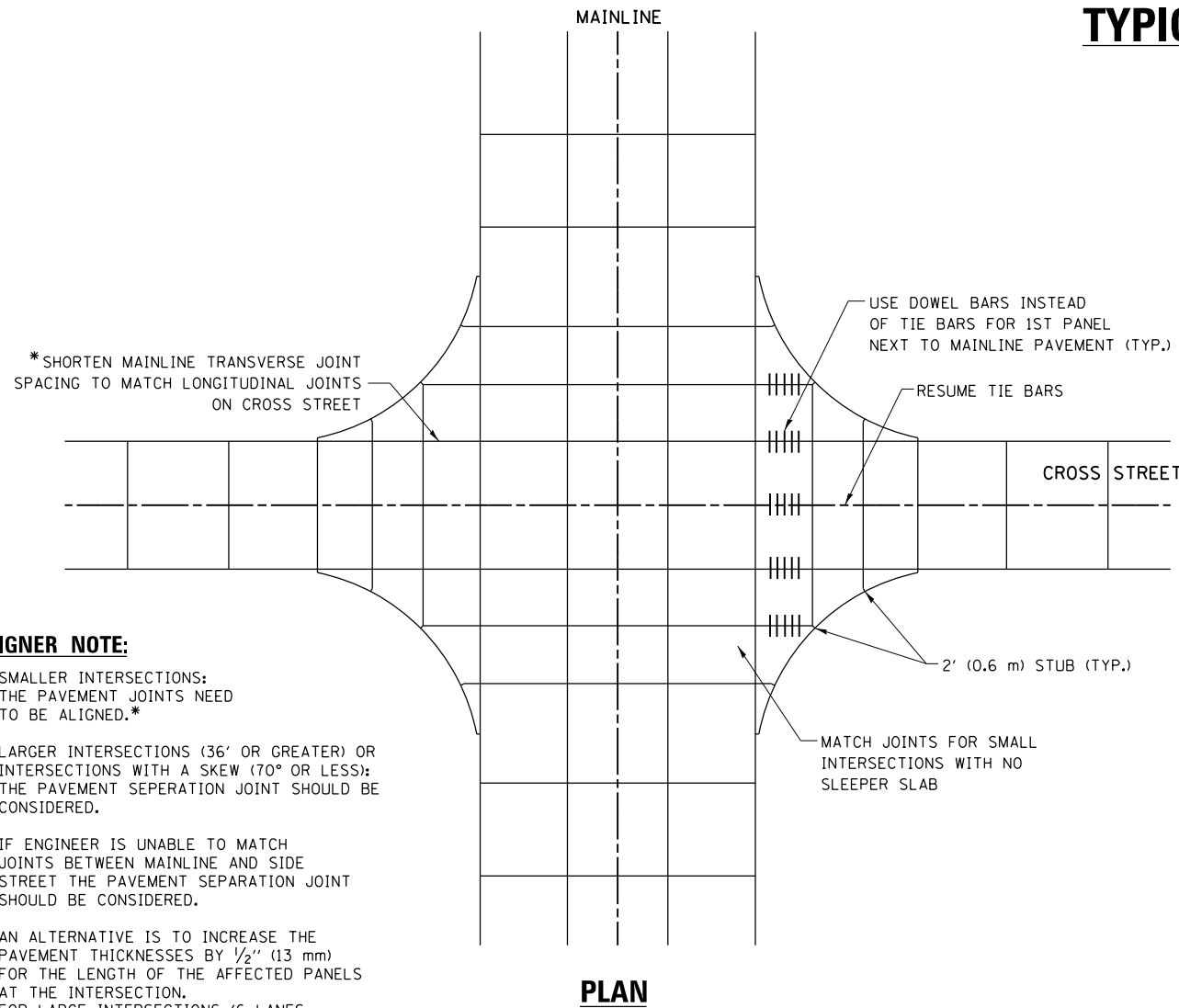
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BENCHING DETAIL FOR EMBANKMENT WIDENING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	354
BD-51		CONTRACT NO. 60B42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TYPICAL APPLICATION

THE USE OF CROSS STREET PAVEMENT SEPARATION JOINTS FOR SKEWED OR LARGE INTERSECTIONS WHERE JOINTS MAY NOT MATCH



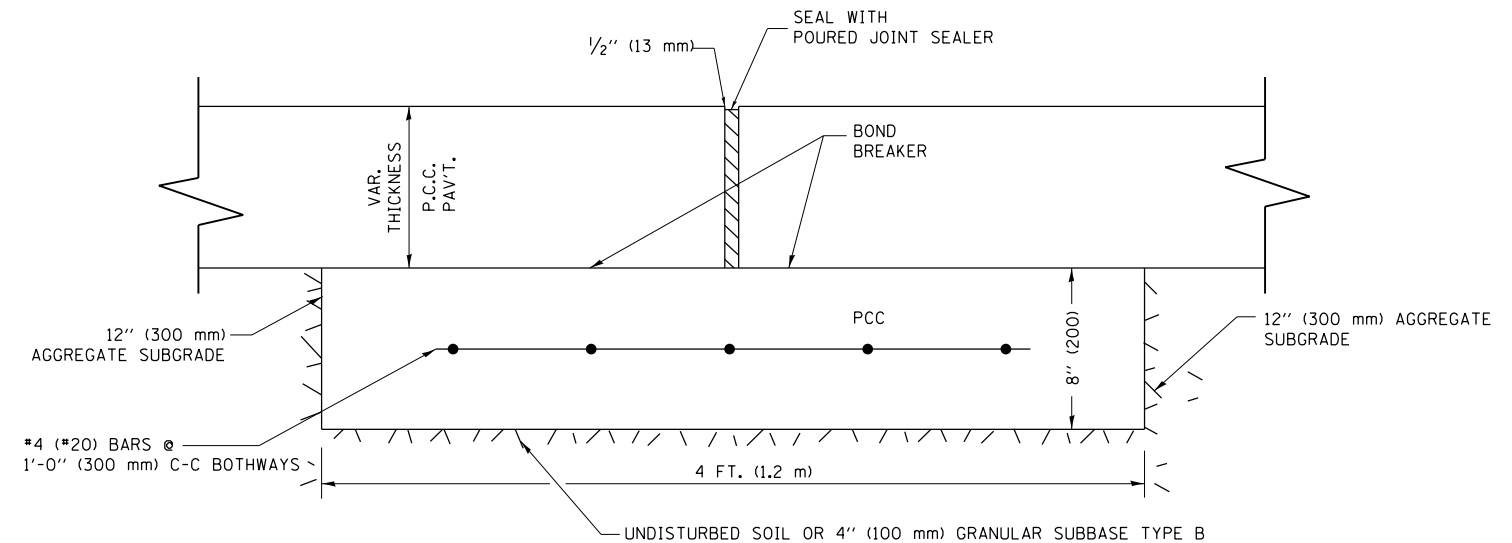
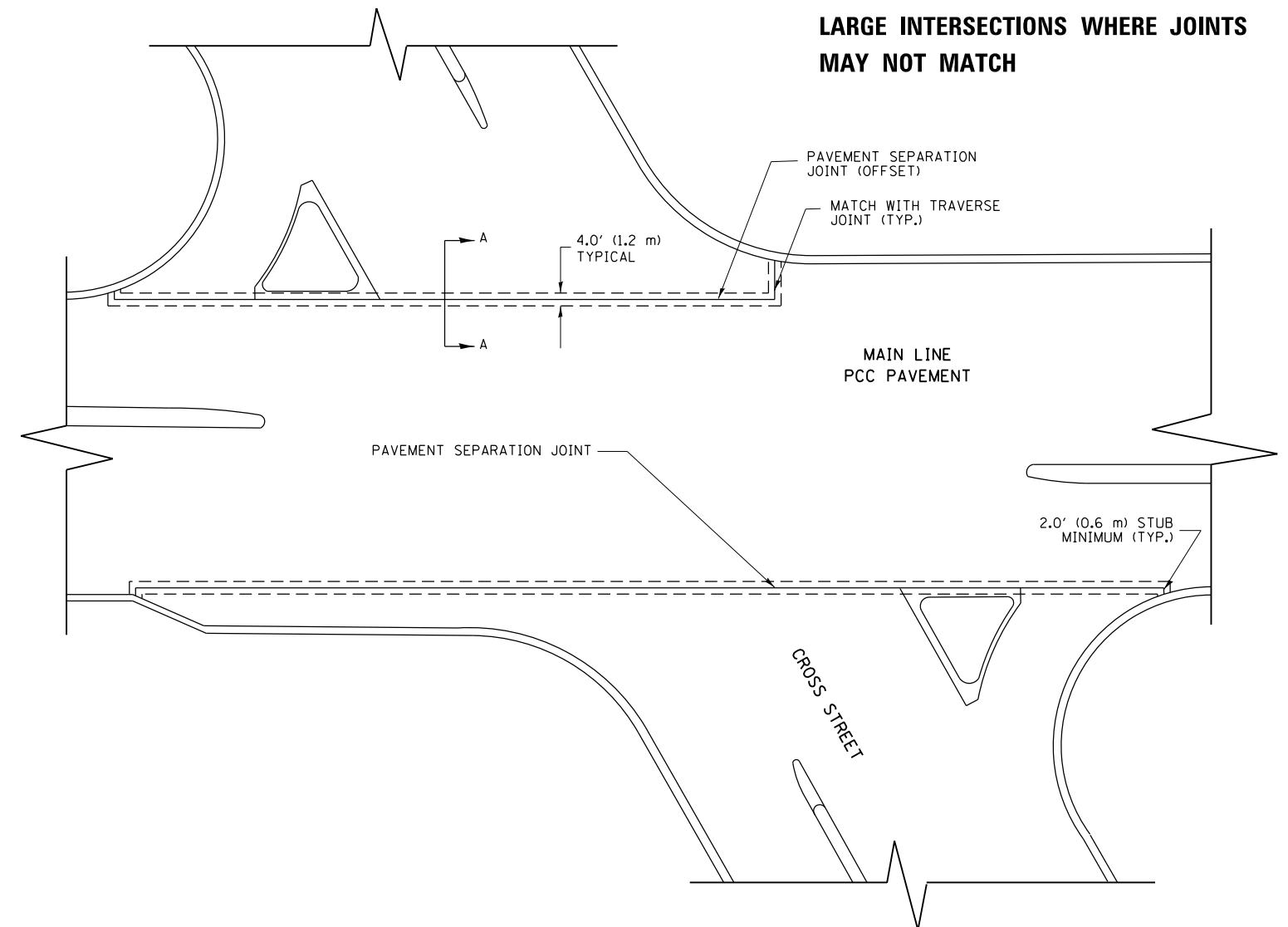
PLAN

DESIGNER NOTE:

1. SMALLER INTERSECTIONS: THE PAVEMENT JOINTS NEED TO BE ALIGNED.*
2. LARGER INTERSECTIONS (36' OR GREATER) OR INTERSECTIONS WITH A SKEW (70° OR LESS): THE PAVEMENT SEPERATION JOINT SHOULD BE CONSIDERED.
3. IF ENGINEER IS UNABLE TO MATCH JOINTS BETWEEN MAINLINE AND SIDE STREET THE PAVEMENT SEPARATION JOINT SHOULD BE CONSIDERED.
4. AN ALTERNATIVE IS TO INCREASE THE PAVEMENT THICKNESSES BY 1/2" (13 mm) FOR THE LENGTH OF THE AFFECTED PANELS AT THE INTERSECTION. FOR LARGE INTERSECTIONS (6 LANES OR MORE) WHERE JOINTS CAN BE MATCHED, USE #8 (25) DOWEL BARS INSTEAD OF #8 (25) TIE BARS AT EDGE OF MAINLINE PAVEMENT WHEN NO PAVEMENT SEPARATION JOINTS USED.

NOTE:

1. JOINT FILLER SHALL CONSIST OF A SHEET OF 1/2" (13 mm) BITUMINOUS PREFORMED FIBER JOINT FILLER CONFORMING TO ARTICLE 1051.03 OF THE STANDARD SPECIFICATIONS.
2. THE JOINT SHALL BE SEALED WITH A HOT POUR JOINT SEALER CONFORMING TO ARTICLE 1050.02 OF THE STANDARD SPECIFICATIONS.
3. A SINGLE LAYER OF FELT ROOFING PAPER SHALL SERVE AS A BOND BREAKER.
4. JOINT SHALL CONTINUE THROUGH COMBINATION CURB & GUTTER OR PCC SHOULDER.
5. PAVEMENT SEPARATION JOINT IS TO BE PAID FOR AS "SLEEPER SLAB" AND IS TO BE MEASURED IN PLACE BY THE LINEAL FOOT.
6. BOND BREAKER AND 1/2" (13 mm) JOINT AND FILLER SHALL BE INCIDENTAL TO THE PAY ITEM "SLEEPER SLAB".



PROPOSED SECTION A-A

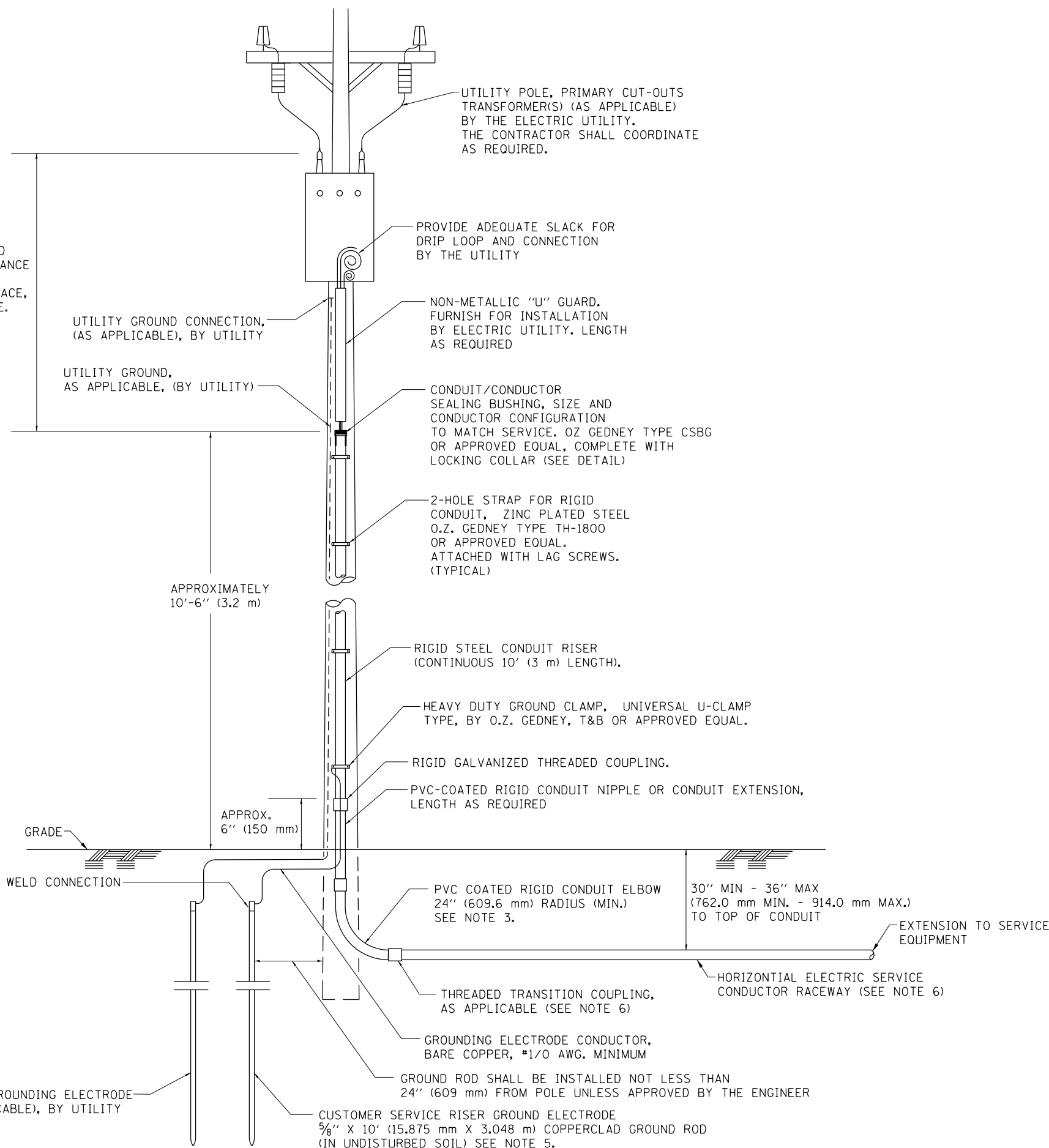
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	PLOT DATE = 12/19/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF PAVEMENT SEPARATION JOINT FOR JOINTED PCC PAVEMENTS AT INTERSECTIONS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	355
BD52		CONTRACT NO. 60B42		
ILLINOIS FED. AID PROJECT				

ASCERTAIN AND ASSURE CLEARANCE FROM UTILITY SECONDARY SPACE, AS APPLICABLE.

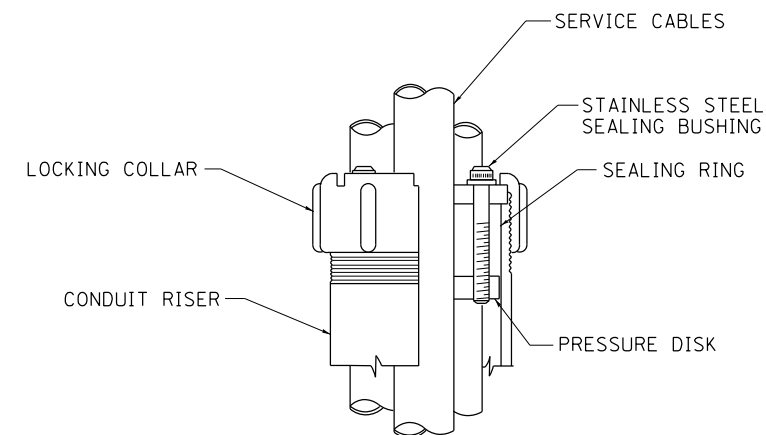


APPLICATION

THIS DETAIL APPLIES FOR LOW VOLTAGE ELECTRIC SERVICE (660 V OR LESS) FROM AN OVERHEAD UTILITY SUPPLY TO SEPERATLY-MOUNTED SERVICE EQUIPMENT.

NOTES

- SERVICE VOLTAGE SHALL BE AS INDICATED ELSEWHERE IN THE DRAWINGS.
- UNLESS OTHERWISE INDICATED, ITEMS AND WORK SHALL BE INCLUDED AND PAID AS PART OF THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.
- CONDUIT AND CONNECTOR DIAMETER SHALL MATCH THE DIAMETER OF THE SERVICE CONDUCTOR RACEWAY AS INDICATED ON THE PLANS.
- PVC COATED RACEWAYS AND ACCESSORIES SHALL BE CAREFULLY INSTALLED WITH MFR RECOMMENDED TOOLS AND PROCEDURES TO AVOID DAMAGE. ANY DAMAGE SHALL BE REPAIRED WITH COMPATIBLE PVC TOUCH-UP MATERIAL TO THE SATISFACTION OF THE ENGINEER OR THE DAMAGED MATERIAL SHALL BE REPLACED AT NO ADDITIONAL COST.
- THE CONTRACTOR SHALL OBTAIN INSPECTION AND APPROVAL BY THE ENGINEER OF SERVICE RISER GROUND ELECTRODE, RISER ELBOW, NIPPLE AND CONNECTION TO SERVICE CONDUCTOR RACEWAY EXTENSION BEFORE BACKFILL AND SHALL ALSO OBTAIN INSPECTION OF SERVICE RISER AND SEALING BUSHING BEFORE UTILITY "U" GUARD INSTALLATION AND SERVICE CONNECTION.
- THE HORIZONTAL ELECTRIC SERVICE CONDUCTOR RACEWAY SHALL BE AS INDICATED AND SHALL BE MEASURED SEPARATELY FOR PAYMENT. WHEN THE RACEWAY IS PVC-COATED RIGID GALVANIZED STEEL, THE COUPLING SHALL BE THE SAME. WHEN THE RACEWAY IS PVC CONDUIT (IN CONCRETE), THE COUPLING SHALL BE A METALIC TO NON METALIC ADAPTER. WHEN THE RACEWAY IS ENCASED IN CONCRETE, THE CONCRETE SHALL EXTEND TO COVER THE COUPLING.
- PLANS AND DETAILS INDICATE THE GENERAL NATURE AND REQUIREMENTS. THEY DO NOT SHOW EVERY ACCESSORY AND ATTACHMENT, AND THEY DO NOT RELIEVE THE CONTRACTOR OF THE REQUIREMENTS OF THE SPECIFICATIONS AND SPECIAL PROVISIONS TO ASCERTAIN UTILITY REQUIREMENTS AND TO COORDINATE ACCORDINGLY, FURNISHING ALL ITEMS AND WORK NOT PROVIDED BY THE UTILITY, BUT NECESSARY FOR A COMPLETE SERVICE INSTALLATION IS REQUIRED AND SHALL BE INCLUDED IN THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.



SEALING BUSHING DETAIL

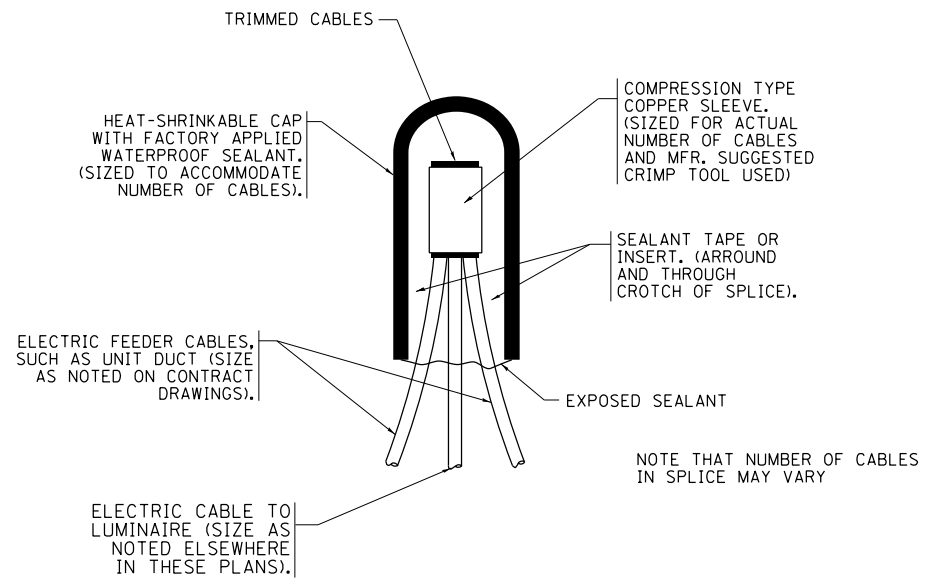
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	PLOT DATE = 12/19/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ELECTRIC SERVICE INSTALLATION
AERIAL, REMOTE DISCONNECT**

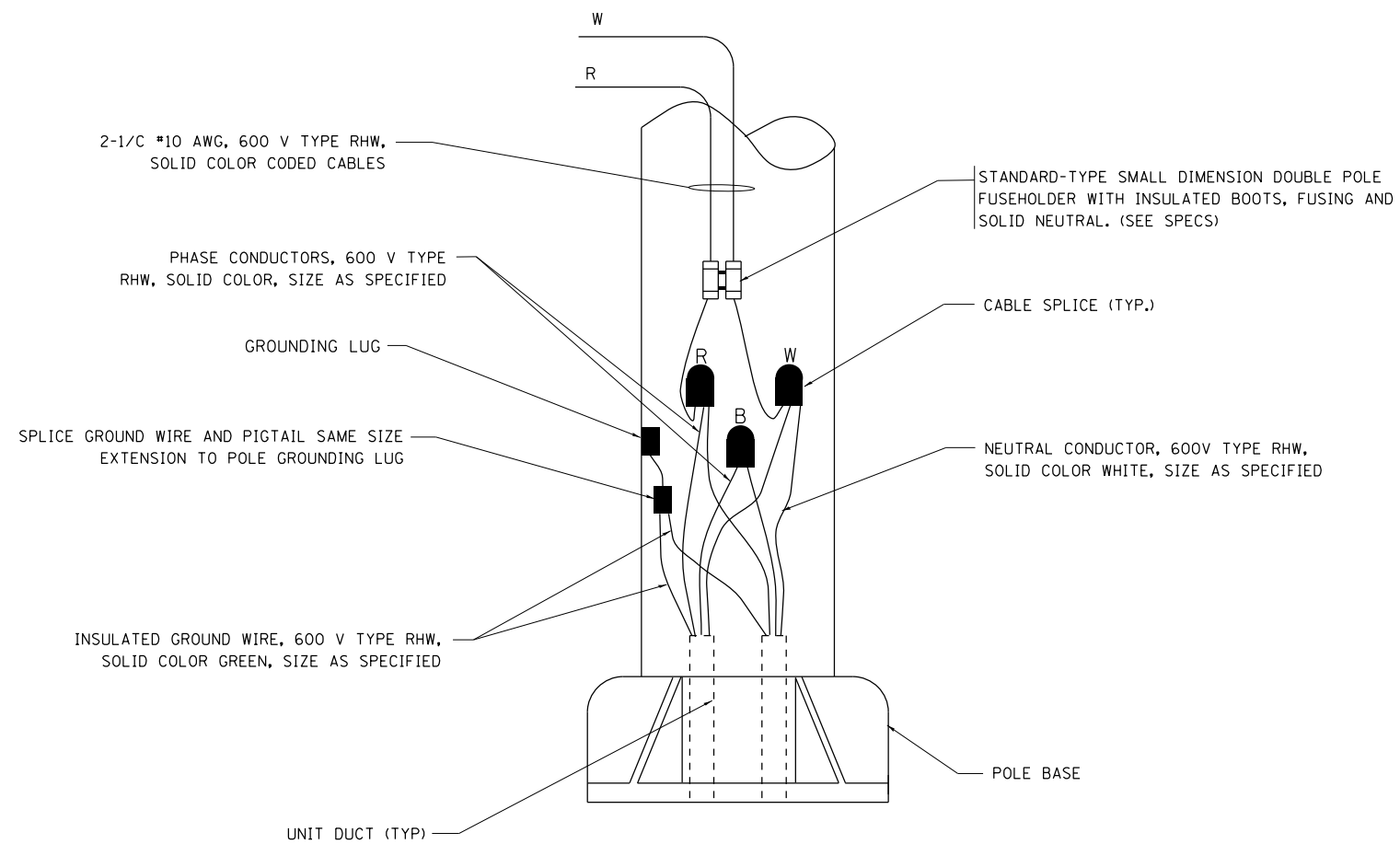
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BE-220			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



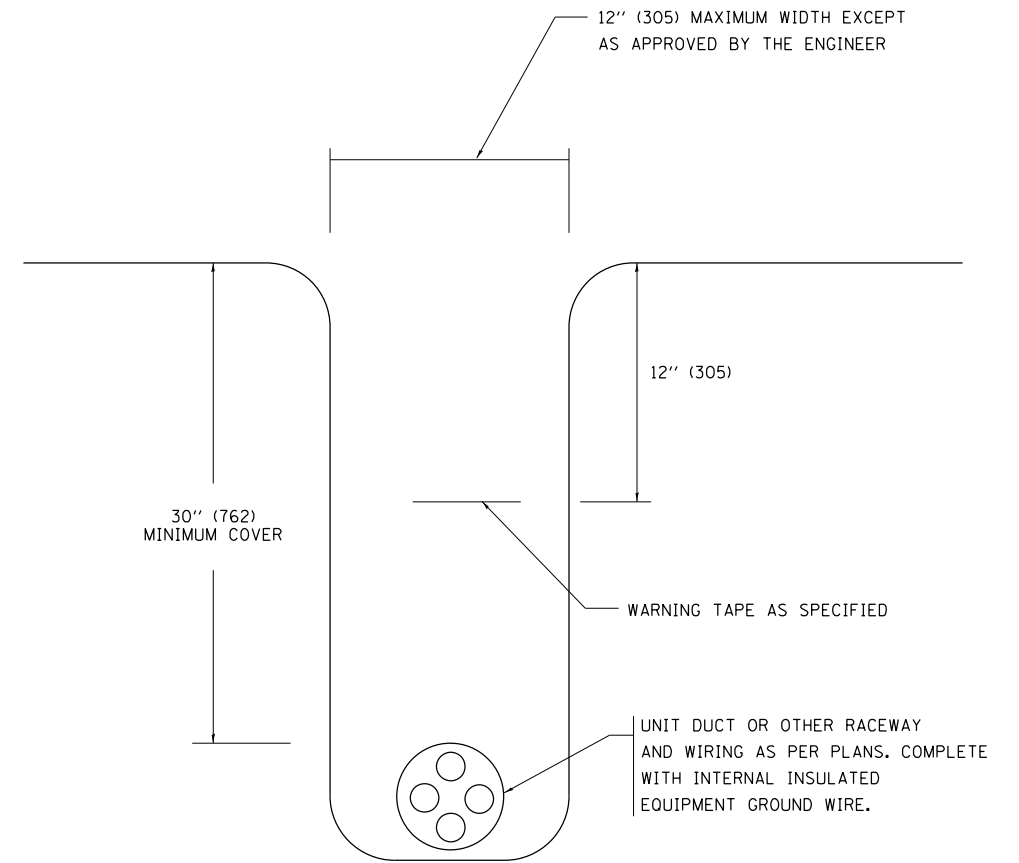
TYPICAL SPLICE DETAIL

N.T.S.



POLE WIRING DETAIL

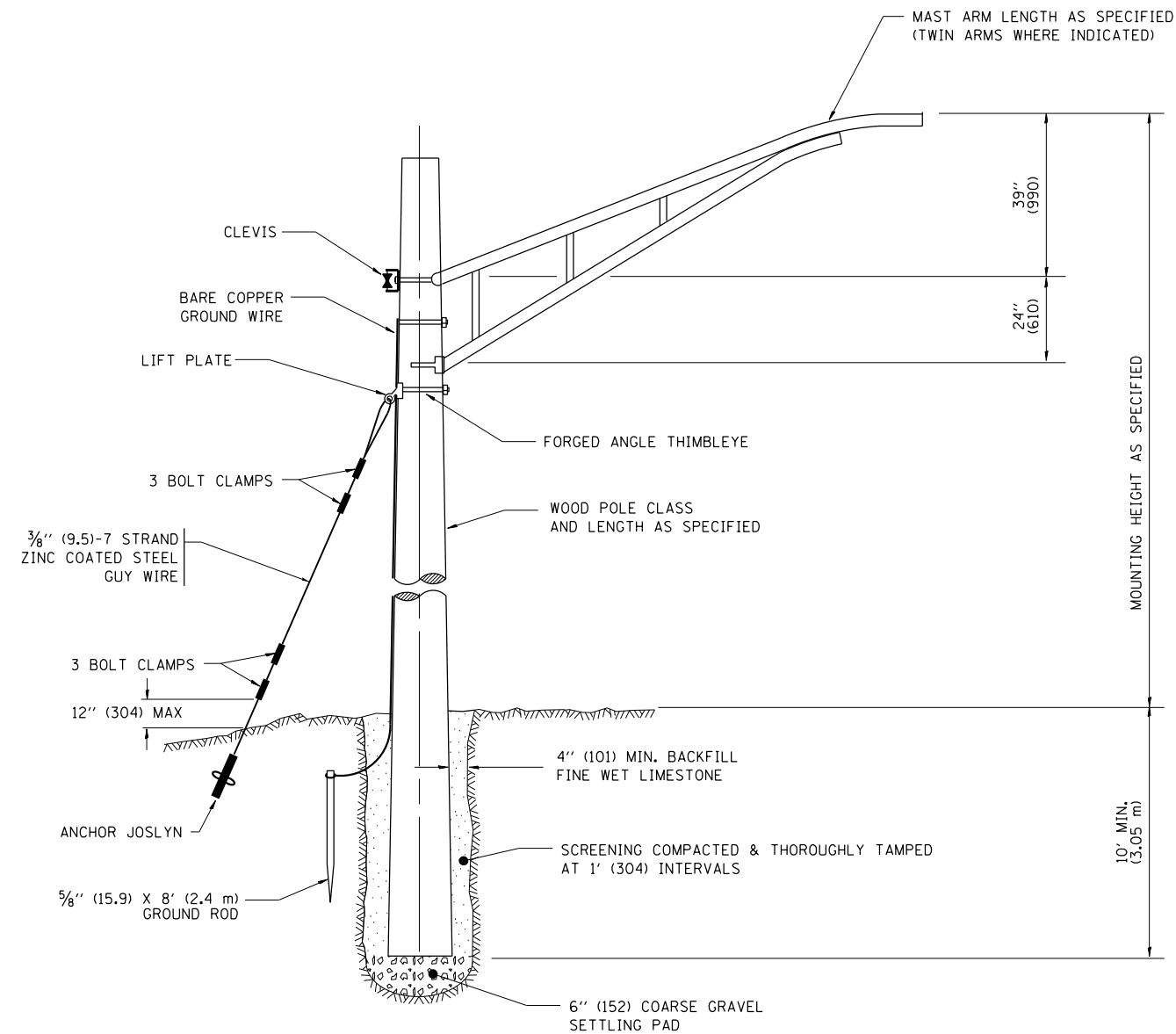
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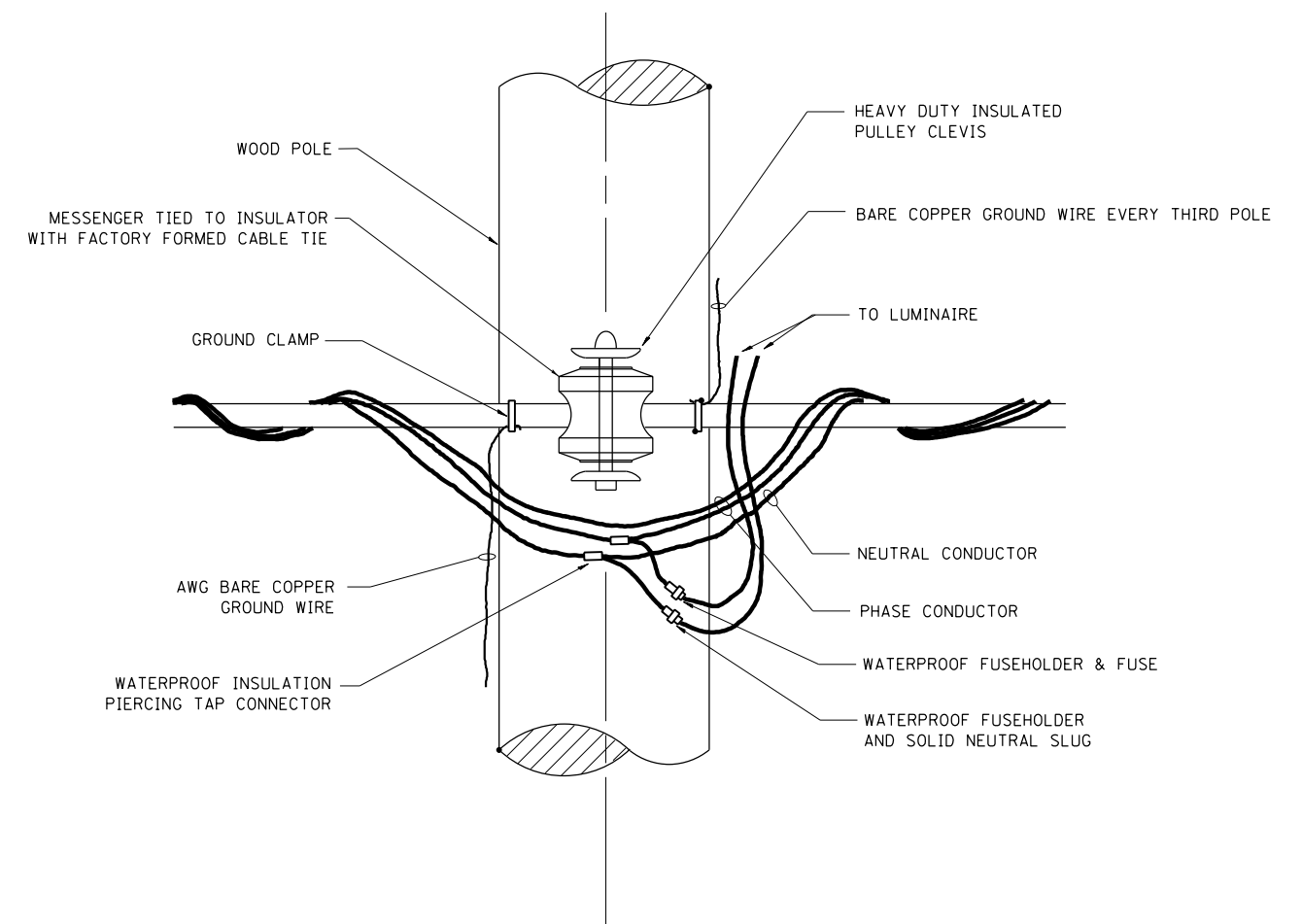
TYPICAL WIRING IN TRENCH DETAIL

N.T.S.

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PLOT DATE = 12/19/2014	DATE -	CHECKED -	REVISED -					BE-702		CONTRACT NO. 60B42		
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



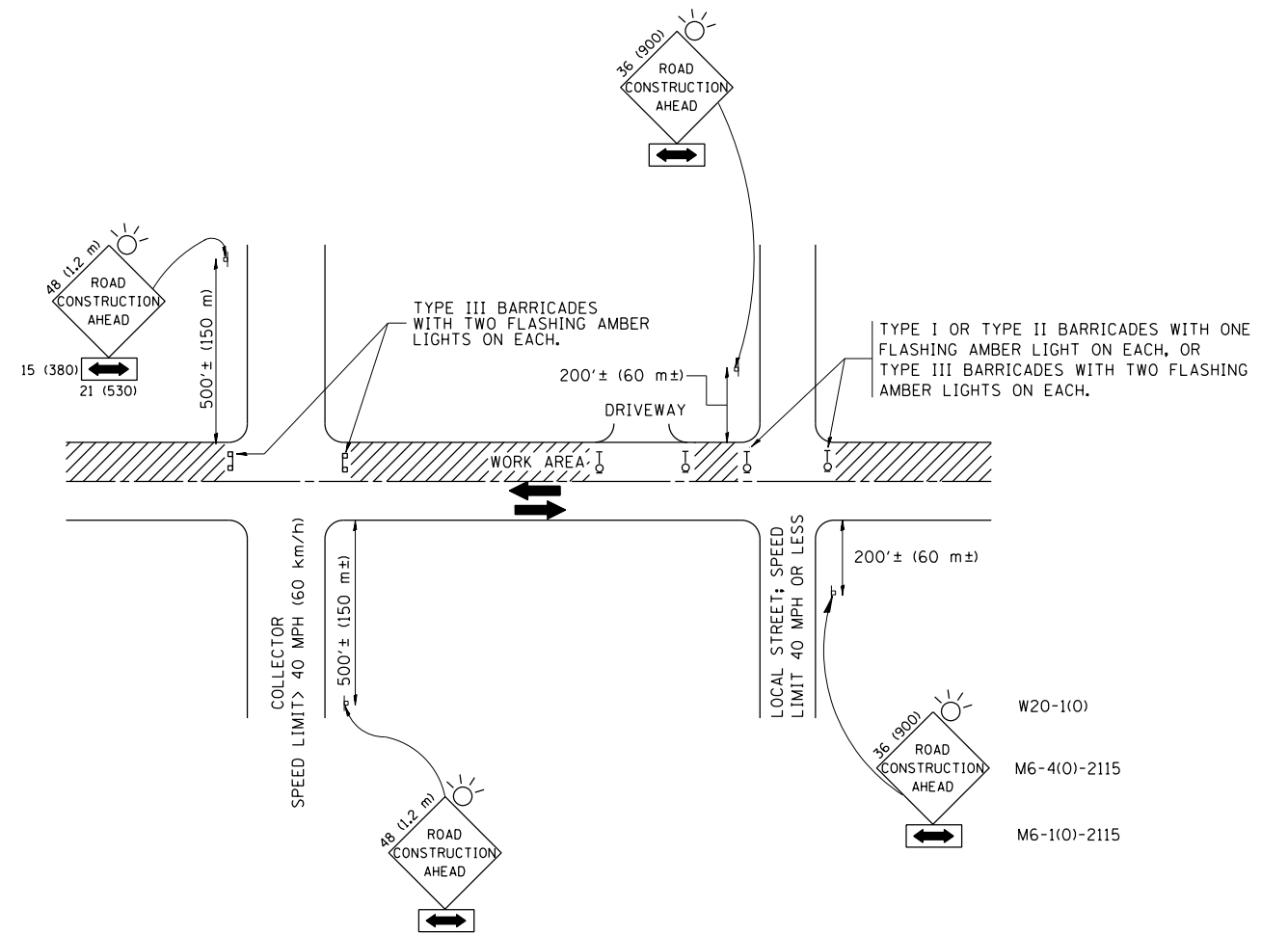
TEMPORARY LIGHT POLE DETAIL



TEMPORARY LIGHT POLE ATTACHMENT DETAIL

- NOTES:**
1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED

FILE NAME = be800.dgn	USER NAME = joburke	DESIGNED -	REVISED - 08-08-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY LIGHT POLE DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.000' / in.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	1321	32RS&N (12)	DUPAGE	495
PLOT DATE = 12/19/2014	CHECKED -	DATE -	REVISED -				BE-800		CONTRACT NO. 60B42			
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>												



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

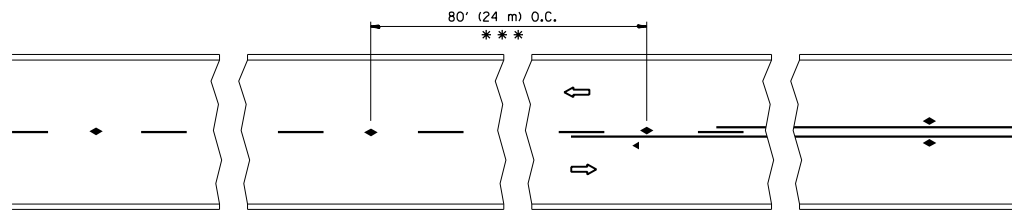
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 100.000 / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 12/19/2014	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

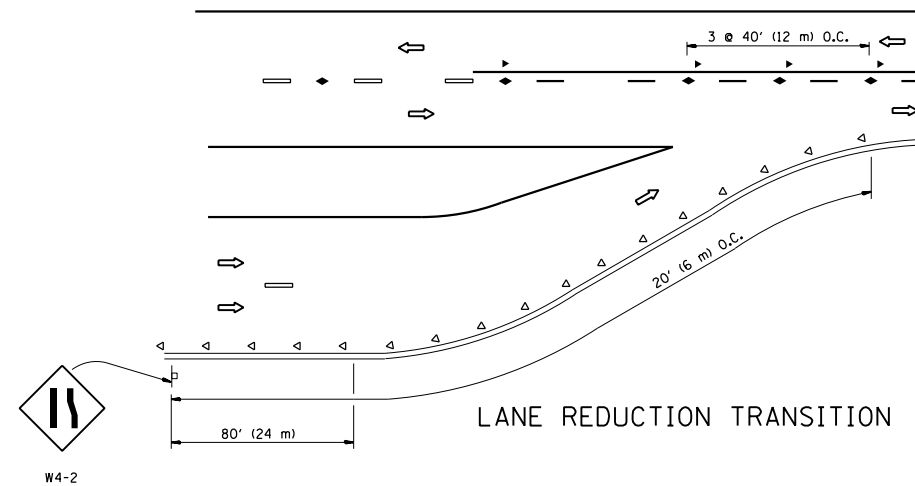
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

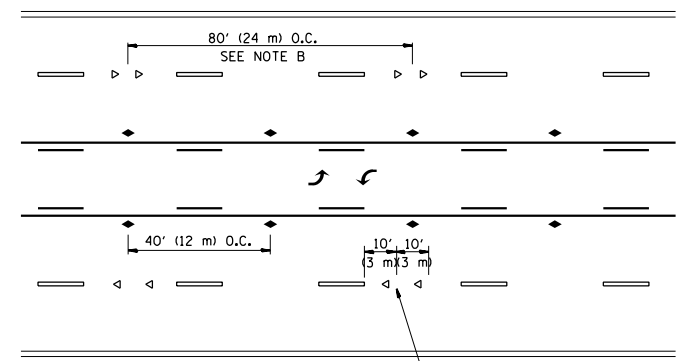


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

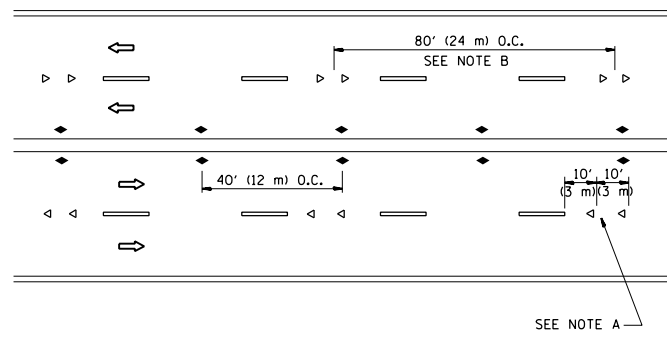
TWO-LANE/TWO-WAY



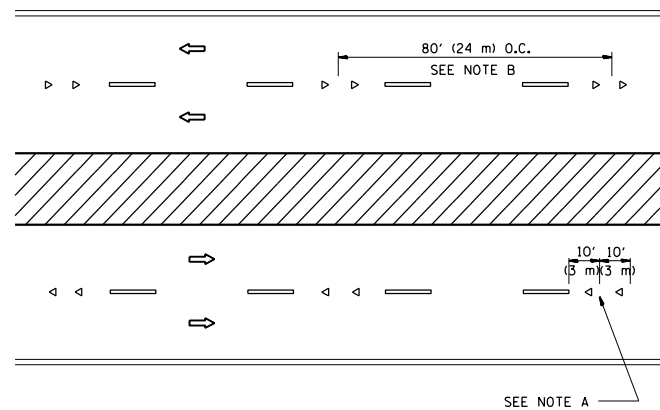
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

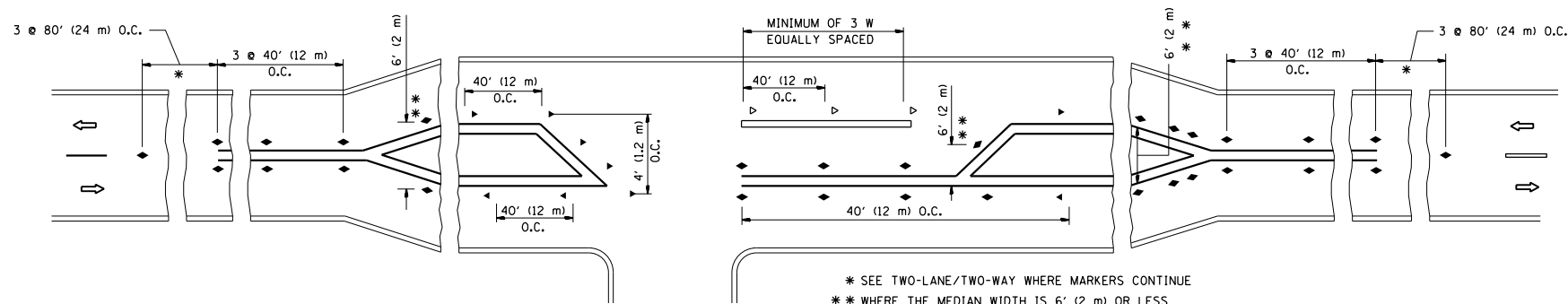
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

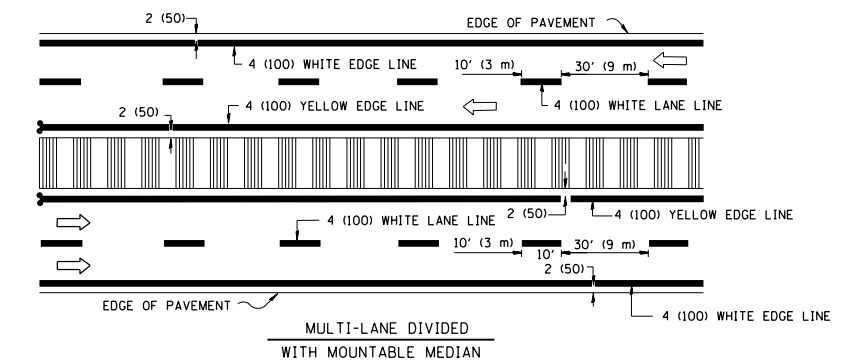
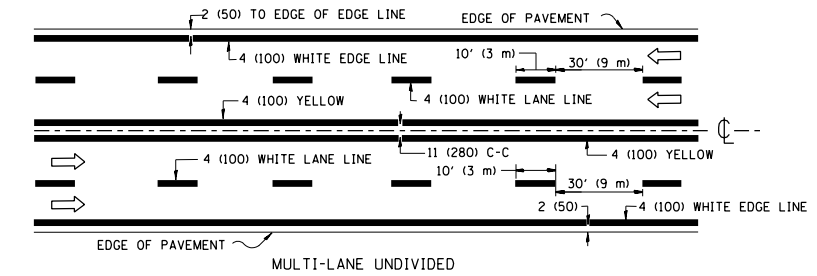
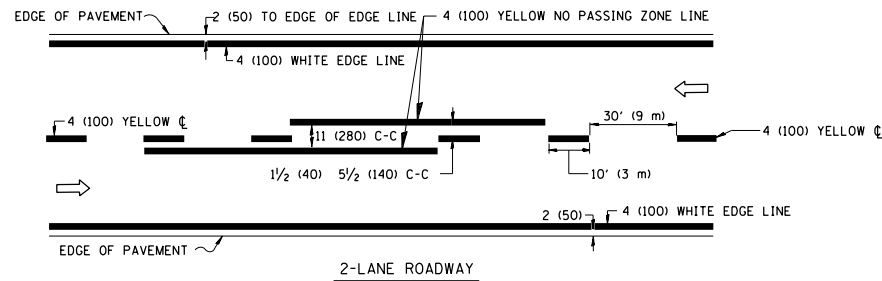
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

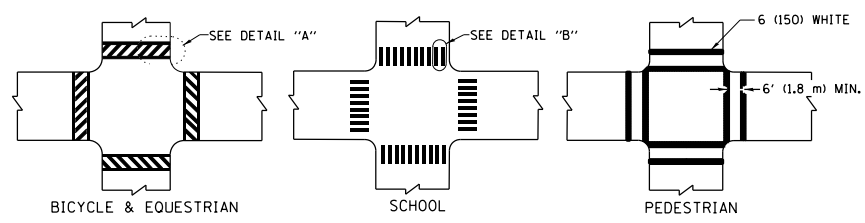
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = tell.dgn	USER NAME = joburke	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.000' / in.	DRAWN -	REVISED - T. RAMMACHER 03-12-99					1321	32RS&N (12)	DUPAGE	495	361
PLOT DATE = 12/19/2014	CHECKED -	DATE -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-11 CONTRACT NO. 60B42			
			REVISED - C. JUCIUS 09-09-09						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

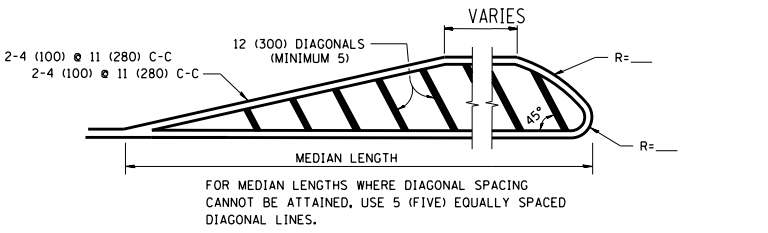
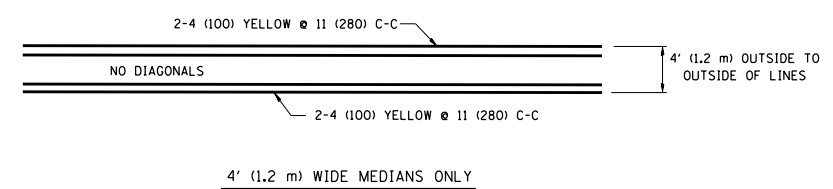


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

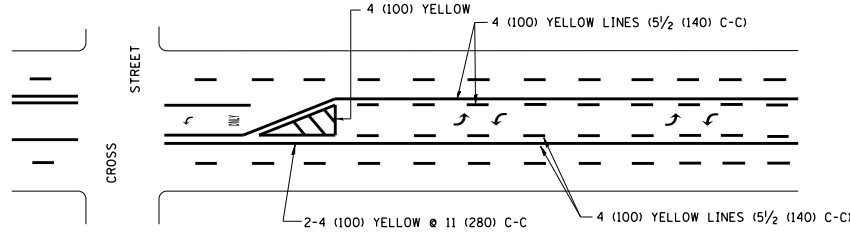


TYPICAL CROSSWALK MARKING

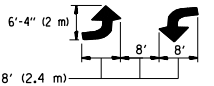


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

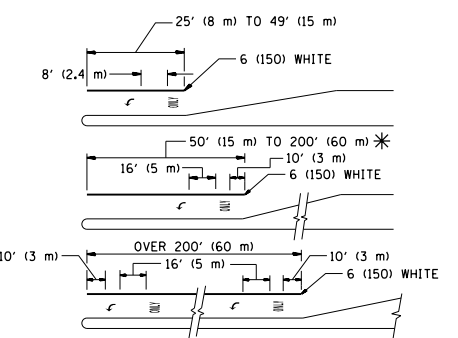


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

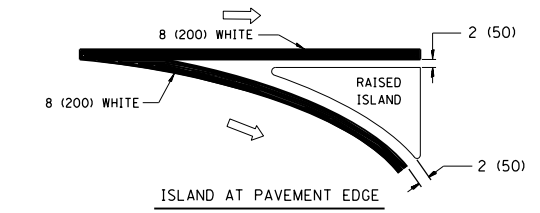
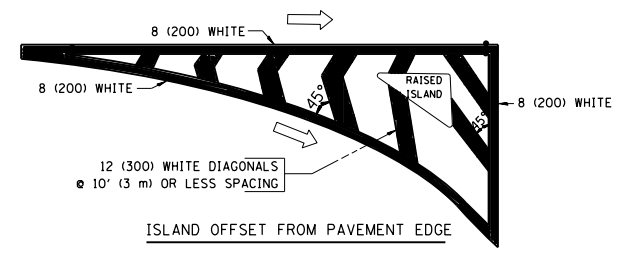


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

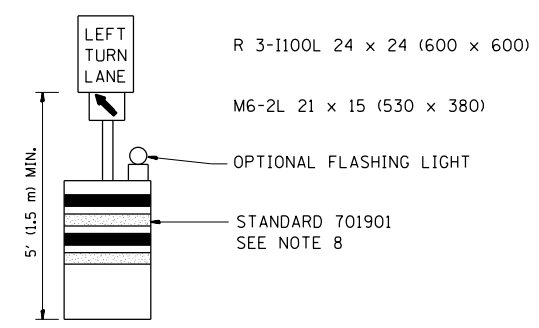
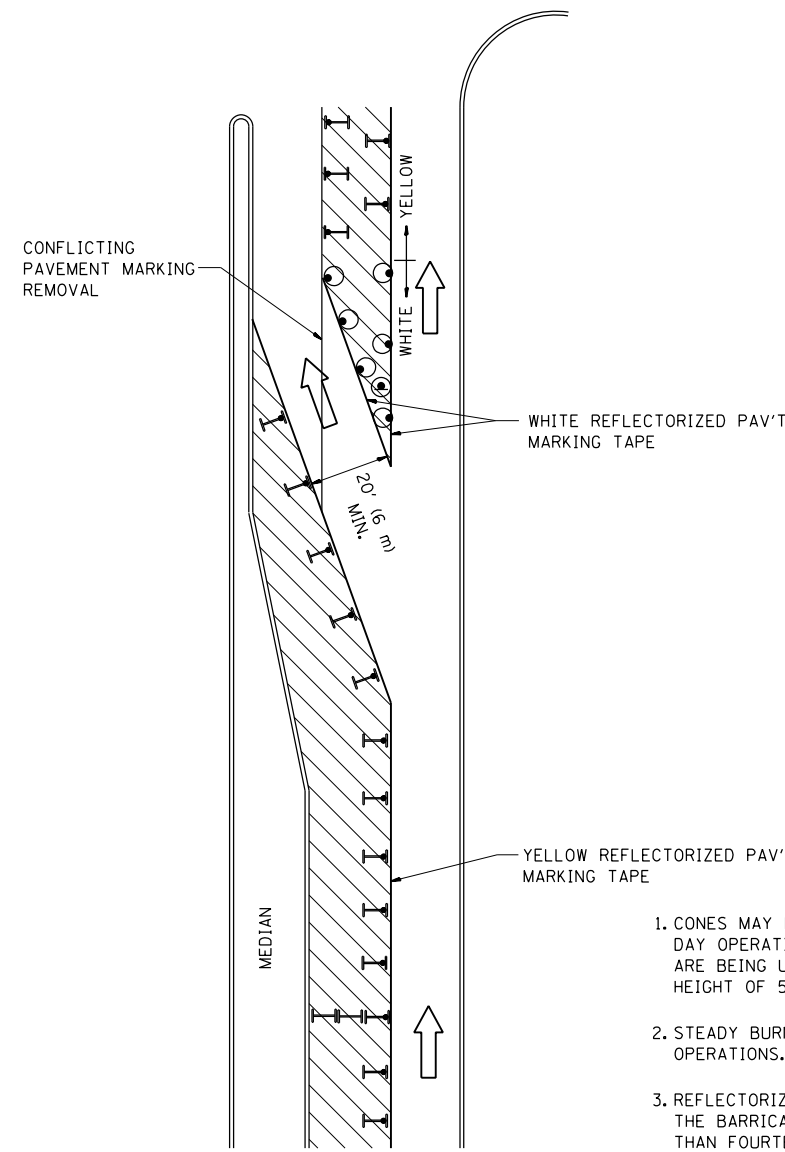


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

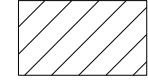







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

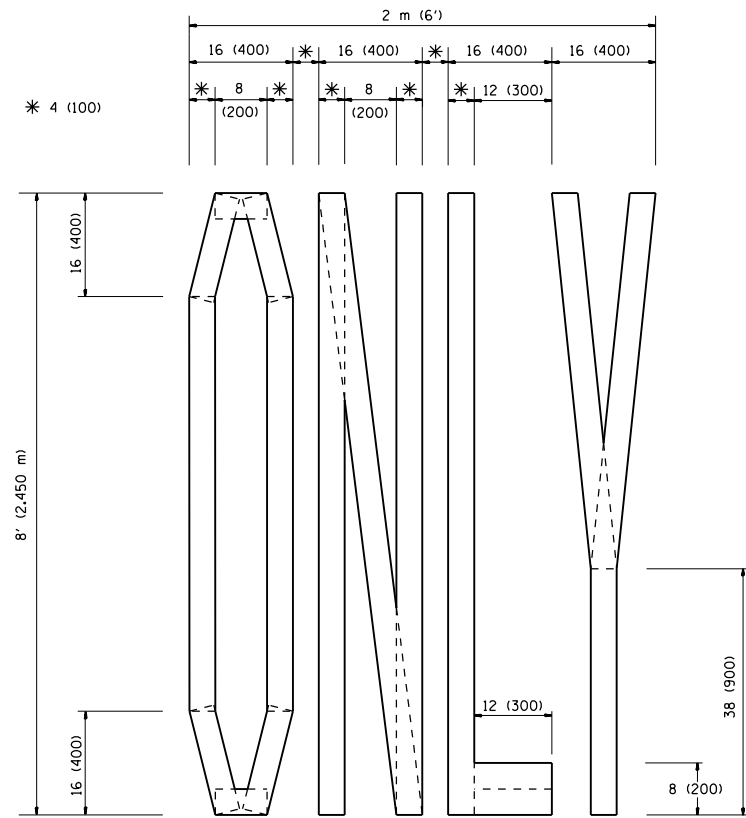
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME = tc14.dgn	USER NAME = joburke	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 99.9998' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 12/19/2014	REVISED -T. RAMMACHER 01-06-00	REVISED -

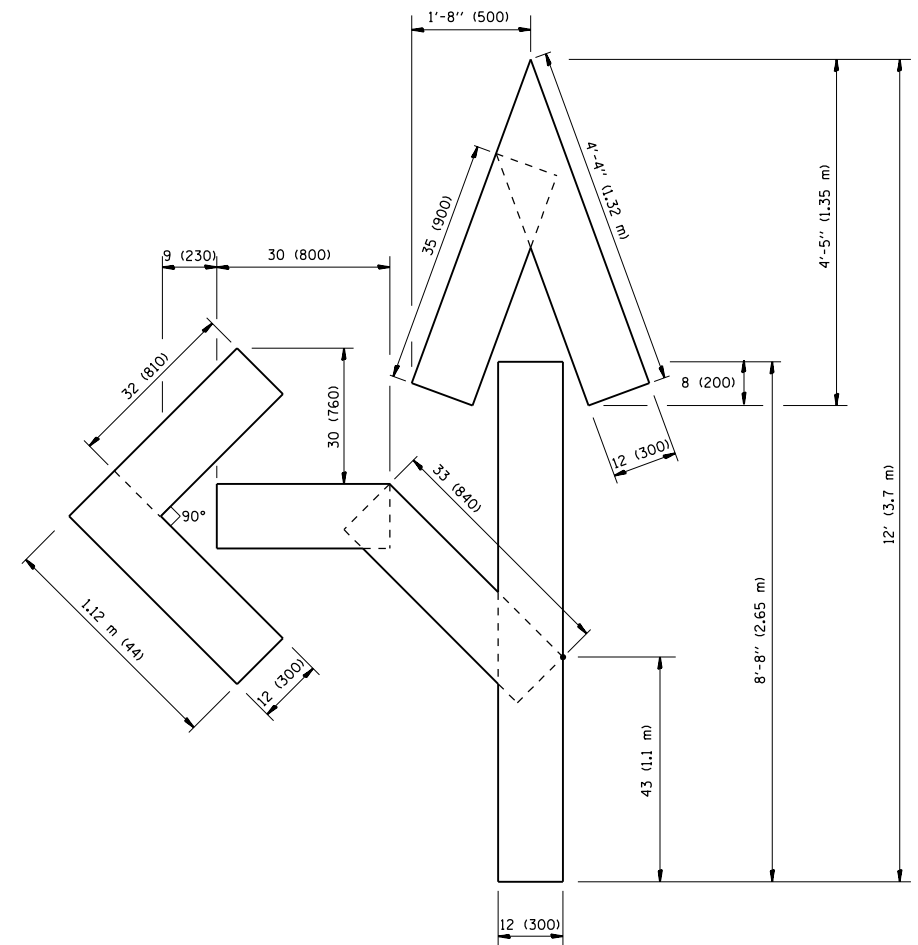
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

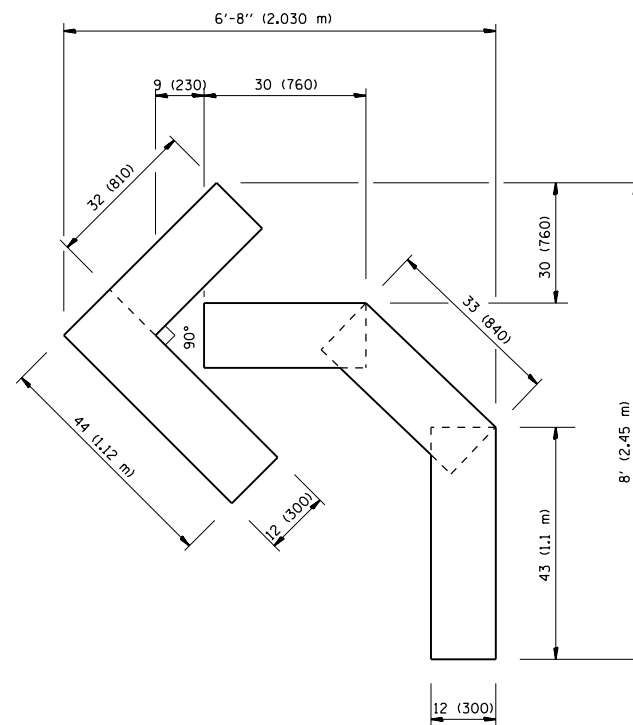
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	363
TC-14			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = tel16.dgn	USER NAME = joburke	DESIGNED - DRAWN -	REVISED -T. RAMMACHER 06-05-96 REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 99.9998 ' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 12/19/2014	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

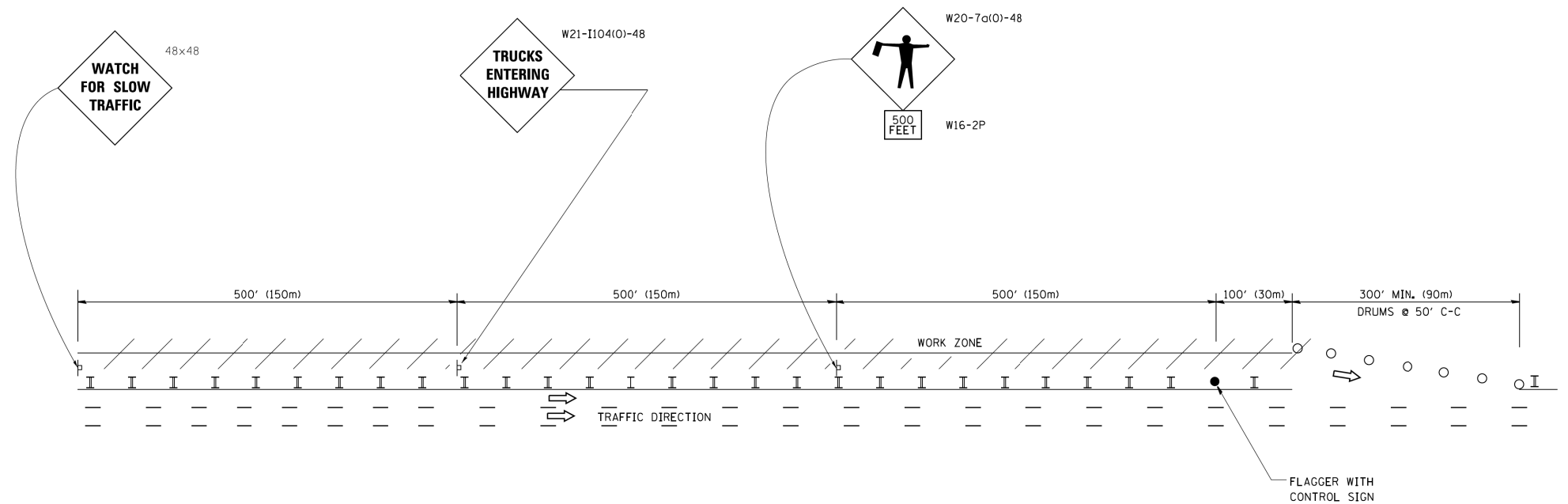
**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

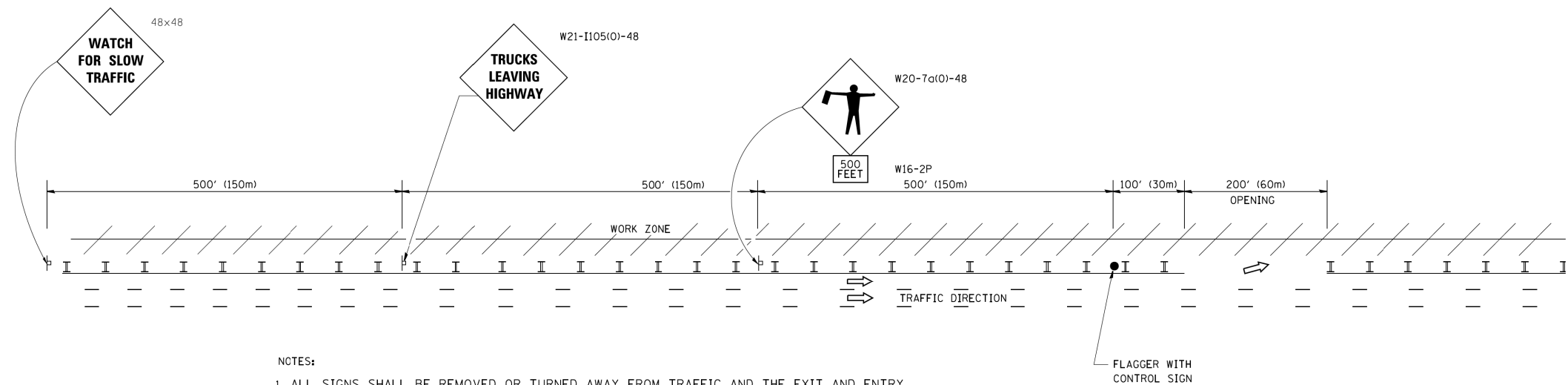
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	364
TC-16			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING






NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.






ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = tc18.dgn	USER NAME = joburke	DESIGNED -	REVISED - J.A.F..02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - S.P.B..01-07			1321	32RS&N (12)	DUPAGE	495	365	
		CHECKED -	REVISED - S.P.B..12-09			TC-18		CONTRACT NO. 60B42			
		DATE -	REVISED - M.D. 06-13			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
				SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.			






ROUTE MARKERS

-  FOR U.S. ROUTES
M1-40-2424
-  FOR ILLINOIS ROUTES
M1-50-2424
-  R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

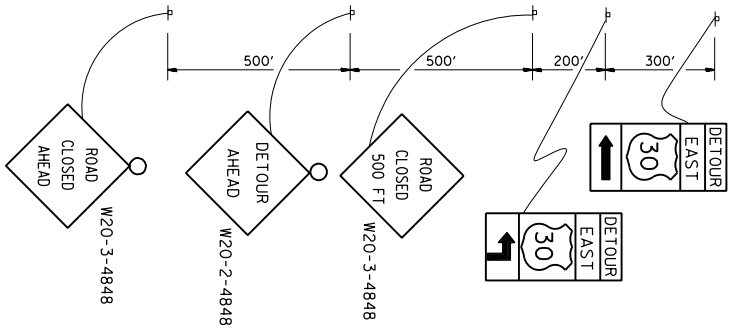
ARROWS SIGNS

-  M5-1L-2115
-  M5-1R-2115
-  M6-1-2115
-  M6-1-2115
-  M6-3-2115

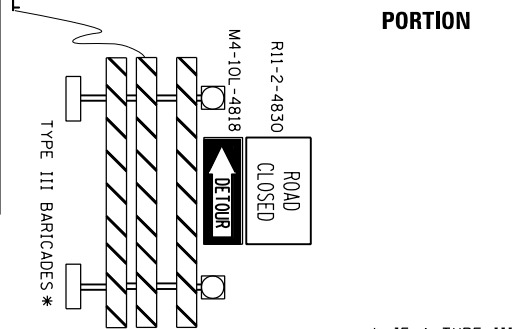
CARDINAL DIRECTION & DETOUR SIGNS

-  NORTH M3-1-2412
-  EAST M3-2-2412
-  SOUTH M3-3-2412
-  WEST M3-4-2412
-  DETOUR M4-8-2412

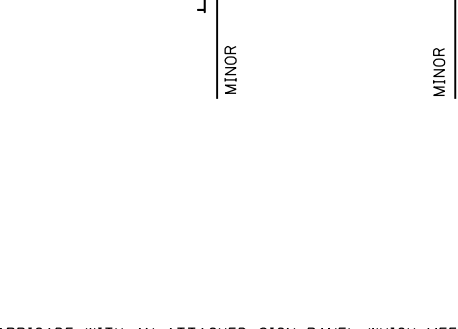
STATE ROUTE



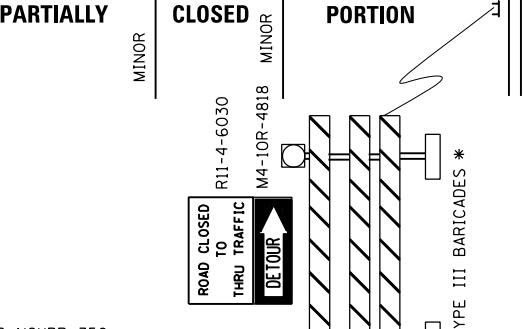
COMPLETELY CLOSED PORTION



PARTIALLY CLOSED PORTION



STATE ROUTE



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =
tc21.dgn

USER NAME = joburke
DESIGNED -
DRAWN -
PLOT SCALE = 99.9998' / in.
CHECKED -
DATE = 12/19/2014

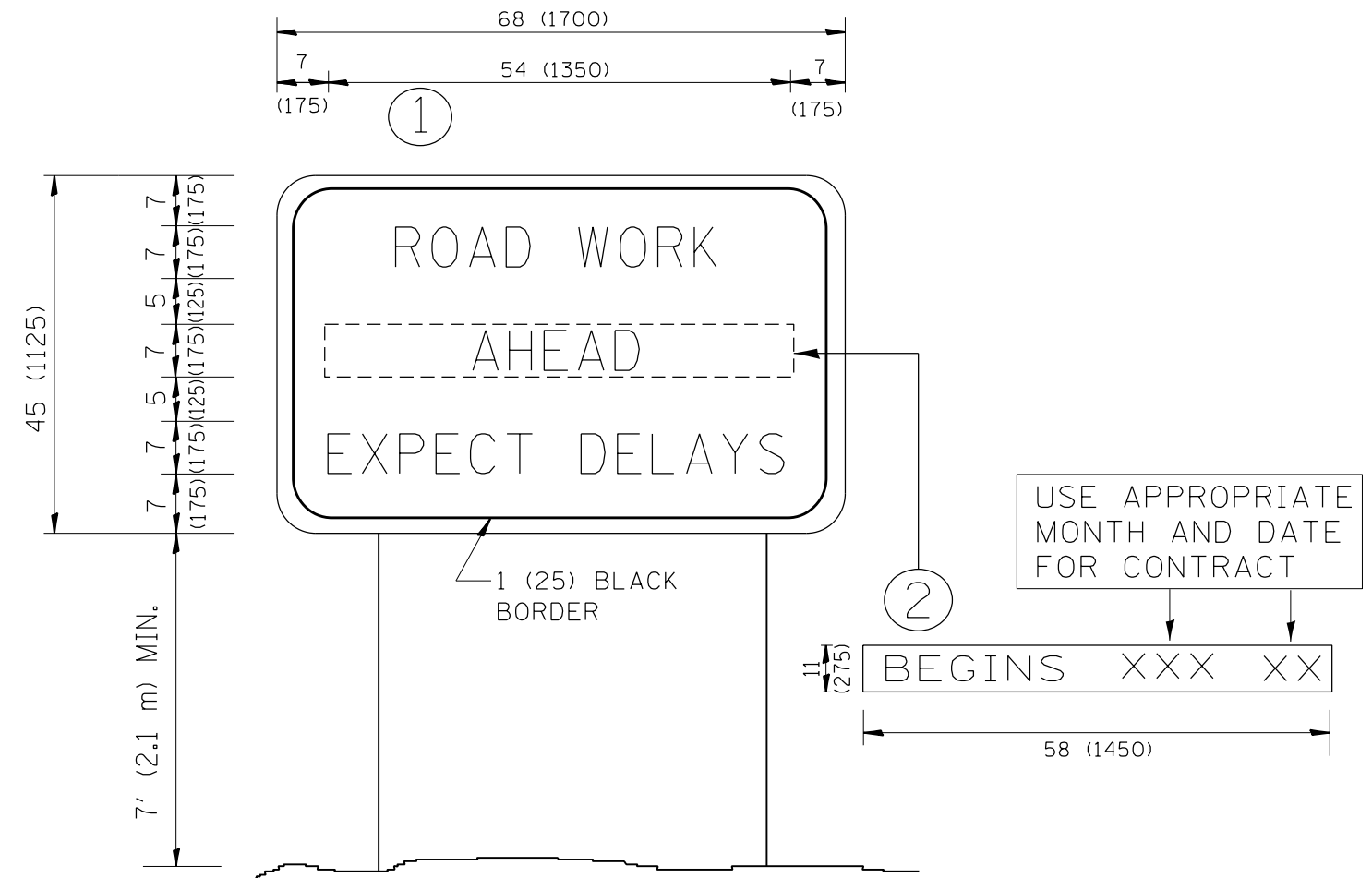
DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED - 10-18-02
REVISED - R. BORO 09-14-09
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETOUR SIGNING
FOR CLOSING STATE HIGHWAYS**
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	366
TC-21		CONTRACT NO. 60B42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

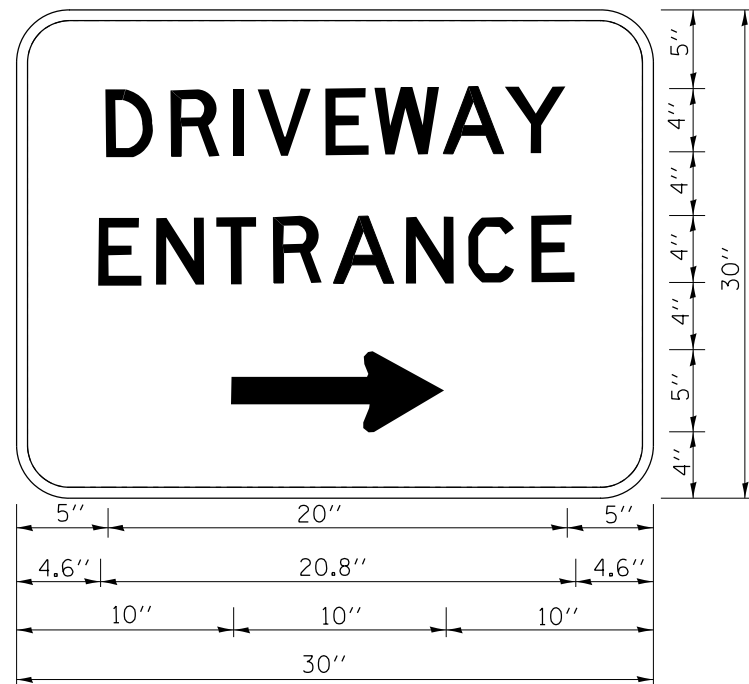
FILE NAME = tc22.dgn	USER NAME = joburke	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.000 / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 12/19/2014	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	367
TC-22			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = tc26.dgn	USER NAME = joburke	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 100.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/19/2014	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

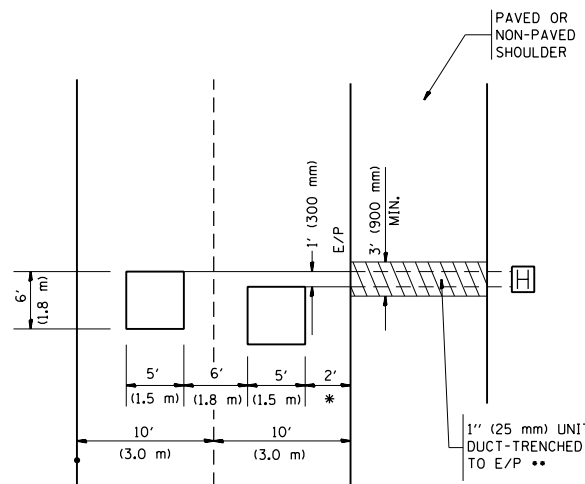
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	368
TC-26			CONTRACT NO. 60B42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



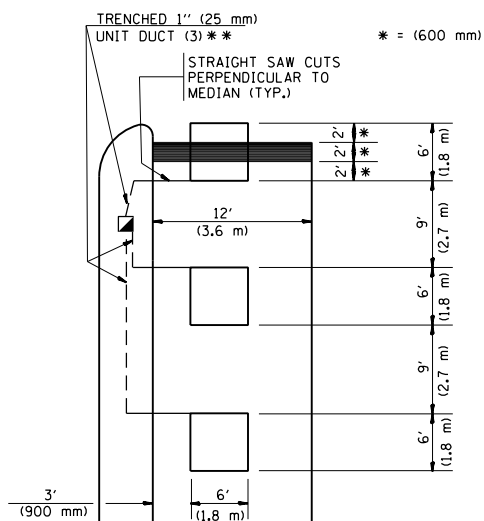
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

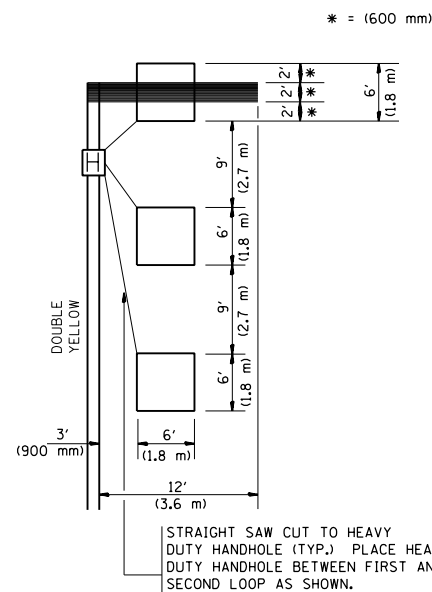


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

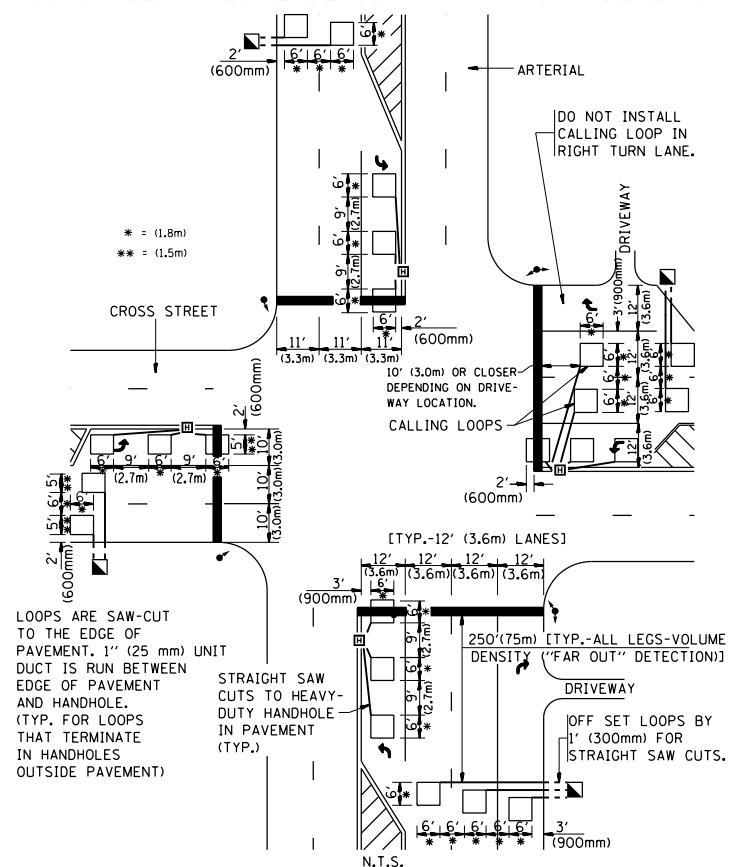
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



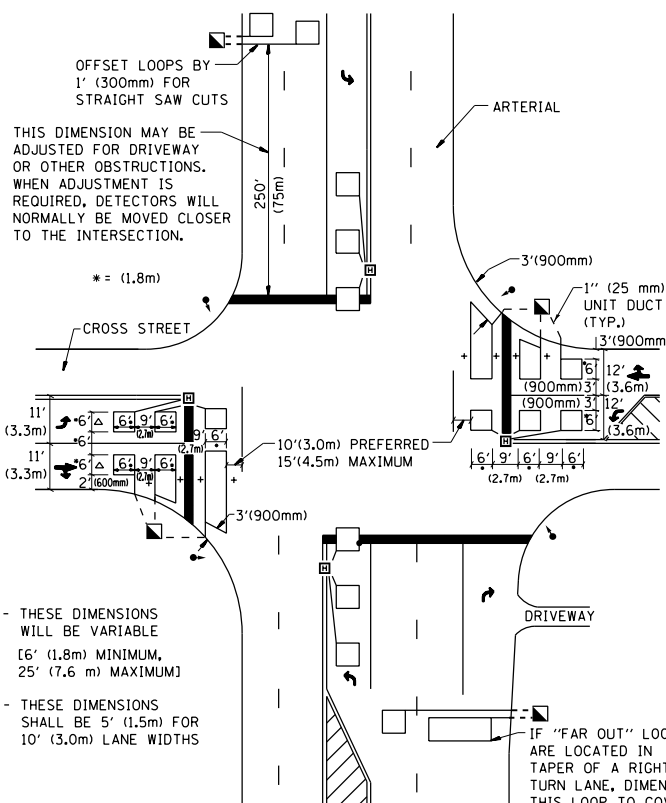
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

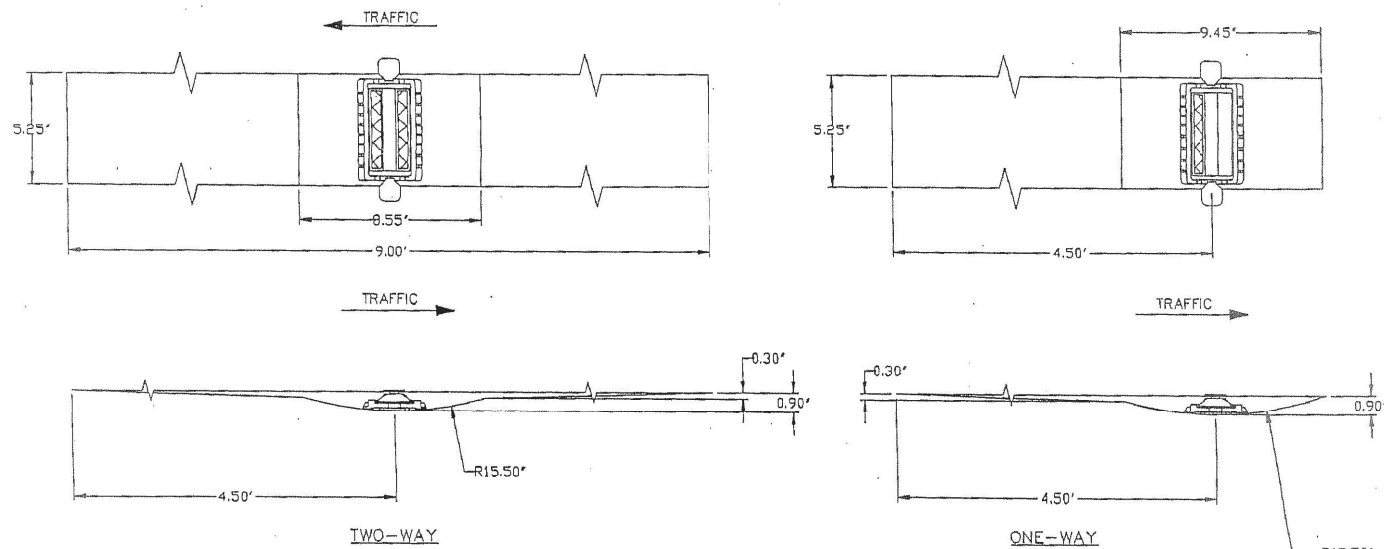
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = ts07.dgn	USER NAME = joburke	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =
	PLOT SCALE = 99.9998' / in.	CHECKED - R.K.F.	REVISED -			1321	32RS&N (12)	DUPAGE	495	369
	PLOT DATE = 12/19/2014	DATE -	REVISED -			TS-07		CONTRACT NO. 60B42		
						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



RECESSED REFLECTIVE PAVEMENT MARKERS

INSTALLATION NOTES:

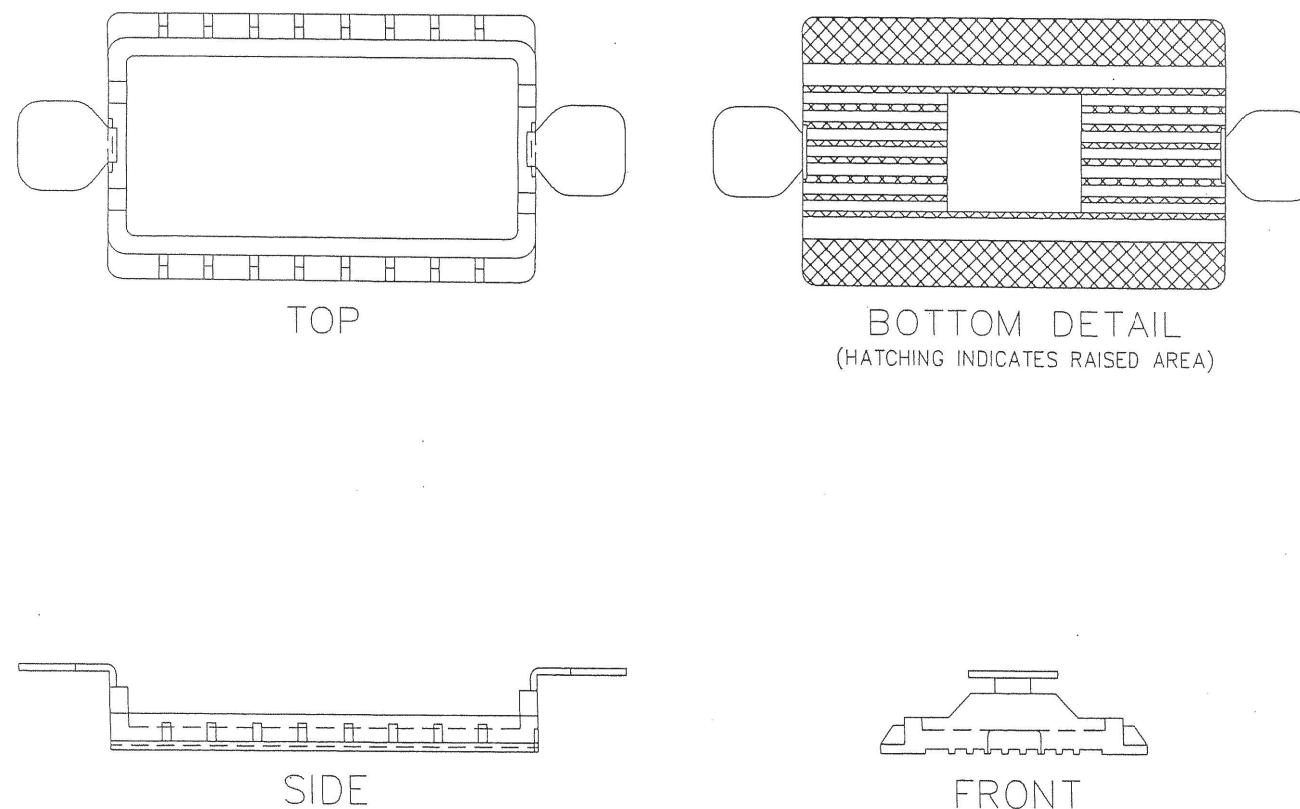
1. SAW CUT TO DIMENSIONS SHOWN.
2. SAW CUT AREAS ARE TO BE DRY AND FREE OF MATERIAL THAT ADVERSELY AFFECTS THE ADHESIVE BOND.
3. INSTALL THE REFLECTOR WITH AN APPROVED TWO-COMPONENT EPOXY ADHESIVE. EPOXY SHOULD NOT OBSCURE OR BLOCK THE LENS.
4. REFLECTOR SHALL BE 3M SERIES 190 OR APPROVED EQUIVALENT

GENERAL NOTES:

1. INSTALLATION SHALL CONFORM TO IDOT HIGHWAY STANDARD 781001-02 (OR LATEST) FOR MARKER PLACEMENT.
2. IDOT STANDARD 781001-02 SHALL BE MODIFIED TO REFLECT RECESSED PAVEMENT MARKERS INSTEAD OF RAISED PAVEMENT MARKERS.

TYPICAL RECESSED REFLECTIVE PAVEMENT MARKERS

REFLECTOR HOLDER



USER NAME = mikelly	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - LGP	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RECESSED REFLECTIVE PAVEMENT MARKERS
DUPAGE COUNTY DETAIL**

SCALE: N.T.S. SHEET NO. 1 OF 18 SHEETS STA. TO STA.

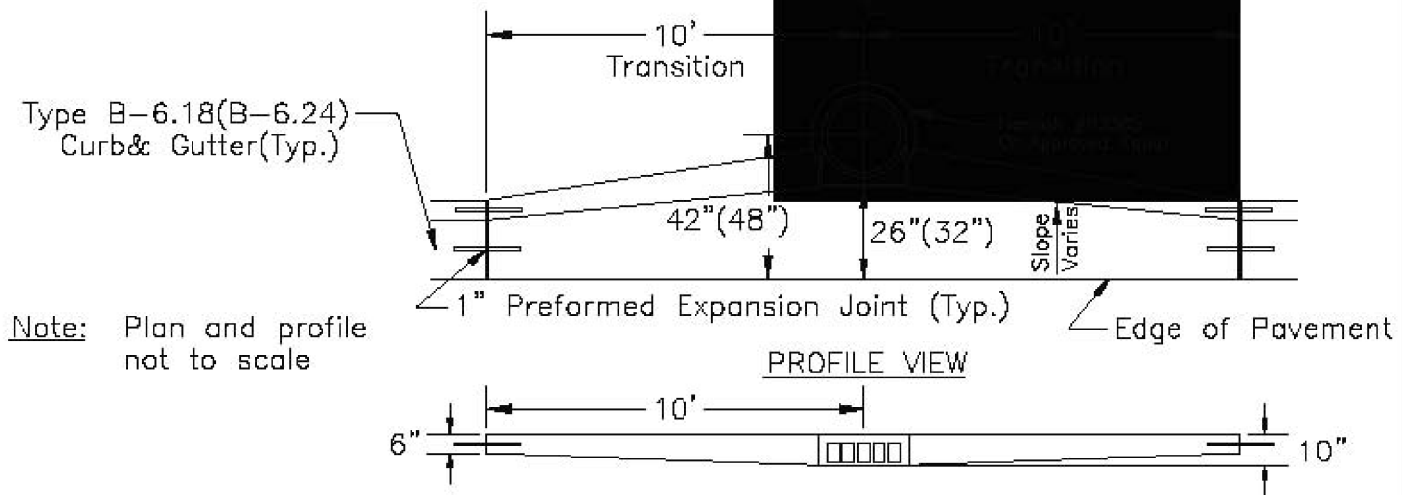
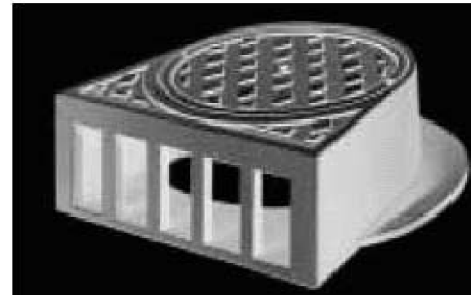
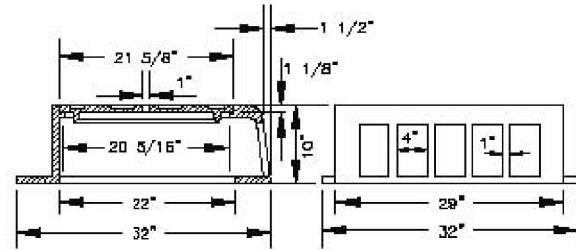
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	370
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

SAG FRAME & LID

Sag Frame and Lid shall be Neenah Foundry Company #R-3305 or equal.

R-3305 Catch Basin Frame and Lid

For behind-the-curb construction.
 Heavy-Duty
 Total weight 937 Kilograms (425 lbs.)



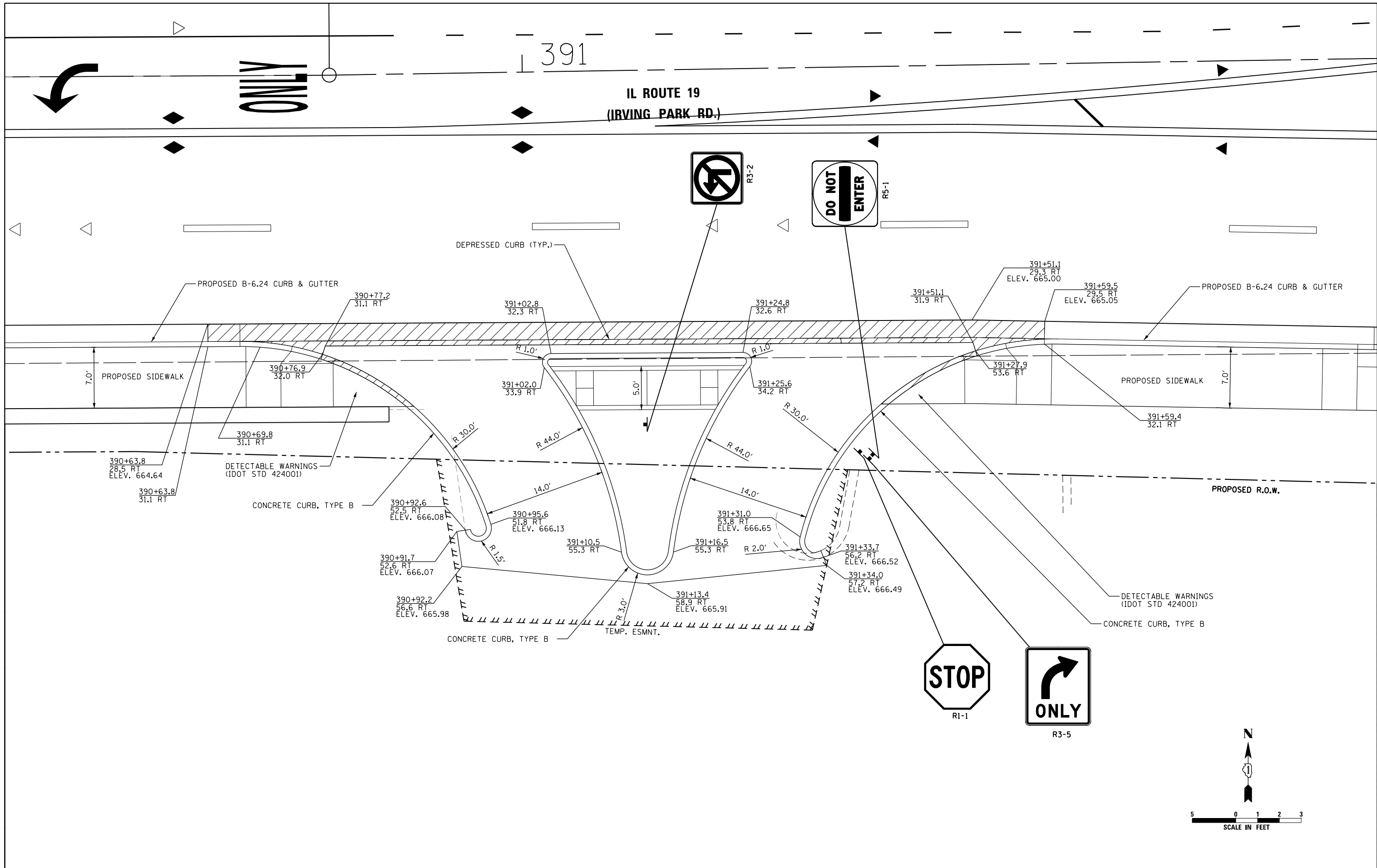
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PLOT SCALE = 20.0000' / in.	DRAWN - ML	REVISED -
PLOT DATE = 12/18/2014	CHECKED - LGP	REVISED -
	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SAG FRAME AND LID
 DUPAGE COUNTY DETAIL**

SCALE: NTS SHEET NO. 2 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	371
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



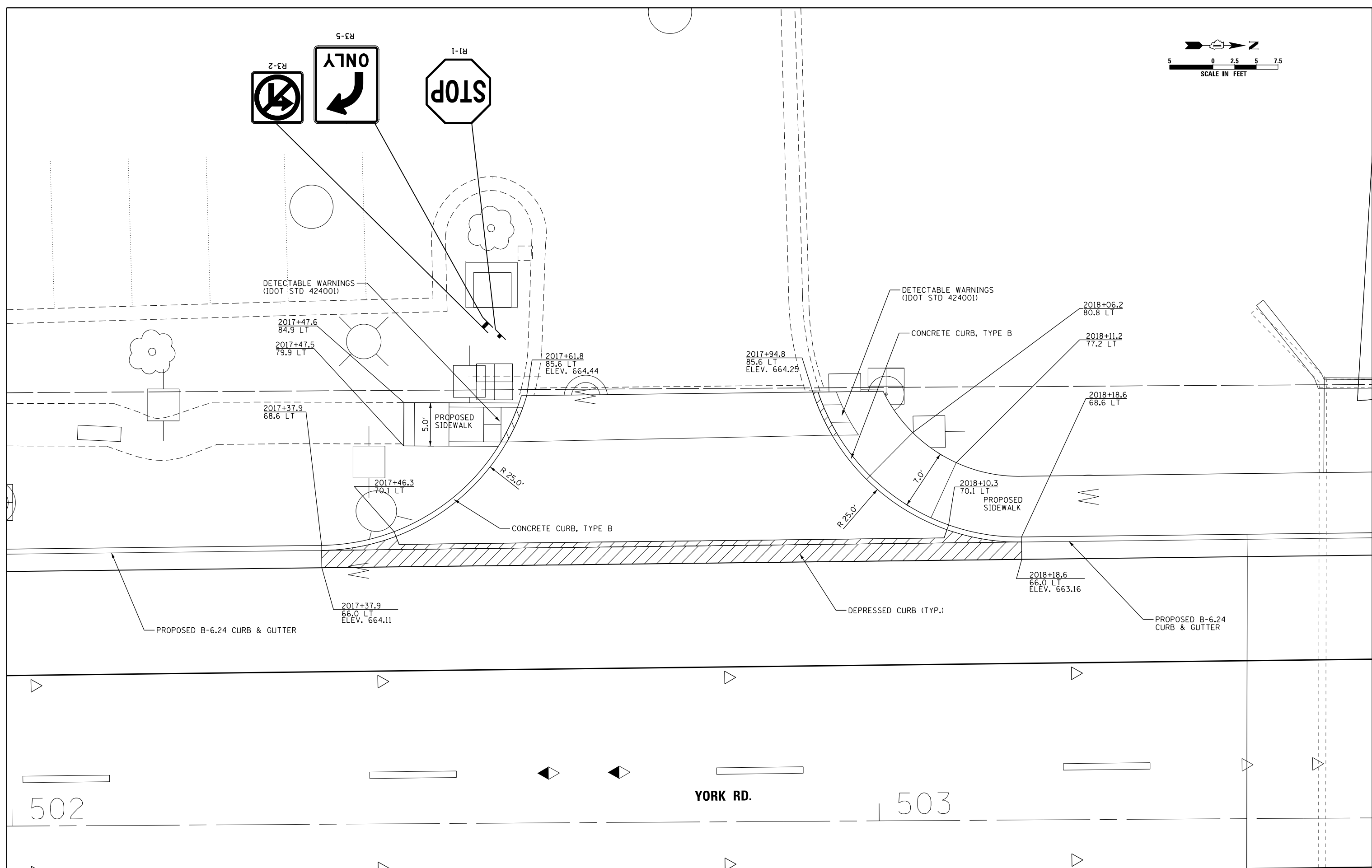
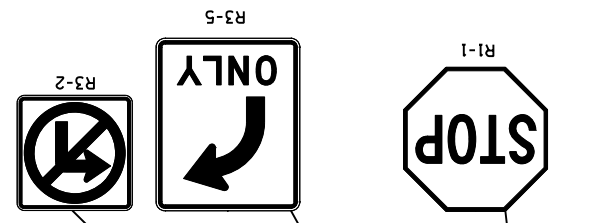
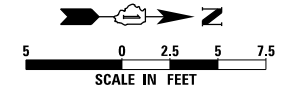
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	DRAWN - ML	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - RTB	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 19 RIGHT-IN/RIGHT OUT
DRIVEWAY DETAIL**

SCALE: 1" = 5' SHEET NO. 3 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	372
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

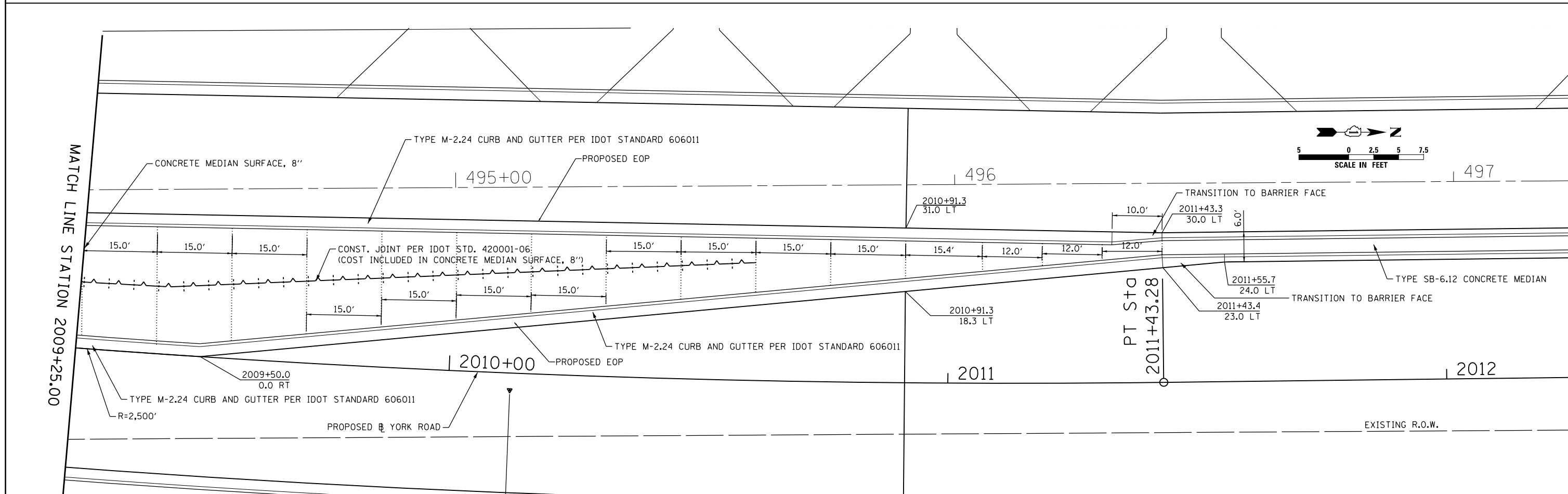
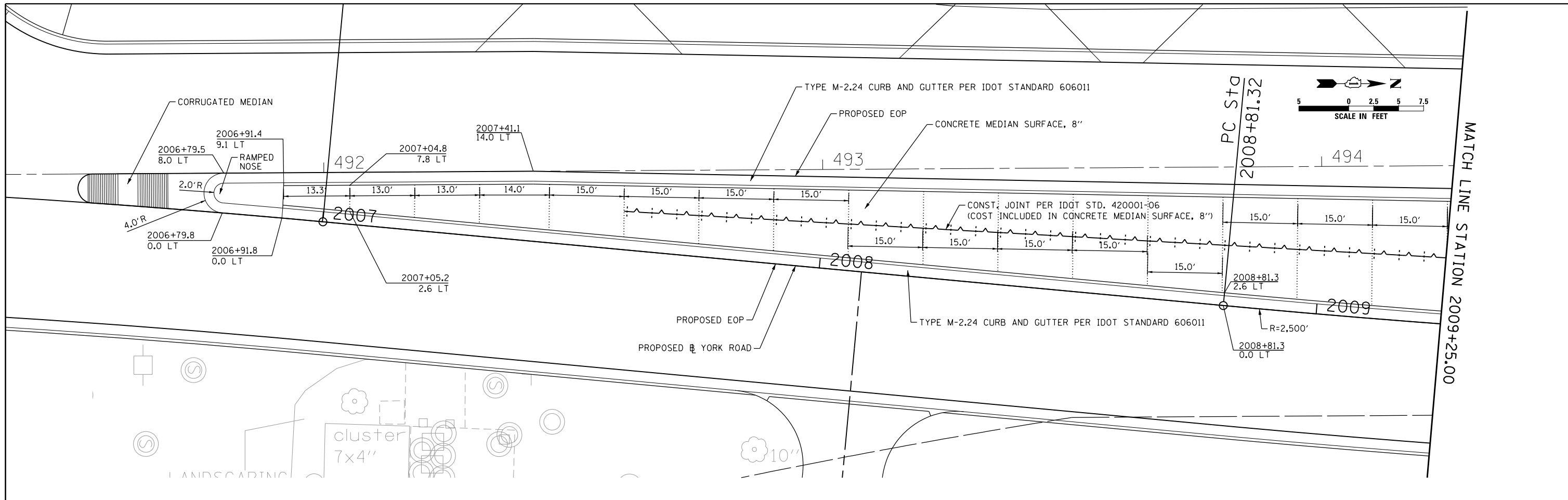


502

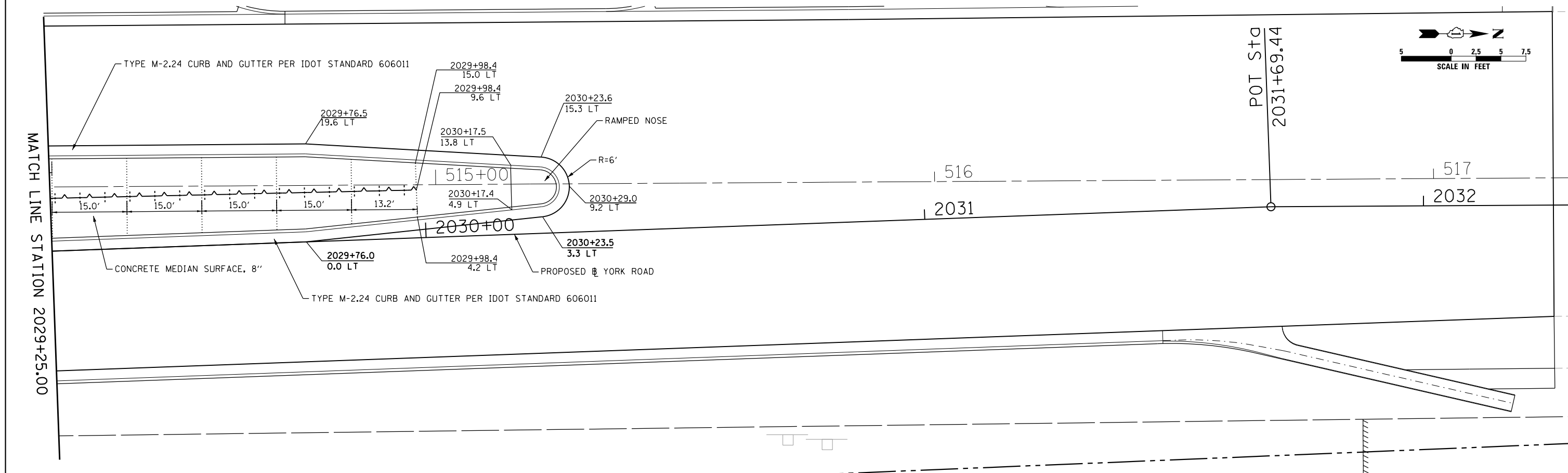
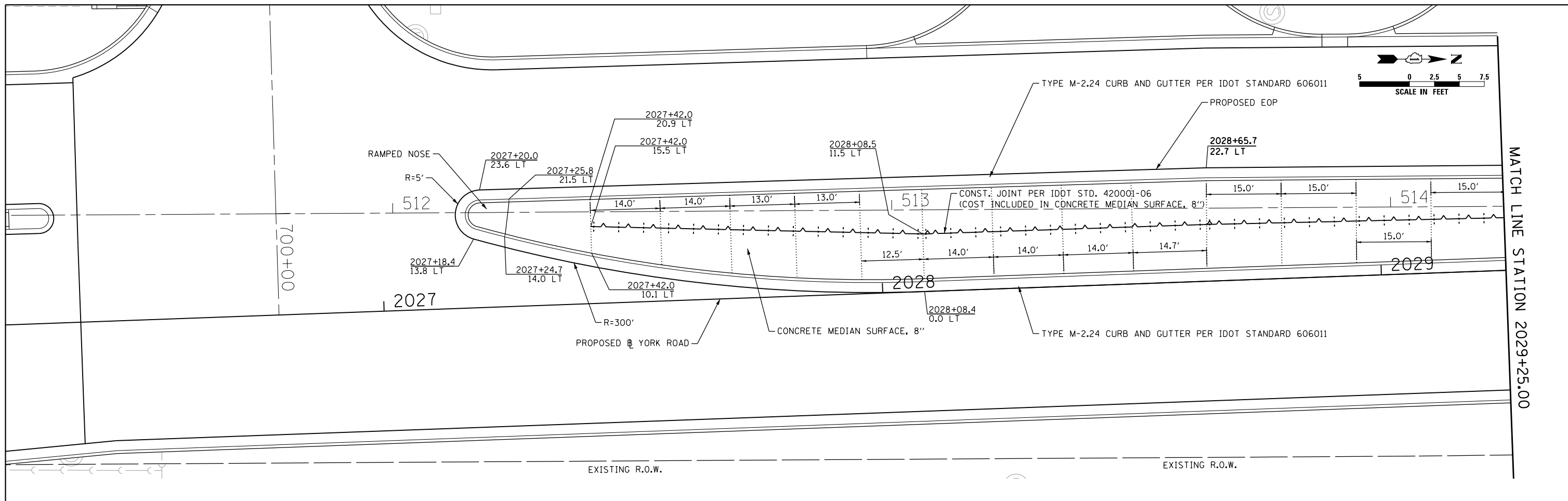
YORK RD.


503

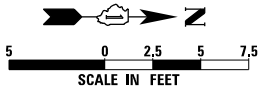
 HDR ENGINEERING, INC.	USER NAME = mikelly	DESIGNED - MPK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WALGREENS RIGHT-IN/RIGHT OUT DRIVEWAY DETAIL		F.A.U. RTE. = 1321	SECTION = 32RS&N (12)	COUNTY = DUPAGE	TOTAL SHEETS = 495	SHEET NO. = 373	
	PLOT SCALE = 10.0000' / 1" =	CHECKED - RTB	REVISED -		SCALE: 1" = 5'	SHEET NO. 4 OF 18 SHEETS	STA. TO STA.	CONTRACT NO. 60B42				
	PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -		ILLINOIS FED. AID PROJECT							



 HDR ENGINEERING, INC.	USER NAME = mikelly	DESIGNED - MPK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	YORK ROAD (SOUTH LEG) CONCRETE MEDIAN DETAIL		F.A.U. RTE. = 1321	SECTION = 32RS&N (12)	COUNTY = DUPAGE	TOTAL SHEETS = 495	SHEET NO. = 374			
	PLOT SCALE = 20.0000' / in.	CHECKED - RTB	REVISED -				CONTRACT NO. 60B42							
	PLOT DATE = 12/18/2014	DATE = 12-19-2014	REVISED -				SCALE: NTS	SHEET NO. 5 OF 18 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				



 HDR ENGINEERING, INC.	USER NAME = mikelly	DESIGNED - MPK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	YORK ROAD (NORTH LEG) CONCRETE MEDIAN DETAIL		F.A.U. RTE. 1321	SECTION 32RS&N (12)	COUNTY DUPAGE	TOTAL SHEETS 495	SHEET NO. 375
	PLOT SCALE = 20.0000' / in.	CHECKED - RTB	REVISED -		SCALE: NTS	SHEET NO. 6 OF 18 SHEETS	STA. TO STA.	CONTRACT NO. 60B42 ILLINOIS FED. AID PROJECT			
	PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -								



YORK ROAD

B-6.24 CURB & GUTTER

HOT-MIX ASPHALT BASE COURSE WIDENING, 13"

2031+71.54
23.94 RT

2031+74.62
27.72 RT

HOT-MIX ASPHALT SHOULDERS, 8"

2031+71.67
28.82 RT
ELEV.=668.10
SEE IDOT STD
DETAIL 606006-02

AGGREGATE SHOULDERS, TYPE B 8"

END B-6.24
CURB & GUTTER
2031+46.82
26.00 RT
ELEV.=668.69

END OUTLET
2032+17.70
39.67 RT
ELEV.=667.18
SEE IDOT STD
DETAIL 606006-02



HDR ENGINEERING, INC.

USER NAME = mikelly	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - RTB	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

YORK ROAD
OUTLET FOR B-6.24 CURB AND GUTTER DETAIL

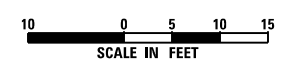
SCALE: 1"=5'

SHEET NO. 7 OF 18 SHEETS

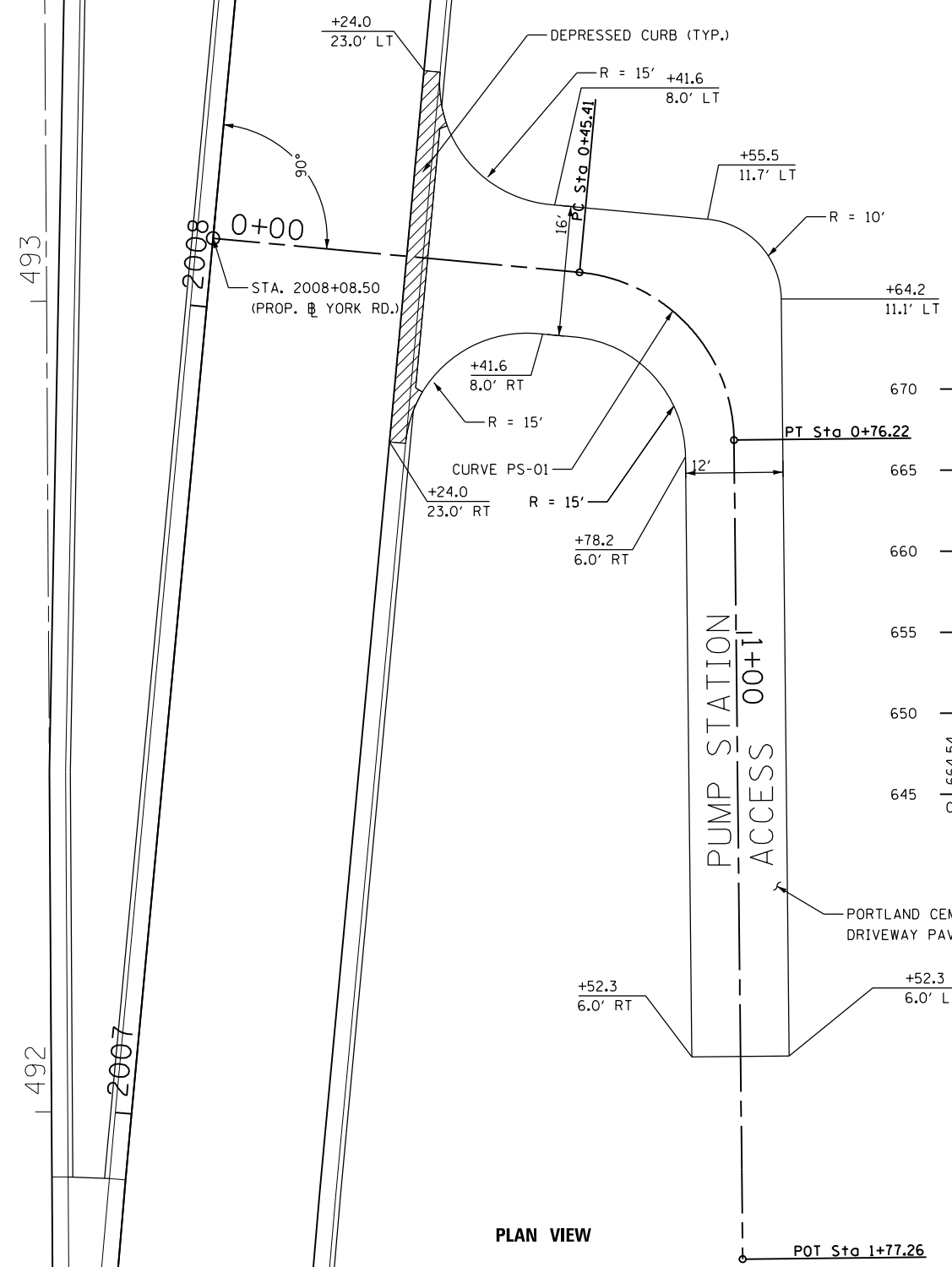
STA.

TO STA.

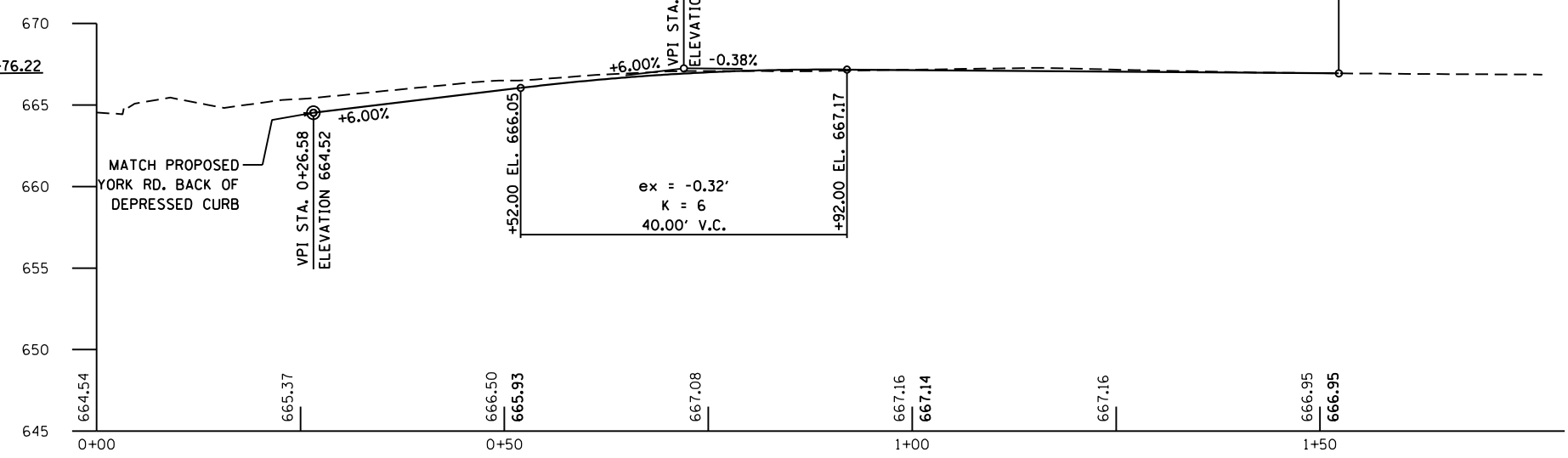
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	376
CONTRACT NO. 60B42			ILLINOIS FED. AID PROJECT	



PROP. CURVE PS-01
 PI STA. = 0+64.34
 $\Delta = 84^\circ 04' 44''$ (RT)
 $D = 272^\circ 50' 13''$
 $R = 21.00'$
 $T = 18.93'$
 $L = 30.82'$
 $E = 7.28'$
 P.C. STA = 0+45.41
 P.T. STA = 0+76.22



PLAN VIEW



PROFILE
 1" = 10' (H)
 1" = 5' (V)



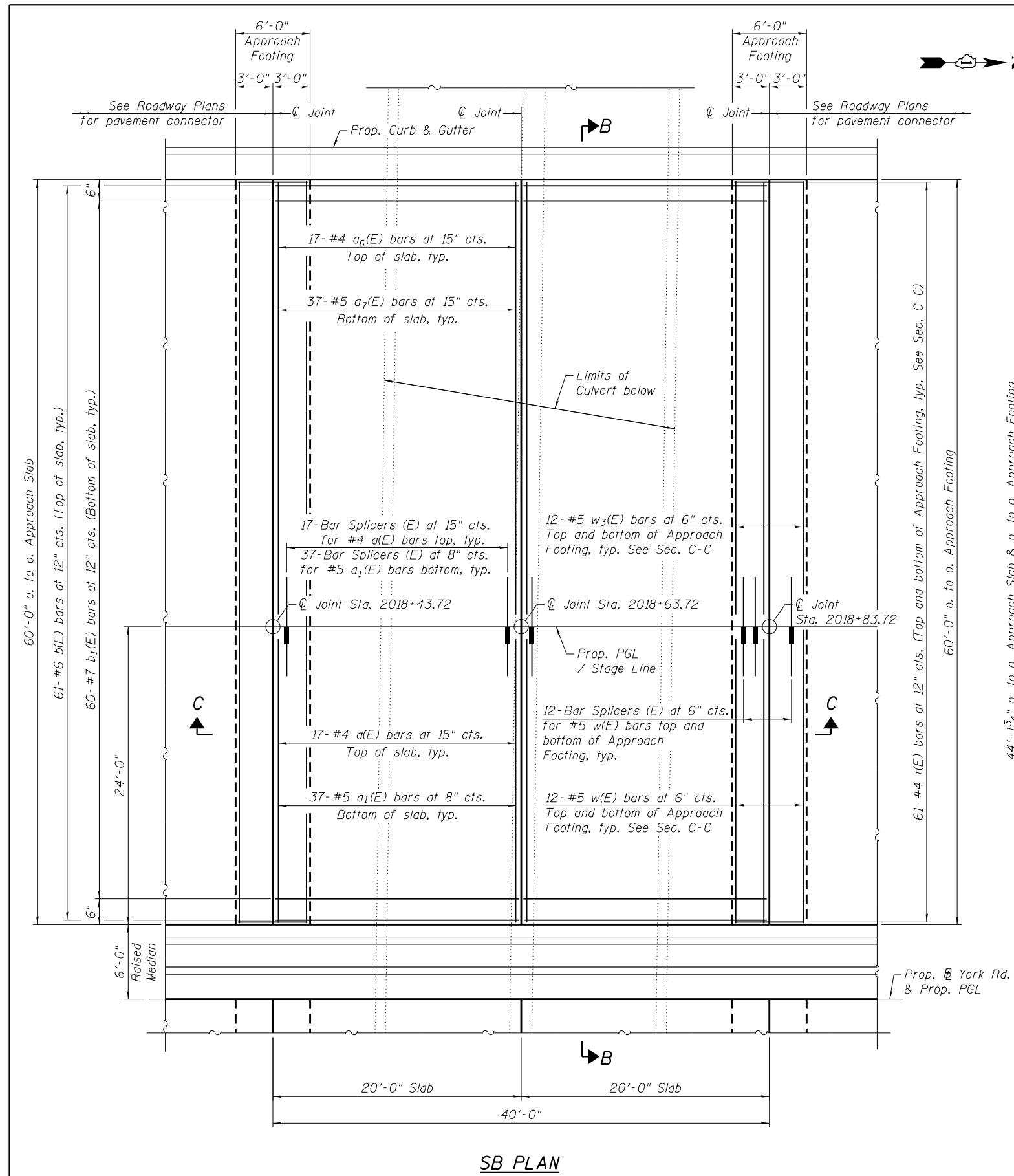
USER NAME = mikelly	DESIGNED - MPK	REVISED -
DRAWN - ML	REVISED -	
PLOT SCALE = 20.0000' / in.	CHECKED - RTB	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

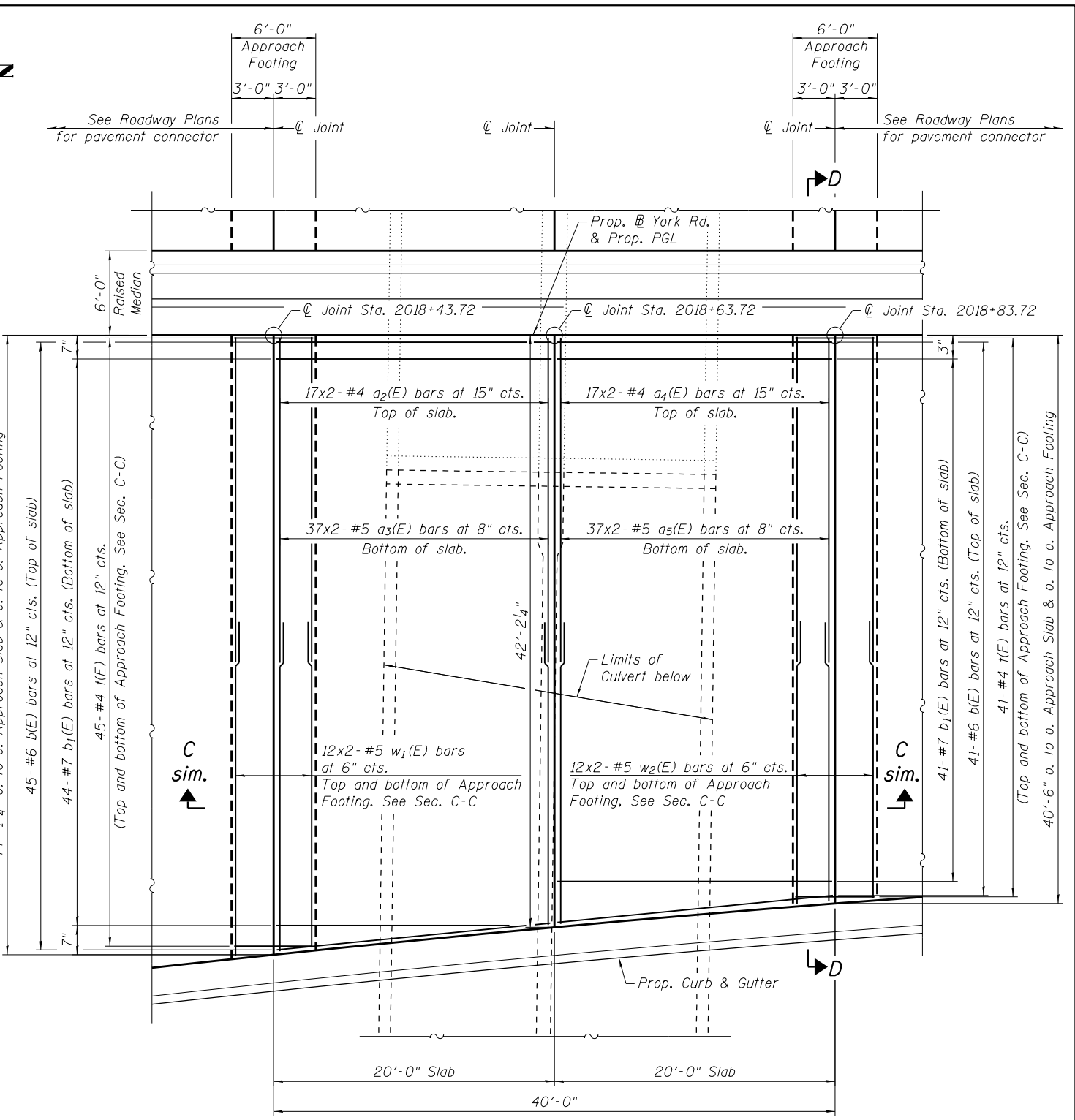
PUMP STATION ACCESS
 DETAIL

SCALE: 1" = 10' SHEET NO. 8 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	377
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



SB PLAN



NB PLAN

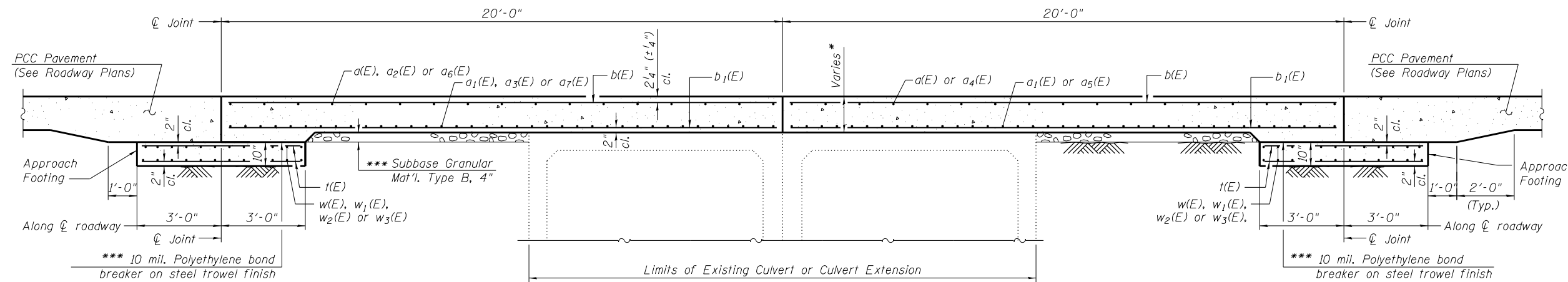
MIN. BAR LAP

- #4 bars - 2'-1"
- #5 bars - 2'-7"

Notes:

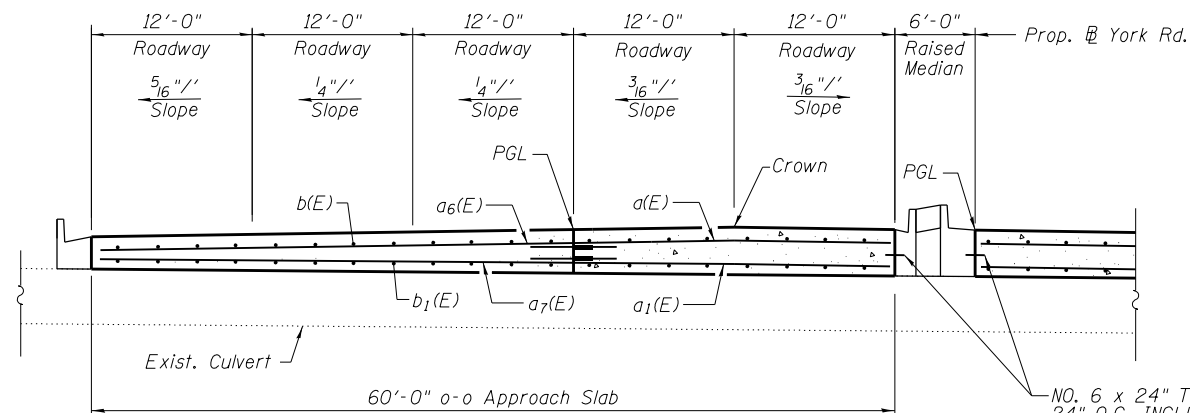
1. See sheet 12 of 12 for Sections B-B, C-C & D-D.
2. a(E) thru a5(E) bar spacings measured along \perp Rdwy.

 HDR ENGINEERING, INC.	USER NAME = mikelly	DESIGNED - MPK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	YORK ROAD APPROACH SLAB DETAIL		F.A.U. RTE. = 1321	SECTION = 32RS&N (12)	COUNTY = DUPAGE	TOTAL SHEETS = 495	SHEET NO. = 378
	PLOT SCALE = 20.0000' / in.	CHECKED - LGP	REVISED -				SCALE: NTS	SHEET NO. 9 OF 18 SHEETS	STA. TO STA.	CONTRACT NO. 60B42	
PLOT DATE = 12/18/2014	DATE = 12-19-2014	REVISED -	REVISED -								



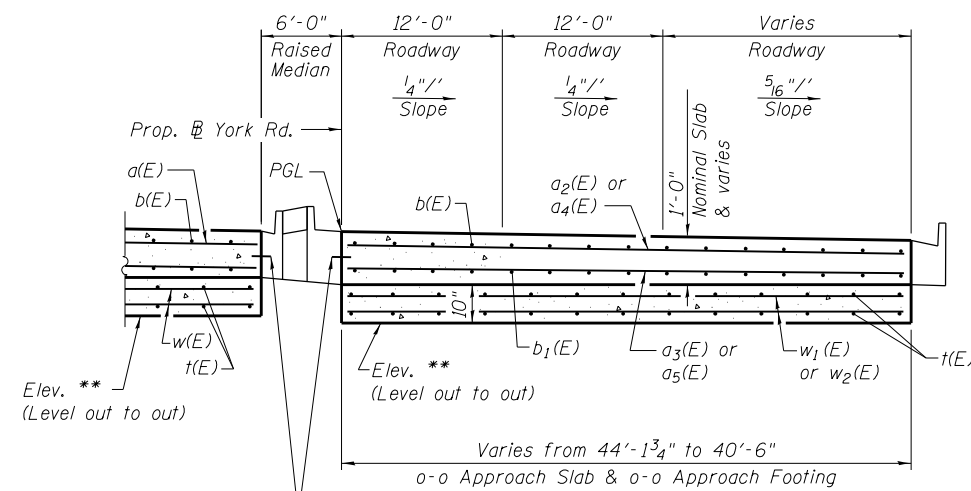
SECTION C-C

* Varies 8" to 1'-6" (1'-3" average)
 *** Cost included with Concrete Superstructure.



SECTION B-B

(See Plan for dimensions not shown)



SECTION D-D

(See Plan for dimensions not shown)

**SB & NB SLABS
 BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	34	#4	23'-8"	—
a1(E)	74	#5	23'-8"	—
a2(E)	34	#4	23'-0"	—
a3(E)	74	#5	23'-3"	—
a4(E)	34	#4	22'-0"	—
a5(E)	74	#5	22'-3"	—
a6(E)	34	#4	35'-8"	—
a7(E)	74	#5	35'-8"	—
b(E)	208	#6	19'-8"	—
b1(E)	205	#7	19'-8"	—
t(E)	208	#4	5'-8"	—
w(E)	48	#5	23'-8"	—
w1(E)	48	#5	23'-3"	—
w2(E)	48	#5	21'-5"	—
w3(E)	48	#5	35'-8"	—
Concrete Superstructure	Cu. Yd.		189.4	
Concrete Structures	Cu. Yd.		37.9	
Reinforcement Bars, Epoxy Coated	Pound		30,840	
Bar Splicers	Each		156	

Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

Notes:

1. Approach slab shall be paid for as Concrete Superstructure.
2. Approach footing concrete shall be paid for as Concrete Structures.
3. Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
4. The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
5. Cost of excavation for approach footing included with Concrete Structures.
6. Cost of thickening PCC pavement adjacent to Approach Slab as shown shall be included in the cost of PCC Pavement, 10 1/4" (Jointed). No extra compensation will be allowed.

Sta.	Elev. **
2018+43.72 (NB)	660.80
2018+83.72 (NB)	660.45
2018+43.72 (SB)	660.80
2018+83.72 (SB)	660.45



USER NAME = mikelly
 PLOT SCALE = 20.0000' / in.
 PLOT DATE = 12/19/2014

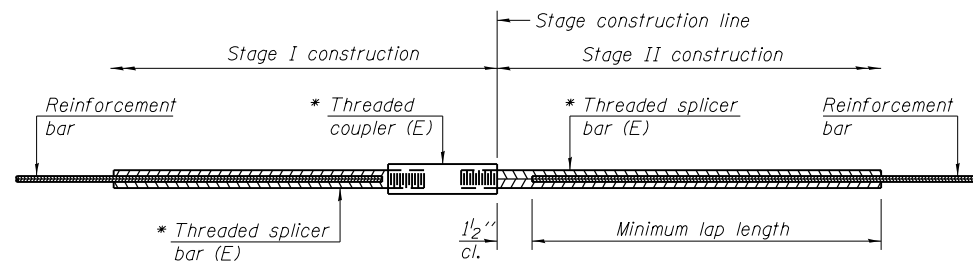
DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**YORK ROAD
 APPROACH SLAB DETAIL**
 SCALE: NTS SHEET NO. 10 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	379
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



STANDARD BAR SPLICER ASSEMBLY

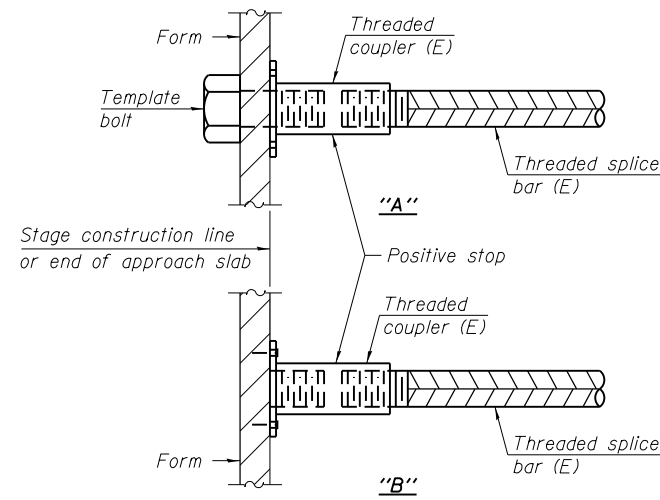
Bar size to be spliced	Minimum Lap Lengths					
	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
York Road Approach Slab	#4	34	Table 3
York Road Approach Slab	#5	122	Table 3



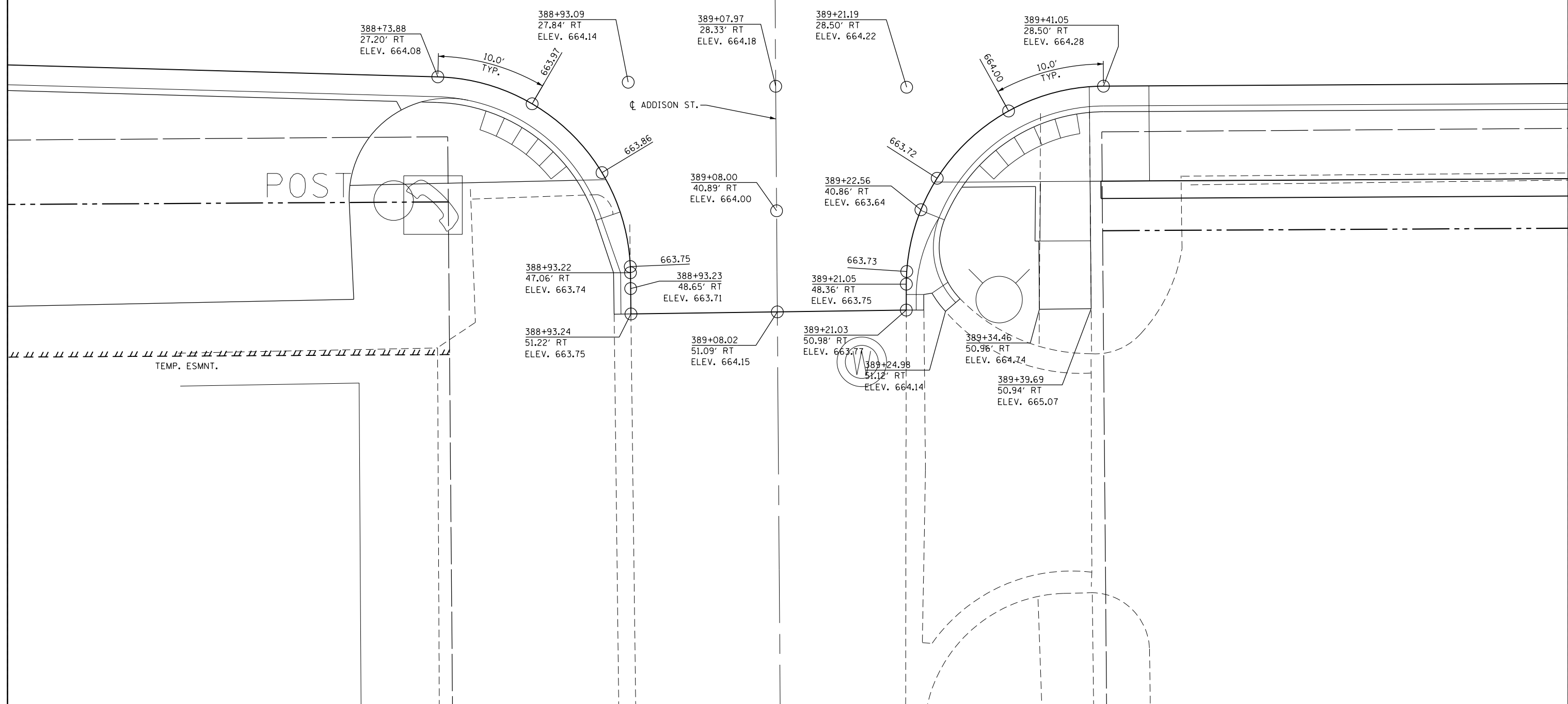
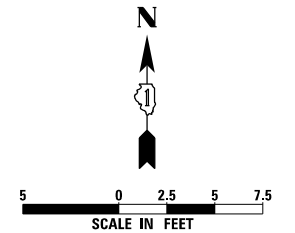
INSTALLATION AND SETTING METHODS

- "A" : Set bar splicer assembly by means of a template bolt.
- "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
- (E) : Indicates epoxy coating.

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

389



USER NAME = mikelly	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - RTB	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

DESIGNED - MPK	REVISED -
DRAWN - ML	REVISED -
CHECKED - RTB	REVISED -
DATE - 12-19-2014	REVISED -

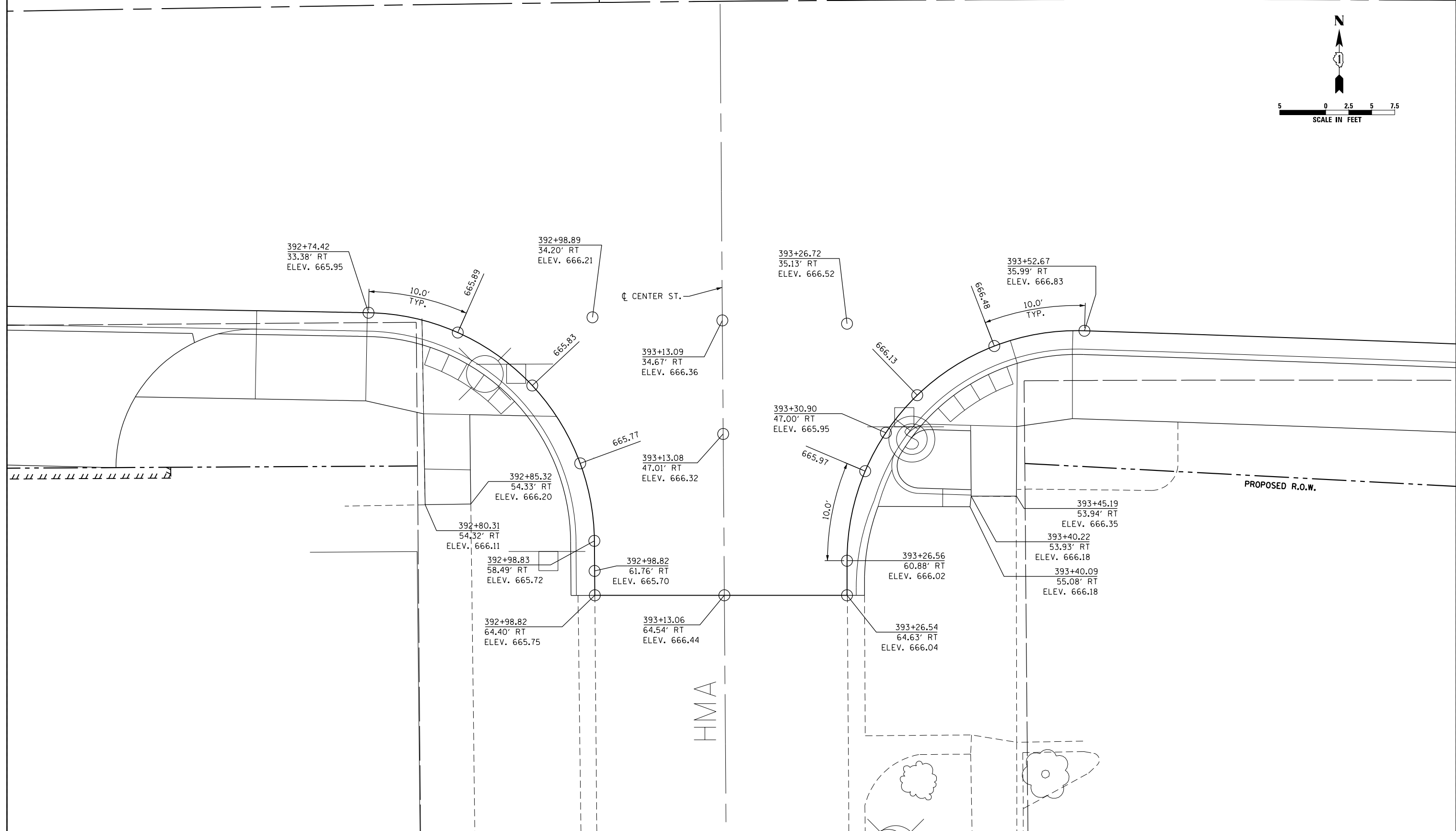
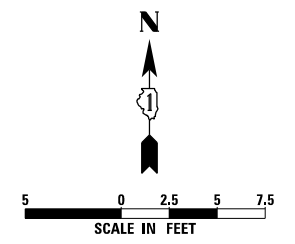
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 19/ADDISON
DRIVEWAY DETAIL**

SCALE: 1" = 5' SHEET NO. 12 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	381
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

393 50+0



USER NAME = mikelly	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - RTB	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

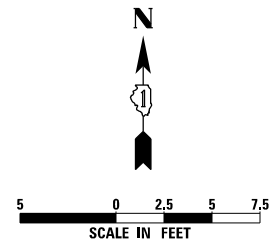
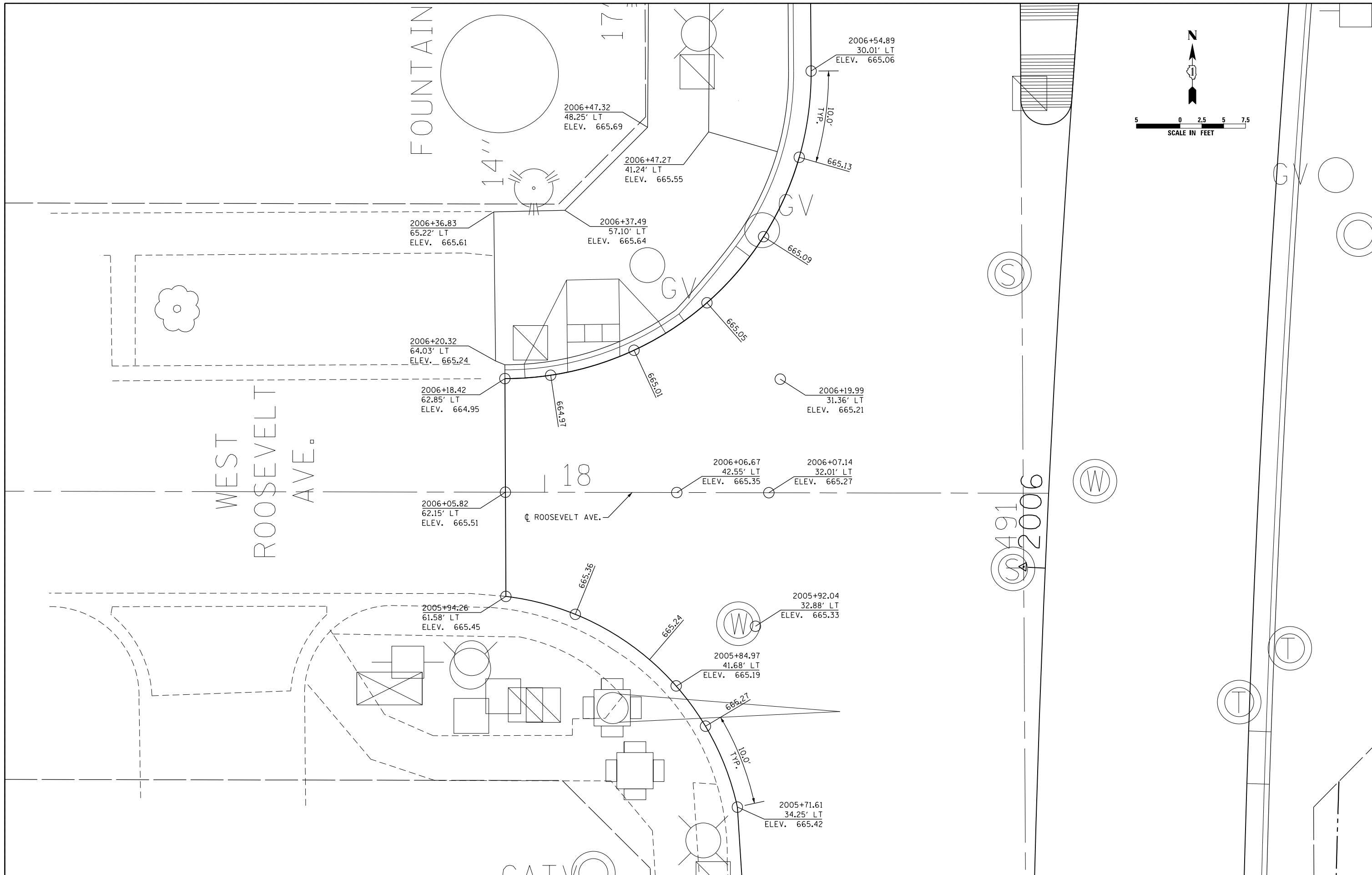
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DRAWN - ML	REVISED -
CHECKED - RTB	REVISED -
DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 19CENTER
DRIVEWAY DETAIL**

SCALE: 1" = 5' SHEET NO. 13 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	382
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



USER NAME = mikelly	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - RTB	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

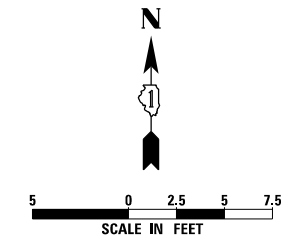
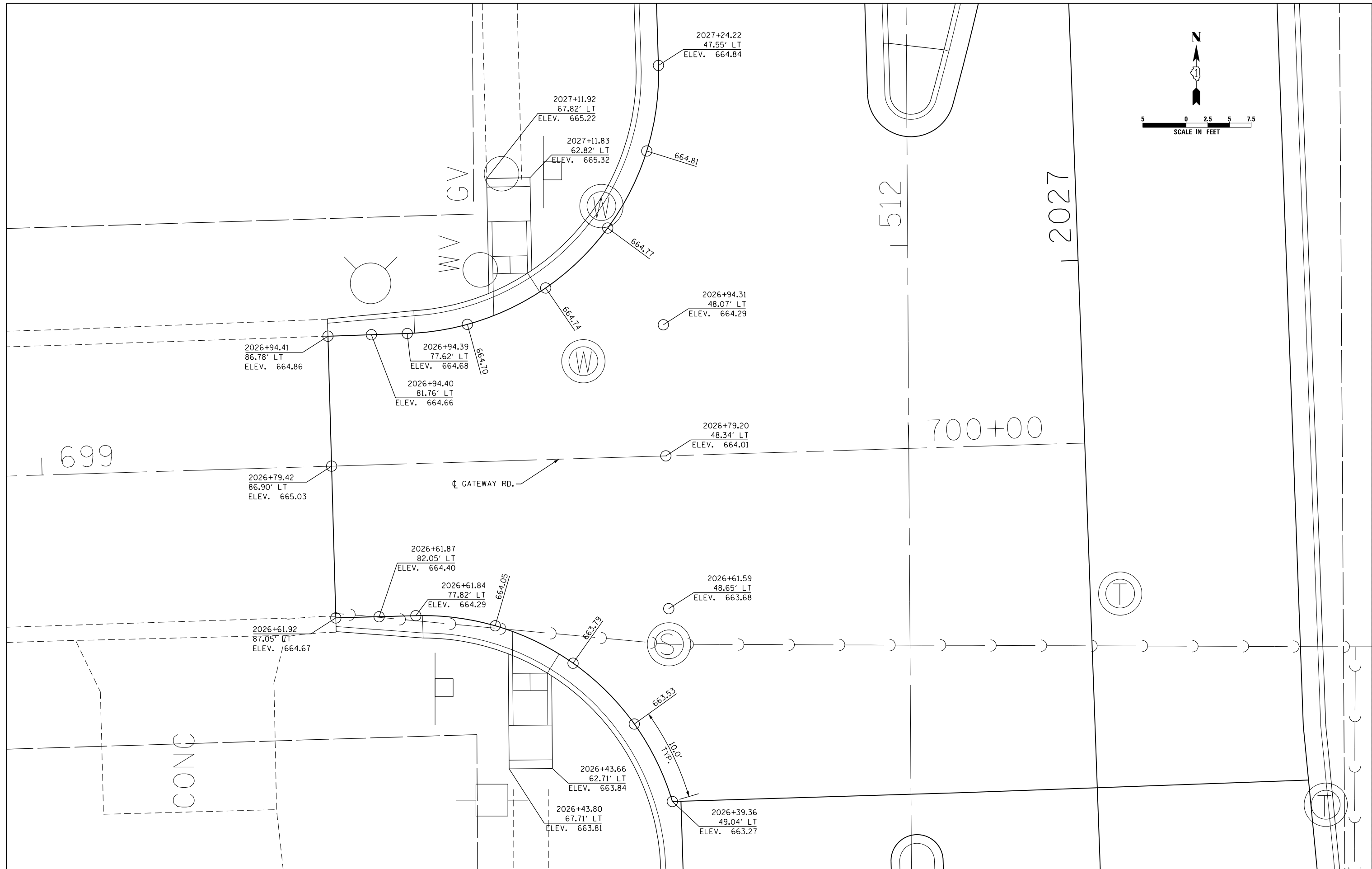
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DRAWN - ML	REVISED -
CHECKED - RTB	REVISED -
DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**YORK/ROOSEVELT
DRIVEWAY DETAIL**

SCALE: 1" = 5' SHEET NO. 14 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	383
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				



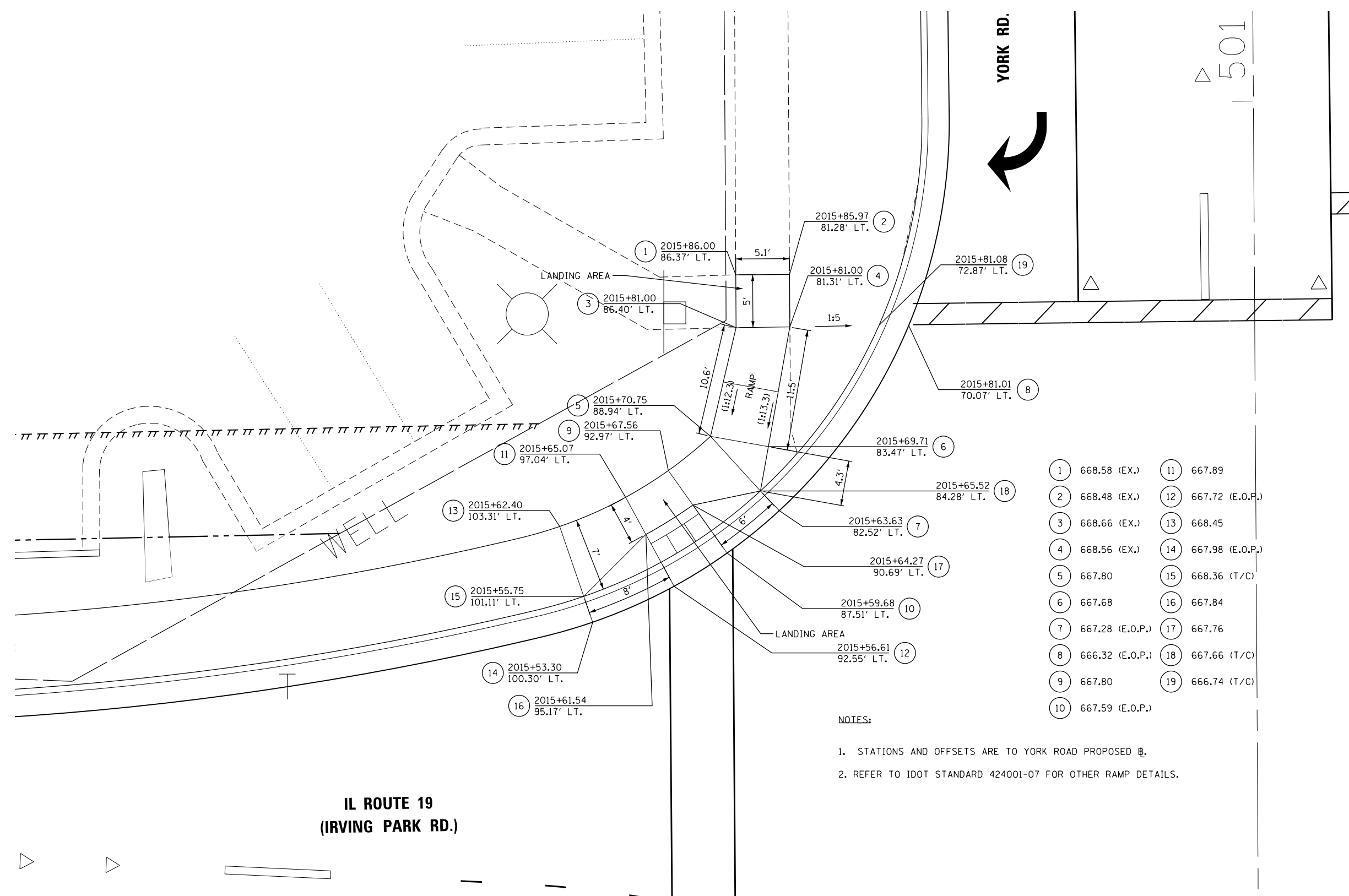
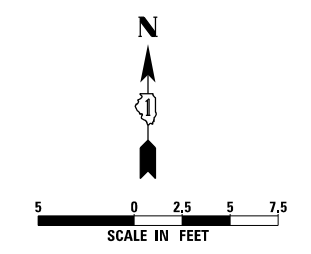
USER NAME = mikelly	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - RTB	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**YORKGATEWAY
DRIVEWAY DETAIL**

SCALE: 1" = 5' SHEET NO. 15 OF 18 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	384
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

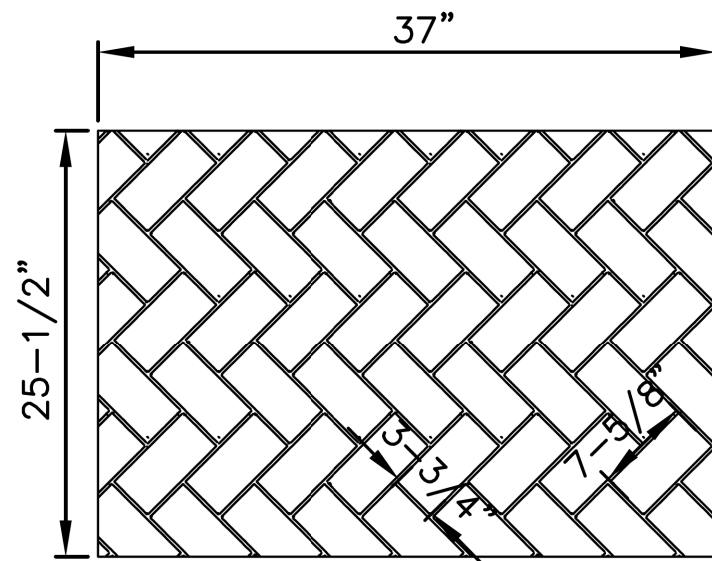


**IL ROUTE 19
(IRVING PARK RD.)**

1	668.58 (EX.)	11	667.89
2	668.48 (EX.)	12	667.72 (E.O.P.)
3	668.66 (EX.)	13	668.45
4	668.56 (EX.)	14	667.98 (E.O.P.)
5	667.80	15	668.36 (T/C)
6	667.68	16	667.84
7	667.28 (E.O.P.)	17	667.76
8	666.32 (E.O.P.)	18	667.66 (T/C)
9	667.80	19	666.74 (T/C)
10	667.59 (E.O.P.)		

NOTES:

1. STATIONS AND OFFSETS ARE TO YORK ROAD PROPOSED \mathbb{B} .
2. REFER TO IDOT STANDARD 424001-07 FOR OTHER RAMP DETAILS.



* GROUT LINES CAN VARY FROM 1/4" TO 1/2".

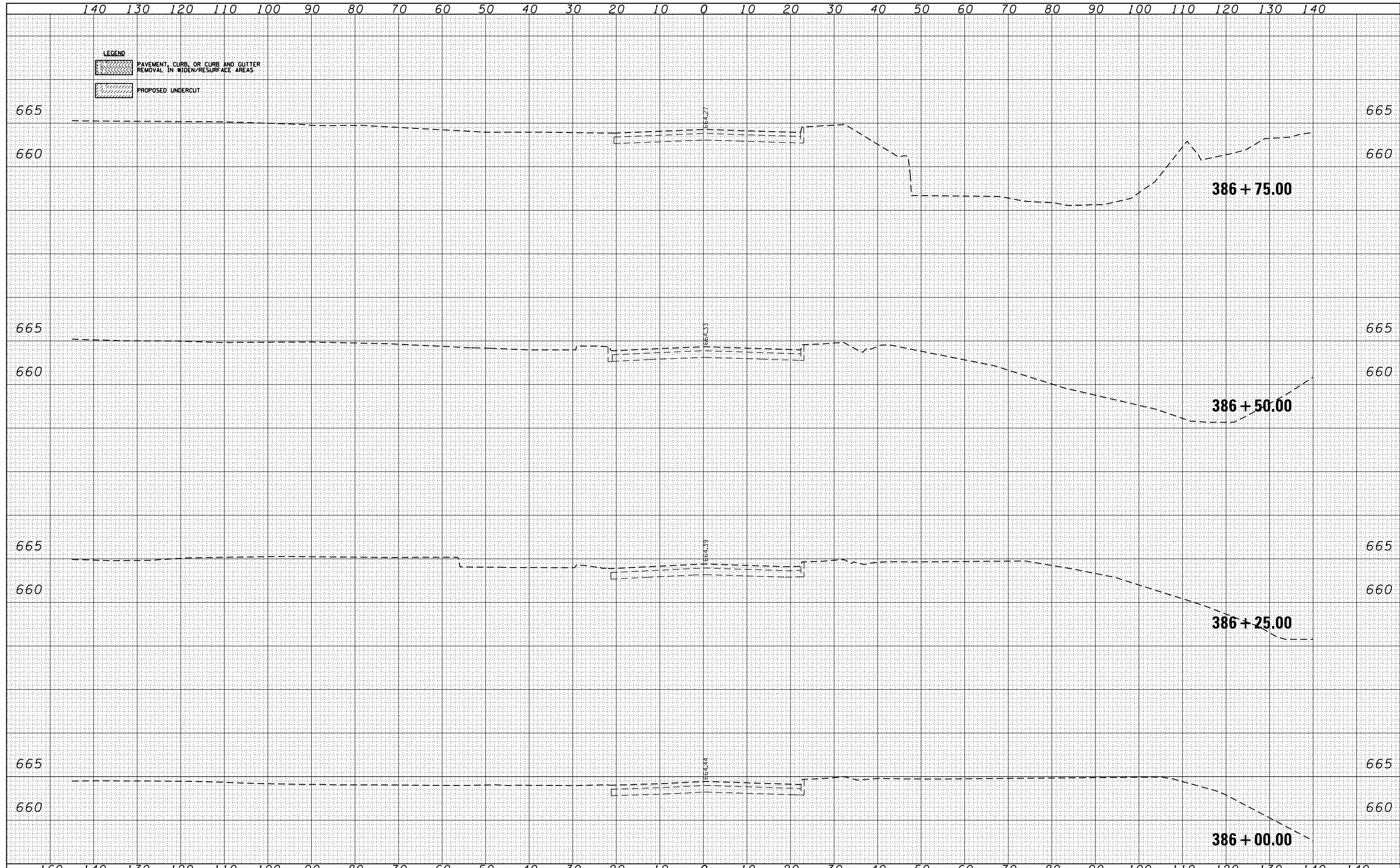
NOTES:

1. COLORED STAMPED CONCRETE SIDEWALK SHALL BE POURED MONOLITHICALLY AND STAMPED WITH BUTTERFIELD COLOR – NEW BRICK HERRINGBONE (BST-6000) PATTERN AS SHOWN IN THE DETAIL.
2. THE COLOR OF THE STAMPED CONCRETE SHALL BE BRICK RED.

	VILLAGE OF BENSENVILLE 12 S. CENTER STREET BENSENVILLE, IL 60106 WWW.BENSENVILLE.IL.US	STAMPED CONCRETE STANDARD	VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS	REVISIONS
			FILE LOCATION	
			G:\ENGINEERING\ STANDARDS\ NEW STANDARDS\ROADWAY	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



USER NAME = mikelly	DESIGNED - MPK	REVISED -
	DRAWN - ML	REVISED -
PLOT SCALE = 20.000000' / in.	CHECKED - LGP	REVISED -
PLOT DATE = 12/18/2014	DATE - 12-19-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

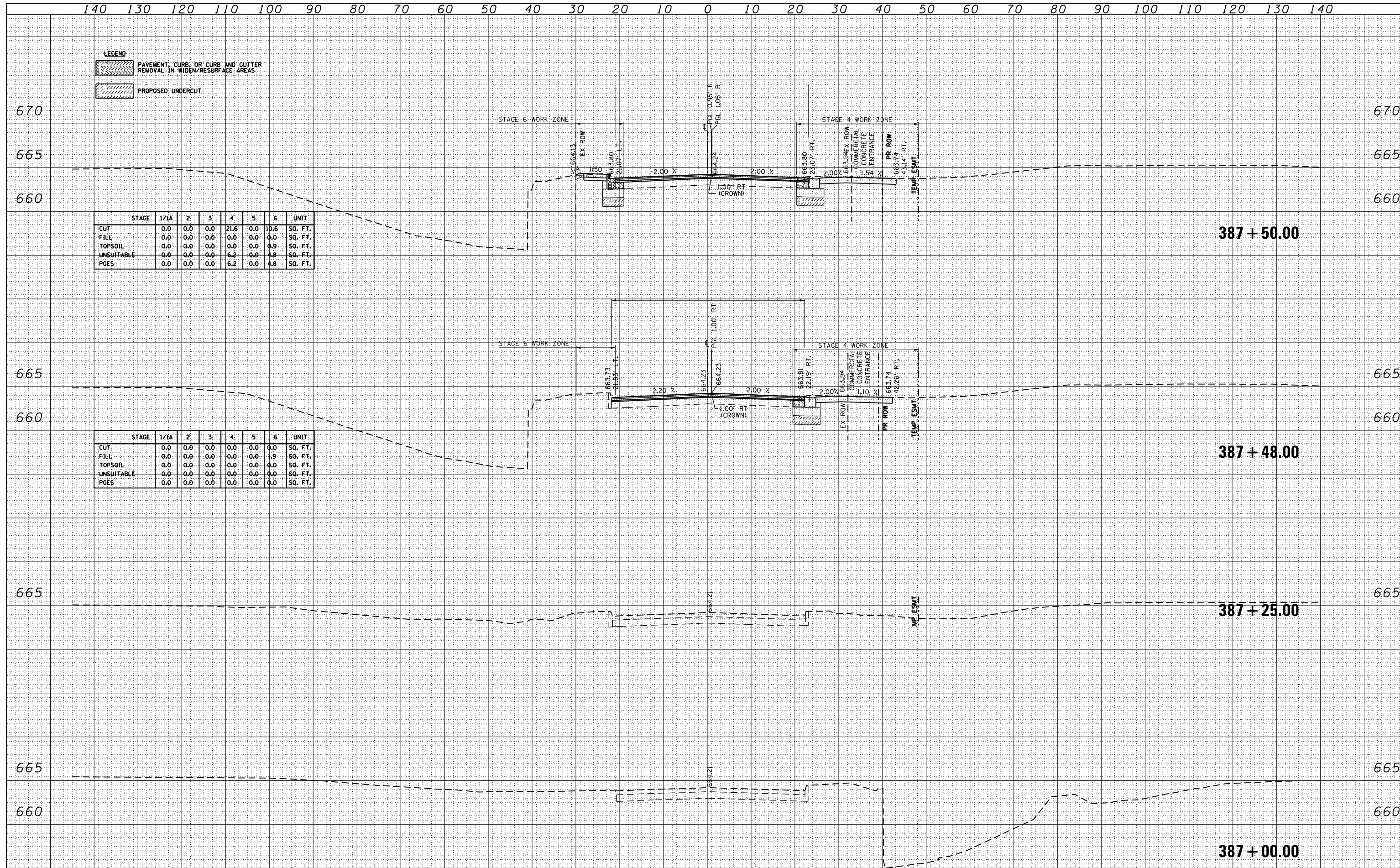
**IRVING PARK (IL 19)
CROSS SECTIONS**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 1 OF 39 SHEETS STA. 386+00.00 TO STA. 386+75.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	388
CONTRACT NO. 60B42			ILLINOIS FED. AID PROJECT	

DATE _____
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 PLOTTED _____
 TEMPLATE _____
 NOTE BOOK _____
 AREAS CHECKED _____

DATE _____
 BY _____
 SURVEYED _____
 PLOTTED _____
 TEMPLATE _____
 NOTE BOOK _____
 AREAS CHECKED _____



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	21.6	0.0	10.6	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.9	SO. FT.
UNSUITABLE	0.0	0.0	0.0	6.2	0.0	4.8	SO. FT.
PGES	0.0	0.0	0.0	6.2	0.0	4.8	SO. FT.

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	1.9	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
PGES	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.



USER NAME = mikelly
 PLOT SCALE = 20.000000' / in.
 PLOT DATE = 12/18/2014

DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS

SCALE: 1"=10'H; 1"=5'V SHEET NO. 2 OF 39 SHEETS STA. 387+00.00 TO STA. 387+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	389

CONTRACT NO. 60B42

ILLINOIS FED. AID PROJECT

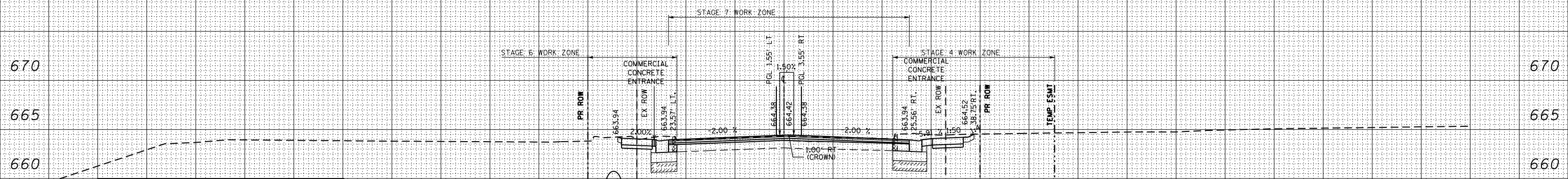
140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

LEGEND

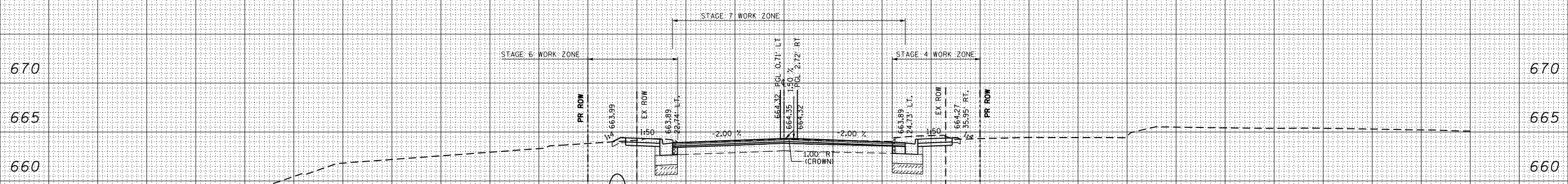
	PAVEMENT, CURB, OR CURB AND GUTTER REMOVAL IN WIDEN/RESURFACE AREAS.
	PROPOSED UNDERCUT

DATE	
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FINISHED SURVEY PLOTTED TEMPLATE	
NO. AREAS CHECKED	

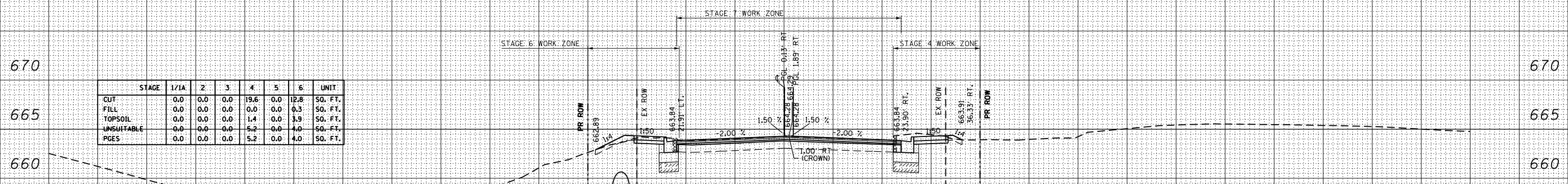
DATE	
BY	
ORIGINAL SURVEY PLOTTED TEMPLATE	
NO. AREAS CHECKED	



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	28.8	0.0	19.6	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	1.1	0.0	0.4	SO. FT.
UNSUITABLE	0.0	0.0	0.0	6.9	0.0	5.3	SO. FT.
PGES	0.0	0.0	0.0	6.9	0.0	5.3	SO. FT.



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	22.9	0.0	15.0	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.8	0.0	1.4	SO. FT.
UNSUITABLE	0.0	0.0	0.0	6.2	0.0	4.6	SO. FT.
PGES	0.0	0.0	0.0	6.2	0.0	4.6	SO. FT.



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	19.6	0.0	12.8	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.3	SO. FT.
TOPSOIL	0.0	0.0	0.0	1.4	0.0	3.9	SO. FT.
UNSUITABLE	0.0	0.0	0.0	5.2	0.0	4.0	SO. FT.
PGES	0.0	0.0	0.0	5.2	0.0	4.0	SO. FT.



USER NAME = mikelly
 PLOT SCALE = 20.000000' / in.
 PLOT DATE = 12/18/2014

DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**


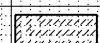
**IRVING PARK (IL 19)
 CROSS SECTIONS**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 3 OF 39 SHEETS STA. 387+75.00 TO STA. 388+25.00

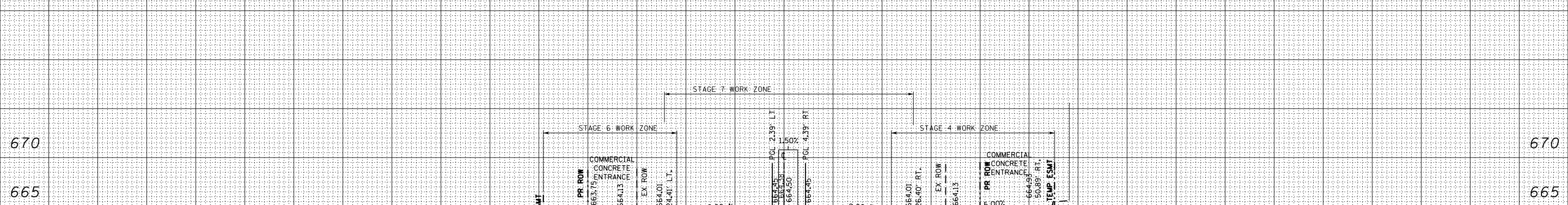
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	390
CONTRACT NO. 60B42				

ILLINOIS FED. AID PROJECT

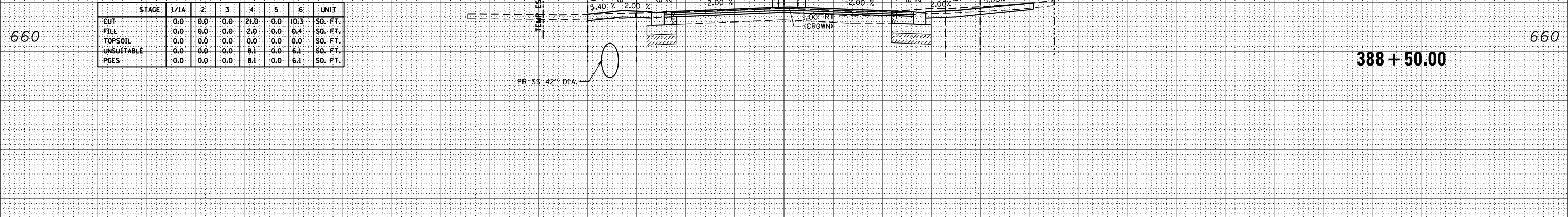
140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

LEGEND
 PAVEMENT, CURB, OR CURB AND GUTTER REMOVAL IN WIDEN/RESURFACE AREAS.
 PROPOSED UNDERCUT

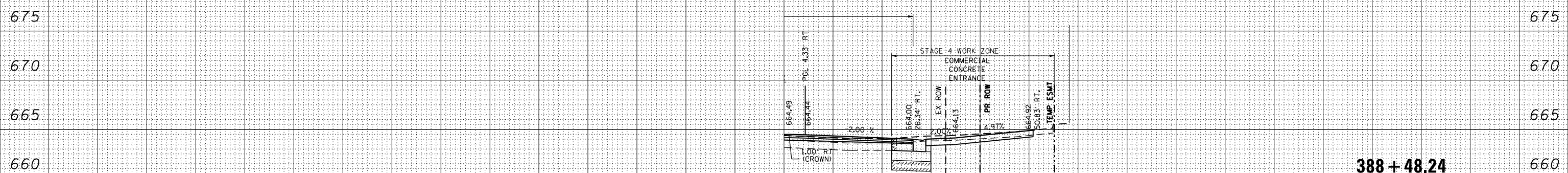
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 AREAS CHECKED _____
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STAGE	1/IA	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	21.0	0.0	10.3	SO. FT.
FILL	0.0	0.0	0.0	2.0	0.0	0.4	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSATURABLE PGES	0.0	0.0	0.0	8.1	0.0	6.1	SO. FT.
PGES	0.0	0.0	0.0	8.1	0.0	6.1	SO. FT.



DATE _____
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140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140



USER NAME = mikelly
 DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014
 PLOT SCALE = 20.000000' / in.
 PLOT DATE = 12/18/2014

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS
 SCALE: 1"=10'H; 1"=5'V
 SHEET NO. 4 OF 39 SHEETS
 STA. 388+48.24 TO STA. 388+51.71

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	391
CONTRACT NO. 60B42				

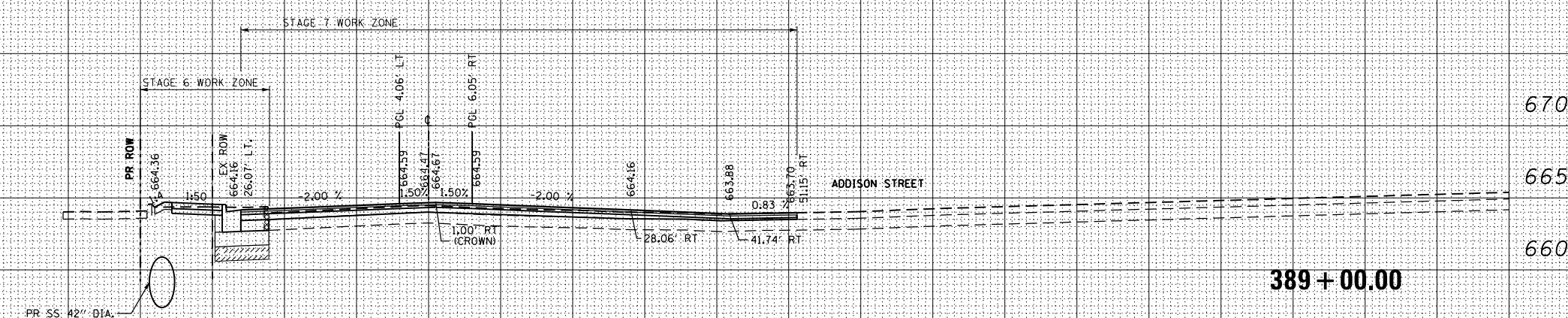
ILLINOIS FED. AID PROJECT

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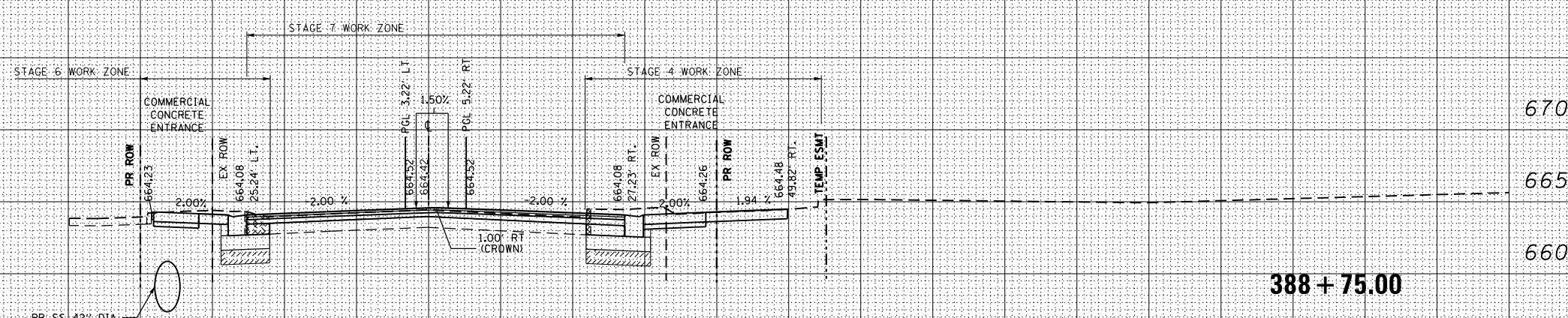
LEGEND

	PAVEMENT, CURB, OR CURB AND GUTTER REMOVAL IN WIDEN/RESURFACE AREAS.
	PROPOSED UNDERCUT

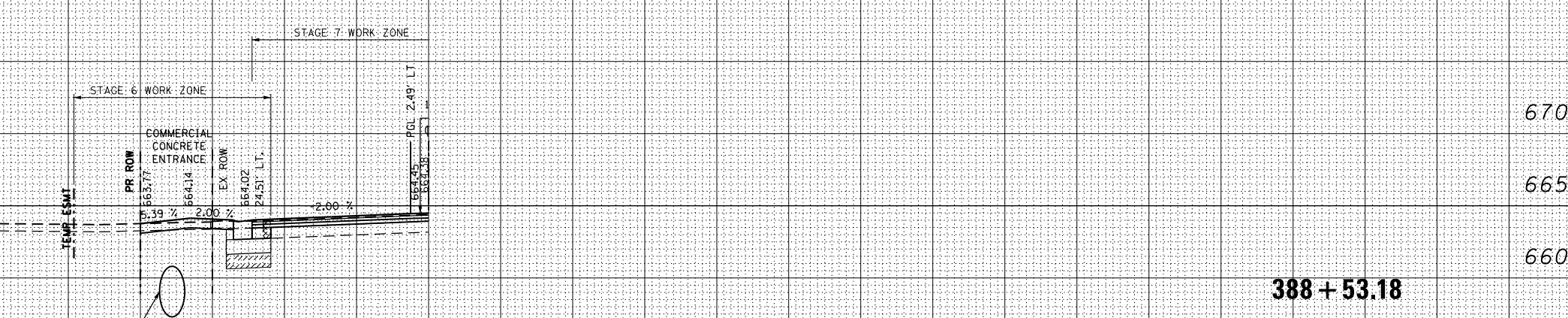
STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	0.0	0.0	22.7	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	1.2	SO. FT.
UNSUITABLE	0.0	0.0	0.0	0.0	0.0	7.5	SO. FT.
PGES	0.0	0.0	0.0	0.0	0.0	7.5	SO. FT.



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	40.6	0.0	21.9	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.1	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	8.9	0.0	6.8	SO. FT.
PGES	0.0	0.0	0.0	8.9	0.0	6.8	SO. FT.



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
PGES	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.



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USER NAME = mikelly
 DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS

SCALE: 1"=10'H; 1"=5'V SHEET NO. 5 OF 39 SHEETS STA. 388+53.18 TO STA. 389+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	392
CONTRACT NO. 60B42				

ILLINOIS FED. AID PROJECT

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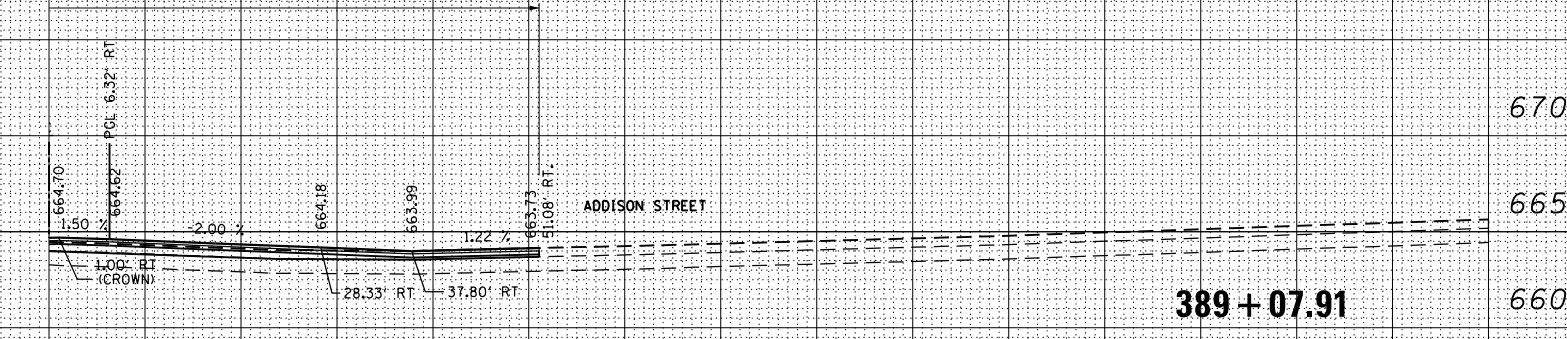
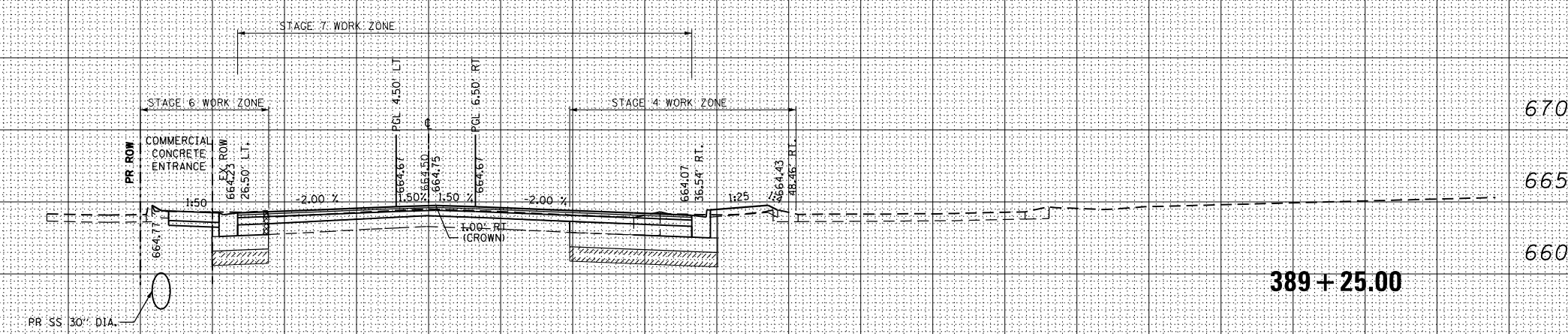
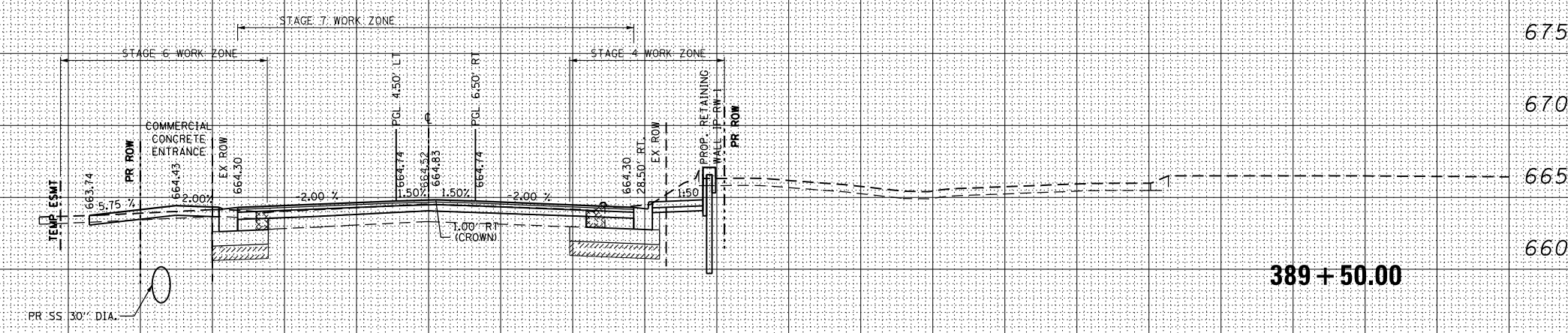
140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

LEGEND

	PAVEMENT, CURB, OR CURB AND GUTTER REMOVAL IN WIDEN/RESURFACE AREAS
	PROPOSED UNDERCUT

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	35.5	0.0	14.1	SO. FT.
FILL	0.0	0.0	0.0	2.8	0.0	1.2	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	10.2	0.0	7.7	SO. FT.
PGES	0.0	0.0	0.0	10.2	0.0	7.7	SO. FT.

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	33.6	0.0	26.7	SO. FT.
FILL	0.0	0.0	0.0	2.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	4.7	0.0	1.2	SO. FT.
UNSUITABLE	0.0	0.0	0.0	11.6	0.0	7.8	SO. FT.
PGES	0.0	0.0	0.0	11.6	0.0	7.8	SO. FT.



USER NAME = mikelly
 PLOT SCALE = 20.000000' / in.
 PLOT DATE = 12/18/2014

DESIGNED - MPK
 DRAWN - ML
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 DATE - 12-19-2014

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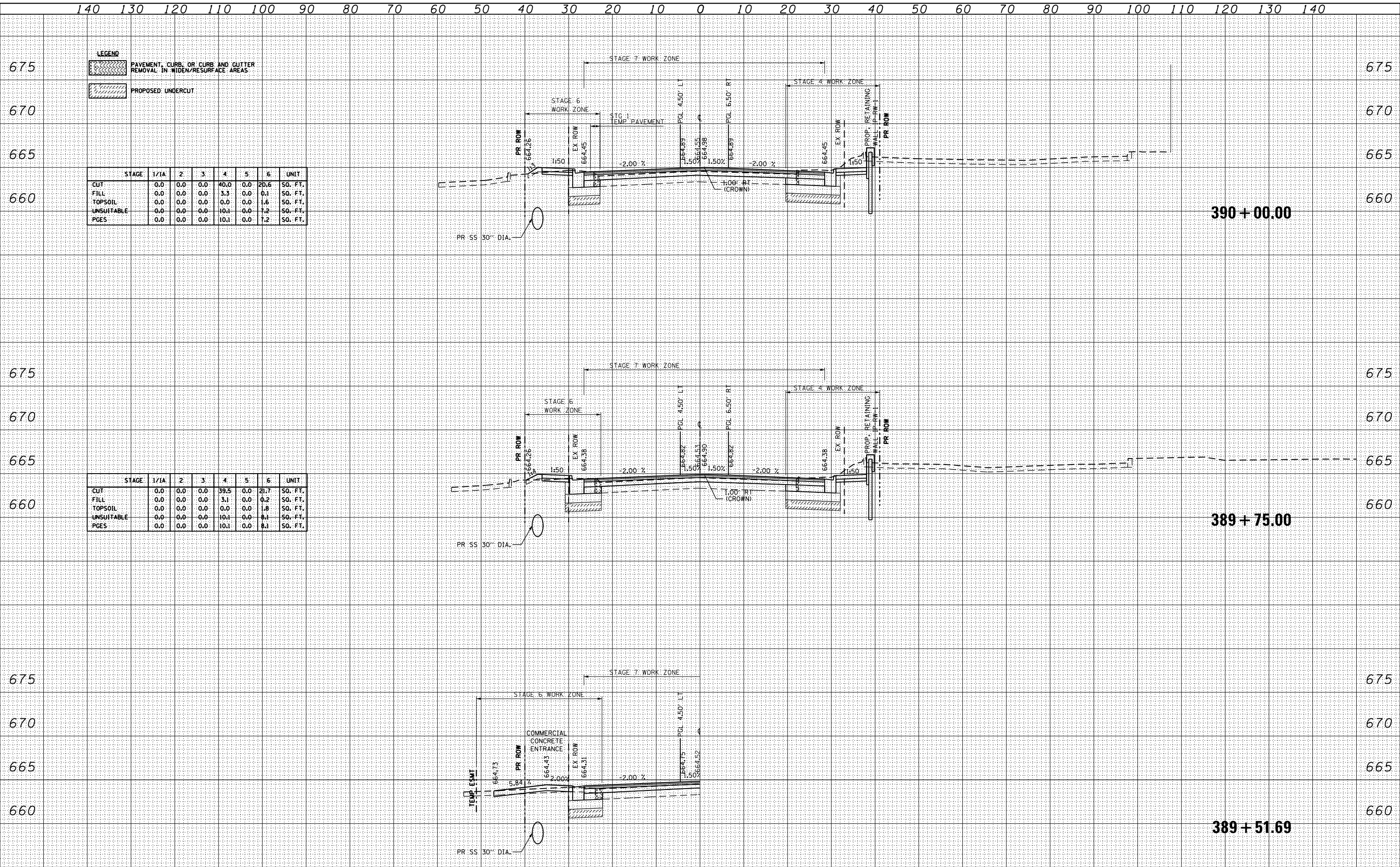
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS
 SCALE: 1"=10'H; 1"=5'V SHEET NO. 6 OF 39 SHEETS STA. 389+07.91 TO STA. 389+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	393
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				

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STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	40.0	0.0	20.6	SO. FT.
FILL	0.0	0.0	0.0	3.3	0.0	0.1	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	1.6	SO. FT.
UNSATURABLE	0.0	0.0	0.0	10.1	0.0	7.2	SO. FT.
PGES	0.0	0.0	0.0	10.1	0.0	7.2	SO. FT.

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	39.5	0.0	21.7	SO. FT.
FILL	0.0	0.0	0.0	3.1	0.0	0.2	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	1.8	SO. FT.
UNSATURABLE	0.0	0.0	0.0	10.1	0.0	8.1	SO. FT.
PGES	0.0	0.0	0.0	10.1	0.0	8.1	SO. FT.



USER NAME = mikelly
 DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS

SCALE: 1"=10'H; 1"=5'V SHEET NO. 7 OF 39 SHEETS STA. 389+51.69 TO STA. 390+00.00

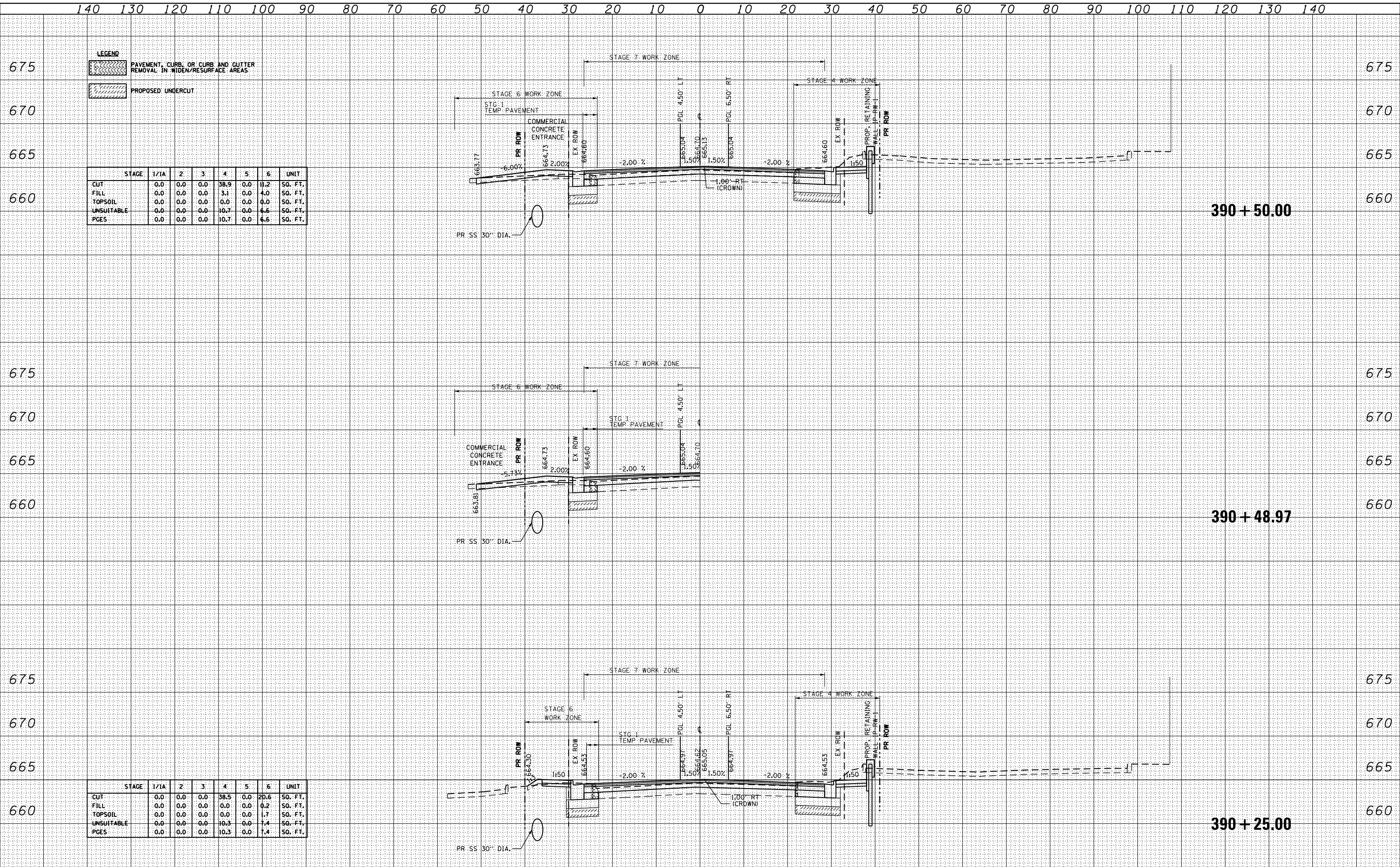
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32R5&N (12)	DUPAGE	495	394

CONTRACT NO. 60B42

ILLINOIS FED. AID PROJECT

DATE _____
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 AREAS CHECKED _____



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	38.9	0.0	11.2	SO. FT.
FILL	0.0	0.0	0.0	3.1	0.0	4.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	10.7	0.0	6.6	SO. FT.
PGES	0.0	0.0	0.0	10.7	0.0	6.6	SO. FT.

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	38.5	0.0	20.6	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.2	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	1.7	SO. FT.
UNSUITABLE	0.0	0.0	0.0	10.3	0.0	7.4	SO. FT.
PGES	0.0	0.0	0.0	10.3	0.0	7.4	SO. FT.



USER NAME = mikelly
 PLOT SCALE = 20.000000' / in.
 PLOT DATE = 12/18/2014

DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS

SCALE: 1"=10'H; 1"=5'V SHEET NO. 8 OF 39 SHEETS STA. 390+25.00 TO STA. 390+50.00

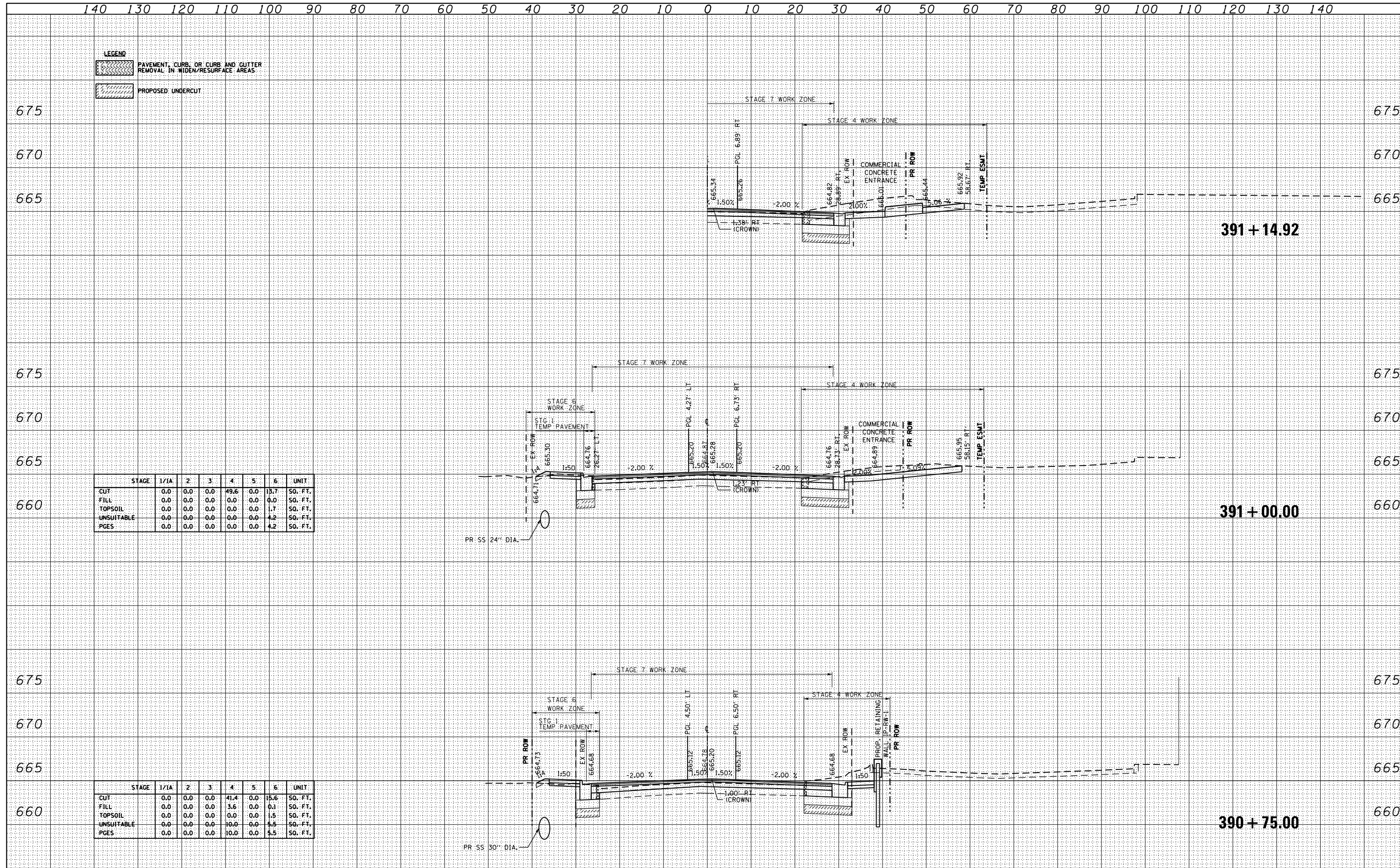
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	395

CONTRACT NO. 60B42

ILLINOIS FED. AID PROJECT

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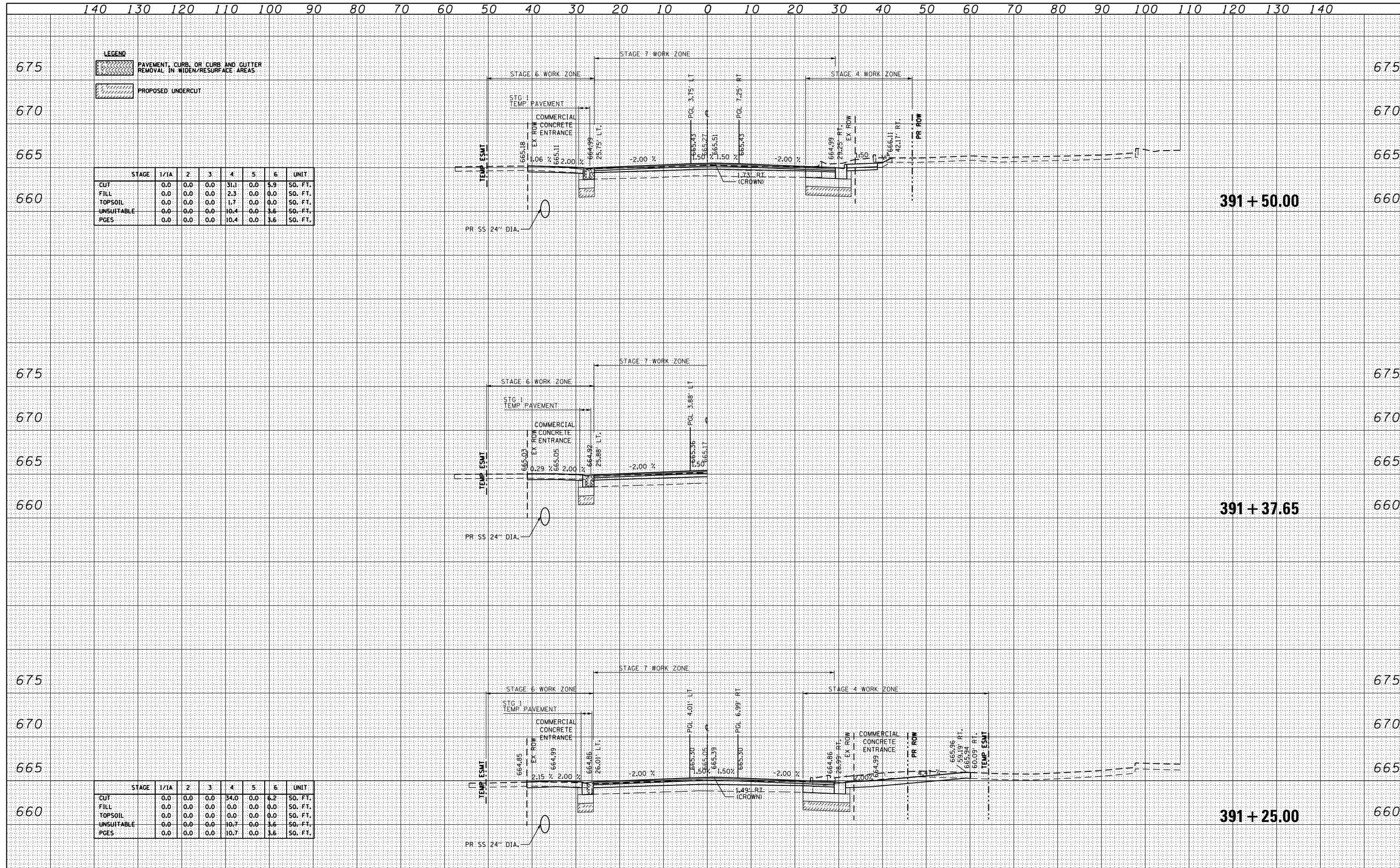


STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	49.6	0.0	15.7	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	1.7	SO. FT.
UNSATURABLE	0.0	0.0	0.0	0.0	0.0	4.2	SO. FT.
PGES	0.0	0.0	0.0	0.0	0.0	4.2	SO. FT.

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	41.4	0.0	15.6	SO. FT.
FILL	0.0	0.0	0.0	3.6	0.0	0.1	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	1.5	SO. FT.
UNSATURABLE	0.0	0.0	0.0	10.0	0.0	5.5	SO. FT.
PGES	0.0	0.0	0.0	10.0	0.0	5.5	SO. FT.

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
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ORIGINAL SURVEY	
SURVEYED	
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TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	31.1	0.0	5.9	SO. FT.
FILL	0.0	0.0	0.0	2.3	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	1.7	0.0	0.0	SO. FT.
UNSATURABLE	0.0	0.0	0.0	10.4	0.0	3.6	SO. FT.
PGES	0.0	0.0	0.0	10.4	0.0	3.6	SO. FT.

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	34.0	0.0	6.2	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSATURABLE	0.0	0.0	0.0	10.7	0.0	3.6	SO. FT.
PGES	0.0	0.0	0.0	10.7	0.0	3.6	SO. FT.



USER NAME = mikelly
 DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

REVISIED -
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 REVISIED -
 REVISIED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IRVING PARK (IL 19)
 CROSS SECTIONS

SCALE: 1"=10'H; 1"=5'V SHEET NO. 10 OF 39 SHEETS STA. 391+25.00 TO STA. 391+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	397
CONTRACT NO. 60B42				

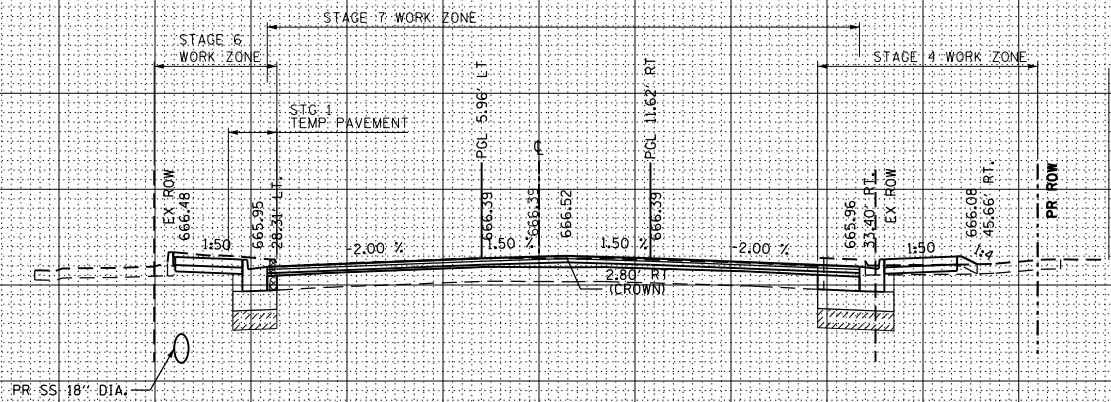
ILLINOIS FED. AID PROJECT

140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

LEGEND

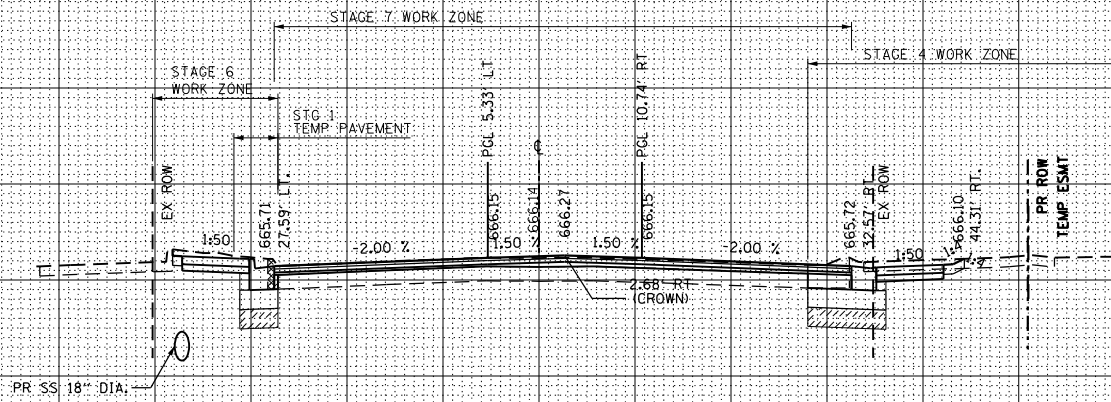
	PAVEMENT, CURB, OR CURB AND GUTTER REMOVAL IN WIDEN/RESURFACE AREAS
	PROPOSED UNDERCUT

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	17.6	0.0	17.0	SO. FT.
FILL	0.0	0.0	0.0	4.5	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	1.1	0.0	0.1	SO. FT.
UNSUITABLE	0.0	0.0	0.0	8.0	0.0	4.6	SO. FT.
PGES	0.0	0.0	0.0	8.0	0.0	4.6	SO. FT.

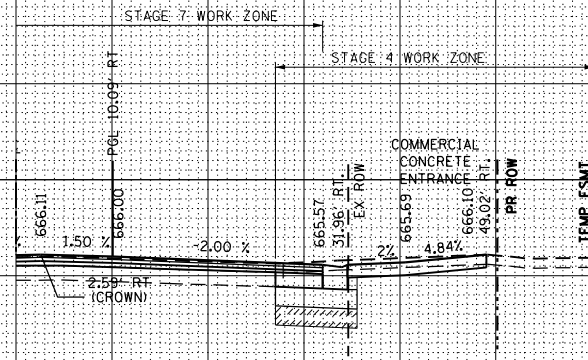


392 + 75.00

STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	16.6	0.0	15.4	SO. FT.
FILL	0.0	0.0	0.0	3.3	0.0	0.0	SO. FT.
TOPSOIL	0.0	0.0	0.0	1.1	0.0	0.5	SO. FT.
UNSUITABLE	0.0	0.0	0.0	8.1	0.0	4.0	SO. FT.
PGES	0.0	0.0	0.0	8.1	0.0	4.0	SO. FT.



392 + 50.00



392 + 31.72

140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME = mikelly
 PLOT SCALE = 20.000000' / in.
 PLOT DATE = 12/18/2014

DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

REVISED -
 REVISED -
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 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**


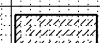
**IRVING PARK (IL 19)
 CROSS SECTIONS**

SCALE: 1"=10'H; 1/4"=5'V SHEET NO. 12 OF 39 SHEETS STA. 392+31.72 TO STA. 392+75.00

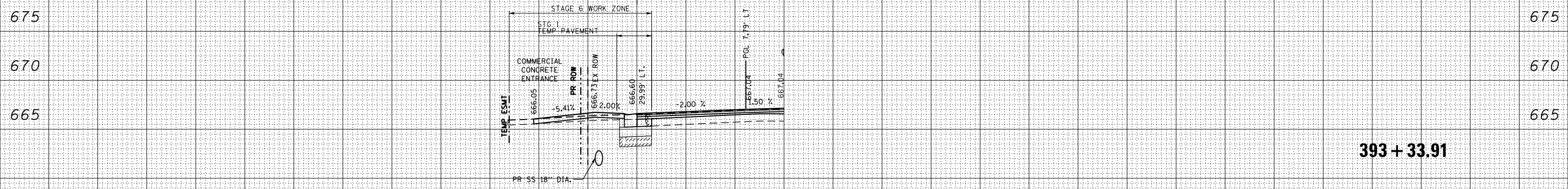
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	399
CONTRACT NO. 60B42				

ILLINOIS FED. AID PROJECT

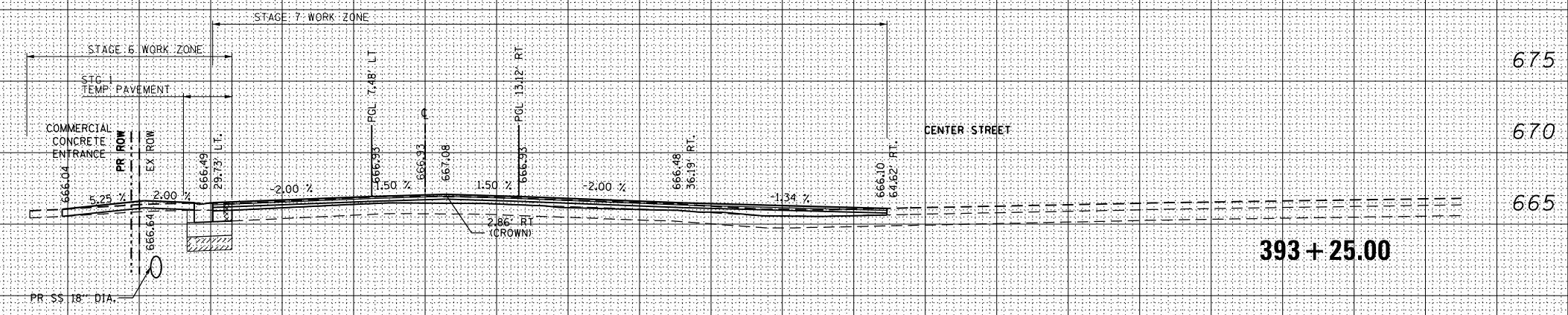
140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140

LEGEND
 PAVEMENT, CURB, OR CURB AND GUTTER REMOVAL IN WIDEN/RESURFACE AREAS
 PROPOSED UNDERCUT

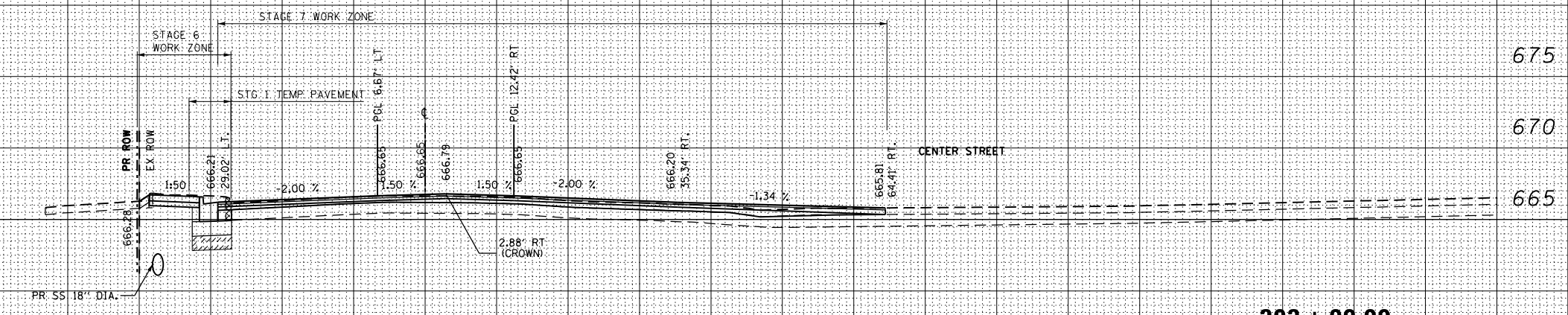
DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	0.0	0.0	10.5	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	2.4	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.0	SO. FT.
UNSUITABLE	0.0	0.0	0.0	0.0	0.0	4.3	SO. FT.
PGES	0.0	0.0	0.0	0.0	0.0	4.3	SO. FT.



STAGE	1/1A	2	3	4	5	6	UNIT
CUT	0.0	0.0	0.0	0.0	0.0	18.4	SO. FT.
FILL	0.0	0.0	0.0	0.0	0.0	0.5	SO. FT.
TOPSOIL	0.0	0.0	0.0	0.0	0.0	0.7	SO. FT.
UNSUITABLE	0.0	0.0	0.0	0.0	0.0	5.4	SO. FT.
PGES	0.0	0.0	0.0	0.0	0.0	5.4	SO. FT.



DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME = mikelly
 PLOT SCALE = 20.000000' / in.
 PLOT DATE = 12/18/2014

DESIGNED - MPK
 DRAWN - ML
 CHECKED - LGP
 DATE - 12-19-2014

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IRVING PARK (IL 19)
 CROSS SECTIONS**

SCALE: 1"=10'H; 1"=5'V SHEET NO. 13 OF 39 SHEETS STA. 393+00.00 TO STA. 393+33.91

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	32RS&N (12)	DUPAGE	495	400
CONTRACT NO. 60B42				
ILLINOIS FED. AID PROJECT				