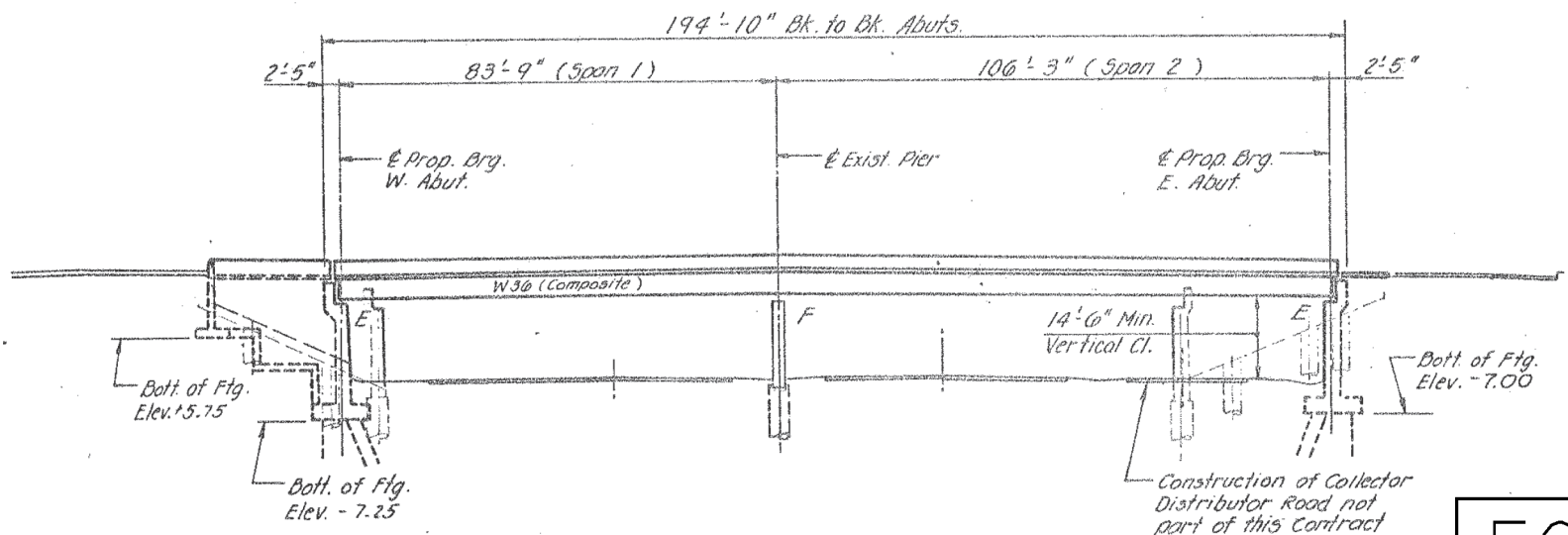


PLAN



ELEVATION

FOR INFORMATION ONLY

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE PLAN SN 016-0478 ROOSEVELT RD OVER I-9094 DAN RYAN	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwwork\pwwork\osbornnp\0311016\015	612-sh-t-schedule.dgn	DRAWN -	REVISED -			2013-012R	COOK	385	301	
Default	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -			CONTRACT NO. 60W30				
	PLOT DATE = 7/10/2014	DATE -	REVISED -			SCALE:	SHEET 1 OF 4 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT

GENERAL NOTES

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT AND PRESERVE THE EXISTING BRIDGE LIGHTING AT ANY LOCATIONS THAT LIGHTING IS ENCOUNTERED ADJACENT TO AN AREA TO BE CLEANED AND PAINTED

TREE CLEARING FOR PAINTING ACCESS SHALL BE DONE ACCORDING TO ARTICLE 201.10

GENERAL PAINT NOTES

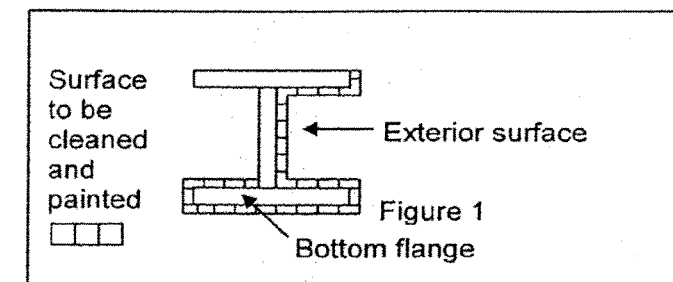
CLEANING AND PAINTING OF THE EXISTING STRUCTURAL STEEL SHALL BE AS SPECIFIED IN THE SPECIAL PROVISION FOR "CLEANING AND PAINTING EXISTING STEEL STRUCTURES".

A MINIMUM OF ONE (1) AIR MONITOR WILL BE REQUIRED TO MONITOR ABRASIVE BLASTING OPERATIONS. SEE SPECIAL PROVISION FOR "CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUE."

THE AREAS DESIGNATED TO BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10 SHALL BE PAINTED ACCORDING TO THE REQUIREMENTS OF PAINT SYSTEM 1 - OZ/E/U. THE COLOR OF THE FINAL FINISH COAT FOR ALL INTERIOR STEEL SURFACES SHALL BE GRAY, MUNSELL NO 5B 7/11. THE COLOR OF THE FINAL FINISH COAT FOR THE EXTERIOR AND BOTTOM FLANGE OF THE FASCIA BEAMS SHALL BE REDDISH BROWN, MUNSELL NO 2.5YR 3/4.

ALL ITEMS (SUCH AS, BUT NOT LIMITED TO" CONDUITS, BRACKETS AND DECK DRAINS) ATTACHED TO THE OUTSIDE OF THE FASCIA BEAMS SHOULD BE CLEANED AND PAINTED.

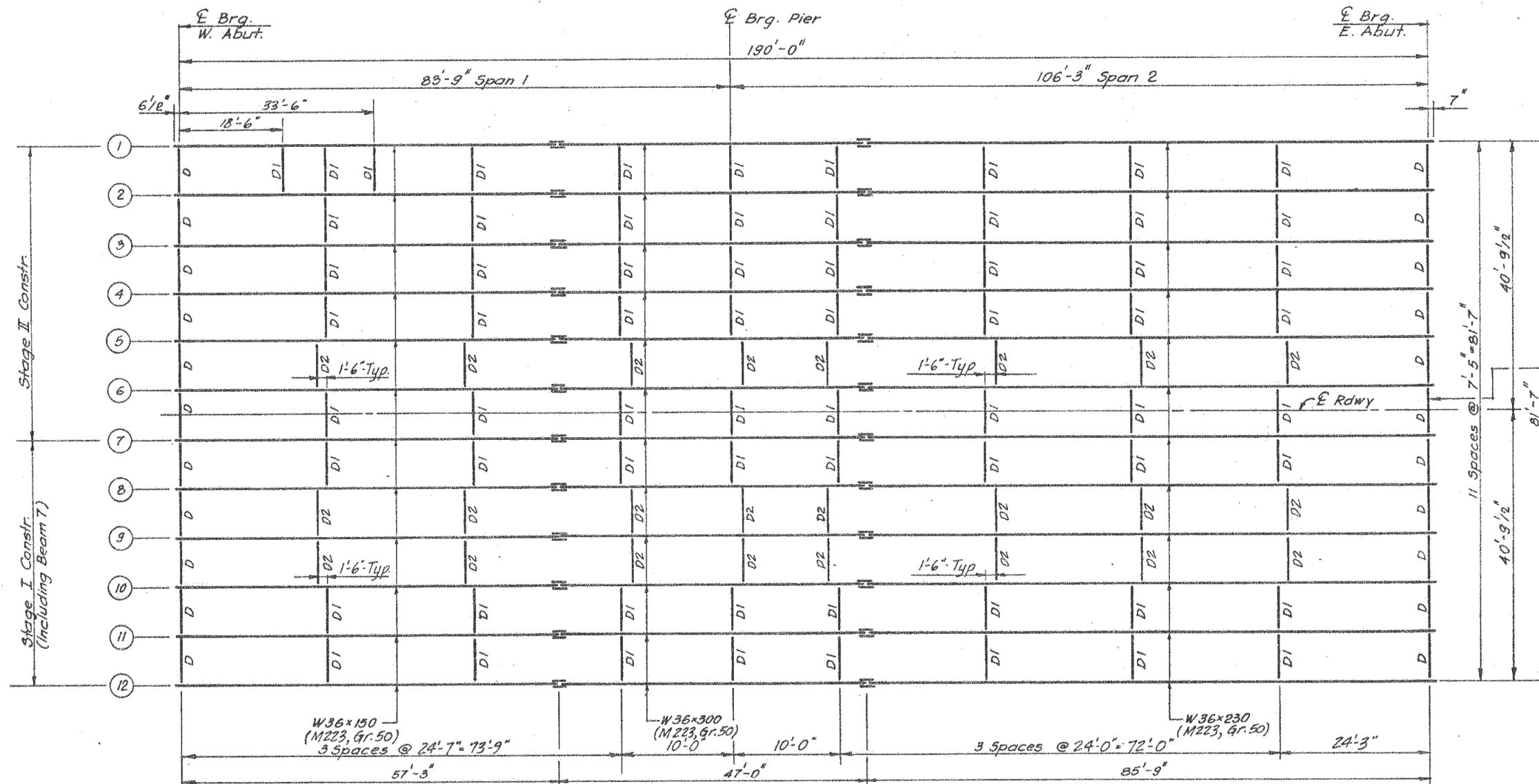
ALL BEAMS, BEARINGS AND OTHER STRUCTURAL STEEL WITHIN FIVE (5) FT., (MEASURED ALONG THE BEAM) OF EITHER SIDE OF ALL DECK JOINTS, SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING SSPC-SP10. THE EXTERIOR SURFACES AND TOP AND BOTTOM OF THE BOTTOM FLANGE OF THE FASCIA BEAMS (SEE FIGURE 1) SHALL BE CLEANED PER NEAR WHITE BLASK CLEANING SSPC-SP10.



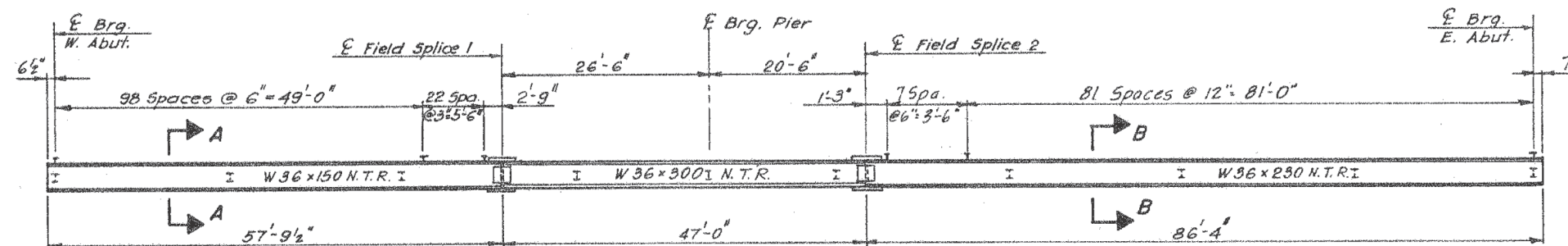
**FASCIA BEAM PAINTING DETAIL
FIGURE 1**

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES	LSUM	0.5
CLEANING AND PAINTING STEEL BRIDGE NO. 1	LSUM	1



FRAMING PLAN



ELEVATION

DIAPHRAGM

D = W12 X 40

FOR INFORMATION ONLY

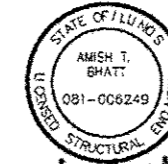
FILE NAME :	USER NAME : osbornnp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE PLAN SN 016-0478 ROOSEVELT RD OVER I-90/94 DAN RYAN	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -					2013-012R	COOK	385	303
		CHECKED -	REVISED -					CONTRACT NO. 60W30			
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT			
Default	PLOT DATE : 7/10/2014					SCALE:	SHEET 3	OF 4 SHEETS	STA.	TO STA.	

Benchmark: Cut square on jaywall at north line of Taylor Street, approximately 100' west of Des Plaines Street and 3' above walk (opposite traffic signal). Elevation 596.30.

Existing Structure: Originally built in 1987. Southwest Retaining Wall is a cast-in-place cantilever type wall on a spread footing foundation. Footing of wall is 1'-6" deep and of variable width. Stem is 10 1/2" wide and of variable height.

Traffic Control: Traffic to be maintained on SB I-90/94 and Union Ave. during construction.

Salvage: No salvage.

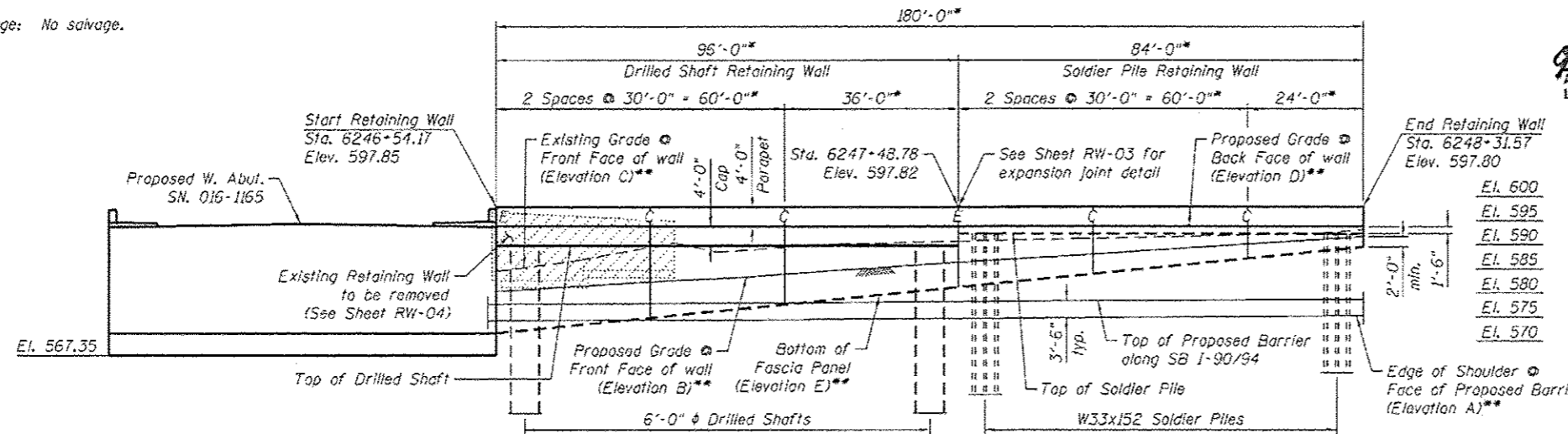


Amish T. Bhatt 12/16/14
 AMISH T. BHATT DATE
 LICENSE EXPIRES 11/30/2016

DESIGN SPECIFICATIONS
 2012 AASHTO LRFD Bridge Design Specifications
 6th Edition with 2013 Interim Revisions

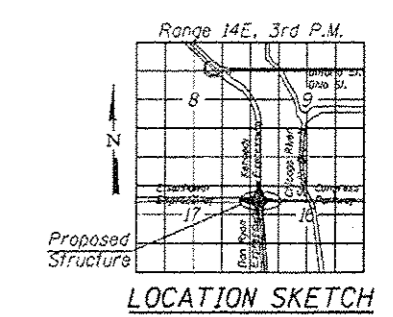
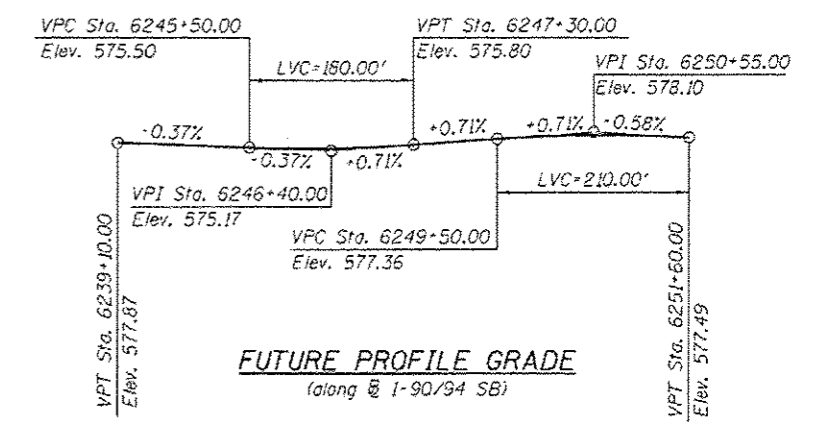
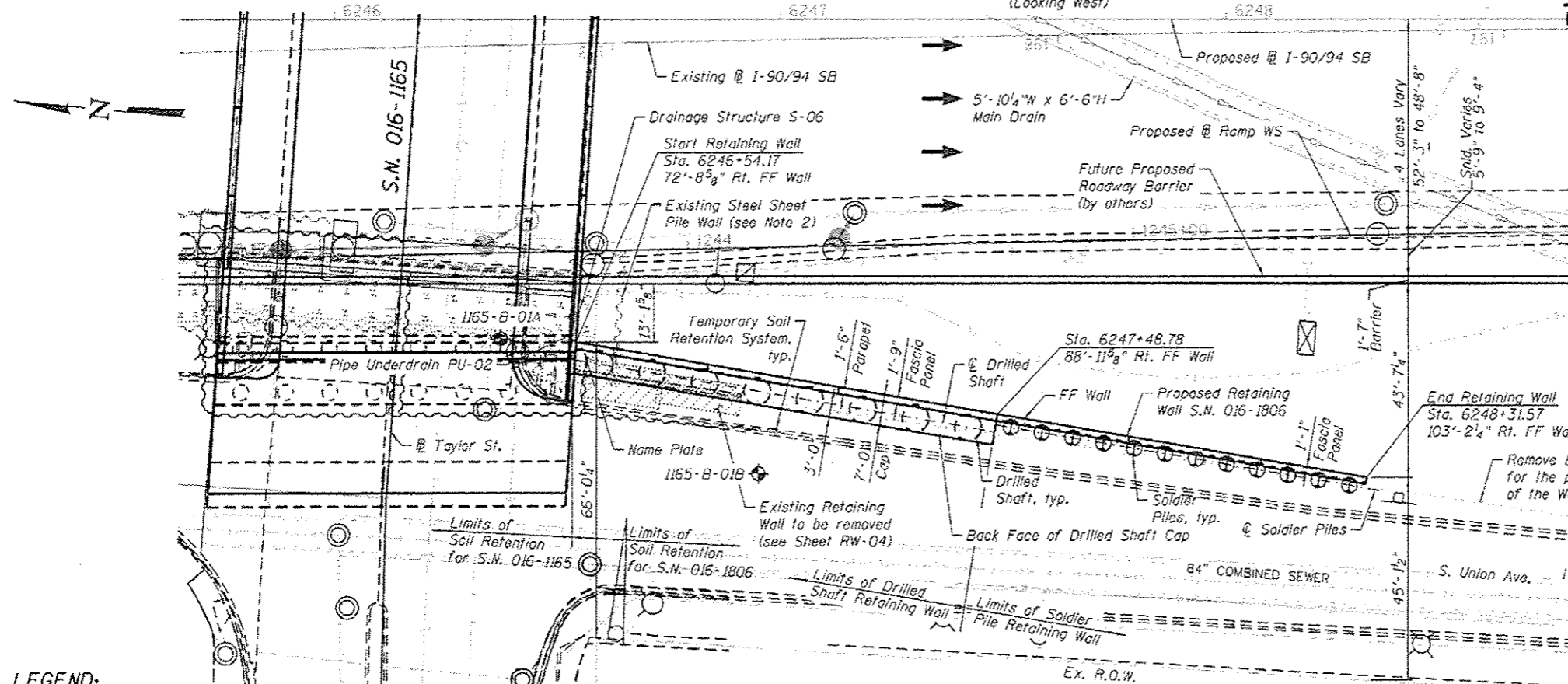
DESIGN STRESSES
 FIELD UNITS
 f'c = 3,500 psi
 f'c = 7,000 psi (Drilled Shaft)
 fy = 60,000 psi (Reinforcement)
 fy = 50,000 psi (M270 Grade 50) (Soldier Pile)

- Suggested Construction Sequence**
1. Install temporary Soil Retention System and remove existing Southwest Retaining Wall.
 2. Construct drilled shaft DS-1 thru DS-8 and soldier piles P-1 thru P-12.
 3. Construct drilled shaft cap & parapet.
 4. Excavate in front of shafts or piles to bottom of cast in place facing, installing lagging and Geocomposite Wall Drain in the process.
 5. Construct concrete fascia panel.
- * Measured along Front Face of wall.



APPROVED ** See Elevation Table Sheet RW-02.
 For Structural Adequacy Only

Amish T. Bhatt
 Engineer of Bridges & Structures



- LEGEND:**
- Combined Sewer
 - Electric
 - Existing Storm Sewer
 - Proposed Storm Sewer
 - Fiber Optic
 - Fire Hydrant
 - Light Pole
 - Soil Boring Location
 - Existing Fence
 - C = Construction Joint
 - E = Expansion Joint
 - FF = Front face of wall
 - BF = Back face of wall

- NOTES:**
1. Stations and Offsets are measured along proposed @ I-90/94 SB.
 2. Contractor shall field verify the location of existing steel sheet piling. If existing steel sheet piling is in conflict with proposed construction, the Contractor shall notify the Engineer and upon concurrence of the Engineer, the Contractor shall remove such conflict as directed by the Engineer. Cost shall be included with Foundation Construction at Existing Obstructions.

GENERAL PLAN & ELEVATION
RETAINING WALL 17
 F.A.I. RTE. 90/94 - SECTION 2013-012R
 COOK COUNTY
 STATION 6246+54.17 TO 6248+31.57
 STRUCTURE NO. 016-1806

0161806-60W30-501-GPE

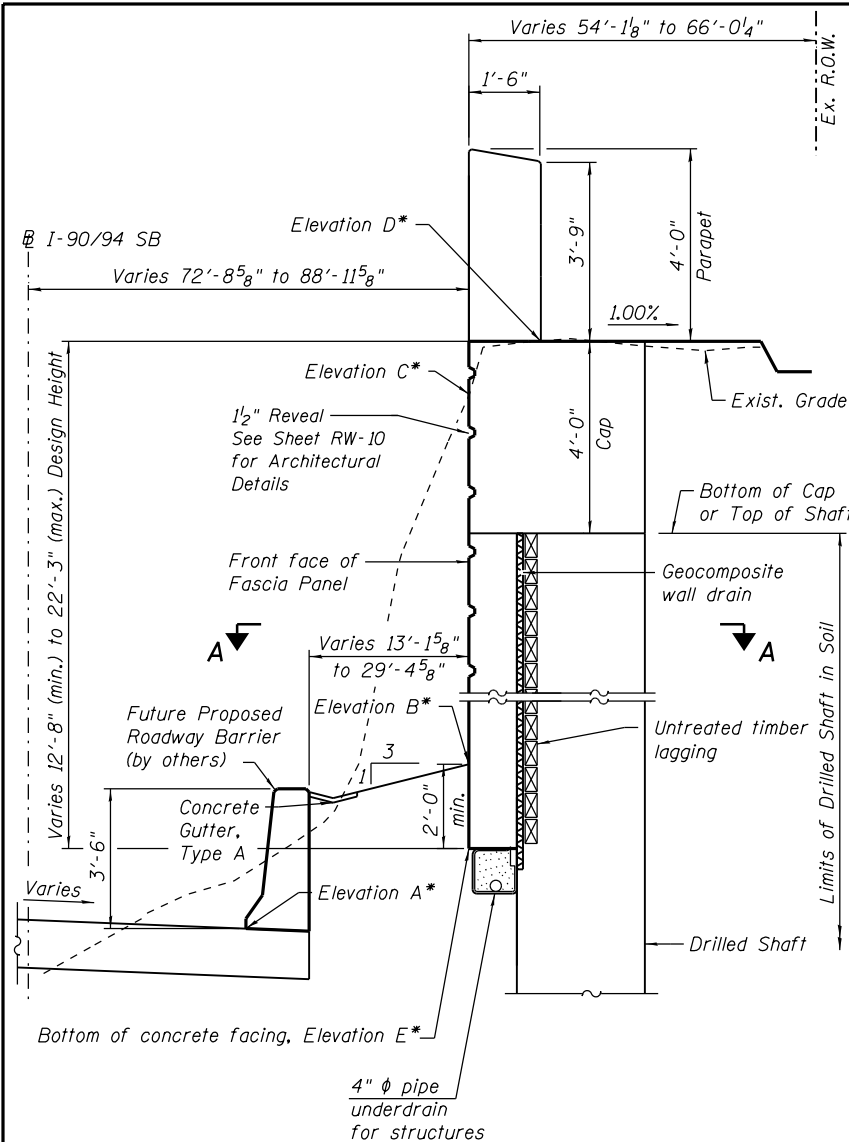


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	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - OF	REVISED
PLOT DATE = 12/16/2014	CHECKED - ATB	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SHEET NO. RW-01 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	365	305
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

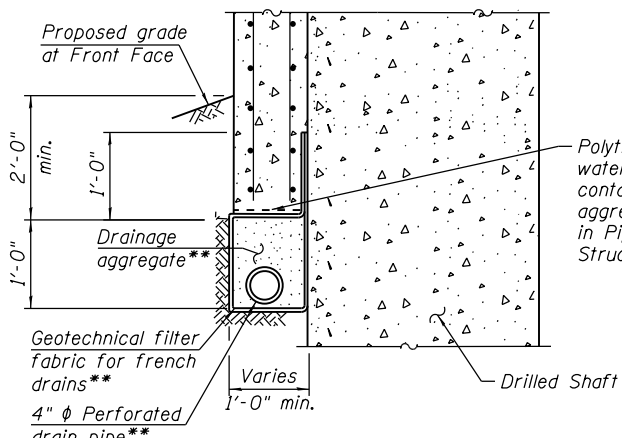


TYPICAL SECTION-DRILLED SHAFT RETAINING WALL

(Looking South)

*See Elevation Table

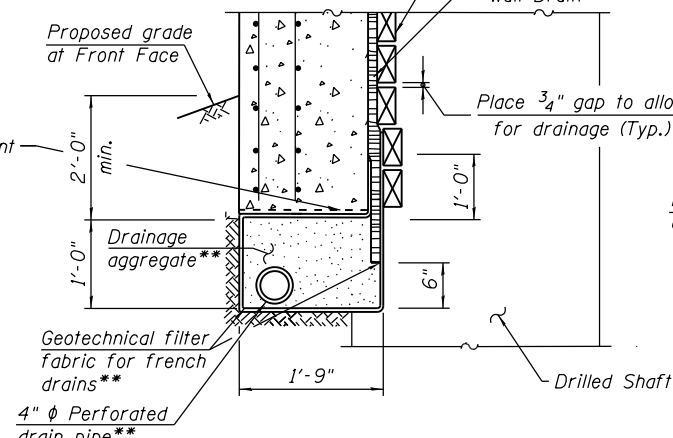
**Included in the cost of Pipe Underdrains for Structures. Between Sta. 6246+54.17 & Sta. 6248+31.57 the pipe shall connect to Pipe Underdrain PU-02 at W. Abutment of (S.N. 016-1165).



PIPE UNDERDRAIN DETAIL

(At Drilled Shaft)

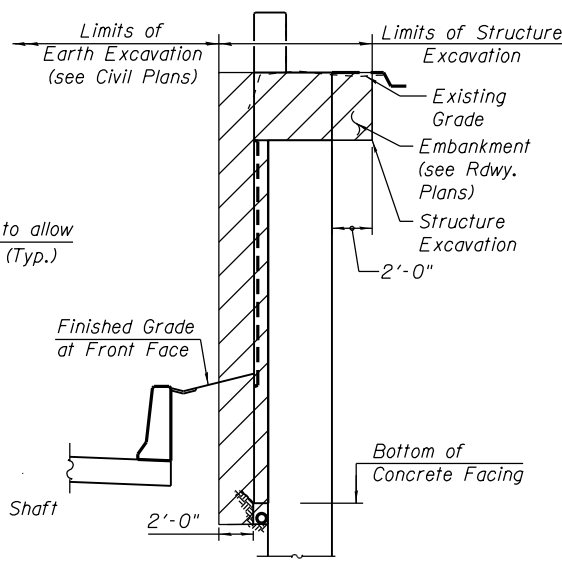
(Proposed Roadway Barrier not shown for clarity)



PIPE UNDERDRAIN DETAIL

(Between Drilled Shaft)

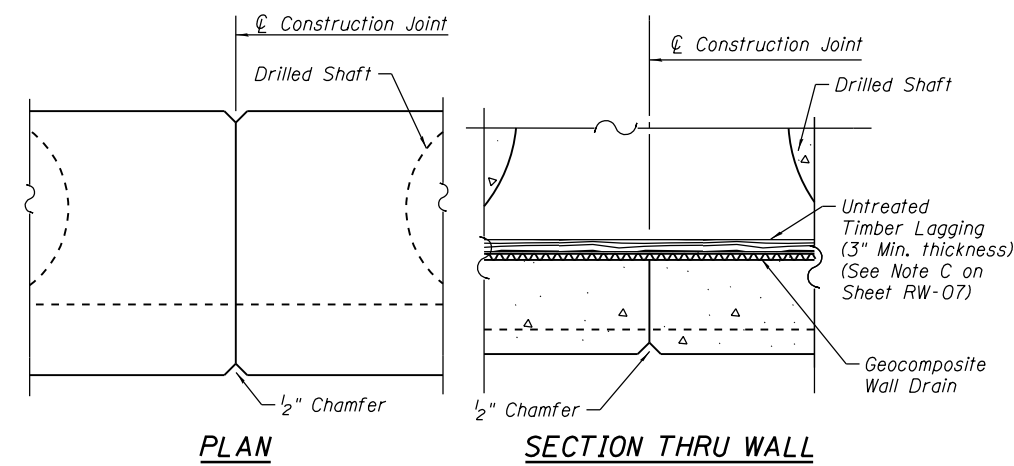
(Proposed Roadway Barrier not shown for clarity)



STRUCTURE EXCAVATION

(For Drilled Shaft Retaining Wall)

(Sta. 6246+54.17 to Sta. 6247+48.78)



PLAN

(Cap)

SECTION THRU WALL

CONSTRUCTION JOINT DETAILS

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Bars noted thus, 3x2-#5 indicates 3 lines of bars with 2 lengths per line.
3. Concrete Sealer shall be applied to the designated areas of the wall.
4. Slipforming of the parapets is not allowed.
5. Concrete for the Drilled Shafts shall be in accordance with Section 516 of Standard Specifications, except that the mix design of concrete shall attain a compressive strength of 7,000 psi at 14 days.
6. The Contractor shall coordinate construction of the retaining wall with the construction of West Abut. & Temp. Soil Ret. Sys. of S.N. 016-1165.
7. Concrete Fascia Panel shall be paid for as Concrete Structures.
8. 1/2" Preformed Flexible Foam Expansion Joint Filler shall be applied per Section 1051.09 of the Standard Specifications. Cost shall be included with Concrete Structures.
9. Limited groundwater elevation data is available in the boring logs. In addition, groundwater may also be present in deeper granular layers. The groundwater may rise in the shafts to an elevation above the top of granular layers. The Contractor shall consider this information when choosing construction methods. The Contractor will not be compensated for issues related to the groundwater elevations.
10. Piezometer installed at Sta. 6234+44.00, Offset 102' Rt.; the groundwater is measured at elevation 549.55.

INDEX OF SHEETS

- RW-01 General Plan and Elevation
- RW-02 Total Bill of Material, Index of Sheets and General Notes
- RW-03 Retaining Wall Cross Sections & Details
- RW-04 Existing Structure Removal & Temporary Soil Retention System Details
- RW-05 Drilled Shafts Retaining Wall Plan & Elevation
- RW-06 Soldier Pile Retaining Wall Plan & Elevation
- RW-07 Drilled Shafts Retaining Wall Sections & Details
- RW-08 Soldier Pile Retaining Wall Bill of Material & Parapet Joint Details
- RW-09 Architectural Details
- RW-10 Bar Splicer Assembly and Mechanical Splicer Details
- RW-11 Boring Logs I
- RW-12 Boring Logs II

ELEVATION TABLE

Station	Offset	Elevation A	Elevation B	Elevation C	Elevation D	Elevation E
6246+54.17	72'-8 ⁵ / ₈ " Rt.	574.23	580.36	584.52	593.85	571.60
6246+71.20	75'-7 ³ / ₄ " Rt.	574.27	581.38	586.77	593.85	573.32
6246+91.20	79'-1" Rt.	574.35	582.59	590.42	593.84	575.34
6247+00.00	80'-7 ⁵ / ₈ " Rt.	574.39	583.14	588.44	593.84	576.23
6247+25.00	84'-10 ⁵ / ₈ " Rt.	574.53	584.71	590.13	593.83	578.75
6247+48.78	88'-11 ⁵ / ₈ " Rt.	574.68	586.23	590.66	593.82	581.16
6247+50.00	89'-2 ¹ / ₈ " Rt.	574.69	586.31	590.69	593.82	581.28
6247+75.00	93'-5 ⁵ / ₈ " Rt.	574.86	587.90	591.25	593.82	583.81
6248+00.00	97'-9 ¹ / ₈ " Rt.	575.02	589.50	591.80	593.81	586.33
6248+25.00	102'-0 ⁵ / ₈ " Rt.	575.19	591.10	592.37	593.80	588.86
6248+28.61	102'-8 ¹ / ₈ " Rt.	575.22	591.28	592.45	593.80	589.22
6248+31.57	103'-2 ¹ / ₄ " Rt.	575.24	591.52	592.51	593.80	589.52

Elevation A - Edge of Shoulder at Face of Future Proposed Roadway Barrier (by others)
 Elevation B - Proposed Grade at Front Face of Wall
 Elevation C - Existing Grade at Front Face of Wall
 Elevation D - Proposed Grade at Back Face of Wall
 Elevation E - Bottom of Fascia Panel

TOTAL BILL OF MATERIAL

DESCRIPTION	UNIT	TOTAL
STRUCTURE EXCAVATION	CU. YD.	426
CONCRETE STRUCTURES	CU. YD.	203
CONCRETE SUPERSTRUCTURE	CU. YD.	39
REINFORCEMENT BARS, EPOXY COATED	POUND	30,020
REINFORCEMENT BARS	POUND	298,542
NAME PLATES	EACH	1
DRILLED SHAFT IN SOIL	CU. YD.	637
CONCRETE SEALER	SQ. FT.	2,916
PROTECTIVE COAT	SQ. YD.	186
PIPE UNDERDRAIN FOR STRUCTURES 4"	FOOT	180
MECHANICAL SPLICERS	EACH	448
TEMPORARY SOIL RETENTION SYSTEM	SQ. FT.	646
CROSSHOLE SONIC LOGGING	EACH	1
REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1
DRILLING AND SETTING SOLDIER PILES (IN SOIL)	CU. FT.	4,215
FURNISHING SOLDIER PILES (W SECTION)	FOOT	420
STUD SHEAR CONNECTORS	EACH	133
UNTREATED TIMBER LAGGING	SQ. FT.	1,517
GEOCOMPOSITE WALL DRAIN	SQ. YD.	153
FOUNDATION CONSTRUCTION AT EXISTING OBSTRUCTIONS	EACH	1

0161806-60W30-502-GenNote



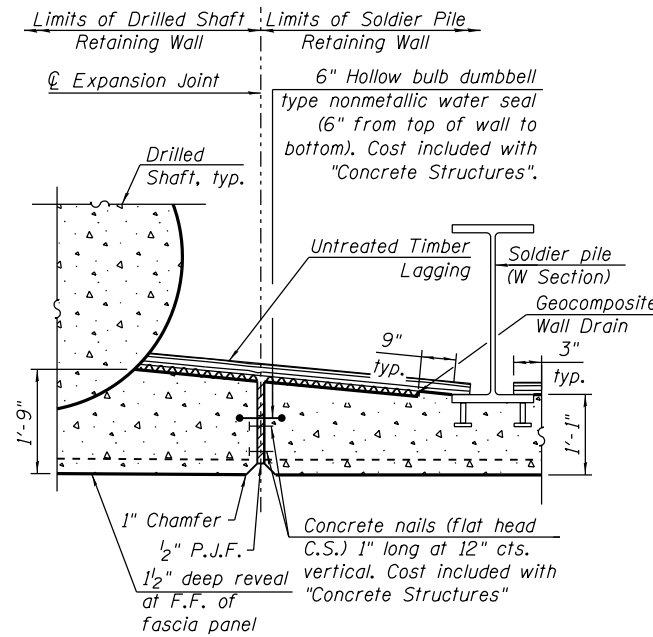
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOTAL BILL OF MATERIAL, INDEX OF SHEETS & GENERAL NOTES
STRUCTURE NO. 016-1806

SHEET NO. RW-02 OF RW-13 SHEETS

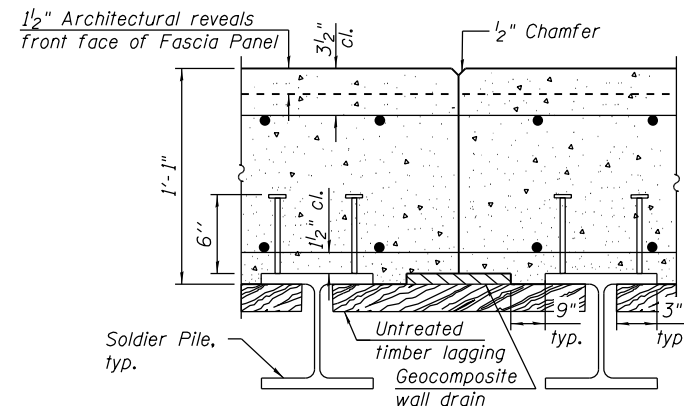
F.A.I. RTE. 90/94	SECTION 2013-012R	COUNTY COOK	TOTAL SHEETS 385	SHEET NO. 306
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



EXPANSION JOINT DETAILS

(At Sta. 6247+48.78)

***Included in the cost of Pipe Underdrains for Structures. Between Sta. 6246+54.17 & Sta. 6248+31.57 the pipe shall connect to Pipe Underdrain PU-02 at W. Abutment of (S.N. 016-1165).

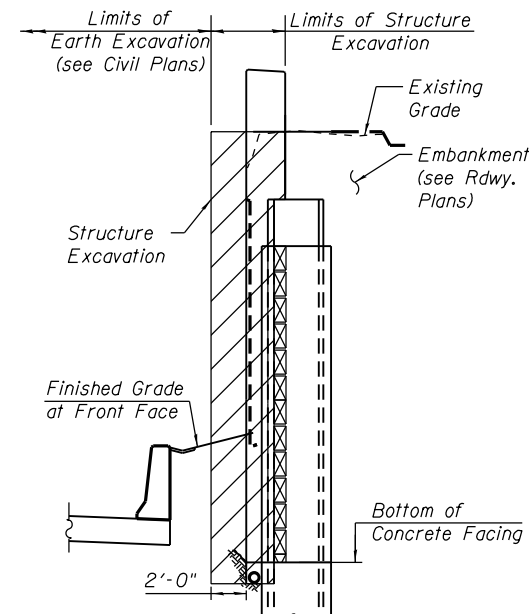


CONSTRUCTION JOINT DETAILS

STATION 6246+54.17
 BUILT BY
 STATE OF ILLINOIS
 F.A.I. RTE. 90/94-SEC. 2013-012R
 LOADING HL-93
 STRUCTURE NO. 016-1806

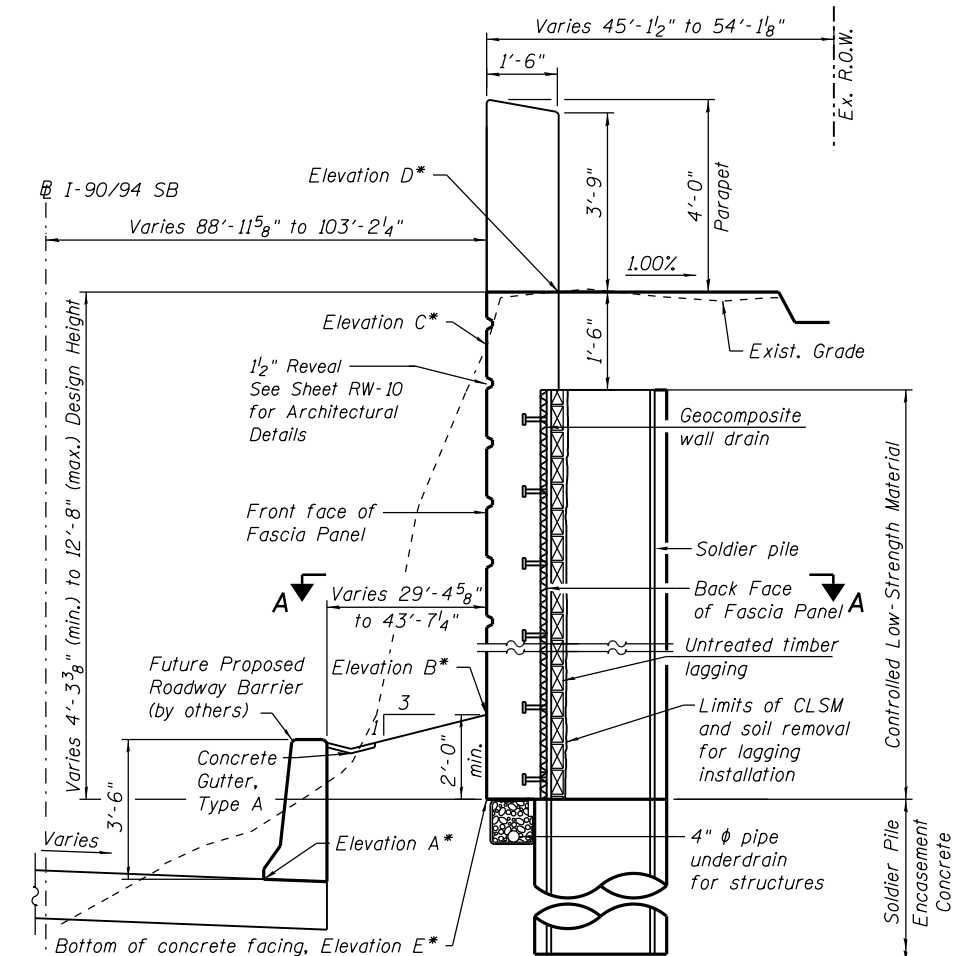
NAME PLATE

See Std. 515001
 (For location of Name Plate, see Sheet RW-01)



STRUCTURE EXCAVATION

(For Soldier Pile Retaining Wall)
 (Sta. 6247+48.78 to 6248+31.57)



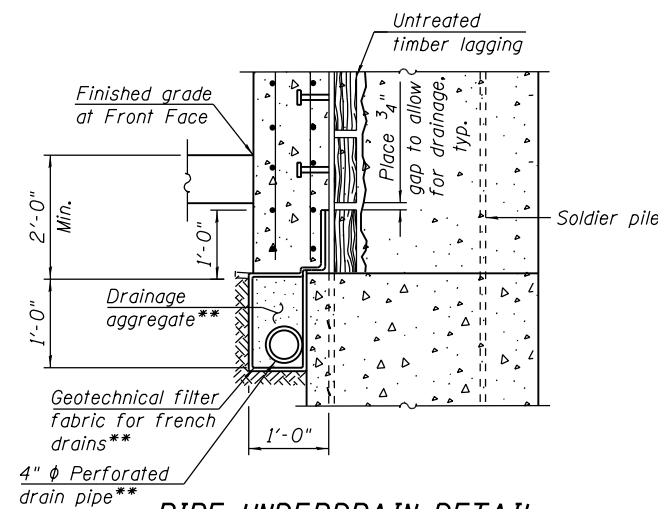
*See Elevation Table Sheet RW-02.

TYPICAL SECTION-SOLDER PILE RETAINING WALL

(Looking South)

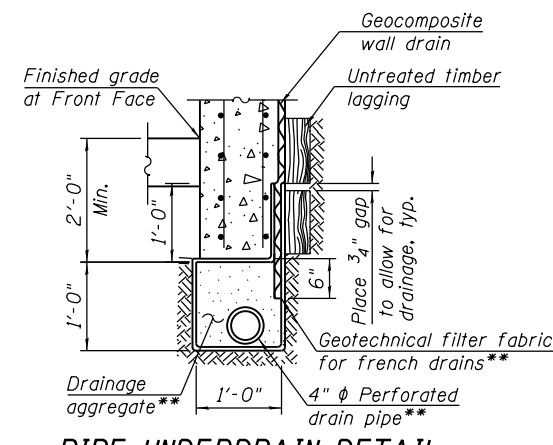
NOTES:

1. Cost of Controlled Low-Strength Material (CLSM) is included in Drilling and Setting Soldier Piles (in Soil) pay item.



PIPE UNDERDRAIN DETAIL

(At Soldier Pile)
 (Proposed Roadway Barrier not shown for clarity)



PIPE UNDERDRAIN DETAIL

(Between Soldier Piles)
 (Proposed Roadway Barrier not shown for clarity)

0161806-60W30-503-WallSect and D115



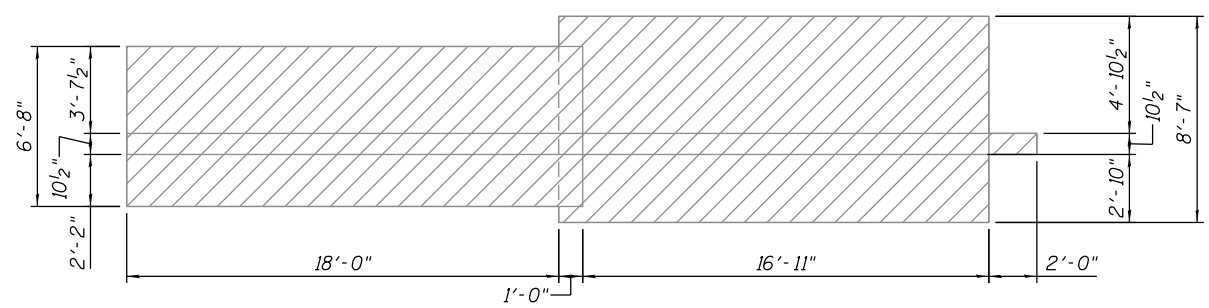
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

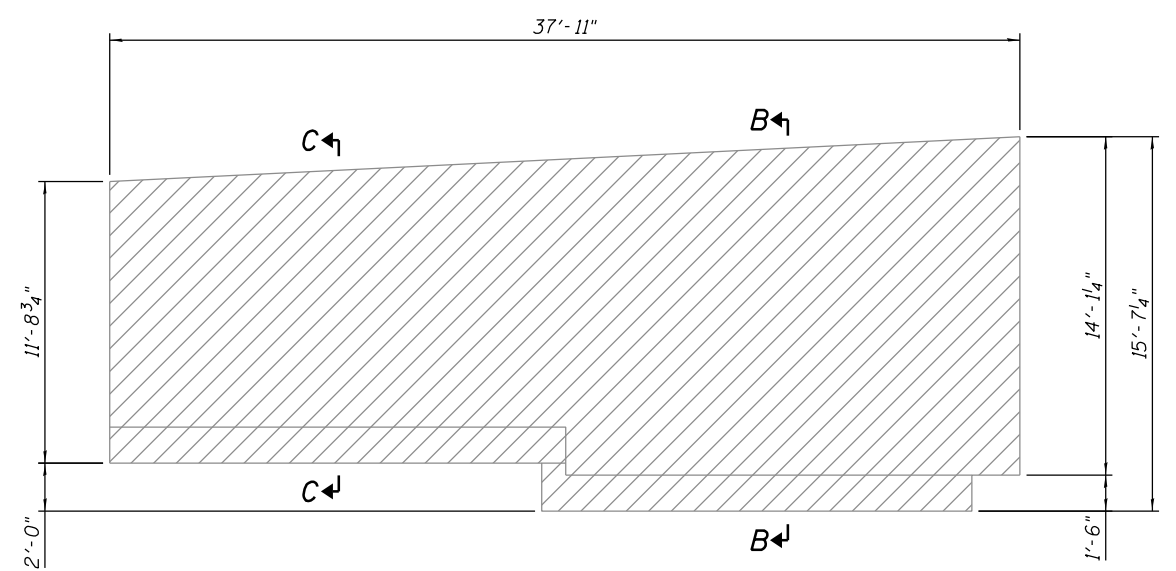
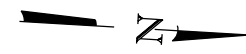
RETAINING WALL CROSS SECTIONS & DETAILS
 STRUCTURE NO. 016-1806

SHEET NO. RW-03 OF RW-13 SHEETS

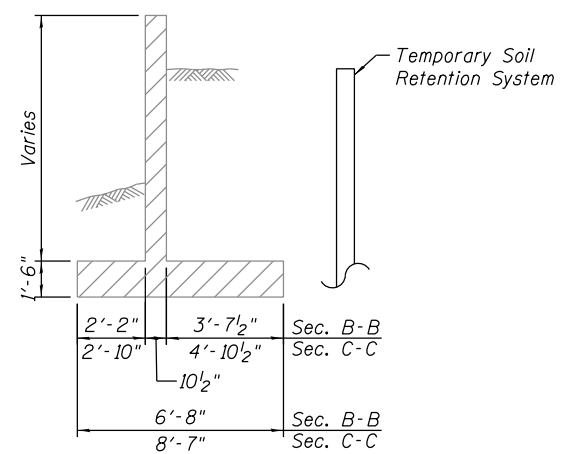
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CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



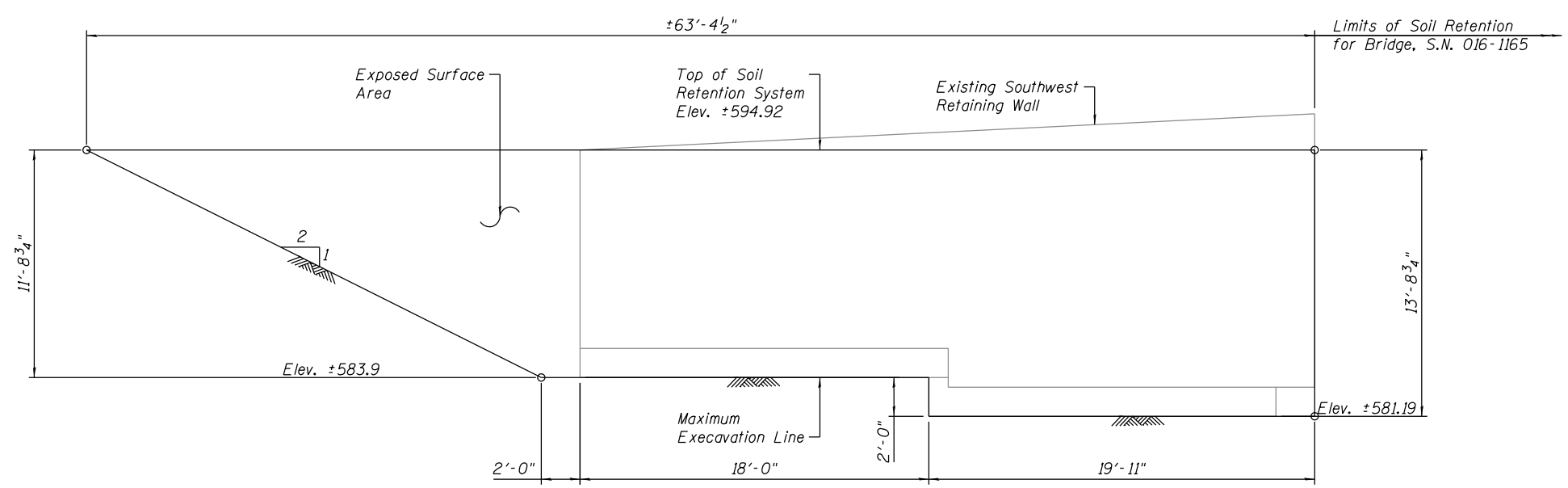
PLAN - EXISTING SOUTHWEST RETAINING WALL



ELEVATION - EXISTING SOUTHWEST RETAINING WALL REMOVAL



SECTION B-B & C-C



TEMPORARY SOIL RETENTION SYSTEM
(Looking West)

NOTES:

1. Removal of existing southwest retaining wall shall be as per special provisions for Removal of Existing Structures No. 2.
2. A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the engineer.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Removal of Existing Southwest Retaining Wall

BILL OF MATERIAL

Item	Unit	Total
Removal of Existing Structures No. 2	Each	1
Temporary Soil Retention System	Sq. Ft.	646

0161806-60W30-504-Removal



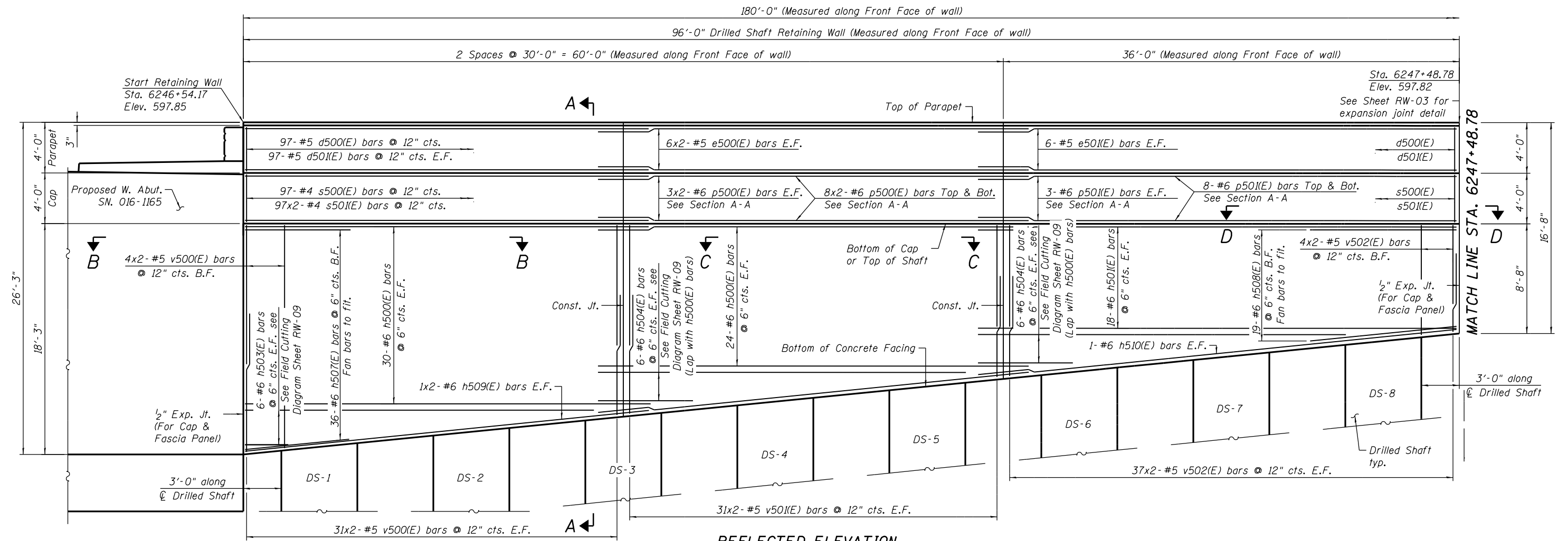
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

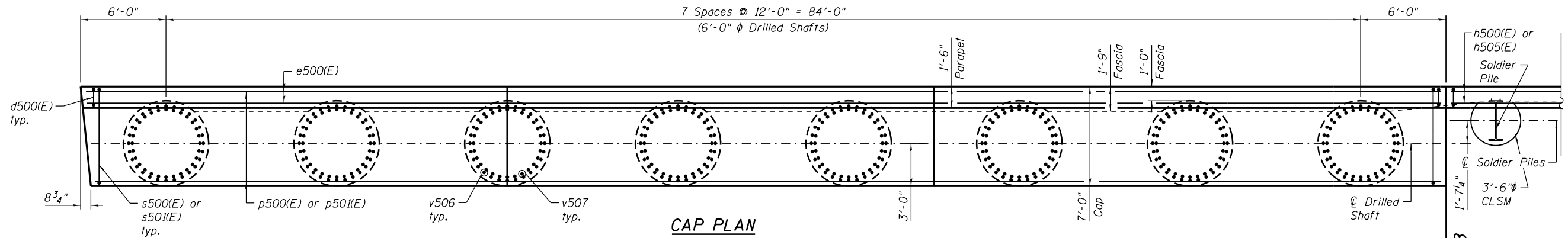
**EXISTING STRUCTURE REMOVAL & TEMP. SOIL RETENTION DETAILS
STRUCTURE NO. 016-1806**

SHEET NO. RW-04 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	308
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



REFLECTED ELEVATION
(Looking West)



CAP PLAN

- NOTES:**
1. Work this sheet with Sheet RW-07 & RW-09.
 2. Parapet concrete shall be paid for as Concrete Superstructure.
 3. Drilled Shaft Cap concrete shall be paid for as Concrete Structures.
 4. B.F. = Back Face,
F.F. = Front Face,
E.F. = Each Face
 5. A drilled shaft shall be tested in accordance with Special Provision for Crosshole Sonic Logging.



USER NAME = potelvl	DESIGNED - VP	REVISED
PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED
PLOT DATE = 12/16/2014	DRAWN - GF	REVISED
	CHECKED - ATB	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

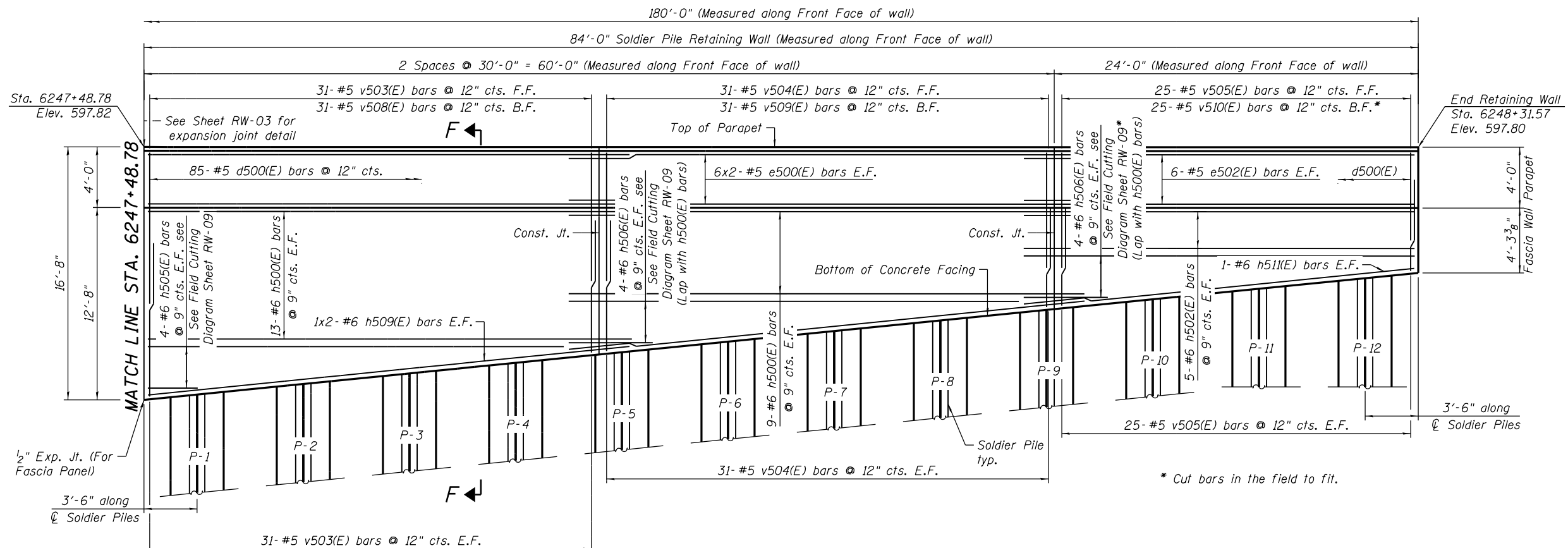
DRILLED SHAFTS RETAINING WALL PLAN & ELEVATION
STRUCTURE NO. 016-1806

SHEET NO. RW-05 OF RW-13 SHEETS

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CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

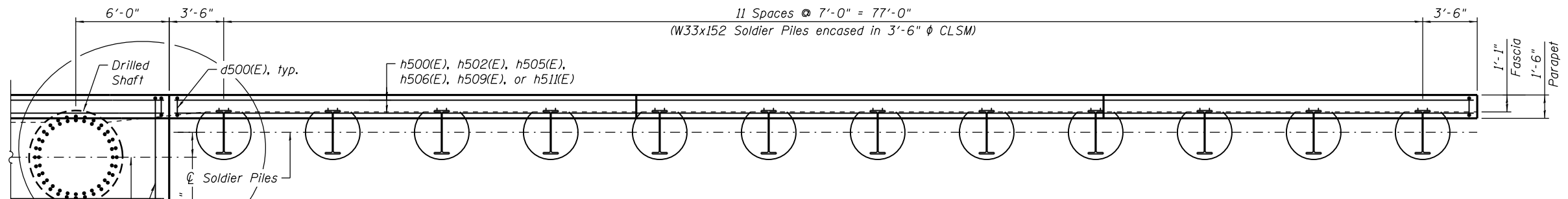
0161806-60W30-505-Drilled Shafts

MATCH LINE STA. 6247+48.78

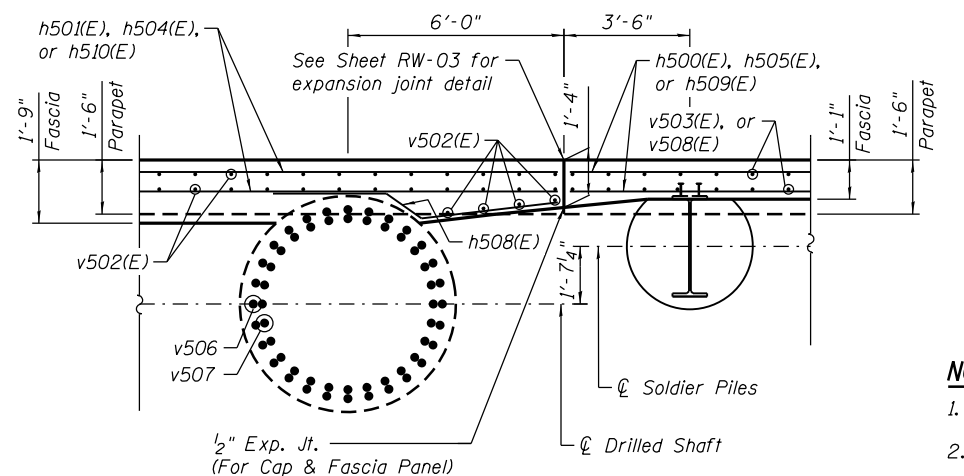
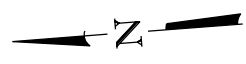


REFLECTED ELEVATION

(Looking West)



PLAN



Detail 1

(Parapet & Cap rebars not shown for clarity.)

NOTES:

1. Work this sheet with Sheet RW-08 & RW-09.
2. Parapet concrete shall be paid for as Concrete Superstructure.
3. B.F. = Back Face, F.F. = Front Face, E.F. = Each Face

0161806-60W30-506-SoldierPile



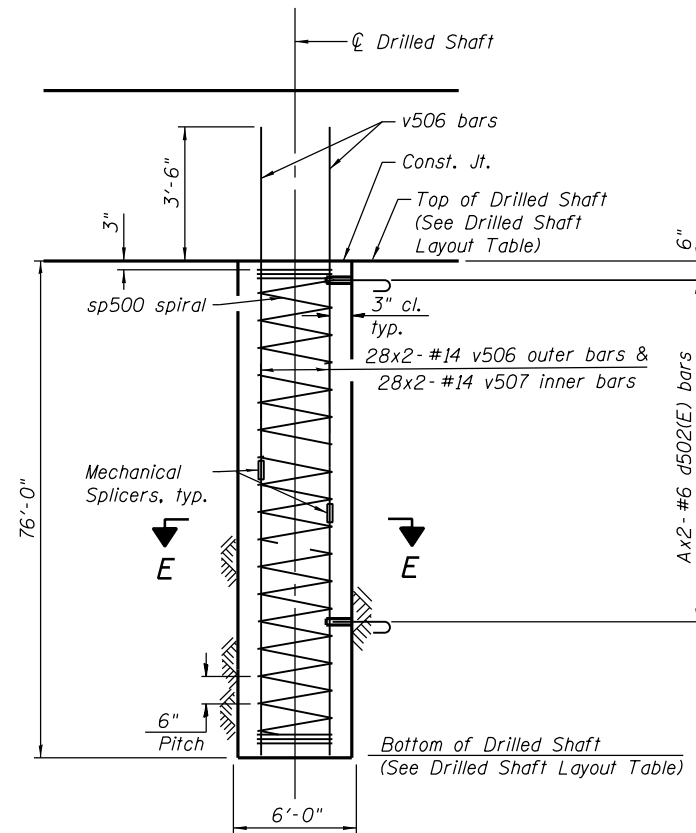
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PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED
PLOT DATE = 12/16/2014	DRAWN - GF	REVISED
	CHECKED - ATB	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

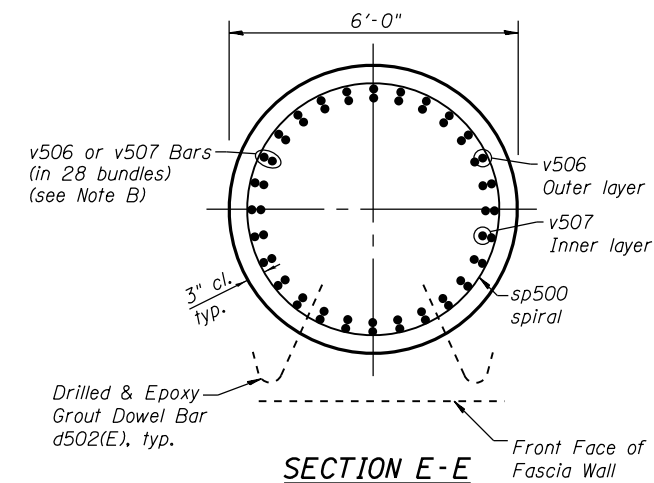
**SOLDIER PILE RETAINING WALL PLAN & ELEVATION
STRUCTURE NO. 016-1806**

SHEET NO. RW-06 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	310
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



TYPICAL SHAFT ELEVATION



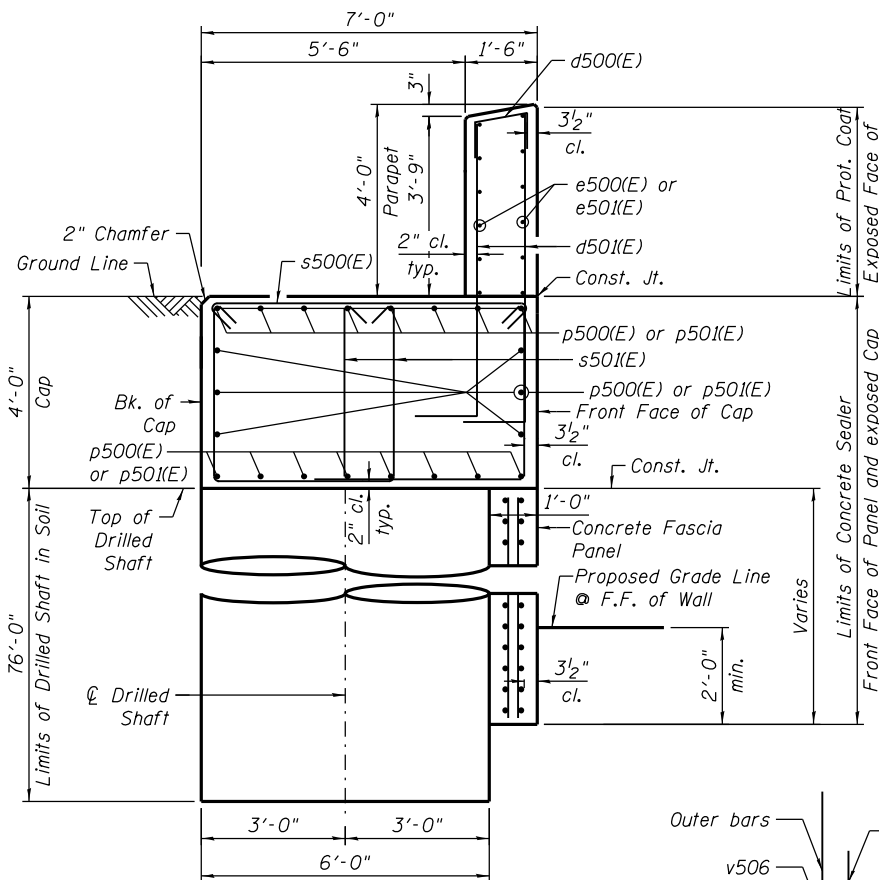
SECTION E-E

NOTES

- When splicing spiral reinforcement is necessary, the spiral shall be provided with 1/2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4 or shall both terminate with a 135° standard hook.
- Work this sheet with Sheet RW-05 & RW-09.
- The geocomposite wall drain shall be as per Section 591 of the Standard Specifications.
- Drilling and epoxy grouting d502(E) bars shall be as per Section 584 of the Standard Specifications. Cost shall be included with Concrete Structures.
- The quantities and detailing are based on the estimated elevations shown on the plans. The actual elevations may differ at each shaft and corresponding adjustments shall be made to the drilled shaft and reinforcement quantities and payment limits.
- Stations and offsets are measured along proposed ϕ I-90/94 SB.

DRILLED SHAFT LAYOUT

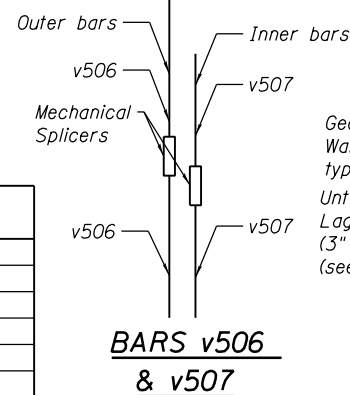
Shaft	Station at ϕ Shaft	Offset at ϕ Shaft	Top of Parapet El.	Top of Shaft El.	Bot. of Wall El.	Bot. of Shaft El.	Shaft Diameter	Shaft Length
DS-1	6246+59.40	77'-8 1/8"	597.85	589.85	572.20	513.85	6'-0"	76'-0"
DS-2	6246+71.23	79'-8 1/2"	597.85	589.85	573.39	513.85	6'-0"	76'-0"
DS-3	6246+83.06	81'-8 7/8"	597.84	589.84	574.59	513.84	6'-0"	76'-0"
DS-4	6246+94.88	83'-9 1/4"	597.84	589.84	575.78	513.84	6'-0"	76'-0"
DS-5	6247+06.71	85'-9 5/8"	597.84	589.84	576.98	513.84	6'-0"	76'-0"
DS-6	6247+18.54	87'-10"	597.83	589.83	578.17	513.83	6'-0"	76'-0"
DS-7	6247+30.36	89'-10 3/8"	597.83	589.83	579.37	513.83	6'-0"	76'-0"
DS-8	6247+42.19	91'-10 3/4"	597.83	589.83	580.56	513.83	6'-0"	76'-0"



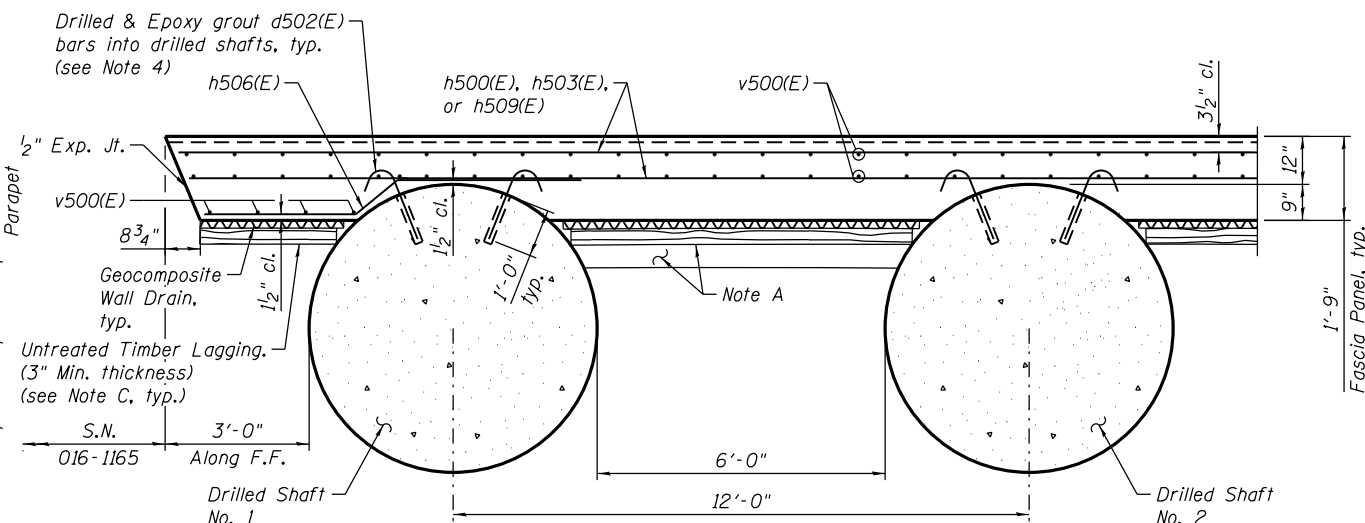
SECTION A-A

TABLE 1

Drilled Shaft No.	A
1	23
2	22
3	20
4	19
5	17
6	15
7	14
8	12

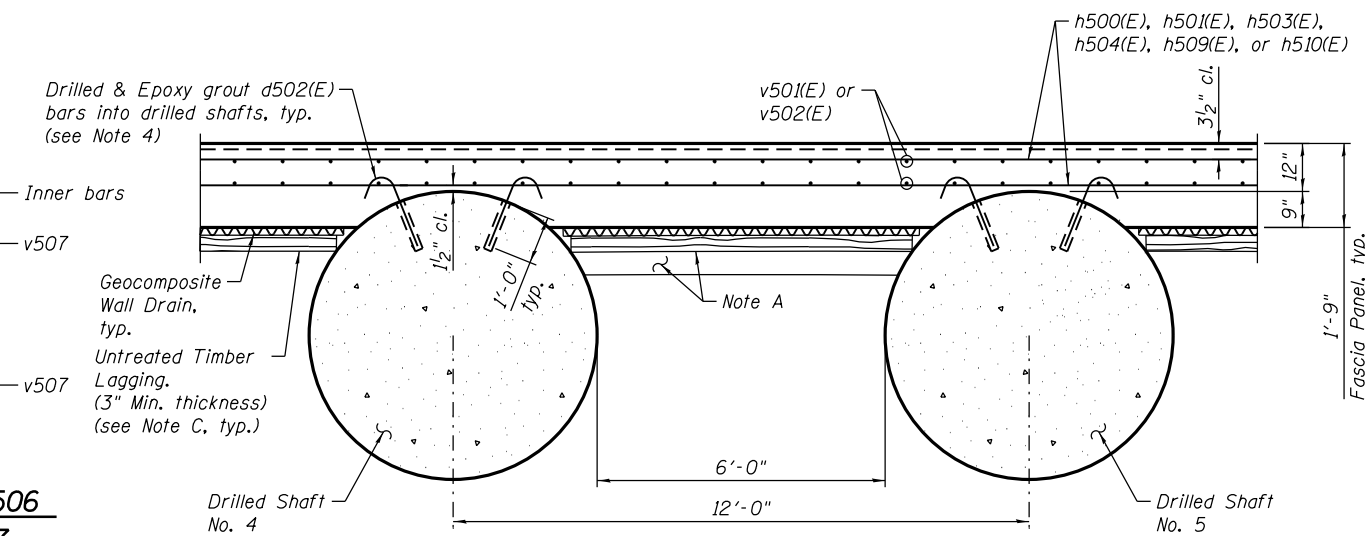


BARS v506 & v507



SECTION B-B

(Shaft rebars are not shown for clarity)



SECTION C-C

(Typical for Shaft 3 thru 8)
(Shaft rebars are not shown for clarity)

* Measured along ϕ Drilled Shaft

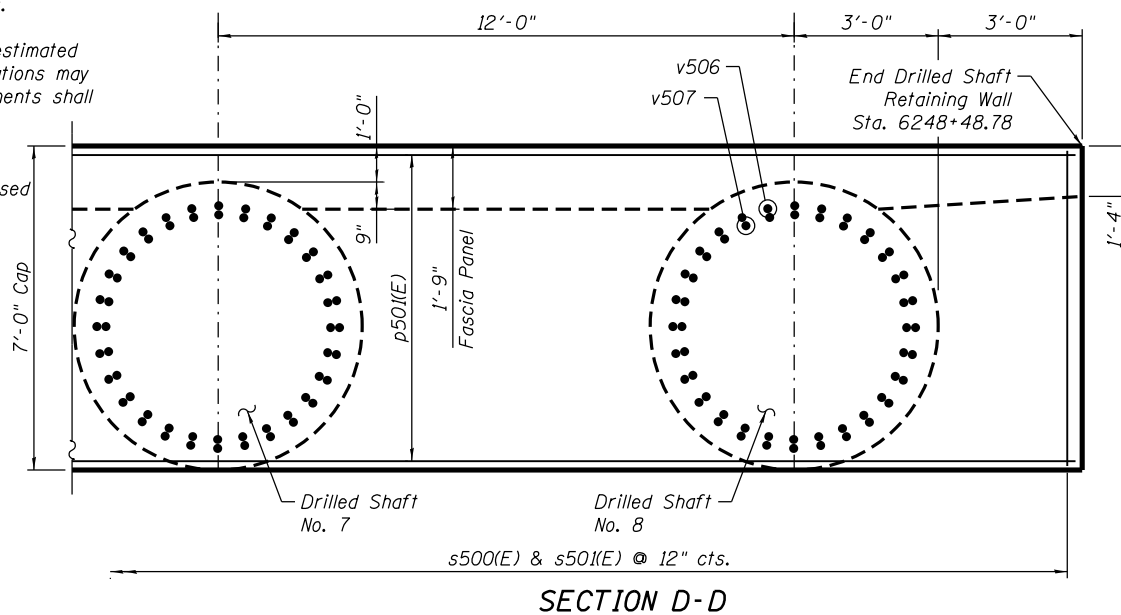
TYP. MIN. BAR LAP

- (Unless Noted Otherwise)
- #5 bar = 3'-3"
- #6 bar = 3'-10"

Note A
Install lagging and Geocomposite Wall Drain from top down as excavation proceeds. Minimize over-excavation and backfill voids with dry loose sand. Cost of over-excavation and backfill sand included with Concrete Structures. Cost of lagging and Geocomposite Wall Drain included in pay item Untreated Timber Lagging and Geocomposite Wall Drain respectively, within limits of Retaining Wall (Sta. 6246+54.17 to Sta. 6248+31.57).

Note B
Bundle v506 bars in outer layer with v507 bars in inner layer.

Note C
The Contractor is responsible for the design and performance of the lagging system, using no less than a 3 in. nominal rough-sawn thickness and timber with a minimum allowable bending stress of 1000 psi, until the concrete facing is installed. The Contractor shall submit design calculations and details prepared by an Illinois Licensed Structural Engineer for the attachment of the lagging to the shaft for approval by the Engineer. Alternative equivalent systems may be submitted for approval by the Engineer.



SECTION D-D

0161806-60W30-507-Super-STRUCT



USER NAME = potelvl	DESIGNED - VP	REVISD
PLOT SCALE = N.T.S.	CHECKED - ATB	REVISD
PLOT DATE = 12/16/2014	DRAWN - GF	REVISD
	CHECKED - ATB	REVISD

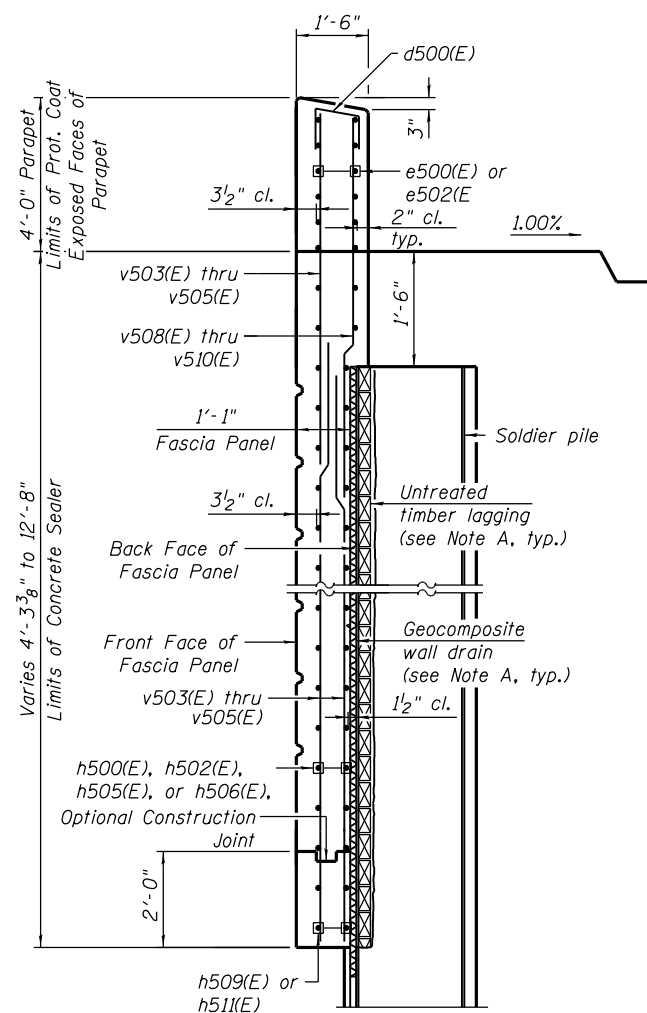
DESIGNED - VP	REVISD
CHECKED - ATB	REVISD
DRAWN - GF	REVISD
CHECKED - ATB	REVISD

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

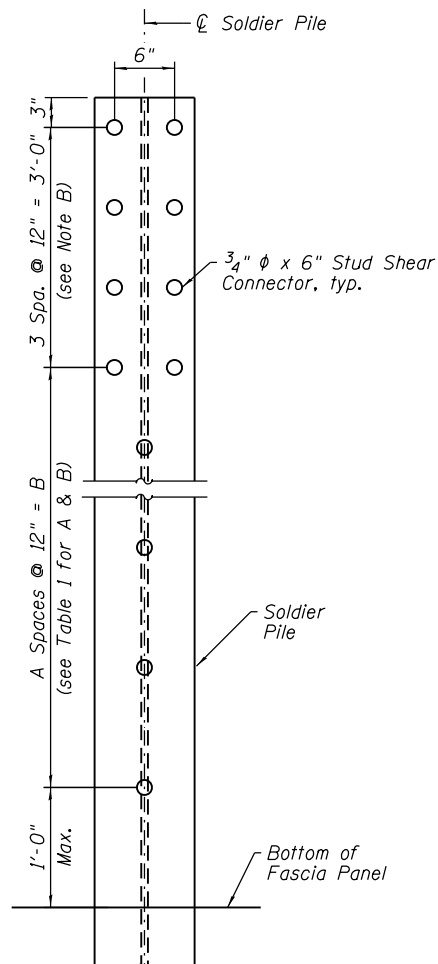
**DRILLED SHAFT RETAINING WALL SECTIONS & DETAILS
STRUCTURE NO. 016-1806**

SHEET NO. RW-07 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	311
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

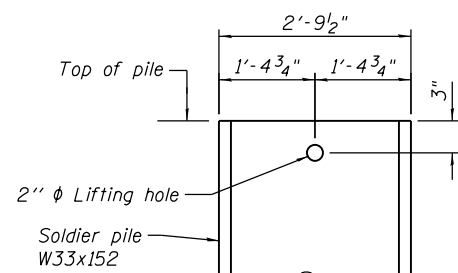


SECTION F-F



STUD SHEAR CONNECTOR DETAIL

(see Table 1 for total number of studs per pile)



LIFTING HOLE DETAIL

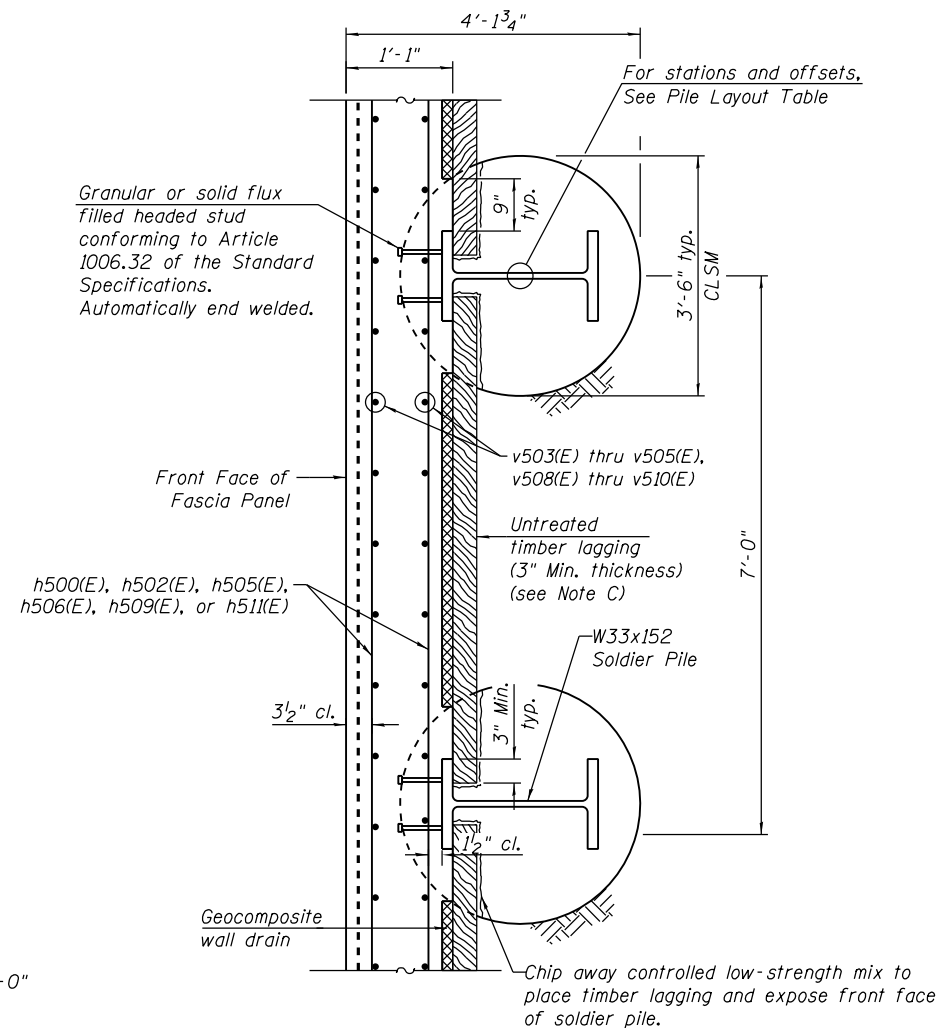
TYP. MIN. BAR LAP

(Unless Noted Otherwise)
 #5 bar = 3'-3"
 #6 bar = 3'-10"

Note A
 Install lagging and Geocomposite Wall Drain from top down as excavation proceeds. Minimize over-excavation and backfill voids with dry loose sand. Cost of over-excavation and backfill sand included with Concrete Structures. Cost of lagging and Geocomposite Wall Drain included in pay item Untreated Timber Lagging and Geocomposite Wall Drain respectively, within limits of Retaining Wall (Sta. 6246+54.17 to Sta. 6248+31.57).

Note B
 Provide number of spaces as shown for piles P-1 thru P-11. For pile P-12 provide 2 stud group spaces at 12" equal to 2'-0" (with total of 6 studs on pile P-12 only).

Note C
 The Contractor is responsible for the design and performance of the lagging system, using no less than a 3 in. nominal rough-sawn thickness and timber with a minimum allowable bending stress of 1000 psi. The Contractor shall submit design calculations and details prepared by an Illinois Licensed Structural Engineer for the attachment of the lagging to the concrete encasement for approval by the Engineer. Alternative equivalent systems may be submitted for approval by the Engineer.



SECTION THRU DRILLED SOLDIER PILE WALL

NOTES:

- The Contractor shall provide a method to assure the soldier piles achieve at least the plan tip elevations. Piles ending in the soft clay layer or extending below it may need special measures. The soldier pile locations and elevations shall meet the tolerances provided in the Special Provisions. Any additional measures required to satisfy the construction tolerances will not be paid for separately but shall be included in Drilling and Setting Soldier Piles (in Soil).
- CLSM extends from bottom of fascia panel (Elevation E) to 1'-6" below existing grade at back face of fascia panel. Soldier pile encasement concrete extends from bottom of fascia panel (Elevation E) to bottom of pile.
- The thickness of geocomposite wall drain shall not exceed 1 9/16". The geocomposite wall drain shall be constructed according to Section 591 of the Standard Specifications.
- Stud shear connectors shall be 3/4" x 6" granular or solid flux filled headed studs, automatically end welded to the front flange of the soldier piles.
- See sheet RW-03 for Pipe Underdrain details.
- See sheet RW-10 for Architectural Reveals pattern and details. Reveals pattern and details are included in the cost of pay item Concrete Structures.
- For Elevations, see Elevation Table on sheet RW-02.
- Stations and offsets are measured along proposed I-90/94 SB.

PILE LAYOUT

Pile	Station at C. Pile	Offset at C. Pile	Top of Parapet El.	Top of Pile El.	Bot. of Wall El.	Bot. of Pile El.	Pile Section	Pile Length
P-1	6247+51.82	91'-11 1/8"	597.82	592.32	581.51	532.32	W33x152	60'-0"
P-2	6247+58.72	93'-1 1/4"	597.82	592.32	582.20	532.32	W33x152	60'-0"
P-3	6247+65.62	94'-3 1/2"	597.82	592.32	582.90	532.32	W33x152	60'-0"
P-4	6247+72.52	95'-5 3/4"	597.82	592.32	583.60	557.32	W33x152	35'-0"
P-5	6247+79.42	96'-7 7/8"	597.81	592.31	584.29	557.31	W33x152	35'-0"
P-6	6247+86.32	97'-10 1/8"	597.81	592.31	584.99	557.31	W33x152	35'-0"
P-7	6247+93.22	99'-0 3/8"	597.81	592.31	585.69	567.31	W33x152	25'-0"
P-8	6248+00.12	100'-2 5/8"	597.81	592.31	586.38	567.31	W33x152	25'-0"
P-9	6248+07.02	101'-4 3/4"	597.81	592.31	587.08	567.31	W33x152	25'-0"
P-10	6248+13.92	102'-7"	597.80	592.30	587.78	572.30	W33x152	20'-0"
P-11	6248+20.81	103'-9 1/4"	597.80	592.30	588.47	572.30	W33x152	20'-0"
P-12	6248+27.71	104'-11 1/2"	597.80	592.30	589.17	572.30	W33x152	20'-0"

TABLE 1

Drilled Shaft No.	A	B	Total Number of Studs
1	7	7'-0"	15
2	6	6'-0"	14
3	6	6'-0"	14
4	5	5'-0"	13
5	4	4'-0"	12
6	4	4'-0"	12
7	3	3'-0"	11
8	2	2'-0"	10
9	1	1'-0"	9
10	1	1'-0"	9
11	-	-	8
12	-	-	6

0161806-60W30-508-Super-STRUCT



USER NAME = potelvl	DESIGNED - VP	REVISED
PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED
PLOT DATE = 12/16/2014	DRAWN - GF	REVISED
	CHECKED - ATB	REVISED

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SOLDIER PILE RETAINING WALL SECTIONS & DETAILS
 STRUCTURE NO. 016-1806**

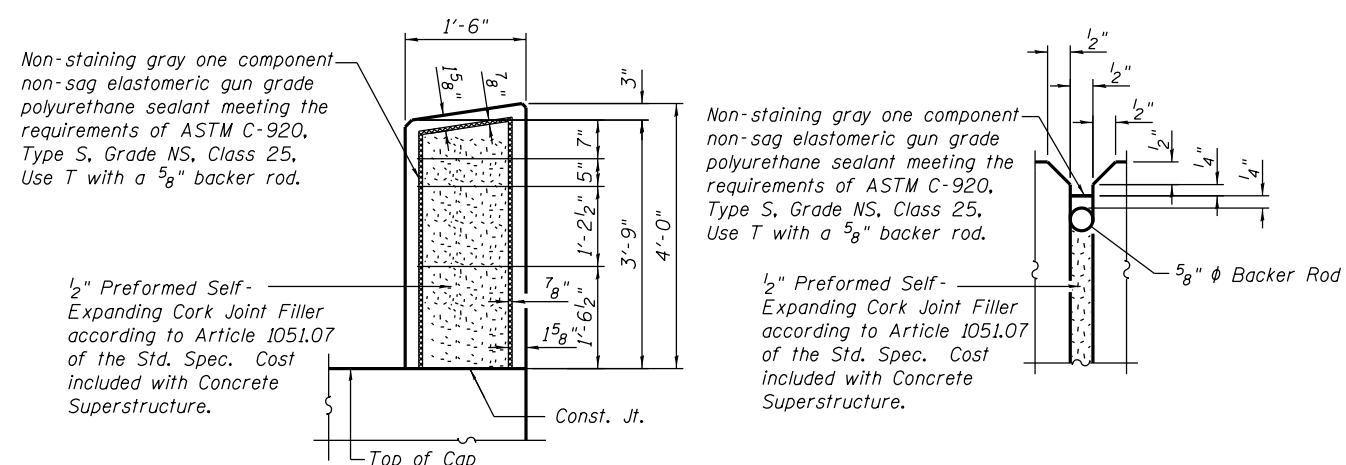
SHEET NO. RW-08 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	312
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

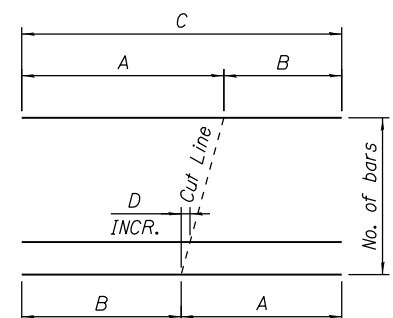
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d500(E)	182	#5	2'-10"	□
d501(E)	194	#5	6'-4"	□
d502(E)	139	#6	2'-2"	□
e500(E)	48	#5	31'-6"	—
e501(E)	12	#5	37'-8"	—
e502(E)	12	#5	25'-8"	—
h500(E)	152	#6	31'-9"	—
h501(E)	36	#6	37'-11"	—
h502(E)	10	#6	25'-11"	—
h503(E)	6	#6	31'-9"	—
h504(E)	12	#6	35'-11"	—
h505(E)	4	#6	28'-10"	—
h506(E)	8	#6	33'-0"	—
h507(E)	36	#6	10'-4"	—
h508(E)	19	#6	10'-3 1/4"	—
h509(E)	8	#6	32'-0"	—
h510(E)	2	#6	38'-2"	—
h511(E)	2	#6	26'-0"	—
p500(E)	44	#6	31'-9"	—
p501(E)	22	#6	37'-11"	—
s500(E)	97	#4	7'-7"	□
s501(E)	194	#4	11'-8 1/2"	□
sp500	8	#6	75'-9"	~
v500(E)	132	#5	10'-7"	—
v501(E)	124	#5	9'-1"	—
v502(E)	156	#5	7'-7"	—
v503(E)	93	#5	9'-8"	—
v504(E)	93	#5	8'-2"	—
v505(E)	75	#5	6'-8"	—
v506	448	#14	39'-9"	—
v507	448	#14	38'-0"	—
v508(E)	31	#5	9'-10"	—
v509(E)	31	#5	8'-4"	—
v510(E)	25	#5	8'-3 1/2"	—
Concrete Structures			Cu. Yd.	203
Concrete Superstructure			Cu. Yd.	39
Structure Excavation			Cu. Yd.	426
Concrete Sealer			Sq. Ft.	2,916
Protective Coat			Sq. Yd.	186
Reinforcement Bars, Epoxy Coated			Pound	30,020
Reinforcement Bars			Pound	298,542
Drilled Shaft in Soil			Cu. Yd.	637
Geocomposite Wall Drain			Sq. Yd.	153
Pipe Underdrain for Structures 4"			Foot	180
Mechanical Splicers			Each	448
Crosshole Sonic Logging			Each	1
Furnishing Soldier Piles (W Section)			Foot	420
Drilling and Setting Soldier Piles (in Soil)			Cu. Ft.	4,215
Stud Sheer Connectors			Each	133
Untreated Timber Lagging			Sq. Ft.	1,517

* Length is height of spiral



PARAPET EXPANSION JOINT DETAILS

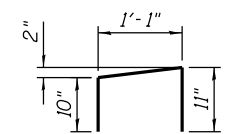


FIELD CUTTING DIAGRAM

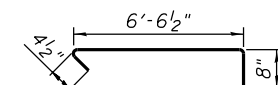
Order bars Full Length. Cut as shown and use remainder of bars on opposite face. See table for dimensions.

BAR TABLE SCHEDULE

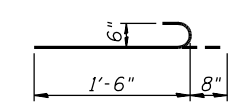
Bar	No. of Sets Req'd	Bar No.	No. of Bars Per Set	A	B	C	D
h503(E)	1	#6	6	28'-9"	3'-0"	31'-9"	5'-1 7/8"
h504(E)	2	#6	6	30'-10"	5'-1"	35'-11"	5'-1 7/8"
h505(E)	1	#6	4	25'-8"	3'-2"	28'-10"	7'-6 1/8"
h506(E)	2	#6	4	27'-9"	5'-3"	33'-0"	7'-6 1/8"



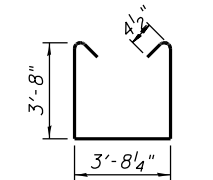
BAR d500(E)



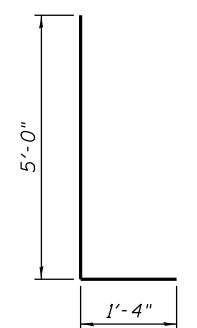
BAR s500(E)



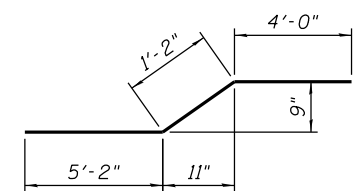
BAR d502(E)



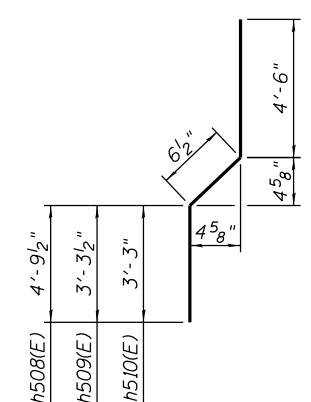
BAR s501(E)



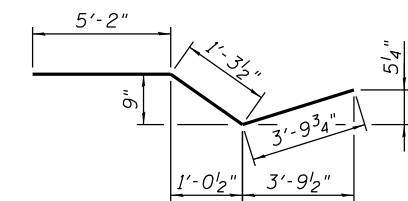
BAR d501(E)



BAR h507(E)



BAR h508(E), h509(E) AND h510(E)



BAR h508(E)

0161806-60W30-509-Super-Struct



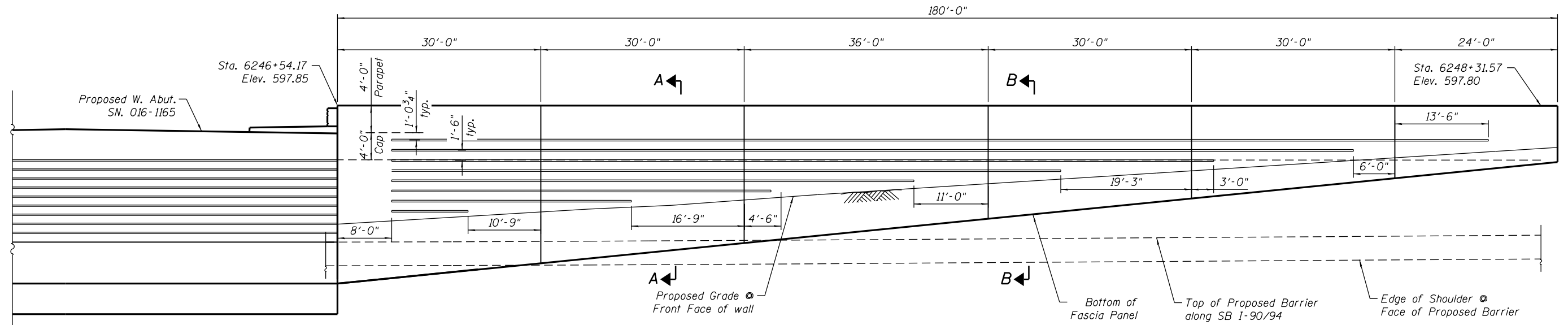
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PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED
PLOT DATE = 12/16/2014	DRAWN - GF	REVISED
	CHECKED - ATB	REVISED

**STATE OF ILLINOIS
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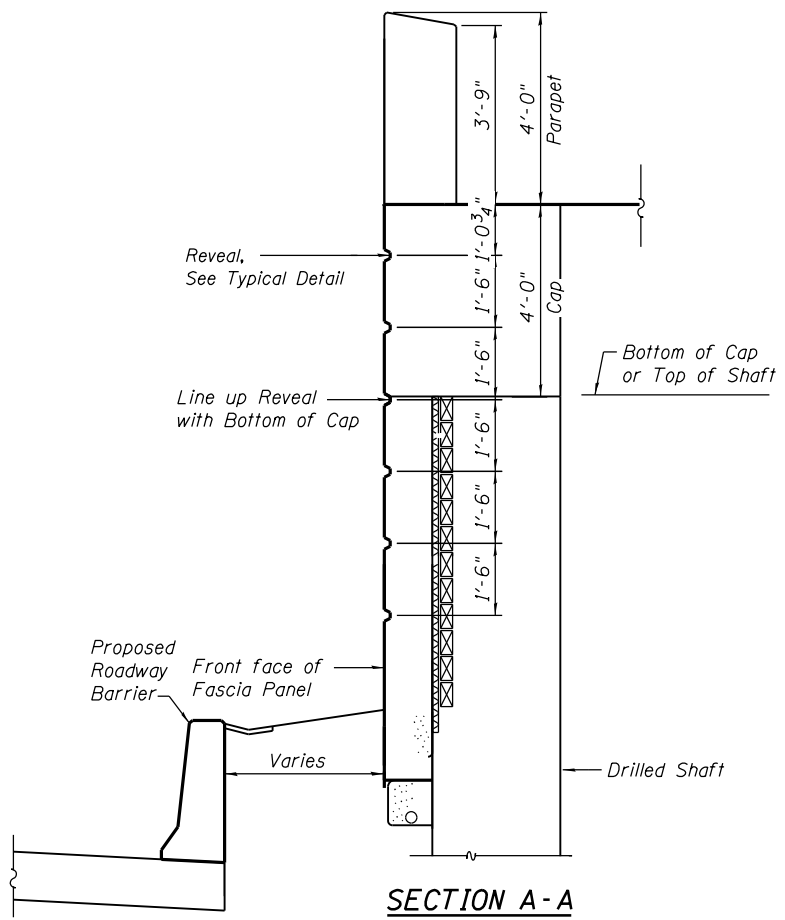
**BILL OF MATERIAL & PARAPET JOINT DETAILS
STRUCTURE NO. 016-1806**

SHEET NO. RW-09 OF RW-13 SHEETS

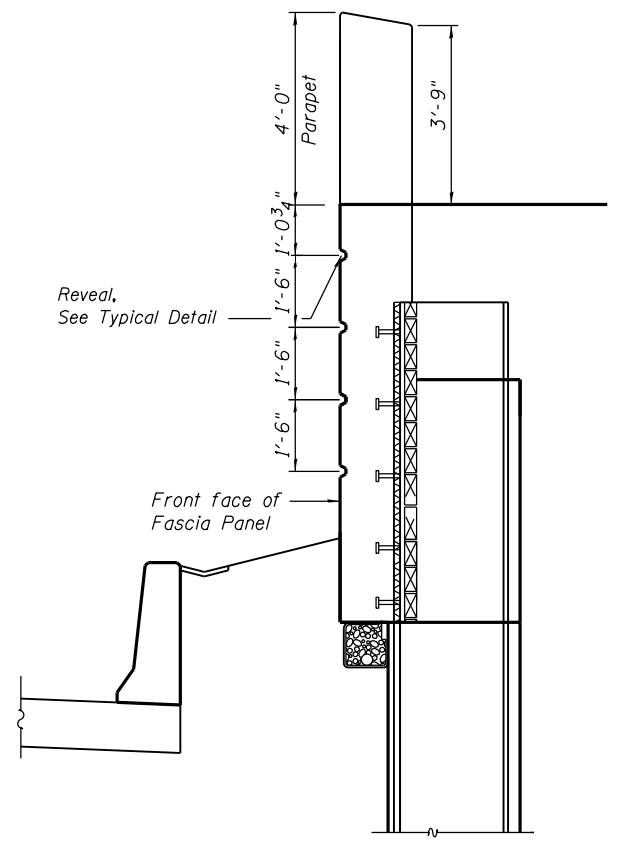
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	313
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



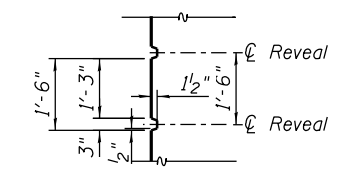
REFLECTED ELEVATION
(Looking West)



SECTION A-A



SECTION B-B



TYPICAL REVEAL DETAIL

NOTES:

1. Reveals in Concrete Facing will not be paid separately and will be included in the cost of the pay item "Concrete Structures".

0161806-60W30-S10-Ar.ch.dgn



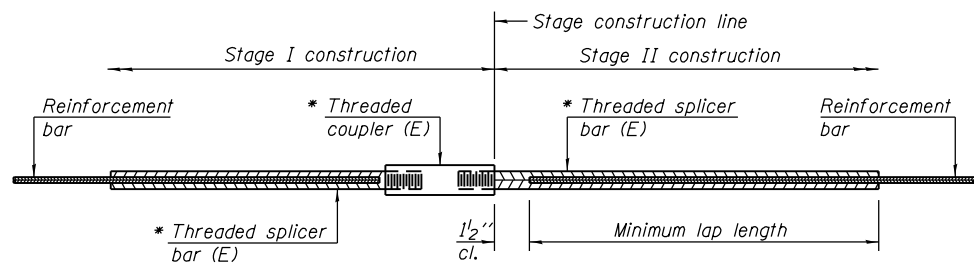
USER NAME = patelvi	DESIGNED - VP	REVISED
PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED
PLOT DATE = 12/16/2014	DRAWN - GF	REVISED
	CHECKED - ATB	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARCHITECTURAL DETAILS
STRUCTURE NO. 016-1806**

SHEET NO. RW-10 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	314
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



STANDARD BAR SPLICER ASSEMBLY

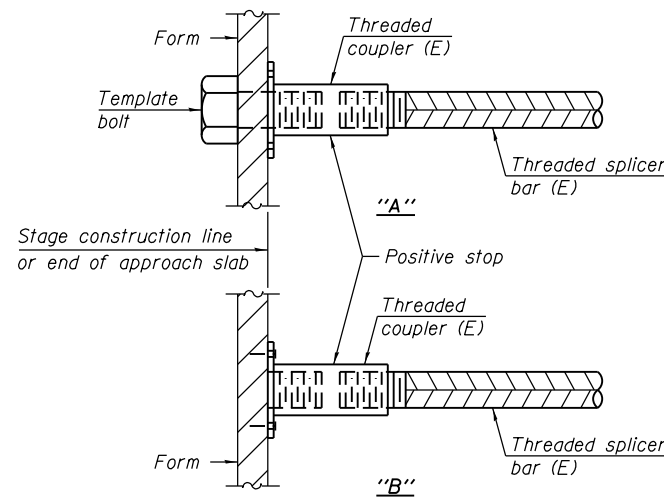
Minimum Lap Lengths						
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

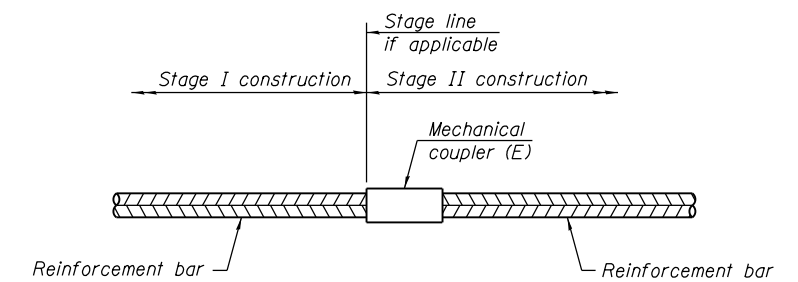
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length



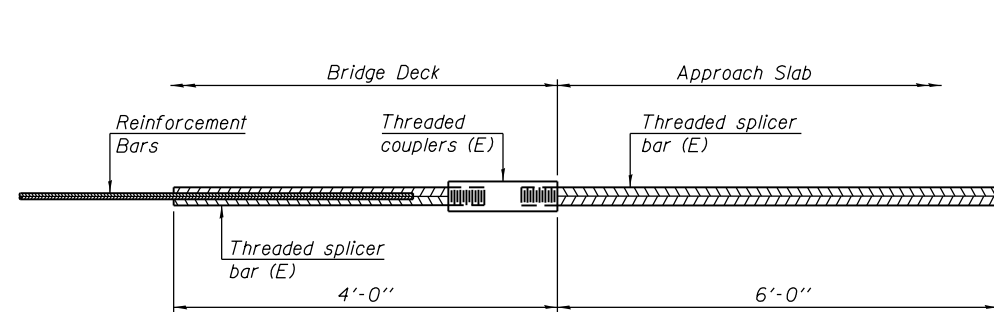
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



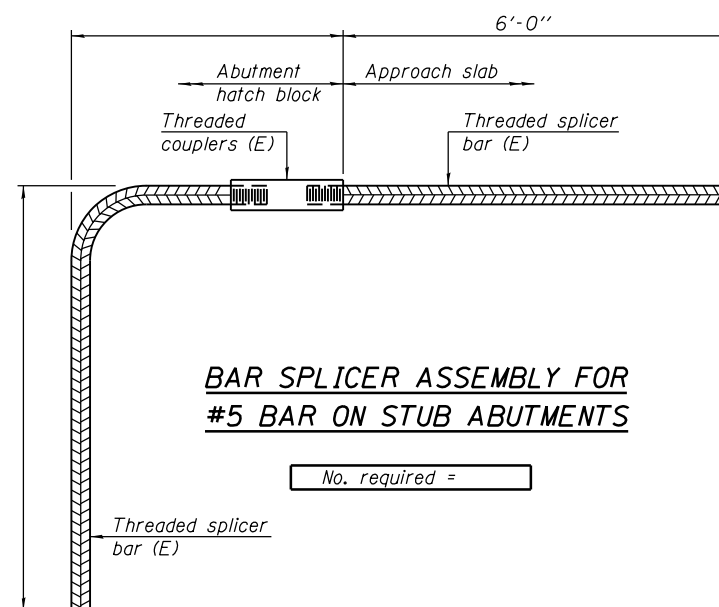
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required
Drilled Shaft, DS-1	#14	56
Drilled Shaft, DS-2	#14	56
Drilled Shaft, DS-3	#14	56
Drilled Shaft, DS-4	#14	56
Drilled Shaft, DS-5	#14	56
Drilled Shaft, DS-6	#14	56
Drilled Shaft, DS-7	#14	56
Drilled Shaft, DS-8	#14	56



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

0161806-60W30-511 - Bar Splicer

BSD-1

1-27-12



USER NAME = patelvl	DESIGNED - VP	REVISED
PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED
PLOT DATE = 12/16/2014	DRAWN - GF	REVISED
	CHECKED - ATB	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 016-1806

SHEET NO. RW-11 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	314A
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W30	

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
592.6	6-inch thick, ASPHALT --PAVEMENT--								--In-Situ Vane Shear, 25.5 feet-- --Su undis = 1502.2 psf-- --Su remold = 1087.8 psf-- --Sensitivity = 1.38--						
591.8	10-inch thick, CONCRETE --PAVEMENT--								--In-Situ Vane Shear, 28 feet-- --Su undis = 1292.5 psf-- --Su remold = 775.5 psf-- --Sensitivity = 1.67--						
590.1	Loose, brown GRAVELLY SAND --FILL--								--In-Situ Vane Shear, 30.5 feet-- --Su undis = 1292.5 psf-- --Su remold = 827.2 psf-- --Sensitivity = 1.56--						
	Loose to medium dense, brown, fine SAND --FILL--								--LL(%)=35, PL(%)=15-- --%Gravel=5.8-- --%Sand=17.8-- --%Silt=46.1-- --%Clay=30.3--35-- --A-6 (14)--						
584.1	Stiff to very stiff, gray SILTY CLAY, trace gravel --FILL--								--In-Situ Vane Shear, 35.5 feet-- --Su undis = 802.9 psf-- --Su remold = 543.9 psf-- --Sensitivity = 1.48--						
580.1	Very soft to medium stiff, gray SILTY CLAY, trace gravel								Dense, gray SILT						

GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling <u>05-23-2013</u>	Complete Drilling <u>05-29-2013</u>	While Drilling <input checked="" type="checkbox"/> Rotary wash	
Drilling Contractor <u>Wang Testing Services</u>	Drill Rig <u>CME-55 TMR</u>	At Completion of Drilling <input checked="" type="checkbox"/> unable to measure	
Driller <u>P&N</u>	Logger <u>F. Bozga</u>	Checked by <u>C. Marin</u>	
Drilling Method <u>2.25" SSA to 16', mud rotary thereafter, boring backfilled upon completion</u>		Time After Drilling <u>NA</u>	
		Depth to Water <input checked="" type="checkbox"/> NA	
The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.			

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
541.4	Hard, gray CLAY to SILTY CLAY, trace gravel								Hard, gray SILTY CLAY LOAM, trace gravel						
									--HARD DRILLING from 79.5' to 80' 82.5'-- --Possible cobbles--						
									Very dense, gray SILTY LOAM, trace gravel						
									--LL(%)=40, PL(%)=16-- --%Gravel=1.2-- --%Sand=5.3--65-- --%Silt=49.0-- --%Clay=44.5-- --A-6 (23)--						
									--HARD DRILLING from 93.5' to 98.5'-- --Possible cobbles--						
									--HARD DRILLING from 99' to 100'-- --Possible cobbles--						
									--AUGER REFUSAL--						
									Boring terminated at 100.00 ft						

GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling <u>05-23-2013</u>	Complete Drilling <u>05-29-2013</u>	While Drilling <input checked="" type="checkbox"/> Rotary wash	
Drilling Contractor <u>Wang Testing Services</u>	Drill Rig <u>CME-55 TMR</u>	At Completion of Drilling <input checked="" type="checkbox"/> unable to measure	
Driller <u>P&N</u>	Logger <u>F. Bozga</u>	Checked by <u>C. Marin</u>	
Drilling Method <u>2.25" SSA to 16', mud rotary thereafter, boring backfilled upon completion</u>		Time After Drilling <u>NA</u>	
		Depth to Water <input checked="" type="checkbox"/> NA	
The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.			

0161806-60W30-S12-Bor-1ng



USER NAME = <u>patelvi</u>	DESIGNED - <u>VP</u>	REVISED
PLOT SCALE = <u>N.T.S.</u>	CHECKED - <u>ATB</u>	REVISED
PLOT DATE = <u>12/16/2014</u>	DRAWN - <u>GF</u>	REVISED
	CHECKED - <u>ATB</u>	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS I
STRUCTURE NO. 016-1806

SHEET NO. RW-12 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	315
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

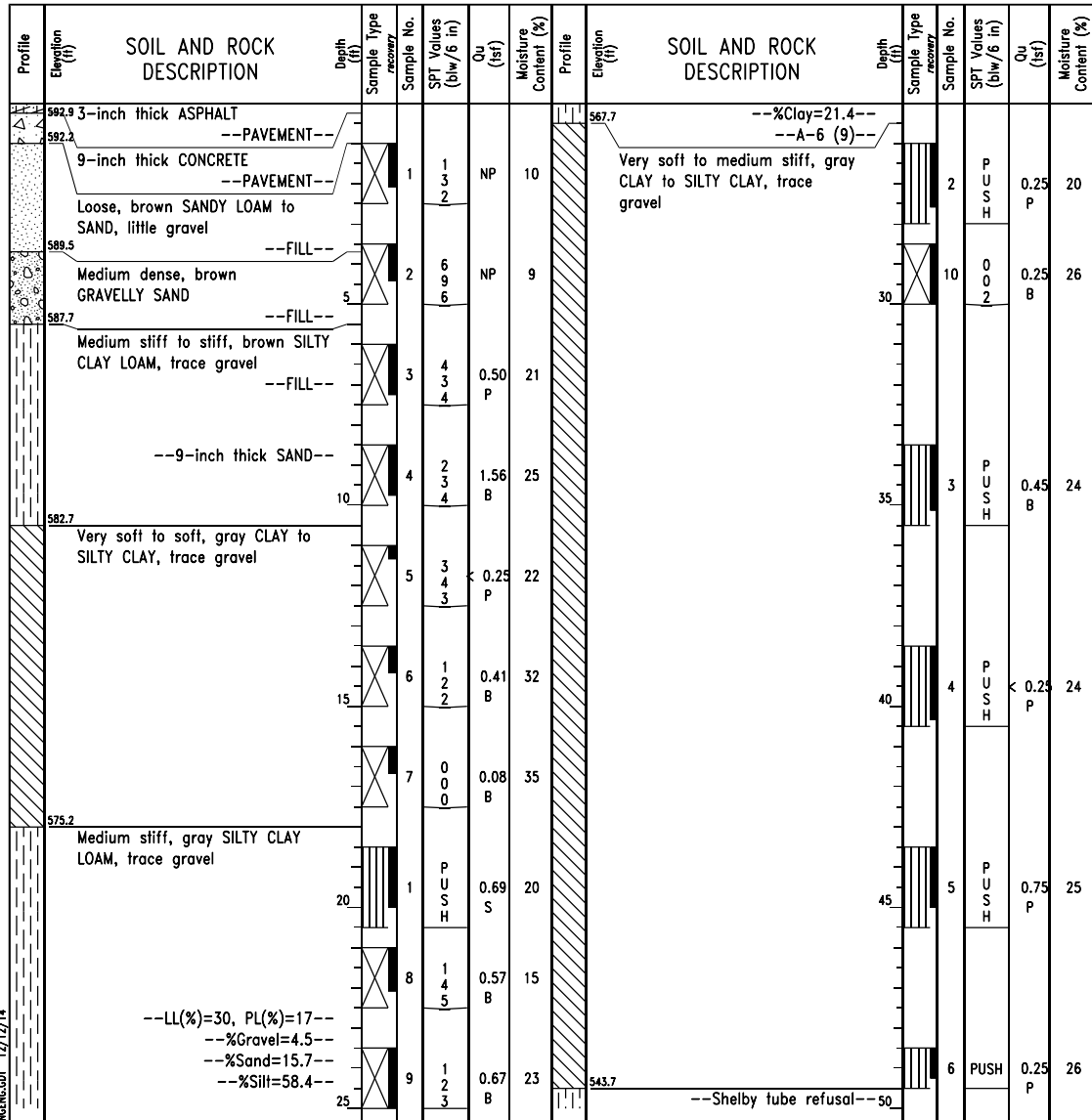
Wang Engineering
 wangeng@wangeng.com
 1145 N Main Street
 Lombard, IL 60148
 Telephone: 630 953-9928
 Fax: 630 953-9938

BORING LOG 17-RWB-01
 WEI Job No.: 1100-04-01

Datum: NAVD 88
 Elevation: 593.18 ft
 North: 1895577.80 ft
 East: 1171637.47 ft
 Station: 6248+83.69
 Offset: 120.0618 RT

Client: AECOM
 Project: Circle Interchange Reconstruction
 Location: Section 17, T39N, R14E of 3rd PM

Page 1 of 2



GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling	06-22-2014	Complete Drilling	06-22-2014
Drilling Contractor	Wang Testing Services	Drill Rig	D-50 TMR
Driller	P&J	Logger	S. Woods
Checked by	C. Marin	Time After Drilling	NA
Drilling Method	2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion	Depth to Water	NA
		The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	

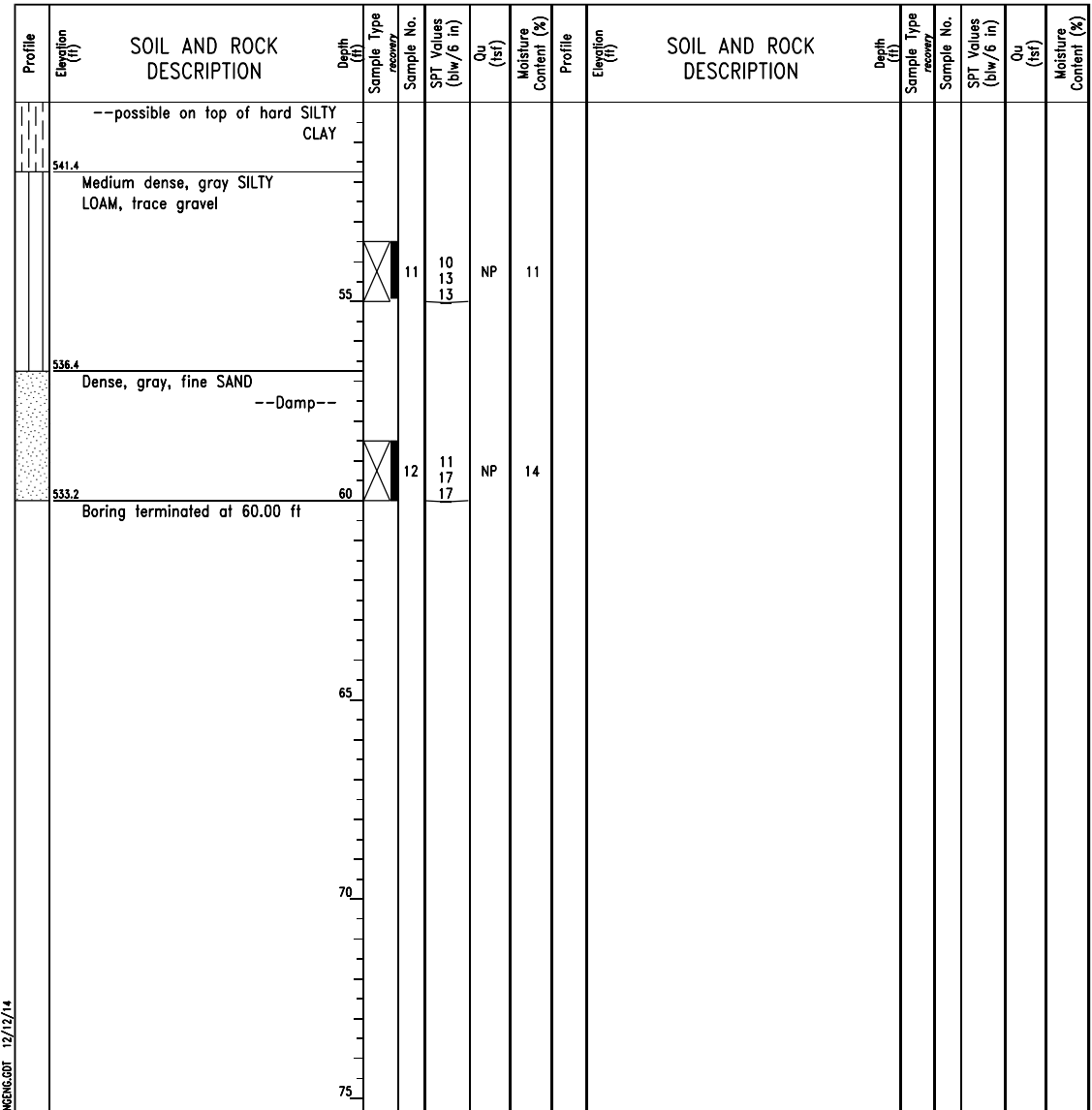
Wang Engineering
 wangeng@wangeng.com
 1145 N Main Street
 Lombard, IL 60148
 Telephone: 630 953-9928
 Fax: 630 953-9938

BORING LOG 17-RWB-01
 WEI Job No.: 1100-04-01

Datum: NAVD 88
 Elevation: 593.18 ft
 North: 1895577.80 ft
 East: 1171637.47 ft
 Station: 6248+83.69
 Offset: 120.0618 RT

Client: AECOM
 Project: Circle Interchange Reconstruction
 Location: Section 17, T39N, R14E of 3rd PM

Page 2 of 2



GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling	06-22-2014	Complete Drilling	06-22-2014
Drilling Contractor	Wang Testing Services	Drill Rig	D-50 TMR
Driller	P&J	Logger	S. Woods
Checked by	C. Marin	Time After Drilling	NA
Drilling Method	2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion	Depth to Water	NA
		The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	

0161806-60W30-S13-Bor-Ing



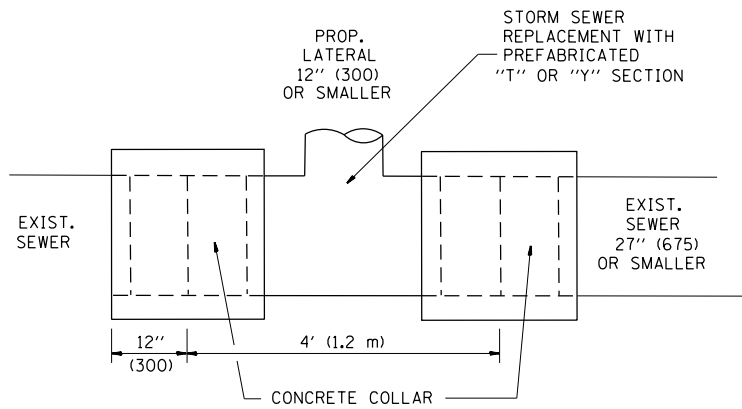
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		CHECKED -	ATB	REVISED
PLOT SCALE =	N.T.S.	DRAWN -	GF	REVISED
PLOT DATE =	12/16/2014	CHECKED -	ATB	REVISED

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BORING LOGS II
 STRUCTURE NO. 016-1806**

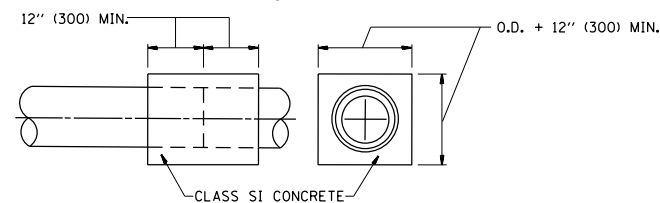
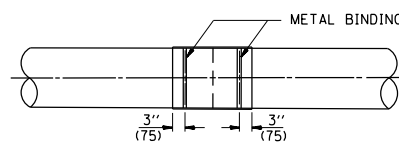
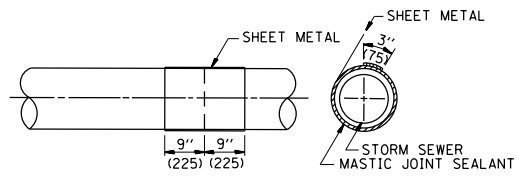
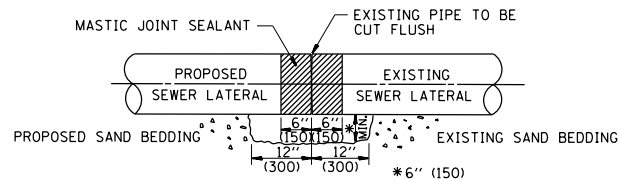
SHEET NO. RW-13 OF RW-13 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	316
CONTRACT NO.			60W30	
ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



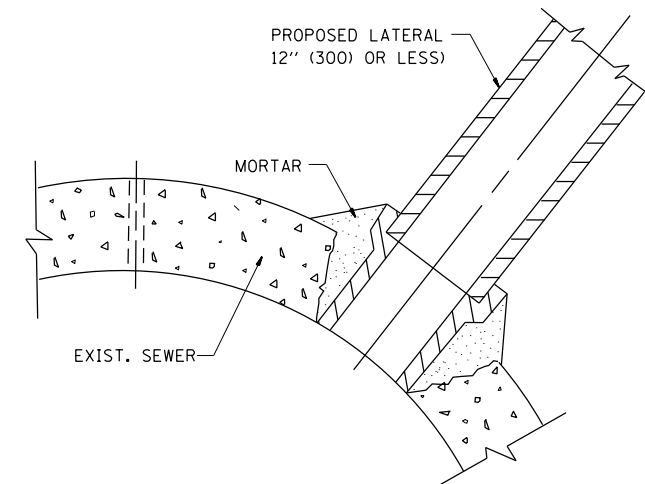
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: NONE SHEET OF SHEETS STA. TO STA.

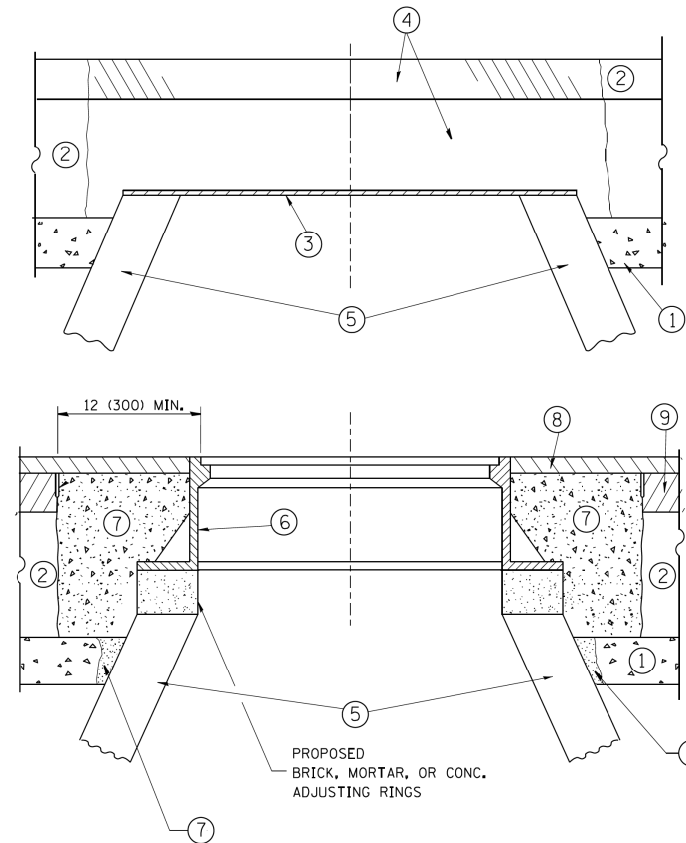
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	317
BD500-01 (BD-7)			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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PLOT SCALE = 50.000' / 1" =
PLOT DATE = 10/23/2014

DESIGNED - M. DE YONG	REVISED-M. DE YONG 05-08-92
DRAWN -	REVISED-R. SHAH 09-09-94
CHECKED -	REVISED-R. SHAH 10-25-94
DATE - 07-25-90	REVISED-R. SHAH 06-12-96

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CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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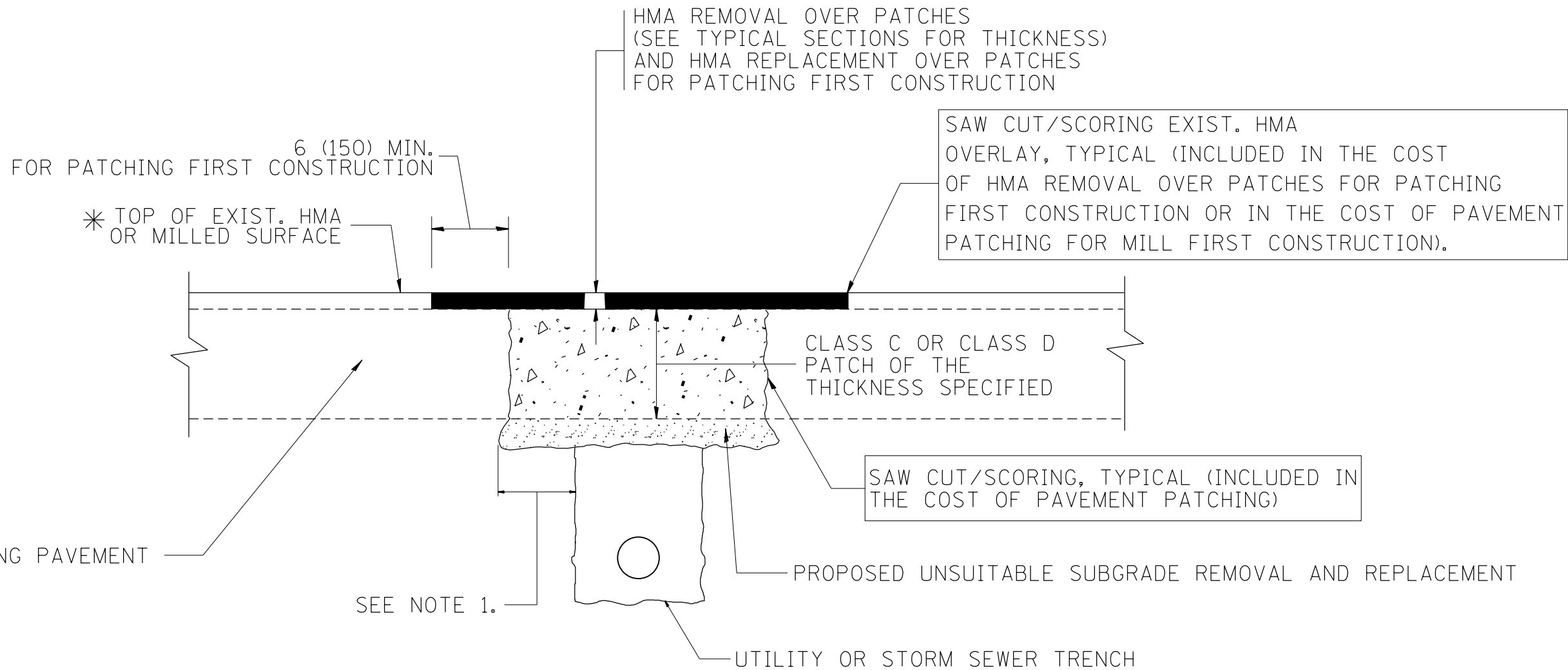


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USER NAME = dishevoz	DRAWN -	REVISED - R. BORO 01-01-0
PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - R. BORO 03-09-1
PLOT DATE = 10/23/2014	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	318
BD-08			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT SCALE = 50.000' / in.
PLOT DATE = 10/23/2014

DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
DRAWN -	REVISED - R. BORO 01-01-07
CHECKED -	REVISED - R. BORO 09-04-07
DATE - 10/24/2014	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

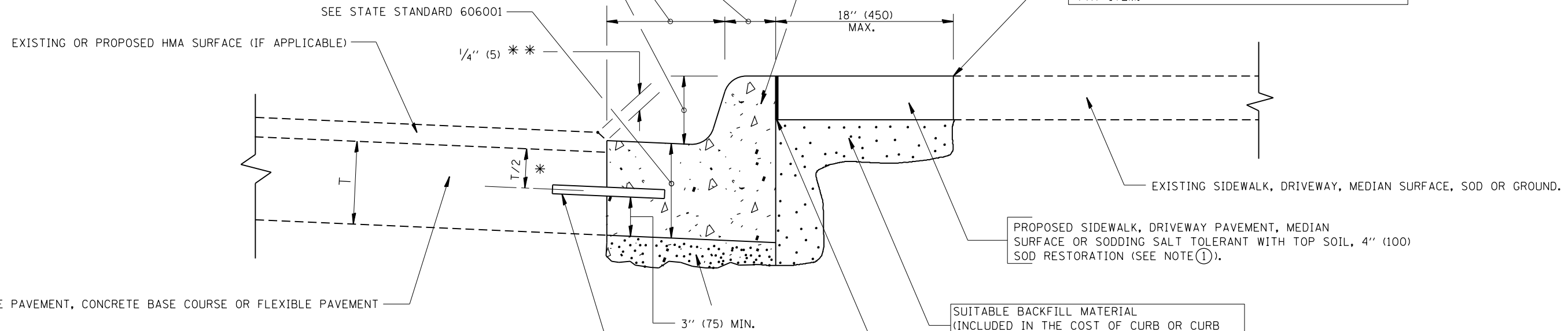
SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	319
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = dishveoz	DRAWN -	REVISED- A. ABBAS 03-21-97
PLOT SCALE = 50.000' / in.	CHECKED -	REVISED- M. GOMEZ 01-22-01
PLOT DATE = 10/23/2014	DATE - Q3-11-94	REVISED- R. BORO 12-15-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			
SCALE: NONE	SHEET	OF SHEETS	STA. TO STA.

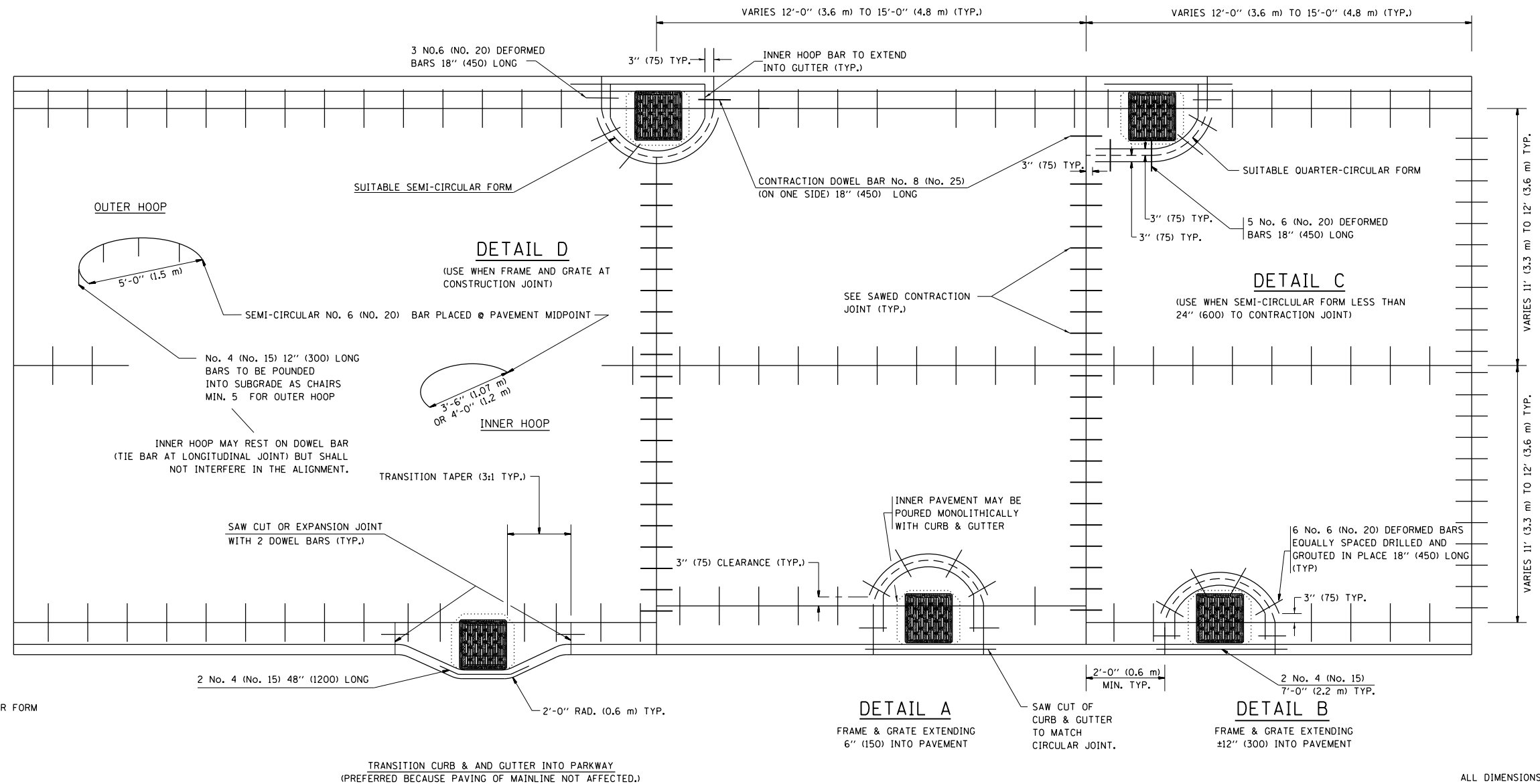
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	320
BD600-06 (BD-24)			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 8" (200)	3'-6" (1.1 m)	4'-0" (1.2 m)	5'-0" (1.5 m)
> 8" (200) TO 14" (360)	4'-0" (1.2 m)	4'-6" (1.4 m)	5'-0" (1.5 m)

DESIGNER NOTE:
THIS DETAIL IS TO BE USED
WHEN THE GUTTER FLAG IS
LESS THAN 24"

NOTES :

1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
2. TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT. EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 12" (300) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 2" (50) CLEARANCE.
6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

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D160W30-SHT-D1-Detail-07-(BD-48).dgn
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PLOT SCALE = 50.0000' / in.
PLOT DATE = 10/23/2014

DESIGNED - A. ABBAS
DRAWN - TOM MATOUSEK
CHECKED - A. ABBAS
DATE - 01-04-99

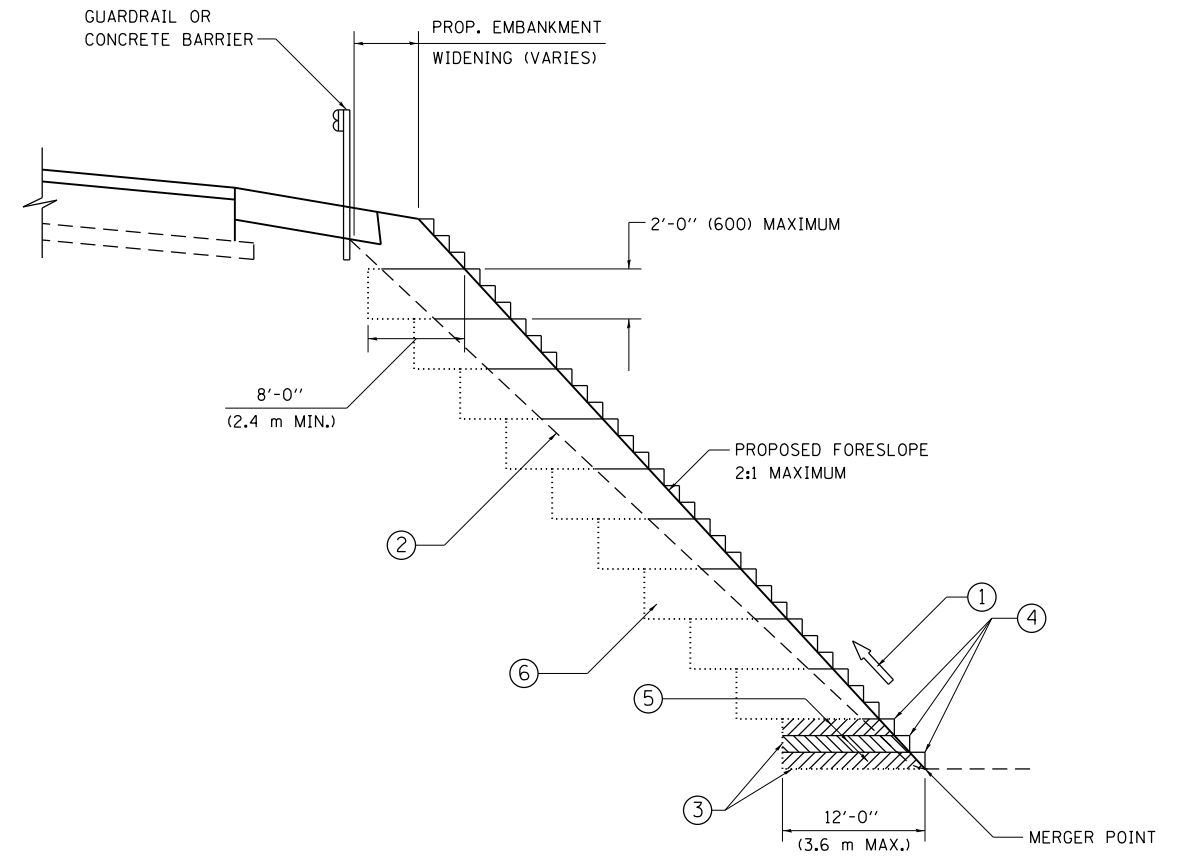
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REVISED-T. MATOUSEK 10-02-00
REVISED-T. MATOUSEK 04-25-02
REVISED-P. LAFLEUR 08-27-02

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PCC PAVEMENT ROUNDOUTS AT
CURB AND GUTTER

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	321
BD-48			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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DESIGNED -
 DRAWN - CADD
 CHECKED - S.E.B.
 DATE - 1 06-16-04

REVISED -
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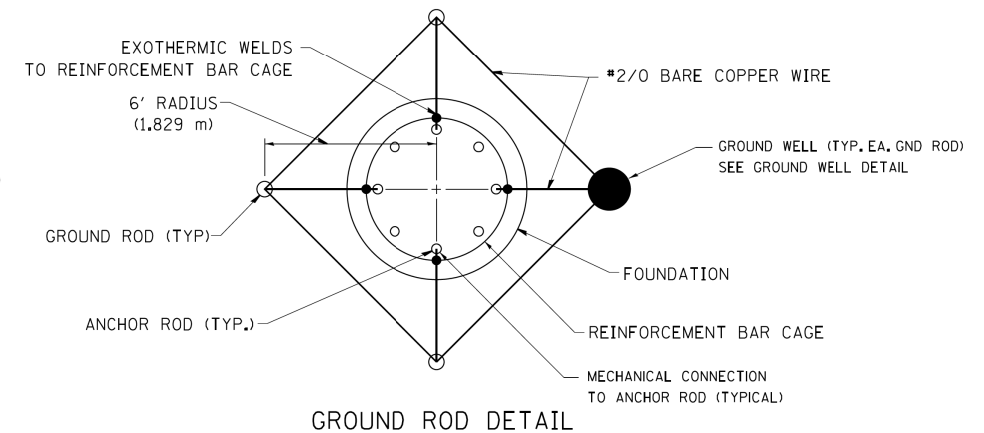
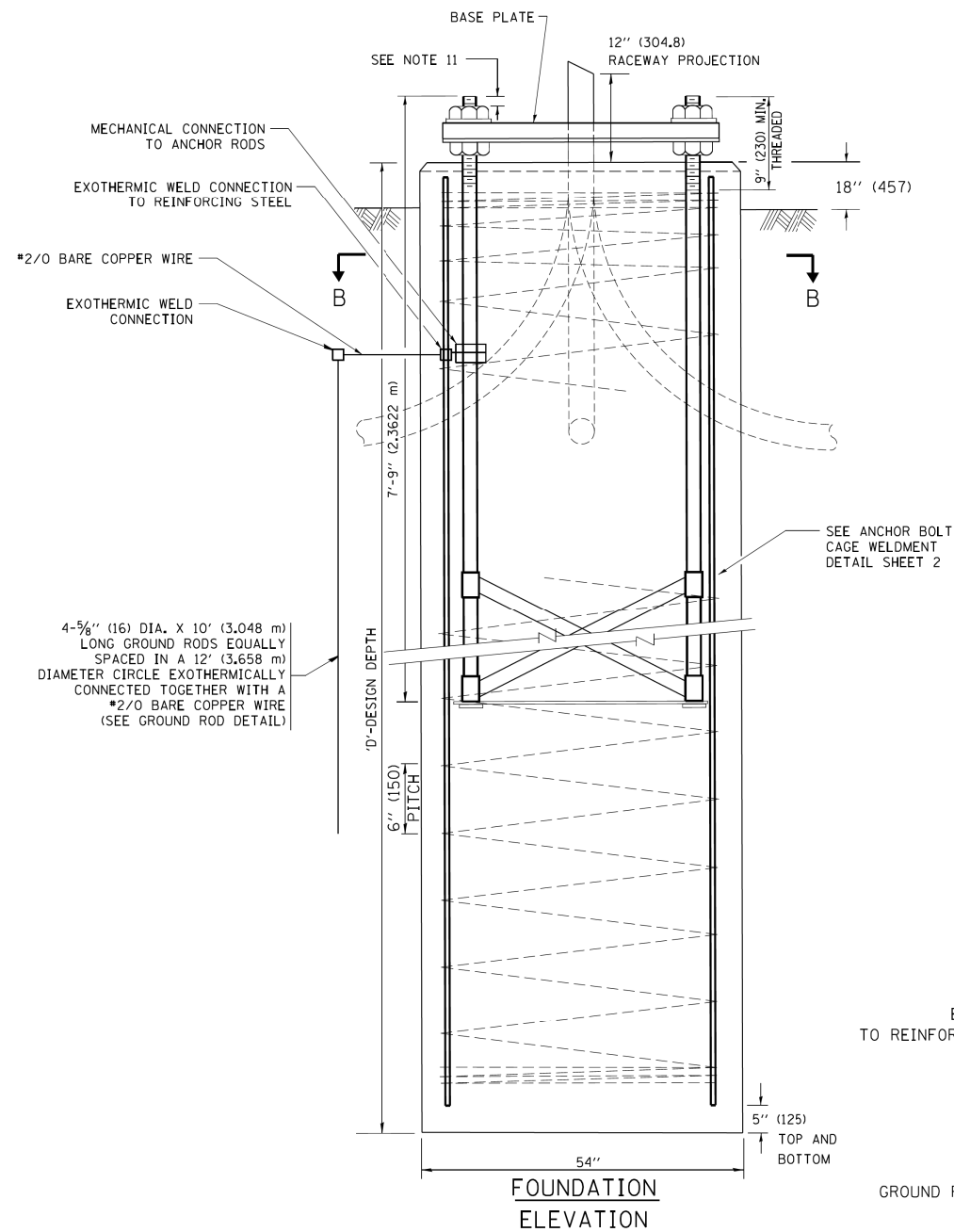
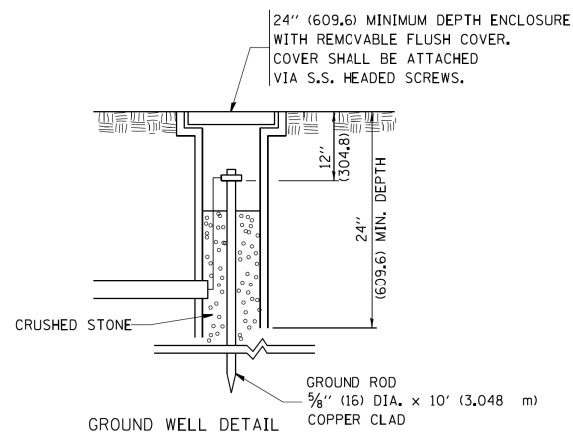
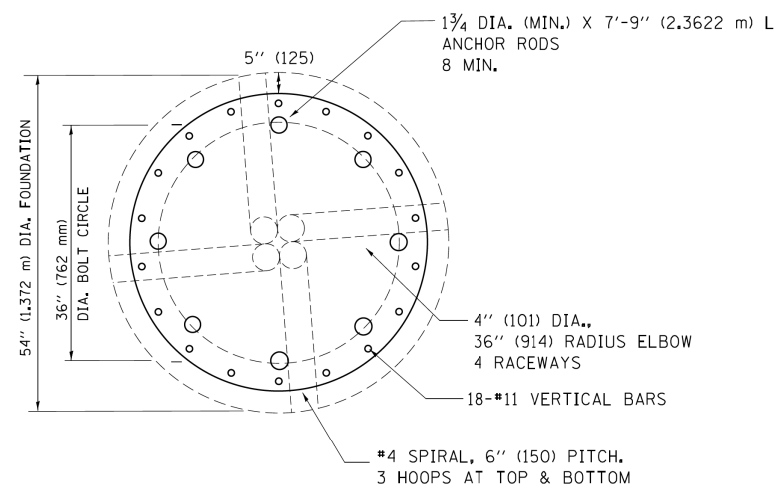
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BENCHING DETAIL
FOR EMBANKMENT WIDENING**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	322
BD-51			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SOIL CONSISTENCY		SHAFT LENGTH (D) TABLE			
		AVERAGE STRENGTH	LIGHT TOWER MOUNTING HEIGHT		
		0u In tsf (0u In kPa)	120 FT. (37 m)	130 FT. (40 m)	140 FT. (43 m)
SOFT		<0.5 (<50)	25'-0" (7.6 m)	26'-6" (8.0 m)	27'-6" (8.3 m)
	MEDIUM	0.5 TO 1 (50 TO 100)	20'-6" (6.2 m)	21'-6" (6.4 m)	22'-0" (6.7 m)
COHESIVE	STIFF	1 TO 2 (100 TO 200)	17'-6" (5.2 m)	18'-0" (5.4 m)	18'-6" (5.5 m)
	VERY STIFF	2 TO 4 (200 TO 400)	15'-0" (4.5 m)	15'-6" (4.6 m)	16'-0" (4.7 m)
	HARD	>4 (>400)	13'-6" (4.0 m)	13'-6" (4.1 m)	14'-0" (4.2 m)
		N In BLOWS/FT. (N In BLOWS/0.3m)			
	VERY LOOSE	<5 (<5)	19'-0" (6.3 m)	20'-0" (6.0 m)	20'-6" (6.2 m)
	LOOSE	5 TO 10 (5 TO 10)	17'-6" (5.7 m)	18'-0" (5.5 m)	18'-6" (5.6 m)
GRANULAR	MEDIUM	10 TO 25 (10 TO 25)	16'-6" (5.5 m)	17'-0" (5.2 m)	17'-6" (5.3 m)
	DENSE	25 TO 50 (25 TO 50)	15'-6" (5.2 m)	16'-6" (4.9 m)	16'-6" (5.0 m)
	VERY DENSE	>50 (>50)	15'-0" (4.5 m)	15'-6" (4.7 m)	16'-0" (4.8 m)



DESIGN NOTES

- (1) ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN
- (2) THE ANCHOR RODS SHALL BE VERTICAL NO ADJUSTMENT SHALL BE ALLOWED AFTER THE FOUNDATION IS PLACED.
- (3) THE GAP BETWEEN THE FOUNDATION AND THE BASE PLATE SHALL BE ENCLOSED WITH A STAINLESS STEEL SCREEN FASTENED WITH A STAINLESS STEEL BAND.
- (4) THE TOP OF THE FOUNDATION TO 18" (450) BELOW GRADE SHALL BE FORMED.
- (5) SURFACE WATER WILL NOT BE PERMITTED TO ENTER THE HOLE AND ALL WATER WHICH MAY HAVE INFILTRATED INTO THE HOLE SHALL BE REMOVED BEFORE PLACING CONCRETE.
- (6) THE LIGHT TOWER SHALL NOT BE ERECTED UNTIL AFTER THE CONCRETE HAS BEEN CURED ACCORDING TO ARTICLE 1020.13.
- (7) ANCHOR RODS SHALL BE STRAIGHT AND SHALL BE ACCORDING TO AASHTO M 314 OR ASTM F1554, GRADE 725 (GRADE 105) AND GALVANIZED ACCORDING TO ARTICLE 1006.9.
- (8) ANCHOR ROD INFORMATION SHALL BE SUBMITTED FOR APPROVAL AND SHALL BE FULLY COORDINATED FOR APPROVAL WITH TOWER MANUFACTURER REQUIREMENTS.
- (9) REINFORCEMENT BARS SHALL BE ACCORDING TO ARTICLE 1006.10
- (10) TWO ANCHOR RODS OPPOSITE EACH OTHER SHALL HAVE THE ANCHOR ROD THREADS PEENED AFTER NUTS ARE INSTALLED.
- (11) A MINIMUM OF THREE FULL THREADS SHALL REMAIN EXPOSED AFTER LIGHT TOWER IS INSTALLED.
- (12) ALL GROUNDING INDICATED IN THE PLANS SHALL BE INCLUDED IN THE COST OF THE LIGHT TOWER FOUNDATION AND SHALL NOT BE PAID FOR SEPARATELY.
- (13) CUT NUTS, OR JAM NUTS, ARE NOT ALLOWED
- (14) ANCHOR ROD QUANTITY, DIAMETER, AND LENGTH SHALL BE DETERMINED BY THE TOWER MANUFACTURER AND APPROVED BY THE ENGINEER. EACH FOUNDATION SHALL HAVE A MINIMUM OF 8 ANCHOR RODS.
- (15) COORDINATE THE ROD CIRCLE DIAMETER OF THE TOWER WITH THE DIAMETER OF THE ANCHOR ROD CAGE.
- (16) THE FOUNDATION SHALL BE POURED MONOLITHICALLY AND SHALL HAVE NO CONSTRUCTION JOINTS.

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 DRAWN -
 CHECKED -
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REVISED - R. TOMSONS 09-02-10
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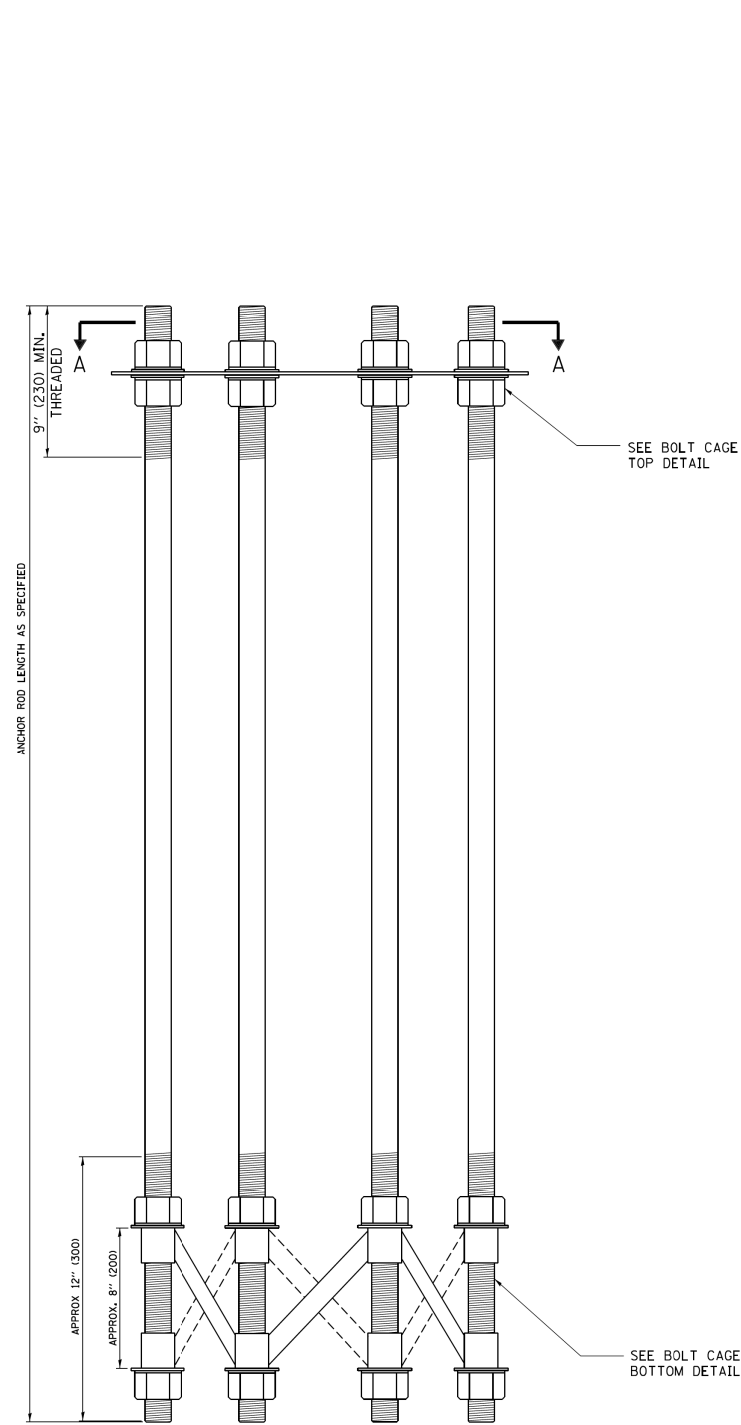
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HIGH MAST LIGHT TOWER
 120 FT TO 140 FT FOUNDATION DETAIL

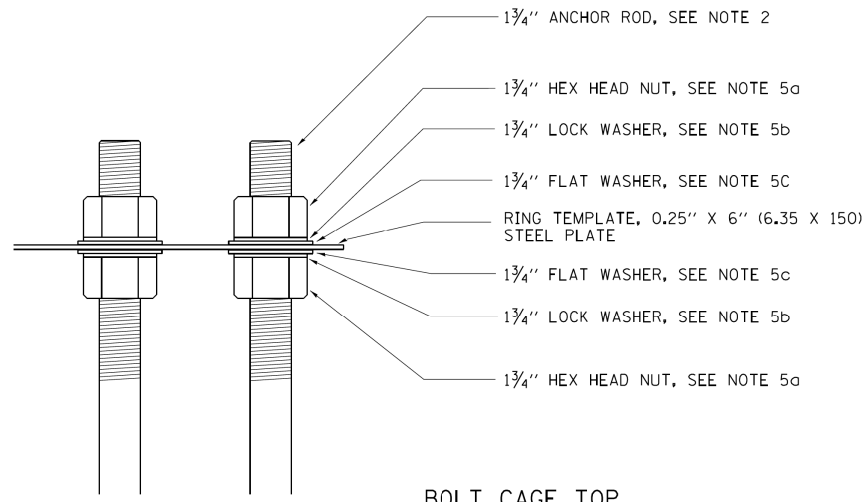
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	323
BE-506		CONTRACT NO. 60W30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

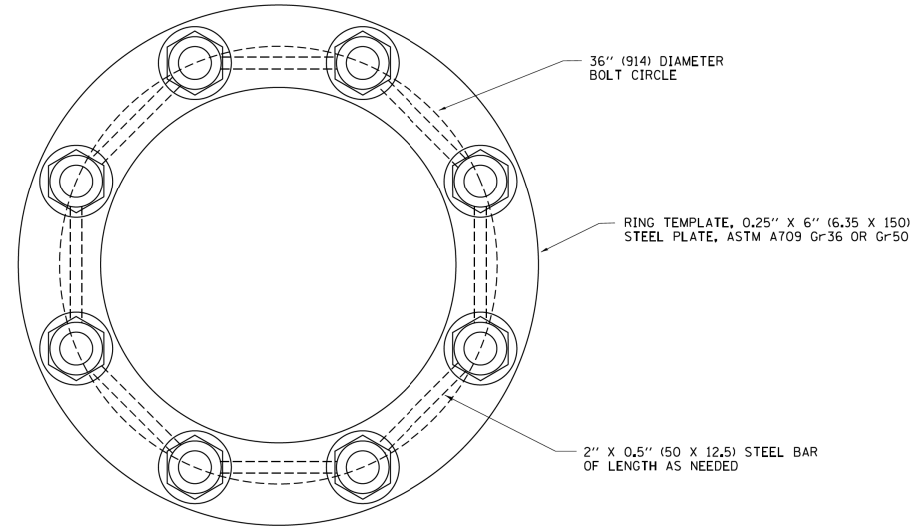
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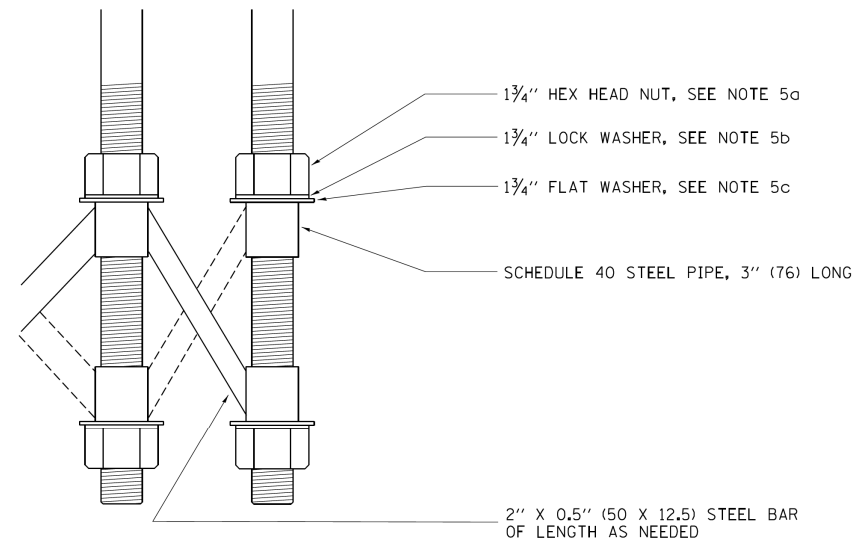
ANCHOR BOLT CAGE



BOLT CAGE TOP



SECTION A-A



BOLT CAGE BOTTOM

NOTES

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN
2. ANCHOR RODS SHALL BE STRAIGHT AND SHALL BE ACCORDING TO AASHTO M 314 OR ASTM F1554, GRADE 725 (GRADE 105) AND GALVANIZED ACCORDING TO ARTICLE 1006.09.
3. ANCHOR ROD INFORMATION SHALL BE SUBMITTED FOR APPROVAL AND SHALL BE FULLY COORDINATED WITH TOWER MANUFACTURERS REQUIREMENTS.
4. CUT NUTS, OR JAM NUTS, ARE NOT ALLOWED
5. ANCHOR ROD CAGE HARDWARE SHALL BE IN ACCORDANCE WITH THE FOLLOWING:
 - a) 1 3/4" HEX HEAD NUTS
AASHTO M291, GRADE C, C3, D ,DH OR DH3
HOT DIPPED GALVANIZED AASHTO M 232
 - b) 1 3/4" HELICAL LOCK WASHERS
ANSI/ASME B18.21.1
I.D. 1.758 - 1.778
O.D. 2.596 MAX.
WIDTH 0.383 MIN.
THICKNESS 0.469 MIN.
HARDNESS 26-45 ROCKWELL C
HOT DIPPED GALVANIZED AASHTO M232
 - c) 1 3/4" FLAT WASHERS
AASHTO M293
O.D. 3.25
I.D. 1.875
THICKNESS 0.16 - 0.25
HARDNESS 26-45 ROCKWELL C.
HOT DIPPED GALVANIZED AASHTO M232
6. THE SHAFT LENGTHS SHALL BE BASED ON SOIL BORINGS IN THE PLANS AND OR A DETERMINATION OF SOIL CONDITIONS BY THE ENGINEER.
7. ALL FOUNDATION REINFORCEMENT STEEL SHALL BE EPOXY COATED.
8. THE FOUNDATION SHALL BE POURED MONOLITHICALLY AND SHALL HAVE NO CONSTRUCTION JOINTS.
9. ANCHOR RODS AND ALL ASSOCIATED HARDWARE ARE SHOWN AS MINIMUMS. SIZING SHALL BE DETERMINED BY THE TOWER MANUFACTURER AND APPROVED BY THE ENGINEER. EACH FOUNDATION SHALL HAVE A MINIMUM OF 8 ANCHOR RODS.



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DESIGNED - R. TOMSONS
 DRAWN -
 CHECKED -
 DATE -

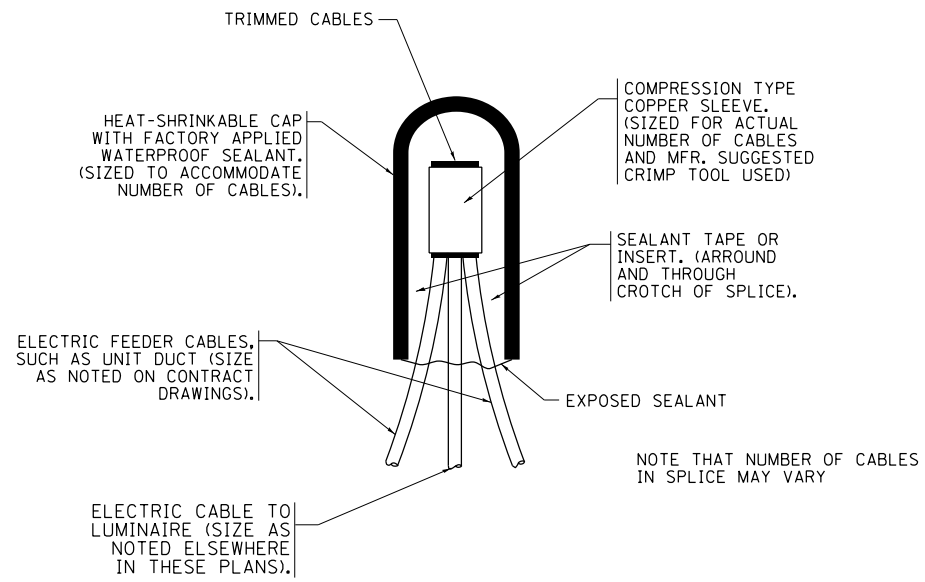
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 REVISED - R. TOMSONS 02-27-13
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**HIGH MAST LIGHT TOWER
 120 FT TO 140 FT FOUNDATION DETAIL**

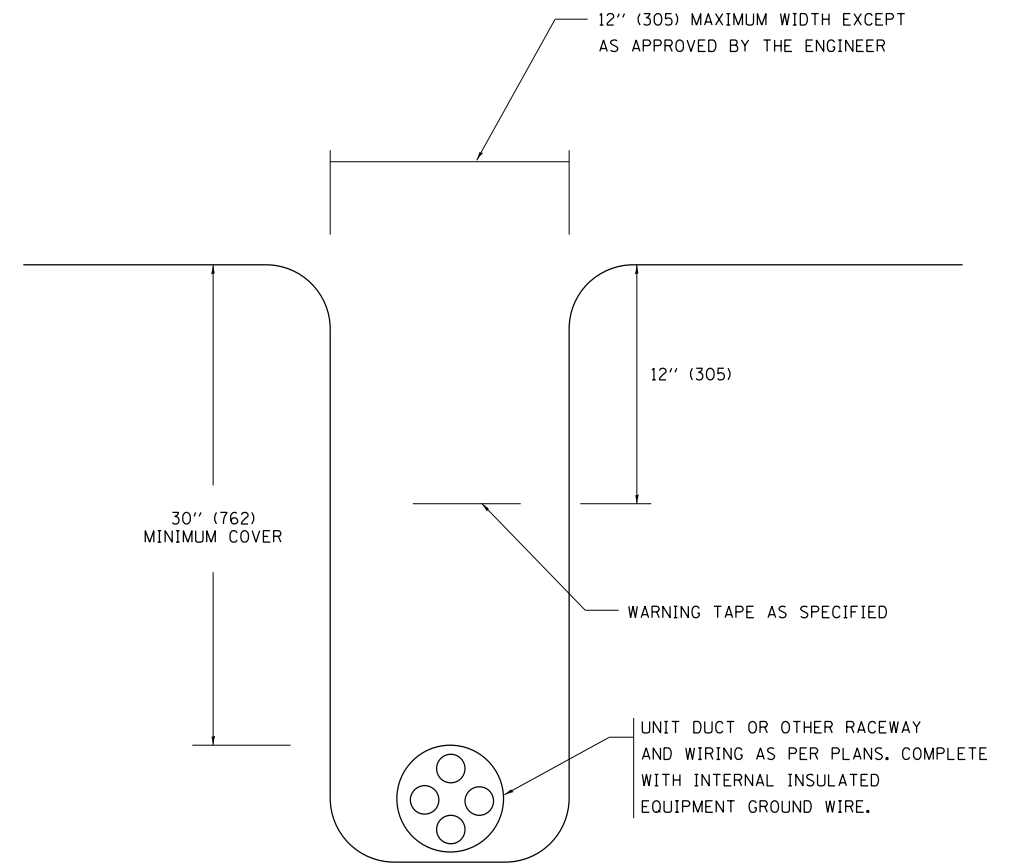
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BE-506		CONTRACT NO. 60W30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



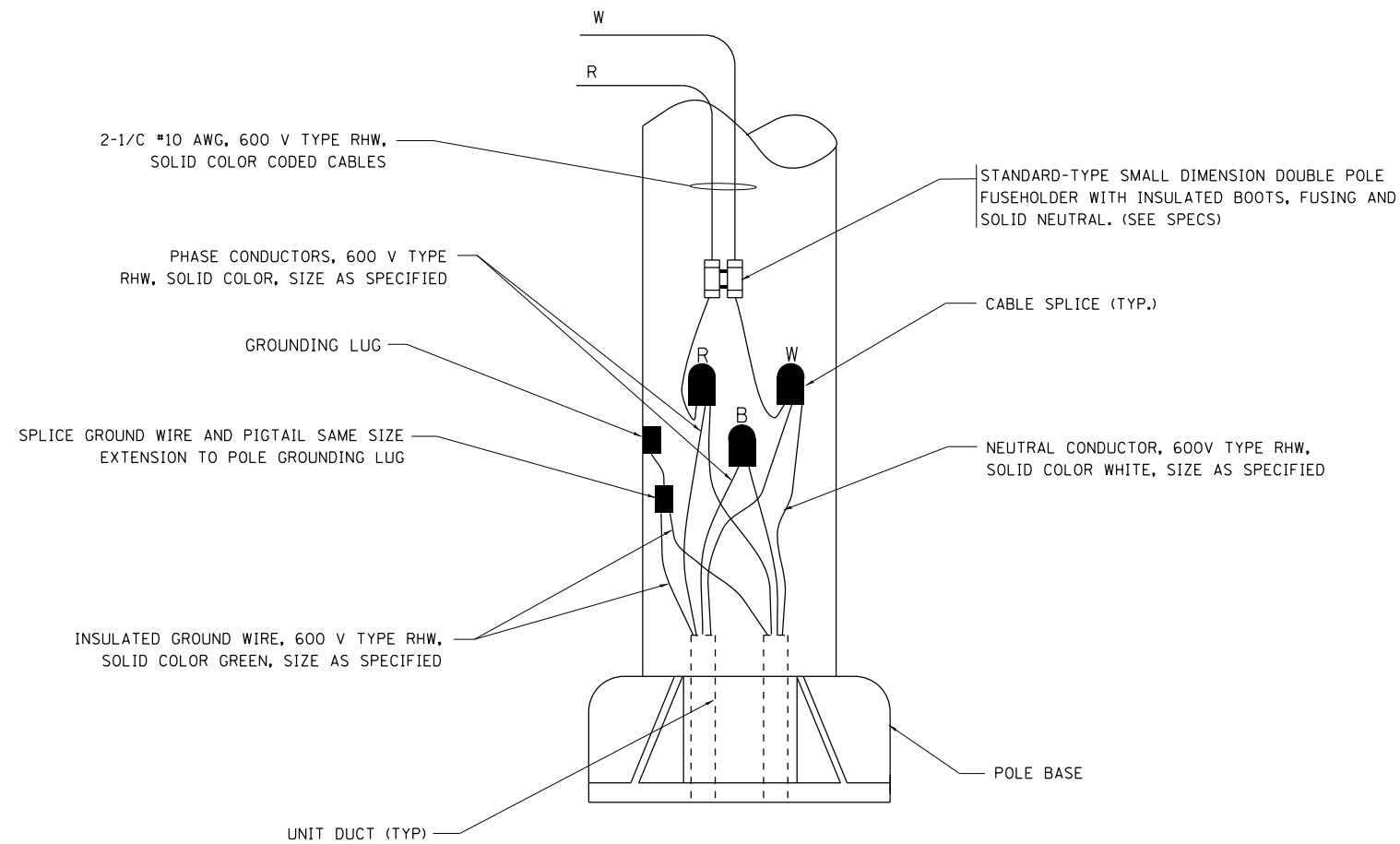
TYPICAL SPLICE DETAIL

N.T.S.



TYPICAL WIRING IN TRENCH DETAIL

N.T.S.



POLE WIRING DETAIL

N.T.S.

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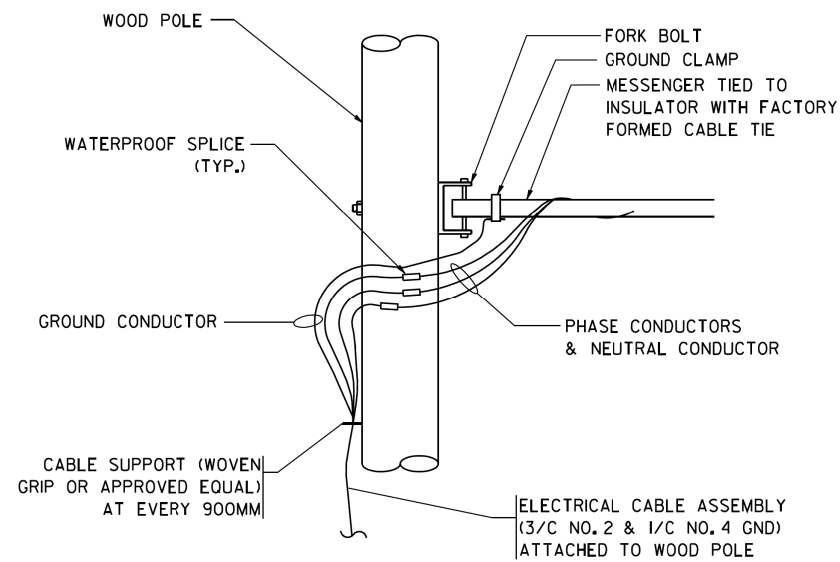
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DRAWN -	REVISD -
CHECKED -	REVISD -
DATE - 10/24/2014	REVISD -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

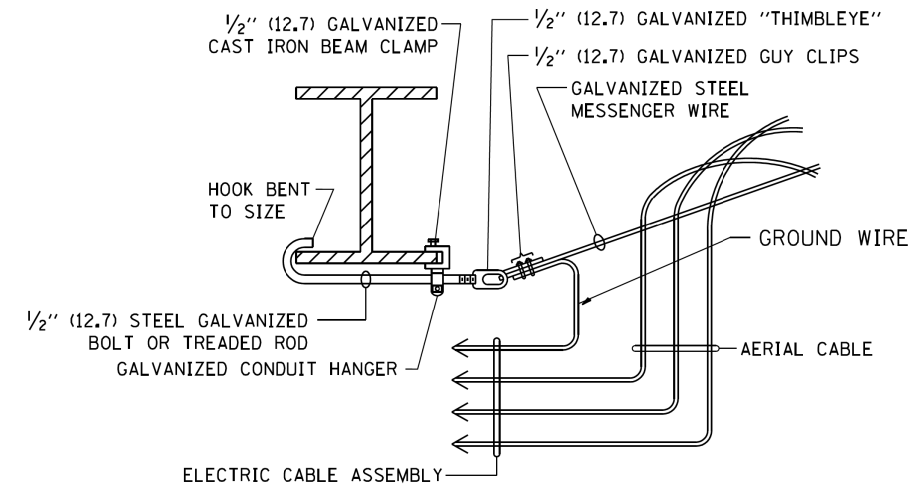
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SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	325
BE-702		CONTRACT NO. 60W30		
ILLINOIS FED. AID PROJECT				



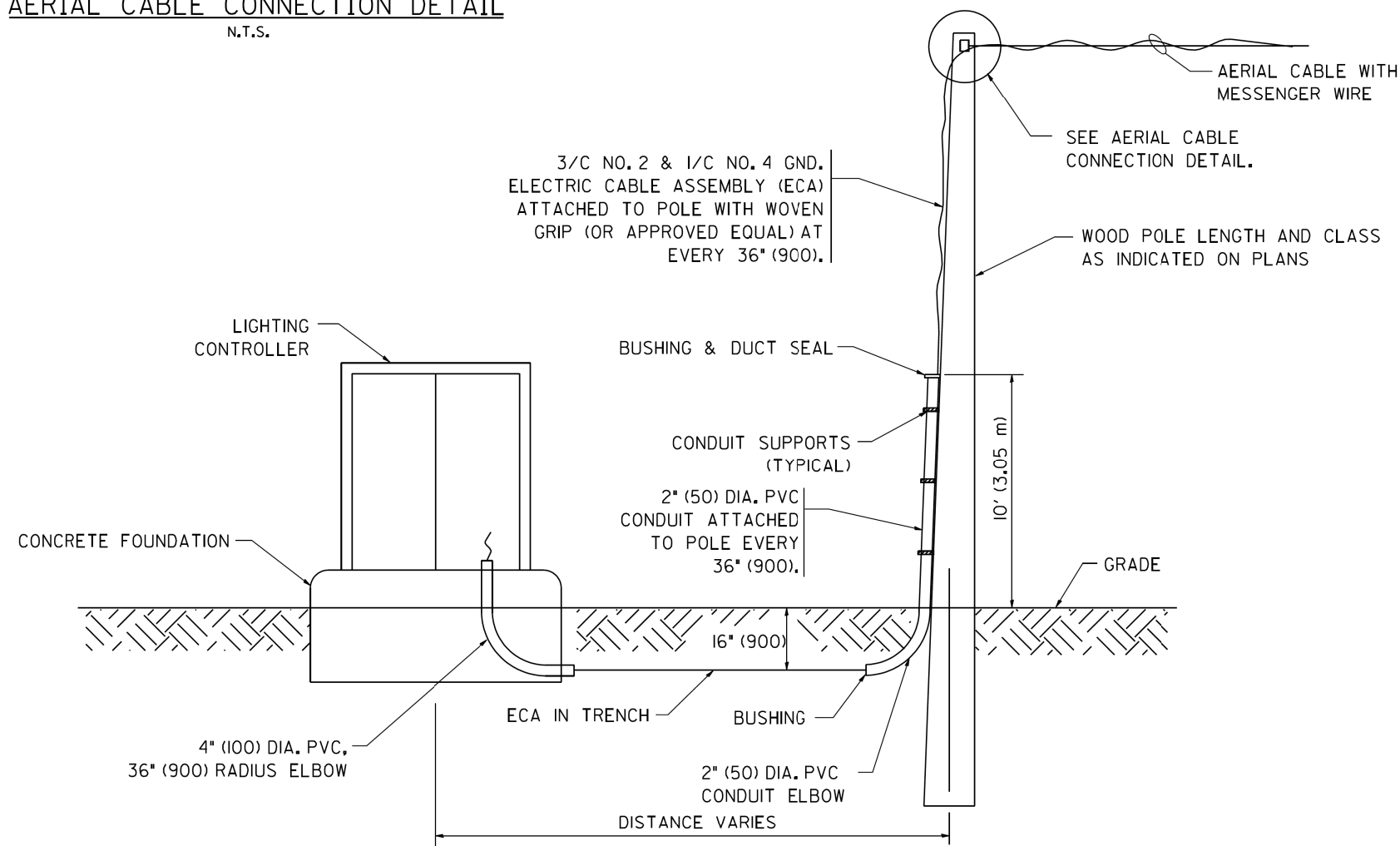
AERIAL CABLE CONNECTION DETAIL
N.T.S.



AERIAL CABLE ATTACHED TO STRUCTURE
NOT TO SCALE

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. SEE PROPOSED LIGHTING PLAN FOR CONDUIT, CABLE AND ROUTING.
3. THE CONTRACTOR SHALL PROVIDE INTERMEDIATE SUPPORTS TO MAINTAIN MINIMUM CLEARANCES. REFER TO AERIAL AERIAL CABLE ATTACHED TO STRUCTURE DETAIL.
4. COST OF SPLICES AND MOUNTING HARDWARE SHALL BE INCLUDED IN THE UNIT PRICE FOR AERIAL CABLE.



WOOD POLE TO LIGHTING CONTROLLER WIRING CONNECTION DETAIL
N.T.S.

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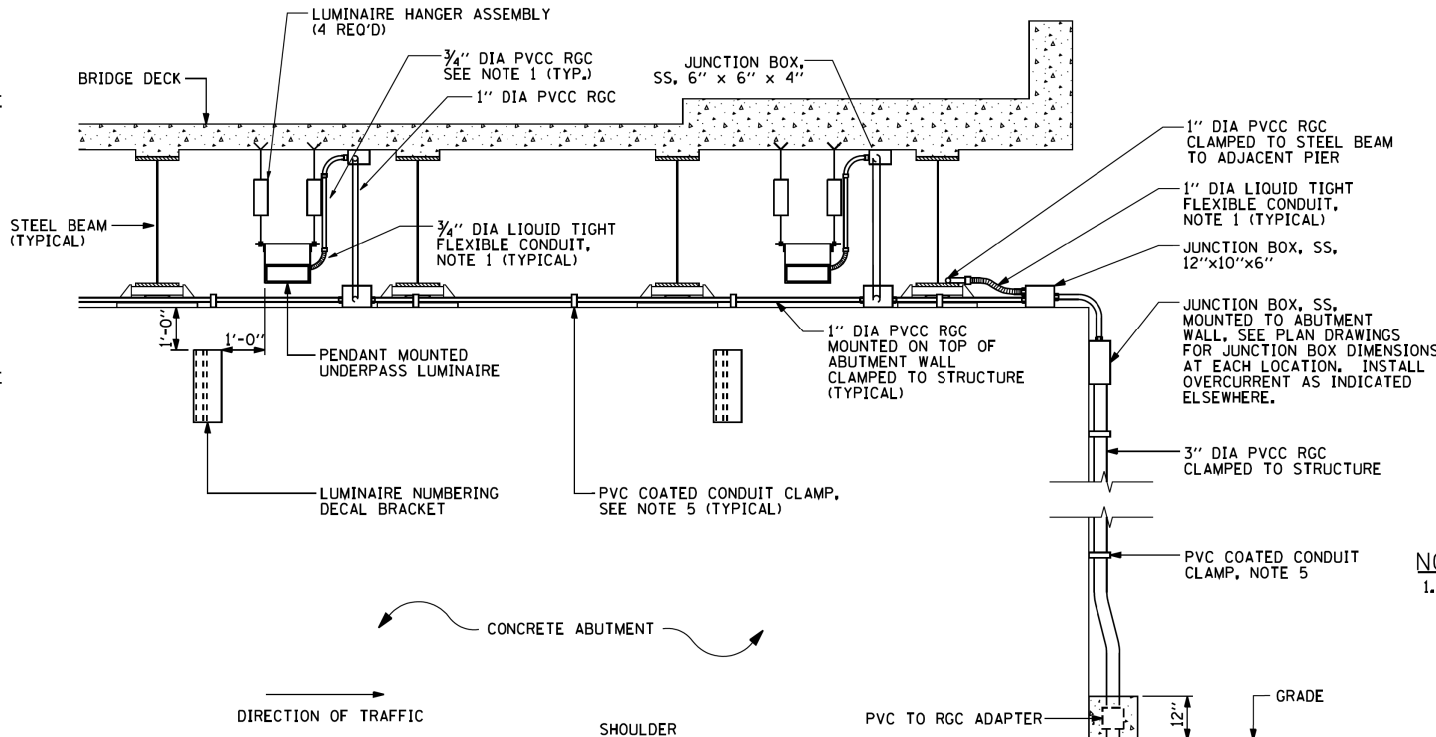
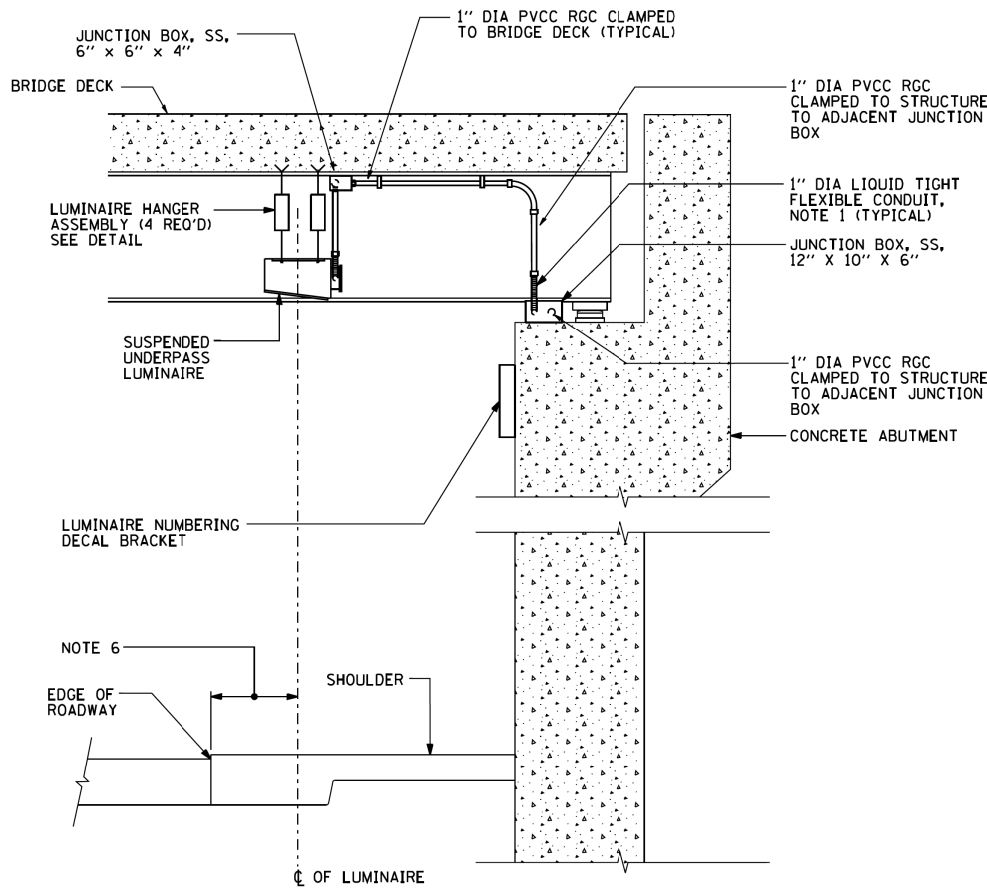
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

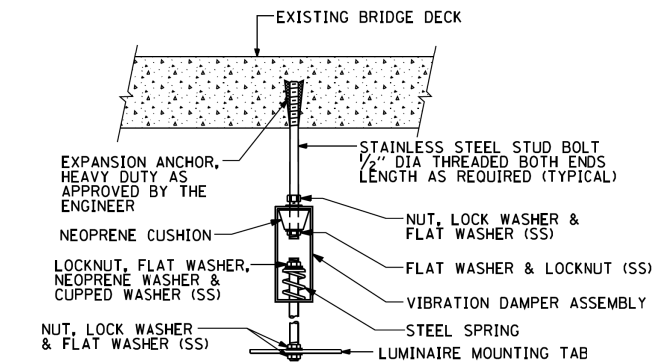
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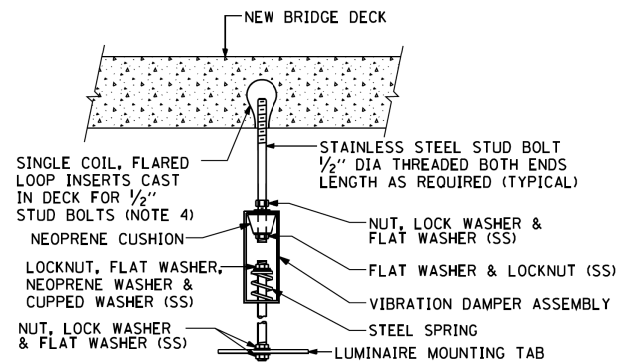
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90/94	2013-012R	COOK	385	326
BE-801			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES:**
- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT 3/4" DIA. CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE COST OF UNDERPASS LUMINAIRE INSTALLATION.
 - SEE UNDERPASS LIGHTING PLANS FOR INSTALLATION LOCATION OF UNDERPASS LIGHTING LUMINAIRES.
 - THE CONTRACTOR SHALL USE APPROVED SINGLE COIL FLARED LOOP INSERTS WHEN SUSPENDED MOUNTING AN UNDERPASS LUMINAIRE TO A NEW BRIDGE DECK. THE FLARED LOOP INSERTS MUST BE CAST INTO THE CONCRETE DECK. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND COORDINATING THE INSERT LOCATIONS FOR MOUNTING THE UNDERPASS LIGHTING SYSTEM AS SHOWN ON THE PLANS WITH THE BRIDGE DECK CONTRACTOR. SEE DETAIL.
 - THE UNDERPASS LUMINAIRE HANGER ASSEMBLY COMPLETE WITH HEAVY DUTY ANCHORS/INSERTS AND ALL APPLICABLE HARDWARE SHALL BE INCLUDED IN THE COST OF THE UNDERPASS LUMINAIRE PAY ITEM.
 - SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
 - ALL UNDERPASS LUMINAIRES MUST BE CENTERED IN THE BEAM SPACE AS INDICATED ON THE PLANS UNLESS OTHERWISE DIRECTED BY THE ENGR. LUMINAIRE SETBACK SHALL BE AS INDICATED IN PLANS FOR EACH SPECIFIC UNDERPASS.
 - THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
 - ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

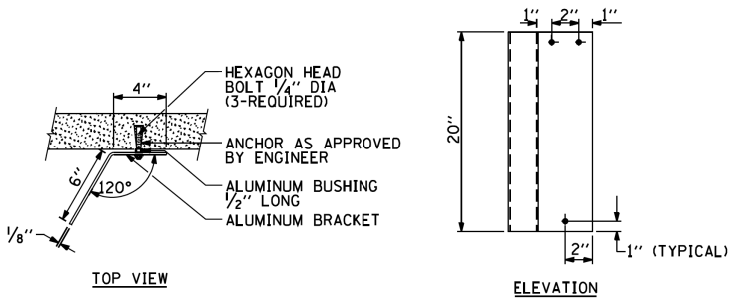


EXISTING BRIDGE DECK INSTALLATION



NEW BRIDGE DECK INSTALLATION

TYPICAL LUMINAIRE HANGER ASSEMBLY DETAILS



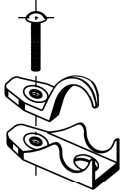
TOP VIEW

ELEVATION

LUMINAIRE NUMBERING DECAL BRACKET NOT TO SCALE



PVC COATED CONDUIT BEAM CLAMP NOT TO SCALE



PVC COATED CONDUIT CLAMP NOT TO SCALE

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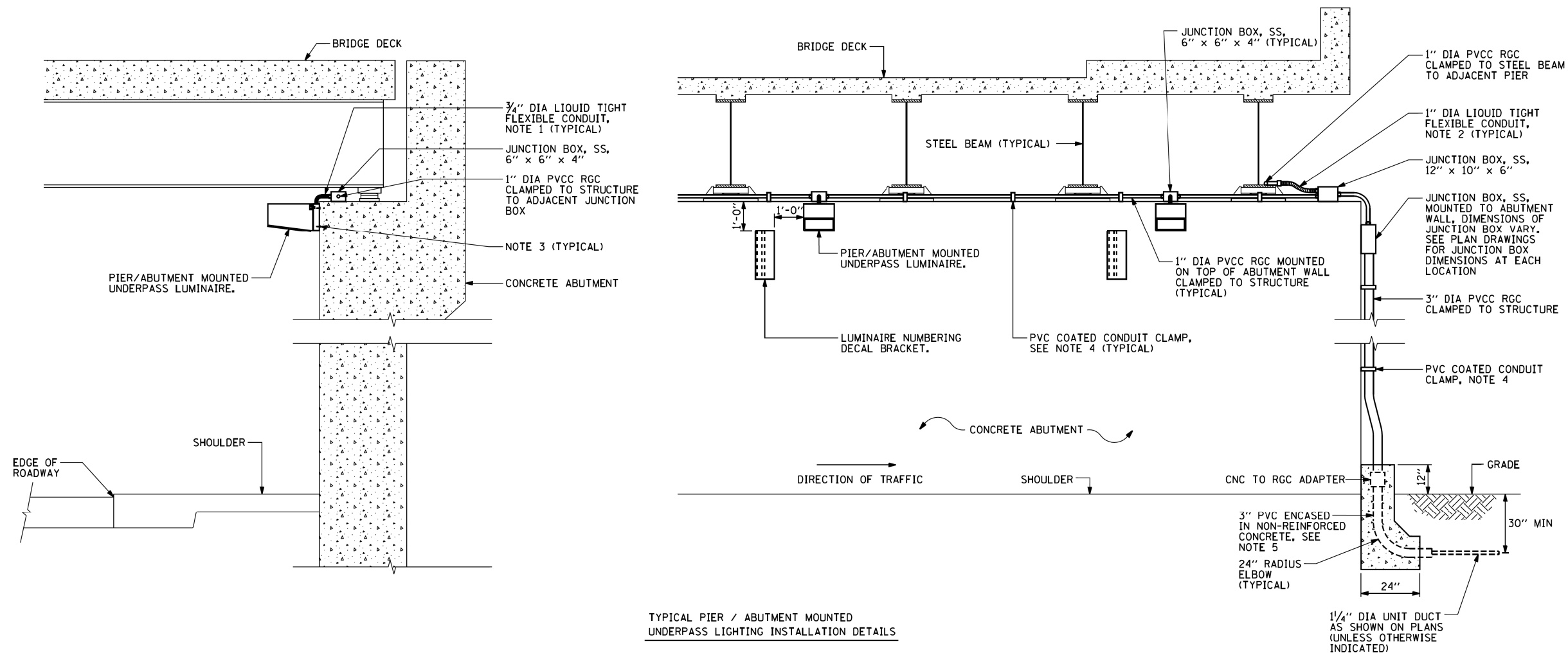
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DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

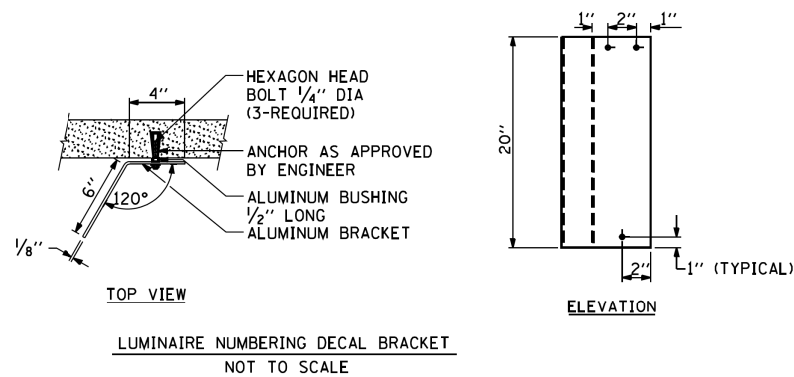
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUSPENDED MOUNT UNDERPASS LUMINAIRE INSTALLATION DETAILS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	327
BE-900		CONTRACT NO. 60W30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TYPICAL PIER / ABUTMENT MOUNTED UNDERPASS LIGHTING INSTALLATION DETAILS



LUMINAIRE NUMBERING DECAL BRACKET NOT TO SCALE



PVC COATED CONDUIT BEAM CLAMP NOT TO SCALE



PVC COATED CONDUIT CLAMP NOT TO SCALE

NOTES:

- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN, PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT THE COST OF THE 3/4" DIA. RIGID STEEL CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE LUMINAIRE INSTALLATION.
- UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL. MOUNTING HEIGHT OF 1" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
- EXPANSION ANCHOR, POWDER ACTUATED FASTENERS WILL NOT BE ALLOWED. EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS.
- SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM.
- THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
- ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

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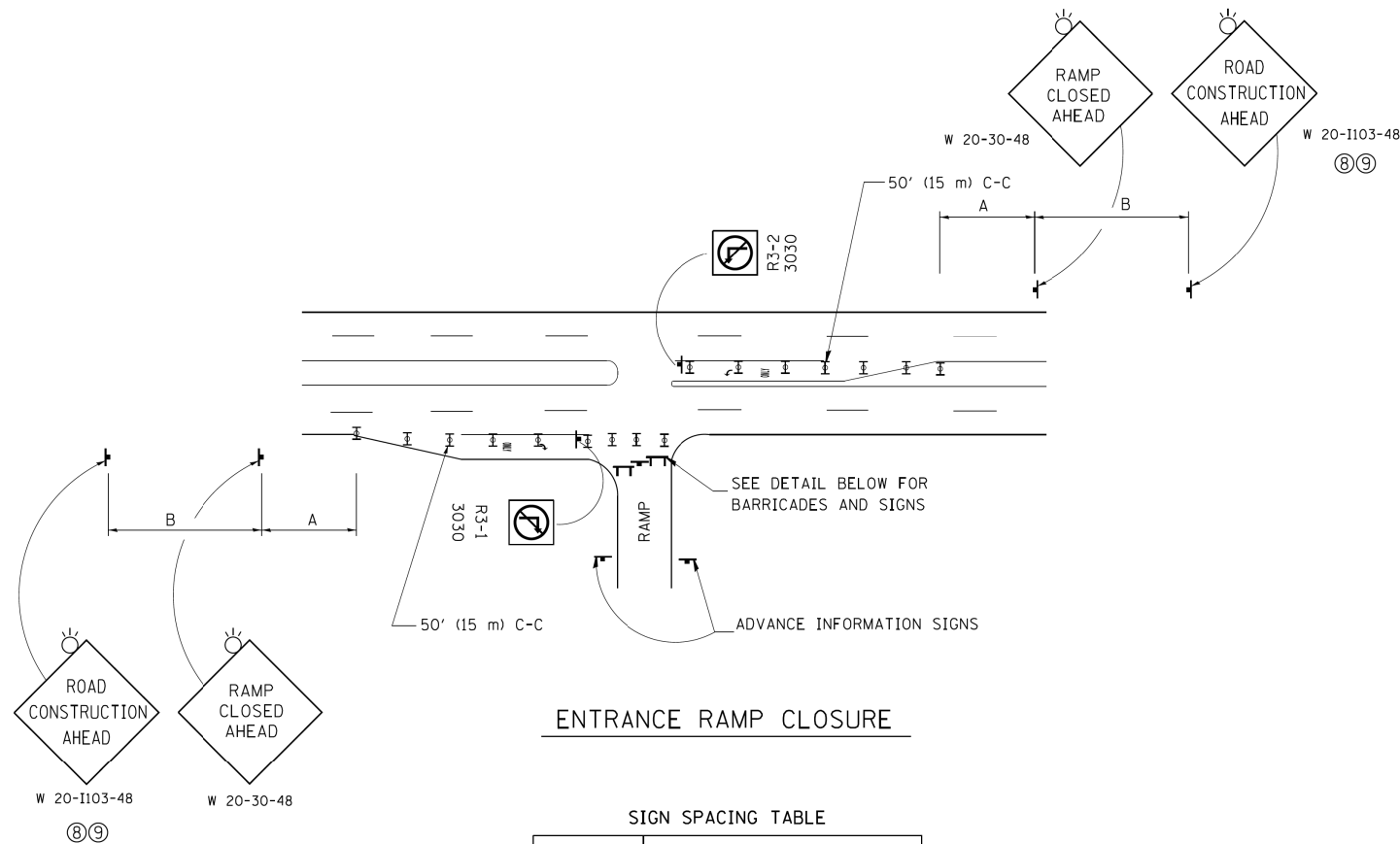
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CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PIER / ABUTMENT MOUNTED UNDERPASS
 LUMINAIRE INSTALLATION DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BE-902		CONTRACT NO. 60W30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

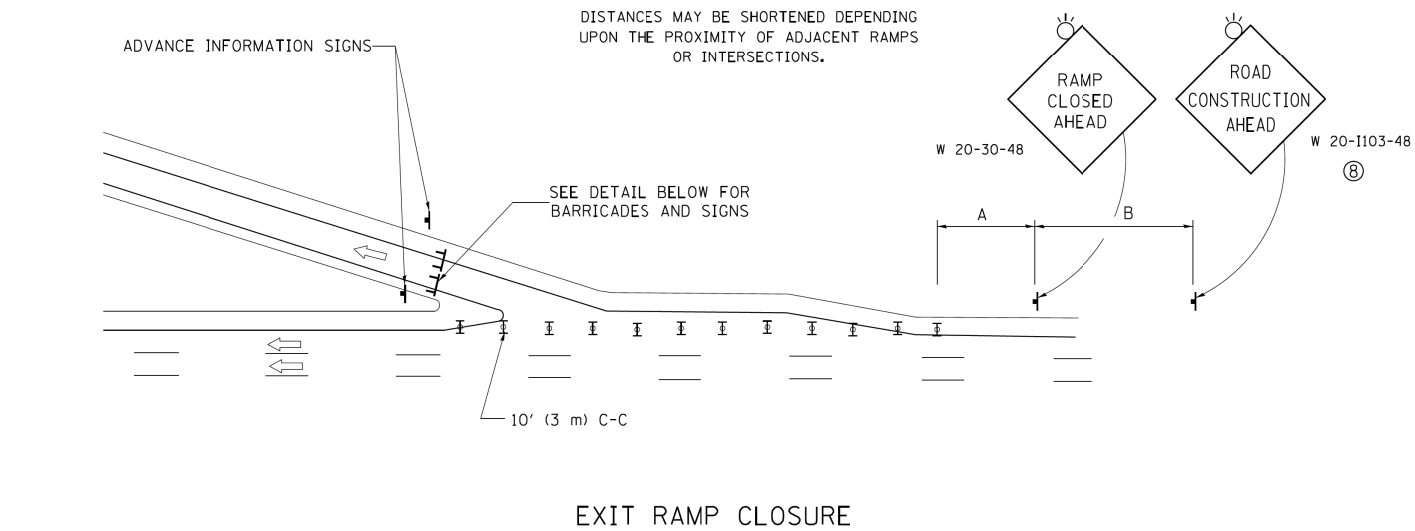


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

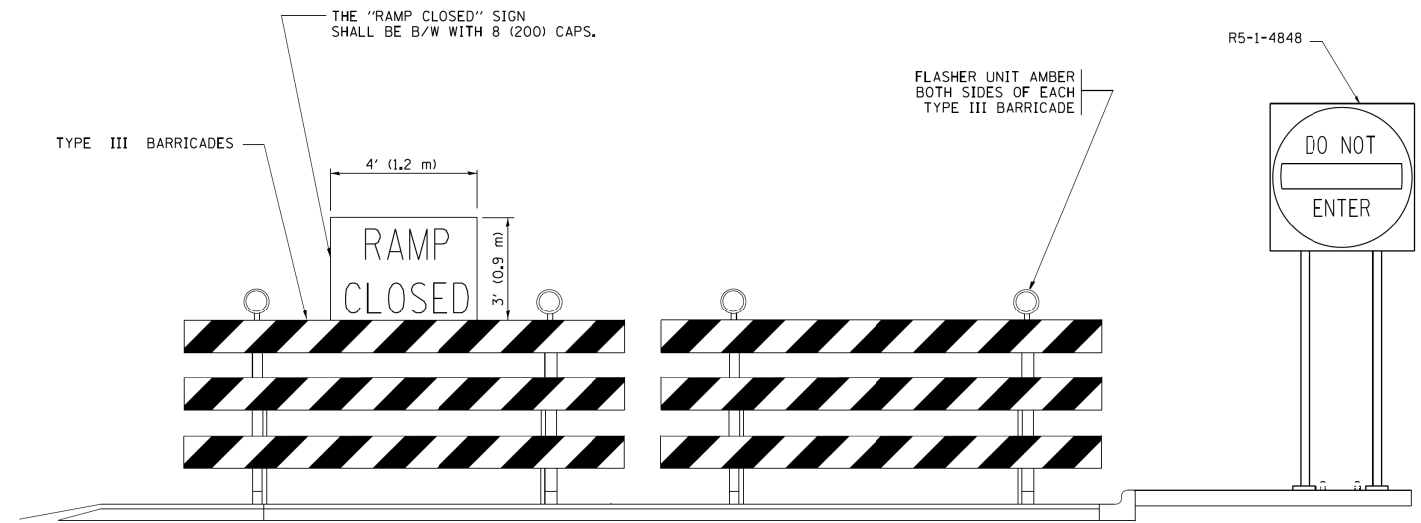
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

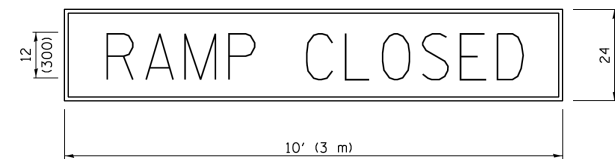
SYMBOLS

- ▬ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ▬ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

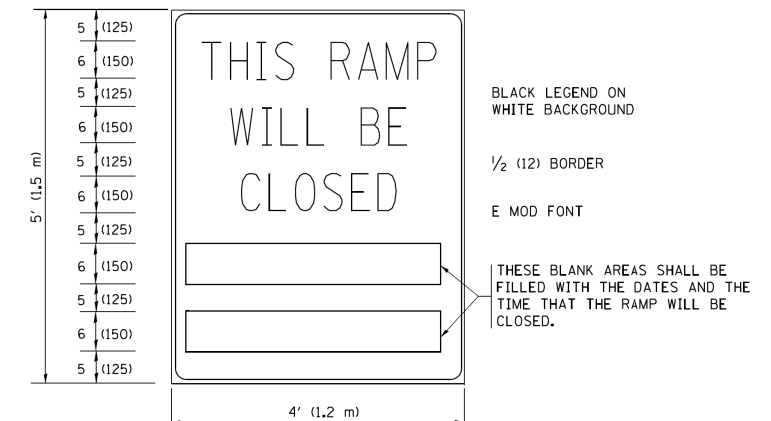
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND
1/2 (12) BORDER
E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT SCALE = 50.000' / in.
PLOT DATE = 10/23/2014

DESIGNED - DWS
DRAWN -
CHECKED -
DATE - 10-23-83

REVISED - JAF 02-06
REVISED - SPB 01-07
REVISED - SPB 12-09
REVISED - MD 06-13

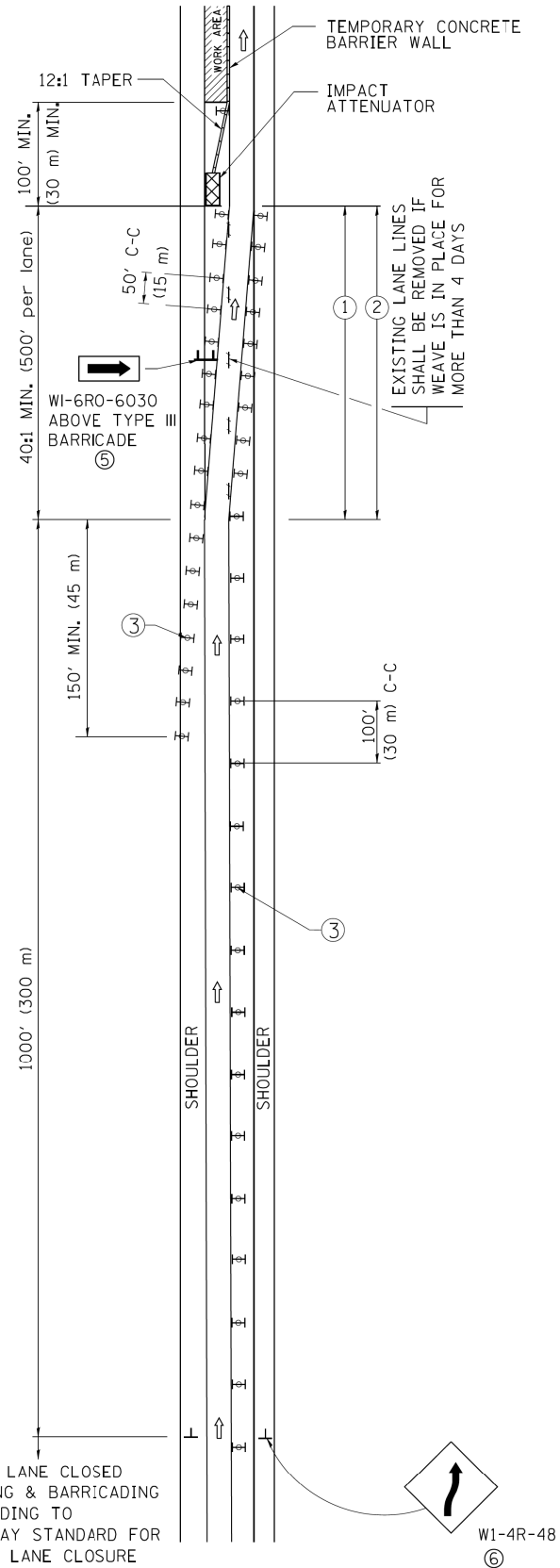
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

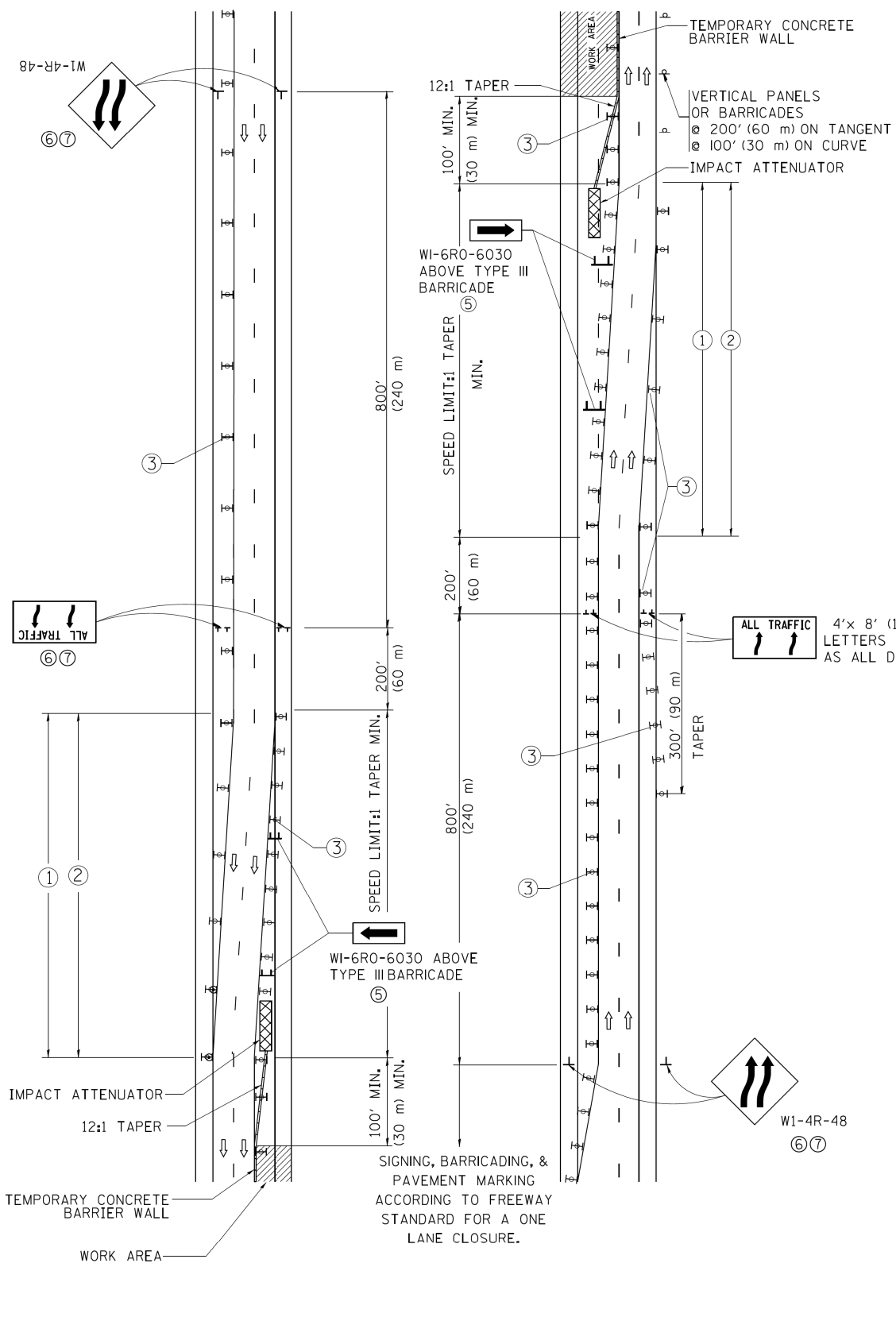
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	329
TC-08			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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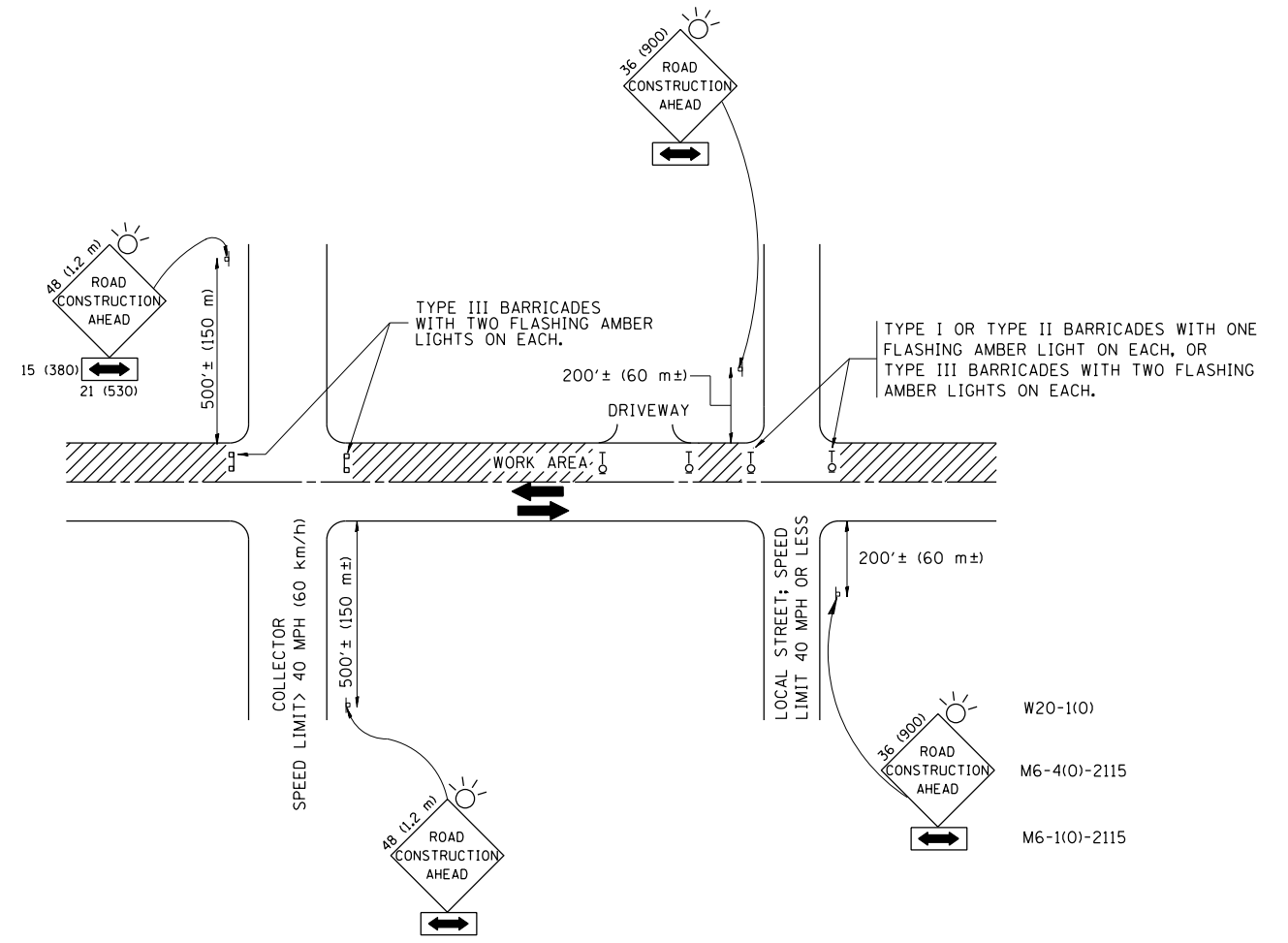
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 REVISED - SPB 01-07
 REVISED - SPB 12-09
 REVISED - MD 06-13

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR
 FREEWAY SINGLE & MULTI-LANE WEAVE

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	330
TC-09			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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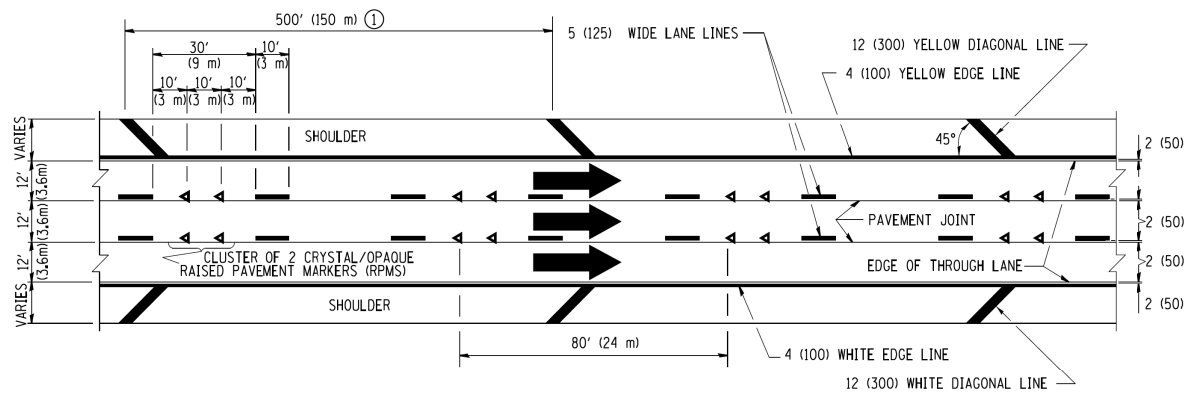


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

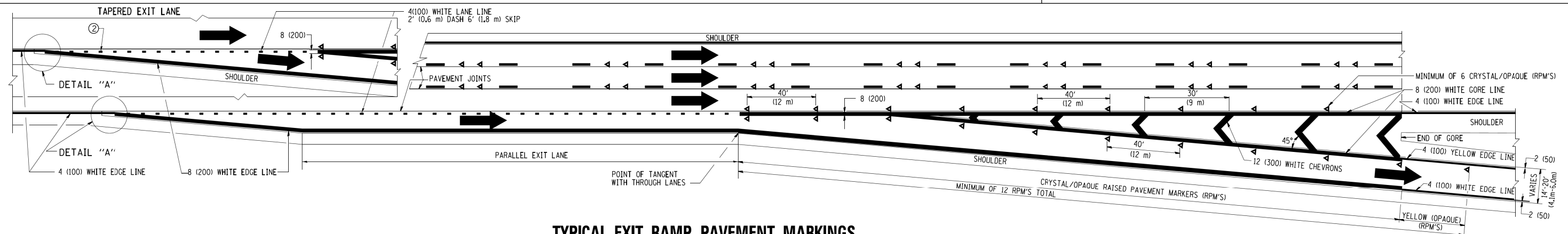
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

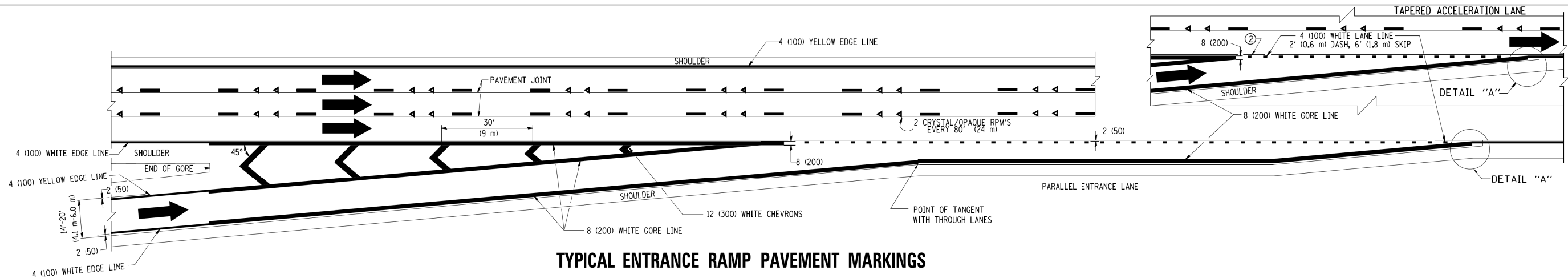


TYPICAL EDGE LINES & LANE LINES

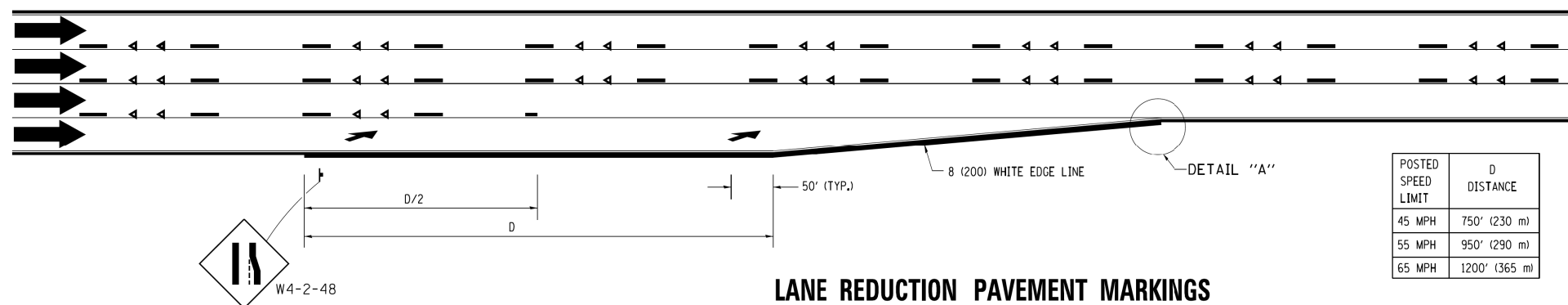
- PAVEMENT MARKING MATERIALS**
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
 2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE; INLAID OR GROOVED IN SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENT PROJECTS.
 3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC PROJECTS.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

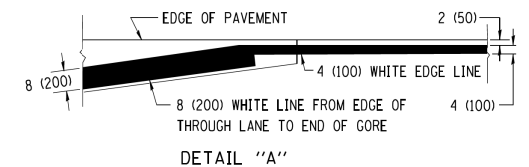


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

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DESIGNED - D.W.S.
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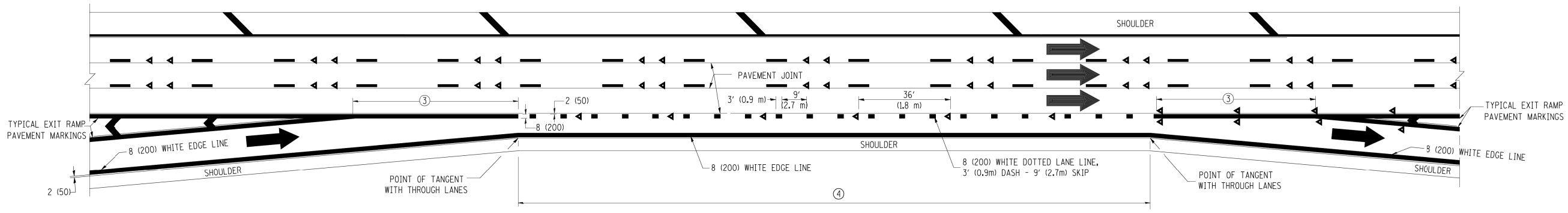
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 REVISED - M.D. 05-13

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

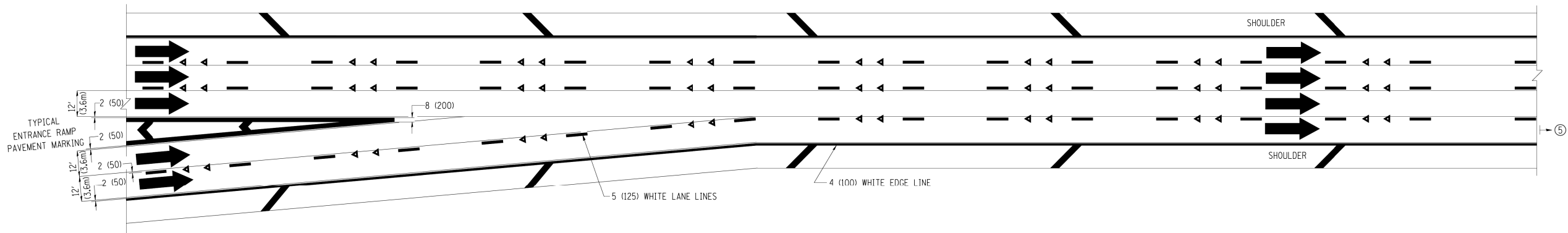
MULTI-LANE FREEWAY
 PAVEMENT MARKING DETAILS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

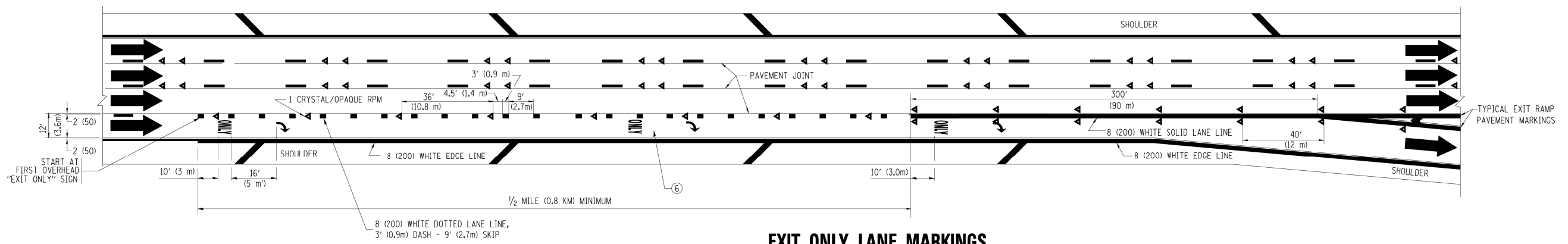
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90/94	2013-012R	COOK	385	332
TC-12			CONTRACT NO. 60W30	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



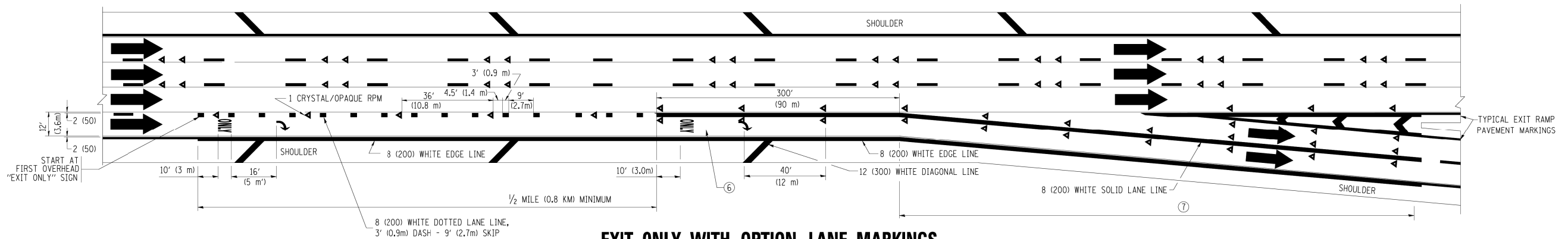
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED CORE.

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DESIGNED - D.W.S.
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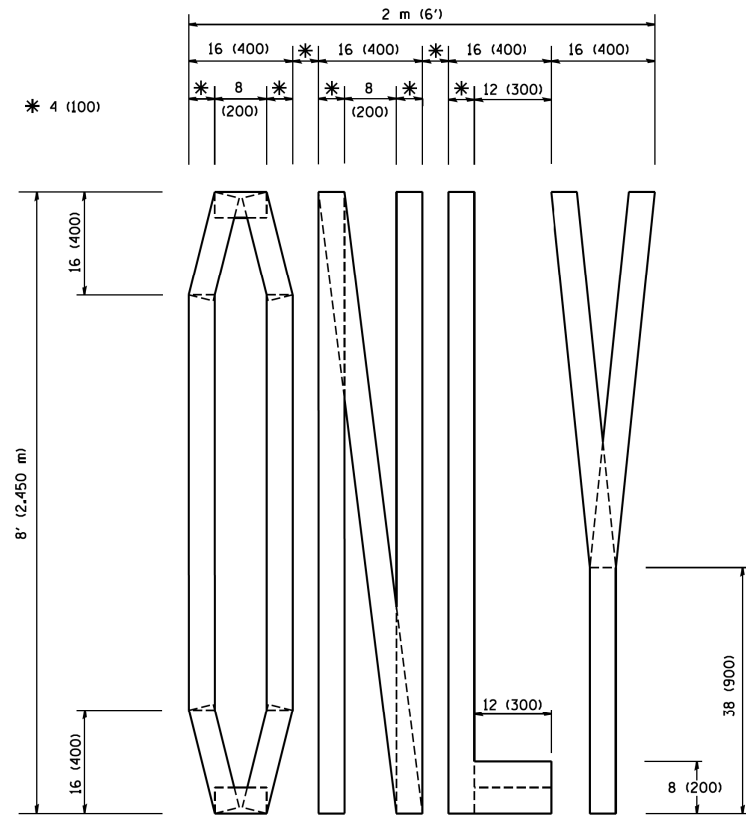
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

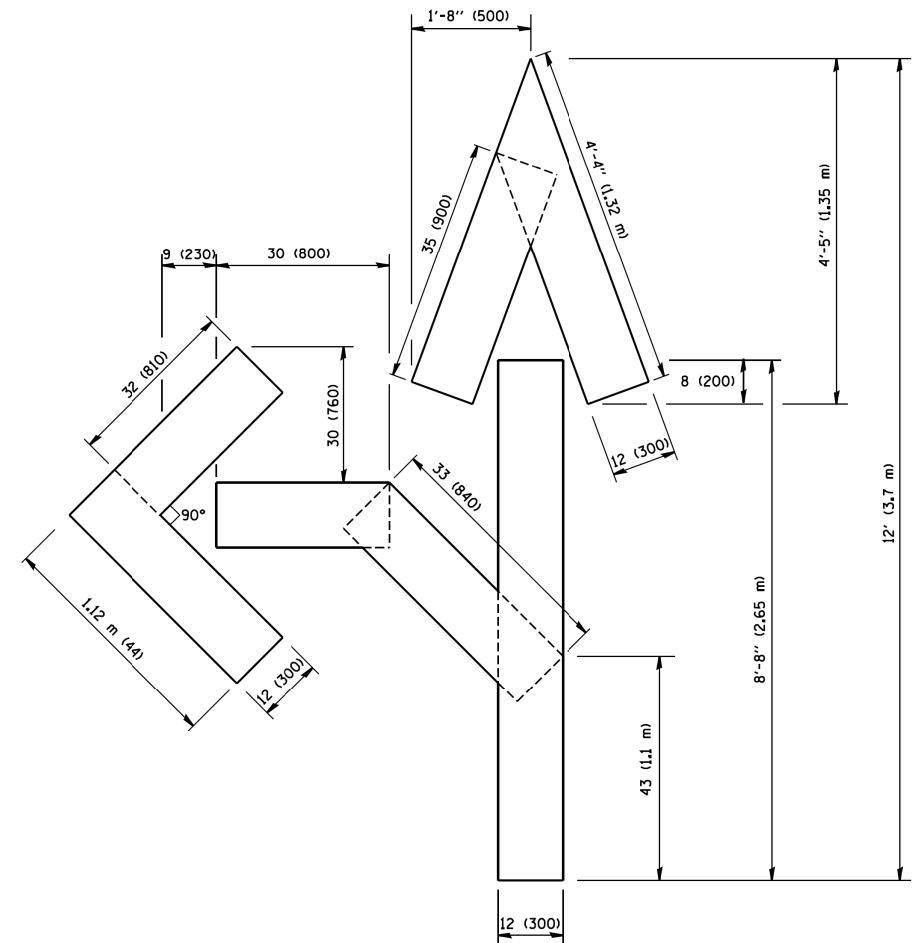
**MULTI-LANE FREEWAY
 PAVEMENT MARKING DETAILS**

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

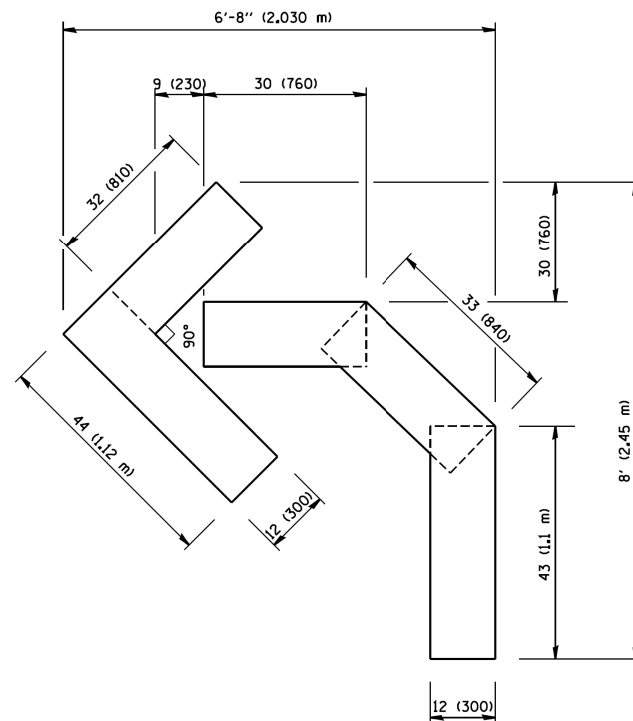
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90/94	2013-012R	COOK	385	333
TC-12			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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DI160W30-SHT-DI-Detail-14-TC-161.dgn
USER NAME = dishveoz
PLOT SCALE = 50.0000' / in.
PLOT DATE = 10/23/2014

DESIGNED -
DRAWN -
CHECKED -
DATE - D9-18-94

REVISED - T.RAMMACHER 06-05-96
REVISED - T.RAMMACHER 11-04-97
REVISED - T.RAMMACHER 03-02-98
REVISED - E.GOMEZ 08-28-00

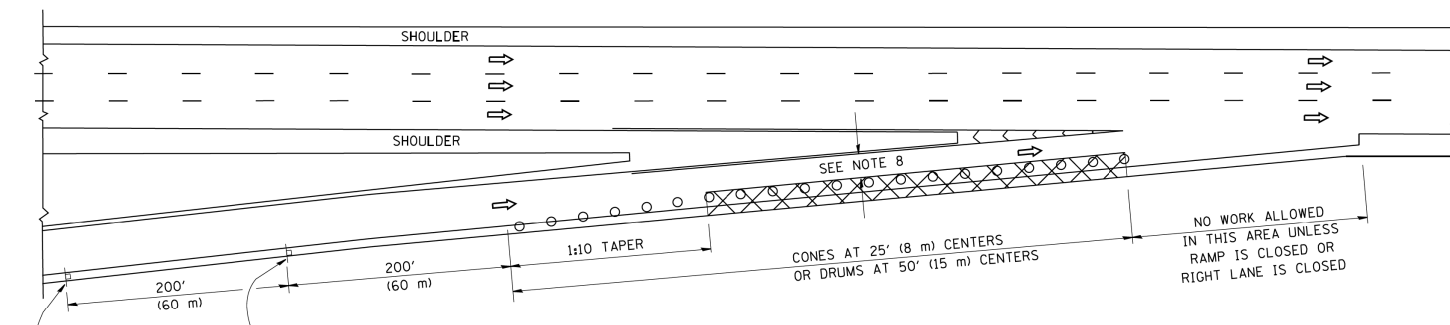
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
FOR TRAFFIC CONTROL

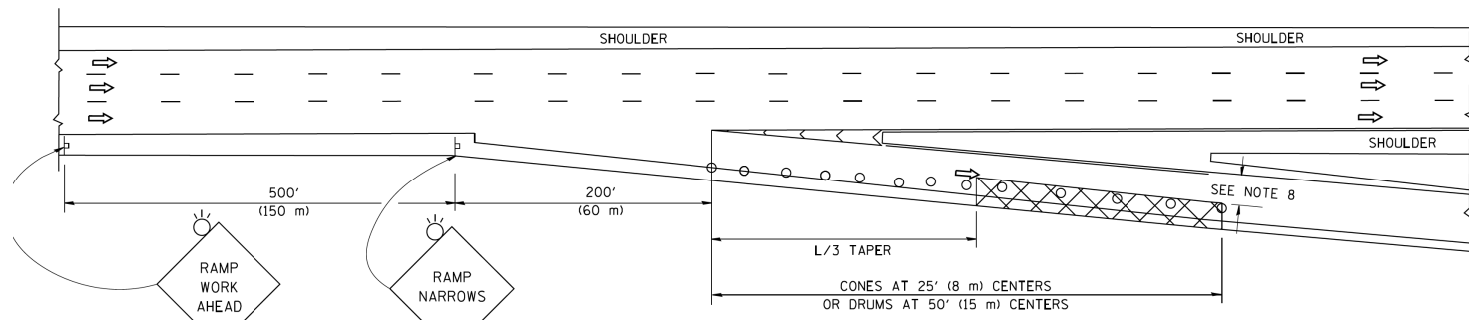
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	334
TC-16			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

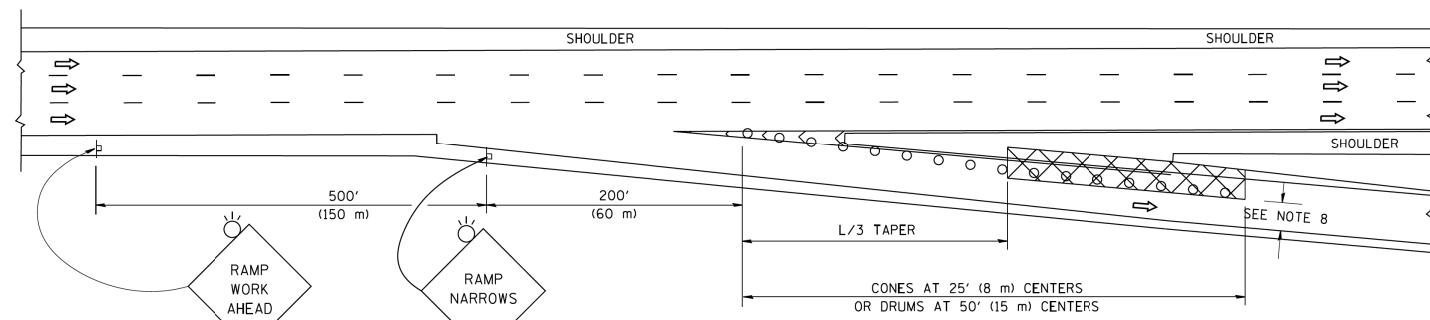
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

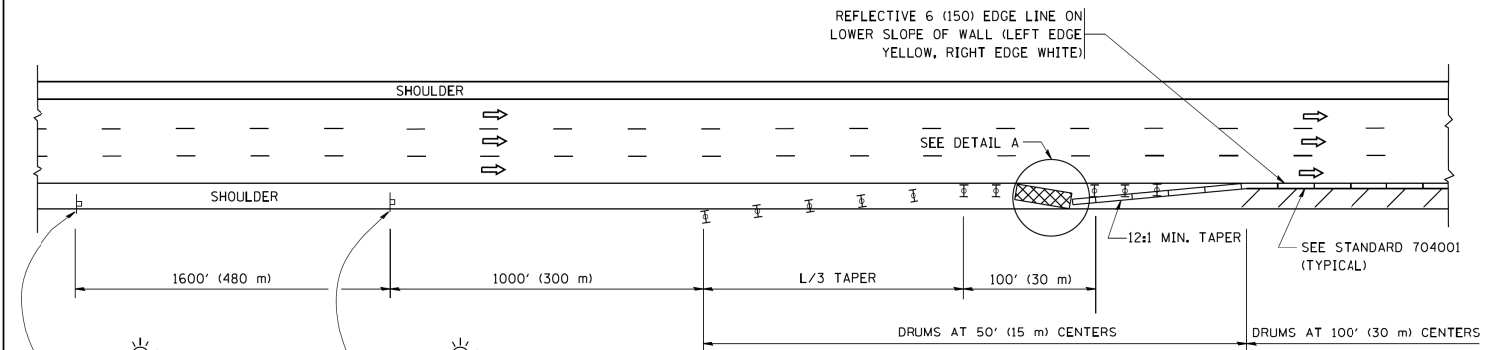
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

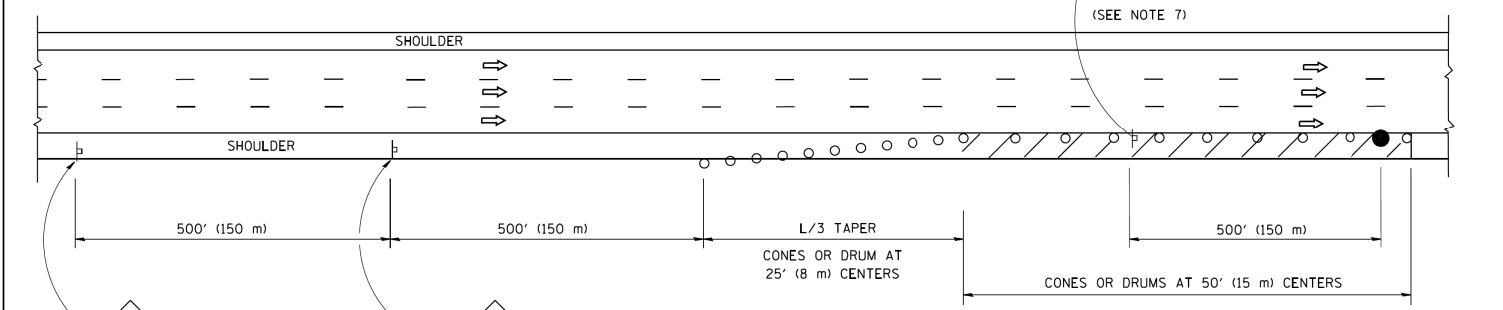
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS



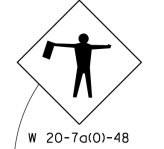
PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE



W 20-1103(10)-48

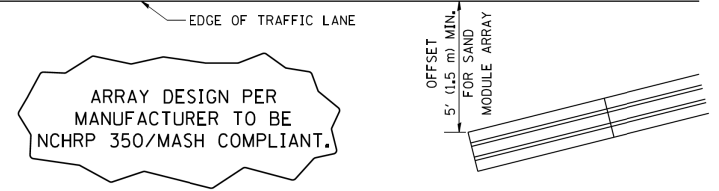


W 20-70(10)-48
(SEE NOTE 7)



W 20-1103(10)-48

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350/MASH COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12" MIN. WIDTH TANGENT SECTION
16" MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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D160W30-SHT-D1-Detail-15-(TC-17).dgn
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PLOT SCALE = 50.0000' / in.
PLOT DATE = 10/23/2014

DESIGNED -
DRAWN - D.W.S.
CHECKED -
DATE - 11-96

REVISED - J.A.F. 12-06
REVISED - SPANMACHER 11-04-97
REVISED - S.P.B. 12-09
REVISED - M.D. 06-13

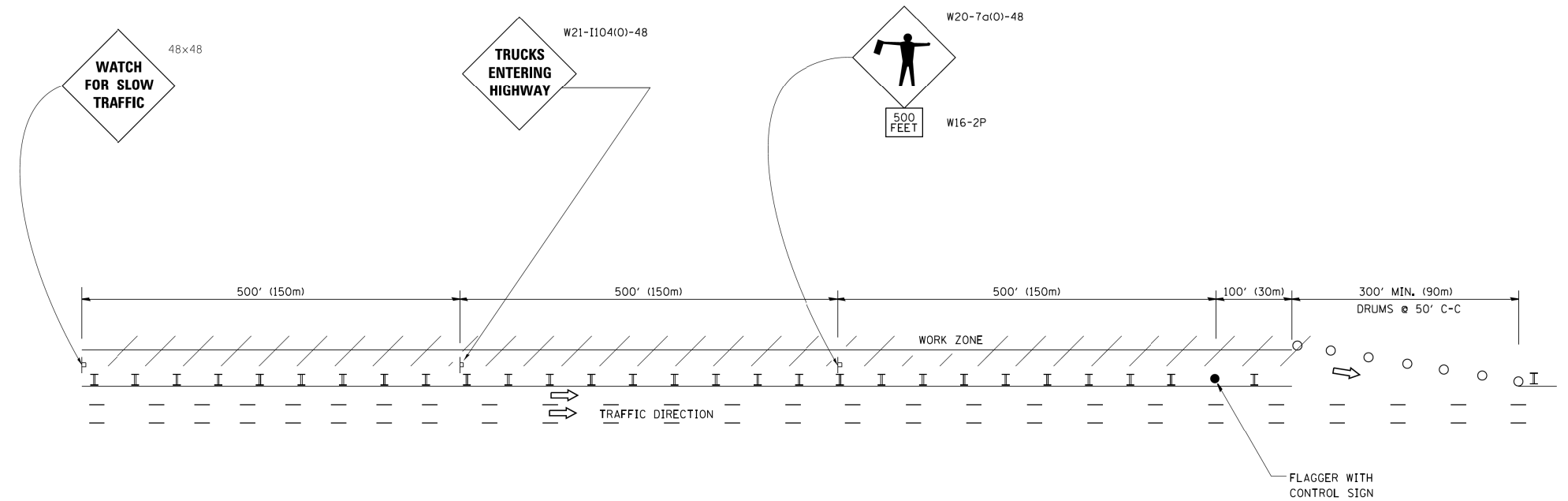
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

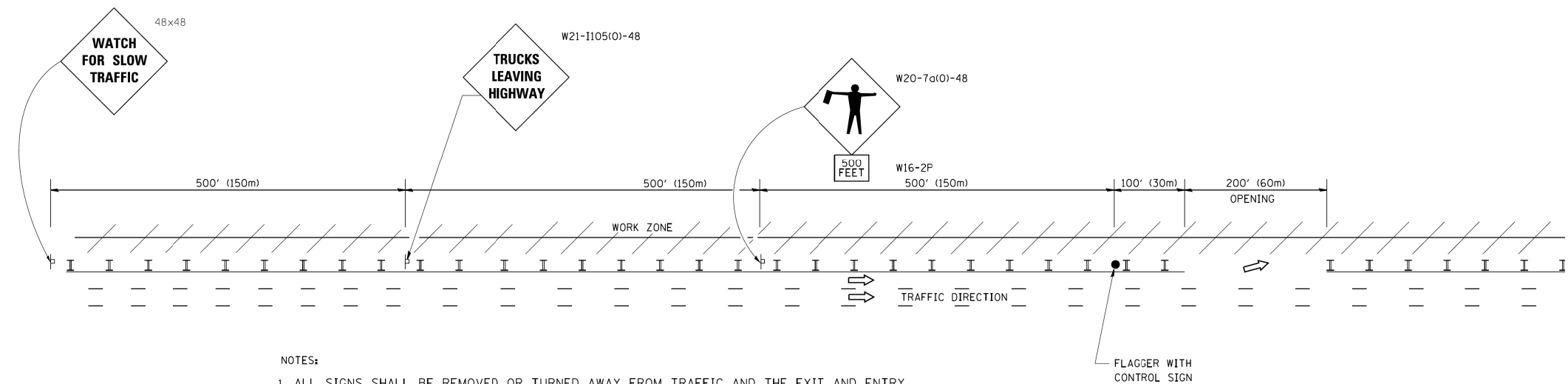
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	335
TC-17			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE PATH = C:\Users\vdshveoz\Desktop\60W30_PLOT\DI160W30-SHT-DI-Detail-15-TC-181.dgn



DI160W30-SHT-DI-Detail-15-TC-181.dgn
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DESIGNED -	REVISD - J.A.F. 02-06
DRAWN -	REVISD - S.P.B. 01-07
CHECKED -	REVISD - S.P.B. 12-09
DATE - 1	REVISD - M.D. 06-13

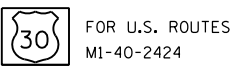
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS
 AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	336
TC-18			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ROUTE MARKERS



FOR U.S. ROUTES
M1-40-2424



FOR ILLINOIS ROUTES
M1-50-2424

MAIN STREET
R,R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS



M5-1L-2115



M5-1R-2115



M6-1-2115



M6-1-2115



M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS



M3-1-2412



M3-2-2412



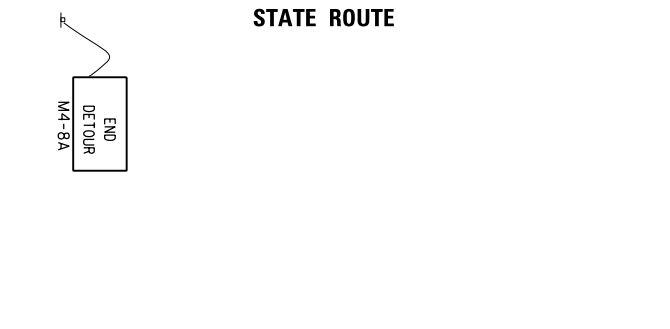
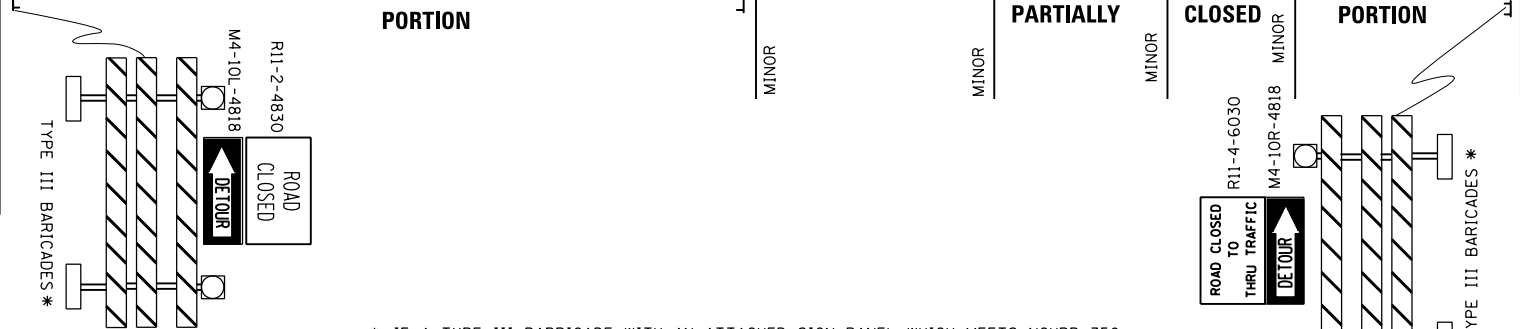
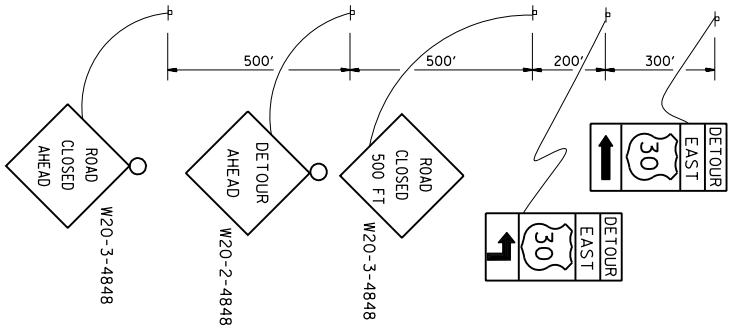
M3-3-2412



M3-4-2412



M4-8-2412



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.



D160W30-SHT-D1-Details-16-TC-211.dgn
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PLOT SCALE = 50.0000' / in.
PLOT DATE = 10/23/2014

DESIGNED -
DRAWN -
CHECKED -
DATE - 10/24/2014

REVISED -10-18-02
REVISED -R. BORO 09-14-09
REVISED -
REVISED -

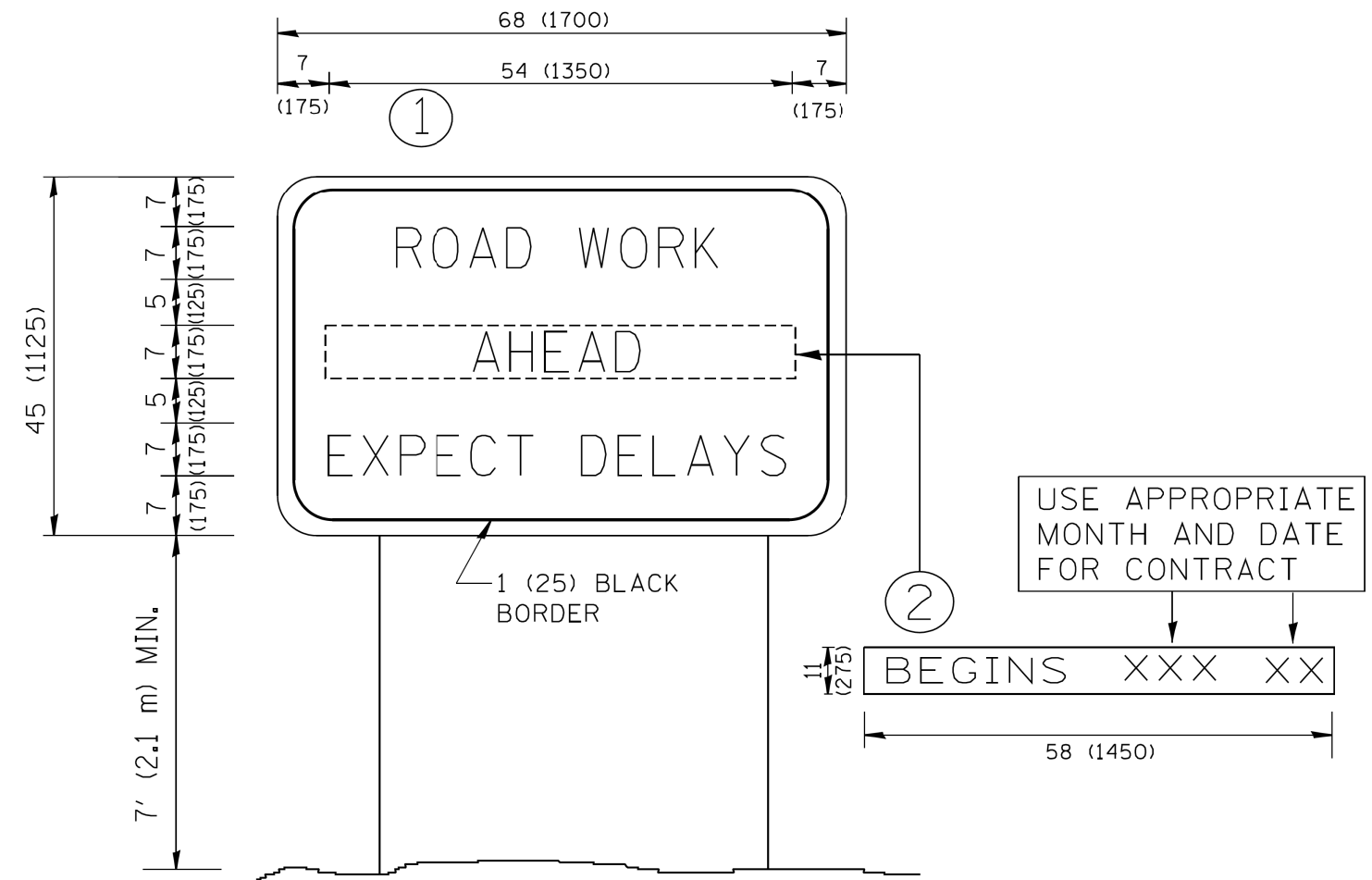
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETOUR SIGNING
FOR CLOSING STATE HIGHWAYS**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	337
TC-21			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE PATH = C:\Users\dishevoz\Desktop\60W30_PLOT\0160W30-SHT-D1-Details-16-TC-211.dgn



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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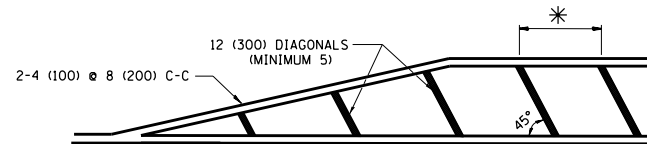
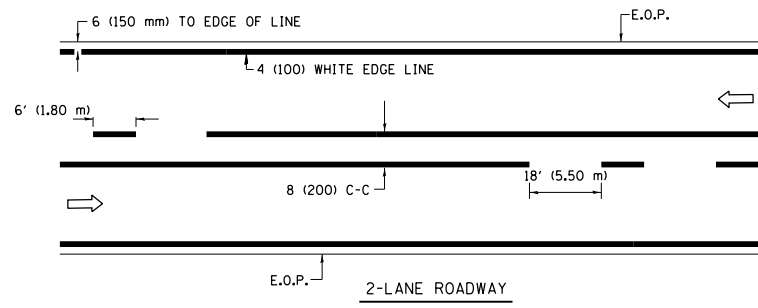
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DESIGNED -	REVISED-R, MIRS 9-15-97
DRAWN -	REVISED-R, MIRS 12-11-97
CHECKED -	REVISED-R, RAMMACHER 02-02-99
DATE - 1C	REVISED-C, JUCIUS 01-31-07

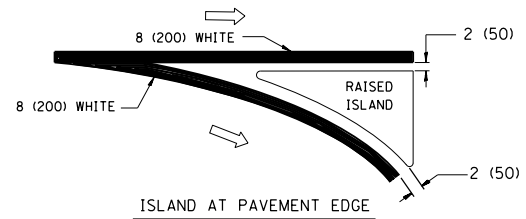
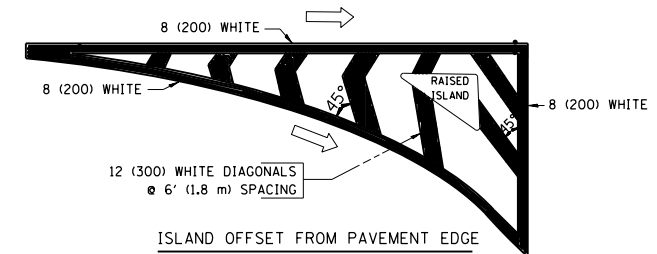
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

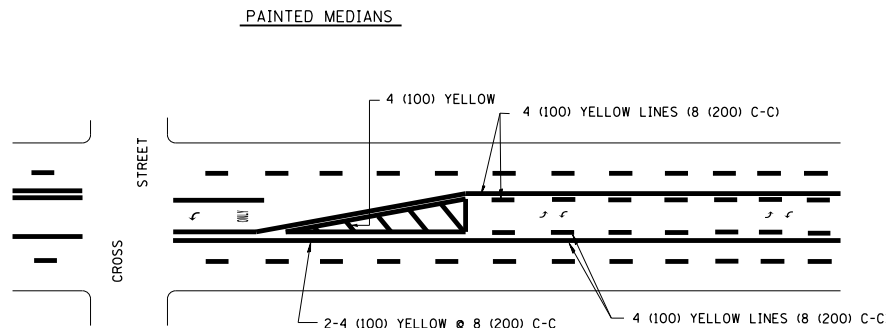
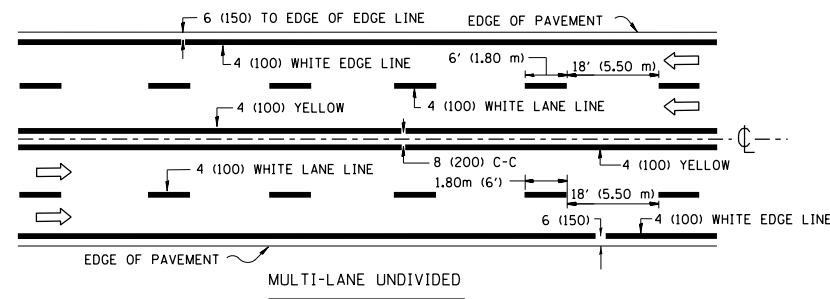
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TC-22			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



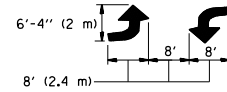
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 * DIAGONAL LINE SPACING: 20' (6.1 m) C-C



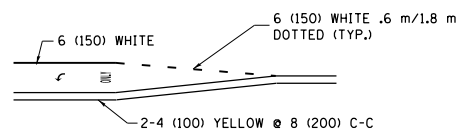
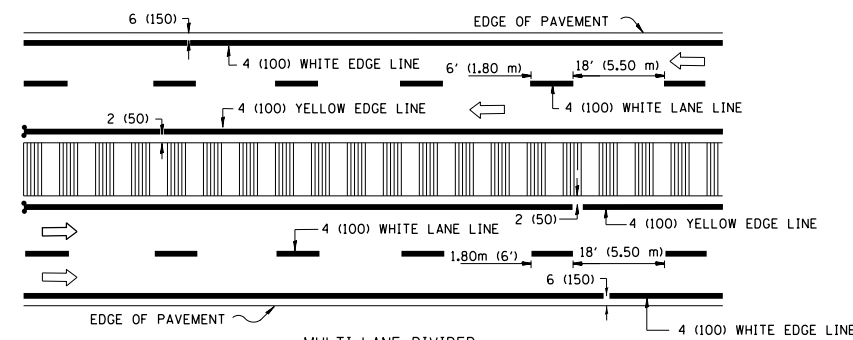
TYPICAL ISLAND MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

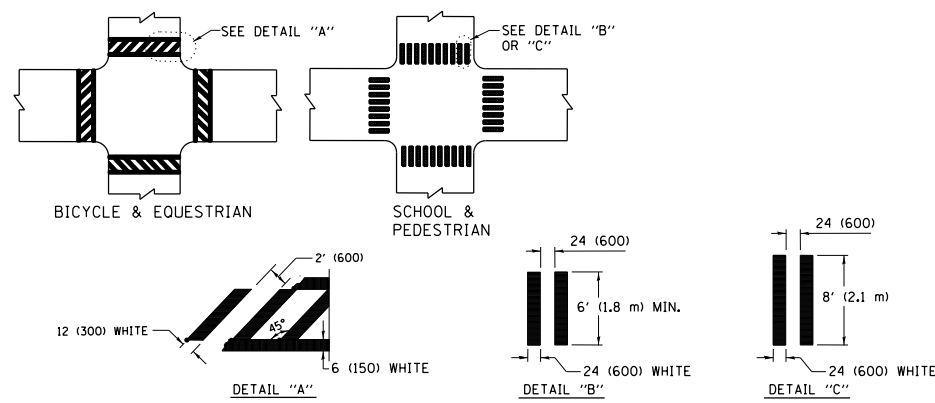
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

TYPICAL TURN LANE MARKING



D160W30-SHT-D1-Detail-18-20-ITC-24.dgn
 USER NAME = dishevoz
 PLOT SCALE = 50.0000' / in.
 PLOT DATE = 10/23/2014

DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 1

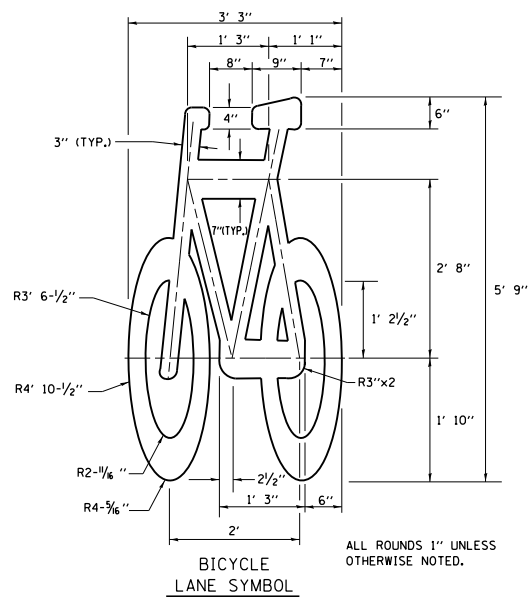
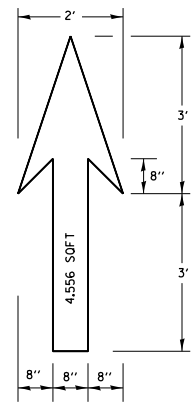
REVISED-T. RAMMACHER 12-07-00
 REVISED-K. ENG 02-28-12
 REVISED-
 REVISED-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET OF SHEETS STA. TO STA.

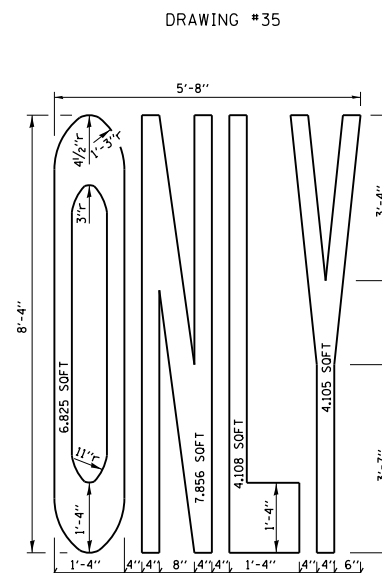
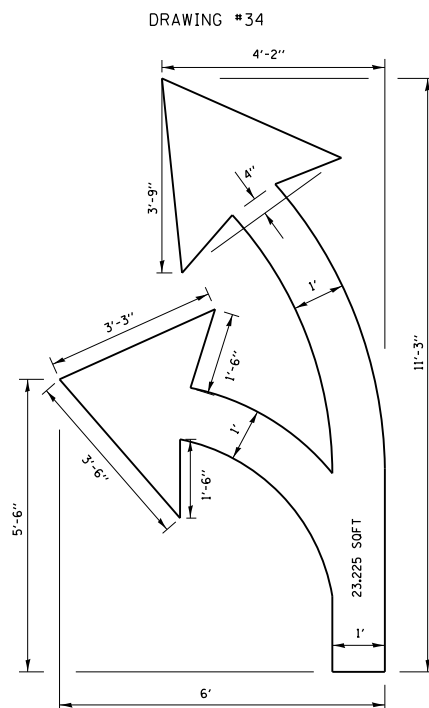
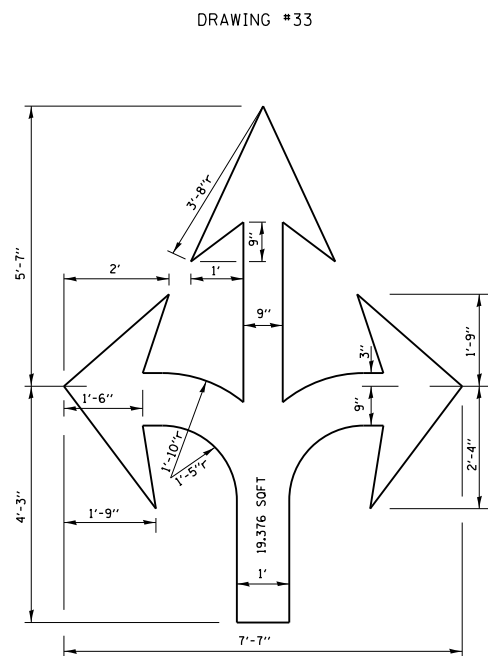
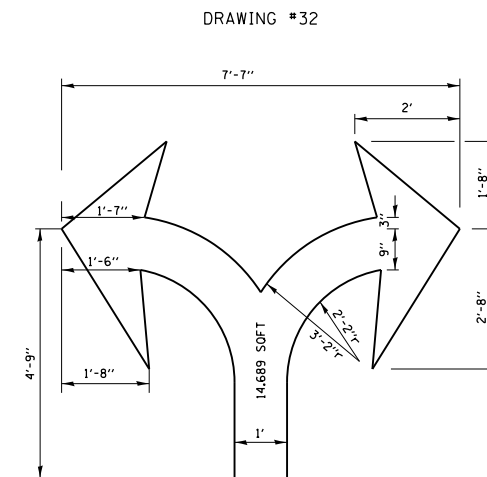
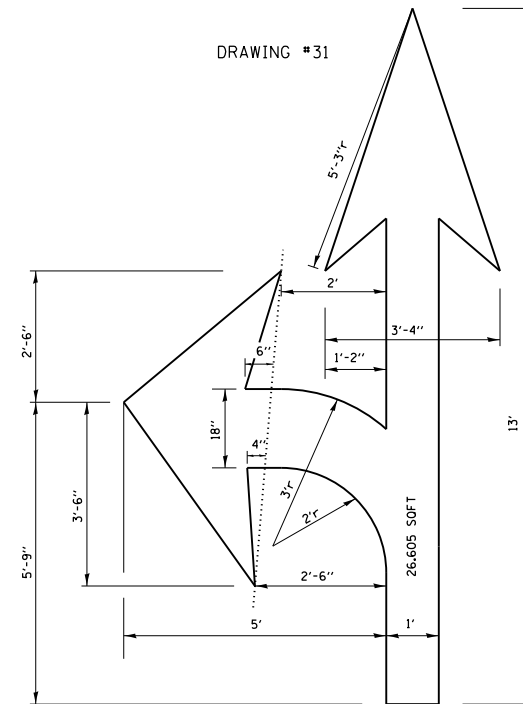
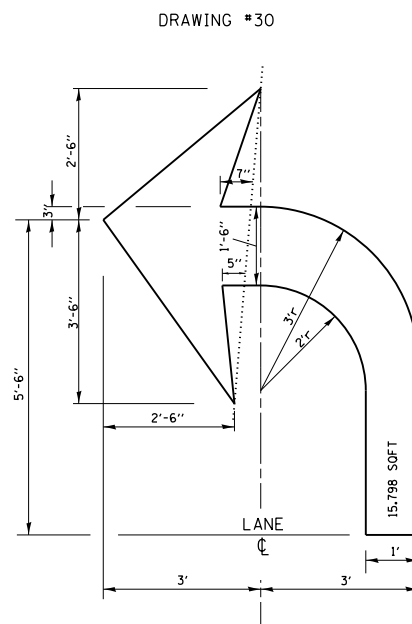
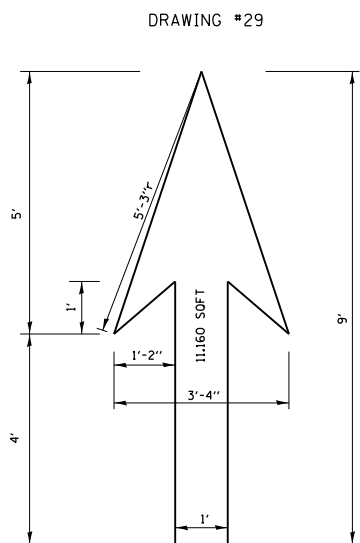
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	339
TC-24			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTE:

- 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
- 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE: ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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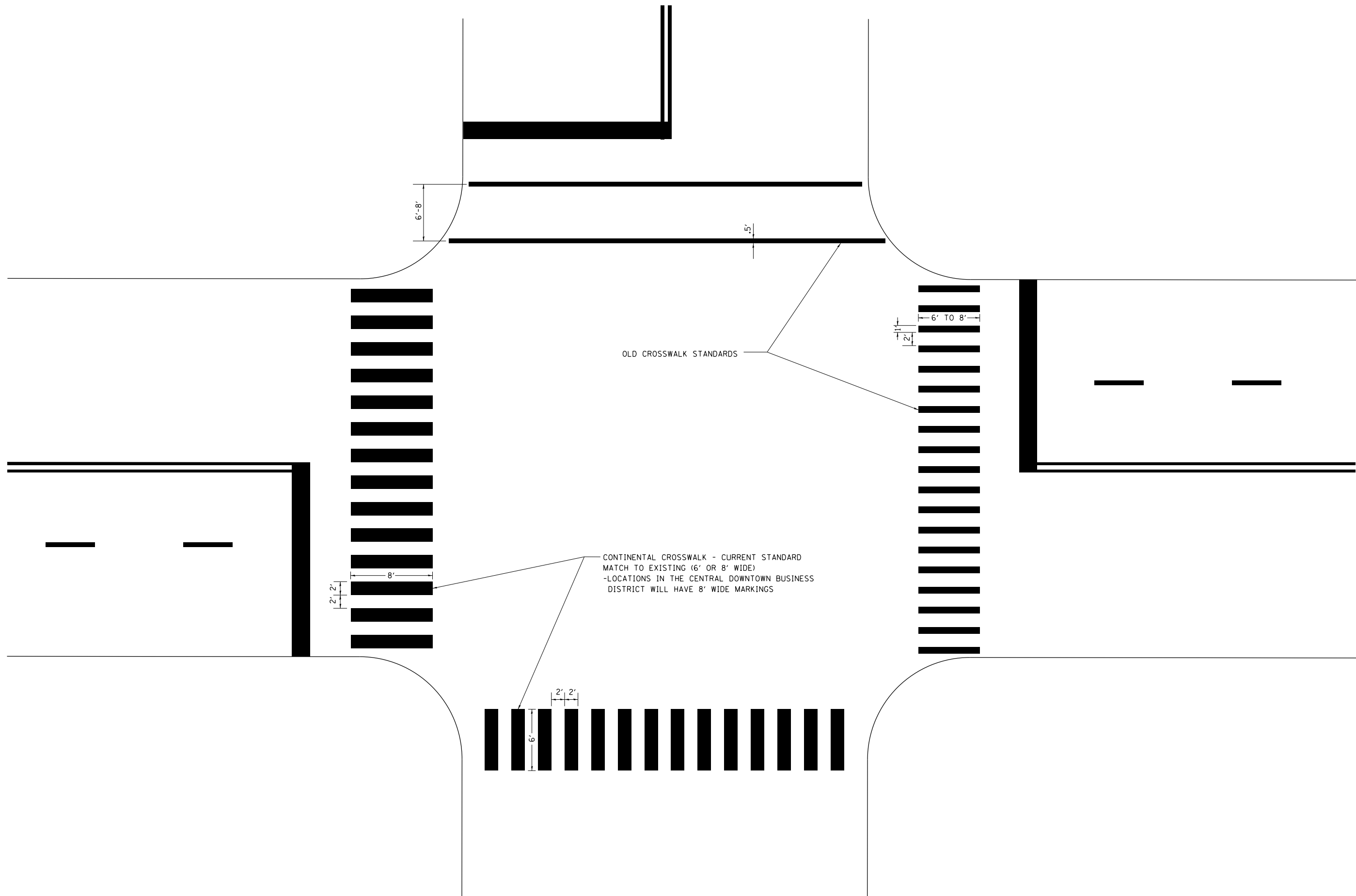
DESIGNED - REVISED-T, RAMMACHER 12-07-00
DRAWN - REVISED-K, ENG 01-12-12
CHECKED - REVISED-
DATE - 1 REVISED-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS
SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	340
TC-24			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE PATH = C:\Users\dishevoz\Desktop\60W30_PLOT\DI60W30-SHT-DI-Detail-18-20-ITC-241.dgn



DI60W30-SHT-DI-Detail-18-20-ITC-241.dgn	DESIGNED -	REVISED-T. RAMMACHER 12-07-00
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PLOT SCALE = 50.000' / in.	CHECKED -	REVISED-
PLOT DATE = 10/23/2014	DATE - 10/23/2014	REVISED-

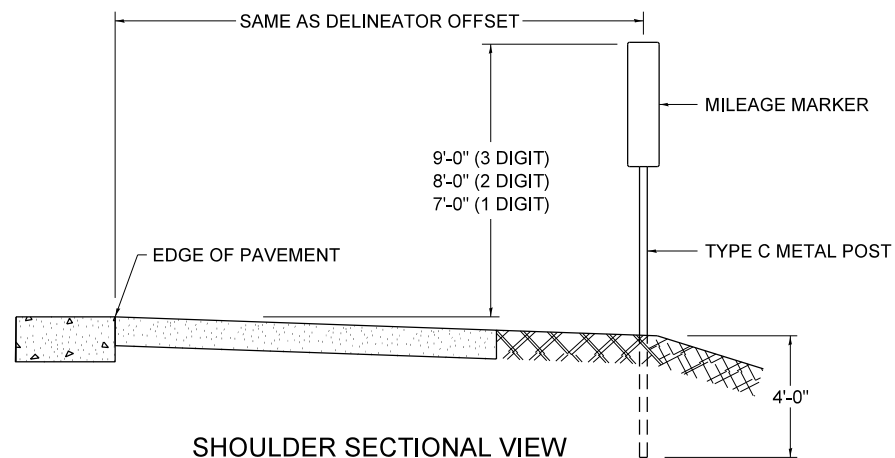
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

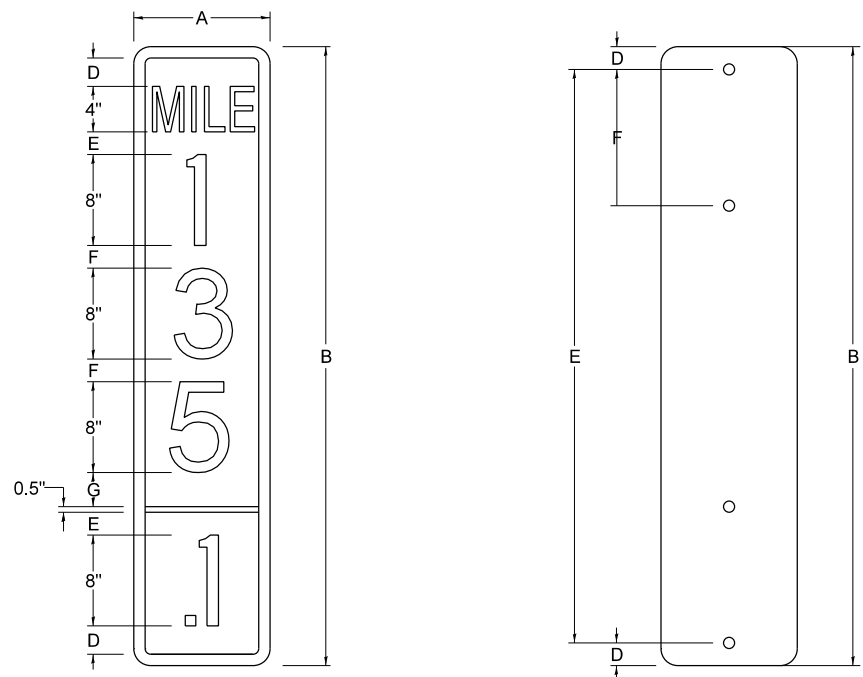
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	341
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W30	

STANDARD DESIGN FOR MILE POST



SHOULDER SECTIONAL VIEW

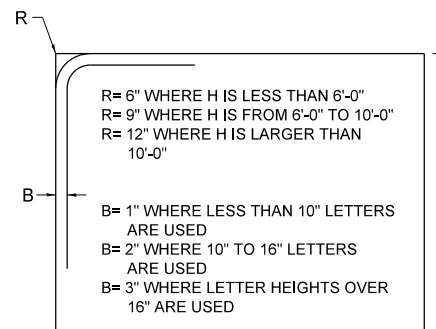


SIGN SIZE	DIMENSIONS							
	A	B	C	D	E	F	G	DIGIT
12 x 24	12.0	24.0	1.5	1.5	1.5	N/A	1.5	1
12 x 36	12.0	36.0	1.5	2.0	2.0	2.0	1.5	2
12 x 48	12.0	48.0	1.5	2.5	2.0	2.0	2.5	3

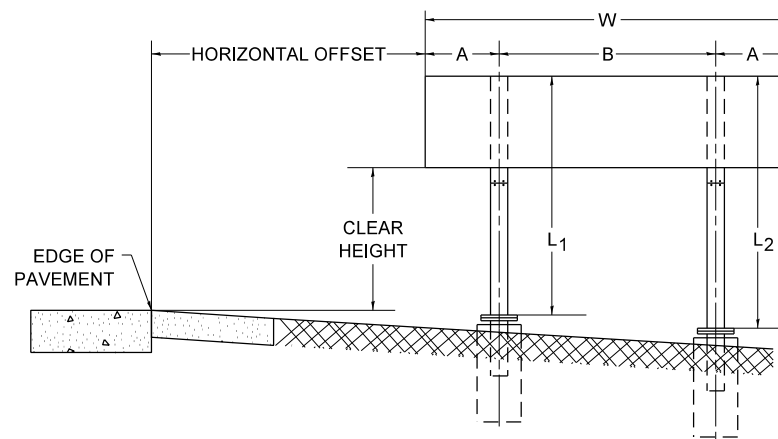
BLANK	A	B	C	D	E	F
B9-1224	12.0	24.0	1.5	2.0	20.0	N/A
B9-1236	12.0	36.0	1.5	2.0	32.0	12.0
B9-1248	12.0	48.0	1.5	2.0	44.0	12.0

SIGN SIZE	SERIES					BLANK STD.	
	LINES						
	1	2	3	4	5	BORDER	
12 x 24	4C	8D	4C	N/A	N/A	0.5	B9-1224
12 x 36	4C	8D	8D	4C	N/A	0.5	B9-1236
12 x 48	4C	8D	8D	8D	4C	0.5	B9-1248

BORDER AND RADIUS LAYOUT



MAJOR GUIDE SIGN LAYOUT

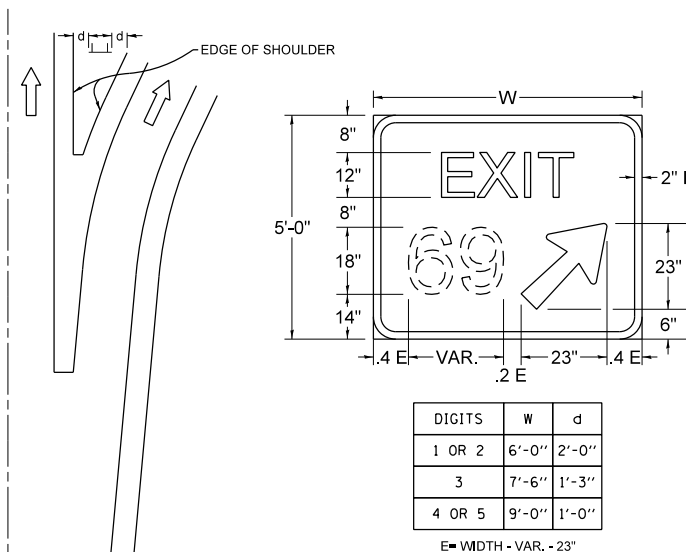


NUMBER OF STEEL SUPPORTS	A	B
2	.2 W	.6 W
3	.15 W	.35 W
4	.125 W	.25 W
5	.1 W	.2 W

"L1 IS THE LENGTH OF SUPPORT, NOT INCLUDING THE STUB PROJECTION, CLOSEST TO THE EDGE OF THE PAVEMENT."

"A" IS THE DISTANCE FROM THE SIGN EDGE TO THE CENTERLINE OF THE NEAREST SUPPORT. "B" IS THE DISTANCE BETWEEN CENTERLINES OF SUPPORTS."

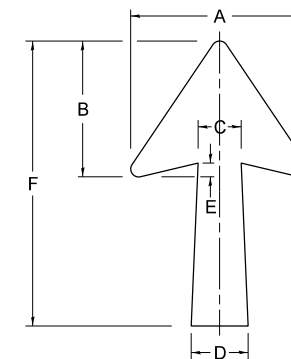
GORE SIGNS



DIGITS	W	d
1 OR 2	6'-0"	2'-0"
3	7'-6"	1'-3"
4 OR 5	9'-0"	1'-0"

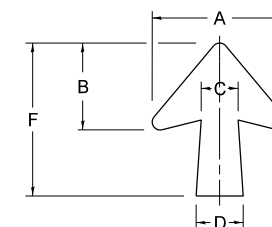
E = WIDTH - VAR. - 23"

STANDARD ARROWS FOR INTERSTATE GUIDE SIGNS



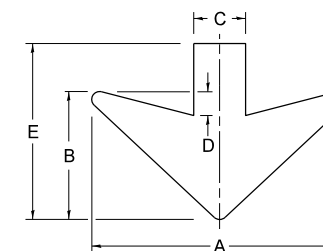
ARROW SYMBOL	A	B	C	D	E	F	R
24 1/4 x 15 1/8	15 1/8	11 3/8	3 3/4	5	1 5/8	24 1/4	1/8
29 1/4 x 18 1/4	18 1/4	14	4 1/2	6	1 1/2	29 1/4	3/4
35 5/8 x 22 1/4	22 1/4	17	5 3/8	7 1/8	1 3/4	35 5/8	1
18 1/4 x 11 1/4	11 1/4	8 3/4	3 1/8	3 3/8		18 1/4	

NOTE: D & F ARE RECOMMENDED DIMENSIONS. TAPER SHOULD BE HELD CONSTANT FOR LONGER OR SHORTER SHAFT LENGTHS



ARROW SYMBOL	A	B	C	D	E	F	R
17 1/4 x 14 1/4	14 1/4	9 1/8	3 3/8	4 1/2	1 5/8	17 1/4	3/4
20 1/4 x 17 1/4	17 1/4	11 3/4	4 3/8	5 5/8	1 1/2	20 1/4	
25 x 21 1/8	21 1/8	14 1/4	5	6 3/4	1 3/4	25	1
9 5/8 x 8 1/8	8 1/8	5 1/8	2 3/8	2 3/8		9 5/8	1/2

DOWN ARROWS



ARROW SYMBOL	A	B	C	D	E	R
16 1/2 x 24	24	12	5	1 1/2	16 1/2	3/4
22 x 32	32	16	6 1/2	3	22	1

FILE PATH = C:\Users\dshveoz\Desktop\60W30_PLOT\DI60W30-SHT-DI-Detail-21-TC-271.dgn



DI60W30-SHT-DI-Detail-21-TC-271.dgn
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 PLOT DATE = 10/23/2014

DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 1003-08-1984

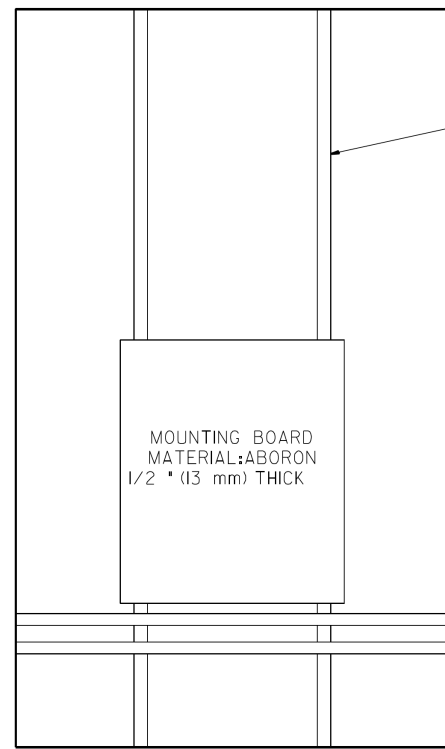
REVISED - 02-04-2009
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

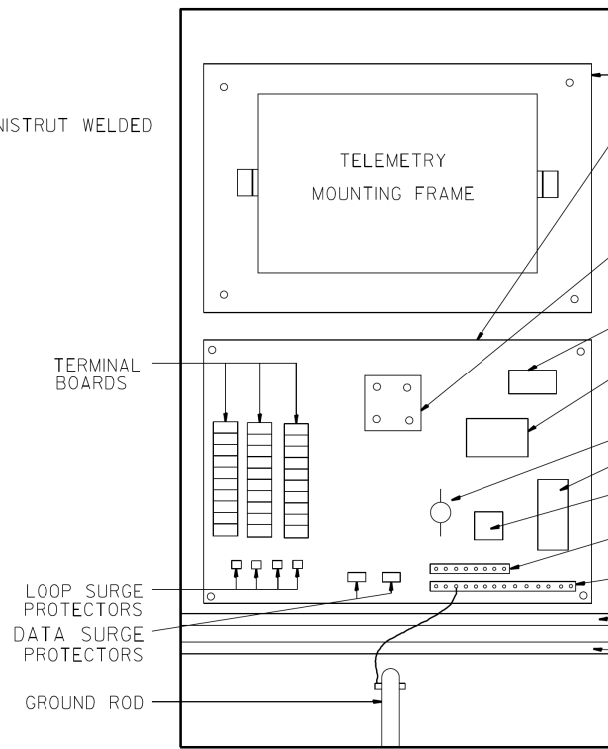
MILE POST MARKERS - GORE SIGNS
 MAJOR GUIDE SIGN LAYOUT - ARROWS

SCALE: NONE SHEET OF SHEETS STA. TO STA.

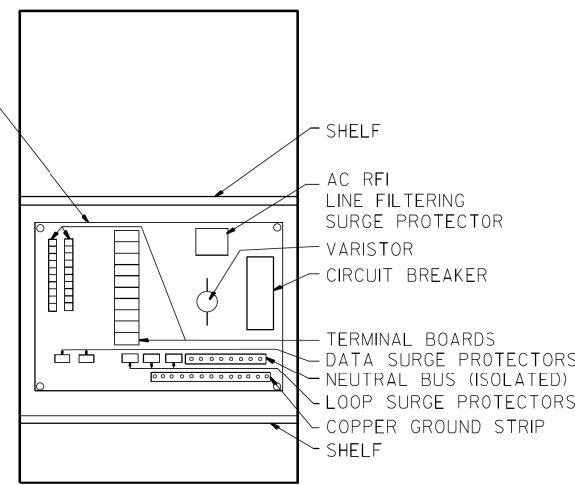
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	342
TC-27 (TS-2341-1)			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SIDE VIEW ESP 3 & 4 CABINET



ESP 3 CABINET



ESP 2 CABINET

TYPICAL CABINET INTERIORS

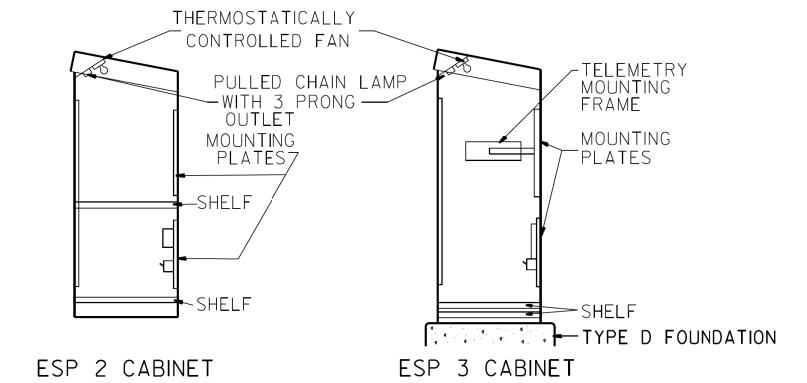
STANDARD TRAFFIC SYSTEMS CENTER CABINETS

MINIMUM DIMENSIONS INSIDE

TYPE	HEIGHT (IN-mm)	WIDTH (IN-mm)	DEPTH (IN-mm)	THICKNESS (IN-mm)	MATERIAL
ESP1	22.5" (571.5 mm)	14.25" (361.95mm)	9.75" (247.65mm)	3/16" (4.76mm)	FABRICATED ALUMINUM
ESP2	36" (914.4mm)	20" (508.0mm)	15" (381.0mm)	7/16" (4.76mm)	FABRICATED ALUMINUM
ESP3	49.5" (1,26 m)	30" (762.0mm)	17" (431.8mm)	3/16" (4.76mm)	FABRICATED ALUMINUM
ESP4	55" (1,4 m)	44" (1,12 m)	26" (660.4mm)	3/16" (4.76mm)	FABRICATED ALUMINUM

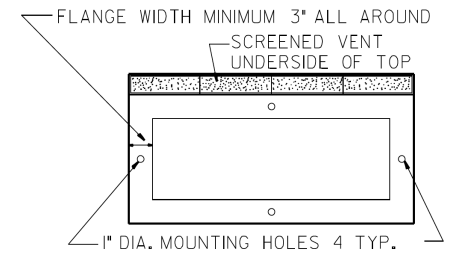
NOTES:

- CABINETS, CABINET POSTS AND CABINET PEDESTALS SHALL BE PRIMED AND PAINTED IN ACCORDANCE WITH SECTION T637 OF THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS". THE FINAL COAT SHALL BE (X) IN COLOR. THE INTERIOR SHALL BE PAINTED WHITE. SIGNAL POSTS AND HEADS TO BE FEDERAL YELLOW 89-19(MAUTZ).
- CABINETS SHALL BE INSTALLED IN ACCORDANCE WITH APPLICABLE PORTIONS OF SECTION T400 OF THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS".
- ALL CABINETS WHICH ARE SERVICED BY 117 VOLTS A.C. POWER SHALL BE EQUIPPED WITH A 10 AMP CIRCUIT BREAKER, A.C. R.F.I. LINE FILTERING SURGE PROTECTOR, VARIATOR, DATA SURGE AND LOOP SURGE PROTECTORS AS INCIDENTAL TO THE COST OF THE CABINET. CMS CABINETS TYPE IV SHALL HAVE A 60 AMP. CIRCUIT BREAKER MINIMUM.
- ESP 2/3/4 CABINETS SHALL BE FITTED WITH A THERMOSTATICALLY CONTROLLED FAN. IT SHALL BE MOUNTED AT THE TOP OF THE CABINET. THE FAN SHALL BE CAPABLE OF OPERATING AT 130 CPM AT 160' (48.8 m) OF STATIC WATER PRESSURE. A PORCLAIN BASED PULL CHAIN FIXTURE WITH 3 PRONG OUTLET SHALL ALSO BE PROVIDED.
- RAMP METERING ESP 3 TYPE CABINETS SHALL ALSO BE EQUIPPED WITH A LOAD RELAY AND 2 CIRCUIT FLASHER. LAMPS, FAN, LOAD RELAY, AND 2 CIRCUIT FLASHER SHALL BE INCIDENTAL TO THE COST OF THE CABINET
- INCIDENTAL TO THE COST OF EACH CABINET THE CONTRACTOR SHALL CONSTRUCT A 5 INCH (130mm) PCC SIDEWALK OF A RECTANGULAR AREA 3 FEET (915 mm) BY 4 FEET (1,25 m) IMMEDIATELY ADJACENT TO THE CABINET FOUNDATION ON THE SAME SIDE OF THE FOUNDATION AS THE CABINET DOOR TO PROVIDE FOOTING DURING INSTALLATION AND MAINTENANCE.
- ANCHOR BOLTS FOR PEDESTAL AND BASE MOUNTED CABINETS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE CABINET.
- ALL CABINETS SHALL HAVE TERMINAL BLOCKS AND SHELVES AS SHOWN. THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE CABINET.
- THE CABINET DOOR SHALL BE HINGED ON THE RIGHT SIDE WHEN FACING THE CABINET. THE DOOR SHALL BE FURNISHED WITH A GASKET THAT SHALL FORM A WEATHER TIGHT SEAL BETWEEN THE CABINET AND DOOR. THE HINGES SHALL BE CONTINUOUS AND BOLTED TO THE CABINET AND DOOR UTILIZING 1/4-20 STAINLESS STEEL CARRIAGE BOLTS AND NY-LOCK NUTS. THE HINGES WILL BE MADE OF STAINLESS STEEL WITH A 0.25 INCH (6.35 mm) DIAMETER STAINLESS STEEL HINGE PIN. THE HINGE PIN SHALL BE CAPPED TOP AND BOTTOM BY WELD TO RENDER IT TAMPER PROOF.
- THE LATCHING MECHANISM SHALL BE A 3 POINT DRAW ROLLER TYPE. THE CENTER CATCH AND PUSHRODS SHALL BE EITHER CADMIUM OR ZINC PLATED, TYPE II CLASS I. PUSHRODS WILL BE TURNED EDGEWISE AT THE OUTWARD SUPPORTS AND SHALL BE 0.25 INCH (6.35 mm) BY 0.75 INCH (19.05 mm), MINIMUM. ROLLERS SHALL HAVE A MINIMUM DIAMETER OF 0.875 INCH (22.22 mm) AND WILL BE MADE OF NYLON. THE CENTER CATCH SHALL BE FABRICATED FROM 0.14 INCH (3.55 mm) STEEL, MINIMUM. WHEN THE DOOR IS CLOSED AND LATCHED, IT WILL BE LOCKED. THE LATCHING HANDLE SHALL HAVE A PROVISION FOR PADLOCKING IN THE CLOSED POSITION. AN OPERATING HANDLE SHALL BE FURNISHED WITH EACH LOCK. THE HANDLE WILL BE STAINLESS STEEL WITH A 0.75 INCH (19.05 mm) DIAMETER SHANK.
- THE ENCLOSURE SHALL BE EQUIPPED WITH TWO ADJUSTABLE "C" MOUNTING CHANNELS WELDED ON BOTH SIDE WALLS AND BACK WALL OF THE ENCLOSURE, ALLOWING VERSATILE POSITIONING OF SHELVES OR PANELS. MOUNTING CHANNELS SHALL BE FACTORY PAINTED SAME COLOR AS INTERIOR OF CABINET.
- CABINET DOOR SHALL NOT HAVE COMPARTMENT DOORS OR LOUVERS.
- ALL FIELD CABINETS SHALL BE FITTED WITH BRASS LOCKS.
- ESP TYPE 2 & 3 CABINETS FITTED WITH TWO SHELVES AS SHOWN.
- POST TOP MOUNTED CABINETS, SHALL HAVE A 0.25 INCH (6.3 mm) BOTTOM OF CABINET WELDED.
- THE CONTROL CABINET SHALL BE SET PLUMB ON THE FOUNDATION AND FASTENED TO THE ANCHOR BOLTS WITH NUTS AND WASHERS. FLAT WASHERS SHALL BE INSTALLED BELOW AND ABOVE THE BASE PLATE OF THE CONTROL CABINET. LOCKWASHERS SHALL BE INSTALLED ON TOP OF THE TOP FLAT WASHER.



PROFILE VIEWS

NOTE: MOUNTING PLATES TO BE MOUNTED TO BACK PANEL OF CABINET



BOTTOM VIEW MOUNTING PATTERN

(X)

- | | |
|------------------|---------------------|
| EDENS | WALNUT * |
| KENNEDY | BLUE STREAK ** |
| EISENHOWER | CARIBBEAN BLUE * |
| I-290/IL53/I-355 | POST OFFICE BLUE ** |
| RYAN | YELLOW STONE II ** |
| I-55 | MEDIUM BRONZE * |
| I 57 | RED BARON ** |
| CAL-KING | BLUE STREAK ** |
| LAKE SHORE DR. | GREEN * |
| I-80 | STATUARY BRONZE ** |
- ALL RAMP METERING CABINETS LIME GREEN ***. ALL POSTS, T.S. HEADS AND SERVICES WILL BE PAINTED FEDERAL YELLOW.
- * MORTON POWDER PAINT COLOR OR EQUIVALENT.
 ** O'BRIEN POWDER PAINT COLOR OR EQUIVALENT.
 *** BENJAMIN MOORE ENAMEL COLOR OR EQUIVALENT.

NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR CONFORMING TO COLOR REQUIREMENTS

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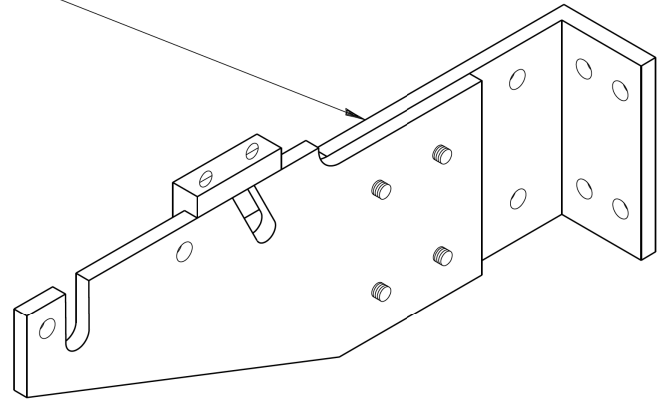
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DRAWN - G.M.	REVISED- 09/96
CHECKED - R.L.	REVISED- 02/98
DATE - 06/21/94	REVISED- 03/99

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

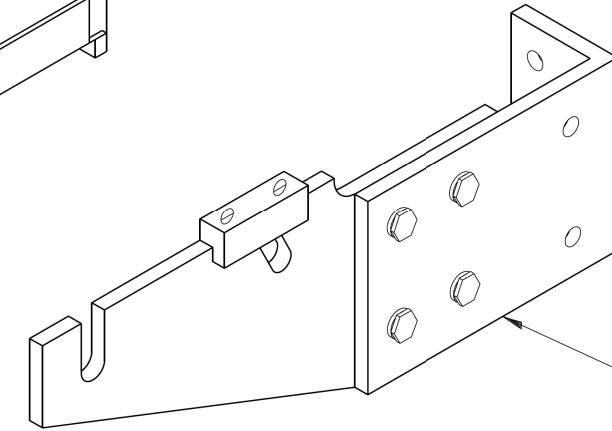
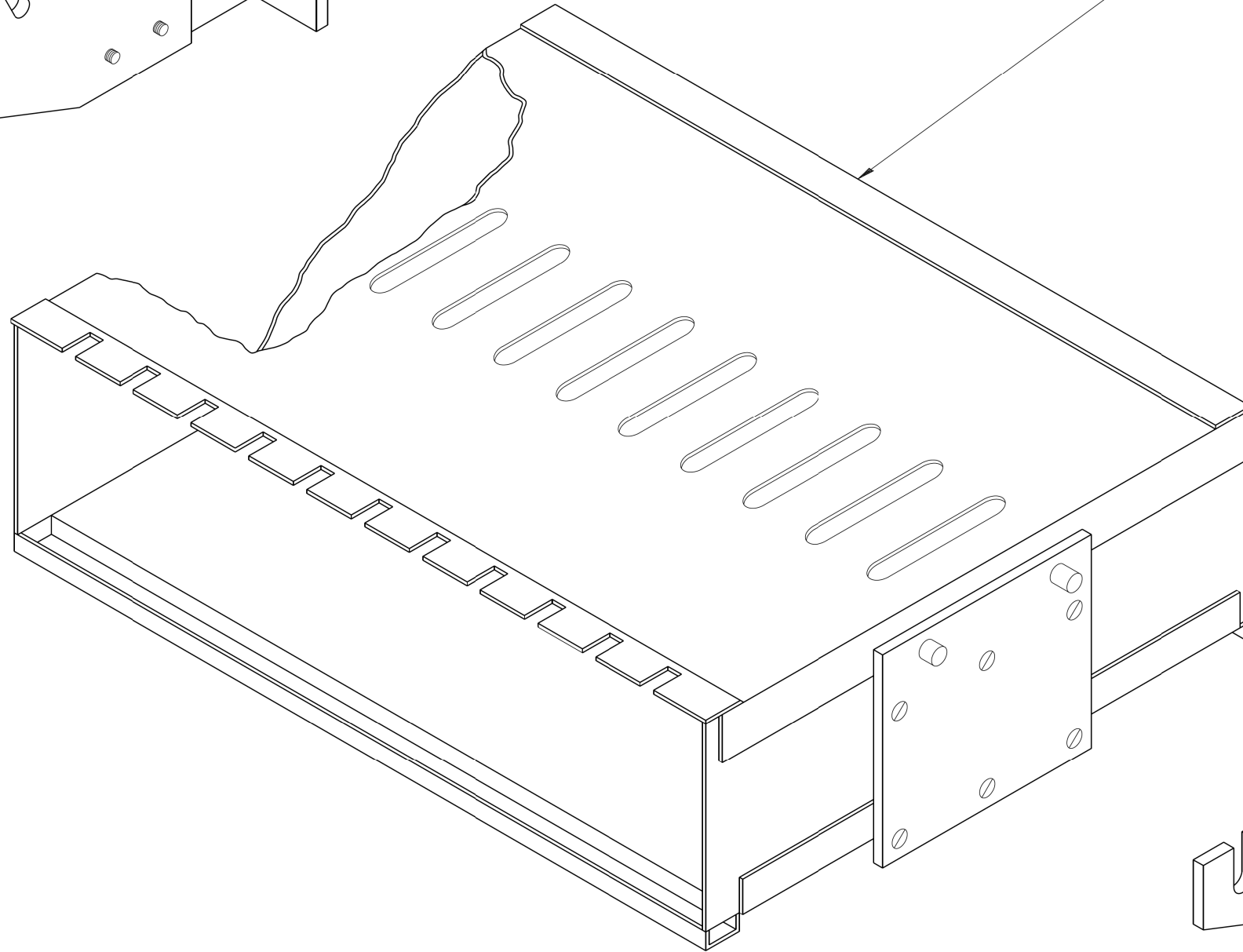
CABINET DETAIL SHEET	
SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	343
TY-ITSC-400#4			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

CRADLE



II MODULE MOUNTING FRAME
(FOR II TYPE "A" PLUG-IN TYPE TONE MODULES)



CRADLE

NOTE:

TYPE "A" TONE MODULES ARE PLUG
IN UNIT MEASURING 5-7/32" (132.55 mm) X 1.5" (38.1mm) X 13-3/4" (349.25 mm)

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PLOT DATE = 10/23/2014

DESIGNED - R.L.
DRAWN - G.M.
CHECKED - R.L.
DATE - 06/21/94

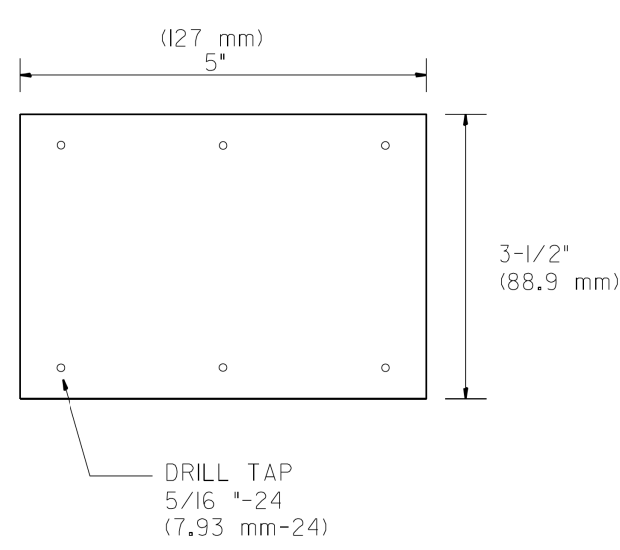
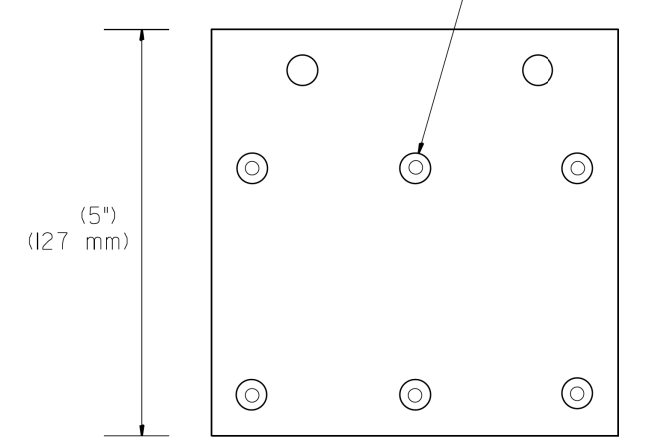
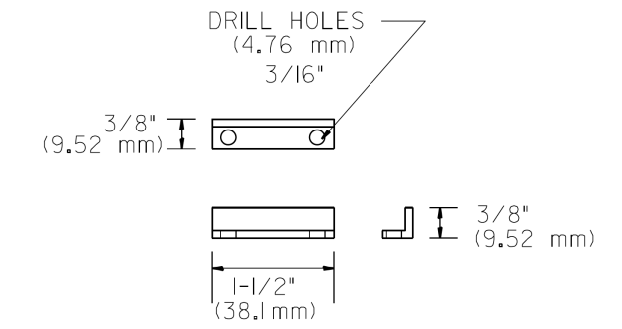
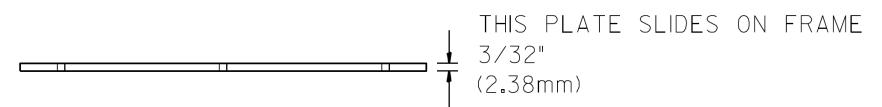
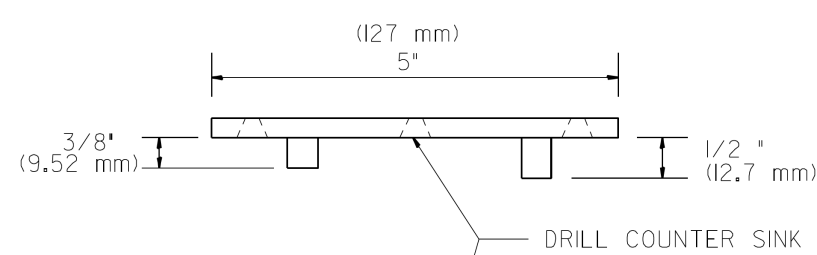
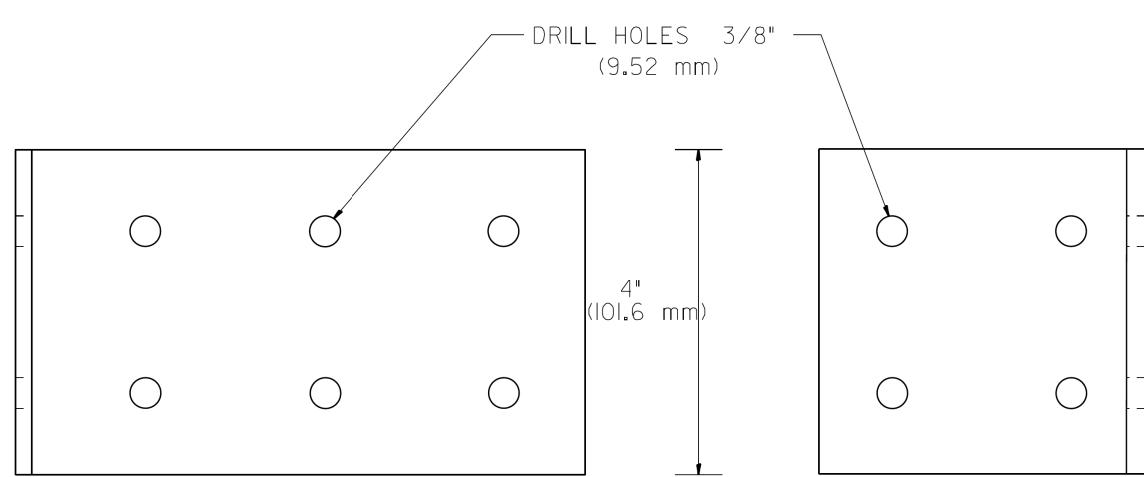
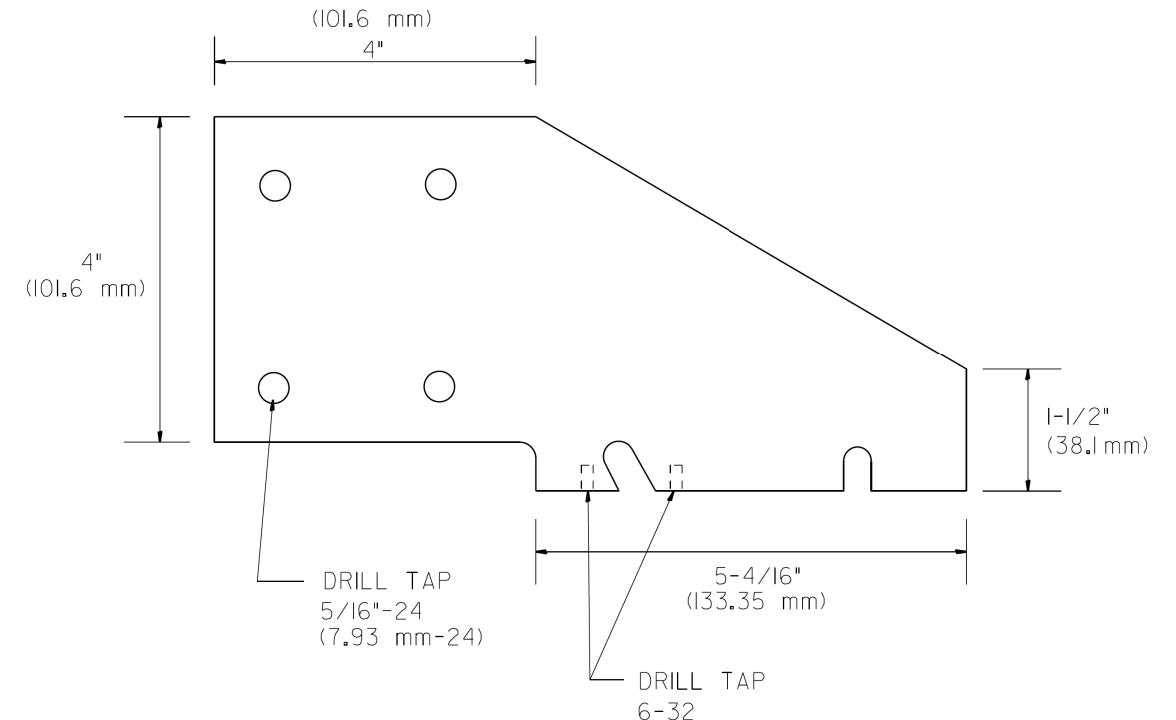
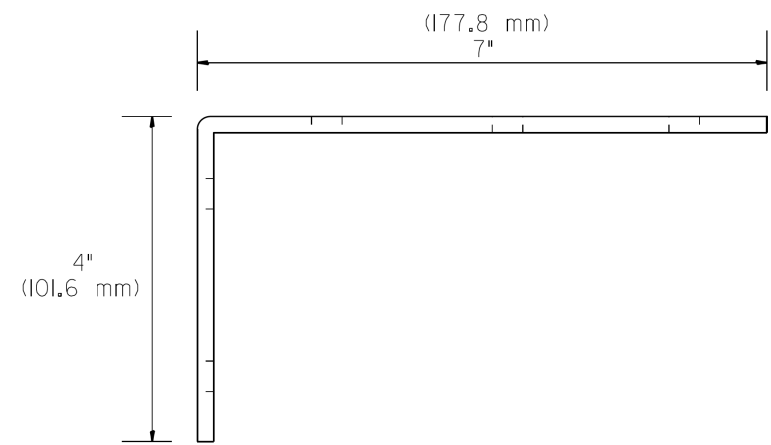
REVISED- 06/94
REVISED- 09/96
REVISED-
REVISED-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FIELD MOUNTING FRAME
WITH CRADLE ASSEMBLY

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	344
TY-ITSC-400#6			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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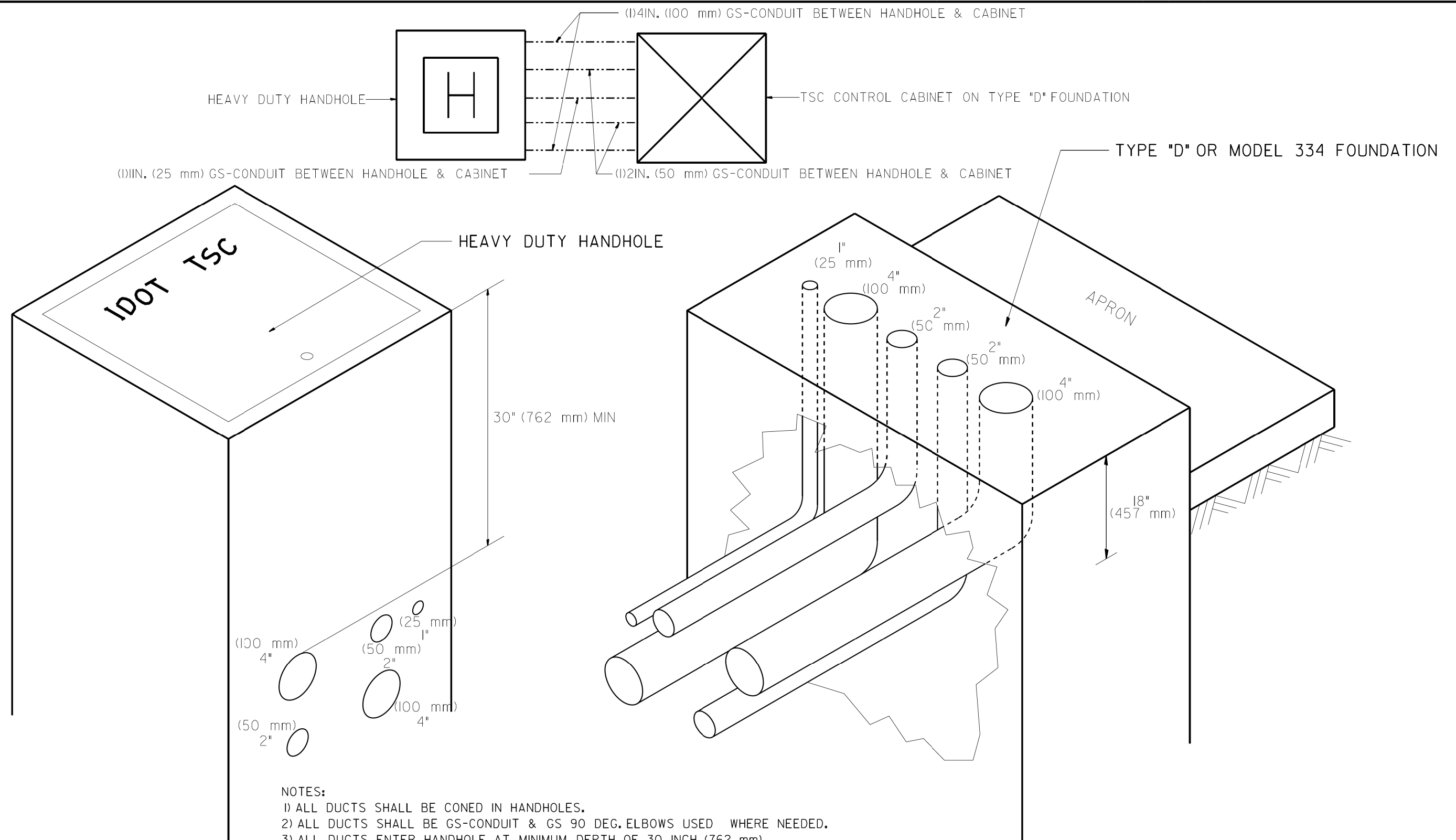
DESIGNED - R.L.	REVISED - 06/94
DRAWN - G.M.	REVISED - 09/96
CHECKED - R.L.	REVISED -
DATE - 06/21/94	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FIELD CRADLE ASSEMBLY

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	345
TY-ITSC-400#7			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

- 1) ALL DUCTS SHALL BE CONED IN HANDHOLES.
- 2) ALL DUCTS SHALL BE GS-CONDUIT & GS 90 DEG. ELBOWS USED WHERE NEEDED.
- 3) ALL DUCTS ENTER HANDHOLE AT MINIMUM DEPTH OF 30 INCH (762 mm)
- 4) ALL HANDHOLE COVERS SHALL READ "IDOT TSC".
- 5) ALL CABINET HANDHOLES SHALL BE HEAVY DUTY.
- 6) DUCTS SHALL BE CENTERED IN CABINET FOUNDATION/HANDHOLE AS SHOWN.
- 7) CONDUITS SHALL BE SPACED 305 mm (1FOOT) CENTER TO CENTER IN HEAVY DUTY HANDHOLE.
- 8) INSTALL 3/4" X 10' (20 mm X 3 m) COPPER CLAD STEEL GROUND ROD IN HDHH PROVIDED AS CABINET PAD. EXOTHERMIC WELD CONNECTION FROM GROUND ROD TO #6 GROUND WIRE INSULATED (GREEN).
- 9) BOND ALL GSC CONDUITS IN CABINET FOUNDATION.
- 10) INSTALL #6 GROUND WIRE IN 1IN. (25 mm) GSC FROM HANDHOLE TO CABINET.
- 11) TYPE "D" FOUNDATION SHALL BE 18" FROM TOP OF FOUNDATION TO FINISHED GRADE.

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DI60W30-SHT-DI-Detail-ITSC-400-11.dgn	DESIGNED - R.L.	REVISED- 09/96
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PLOT SCALE = 50.000' / in.	CHECKED - R.L.	REVISED- 04/99
PLOT DATE = 10/23/2014	DATE - 06/05/95	REVISED- 07/2010

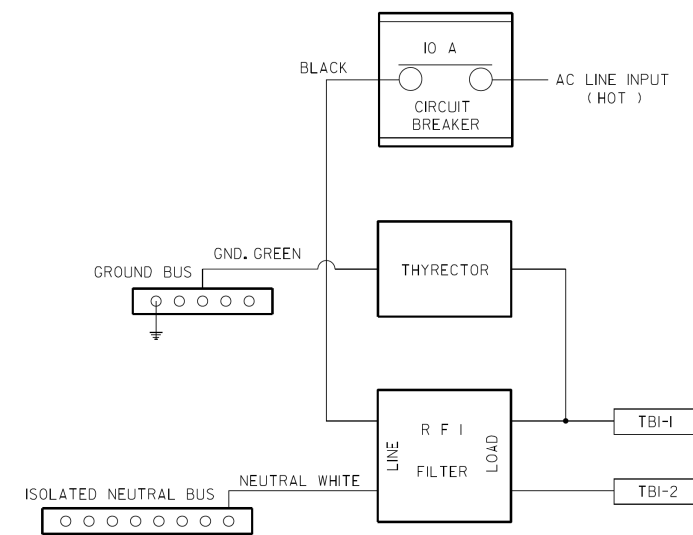
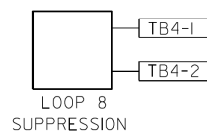
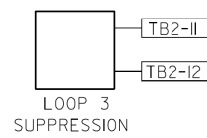
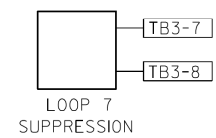
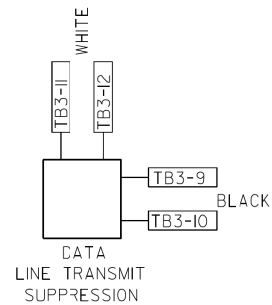
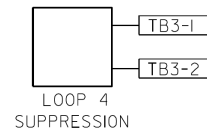
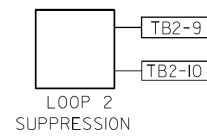
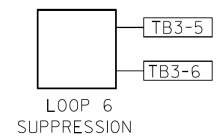
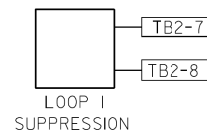
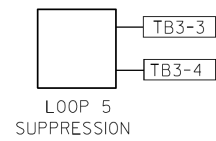
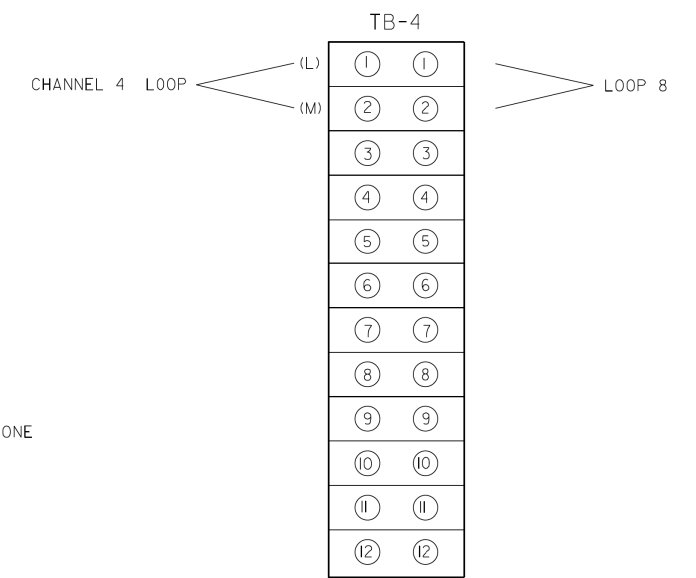
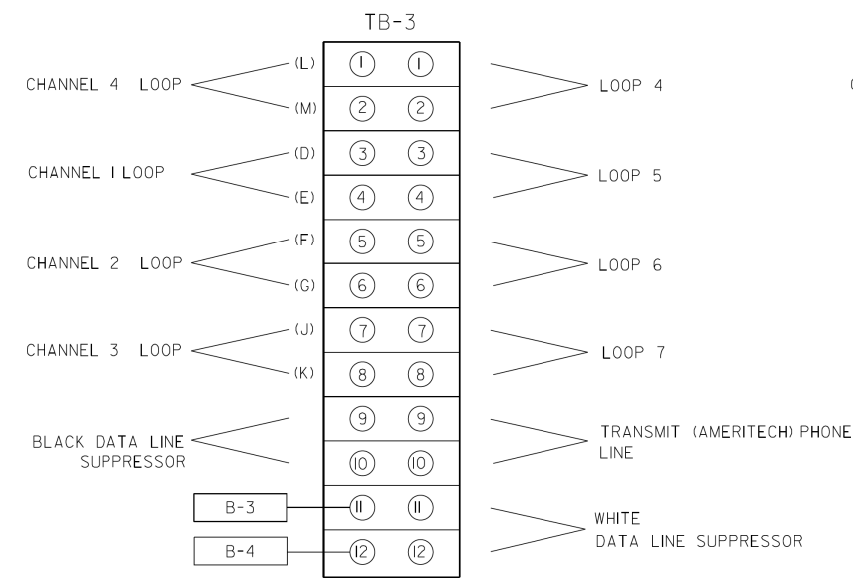
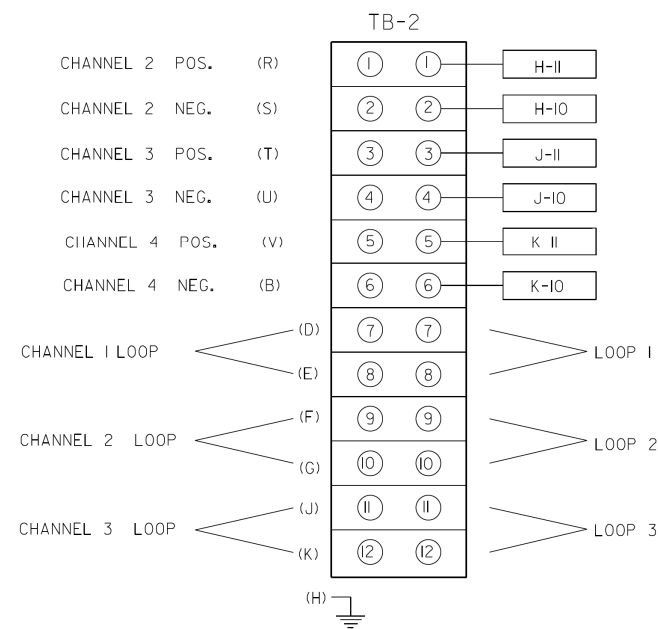
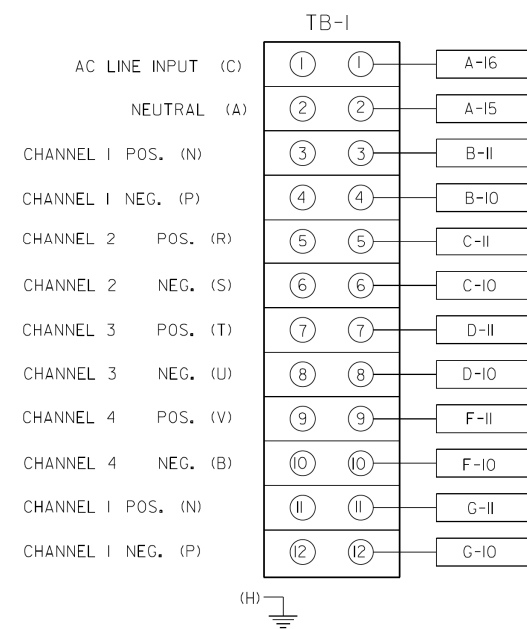
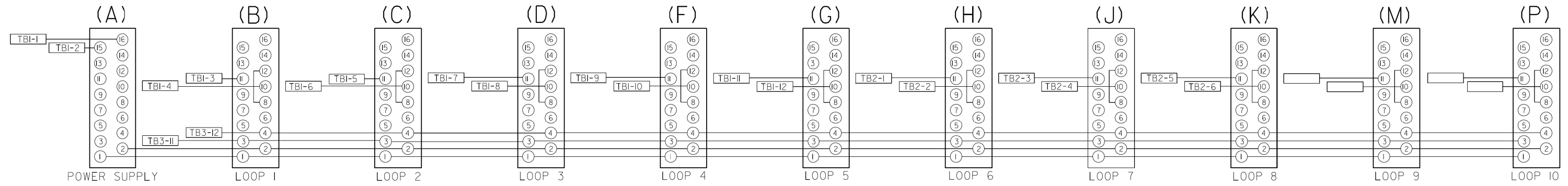
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CABINET - HANDHOLE
CONDUIT DETAIL**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	346
TY-ITSC-400#11			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

BACK VIEW OF TONE RACK



FILE PATH = C:\Users\dishveoz\Desktop\60W30-PL01\DI60W30-SHT-DI-Detail-ITSC-400-13.dgn



DI60W30-SHT-DI-Detail-ITSC-400-13.dgn
 USER NAME = dishveoz
 PLOT SCALE = 50.000' / in.
 PLOT DATE = 10/23/2014

DESIGNED - R.L.
 DRAWN - G.M.
 CHECKED - R.L.
 DATE - 10-17-95

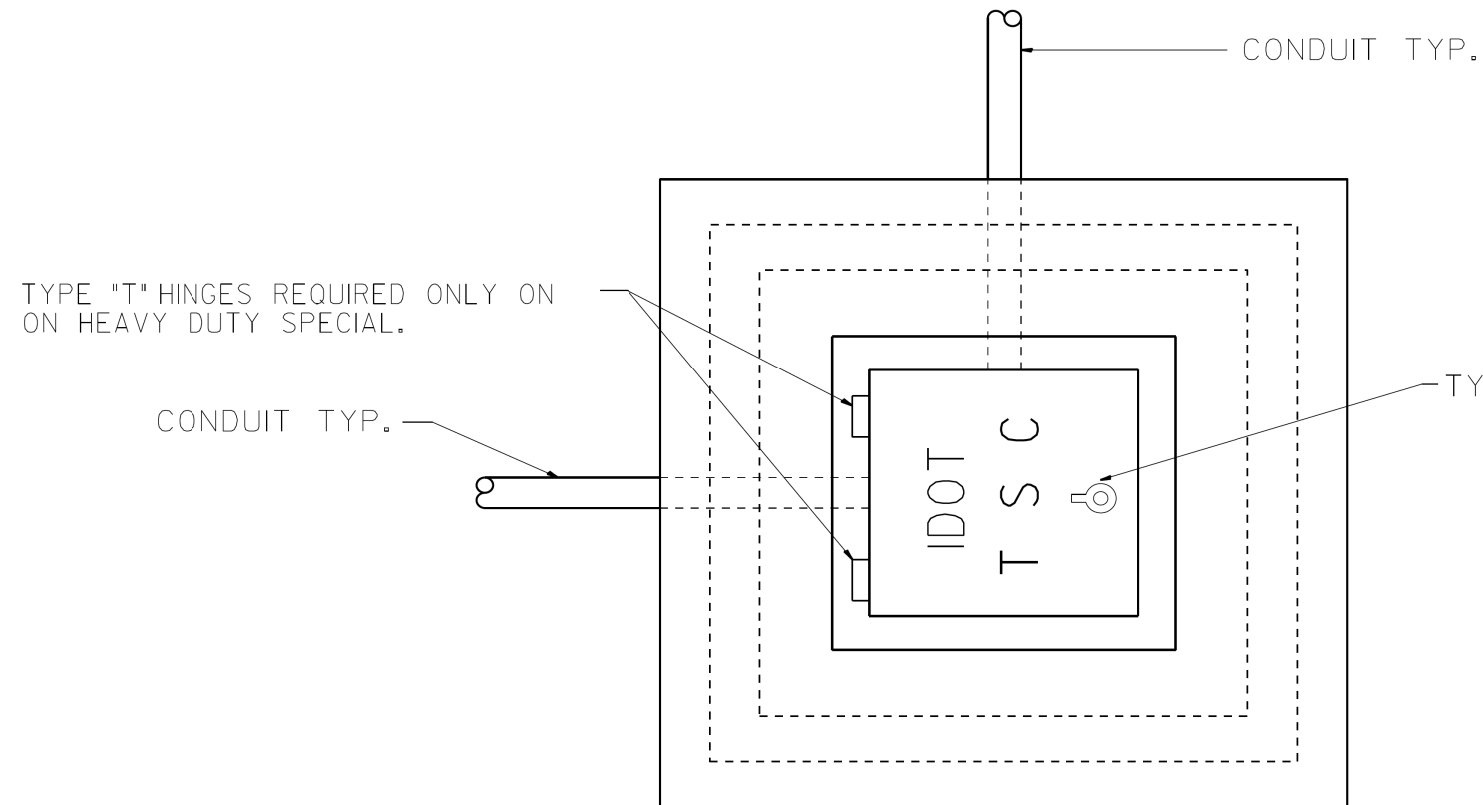
REVISED- 02-98
 REVISED- 05-99
 REVISED-
 REVISED-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPE 3 CABINET
 WIRING DIAGRAM

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	347
TY-ITSC-400#13			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

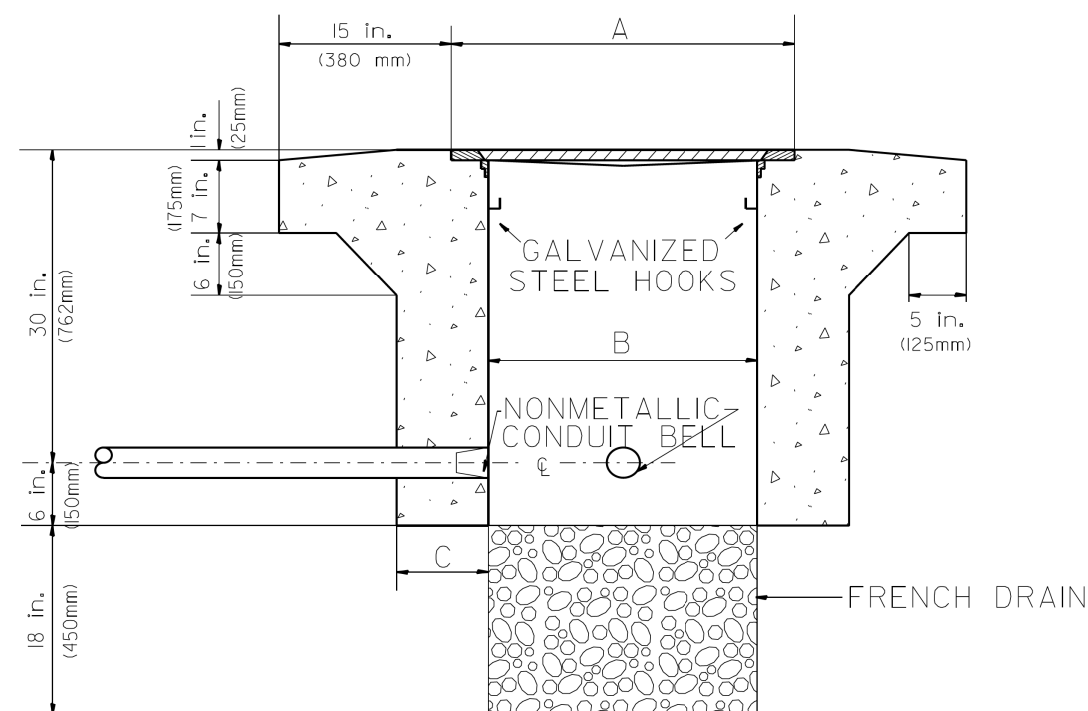


PLAN

HEAVY DUTY HANDHOLE MINIMUM DIMENSIONS (UNHINGED)

A	28" (711 mm)
B	22" (559 mm)
C	8" (200 mm)

(FRAME AND COVER 260 LBS. (118 Kg.) MIN.)



ELEVATION

HEAVY DUTY HANDHOLE SPECIAL MINIMUM DIMENSIONS

A	31.5" (800 mm)
B	30.0" (762 mm)
C	10.0" (250 mm)

(FRAME AND COVER 405 LBS. (184 Kg. (405))

PC CONCRETE - HEAVY DUTY HAND HOLE

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DI60W30-SHT-DI-Detail-ITSC-400-15.dgn
 USER NAME = dishevoz
 PLOT SCALE = 50.000' / in.
 PLOT DATE = 10/23/2014

DESIGNED - R.L.
 DRAWN - G.M.
 CHECKED - R.L.
 DATE - 09/11/96

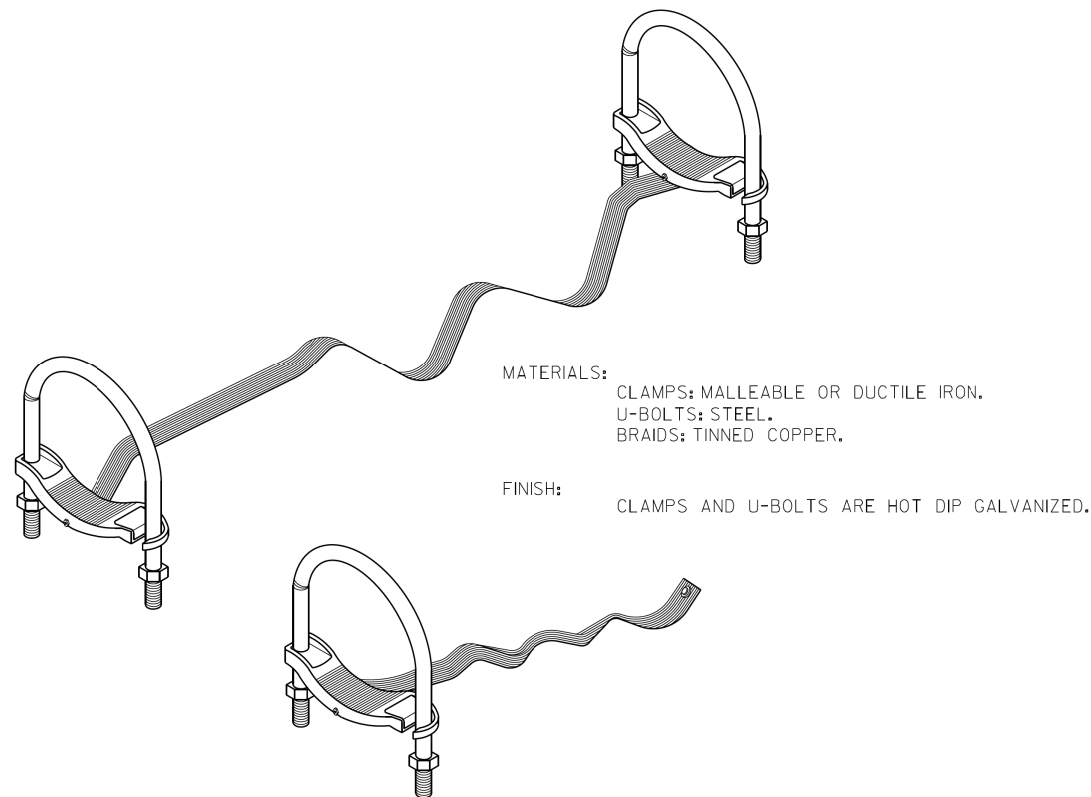
REVISED- 04/97
 REVISED-
 REVISED-
 REVISED-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PC CONCRETE - HEAVY DUTY
 HAND HOLE

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

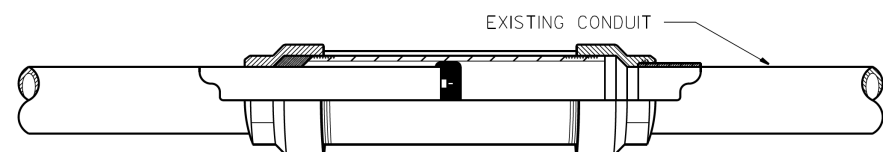
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	348
TY-ITSC-400#15			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



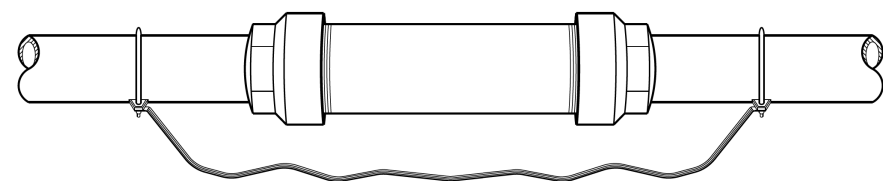
MATERIALS:
 CLAMPS: MALLEABLE OR DUCTILE IRON.
 U-BOLTS: STEEL.
 BRAIDS: TINNED COPPER.

FINISH:
 CLAMPS AND U-BOLTS ARE HOT DIP GALVANIZED.

GROUNDING & BONDING JUMPERS FOR RIGID STEEL, IMC & EMT



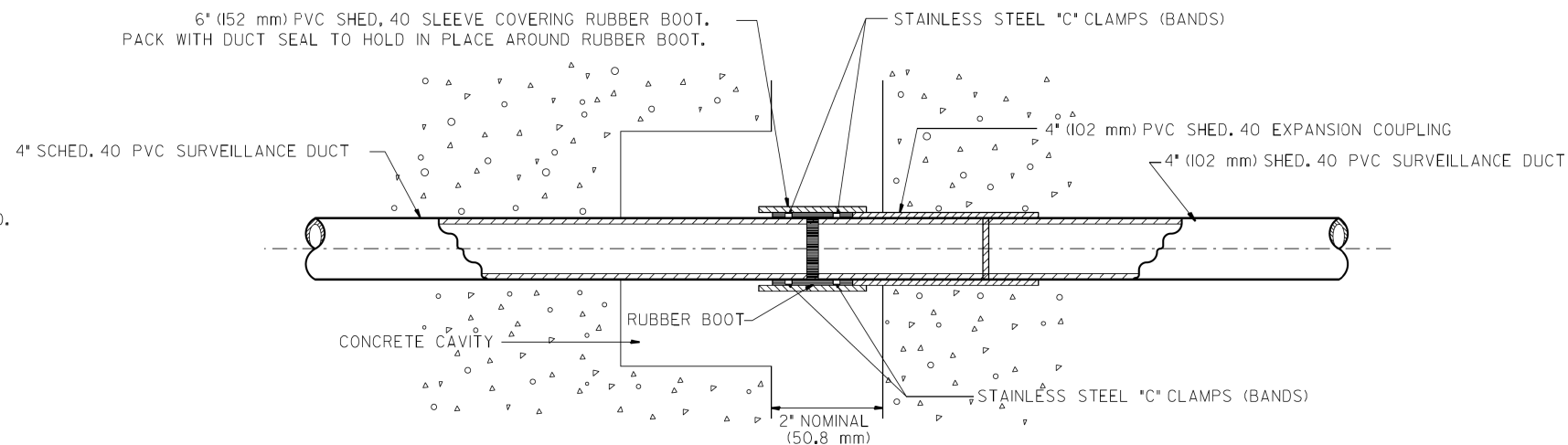
CROSS SECTION



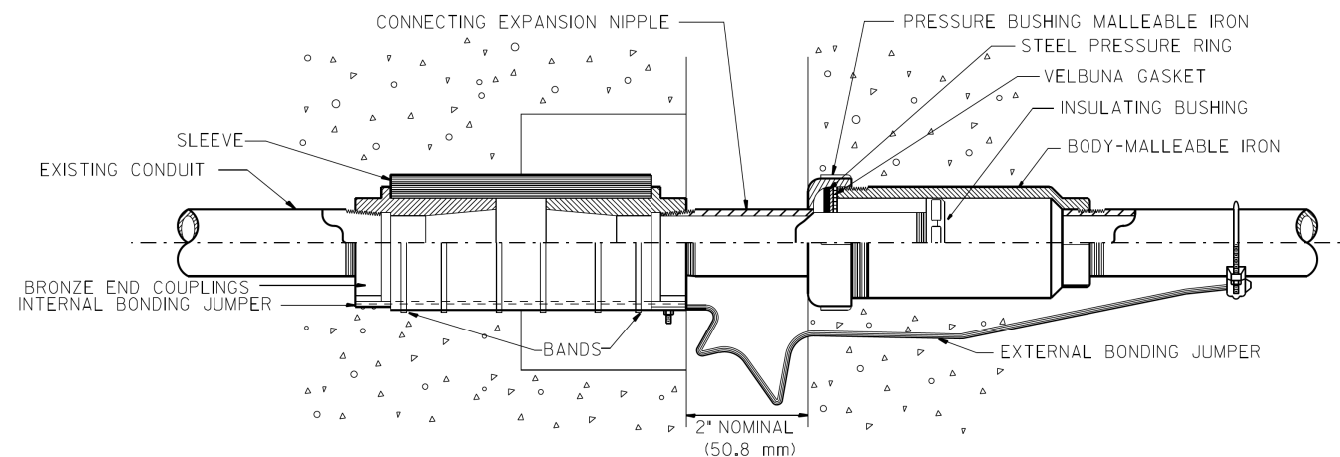
BONDING EXPANSION FITTINGS

MATERIALS:
 HEAD: MALLEABLE OR DUCTILE IRON.
 SLEEVE: STEEL.
 INSULATING BUSHING: PHENOLIC.

FINISH:
 HOT DIP GALVANIZED.



EXPANSION/DEFLECTION FITTING



COMBINATION DEFLECTION/EXPANSION FITTINGS FOR RIGID METAL CONDUIT & IMC

FITTING CAN BE USED EXPOSED OR EMBEDDED IN CONCRETE.

MATERIALS:
 SLEEVE: NEOPRENE.
 END COUPLINGS: BRONZE.
 BONDING JUMPER: TINNED COPPER BRAIDS.
 BANDS: STAINLESS STEEL.

FINISH:
 ALL MALLEABLE, DUCTILE IRON OR STEEL PARTS
 ARE HOT DIP GALVANIZED.

FILE PATH = C:\Users\dishevoz\Desktop\60W30_PLOT\DI160W30-SHT-DI-Details-ITSC-400-18.dgn



DI160W30-SHT-DI-Details-ITSC-400-18.dgn
 USER NAME = dishevoz
 PLOT SCALE = 50.0000' / in.
 PLOT DATE = 10/23/2014

DESIGNED - R.L.
 DRAWN - G.M.
 CHECKED - R.L.
 DATE - 01/22/98

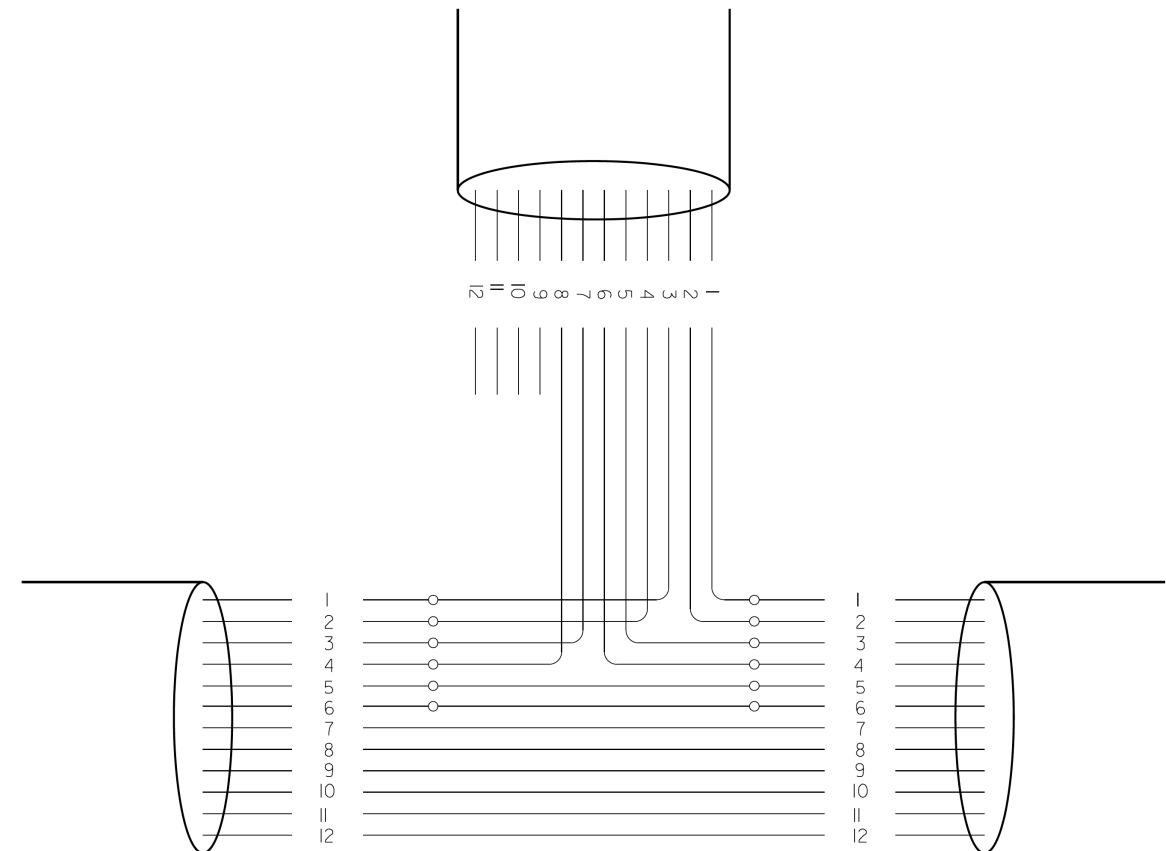
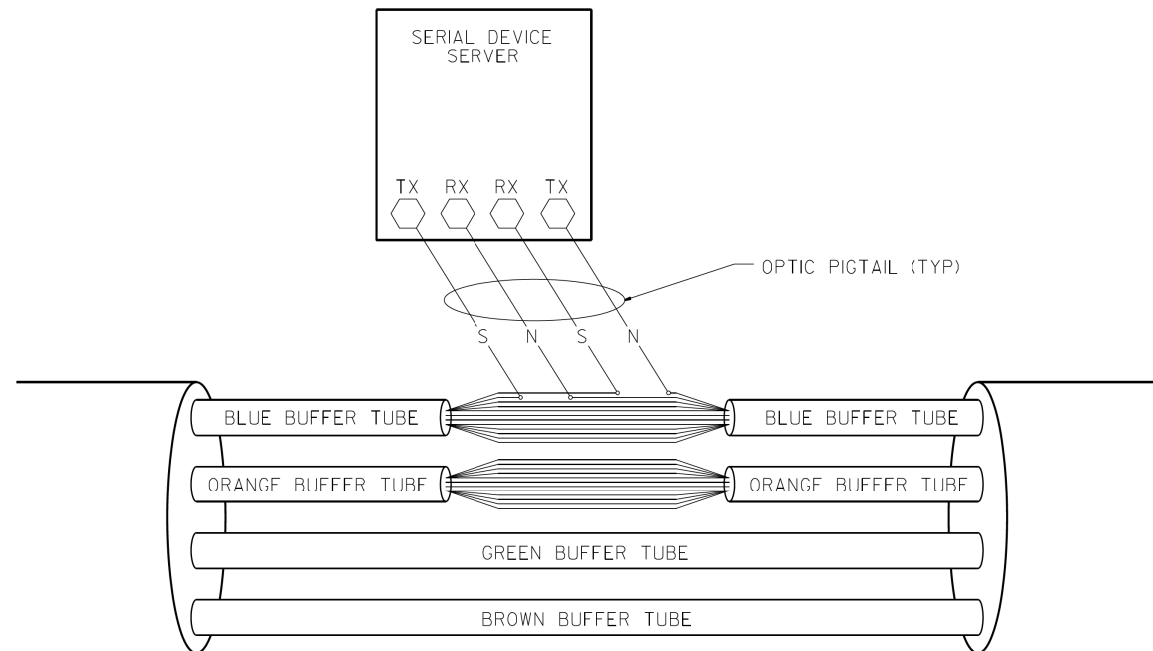
REVISED- 03/99
 REVISED-
 REVISED-
 REVISED-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXPANSION FITTING
 DETAIL SHEET

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	349
TY-ITSC-400#18			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PHYSICAL SPLICE DETAILS (TYP)
(NOT TO SCALE)

FIBERS		FUNCTION	APPLICATION DESCRIPTION
FIBER NO.	COLOR CODE		
1	BLUE	IN TX	DATA CIRCUIT
2	ORANGE	IN RX	
3	GREEN	OUT RX	
4	BROWN	OUT TX	
5	SLATE	IN TX	DMS
6	WHITE	IN RX	
7	RED	OUT RX	
8	BLACK	OUT TX	
9	YELLOW	IN TX	CCTV
10	VIOLET	IN TX	
11	ROSE	OUT RX	
12	AQUA	OUT TX	

NOTE:

1.- THIS DIAGRAM IS PROVIDED FOR ILLUSTRATION PURPOSES ONLY AND DEPICTS A TYPICAL FIBER OPTIC SPLICE.

FILE PATH = C:\Users\dshveoz\Desktop\60W30_PLOT\DI160W30-SHT-DI-Detail-ITSC-400-41.dgn



DI160W30-SHT-DI-Detail-ITSC-400-41.dgn
 USER NAME = dshveoz
 PLOT SCALE = 50.000' / in.
 PLOT DATE = 10/23/2014

DESIGNED - J.G.
 DRAWN - G.M.
 CHECKED - J.G.
 DATE - 12/29/09

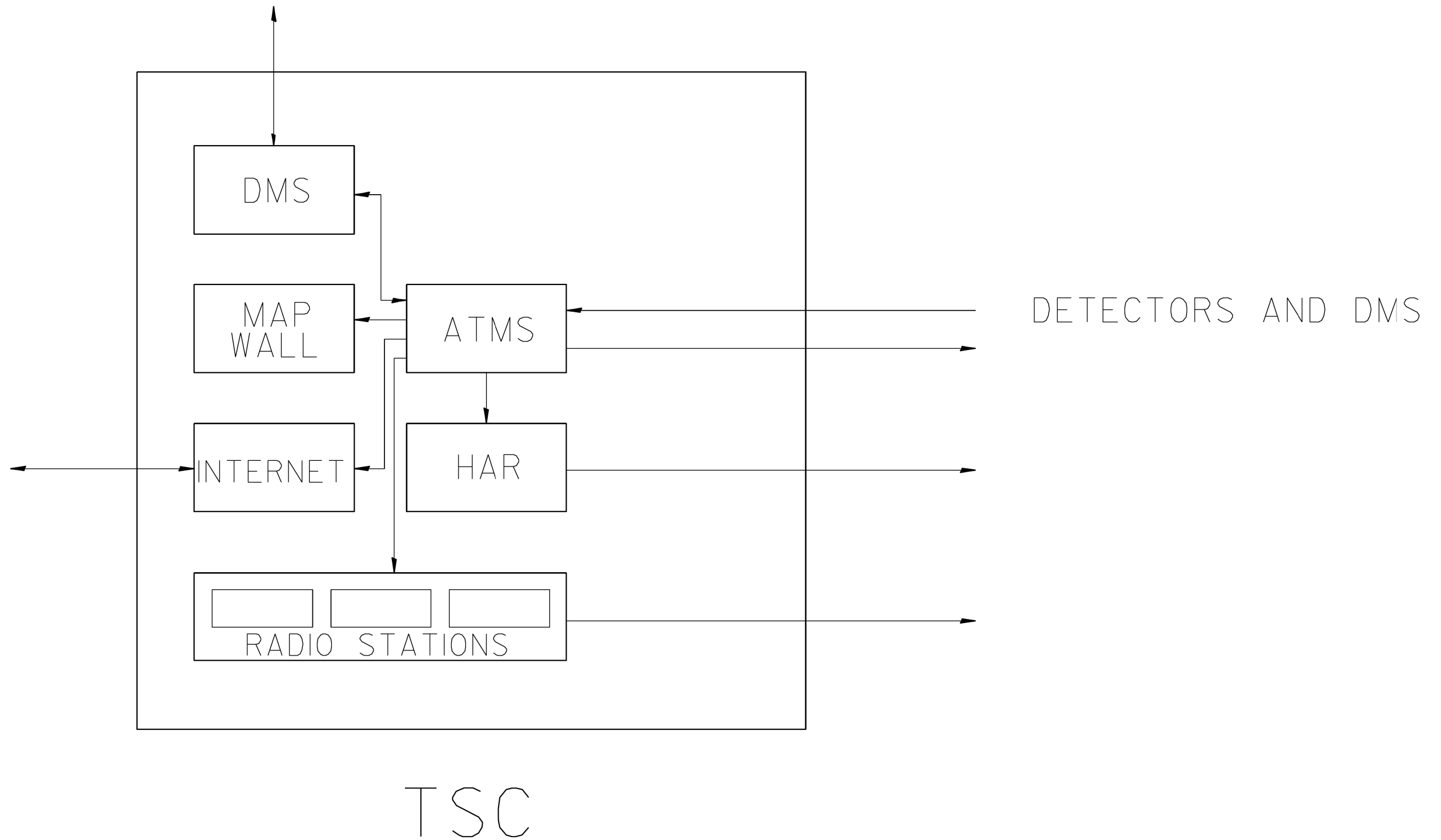
REVISED- 03/04/2010
 REVISED-
 REVISED-
 REVISED-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FIBER OPTIC
 SPLICING TYPICAL

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	350
TY-ITSC-400#41			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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D160W30-SHT-D1-Detail-ITSC-400-43.dgn
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DESIGNED - J.G.
 DRAWN - G.M.
 CHECKED - J.G.
 DATE - 01/07/2010

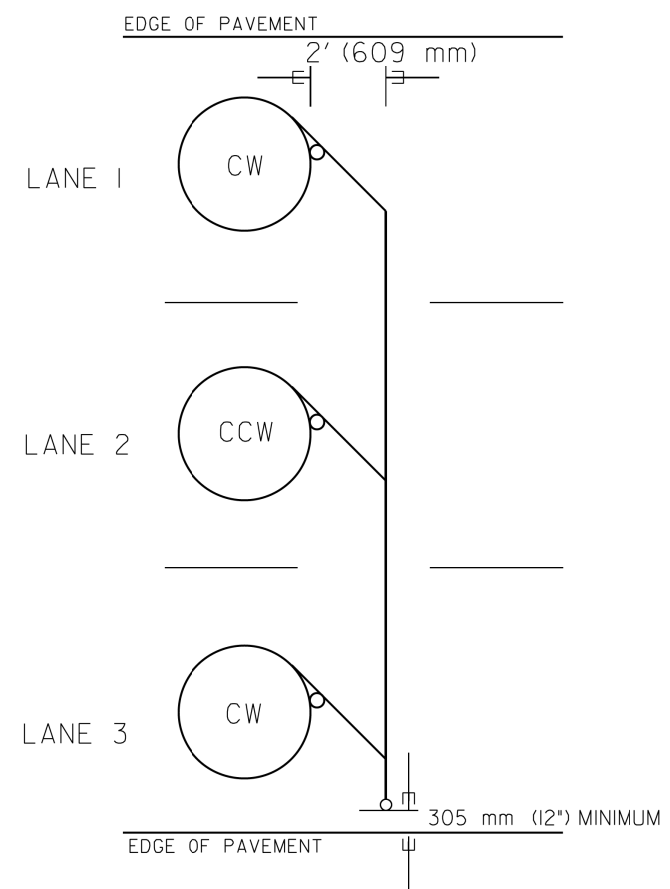
REVISED-
 REVISED-
 REVISED-
 REVISED-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

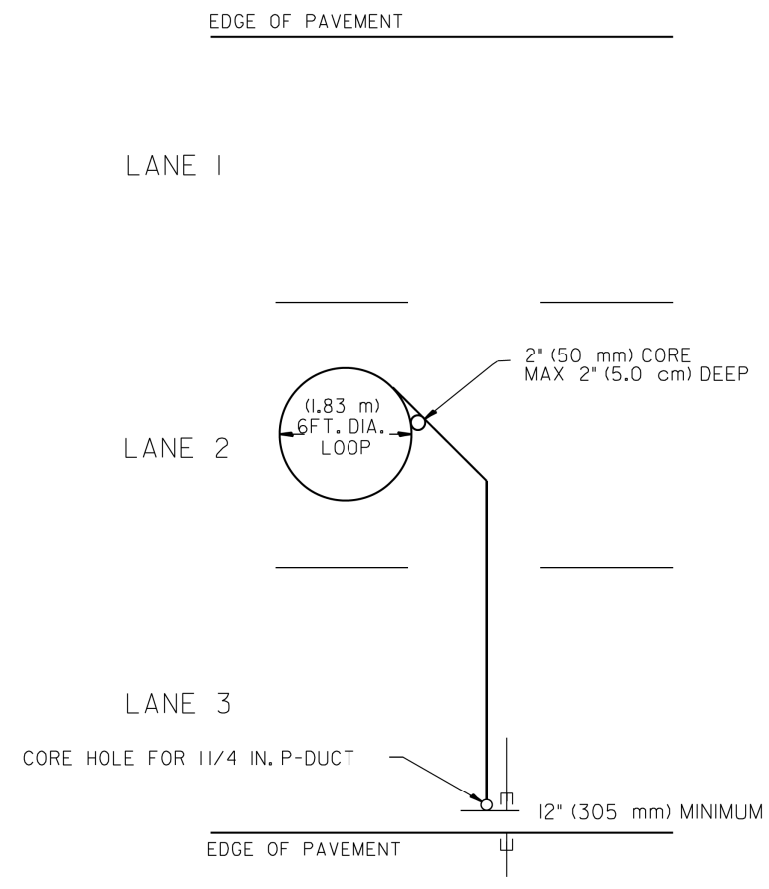
TSC ATMS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

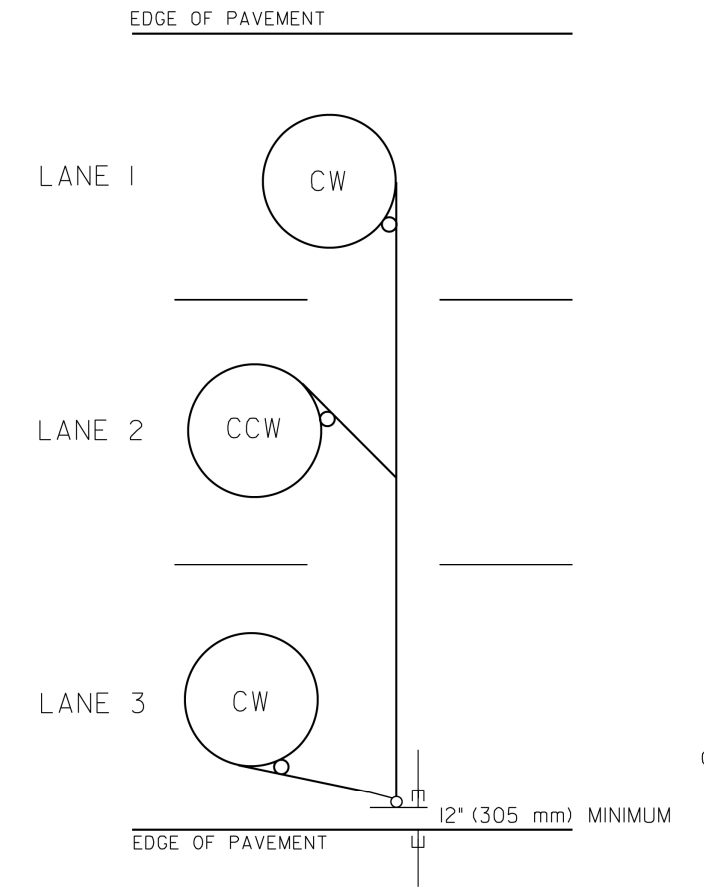
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



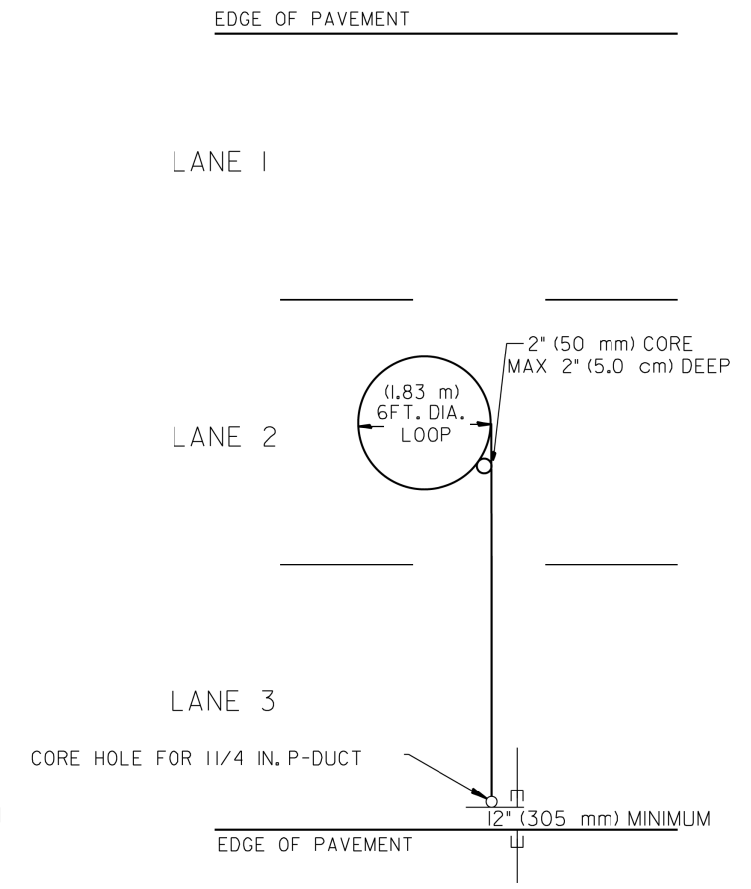
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



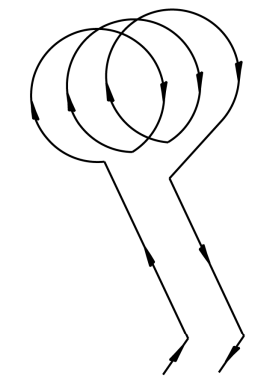
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



WIRING DETAILS

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

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DI60W30-SHT-DI-Detail-ITSC-418-1.dgn
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PLOT SCALE = 50.000' / in.
PLOT DATE = 10/23/2014

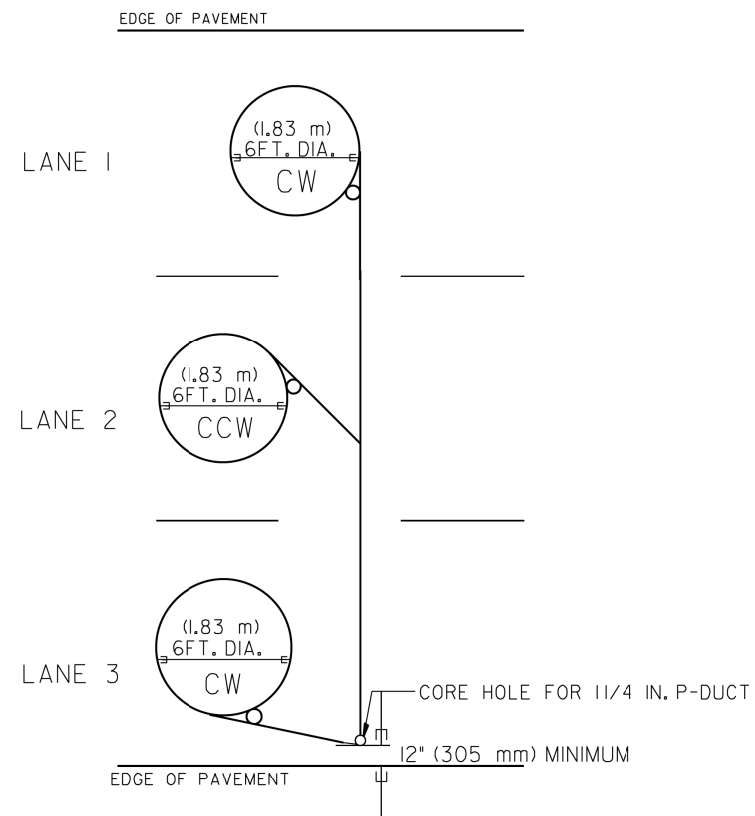
DESIGNED - R.L.	REVISED - 06/94
DRAWN - G.M.	REVISED - 9/96
CHECKED - R.L.	REVISED - R.L. 03/2011
DATE - 06/22/94	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING ROUND LOOP
INSTALLATION

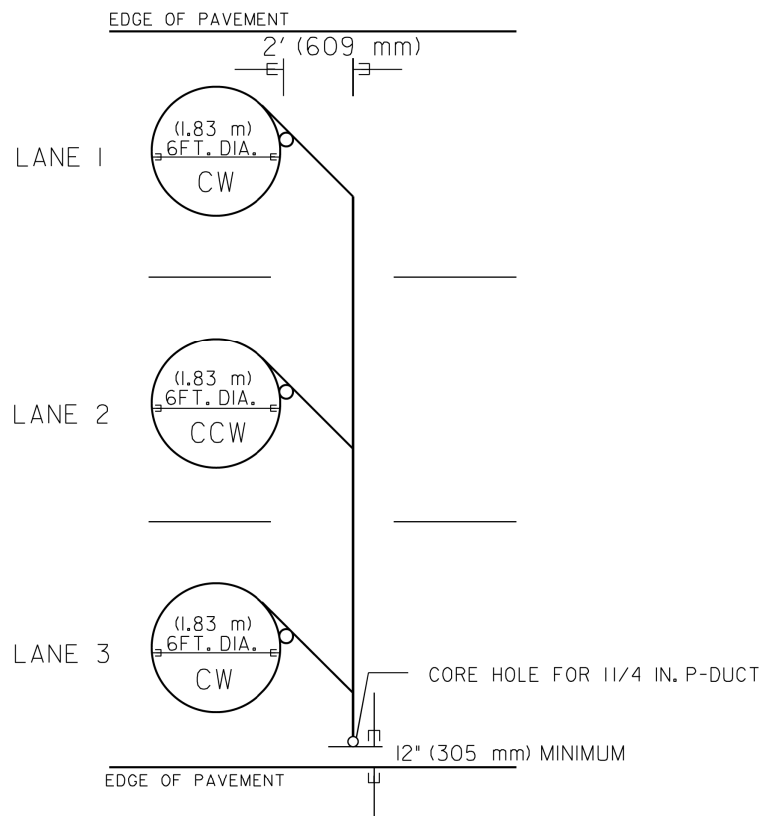
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	352
TY-ITSC-418#1			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



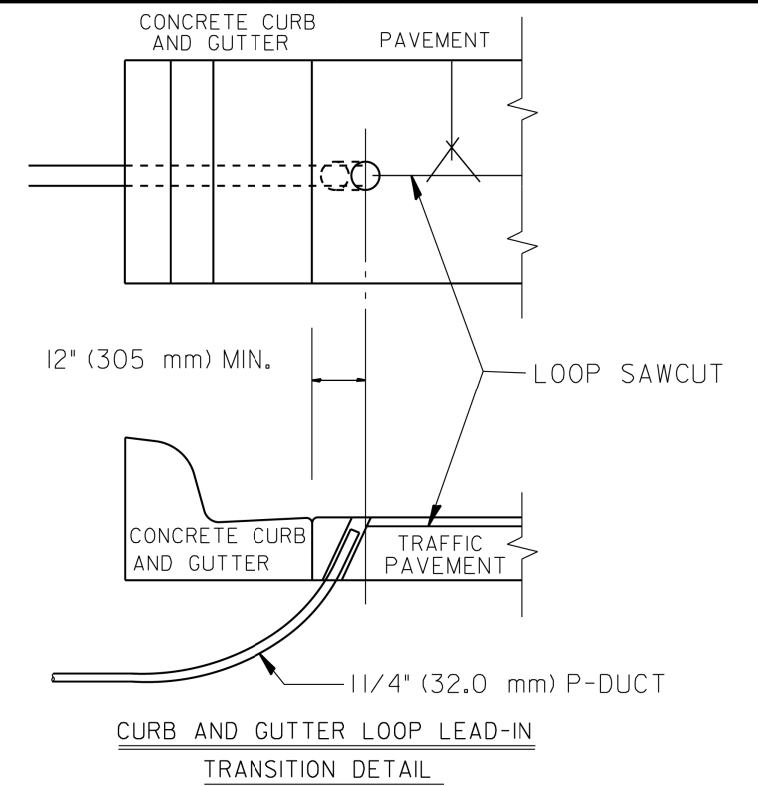
TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL

LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL

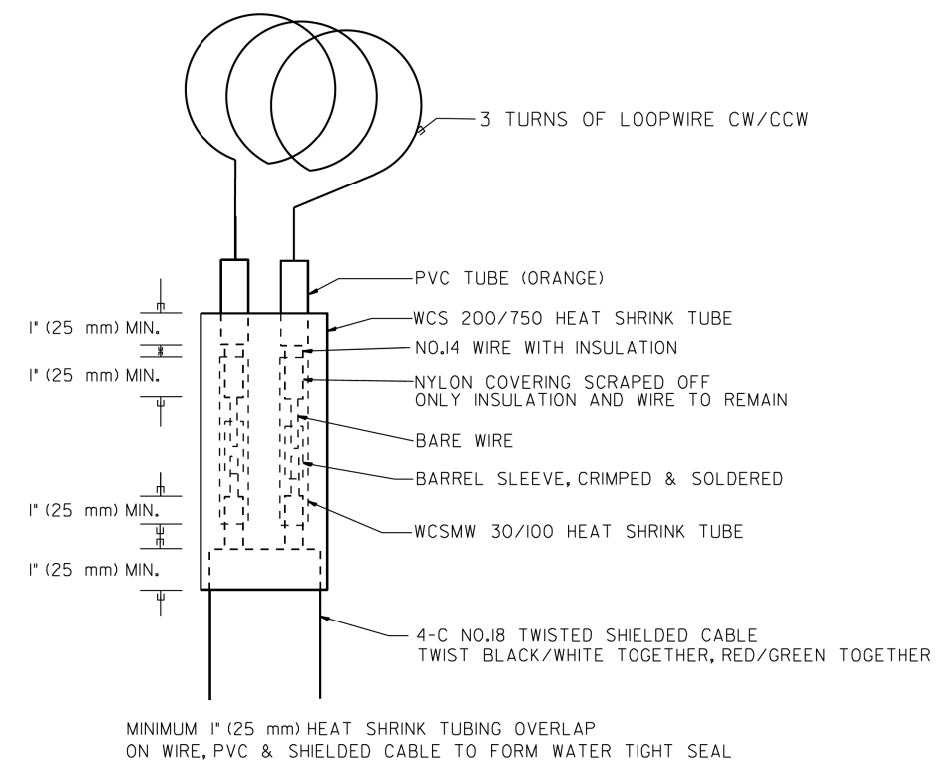
LAYOUT FOR MULTIPLE LANE ROADWAY



CURB AND GUTTER LOOP LEAD-IN
TRANSITION DETAIL

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150FT. (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.



LOOP SPLICING REQUIREMENTS

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USER NAME = dishevoz
PLOT SCALE = 50.000' / in.
PLOT DATE = 10/23/2014

DESIGNED - R.L.
DRAWN - G.M.
CHECKED - R.L.
DATE - 06/22/94

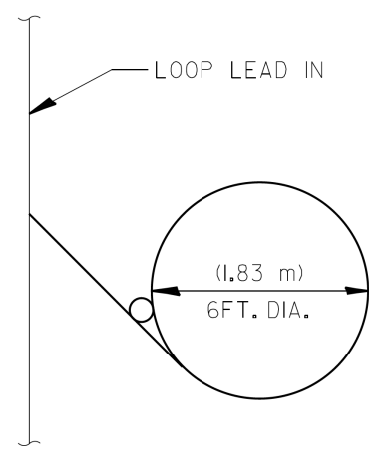
REVISED- 06/94
REVISED- 10/96
REVISED- R.L. 03/2011
REVISED-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

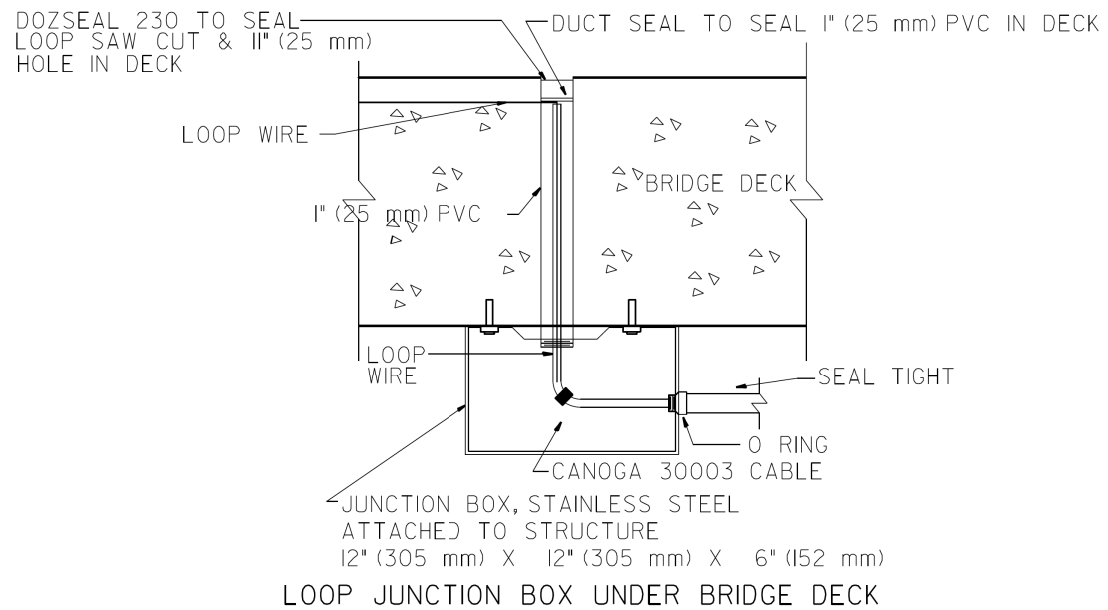
EXISTING ROUND INDUCTION LOOP
TYPICALS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

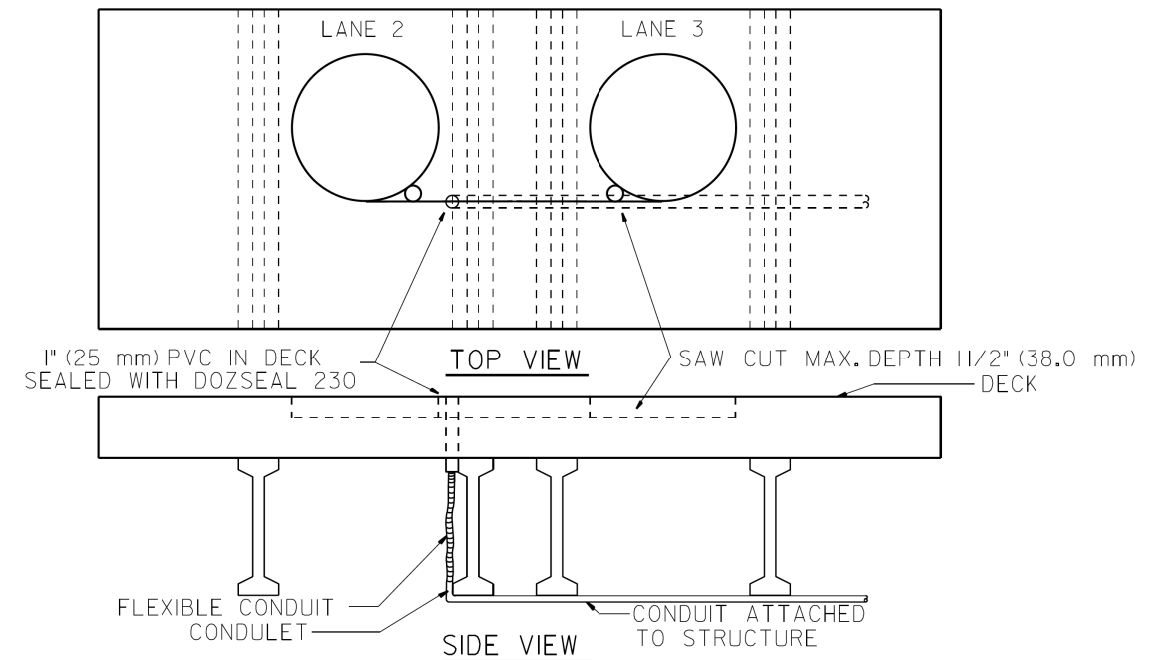
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TY-ITSC-418#2			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



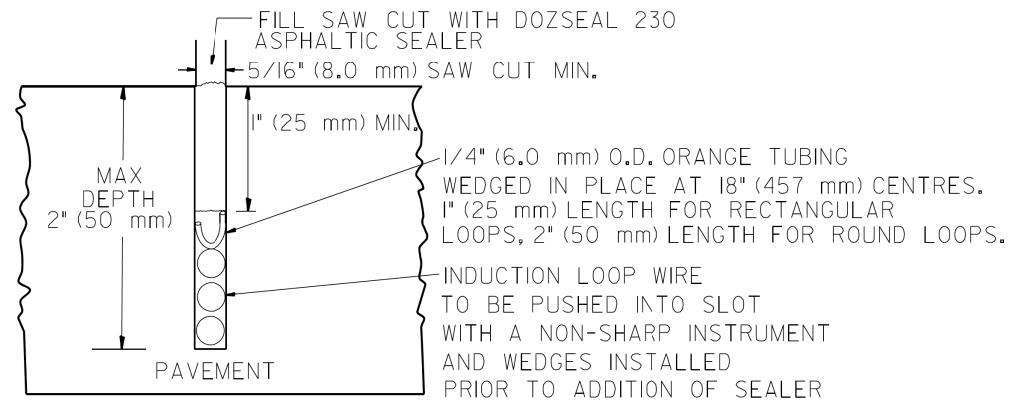
TYPICAL LOOP SAWCUT LAYOUT



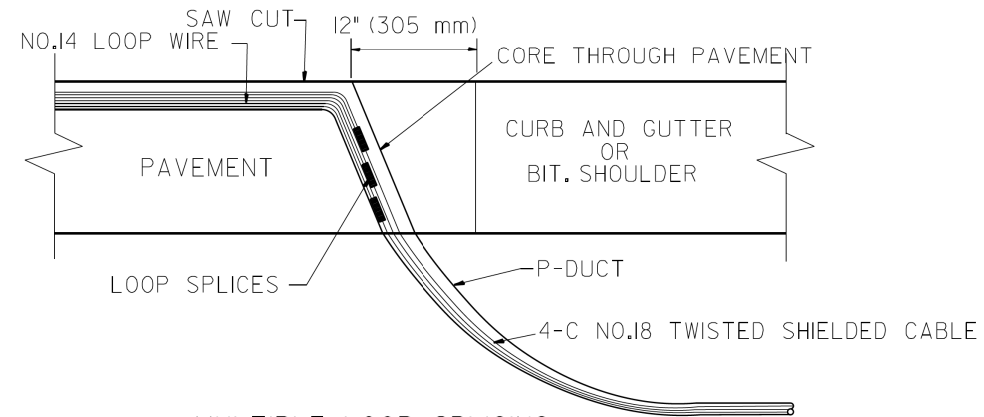
LOOP JUNCTION BOX UNDER BRIDGE DECK



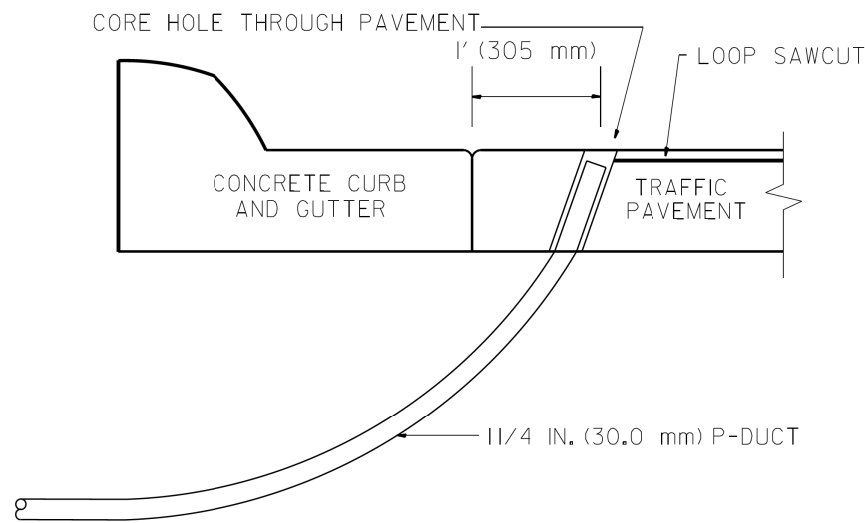
LOOP TRANSITION IN BRIDGE DECK



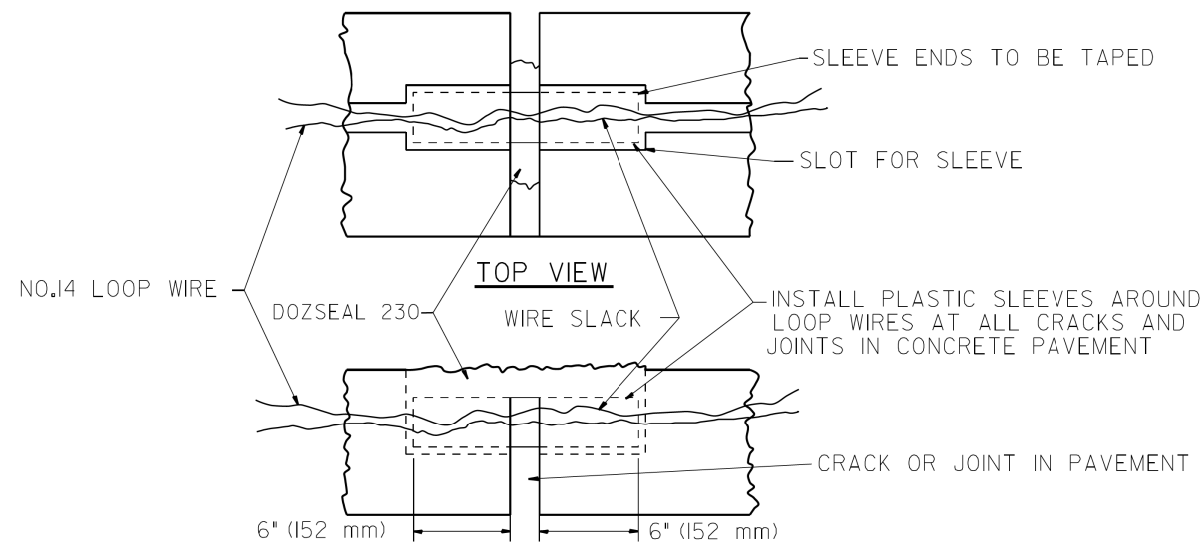
LOOP CROSS SECTION IN PAVEMENT



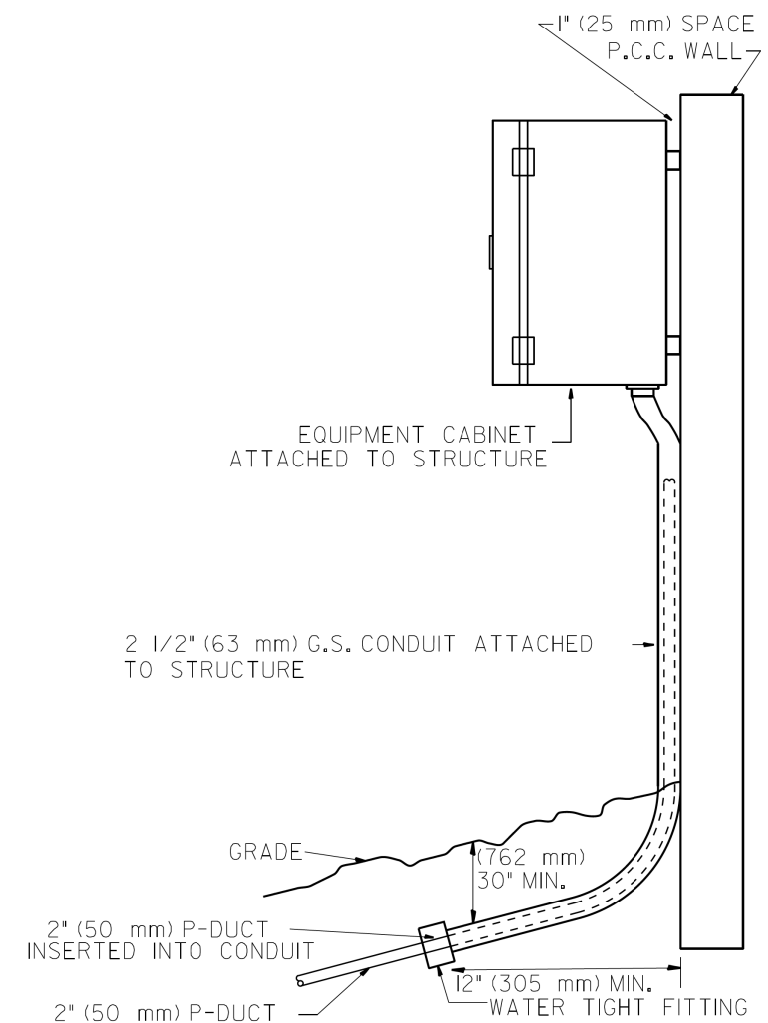
MULTIPLE LOOP SPLICING



SIDE SECTION LOOP LEAD-IN TRANSITION DETAIL



PAVEMENT CRACK TRANSVERSE SLEEVE



POLYETHYLENE DUCT/GS. CONDUIT TRANSITION

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 PLOT SCALE = 50.000' / in.
 PLOT DATE = 10/23/2014

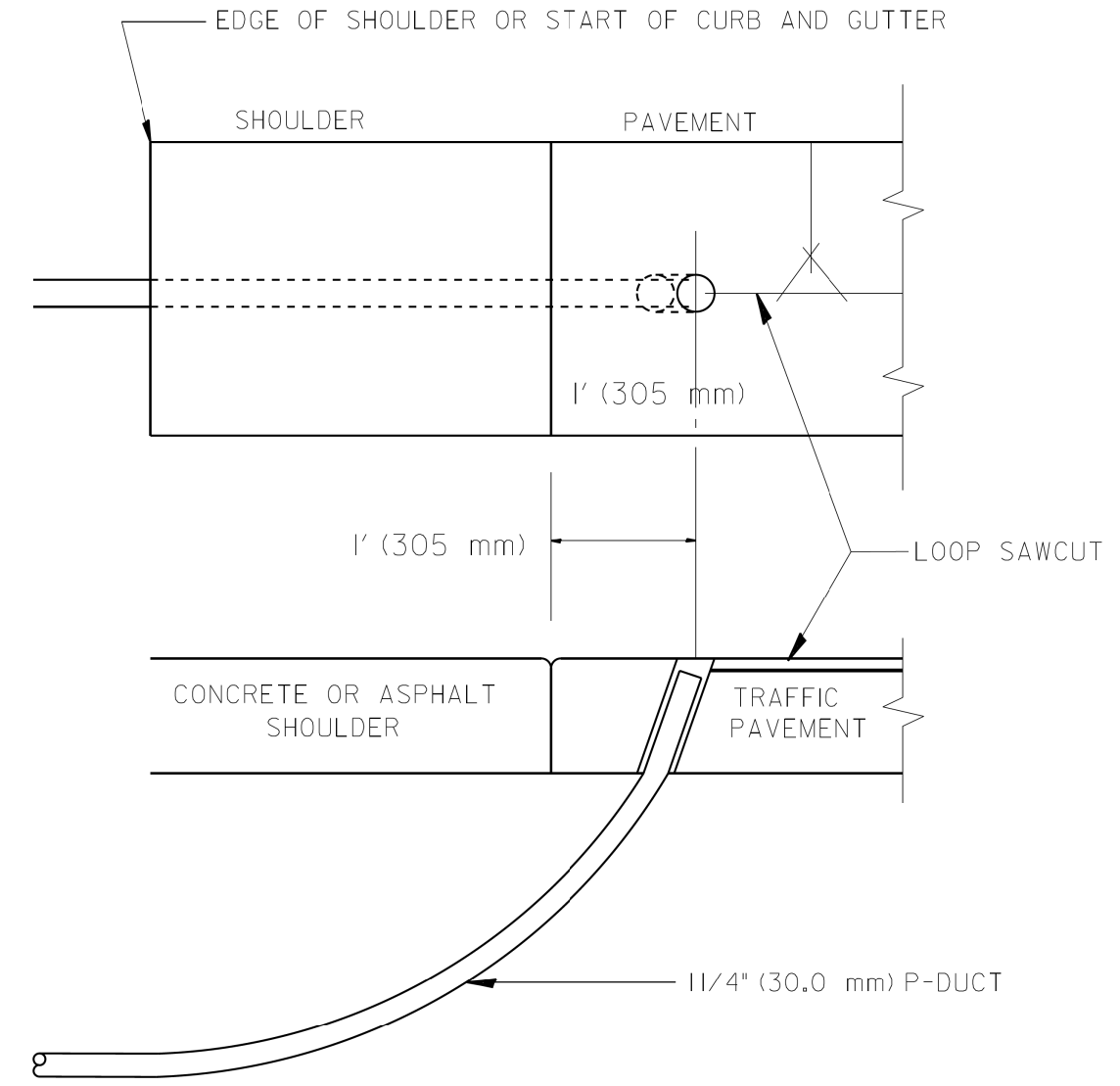
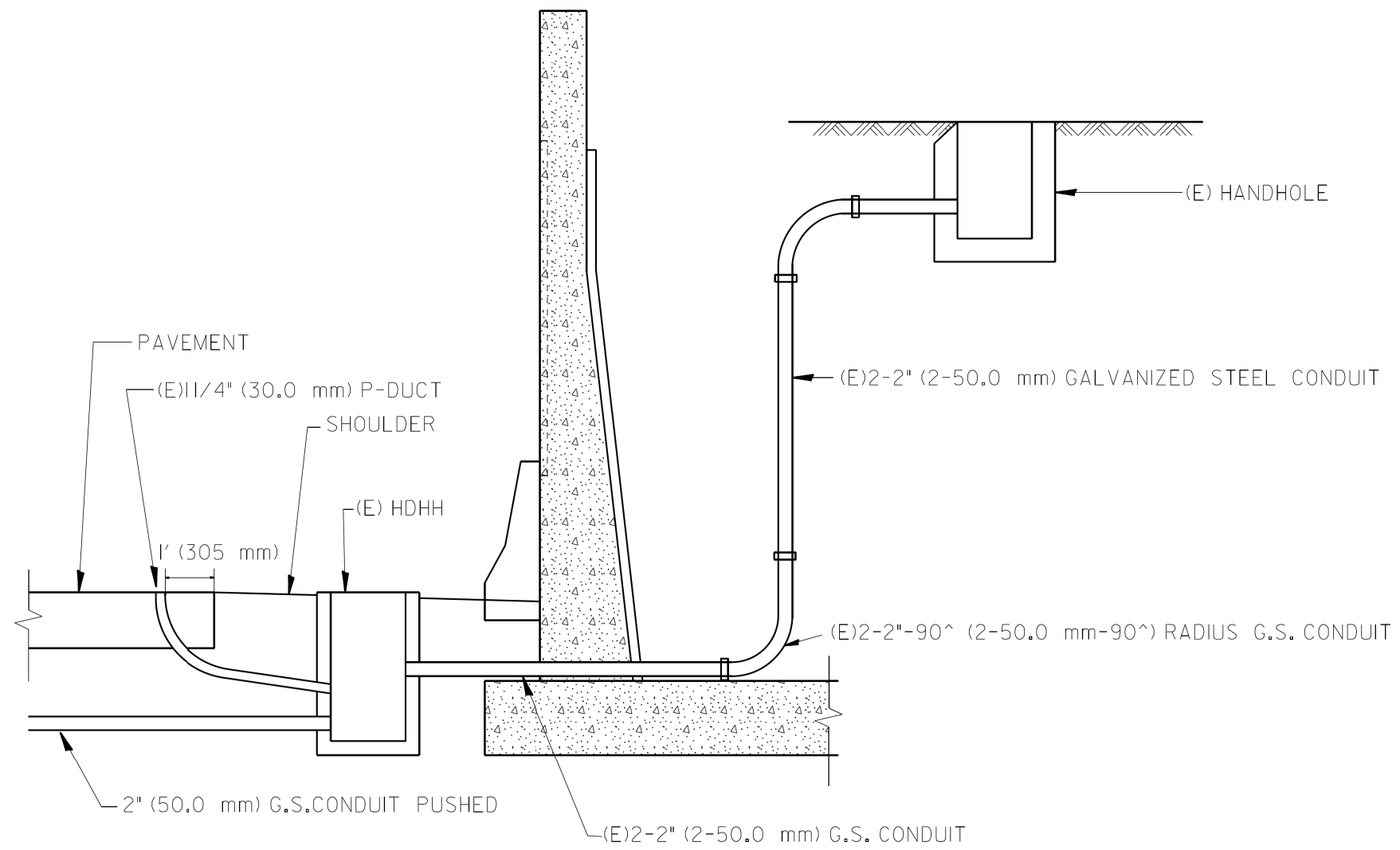
DESIGNED - R.L.	REVISED - 06/94
DRAWN - G.M.	REVISED - 03/95
CHECKED - R.L.	REVISED - 11/95
DATE - 06/22/94	REVISED - 10/96

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

LOOP, CONDUIT & DUCT
 INSTALLATION DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	354
TY-ITSC-418#4			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SHOULDER LOOP LEAD-IN
TRANSITION DETAIL

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 PLOT SCALE = 50.000' / in.
 PLOT DATE = 10/23/2014

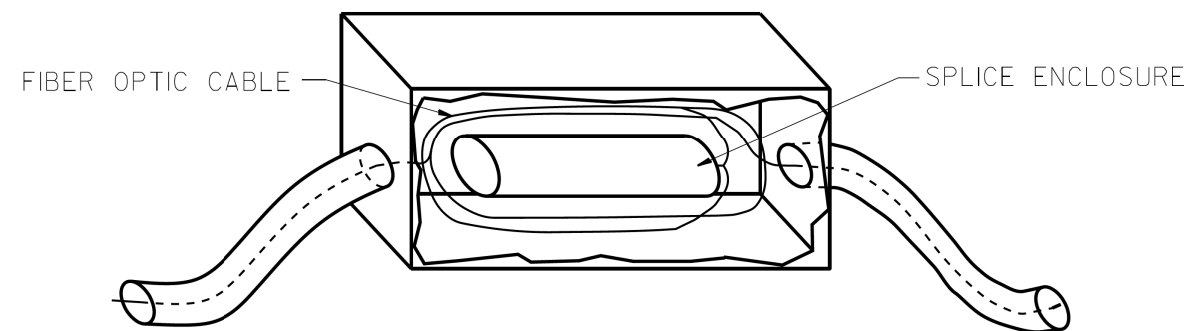
DESIGNED - R.L.	REVISED - 06/94
DRAWN - G.M.	REVISED - 11/95
CHECKED - R.L.	REVISED - 05/96
DATE - 06/22/94	REVISED - 10/96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

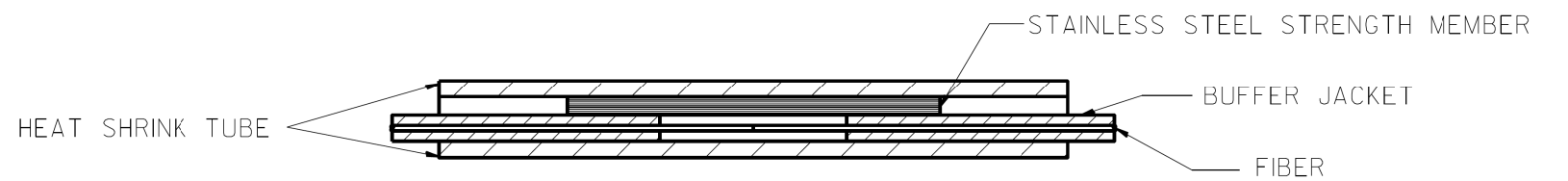
TYPICAL INDUCTION LOOP LEAD-IN
TRANSITION DIAGRAMS

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	355
TY-ITSC-418#5			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



JUNCTION BOX DETAIL



FIBER SPLICE DETAIL

FILE PATH = C:\Users\dishevoz\Desktop\60W30_PLOT\DI160W30-SHT-DI-Detail-ITSC-663-13.dgn



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 PLOT SCALE = 50.000' / in.
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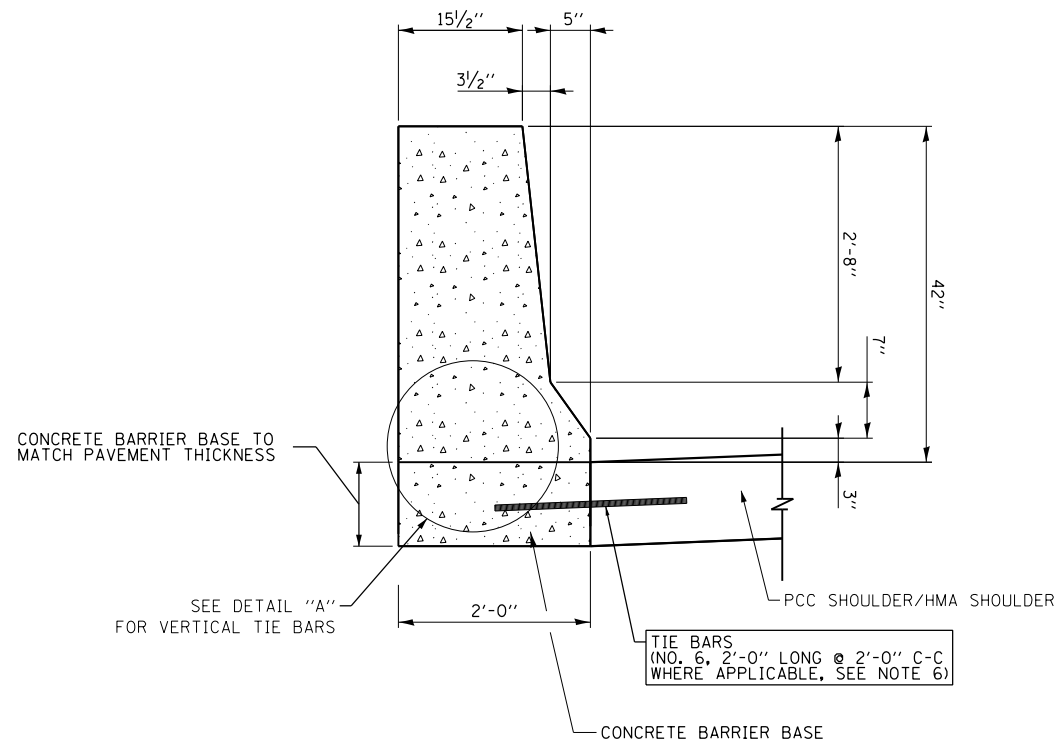
DESIGNED -	D.S.	REVISED-
DRAWN -	G.M.	REVISED-
CHECKED -	D.S.	REVISED-
DATE -	04/26/93	REVISED-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FIBER OPTIC
 WIRING DETAIL

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

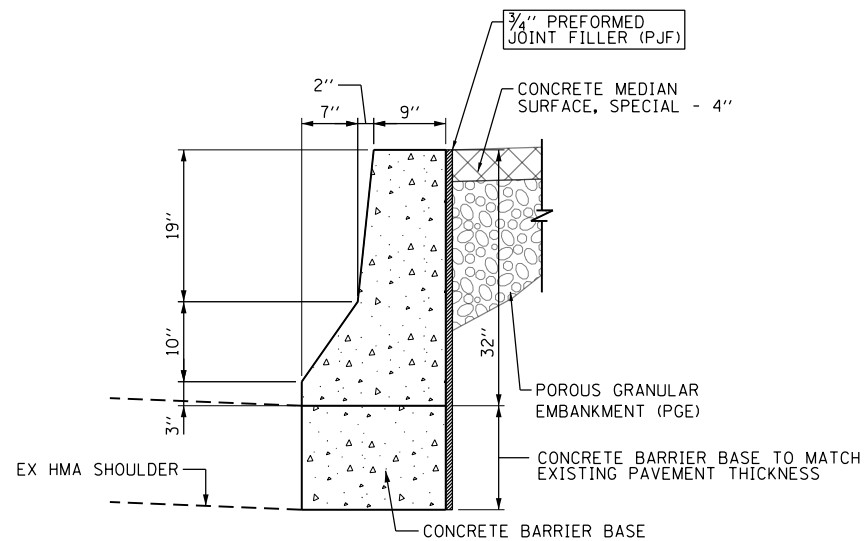
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	356
TY-ITSC-663#13			CONTRACT NO. 60W30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAILS OF CONCRETE BARRIER, SINGLE FACE,

42 INCH HEIGHT

SCALE: NONE



DETAILS OF CONCRETE BARRIER, SINGLE FACE,

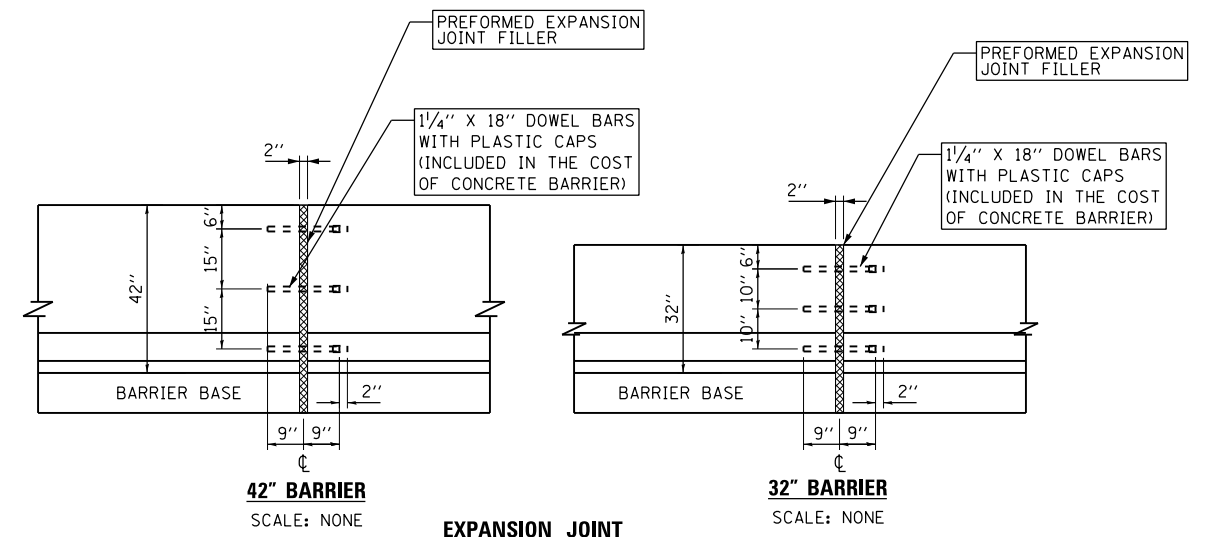
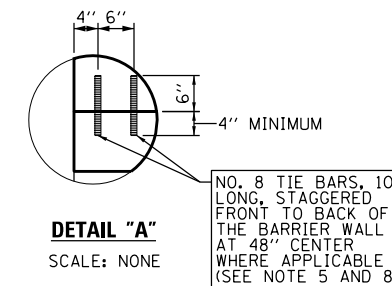
32 INCH HEIGHT (SPECIAL) **

SCALE: NONE

** ALL DIMENSIONS OF THIS TYPE OF BARRIER SHALL MATCH EXISTING CONCRETE BARRIER WALL. FIELD VERIFY DIMENSIONS PRIOR TO COMMENCING THE WORK.

NOTES:

1. EXPANSION JOINTS SHOWN ON THIS DRAWING SHALL BE PREFORMED JOINT MATERIAL (BITUMINOUS TYPE) FILLER AND SHALL MEET AASHTO DESIGNATION M-33.
2. ALL WORK AND MATERIALS DETAILED HEREIN SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONCRETE BARRIER PAY ITEMS UNLESS OTHERWISE NOTED.
3. PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE COST OF OF CONCRETE BARRIER OR CONCRETE BARRIER TRANSITION.
4. JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 637.08 OF THE STANDARD SPECIFICATIONS
5. TWO VERTICAL EPOXY COATED, NO. 8 TIE BARS, 10" LONG, SHALL BE PLACED STAGGERED FRONT TO BACK OF THE BARRIER WALL AT 48" CENTERS ALONG THE CONCRETE BARRIER TRANSITION. TIE BARS SHALL BE INCLUDED IN THE COST OF CONCRETE BARRIER TRANSITION.
6. HORIZONTAL TIE BARS SHALL BE NO. 6 EPOXY COATED, 24" LONG, 24" C-C AND SHALL BE INCLUDED IN THE COST OF THE BARRIER BASE. HORIZONTAL TIE BARS ARE REQUIRED WHEN PAVEMENT IS PCC.
7. EXPANSION JOINTS SHALL BE CONSTRUCTED IN BARRIER WALL AT MAXIMUM JOINT SPACING OF 90 FEET.
8. VERTICAL TIE BARS ARE REQUIRED WHEN CONCRETE BARRIER AND CONCRETE BARRIER BASE ARE NOT POURED MONOLITHICALLY.



42" BARRIER

SCALE: NONE

EXPANSION JOINT

32" BARRIER

SCALE: NONE

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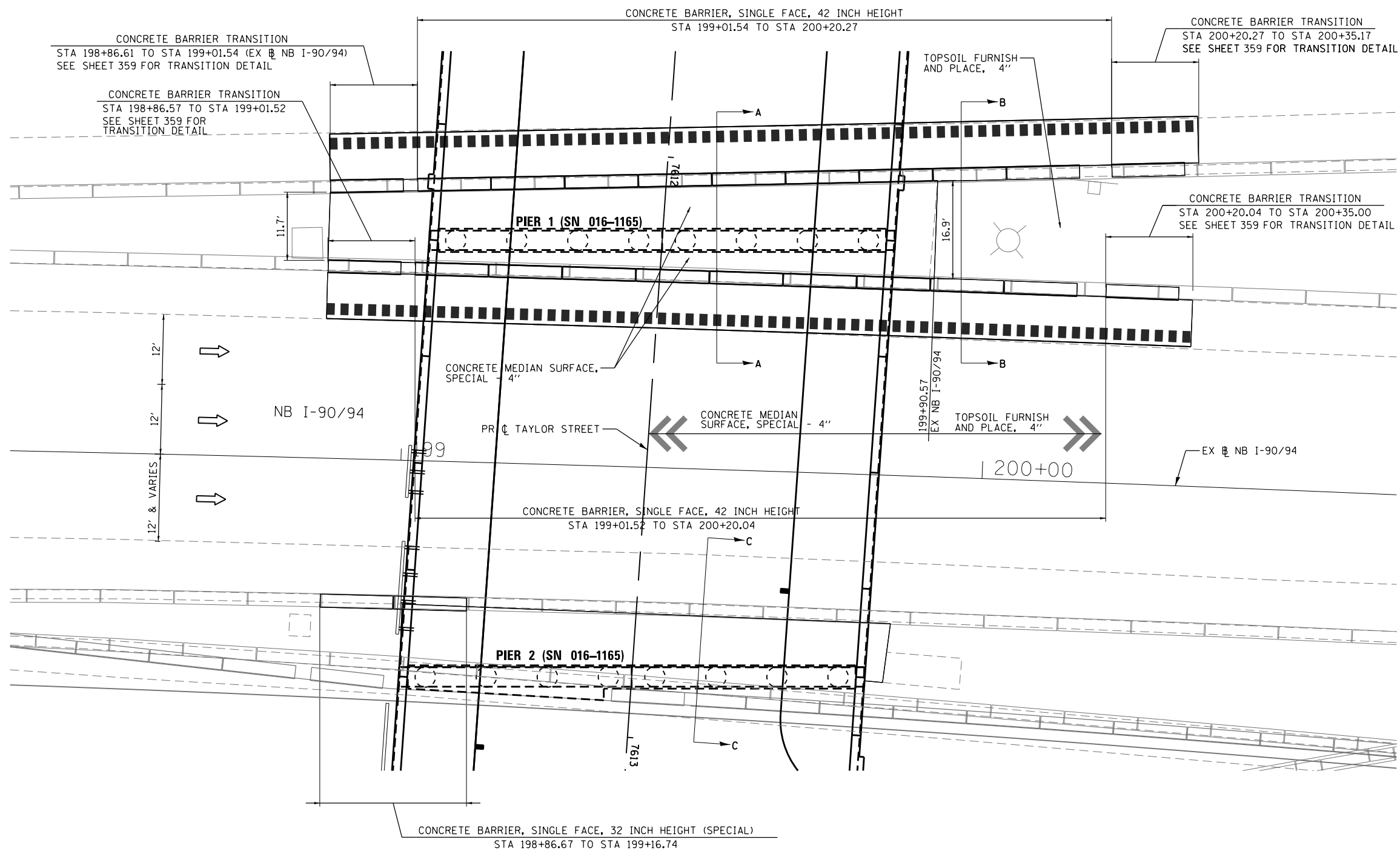
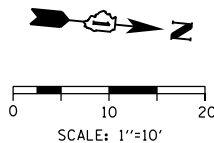
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY DETAILS - CONCRETE BARRIERS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	357
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



PLAN VIEW
CONCRETE BARRIER AT
PIER 1 AND PIER 2

SCALE: NONE

NOTES:

1. SEE SHEETS 357 FOR CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT AND CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (SPECIAL).
2. SEE SHEETS 359 FOR SECTIONS A-A, B-B AND C-C AND ELEVATION VIEW.

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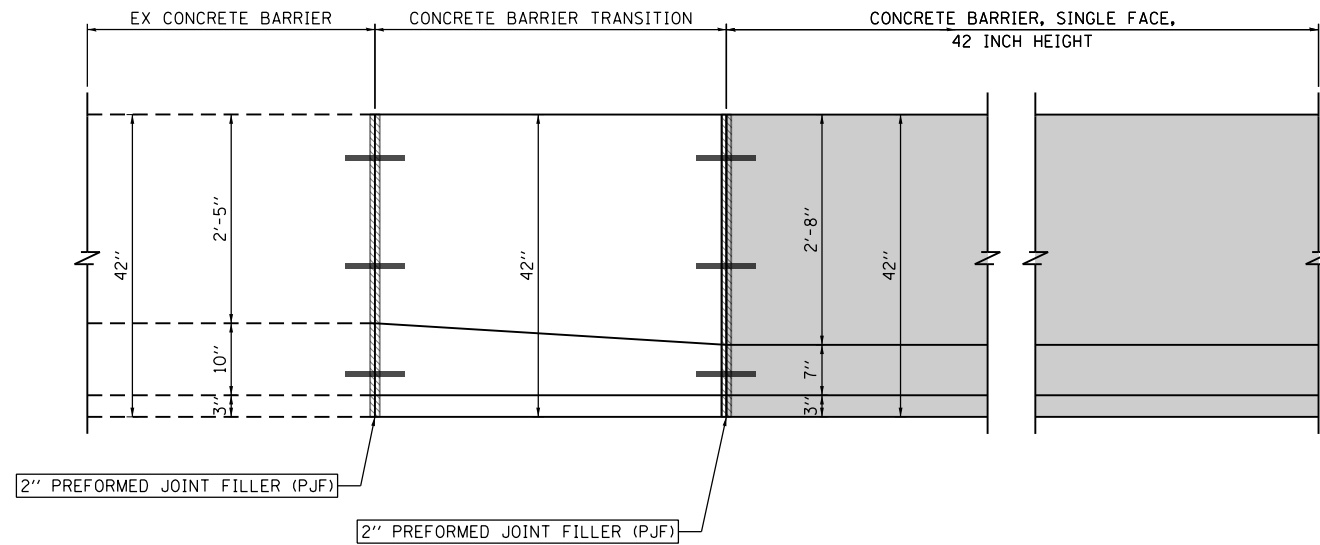
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS - CONCRETE BARRIER AT PIER 1 AND PIER 2

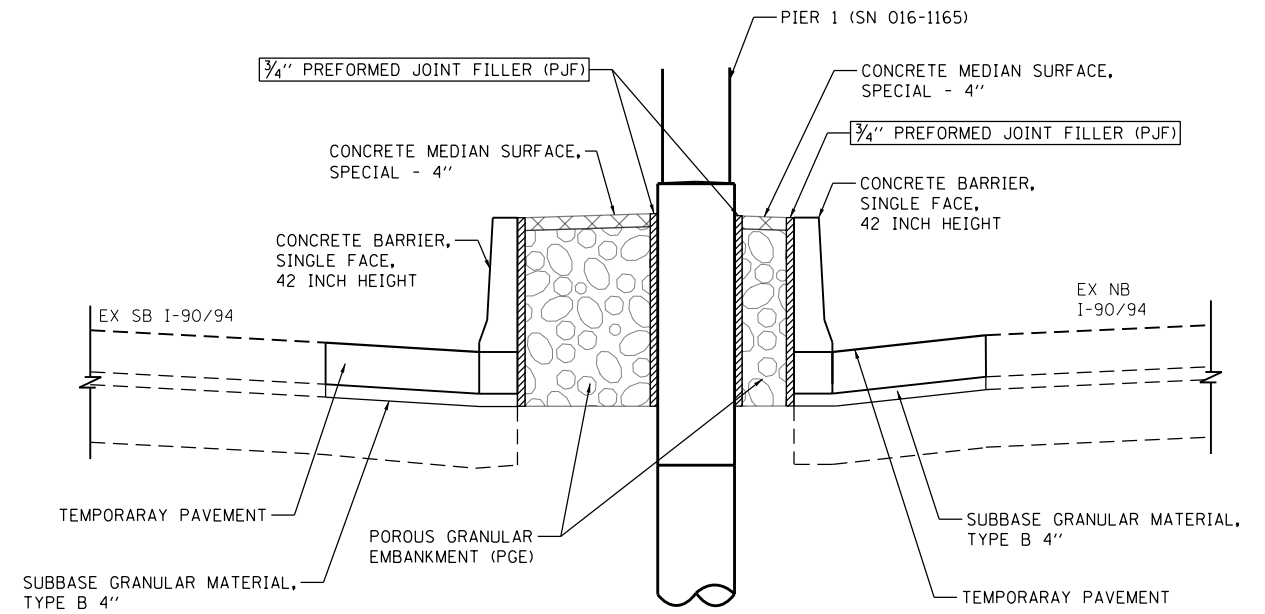
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

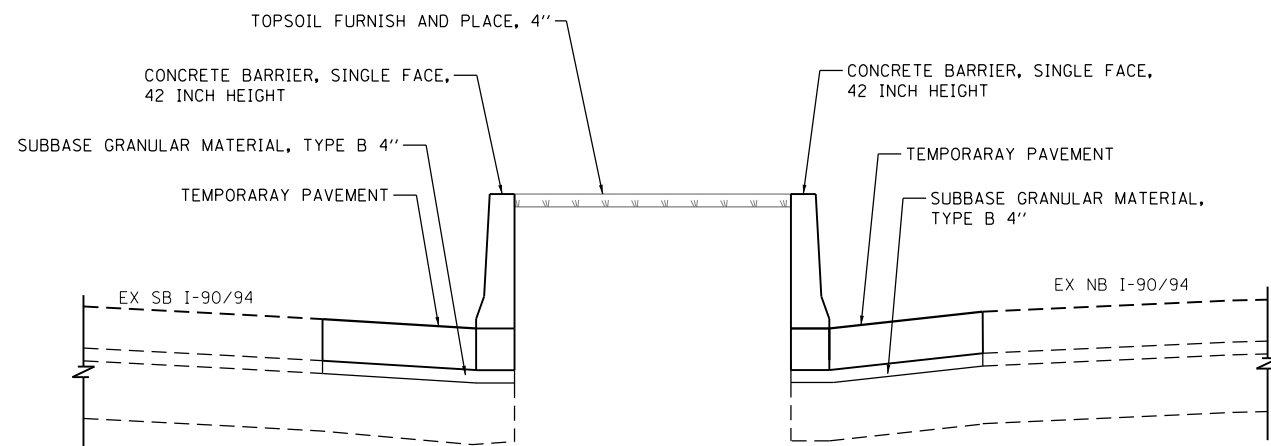


ELEVATION VIEW
CONCRETE BARRIER TRANSITION
AT EXISTING BARRIER AT
PIER 1 (SN 016-1165)

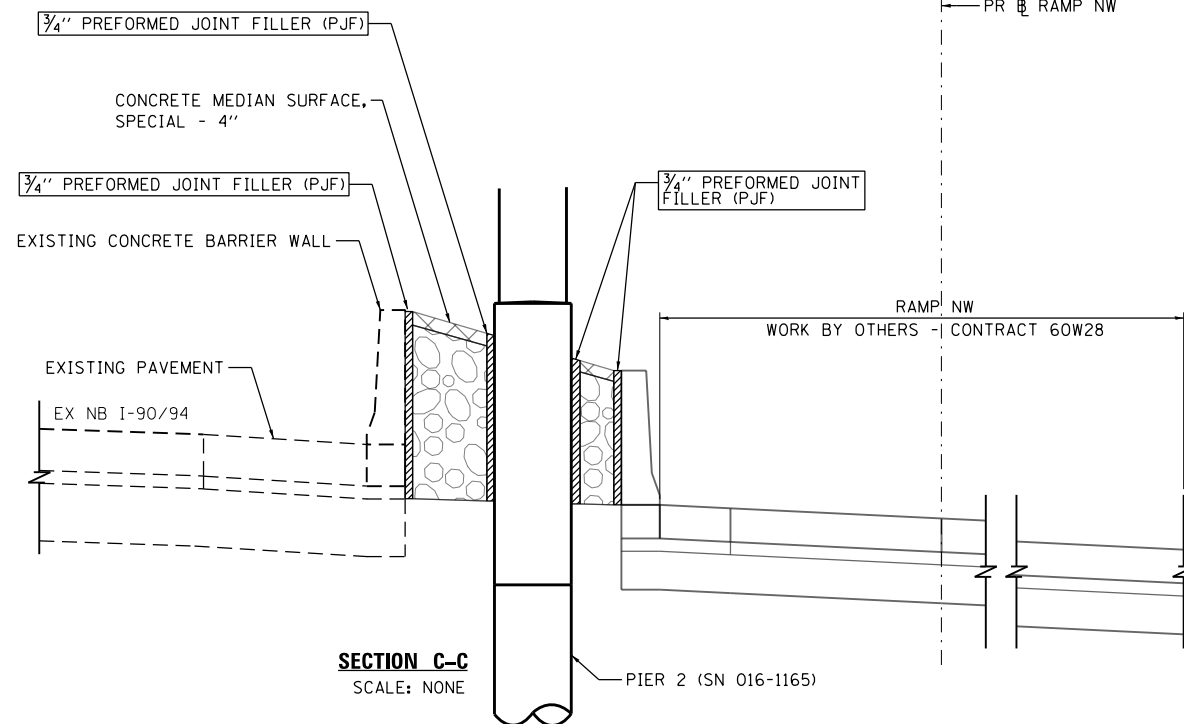
SCALE: NONE



SECTION A-A
 SCALE: NONE
 STA 198+86.57 TO STA 200+20.27



SECTION B-B
 SCALE: NONE
 STA 200+20.27 TO STA 200+35.00



SECTION C-C
 SCALE: NONE

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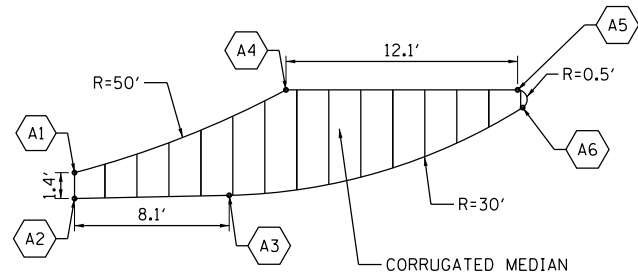
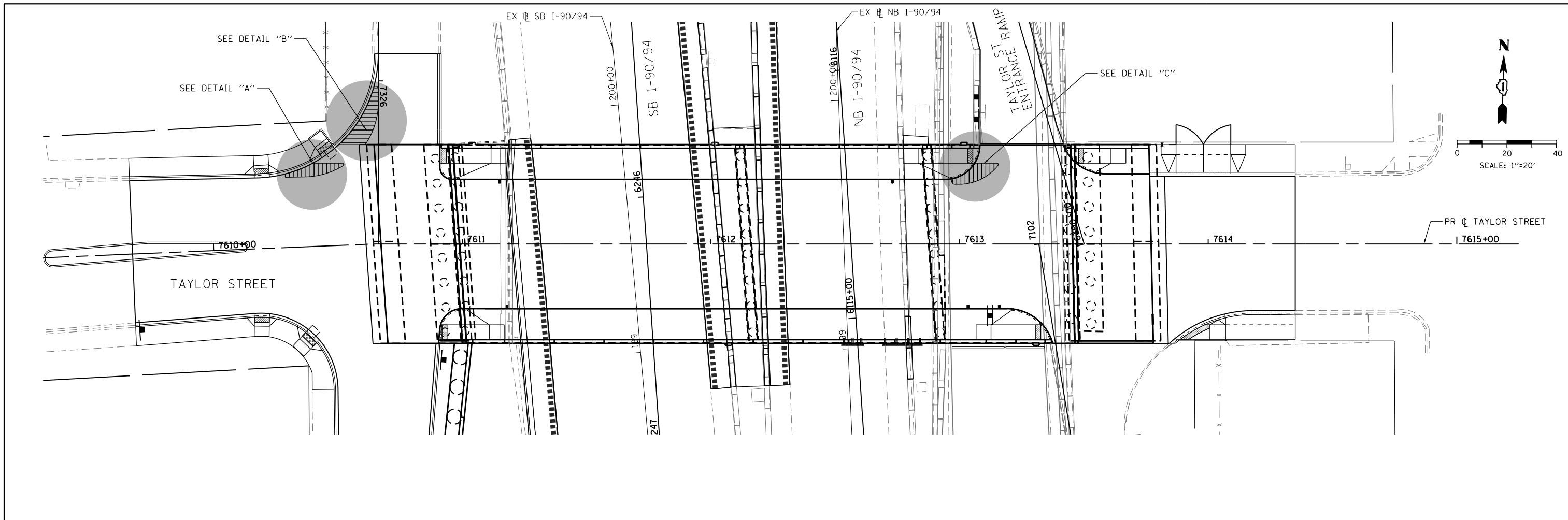
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS - CONCRETE BARRIER AT PIER 1 AND PIER 2

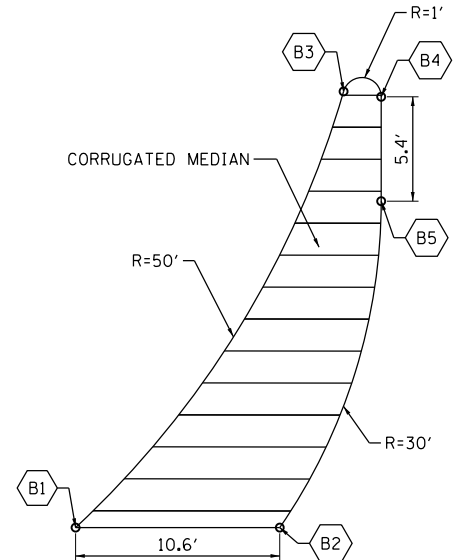
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				



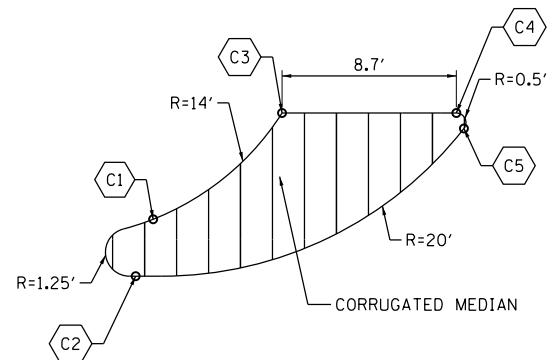
- A1 STA 7610+30.05, 29.65' LT
- A2 STA 7610+29.99, 28.30' LT
- A3 STA 7610+38.05, 28.13' LT
- A4 STA 7610+41.24, 33.49' LT
- A5 STA 7610+53.29, 32.99' LT
- A6 STA 7610+53.52, 32.05' LT

CORRUGATED MEDIAN: DETAIL "A"
SCALE: NONE



- B1 STA 7610+52.03, 40.55' LT
- B2 STA 7610+62.66, 40.10' LT
- B3 STA 7610+64.30, 62.72' LT
- B4 STA 7610+66.27, 62.44' LT
- B5 STA 7610+66.27, 57.00' LT

CORRUGATED MEDIAN: DETAIL "B"
SCALE: NONE



- C1 STA 7612+97.79, 26.46' LT
- C2 STA 7612+98.11, 24.00' LT
- C3 STA 7613+06.06, 32.50' LT
- C4 STA 7613+15.14, 32.50' LT
- C5 STA 7613+15.53, 31.69' LT

CORRUGATED MEDIAN: DETAIL "C"
SCALE: NONE

NOTE:
SEE STRUCTURAL PLANS
FOR CORRUGATED MEDIAN
CROSS SECTION DETAIL.

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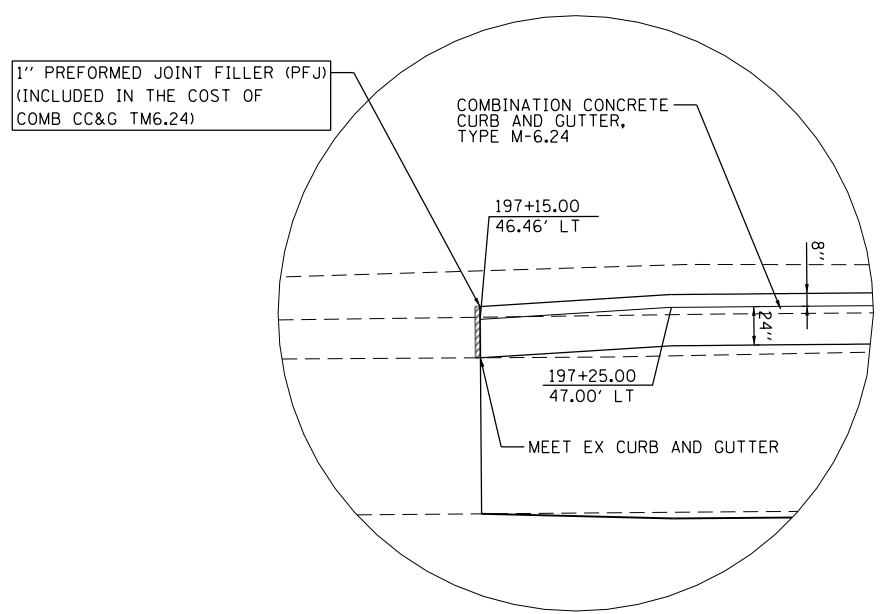
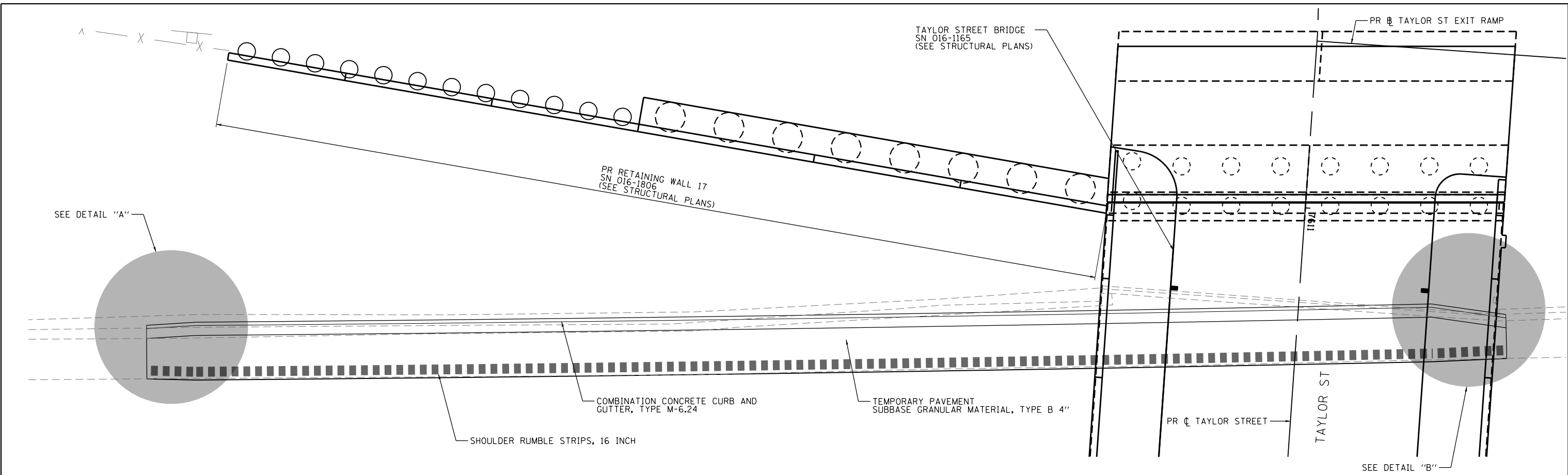
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CHECKED - NJH	REVISED -
DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

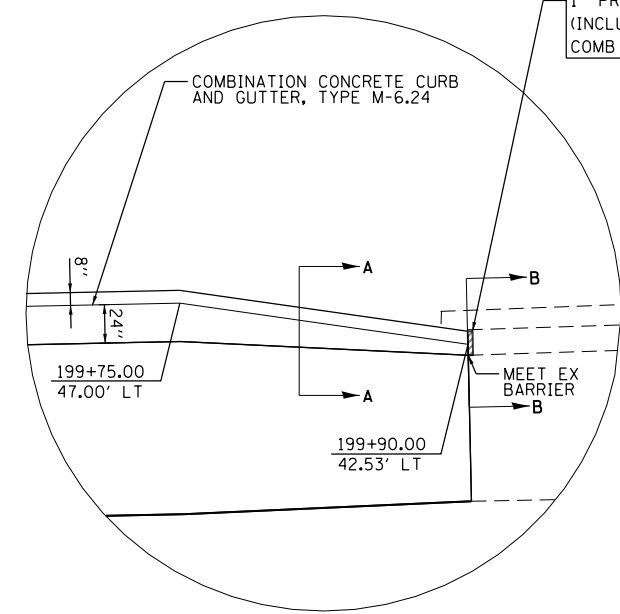
ROADWAY DETAILS - CORRUGATED MEDIAN LAYOUT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

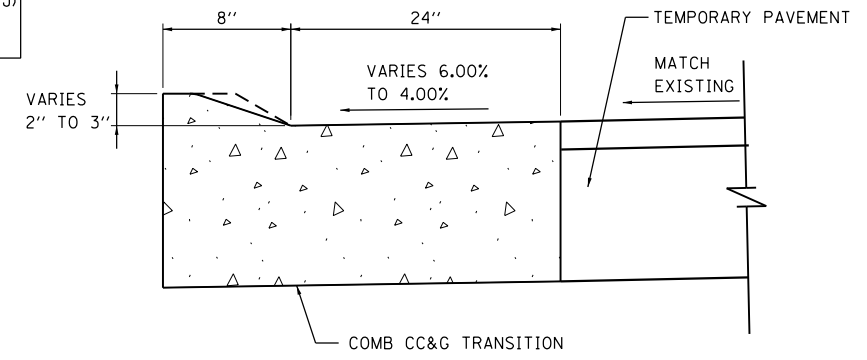
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90/94	2013-012R	COOK	385	360
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	



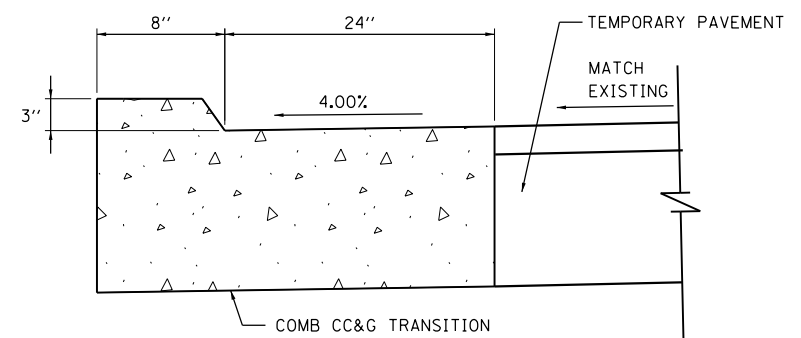
DETAIL "A"
COMB CC&G TRANSITION
 SCALE: NONE
 STA 197+15.00 TO STA 197+25.00 EX @ SB I-90/94
 (PAID FOR AS COMB CC&G TM6.24)



DETAIL "B"
COMB CC&G TRANSITION
 SCALE: NONE
 STA 199+75.00 TO STA 199+90.00 EX @ SB I-90/94
 (PAID FOR AS COMB CC&G TM6.24)



SECTION A-A
COMB CC&G TRANSITION
 SCALE: NONE
 STA 199+75.00 TO STA 199+90.00 (EX @ SB I-90/94)
 (PAID FOR AS COMB CC&G TM6.24)



SECTION B-B
COMB CC&G TRANSITION
 SCALE: NONE
 STA 199+90.00 (EX @ SB I-90/94)
 (PAID FOR AS COMB CC&G TM6.24)

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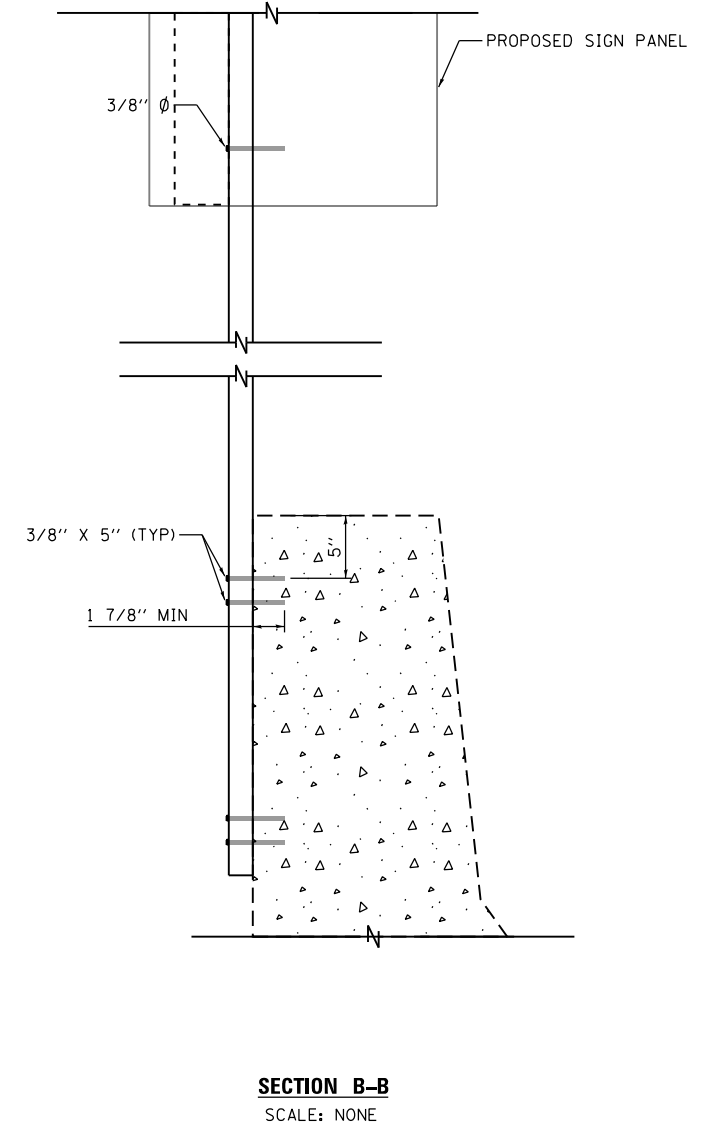
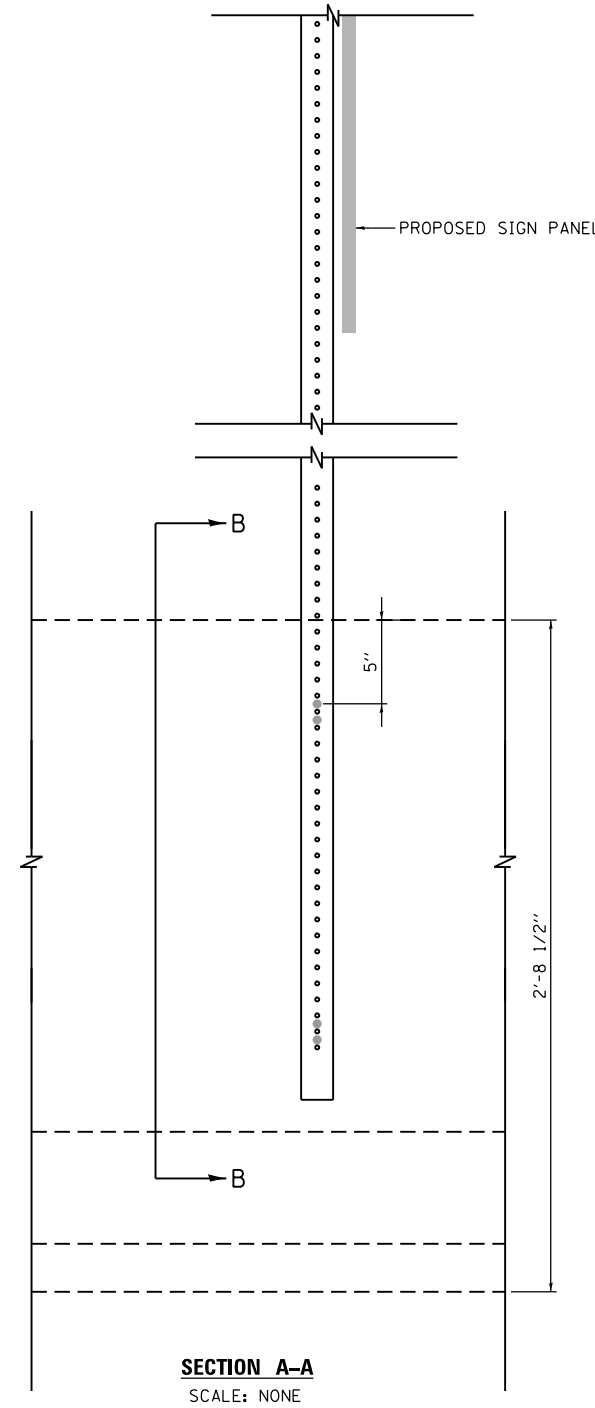
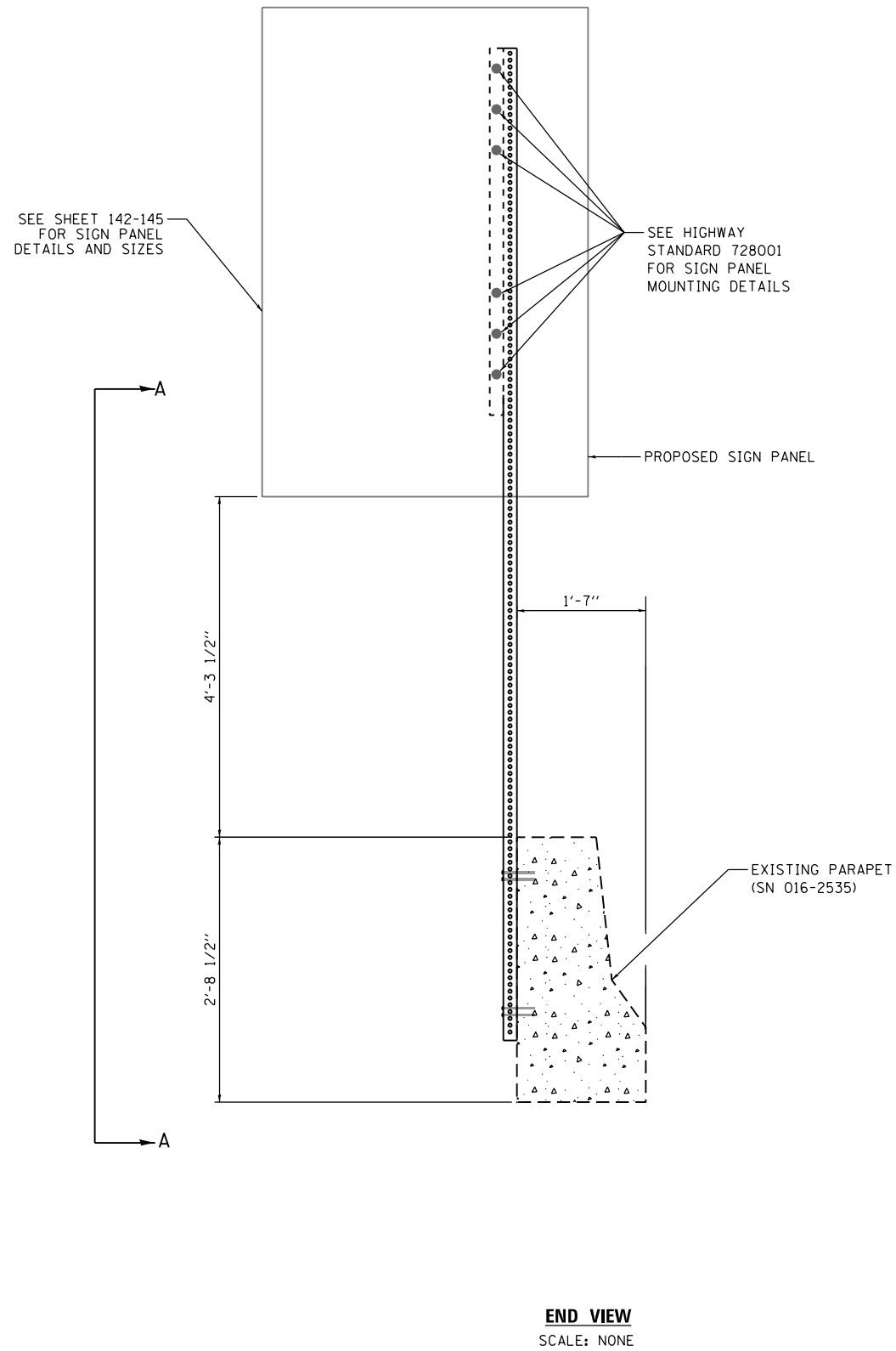
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DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS - CONCRETE CURB AND GUTTER TRANSITION
 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	361
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

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CHECKED - NJH
DATE - 10/24/2014

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS - WALL MOUNTED SIGN BASE DETAIL
FOR TELESCOPING STEEL SIGN SUPPORT (SPECIAL)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	361A
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

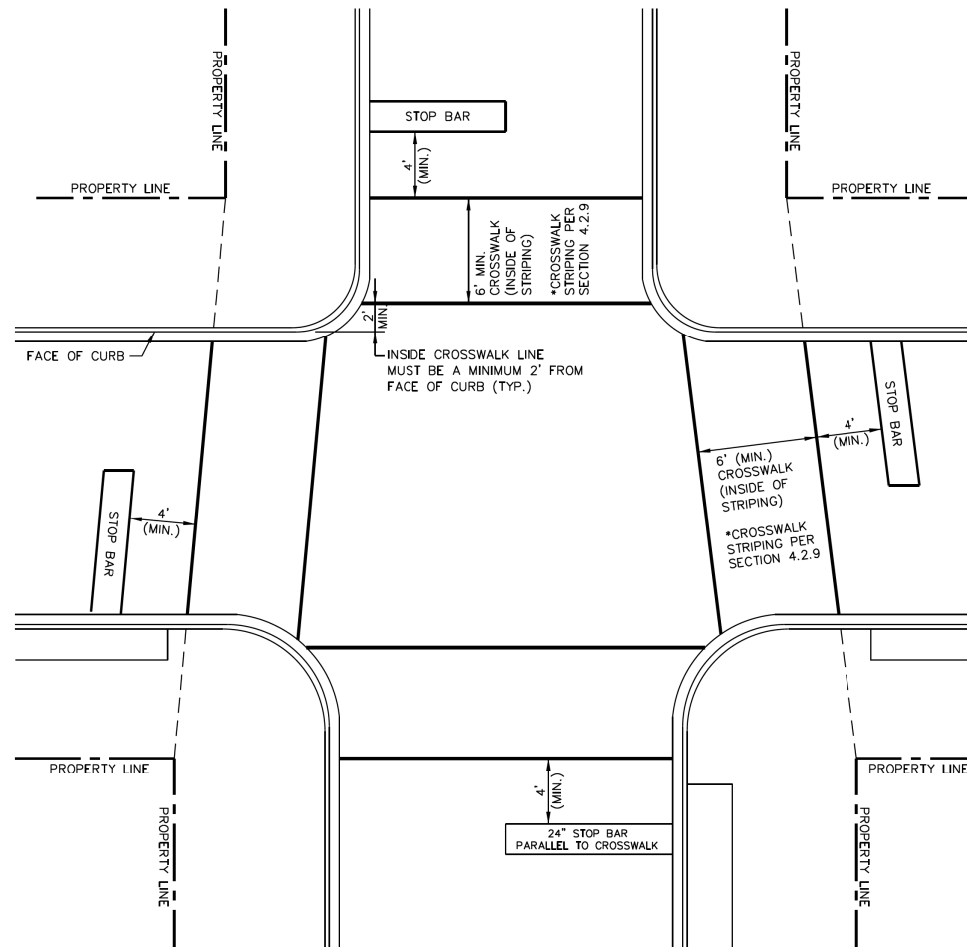
NOTES:

TYPICAL CROSSWALK CONSISTS OF TWO PROJECTED LINES, ONE OF THEM A PROJECTION OF THE PROPERTY LINE AND THE OTHER PARALLEL AS DEFINED BY SIDEWALK WIDTH.

WHERE CROSSWALK LOCATIONS ARE DEFINED BY SPECIFIC CURB RAMP SITUATIONS, THE ABOVE TYPICAL LAYOUT MAY NOT APPLY. SEE CURB RAMP LAYOUTS IN APPENDIX B FOR ADDITIONAL CROSSWALK DETAILS.

FOR CROSSWALKS AT INTERSECTIONS WHERE PROPERTY LINES ARE NOT AT 90 DEGREES, ALIGN THE PROPERTY LINES (SEE DASHED LINE BELOW) TO LOCATE INNER CROSSWALK LINE.

SEE SECTION 4.2.9 AND SHEET A-7-1 FOR GUIDELINES REGARDING THE LAYOUT OF CONTINENTAL AND LADDER MARKINGS.



DATE	REVISION	CITY OF CHICAGO		
1/1/2014	REVISION 1	INTERSECTION CROSSWALK LAYOUT		
DATE	SHEET	DRAWN BY		
01/10/07	A-1-3	CDOT		

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 DATE - 10/24/2014

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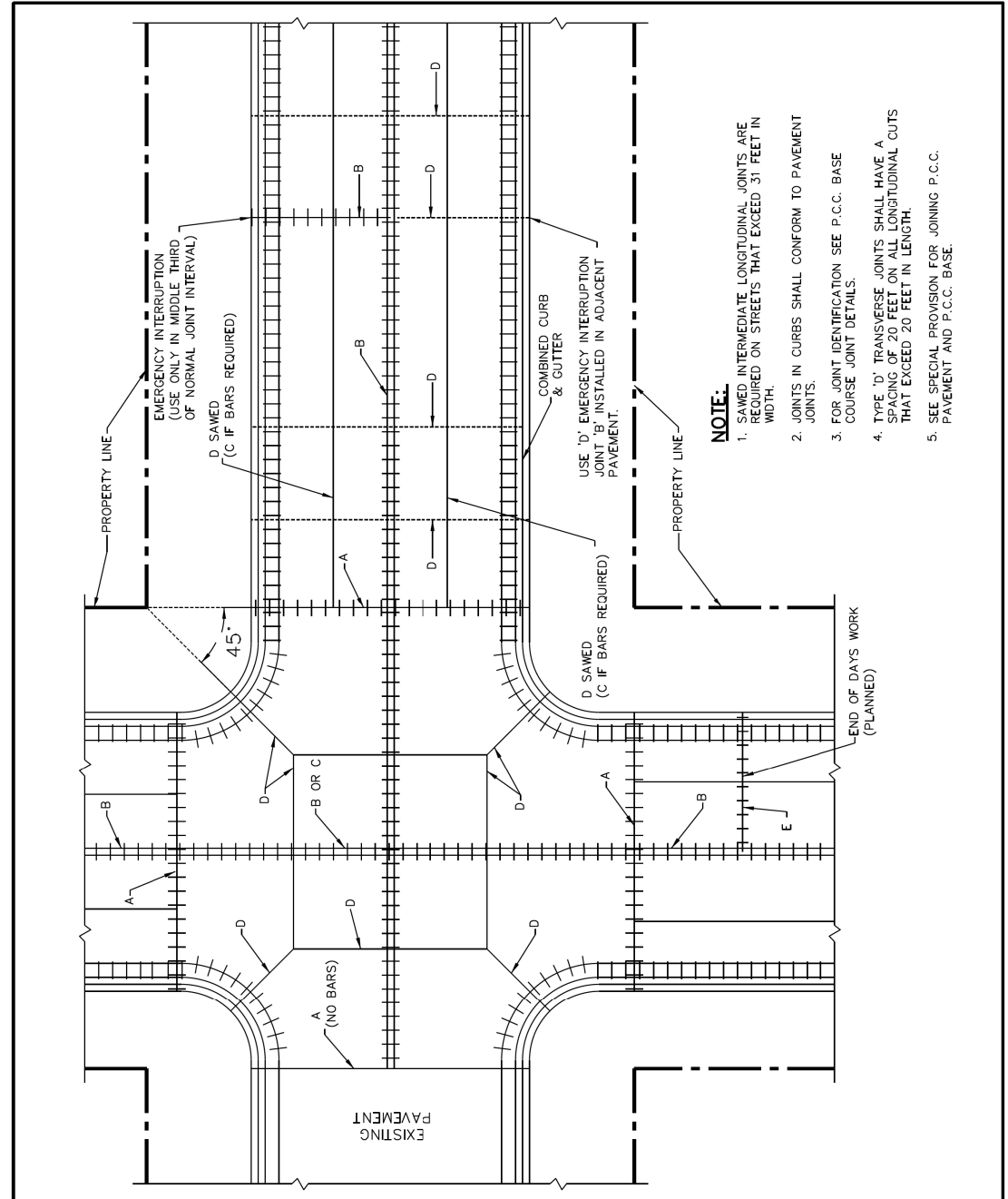
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION
 STANDARDS

SCALE: N.T.S. SHEET 1 OF 5 SHEETS STA. TO STA.

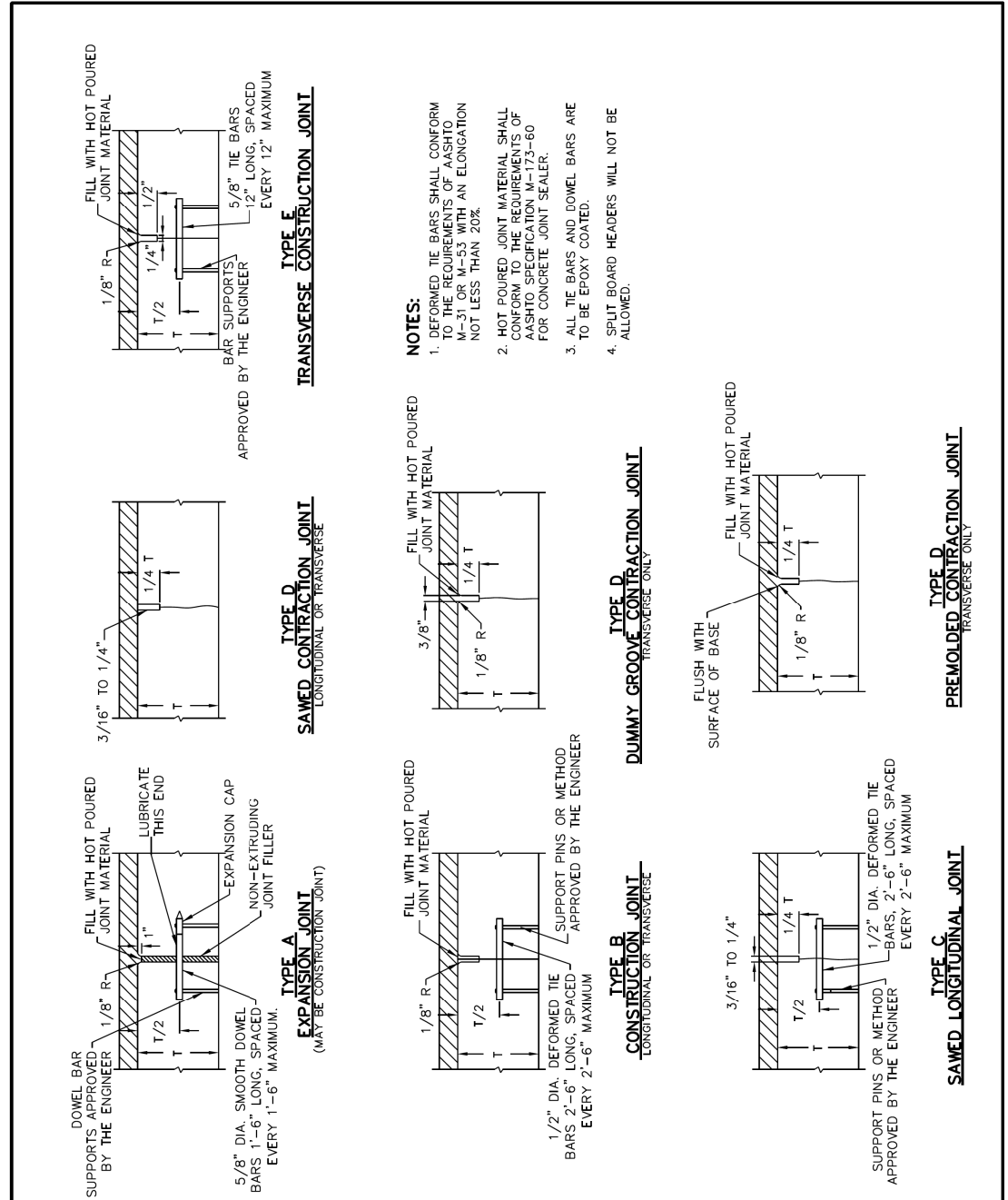
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	362
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

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- NOTE:**
- SAWED INTERMEDIATE LONGITUDINAL JOINTS ARE REQUIRED ON STREETS THAT EXCEED 31 FEET IN WIDTH.
 - JOINTS IN CURBS SHALL CONFORM TO PAVEMENT JOINTS.
 - FOR JOINT IDENTIFICATION SEE P.C.C. BASE COURSE JOINT DETAILS.
 - TYPE 'D' TRANSVERSE JOINTS SHALL HAVE A SPACING OF 20 FEET ON ALL LONGITUDINAL CUTS THAT EXCEED 20 FEET IN LENGTH.
 - SEE SPECIAL PROVISION FOR JOINING P.C.C. PAVEMENT AND P.C.C. BASE.

 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	TYPICAL JOINT LAYOUT FOR P.C. CONCRETE PAVEMENTS		
	DATE	SHEET	DRAWN BY		
	12/26/06	A-2-3A	CDOT		



- NOTES:**
- DEFORMED TIE BARS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M-31 OR M-53 WITH AN ELONGATION NOT LESS THAN 20%.
 - HOT POURED JOINT MATERIAL SHALL CONFORM TO THE REQUIREMENTS OF AASHTO SPECIFICATION M-173-60 FOR CONCRETE JOINT SEALER.
 - ALL TIE BARS AND DOWEL BARS ARE TO BE EPOXY COATED.
 - SPLIT BOARD HEADERS WILL NOT BE ALLOWED.

 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	P.C.C. BASE COURSE JOINT DETAILS		
	DATE	SHEET	DRAWN BY		
	12/27/06	A-2-3C	CDOT		



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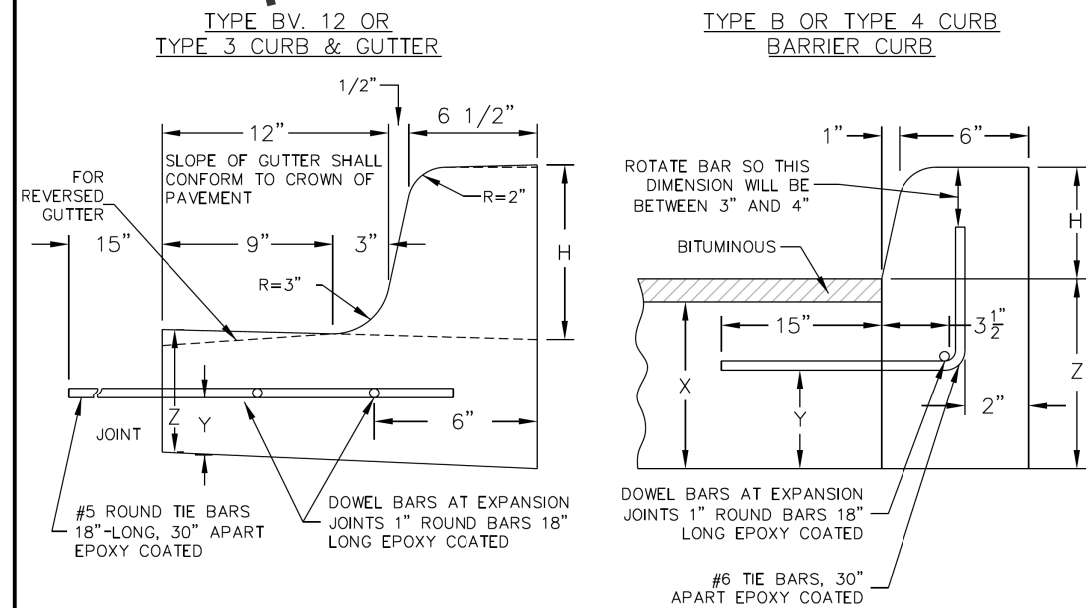
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION
 STANDARDS

SCALE: N.T.S. SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	363
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

NOTE: H = VARIABLE 3" TO 9"
 X = THICKNESS OF PAVEMENT
 Y = ONE HALF THE THICKNESS OF CONCRETE PAVEMENT OR CONCRETE BASE.
 Z = 10" OR THICKNESS OF PAVEMENT - WHICHEVER IS GREATER



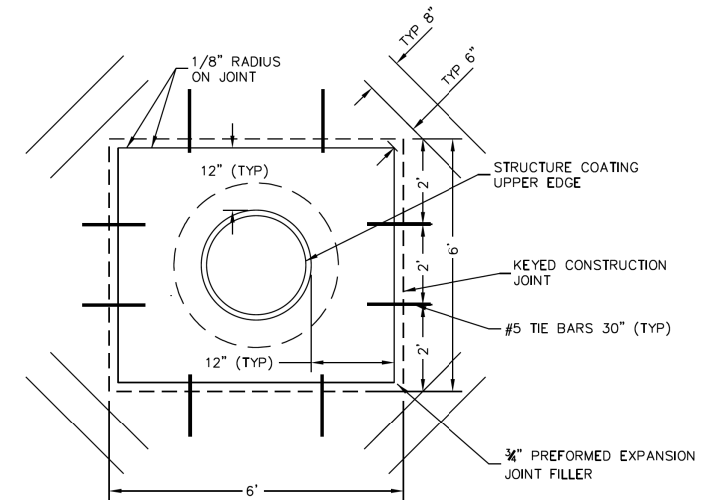
JOINTS IN CURB, COMBINED CURB & GUTTER
 TRANSVERSE JOINTS OF A TYPE SIMILAR TO THAT USED IN THE ADJACENT PAVEMENT SHALL BE INSTALLED IN THE CURB, GUTTER AND COMBINED CURB & GUTTER IN PROLONGATION WITH THE JOINTS IN THE PAVEMENT. THE DETAILS OF THE TRANSVERSE JOINTS IN THE CURB, GUTTER AND COMBINED CURB & GUTTER SHALL BE APPROVED BY THE COMMISSIONER. CURB, GUTTER OR COMBINED CURB & GUTTER IS CONSTRUCTED ADJACENT TO A FLEXIBLE BASE PAVEMENT, 1" THICK EXPANSION JOINTS COMPOSED OF BITUMINOUS PERFORMED JOINT FILLER SHALL BE INSTALLED IN THE CURB AND/OR GUTTER AT POINTS OF CURVATURE AND AT CONSTRUCTION JOINTS. CONTRACTION JOINTS SHALL ALSO BE PLACED BETWEEN THESE EXPANSION JOINTS AT DISTANCES NOT EXCEEDING 20 FEET. ALL TIE BARS SHALL BE DEFORMED-ALL DOWEL BARS SHALL BE SMOOTH.

NOTE: ALL TIE BARS AND DOWEL BARS TO BE EPOXY COATED.

*AT LOCATIONS REQUIRING DEPRESSED CURBS SEE THE ADA STANDARDS FOR CONSTRUCTION DETAILS

 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	CONCRETE CURB & GUTTER DETAIL		
			DATE	SHEET	DRAWN BY
			12/12/06	A-2-6	CDOT

2-#4 DEFORMED TIE BARS 30" LONG AT A MID DEPTH (TYPICAL) SEE SPECIAL CONDITION BELOW.



SPECIAL CONDITIONS:

TIE BARS SHALL NOT BE INSTALLED AT ISOLATION BOX CORNERS WHERE EITHER SIDE OF THE BOX FORMING SAID CORNER IS A LONGITUDINAL OR TRAVERSE JOINT. MOREOVER, AT NO TIME SHALL A TIE BAR CROSS A JOINT (ALREADY FORMED OR PROPOSED) IN THE VICINITY OF THE ISOLATION BOX. IF THIS SITUATION OCCURS, THE TIE BAR SHALL BE ADJUSTED PARALLEL TO THE AXIS OF THE BAR SO THAT THE END OF THE BAR IS NO CLOSER THAN 1 1/2" TO THE JOINT.

ONLY BY THE DIRECTION OR APPROVAL OF THE COMMISSIONER SHALL THE DISTANCE BETWEEN THE UPPER EXTERNAL CASTING EDGE AND THE EDGE OF STANDARD ISOLATION BOX, SHOWN AS 12", BE INCREASED SO THAT AN IMMOVABLE LONGITUDINAL JOINT AND (OR) TRAVERSE JOINT WILL THEN THENCE FORM (O) SIDE (S) OF THE BOX. THIS ADJUSTMENT WILL BE ALLOWED ONLY WHEN THE DISTANCE BETWEEN THE SIDE OF THE STANDARD ISOLATION BOX AND IMMOVABLE JOINT IS 18" OR LESS.

BACKFILL MATERIAL AROUND STRUCTURE WILL BE COMPACTED TO 95% MODIFIED PROCTOR PRIOR TO THE PLACEMENT OF CONCRETE WITHIN THE ISOLATION BOX.

 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	DETAILS OF STRUCTURE CASTING ISOLATION BOX		
			DATE	SHEET	DRAWN BY
			12/21/06	A-2-10A	CDOT

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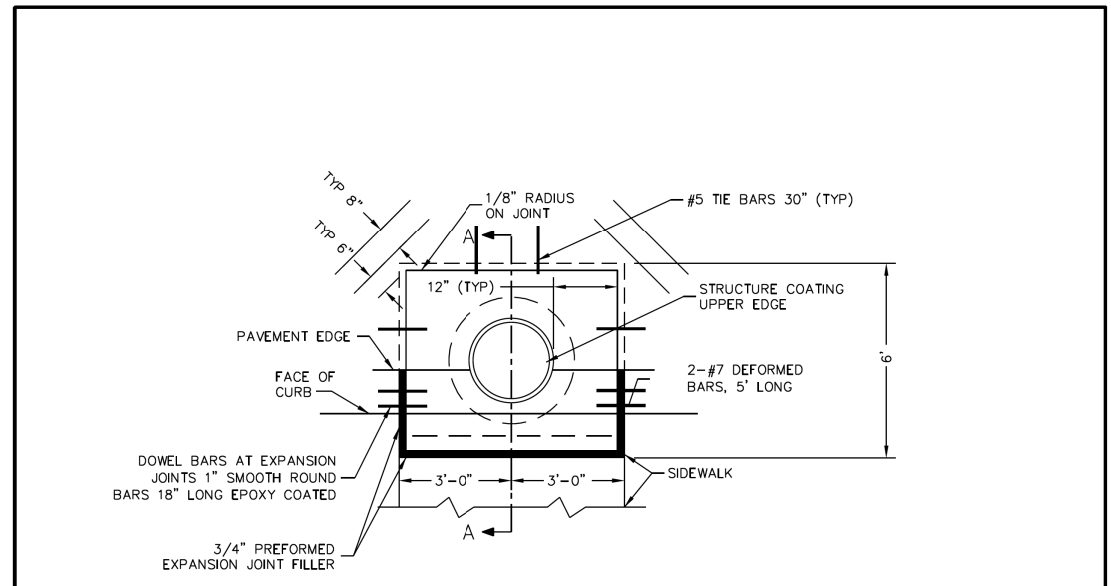
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION
 STANDARDS

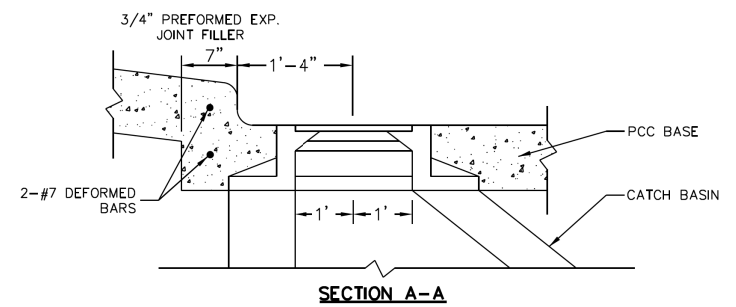
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	364
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W30	



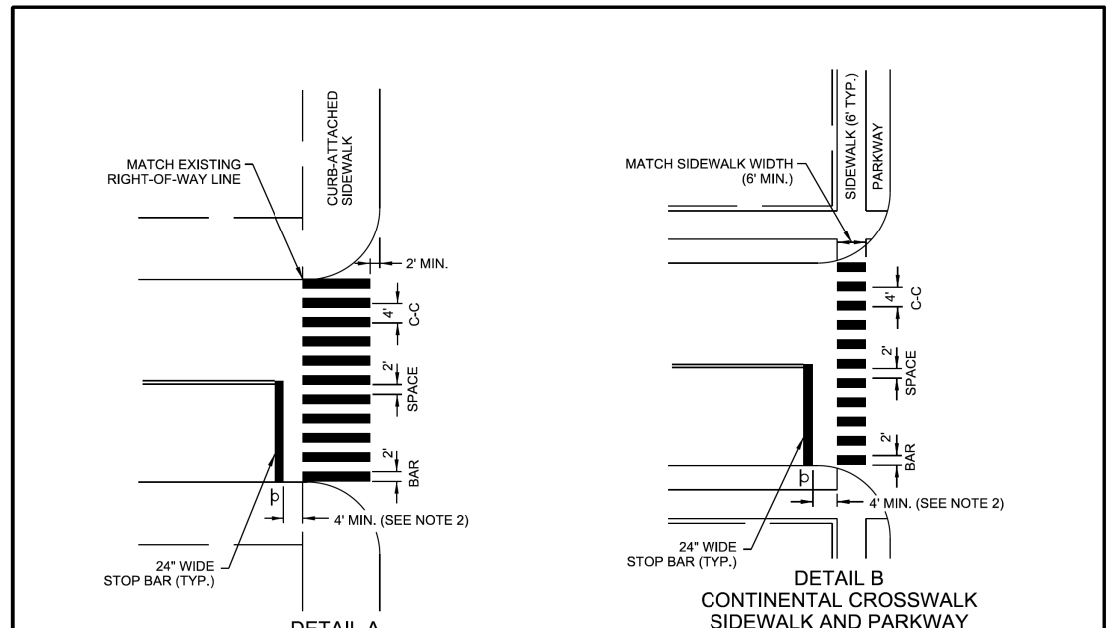
NOTES:

1. STRUCTURE CASTING MAY BE ROUND OR RECTANGULAR.
2. CONCRETE WITHIN THE ISOLATION BOX WILL BE OF THE SAME TYPE AND THICKNESS AS CONCRETE IN THE ADJACENT PAVEMENT.

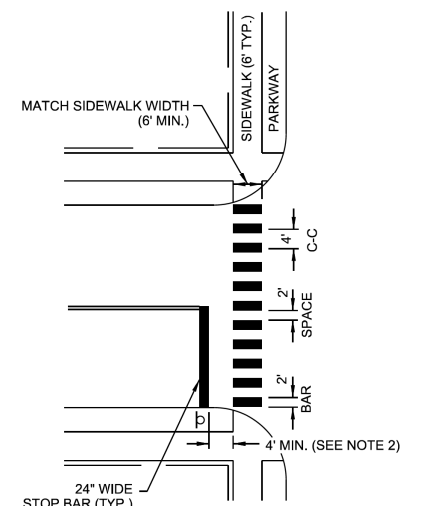


SECTION A-A

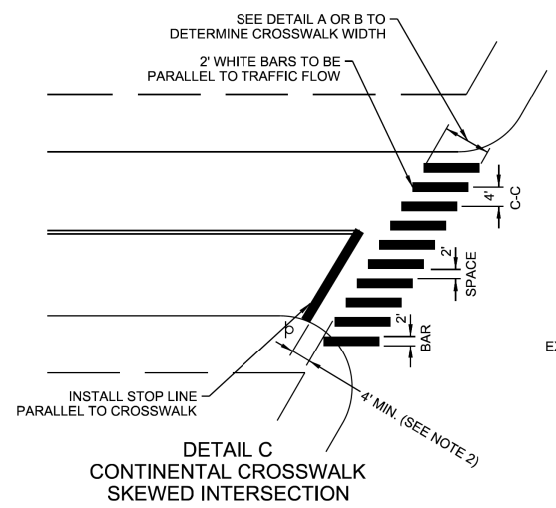
 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	DETAILS OF STRUCTURE CASTING ISOLATION BOX FOR P.C.C. PAVEMENT AND BASE COURSE		
			DATE	SHEET	DRAWN BY
			12/22/06	A-2-10B	CDOT



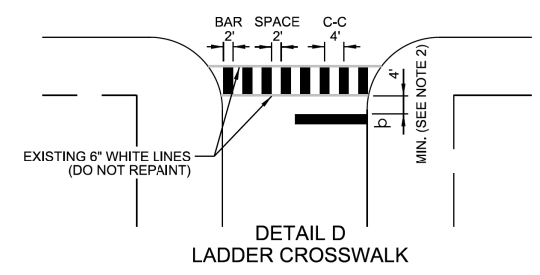
DETAIL A
CONTINENTAL CROSSWALK
CURB-ATTACHED SIDEWALK



DETAIL B
CONTINENTAL CROSSWALK
SIDEWALK AND PARKWAY



DETAIL C
CONTINENTAL CROSSWALK
SKEWED INTERSECTION



DETAIL D
LADDER CROSSWALK

 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	CROSSWALK MARKING DETAIL		
			DATE	SHEET	DRAWN BY
			06/25/12	A-7-1A	CDOT

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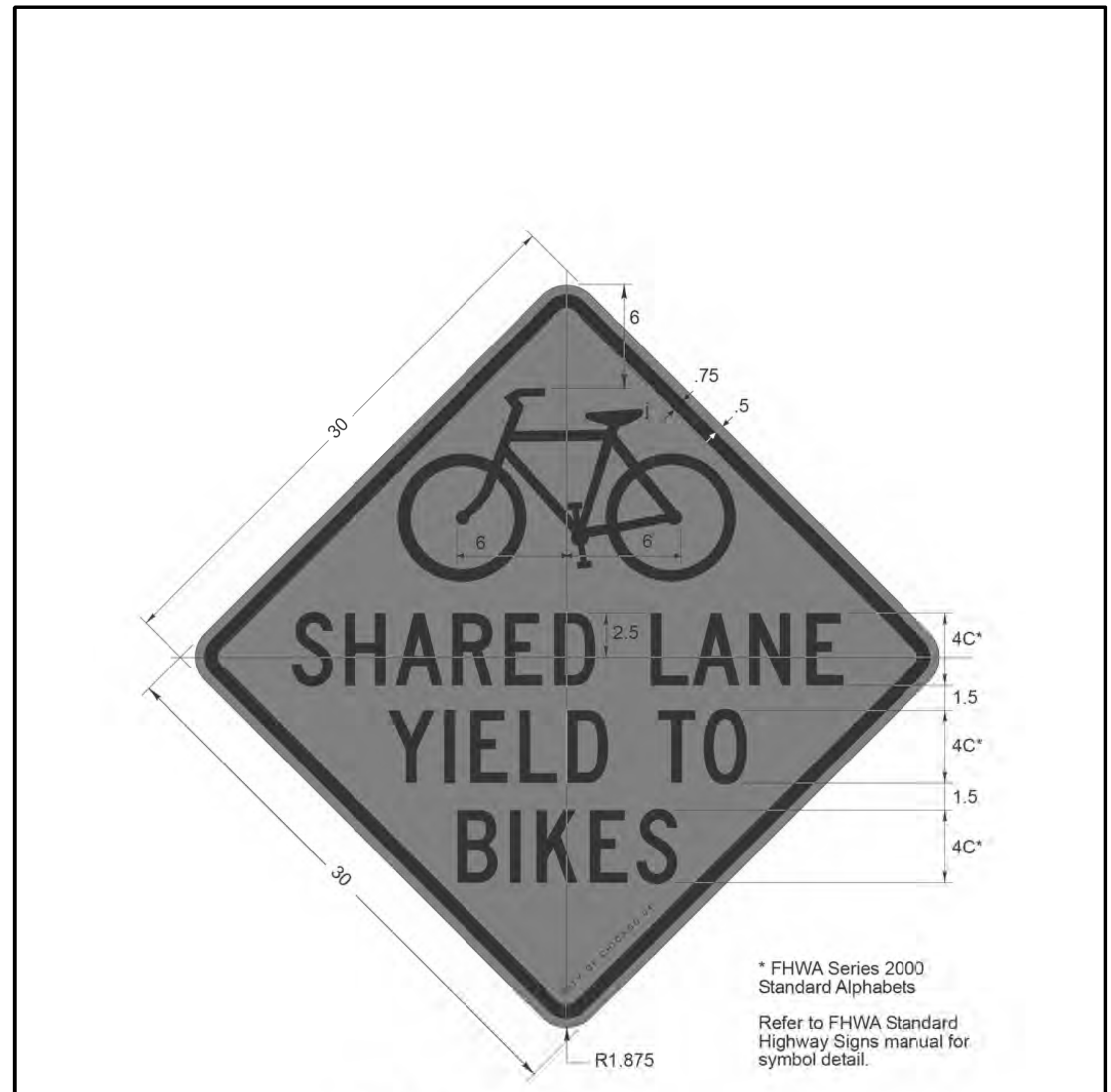
D160W30-Sht-CDOT-DETAIL-04.dgn
 USER NAME = dishveoz
 PLOT SCALE = 1.0000' / 1" =
 PLOT DATE = 10/23/2014

DESIGNED - CDOT
 DRAWN - CDOT
 CHECKED - CDOT
 DATE - 10/24/2014

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION
 STANDARDS
 SCALE: N.T.S. SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	365
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				



Colors: Legend -Black
Background -Orange

	DATE	REVISION	CITY OF CHICAGO		
			SHARED LANE YIELD TO BIKES SIGN		
			DATE	SHEET	DRAWN BY
			1/1/14	A-7-9	CDOT

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D160W30-Sht-CDOT-DETAIL-05.dgn
 USER NAME = dishveoz
 PLOT SCALE = 1.0000' / 1" = 10/23/2014

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DRAWN -	CDOT	REVISED -	
CHECKED -	CDOT	REVISED -	
DATE -	10/24/2014	REVISED -	

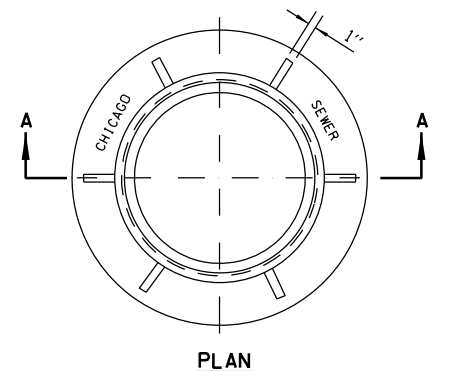
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION
STANDARDS

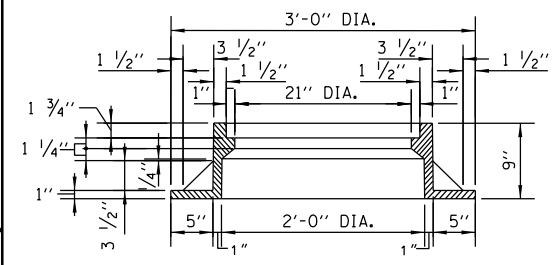
SCALE: N.T.S. SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	366
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

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PLAN

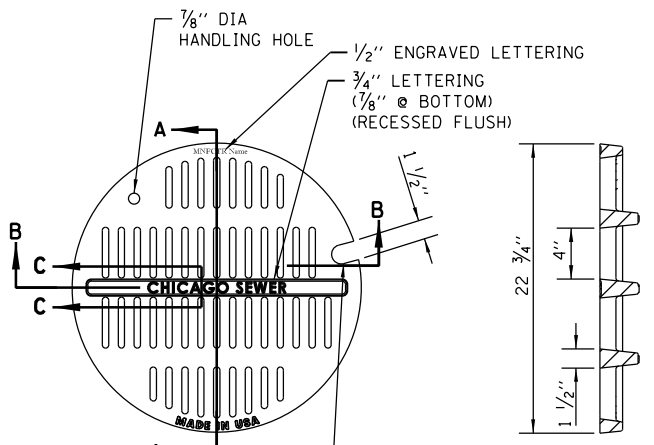


SECTION A-A

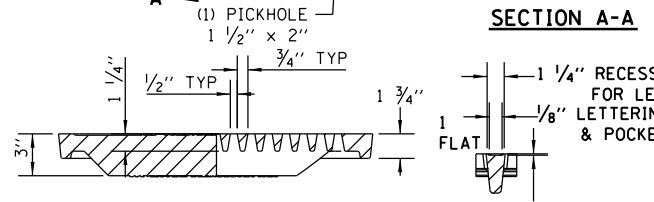
NOTE: METAL PLATES MUST BE FURNISHED FOR PERFORATED LIDS ON MANHOLES.

HEAVYWEIGHT MANHOLE FRAME

MATERIAL: CAST IRON



SECTION A-A

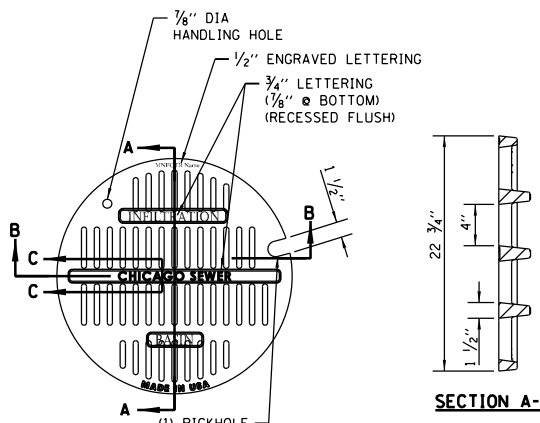


SECTION B-B

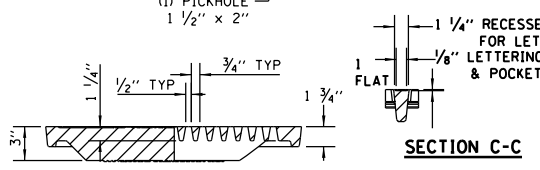
SECTION C-C

MATERIALS
 GRATE-GRAY IRON
 ASTM A48 CL35B
 DESIGN LOAD
 HEAVY DUTY
 COATING
 UNDIPPED
 OPEN AREA
 85.6 SQ. IN.

STANDARD CB/MH LID



SECTION A-A

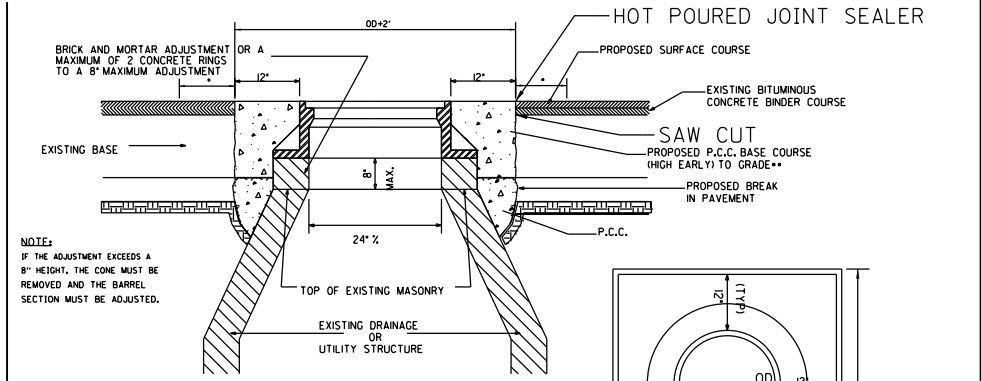


SECTION B-B

SECTION C-C

MATERIALS
 GRATE-GRAY IRON
 ASTM A48 CL35B
 DESIGN LOAD
 HEAVY DUTY
 COATING
 UNDIPPED
 OPEN AREA
 85.6 SQ. IN.

STANDARD LID FOR INFILTRATION SYSTEMS



NOTE: IF THE ADJUSTMENT EXCEEDS A 8" HEIGHT, THE CONE MUST BE REMOVED AND THE BARREL SECTION MUST BE ADJUSTED.

*REFER TO CDOT FOR ASPHALT RESTORATION REQUIREMENTS & CRACK SEALING.
 ** ON NON-MORATORIUM STREETS, FULL-DEPTH PCC BASE COURSE MAY BE UTILIZED. ON MORATORIUM STREETS, PAVEMENT RESTORATION SHALL ADHERE TO CDOT REQUIREMENTS.

NOTES:

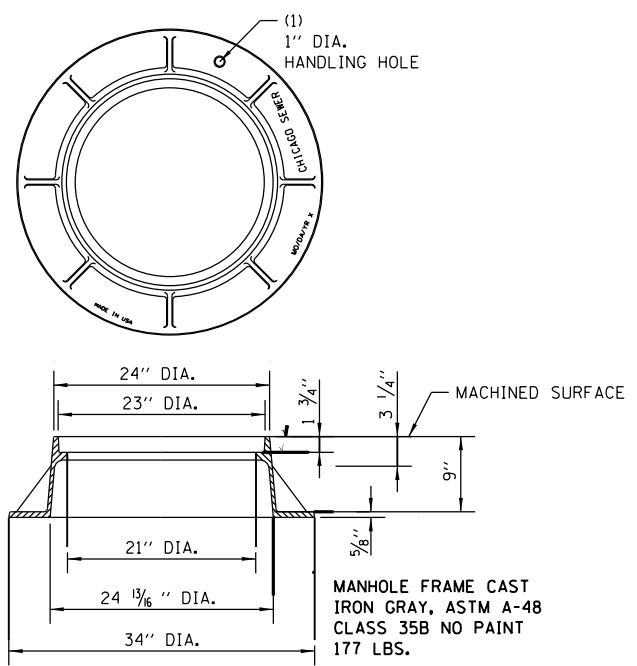
IF THE ADJUSTMENT EXCEEDS AN 8" HEIGHT, THE CONE MUST BE REMOVED AND THE BARREL SECTION MUST BE ADJUSTED.

THE CONTRACTOR IS REQUIRED TO REPLACE ANY BROKEN FRAMES AND LIDS OF SEWER STRUCTURES WITH STANDARD FRAMES AND LIDS OF THE DWM. IN ADJUSTMENT OR RECONSTRUCTION OF SEWER STRUCTURES, ANY NON-STANDARD FRAMES AND LIDS MUST BE REPLACED WITH STANDARD FRAMES AND LIDS. IN ADJUSTMENT OR RECONSTRUCTION OF INLETS, ANY NON-STANDARD INLETS (GUTTER BOXES) MUST BE REPLACED WITH DWM STANDARD INLETS.

THE FRAMES AND LIDS OF SEWER STRUCTURES TO BE ABANDONED, REMOVED, OR FILLED MUST BE SALVAGED AND THE DWM NOTIFIED FOR PICK UP.

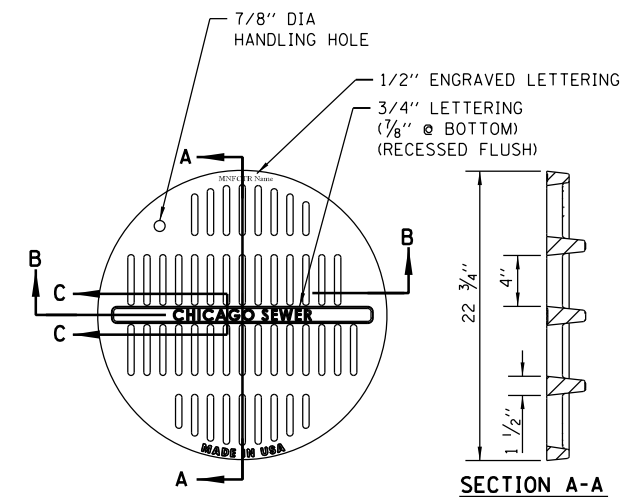
MANHOLES, CATCH BASINS AND INLETS MUST BE PROTECTED FROM THE ENTRY OF ASPHALT/DEBRIS INTO THE SEWER SYSTEM DURING CONSTRUCTION. THE CONTRACTOR MUST MARK LOCATIONS OF ALL SEWER STRUCTURES ON THE SIDEWALK BEFORE STARTING PAVEMENT REMOVAL/REPLACEMENT. ADJUSTMENT OF FRAMES AND LIDS OF SEWER STRUCTURES MUST BE COMPLETED PRIOR TO STREET RESURFACING.

PLAN VIEW (BASE TO GRADE)

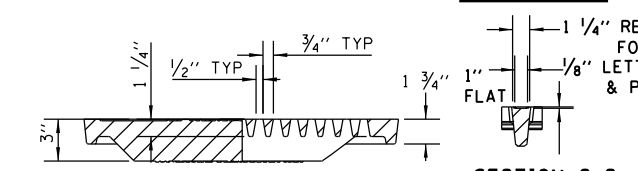


LIGHTWEIGHT MANHOLE FRAME

MANHOLE FRAME CAST IRON GRAY, ASTM A-48 CLASS 35B NO PAINT 177 LBS.



SECTION A-A



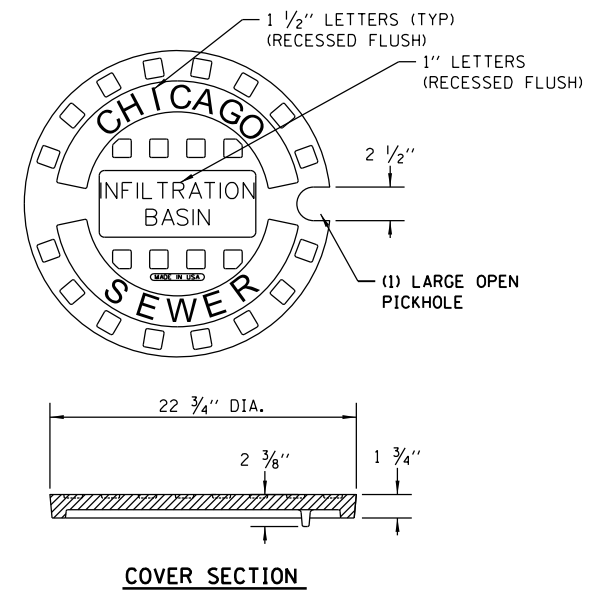
SECTION B-B

SECTION C-C

MATERIALS
 GRATE-GRAY IRON
 ASTM A48 CL35B
 DESIGN LOAD
 HEAVY DUTY
 COATING
 UNDIPPED
 OPEN AREA
 85.6 SQ. IN.

ADA MANHOLE LID

NOTE: NOT TO BE USED WITHOUT PRIOR APPROVAL OF CDWM

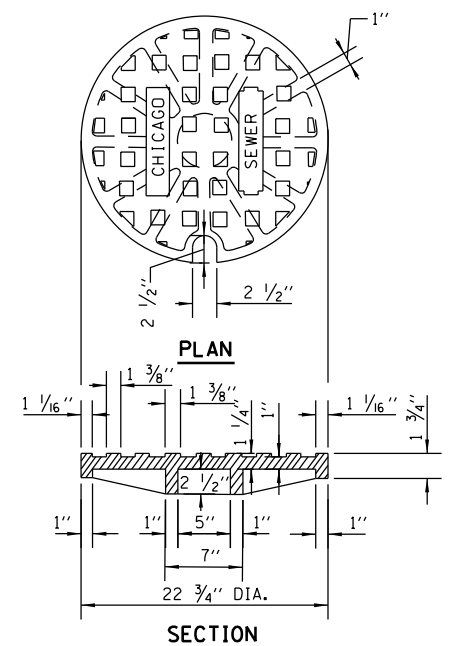


COVER SECTION

INFILTRATION SOLID LID FOR CATCH BASIN

STANDARD REVISIONS	
DATE	DESCRIPTION
1/22/14	Approved

DETAIL OF FRAME ADJUSTMENT



PLAN

SECTION

SOLID LID FOR MANHOLES

PERCENT COMPLETE	DATE	CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES		DESIGNED: SBW
30		MANHOLE LIDS AND FRAMES		A.3
60				
75				
90				
100				
BULLETIN		OF		PN

<p>303 EAST WACKER DRIVE, SUITE 1400 CHICAGO, IL 60601-5276 PHONE: (312) 373-1700 FAX: (312) 373-6800</p>	D160W30-sht-CDWM-Detail-01.dgn	DESIGNED -	REVISED -
	USER NAME = pimsarno	DRAWN -	REVISED -
	PLOT SCALE = 2.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/24/2014	DATE = 10/24/2014	REVISED -

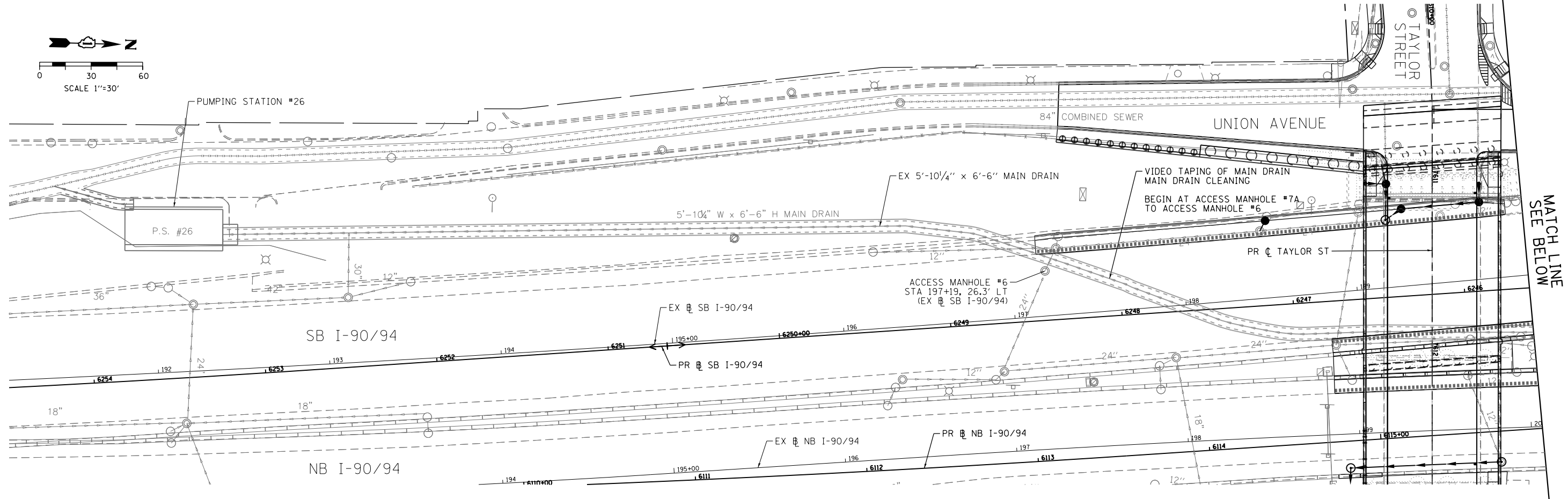
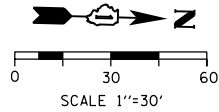
DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE = 10/24/2014	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

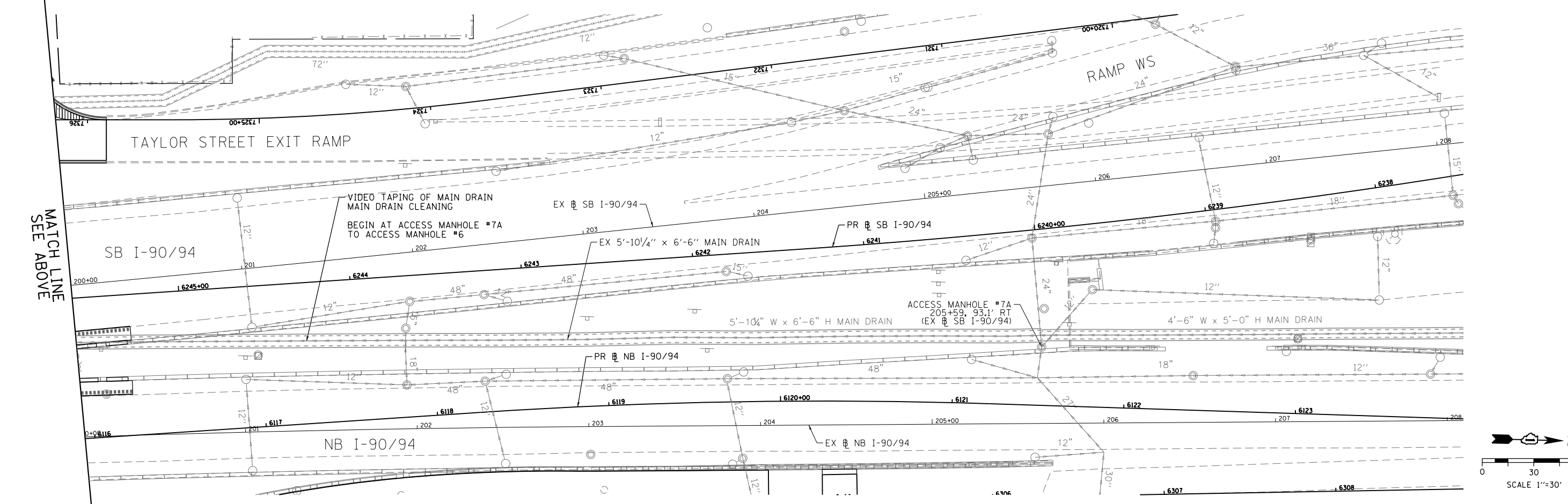
CHICAGO DEPARTMENT OF WATER MANAGEMENT (CDWM)
 STANDARD DETAILS

F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	367
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

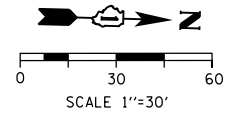
SCALE: NTS SHEET 1 OF 1 SHEETS STA. TO STA.



MATCH LINE
SEE BELOW



MATCH LINE
SEE ABOVE



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D160W30-Sht-Drain-Details-01.dgn
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 PLOT SCALE = 30.0000' / in.
 PLOT DATE = 10/24/2014

DESIGNED - AFC	REVISED -
DRAWN - AFC	REVISED -
CHECKED - DBM	REVISED -
DATE - 10/24/2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRAINAGE DETAILS

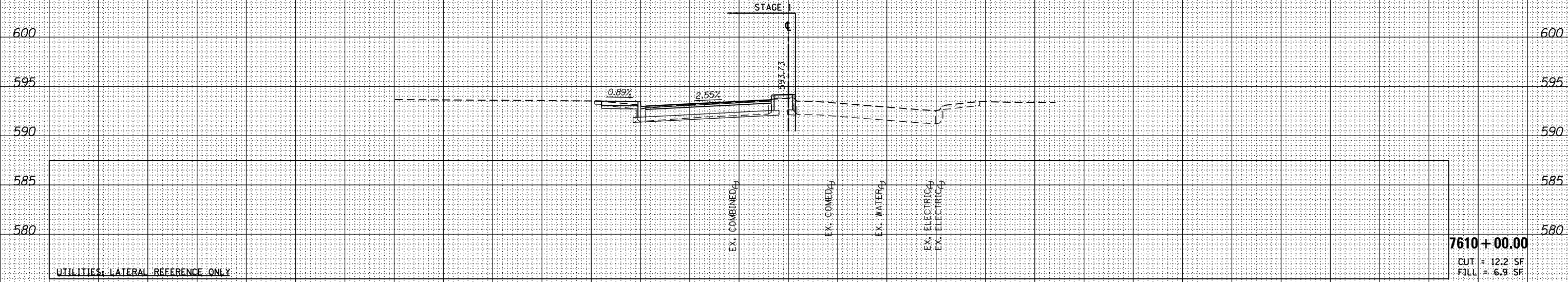
SCALE: 1" = 30' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	368
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

NOTE:
 EXISTING UTILITIES SHOWN ARE FOR LATERAL REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL EXISTING SEWERS AND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REFER TO THE UTILITY LOCATION PLANS FOR ADDITIONAL UTILITY INFORMATION.

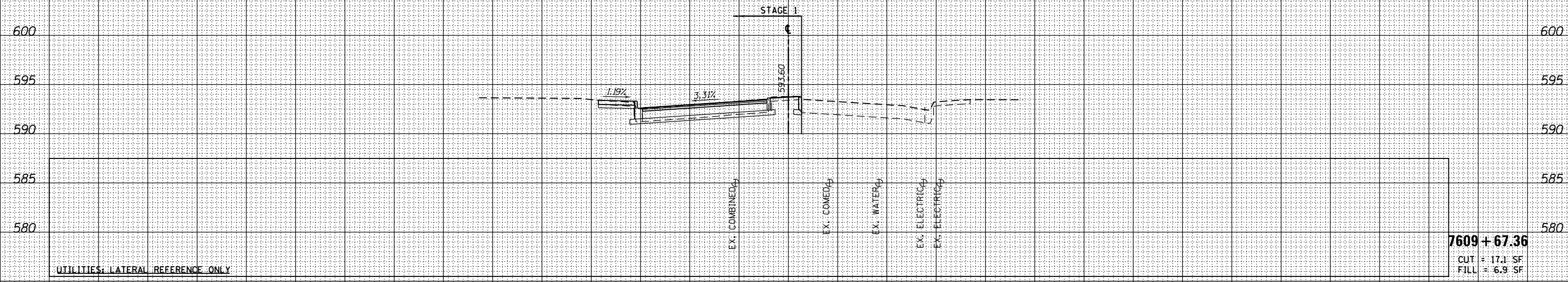
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BY	
FINAL SURVEY NO.	
SURVEYED PLOTTED AREAS CHECKED	
NOTE BOOK	
TEMPLATE	
AREAS CHECKED	



UTILITIES: LATERAL REFERENCE ONLY

7610 + 00.00
 CUT = 12.2 SF
 FILL = 6.9 SF

DATE	
BY	
ORIGINAL SURVEY NO.	
SURVEYED PLOTTED AREAS CHECKED	
NOTE BOOK	
TEMPLATE	
AREAS CHECKED	



UTILITIES: LATERAL REFERENCE ONLY

7609 + 67.36
 CUT = 17.1 SF
 FILL = 6.9 SF

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



D:\160W30-sht-XS-Taylor-Stage1.dgn	DESIGNED - NJH	REVISED -
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PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 TAYLOR STREET STAGE 1A-1B

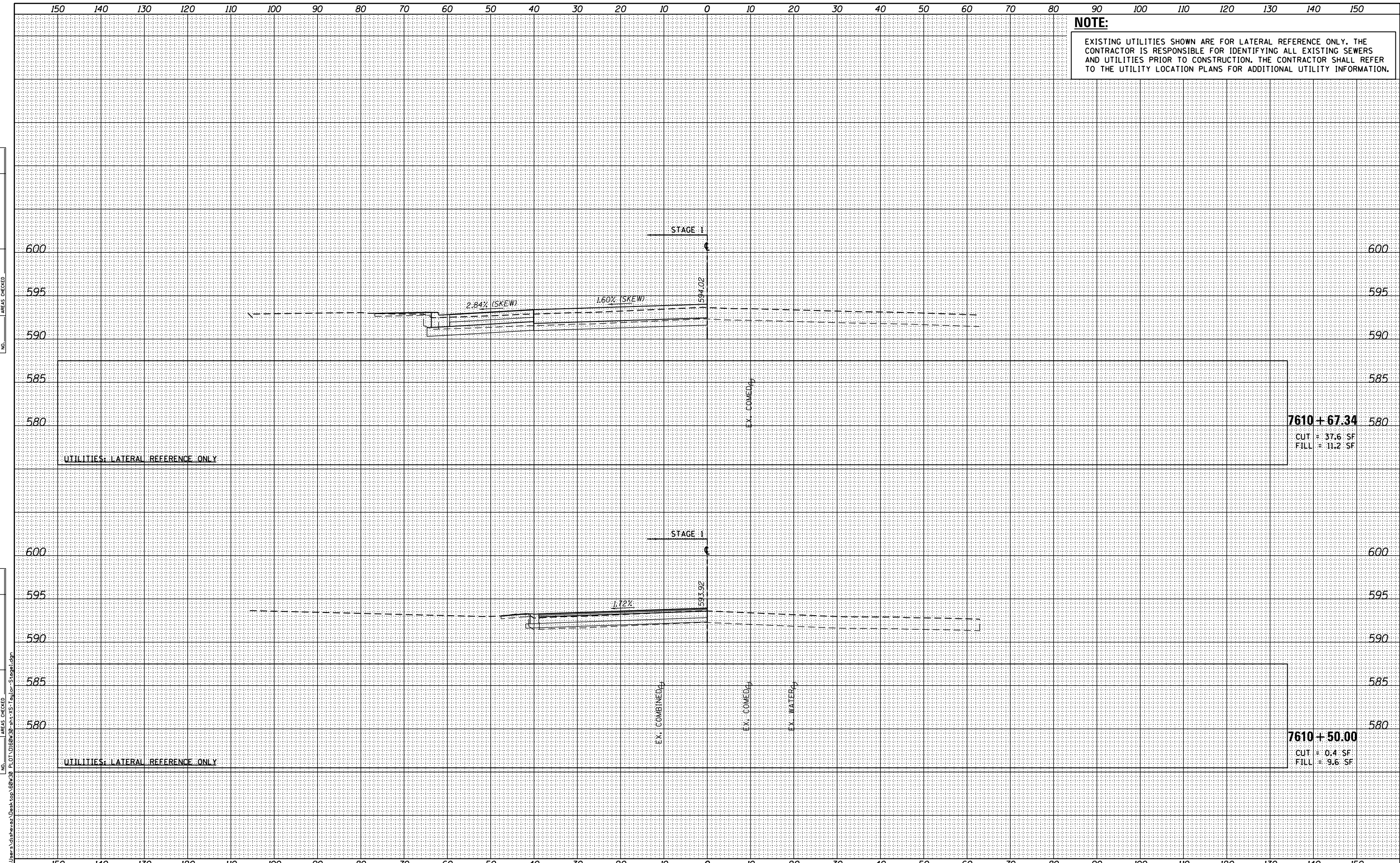
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	369
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

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DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED



NOTE:
EXISTING UTILITIES SHOWN ARE FOR LATERAL REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL EXISTING SEWERS AND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REFER TO THE UTILITY LOCATION PLANS FOR ADDITIONAL UTILITY INFORMATION.

UTILITIES: LATERAL REFERENCE ONLY

7610 + 67.34
CUT = 37.6 SF
FILL = 11.2 SF

UTILITIES: LATERAL REFERENCE ONLY

7610 + 50.00
CUT = 0.4 SF
FILL = 9.6 SF



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PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
TAYLOR STREET STAGE 1A-1B

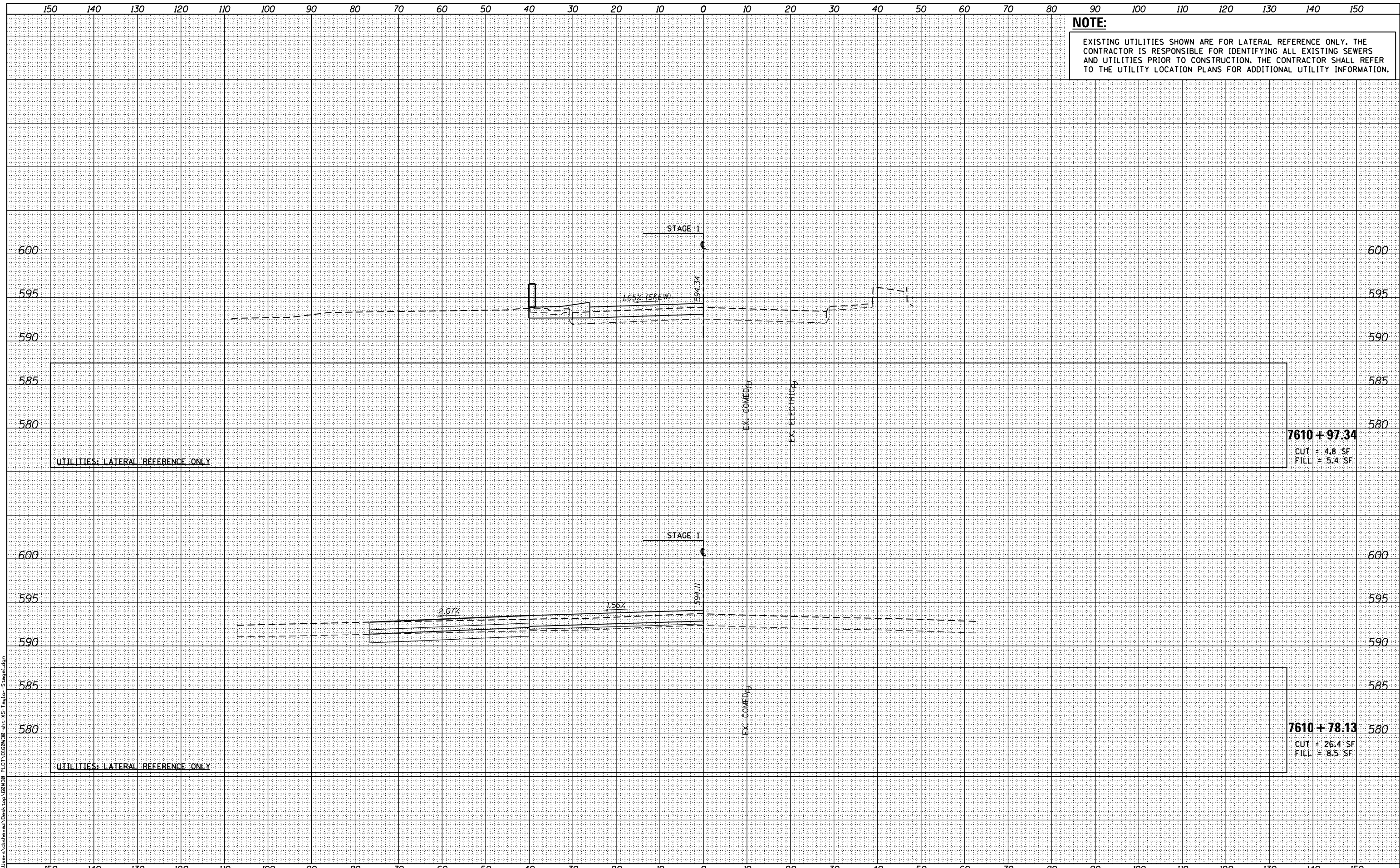
SCALE: 1:5V,1:10H SHEET 2 OF 12 SHEETS STA. 7610+50.00 TO STA. 7610+67.34

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	370
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED

NOTE:
EXISTING UTILITIES SHOWN ARE FOR LATERAL REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL EXISTING SEWERS AND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REFER TO THE UTILITY LOCATION PLANS FOR ADDITIONAL UTILITY INFORMATION.



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PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
TAYLOR STREET STAGE 1A-1B**

SCALE: 1:5V,1:10H SHEET 3 OF 12 SHEETS STA. 7610+78.13 TO STA. 7610+97.34

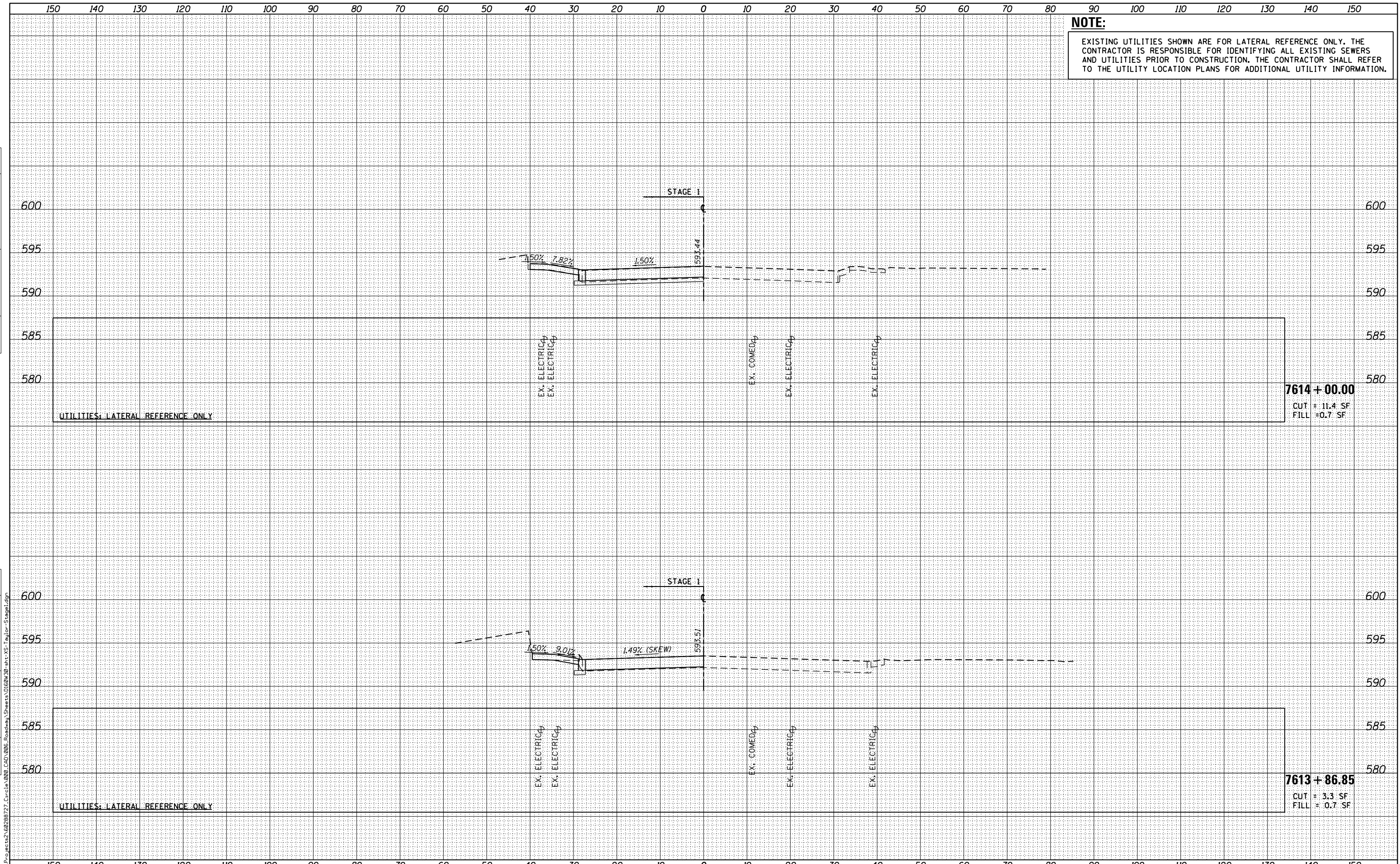
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	371
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

NOTE:

EXISTING UTILITIES SHOWN ARE FOR LATERAL REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL EXISTING SEWERS AND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REFER TO THE UTILITY LOCATION PLANS FOR ADDITIONAL UTILITY INFORMATION.

DATE	
BY	
NO.	
FINAL SURVEY	
NO. CHECKED	
AREAS	
PLOTTED	
TEMPLATE	
NOTE BOOK	
NO.	
ORIGINAL SURVEY	
NO. CHECKED	
AREAS	
PLOTTED	
TEMPLATE	
NOTE BOOK	
NO.	

DATE	
BY	
NO.	
ORIGINAL SURVEY	
NO. CHECKED	
AREAS	
PLOTTED	
TEMPLATE	
NOTE BOOK	
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ORIGINAL SURVEY	
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AREAS	
PLOTTED	
TEMPLATE	
NOTE BOOK	
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USER NAME = holmann	DRAWN - NJH	REVISED -
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PLOT DATE = 12/4/2014	DATE - 12/05/14	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
TAYLOR STREET STAGE 1A-1B**

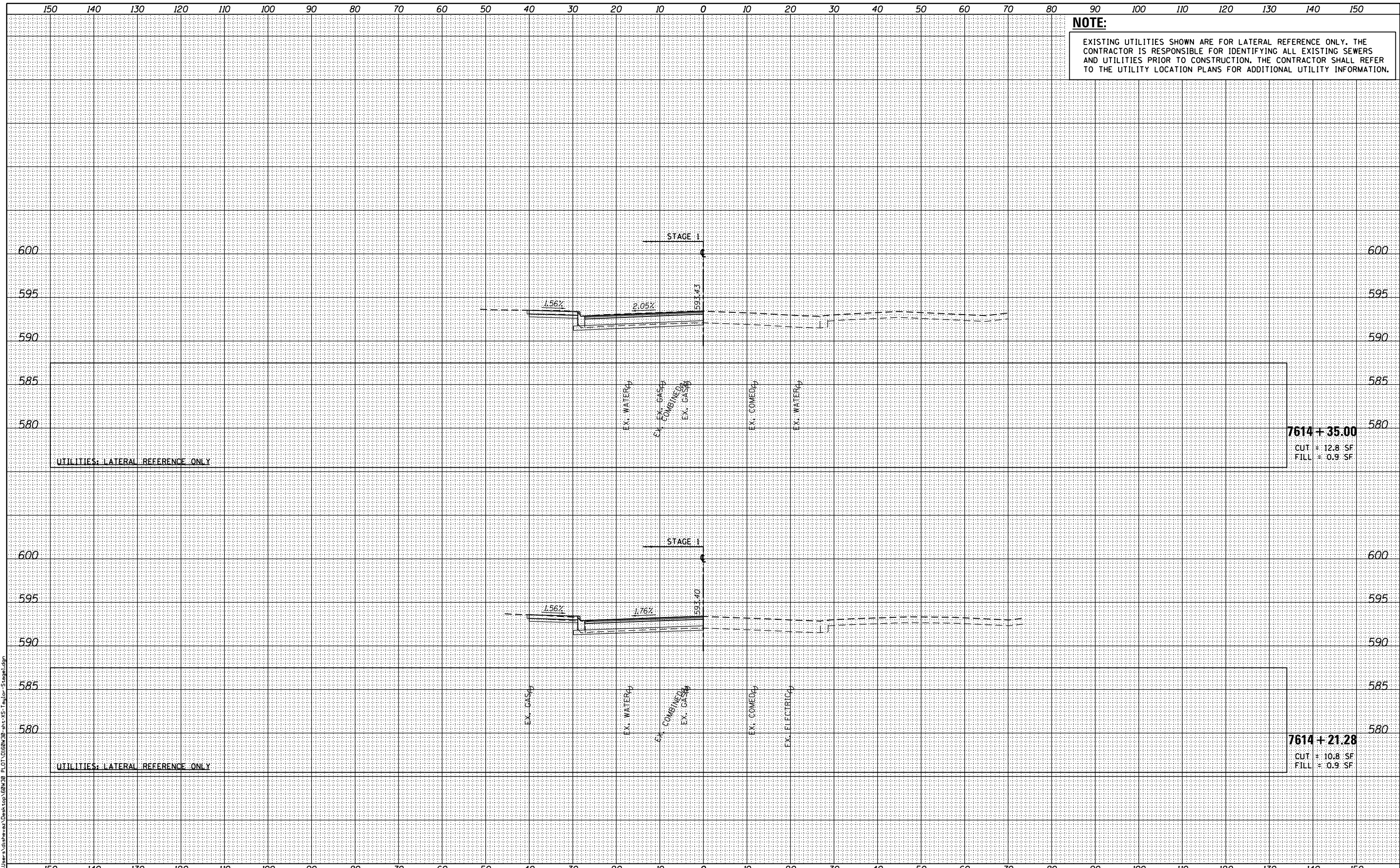
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	372
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

NOTE:
EXISTING UTILITIES SHOWN ARE FOR LATERAL REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL EXISTING SEWERS AND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REFER TO THE UTILITY LOCATION PLANS FOR ADDITIONAL UTILITY INFORMATION.



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PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
TAYLOR STREET STAGE 1A-1B

SCALE: 1:5V,1:10H SHEET 6 OF 12 SHEETS STA. 7614+21.28 TO STA. 7614+35.00

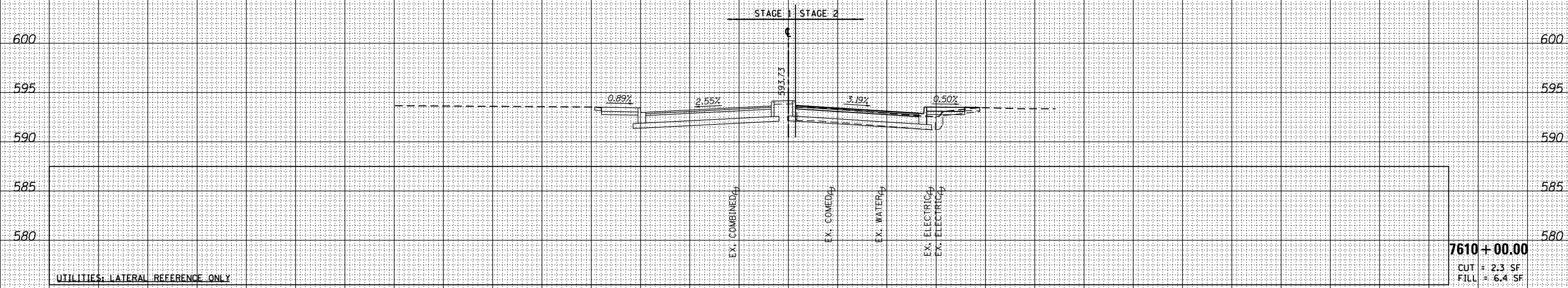
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	374
CONTRACT NO. 60W30				

ILLINOIS FED. AID PROJECT

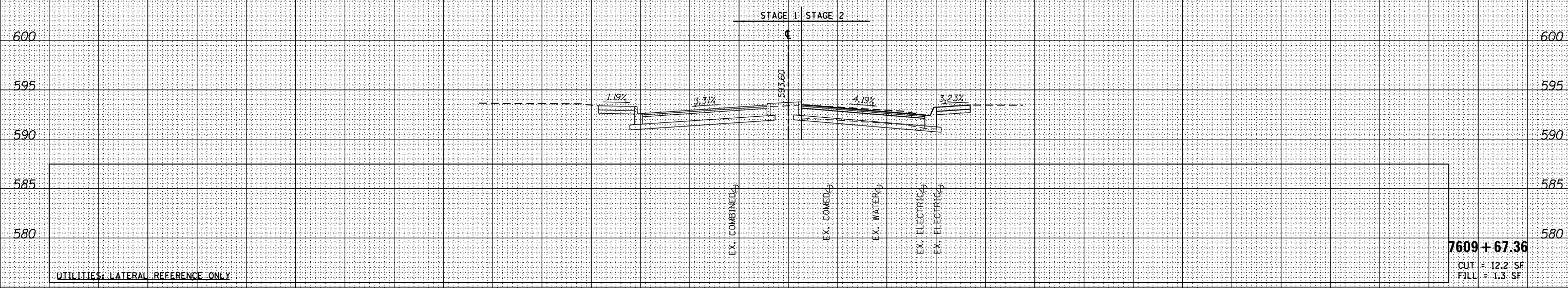
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NOTE:
EXISTING UTILITIES SHOWN ARE FOR LATERAL REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL EXISTING SEWERS AND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REFER TO THE UTILITY LOCATION PLANS FOR ADDITIONAL UTILITY INFORMATION.

DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
NO.	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	
UTILITIES: LATERAL REFERENCE ONLY	



DATE	
BY	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	
UTILITIES: LATERAL REFERENCE ONLY	



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PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
TAYLOR STREET STAGE 2A-2B

SCALE: 1:5V,1:10H SHEET 7 OF 12 SHEETS STA. 7609+67.36 TO STA. 7610+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	375
CONTRACT NO. 60W30				
ILLINOIS FED. AID PROJECT				

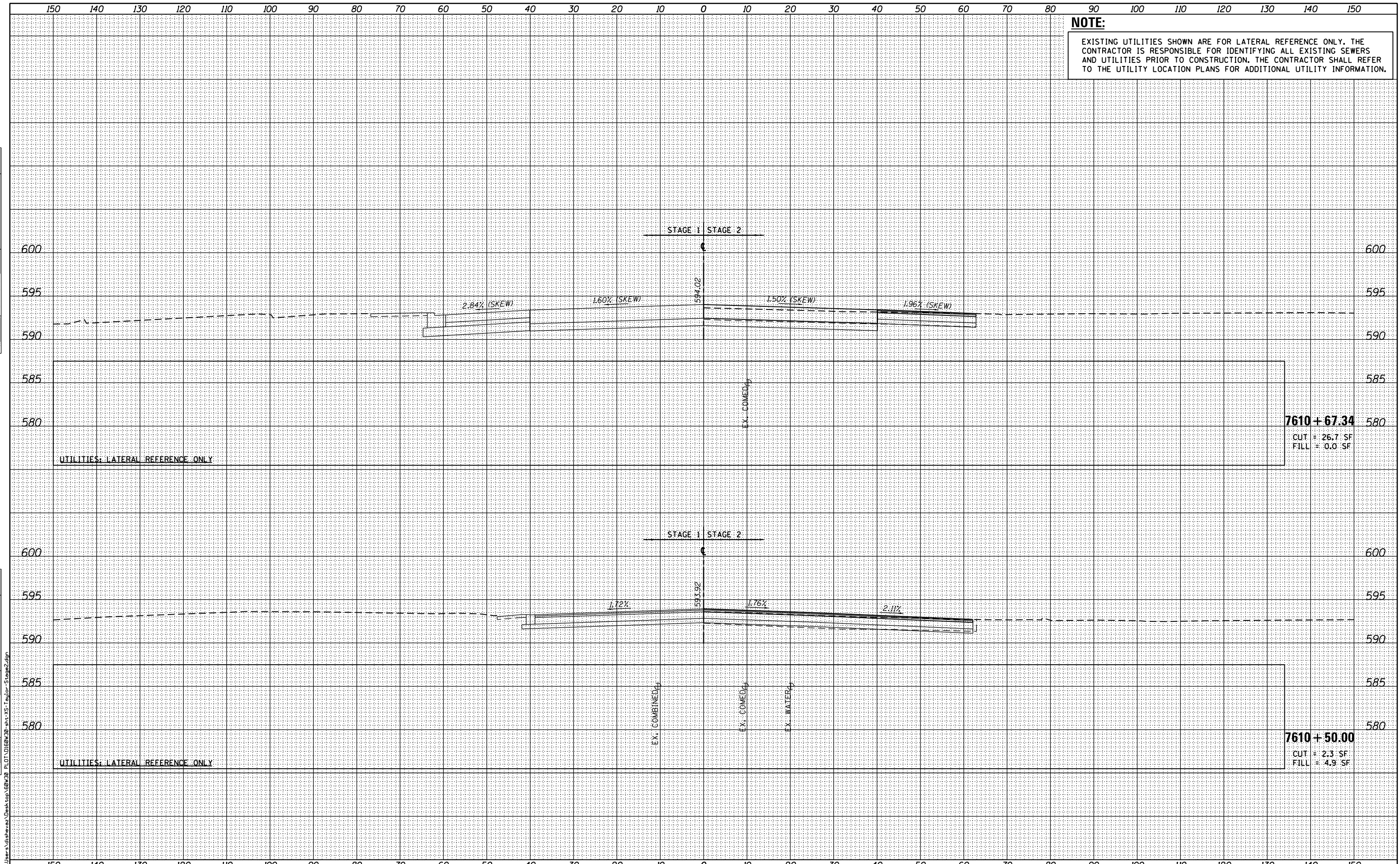
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NOTE:

EXISTING UTILITIES SHOWN ARE FOR LATERAL REFERENCE ONLY. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL EXISTING SEWERS AND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REFER TO THE UTILITY LOCATION PLANS FOR ADDITIONAL UTILITY INFORMATION.

DATE	BY
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	BY
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



7610 + 67.34
 CUT = 26.7 SF
 FILL = 0.0 SF

7610 + 50.00
 CUT = 2.3 SF
 FILL = 4.9 SF



D:\60W30-sht-XS-Taylor-Stage2.dgn	DESIGNED - NJH	REVISED -
USER NAME = dshevoz	DRAWN - NJH	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

**CROSS SECTIONS
 TAYLOR STREET STAGE 2A-2B**

SCALE: 1:5V,1:10H SHEET 8 OF 12 SHEETS STA. 7610+50.00 TO STA. 7610+67.34

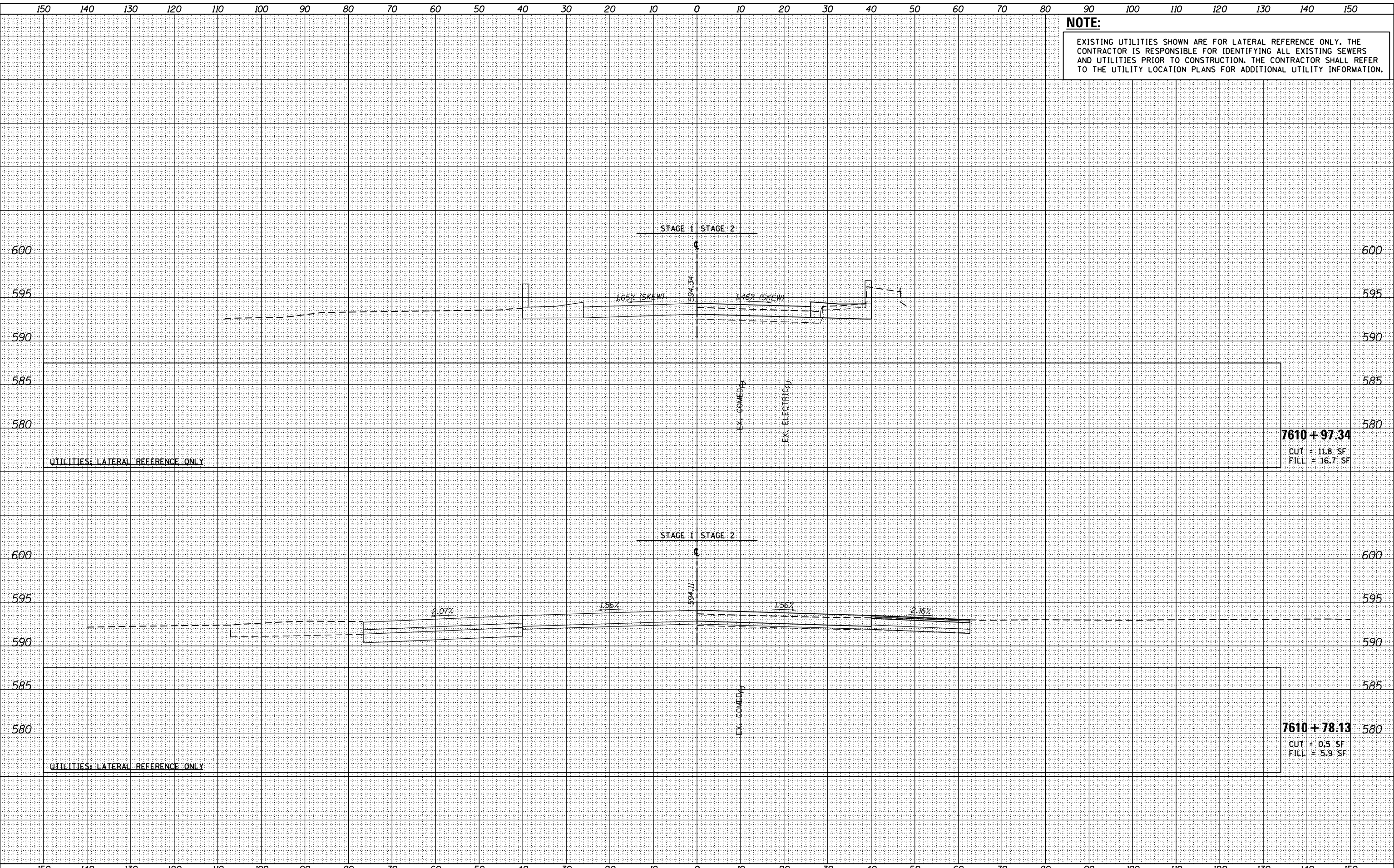
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	376
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

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UTILITIES: LATERAL REFERENCE ONLY

7610 + 97.34
CUT = 11.8 SF
FILL = 16.7 SF

UTILITIES: LATERAL REFERENCE ONLY

7610 + 78.13
CUT = 0.5 SF
FILL = 5.9 SF



D:\60W30-sht-XS-Taylor-Stage2.dgn	DESIGNED - NJH	REVISED -
USER NAME = dshevoz	DRAWN - NJH	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
TAYLOR STREET STAGE 2A-2B

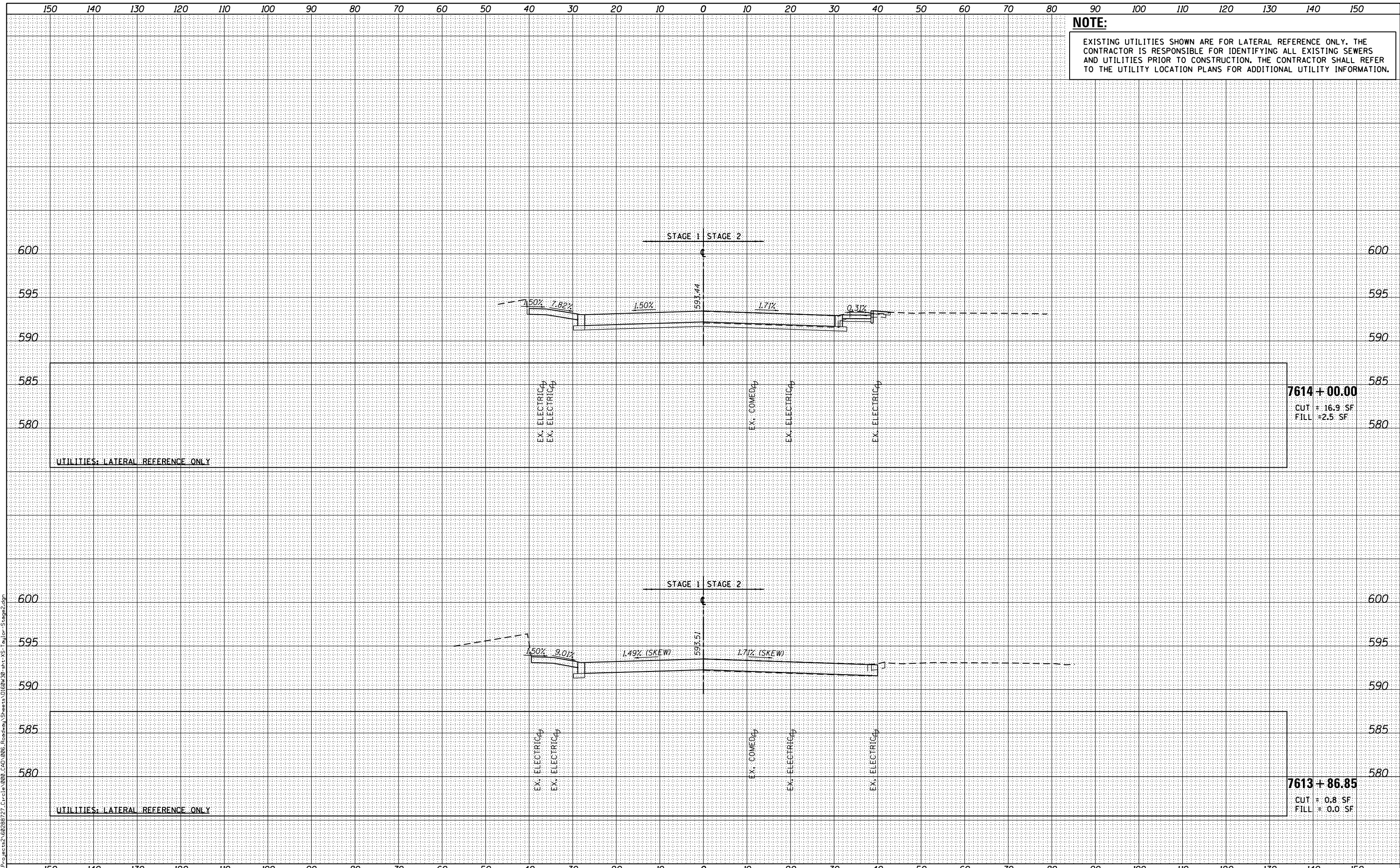
SCALE: 1:5V,1:10H SHEET 9 OF 12 SHEETS STA. 7610+78.13 TO STA. 7610+97.34

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	377
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

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FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE



UTILITIES: LATERAL REFERENCE ONLY

UTILITIES: LATERAL REFERENCE ONLY

7614 + 00.00
CUT = 16.9 SF
FILL = 2.5 SF

7613 + 86.85
CUT = 0.8 SF
FILL = 0.0 SF



D:\160w30-sht-XS-Taylor-Stage2.dgn	DESIGNED - NJH	REVISED -
USER NAME = holmann	DRAWN - NJH	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 12/4/2014	DATE - 12/05/14	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
TAYLOR STREET STAGE 2A-2B**

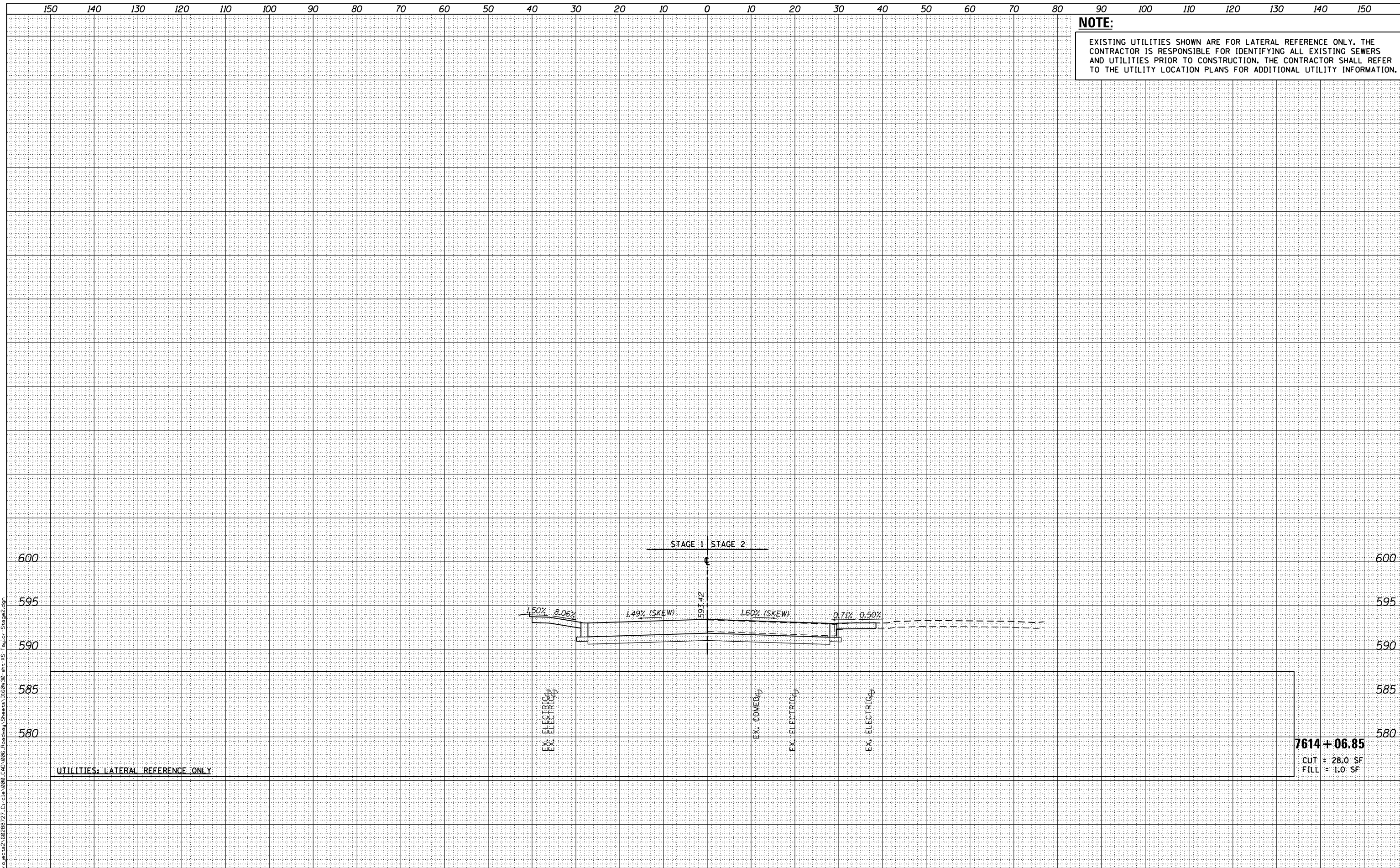
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	378
CONTRACT NO. 60W30				ILLINOIS FED. AID PROJECT

DATE	
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ORIGINAL SURVEY	
NOTE BOOK	
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ORIGINAL SURVEY	
NOTE BOOK	
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UTILITIES: LATERAL REFERENCE ONLY

7614 + 06.85
CUT = 28.0 SF
FILL = 1.0 SF



D160W30-sht-XS-Taylor-Stage2.dgn	DESIGNED - NJH	REVISED -
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PLOT DATE = 12/4/2014	DATE - 12/05/14	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
TAYLOR STREET STAGE 2A-2B

SCALE: 1:5V,1:10H SHEET 11 OF 12 SHEETS STA. 7613+77.05 TO STA. 7614+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	379
CONTRACT NO. 60W30				

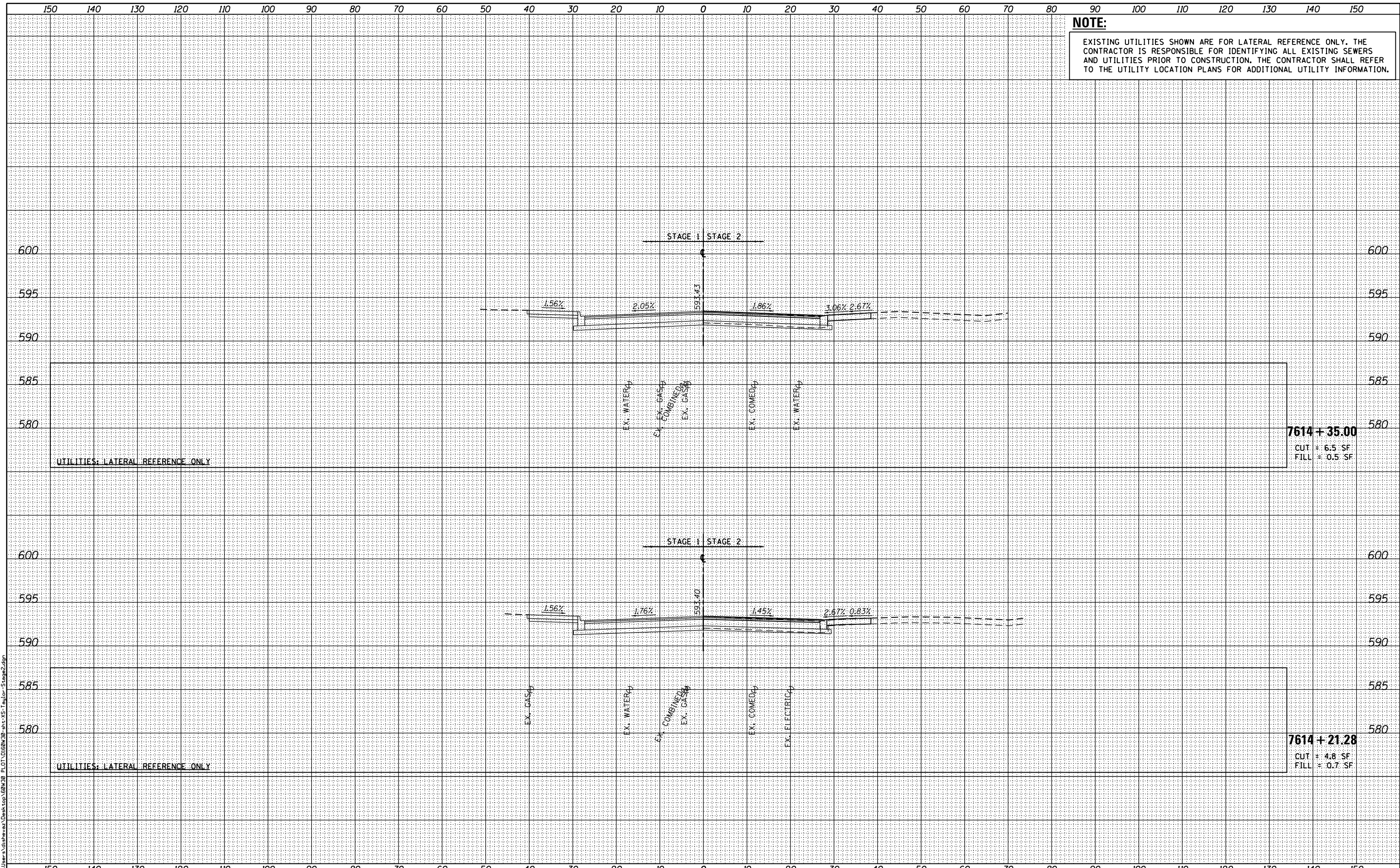
ILLINOIS FED. AID PROJECT

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FINAL SURVEY	
NOTE BOOK	
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TEMPLATE	
AREAS CHECKED	

DATE	
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ORIGINAL SURVEY	
NOTE BOOK	
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AREAS CHECKED	

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D160W30-sht-XS-Taylor-Stage2.dgn	DESIGNED - NJH	REVISED -
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PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
TAYLOR STREET STAGE 2A-2B

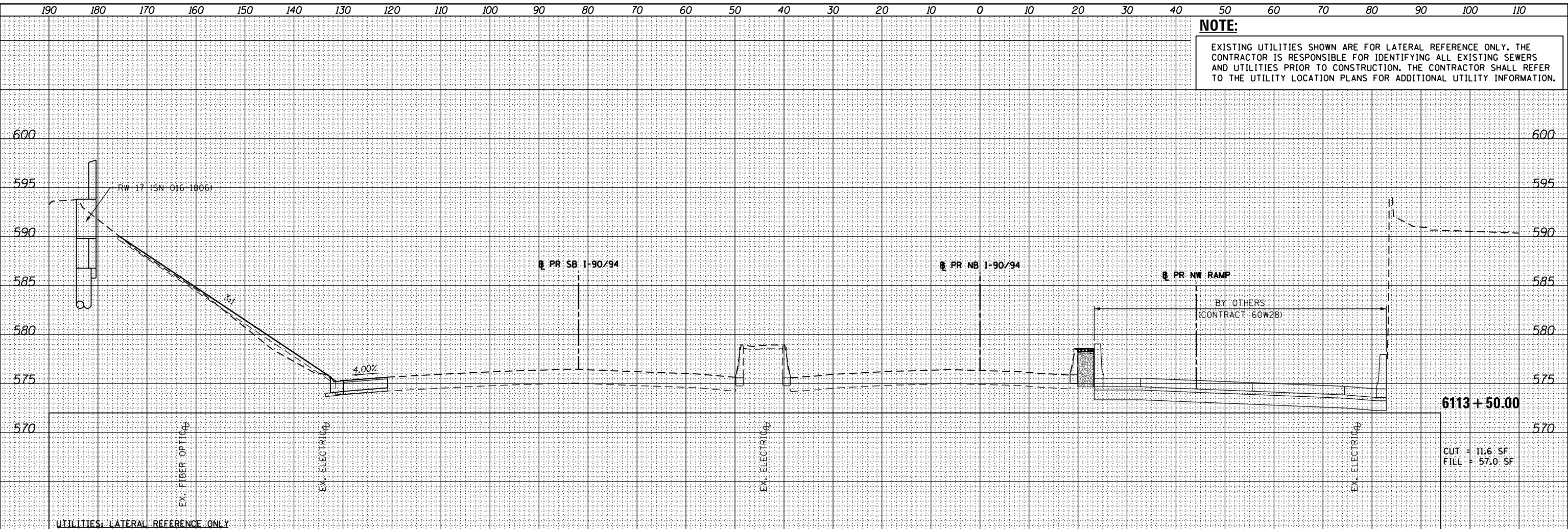
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	380
CONTRACT NO. 60W30				

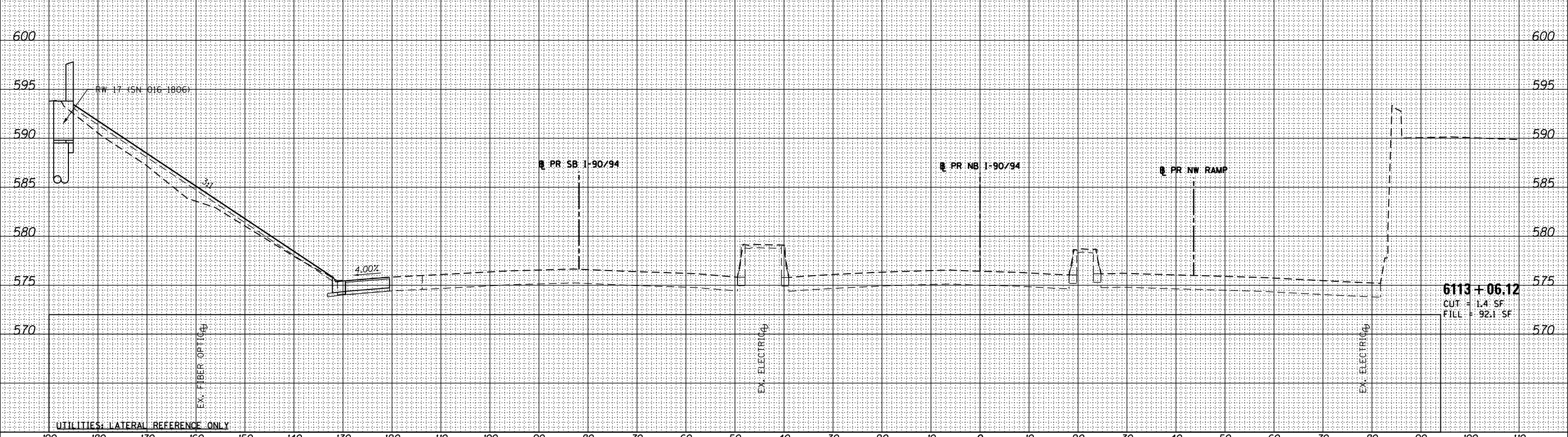
ILLINOIS FED. AID PROJECT

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DATE	
BY	
FINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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D160W30-sht-XS-Mainline-Interim.dgn
DESIGNED - NJH
REVISER -
USER NAME = dshevoz
DRAWN - NJH
REVISER -
PLOT SCALE = 20.0000' / in.
CHECKED - OPS
REVISER -
PLOT DATE = 10/24/2014
DATE - 10/24/2014
REVISER -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
I-90/94**

SCALE: 1:5V, 1:10H SHEET 1 OF 5 SHEETS STA. 6113+06.12 TO STA. 6113+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	381
CONTRACT NO. 60W30				

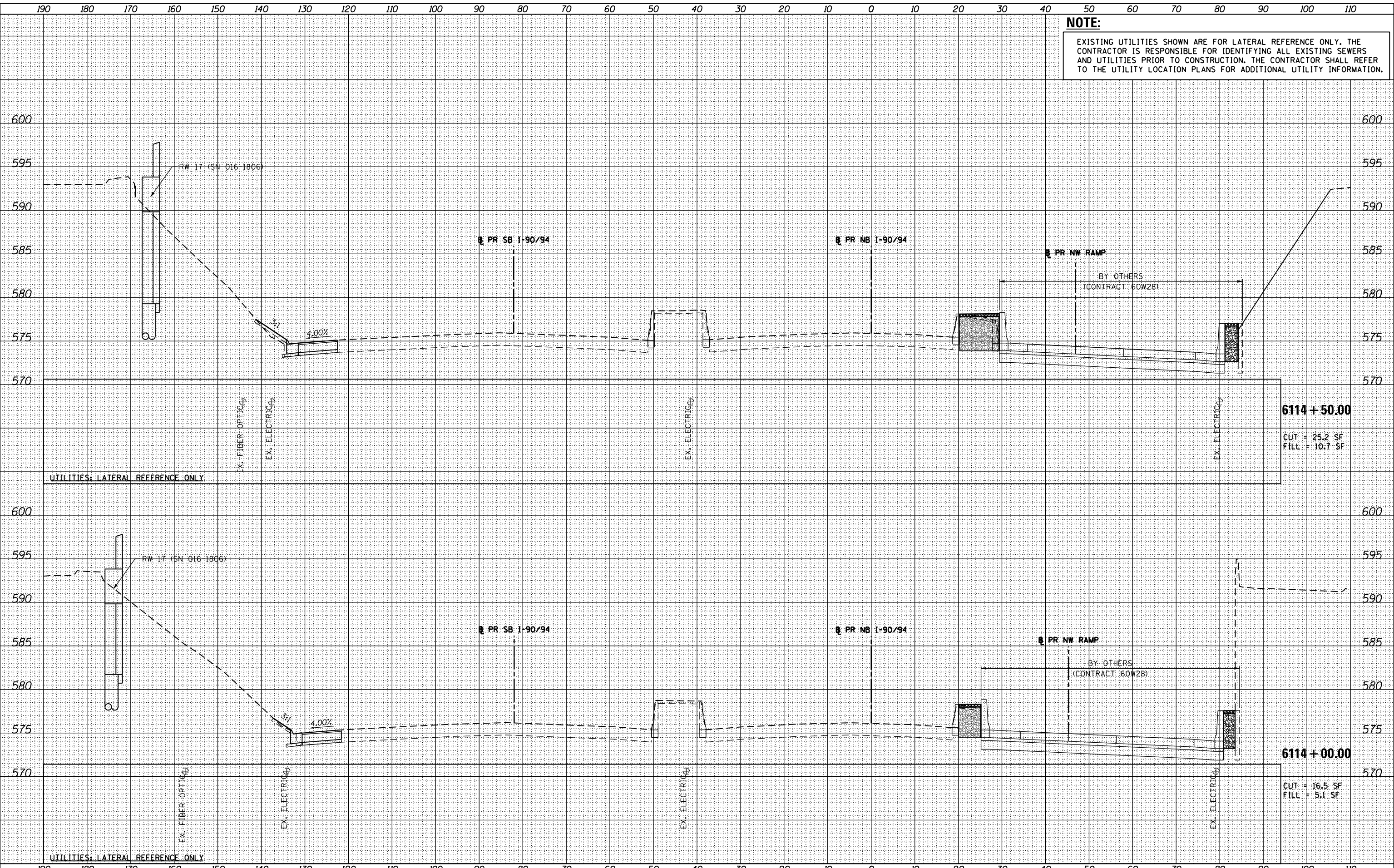
ILLINOIS FED. AID PROJECT

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DATE	
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FINAL SURVEY	
SURVEYED	
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TEMPLATE	
AREAS	
CHECKED	
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DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

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USER NAME = dishvaz	DRAWN - NJH	REVISED -
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PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
I-90/94**

SCALE: 1:5V,1:10H SHEET 2 OF 5 SHEETS STA. 6114+00.00 TO STA. 6114+50.00

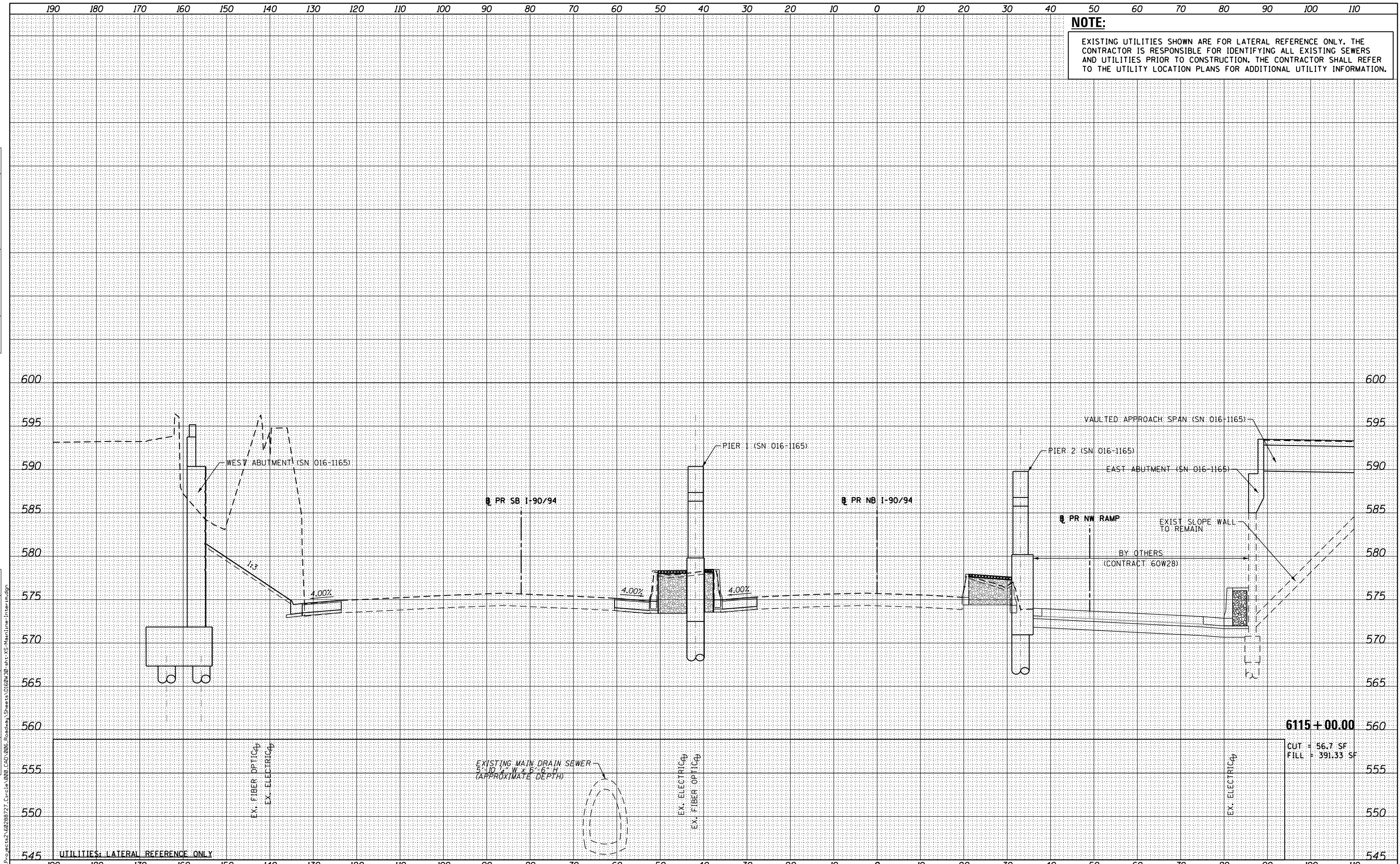
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	382
			CONTRACT NO. 60W30	
ILLINOIS FED. AID PROJECT				

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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



AECOM
 303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5275
 PHONE: (312) 373-7700 FAX: (312) 373-6800

D160W30-sht-XS-Mainline-Interim.dgn	DESIGNED - NJH	REVISED -
USER NAME = holmann	DRAWN - NJH	REVISED -
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PLOT DATE = 12/4/2014	DATE - 12/05/14	REVISED -

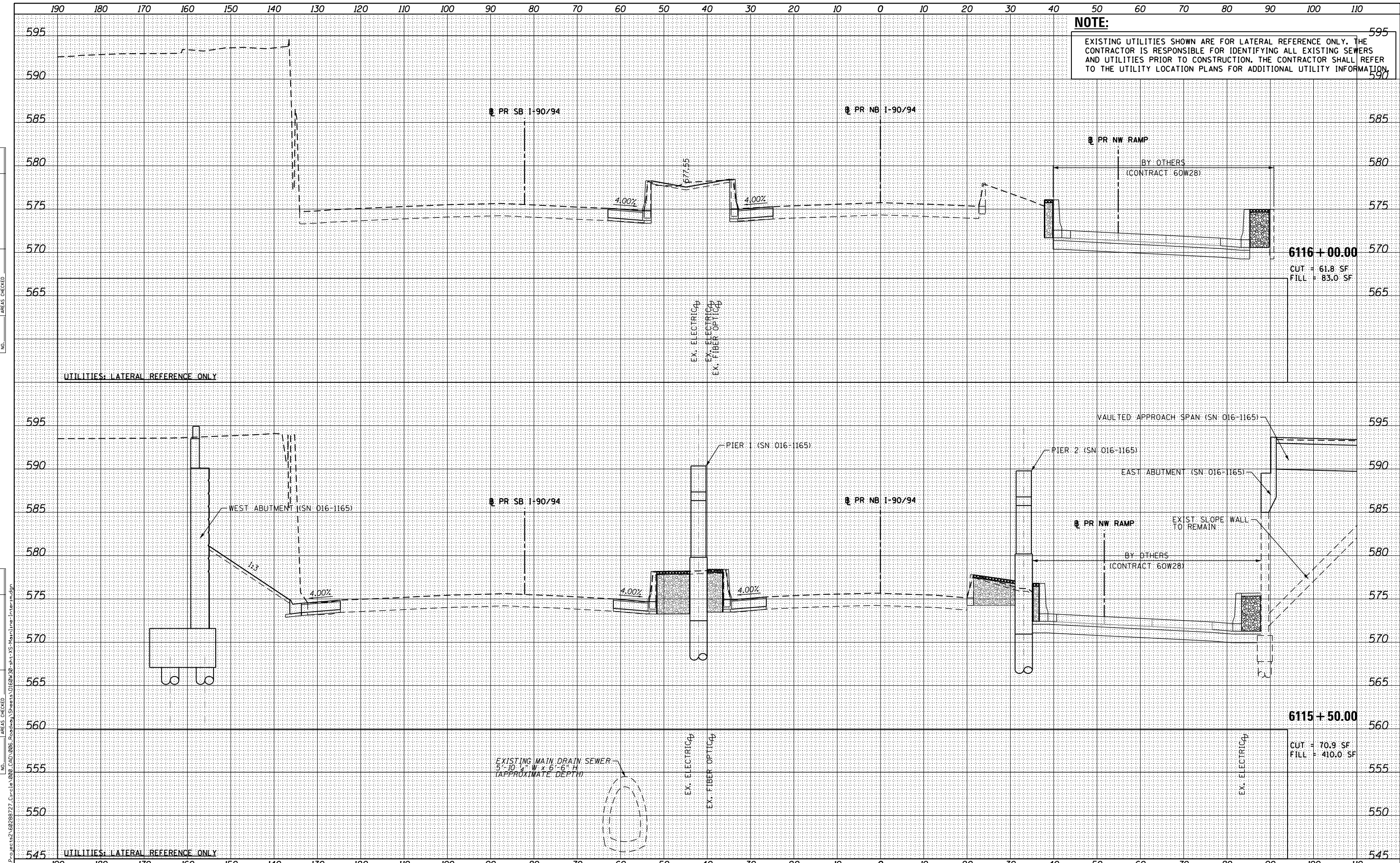
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS I-9094	
SCALE: 1:5V,1:10H	SHEET 3 OF 5 SHEETS
STA. 6115+00.00 TO STA. 6115+00.00	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	383
CONTRACT NO. 60W30			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



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6116 + 00.00
 CUT = 61.8 SF
 FILL = 83.0 SF

6115 + 50.00
 CUT = 70.9 SF
 FILL = 410.0 SF



D160W30-sht-XS-Mainline-Interim.dgn	DESIGNED - NJH	REVISED -
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PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 12/4/2014	DATE - 12/05/14	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
 I-90/94**

SCALE: 1:5V, 1:10H SHEET 4 OF 5 SHEETS STA. 6115+50.00 TO STA. 6116+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	384
CONTRACT NO. 60W30				

ILLINOIS FED. AID PROJECT

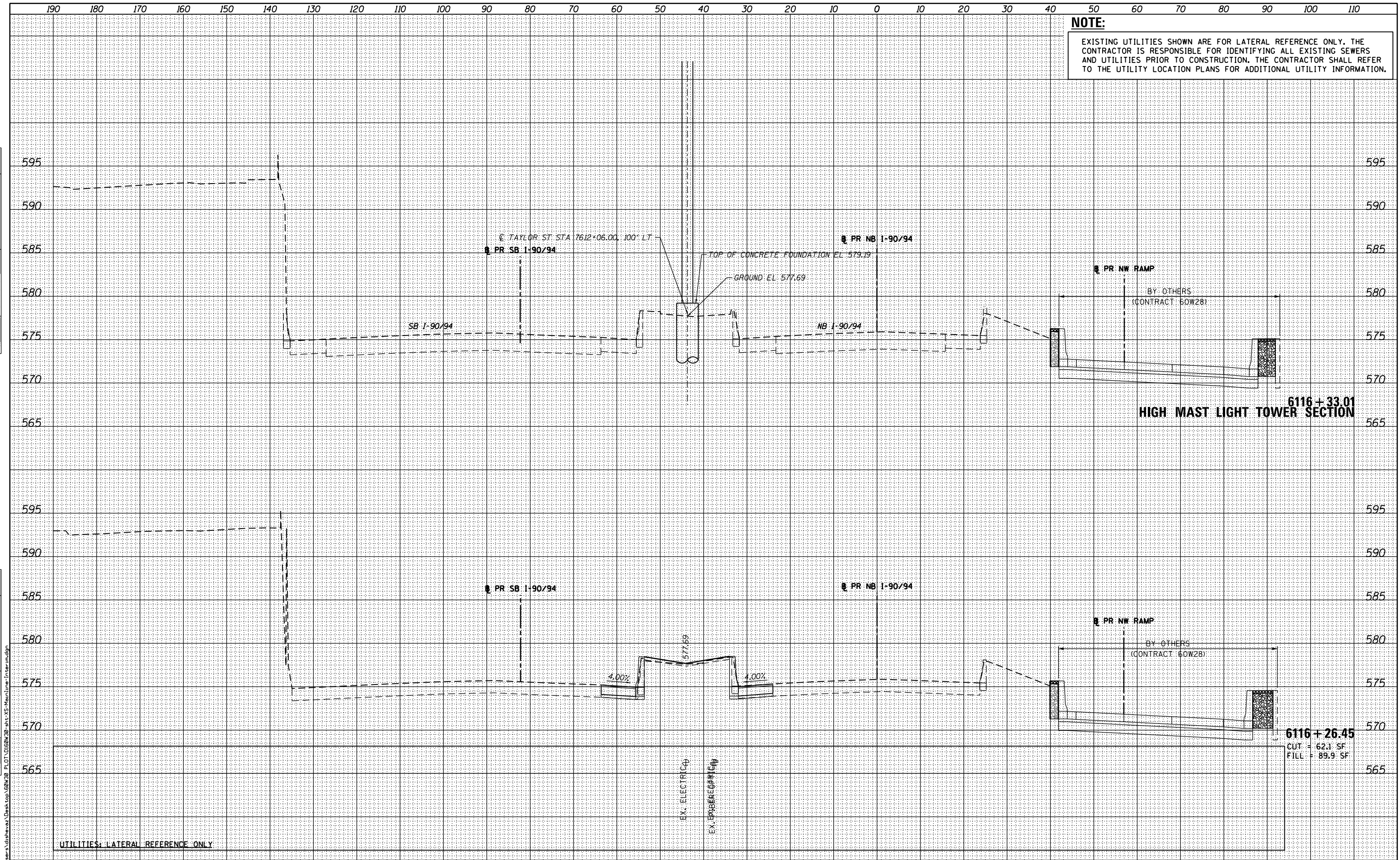
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DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED



UTILITIES: LATERAL REFERENCE ONLY



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USER NAME = dshevoz	DRAWN - NJH	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 10/24/2014	DATE - 10/24/2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
I-90/94**

SCALE: 1:5V, 1:10H SHEET 5 OF 5 SHEETS STA. 6116+26.45 TO STA. 6116+26.45

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2013-012R	COOK	385	385
CONTRACT NO. 60W30				

ILLINOIS FED. AID PROJECT

FILE PATH = C:\Users\dshevoz\Desktop\60W30_PLOT\0160W30-sht-XS-Mainline-Interim.dgn