

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

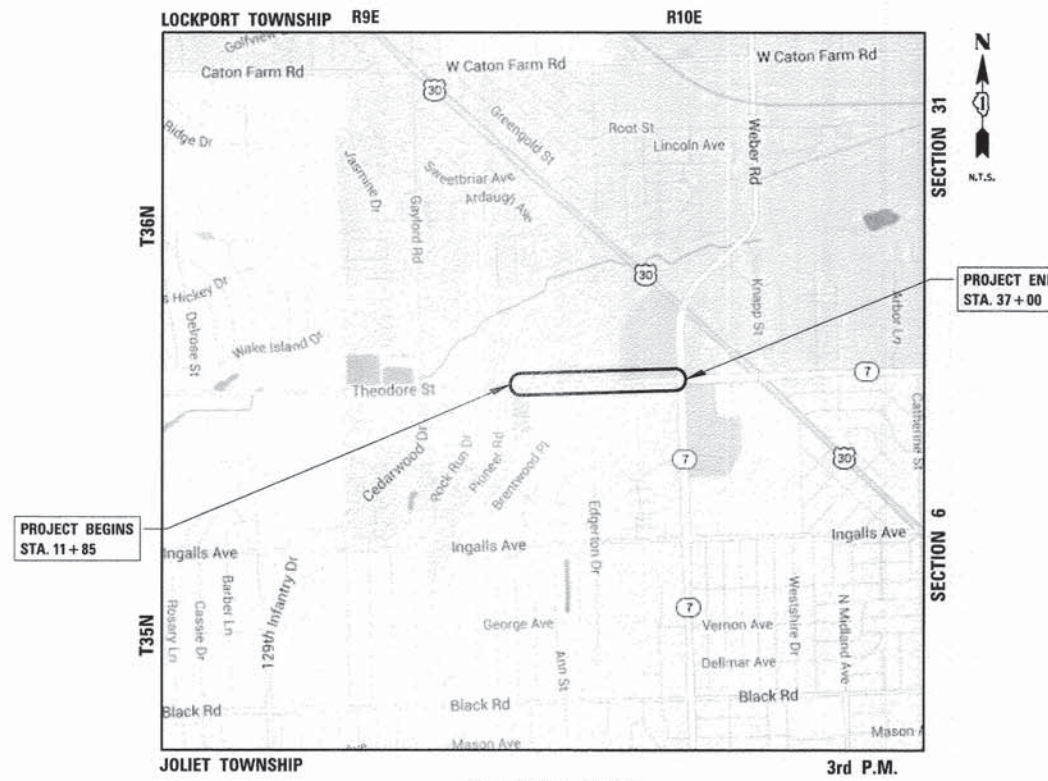
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU 0295 (THEODORE STREET)
GAYLORD AVE. TO IL ROUTE 7 (LARKIN AVE)
RECONSTRUCTION

SECTION NO. 14-00046-00-FP
PROJECT NO. M-4003 (416)
CITY OF CREST HILL
WILL COUNTY
JOB NO. C-91-115-15

FOR INDEX OF SHEETS, SEE SHEET NO. 2

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0295	14-00046-00-FP	WILL	16	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 61B10		



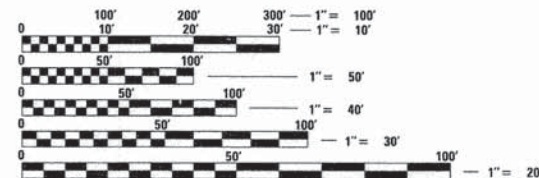
LOCATION MAP

THEODORE STREET
GROSS LENGTH OF PROJECT = 2,515 LINEAL FEET (0.476 MILES)
NET LENGTH OF PROJECT = 2,515 LINEAL FEET (0.476 MILES)

TRAFFIC DATA

THEODORE STREET
ADT (YEAR) = 9000 (2013)
POSTED SPEED LIMIT = 30 MPH

DESIGN DESIGNATION: MINOR ARTERIAL



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
8-1-1 OR 1-800-892-0123

CB CHRISTOPHER B. BURKE ENGINEERING LTD.
1938 E. Lincoln Highway, Suite 212
New Lenox, Illinois 60451 (815) 463-9050

PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/15

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED December 18, 2014
[Signature]
MAYOR, CITY OF CREST HILL

PASSED DECEMBER 29, 2014
[Signature]
DISTRICT 3 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR
BID BASED ON
LIMITED REVIEW DECEMBER 30, 2014
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

Casey J. McCollom
062-059173 REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS
[Signature] 12/16/14
ENGINEER DATE

CASEY J. MCCOLLOM
ILLINOIS REGISTRATION No. 062-059173
EXPIRATION DATE: 11/30/2015

PROGRAM AND OFFICE ENGINEER: FAWAD AOUEEL, P.E. (847) 705-4021
SCHAUMBURG, ILLINOIS

CONTRACT NO. 61B10

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE LATEST REVISION; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", (SSTCI); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", JULY 2014 SEVENTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

CODES OF THE IEPA TITLE 35, AND O.S.H.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

ALL REQUIRED PERMITS FROM THE PROPER GOVERNING AGENCY SHALL BE OBTAINED FOR CONSTRUCTION ALONG OR ACROSS EXISTING STREETS OR HIGHWAYS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE AGENCY. AT THE CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITY FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE CITY DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE CITY. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT-MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENT OR PROPERTY OR REFERENCE MARKERS UNTIL THE CITY, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF INLET FILTERS.

ALL EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR, AND INCLUDED IN THE COST OF PAVEMENT REMOVAL, VARIABLE DEPTH.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE CITY OF CREST HILL WATER DEPARTMENT (TEL. 815-741-5400) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM AGGREGATE FOR TEMPORARY ACCESS.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED IN ADJACENT REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, STRUCTURES TO BE ADJUSTED, AND STRUCTURES TO BE RECONSTRUCTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

SALT TOLERANT SODDING AND TOPSOIL FURNISH AND PLACE, 4" SHALL NOT BE PAID FOR BUT SHALL BE INCLUDED IN THE COSTS FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL, AND SIDEWALK REMOVAL.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS SHOWN ON THE PLANS (SEE IDOT ST. 424001-07 (INCLUDED IN THE SPECIFICATIONS)).

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE INCLUDED IN TRAFFIC CONTROL AND PROTECTION, SPECIAL.

ADVANCED WARNING CHANGEABLE MESSAGE BOARDS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY 2 WEEKS PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION WITHIN THE CITY'S RIGHT OF WAY NOTIFYING THE MOTORING PUBLIC OF THE UPCOMING WORK (ROAD CONSTRUCTION/DETOUR BEGINNING, EXPECT DELAYS. THE LANGUAGE MUST BE PROVIDED TO THE ENGINEER FOR REVIEW APPROVAL PRIOR TO THEIR ACTIVATION. THE MESSAGE BOARD LOCATION SHALL BE REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. THIS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH AS "CHANGEABLE MESSAGE SIGN."

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

ANY REPAIRS FOR DAMAGE BY THE CONTRACTOR OUTSIDE THE LIMITS OF WORK TO SIDEWALKS AND DRIVEWAY APRONS SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

VANDALISM - SPECIAL ATTENTION IS CALLED TO THE SPECIAL PROVISION FOR "INSPECTION" AS WELL AS ARTICLE 107.30 OF THE "STANDARD SPECIFICATIONS." ANY DEFACED WORK AS DETERMINED BY THE CITY SHALL BE CORRECTED OR REPLACED TO THE SATISFACTION OF THE ENGINEER BY THE CONTRACTOR AT HIS SOLE EXPENSE PRIOR TO FINAL PAYMENT. THE CITY OF CREST HILL WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE. THE CITY WILL NOT BE RESPONSIBLE FOR THE SECURITY OF THE WORK SITE IN THIS REGARD, OTHER THAN NORMAL PATROLLING AND RESPONSE TO EMERGENCIES. THE COST OF ADDITIONAL SECURITY REQUIRED TO MEET THIS SPECIAL PROVISION SHALL INCLUDED IN THE COST OF MOBILIZATION.

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

INDEX OF SHEETS

DESCRIPTION	SHEET NO.
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HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-04	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701427-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 40 MPH
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES

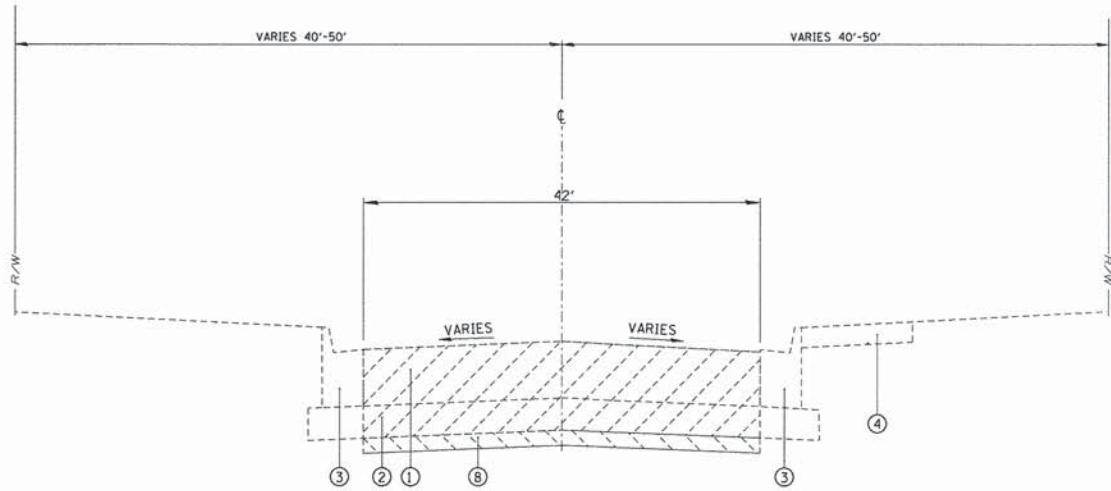
DISTRICT ONE DETAILS

TC-10	TRAFFIC CONTROL & PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-22	ARTERIAL INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-05	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

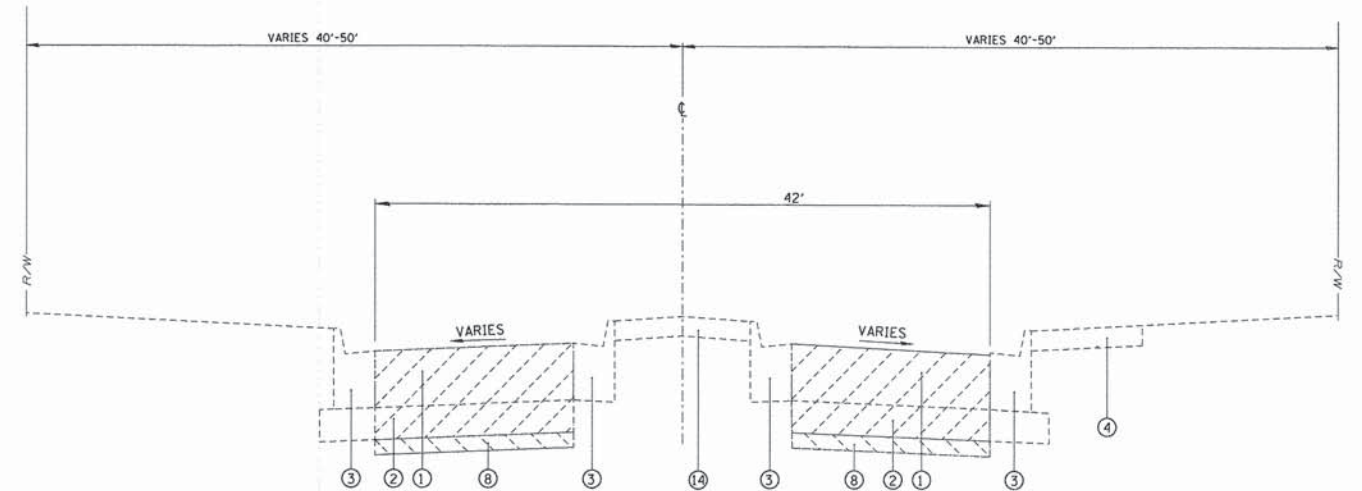
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				ROADWAY 0004	URBAN
20200100	EARTH EXCAVATION	CU YD	1300	1300	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	200	200	
20700220	POROUS GRANULAR EMBANKMENT	CU YD	200	200	
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	600	600	
28000510	INLET FILTERS	EACH	25	25	
30300112	AGGREGATE SUBGRADE IMPROVEMENT, 12"	SQ YD	11700	11700	
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	35000	35,000	
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	4040	4040	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1350	1350	
42400800	DETECTABLE WARNINGS	SQ FT	100	100	
44000100	PAVEMENT REMOVAL	SQ YD	11700	11700	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	450	450	
44000600	SIDEWALK REMOVAL	SQ FT	700	700	
67100100	MOBILIZATION	L SUM	1	1	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	3	3	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4500	4500	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1500	1500	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	40	40	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3000	3000	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	200	200	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	240	240	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	15	15	
* ~ 88600600	DETECTOR LOOP REPLACEMENT	FOOT	100	100	
~ X0326862	STRUCTURES TO BE ADJUSTED	EACH	50	50	
~ X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	2	2	
~ X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	10	10	
~ X4023000	TEMPORARY ACCESS (ROAD)	EACH	6	6	
X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	700	700	
X8064200	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)	FOOT	450	450	
~ X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
~ Z0013797	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	450	450	
~ Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
~ Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	248.5	248.5	

* INDICATES SPECIALTY ITEM
~ INDICATES SPECIAL PROVISION

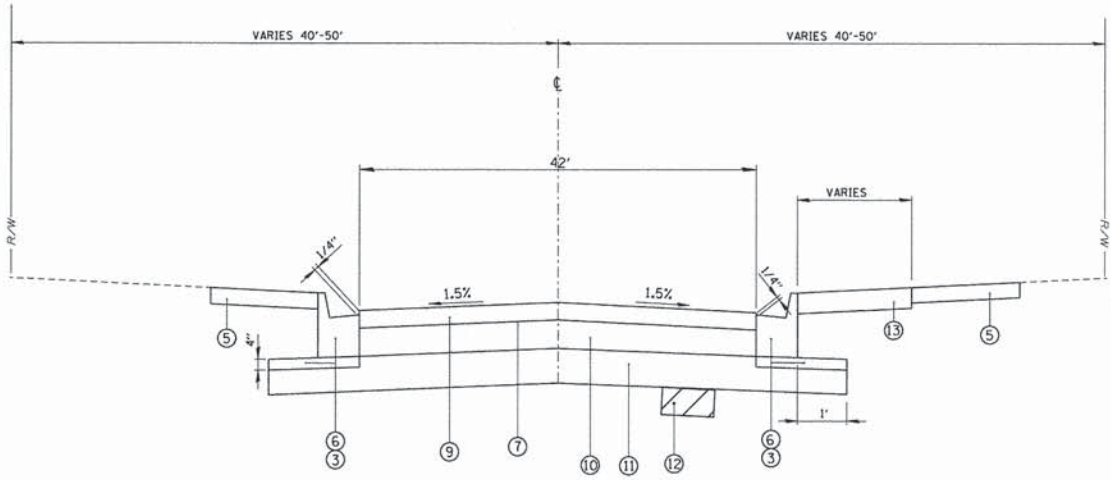
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PLOT DATE = 1/15/2015	DATE - 1/5/15	REVISED -	CONTRACT NO. 61B10							
SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA. ILLINOIS FED. AID PROJECT										



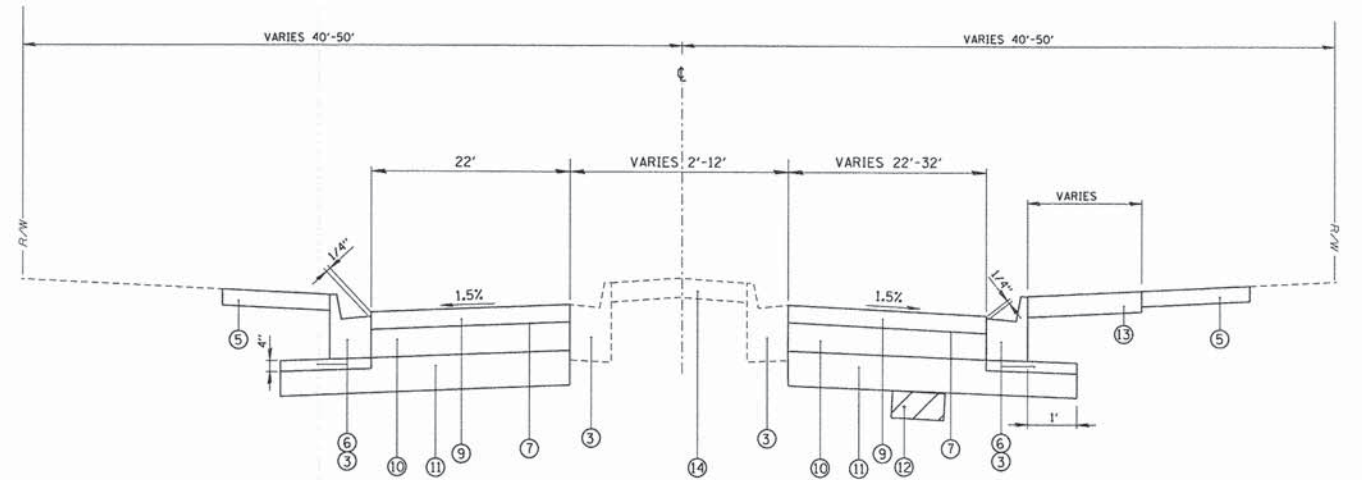
EXISTING TYPICAL SECTION
STATION 11+85.00 TO STATION 31+96.00, THEODORE STREET



EXISTING TYPICAL SECTION
STATION 31+96.00 TO STATION 37+00.00, THEODORE STREET



PROPOSED TYPICAL SECTION
STATION 11+85.00 TO STATION 31+96.00, THEODORE STREET



PROPOSED TYPICAL SECTION
STATION 31+96.00 TO STATION 37+00.00, THEODORE STREET

LEGEND

- | | |
|--|---|
| <ul style="list-style-type: none"> ① EXISTING HOT-MIX ASPHALT PAVEMENT - 12" (TO BE REMOVED) ② EXISTING AGGREGATE SUBBASE - 4" (TO BE REMOVED, INCLUDED IN THE COST OF PAVEMENT REMOVAL) ③ EXISTING CURB AND GUTTER ④ EXISTING PCC SIDEWALK ⑤ SODDING, SALT TOLERANT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND SIDEWALK REMOVAL AND REPLACEMENT) ⑥ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER), INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B. | <ul style="list-style-type: none"> ⑦ PROPOSED BITUMINOUS MATERIAL (PRIME COAT) ⑧ EARTH EXCAVATION ⑨ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 2" ⑩ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 6" ⑪ AGGREGATE SUBGRADE IMPROVEMENT, 12" ⑫ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL / POROUS GRANULAR EMBANKMENT (AS DIRECTED BY THE ENGINEER) ⑬ PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL AS DIRECTED BY THE ENGINEER ⑭ EXISTING PCC MEDIAN |
|--|---|

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
ITEM	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 2"	4% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 6"	4% @ 50 GYR.

NOTE:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
2. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
3. ANY EXCAVATION NECESSARY TO OBTAIN THE NECESSARY DEPTH FOR THE PROPOSED PAVEMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PAVEMENT REMOVAL.

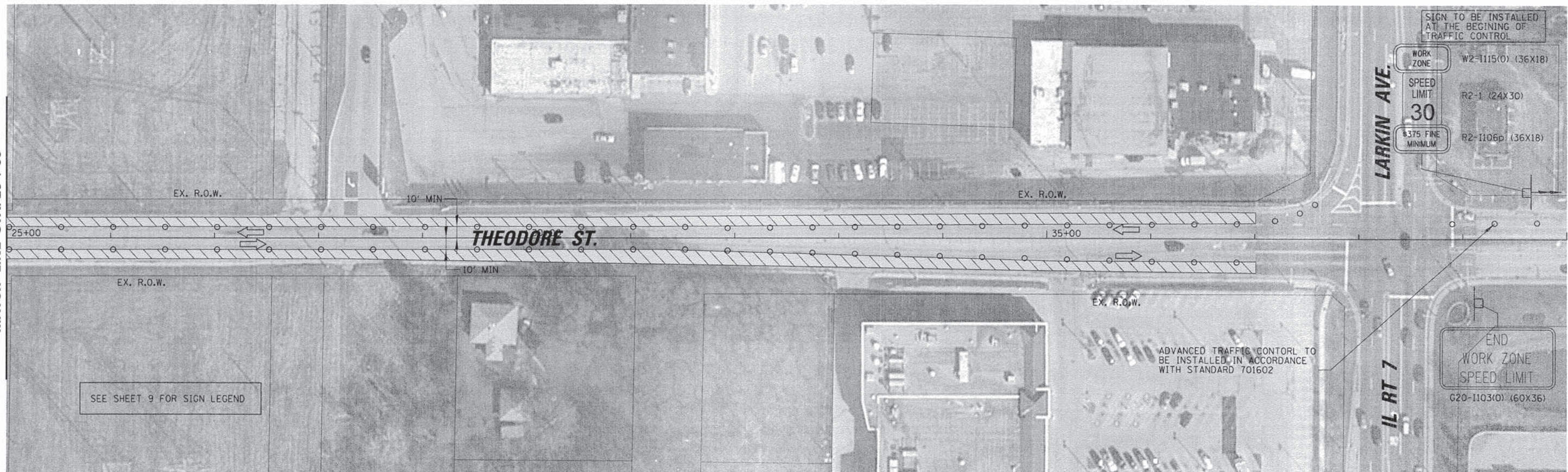
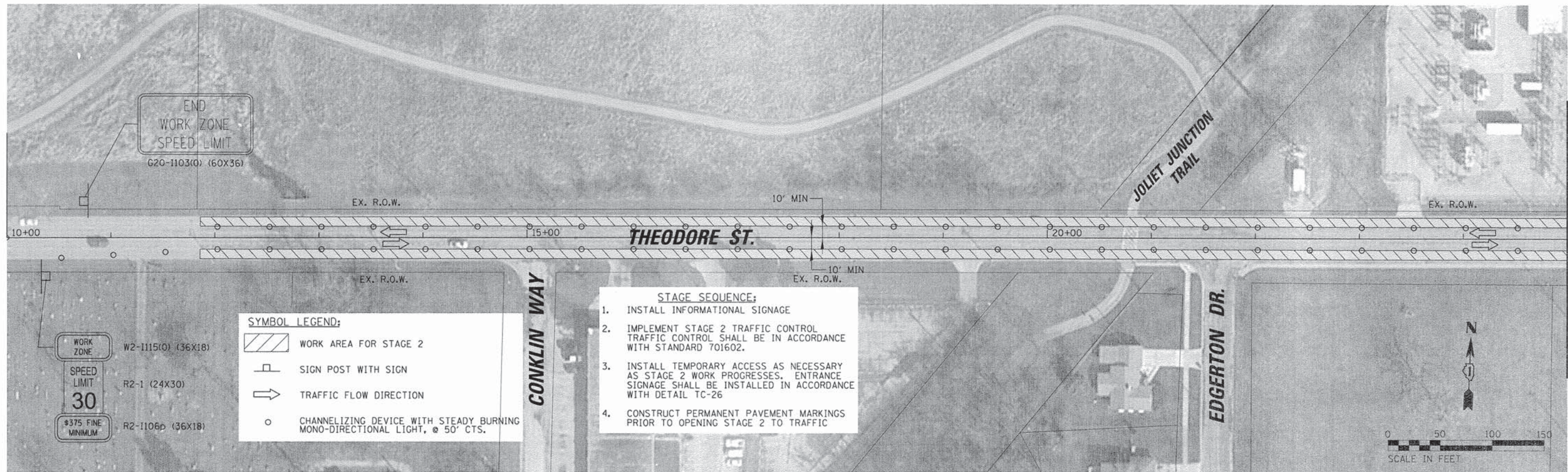
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	PLOT DATE = 1/15/2015	DATE - 1/5/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
THEODORE STREET**

SCALE: N7S SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 0295	SECTION 14-00046-00-FP	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 4
CONTRACT NO. 61B10				
[ILLINOIS] FED. AID PROJECT				



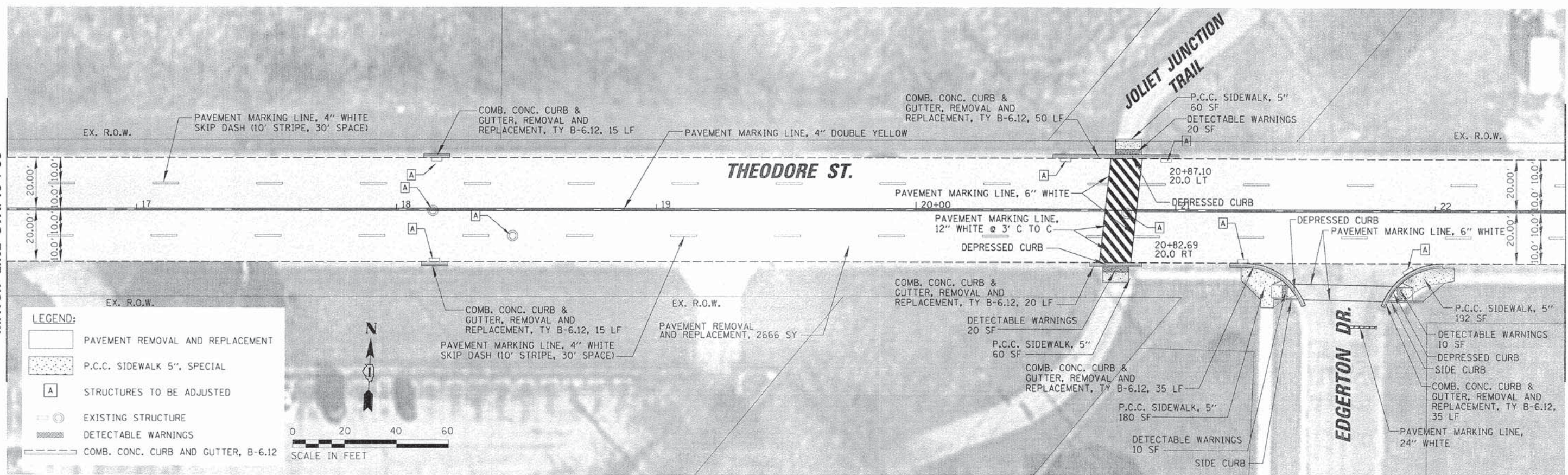
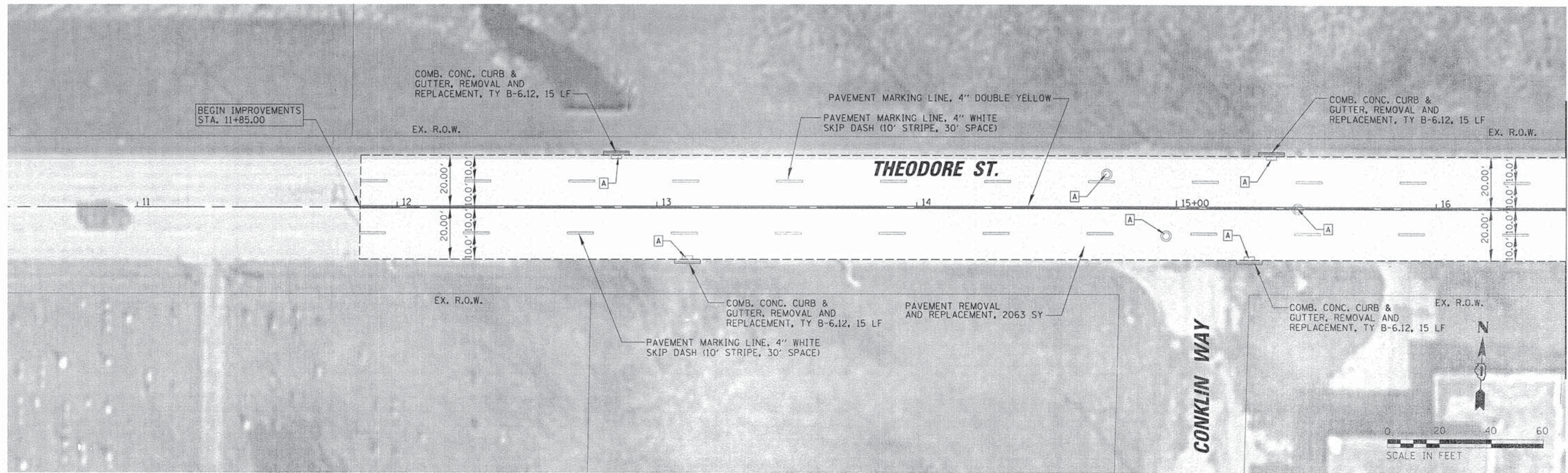
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	PLOT DATE = 1/15/2015	DATE - 1/5/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**THEODORE STREET - STAGE 2
STAGES OF CONSTRUCTION & TRAFFIC CONTROL PLAN**

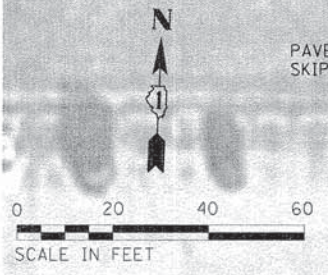
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61B10				
[ILLINOIS] FED. AID PROJECT				

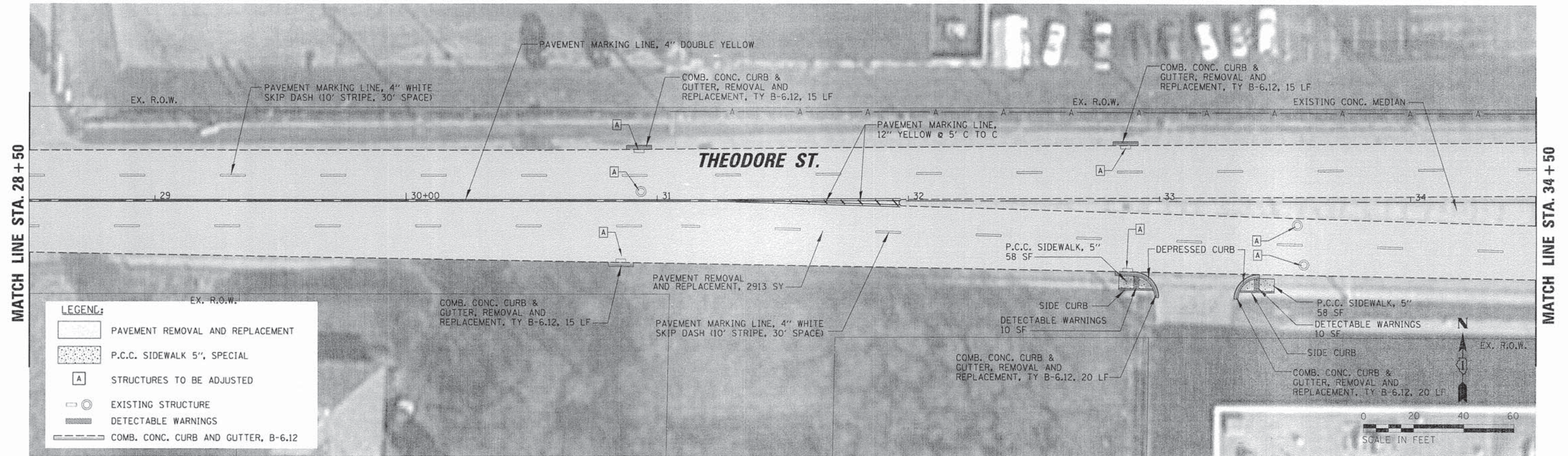
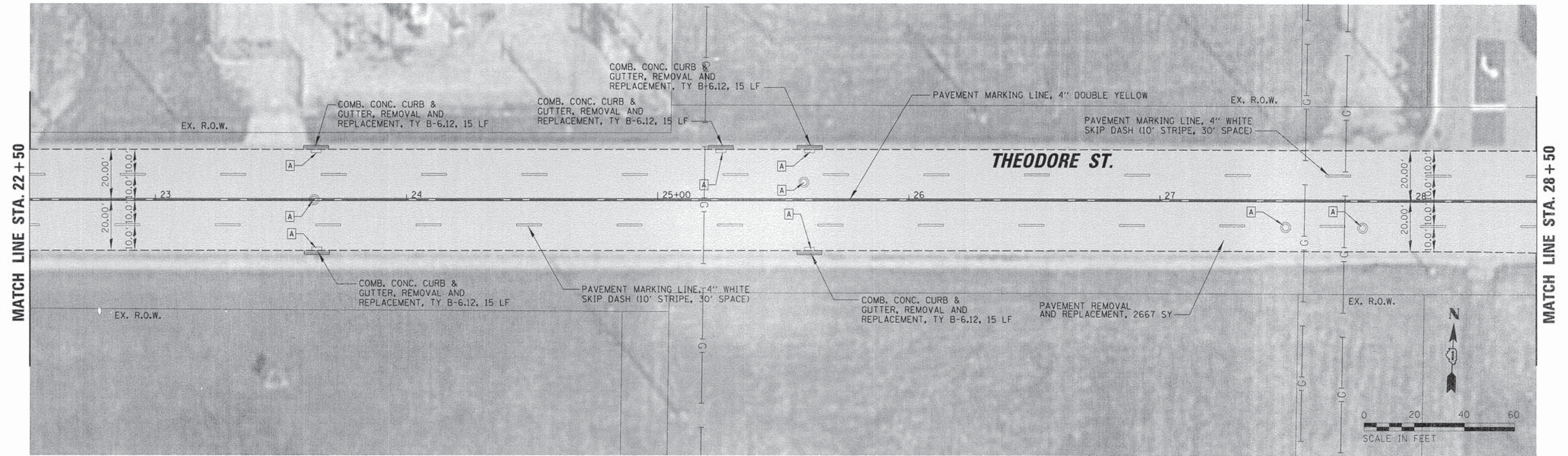


LEGEND:

- PAVEMENT REMOVAL AND REPLACEMENT
- P.C.C. SIDEWALK 5", SPECIAL
- A STRUCTURES TO BE ADJUSTED
- EXISTING STRUCTURE
- DETECTABLE WARNINGS
- COMB. CONC. CURB AND GUTTER, B-6.12



PROJECT NAME 14-00046-00-FP	USER NAME CUM	DESIGNED CUM	REVISIONS REVISIONS REVISIONS REVISIONS	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	THEODORE STREET PROPOSED PLAN	F.A.U. RTE. 0295	SECTION 14-00046-00-FP	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 7	CONTRACT NO. 61B10
DATE 1/5/15		CHECKED DV		SCALE: 1" = 20' SHEET NO. 1 OF 3 SHEETS STA. _____ TO STA. _____		ILLINOIS FED. AID PROJECT					



LEGEND:

	PAVEMENT REMOVAL AND REPLACEMENT
	P.C.C. SIDEWALK 5", SPECIAL
	STRUCTURES TO BE ADJUSTED
	EXISTING STRUCTURE
	DETECTABLE WARNINGS
	COMB. CONC. CURB AND GUTTER, B-6.12

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		CHECKED - DV	REVISED -
		DATE - 1/5/15	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**THEODORE STREET
PROPOSED PLAN**

SCALE: 20' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

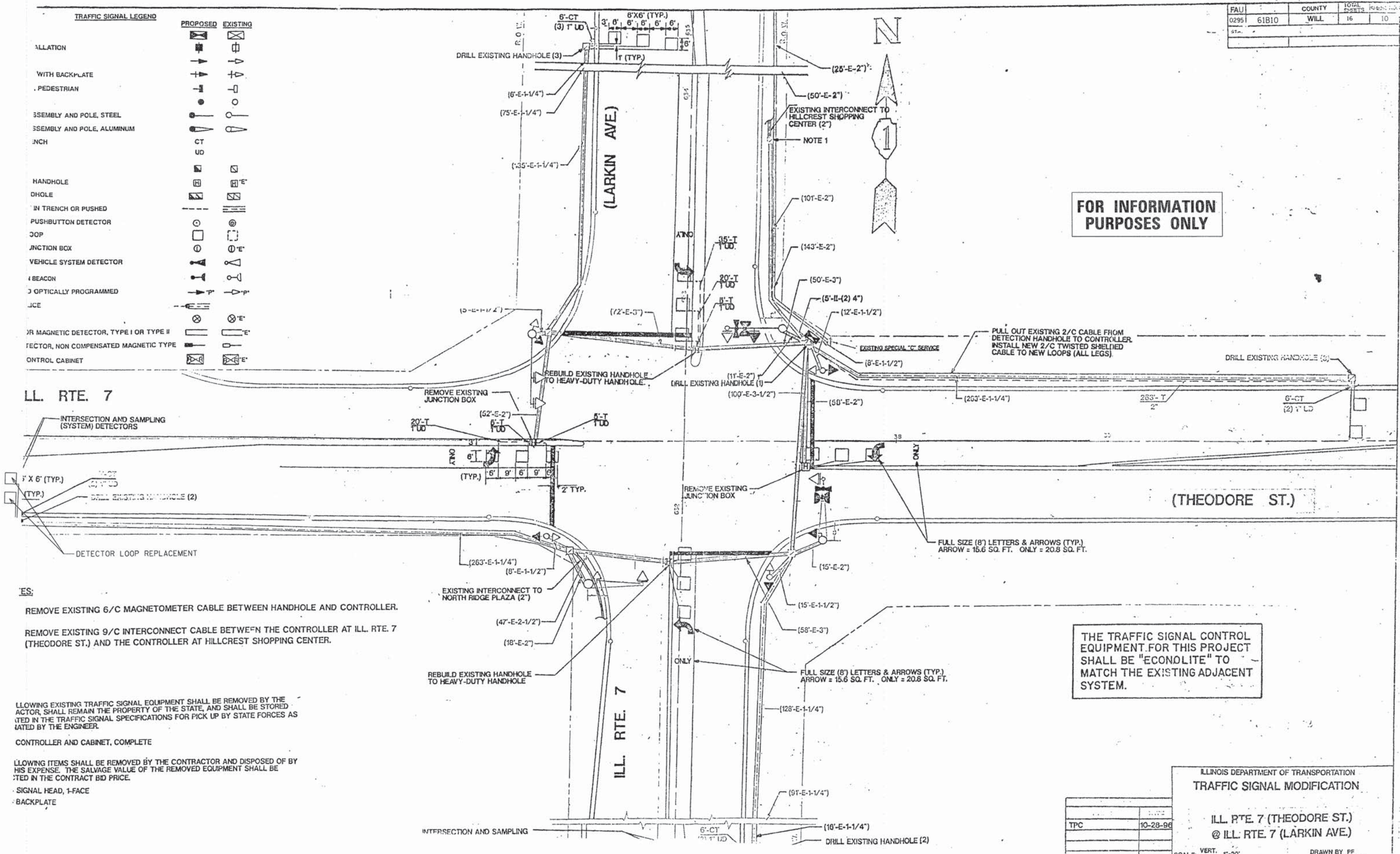
F.A.U. RTE. 0295	SECTION 14-00046-00-PP	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 8
CONTRACT NO. 61B10				
ILLINOIS FED. AID PROJECT				

FAU	COUNTY	TOTAL SHEETS	DATE
02951	61B10	16	10
PROJECT		SHEET NO.	
ILL. RTE. 7		10	

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
INSTALLATION		
WITH BACKPLATE		
PEDESTRIAN		
ASSEMBLY AND POLE, STEEL		
ASSEMBLY AND POLE, ALUMINUM		
ANCHOR		
HANDHOLE		
DRILL HOLE		
IN TRENCH OR PUSHED		
PUSHBUTTON DETECTOR		
LOOP		
JUNCTION BOX		
VEHICLE SYSTEM DETECTOR		
BEACON		
OPTICALLY PROGRAMMED		
INDICATOR		
MAGNETIC DETECTOR, TYPE I OR TYPE II		
INDICATOR, NON COMPENSATED MAGNETIC TYPE		
CONTROL CABINET		

ILL. RTE. 7



FOR INFORMATION PURPOSES ONLY

INTERSECTION AND SAMPLING (SYSTEM) DETECTORS

6" X 6" (TYP.)
 (TYP.)
 DRILL EXISTING HANDHOLE (2)

DETECTOR LOOP REPLACEMENT

- NOTES:**
- REMOVE EXISTING 6/C MAGNETOMETER CABLE BETWEEN HANDHOLE AND CONTROLLER.
 - REMOVE EXISTING 9/C INTERCONNECT CABLE BETWEEN THE CONTROLLER AT ILL. RTE. 7 (THEODORE ST.) AND THE CONTROLLER AT HILLCREST SHOPPING CENTER.

ALLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR. ALL REMOVED EQUIPMENT SHALL REMAIN THE PROPERTY OF THE STATE, AND SHALL BE STORED AND MAINTAINED IN THE TRAFFIC SIGNAL SPECIFICATIONS FOR PICK UP BY STATE FORCES AS DETERMINED BY THE ENGINEER.

CONTROLLER AND CABINET, COMPLETE

ALLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF BY HIS EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE INCLUDED IN THE CONTRACT BID PRICE.

- SIGNAL HEAD, 1-FACE
- BACKPLATE

FULL SIZE (8") LETTERS & ARROWS (TYP.)
 ARROW = 15.6 SQ. FT. ONLY = 20.8 SQ. FT.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC SIGNAL MODIFICATION**

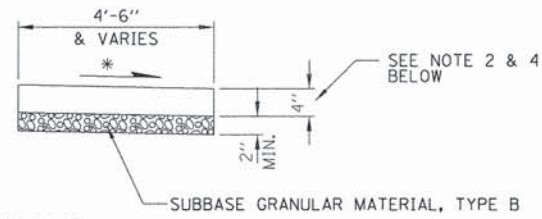
**ILL. RTE. 7 (THEODORE ST.)
 @ ILL. RTE. 7 (LARKIN AVE.)**

DATE	10-28-96
TPC	
SCALE	VERT. 1"=20'

DRAWN BY PF

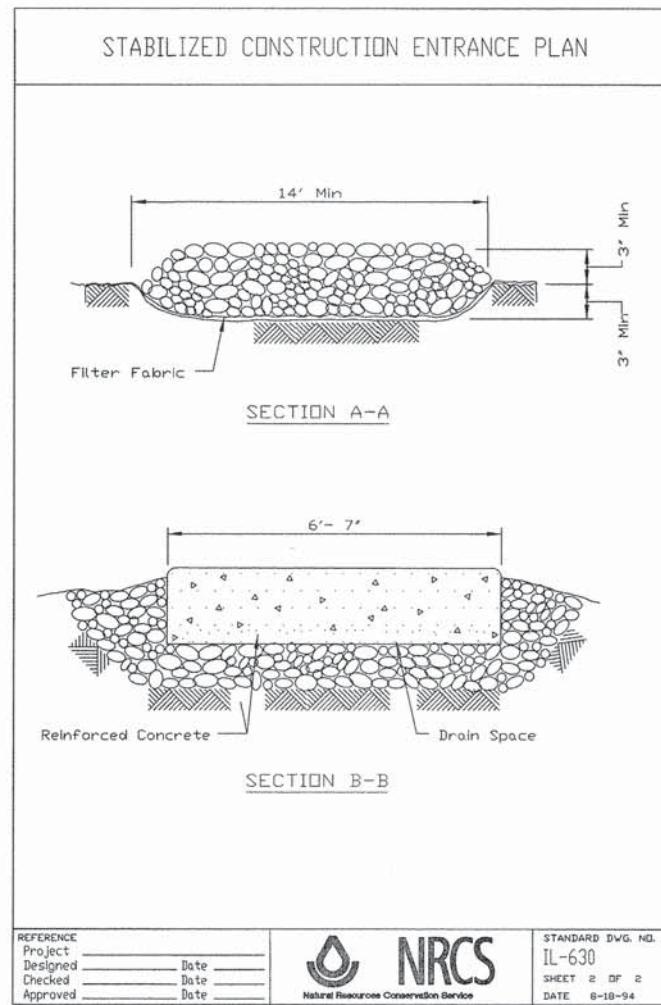
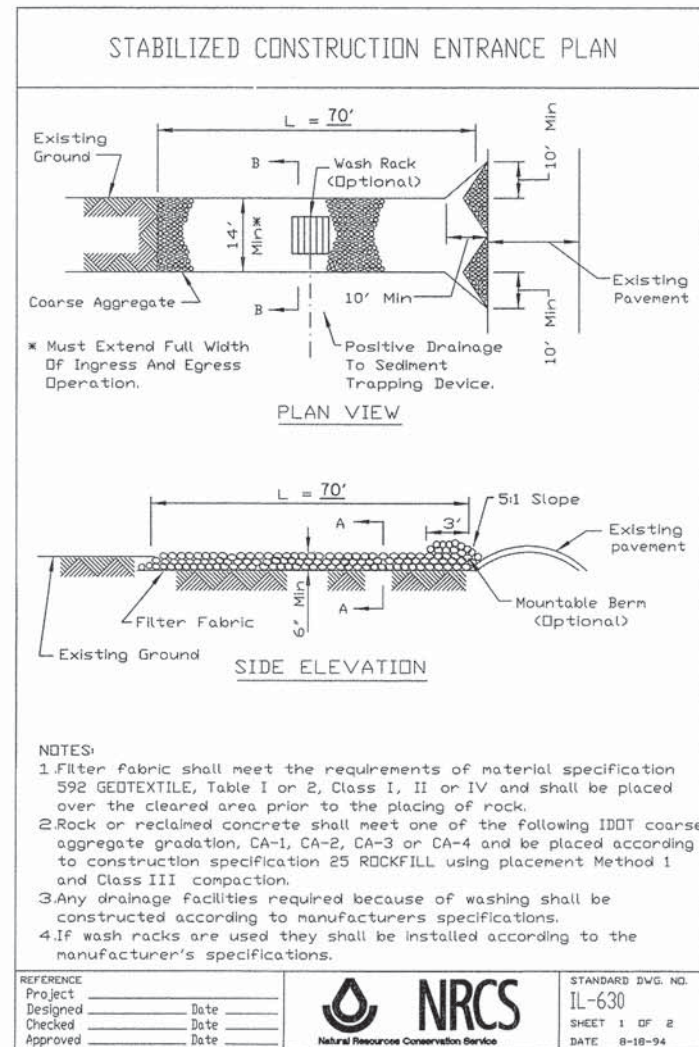
NOTES

- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

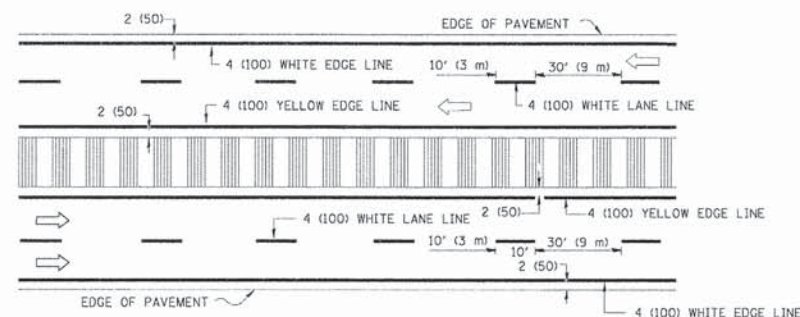
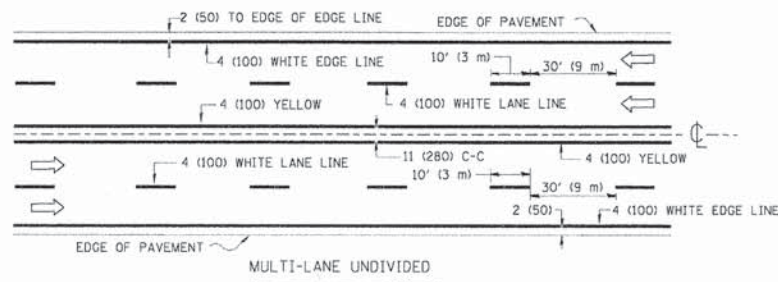
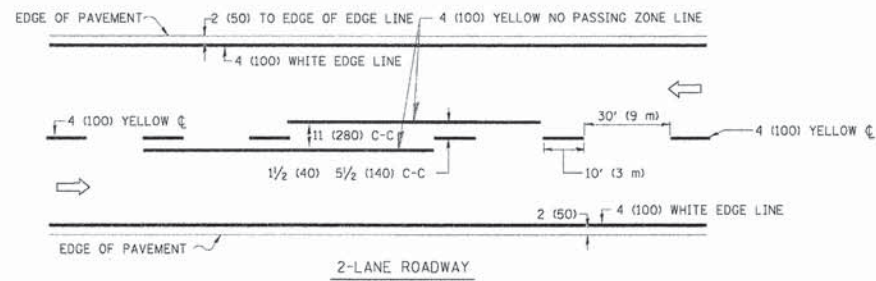


- CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS
- ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCLUDED IN THE COST OF P.C.C. SIDEWALK 5 INCH.
 - WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFIELD WITHIN 24 HOURS.
 - SODDING AND TOPSOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF SIDEWALK REMOVAL.
 - PUBLIC SIDEWALK SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS.

P.C.C. SIDEWALK DETAIL

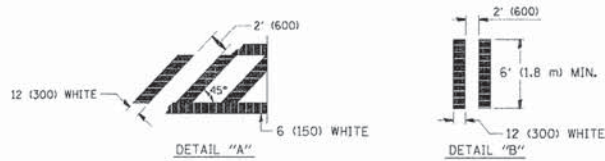
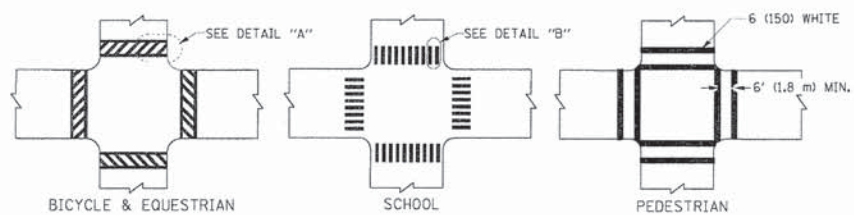


FILE NAME =	USER NAME = CMCCOLLON	DESIGNED = CJM	REVISED =	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	THEODORE STREET CONSTRUCTION DETAILS	F.A.U. RTE. 0295	SECTION 14-00046-00-FP	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 11
	PLOT SCALE = 5'	CHECKED = DV	REVISED =	SCALE: 5'	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 61B10		
	PLOT DATE = 1/15/2015	DATE = 1/5/15	REVISED =	ILLINOIS FED. AID PROJECT						

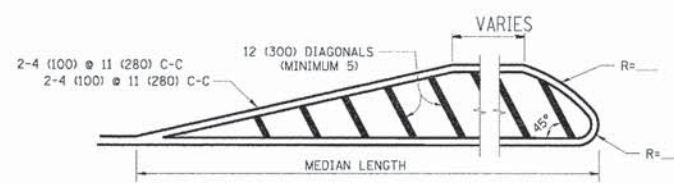
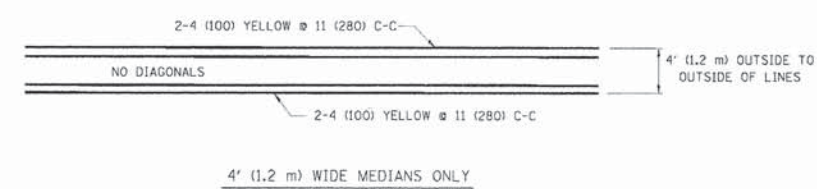


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

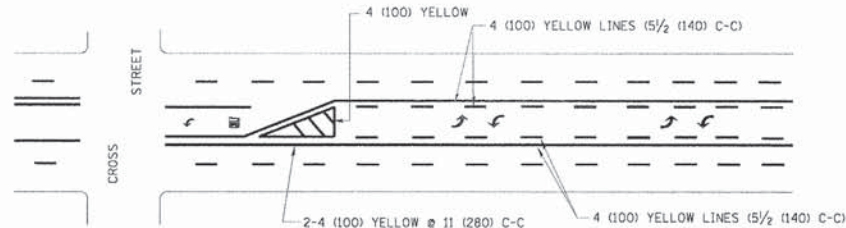


TYPICAL CROSSWALK MARKING

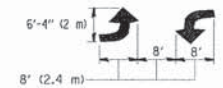


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

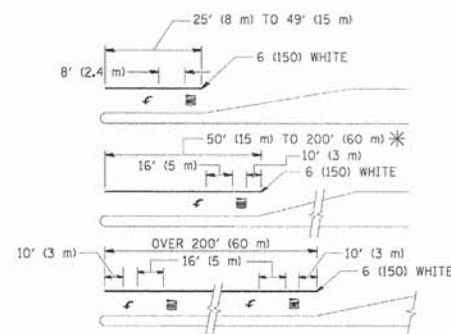


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

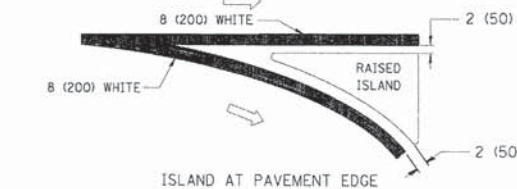
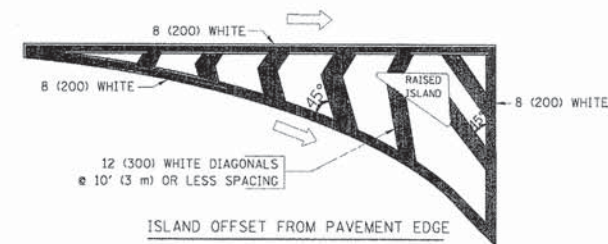


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

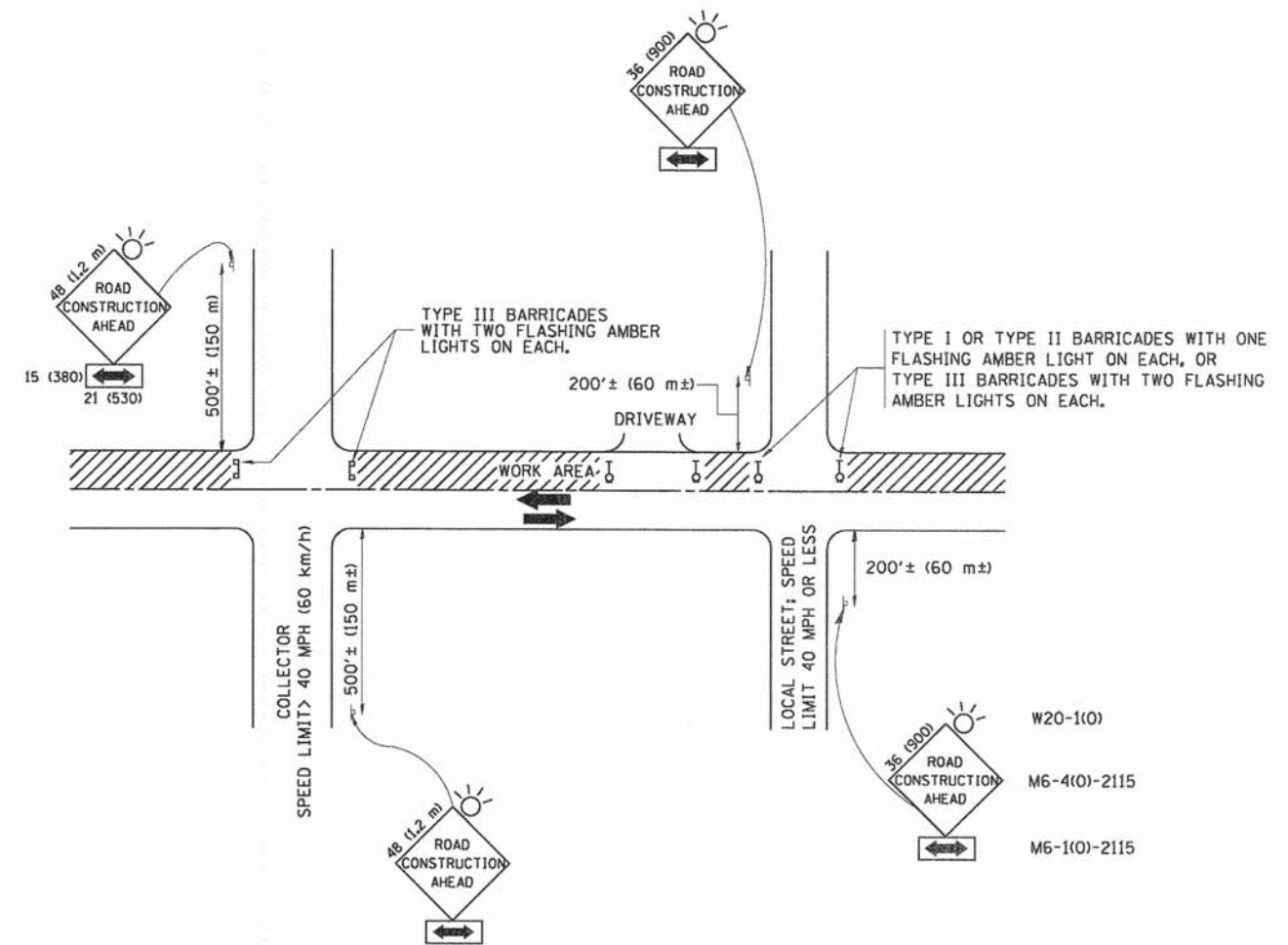


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2' (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2' (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4 m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

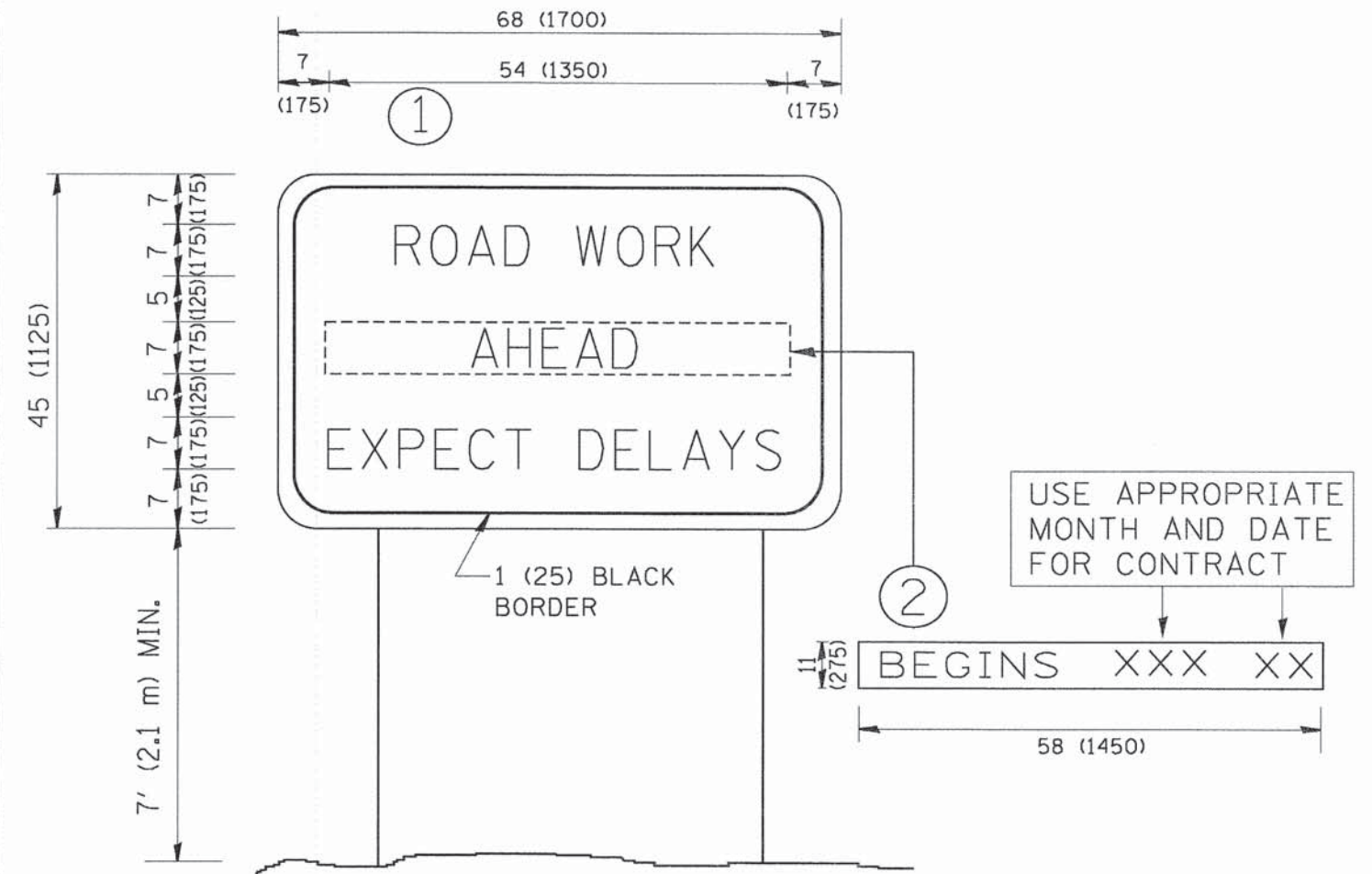
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\distatd\22x34\to10.dgn	USER NAME = gaglisenobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 "/ IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 2 OF 5 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0295	14-00046-00-FP	WILL	16	13
TC-10		CONTRACT NO. 61B10		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

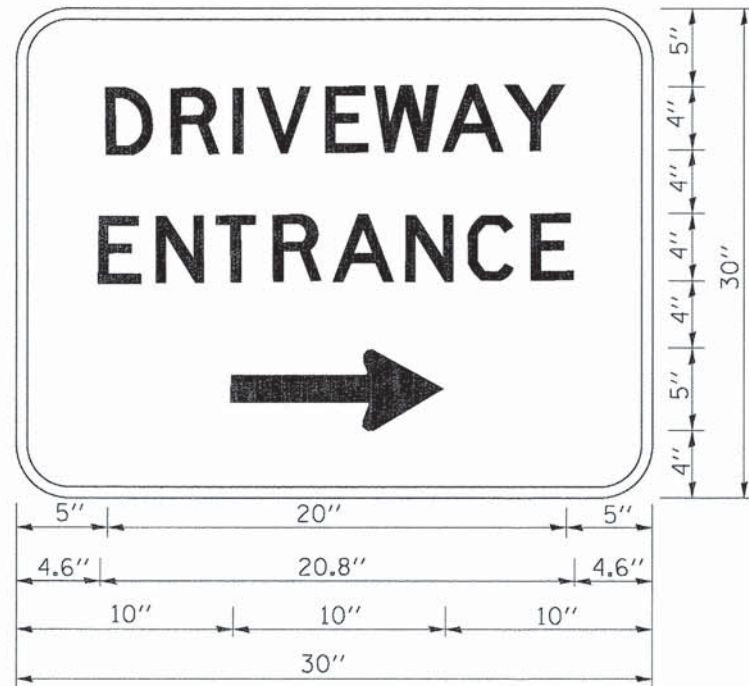


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\te22.dgn	USER NAME = goglionobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 3 OF 5 SHEETS	STA.	TO STA.	0295	14-00046-00-FP	WILL	16 14
		PLOT SCALE = 50.000 X / IN.	REVISED - T. RAMMACHER 02-02-99		TC-22		CONTRACT NO. 61B10		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
		PLOT DATE = 1/4/2008	REVISED - C. JUICIUS 01-31-07									



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gaglienobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
at\pwork\pwork\gaglienobt\d0108315\to	Edgn	DRAWN -	REVISED -
	PLOT SCALE = 50.000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

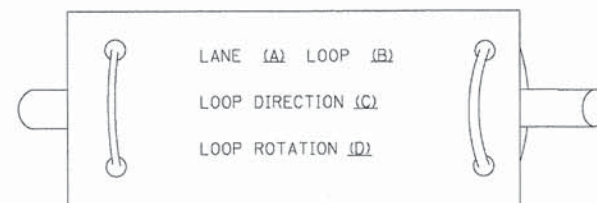
SCALE: NONE SHEET NO. 4 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0295	14-00046-00-FP	WILL	16	15
TC-26			CONTRACT NO. 61B10	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

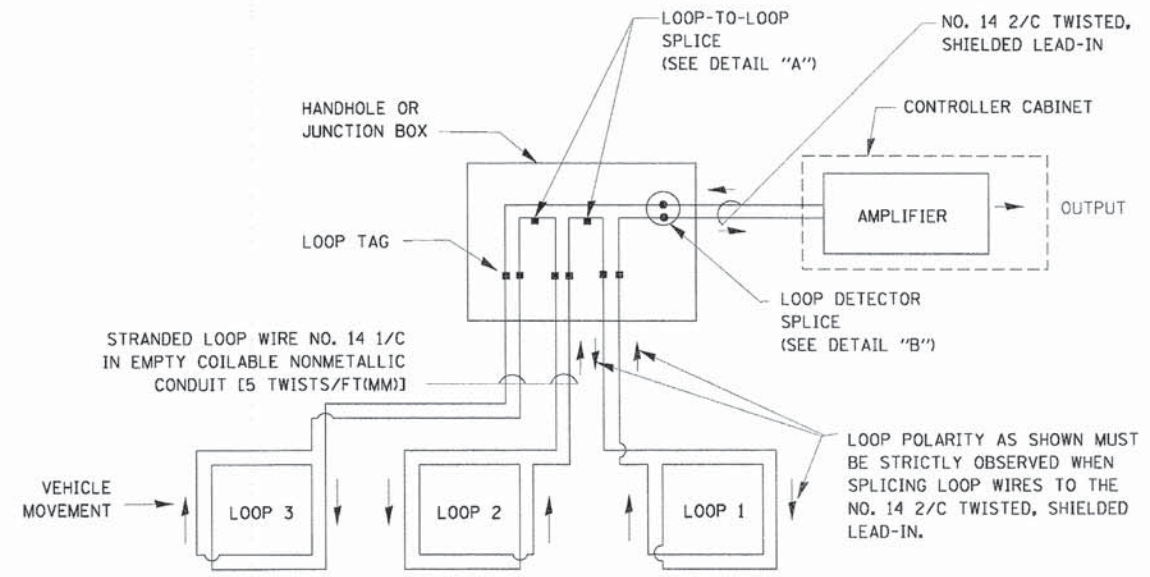
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

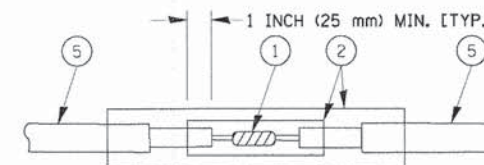


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

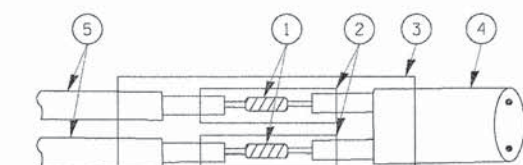


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

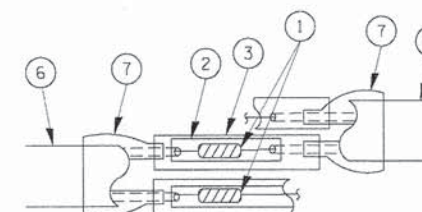


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

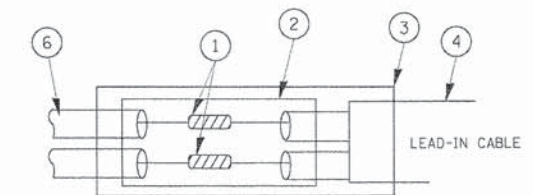


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bboard1	DESIGNED - DAD	REVISED -
os\p\work\p\DOT\BAUERDL\0128315\1409.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 5 OF 5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0295	14-00046-00-FP	WILL	16	16
TS-05		CONTRACT NO.	61B10	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				