

03-06-2020 LETTING ITEM 011

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	1
		ILLINOIS	CONTRACT NO. 61G37	

FOR INDEX OF SHEETS AND STANDARDS, SEE SHEET NO. 2

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 1551 (83rd STREET)
IL ROUTE 53 TO JANES AVENUE
RESURFACING, CURB AND GUTTER, PAVEMENT MARKING
SECTION NO.: 19-00081-00-RS
PROJECT NO.: 5M7P(264)
VILLAGE OF WOODRIDGE
DUPAGE COUNTY
C-91-360-19

TRAFFIC DATA

83rd STREET:

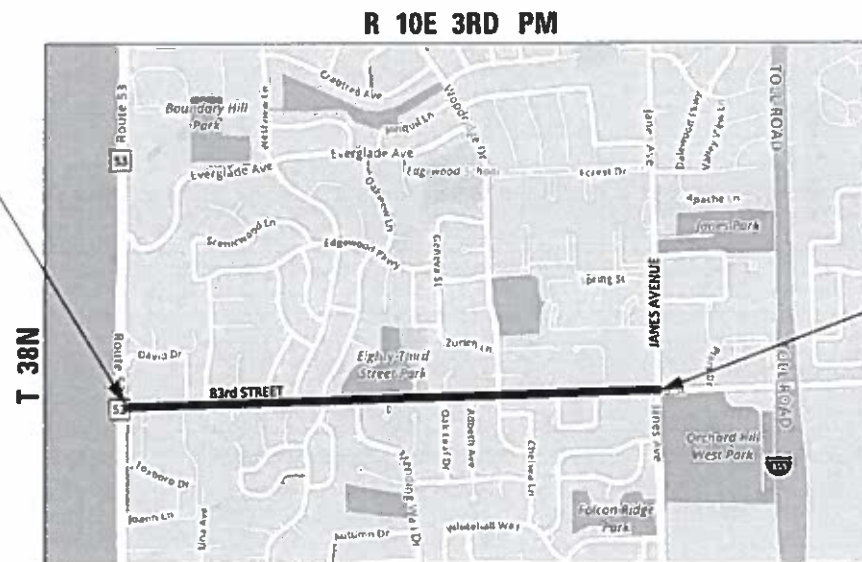
ADT (2016) = 9700

POSTED SPEED LIMIT = 30 MPH

HIGHWAY CLASSIFICATION: MAJOR COLLECTOR

THIS PROJECT IS LOCATED IN THE
VILLAGE OF WOODRIDGE

PROJECT BEGINS
STA 6+19



LOCATION MAP
(NOT TO SCALE)

PROJECT ENDS
STA 67+17.4

LISLE TOWNSHIP

GROSS LENGTH = 6,098.5 FT. = 1.155 MILE
NET LENGTH = 6,098.5 FT. = 1.155 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

JANICE K. PANG
062-060897
LICENSED
PROFESSIONAL
ENGINEER
OF
STATE OF ILLINOIS
DATE SIGN: 12/19/2019
EXP DATE: 11/30/2021



LOCATION OF SECTION INDICATED THIS: — ■ —

CHASTAIN & ASSOCIATES LLC
CONSULTING ENGINEERS
SERVICE | SOLUTIONS | COMMITMENT™

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED Dec 18 2019
C.P.R.
VILLAGE OF WOODRIDGE, DIRECTOR OF PUBLIC WORKS

PASSED DEC 19 2019
C.H. Sale
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR
BID BASED ON
LIMITED REVIEW December 20 2019
Anthony J. Ramsey / OES
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

CONTRACT NO. 61G37

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, IL

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-6	TYPICAL SECTIONS
7-12	ROADWAY AND PAVEMENT MARKING PLAN
13-18	ADA RAMP DETAILS
19	BD-08: DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
20	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
21	BD-32: BUTT JOINT AND HMA TAPER DETAILS
22	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTION AND DRIVEWAYS
23	TC-13: DISTRICT ONE TYPICAL PAVEMENT MARKINGS
24	TC-14: TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
25	TC-16: SHORT TERM PAVEMENT MARKING LETTERS & SYMBOLS
26	TC-22: ARTERIAL ROAD INFORMATION SIGN
27	TS-07: DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF ROAD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701502-09	URBAN LANE CLOSURE, 2L, 2W BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED). THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNER OF ALL EXISTING UTILITIES FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AND VILLAGE.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE ENGINEER. THE CONTRACTOR SHALL CONTACT THE VILLAGE OF WOODRIDGE AT (630) 719-4753 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON PUBLIC PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER.
- THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY OF ANY STREET AND/OR PARK PROPERTY SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR SHALL VERIFY THAT ALL CRACKS, JOINTS, AND FLANGEWAYS ARE CLEAN AND DRY PRIOR TO PLACEMENT OF MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS.
- PRIOR TO APPLYING HOT-MIX ASPHALT TACK COAT, THE BASE SURFACE INCLUDING GUTTERS SHALL BE CLEANED OF LOOSE MATERIALS. ALL CRACK FILL MATERIAL SHALL BE REMOVED IN ITS ENTIRETY ALONG THE CURB LINE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKINGS LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- PAVEMENT MARKING TAPE, TYPE 4, SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL COORDINATE ALL WORK IMPACTING JOHN L. SIPLEY ELEMENTARY SCHOOL WITH REPRESENTATIVES OF WOODRIDGE SCHOOL DISTRICT 68 AT (630) 985-7925.
- THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES, AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- THE CONTRACTOR SHALL VERIFY THE RIM & INVERTS OF ALL EXISTING AND PROPOSED STORM SEWER STRUCTURES PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
- ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS, AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.
- THE MINIMUM THICKNESS OF THE PROPOSED GUTTER FLAG SHALL BE 10 INCHES UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAKE FULL DEPTH SAW CUTS AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL COMBINATION CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, AND DRIVEWAYS OR AS DIRECTED BY THE ENGINEER.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER, DEBRIS AND SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED BY THE CONTRACTOR OFF-SITE.
- CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE ANY TREES WITHIN THE PROJECT AREA. SHOULD ANY TREES BE DAMAGED OR DISTURBED DUE TO CONSTRUCTION ACTIVITIES, CONTRACTOR SHALL REPLACE THEM IN KIND.
- UNLESS OTHERWISE APPROVED BY THE ENGINEER, CONTRACTOR SHALL BE REQUIRED TO KEEP ALL DRIVEWAY ENTRANCES OPEN TO TRAFFIC. ALL CONCRETE WORK AT ENTRANCES SHALL BE CONSTRUCTED 1/2 AT A TIME TO ALLOW ACCESS DURING THE CONCRETE CURING PERIOD.
- EXISTING TREES IN PARKWAYS OUTSIDE OF CONSTRUCTION LIMITS SHALL REMAIN DURING THE CONSTRUCTION. ANY DAMAGE TO EXISTING TREES BY THE CONTRACTOR SHALL BE REPLACED IN KIND BY THE CONTRACTOR.

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PLOT DATE = 12/23/2019										

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SUMMARY OF QUANTITIES				STU FUNDS 70% FED 30% LOCAL	100% LOCAL 0043
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN	NON PARTICIPATING
20200100	EARTH EXCAVATION	CU YD	20	20	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1294	1294	
25000210	SEEDING, CLASS 2A	ACRE	0.6	0.6	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	30	30	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	30	30	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	30	30	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	60	60	
28000510	INLET FILTERS	EACH	75	75	
35101400	AGGREGATE BASE COURSE, TYPE B	TON	20	20	
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	40	40	
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	239	239	
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	70	70	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	18073	18073	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	10	10	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	320	320	
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1120	1120	
40604060	HOT -MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50	TON	3040	3040	
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	20	20	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	8039	8039	

* = SPECIALTY ITEMS

SUMMARY OF QUANTITIES				STU FUNDS 70% FED 30% LOCAL	100% LOCAL 0043
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN	NON PARTICIPATING
42400800	DETECTABLE WARNINGS	SQ FT	476	476	
44000100	PAVEMENT REMOVAL	SQ YD	164	164	
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	26775	26775	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	93	93	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	214	214	
44000600	SIDEWALK REMOVAL	SQ FT	7062	7062	
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	4	4	
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	97	97	
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	80	80	
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	201	201	
550A0050	STORM SEWERS, CLASS A, TYPE 1, 12"	FOOT	32		32
60202405	CATCH BASINS, TYPE A, 4' DIAMETER	EACH	6		6
60238800	INLETS, TYPE A	EACH	2	2	
60250200	CATCH BASINS TO BE ADJUSTED	EACH	20	20	
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1	
60255500	MANHOLES TO BE ADJUSTED	EACH	3	3	
60260100	INLETS TO BE ADJUSTED	EACH	1	1	
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1	
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	12	12	

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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**83rd STREET BETWEEN IL 53 & JANES AVENUE
SUMMARY OF QUANTITIES**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE. 1551	SECTION 19-00081-00-RS	COUNTY DU PAGE	TOTAL SHEETS 27	SHEET NO. 3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61G37	

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SUMMARY OF QUANTITIES				STU FUNDS 70% FED 30% LOCAL	100% LOCAL 0043
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN	NON PARTICIPATING
60404950	FRAMES AND GRATES, TYPE 24	EACH	3		3
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	5	
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1	
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	200	200	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10	10	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1	
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1	
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1	
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5	5	
67100100	MOBILIZATION	LSUM	1	1	
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	LSUM	1	1	
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	28	28	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4260	4260	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1420	1420	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	2220	2220	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	38708	38708	

* = SPECIALTY ITEMS

SUMMARY OF QUANTITIES				STU FUNDS 70% FED 30% LOCAL	100% LOCAL 0043
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN	NON PARTICIPATING
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2698	2698	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	3184	3184	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	188	188	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1110	1110	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19354	19354	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1349	1349	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1592	1592	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	94	94	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	36	36	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	120	120	
X0327036	BIKE PATH REMOVAL	SQ YD	239	239	
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	1	1	
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	9	9	
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3030	3015	15
Z0018600	DRAINAGE SRUCTURE TO BE RECONSTRUCTED	EACH	7	7	
Z0018700	DRAINAGE SRUCTURE TO BE REMOVED	EACH	6		6
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	12437	12437	

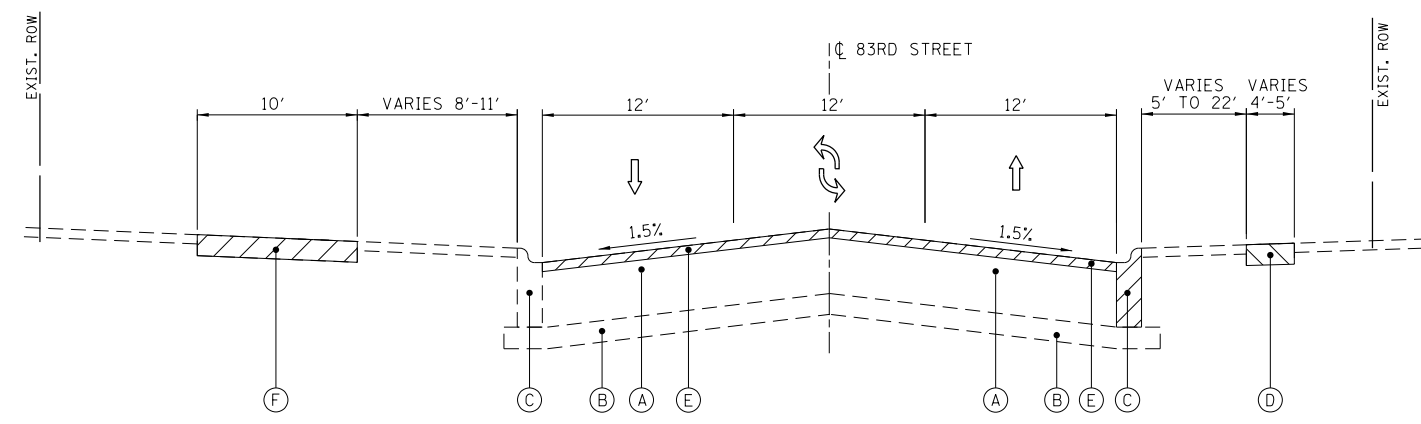
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

83rd STREET BETWEEN IL 53 & JANES AVENUE
 SUMMARY OF QUANTITIES

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

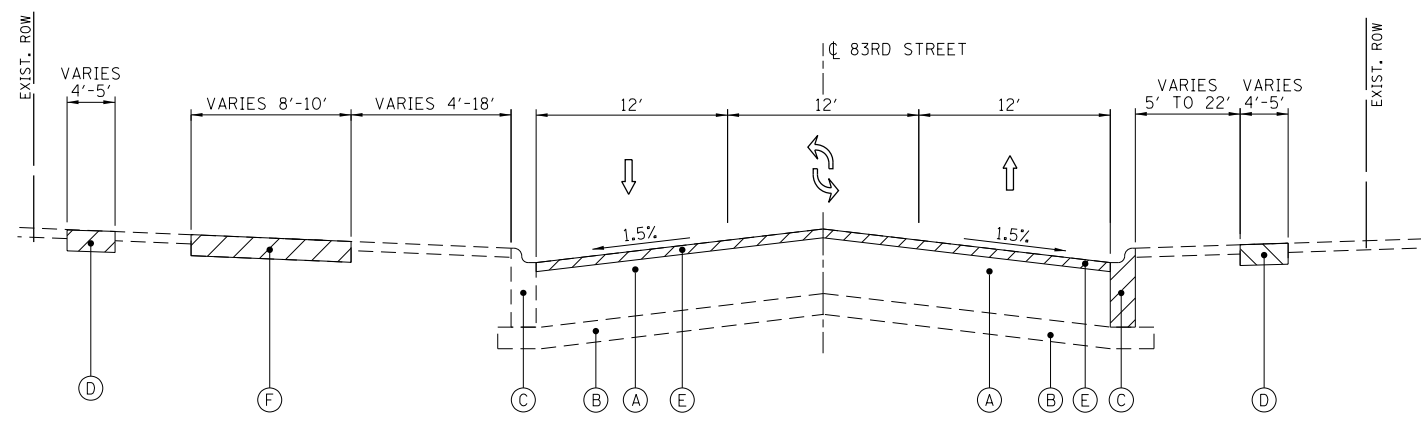
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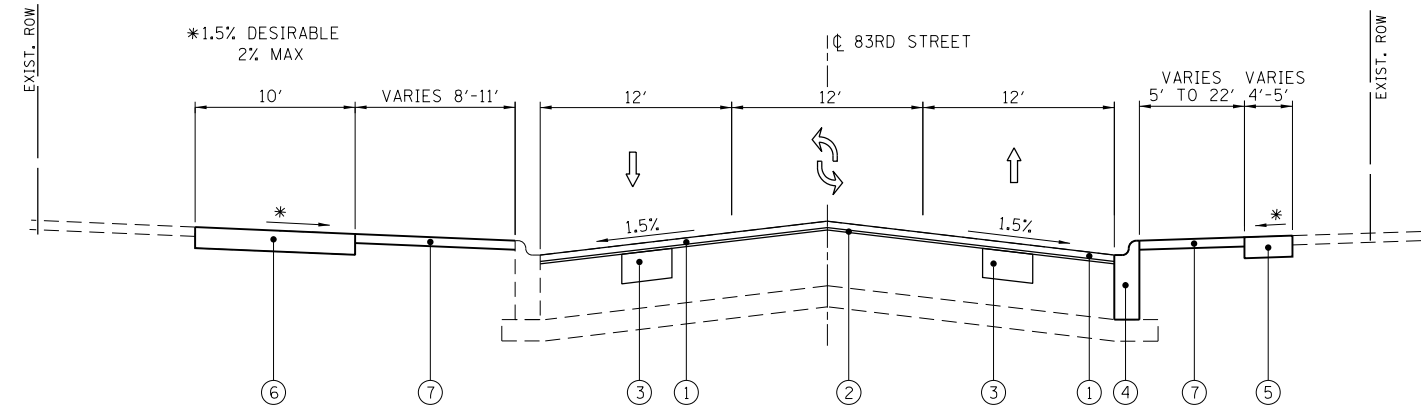
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83RD STREET
STA 6+00 TO STA 40+70



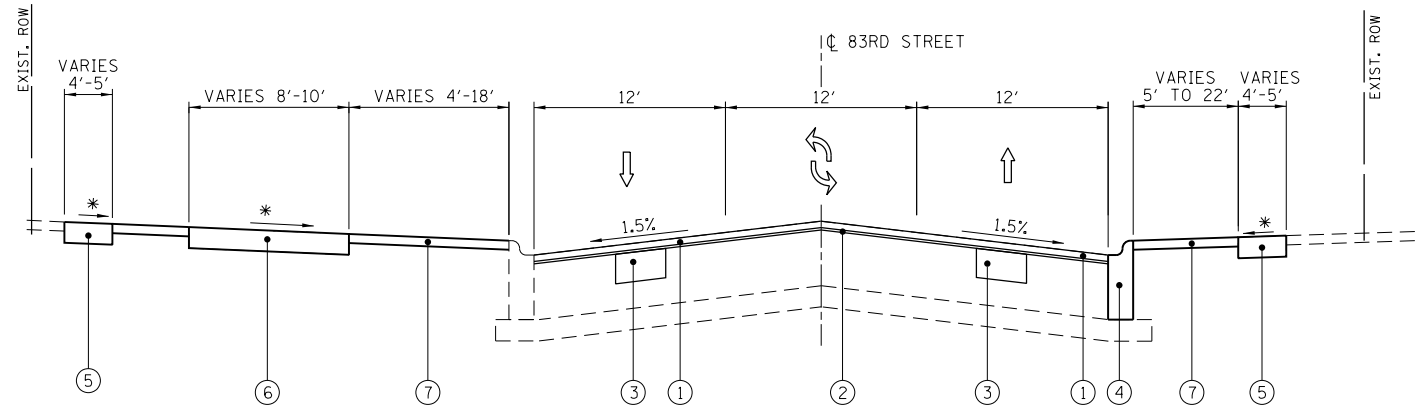
EXISTING TYPICAL SECTION

83RD STREET
STA 40+70 TO STA 62+50



PROPOSED TYPICAL SECTION

83RD STREET
STA 6+00 TO STA 40+70



PROPOSED TYPICAL SECTION

83RD STREET
STA 40+70 TO STA 62+50

EXISTING LEGEND

- (A) HMA PAVEMENT, 11 1/2"
- (B) SUB-BASE GRANULAR MATERIAL, 4"
- (C) COMBINATION CONCRETE CURB & GUTTER
- (D) PCC SIDEWALK
- (E) HMA SURFACE REMOVAL, 2 3/4"
- (F) MULTI-USE PATH, 3"
- [Hatched Box] REMOVAL ITEMS

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 3/4"
- (3) CLASS D PATCHES, 8"
- (4) COMBINATION CONCRETE CURB & GUTTER, B-6.12 6+00 - 14+00
- (5) PCC SIDEWALK 5"
- (6) MULTI-USE PATH
- (7) TOPSOIL, 4"

- NOTES:
- CONTRACTOR SHALL MILL PRIOR TO PATCHING OPERATIONS.
 - LIMITS OF PAVEMENT PATCHING AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN FIELD BY THE ENGINEER.
 - COMBINATION CURB & GUTTER WILL BE REPLACED WITH THE SAME TYPE AS REMOVED.
 - LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HMA BINDER, IL-4.75, N50 LIFT.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @NDES
PAVEMENT RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2" POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 3/4"	4% @ 50 GYR. 3.5% @ 50 GYR.
COMMERCIAL ENTRANCE	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2" HOT-MIX ASPHALT BASE COURSE, 8"	4% @ 50 GYR. 4% @ 50 GYR.
PRIVATE DRIVEWAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 3"	4% @ 50 GYR.
CLASS D PATCHES	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, 8"	4% @ 70 GYR.
MULTI-USE PATH	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 3"	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

FILE NAME = 7204-sht-typreal.dgn
PLOT DRIVER = Default

USER NAME = .USER.	DESIGNED - MH	REVISED -
PEN TABLE = Ido+2014.tbl	DRAWN - MH	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/23/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**83rd STREET BETWEEN IL 53 & JANES AVENUE
TYPICAL SECTIONS**
SCALE: SHEET 1 OF 2 SHEETS STA. 6+00 TO STA. 62+50

F.A.U. RTE. 1551	SECTION 19-00081-00-RS	COUNTY DU PAGE	TOTAL SHEETS 27	SHEET NO. 5
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				

PRINTER =

PEN TABLE = H:\Cadd\cepack\Support\F11\pen\plotdrv\do2014.tbl

PLOT DRIVER =

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 PEN TABLE = Ido+2014.tbl
 PLOT SCALE = 20.0000' / in.
 Default

USER NAME = _USER_
 DRAWN - MH
 CHECKED -
 DATE -

DESIGNED - MH
 DRAWN - MH
 CHECKED -
 DATE -

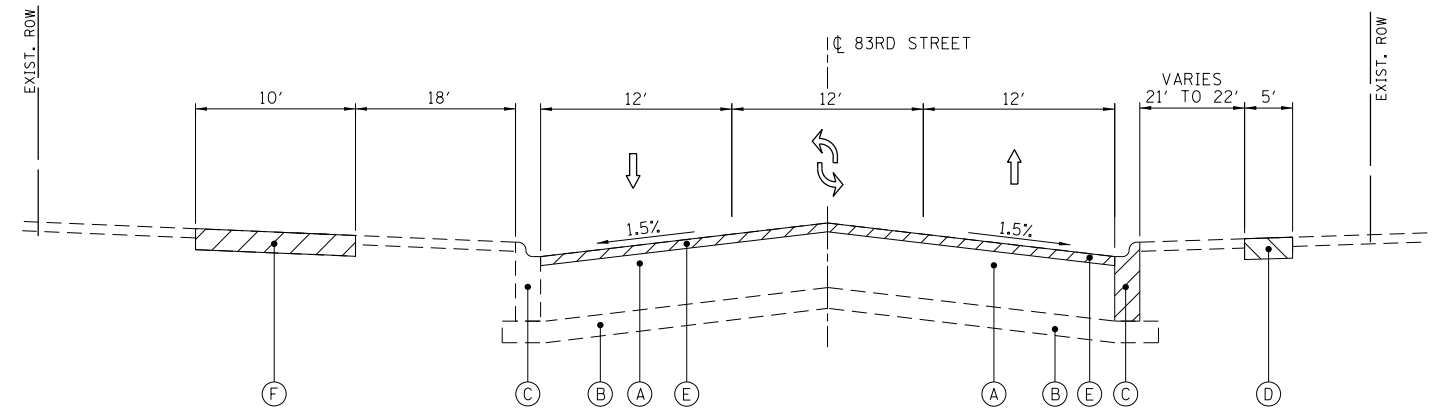
REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**83rd STREET BETWEEN IL 53 & JANES AVENUE
 TYPICAL SECTIONS**

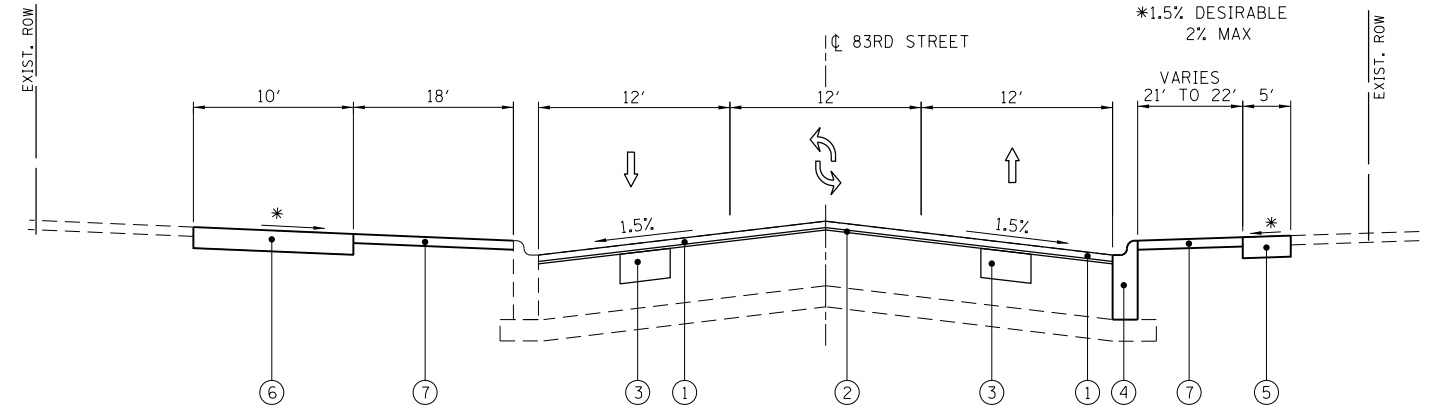
SCALE: SHEET 2 OF 2 SHEETS STA. 62+50 TO STA. 66+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	6
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION

83RD STREET
 STA 62+50 TO STA 66+50



PROPOSED TYPICAL SECTION

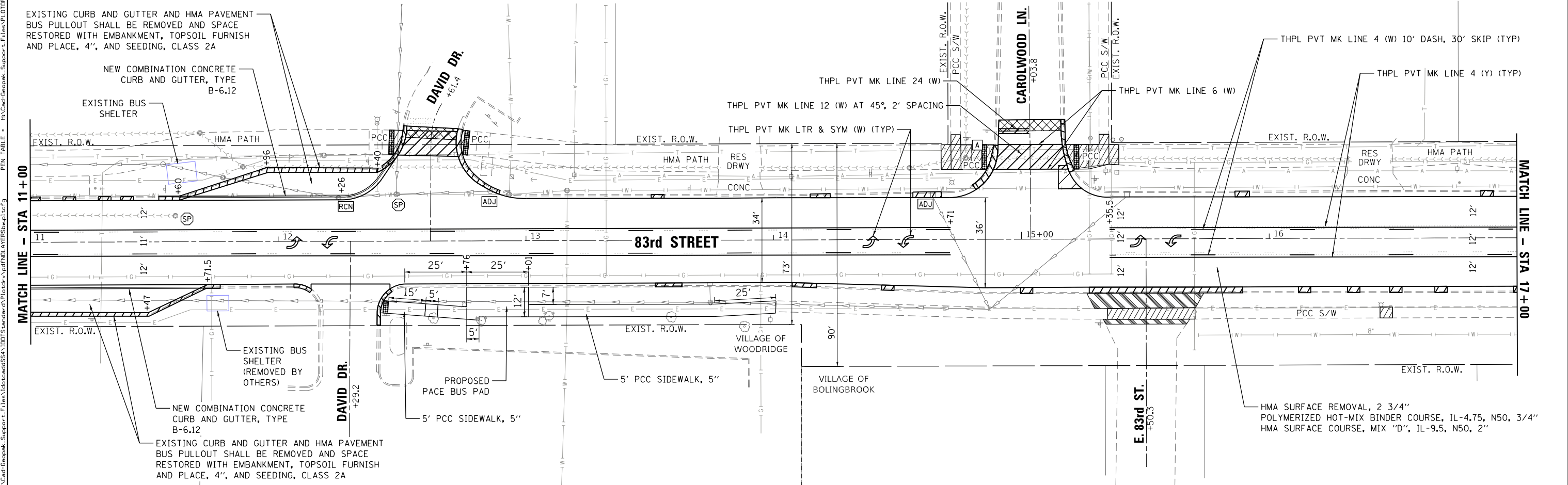
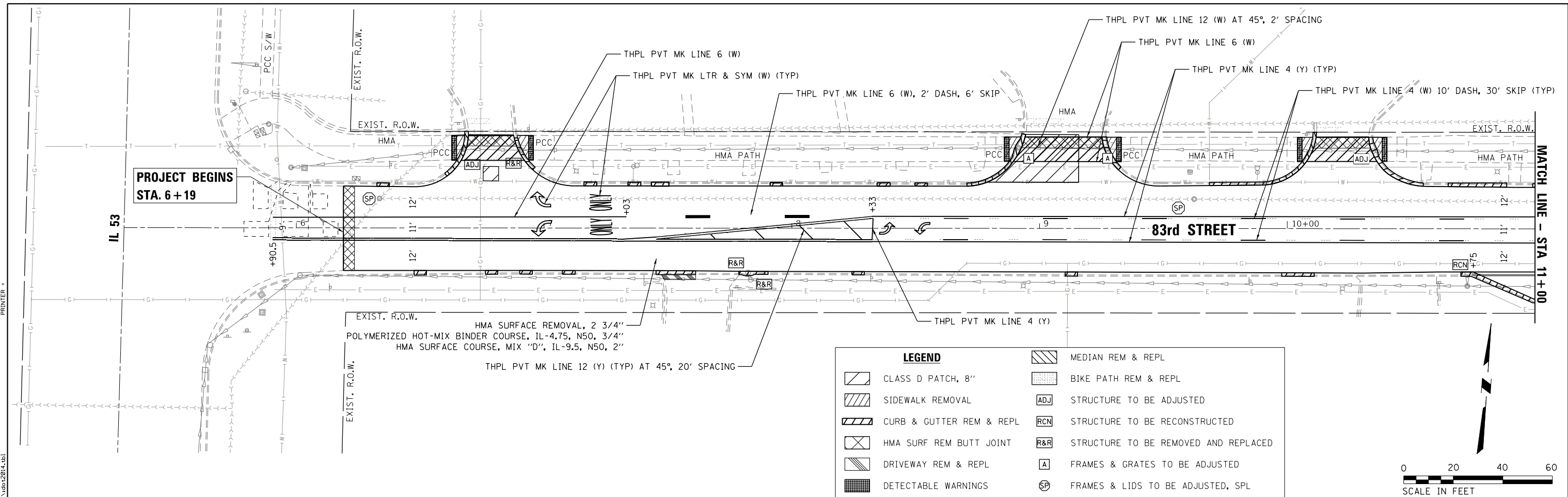
83RD STREET
 STA 62+50 TO STA 66+50

EXISTING LEGEND

- (A) HMA PAVEMENT, 11 1/2"
- (B) SUB-BASE GRANULAR MATERIAL, 4"
- (C) COMBINATION CONCRETE CURB & GUTTER
- (D) PCC SIDEWALK
- (E) HMA SURFACE REMOVAL, 2 3/4"
- (F) MULTI-USE PATH, 3"
- [Hatched] REMOVAL ITEMS

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 3/4"
- (3) CLASS D PATCHES, 8"
- (4) COMBINATION CONCRETE CURB & GUTTER, B-6.12 6+00 - 14+00
- (5) PCC SIDEWALK 5"
- (6) MULTI-USE PATH
- (7) TOPSOIL, 4"



FILE NAME = 7204-sht-plen.lgdg
 PEN TABLE = idot2014.tbl
 PLOT SCALE = 40.0000' / in.
 PLOT DATE = 12/23/2019

USER NAME = _USER_	DESIGNED - MH	REVISED -
DRAWN - MH	REVISOR -	REVISOR -
CHECKED -	REVISOR -	REVISOR -
DATE -	REVISOR -	REVISOR -

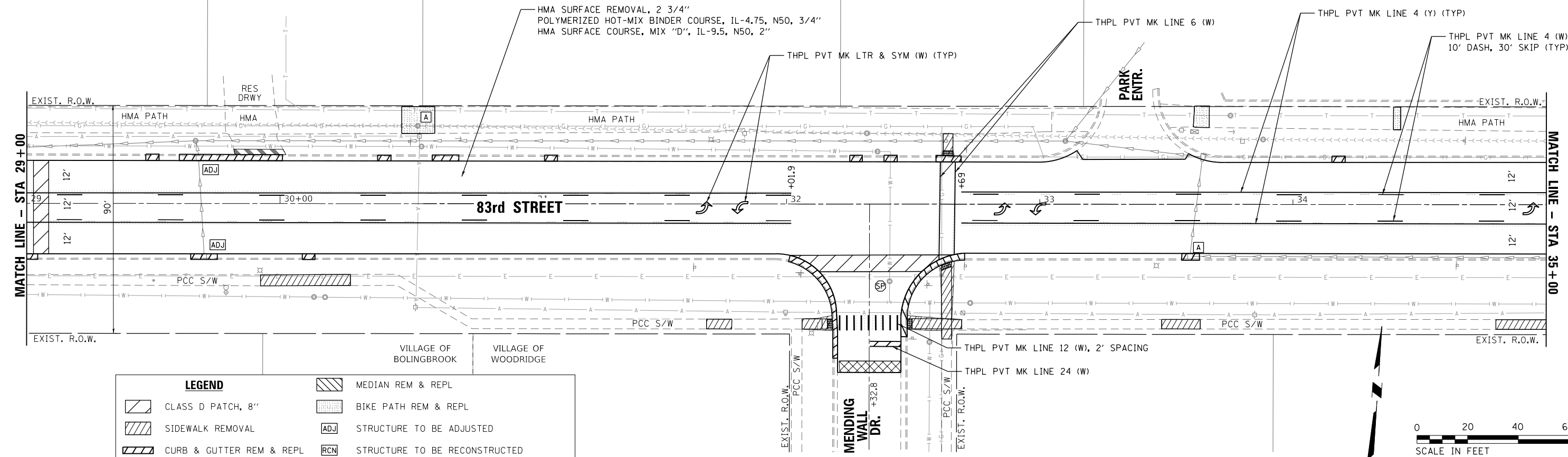
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**83rd STREET BETWEEN IL 53 & JANES AVENUE
 ROADWAY AND PAVEMENT MARKING PLAN**

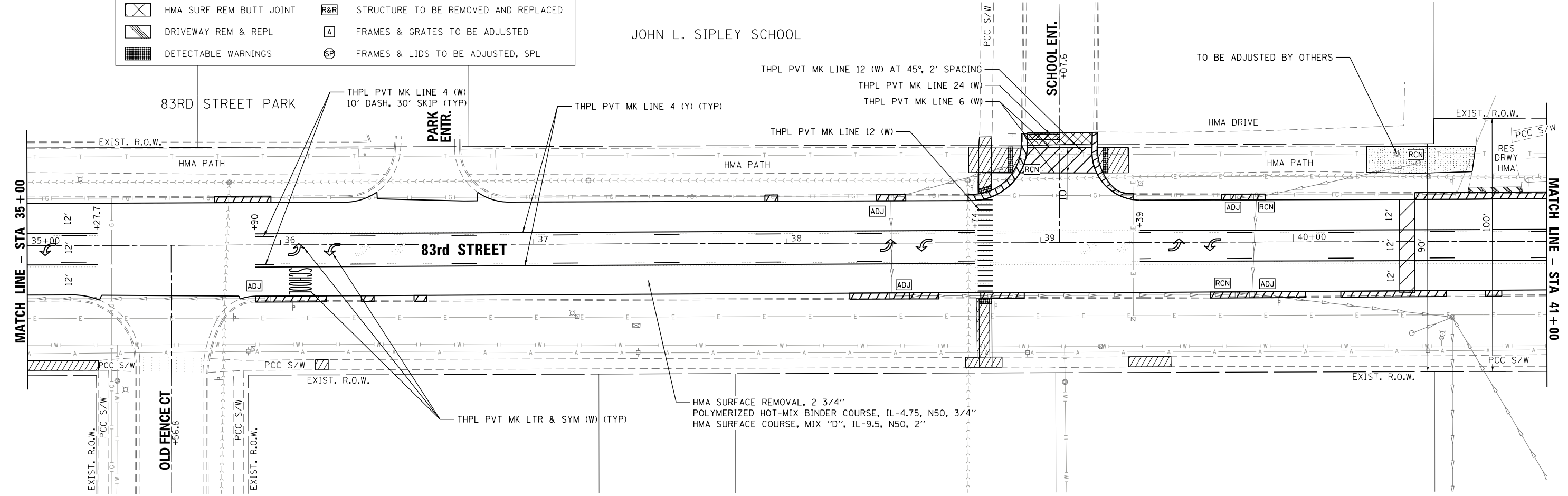
SCALE: 1" = 20' SHEET 1 OF 6 SHEETS STA. 5+30.5 TO STA. 17+00

F.A.U. RTE. 1551	SECTION 19-00081-00-RS	COUNTY DU PAGE	TOTAL SHEETS 27	SHEET NO. 7
CONTRACT NO. 61G37				ILLINOIS FED. AID PROJECT

83RD STREET PARK



LEGEND	
	CLASS D PATCH, 8"
	SIDEWALK REMOVAL
	CURB & GUTTER REM & REPL
	HMA SURF REM BUTT JOINT
	DRIVEWAY REM & REPL
	DETECTABLE WARNINGS
	MEDIAN REM & REPL
	BIKE PATH REM & REPL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE RECONSTRUCTED
	STRUCTURE TO BE REMOVED AND REPLACED
	FRAMES & GRATES TO BE ADJUSTED
	FRAMES & LIDS TO BE ADJUSTED, SPL



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

83rd STREET BETWEEN IL 53 & JANES AVENUE
ROADWAY AND PAVEMENT MARKING PLAN

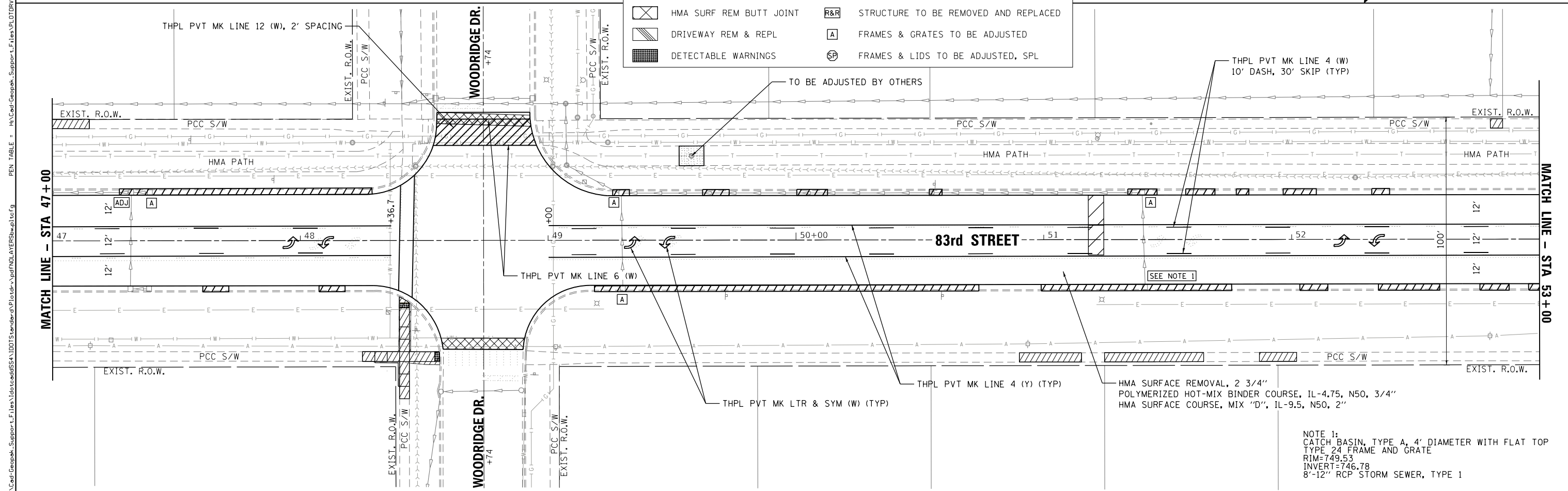
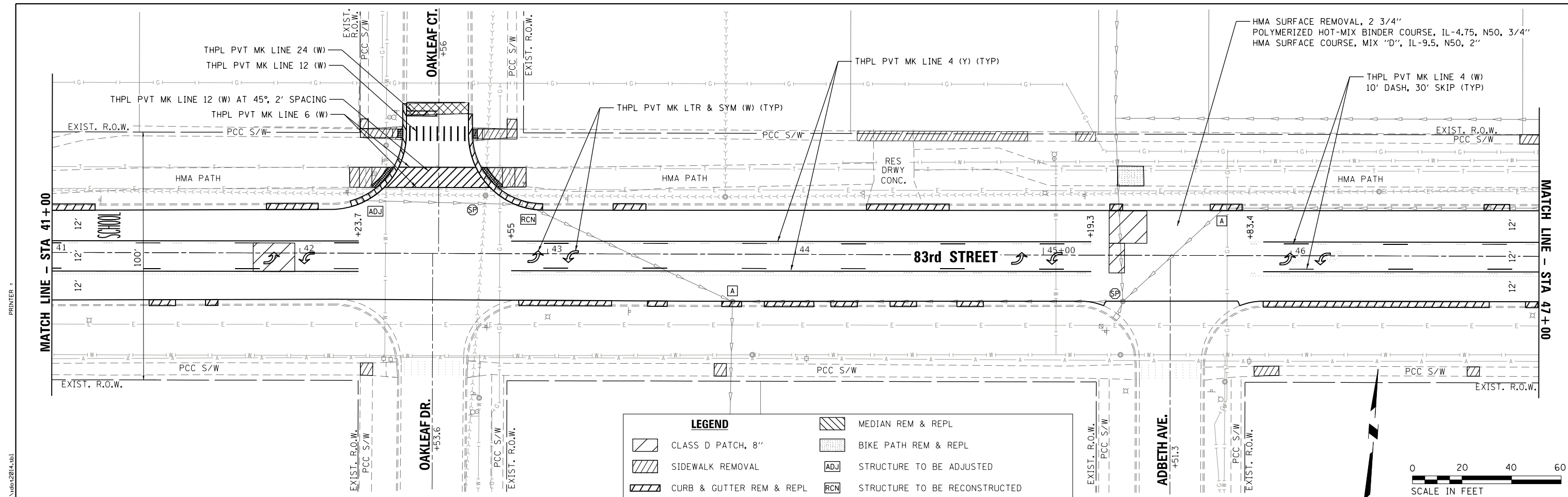
F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	9
CONTRACT NO. 61G37				

SCALE: 1" = 20' SHEET 3 OF 6 SHEETS STA. 29+00 TO STA. 41+00

PEN TABLE = H:\Cadd\Geopak_Support...
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 FILE NAME = 7204-shr-plan_3.dgn
 USER NAME = _USER_
 DESIGNED - MH
 DRAWN - MH
 CHECKED -
 DATE -
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 REVISED -

FILE NAME = 7204-shr-plan_3.dgn
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 DESIGNED - MH
 DRAWN - MH
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 REVISED -

ILLINOIS FED. AID PROJECT

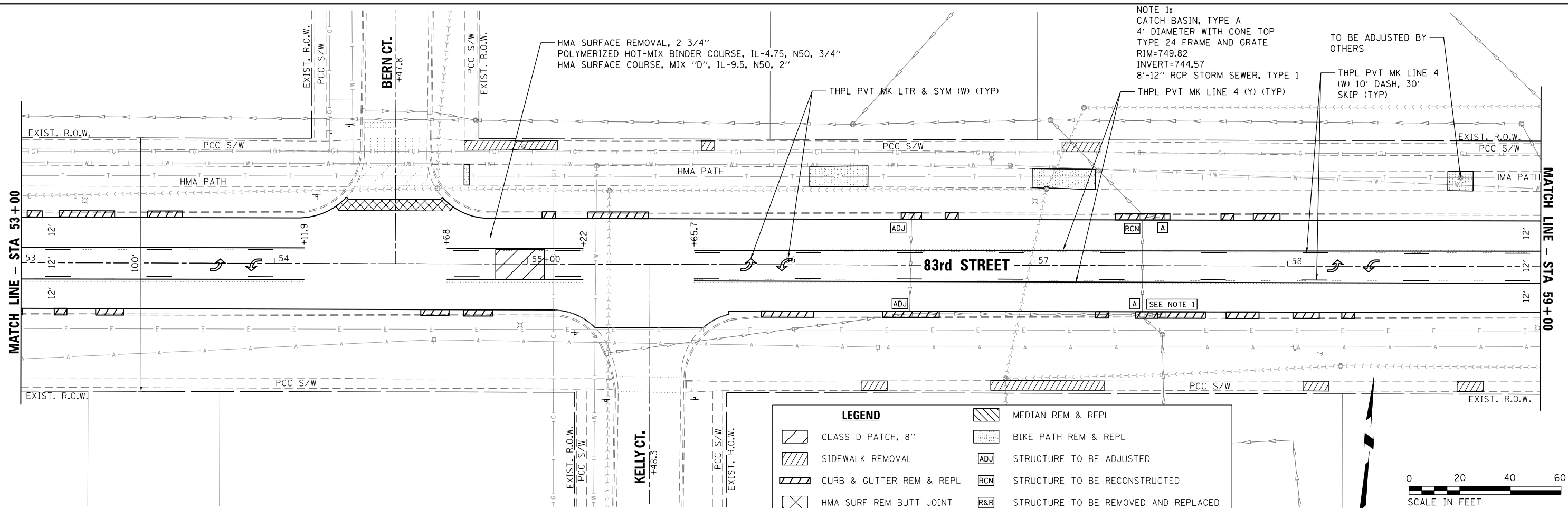


FILE NAME = 7204-sht-plen-4.dgn	USER NAME = _USER_	DESIGNED - MH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	83rd STREET BETWEEN IL 53 & JANES AVENUE ROADWAY AND PAVEMENT MARKING PLAN	F.A.U. RTE. 1551	SECTION 19-00081-00-RS	COUNTY DU PAGE	TOTAL SHEETS 27	SHEET NO. 10		
PLOT SCALE = 40.0000' / in.	DRAWN - MH	CHECKED -	REVISED -			SCALE: 1" = 20'	SHEET 4 OF 6 SHEETS	STA. 41+00	TO STA. 53+00	CONTRACT NO. 61G37		
PLOT DATE = 12/23/2019	DATE -	REVISOR -	REVISOR -			ILLINOIS FED. AID PROJECT						

PRINTER =

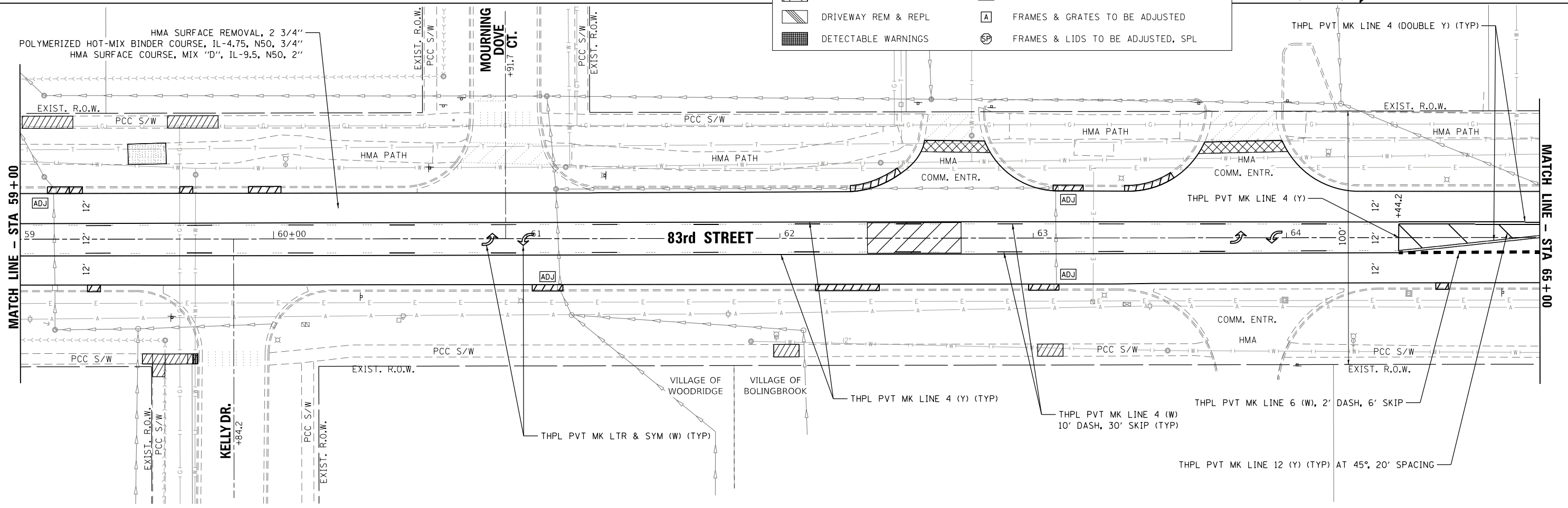
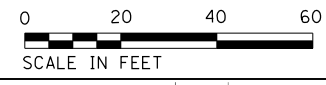
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PLOT DRIVER = H:\Cadd\Geopak_Support\Files\dotcadd\SSA\DOTStandard\Plotdrv\pdfrnd\GEPERShp\lctc.g



LEGEND

	CLASS D PATCH, 8"		BIKE PATH REM & REPL
	SIDEWALK REMOVAL		STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL		STRUCTURE TO BE RECONSTRUCTED
	HMA SURF REM BUTT JOINT		STRUCTURE TO BE REMOVED AND REPLACED
	DRIVEWAY REM & REPL		FRAMES & GRATES TO BE ADJUSTED
	DETECTABLE WARNINGS		FRAMES & LIDS TO BE ADJUSTED, SPL



FILE NAME = 7204-sht-plen_5.dgn
 USER NAME = _USER_
 DESIGNED - MH
 DRAWN - MH
 CHECKED -
 DATE -

REVISIONS:
 REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

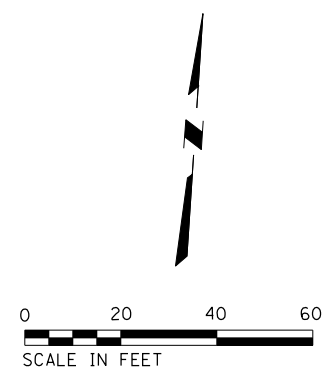
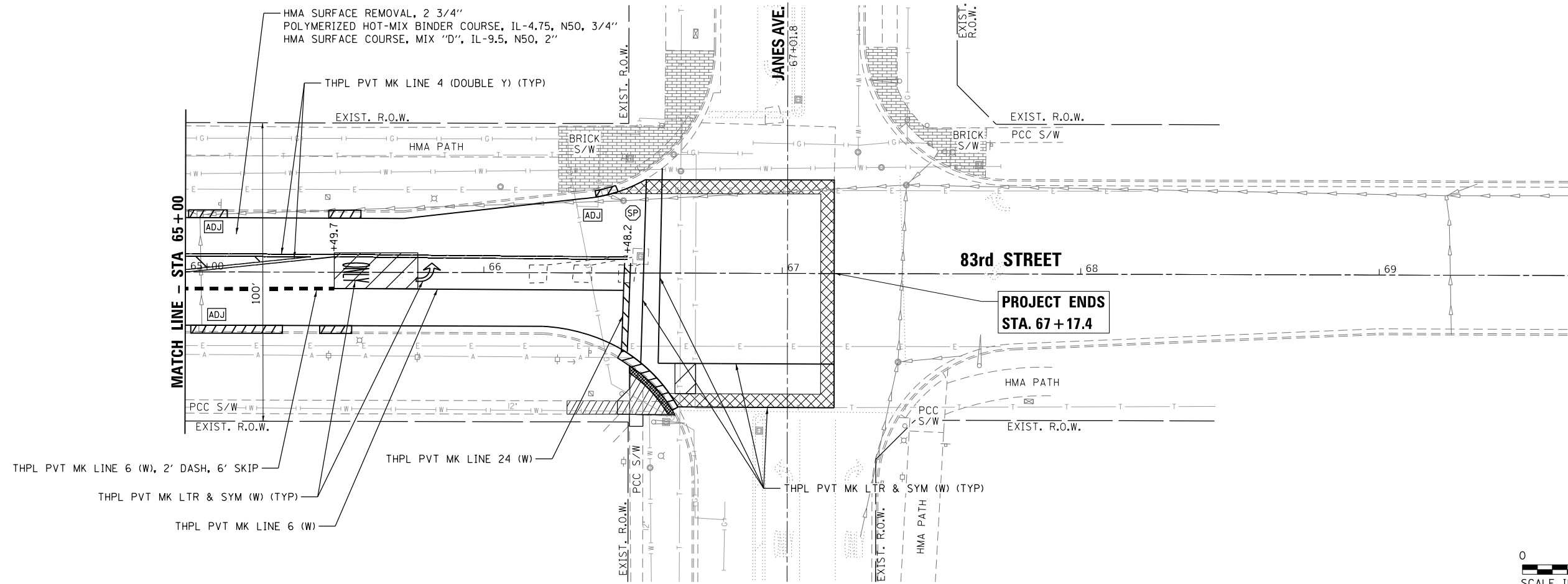
**83rd STREET BETWEEN IL 53 & JANES AVENUE
 ROADWAY AND PAVEMENT MARKING PLAN**
 SCALE: 1" = 20' SHEET 5 OF 6 SHEETS STA. 53+00 TO STA. 65+00

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	11
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				

PRINTER =

PEN TABLE = H:\Cadd\Geopak_Support\Files\DOT\DRV\1402014.tbl

PLOT DRIVER = H:\Cadd\Geopak_Support\Files\dotcad\SSA\DOT\Standard\Plotdrv\pdflnld\XERSBwplctcPg



LEGEND	
	CLASS D PATCH, 8"
	SIDEWALK REMOVAL
	CURB & GUTTER REM & REPL
	HMA SURF REM BUTT JOINT
	DRIVEWAY REM & REPL
	DETECTABLE WARNINGS
	MEDIAN REM & REPL
	BIKE PATH REM & REPL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE RECONSTRUCTED
	STRUCTURE TO BE REMOVED AND REPLACED
	FRAMES & GRATES TO BE ADJUSTED
	FRAMES & LIDS TO BE ADJUSTED, SPL

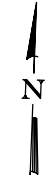
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PLOT SCALE = 40.0000' / in.	DRAWN - MH	CHECKED -	REVISED -
PLOT DATE = 12/23/2019	DATE -	REVISOR -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

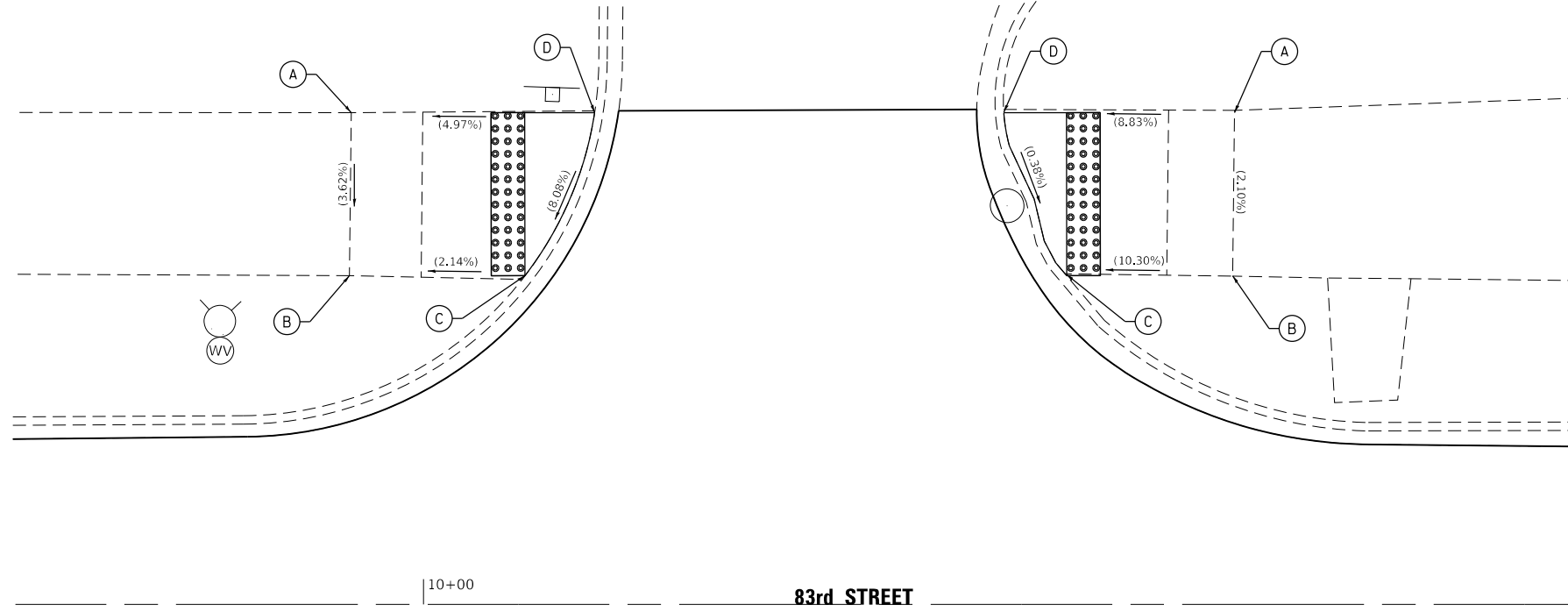
83rd STREET BETWEEN IL 53 & JANES AVENUE ROADWAY AND PAVEMENT MARKING PLAN			
SCALE: 1" = 20'	SHEET 6 OF 6 SHEETS	STA. 65+00	TO STA. 69+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	12
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				

THE TOWNHOMES
OF
HIGHCREST



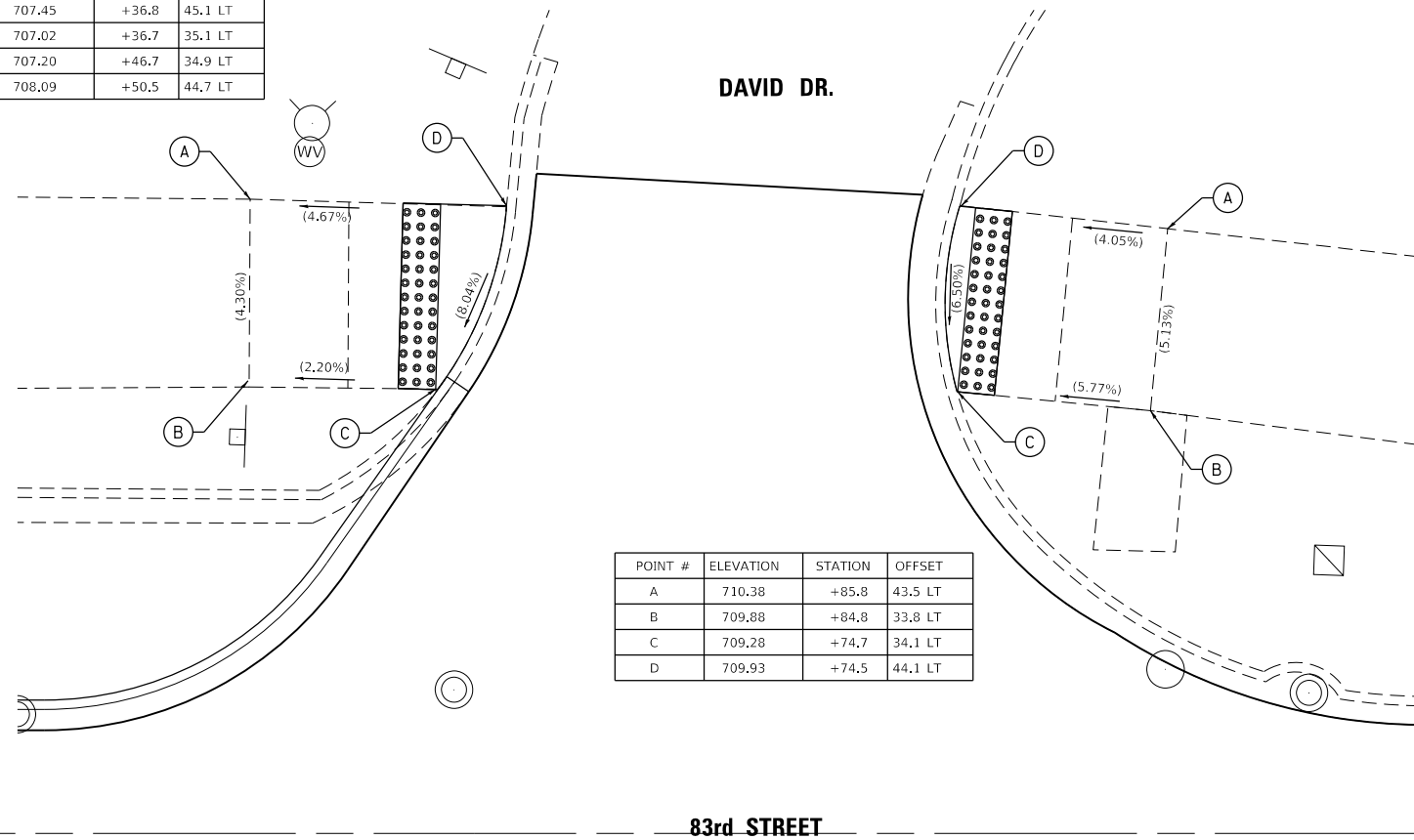
POINT #	ELEVATION	STATION	OFFSET
A	692.20	+95.6	36.8 LT
B	691.84	+95.6	27.2 LT
C	692.06	+06.0	27.1 LT
D	692.92	+10.2	36.8 LT



POINT #	ELEVATION	STATION	OFFSET
A	695.45	+48.2	36.8 LT
B	695.24	+48.2	27.1 LT
C	694.21	+38.2	27.1 LT
D	694.24	+34.5	36.8 LT

PRINTER = \$SYSPRINTER.NAMES

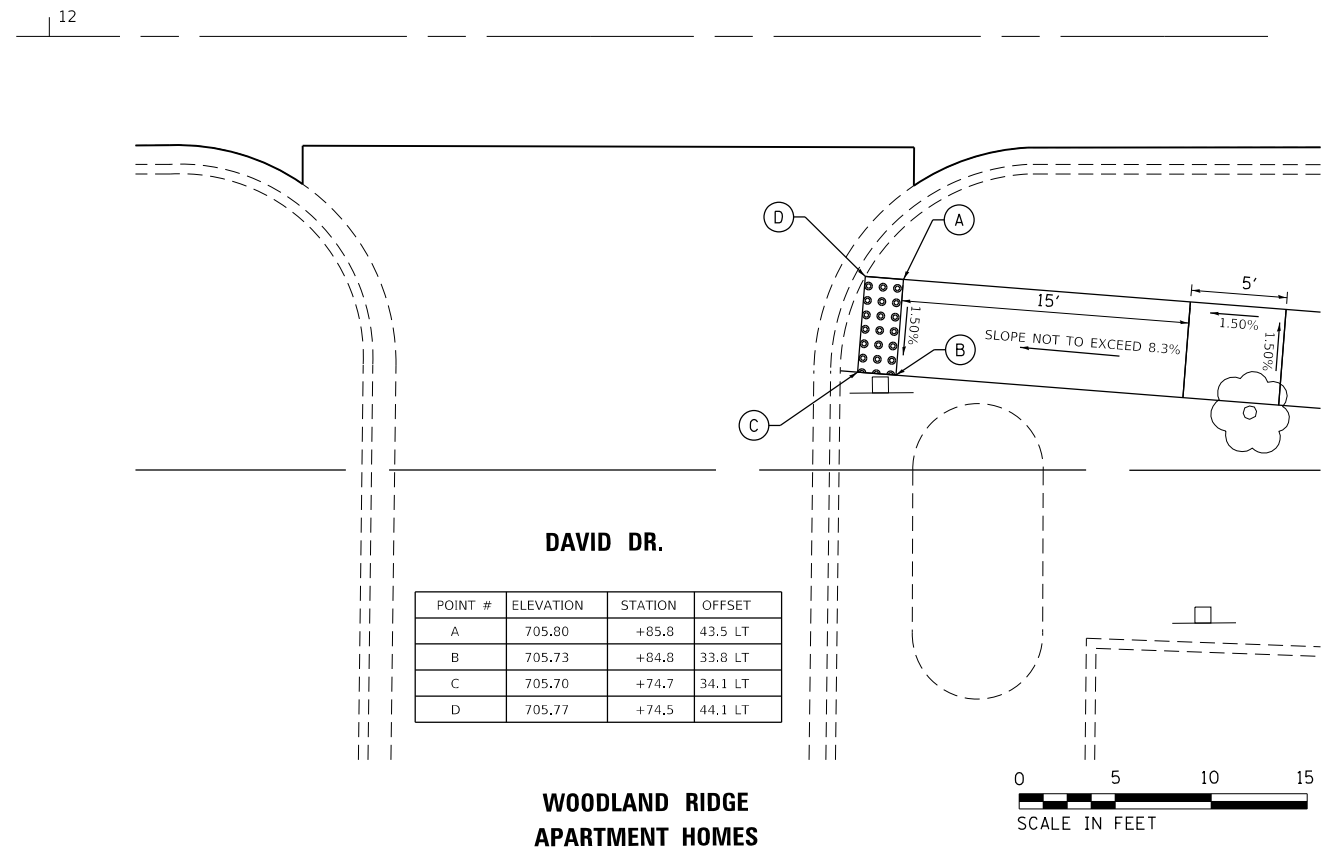
POINT #	ELEVATION	STATION	OFFSET
A	707.45	+36.8	45.1 LT
B	707.02	+36.7	35.1 LT
C	707.20	+46.7	34.9 LT
D	708.09	+50.5	44.7 LT



POINT #	ELEVATION	STATION	OFFSET
A	710.38	+85.8	43.5 LT
B	709.88	+84.8	33.8 LT
C	709.28	+74.7	34.1 LT
D	709.93	+74.5	44.1 LT

PEN TABLE = #PENTBL1
PEN TABLE = #PENTBL2

PLOT DRIVER = #PLTDRIVL



POINT #	ELEVATION	STATION	OFFSET
A	705.80	+85.8	43.5 LT
B	705.73	+84.8	33.8 LT
C	705.70	+74.7	34.1 LT
D	705.77	+74.5	44.1 LT



FILE NAME = H:\Prj_Municipal\7204 Woodridge - 83rd	USER NAME = dwozniarski	DESIGNED - MH	REVISED -
READABLE\CAD\DWG\7204-sht-ADA.2.dgn		DRAWN - MH	REVISED -
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PLOT DATE = 12/23/2019		DATE -	REVISED -

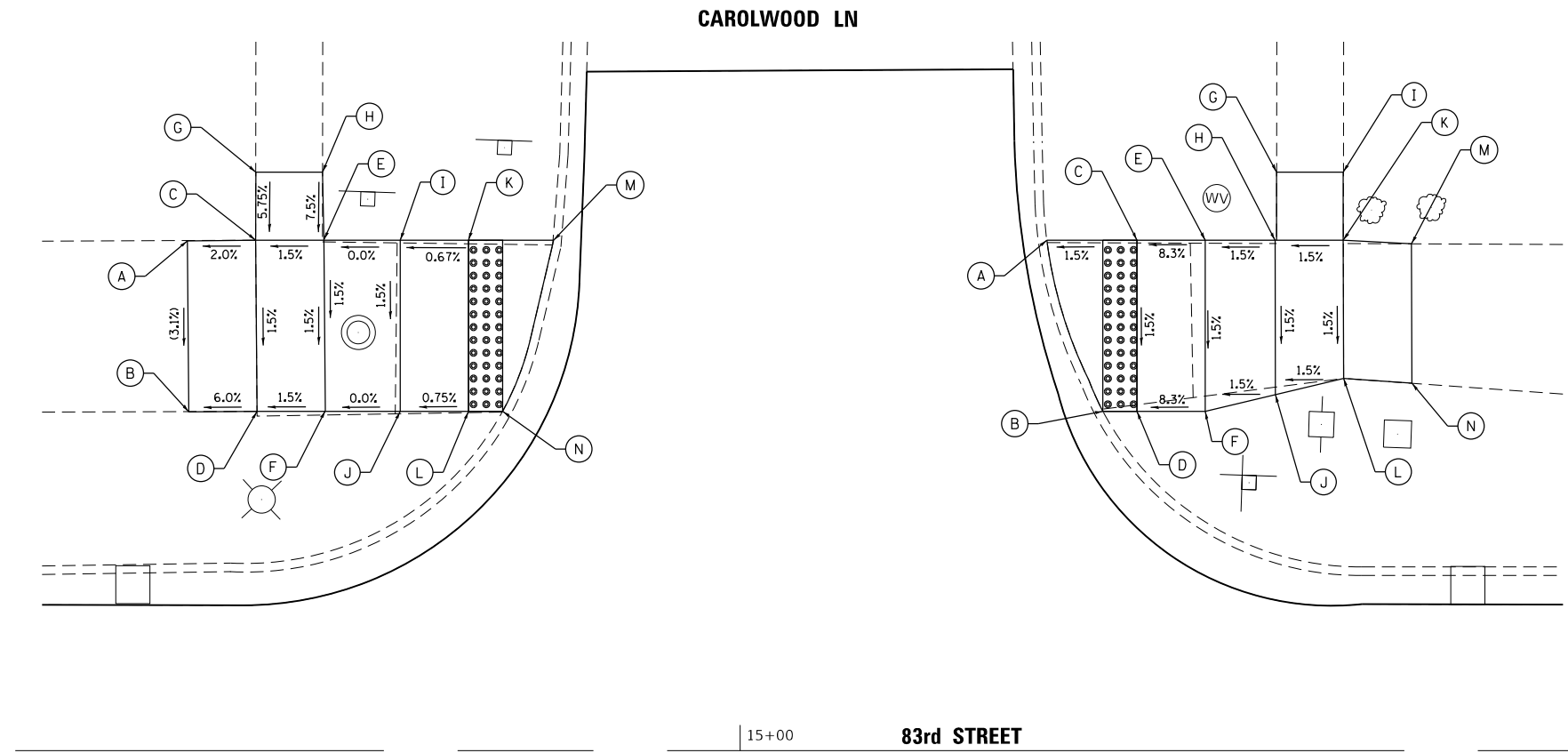
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

83rd STREET BETWEEN IL 53 & JANES AVENUE
ADA RAMP DETAILS

SCALE: 1"=5' SHEET 2 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	14
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				

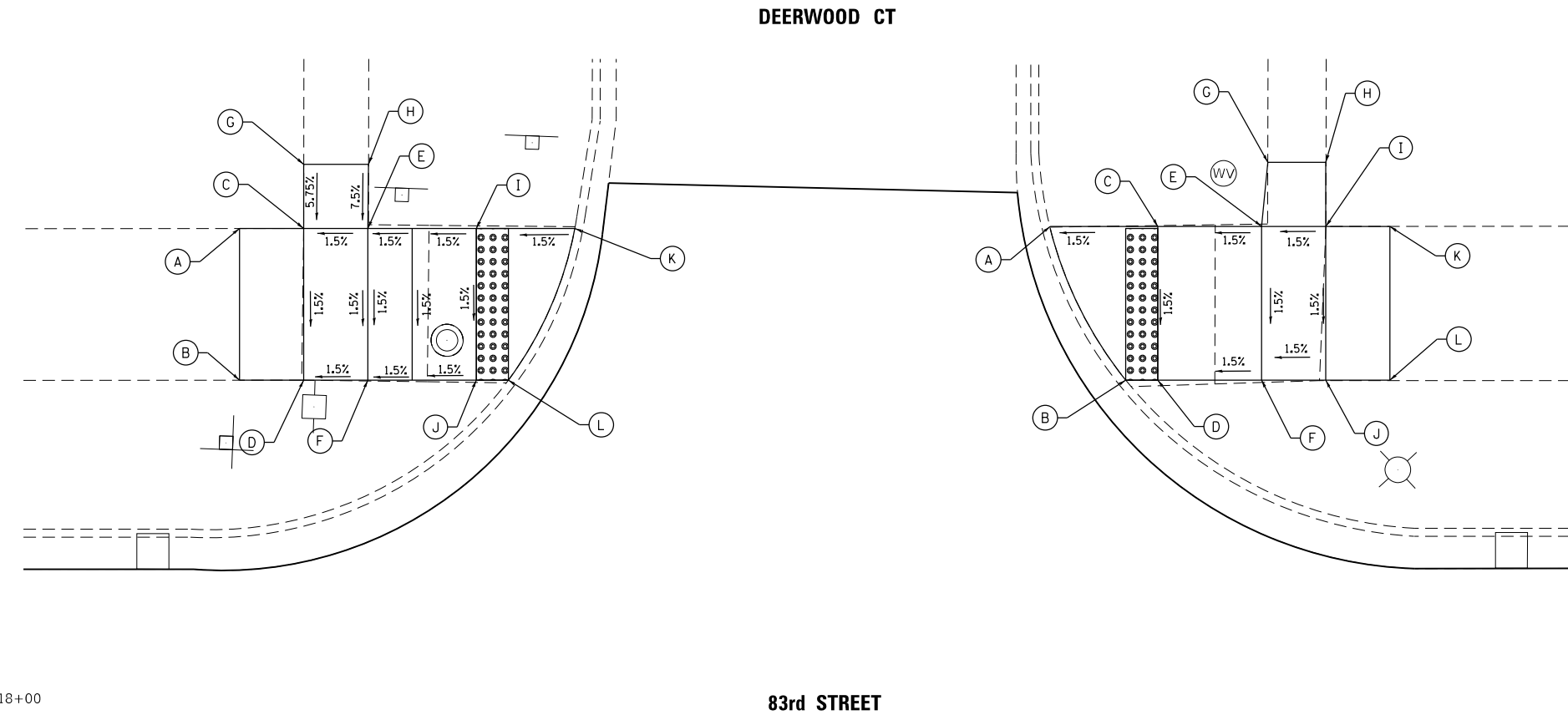
POINT #	ELEVATION	STATION	OFFSET
A	(715.08)	+67.6	38.3 LT
B	(714.77)	+67.7	28.2 LT
C	715.16	+71.6	38.1 LT
D	715.01	+71.7	28.3 LT
E	715.22	+75.6	38.3 LT
F	715.07	+75.7	28.3 LT
G	(715.46)	+71.6	42.3 LT
H	(715.47)	+75.5	42.3 LT
I	715.22	+80.1	38.3 LT
J	715.07	+80.1	28.3 LT
K	715.22	+84.1	38.3 LT
L	715.07	+84.1	28.3 LT
M	(215.29)	+89.1	38.3 LT
N	(715.01)	+86.1	28.3 LT



POINT #	ELEVATION	STATION	OFFSET
A	(715.40)	+18.0	38.3 LT
B	(715.44)	+21.3	28.3 LT
C	715.48	+23.3	38.3 LT
D	715.33	+23.3	28.3 LT
E	715.81	+27.3	38.3 LT
F	715.66	+27.3	28.3 LT
G	(716.32)	+31.5	42.3 LT
H	715.87	+31.4	38.3 LT
I	(716.58)	+35.4	42.3 LT
J	715.72	+31.4	29.2 LT
K	715.93	+35.4	38.3 LT
L	715.78	+35.4	30.2 LT
M	(215.29)	+39.4	38.1 LT
N	(715.01)	+39.4	29.9 LT



POINT #	ELEVATION	STATION	OFFSET
A	716.65	+15.5	38.0 LT
B	716.54	+15.5	28.0 LT
C	716.22	+19.5	38.0 LT
D	716.07	+19.5	28.5 LT
E	716.16	+23.5	38.0 LT
F	716.01	+23.5	28.5 LT
G	716.76	+19.5	42.0 LT
H	716.58	+23.5	42.0 LT
I	716.06	+30.3	38.0 LT
J	715.91	+30.3	28.5 LT
K	715.97	+36.4	38.0 LT
L	715.98	+32.3	28.5 LT

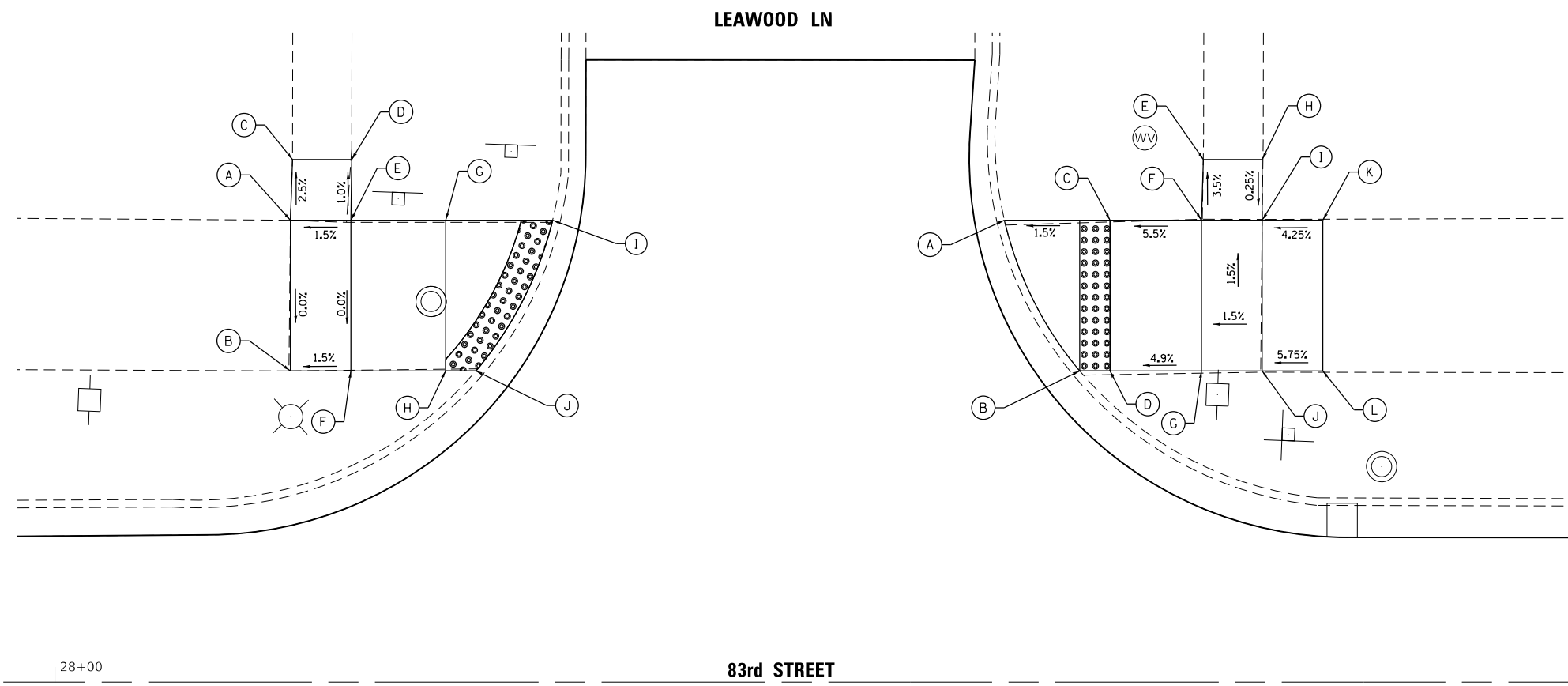


POINT #	ELEVATION	STATION	OFFSET
A	715.52	+66.1	38.1 LT
B	715.27	+70.8	28.5 LT
C	715.63	+72.8	38.1 LT
D	715.48	+72.8	28.5 LT
E	715.72	+79.2	38.1 LT
F	715.57	+79.2	28.5 LT
G	715.91	+79.6	42.1 LT
H	715.87	+83.2	42.1 LT
I	715.78	+83.2	38.1 LT
J	715.63	+83.2	28.5 LT
K	715.66	+87.2	38.1 LT
L	715.38	+87.2	28.5 LT



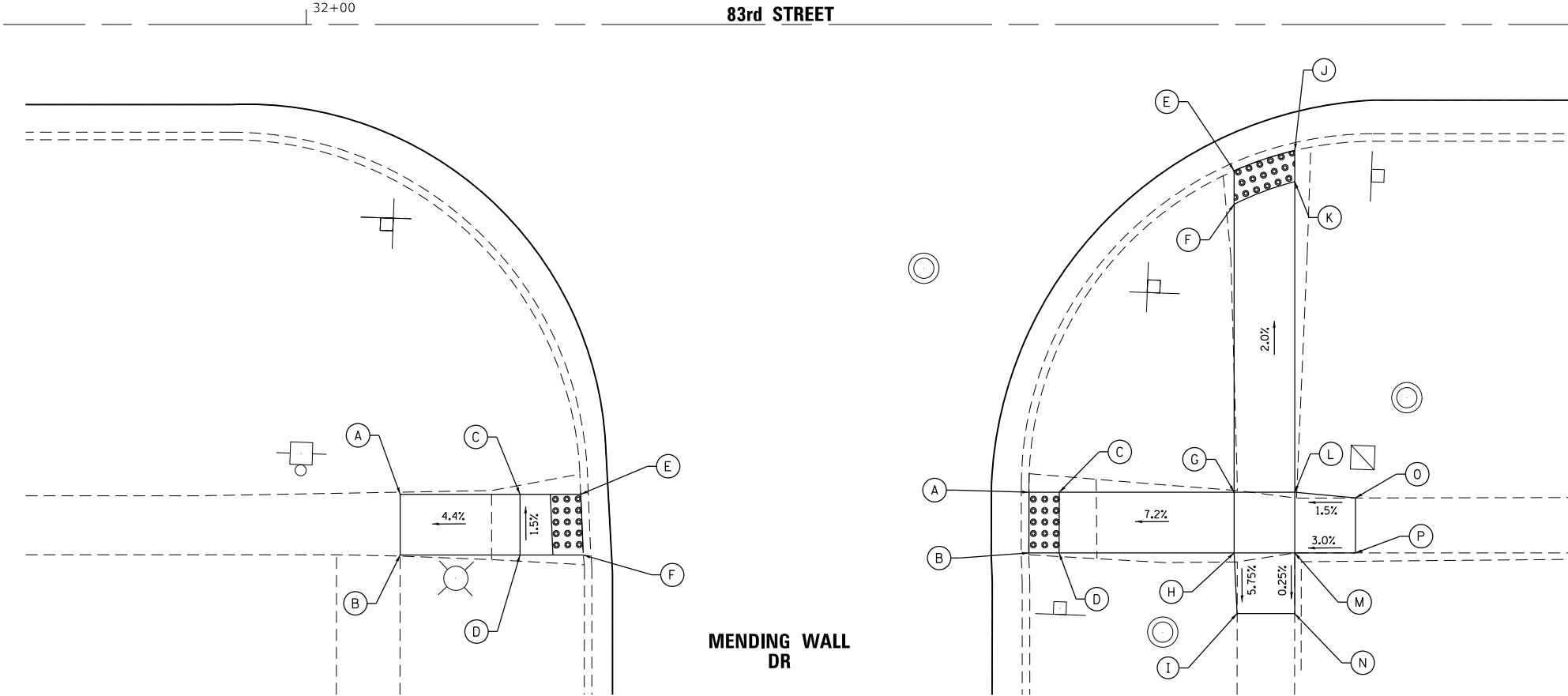
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 PEN TABLE = \$PENTBL\$
 PLOT DRIVER = \$PLTDRI\$

POINT #	ELEVATION	STATION	OFFSET
A	(732.23)	+15.5	37.9 LT
B	(732.23)	+15.5	28.0 LT
C	(732.33)	+15.7	41.9 LT
D	(732.25)	+19.6	41.9 LT
E	732.29	+19.5	37.9 LT
F	732.29	+19.5	28.0 LT
G	732.29	+25.8	37.9 LT
H	732.14	+25.8	28.0 LT
I	732.25	+32.8	37.9 LT
J	732.10	+27.8	28.0 LT



POINT #	ELEVATION	STATION	OFFSET
A	733.10	+62.6	37.9 LT
B	733.40	+67.6	28.0 LT
C	733.20	+69.6	37.9 LT
D	733.40	+69.6	28.0 LT
E	(733.50)	+75.7	41.9 LT
F	733.64	+75.6	37.9 LT
G	733.79	+75.6	28.0 LT
H	(733.71)	+79.6	41.9 LT
I	733.70	+79.6	28.0 LT
J	733.85	+79.6	28.0 LT
K	(733.87)	+83.6	37.9 LT
L	(734.07)	+83.6	28.0 LT

POINT #	ELEVATION	STATION	OFFSET
A	(742.76)	+06.2	45.4 RT
B	(742.61)	+06.2	49.4 RT
C	742.92	+12.1	45.4 RT
D	742.96	+12.1	49.4 RT
E	742.86	+18.1	45.4 RT
F	742.90	+18.3	49.4 RT



POINT #	ELEVATION	STATION	OFFSET
A	743.66	+47.7	45.3 RT
B	743.60	+47.7	49.3 RT
C	743.72	+53.7	45.3 RT
D	743.66	+53.7	49.3 RT
E	744.04	+61.2	24.0 RT
F	744.13	+61.2	26.2 RT
G	744.51	+61.2	45.3 RT
H	744.57	+61.2	49.3 RT
I	(744.34)	+61.4	53.3 RT
J	744.10	+61.2	24.0 RT
K	744.19	+65.2	24.8 RT
L	744.57	+65.2	45.3 RT
M	744.63	+65.2	49.3 RT
N	(744.61)	+65.2	53.3 RT
O	(744.63)	+69.2	45.6 RT
P	(744.75)	+69.2	49.3 RT



PLOT DRIVER = \$PLTDRAWL\$
 PEN TABLE = \$PENTBL\$
 PRINTER = \$SPRINTERNAME\$

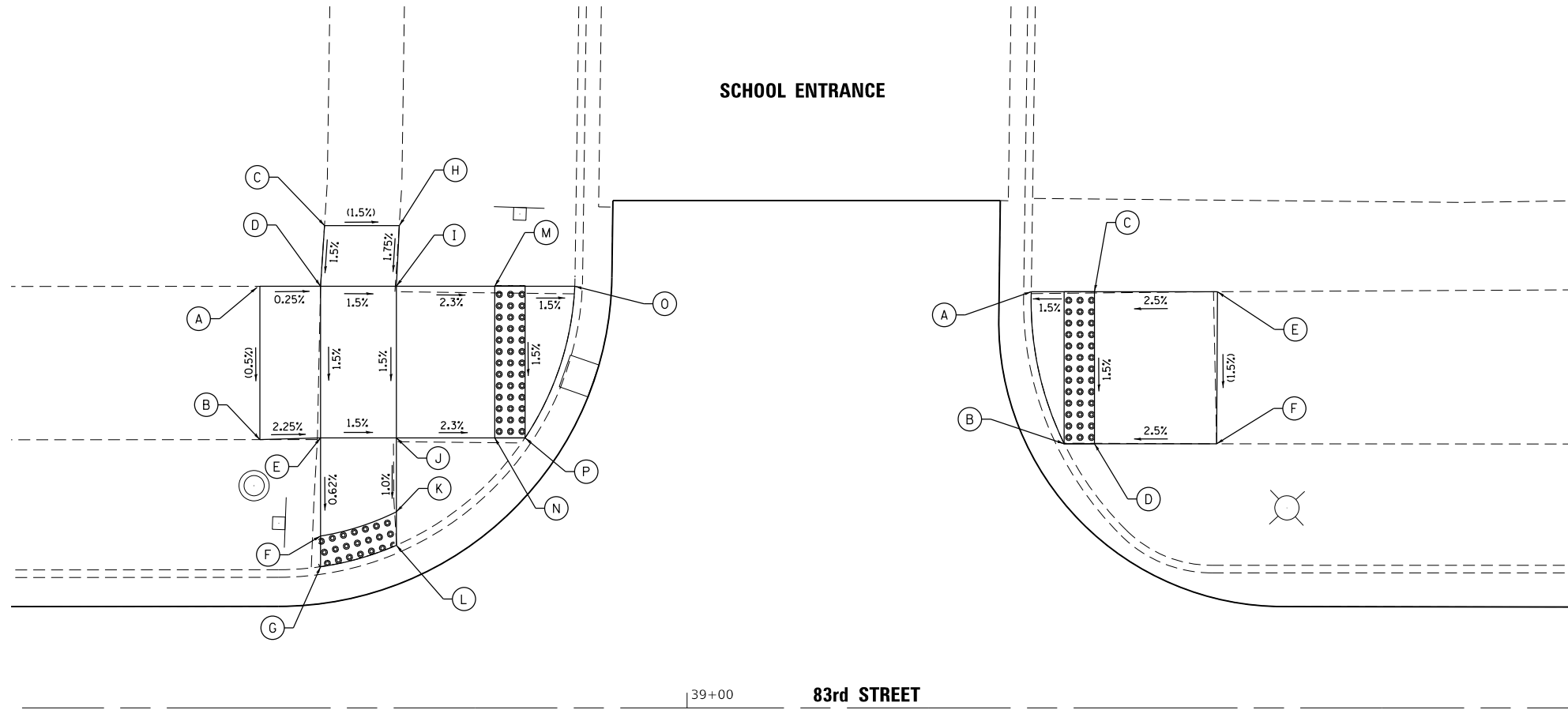
FILE NAME =	USER NAME = dwoznarski	DESIGNED - MH	REVISED -
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	PLOT SCALE = 10.0000 / in.	CHECKED -	REVISED -
Default	PLOT DATE = 12/23/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

83rd STREET BETWEEN IL 53 & JANES AVENUE
ADA RAMP DETAILS
 SCALE: 1"=5' SHEET 4 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	16
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				

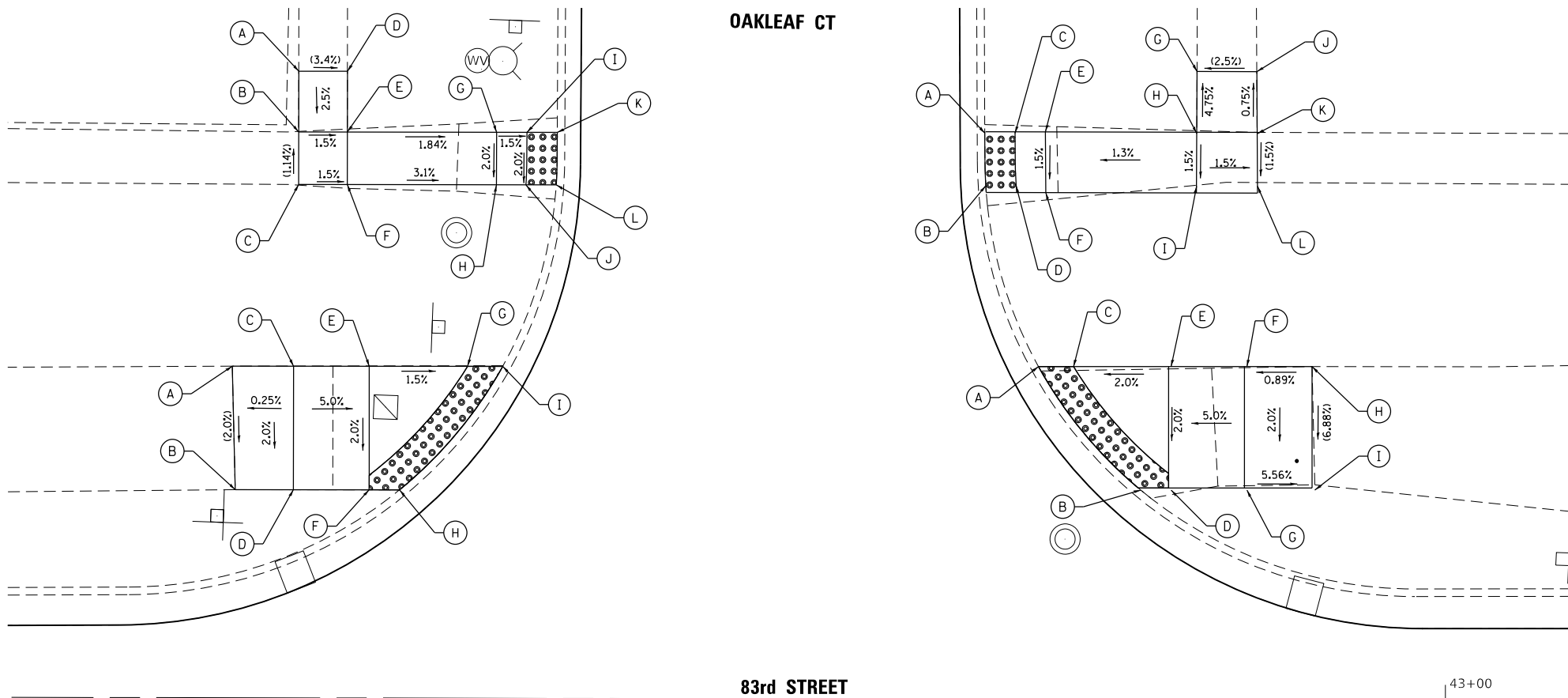
POINT #	ELEVATION	STATION	OFFSET
A	(747.09)	+71.8	37.8 LT
B	(747.04)	+71.8	27.7 LT
C	(747.16)	+76.1	41.8 LT
D	747.10	+75.8	37.8 LT
E	746.95	+75.8	27.8 LT
F	746.91	+75.8	21.4 LT
G	746.88	+75.8	19.3 LT
H	(747.10)	+81.0	41.8 LT
I	747.03	+80.8	37.8 LT
J	746.88	+80.8	27.8 LT
K	746.83	+80.8	23.0 LT
L	746.80	+80.8	20.8 LT
M	746.88	+87.3	37.9 LT
N	746.73	+87.3	27.8 LT
O	746.80	+92.6	37.8 LT
P	746.69	+89.3	27.8 RT



POINT #	ELEVATION	STATION	OFFSET
A	746.70	+22.7	37.5 LT
B	746.55	+24.9	27.5 LT
C	746.76	+26.9	37.5 LT
D	746.61	+26.9	27.5 LT
E	746.96	+35.0	37.5 LT
F	746.81	+34.9	27.5 LT

39+00 83rd STREET

POINT #	ELEVATION	STATION	OFFSET
A	(750.05)	+24.3	54.2 LT
B	(749.95)	+24.3	50.2 LT
C	(749.99)	+24.3	46.7 LT
D	(749.94)	+27.5	54.2 LT
E	749.89	+27.5	50.2 LT
F	749.93	+27.5	46.7 LT
G	749.71	+37.3	50.2 LT
H	749.63	+37.3	46.7 LT
I	749.68	+39.3	50.2 LT
J	749.60	+39.3	46.7 LT
K	749.65	+41.3	50.2 LT
L	749.57	+41.3	46.7 LT



POINT #	ELEVATION	STATION	OFFSET
A	749.66	+69.5	50.2 LT
B	749.60	+69.6	46.7 LT
C	749.69	+71.5	50.2 LT
D	749.63	+73.6	46.2 LT
E	749.72	+73.5	50.2 LT
F	749.66	+73.6	46.2 LT
G	(749.40)	+83.5	54.2 LT
H	749.59	+83.5	50.2 LT
I	749.53	+83.5	46.7 LT
J	(749.50)	+87.5	54.2 LT
K	(749.53)	+87.5	54.2 LT
L	(749.47)	+87.5	46.7 LT

43+00 83rd STREET



POINT #	ELEVATION	STATION	OFFSET
A	(749.17)	+19.9	34.7 LT
B	(749.01)	+20.1	26.6 LT
C	749.18	+24.0	34.7 LT
D	749.02	+24.0	26.6 LT
E	748.93	+29.0	34.7 LT
F	748.77	+29.0	26.6 LT
G	748.83	+35.5	34.7 LT
H	748.73	+31.0	26.6 LT
I	748.80	+37.8	34.7 LT

POINT #	ELEVATION	STATION	OFFSET
A	749.00	+73.1	34.8 LT
B	748.93	+79.9	26.7 LT
C	749.03	+75.4	34.8 LT
D	748.97	+81.9	26.7 LT
E	749.13	+81.9	34.8 LT
F	749.38	+86.9	34.8 LT
G	749.22	+86.9	26.7 LT
H	(749.42)	+91.2	34.8 LT
I	(748.97)	+91.3	26.7 LT

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PLOT DRIVER = \$PLTDRIVL\$

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DESIGNED - MH
DRAWN - MH
CHECKED -
DATE -

REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

83rd STREET BETWEEN IL 53 & JANES AVENUE
ADA RAMP DETAILS
SCALE: 1"=5' SHEET 5 OF 6 SHEETS STA. TO STA.

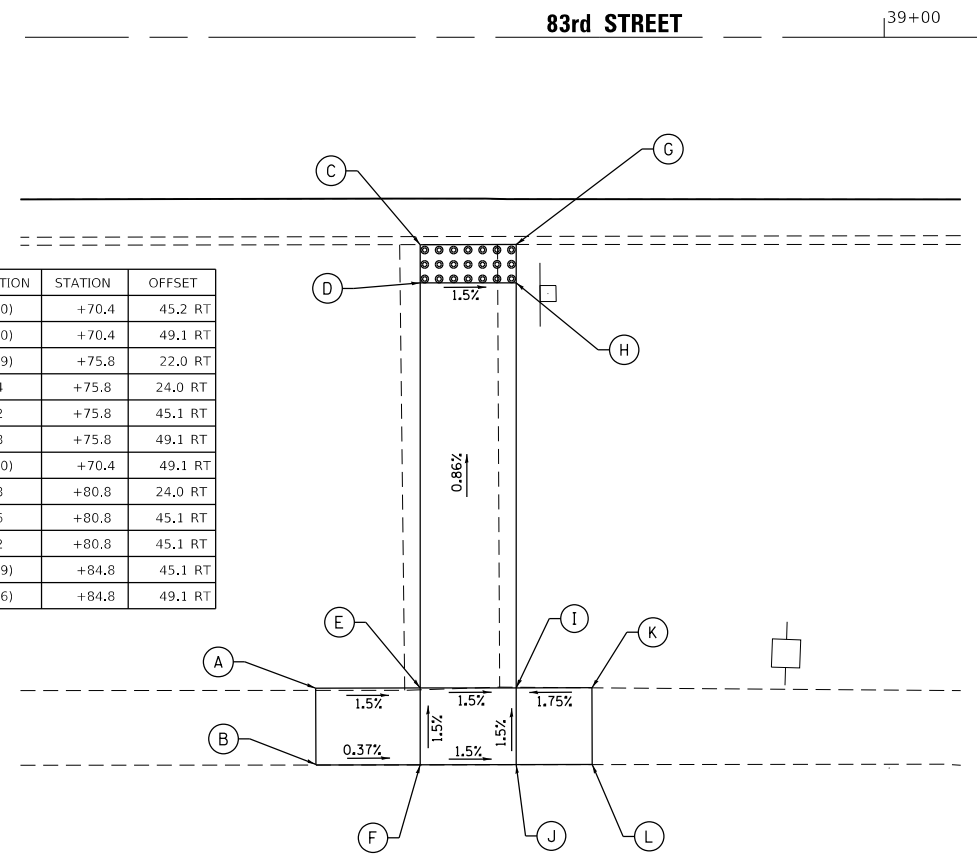
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	17
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				

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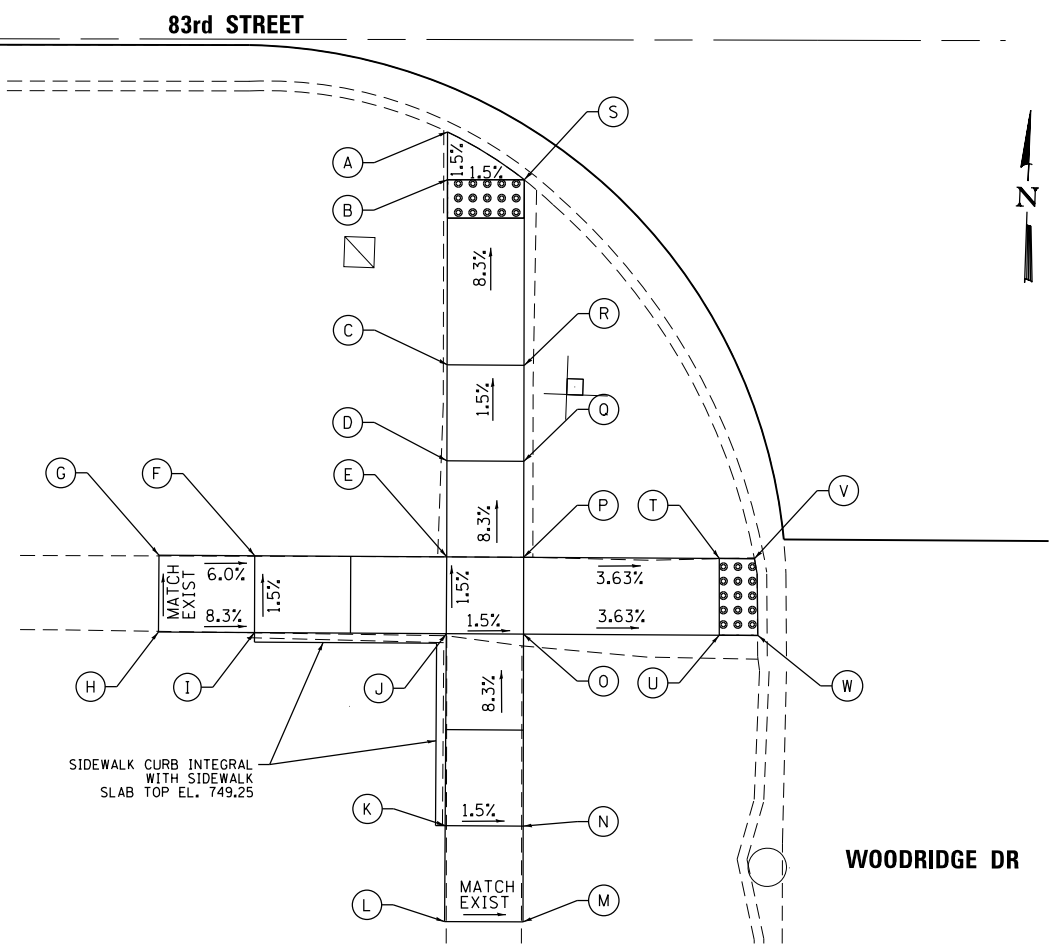
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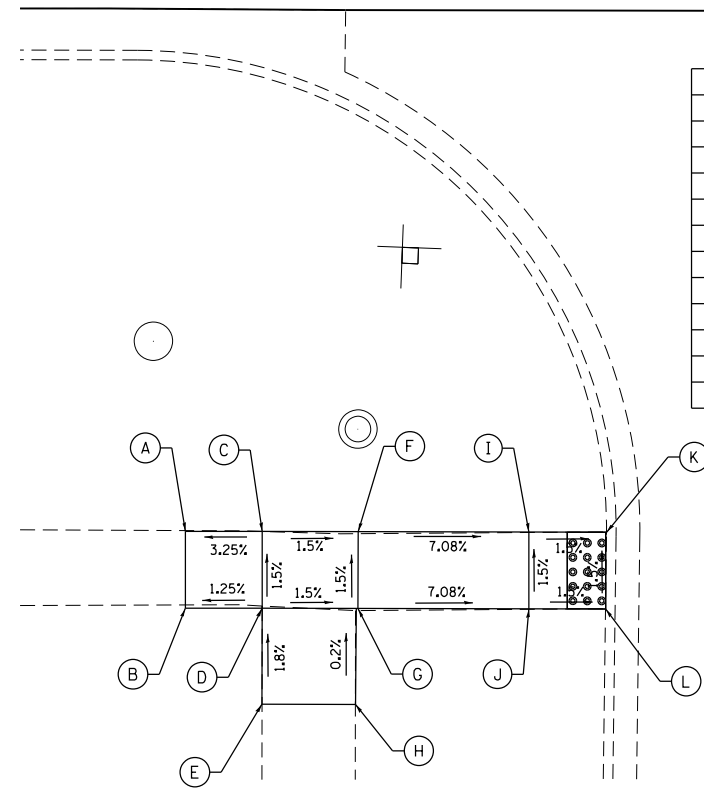
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A	(747.20)	+70.4	45.2 RT
B	(747.20)	+70.4	49.1 RT
C	(746.79)	+75.8	22.0 RT
D	746.94	+75.8	24.0 RT
E	747.12	+75.8	45.1 RT
F	747.18	+75.8	49.1 RT
G	(747.20)	+70.4	49.1 RT
H	746.88	+80.8	24.0 RT
I	747.06	+80.8	45.1 RT
J	747.12	+80.8	45.1 RT
K	(746.99)	+84.8	45.1 RT
L	(747.06)	+84.8	49.1 RT



POINT #	ELEVATION	STATION	OFFSET
A	(746.77)	+40.0	22.7 RT
B	746.81	+40.0	25.2 RT
C	747.64	+40.0	35 RT
D	747.72	+40.0	40 RT
E	748.11	+40.0	45 RT
F	748.94	+30.0	45 RT
G	(749.24)	+25.0	45 RT
H	(749.42)	+25.0	49 RT
I	749.00	+30.0	49 RT
J	748.17	+40.0	49 RT
K	749.00	+40.0	59 RT
L	(MATCH EX)	+40.0	64 RT
M	(MATCH EX)	+40.0	64 RT
N	748.94	+44.0	59 RT
O	748.11	+44.0	49 RT
P	748.05	+44.0	45 RT
Q	747.66	+44.0	40 RT
R	747.58	+44.0	35 RT
S	(746.84)	+44.0	25.2 RT
T	747.68	+54.2	45 RT
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V	(747.65)	+56	45 RT
W	(747.71)	+56.2	49 RT



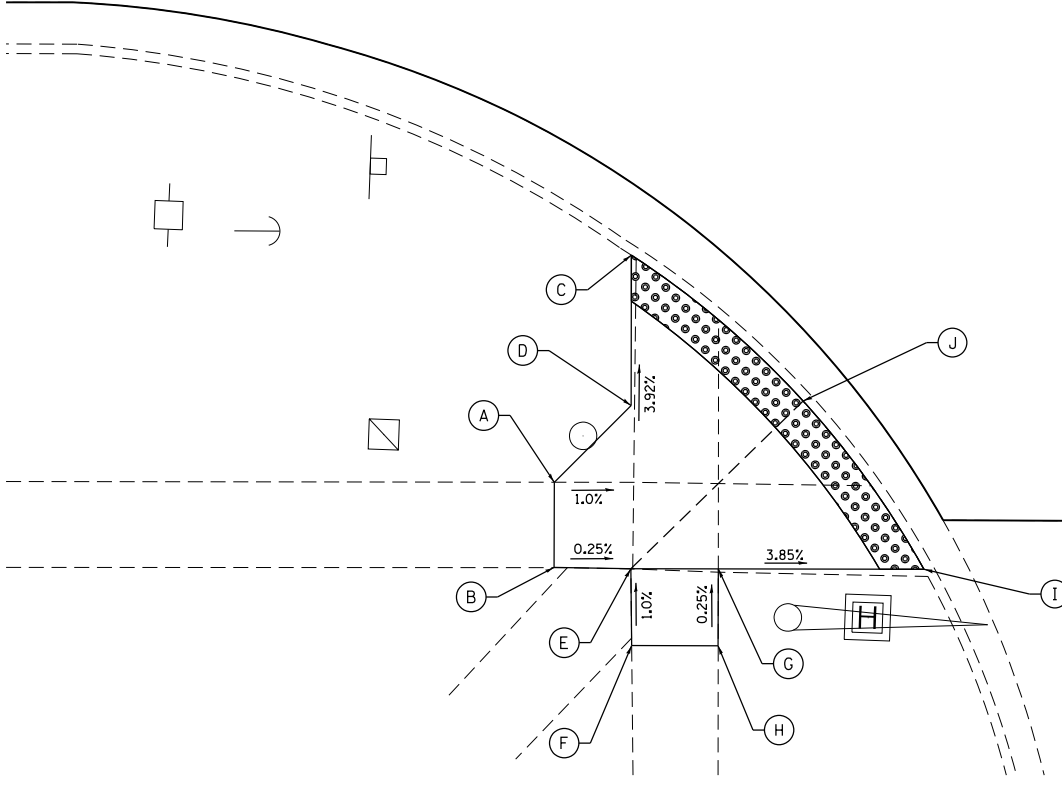
59+50 83rd STREET



POINT #	ELEVATION	STATION	OFFSET
A	(752.12)	+48.2	45.3 RT
B	(752.26)	+48.2	49.3 RT
C	752.25	+52.2	45.3 RT
D	752.31	+52.2	49.3 RT
E	(752.40)	+52.2	53.8 RT
F	752.18	+57.2	45.3 RT
G	752.24	+57.2	49.3 RT
H	(752.23)	+57.1	53.8 RT
I	751.55	+66.1	45.3 RT
J	751.61	+66.1	49.3 RT
K	(751.60)	+70.1	45.3 RT
L	(751.71)	+70.1	49.3 RT

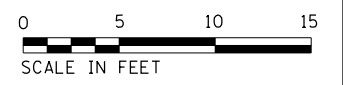
KELLY DRIVE

66+50 83rd STREET



POINT #	ELEVATION	STATION	OFFSET
A	(757.90)	+44.9	42.9 RT
B	(758.06)	+44.9	47.3 RT
C	757.40	+48.9	31.0 RT
D	757.71	+49.0	38.9 RT
E	758.04	+48.9	47.4 RT
F	758.00	+48.9	51.4 RT
G	757.87	+53.4	47.4 RT
H	(757.89)	+53.4	51.4 RT
I	757.45	+64.1	47.4 RT
J	757.43	+57.8	38.6 RT

JANES DRIVE

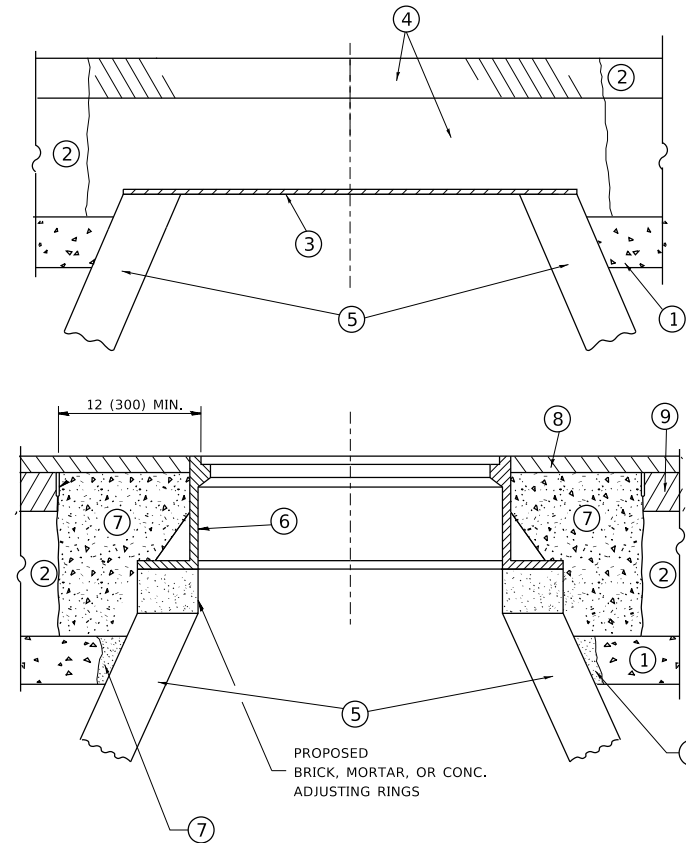


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PLOT SCALE = 10.0000 / in.		CHECKED -	REVISED -
PLOT DATE = 12/23/2019		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

83rd STREET BETWEEN IL 53 & JANES AVENUE
ADA RAMP DETAILS
SCALE: 1"=5' SHEET 6 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	18
CONTRACT NO. 61G37				
ILLINOIS FED. AID PROJECT				



NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.*

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	19
BD600-03 (BD-8)		CONTRACT NO. 61G37		
ILLINOIS		FED. AID PROJECT		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE 2)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE 2)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

¼" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE 1).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

PROPOSED ¾" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE. REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

NOTE:

- SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED.
- CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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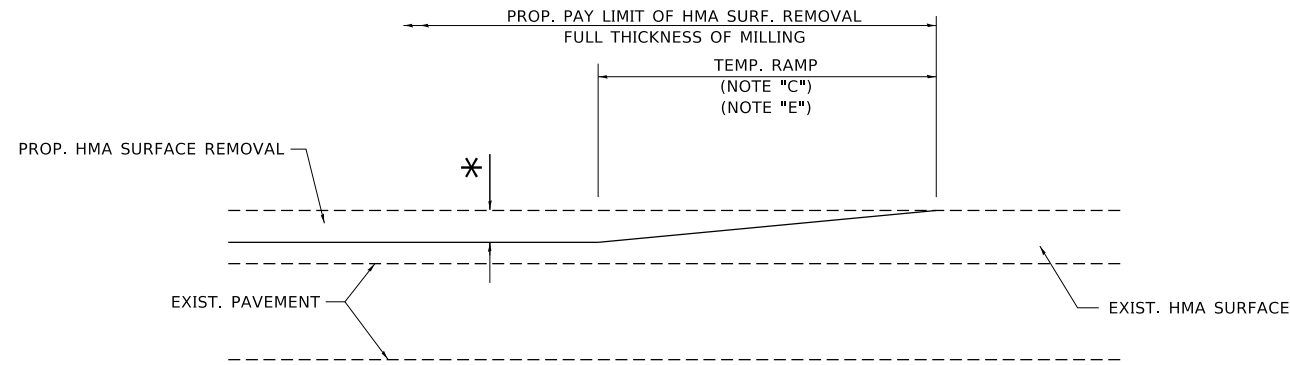
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

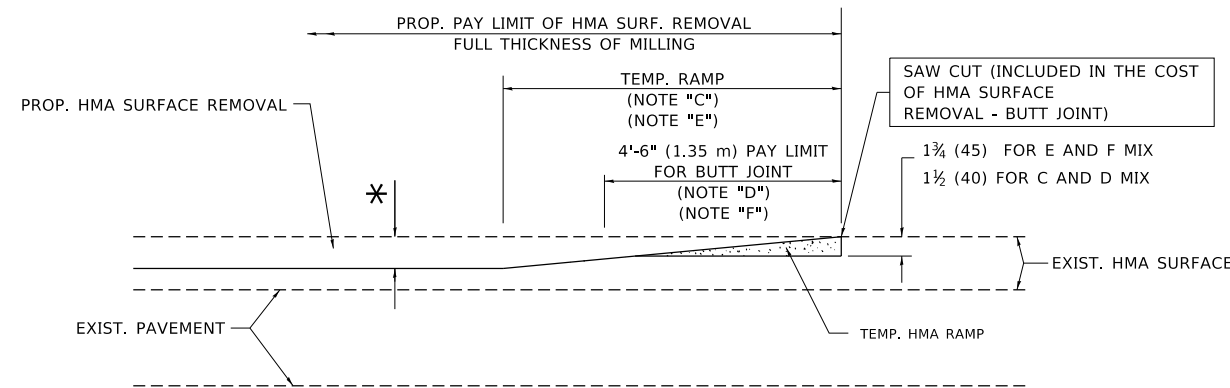
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1551	19-00081-00-RS	DU PAGE	27	20
BD600-06 (BD-24)			CONTRACT NO. 61G37	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

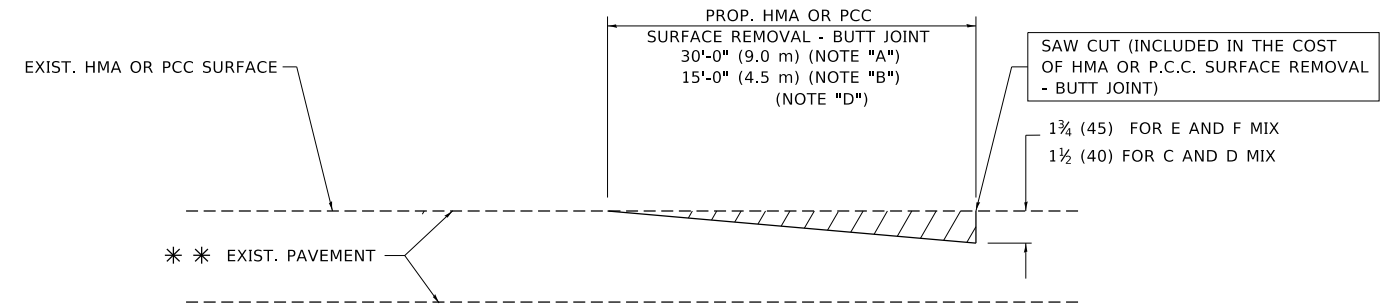


HMA CONSTRUCTED TEMPORARY RAMP

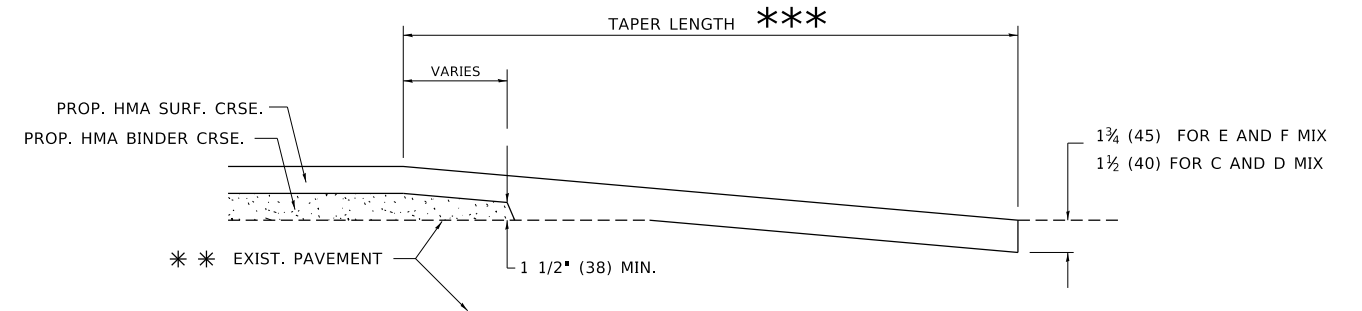
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

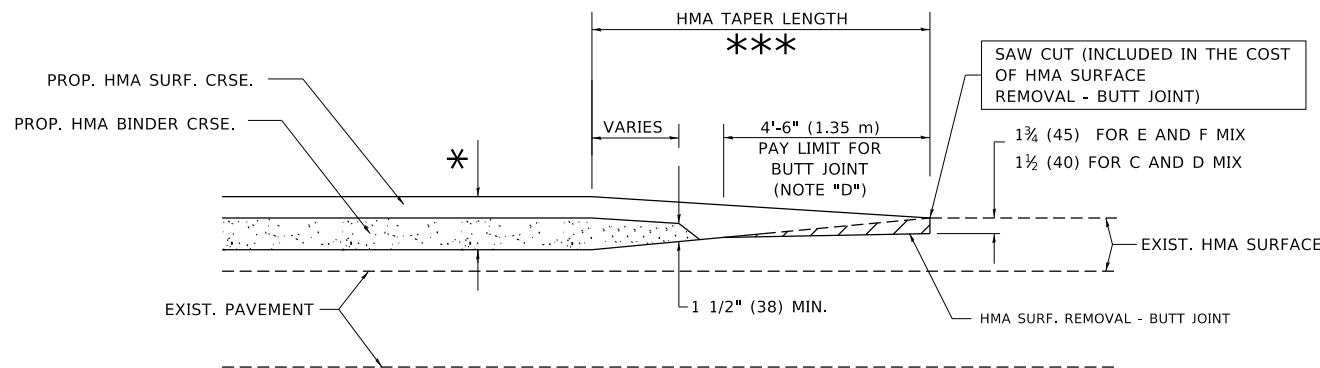
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

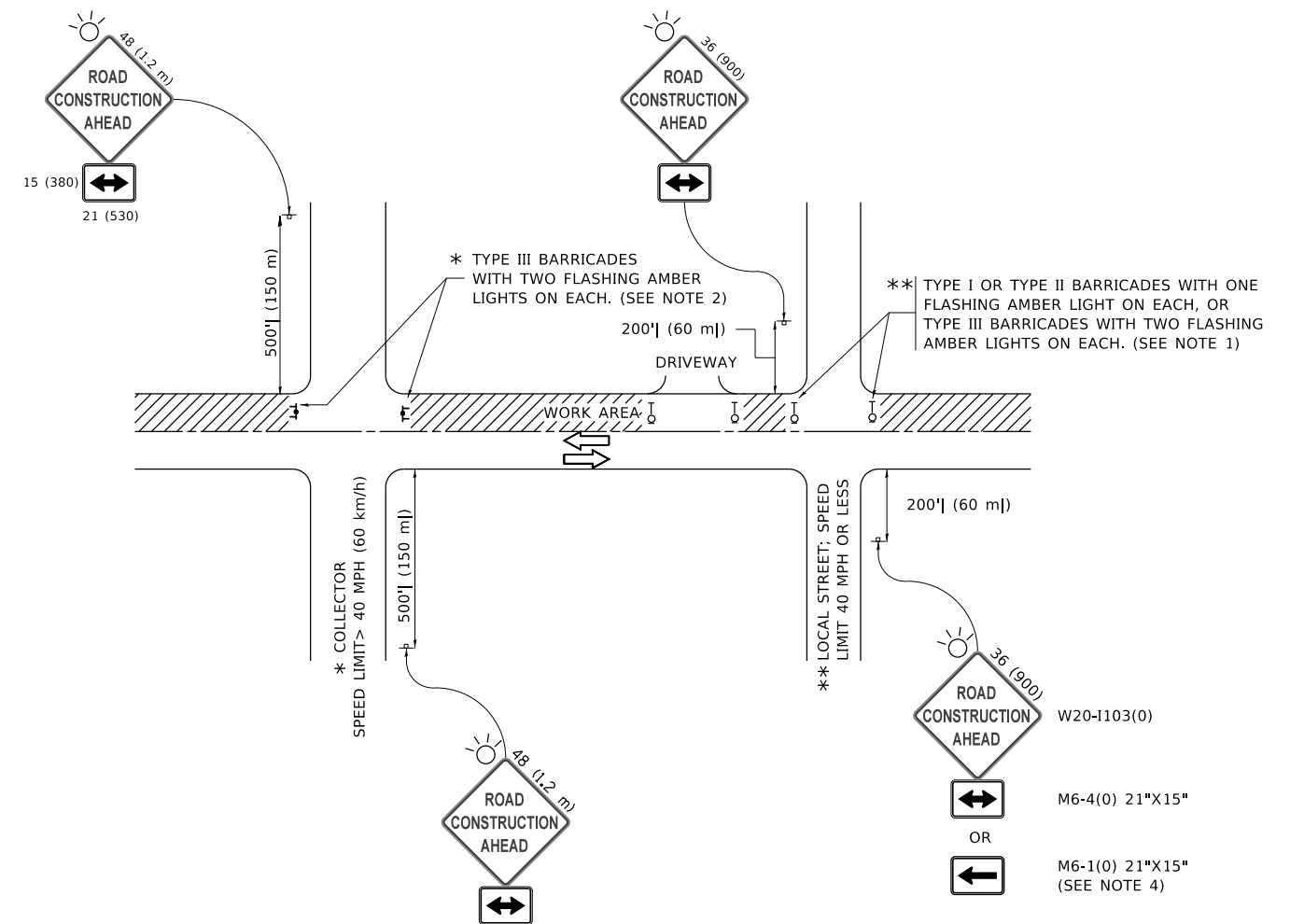
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	21
BD400-05 BD32			CONTRACT NO. 61G37	
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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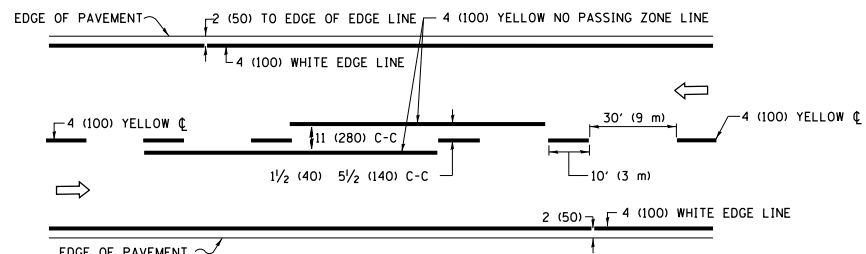
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PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

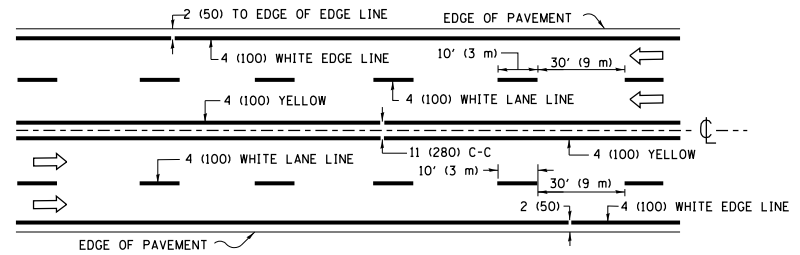
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

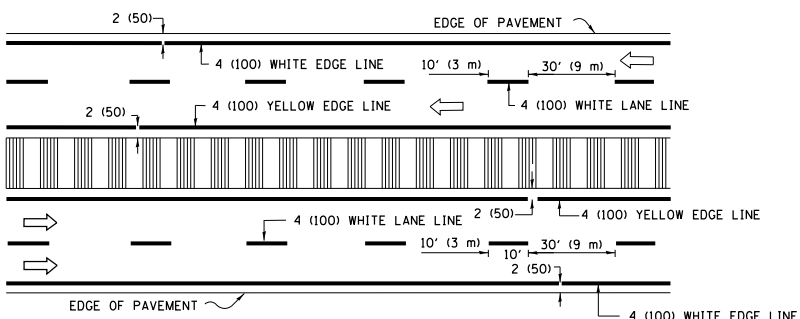
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	22
TC-10			CONTRACT NO. 61G37	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

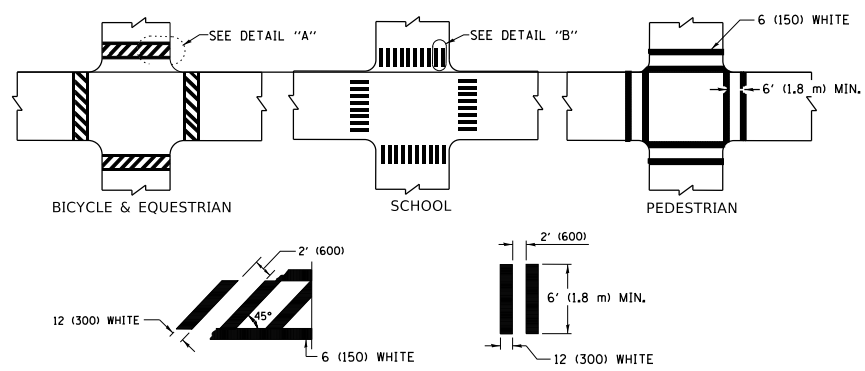


MULTI-LANE UNDIVIDED



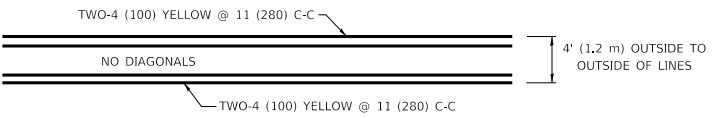
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

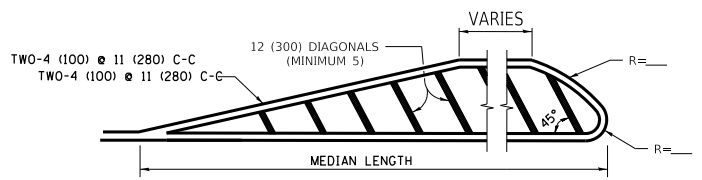


TYPICAL CROSSWALK MARKING

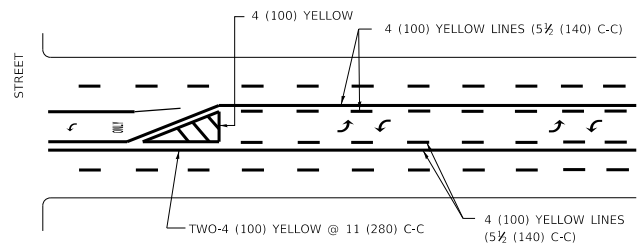
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



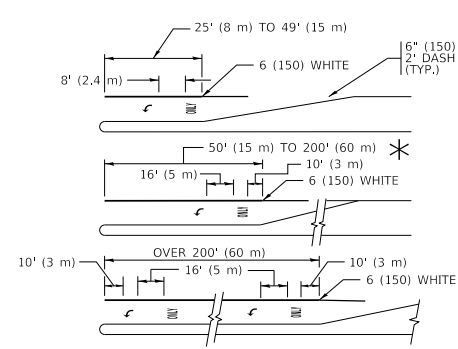
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

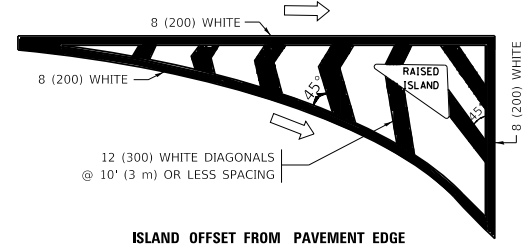


MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

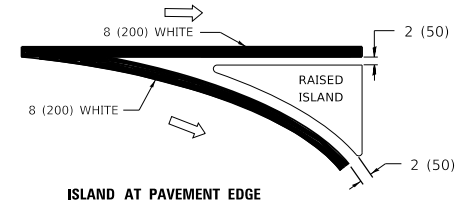


TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

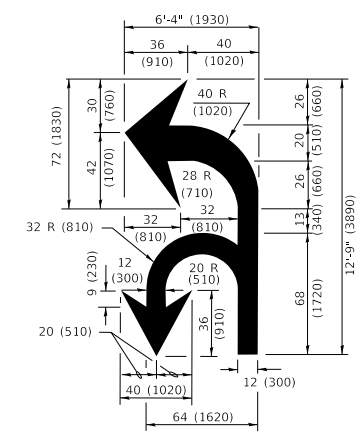
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



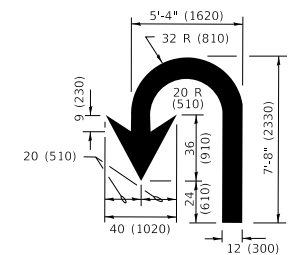
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 50.0000 "/in.	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	23
TC-13		CONTRACT NO. 61G37		
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

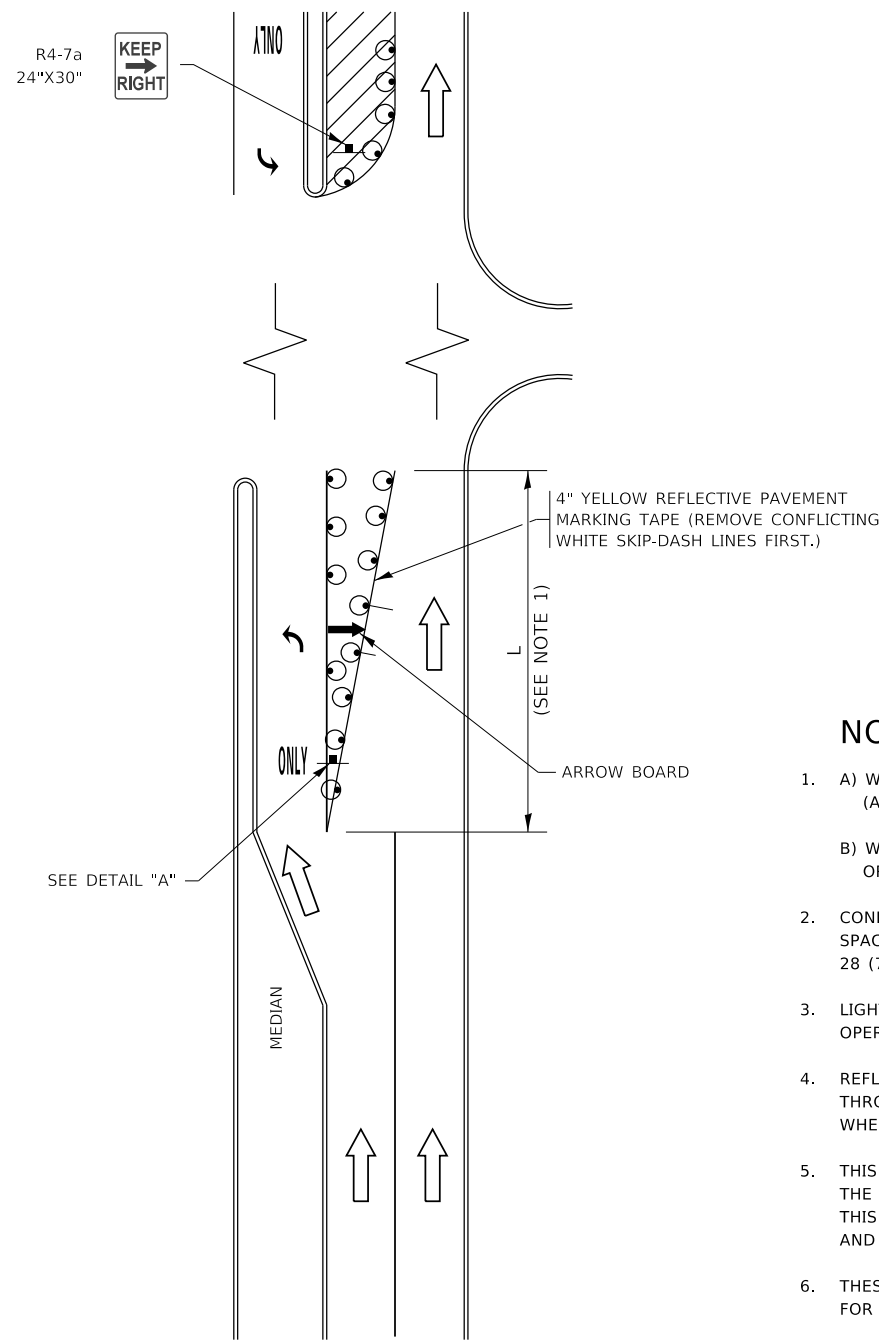


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

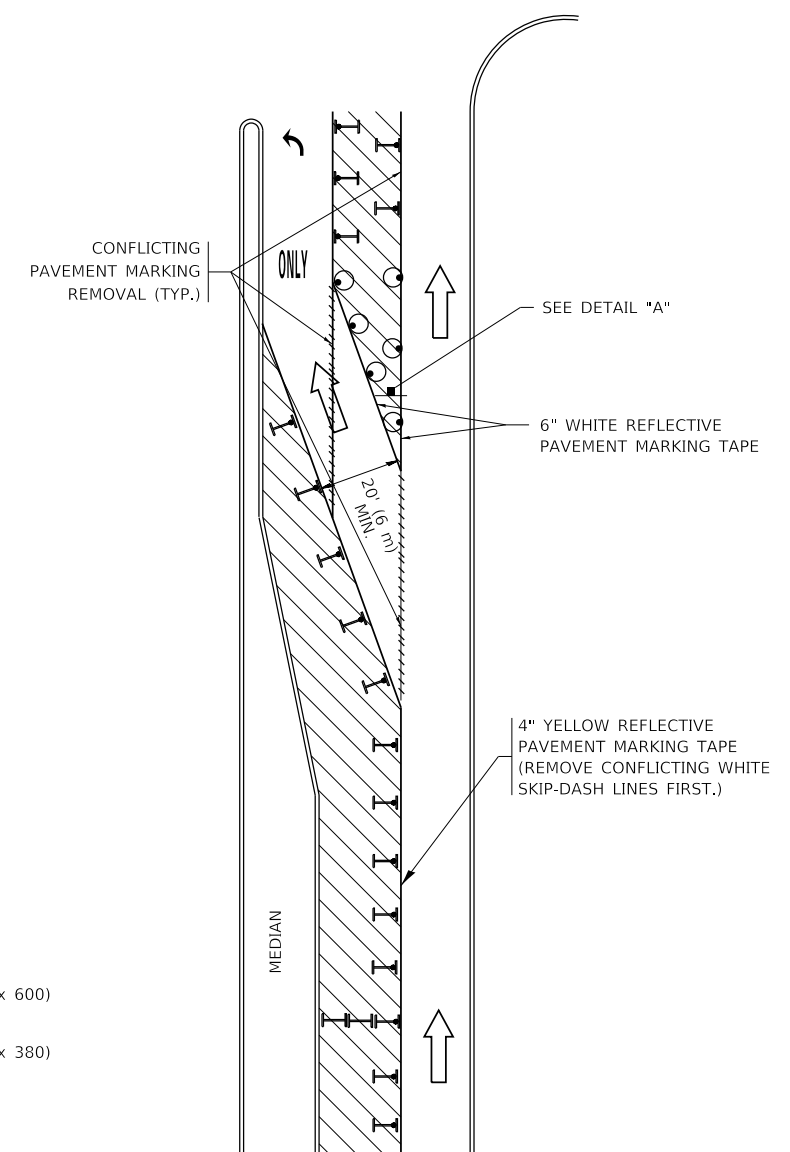
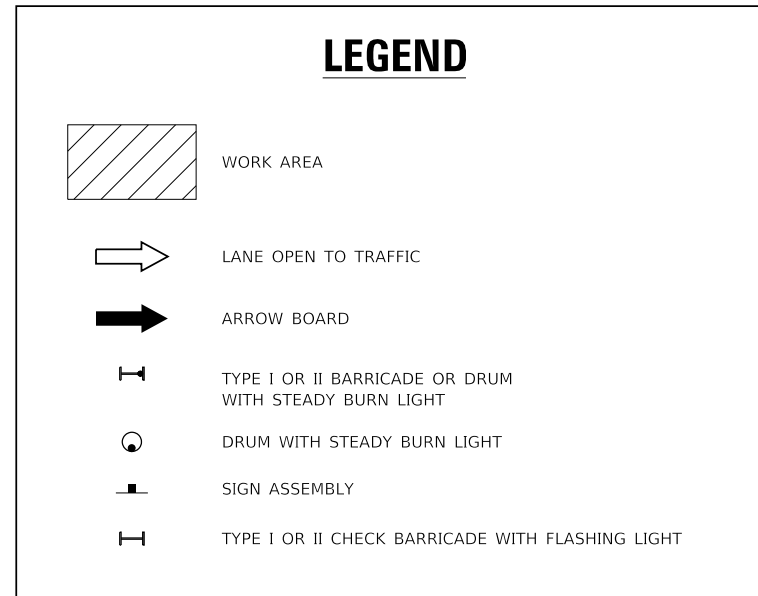
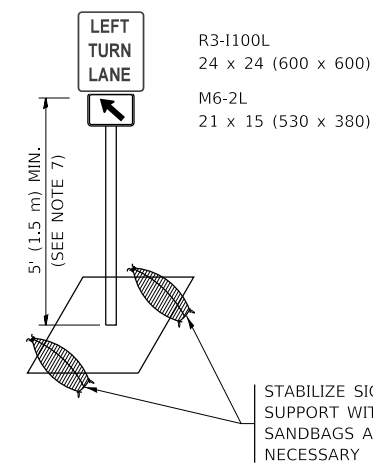


FIGURE 2



NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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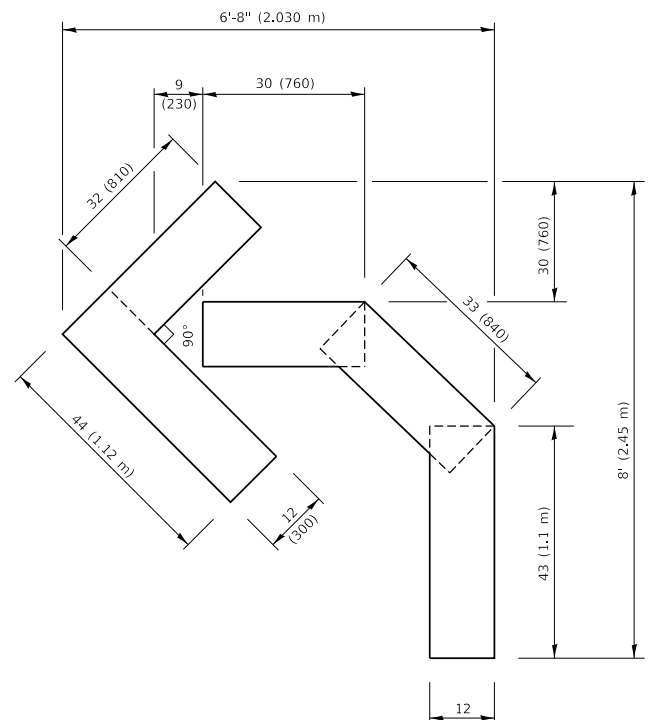
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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 50.0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

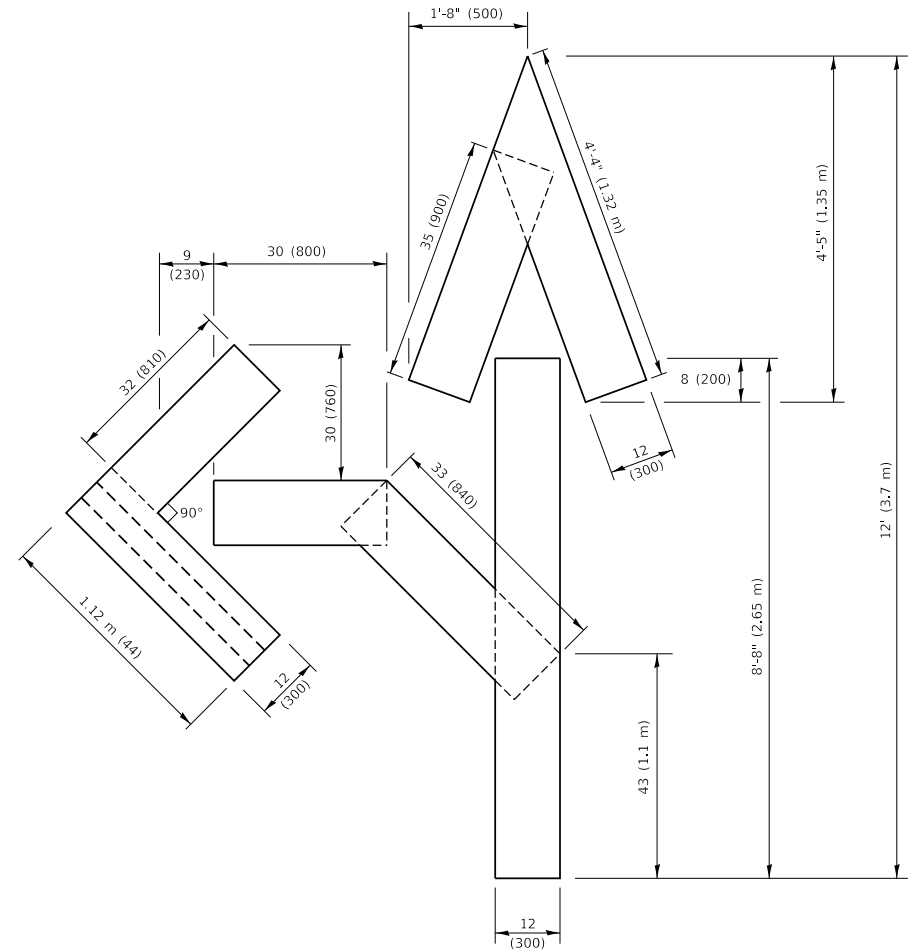
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

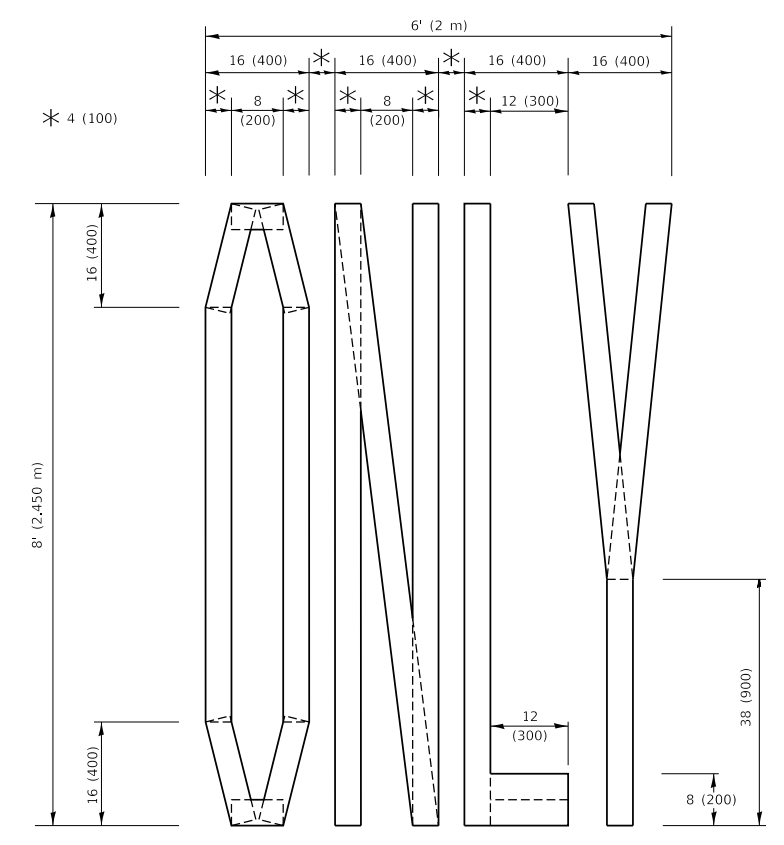
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	24
TC-14		CONTRACT NO. 61G37		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

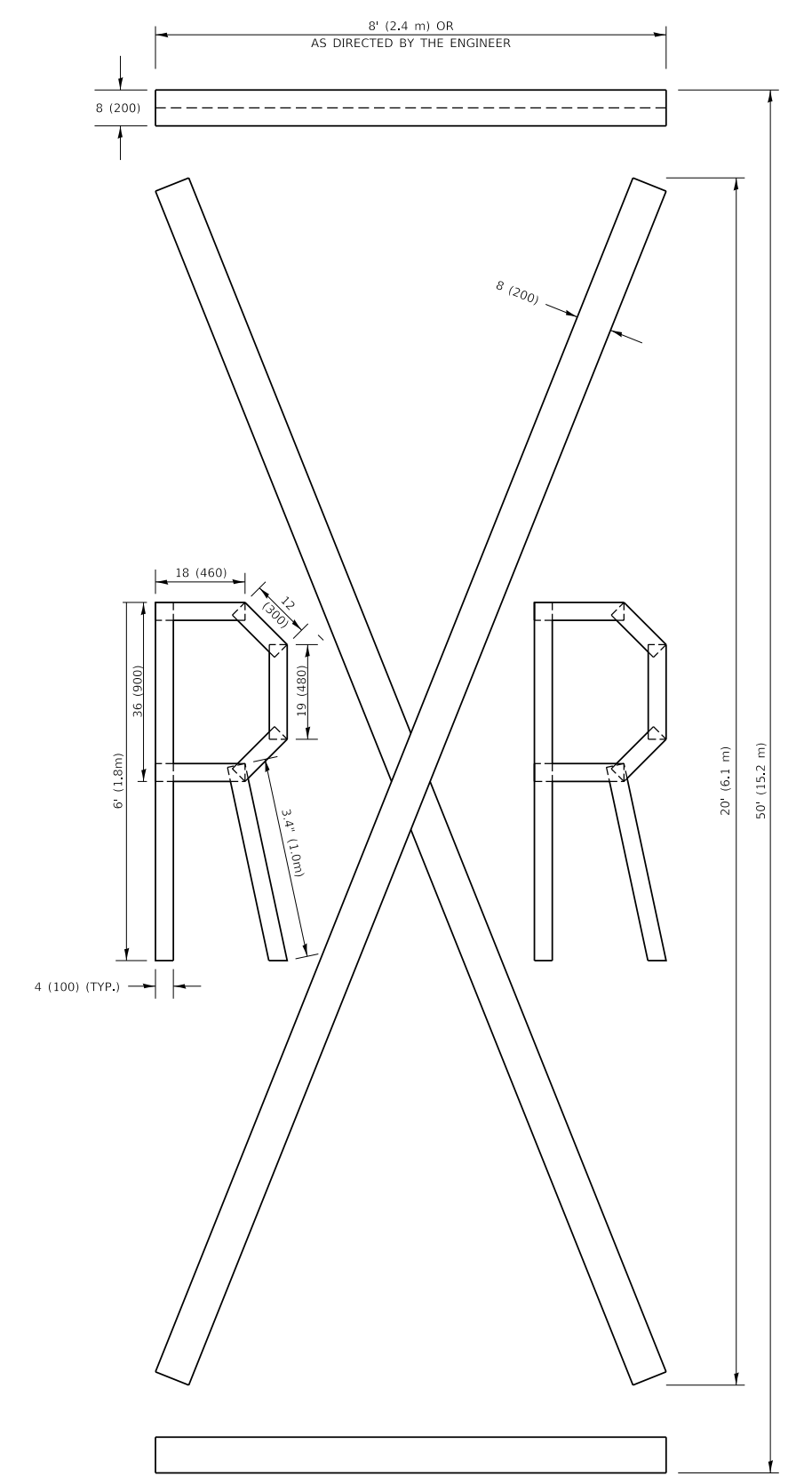


QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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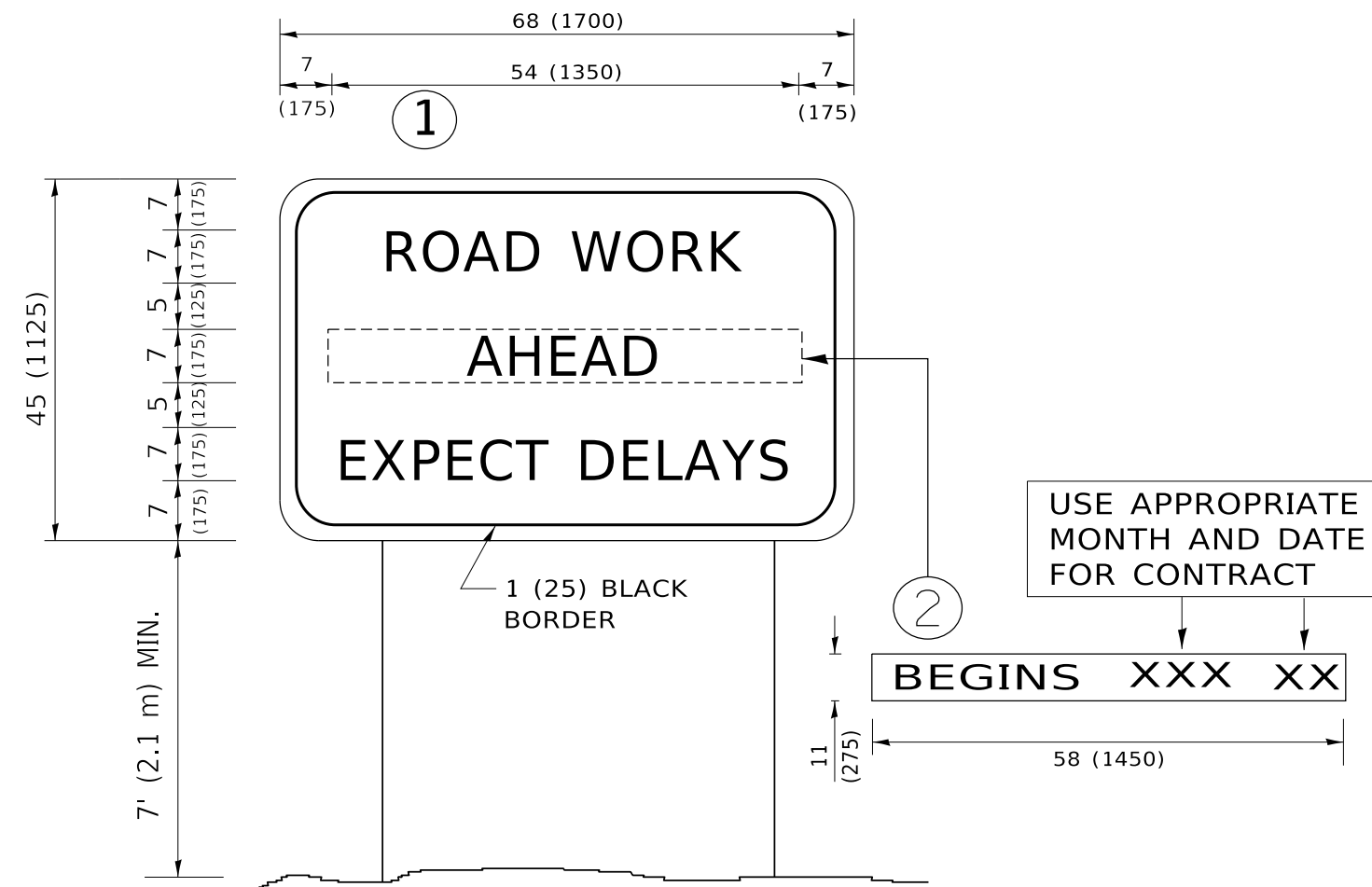
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PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	25
TC-16		CONTRACT NO. 61G37		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: D:\draft...
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PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

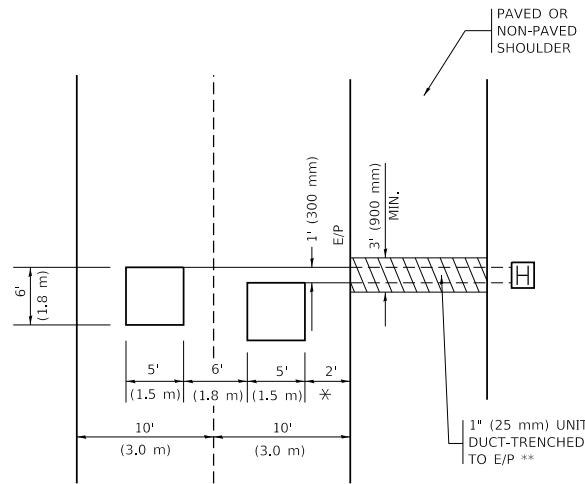
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	26
TC-22			CONTRACT NO. 61G37	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

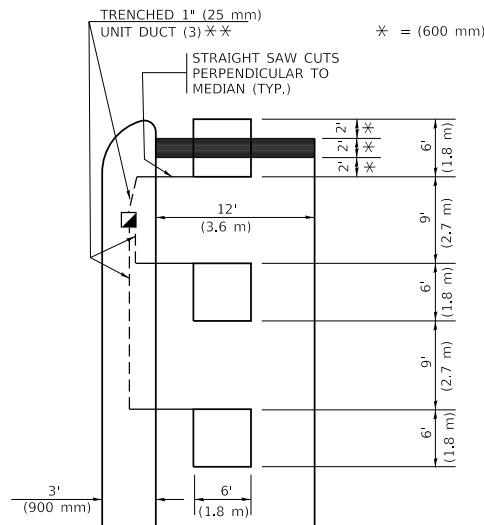


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

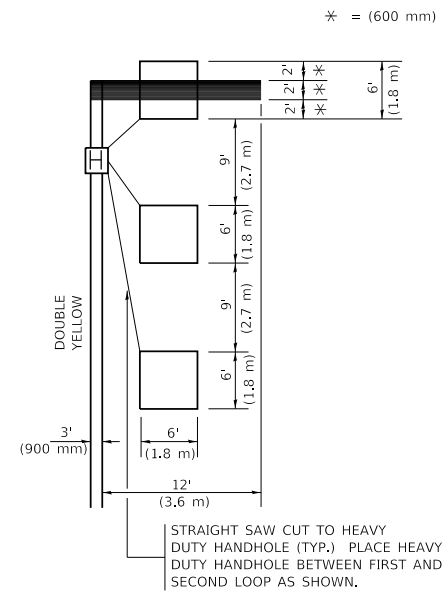


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

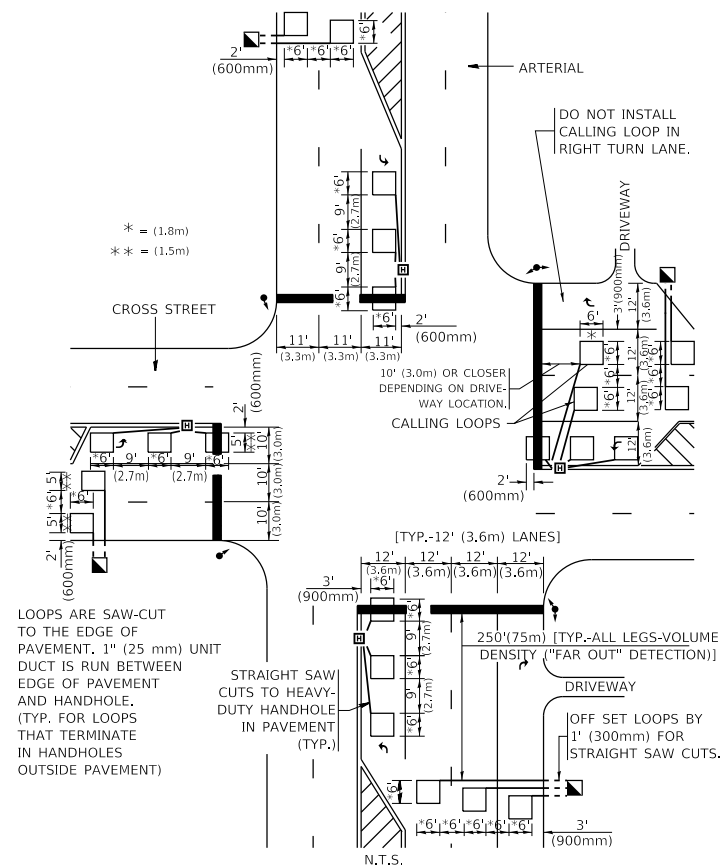
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



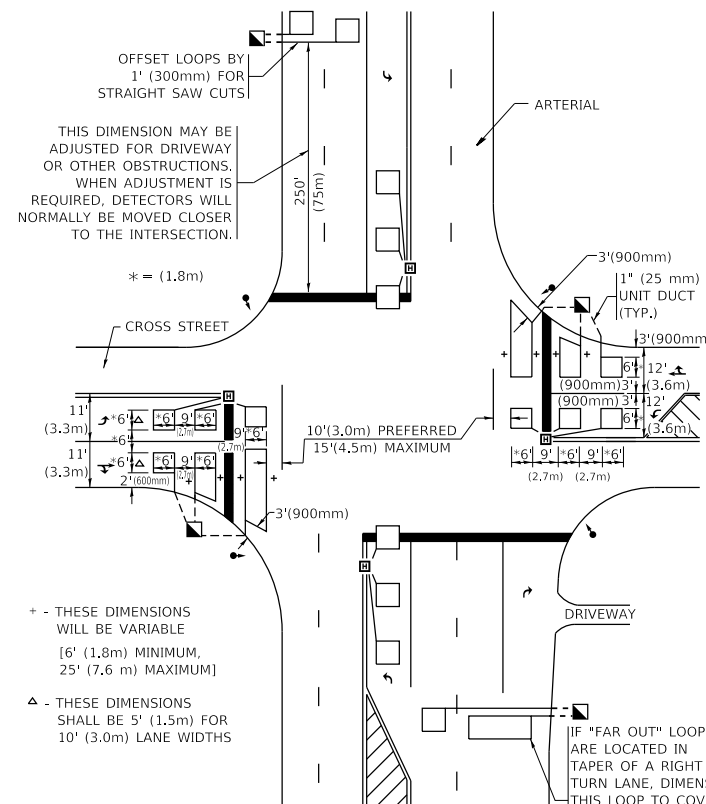
* = (1.8m)
 ** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

* - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
 Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
 N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

USER NAME = footemj	DESIGNED -	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 3/4/2019	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1551	19-00081-00-RS	DU PAGE	27	27
TS-07		CONTRACT NO. 61G37		
ILLINOIS FED. AID PROJECT				