

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION
**PLANS FOR PROPOSED
 FEDERAL AID HIGHWAY**

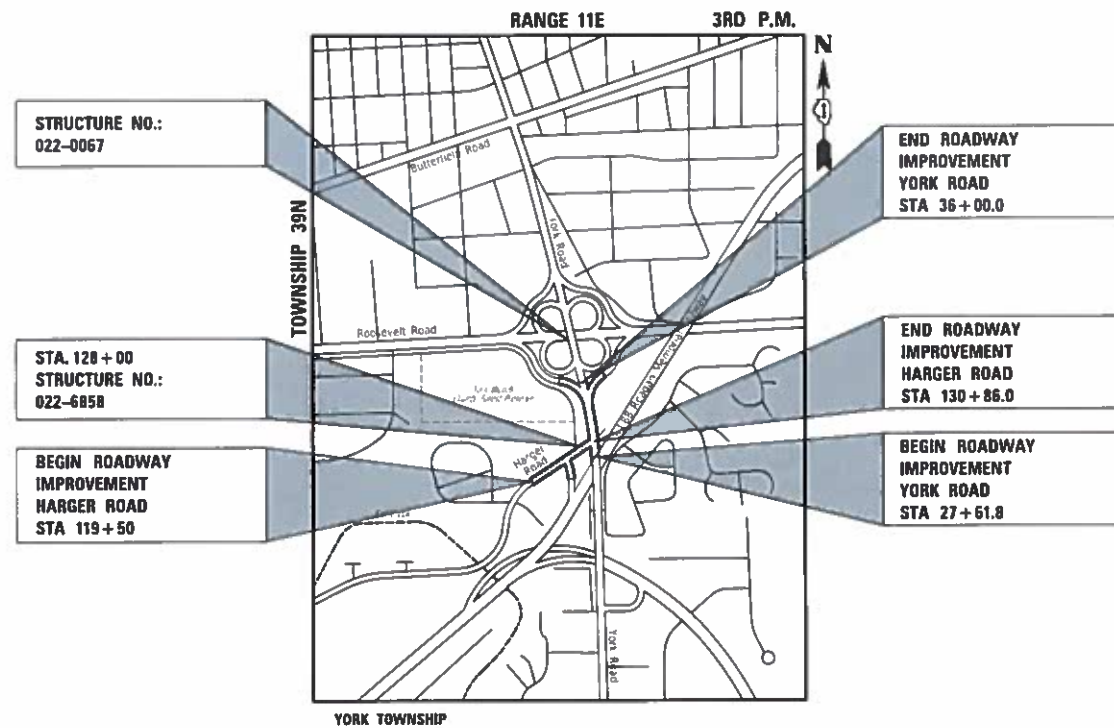
FAU RTE 2678 (NORTH YORK ROAD) AND
 FAU RTE 1446 (HARGER ROAD)
 INTERSECTION IMPROVEMENT, TRAFFIC SIGNAL,
 LIGHTING AND RETAINING WALL
 SECTION NO.: 17-00049-00-PV
 PROJECT NO.: BFN9 (911)
 VILLAGE OF OAK BROOK
 DUPAGE COUNTY
 C-91-007-18

FOR INDEX OF SHEETS AND
 LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

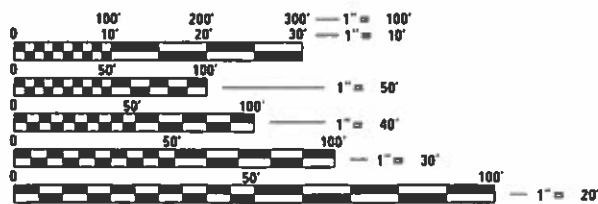
NORTH YORK ROAD
 DESIGN DESIGNATION = MINOR ARTERIAL
 POSTED SPEED = 45 MPH
 2017 ADT = 30,800 VPD

HARGER ROAD
 DESIGN DESIGNATION = MINOR COLLECTOR
 POSTED SPEED = 30 MPH
 2017 ADT = 7,700 VPD

| | | | | |
|---|---------------------------|------------------|---------------------|----------------|
| F.A.U. RTE: 17-00049-00-PV | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 1 |
| FED. ROAD DIST. NO. 1 ILLINOIS ILL. AID PROJECT | | | CONTRACT NO. 61G16 | |



LOCATION MAP
 NOT TO SCALE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

GROSS AND NET LENGTH:

YORK ROAD (STA 27+61.8 TO STA 36+00.0) - 838.2 FT (0.16 MI)
 HARGER ROAD (STA 119+50.0 TO STA 130+86.0) - 1,136.0 FT (0.22 MI)
 GROSS AND NET LENGTH - 1,974.2 FT (0.38 MI)

PLANS PREPARED BY:



STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

Approved December 2, 2019
 Village of Oak Brook, Director of Public Works

Approved December 23, 2019
 Forest Preserve District of DuPage County

Passed DEC 30, 2019
 District O.A. Engineer of Local Roads & Streets

Releasing for Bid Based on Limited Review
 January 6, 2020
 Anthony J. Rungty/CAS

APPLIES TO SHEETS 1-94;156-208

REGISTERED P.E., STATE OF ILLINOIS EXPIRES 11/30/2023

APPLIES TO SHEETS 95-117

REGISTERED P.E., STATE OF ILLINOIS EXPIRES 11/30/2023

APPLIES TO SHEETS 118-135

REGISTERED P.E., STATE OF ILLINOIS EXPIRES 11/30/2023

APPLIES TO SHEETS 136-155

REGISTERED P.E., STATE OF ILLINOIS EXPIRES 11/30/2023

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

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HIGHWAY STANDARDS

| | |
|-----------|---|
| 000001-07 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS |
| 424001-11 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 424011-04 | CORNER PARALLEL CURB RAMPS FOR SIDEWALKS |
| 424021-05 | DEPRESSED CORNER FOR SIDEWALKS |
| 442101-09 | CLASS B PATCHES |
| 542301-03 | PRECAST REINFORCED CONCRETE FLARED END SECTION |
| 542306-03 | PRECAST REINFORCED CONCRETE ELLIPTICAL FLARED END SECTION |
| 601001-05 | PIPE UNDERDRAINS |
| 601101-02 | CONCRETE HEADWALL FOR PIPE UNDERDRAINS |
| 602001-02 | CATCH BASIN TYPE A |
| 602011-02 | CATCH BASIN TYPE C |
| 602301-04 | INLET TYPE A |
| 602306-03 | INLET TYPE B |
| 602402-02 | PRECAST MANHOLE TYPE A 5' DIAMETER |
| 602406-10 | PRECAST MANHOLE TYPE A 6' DIAMETER |
| 602411-08 | PRECAST MANHOLE TYPE A 7' DIAMETER |
| 604001-05 | FRAME AND LIDS TYPE 1 |
| 604036-03 | GRATE TYPE 8 |
| 604051-04 | FRAME AND GRATE TYPE 11 |
| 604091-03 | FRAME AND GRATE TYPE 24 |
| 606001-07 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 606301-04 | PC CONCRETE ISLANDS AND MEDIANS |
| 630001-12 | STEEL PLATE BEAM GUARDRAIL |
| 631031-16 | TRAFFIC BARRIER TERMINAL, TYPE 6 |
| 667101-02 | PERMANENT SURVEY MARKERS |
| 701001-02 | OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY |
| 701006-05 | OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701101-05 | OFF-RD OPERATIONS MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701311-03 | LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY |
| 701326-04 | LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS ≥ 45 MPH |
| 701411-09 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH |
| 701426-09 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH |
| 701427-05 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |
| 701601-09 | URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN |
| 701701-10 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-06 | SIDEWALK, CORNER OR CROSSWALK CLOSURE |
| 701901-08 | TRAFFIC CONTROL DEVICES |
| 704001-08 | TEMPORARY CONCRETE BARRIER |
| 720001-01 | SIGN PANEL MOUNTING DETAILS |
| 720006-04 | SIGN PANEL ERECTION DETAILS |
| 720016-04 | MAST ARM MOUNTED STREET NAME SIGNS |
| 728001-01 | TELESCOPING STEEL SIGN SUPPORT |
| 729001-01 | APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS) |
| 780001-05 | TYPICAL PAVEMENT MARKINGS |
| 782006-01 | GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS |
| 805001-01 | ELECTRICAL SERVICE INSTALLATION DETAILS |
| 814001-03 | HANDHOLES |
| 814006-02 | DOUBLE HANDHOLES |
| 857001-01 | STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES |
| 862001-01 | UNINTERRUPTABLE POWER SUPPLY (UPS) |
| 873001-02 | TRAFFIC SIGNAL GROUNDING & BONDING |
| 877011-10 | STEEL COMB. MAST ARM ASSEMBLY AND POLE 16' THROUGH 55' |
| 878001-10 | CONCRETE FOUNDATION DETAILS |
| 880001-01 | SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION |
| 880006-01 | TRAFFIC SIGNAL MOUNTING DETAILS |
| 886001-01 | DETECTOR LOOP INSTALLATIONS |

GENERAL

- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL LIMIT ANY DROP-OFF BETWEEN LANES TO 2" DURING ANY OVERNIGHT PERIOD.
- PROPOSED LINES AND GRADES SHOWN ON THE CONSTRUCTION PLANS REPRESENT FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
- ALL DIMENSIONS, INCLUDING RADII, ARE GIVEN TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- WHEN REMOVING CURB AND GUTTER, PAVEMENT OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE THE PRECAUTIONS NECESSARY TO AVOID DAMAGE TO UNDERGROUND PUBLIC OR PRIVATE UTILITIES. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE ALLOWED.
- THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK (LUST) CLEANUPS OR THAT IS PRE -QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. WHERE NEW WORK IS PROPOSED TO MEET EXISTING FEATURES, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD CHECK ALL DIMENSIONS AND ELEVATIONS AND NOTIFY THE ENGINEER OF DISCREPANCIES BEFORE PROCEEDING WITH CONSTRUCTION.
- ALL EXCAVATION AND EMBANKMENT SHALL BE CONSTRUCTED 4 INCHES BELOW FINISHED GRADE LINE TO ALLOW FOR TOP SOIL PLACEMENT (WHERE TOP SOIL PLACEMENT IS SPECIFIED).
- ALL EXCESS MATERIAL SHALL BE DISPOSED OF OFFSITE ON THE DAY IT IS EXCAVATED OR REMOVED.
- ALL SYSTEMS TO DIVERT WATER AWAY FROM WORK AREAS SHALL BE INSTALLED PRIOR TO THE START OF WORK. WATER SHALL BE DIVERTED OR BYPASS PUMPED SO THAT FLOWING WATER IS NOT WITHIN EXCAVATION AREAS.
- THE STATIONS NOTED FOR DRIVEWAYS REFER TO THE POSITION OF THE CENTER OF THE DRIVEWAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYOUT OF EACH DRIVEWAY BASED UPON DESIGN DETAILS.
- THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER AND AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
- AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED, AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
- THE CONTRACTOR SHALL CONTACT THE IDOT D1 TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV AT LEAST 72 HOURS IN ADVANCE OF BEGINNING WORK.

STORM SEWERS, STRUCTURES, AND UTILITIES

16. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
17. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND APPLICABLE ARTICLES INCLUDED IN THE "STANDARD SPECIFICATIONS" INCLUDING, BUT NOT LIMITED TO, ARTICLES 105.07 AND 107.31. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING LOCAL AGENCIES MAINTAINING SANITARY SEWERS AND WATER MAINS TO VERIFY THE MATERIALS AND METHODS ALLOWED FOR THE ADJUSTMENT OR RELOCATION OF THEIR FACILITIES.
19. MATERIALS CONSIDERED SUITABLE BY THE ENGINEER FOR SALVAGE SHALL BE STORED WITHIN THE RIGHT-OF-WAY FOR LATER REMOVAL BY THE LOCAL AGENCY.
20. THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATION OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK MAY LEAD TO THE REJECTION OF THE STRUCTURE IN THE FIELD.
21. HMA OR CONCRETE PAVEMENT CROSSINGS REMOVED DUE TO STORM SEWER, WATER MAIN, SANITARY SEWER, OR CULVERT WORK SHALL NOT BE LEFT IN GRAVEL OVERNIGHT. THIS INCLUDES THE MAIN ROADS, SIDE STREETS, PRIVATE ENTRANCES, COMMERCIAL ENTRANCES AND PARKING AREAS. TEMPORARY HMA PAVEMENT MAY BE USED IN LIEU OF IMMEDIATE PAVEMENT REPLACEMENT.
22. THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE. FLAT TOPS AND CONES ARE TO BE TURNED SO THAT THE FRAME IS CLOSEST TO THE CENTER LINE OF THE ROAD, UNLESS OTHERWISE NOTED ON THE STRUCTURE IN THE PLANS. FRAMES AND LIDS FOR MANHOLES LOCATED ALONG THE SHARED USE PATH SHALL BE PLACED/ROTATED OUTSIDE OF THE PATH PAVEMENT.
23. FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACK FILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 WILL NOT BE ALLOWED.
24. THE CONTRACTOR SHALL DETERMINE WHEN FLAT TOP SLABS ARE REQUIRED ON DRAINAGE STRUCTURES.
25. ALL STRUCTURES DESIGNATED TO BE ADJUSTED, SPECIAL ADJUSTED OR RECONSTRUCTED SHALL HAVE NEW FRAMES, LIDS AND GRATES PROVIDED AT THE DIRECTION OF THE ENGINEER.
26. UNDERDRAINS SHALL BE INSTALLED AT THE OUTSIDE EDGE OF THE PAVEMENT TO DRAIN THE AGGREGATE SUBGRADE. TRANSVERSE UNDERDRAINS SHALL BE INSTALLED AT THE LOW POINTS OF THE PROFILE AND AT ANY UNDERCUTS DETERMINED IN THE FIELD.
27. STATIONS, OFFSETS, AND INVERT ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE CENTERLINE OF THE OUTLET END OF THE FLARED END SECTION. THE FLARED END SECTION SHALL BE INSTALLED AT THE SAME SLOPE AS THE OUTLET PIPE.
28. PIPE UNDERDRAINS SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED MINIMUM 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER. THE COST OF MAKING PIPE UNDERDRAINS CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PIPE UNDERDRAINS.
29. BACKFILLING STORM SEWER CONSTRUCTED UNDER THE ROADWAY SPECIFIED UNDER ART. 550.07(b, c) OF THE SSRBC WILL NOT BE ALLOWED.

SIGNING, STRIPING & LANDSCAPING

30. ALL UNUSED SIGNS SHALL BE RETURNED TO THE OWNER AT THE DIRECTION OF THE ENGINEER.
31. WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SEEDED/SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS" AND SPECIAL PROVISIONS.
32. TEMPORARY FENCE SHALL BE PLACED OUTSIDE OF THE DRIPLINE OF ALL TREES THAT ARE TO BE PROTECTED UNLESS OTHERWISE DIRECTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR MORE DETAIL.
33. THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. MAJOR ROOTS OF A TREE THAT ARE TO REMAIN IN PLACE EXTENDING INTO THE EXCAVATION AREAS AT AN ELEVATION THAT WOULD INTERFERE WITH ANY PORTION OF THE PLANNED CONSTRUCTION SHALL BE SAWCUT AT A POINT IMMEDIATELY OUTSIDE OF THE EXCAVATION AREA IN A MANNER THAT WILL CAUSE THE LEAST AMOUNT OF DAMAGE TO THE REMAINING TREE STRUCTURE.
34. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR, SHALL BE RESTORED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.
35. THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.

COMMITMENTS

36. FAA FORM 7460-2 WILL NEED TO BE FILED POST CONSTRUCTION OF THE RELOCATED LIGHT POLES ALONG YORK ROAD.
37. ALL WORK WITHIN FOREST PRESERVE PROPERTY NORTH OF HARGER ROAD AND WEST OF TIMBER EDGE DRIVE NEED TO BE COMPLETED IN LESS THAN 6 MONTHS.

POINTS OF CONTACT

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| | |
|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SHEET 2 OF 2 SHEETS

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 3 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 1,808 | 1,808 | | | | | |
| ^ 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 541 | 541 | | | | | |
| ^ 20101000 | TEMPORARY FENCE | FOOT | 4,000 | 4,000 | | | | | |
| ^ 20101200 | TREE ROOT PRUNING | EACH | 55 | 55 | | | | | |
| ^ 20101300 | TREE PRUNING (1 TO 10 INCH DIAMETER) | EACH | 43 | 43 | | | | | |
| ^ 20101350 | TREE PRUNING (OVER 10 INCH DIAMETER) | EACH | 12 | 12 | | | | | |
| * 20200100 | EARTH EXCAVATION | CU YD | 13,843 | 13,843 | | | | | |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 6,897 | 6,897 | | | | | |
| 20400800 | FURNISHED EXCAVATION | CU YD | 19,530 | 19,530 | | | | | |
| 20700220 | POROUS GRANULAR EMBANKMENT | CU YD | 8,679 | | 8,679 | | | | |
| 20800150 | TRENCH BACKFILL | CU YD | 3,522 | 3,522 | | | | | |
| 21001000 | GEOTECHNICAL FABRIC FOR GROUND STABILIZATION | SQ YD | 3,548 | 3,548 | | | | | |
| 21101505 | TOPSOIL EXCAVATION AND PLACEMENT | CU YD | 1,949 | 1,949 | | | | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ 25000210 | SEEDING, CLASS 2A | ACRE | 2.25 | 2.25 | | | | | |
| ^ 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 298 | 298 | | | | | |
| ^ 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 298 | 298 | | | | | |
| ^ 25100630 | EROSION CONTROL BLANKET | SQ YD | 12,332 | 12,332 | | | | | |
| ^ 25200110 | SODDING, SALT TOLERANT | SQ YD | 2,149 | 2,149 | | | | | |
| ^ 25200200 | SUPPLEMENTAL WATERING | UNIT | 5 | 5 | | | | | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 674 | 674 | | | | | |
| 28000305 | TEMPORARY DITCH CHECKS | FOOT | 420 | 420 | | | | | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 3,308 | 3,308 | | | | | |
| 28000510 | INLET FILTERS | EACH | 41 | 41 | | | | | |
| 28001100 | TEMPORARY EROSION CONTROL BLANKET | SQ YD | 32,588 | 32,588 | | | | | |
| * 28001200 | TEMPORARY HEAVY DUTY EROSION CONTROL BLANKET | SQ YD | 125 | 125 | | | | | |
| 28100105 | STONE RIPRAP, CLASS A3 | SQ YD | 35 | 35 | | | | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|---|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| 28200200 | FILTER FABRIC | SQ YD | 42 | 42 | | | | | |
| * 30300001 | AGGREGATE SUBGRADE IMPROVEMENT | CU YD | 1,360 | 1,360 | | | | | |
| * 30300112 | AGGREGATE SUBGRADE IMPROVEMENT 12" | SQ YD | 11,180 | 11,180 | | | | | |
| 35101400 | AGGREGATE BASE COURSE, TYPE B | TON | 1,309 | 550 | 759 | | | | |
| 35400400 | PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 9" | SQ YD | 302 | 302 | | | | | |
| * 40201000 | AGGREGATE FOR TEMPORARY ACCESS | TON | 20 | 20 | | | | | |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 5,014 | 5,014 | | | | | |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 4,600 | 4,600 | | | | | |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 20 | 20 | | | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 96 | 96 | | | | | |
| 40600985 | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | SQ YD | 339 | 339 | | | | | |
| * 40602985 | HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 | TON | 99 | 99 | | | | | |
| * 40603085 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | TON | 3,989 | 3,989 | | | | | |

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| DESIGNED - PK | REVISED - 1/17/2020 |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SHEET 3 OF 21 SHEETS STA. TO STA.

| | | | | |
|---|---------------------------|------------------|---------------------|----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 6 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| * 40603200 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 | TON | 274 | 274 | | | | | |
| * 40604060 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 | TON | 348 | | 348 | | | | |
| * 40604062 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70 | TON | 832 | 832 | | | | | |
| * 40604172 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70 | TON | 639 | 639 | | | | | |
| 42001300 | PROTECTIVE COAT | SQ YD | 2,147 | 2,147 | | | | | |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SQ FT | 389 | | 389 | | | | |
| 42400800 | DETECTABLE WARNINGS | SQ FT | 81 | | 81 | | | | |
| * 44000100 | PAVEMENT REMOVAL | SQ YD | 7,085 | 7,085 | | | | | |
| 44000151 | HOT-MIX ASPHALT SURFACE REMOVAL, 1/2" | SQ YD | 2,332 | 2,332 | | | | | |
| * 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 140 | 140 | | | | | |
| * 44003100 | MEDIAN REMOVAL | SQ FT | 1,808 | 1,808 | | | | | |
| * 44004250 | PAVED SHOULDER REMOVAL | SQ YD | 1,726 | 1,726 | | | | | |
| 44200966 | CLASS B PATCHES, TYPE I, 10 INCH | SQ YD | 50 | 50 | | | | | |

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| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

| | | | |
|---------|--------------|------|---------|
| SHEET 4 | OF 21 SHEETS | STA. | TO STA. |
|---------|--------------|------|---------|

| | | | | |
|---|---------------------------|------------------|---------------------|----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 7 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| 44200970 | CLASS B PATCHES, TYPE II, 10 INCH | SQ YD | 50 | 50 | | | | | |
| 44200974 | CLASS B PATCHES, TYPE III, 10 INCH | SQ YD | 50 | 50 | | | | | |
| 44200976 | CLASS B PATCHES, TYPE IV, 10 INCH | SQ YD | 50 | 50 | | | | | |
| 45200100 | JOINT OR CRACK ROUTING (PC CONCRETE PAVEMENT AND SHOULDER) | FOOT | 7,396 | 7,396 | | | | | |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 224 | 224 | | | | | |
| 48203021 | HOT-MIX ASPHALT SHOULDERS, 6" | SQ YD | 732 | 732 | | | | | |
| 50200100 | STRUCTURE EXCAVATION | CU YD | 8,213 | | 8,213 | | | | |
| 50300225 | CONCRETE STRUCTURES | CU YD | 703.4 | | 703.4 | | | | |
| 50300254 | RUBBED FINISH | SQ FT | 4,358 | | 4,358 | | | | |
| 50500505 | STUD SHEAR CONNECTORS | EACH | 291 | | 291 | | | | |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 112,306 | 196 | 112,110 | | | | |
| 50800515 | BAR SPLICERS | EACH | 130 | | 130 | | | | |
| 51500100 | NAME PLATES | EACH | 1 | | 1 | | | | |

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| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SHEET 5 OF 21 SHEETS STA. TO STA.

| | | | | |
|--------------------------------|---------------------------|------------------|---------------------|----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 8 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 61G16 | |
| FED. AID PROJECT | | | | |

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|---|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| 52200020 | TEMPORARY SOIL RETENTION SYSTEM | SQ FT | 2,014 | | 2,014 | | | | |
| 52200100 | FURNISHING SOLDIER PILES (HP SECTION) | FOOT | 643 | | 643 | | | | |
| 52200150 | DRIVING SOLDIER PILES | FOOT | 643 | | 643 | | | | |
| 52200250 | UNTREATED TIMBER LAGGING | SQ FT | 1,599 | | 1,599 | | | | |
| 52200900 | CONCRETE STRUCTURES (RETAINING WALL) | CU YD | 91.1 | | 91.1 | | | | |
| 54213657 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12" | EACH | 1 | 1 | | | | | |
| 54213663 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18" | EACH | 1 | 1 | | | | | |
| 54213669 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24" | EACH | 3 | 3 | | | | | |
| 550A0090 | STORM SEWERS, CLASS A, TYPE 1 18" | FOOT | 17 | 17 | | | | | |
| 550A0340 | STORM SEWERS, CLASS A, TYPE 2 12" | FOOT | 879 | 879 | | | | | |
| 550A0380 | STORM SEWERS, CLASS A, TYPE 2 18" | FOOT | 603 | 603 | | | | | |
| 550A0410 | STORM SEWERS, CLASS A, TYPE 2 24" | FOOT | 417 | 417 | | | | | |
| 550A0430 | STORM SEWERS, CLASS A, TYPE 2 30" | FOOT | 534 | 534 | | | | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| 550A0680 | STORM SEWERS, CLASS A, TYPE 3 18" | FOOT | 149 | 149 | | | | | |
| 550A0710 | STORM SEWERS, CLASS A, TYPE 3 24" | FOOT | 367 | 367 | | | | | |
| 55100100 | STORM SEWER REMOVAL 4" | FOOT | 34 | 34 | | | | | |
| 55100400 | STORM SEWER REMOVAL 10" | FOOT | 28 | 28 | | | | | |
| 55100500 | STORM SEWER REMOVAL 12" | FOOT | 329 | 329 | | | | | |
| 55100900 | STORM SEWER REMOVAL 18" | FOOT | 236 | 236 | | | | | |
| 55101200 | STORM SEWER REMOVAL 24" | FOOT | 446 | 446 | | | | | |
| 59100100 | GEOCOMPOSITE WALL DRAIN | SQ YD | 1,066 | | 1,066 | | | | |
| 60100060 | CONCRETE HEADWALLS FOR PIPE DRAINS | EACH | 2 | 2 | | | | | |
| 60108204 | PIPE UNDERDRAINS, TYPE 2, 4" | FOOT | 3,126 | 3,126 | | | | | |
| 60204505 | CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 8 GRATE | EACH | 14 | 14 | | | | | |
| 60204805 | CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 11 FRAME AND GRATE | EACH | 3 | 3 | | | | | |
| 60205040 | CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE | EACH | 12 | 12 | | | | | |

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| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|---|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| 60221100 | MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 16 | 16 | | | | | |
| 60223800 | MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 5 | 5 | | | | | |
| 60224446 | MANHOLES, TYPE A, 7'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 3 | 3 | | | | | |
| 60236200 | INLETS, TYPE A, TYPE 8 GRATE | EACH | 1 | 1 | | | | | |
| 60255500 | MANHOLES TO BE ADJUSTED | EACH | 3 | 3 | | | | | |
| 60257900 | MANHOLES TO BE RECONSTRUCTED | EACH | 1 | 1 | | | | | |
| 60265700 | VALVE VAULTS TO BE ADJUSTED | EACH | 1 | 1 | | | | | |
| 60500040 | REMOVING MANHOLES | EACH | 4 | 4 | | | | | |
| 60500060 | REMOVING INLETS | EACH | 3 | 3 | | | | | |
| 60600605 | CONCRETE CURB, TYPE B | FOOT | 175 | 175 | | | | | |
| * 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 1,050 | 1,050 | | | | | |
| * 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 2,411 | 2,411 | | | | | |
| 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH | SQ FT | 4,781 | 4,781 | | | | | |

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| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|--------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| 60619600 | CONCRETE MEDIAN, TYPE SB-6.12 | SQ FT | 3,719 | 3,719 | | | | | |
| ^ 63000001 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | FOOT | 665 | 665 | | | | | |
| ^ 63100085 | TRAFFIC BARRIER TERMINAL, TYPE 6 | EACH | 2 | 2 | | | | | |
| ^* 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 1 | 1 | | | | | |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 1,272 | 1,272 | | | | | |
| 66700205 | PERMANENT SURVEY MARKERS, TYPE I | EACH | 3 | 3 | | | | | |
| ^* 66900200 | NON-SPECIAL WASTE DISPOSAL | CU YD | 15,100 | 15,100 | | | | | |
| ^* 66900530 | SOIL DISPOSAL ANALYSIS | EACH | 1 | 1 | | | | | |
| ^* 66901001 | REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN | LSUM | 1 | 1 | | | | | |
| ^* 66901003 | REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT | LSUM | 1 | 1 | | | | | |
| ^ 66901006 | REGULATED SUBSTANCES MONITORING | CAL DA | 40 | 40 | | | | | |
| ^* 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 10 | 10 | | | | | |
| 67100100 | MOBILIZATION | LSUM | 1 | 1 | | | | | |

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| DRAWN - MC | REVISED - |
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| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SHEET 9 OF 21 SHEETS STA. TO STA.

| | | | | |
|---|---------------------------|------------------|---------------------|-----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 12 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|--------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| * 70107005 | PAVEMENT MARKING BLACKOUT TAPE, 5" | FOOT | 3,800 | 3,800 | | | | | |
| * 70107016 | PAVEMENT MARKING BLACKOUT TAPE, 16" | FOOT | 290 | 290 | | | | | |
| 70107025 | CHANGEABLE MESSAGE SIGN | CAL DA | 420 | 420 | | | | | |
| * 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 3,585 | 3,585 | | | | | |
| * 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SQ FT | 1,195 | 1,195 | | | | | |
| * 70300904 | PAVEMENT MARKING TAPE, TYPE IV 4" | FOOT | 34,163 | 34,163 | | | | | |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 2,165 | 2,165 | | | | | |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 4,355 | 4,355 | | | | | |
| 70600241 | IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 5 | 5 | | | | | |
| 70600341 | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 10 | 10 | | | | | |
| ^ 72000100 | SIGN PANEL - TYPE 1 | SQ FT | 144 | 118 | | 26 | | | |
| 72400100 | REMOVE SIGN PANEL ASSEMBLY - TYPE A | EACH | 7 | 7 | | | | | |
| 72400200 | REMOVE SIGN PANEL ASSEMBLY - TYPE B | EACH | 2 | 2 | | | | | |

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SHEET 10 OF 21 SHEETS STA. TO STA.

| | | | | |
|---|---------------------------|------------------|---------------------|-----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 13 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

CONTRACT NO. 61G16

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|--|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| 72400310 | REMOVE SIGN PANEL - TYPE 1 | SQ FT | 9 | 9 | | | | | |
| 72400330 | REMOVE SIGN PANEL - TYPE 3 | SQ FT | 180 | 180 | | | | | |
| ^ 72400500 | RELOCATE SIGN PANEL ASSEMBLY - TYPE A | EACH | 12 | 12 | | | | | |
| ^ 72400600 | RELOCATE SIGN PANEL ASSEMBLY - TYPE B | EACH | 2 | 2 | | | | | |
| ^ 72400710 | RELOCATE SIGN PANEL - TYPE 1 | SQ FT | 2 | 2 | | | | | |
| ^ 72400730 | RELOCATE SIGN PANEL - TYPE 3 | SQ FT | 180 | 180 | | | | | |
| ^ 72501000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 1 | 1 | | | | | |
| ^ 72700100 | STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY | POUND | 274 | 274 | | | | | |
| ^ 72800100 | TELESCOPING STEEL SIGN SUPPORT | FOOT | 435 | 435 | | | | | |
| ^ 73400100 | CONCRETE FOUNDATIONS | CU YD | 3 | 3 | | | | | |
| ^ 73500300 | REMOVE AND RELOCATE GROUND MOUNTED SIGN SUPPORT | EACH | 2 | 2 | | | | | |
| 73700200 | REMOVE CONCRETE FOUNDATION - GROUND MOUNT | EACH | 2 | 2 | | | | | |
| ^ 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 281 | 281 | | | | | |

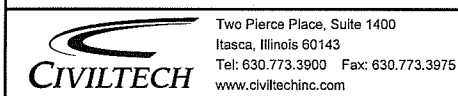
^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|--------------|--|------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 5,717 | 5,717 | | | | | |
| ^ 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 1,138 | 1,138 | | | | | |
| ^ 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8" | FOOT | 608 | 608 | | | | | |
| ^ 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 796 | 796 | | | | | |
| ^ 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 152 | 152 | | | | | |
| ^ 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 57 | 57 | | | | | |
| ^ 78200011 | BARRIER WALL REFLECTORS, TYPE C | EACH | 176 | 176 | | | | | |
| ^ 80400100 | ELECTRIC SERVICE INSTALLATION | EACH | 1 | | | | 1 | | |
| ^ * 80400200 | ELECTRIC UTILITY SERVICE CONNECTION | LSUM | 1 | | | | 1 | | |
| ^ 80500010 | SERVICE INSTALLATION - GROUND MOUNTED | EACH | 1 | | | 1 | | | |
| ^ 81028170 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 1" DIA. | FOOT | 100 | | | | 100 | | |
| ^ 81028200 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 147 | | | 147 | | | |
| ^ 81028210 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA. | FOOT | 535 | | | | 535 | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|---|------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ 81028220 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA. | FOOT | 88 | | | 88 | | | |
| ^ 81028240 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA. | FOOT | 368 | | | 368 | | | |
| ^ 81028730 | UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/4" DIA. | FOOT | 40 | | | | 40 | | |
| ^ 81200100 | CONDUIT EMBEDDED IN STRUCTURE, 1" DIA., GALVANIZED STEEL | FOOT | 239 | | | | 239 | | |
| ^ 81300320 | JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 8" X 8" X 6" | EACH | 2 | | | | 2 | | |
| ^ 81400100 | HANDHOLE | EACH | 6 | | | 4 | 2 | | |
| ^ 81400300 | DOUBLE HANDHOLE | EACH | 1 | | | 1 | | | |
| ^ 81603010 | UNIT DUCT, 600V, 2-1C NO.10, 1/C NO.10 GROUND, (XLP-TYPE USE), 3/4" DIA. POLYETHYLENE | FOOT | 373 | | | | 373 | | |
| ^ 81603050 | UNIT DUCT, 600V, 3-1C NO.6, 1/C NO.8 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE | FOOT | 710 | | | | 710 | | |
| ^ 81702110 | ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10 | FOOT | 2,998 | | | 1,782 | 1,216 | | |
| ^ 81702170 | ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 2/0 | FOOT | 1,671 | | | | 1,671 | | |
| ^ 81800330 | AERIAL CABLE, 3-1/C NO. 6 WITH MESSENGER WIRE | FOOT | 660 | | | | 660 | | |
| ^ 82102400 | LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT | EACH | 1 | | | | 1 | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION



| | |
|-------------------|---------------------|
| DESIGNED - PK | REVISED - 1/17/2020 |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SHEET 13 OF 21 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|--------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 16 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|----------|--|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^* | 82110008 | LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION H | EACH | 8 | | | 4 | 4 | |
| ^ | 82500330 | LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 60AMP | EACH | 1 | | | | 1 | |
| ^ | 83050810 | LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM | EACH | 5 | | | | 5 | |
| ^ | 83600200 | LIGHT POLE FOUNDATION, 24" DIAMETER | FOOT | 90 | | | | 90 | |
| ^ | 83800205 | BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE | EACH | 9 | | | | 9 | |
| ^* | 84200500 | REMOVAL OF LIGHTING UNIT, SALVAGE | EACH | 1 | | | | 1 | |
| ^ | 84200804 | REMOVAL OF POLE FOUNDATION | EACH | 4 | | | | 4 | |
| ^ | 84400105 | RELOCATE EXISTING LIGHTING UNIT | EACH | 4 | | | | 4 | |
| ^ | 87301225 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 306 | | | 306 | | |
| ^ | 87301245 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 2,173 | | | 2,173 | | |
| ^ | 87301255 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 790 | | | 790 | | |
| ^ | 87301805 | ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | FOOT | 188 | | | 188 | | |
| ^ | 87301900 | ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 779 | | | 779 | | |

^ DENOTES SPECIALTY ITEM
 * DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|--------------|---|------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ 87502500 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 2 | | | 2 | | | |
| ^ 87700260 | STEEL MAST ARM ASSEMBLY AND POLE, 44 FT. | EACH | 1 | | | 1 | | | |
| ^ 87702880 | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 30 FT. | EACH | 1 | | | 1 | | | |
| ^ 87702890 | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 32 FT. | EACH | 1 | | | 1 | | | |
| ^ 87702940 | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 42 FT. | EACH | 1 | | | 1 | | | |
| ^ 87800100 | CONCRETE FOUNDATION, TYPE A | FOOT | 8 | | | 8 | | | |
| ^ 87800150 | CONCRETE FOUNDATION, TYPE C | FOOT | 4 | | | 4 | | | |
| ^ * 87800415 | CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER | FOOT | 48 | | | 48 | | | |
| ^ 88030020 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 7 | | | 7 | | | |
| ^ 88030050 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 4 | | | 4 | | | |
| ^ 88030100 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 2 | | | 2 | | | |
| ^ 88030110 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 2 | | | 2 | | | |
| ^ 88200410 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 9 | | | 9 | | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|--------------|--|------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ 88700200 | LIGHT DETECTOR | EACH | 2 | | | 2 | | | |
| ^ 88700300 | LIGHT DETECTOR AMPLIFIER | EACH | 1 | | | 1 | | | |
| ^ 89000100 | TEMPORARY TRAFFIC SIGNAL INSTALLATION | EACH | 1 | | | 1 | | | |
| ^ 89502300 | REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 2,514 | | | | 2,514 | | |
| ^ * A2000220 | TREE, ACER X FREEMANII MARMO (MARMO FREEMAN MAPLE), 2-1/2" CALIPER, BALLED AND BURLAPPED | EACH | 3 | 3 | | | | | |
| ^ * A2001820 | TREE, ACER SACCHARUM GREEN MOUNTAIN (GREEN MOUNTAIN SUGAR MAPLE), 2 -1/2" CALIPER, BALLED AND BURLAPPED | EACH | 5 | 5 | | | | | |
| ^ * A2002520 | TREE, CARPINUS CAROLINIANA (AMERICAN HORNBEAM), 2-1/2" CALIPER, BALLED AND BURLAPPED | EACH | 5 | 5 | | | | | |
| ^ * A2004820 | TREE, GLEDITSIA TRIACANTHOS INERMIS SKYLINE (SKYLINE THORNLESS COMMON HONEYLOCUST), 2-1/2" CALIPER, BALLED AND BURLAPPED | EACH | 7 | 7 | | | | | |
| ^ * A2005020 | TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 2-1/2" CALIPER, BALLED AND BURLAPPED | EACH | 8 | 8 | | | | | |
| ^ * A2007116 | TREE, QUERCUS RUBRA (RED OAK), 2" CALIPER, BALLED AND BURLAPPED | EACH | 11 | 11 | | | | | |
| ^ * A2007920 | TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2-1/2" CALIPER, BALLED AND BURLAPPED | EACH | 5 | 5 | | | | | |
| ^ * A2008120 | TREE, TILIA CORDATA GREENSPIRE (GREENSPIRE LITTLE LEAF LINDEN), 2-1/2" CALIPER, BALLED AND BURLAPPED | EACH | 4 | 4 | | | | | |
| ^ * B2000566 | TREE, AMELANCHIER CANADENSIS (SHADBLOW SERVICEBERRY), 6' HEIGHT, SHRUB FORM, BALLED AND BURLAPPED | EACH | 7 | 7 | | | | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|--------------|---|-------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ * C2000660 | SHRUB, ARONIA MELANOCARPA MORTON (IROQUOIS BEAUTY BLACK CHOKEBERRY), 2'-6" HEIGHT, BALLED AND BURLAPPED | EACH | 100 | 100 | | | | | |
| ^ * X0324085 | EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C | FOOT | 306 | | | 306 | | | |
| ^ * X0326148 | TEMPORARY WOOD POLE, 60 FT., CLASS 4, 15 FT. MAST ARM | EACH | 4 | | | | 4 | | |
| * X0327036 | BIKE PATH REMOVAL | SQ YD | 908 | 908 | | | | | |
| ^ * X0327236 | TEMPORARY WOOD POLE, 50 FT., CLASS 4 | EACH | 1 | | | | 1 | | |
| * X0327980 | PAVEMENT MARKING REMOVAL - WATER BLASTING | SQ FT | 1,219 | 1,219 | | | | | |
| * X0426200 | DEWATERING | L SUM | 1 | 1 | | | | | |
| * X0900064 | MEMBRANE WATERPROOFING SYSTEM FOR BURIED STRUCTURES | SQ YD | 293 | | 293 | | | | |
| ^ * X1400107 | FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET | EACH | 1 | | | 1 | | | |
| * X1700034 | FORM LINER TEXTURED SURFACE, SPECIAL | SQ FT | 4,542 | | 4,542 | | | | |
| * X2130010 | EXPLORATION TRENCH, SPECIAL | FOOT | 100 | 100 | | | | | |
| ^ * X2200018 | ORNAMENTAL FENCE | FOOT | 201 | 201 | | | | | |
| ^ * X2501750 | SEEDING, CLASS 4 (SPECIAL) | ACRE | 1.00 | 1.00 | | | | | |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|----------|---|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ * | X2511630 | EROSION CONTROL BLANKET (SPECIAL) | SQ YD | 3,075 | | 3,075 | | | |
| * | X2800302 | TEMPORARY DITCH CHECKS (SPECIAL) | FOOT | 495 | | 495 | | | |
| * | X4023000 | TEMPORARY ACCESS (ROAD) | EACH | 4 | | 4 | | | |
| ^ * | X5030290 | STAINING CONCRETE STRUCTURES | SQ FT | 4,542 | | 4,542 | | | |
| ^ * | X5090850 | ORNAMENTAL RAILING | FOOT | 171 | | 171 | | | |
| * | X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | LSUM | 1 | | 1 | | | |
| * | X7030005 | TEMPORARY PAVEMENT MARKING REMOVAL | SQ FT | 9,869 | | 9,869 | | | |
| * | X7040125 | PINNING TEMPORARY CONCRETE BARRIER | EACH | 520 | | 520 | | | |
| ^ * | X7310110 | BASE FOR TELESCOPING SIGN SUPPORT, SPECIAL | EACH | 21 | | 21 | | | |
| ^ * | X7800100 | PAINT PAVEMENT MARKING - RAISED MEDIAN | SQ FT | 777 | | 777 | | | |
| ^ * | X8210040 | TEMPORARY LUMINAIRE, HIGH PRESSURE SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT | EACH | 4 | | | 4 | | |
| ^ * | X8211000 | UNDERPASS LUMINAIRE (SPECIAL) | EACH | 10 | | | 10 | | |
| ^ * | X8250091 | COMBINATION LIGHTING CONTROLLER | EACH | 1 | | | 1 | | |


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* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|-------------|----------|--|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| ^ * | X8360215 | LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET | FOOT | 24 | | | 24 | | |
| ^ * | X8620200 | UNINTERRUPTABLE POWER SUPPLY, SPECIAL | EACH | 1 | | 1 | | | |
| * | XX003668 | PRECONSTRUCTION VIDEO TAPING | LSUM | 1 | | | | | 1 |
| ^ * | XX005723 | VIDEO DETECTION SYSTEM COMPLETE INTERSECTION | EACH | 1 | | 1 | | | |
| * | XX006658 | FLOCCULATION LOGS | EACH | 25 | 25 | | | | |
| * | XX006659 | FLOCCULATION POWDER | POUND | 5 | 5 | | | | |
| * | XX006821 | CONCRETE TRUCK WASHOUT | L SUM | 1 | 1 | | | | |
| ^ * | XX009242 | ANTI-GRAFFITI PROTECTION SYSTEM | SQ FT | 8,992 | | | | | 8,992 |
| * | XZ127900 | RETAINING WALL REMOVAL | FOOT | 171 | 171 | | | | |
| * | Z0007430 | TEMPORARY SIDEWALK | SQ FT | 5,876 | 5,876 | | | | |
| ^ * | Z0010600 | CLEANING DRAINAGE SYSTEM | FOOT | 100 | | | | | 100 |
| * | Z0013798 | CONSTRUCTION LAYOUT | LSUM | 1 | 1 | | | | |
| ^ * | Z0018500 | DRAINAGE STRUCTURES TO BE CLEANED | EACH | 2 | | | | | 2 |

^ DENOTES SPECIALTY ITEM
* DENOTES SPECIAL PROVISION

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|--------------|---|--------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| * Z0019598 | DUST CONTROL, SPECIAL | GALLON | 2,000 | 2,000 | | | | | |
| * Z0030850 | TEMPORARY INFORMATION SIGNING | SQ FT | 90 | 90 | | | | | |
| ^ * Z0033028 | MAINTENANCE OF LIGHTING SYSTEM | CAL MO | 8 | | | | 8 | | |
| * Z0033700 | LONGITUDINAL JOINT SEALANT | FOOT | 10,295 | 10,295 | | | | | |
| * Z0038115 | PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1/2" | SQ YD | 4,277 | 4,277 | | | | | |
| * Z0046304 | PIPE UNDERDRAINS FOR STRUCTURES 4" | FOOT | 926 | | 926 | | | | |
| * Z0056608 | STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH | FOOT | 219 | 219 | | | | | |
| * Z0056612 | STORM SEWER (WATER MAIN REQUIREMENTS) 18 INCH | FOOT | 61 | 61 | | | | | |
| * Z0062456 | TEMPORARY PAVEMENT | SQ YD | 2,330 | 2,330 | | | | | |
| * Z0062458 | TEMPORARY PAVEMENT (VARIABLE DEPTH) | TON | 9 | 9 | | | | | |
| * Z0064800 | SELECTIVE CLEARING | UNIT | 14 | 14 | | | | | |
| ^ * Z0073510 | TEMPORARY TRAFFIC SIGNAL TIMING | EACH | 1 | | | 1 | | | |
| * Z0076600 | TRAINEES | HOUR | 1,000 | | | | | 1,000 | |

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| | |
|-------------------|---------------------|
| DESIGNED - PK | REVISED - 1/17/2020 |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| SUMMARY OF QUANTITIES | |
|-----------------------|--------------|
| SHEET 20 OF 21 SHEETS | STA. TO STA. |

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 23 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

| CODE NUMBER | ITEM | UNIT | TOTAL QUANTITY | STP FUNDS | CMAQ FUNDS | STP FUNDS | STP FUNDS | STP FUNDS | 0044 |
|--------------|---|------|----------------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | | | 70% FED 30% VILLAGE | 80% FED 20% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 70% FED 30% VILLAGE | 100% VILLAGE |
| | | | | ROADWAY 0004 | ROADWAY 0004 | SIGNALS 0021 | LIGHTING 0021 | TRAINEES 0042 | NON-PARTI- CIPATING |
| * Z0076604 | TRAINEES TRAINING PROGRAM GRADUATE | HOUR | 1,000 | | | | | 1,000 | |
| ^ * XX009366 | UNIT DUCT, 600V, 2-1C NO.2, 1/C NO.8 GROUND, (EPR-TYPE RHW), 2" DIA. POLYETHYLENE | FOOT | 226 | | | | 226 | | |

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CIVILTECH
 Two Pierce Place, Suite 1400
 Itasca, Illinois 60143
 Tel: 630.773.3900 Fax: 630.773.3975
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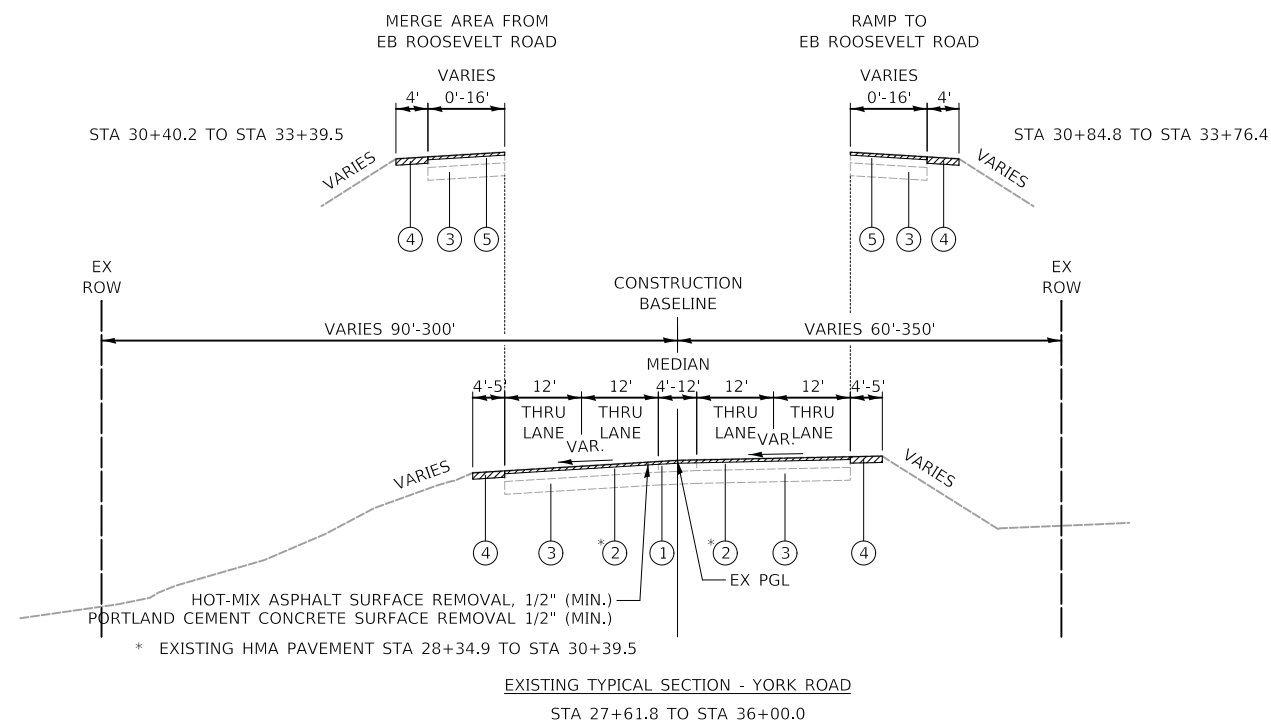
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|-------------------|---------------------|
| DESIGNED - PK | REVISED - 1/17/2020 |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |


**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

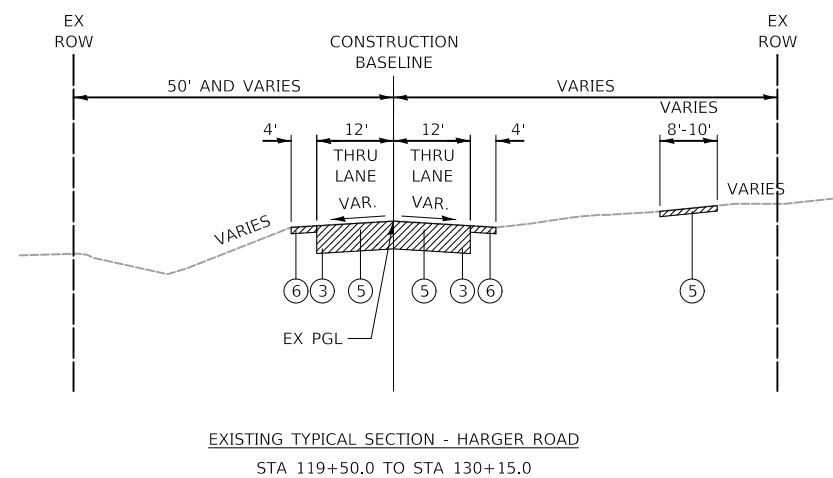
SUMMARY OF QUANTITIES

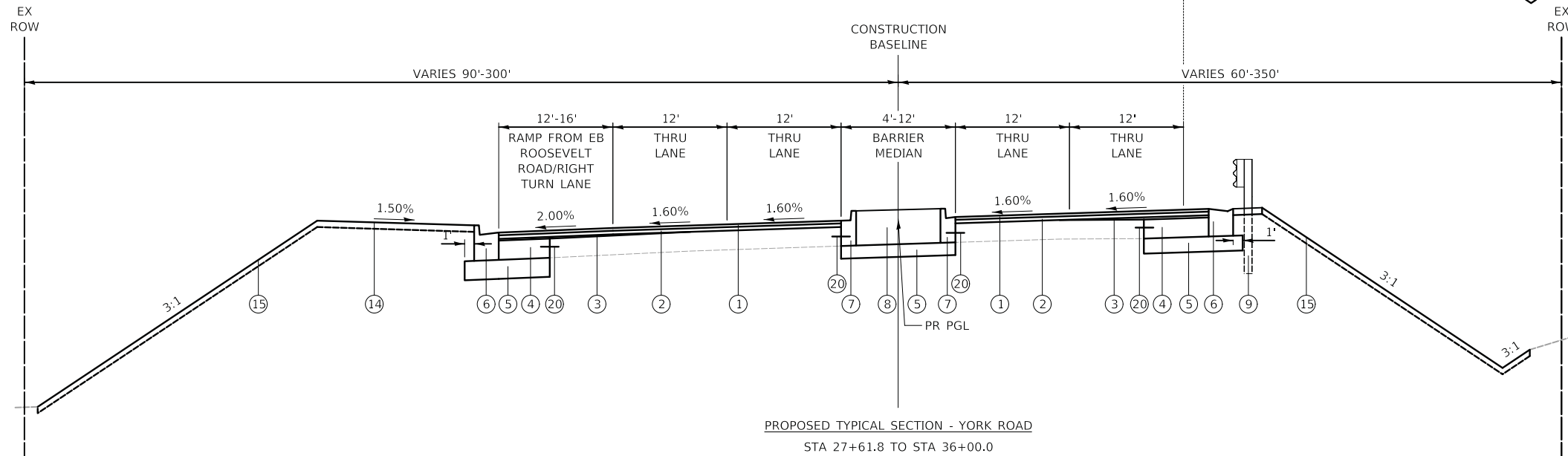
SHEET 21 OF 21 SHEETS STA. TO STA.

| | | | | |
|---|---------------------------|------------------|---------------------|-----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 24 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |



- EXISTING LEGEND
- ① EXISTING CONCRETE MEDIAN
 - ② EXISTING PCC PAVEMENT
 - ③ EXISTING AGGREGATE BASE
 - ④ EXISTING ASPHALT SHOULDER
 - ⑤ EXISTING HOT-MIX ASPHALT PAVEMENT
 - ⑥ EXISTING AGGREGATE SHOULDER
 -  REMOVAL





- PROPOSED LEGEND**
- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1 3/4"
 - ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
 - ③* HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1 1/2 MIN. - 2 1/4 MAX.) or HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4 MIN.)
 - ④ PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 9"
 - ⑤ AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - ⑥ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - ⑦ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑧ CONCRETE MEDIAN TYPE SB-6.12 OR CONCRETE MEDIAN SURFACE, 4" WITH AGGREGATE BASE COURSE, TYPE B
 - ⑨ STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
 - ⑩ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"
 - ⑪ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 7"
 - ⑫ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 3"
 - ⑬ HOT-MIX ASPHALT SHOULDERS, 6"
 - ⑭ SODDING, SALT TOLERANT TOPSOIL, FURNISH AND REPLACE, 4"
 - ⑮ SEEDING, CLASS 2A or CLASS 4 (SPECIAL) TOPSOIL, FURNISH AND REPLACE, 4"
 - ⑯ AGGREGATE SHOULDERS, TYPE B, 5"
 - ⑰ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 3"
 - ⑱ AGGREGATE BASE COURSE, TYPE B, 4"
 - ⑲ AGGREGATE BASE COURSE, TYPE B, 6"
 - ⑳ PROPOSED 24" LONG NO. 6 EPOXY COATED TIE BARS AT 24" C-C (INCLUDED IN THE COST OF CONCRETE ITEMS)

PROPOSED TYPICAL SECTION - YORK ROAD
STA 27+61.8 TO STA 36+00.0

| HOT-MIX ASPHALT MIXTURE REQUIREMENTS | |
|--|------------------|
| MIXTURE TYPE | AIR VOIDS @ Ndes |
| PAVEMENT RECONSTRUCTION (HARGER ROAD) | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70; 2" | 4% @ 70 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 7" | 4% @ 70 Gyr. |
| HMA SHOULDER (HARGER ROAD) | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70; 2" | 4% @ 70 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 3" | 4% @ 70 Gyr. |
| PAVEMENT RESURFACING (YORK ROAD) | |
| POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70; 1-3/4" | 4% @ 70 Gyr. |
| POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 3/4" | 3.5% @ 50 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70; (1-1/2" MIN. TO 2-1/4" MAX.) * | 4% @ 70 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; (2-1/4" MIN.) * | 4% @ 70 Gyr. |
| PAVEMENT WIDENING (YORK ROAD) | |
| POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70; 1-3/4" | 4% @ 70 Gyr. |
| POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 3/4" | 3.5% @ 50 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70; (1-1/2" MIN. TO 2-1/4" MAX.) * | 4% @ 70 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; (2-1/4" MIN.) * | 4% @ 70 Gyr. |
| HMA SHOULDER (YORK ROAD) 6" | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70; 2" | 4% @ 70 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 4" | 4% @ 70 Gyr. |
| TEMPORARY PAVEMENT | |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 8" | 4% @ 70 Gyr. |
| BIKE PATH PAVEMENT | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 3" | 4% @ 50 Gyr. |
| TEMPORARY PAVEMENT (VARIABLE DEPTH) | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70; VAR. DEPTH | 4% @ 70 Gyr. |
| TEMPORARY SIDEWALK | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 3" | 4% @ 50 Gyr. |

NOTE:

- APPLY LONGITUDINAL JOINT SEALANT UNDER THE SURFACE LIFT AND UNDER THE TOP BINDER LIFT AT THE PAVED LANE LINES OF FULL-DEPTH PAVEMENT AND UNDER THE SURFACE LIFT AT THE PAVED LANE LINES OF PAVEMENT RESURFACING.
- PIPE UNDERDRAINS, TYPE 2, 4" SHALL BE INSTALLED AT THE OUTSIDE EDGE OF THE PAVEMENT. TRANSVERSE UNDERDRAINS SHALL BE INSTALLED AT THE LOW POINTS OF THE PROFILE, AT ANY UNDERCUTS DETERMINED IN THE FIELD, AND EVERY 300 FEET.

* HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 SHALL BE USED AS LEVELING BINDER FOR GRADE CORRECTIONS OF LESS THAN 2 1/4". FOR GRADE CORRECTIONS OF MORE THAN 2 1/4" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 SHALL BE USED.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-12" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

CIVILTECH
Two Pierce Place, Suite 1400
Itasca, Illinois 60143
Tel: 630.773.3900 Fax: 630.773.3975
www.civiltechinc.com

| | |
|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

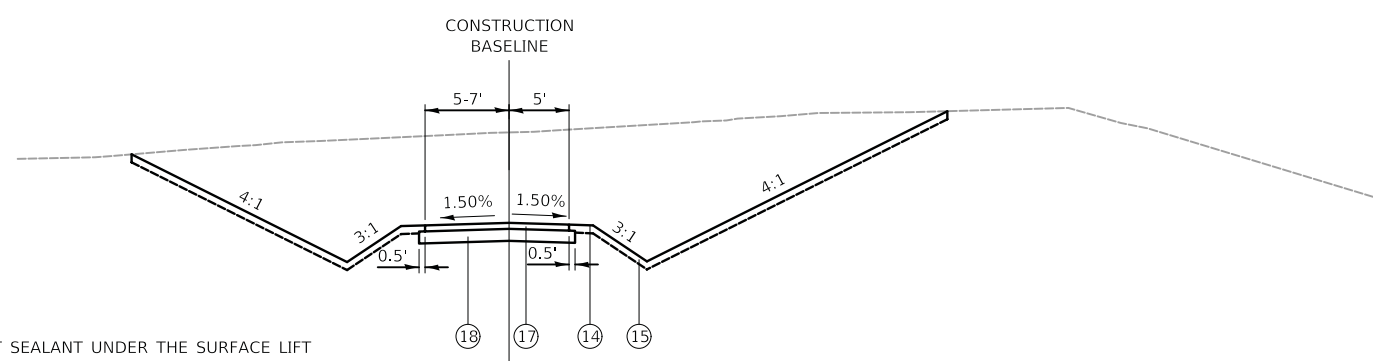
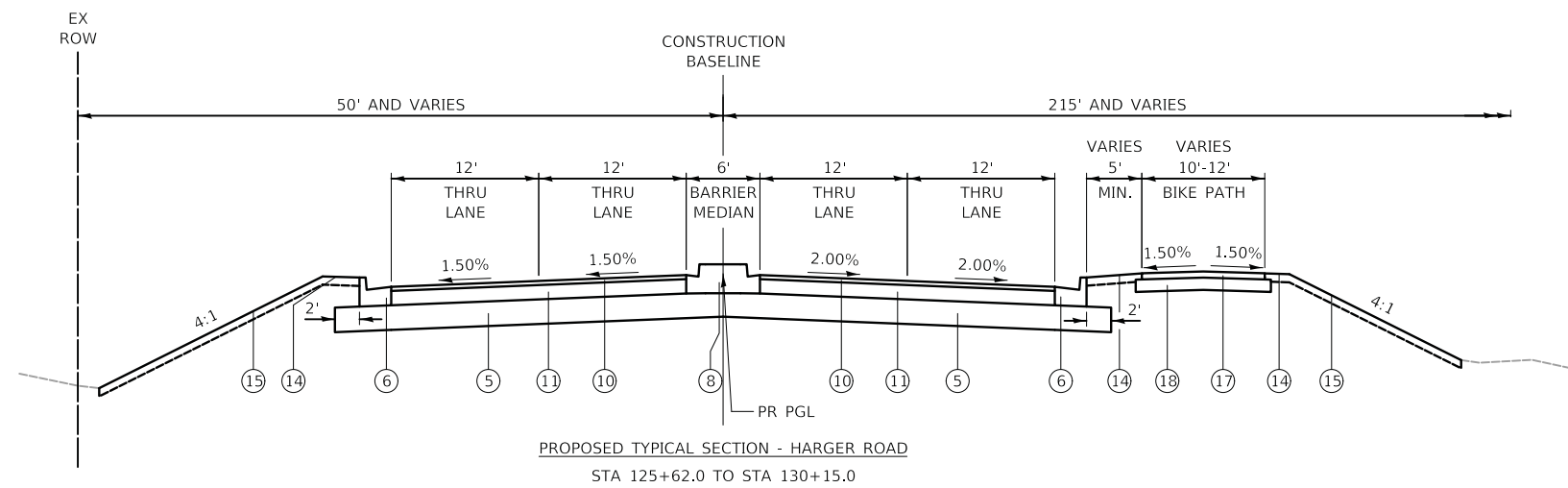
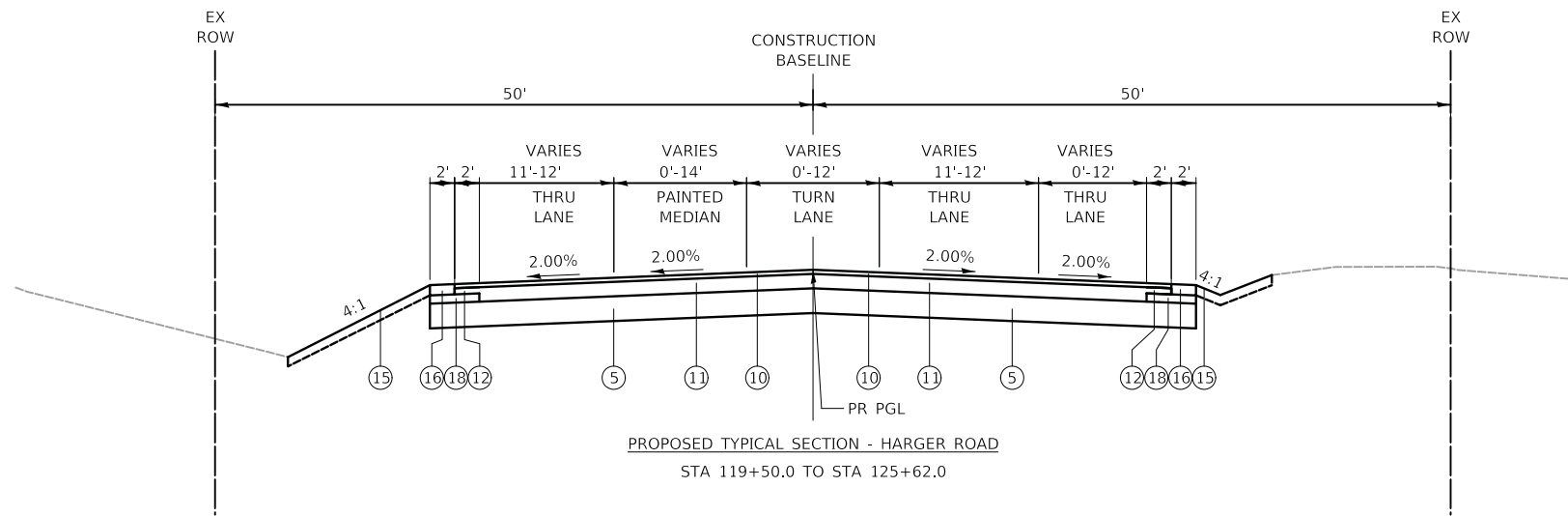
| | | | | | | |
|----------------------------------|--|--------------|----------------|--------------------|--------------|-----------|
| PROPOSED TYPICAL SECTIONS | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 26 |
| SHEET 1 OF 2 SHEETS | | STA. | TO STA. | CONTRACT NO. 61G16 | | |

| | | |
|-----------------------|----------|------------------|
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT |
|-----------------------|----------|------------------|

PROPOSED LEGEND

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1¾"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, ¾"
- ③* HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 (1½ MIN. - 2¼ MAX.) or HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2¼ MIN.)
- ④ PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 9"
- ⑤ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑥ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑦ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑧ CONCRETE MEDIAN TYPE SB-6.12 OR CONCRETE MEDIAN SURFACE, 4" WITH AGGREGATE BASE COURSE, TYPE B
- ⑨ STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
- ⑩ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"
- ⑪ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 7"
- ⑫ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 3"
- ⑬ HOT-MIX ASPHALT SHOULDERS, 6"
- ⑭ SODDING, SALT TOLERANT TOPSOIL, FURNISH AND REPLACE, 4"
- ⑮ SEEDING, CLASS 2A or CLASS 4 (SPECIAL) TOPSOIL, FURNISH AND REPLACE, 4"
- ⑯ AGGREGATE SHOULDERS, TYPE B, 5"
- ⑰ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 3"
- ⑱ AGGREGATE BASE COURSE, TYPE B, 4"
- ⑲ AGGREGATE BASE COURSE, TYPE B, 6"
- ⑳ PROPOSED 24" LONG NO. 6 EPOXY COATED TIE BARS AT 24" C-C (INCLUDED IN THE COST OF CONCRETE ITEMS)

* HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 SHALL BE USED AS LEVELING BINDER FOR GRADE CORRECTIONS OF LESS THAN 2¼". FOR GRADE CORRECTIONS OF MORE THAN 2¼" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 SHALL BE USED.



NOTE:

1. APPLY LONGITUDINAL JOINT SEALANT UNDER THE SURFACE LIFT AND UNDER THE TOP BINDER LIFT AT THE PAVED LANE LINES OF FULL-DEPTH PAVEMENT AND UNDER THE SURFACE LIFT AT THE PAVED LANE LINES OF PAVEMENT RESURFACING.
2. PIPE UNDERDRAINS, TYPE 2, 4" SHALL BE INSTALLED AT THE OUTSIDE EDGE OF THE PAVEMENT. TRANSVERSE UNDERDRAINS SHALL BE INSTALLED AT THE LOW POINTS OF THE PROFILE, AT ANY UNDERCUTS DETERMINED IN THE FIELD, AND EVERY 300 FEET.

SOILS NOTE:

POROUS GRANULAR EMBANKMENT HAS BEEN PROVIDED FOR SOILS WHICH TEND TO BE UNSTABLE WHEN WET AT THE LOCATIONS INDICATED ON GEOTECHNICAL REPORTS PREPARED BY MIDLAND STANDARD ENGINEERING & TESTING, INC. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE MANUAL). IF UNSTABLE SOILS ARE ENCOUNTERED, THE SOILS SHALL BE REMOVED AND REPLACED. POROUS GRANULAR EMBANKMENT SHALL BE PAID FOR AS "AGGREGATE SUBGRADE IMPROVEMENT".

THE LIMITS OF UNSTABLE SOILS SHOWN IN THE PLANS FOR CONTRACTOR'S REFERENCE ONLY AT THE ANTICIPATED LOCATIONS. ADDITIONAL UNDERCUT QUANTITY IS ESTIMATED AS 25% OF PLANNED FULL DEPTH PAVEMENT AREA ASSUMING A THICKNESS OF 12 INCHES. THE ACTUAL NEED FOR UNDERCUTS WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.

THE LIMITS OF UNSTABLE SOILS ARE AT THE APPROXIMATE LOCATIONS AS FOLLOWS:

| STATION RANGE | ESTIMATED UNDERCUT BELOW DESIGN SUBGRADE |
|------------------|--|
| 121+66 TO 124+63 | 10.5' AND VARIES |

| 6 SCHEDULE OF EARTHWORK QUANTITIES (BREAKDOWN) | | | | | | | | | | | | | | | | |
|--|----------|--|------------------------|---|--------------------------------------|-----------------------------|--|------------------|---|--------------------------------------|----------------------|--|------------------|---|--------------------------------------|----------------------|
| STATION | DISTANCE | | EARTH EXCAVATION (CUT) | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION (FILL) | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION |
| (XX+XX) | (FT) | | (SQ FT) | (SQ FT) | (SQ FT) | (SQ FT) | | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | | (CU FT) | (CU FT) | (CU FT) | (CU FT) |
| Harger Road | | | | | | | | | | | | | | | | |
| 118+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 5 | 11 | 0 | 12 | | 250 | 525 | 0 | 575 |
| 118+50 | | | 10 | 21 | 0 | 23 | | | | | | | | | | |
| | 50 | | | | | | | 13 | 23 | 0 | 29 | | 625 | 1,125 | 0 | 1,425 |
| 119+00 | | | 15 | 24 | 0 | 34 | | | | | | | | | | |
| | 50 | | | | | | | 38 | 26 | 0 | 45 | | 1,875 | 1,300 | 0 | 2,225 |
| 119+50 | | | 60 | 28 | 0 | 55 | | | | | | | | | | |
| | 50 | | | | | | | 59 | 31 | 0 | 57 | | 2,950 | 1,550 | 0 | 2,850 |
| 120+00 | | | 58 | 34 | 0 | 59 | | | | | | | | | | |
| | 50 | | | | | | | 59 | 36 | 0 | 61 | | 2,950 | 1,800 | 0 | 3,025 |
| 120+50 | | | 60 | 38 | 0 | 62 | | | | | | | | | | |
| | 50 | | | | | | | 57 | 37 | 0 | 60 | | 2,825 | 1,850 | 0 | 2,975 |
| 121+00 | | | 53 | 36 | 0 | 57 | | | | | | | | | | |
| | 50 | | | | | | | 60 | 61 | 24 | 77 | | 3,000 | 3,050 | 1,200 | 3,850 |
| 121+50 | | | 67 | 86 | 48 | 97 | | | | | | | | | | |
| | 50 | | | | | | | 87 | 94 | 53 | 127 | | 4,325 | 4,675 | 2,650 | 6,325 |
| 122+00 | | | 106 | 101 | 58 | 156 | | | | | | | | | | |
| | 50 | | | | | | | 139 | 104 | 57 | 192 | | 6,925 | 5,175 | 2,825 | 9,575 |
| 122+50 | | | 171 | 106 | 55 | 227 | | | | | | | | | | |
| | 50 | | | | | | | 205 | 114 | 56 | 277 | | 10,250 | 5,700 | 2,800 | 13,825 |
| 123+00 | | | 239 | 122 | 57 | 326 | | | | | | | | | | |
| | 50 | | | | | | | 212 | 124 | 59 | 292 | | 10,575 | 6,200 | 2,950 | 14,575 |
| 123+50 | | | 184 | 126 | 61 | 257 | | | | | | | | | | |
| | 50 | | | | | | | 171 | 118 | 59 | 214 | | 8,525 | 5,900 | 2,925 | 10,700 |
| 124+00 | | | 157 | 110 | 56 | 171 | | | | | | | | | | |
| | 50 | | | | | | | 123 | 87 | 28 | 125 | | 6,125 | 4,350 | 1,400 | 6,250 |
| 124+50 | | | 88 | 64 | 0 | 79 | | | | | | | | | | |
| | 50 | | | | | | | 118 | 72 | 0 | 49 | | 5,875 | 3,600 | 0 | 2,450 |
| 125+00 | | | 147 | 80 | 0 | 19 | | | | | | | | | | |
| | 50 | | | | | | | 109 | 110 | 0 | 106 | | 5,425 | 5,475 | 0 | 5,275 |
| 125+50 | | | 70 | 139 | 0 | 192 | | | | | | | | | | |
| | 50 | | | | | | | 264 | 149 | 0 | 178 | | 13,175 | 7,425 | 0 | 8,900 |
| 126+00 | | | 457 | 158 | 0 | 164 | | | | | | | | | | |
| | 50 | | | | | | | 491 | 159 | 0 | 183 | | 24,550 | 7,925 | 0 | 9,150 |
| 126+50 | | | 525 | 159 | 0 | 202 | | | | | | | | | | |
| | 50 | | | | | | | 467 | 152 | 0 | 243 | | 23,325 | 7,600 | 0 | 12,150 |
| 127+00 | | | 408 | 145 | 0 | 284 | | | | | | | | | | |
| | 50 | | | | | | | 607 | 164 | 0 | 382 | | 30,325 | 8,200 | 0 | 19,100 |
| 127+50 | | | 805 | 183 | 0 | 480 | | | | | | | | | | |
| | 50 | | | | | | | 837 | 134 | 0 | 299 | | 41,850 | 6,700 | 0 | 14,950 |
| 128+00 | | | 869 | 85 | 0 | 118 | | | | | | | | | | |
| | 50 | | | | | | | 459 | 104 | 0 | 553 | | 22,925 | 5,200 | 0 | 27,625 |
| 128+50 | | | 48 | 123 | 0 | 987 | | | | | | | | | | |
| | 50 | | | | | | | 24 | 97 | 0 | 679 | | 1,200 | 4,825 | 0 | 33,925 |
| 129+00 | | | 0 | 70 | 0 | 370 | | | | | | | | | | |
| | 50 | | | | | | | 32 | 69 | 0 | 214 | | 1,600 | 3,425 | 0 | 10,675 |
| 129+50 | | | 64 | 67 | 0 | 57 | | | | | | | | | | |
| | 50 | | | | | | | 32 | 34 | 0 | 29 | | 1,600 | 1,675 | 0 | 1,425 |
| 130+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 130+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 36 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 130+86 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| Harger Road Subtotal | | | | | | | | | | | | | 233,050 | 105,250 | 16,750 | 223,800 |

| 6 SCHEDULE OF EARTHWORK QUANTITIES (BREAKDOWN) | | | | | | | | | | | | | | | | |
|--|----------|--|------------------------|---|--------------------------------------|-----------------------------|--|------------------|---|--------------------------------------|----------------------|--------|------------------|---|--------------------------------------|----------------------|
| STATION | DISTANCE | | EARTH EXCAVATION (CUT) | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION (FILL) | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION |
| (XX+XX) | (FT) | | (SQ FT) | (SQ FT) | (SQ FT) | (SQ FT) | | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | | (CU FT) | (CU FT) | (CU FT) | (CU FT) |
| York Road | | | | | | | | | | | | | | | | |
| 25+00 | | | 54 | 25 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 151 | 42 | 0 | 0 | | 7,525 | 2,075 | 0 | 0 |
| 25+50 | | | 247 | 58 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 255 | 78 | 0 | 1 | | 12,725 | 3,900 | 0 | 25 |
| 26+00 | | | 262 | 98 | 0 | 1 | | | | | | | | | | |
| | 50 | | | | | | | 355 | 105 | 0 | 3 | | 17,750 | 5,225 | 0 | 150 |
| 26+50 | | | 448 | 111 | 0 | 5 | | | | | | | | | | |
| | 50 | | | | | | | 408 | 115 | 0 | 11 | | 20,400 | 5,750 | 0 | 525 |
| 27+00 | | | 368 | 119 | 0 | 16 | | | | | | | | | | |
| | 50 | | | | | | | 254 | 139 | 0 | 135 | | 12,700 | 6,950 | 0 | 6,750 |
| 27+50 | | | 140 | 159 | 0 | 254 | | | | | | | | | | |
| | 50 | | | | | | | 76 | 163 | 0 | 607 | | 3,775 | 8,125 | 0 | 30,350 |
| 28+00 | | | 11 | 166 | 0 | 960 | | | | | | | | | | |
| | 50 | | | | | | | 8 | 88 | 0 | 512 | | 375 | 4,375 | 0 | 25,600 |
| 28+50 | | | 4 | 9 | 0 | 64 | | | | | | | | | | |
| | 50 | | | | | | | 17 | 51 | 0 | 255 | | 850 | 2,525 | 0 | 12,725 |
| 29+00 | | | 30 | 92 | 0 | 445 | | | | | | | | | | |
| | 50 | | | | | | | 28 | 86 | 0 | 406 | | 1,375 | 4,275 | 0 | 20,275 |
| 29+50 | | | 25 | 79 | 0 | 366 | | | | | | | | | | |
| | 50 | | | | | | | 22 | 82 | 0 | 377 | | 1,100 | 4,075 | 0 | 18,825 |
| 30+00 | | | 19 | 84 | 0 | 387 | | | | | | | | | | |
| | 50 | | | | | | | 21 | 97 | 0 | 403 | | 1,025 | 4,825 | 0 | 20,125 |
| 30+50 | | | 22 | 109 | 0 | 418 | | | | | | | | | | |
| | 50 | | | | | | | 21 | 110 | 0 | 416 | | 1,050 | 5,500 | 0 | 20,775 |
| 31+00 | | | 20 | 111 | 0 | 413 | | | | | | | | | | |
| | 50 | | | | | | | 18 | 98 | 0 | 336 | | 900 | 4,900 | 0 | 16,775 |
| 31+50 | | | 16 | 85 | 0 | 258 | | | | | | | | | | |
| | 50 | | | | | | | 16 | 79 | 0 | 225 | | 775 | 3,925 | 0 | 11,225 |
| 32+00 | | | 15 | 72 | 0 | 191 | | | | | | | | | | |
| | 50 | | | | | | | 15 | 68 | 0 | 168 | | 725 | 3,375 | 0 | 8,400 |
| 32+50 | | | 14 | 63 | 0 | 145 | | | | | | | | | | |
| | 50 | | | | | | | 14 | 71 | 0 | 134 | | 700 | 3,550 | 0 | 6,700 |
| 33+00 | | | 14 | 79 | 0 | 123 | | | | | | | | | | |
| | 50 | | | | | | | 23 | 79 | 0 | 113 | | 1,125 | 3,925 | 0 | 5,650 |
| 33+50 | | | 31 | 78 | 0 | 103 | | | | | | | | | | |
| | 50 | | | | | | | 21 | 58 | 0 | 66 | | 1,050 | 2,875 | 0 | 3,275 |
| 34+00 | | | 11 | 37 | 0 | 28 | | | | | | | | | | |
| | 50 | | | | | | | 13 | 27 | 0 | 18 | | 625 | 1,325 | 0 | 900 |
| 34+50 | | | 14 | 16 | 0 | 8 | | | | | | | | | | |
| | 50 | | | | | | | 16 | 17 | 0 | 8 | | 800 | 825 | 0 | 400 |
| 35+00 | | | 18 | 17 | 0 | 8 | | | | | | | | | | |
| | 50 | | | | | | | 22 | 17 | 0 | 7 | | 1,075 | 825 | 0 | 350 |
| 35+50 | | | 25 | 16 | 0 | 6 | | | | | | | | | | |
| | 50 | | | | | | | 13 | 8 | 0 | 3 | | 625 | 400 | 0 | 150 |
| 36+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| York Road Subtotal | | | | | | | | | | | | 89,050 | 83,525 | 0 | 209,950 | |

| 6 SCHEDULE OF EARTHWORK QUANTITIES (BREAKDOWN) | | | | | | | | | | | | | | | | |
|--|----------|--|------------------------|---|--------------------------------------|-----------------------------|--|------------------|---|--------------------------------------|----------------------|--|------------------|---|--------------------------------------|----------------------|
| STATION | DISTANCE | | EARTH EXCAVATION (CUT) | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION (FILL) | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION |
| (XX+XX) | (FT) | | (SQ FT) | (SQ FT) | (SQ FT) | (SQ FT) | | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | | (CU FT) | (CU FT) | (CU FT) | (CU FT) |
| Bike Path | | | | | | | | | | | | | | | | |
| 200+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 200+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 201+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 201+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 202+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 202+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 203+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 203+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 204+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 204+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 205+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 205+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 206+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 206+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 207+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 207+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 208+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 208+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 209+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 209+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 210+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 210+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 211+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 286 | 89 | 0 | 81 | | 14,275 | 4,450 | 0 | 4,050 |
| 211+50 | | | 571 | 178 | 0 | 162 | | | | | | | | | | |
| | 50 | | | | | | | 434 | 175 | 0 | 146 | | 21,700 | 8,725 | 0 | 7,300 |
| 212+00 | | | 297 | 171 | 0 | 130 | | | | | | | | | | |
| | 50 | | | | | | | 224 | 179 | 0 | 167 | | 11,200 | 8,950 | 0 | 8,325 |
| 212+50 | | | 151 | 187 | 0 | 203 | | | | | | | | | | |
| | 50 | | | | | | | 83 | 127 | 0 | 102 | | 4,125 | 6,325 | 0 | 5,075 |
| 213+00 | | | 14 | 66 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 7 | 33 | 0 | 0 | | 350 | 1,650 | 0 | 0 |
| 213+50 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | 50 | | | | | | | 0 | 0 | 0 | 0 | | 0 | 0 | 0 | 0 |
| 214+00 | | | 0 | 0 | 0 | 0 | | | | | | | | | | |
| | | | | | | | | | | | | | | | | |
| Bike Path Subtotal | | | | | | | | | | | | | 51,650 | 30,100 | 0 | 24,750 |

| 6 SCHEDULE OF EARTHWORK QUANTITIES (BREAKDOWN) | | | | | | | | | | | | | | | | | |
|--|----------|--|------------------------|---|--------------------------------------|-----------------------------|--|------------------|---|--------------------------------------|----------------------|--|--|---|--------------------------------------|----------------------|---------|
| STATION | DISTANCE | | EARTH EXCAVATION (CUT) | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION (FILL) | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION | | EARTH EXCAVATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | AGGREGATE SUBGRADE IMPROVEMENT (PGE) | FURNISHED EXCAVATION | |
| (XX+XX) | (FT) | | (SQ FT) | (SQ FT) | (SQ FT) | (SQ FT) | | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | AVG. (SQ FT) | | (CU FT) | (CU FT) | (CU FT) | (CU FT) | |
| | | | | | | | | | | | | | PROJECT SUBTOTAL (CU. FT.) | 373,750 | 218,875 | 16,750 | 458,500 |
| | | | | | | | | | | | | | PROJECT SUBTOTAL (CU. YD.) | 13,843 | 8,107 | 621 | 16,982 |
| | | | | | | | | | | | | | ADDITIONAL UNDERCUT QUANTITY (CU. YD.) (SEE NOTE 3 BELOW) | - | 739 | 739 | - |
| | | | | | | | | | | | | | (21101505) TOPSOIL EXCAVATION AND PLACEMENT (CU. YD.) (SEE NOTE 4 BELOW) | - | -1,949 | - | - |
| | | | | | | | | | | | | | SHRINKAGE FACTOR ADJUSTMENT (15%) | - | - | - | x 1.15 |
| | | | | | | | | | | | | | PROJECT TOTAL (CU. YD.) | 13,843 | 6,897 | 1,360 | 19,530 |

NOTE:

1. TOPSOIL STRIPPING IS ESTIMATED AS 12 INCHES DEEP.
2. ALL SUITABLE EXCAVATION MATERIAL SHALL BE USED IN THE CONSTRUCTION OF THE IMPROVEMENT.
3. ADDITIONAL UNDERCUT QUANTITY IS ESTIMATED AS 25% OF PLANNED FULL DEPTH PAVEMENT AREA ASSUMING A THICKNESS OF 12 INCHES.
THE ACTUAL NEED FOR UNDERCUTS WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
4. QUANTITY OF TOPSOIL EXCAVATION AND PLACEMENT HAS BEEN SUBTRACTED FROM REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS SINCE THE TOPSOIL WILL BE REUSED ONSITE.

BENCHMARKS

| BM NO. | LOCATION | ELEVATION | DESCRIPTION |
|--------|--------------------------|-----------|--|
| TBM #1 | STA. 122+50.86, 31.8' RT | 667.68 | CROSS CUT (SET) IN SOUTH FLANGE BOLT OF FIRST FIRE HYDRANT SOUTHWEST OF I-88 ENTRANCE RAMP ON THE SOUTHEAST SIDE OF HARGER ROAD. |
| TBM #2 | STA. 27+27.73, 36.7' RT | 689.92 | BOX CUT (SET) IN NORTHEAST CORNER OF CONCRETE ABUTMENT OF YORK ROAD BRIDGE, OVER I-88. |
| TBM #3 | STA. 37+49.89, 43.6' RT | 676.09 | BOX CUT (SET) IN CONCRETE BASE OF FOURTH LIGHT POLE NORTH OF HARGER ROAD ON THE EAST SIDE OF YORK ROAD. |

CONTROL POINTS

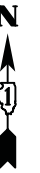
| POINT NO. | NORTHING | EASTING | STATION | OFFSET | DESCRIPTION |
|-----------|---------------|---------------|-----------|-----------|------------------|
| CP #1 | 1,890,168.237 | 1,093,115.361 | - | - | 5/8"IR W/CAP SET |
| CP #2 | 1,890,444.665 | 1,093,399.370 | 120+45.59 | 19.77' RT | 5/8"IR W/CAP SET |
| CP #3 | 1,891,660.865 | 1,094,116.925 | 34+48.59 | 39.99' RT | 5/8"IR W/CAP SET |
| CP #4 | 1,891,054.265 | 1,093,954.484 | 128+84.42 | 72.23' LT | MAG NAIL SET |
| CP #5 | 1,891,133.795 | 1,094,212.463 | 29+17.92 | 37.94' RT | X-CUT SET |
| CP #6 | 1,890,841.540 | 1,094,161.356 | 26+31.11 | 34.30' LT | MAG NAIL SET |
| CP #7 | 1,890,391.001 | 1,094,253.643 | 21+76.61 | 38.96' RT | 5/8"IR W/CAP SET |

**NORTH YORK ROAD
ALIGNMENT DATA**

| POINT | STATION | NORTHING | EASTING |
|-------|----------|---------------|---------------|
| BOA | 10+00.00 | 1,889,213.769 | 1,094,261.645 |
| PC | 26+36.11 | 1,890,848.580 | 1,094,196.382 |
| PI | 30+65.12 | 1,891,277.250 | 1,094,179.269 |
| PT | 34+90.56 | 1,891,691.444 | 1,094,067.490 |
| EOA | 44+19.75 | 1,892,588.543 | 1,093,825.389 |

**HARGER ROAD
ALIGNMENT DATA**

| POINT | STATION | NORTHING | EASTING |
|--------|-----------|---------------|---------------|
| BOA/PC | 118+59.83 | 1,890,350.510 | 1,093,239.775 |
| PI | 119+30.08 | 1,890,398.029 | 1,093,291.508 |
| PT | 120+00.00 | 1,890,436.354 | 1,093,350.377 |
| PC | 125+41.28 | 1,890,731.670 | 1,093,803.996 |
| PI | 126+47.99 | 1,890,789.889 | 1,093,893.424 |
| PRC | 127+47.69 | 1,890,889.446 | 1,093,931.833 |
| PI | 128+78.63 | 1,891,011.610 | 1,093,978.965 |
| PT | 129+97.00 | 1,891,068.225 | 1,094,097.034 |
| EOA | 130+86.01 | 1,891,106.713 | 1,094,177.300 |

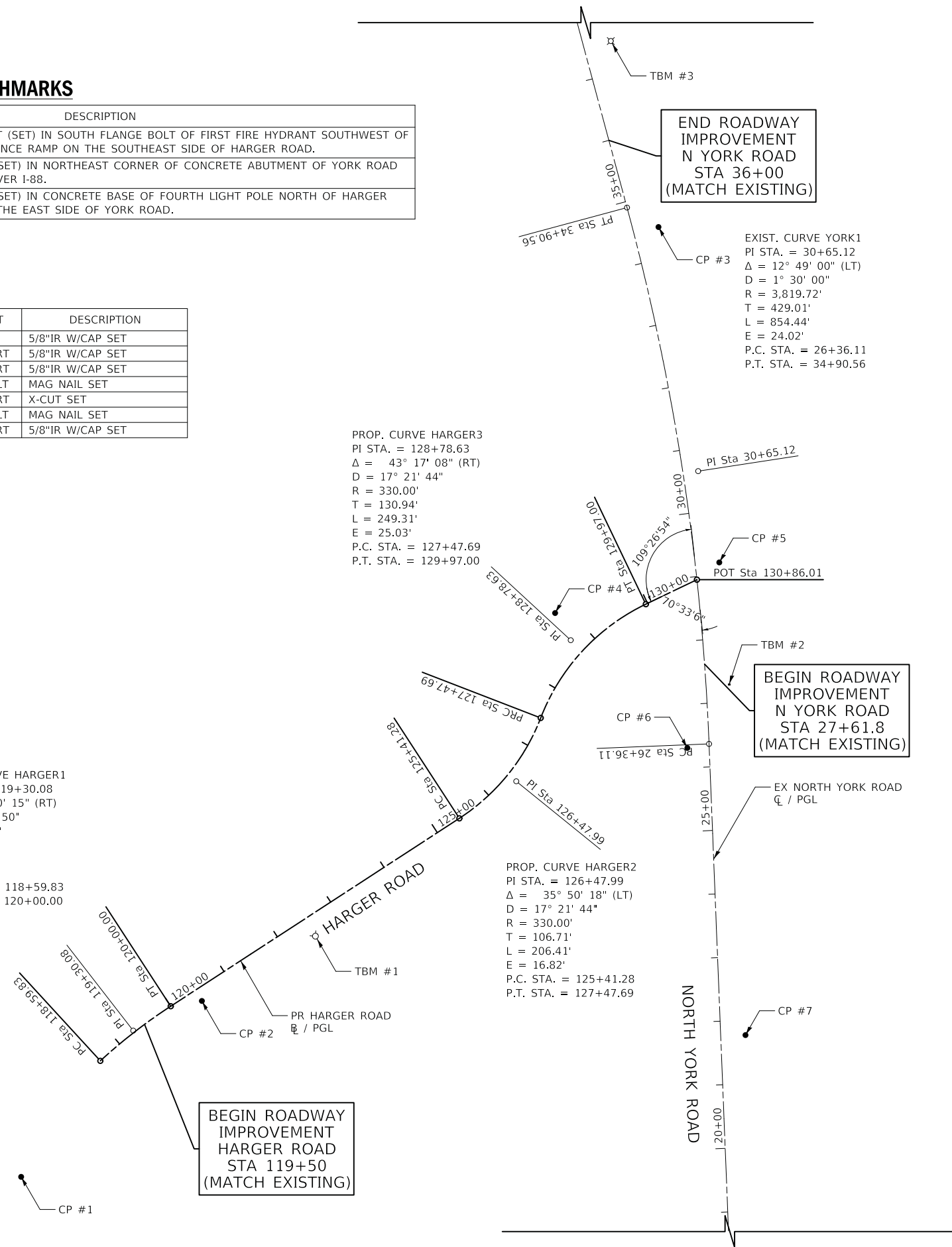


PROP. CURVE HARGER1
 PI STA. = 119+30.08
 $\Delta = 9^\circ 30' 15''$ (RT)
 D = 6° 46' 50"
 R = 845.00'
 T = 70.25'
 L = 140.17'
 E = 2.91'
 P.C. STA. = 118+59.83
 P.T. STA. = 120+00.00

PROP. CURVE HARGER3
 PI STA. = 128+78.63
 $\Delta = 43^\circ 17' 08''$ (RT)
 D = 17° 21' 44"
 R = 330.00'
 T = 130.94'
 L = 249.31'
 E = 25.03'
 P.C. STA. = 127+47.69
 P.T. STA. = 129+97.00

PROP. CURVE HARGER2
 PI STA. = 126+47.99
 $\Delta = 35^\circ 50' 18''$ (LT)
 D = 17° 21' 44"
 R = 330.00'
 T = 106.71'
 L = 206.41'
 E = 16.82'
 P.C. STA. = 125+41.28
 P.T. STA. = 127+47.69

EXIST. CURVE YORK1
 PI STA. = 30+65.12
 $\Delta = 12^\circ 49' 00''$ (LT)
 D = 1° 30' 00"
 R = 3,819.72'
 T = 429.01'
 L = 854.44'
 E = 24.02'
 P.C. STA. = 26+36.11
 P.T. STA. = 34+90.56

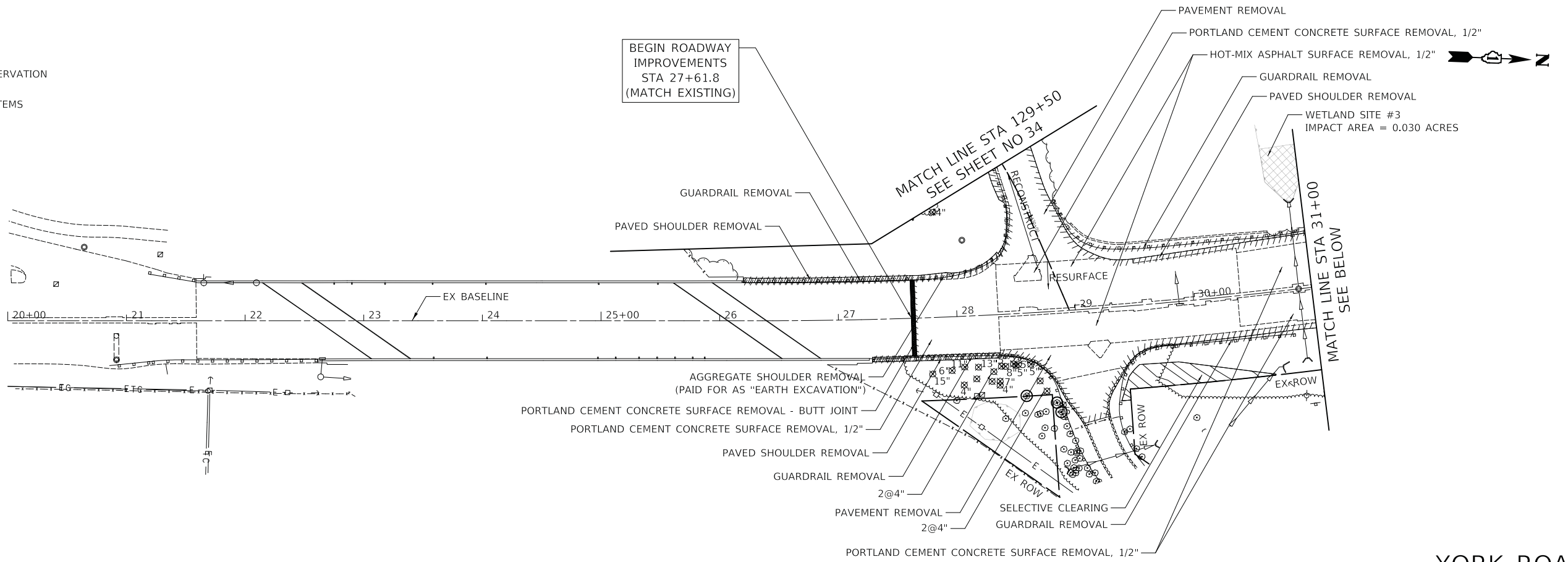


NOTE:

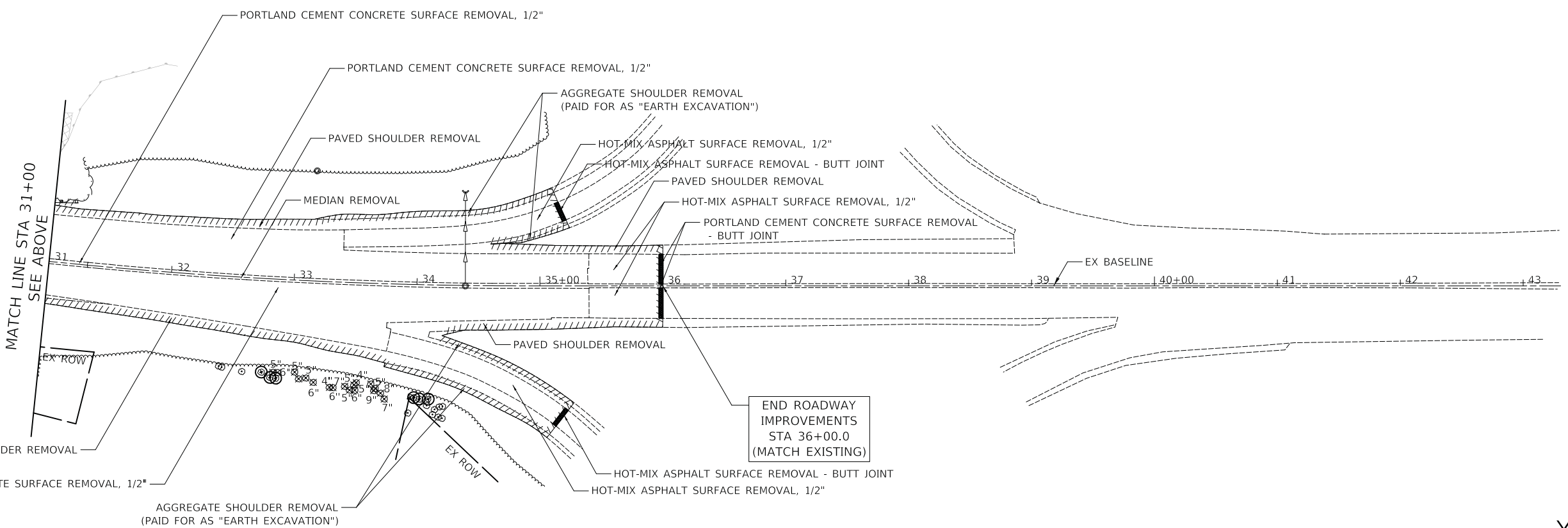
1. BEARINGS AND COORDINATES ARE REFERENCED TO THE ILLINOIS COORDINATE SYSTEM NAD 83(2011) EAST ZONE.

LEGEND

- ⊗ XX" TREE REMOVAL & SIZE
- ⊙ TREE PROTECTION AND PRESERVATION (ALSO SHOWN ON EC PLANS)
- ▨ REMOVAL LIMITS (REMOVAL ITEMS CALLED OUT ON PLANS)
- ▧ WETLAND IMPACTS
- ▩ SELECTIVE CLEARING



YORK ROAD

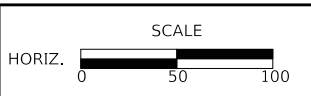


YORK ROAD

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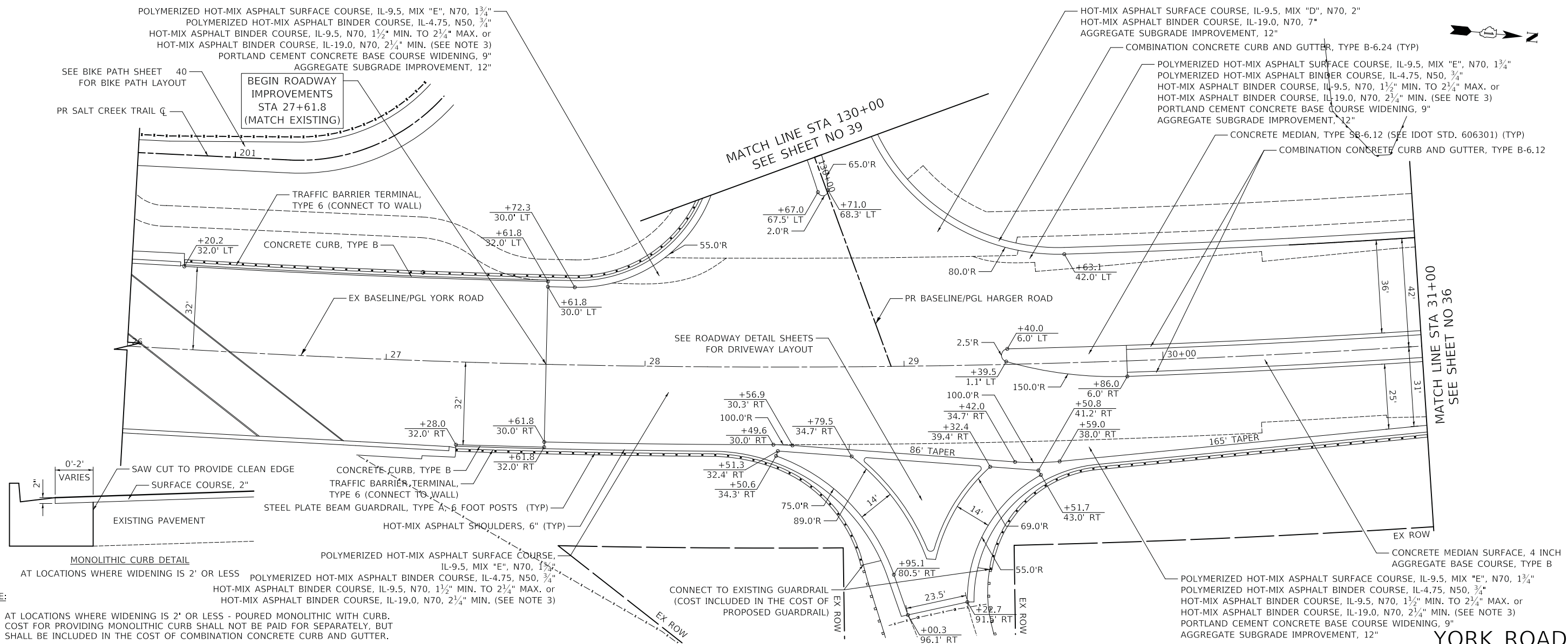
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|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



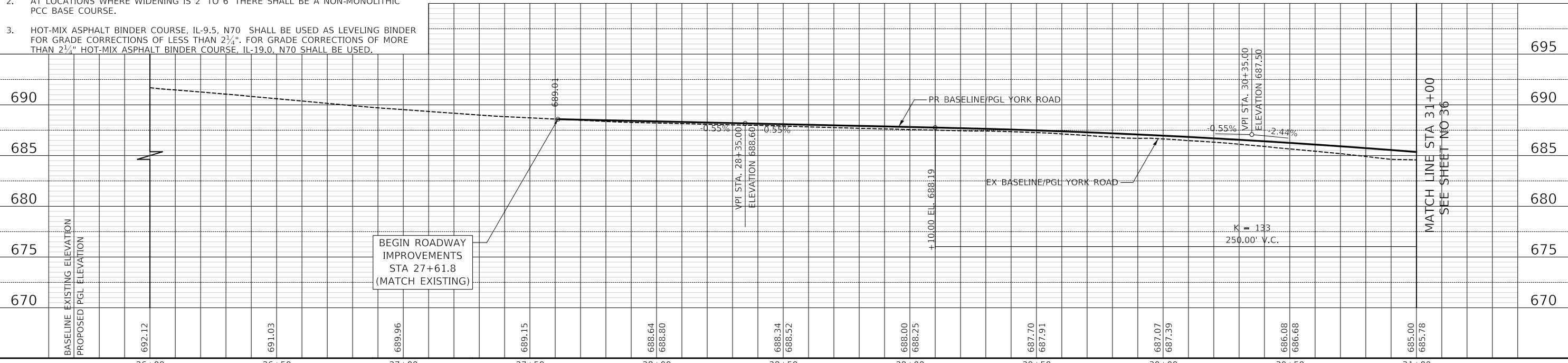
| | |
|---------------------|--------------|
| REMOVAL PLAN | |
| SHEET 1 OF 2 SHEETS | STA. TO STA. |

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 33 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTE:

- AT LOCATIONS WHERE WIDENING IS 2' OR LESS - POURED MONOLITHIC WITH CURB. COST FOR PROVIDING MONOLITHIC CURB SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER.
- AT LOCATIONS WHERE WIDENING IS 2' TO 6' THERE SHALL BE A NON-MONOLITHIC PCC BASE COURSE.
- HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 SHALL BE USED AS LEVELING BINDER FOR GRADE CORRECTIONS OF LESS THAN 2 1/4". FOR GRADE CORRECTIONS OF MORE THAN 2 1/4" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 SHALL BE USED.

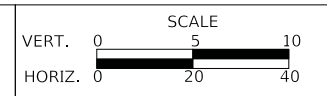


| STATION | BASELINE EXISTING ELEVATION | PROPOSED PGL ELEVATION |
|---------|-----------------------------|------------------------|
| 26+00 | 692.12 | |
| 26+50 | 691.03 | |
| 27+00 | 689.96 | |
| 27+50 | 689.15 | |
| 28+00 | 688.64 | 688.90 |
| 28+50 | 688.34 | 688.52 |
| 29+00 | 688.00 | 688.25 |
| 29+50 | 687.70 | 687.91 |
| 30+00 | 687.07 | 687.39 |
| 30+50 | 686.08 | 686.68 |
| 31+00 | 685.00 | 685.78 |

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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

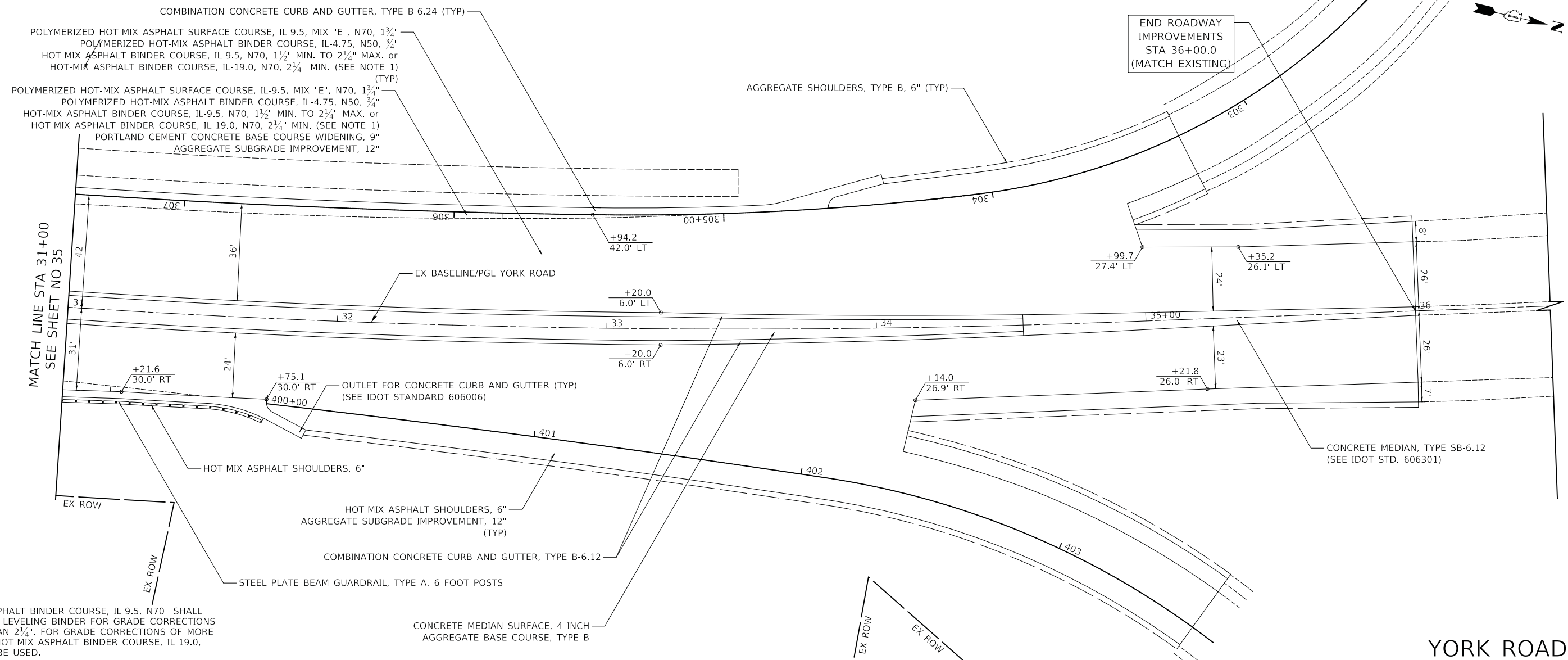
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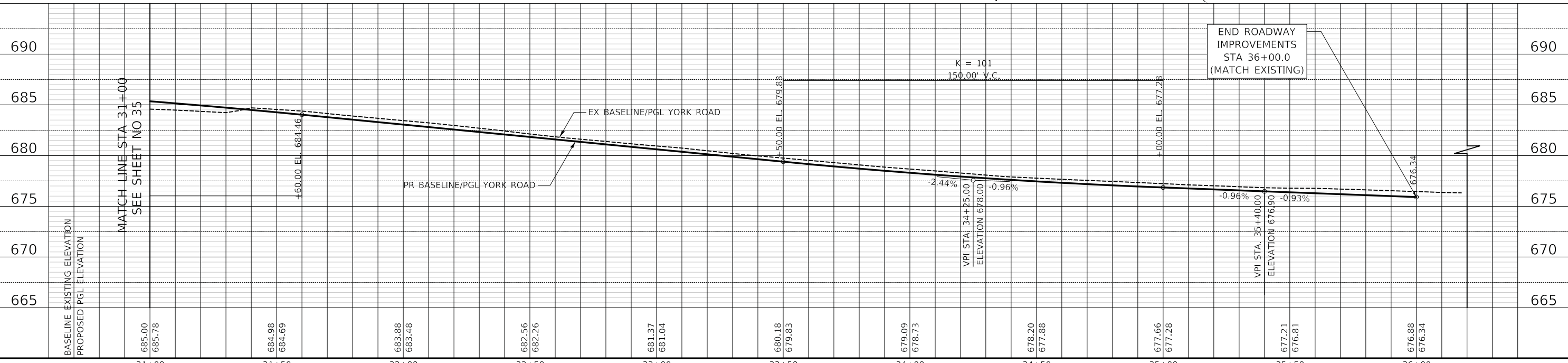
PLAN AND PROFILE

SHEET 1 OF 5 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 35 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTE:
 1. HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 SHALL BE USED AS LEVELING BINDER FOR GRADE CORRECTIONS OF LESS THAN 2 1/4". FOR GRADE CORRECTIONS OF MORE THAN 2 1/4" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 SHALL BE USED.



| STATION | BASELINE EXISTING ELEVATION | PROPOSED PGL ELEVATION |
|---------|-----------------------------|------------------------|
| 31+00 | 685.00 | 685.78 |
| 31+50 | 684.98 | 684.69 |
| 32+00 | 683.88 | 683.48 |
| 32+50 | 682.56 | 682.26 |
| 33+00 | 681.37 | 681.04 |
| 33+50 | 680.18 | 679.83 |
| 34+00 | 679.09 | 678.73 |
| 34+50 | 678.20 | 677.88 |
| 35+00 | 677.66 | 677.28 |
| 35+50 | 677.21 | 676.81 |
| 36+00 | 676.88 | 676.34 |

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 DRAWN - MC
 CHECKED - JRV
 DATE - 12/27/2019

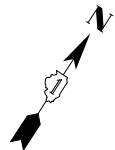
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DEPARTMENT OF TRANSPORTATION

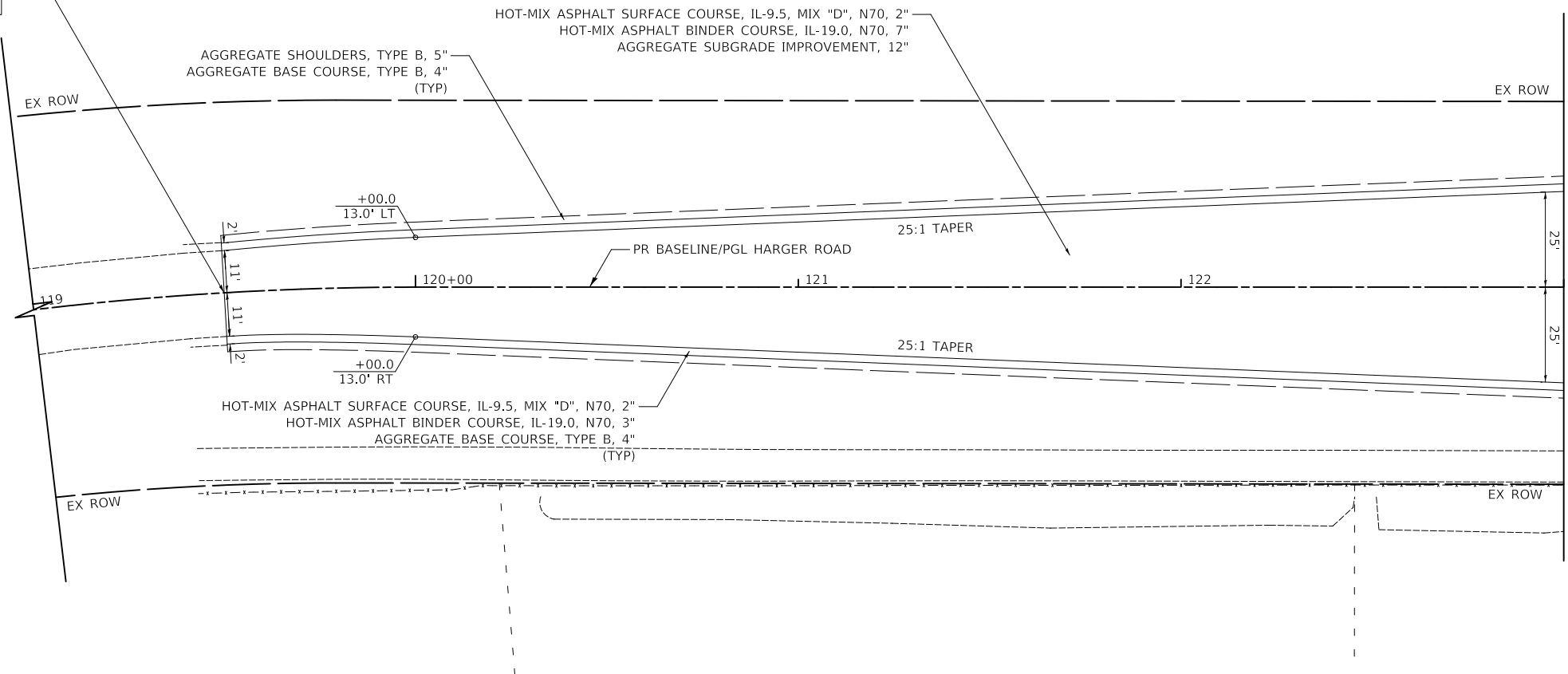
SCALE
 VERT. 0 5 10
 HORIZ. 0 20 40

PLAN AND PROFILE
 SHEET 2 OF 5 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|--------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY | TOTAL SHEETS 208 | SHEET NO. 36 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

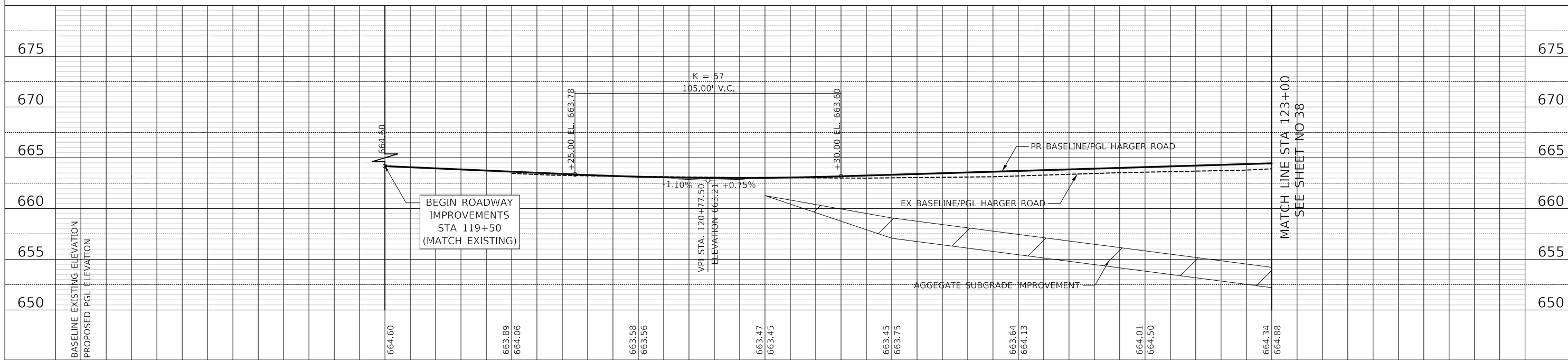


BEGIN ROADWAY IMPROVEMENTS STA 119+50 (MATCH EXISTING)



MATCH LINE STA 123+00
SEE SHEET NO 38

HARGER ROAD

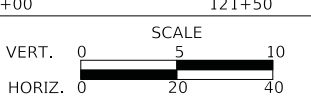


MATCH LINE STA 123+00
SEE SHEET NO 38

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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

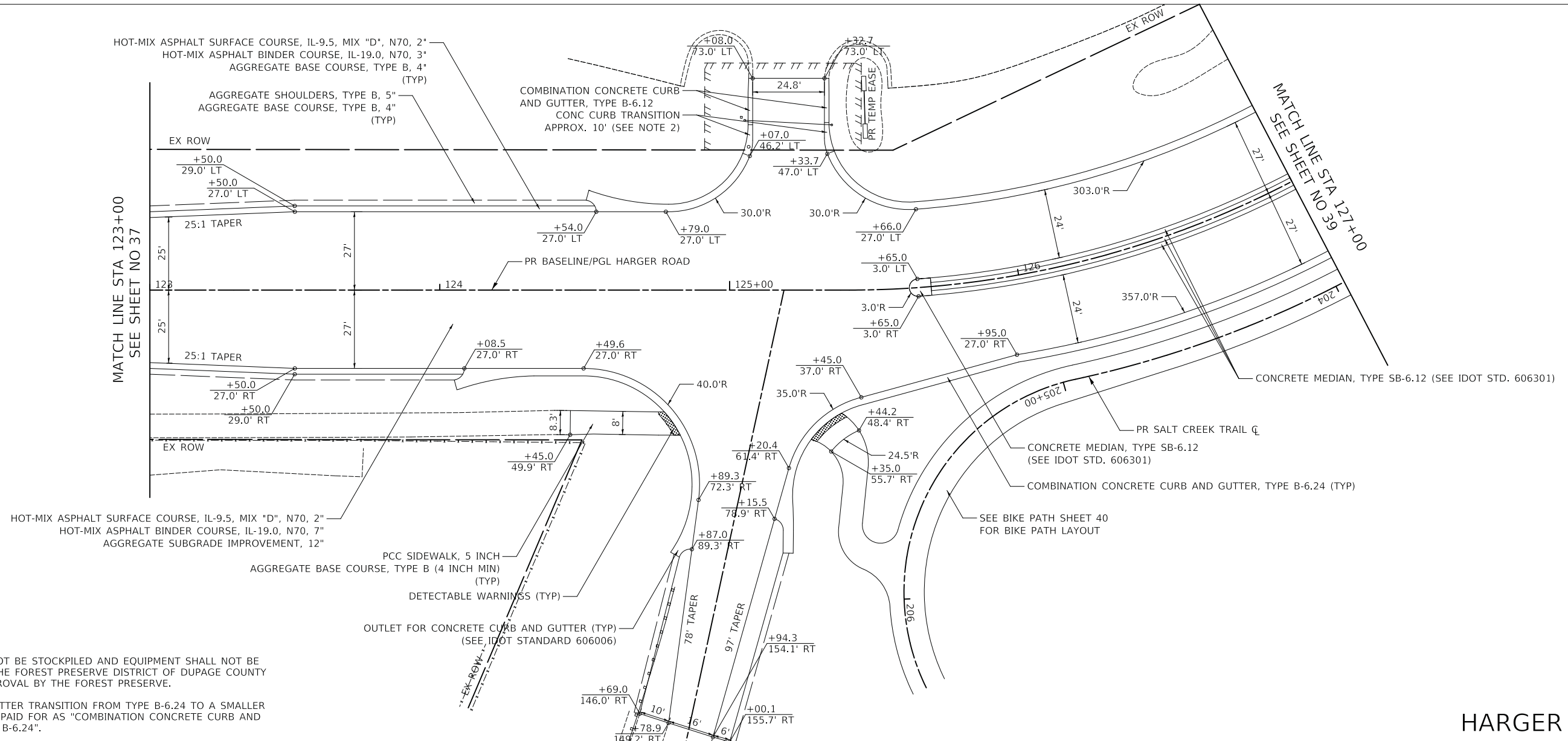
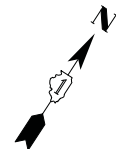
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DEPARTMENT OF TRANSPORTATION**



PLAN AND PROFILE

SHEET 3 OF 5 SHEETS STA. TO STA.

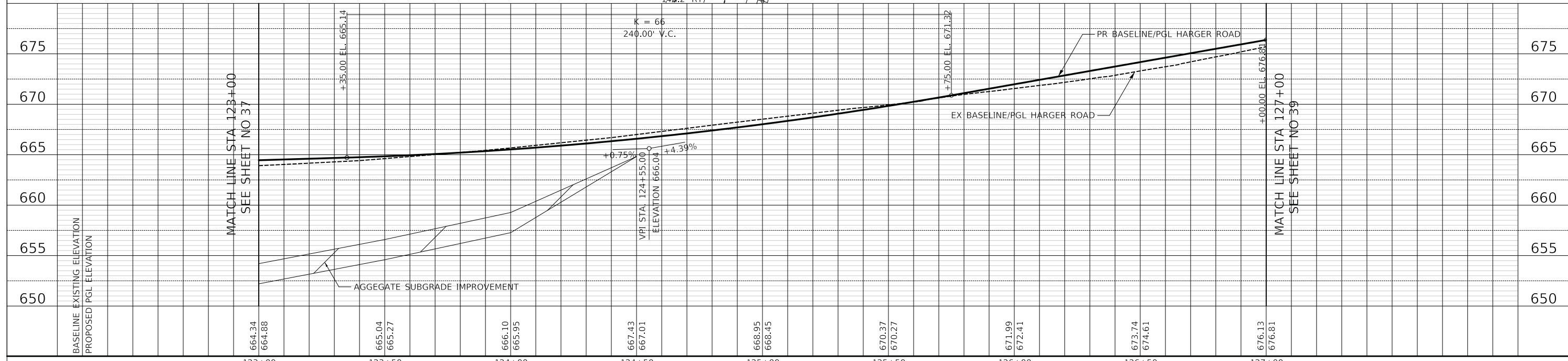
| | | | | |
|-----------------------|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 37 |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |



NOTE:

- SOIL SHALL NOT BE STOCKPILED AND EQUIPMENT SHALL NOT BE STORED ON THE FOREST PRESERVE DISTRICT OF DUPAGE COUNTY WITHOUT APPROVAL BY THE FOREST PRESERVE.
- CURB AND GUTTER TRANSITION FROM TYPE B-6.24 TO A SMALLER TYPE WILL BE PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24".

HARGER ROAD

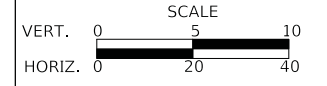


| STATION | PROPOSED PGL ELEVATION | BASELINE EXISTING ELEVATION |
|---------|------------------------|-----------------------------|
| 123+00 | 664.34 | 664.34 |
| 123+05 | 664.88 | 664.88 |
| 123+10 | 665.04 | 665.04 |
| 123+15 | 665.27 | 665.27 |
| 123+20 | 666.10 | 666.10 |
| 123+25 | 665.95 | 665.95 |
| 123+30 | 667.43 | 667.43 |
| 123+35 | 667.01 | 667.01 |
| 123+40 | 668.95 | 668.95 |
| 123+45 | 668.45 | 668.45 |
| 123+50 | 670.37 | 670.37 |
| 123+55 | 670.27 | 670.27 |
| 123+60 | 671.99 | 671.99 |
| 123+65 | 672.41 | 672.41 |
| 123+70 | 673.74 | 673.74 |
| 123+75 | 674.61 | 674.61 |
| 123+80 | 676.13 | 676.13 |
| 123+85 | 676.81 | 676.81 |

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| | |
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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

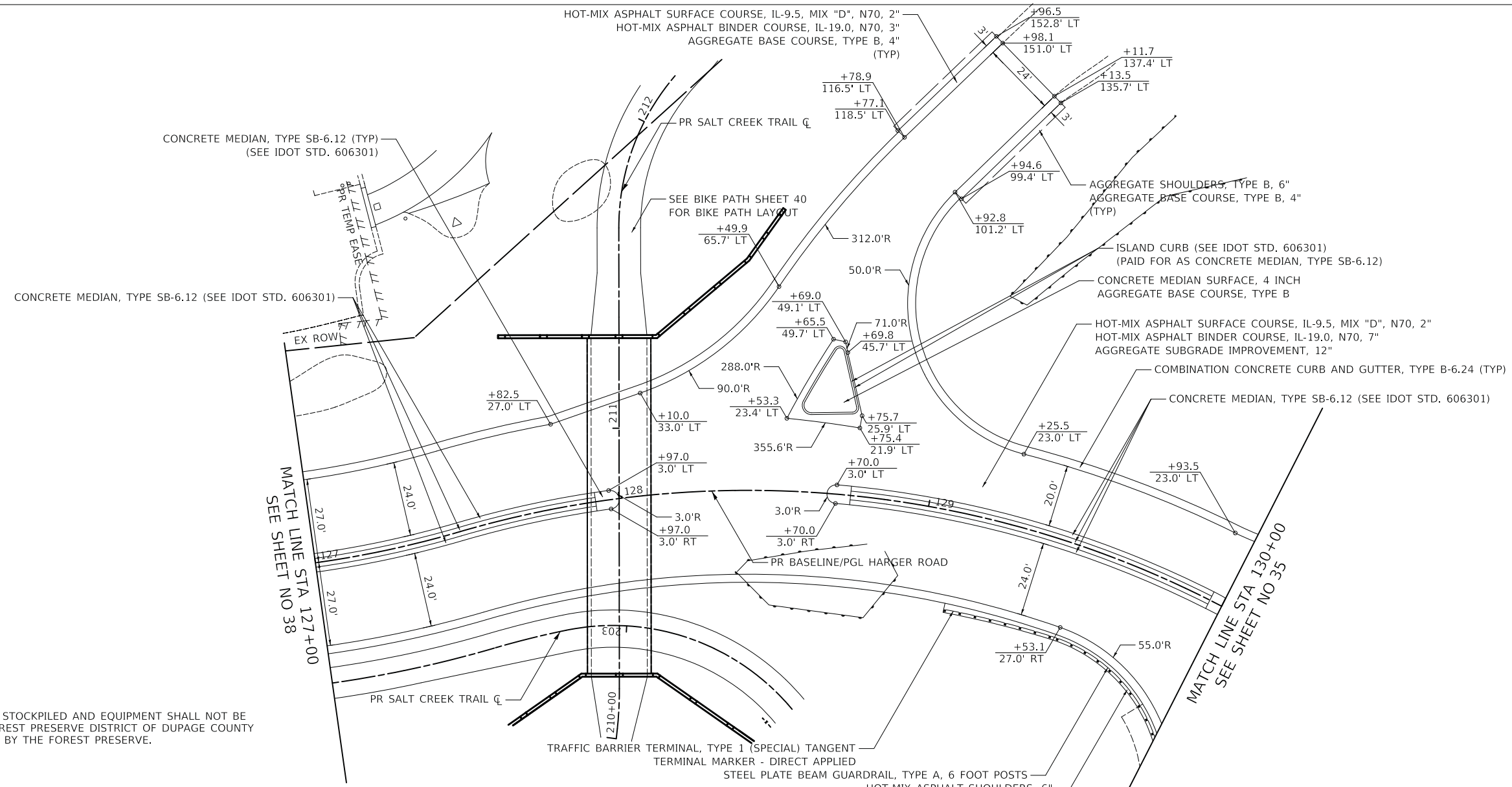
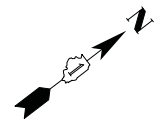
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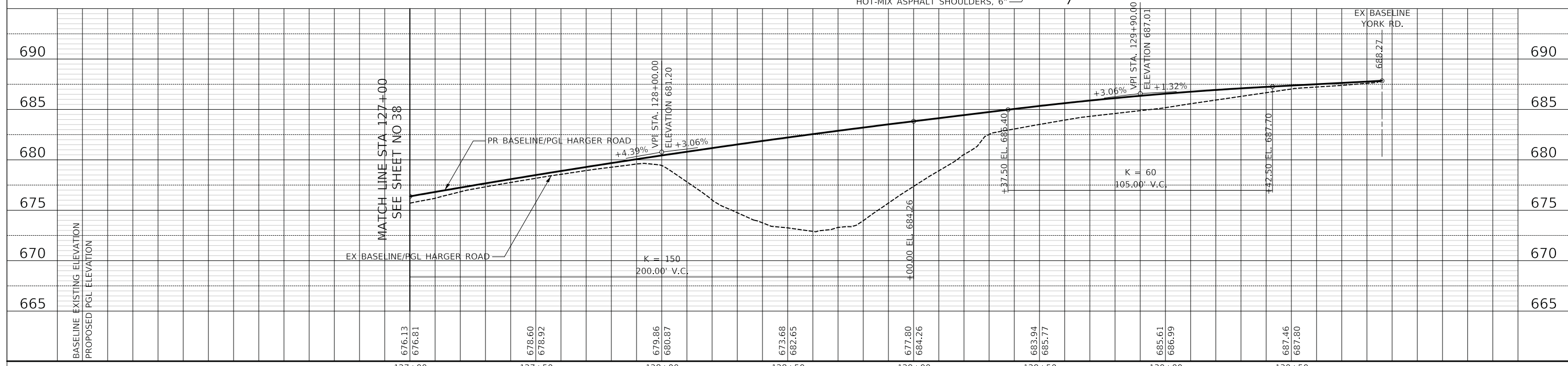
PLAN AND PROFILE

SHEET 4 OF 5 SHEETS STA. TO STA.

| | | | | |
|-----------------------|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 38 |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |



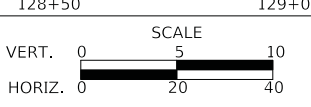
HARGER ROAD



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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

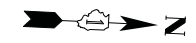
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 DEPARTMENT OF TRANSPORTATION**



PLAN AND PROFILE

SHEET 5 OF 5 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 39 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



PROP. CURVE BP1
 PI STA. = 204+16.57
 $\Delta = 151^\circ 29' 59''$ (LT)
 $D = 76^\circ 23' 40''$
 $R = 75.00'$
 $T = 295.31'$
 $L = 198.31'$
 $E = 229.69'$
 P.C. STA. = 201+21.26
 P.C.C. STA. = 203+19.57
 DESIGN SPEED = 20 MPH
 WITH 20° LEAN ANGLE

PROP. CURVE BP2
 PI STA. = 203+31.51
 $\Delta = 4^\circ 41' 53''$ (LT)
 $D = 19^\circ 41' 42''$
 $R = 290.92'$
 $T = 11.93'$
 $L = 23.85'$
 $E = 0.24'$
 P.C.C. STA. = 203+19.57
 P.R.C. STA. = 203+43.43
 DESIGN SPEED = 18 MPH
 WITH 20° LEAN ANGLE

PROP. CURVE BP3
 PI STA. = 204+20.65
 $\Delta = 23^\circ 38' 04''$ (RT)
 $D = 15^\circ 31' 26''$
 $R = 369.08'$
 $T = 77.22'$
 $L = 152.25'$
 $E = 7.99'$
 P.R.C. STA. = 203+43.43
 P.R.C. STA. = 204+95.67
 DESIGN SPEED = 20 MPH
 WITH 20° LEAN ANGLE

PROP. CURVE BP4
 PI STA. = 208+65.41
 $\Delta = 202^\circ 56' 01''$ (LT)
 $D = 76^\circ 23' 40''$
 $R = 75.00'$
 $T = 369.73'$
 $L = 265.64'$
 $E = 452.26'$
 P.R.C. STA. = 204+95.67
 P.T. STA. = 207+61.31
 DESIGN SPEED = 20 MPH
 WITH 20° LEAN ANGLE

PROP. CURVE BP5
 PI STA. = 209+48.69
 $\Delta = 74^\circ 27' 43''$ (LT)
 $D = 45^\circ 50' 12''$
 $R = 125.00'$
 $T = 94.99'$
 $L = 162.45'$
 $E = 32.00'$
 P.C. STA. = 208+53.70
 P.T. STA. = 210+16.15
 DESIGN SPEED = 20 MPH
 WITH 20° LEAN ANGLE

PROP. CURVE BP6
 PI STA. = 211+95.25
 $\Delta = 44^\circ 33' 04''$ (RT)
 $D = 76^\circ 23' 40''$
 $R = 75.00'$
 $T = 30.72'$
 $L = 58.32'$
 $E = 6.05'$
 P.C. STA. = 211+64.53
 P.T. STA. = 212+22.84
 DESIGN SPEED = 20 MPH
 WITH 20° LEAN ANGLE

PROP. CURVE BP7
 PI STA. = 212+94.56
 $\Delta = 87^\circ 26' 20''$ (LT)
 $D = 76^\circ 23' 40''$
 $R = 75.00'$
 $T = 71.72'$
 $L = 114.46'$
 $E = 28.77'$
 P.C. STA. = 212+22.84
 P.T. STA. = 213+37.30
 DESIGN SPEED = 20 MPH
 WITH 20° LEAN ANGLE

HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 3"
 AGGREGATE BASE COURSE, TYPE B, 6"

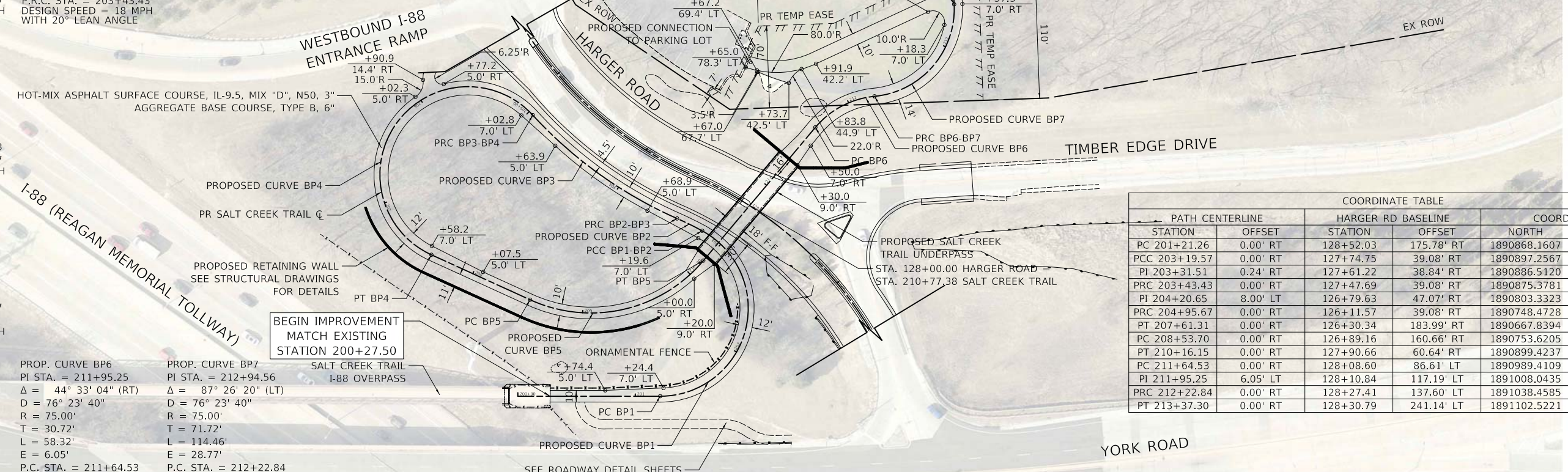
WESTBOUND I-88
 ENTRANCE RAMP
 PROPOSED CURVE BP4
 PR SALT CREEK TRAIL
 PROPOSED RETAINING WALL
 SEE STRUCTURAL DRAWINGS
 FOR DETAILS
 PT BP4
 BEGIN IMPROVEMENT
 MATCH EXISTING
 STATION 200+27.50

SALT CREEK TRAIL
 I-88 OVERPASS

SEE ROADWAY DETAIL SHEETS
 FOR SHELF LAYOUT

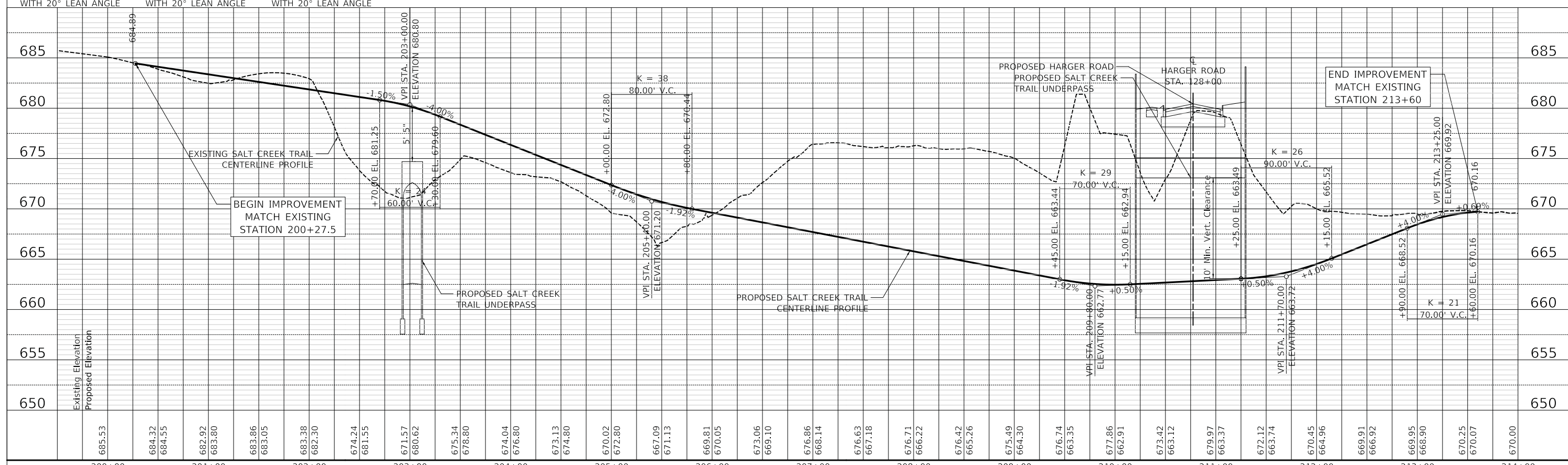
END IMPROVEMENT
 MATCH EXISTING
 STATION 213+60

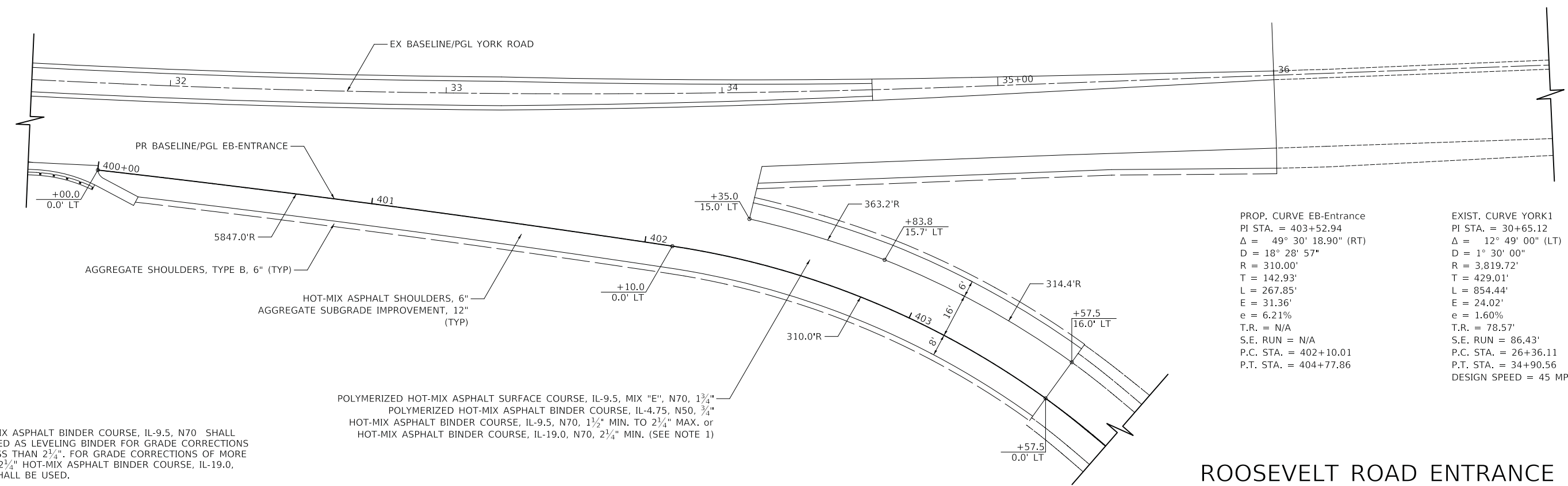
YORK WOODS FOREST PRESERVE
 (FOREST PRESERVE DISTRICT OF DUPAGE COUNTY)



| COORDINATE TABLE | | | | | |
|------------------|----------|--------------------|------------|--------------|--------------|
| PATH CENTERLINE | | HARGER RD BASELINE | | COORDINATES | |
| STATION | OFFSET | STATION | OFFSET | NORTH | EAST |
| PC 201+21.26 | 0.00' RT | 128+52.03 | 175.78' RT | 1890868.1607 | 1094120.2284 |
| PCC 203+19.57 | 0.00' RT | 127+74.75 | 39.08' RT | 1890897.2567 | 1093977.7851 |
| PI 203+31.51 | 0.24' RT | 127+61.22 | 38.84' RT | 1890886.5120 | 1093972.5921 |
| PRC 203+43.43 | 0.00' RT | 127+47.69 | 39.08' RT | 1890875.3781 | 1093968.2967 |
| PI 204+20.65 | 8.00' LT | 126+79.63 | 47.07' RT | 1890803.3323 | 1093940.5013 |
| PRC 204+95.67 | 0.00' RT | 126+11.57 | 39.08' RT | 1890748.4728 | 1093886.1540 |
| PT 207+61.31 | 0.00' RT | 126+30.34 | 183.99' RT | 1890667.8394 | 1094009.0727 |
| PC 208+53.70 | 0.00' RT | 126+89.16 | 160.66' RT | 1890753.6205 | 1094043.3787 |
| PT 210+16.15 | 0.00' RT | 127+90.66 | 60.64' RT | 1890899.4237 | 1094003.1265 |
| PC 211+64.53 | 0.00' RT | 128+08.60 | 86.61' LT | 1890989.4109 | 1093885.1540 |
| PI 211+95.25 | 6.05' LT | 128+10.84 | 117.19' LT | 1891008.0435 | 1093860.7267 |
| PRC 212+22.84 | 0.00' RT | 128+27.41 | 137.60' LT | 1891038.4585 | 1093856.3909 |
| PT 213+37.30 | 0.00' RT | 128+30.79 | 241.14' LT | 1891102.5221 | 1093774.8853 |

SALT CREEK TRAIL



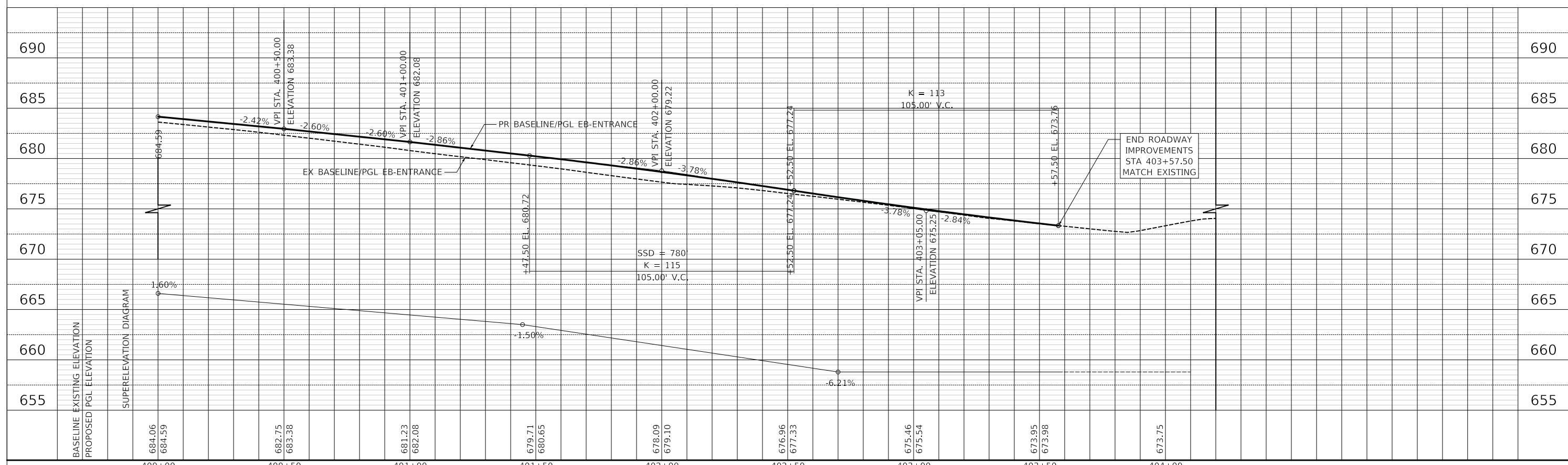


| PROP. CURVE EB-Entrance | EXIST. CURVE YORK1 |
|--------------------------------------|-----------------------------------|
| PI STA. = 403+52.94 | PI STA. = 30+65.12 |
| $\Delta = 49^\circ 30' 18.90''$ (RT) | $\Delta = 12^\circ 49' 00''$ (LT) |
| D = 18° 28' 57" | D = 1° 30' 00" |
| R = 310.00' | R = 3,819.72' |
| T = 142.93' | T = 429.01' |
| L = 267.85' | L = 854.44' |
| E = 31.36' | E = 24.02' |
| e = 6.21% | e = 1.60% |
| T.R. = N/A | T.R. = 78.57' |
| S.E. RUN = N/A | S.E. RUN = 86.43' |
| P.C. STA. = 402+10.01 | P.C. STA. = 26+36.11 |
| P.T. STA. = 404+77.86 | P.T. STA. = 34+90.56 |
| | DESIGN SPEED = 45 MPH |

NOTE:
 1. HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 SHALL BE USED AS LEVELING BINDER FOR GRADE CORRECTIONS OF LESS THAN 2 1/4". FOR GRADE CORRECTIONS OF MORE THAN 2 1/4" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 SHALL BE USED.

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1 3/4"
 POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
 HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1 1/2" MIN. TO 2 1/4" MAX. or
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/4" MIN. (SEE NOTE 1)

ROOSEVELT ROAD ENTRANCE RAMP



| | | | | | | | | | | | | | | | | |
|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| 684.06 | 684.59 | 682.75 | 683.38 | 681.23 | 682.08 | 679.71 | 680.65 | 678.09 | 679.10 | 676.96 | 677.33 | 675.46 | 675.54 | 673.95 | 673.98 | 673.75 |
| 400+00 | 400+50 | 401+00 | 401+50 | 402+00 | 402+50 | 403+00 | 403+50 | 404+00 | | | | | | | | |

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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

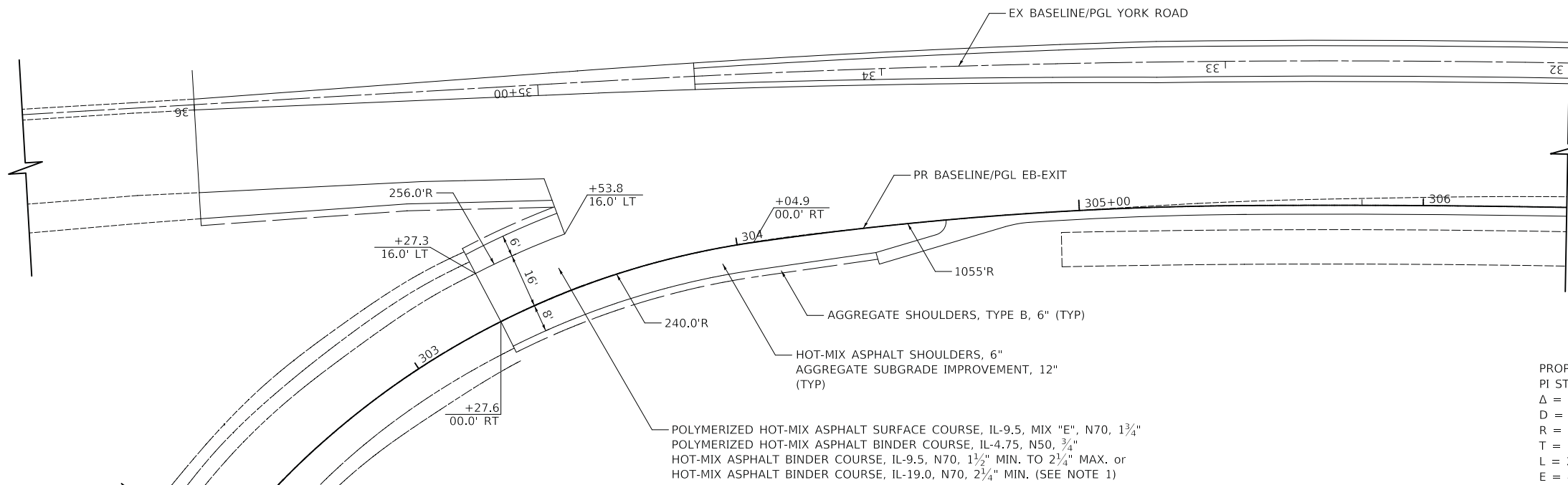
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROOSEVELT ROAD ENTRANCE RAMP PLAN AND PROFILE

SHEET 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 41 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

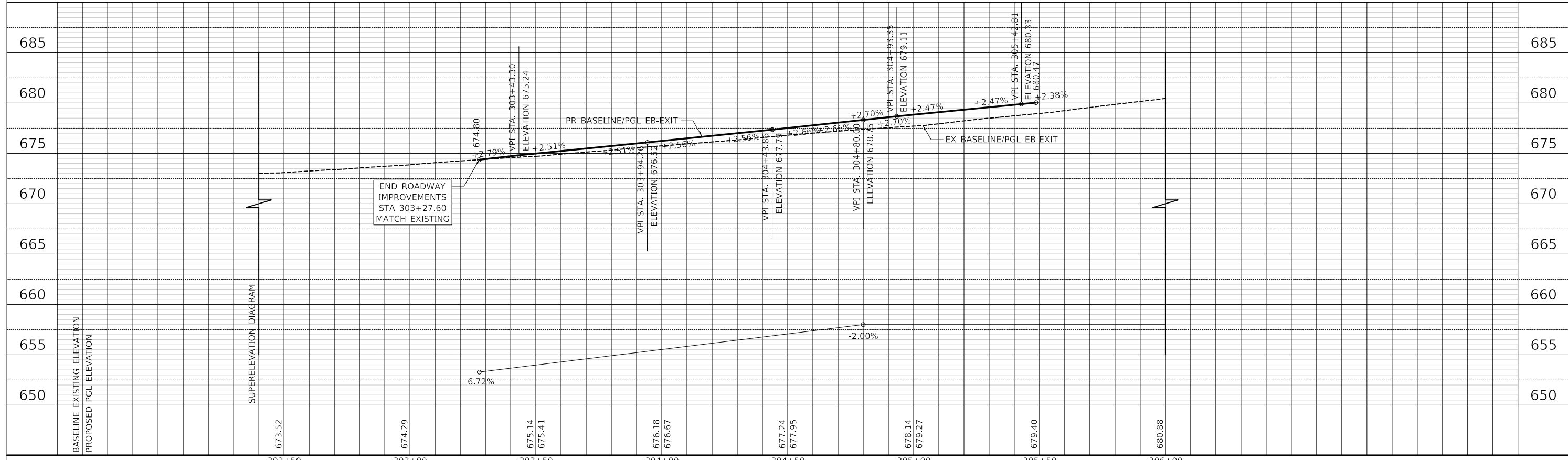


| PROP. CURVE EB-EXIT | EXIST. CURVE YORK1 |
|-----------------------------------|-----------------------------------|
| PI STA. = 302+88.49 | PI STA. = 30+65.12 |
| $\Delta = 62^\circ 40' 04''$ (RT) | $\Delta = 12^\circ 49' 00''$ (LT) |
| $D = 23^\circ 52' 24''$ | $D = 1^\circ 30' 00''$ |
| R = 240.00' | R = 3,819.72' |
| T = 146.12' | T = 429.01' |
| L = 262.50' | L = 854.44' |
| E = 40.98' | E = 24.02' |
| e = 6.72% | e = 1.60% |
| T.R. = N/A | T.R. = 78.57' |
| S.E. RUN = N/A | S.E. RUN = 86.43' |
| P.C. STA. = 301+42.38 | P.C. STA. = 26+36.11 |
| P.T. STA. = 304+04.88 | P.T. STA. = 34+90.56 |
| | DESIGN SPEED = 45 MPH |

NOTE:

- HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 SHALL BE USED AS LEVELING BINDER FOR GRADE CORRECTIONS OF LESS THAN $2\frac{1}{4}$ ". FOR GRADE CORRECTIONS OF MORE THAN $2\frac{1}{4}$ " HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 SHALL BE USED.

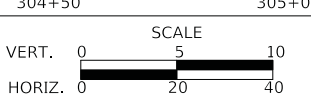
ROOSEVELT ROAD EXIT RAMP



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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

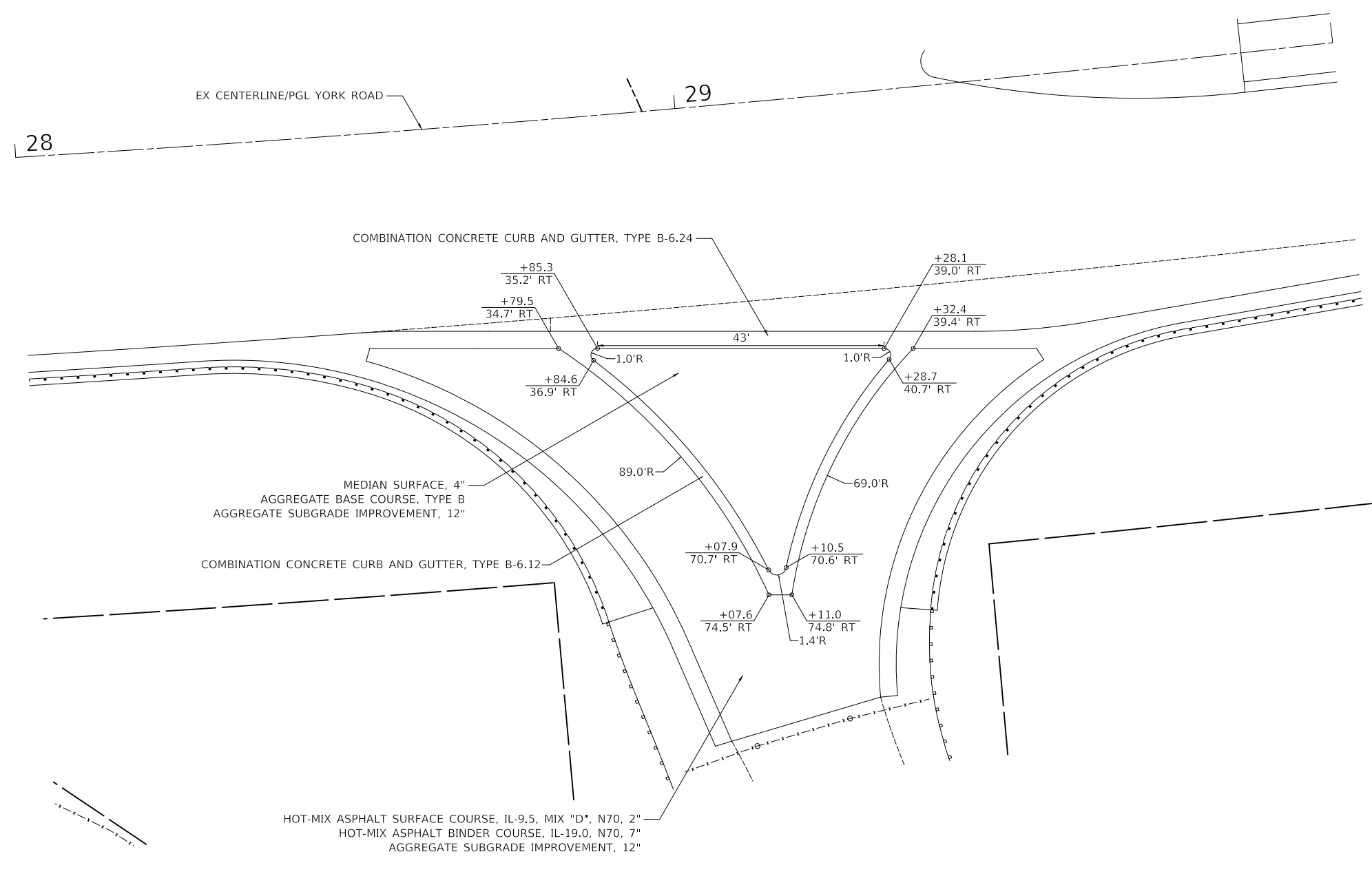
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



ROOSEVELT ROAD EXIT RAMP PLAN AND PROFILE

SHEET 1 OF 1 SHEETS STA. TO STA.

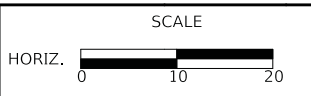
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|---|------------------------|--------|--------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY | TOTAL SHEETS 208 | SHEET NO. 42 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |




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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

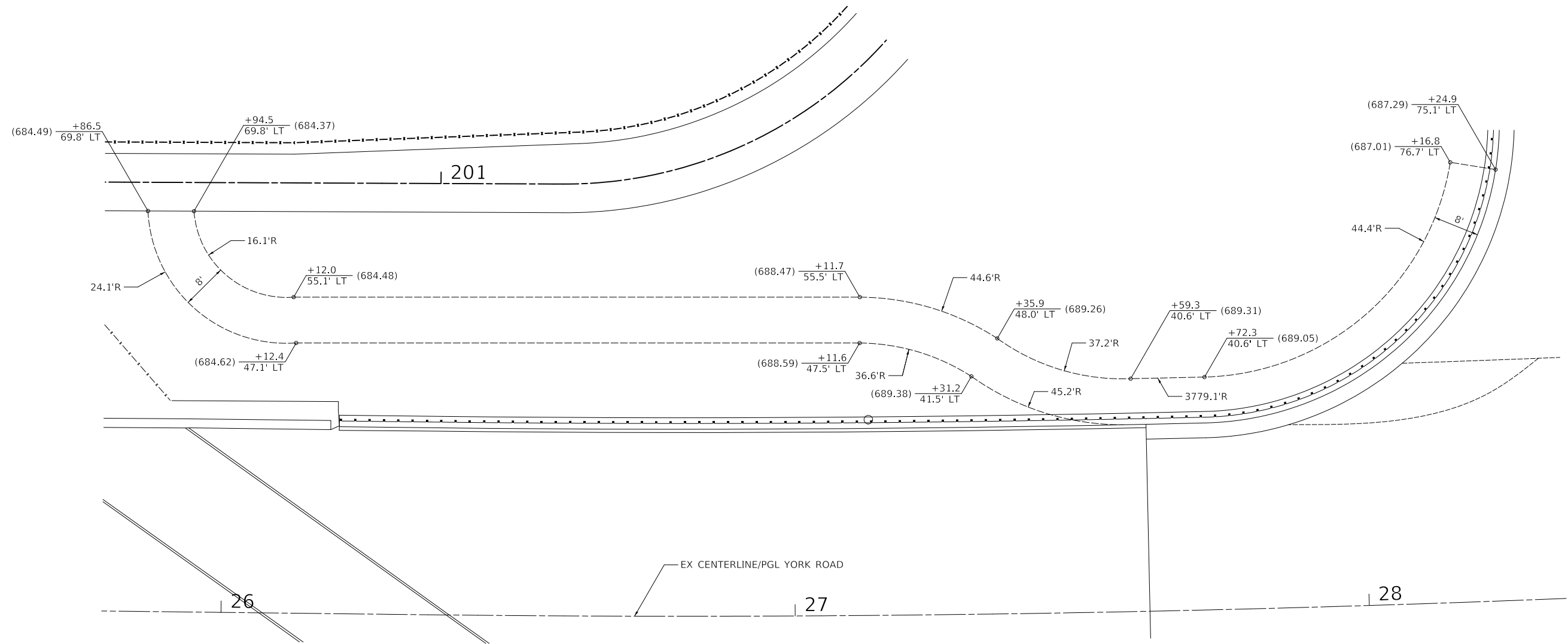
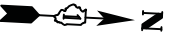
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY DETAILS

SHEET 1 OF 5 SHEETS STA. TO STA.

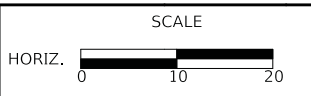
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 43 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |




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|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

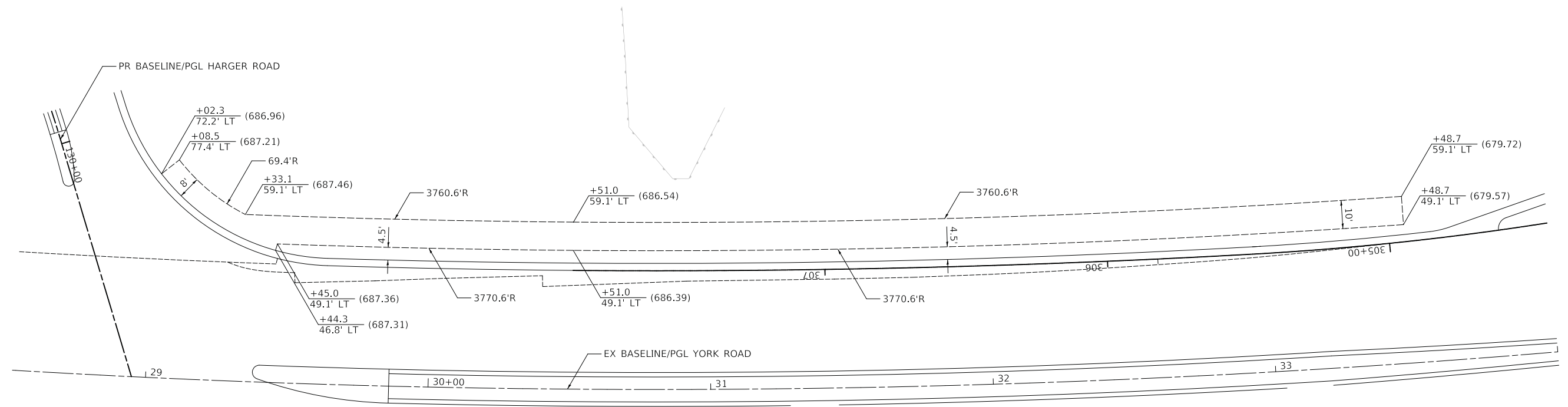
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY DETAILS

SHEET 2 OF 5 SHEETS STA. TO STA.

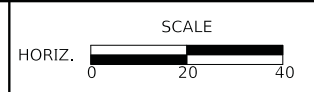
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 44 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |




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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

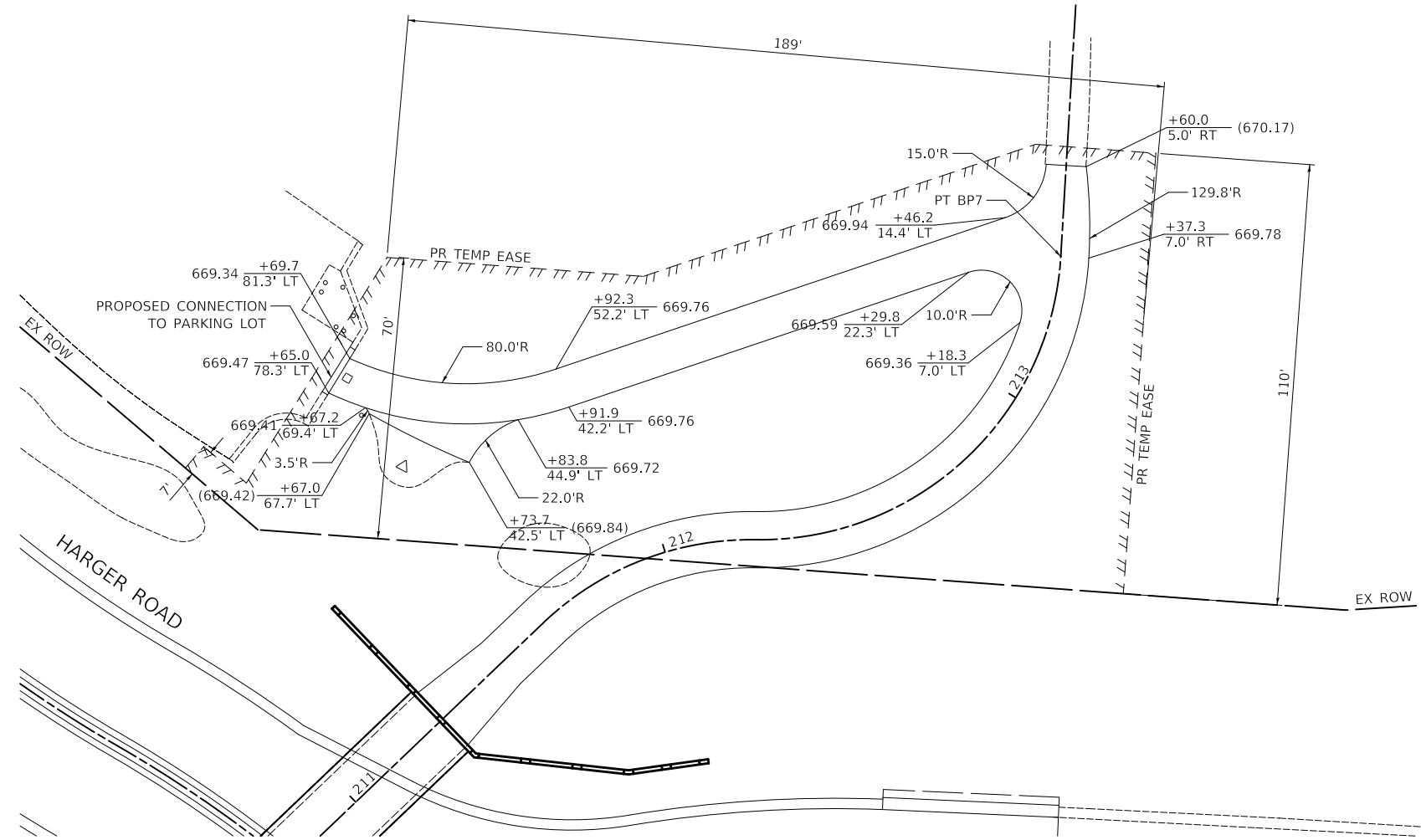
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY DETAILS

SHEET 3 OF 5 SHEETS STA. TO STA.

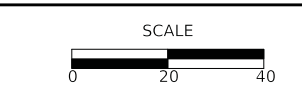
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 45 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



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|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

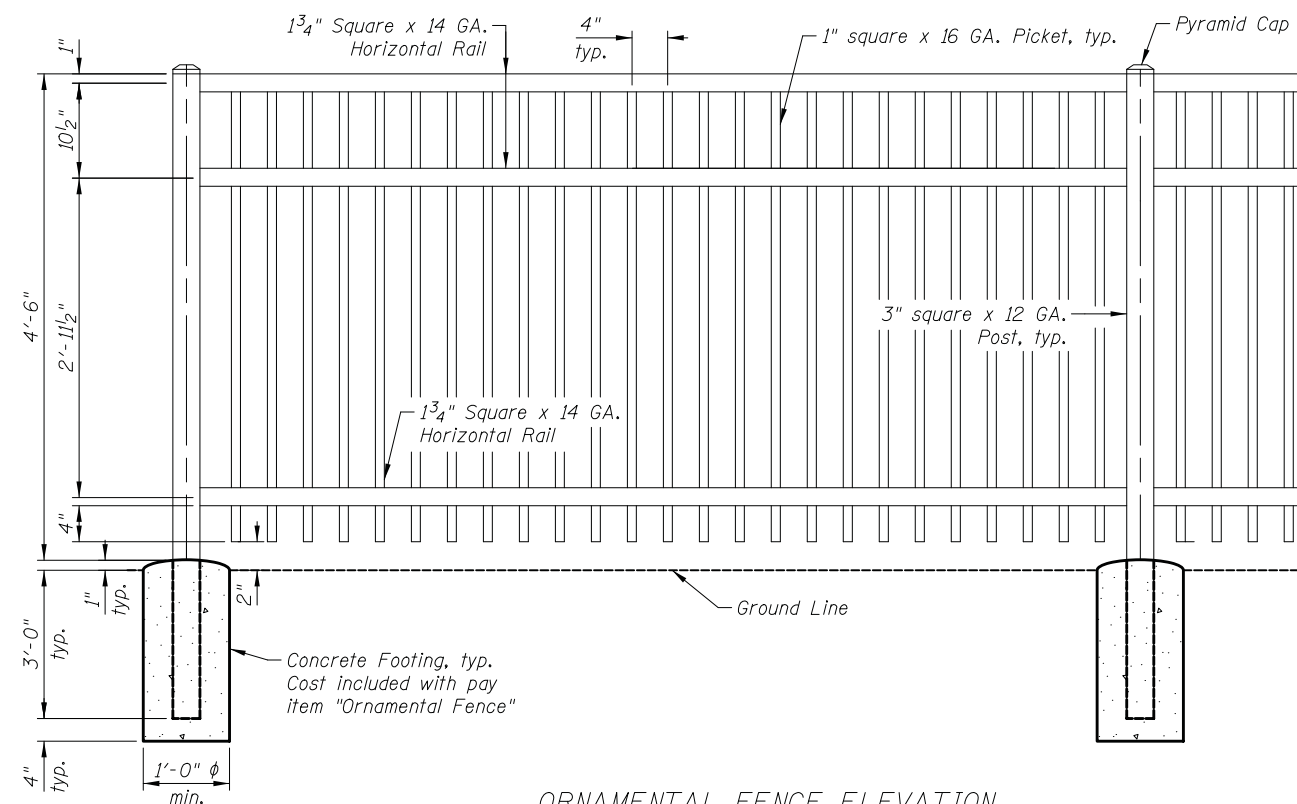
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



ROADWAY DETAILS

SHEET 4 OF 5 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 46 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |



ORNAMENTAL FENCE ELEVATION

All posts, railings, splices, anchor devices, and bent plates shall be painted black.

NOTES

1. THE CONTRACTOR IS REQUIRED TO KEEP A TRAVELED WAY (CAPABLE OF SUPPORTING VEHICLE LOADING) OPEN FOR EMERGENCY VEHICLE ACCESS AT ALL TIMES.
2. THE CONTRACTOR SHALL MAINTAIN AN ADA COMPLIANT PEDESTRIAN PATH THROUGH THE LIMITS OF CONSTRUCTION WHERE ONE EXISTED BEFORE THE START OF THE CONSTRUCTION. IF THE PEDESTRIAN PATH CONNECTION NEEDS TO BE INTERRUPTED FOR A SHORT DURATION OF TIME, A SIDEWALK DETOUR ROUTE WILL NEED TO BE PROVIDED THROUGH SIDE STREETS IN ACCORDANCE WITH IDOT STANDARD 701801. COMPLIANCE WITH THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
3. SIGNS W21-1(O) AND W20-7(O) SHALL BE REMOVED OR COVERED WHEN WORKERS OR FLAGGERS ARE NOT PRESENT FOR MORE THAN ONE HOUR.

PRE-STAGE

1. ERECT ALL ADVANCE CONSTRUCTION SIGNING.
2. IMPLEMENT SOIL & SEDIMENT EROSION CONTROL PLAN AND PROVIDE TEMPORARY FENCE WHERE PERIMETER EROSION CONTROL BARRIER IS NOT SHOWN.
3. REMOVE TREES AND RELOCATE TRAFFIC SIGNS WHERE NECESSARY.
4. CONSTRUCT TEMPORARY PAVEMENT WHERE REQUIRED IN STAGE 1.
5. CONSTRUCT TEMPORARY PATH WHERE REQUIRED IN STAGE 1.
6. RELOCATE EXISTING LIGHT POLES AT YORK ROAD AND I-88 ENTRANCE RAMP.
7. CONSTRUCT AGGREGATE SUBGRADE, BINDER COURSE, AND CURB AND GUTTER ON THE WIDENING AREA OF SOUTHBOUND YORK ROAD BETWEEN HARGER ROAD AND THE EXIT RAMP OF EASTBOUND ROOSEVELT ROAD.
8. REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY MARKINGS, INSTALL TEMPORARY SIGNS AND SHIFT TRAFFIC, INSTALL CONSTRUCTION DRUMS AND TEMPORARY CONCRETE BARRIERS.

STAGE 1

1. REMOVE TREES AND RELOCATE TRAFFIC SIGNS WHERE NECESSARY.
2. REMOVE EXISTING PAVEMENT, SHOULDER, SIDEWALK, AND TRAIL ON EASTBOUND HARGER ROAD FROM STA 119+50 TO TIMBER EDGE DRIVE.
3. REMOVE THE EXISTING PAVEMENT AND SHOULDER ON THE GORE AREA AT THE EXIT RAMP OF EASTBOUND ROOSEVELT ROAD.
4. REMOVE EXISTING SHOULDER ON YORK ROAD FROM HARGER ROAD TO THE BRIDGE.
5. CONSTRUCT AGGREGATE SUBGRADE, BINDER COURSE, CURB AND GUTTER, SIDEWALK, AND BIKEPATH ON EASTBOUND HARGER ROAD.
6. CONSTRUCT THE AGGREGATE SUBGRADE AND BINDER COURSE ON THE GORE AREA AT THE EXIT RAMP OF EASTBOUND ROOSEVELT ROAD.
7. CONSTRUCT CURB AND GUTTER ON YORK ROAD FROM HARGER ROAD TO THE BRIDGE.
8. IMPLEMENT SOIL & SEDIMENT EROSION CONTROL PLAN AND PROVIDE TEMPORARY FENCE WHERE PERIMETER EROSION CONTROL BARRIER IS NOT SHOWN.
9. CONSTRUCT TEMPORARY PAVEMENT WHERE REQUIRED IN STAGE 2.
10. REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY MARKINGS, REMOVE OR RELOCATE TEMPORARY SIGNS, INSTALL NEW TEMPORARY SIGNS AND SHIFT TRAFFIC, REMOVE OR RELOCATE CONSTRUCTION DRUMS AND TEMPORARY CONCRETE BARRIERS.

STAGE 2

1. REMOVE TREES AND RELOCATE TRAFFIC SIGNS WHERE NECESSARY.
2. REMOVE EXISTING PAVEMENT ON WESTBOUND HARGER ROAD FROM STA 119+50 TO STA 122+00.
3. REMOVE EXISTING PAVEMENT AND SHOULDER ON HARGER ROAD BETWEEN TIMBER EDGE DRIVE AND YORK ROAD.
4. REMOVE REMAINING SHOULDER ON THE EXIT RAMP OF EASTBOUND ROOSEVELT ROAD.
5. CONSTRUCT AGGREGATE SUBGRADE, AND BINDER COURSE ON WESTBOUND HARGER ROAD FROM STA 119+50 TO STA 122+00.
6. CONSTRUCT THE REMAINING AGGREGATE SUBGRADE, AND BINDER COURSE ON EASTBOUND HARGER ROAD.
7. CONSTRUCT AGGREGATE SUBGRADE, BINDER COURSE, AND CURB AND GUTTER ON THE EXIT RAMP OF EASTBOUND ROOSEVELT ROAD.
8. REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY MARKINGS, REMOVE OR RELOCATE TEMPORARY SIGNS, INSTALL NEW TEMPORARY SIGNS AND SHIFT TRAFFIC, REMOVE OR RELOCATE CONSTRUCTION DRUMS AND TEMPORARY CONCRETE BARRIERS.

STAGE 3

1. REMOVE TREES AND RELOCATE TRAFFIC SIGNS WHERE NECESSARY.
2. REMOVE EXISTING PAVEMENT, TEMPORARY PAVEMENT, TEMPORARY TRAIL, AND BIKEPATH ON WESTBOUND HARGER ROAD.
3. REMOVE EXISTING PAVEMENT AND SHOULDER ON HARGER ROAD BETWEEN TIMBER EDGE DRIVE AND YORK ROAD.
4. REMOVE EXISTING PAVEMENT ON TIMBER EDGE DRIVE.
5. REMOVE EXISTING PAVEMENT ON THE DRIVEWAY ENTRANCE TO THE FOREST PRESERVE.
6. CONSTRUCT AGGREGATE SUBGRADE, BINDER COURSE, CURB AND GUTTER, AND BIKEPATH ON WESTBOUND HARGER ROAD.
7. CONSTRUCT AGGREGATE SUBGRADE, BINDER COURSE, AND CURB AND GUTTER ON TIMBER EDGE DRIVE.
8. CONSTRUCT THE DRIVEWAY ENTRANCE TO THE FOREST PRESERVE.
9. REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY MARKINGS, REMOVE OR RELOCATE TEMPORARY SIGNS, INSTALL NEW TEMPORARY SIGNS AND SHIFT TRAFFIC, REMOVE OR RELOCATE CONSTRUCTION DRUMS AND TEMPORARY CONCRETE BARRIERS.

STAGE 3A

1. REMOVE REMAINING EXISTING AND TEMPORARY PAVEMENT ON SOUTHBOUND TIMBER EDGE DRIVE.
2. CONSTRUCT AGGREGATE SUNGRADE, AND BINDER COURSE ON SOUTHBOUND TIMBER EDGE DRIVE.
3. REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY MARKINGS, REMOVE OR RELOCATE TEMPORARY SIGNS, INSTALL NEW TEMPORARY SIGNS AND SHIFT TRAFFIC, REMOVE OR RELOCATE CONSTRUCTION DRUMS AND TEMPORARY CONCRETE BARRIERS.

STAGE 3B

1. REMOVE REMAINING EXISTING AND TEMPORARY PAVEMENT ON NORTHBOUND TIMBER EDGE DRIVE.
2. CONSTRUCT AGGREGATE SUNGRADE, AND BINDER COURSE ON NORTHBOUND TIMBER EDGE DRIVE.
3. REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY MARKINGS, REMOVE OR RELOCATE TEMPORARY SIGNS, INSTALL NEW TEMPORARY SIGNS AND SHIFT TRAFFIC, REMOVE OR RELOCATE CONSTRUCTION DRUMS AND TEMPORARY CONCRETE BARRIERS.

STAGE 4

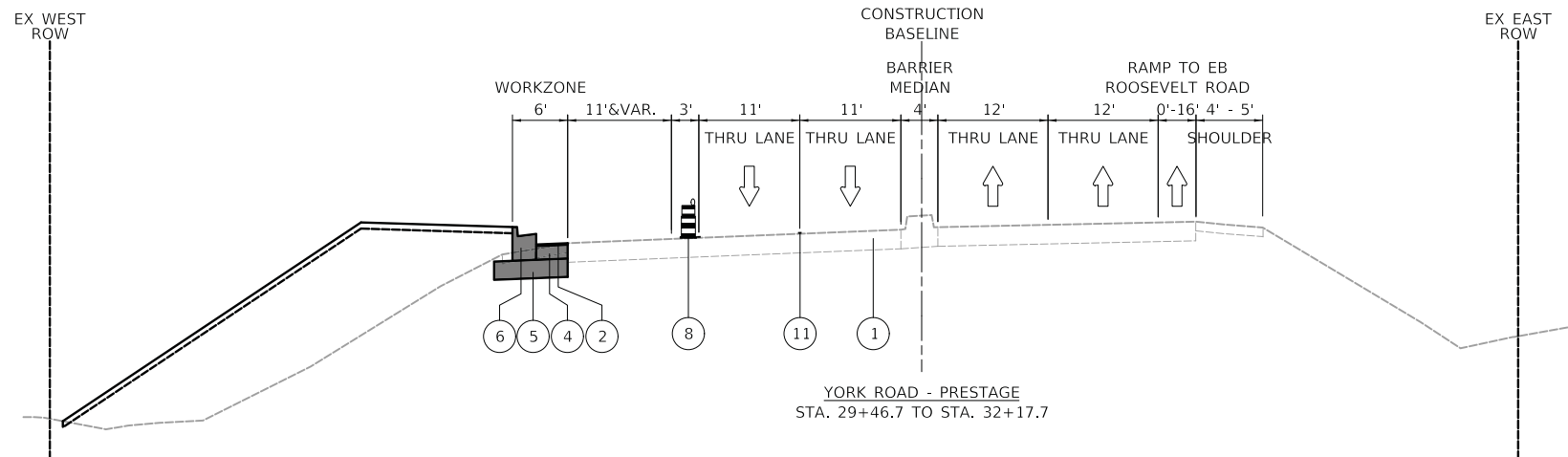
1. REMOVE EXISTING PAVEMENT ON THE DRIVEWAY AT THE INTERSECTION OF YORK ROAD AND HARGER ROAD.
2. REMOVE EXISTING SHOULDER FROM NORTHBOUND YORK AND THE EXIT RAMP.
3. CONSTRUCT THE CONCRETE MEDIAN AND CORNER ISLAND ON HARGER ROAD.
4. CONSTRUCT THE DRIVEWAY ENTRANCE AT THE INTERSECTION OF YORK ROAD AND HARGER ROAD.
5. CONSTRUCT SHOULDER, AND CURB AND GUTTER ON NORTHBOUND YORK ROAD.
6. CONSTRUCT SHOULDER ON THE EXIT RAMP.
7. REMOVE CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY MARKINGS, REMOVE OR RELOCATE TEMPORARY SIGNS, INSTALL NEW TEMPORARY SIGNS AND SHIFT TRAFFIC, REMOVE OR RELOCATE CONSTRUCTION DRUMS AND TEMPORARY CONCRETE BARRIERS.

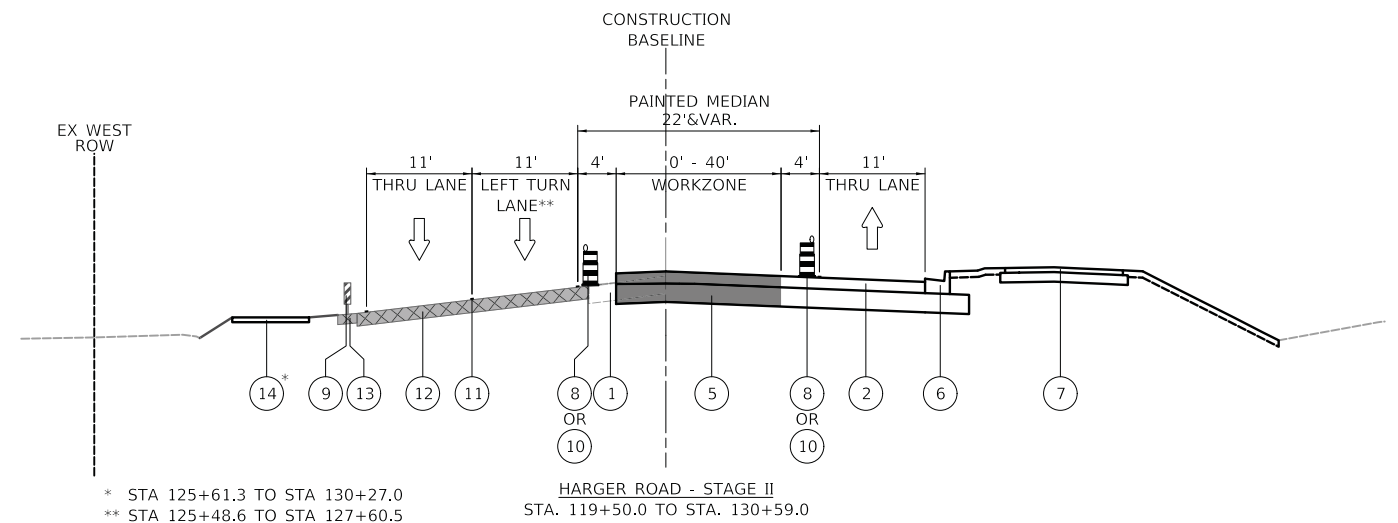
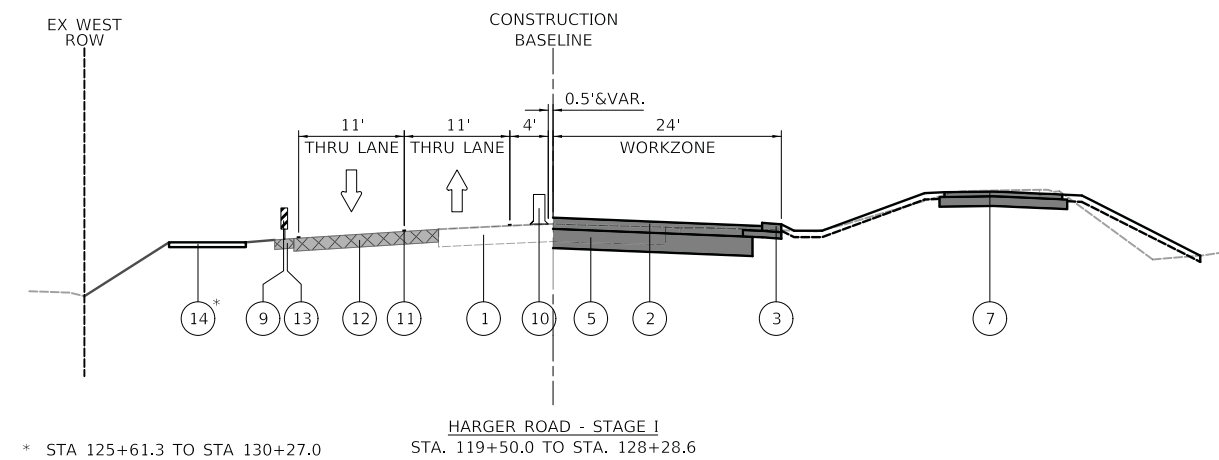
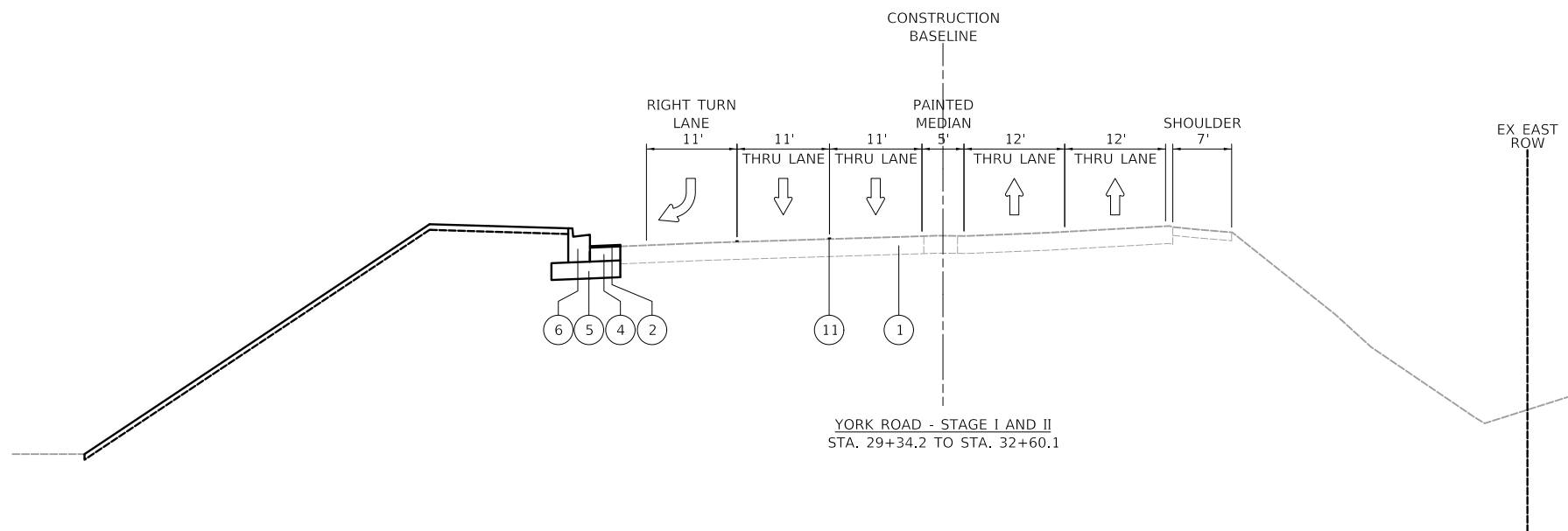
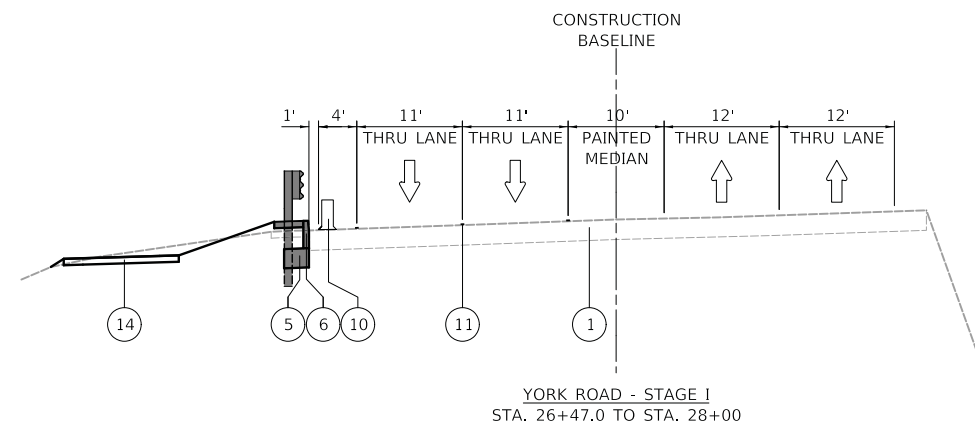
STAGE 5

1. REMOVE EXISTING PAVEMENT ON THE MEDIAN OF YORK ROAD.
2. CONSTRUCT THE CONCRETE MEDIAN ON YORK ROAD.
3. MILL YORK ROAD AND RAMPS.
4. PLACE SURFACE COURSE TO FINISHED GRADE WITHIN THE LIMITS OF THE PROJECT AND PLACE SHORT TERM PAVEMENT MARKINGS AS REQUIRED.
5. COMPLETE AND ACTIVATE ALL PERMANENT TRAFFIC SIGNAL INSTALLATIONS.
6. REMOVE ALL CONFLICTING PAVEMENT MARKINGS, PLACE PERMANENT MARKINGS AND SIGNING, REMOVE ALL BARRICADES AND TEMPORARY TRAFFIC SIGNS.
7. COMPLETE ALL REMAINING LANDSCAPING AND CLEAN-UP WORK.

LEGEND

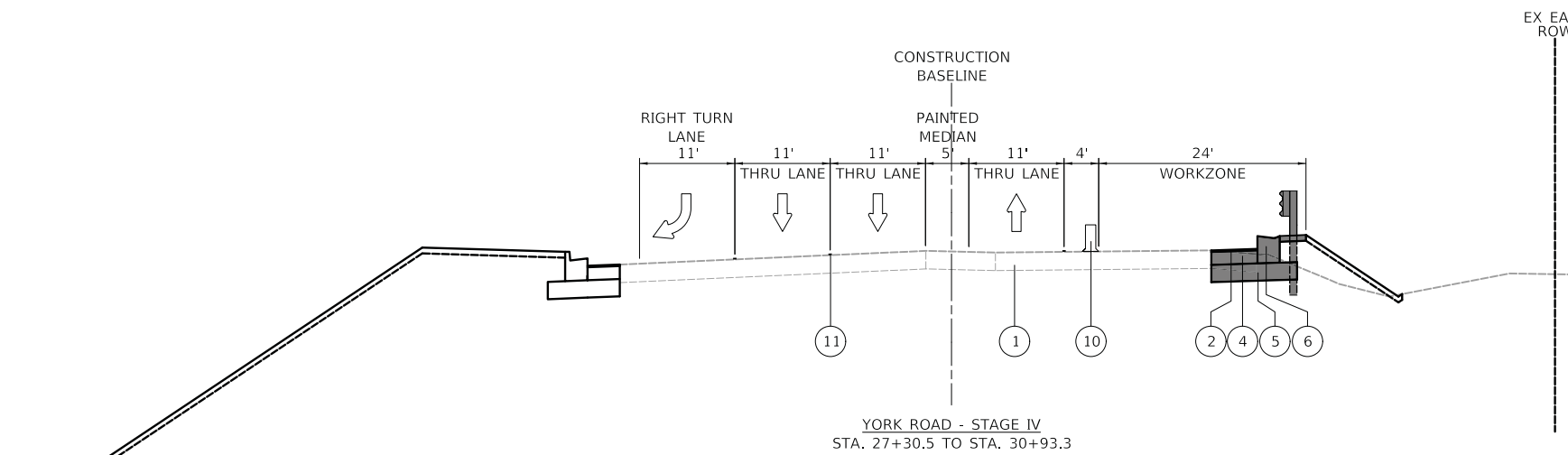
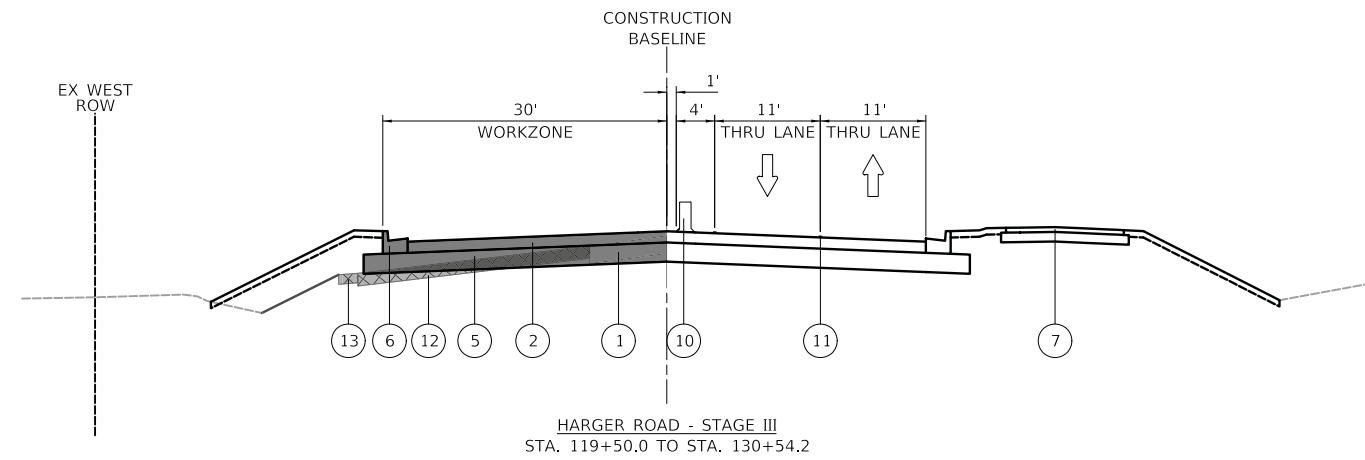
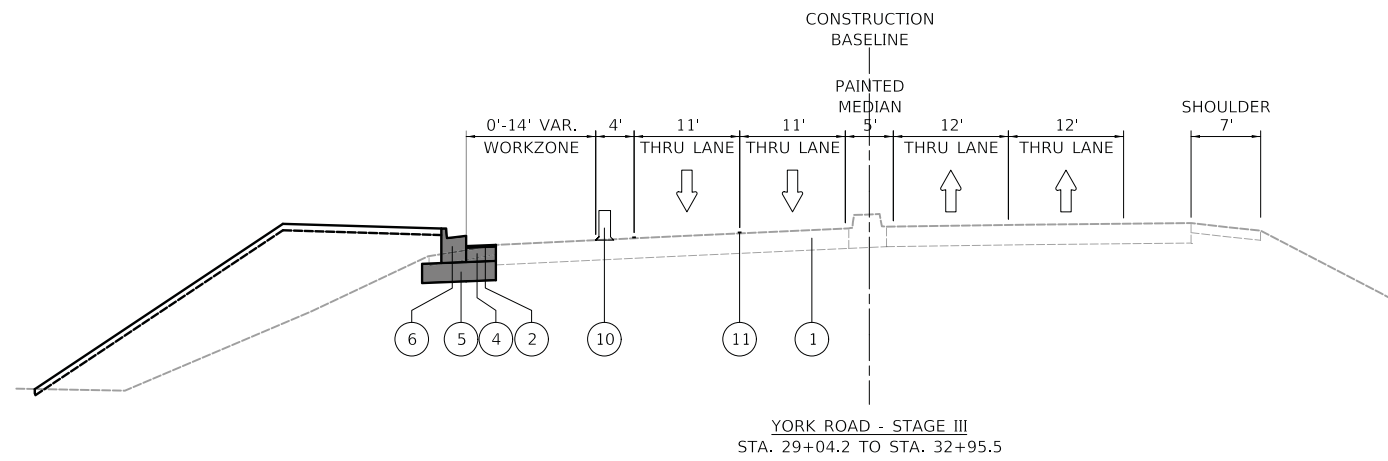
- ① EXISTING BITUMINOUS PAVEMENT
 - ② PROPOSED HOT-MIX ASPHALT BINDER COURSE
 - ③ PROPOSED AGGREGATE SHOULDERS
 - ④ PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE
 - ⑤ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT
 - ⑥ PROPOSED COMBINATION CONCRETE CURB AND GUTTER
PROPOSED CONCRETE CURB
 - ⑦ PROPOSED TRAIL
 - ⑧ DRUM OR TYPE II BARRICADE
 - ⑨ DOUBLE VERTICAL PANEL @ 50' C-C
 - ⑩ TEMPORARY CONCRETE BARRIER
 - ⑪ TEMPORARY PAVEMENT MARKING
 - ⑫ TEMPORARY PAVEMENT, 8" - PAID FOR AS "TEMPORARY PAVEMENT"
 - ⑬ TEMPORARY AGGREGATE SHOULDER, 6" - PAID FOR AS "AGGREGATE SHOULDERS, TYPE B"
 - ⑭ TEMPORARY TRAIL
- PERMANENT CONSTRUCTION DURING STAGE
(SEE STAGES OF CONSTRUCTION PLAN SHEETS)





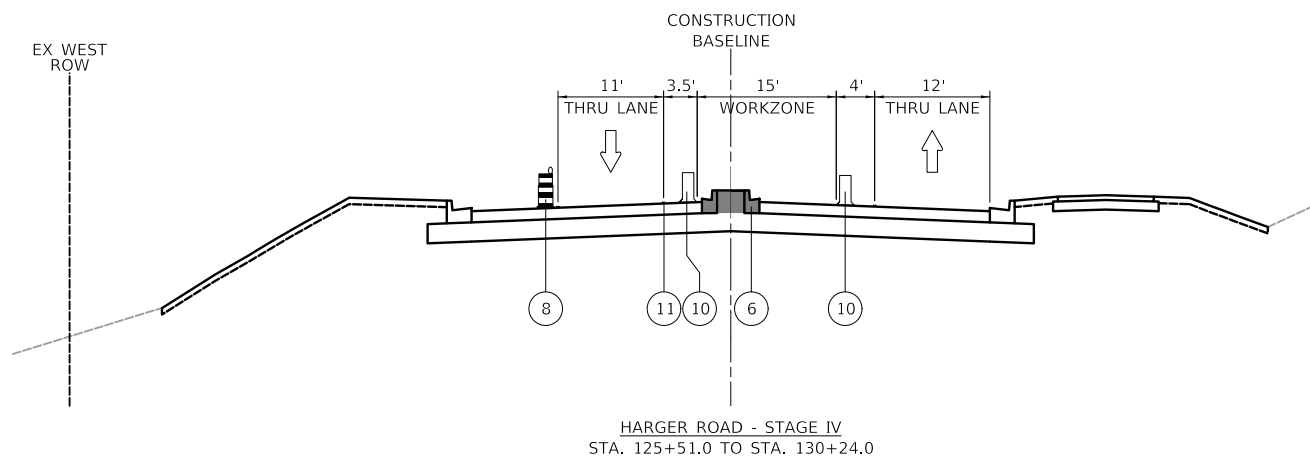
LEGEND

- ① EXISTING BITUMINOUS PAVEMENT
- ② PROPOSED HOT-MIX ASPHALT BINDER COURSE
- ③ PROPOSED AGGREGATE SHOULDERS
- ④ PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE
- ⑤ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT
- ⑥ PROPOSED COMBINATION CONCRETE CURB AND GUTTER
PROPOSED CONCRETE CURB
- ⑦ PROPOSED TRAIL
- ⑧ DRUM OR TYPE II BARRICADE
- ⑨ DOUBLE VERTICAL PANEL @ 50' C-C
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- ⑭ TEMPORARY TRAIL
- PERMANENT CONSTRUCTION DURING STAGE
(SEE STAGES OF CONSTRUCTION PLAN SHEETS)
- ▬ PROPOSED GUARDRAIL

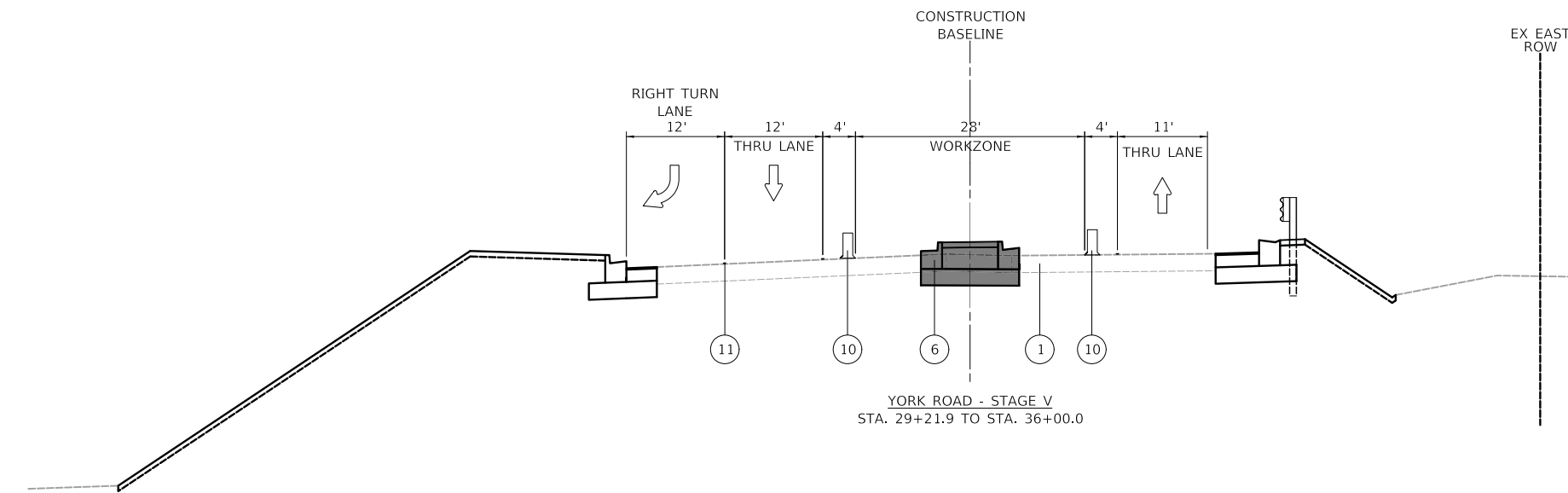


LEGEND

- ① EXISTING BITUMINOUS PAVEMENT
- ② PROPOSED HOT-MIX ASPHALT BINDER COURSE
- ③ PROPOSED AGGREGATE SHOULDERS
- ④ PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE
- ⑤ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT
- ⑥ PROPOSED COMBINATION CONCRETE CURB AND GUTTER
PROPOSED CONCRETE CURB
- ⑦ PROPOSED TRAIL
- ⑧ DRUM OR TYPE II BARRICADE
- ⑨ DOUBLE VERTICAL PANEL @ 50' C-C
- ⑩ TEMPORARY CONCRETE BARRIER
- ⑪ TEMPORARY PAVEMENT MARKING
- ⑫ TEMPORARY PAVEMENT, 8" - PAID FOR AS "TEMPORARY PAVEMENT"
- ⑬ TEMPORARY AGGREGATE SHOULDER, 6" - PAID FOR AS "AGGREGATE SHOULDERS, TYPE B"
- ⑭ TEMPORARY TRAIL
- PERMANENT CONSTRUCTION DURING STAGE
(SEE STAGES OF CONSTRUCTION PLAN SHEETS)
- ▬ PROPOSED GUARDRAIL



HARGER ROAD - STAGE IV
STA. 125+51.0 TO STA. 130+24.0





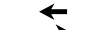

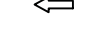

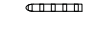
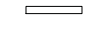



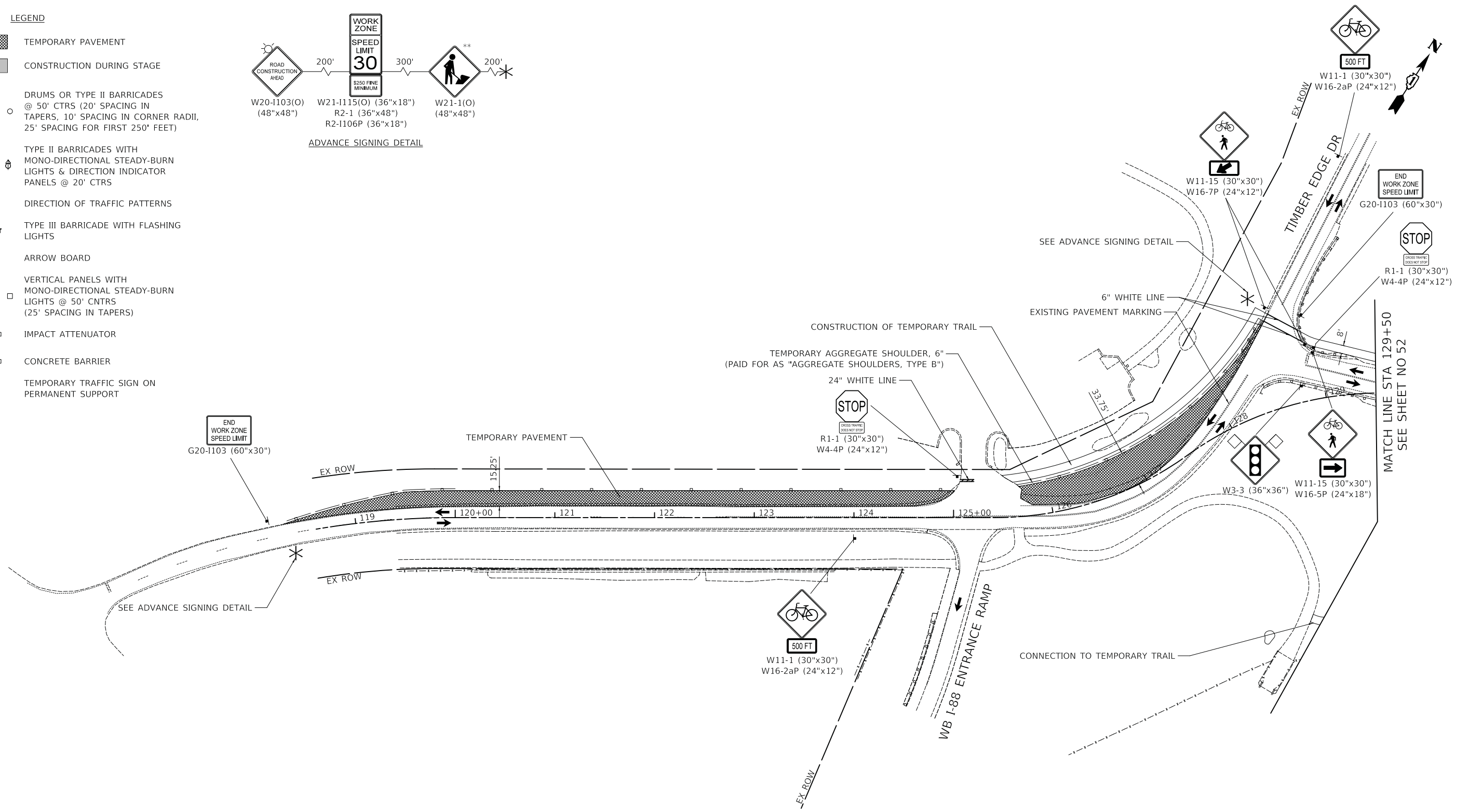
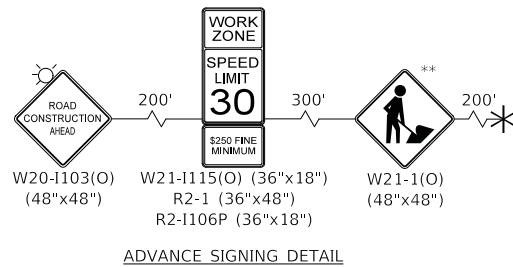
YORK ROAD - STAGE V
STA. 29+21.9 TO STA. 36+00.0

LEGEND

- ① EXISTING BITUMINOUS PAVEMENT
- ② PROPOSED HOT-MIX ASPHALT BINDER COURSE
- ③ PROPOSED AGGREGATE SHOULDERS
- ④ PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE
- ⑤ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT
- ⑥ PROPOSED COMBINATION CONCRETE CURB AND GUTTER
PROPOSED CONCRETE CURB
- ⑦ PROPOSED TRAIL
- ⑧ DRUM OR TYPE II BARRICADE
- ⑨ DOUBLE VERTICAL PANEL @ 50' C-C
- ⑩ TEMPORARY CONCRETE BARRIER
- ⑪ TEMPORARY PAVEMENT MARKING
- ⑫ TEMPORARY PAVEMENT, 8" - PAID FOR AS "TEMPORARY PAVEMENT"
- ⑬ TEMPORARY AGGREGATE SHOULDER, 6" - PAID FOR AS "AGGREGATE SHOULDERS, TYPE B"
- ⑭ TEMPORARY TRAIL
- PERMANENT CONSTRUCTION DURING STAGE
(SEE STAGES OF CONSTRUCTION PLAN SHEETS)
- ▬ PROPOSED GUARDRAIL

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



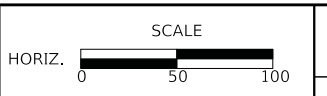
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HARGER ROAD

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| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**














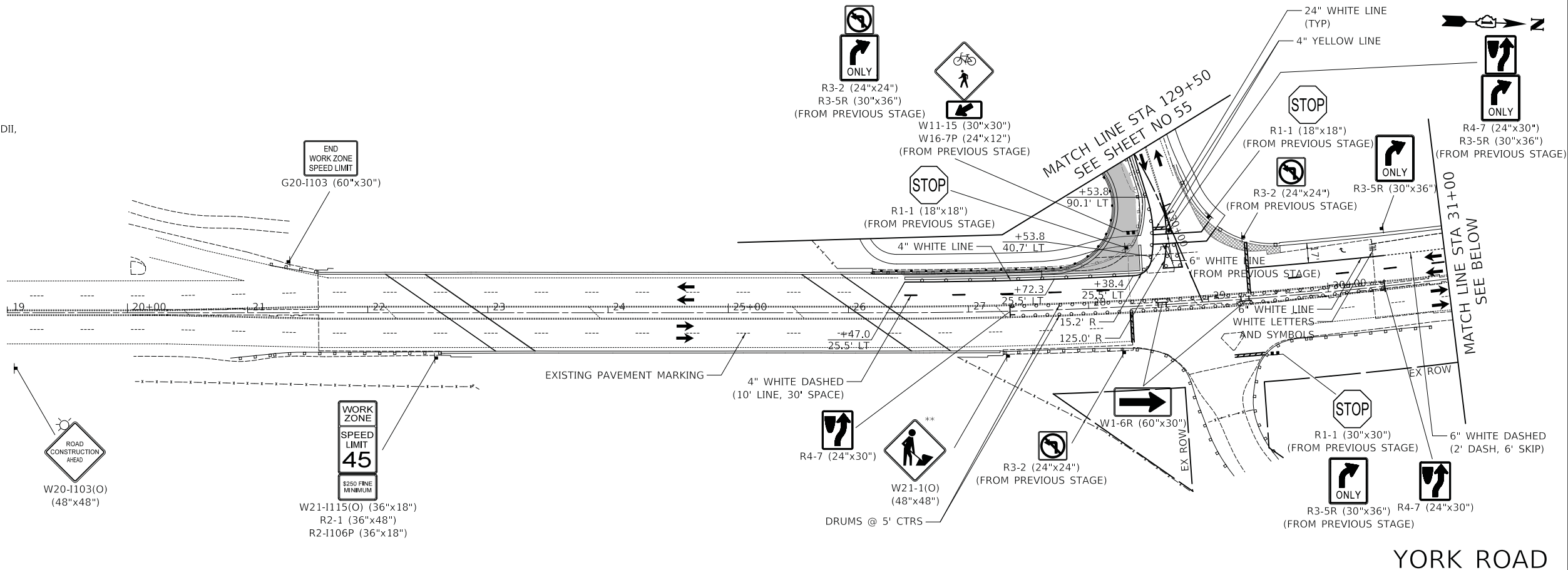
MAINTENANCE OF TRAFFIC PLAN - PRESTAGE

SHEET 2 OF 2 SHEETS STA. TO STA.

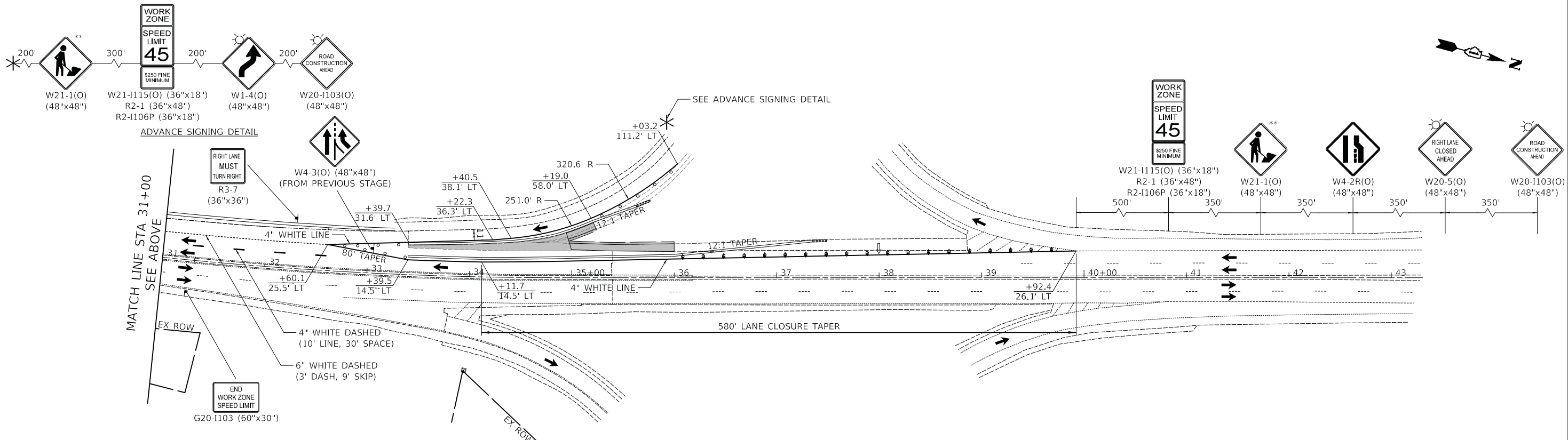
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| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 53 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 61G16 | |
| FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



YORK ROAD



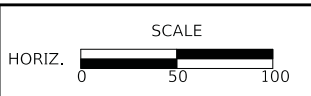
YORK ROAD

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| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
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












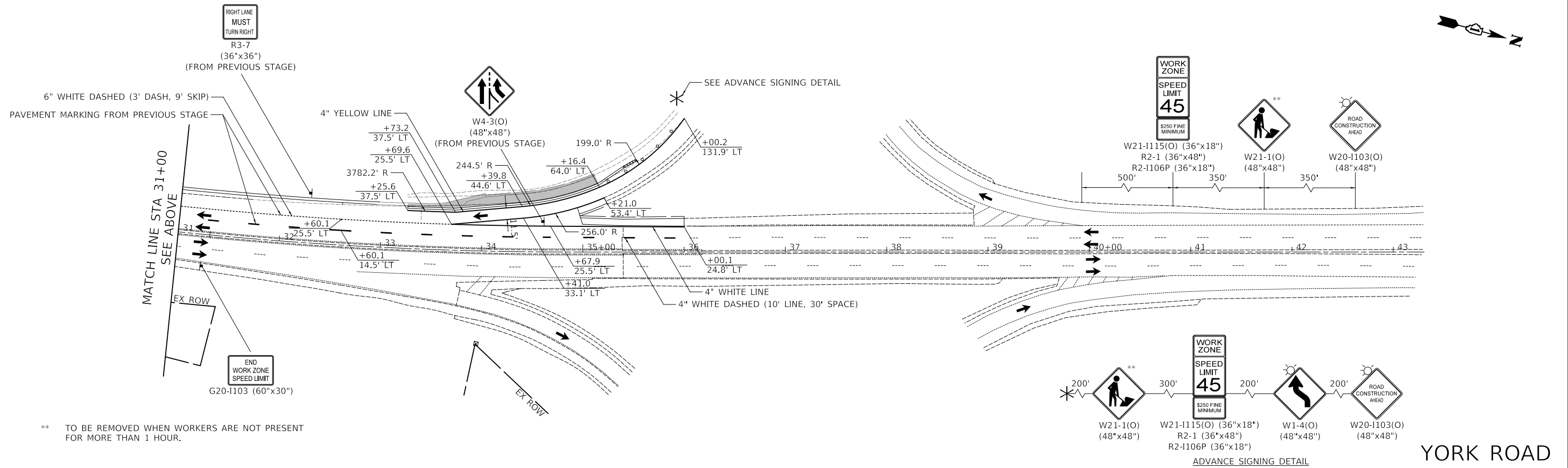
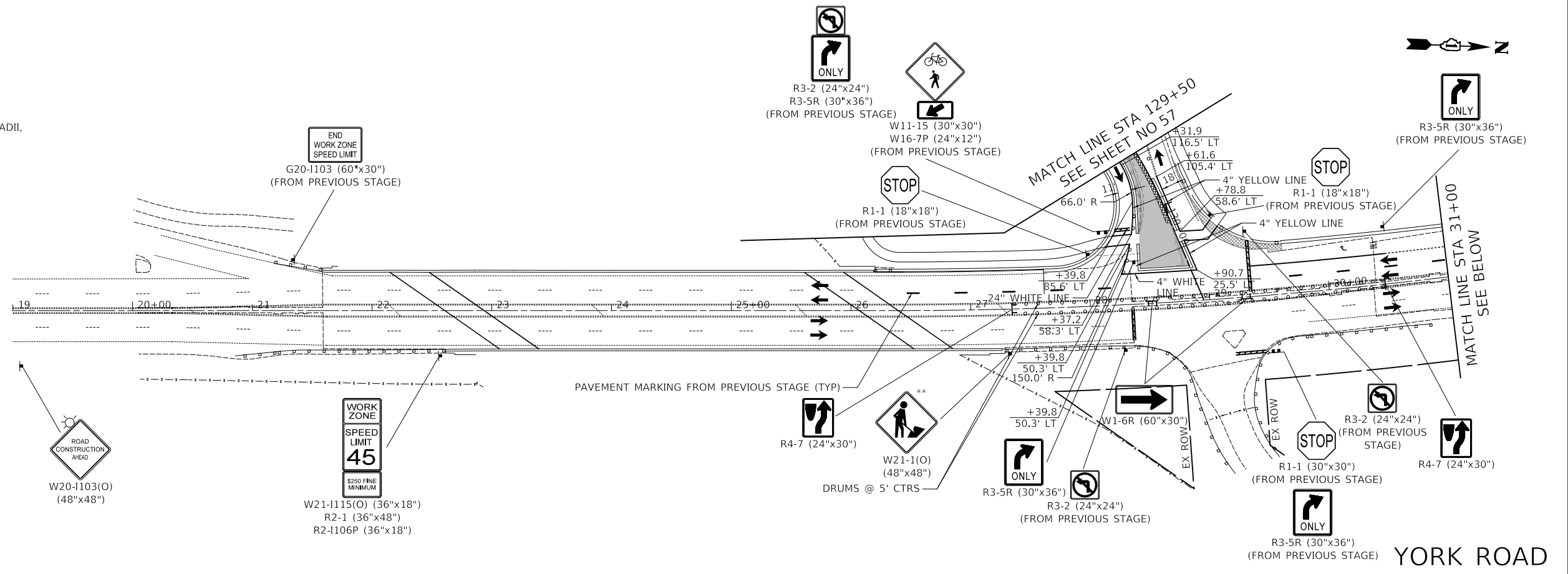
MAINTENANCE OF TRAFFIC PLAN - STAGE 1

SHEET 1 OF 2 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 54 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT

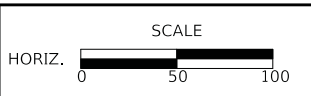


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| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
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






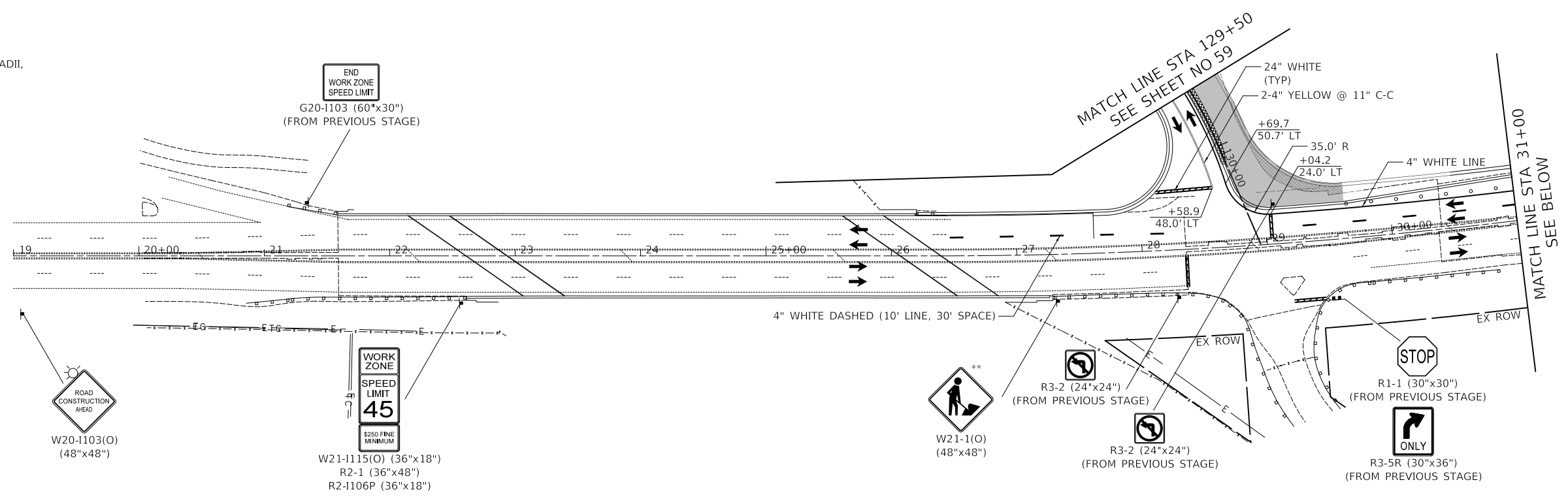
MAINTENANCE OF TRAFFIC PLAN - STAGE 2

SHEET 1 OF 2 SHEETS STA. TO STA.

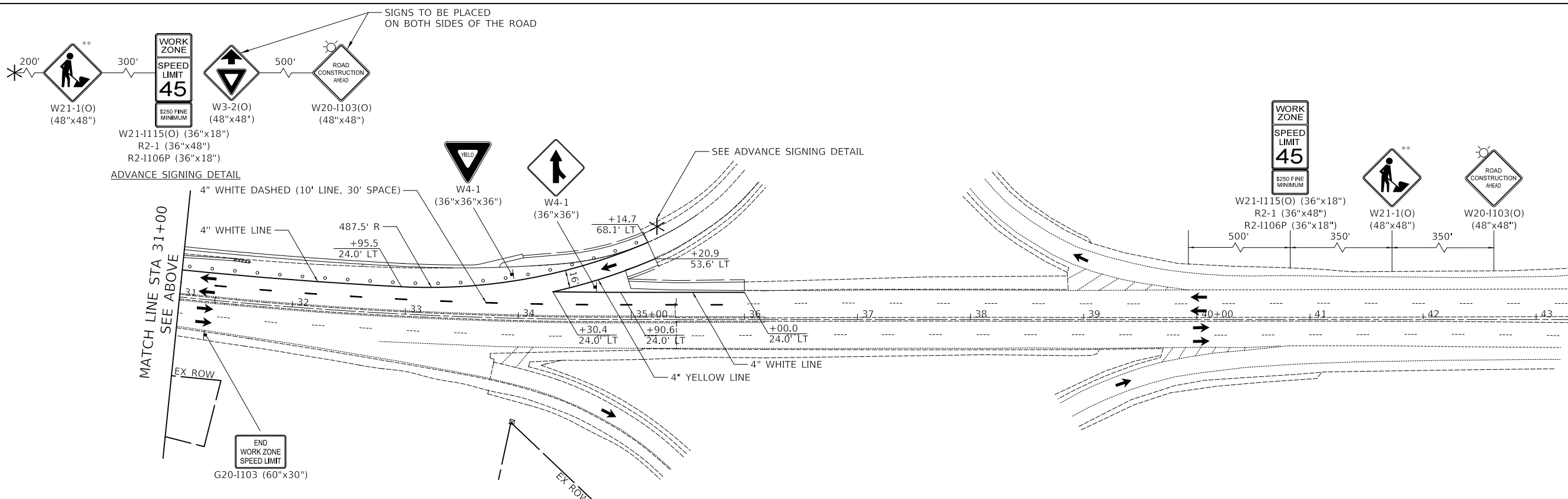
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| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 56 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



YORK ROAD



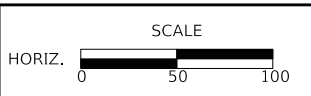
YORK ROAD

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| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

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






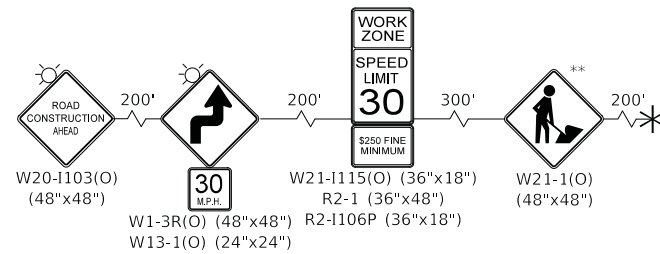
MAINTENANCE OF TRAFFIC PLAN - STAGE 3

SHEET 1 OF 2 SHEETS STA. TO STA.

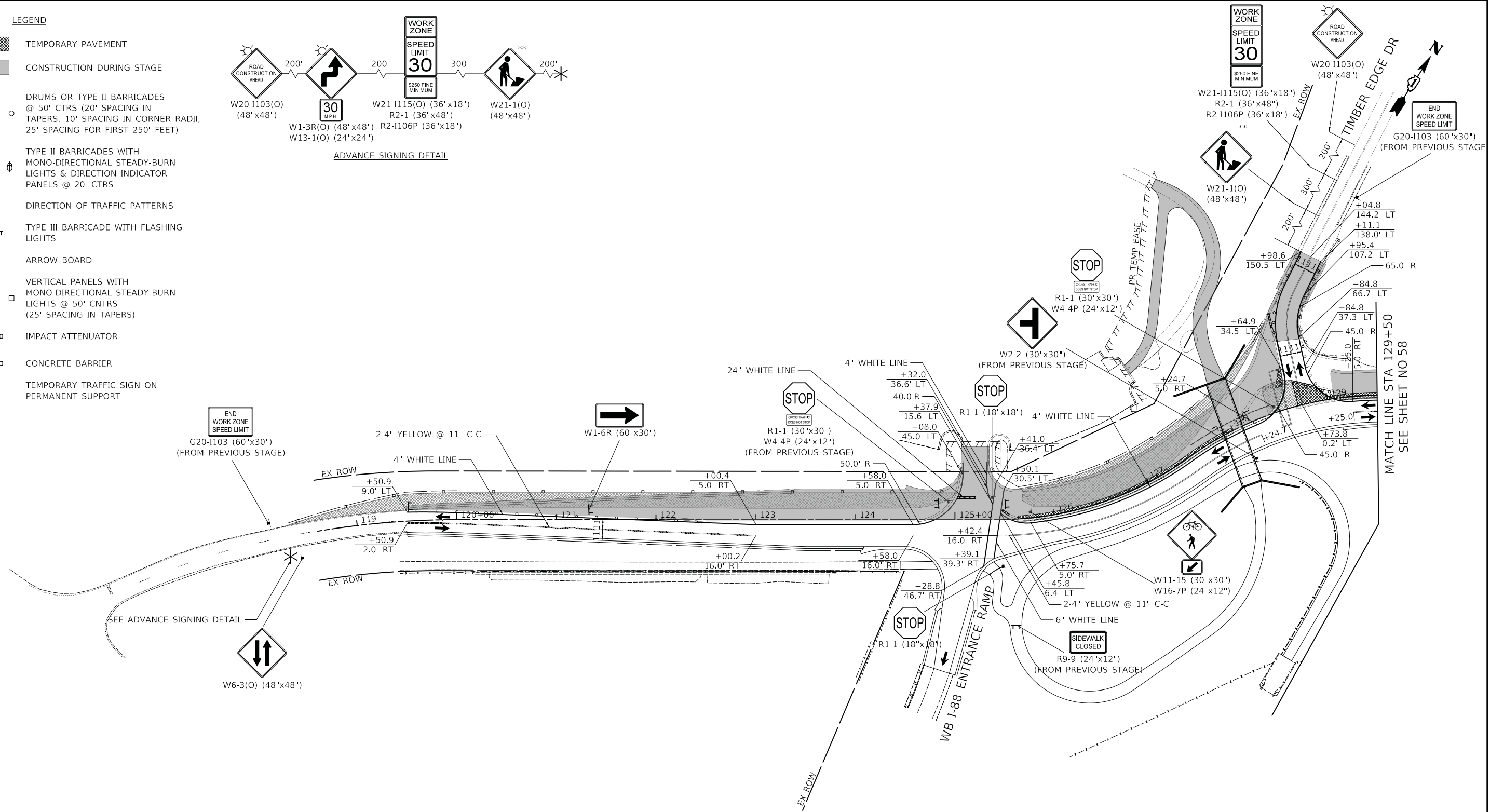
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| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 58 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



ADVANCE SIGNING DETAIL



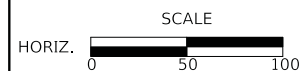
** TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

HARGER ROAD

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| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
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






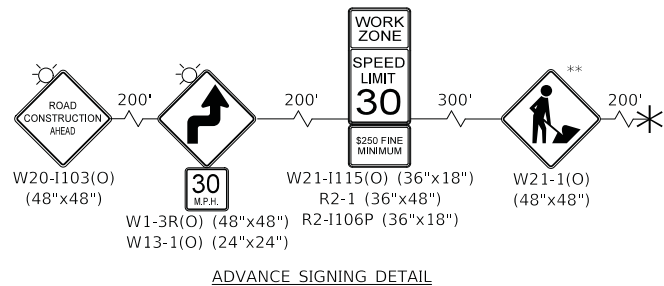
MAINTENANCE OF TRAFFIC PLAN - STAGE 3

SHEET 2 OF 2 SHEETS STA. TO STA.

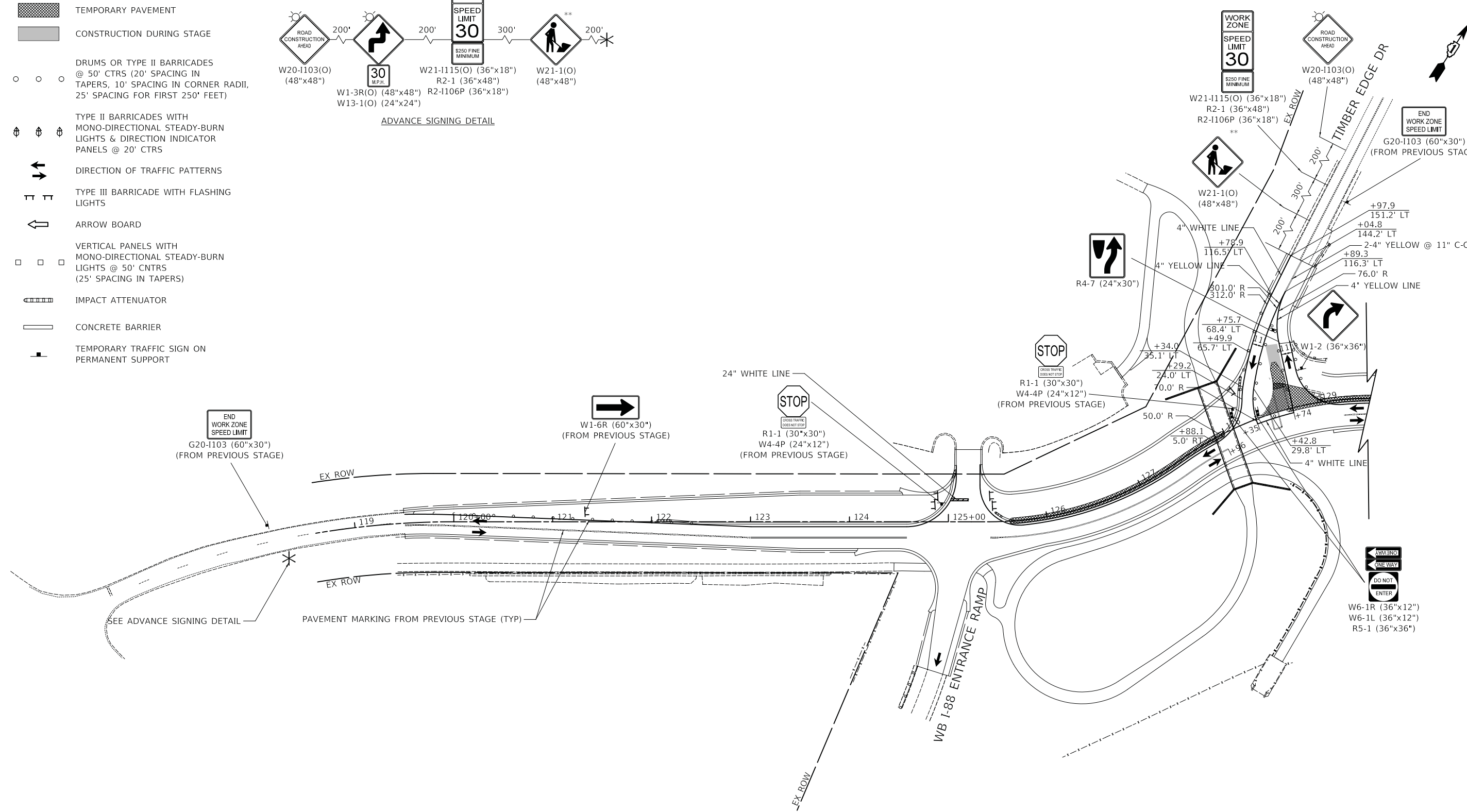
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| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 59 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 61G16 | |
| FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CNTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



ADVANCE SIGNING DETAIL



** TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

HARGER ROAD

CIVILTECH
 Two Pierce Place, Suite 1400
 Itasca, Illinois 60143
 Tel: 630.773.3900 Fax: 630.773.3975
 www.civiltechinc.com

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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**














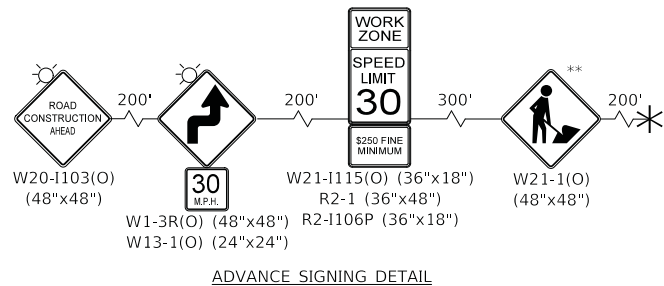
MAINTENANCE OF TRAFFIC PLAN - STAGE 3A

SHEET 1 OF 1 SHEETS STA. TO STA.

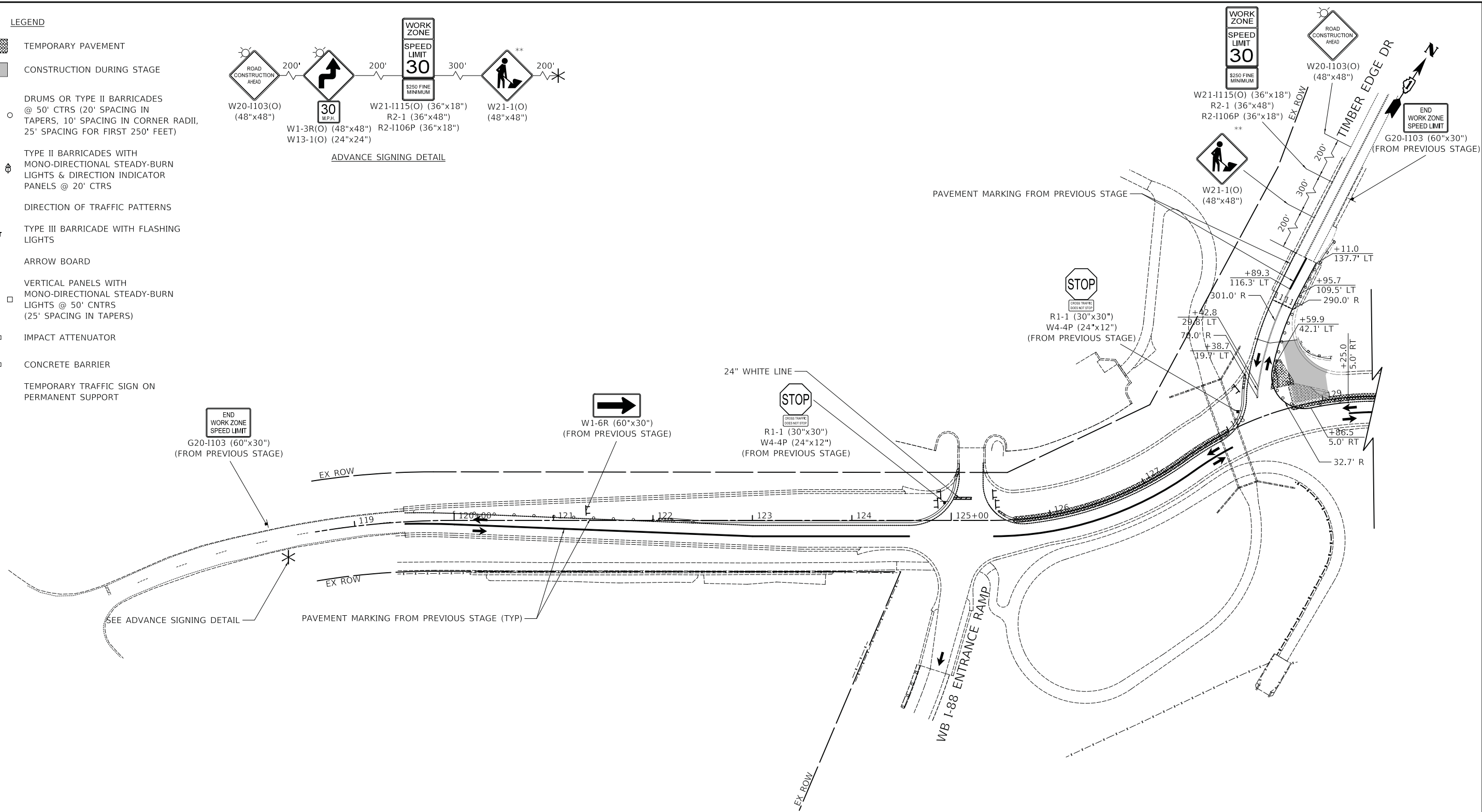
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| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 60 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CNTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



ADVANCE SIGNING DETAIL



** TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

HARGER ROAD

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| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**














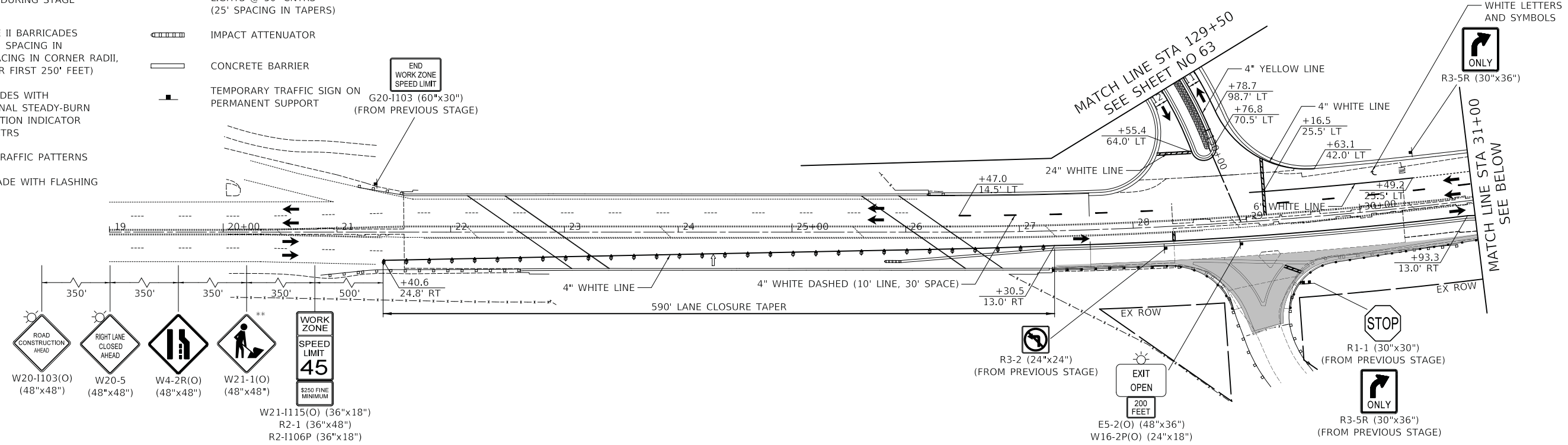
MAINTENANCE OF TRAFFIC PLAN - STAGE 3B

SHEET 1 OF 1 SHEETS STA. TO STA.

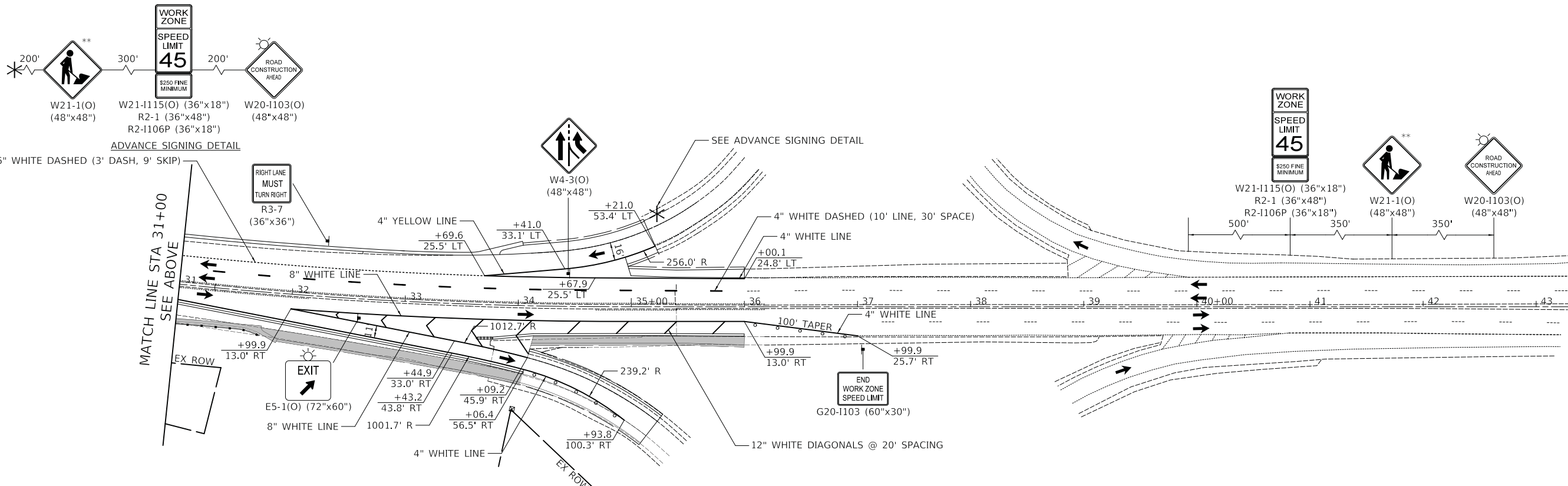
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 61 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



YORK ROAD



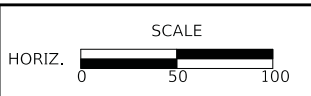
** TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

YORK ROAD

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| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**














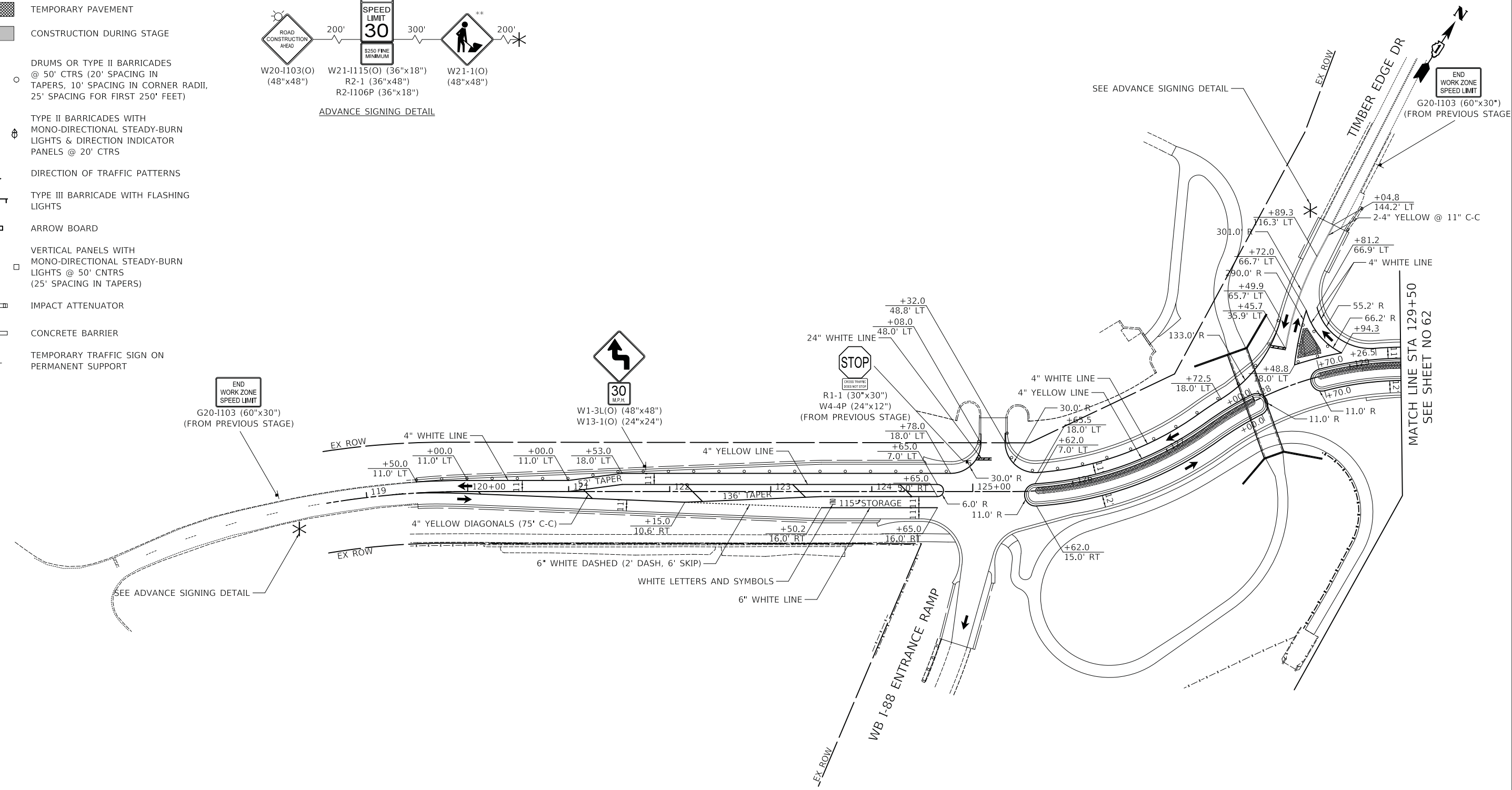
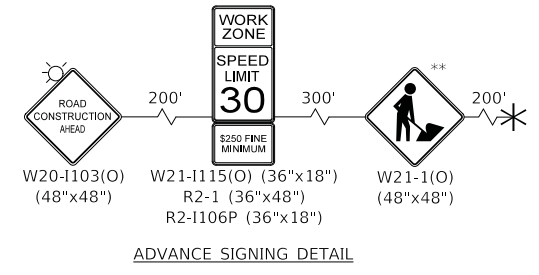
MAINTENANCE OF TRAFFIC PLAN - STAGE 4

SHEET 1 OF 2 SHEETS STA. TO STA.

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|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 62 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



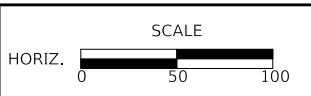
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HARGER ROAD

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| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



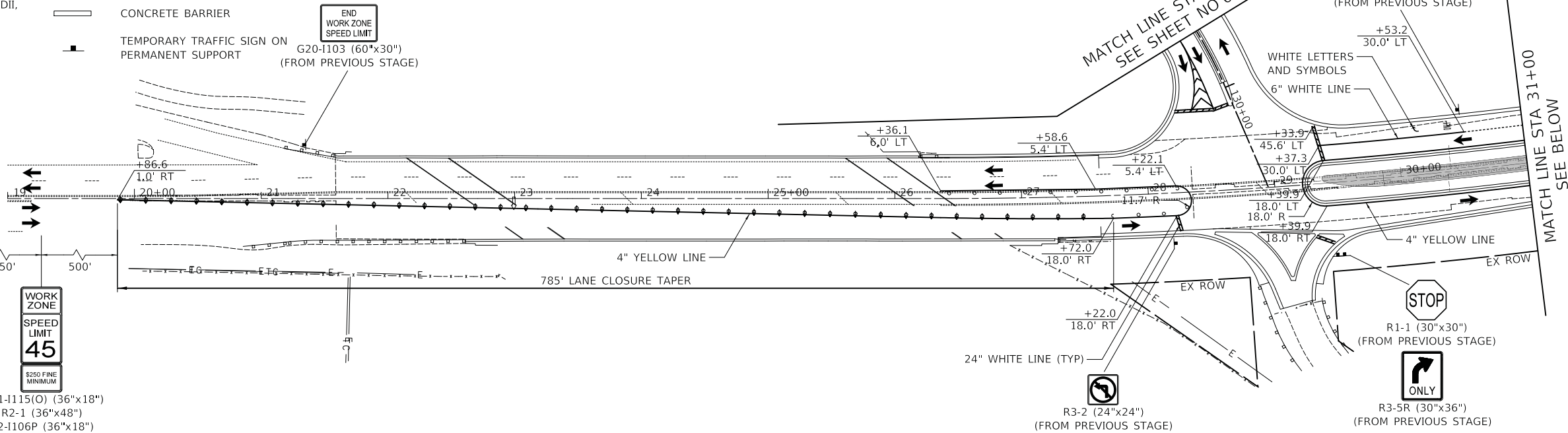
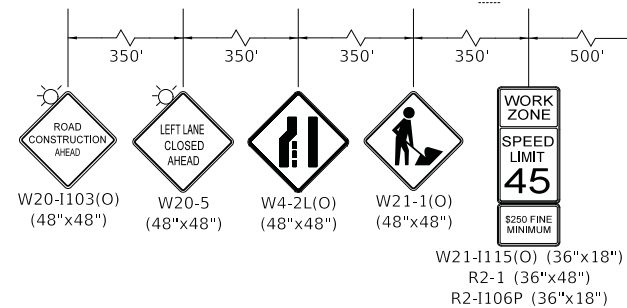
MAINTENANCE OF TRAFFIC PLAN - STAGE 4

SHEET 2 OF 2 SHEETS STA. TO STA.

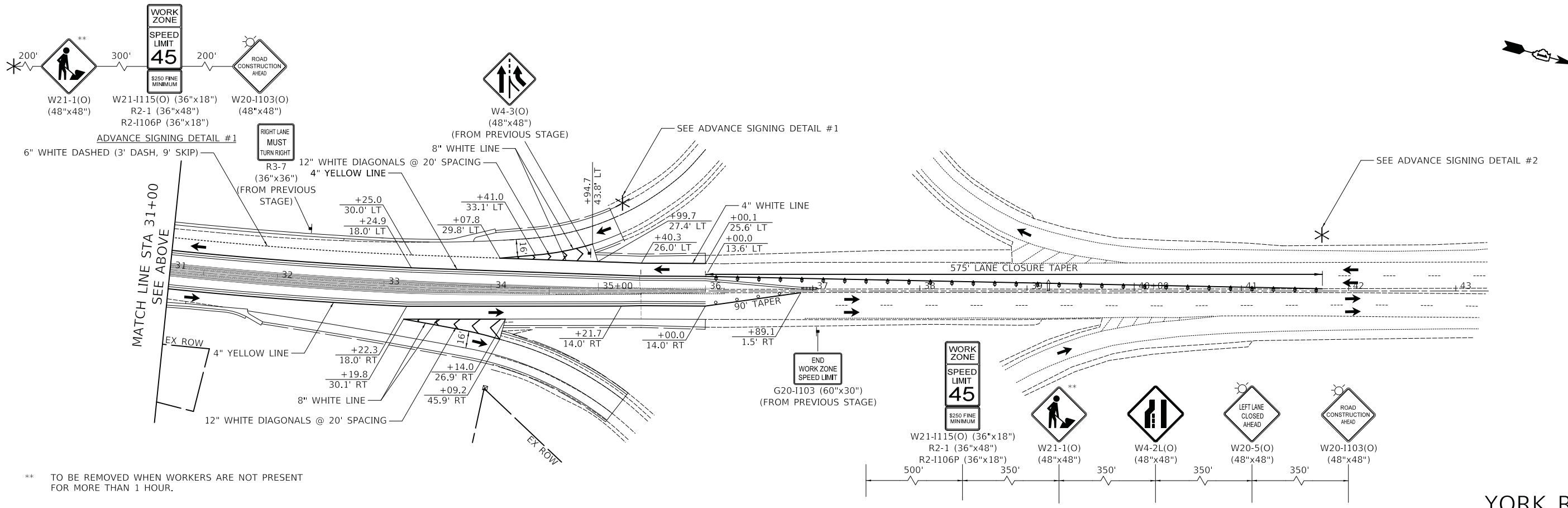
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| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 63 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 61G16 | |
| FED. AID PROJECT | | | | |

LEGEND

- TEMPORARY PAVEMENT
- CONSTRUCTION DURING STAGE
- DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
- TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
- DIRECTION OF TRAFFIC PATTERNS
- TYPE III BARRICADE WITH FLASHING LIGHTS
- ARROW BOARD
- VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CNTRS (25' SPACING IN TAPERS)
- IMPACT ATTENUATOR
- CONCRETE BARRIER
- TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT



YORK ROAD



YORK ROAD

** TO BE REMOVED WHEN WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION














MAINTENANCE OF TRAFFIC PLAN - STAGE 5

SHEET 1 OF 2 SHEETS STA. TO STA.

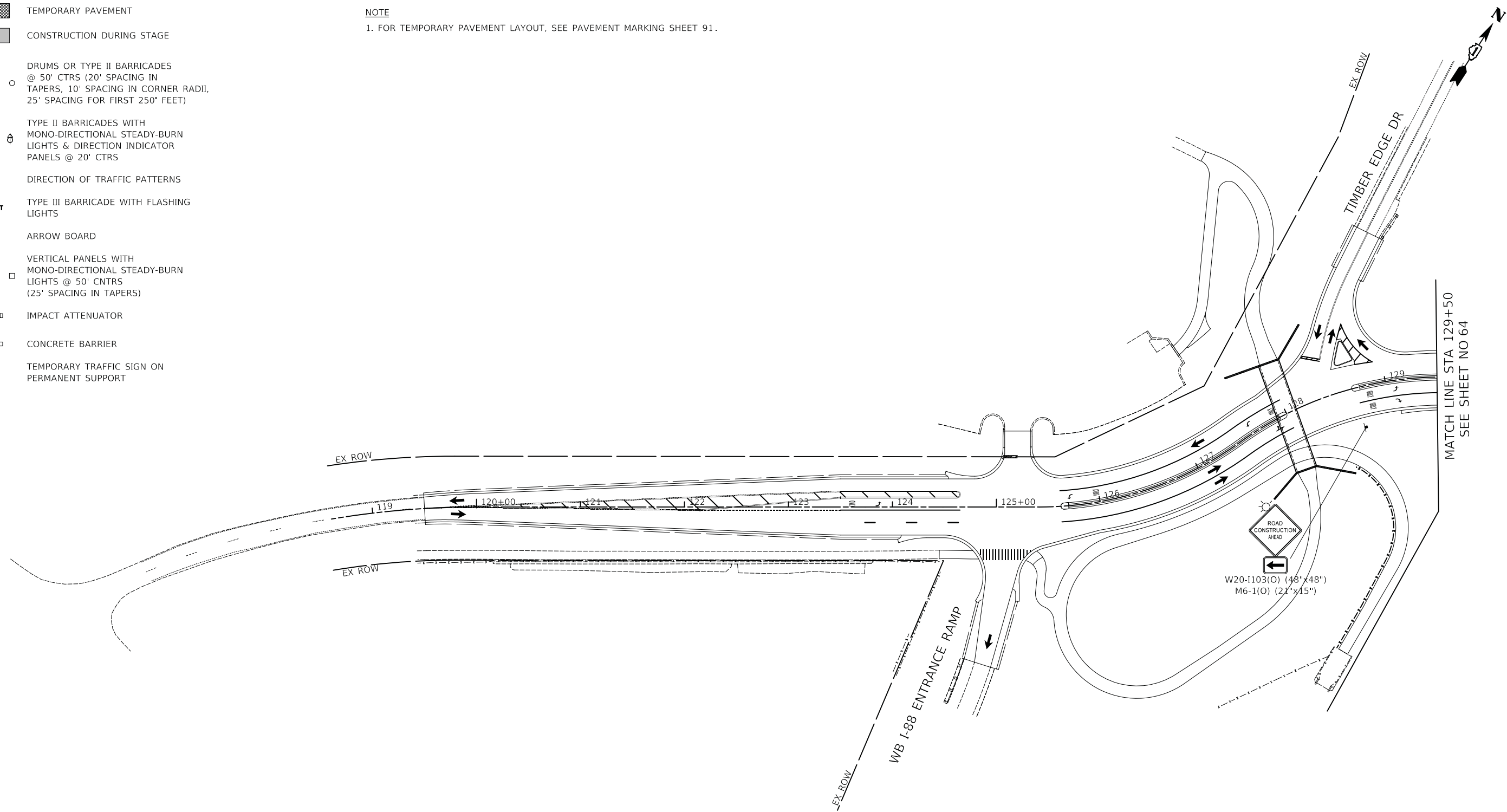
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| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 64 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

LEGEND

-  TEMPORARY PAVEMENT
-  CONSTRUCTION DURING STAGE
-  DRUMS OR TYPE II BARRICADES @ 50' CTRS (20' SPACING IN TAPERS, 10' SPACING IN CORNER RADII, 25' SPACING FOR FIRST 250' FEET)
-  TYPE II BARRICADES WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS & DIRECTION INDICATOR PANELS @ 20' CTRS
-  DIRECTION OF TRAFFIC PATTERNS
-  TYPE III BARRICADE WITH FLASHING LIGHTS
-  ARROW BOARD
-  VERTICAL PANELS WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CNTRS (25' SPACING IN TAPERS)
-  IMPACT ATTENUATOR
-  CONCRETE BARRIER
-  TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT

NOTE

1. FOR TEMPORARY PAVEMENT LAYOUT, SEE PAVEMENT MARKING SHEET 91.



MATCH LINE STA 129+50
SEE SHEET NO 64

HARGER ROAD

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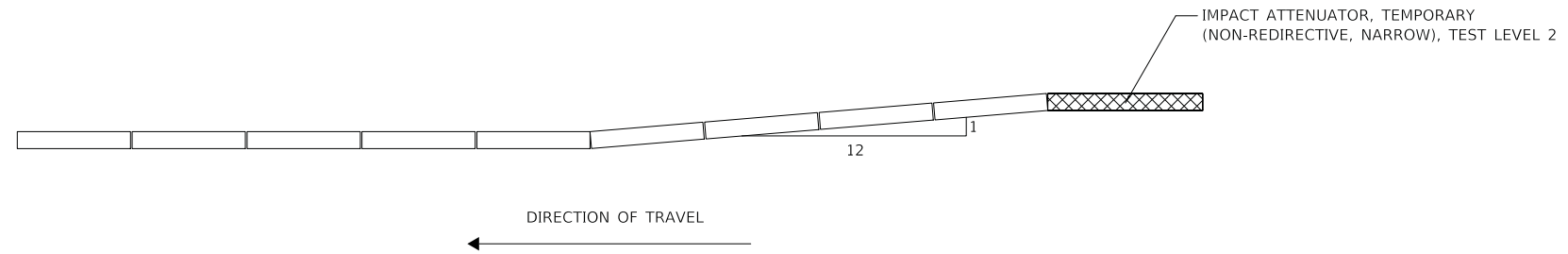
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HORIZ. SCALE
0 50 100

MAINTENANCE OF TRAFFIC PLAN - STAGE 5

SHEET 2 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 65 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |



TEMPORARY CONCRETE BARRIER END TREATMENT DETAIL

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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 66 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |

EROSION CONTROL NOTES

1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH ARTICLE VII OF THE DuPAGE COUNTY COUNTYWIDE STORMWATER AND FLOOD PLAIN ORDINANCE, EFFECTIVE APRIL 2013 AND ALL SUBSEQUENT REVISIONS. ALL SEDIMENT AND EROSION CONTROL MEASURES WILL BE INSTALLED PER IDOT STANDARD 280001 OR AS SPECIFIED HEREIN AND PAID FOR IN ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS. ALL CONSTRUCTION ACTIVITIES WILL BE IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM STORM WATER PERMITS ILR10 AND ILR40.
2. EROSION CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE SEQUENCE OF STAGE CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE FOR APPROVAL.
3. SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE THE PROJECT SITE IS OTHERWISE DISTURBED.
4. ALL DISTURBED AREAS SHALL BE SEEDED OR SODDED AS SOON AS PRACTICAL AFTER CONSTRUCTION ACTIVITIES IN THAT AREA HAVE CONCLUDED. ALL ERODIBLE/BARE AREAS SHALL BE SEEDED EVERY 7 DAYS WITH TEMPORARY EROSION CONTROL SEEDING. IF A TOPSOIL STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES WILL BE PROVIDED.
5. WHERE WETLANDS ARE TO REMAIN, THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT OR BY HIS/HER WORK CREWS. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF OR STOCKPILED IN WETLANDS.
6. STOCKPILES AND MATERIAL STORAGE ARE PROHIBITED IN SPECIAL MANAGEMENT AREAS INCLUDING WETLANDS, WETLAND BUFFERS, AND FLOOD PLAINS. LOCATIONS OF STOCKPILES MUST BE APPROVED BY THE ENGINEER AND HAVE PROPER EROSION CONTROL MEASURES.
7. RECEPTACLES FOR CONSTRUCTION DEBRIS, INCLUDING CONCRETE TRUCK WASHOUT WASTE, SHALL BE PROVIDED AND MAINTAINED BY THE CONTRACTOR. THESE WILL NOT BE ALLOWED IN SPECIAL MANAGEMENT AREAS. RECEPTACLES AND THEIR LOCATIONS MUST BE APPROVED BY THE ENGINEER AND HAVE PROPER EROSION CONTROL MEASURES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE APPLICABLE ITEMS OF WORK.
8. HAY OR STRAW BALES WILL NOT BE ALLOWED AS PERIMETER EROSION BARRIER OR AS A DITCH CHECK.
9. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED.
10. WHEN TEMPORARY DRAINAGE IS ESTABLISHED, EROSION CONTROL MEASURES MAY BE REQUIRED BY THE ENGINEER.
11. GRAVEL ROADS, ACCESS DRIVES, PARKING AREAS OF SUFFICIENT WIDTH AND LENGTH, AND VEHICLE WASH DOWN FACILITIES IF NECESSARY, SHALL BE PROVIDED TO PREVENT SOIL FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SOIL REACHING A PUBLIC OR PRIVATE ROADWAY SHALL BE REMOVED BEFORE THE END OF EACH WORKDAY AND AS NEEDED.
12. CLEANING OF VEHICLES AND EQUIPMENT, INCLUDING CONCRETE MIXERS, SHALL BE PERFORMED IN A MANNER TO REDUCE THE AMOUNT OF POLLUTANTS TRIBUTARY TO STORM SEWERS AND OPEN WATERS TO THE MAXIMUM EXTENT PRACTICAL.
13. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTION RUNOFF. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
14. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE FILTER DEVICE.
15. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SEDIMENT AND EROSION CONTROL MEASURES ARE OPERATIONAL.
16. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED.
17. THE ENGINEER SHALL INSPECT EROSION CONTROL MEASURES PERIODICALLY AND WITHIN 24 HOURS OF ANY STORM EXCEEDING 1/2 INCH PRECIPITATION. DAMAGED AND INEFFECTIVE EROSION CONTROL MEASURES SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR WITHIN 24 HOURS. EROSION CONTROL SYSTEMS REPLACED DUE TO SEDIMENT LOADING WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE APPLICABLE EROSION CONTROL ITEM.
18. THE COST OF REMOVING SEDIMENT OR REPAIRING EROSION CONTROL SYSTEMS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPLICABLE EROSION CONTROL ITEM.
19. QUANTITIES FOR TEMPORARY FENCE, TREE PRUNING AND TREE ROOT PRUNING ARE ESTIMATED ONLY AND WILL BE DETERMINED BY RESIDENT ENGINEER (RE). CONTRACTOR SHALL SCHEDULE A WALK THROUGH WITH THE ROADSIDE DEVELOPMENT UNIT AT LEAST (72) HOURS PRIOR TO PROJECT COMMENCEMENT.

STABILIZING CONSTRUCTION AREAS

1. TEMPORARY STABILIZATION OF THE CONSTRUCTION AREA SHALL TAKE PLACE AT THE END OF EACH WORK DAY.
2. PERMANENT STABILIZATION OF THE CONSTRUCTION AREA SHALL BE COMPLETED AT THE END OF EACH MAJOR STAGE OF WORK.
3. ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 14 DAYS OF FINAL GRADING OR WHEN LEFT IDLE FOR MORE THAN 14 DAYS.
4. THE COMPLETED SLOPES SHALL BE PERMANENTLY SEEDED WHERE PRACTICAL AS THE EXCAVATION PROCEEDS TO ANOTHER STAGE OF CONSTRUCTION. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY LANDSCAPED AT ONE TIME.

NPDES STATEMENT:

THIS PROJECT DISTURBS 9.70 ACRES OF TOTAL LAND AREA. COMPLIANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) STORMWATER PERMIT IS ONLY NECESSARY IF A PROJECT DISTURBS 1.0 OR MORE ACRES OF TOTAL LAND AREA; AN NPDES STORMWATER PERMIT IS REQUIRED FOR THIS PROJECT.



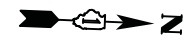
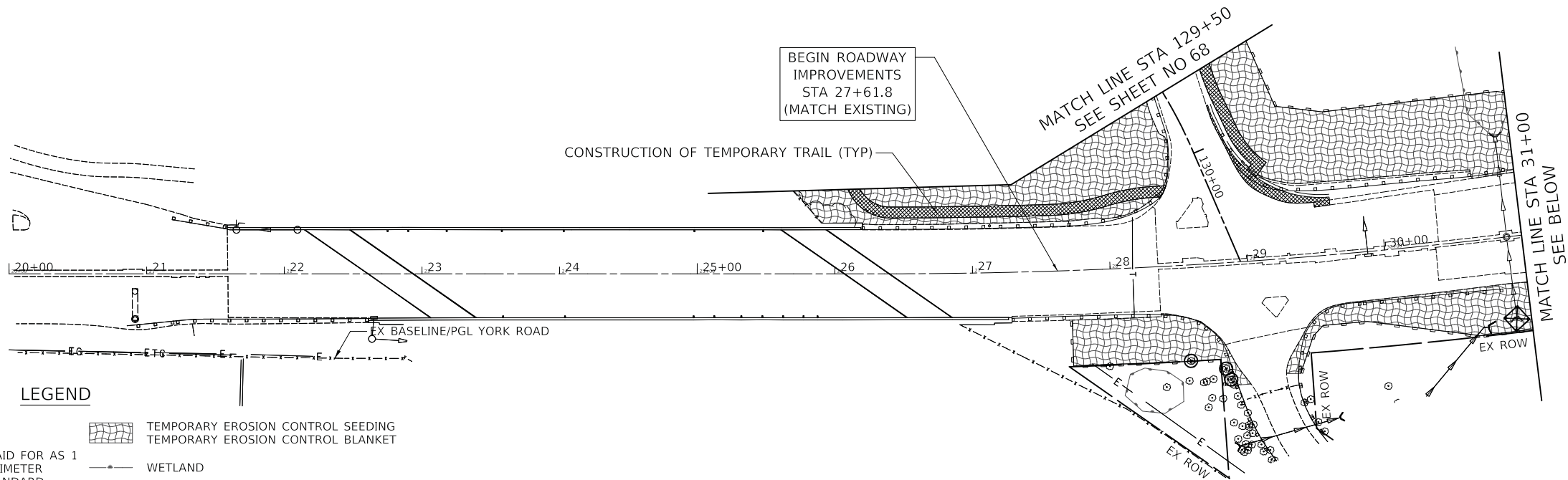
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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL PLAN

SHEET 1 OF 1 SHEETS STA. TO STA.

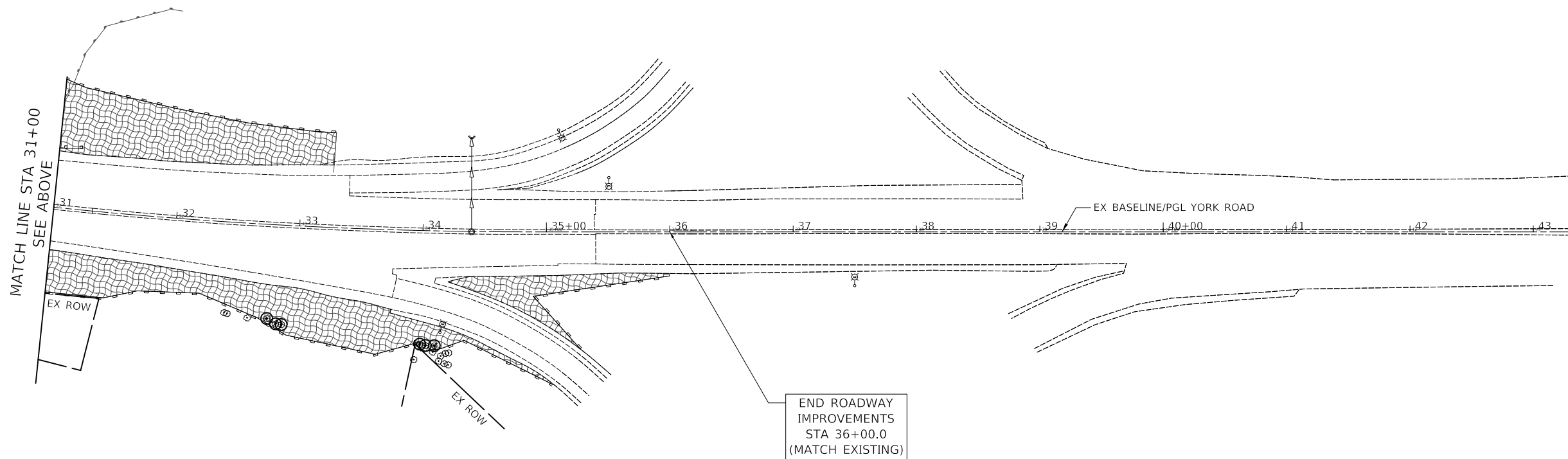
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 67 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



LEGEND

- TEMPORARY DITCH CHECKS
- INLET AND PIPE PROTECTION (PAID FOR AS 1 INLET FILTER AND 16 LF OF PERIMETER EROSION BARRIER PER IDOT STANDARD 280001-07)
- INLET FILTERS
- PERIMETER EROSION BARRIER
- EXISTING TREES
- PROPOSED STORM SEWER
- TREE PROTECTION AND PRESERVATION (AT THE DIRECTION OF ENGINEER)
- TEMPORARY EROSION CONTROL SEEDING
- TEMPORARY EROSION CONTROL BLANKET
- WETLAND
- TEMPORARY PAVING DURING STAGE

YORK ROAD



YORK ROAD

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 Two Pierce Place, Suite 1400
 Itasca, Illinois 60143
 Tel: 630.773.3900 Fax: 630.773.3975
 www.civiltechinc.com

| | |
|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

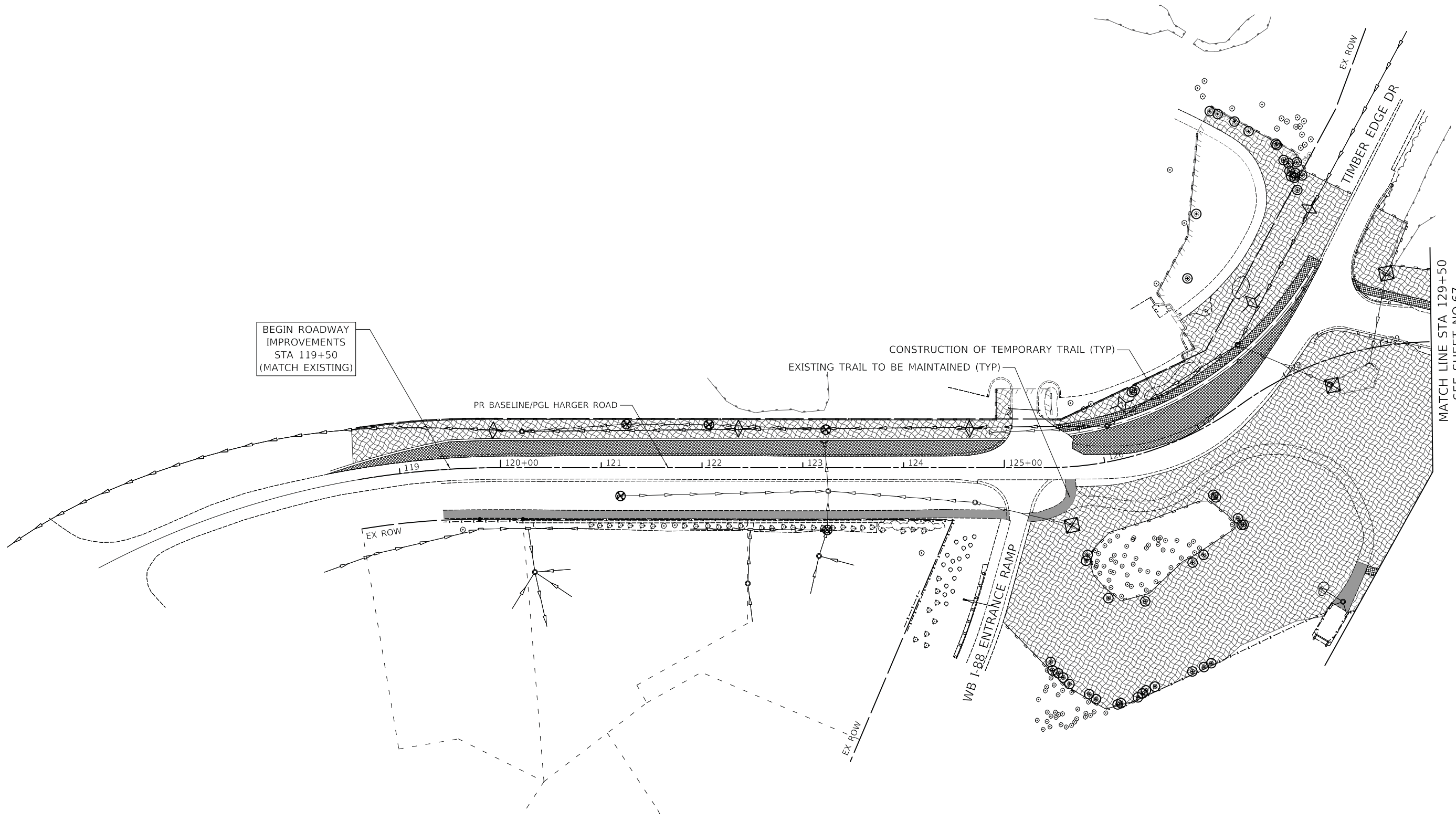
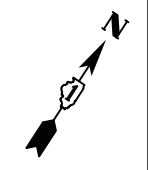
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



EROSION CONTROL PLAN - PRESTAGE

SHEET 1 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 68 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |



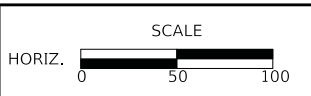
MATCH LINE STA 129+50
SEE SHEET NO 67

HARGER ROAD

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|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

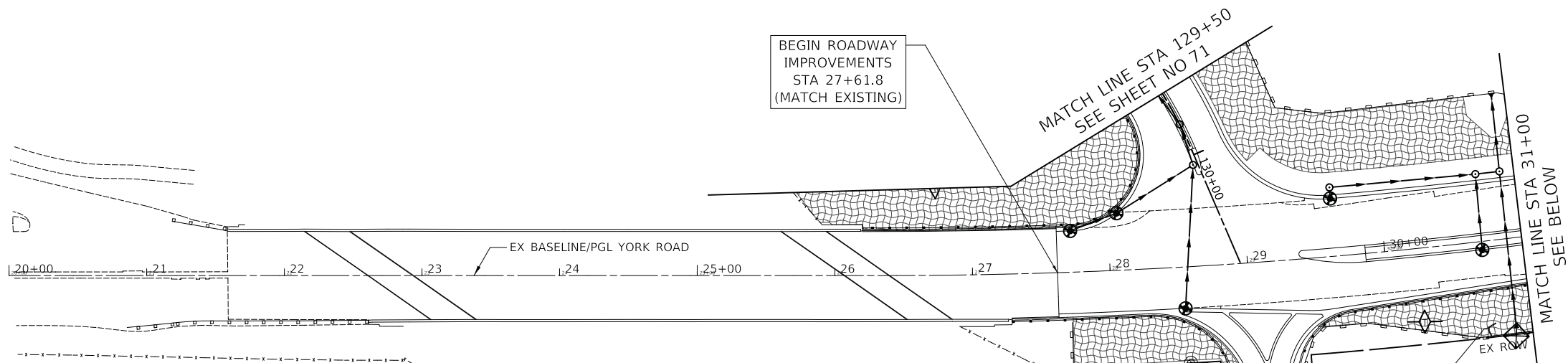
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



EROSION CONTROL PLAN - PRESTAGE

SHEET 2 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 69 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



BEGIN ROADWAY IMPROVEMENTS STA 27+61.8 (MATCH EXISTING)

MATCH LINE STA 129+50 SEE SHEET NO 71

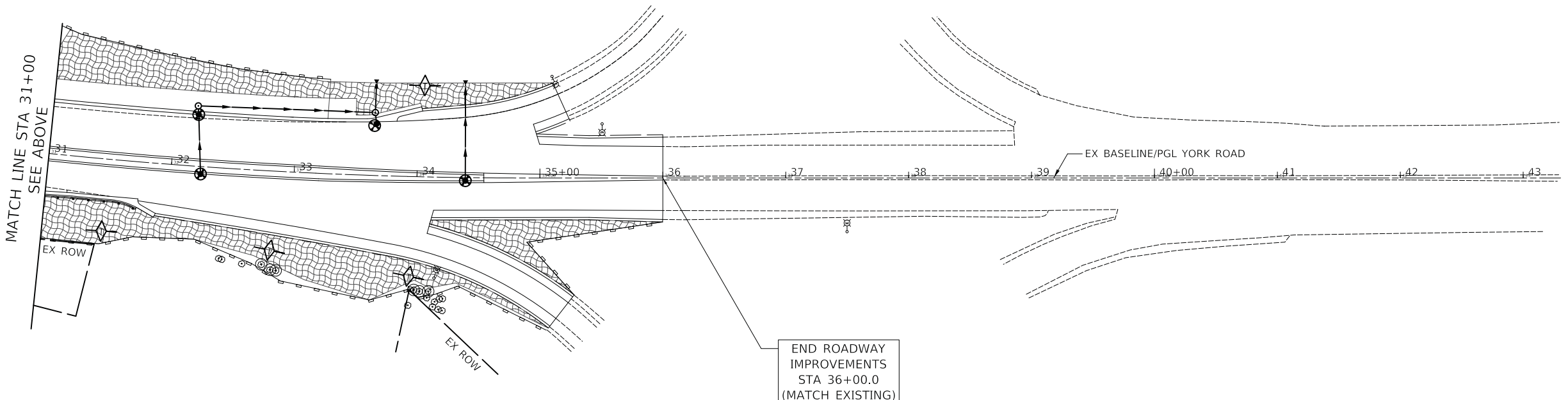
MATCH LINE STA 31+00 SEE BELOW

LEGEND

- TEMPORARY DITCH CHECKS
- INLET AND PIPE PROTECTION (PAID FOR AS 1 INLET FILTER AND 16 LF OF PERIMETER EROSION BARRIER PER IDOT STANDARD 280001-07)
- INLET FILTERS
- PERIMETER EROSION BARRIER
- EXISTING TREES
- PROPOSED STORM SEWER
- TREE PROTECTION AND PRESERVATION (AT THE DIRECTION OF ENGINEER)
- TEMPORARY EROSION CONTROL SEEDING
- TEMPORARY EROSION CONTROL BLANKET
- WETLAND
- TEMPORARY PAVING DURING STAGE
- TEMPORARY DITCH CHECKS INSTALLED IN PREVIOUS STAGE
- INLET AND PIPE PROTECTION INSTALLED IN PREVIOUS STAGE
- INLET FILTERS INSTALLED IN PREVIOUS STAGE
- PERIMETER EROSION BARRIER INSTALLED IN PREVIOUS STAGE
- PROPOSED STORM SEWER INSTALLED IN PREVIOUS STAGE



YORK ROAD



END ROADWAY IMPROVEMENTS STA 36+00.0 (MATCH EXISTING)



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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

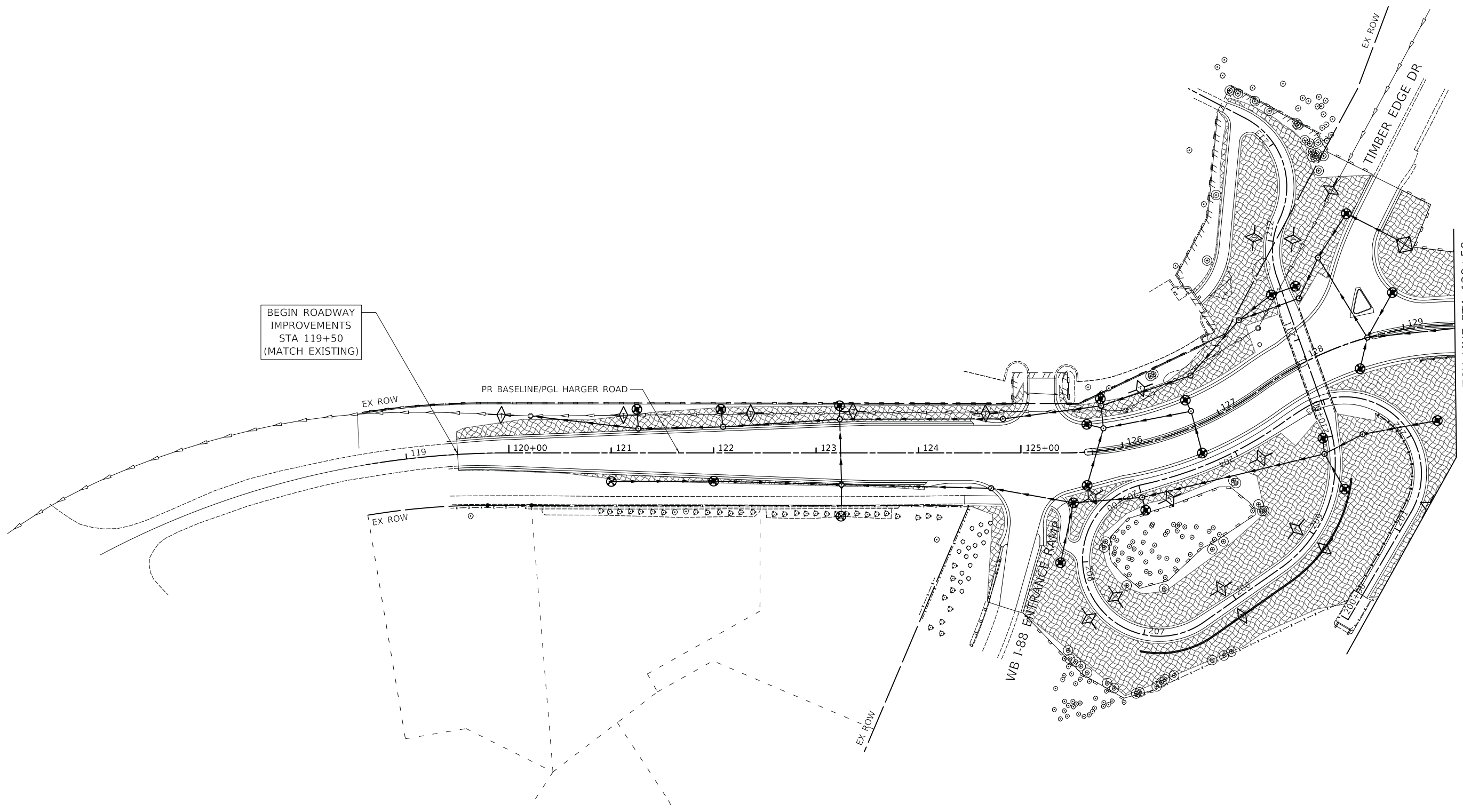
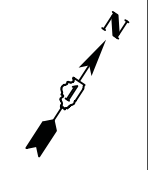
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



EROSION CONTROL PLAN - STAGE 1 TO 5

SHEET 1 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 70 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

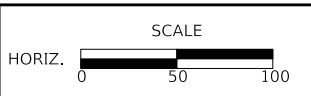


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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**








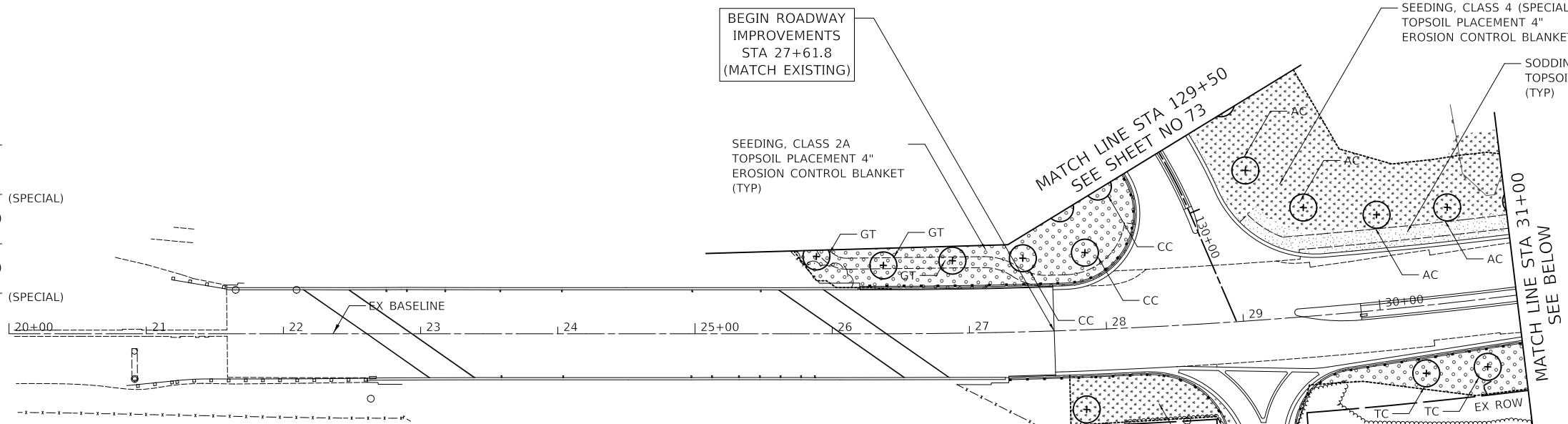
EROSION CONTROL PLAN - STAGE 1 TO 5

SHEET 2 OF 2 SHEETS STA. TO STA.

| | | | | |
|---|---------------------------|------------------|---------------------|-----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 71 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 61G16 | |

LEGEND

-  EXISTING TREES
-  PROPOSED TREES
-  SODDING, SALT TOLERANT TOPSOIL PLACEMENT 4"
-  SEEDING, CLASS 2A TOPSOIL PLACEMENT 4" EROSION CONTROL BLANKET
-  SEEDING, CLASS 2A TOPSOIL PLACEMENT 4" EROSION CONTROL BLANKET (SPECIAL)
-  SEEDING, CLASS 4 (SPECIAL) TOPSOIL PLACEMENT 4" EROSION CONTROL BLANKET
-  SEEDING, CLASS 4 (SPECIAL) TOPSOIL PLACEMENT 4" EROSION CONTROL BLANKET (SPECIAL)
-  EXISTING WETLAND

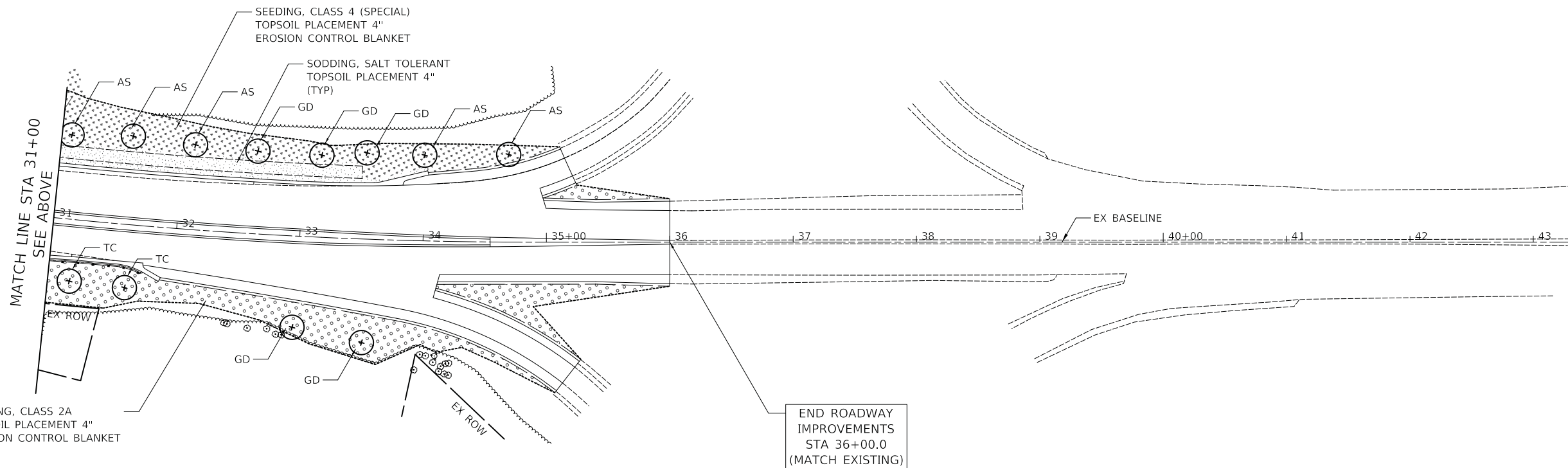


| Symbol | Pay Item # | Botanic Name | Common Name | Size | Qty. |
|--------|------------|---------------------------------------|--------------------------------------|------------|------|
| AF | A2000220 | ACER X FREEMANII MARMO | MARMO FREEMAN MAPLE | 2 1/2" DIA | 3 |
| AS | A2001820 | ACER SACCHARUM GREEN MOUNTAIN | GREEN MOUNTAIN SUGAR MAPLE | 2 1/2" DIA | 5 |
| AC | B2000566 | AMELANCHIER CANADENSIS | SHADBLOW SERVICEBERRY | 6' HEIGHT | 7 |
| CC | A2002520 | CARPINUS CAROLINIANA | AMERICAN HORNBEAM | 2 1/2" DIA | 5 |
| GT | A2004820 | GLEDITSIA TRIACANTHOS INERMIS SKYLINE | SKYLINE THORNLESS COMMON HONEYLOCUST | 2 1/2" DIA | 7 |
| GD | A2005020 | GYMNOCLADUS DIOICUS | KENTUCKY COFFEETREE | 2 1/2" DIA | 8 |
| QR | A2007116 | QUERCUS RUBRA | RED OAK | 2" DIA | 11 |
| TA | A2007920 | TILIA AMERICANA REDMOND | REDMOND AMERICAN LINDEN | 2 1/2" DIA | 5 |
| TC | A2008120 | TILIA CORDATA GREENSPIRE | GREENSPIRE LITTLE LEAF LINDEN | 2 1/2" DIA | 4 |

NOTE:




1. TOPSOIL PLACEMENT SHALL BE PAID FOR AS "TOPSOIL EXCAVATION AND PLACEMENT".
2. TYPICAL TREE SPACING IS 50'.

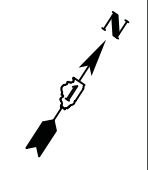
YORK ROAD



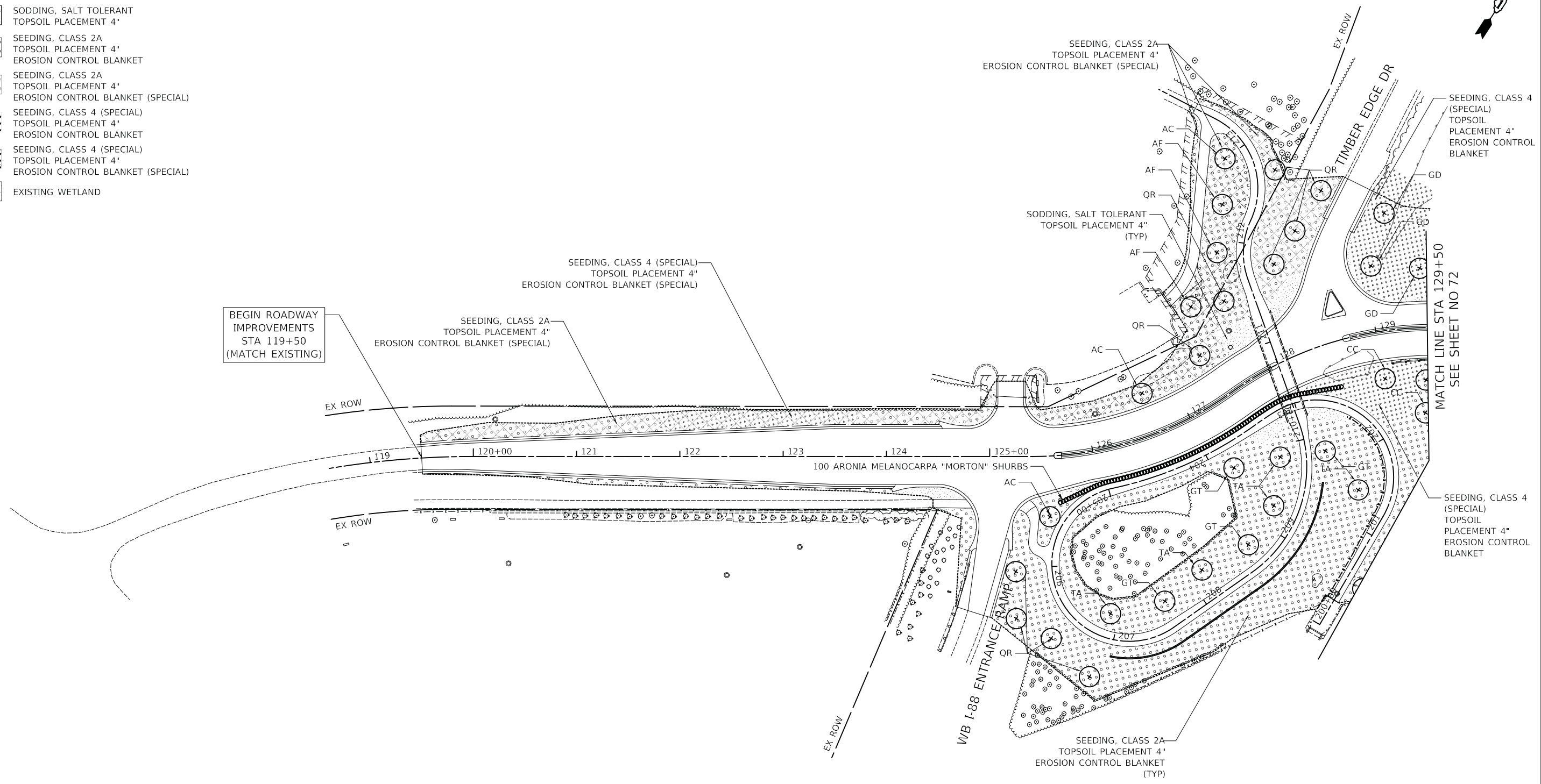
YORK ROAD

LEGEND

-  EXISTING TREES
-  PROPOSED TREES
-  SODDING, SALT TOLERANT
TOPSOIL PLACEMENT 4"
-  SEEDING, CLASS 2A
TOPSOIL PLACEMENT 4"
EROSION CONTROL BLANKET
-  SEEDING, CLASS 2A
TOPSOIL PLACEMENT 4"
EROSION CONTROL BLANKET (SPECIAL)
-  SEEDING, CLASS 4 (SPECIAL)
TOPSOIL PLACEMENT 4"
EROSION CONTROL BLANKET
-  SEEDING, CLASS 4 (SPECIAL)
TOPSOIL PLACEMENT 4"
EROSION CONTROL BLANKET (SPECIAL)
-  EXISTING WETLAND



BEGIN ROADWAY
IMPROVEMENTS
STA 119+50
(MATCH EXISTING)



NOTE:

1. TOPSOIL PLACEMENT SHALL BE PAID FOR AS "TOPSOIL EXCAVATION AND PLACEMENT".

HARGER ROAD



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| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

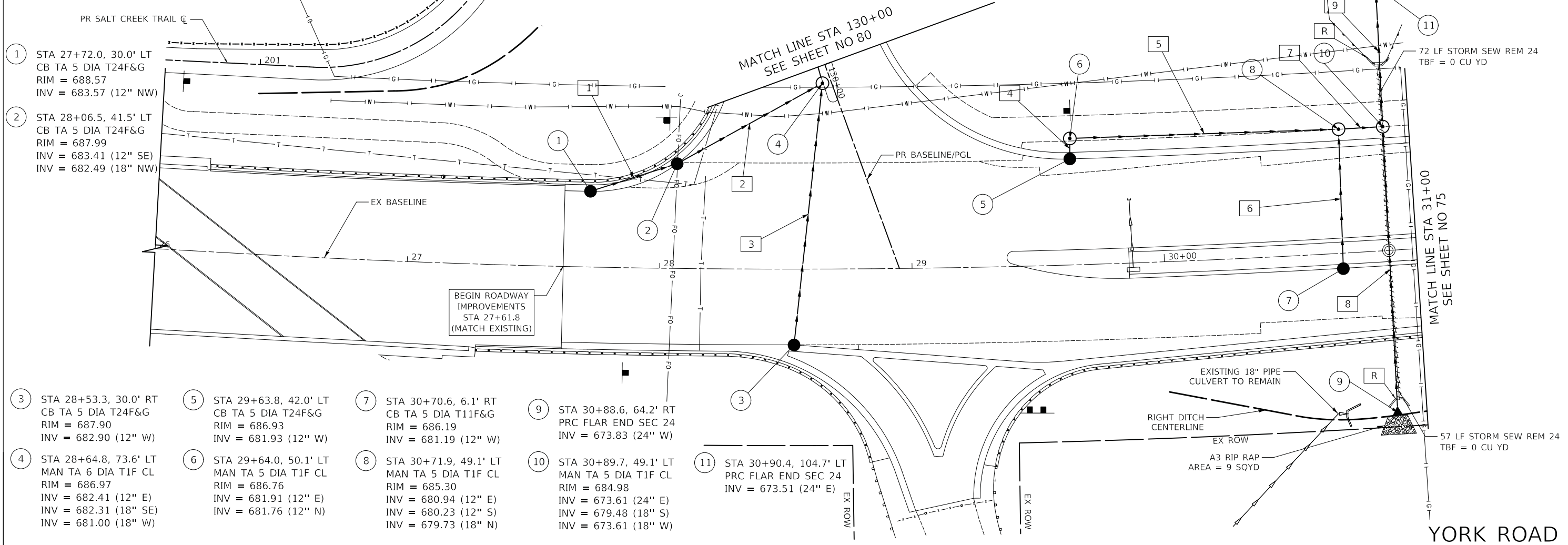


LANDSCAPING PLAN

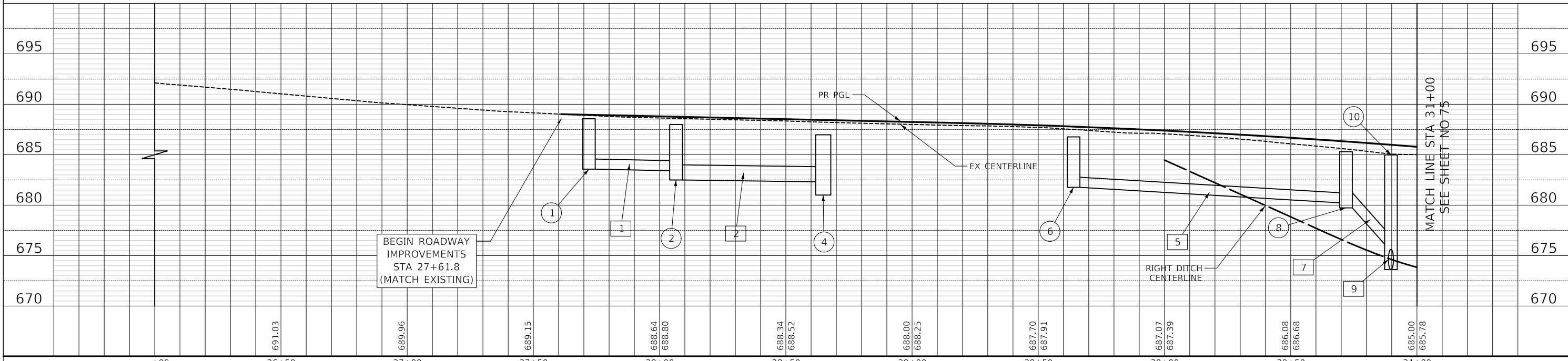
SHEET 2 OF 2 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 73 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

- 1 31' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 7 CU YD
- 2 61' - STORM SEW WM REQ 18" @ 0.30%
T.B.F. = 16 CU YD
- 3 99' - STORM SEW WM REQ 12" @ 0.50%
T.B.F. = 23 CU YD
- 4 3' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 1 CU YD
- 5 102' - STORM SEW CL A 2, 12" @ 1.50%
T.B.F. = 26 CU YD
- 6 50' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 11 CU YD
- 7 13' - STORM SEW CL A 2, 18" @ 1.90%
T.B.F. = 4 CU YD
- 8 108' - STORM SEW CL A 2, 24" @ 0.20%
T.B.F. = 173 CU YD
- 9 51' - STORM SEW CL A 2, 24" @ 0.20%
T.B.F. = 79 CU YD



- 1 STA 27+72.0, 30.0' LT
CB TA 5 DIA T24F&G
RIM = 688.57
INV = 683.57 (12" NW)
- 2 STA 28+06.5, 41.5' LT
CB TA 5 DIA T24F&G
RIM = 687.99
INV = 683.41 (12" SE)
INV = 682.49 (18" NW)
- 3 STA 28+53.3, 30.0' RT
CB TA 5 DIA T24F&G
RIM = 687.90
INV = 682.90 (12" W)
- 4 STA 28+64.8, 73.6' LT
MAN TA 6 DIA T1F CL
RIM = 686.97
INV = 682.41 (12" E)
INV = 682.31 (18" SE)
INV = 681.00 (18" W)
- 5 STA 29+63.8, 42.0' LT
CB TA 5 DIA T24F&G
RIM = 686.93
INV = 681.93 (12" W)
- 6 STA 29+64.0, 50.1' LT
MAN TA 5 DIA T1F CL
RIM = 686.76
INV = 681.91 (12" E)
INV = 681.76 (12" N)
- 7 STA 30+70.6, 6.1' RT
CB TA 5 DIA T11F&G
RIM = 686.19
INV = 681.19 (12" W)
- 8 STA 30+71.9, 49.1' LT
MAN TA 5 DIA T1F CL
RIM = 685.30
INV = 680.94 (12" E)
INV = 680.23 (12" S)
INV = 679.73 (18" N)
- 9 STA 30+88.6, 64.2' RT
PRC FLAR END SEC 24
INV = 673.83 (24" W)
- 10 STA 30+89.7, 49.1' LT
MAN TA 5 DIA T1F CL
RIM = 684.98
INV = 673.61 (24" E)
INV = 679.48 (18" S)
INV = 673.61 (18" W)
- 11 STA 30+90.4, 104.7' LT
PRC FLAR END SEC 24
INV = 673.51 (24" E)



| | | | | | | | | | | | |
|--|-----|--------|--------|--------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|
| | +00 | 26+50 | 27+00 | 27+50 | 28+00 | 28+50 | 29+00 | 29+50 | 30+00 | 30+50 | 31+00 |
| | | 691.03 | 689.96 | 689.15 | 688.64 688.90 | 688.34 688.52 | 688.00 688.25 | 687.70 687.91 | 687.07 687.39 | 686.08 686.68 | 685.00 685.78 |

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE
VERT. 0 5 10
HORIZ. 0 20 40

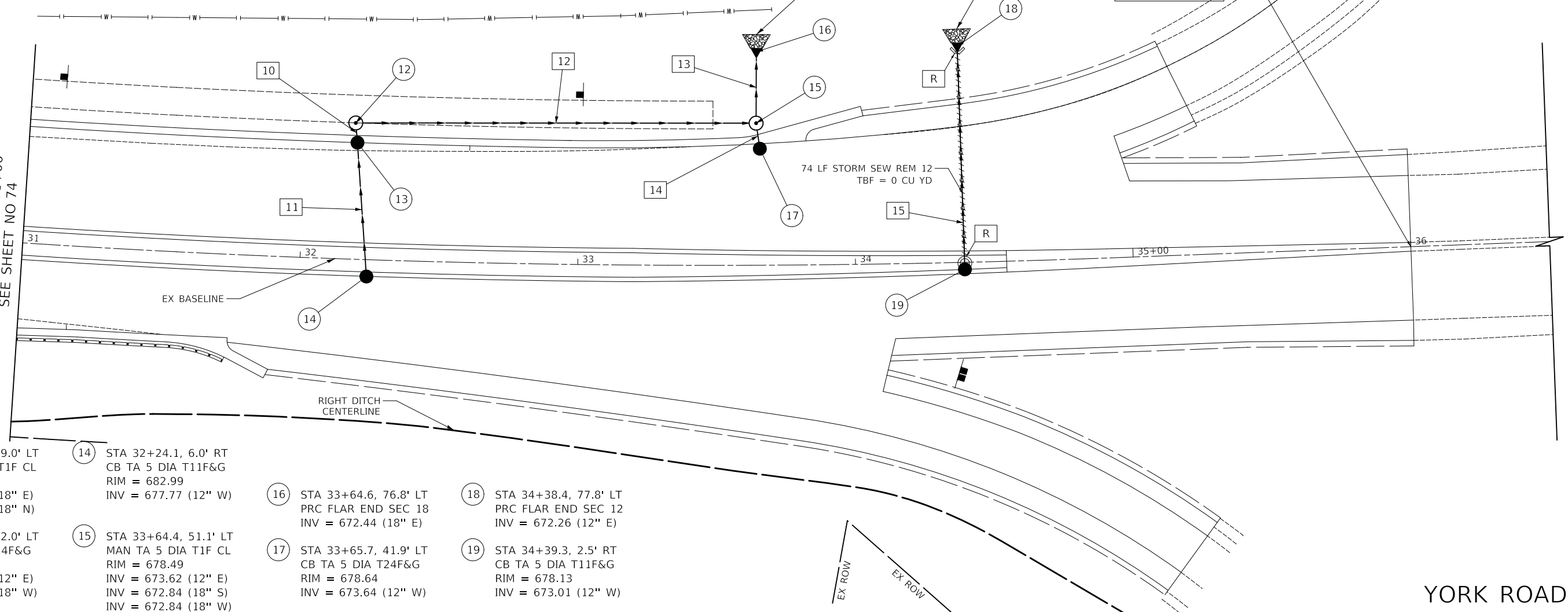
DRAINAGE AND UTILITIES

| | | | | |
|---|---------------------------|------------------|---------------------|-----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 74 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

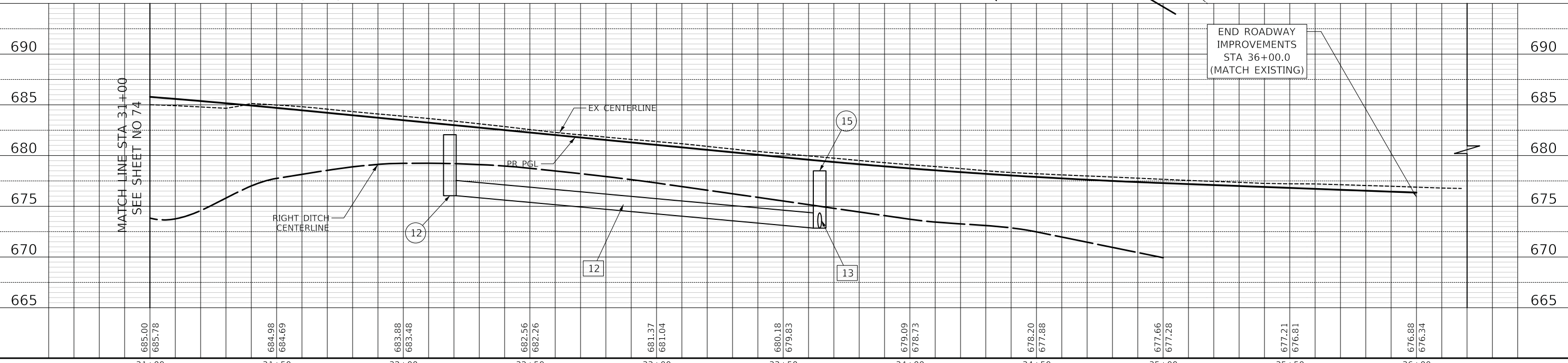
- 10 2' - STORM SEW CL A 2, 18" @ 0.30%
T.B.F. = 1 CU YD
- 11 43' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 10 CU YD
- 12 139' - STORM SEW CL A 2, 18" @ 2.30%
T.B.F. = 50 CU YD
- 13 21' - STORM SEW CL A 2, 18" @ 1.90%
T.B.F. = 7 CU YD
- 14 4' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 1 CU YD
- 15 75' - STORM SEW CL A 2, 12" @ 1.00%
T.B.F. = 16 CU YD

MATCH LINE STA 31+00
SEE SHEET NO 74

END ROADWAY
IMPROVEMENTS
STA 36+00.0
(MATCH EXISTING)



- 12 STA 32+18.4, 49.0' LT
MAN TA 5 DIA T1F CL
RIM = 682.05
INV = 676.66 (18" E)
INV = 676.04 (18" N)
- 13 STA 32+19.3, 42.0' LT
CB TA 5 DIA T24F&G
RIM = 682.30
INV = 677.56 (12" E)
INV = 676.67 (18" W)
- 14 STA 32+24.1, 6.0' RT
CB TA 5 DIA T11F&G
RIM = 682.99
INV = 677.77 (12" W)
- 15 STA 33+64.4, 51.1' LT
MAN TA 5 DIA T1F CL
RIM = 678.49
INV = 673.62 (12" E)
INV = 672.84 (18" S)
INV = 672.84 (18" W)
- 16 STA 33+64.6, 76.8' LT
PRC FLAR END SEC 18
INV = 672.44 (18" E)
- 17 STA 33+65.7, 41.9' LT
CB TA 5 DIA T24F&G
RIM = 678.64
INV = 673.64 (12" W)
- 18 STA 34+38.4, 77.8' LT
PRC FLAR END SEC 12
INV = 672.26 (12" E)
- 19 STA 34+39.3, 2.5' RT
CB TA 5 DIA T11F&G
RIM = 678.13
INV = 673.01 (12" W)

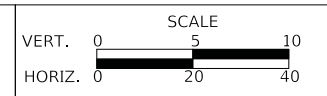


| | | | | | | | | | | | | |
|--|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|--|
| | 31+00 | 31+50 | 32+00 | 32+50 | 33+00 | 33+50 | 34+00 | 34+50 | 35+00 | 35+50 | 36+00 | |
| | 685.00 685.78 | 684.98 684.69 | 683.88 683.48 | 682.56 682.26 | 681.37 681.04 | 680.18 679.83 | 679.09 678.73 | 678.20 677.88 | 677.66 677.28 | 677.21 676.81 | 676.88 676.34 | |

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| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - 12/27/2019 | REVISED - |

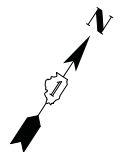
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



DRAINAGE AND UTILITIES

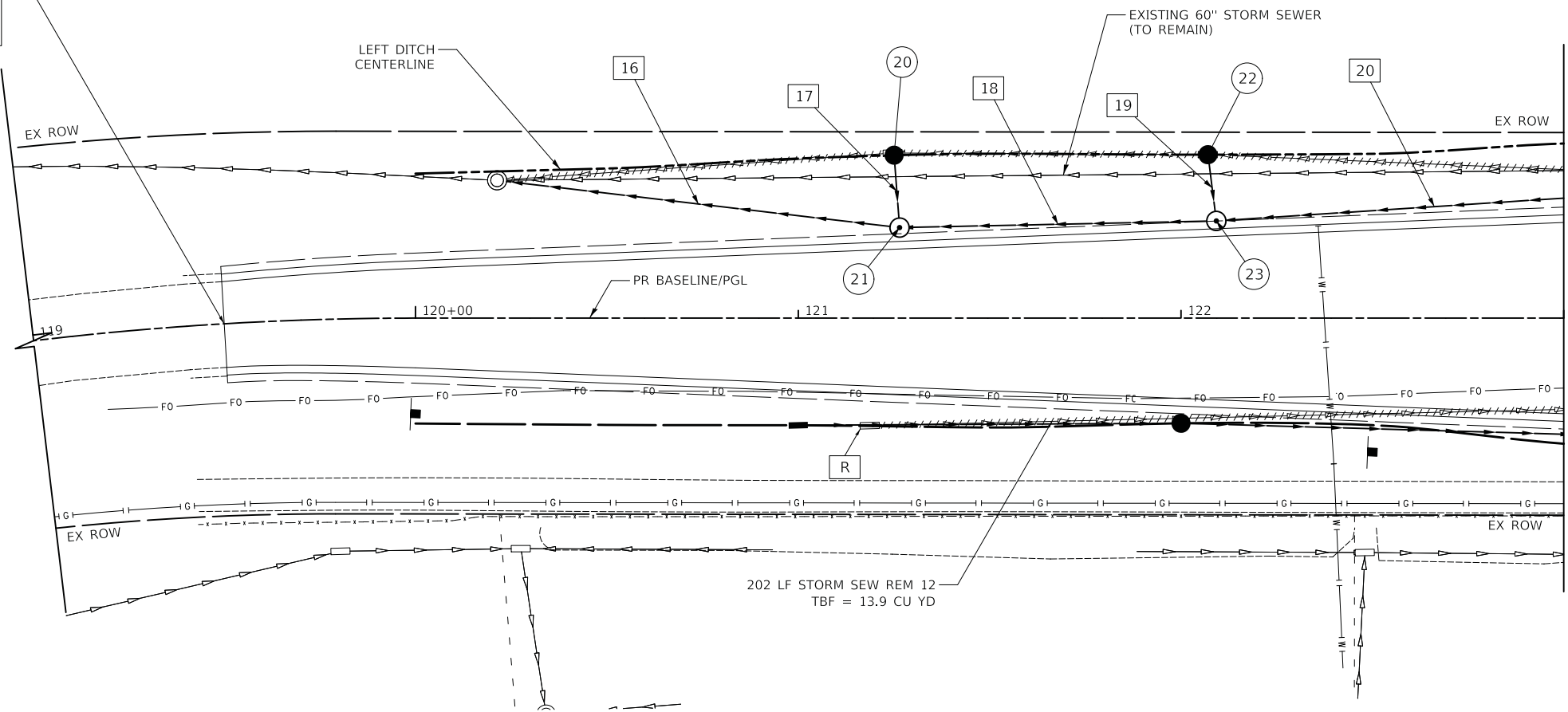
SHEET 2 OF 7 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 75 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



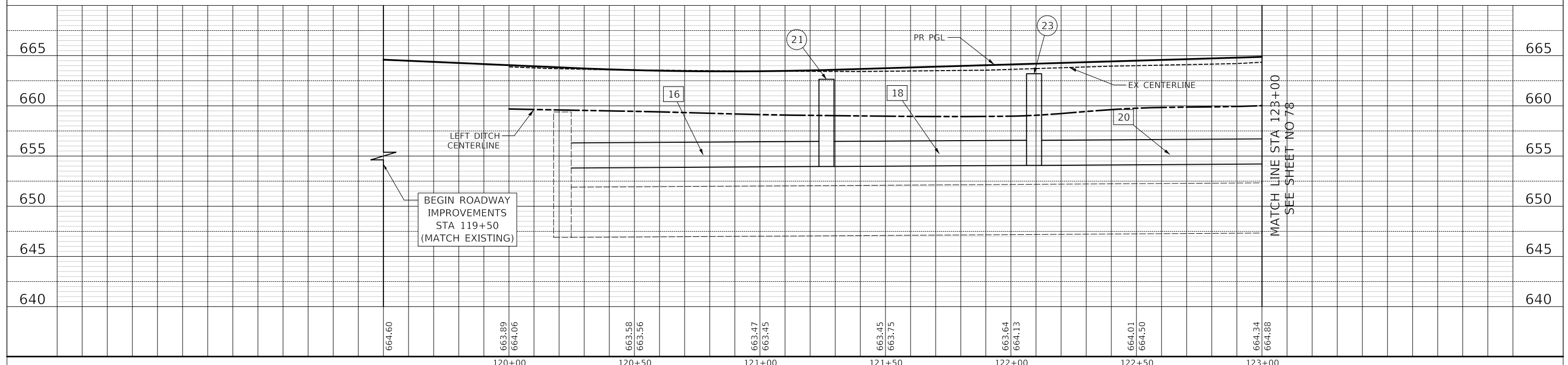
- 16 101' - STORM SEW CL A 2, 30" @ 0.15%
T.B.F. = 126 CU YD
- 17 14' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 9 CU YD
- 18 78' - STORM SEW CL A 2, 30" @ 0.15%
T.B.F. = 100 CU YD
- 19 12' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 8 CU YD
- 20 109' - STORM SEW CL A 2, 30" @ 0.15%
T.B.F. = 152 CU YD

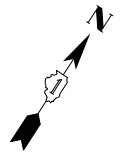
BEGIN ROADWAY IMPROVEMENTS STA 119+50 (MATCH EXISTING)



- 20 STA 121+25.0, 42.6' LT
CB TA 5 DIA T8G
RIM = 659.00
INV = 654.50 (12" SE)
- 21 STA 121+26.5, 23.7' LT
MAN TA 6 DIA T1F CL
RIM = 663.14
INV = 653.96 (30" NE)
INV = 653.96 (30" SW)
INV = 654.43 (12" NW)
- 22 STA 122+07.0, 42.7' LT
CB TA 5 DIA T8G
RIM = 658.98
INV = 654.50 (12" SE)
- 23 STA 122+09.2, 25.4' LT
MAN TA 6 DIA T1F CL
RIM = 663.69
INV = 654.08 (30" NE)
INV = 654.08 (30" SW)
INV = 654.44 (12" NW)

HARGER ROAD (LEFT PROFILE)

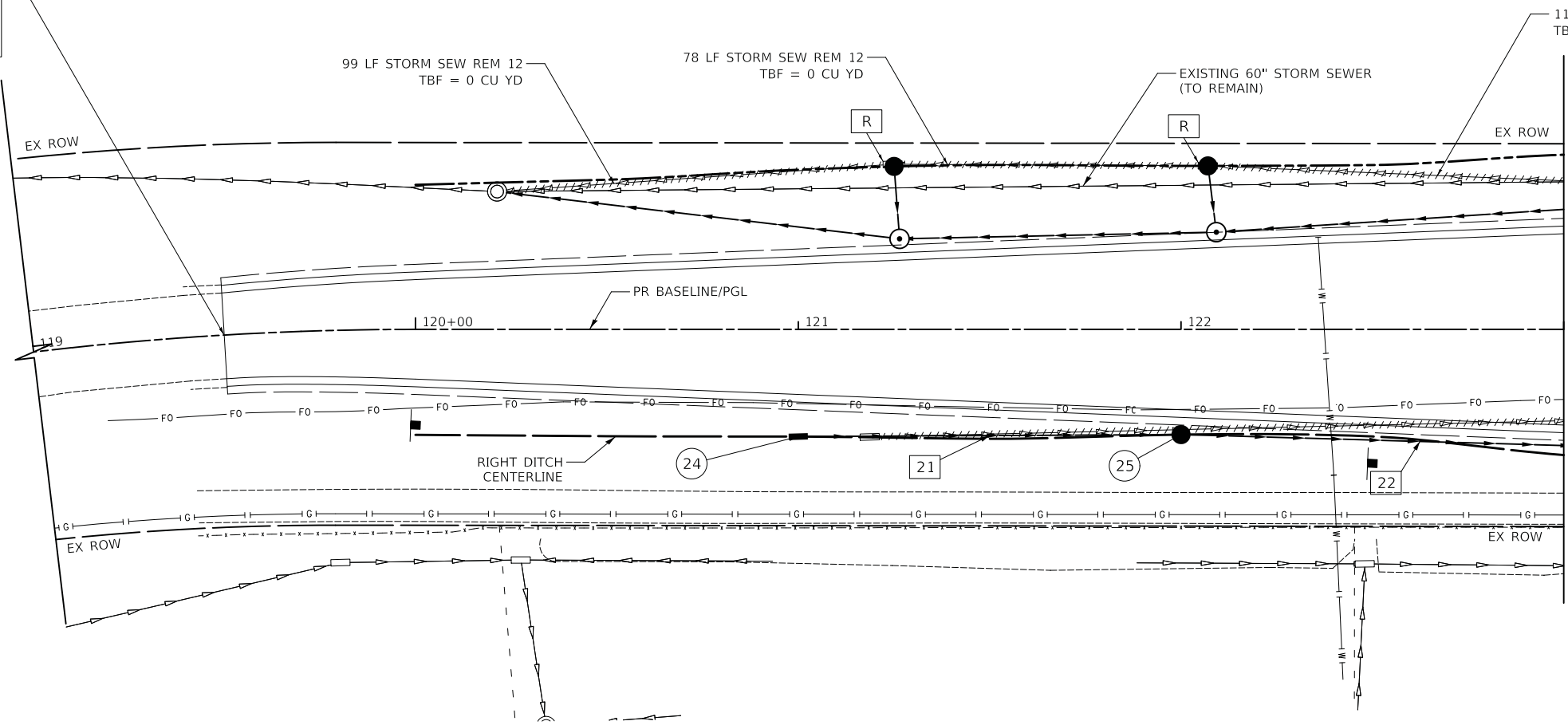




21 95' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 36 CU YD

22 120' - STORM SEW WM REQ 12" @ 0.50%
T.B.F. = 102 CU YD

BEGIN ROADWAY IMPROVEMENTS STA 119+50 (MATCH EXISTING)



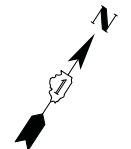
MATCH LINE STA 123+00
SEE SHEET NO 79

- 24 STA 121+00.0, 28.0' RT INTLETS TA T8G
RIM = 662.25
INV = 657.00 (12" NE)
- 25 STA 122+00.0, 27.5' RT CB TA 5 DIA T8G
RIM = 663.58
INV = 656.53 (12" NE)

HARGER ROAD (RIGHT PROFILE)



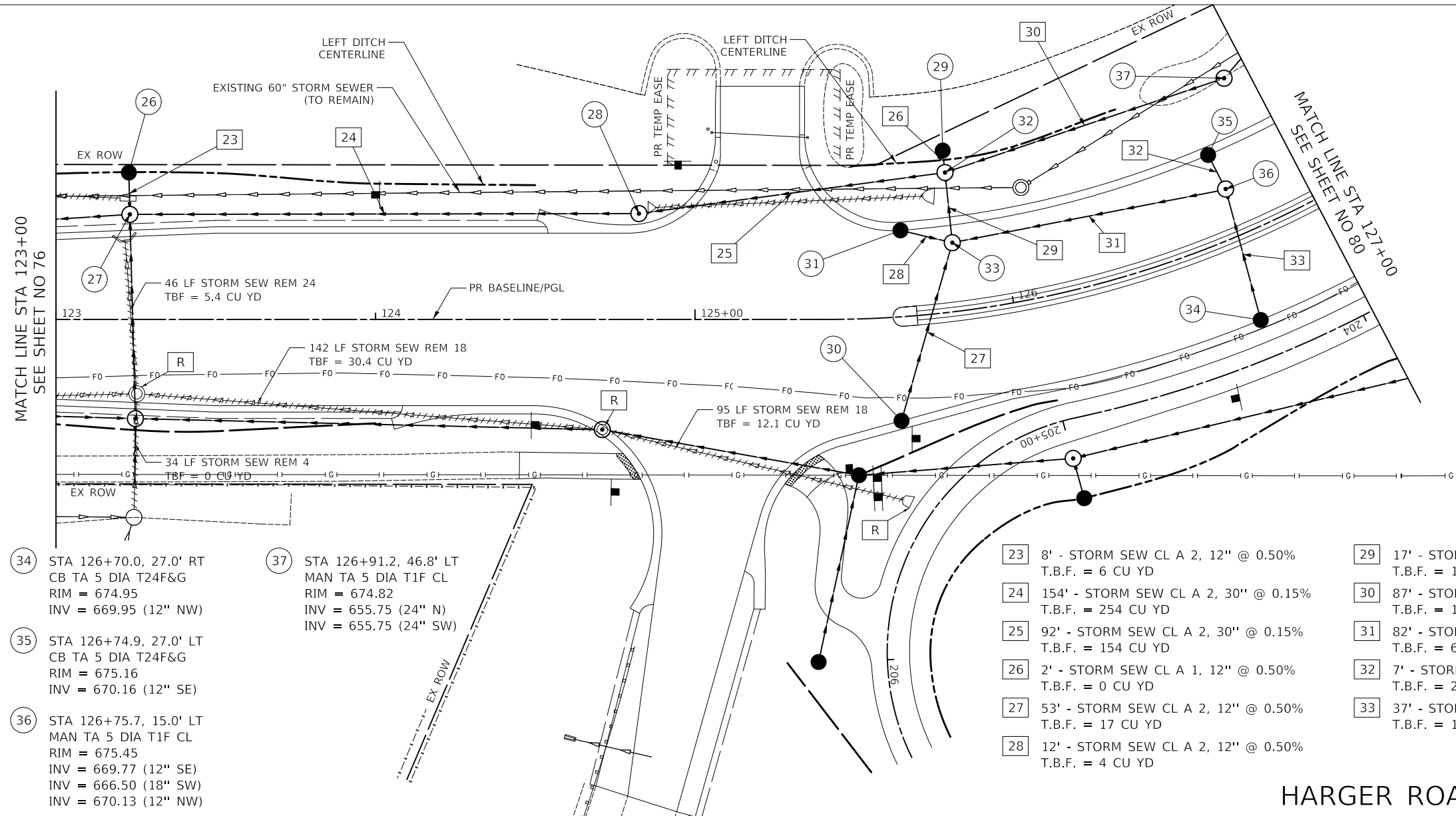
| | | | | | | | | | |
|--|---|---|--|---|--------------------------------|--------------------------|-------------------------------|-------------------------------------|---|
| | Two Pierce Place, Suite 1400 Itasca, Illinois 60143 Tel: 630.773.3900 Fax: 630.773.3975 www.civiltechinc.com | DESIGNED - DRAWN - CHECKED - DATE - 12/27/2019 | REVISED - REVISED - REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | VERT. 0 5 10 HORIZ. 0 20 40 | SCALE 1" = 20' | DRAINAGE AND UTILITIES | SHEET 4 OF 7 SHEETS STA. TO STA. | F.A.U. RTE. 2678/1446 SECTION 17-00049-00-PV COUNTY DUPAGE TOTAL SHEETS 208 SHEET NO. 77 CONTRACT NO. 61G16 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |
|--|---|---|--|---|--------------------------------|--------------------------|-------------------------------|-------------------------------------|---|



- 26 STA 123+22.8, 46.0' LT
CB TA 5 DIA T8G
RIM = 660.50
INV = 655.50 (12" SE)
- 27 STA 123+23.1, 32.9' LT
MAN TA 7 DIA T1F CL
RIM = 664.40
INV = 654.73 (30" NE)
INV = 654.24 (24" SE)
INV = 654.24 (30" SW)
INV = 655.46 (12" NW)
- 28 STA 124+82.4, 33.1' LT
MAN TA 5 DIA T1F CL
RIM = 667.28
INV = 654.94 (30" NE)
INV = 654.94 (30" SW)
- 29 STA 125+84.2, 50.5' LT
CB TA 5 DIA T8G
RIM = 668.40
INV = 663.40 (12" SE)
- 30 STA 125+62.6, 32.5' RT
CB TA 5 DIA T24F&G
RIM = 670.18
INV = 665.16 (12" N)
- 31 STA 125+66.4, 27.0' LT
CB TA 5 DIA T24F&G
RIM = 670.43
INV = 665.43 (12" E)
- 32 STA 125+84.0, 43.6' LT
MAN TA 6 DIA T1F CL
RIM = 665.00
INV = 655.58 (24" NE)
INV = 664.84 (18" SE)
INV = 655.08 (30" SW)
INV = 663.39 (12" NW)
- 33 STA 125+83.4, 21.5' LT
MAN TA 5 DIA T1F CL
RIM = 671.27
INV = 665.68 (18" NE)
INV = 664.89 (12" S)
INV = 665.37 (12" W)
INV = 664.89 (18" NW)

MATCH LINE STA 123+00
SEE SHEET NO 76

MATCH LINE STA 127+00
SEE SHEET NO 80



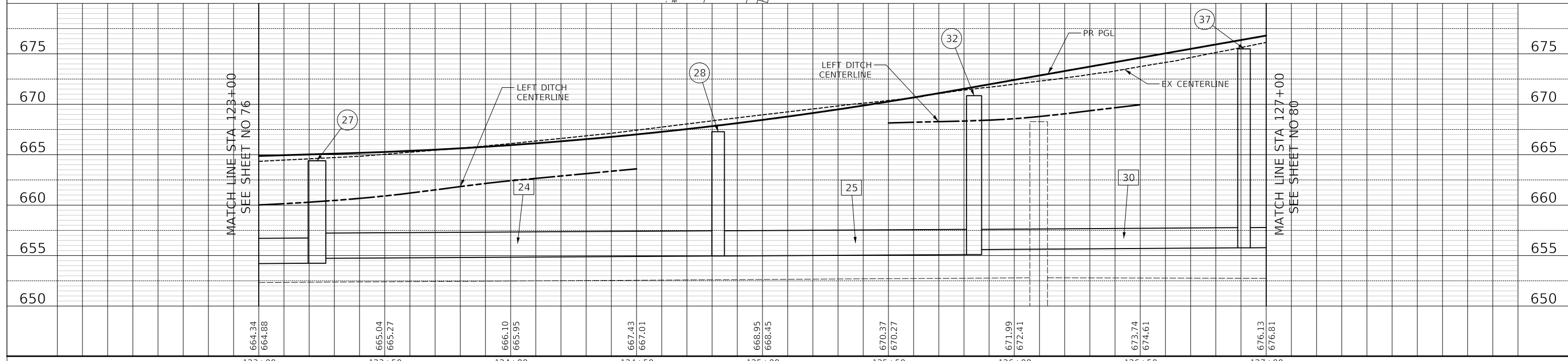
- 34 STA 126+70.0, 27.0' RT
CB TA 5 DIA T24F&G
RIM = 674.95
INV = 669.95 (12" NW)
- 35 STA 126+74.9, 27.0' LT
CB TA 5 DIA T24F&G
RIM = 675.16
INV = 670.16 (12" SE)
- 36 STA 126+75.7, 15.0' LT
MAN TA 5 DIA T1F CL
RIM = 675.45
INV = 669.77 (12" SE)
INV = 666.50 (18" SW)
INV = 670.13 (12" NW)

- 37 STA 126+91.2, 46.8' LT
MAN TA 5 DIA T1F CL
RIM = 674.82
INV = 655.75 (24" N)
INV = 655.75 (24" SW)

- 23 8' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 6 CU YD
- 24 154' - STORM SEW CL A 2, 30" @ 0.15%
T.B.F. = 254 CU YD
- 25 92' - STORM SEW CL A 2, 30" @ 0.15%
T.B.F. = 154 CU YD
- 26 2' - STORM SEW CL A 1, 12" @ 0.50%
T.B.F. = 0 CU YD
- 27 53' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 17 CU YD
- 28 12' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 4 CU YD

- 29 17' - STORM SEW CL A 1, 18" @ 0.30%
T.B.F. = 1 CU YD
- 30 87' - STORM SEW CL A 3, 24" @ 0.20%
T.B.F. = 194 CU YD
- 31 82' - STORM SEW CL A 2, 18" @ 1.00%
T.B.F. = 66 CU YD
- 32 7' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 2 CU YD
- 33 37' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 11 CU YD

HARGER ROAD (LEFT PROFILE)

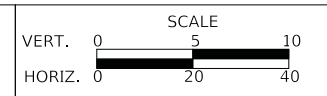


| | | | | | | | | | | |
|--|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|--|
| | 123+00 | 123+50 | 124+00 | 124+50 | 125+00 | 125+50 | 126+00 | 126+50 | 127+00 | |
| | 664.34 664.88 | 665.04 665.27 | 666.10 665.95 | 667.43 667.01 | 668.95 668.45 | 670.37 670.27 | 671.99 672.41 | 673.74 674.61 | 676.13 676.81 | |

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| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - 12/27/2019 | REVISED - |

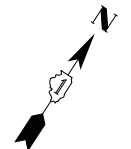
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



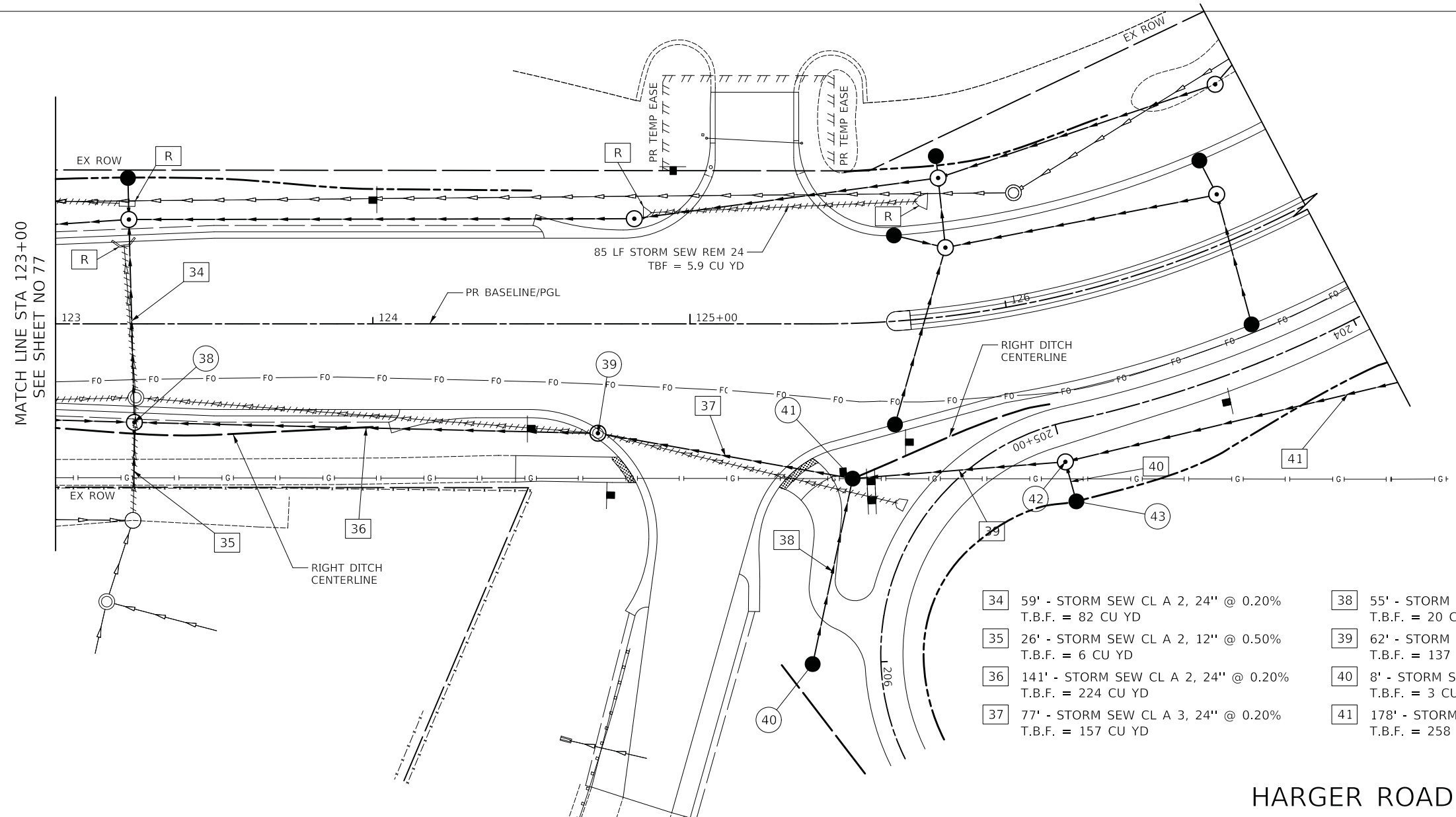
DRAINAGE AND UTILITIES

SHEET 5 OF 7 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 78 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

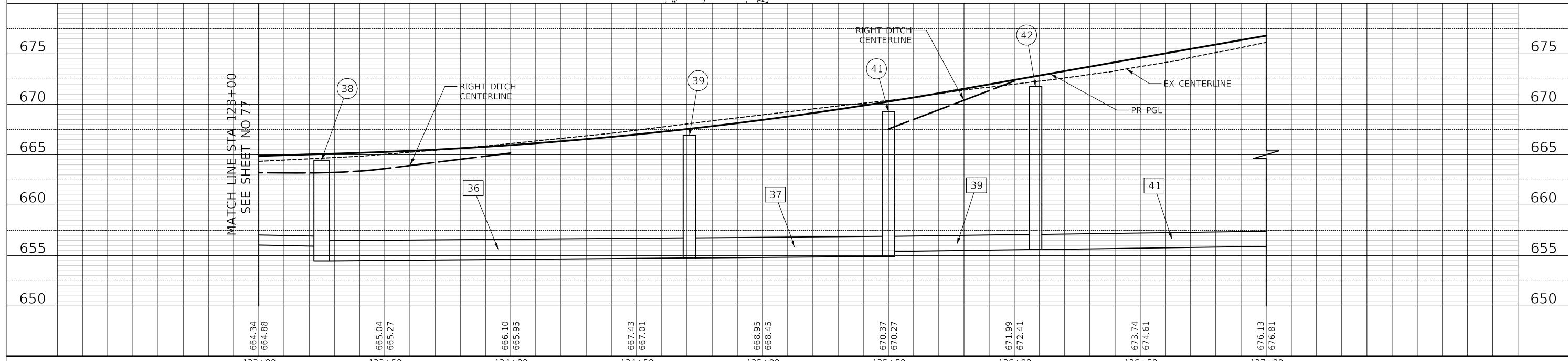


- 38 STA 123+24.9, 31.1' RT
MAN TA 6 DIA T1F CL
RIM = 664.45
INV = 654.47 (24" NE)
INV = 658.97 (12" SE)
INV = 655.93 (12" SW)
INV = 654.36 (24" NW)
- 39 STA 124+71.0, 34.5' RT
MAN TA 5 DIA T1F CL
RIM = 667.05
INV = 654.76 (24" NE)
INV = 654.76 (24" SW)
- 40 STA 125+38.7, 107.3' RT
CB TA 5 DIA T8G
RIM = 667.46
INV = 662.46 (12" N)
- 41 STA 125+50.0, 48.9' RT
CB TA 5 DIA T8G
RIM = 669.29
INV = 655.41 (18" NE)
INV = 662.19 (12" S)
INV = 654.91 (24" SW)
- 42 STA 126+08.4, 51.4' RT
MAN TA 5 DIA T1F CL
RIM = 671.75
INV = 655.60 (18" NE)
INV = 663.84 (12" SE)
INV = 655.60 (18" SW)
- 43 STA 126+09.2, 64.3' RT
CB TA 5 DIA T8G
RIM = 668.88
INV = 663.88 (12" NW)



- 34 59' - STORM SEW CL A 2, 24" @ 0.20%
T.B.F. = 82 CU YD
- 35 26' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 6 CU YD
- 36 141' - STORM SEW CL A 2, 24" @ 0.20%
T.B.F. = 224 CU YD
- 37 77' - STORM SEW CL A 3, 24" @ 0.20%
T.B.F. = 157 CU YD
- 38 55' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 20 CU YD
- 39 62' - STORM SEW CL A 3, 18" @ 0.30%
T.B.F. = 137 CU YD
- 40 8' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 3 CU YD
- 41 178' - STORM SEW CL A 2, 18" @ 0.30%
T.B.F. = 258 CU YD

HARGER ROAD (RIGHT PROFILE)



| | | | | | | | | | | |
|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|--------|--|
| | 123+00 | 123+50 | 124+00 | 124+50 | 125+00 | 125+50 | 126+00 | 126+50 | 127+00 | |
| 664.34 664.88 | 665.04 665.27 | 666.10 665.95 | 667.43 667.01 | 668.95 668.45 | 670.37 670.27 | 671.99 672.41 | 673.74 674.61 | 676.13 676.81 | | |

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| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - 12/27/2019 | REVISED - |

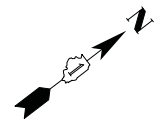
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



DRAINAGE AND UTILITIES

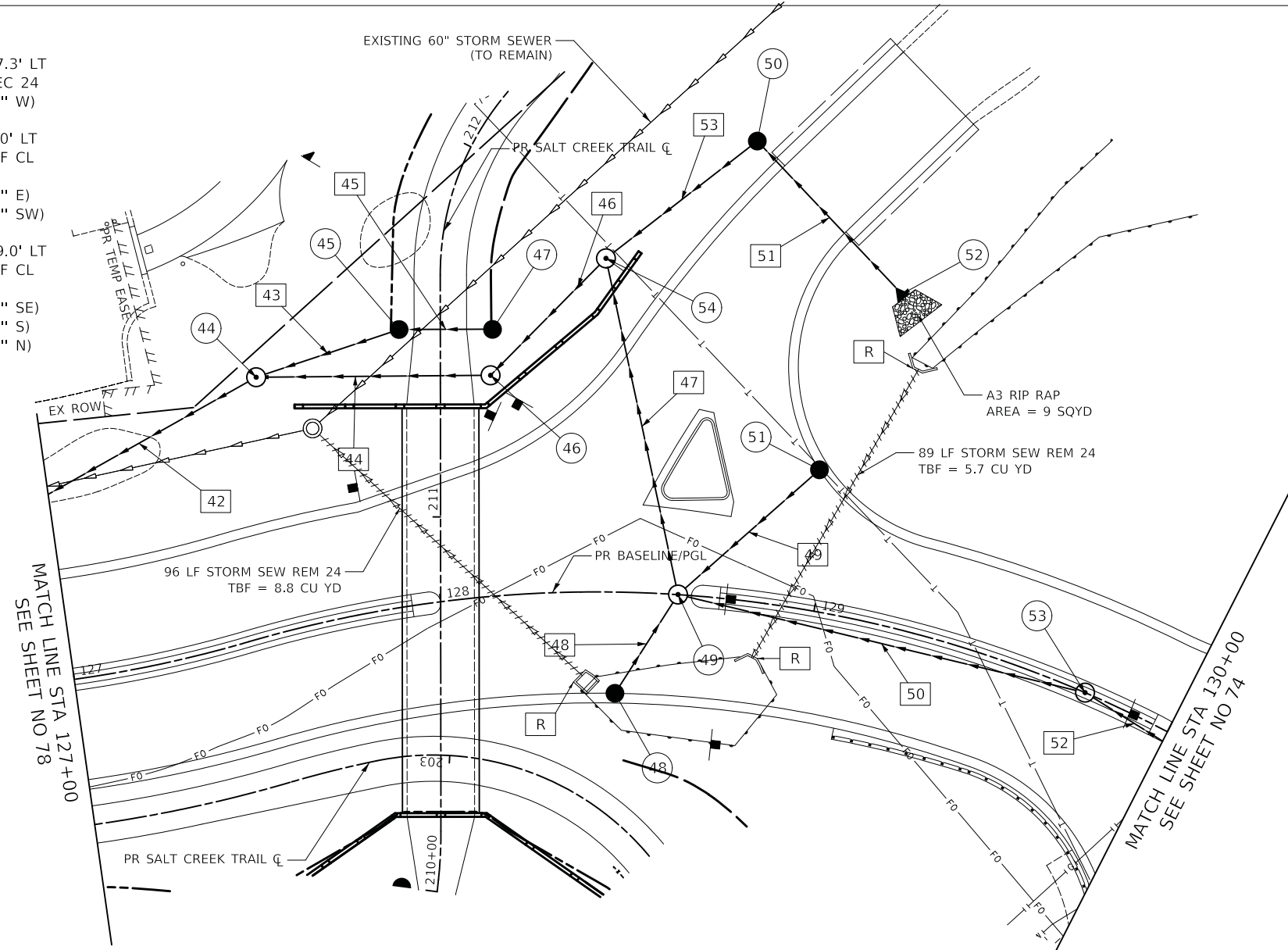
SHEET 6 OF 7 SHEETS STA. TO STA.

| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 79 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



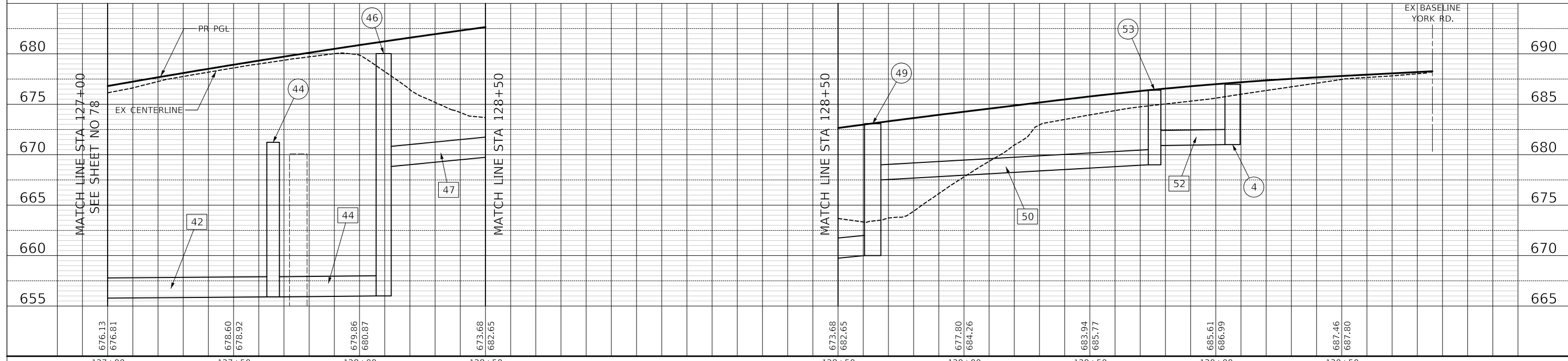
- 44 STA 127+65.7, 67.7' LT
MAN TA 5 DIA T1F CL
RIM = 671.23
INV = 656.50 (18" N)
INV = 655.88 (24" NE)
INV = 655.88 (24" S)
- 45 STA 127+98.5, 73.4' LT
CB TA 5 DIA T8G
RIM = 662.10
INV = 657.52 (12" NE)
INV = 656.60 (18" S)
- 46 STA 128+09.6, 59.5' LT
MAN TA 5 DIA T1F CL
RIM = 663.48
INV = 656.00 (24" SW)
INV = 656.92 (24" N)
- 47 STA 128+18.8, 71.0' LT
CB TA 5 DIA T8G
RIM = 662.62
INV = 657.62 (12" SW)
- 48 STA 128+47.1, 27.0' RT
CB TA 5 DIA T24F&G
RIM = 682.01
INV = 677.01 (12" N)
- 49 STA 128+63.5, 0.0' LT
MAN TA 7 DIA T1F CL
RIM = 683.08
INV = 677.50 (18" NE)
INV = 676.88 (12" S)
INV = 670.00 (18" NW)
INV = 678.14 (12" N)
- 50 STA 128+73.0, 122.3' LT
CB TA 5 DIA T8G
RIM = 680.95
INV = 667.67 (24" E)
INV = 667.67 (24" S)
- 51 STA 128+95.3, 37.4' LT
CB TA 5 DIA T24F&G
RIM = 683.24
INV = 678.36 (12" S)

- 52 STA 129+06.3, 87.3' LT
PRC FLAR END SEC 24
INV = 667.77 (24" W)
- 53 STA 129+75.7, 0.0' LT
MAN TA 5 DIA T1F CL
RIM = 686.40
INV = 680.90 (18" E)
INV = 679.00 (18" SW)
- 54 STA 128+43.7, 89.0' LT
MAN TA 7 DIA T1F CL
RIM = 678.07
INV = 668.70 (18" SE)
INV = 657.00 (24" S)
INV = 667.58 (24" N)



- 42 67' - STORM SEW CL A 3, 24" @ 0.20%
T.B.F. = 154 CU YD
- 43 35' - STORM SEW CL A 2, 18" @ 0.30%
T.B.F. = 29 CU YD
- 44 58' - STORM SEW CL A 2, 24" @ 0.20%
T.B.F. = 65 CU YD
- 45 20' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 5 CU YD
- 46 39' - STORM SEW CL A 3, 24" @ 0.20%
T.B.F. = 94 CU YD
- 47 87' - STORM SEW CL A 3, 18" @ 1.50%
T.B.F. = 153 CU YD
- 48 26' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 8 CU YD
- 49 45' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 11 CU YD
- 50 107' - STORM SEW CL A 2, 18" @ 1.40%
T.B.F. = 47 CU YD
- 51 51' - STORM SEW CL A 3, 24" @ 0.20%
T.B.F. = 112 CU YD
- 52 26' - STORM SEW CL A 2, 18" @ 0.40%
T.B.F. = 9 CU YD
- 53 46' - STORM SEW CL A 3, 24" @ 0.20%
T.B.F. = 93 CU YD

HARGER ROAD



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| CHECKED - | REVISED - |
| DATE - 12/27/2019 | REVISED - |

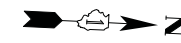
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



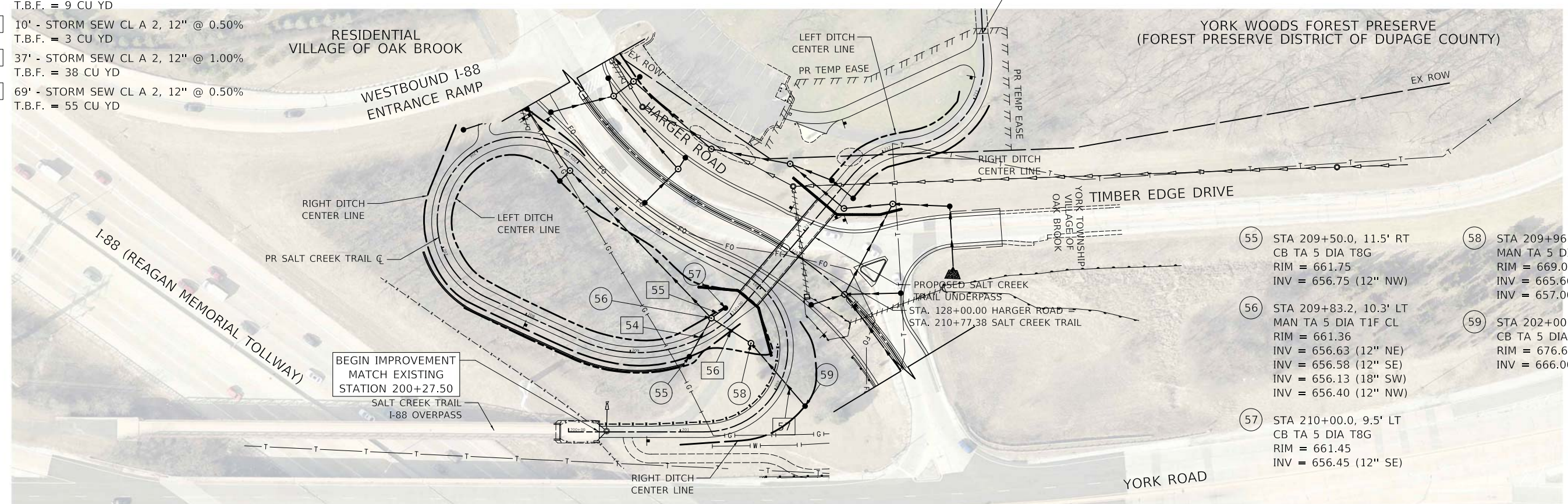
DRAINAGE AND UTILITIES

SHEET 7 OF 7 SHEETS STA. TO STA.

| | | | | |
|--------------------------------|------------------------|--------------------|------------------|--------------|
| F.A.U. R.T.E. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 80 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | CONTRACT NO. 61G16 | | |

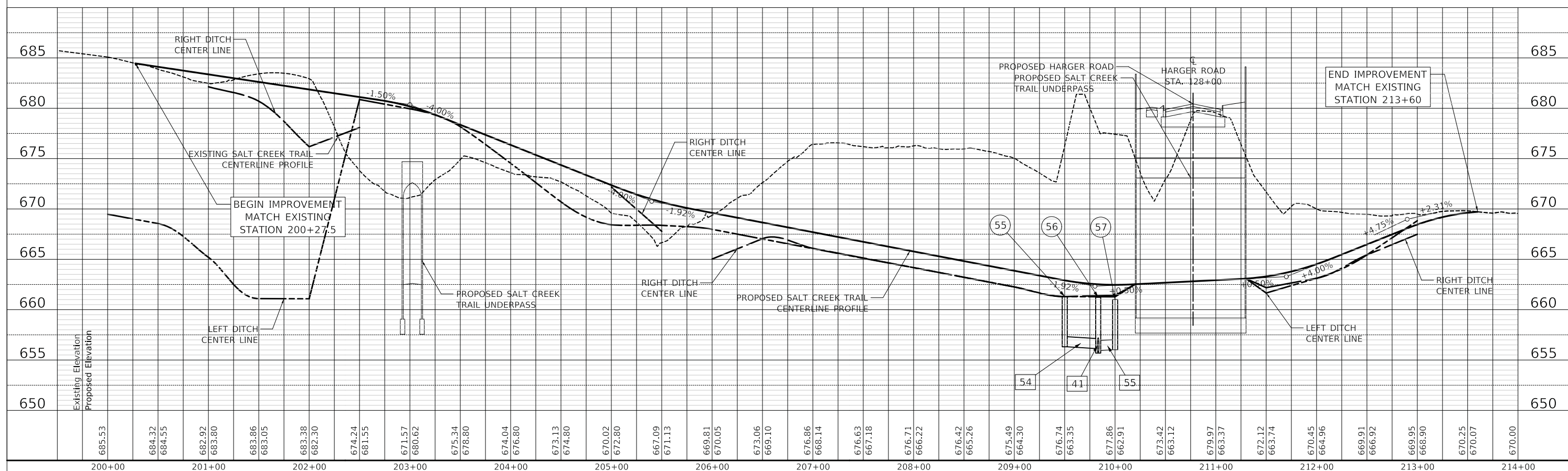


- 54 35' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 9 CU YD
- 55 10' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 3 CU YD
- 56 37' - STORM SEW CL A 2, 12" @ 1.00%
T.B.F. = 38 CU YD
- 57 69' - STORM SEW CL A 2, 12" @ 0.50%
T.B.F. = 55 CU YD



- 55 STA 209+50.0, 11.5' RT
CB TA 5 DIA T8G
RIM = 661.75
INV = 656.75 (12" NW)
- 56 STA 209+83.2, 10.3' LT
MAN TA 5 DIA T1F CL
RIM = 661.36
INV = 656.63 (12" NE)
INV = 656.58 (12" SE)
INV = 656.13 (18" SW)
INV = 656.40 (12" NW)
- 57 STA 210+00.0, 9.5' LT
CB TA 5 DIA T8G
RIM = 661.45
INV = 656.45 (12" SE)
- 58 STA 209+96.4, 29.2' RT
MAN TA 5 DIA T1F CL
RIM = 669.04
INV = 665.66 (12" NE)
INV = 657.00 (12" SW)
- 59 STA 202+00.0, 29.3' RT
CB TA 5 DIA T8G
RIM = 676.66
INV = 666.00 (12" SW)

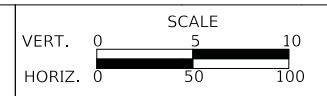
SALT CREEK TRAIL



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| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - 12/27/2019 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



DRAINAGE AND UTILITIES

SHEET 1 OF 1 SHEETS STA. TO STA.

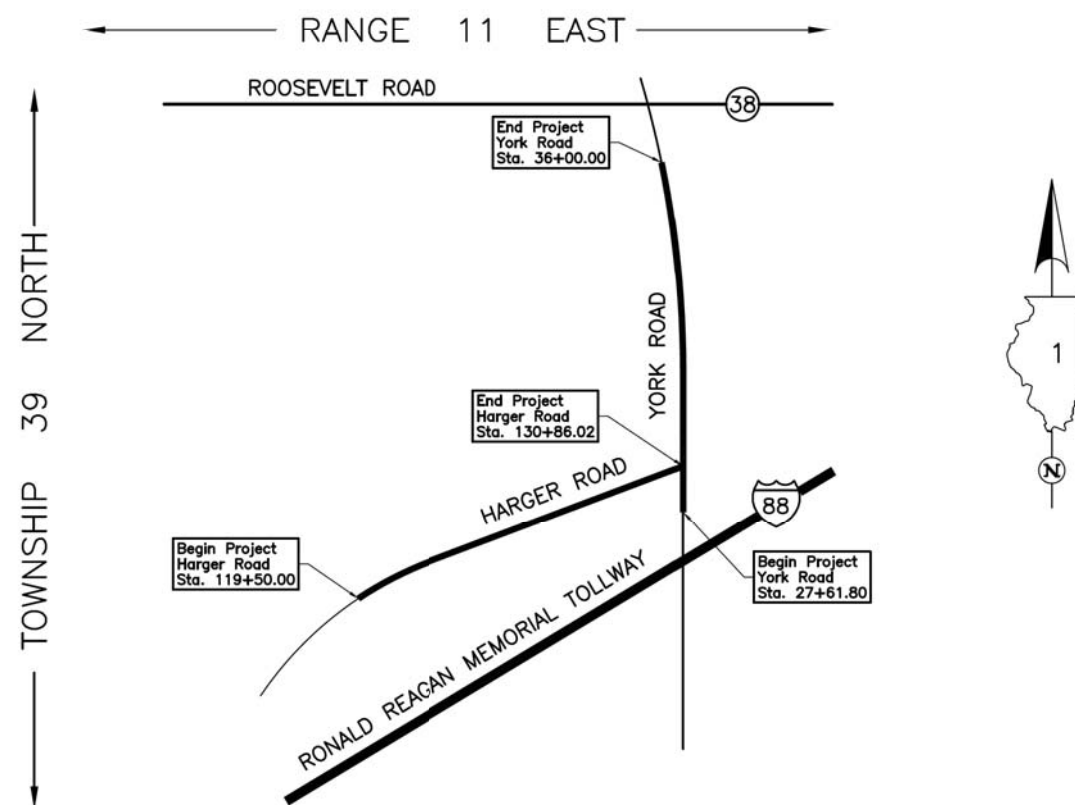
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|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 81 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLAT OF HIGHWAYS

**ROUTE: F.A.U. 1446 (HARGER ROAD)
F.A.U. 2678A (YORK ROAD)**
SECTION: 17-00049-00-PV
COUNTY: DuPAGE
LIMITS: HARGER ROAD: I-88 WEST BOUND RAMP TO YORK ROAD
YORK ROAD: HARGER ROAD TO ROOSEVELT ROAD
JOB NO.: R-55-001-97

| PARCEL NUMBER | OWNER | SHEET NUMBER | PROPERTY ACQUIRED BY |
|--------------------------|--|--------------|----------------------|
| 0001T.E.-A 0001T.E.-B | The Forest Preserve District of DuPage County | 2 & 3 | |



LOCATION MAP

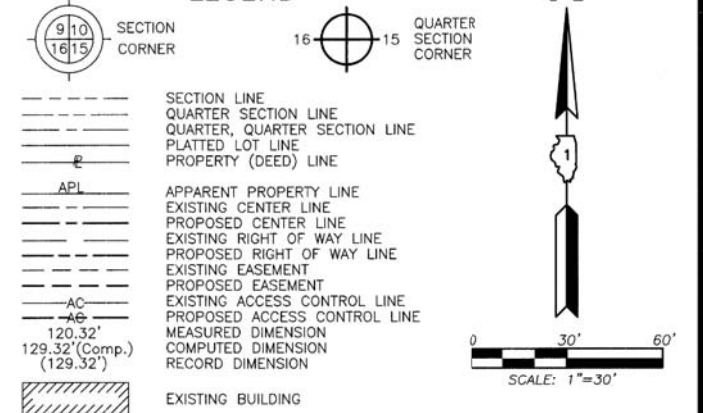
PROJECT LENGTH = 1,136.02 LIN. FT. = 0.215 MILE, HARGER ROAD
= 838.20 LIN. FT. = 0.159 MILE, YORK ROAD
TOTAL LENGTH = 1,974.22 LIN. FT. = 0.374 MILE

DISTRICT HEADQUARTERS: - * -
LOCATION OF SECTION INDICATED THUS: - **█** -

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

IDOT USE ONLY

LEGEND



- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.
- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 667101-02 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
 COUNTY OF LAKE }
 THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 24, TOWNSHIP 39N., RANGE 11E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.
 DATED AT LAKE VILLA, ILLINOIS THIS 23rd DAY OF September 2019 A.D.



Christian H. Jorgensen PRESIDENT
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2020
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
 All dimensions are measured unless otherwise specified.
 Areas shown on this plat are ground.
 All measured and computed distances are grid not ground.
 To obtain ground distances, divide grid distances shown by the combined factor of 0.999951240.

| PROJECT COORDINATES | | | |
|---|-------------|---------------|---------------|
| Illinois Coordinate System NAD 83(2011) East Zone | | | |
| STATION | OFFSET | NORTH | EAST |
| 28+23.18 | 389.91' Lt. | 1,891,000.629 | 1,093,795.975 |
| 29+30.45 | 427.98' Lt. | 1,891,092.002 | 1,093,748.268 |
| 29+59.04 | 316.92' Lt. | 1,891,130.982 | 1,093,855.415 |
| 29+64.21 | 426.82' Lt. | 1,891,121.899 | 1,093,745.791 |
| 30+19.40 | 314.17' Lt. | 1,891,186.234 | 1,093,850.838 |
| 124+49.04 | 51.66' Rt. | 1,890,638.050 | 1,093,754.877 |
| 124+91.38 | 48.29' Lt. | 1,890,744.917 | 1,093,735.834 |

JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 F.A.U. 1446 (HARGER ROAD)
 F.A.U. 2678A (YORK ROAD)
 LIMITS: HARGER RD.: I-88 WEST BOUND RAMP TO YORK RD.
 YORK RD.: HARGER ROAD TO ROOSEVELT ROAD
 COUNTY: DuPAGE
 SECTION: 17-00049-00-PV JOB NO.: R-55-001-97
 STATION 24+00 TO STATION 30+00
 STATION 123+00 TO STATION 130+86.02
 SCALE: 1"=30' SHEET 2 OF 4

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

See Sheet 3 for Total Holdings Parcel 0001T.E.-A & B

| PROJECT COORDINATES | | | |
|---|-------------|---------------|---------------|
| Illinois Coordinate System NAD 83(2011) East Zone | | | |
| STATION | OFFSET | NORTH | EAST |
| 124+91.41 | 78.29' Lt. | 1,890,770.076 | 1,093,719.493 |
| 125+46.08 | 48.20' Lt. | 1,890,774.331 | 1,093,781.120 |
| 125+46.70 | 78.20' Lt. | 1,890,799.490 | 1,093,764.780 |
| 125+58.95 | 47.82' Lt. | 1,890,780.322 | 1,093,790.345 |
| 127+22.58 | 67.61' Lt. | 1,890,895.444 | 1,093,860.870 |
| 127+23.53 | 74.57' Lt. | 1,890,899.101 | 1,093,854.901 |
| 127+41.33 | 72.69' Lt. | 1,890,910.998 | 1,093,862.190 |
| 127+42.88 | 138.86' Lt. | 1,890,936.836 | 1,093,801.260 |
| 127+50.52 | 64.10' Lt. | 1,890,915.660 | 1,093,873.254 |

| Parcel | Document No. | Date Recorded |
|--------|--------------|----------------|
| 0001 | 57-1061 | *1957 |
| 0001 | R65-8736 | March 22, 1965 |

* Date Filed

| Proposed Pavement Bicycle Path Curve #8 | | Proposed Pavement Bicycle Path Curve #9 | |
|---|-----------------------|---|-----------------------|
| P.I. = Sta. 211+95.25 | Δ = 44°33'04" | P.I. = Sta. 212+94.57 | Δ = 43°17'08" |
| R = 75.00' | T = 30.72' | R = 75.00' | T = 71.72' |
| L = 58.32' | E = 6.05' | L = 114.46' | E = 28.77' |
| P.C. = Sta. 211+64.53 | P.T. = Sta. 212+22.85 | P.C. = Sta. 212+22.85 | P.T. = Sta. 213+37.31 |

| Existing & Proposed Pavement York Road Curve #3 | | Existing & Proposed Pavement Harger Road Curve #4 | | Existing & Proposed Pavement Harger Road Curve #5 | |
|---|----------------------|---|-----------------------|---|-----------------------|
| P.I. = Sta. 30+65.12 | Δ = 124°00'00" | P.I. = Sta. 126+47.99 | Δ = 35°50'18" | P.I. = Sta. 128+78.63 | Δ = 43°17'08" |
| R = 3819.72' | T = 429.01' | R = 330.00' | T = 106.71' | R = 330.00' | T = 130.94' |
| L = 854.45' | E = 24.02' | L = 206.41' | E = 16.82' | L = 249.31' | E = 25.03' |
| P.C. = Sta. 26+36.11 | P.T. = Sta. 34+90.56 | P.C. = Sta. 125+41.28 | P.T. = Sta. 127+47.69 | P.C. = Sta. 127+47.69 | P.T. = Sta. 129+97.00 |

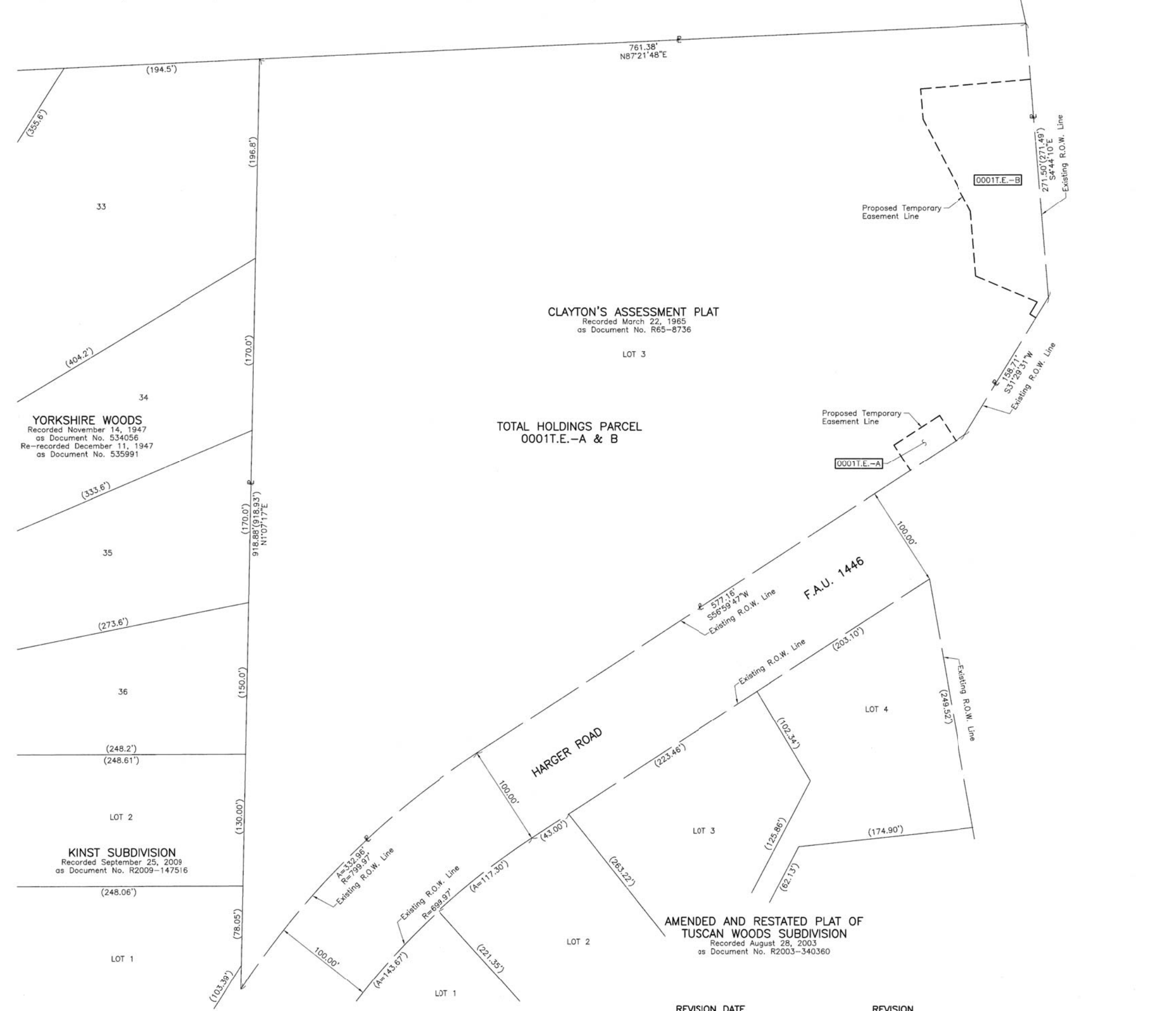
| PARCEL NUMBER | TOTAL HOLDINGS ACRES | PART TAKEN ACRES | AREA IN EXISTING R.O.W. ACRES | REMAINDER AREA ACRES | EASEMENT AREA ACRES | EASEMENT PURPOSE | PERMANENT INDEX NUMBER |
|--------------------------|----------------------|------------------|-------------------------------|----------------------|------------------------------|--|------------------------|
| 0001T.E.-A 0001T.E.-B | 10.732 | N/A | N/A | 10.732 | T.E.-A=0.037 T.E.-B=0.406 | Driveway Construction Construction Purposes | 06-24-100-006 |

REVISION DATE REVISION

CLAYTON'S ASSESSMENT PLAT
 Recorded March 22, 1965
 as Document No. R65-8736

AMENDED AND RESTATED PLAT OF
 TUSCAN WOODS SUBDIVISION
 Recorded August 28, 2003
 as Document No. R2003-340360

PART OF THE NW 1/4 OF SEC. 24, TWP. 39 N., R. 11 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.



F.A.U. 2678A

YORK ROAD

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL
- APPARENT PROPERTY LINE
- EXISTING CENTER LINE
- PROPOSED CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- AC
- AE
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DIMENSION
- EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

- IRON PIPE OR ROD FOUND
- CUT CROSS FOUND OR SET
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T STD 667101-02 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
 COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 24, TOWNSHIP 39N., RANGE 11E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 23rd DAY OF September 2019 A.D.



Christian H. Jorgensen PRESIDENT
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2020
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

All dimensions are measured unless otherwise specified.
 Areas shown on this plat are ground.
 All measured and computed distances are grid not ground.
 To obtain ground distances, divide grid distances shown by the combined factor of 0.9999611240.

JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371

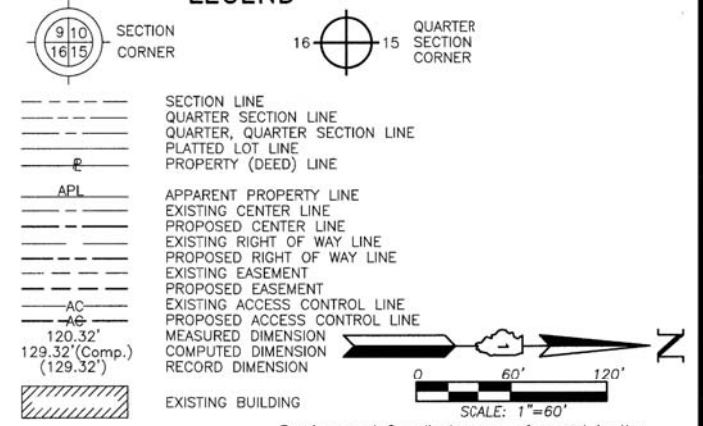
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 F.A.U. 1446 (HARGER ROAD)
 F.A.U. 2678A (YORK ROAD)
 LIMITS: HARGER RD.: I-88 WEST BOUND RAMP TO YORK RD.
 YORK RD.: HARGER ROAD TO ROOSEVELT ROAD
 COUNTY: DuPAGE
 SECTION: 17-00049-00-PV JOB NO.: R-55-001-97
 STATION NONE TO STATION
 SCALE: 1"=50' SHEET 3 OF 4

DOT USE ONLY

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

REVISION DATE REVISION

LEGEND



Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

○ IRON PIPE OR ROD FOUND ⊕ "MAG" NAIL SET
 + CUT CROSS FOUND OR SET ● 5/8" REBAR SET

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● PERMANENT SURVEY MARKER, I.D.O.T STD 667101-02 (TO BE SET BY OTHERS)
 □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }
 COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 24, TOWNSHIP 39N., RANGE 11E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 23rd DAY OF September 20, 2019 A.D.

Christian H. Jorgensen PRESIDENT
 2797 PROFESSIONAL LAND SURVEYOR NO. 35-2797
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2020
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

All dimensions are measured unless otherwise specified. Areas shown on this plat are ground. All measured and computed distances are grid not ground. To obtain ground distances, divide grid distances shown by the combined factor of 0.9999611240.



JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371

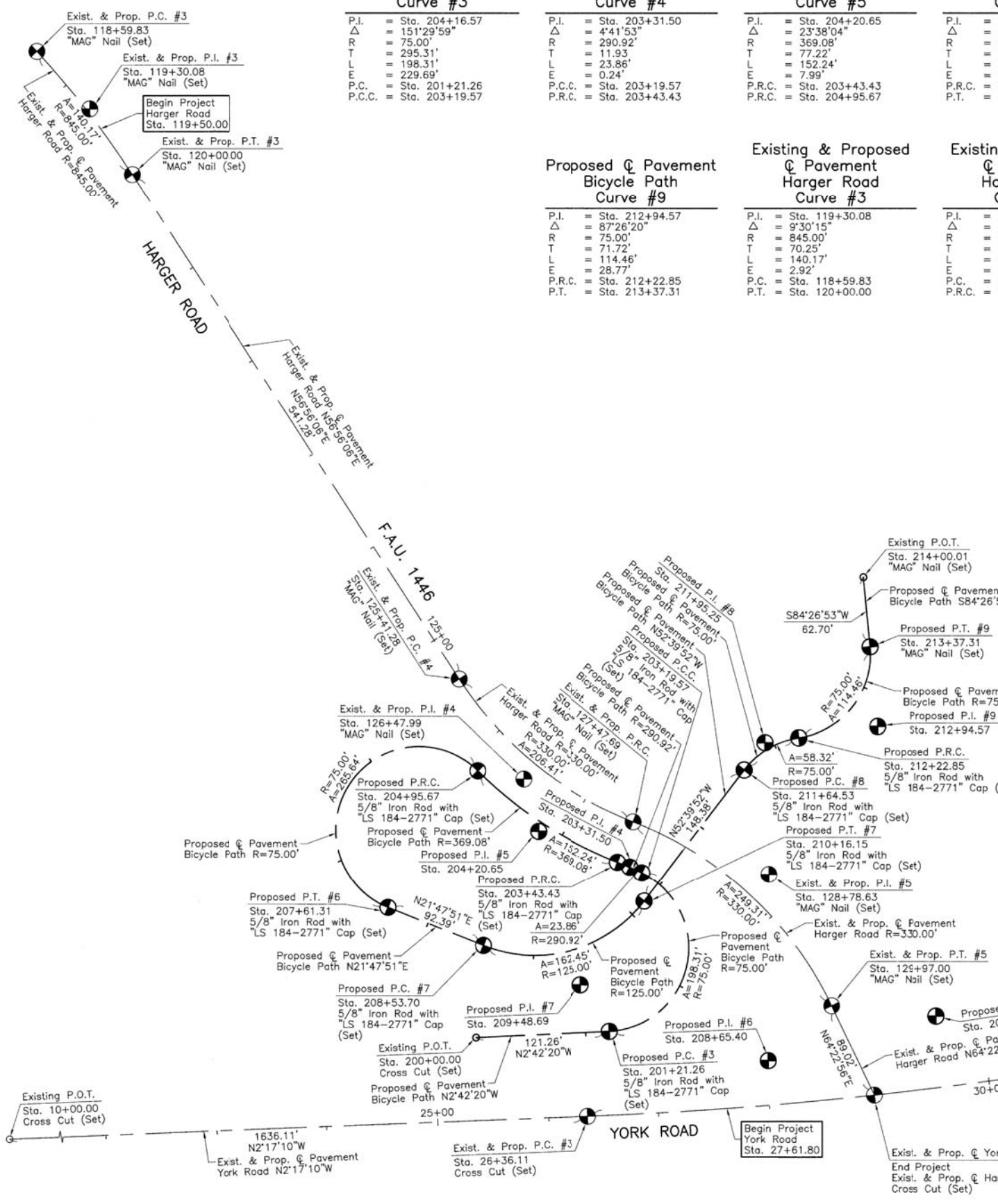
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 F.A.U. 1446 (HARGER ROAD)
 F.A.U. 2678A (YORK ROAD)
 LIMITS: HARGER RD.: I-88 WEST BOUND RAMP TO YORK RD.
 YORK RD.: HARGER ROAD TO ROOSEVELT ROAD
 COUNTY: DuPAGE
 SECTION: 17-00049-00-PV JOB NO.: R-55-001-97
 STATION 10+00.00 TO STATION 40+00.00
 STATION 118+59.83 TO STATION 130+86.02
 STATION 200+00.00 TO STATION 214+00.01
 SCALE: 1"=60' SHEET 4 OF 4

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

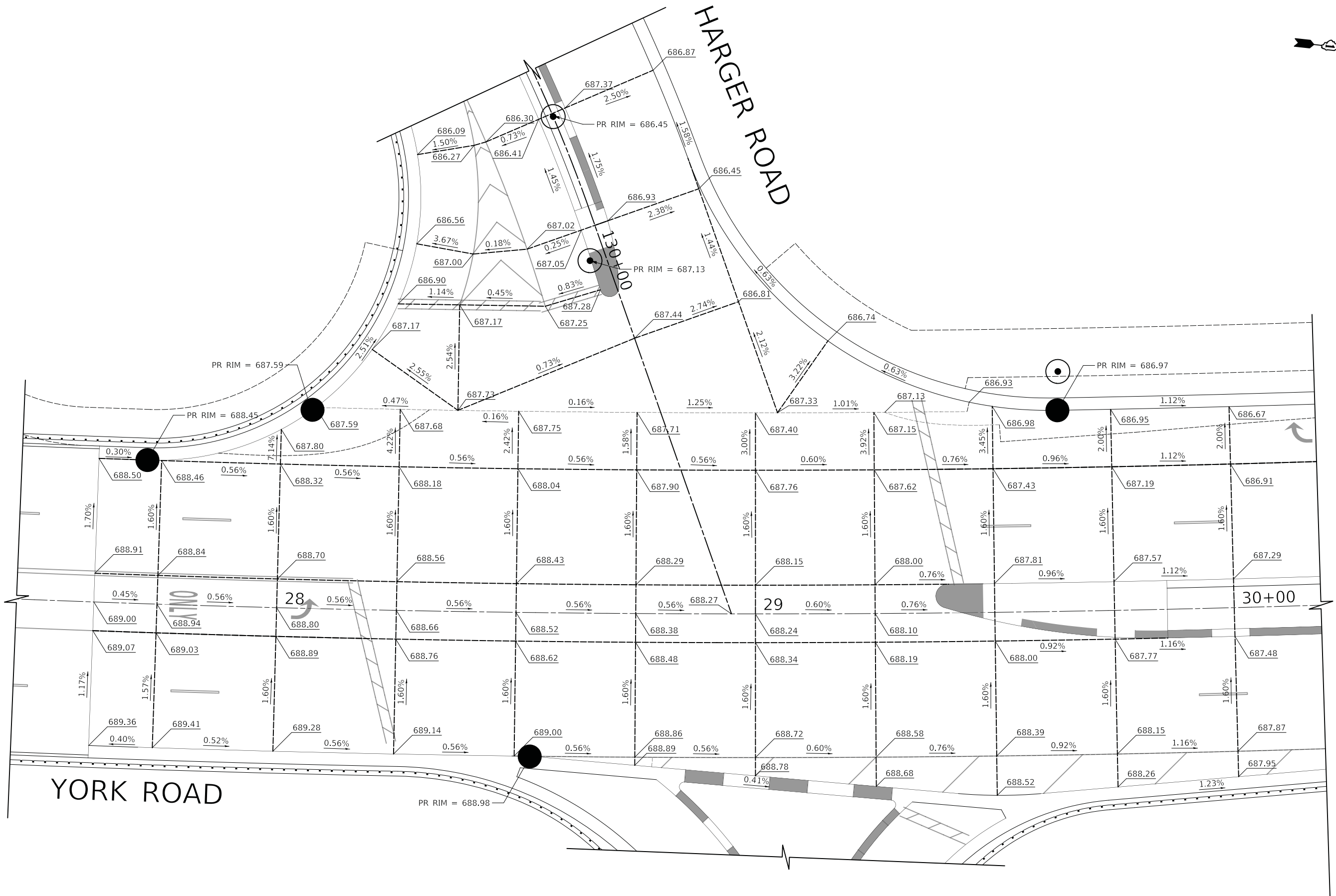
| Proposed Pavement Bicycle Path Curve #3 | Proposed Pavement Bicycle Path Curve #4 | Proposed Pavement Bicycle Path Curve #5 | Proposed Pavement Bicycle Path Curve #6 | Proposed Pavement Bicycle Path Curve #7 | Proposed Pavement Bicycle Path Curve #8 |
|--|---|---|--|--|---|
| P.I. = Sta. 204+16.57 Δ = 151°29'59" R = 75.00' T = 295.31' L = 198.31' E = 229.69' P.C. = Sta. 201+21.26 P.C.C. = Sta. 203+19.57 | P.I. = Sta. 203+31.50 Δ = 4°41'53" R = 290.92' T = 11.93' L = 23.86' E = 0.24' P.C.C. = Sta. 203+19.57 P.R.C. = Sta. 203+43.43 | P.I. = Sta. 204+20.65 Δ = 23°38'04" R = 369.08' T = 77.22' L = 152.24' E = 7.99' P.R.C. = Sta. 203+43.43 P.R.C. = Sta. 204+95.67 | P.I. = Sta. 208+65.40 Δ = 202°56'01" R = 75.00' T = -369.73' L = 265.64' E = -452.26' P.R.C. = Sta. 204+95.67 P.T. = Sta. 207+61.31 | P.I. = Sta. 209+48.69 Δ = 74°27'43" R = 125.00' T = 94.99' L = 182.45' E = 32.00' P.C. = Sta. 208+53.70 P.T. = Sta. 210+16.15 | P.I. = Sta. 211+95.25 Δ = 44°33'04" R = 75.00' T = 30.72' L = 58.32' E = 6.05' P.C. = Sta. 211+64.53 P.R.C. = Sta. 212+22.85 |

| Proposed Pavement Bicycle Path Curve #9 | Existing & Proposed Pavement Harger Road Curve #3 | Existing & Proposed Pavement Harger Road Curve #4 | Existing & Proposed Pavement Harger Road Curve #5 | Existing & Proposed Pavement York Road Curve #3 |
|---|--|---|---|--|
| P.I. = Sta. 212+94.57 Δ = 57°26'20" R = 75.00' T = 71.72' L = 114.46' E = 28.77' P.R.C. = Sta. 212+22.85 P.T. = Sta. 213+37.31 | P.I. = Sta. 119+30.08 Δ = 845.00' R = 70.25' L = 140.17' E = 2.92' P.C. = Sta. 118+59.83 P.T. = Sta. 120+00.00 | P.I. = Sta. 126+47.99 Δ = 35°50'18" R = 330.00' T = 106.71' L = 206.41' E = 16.82' P.C. = Sta. 125+41.28 P.R.C. = Sta. 127+47.69 | P.I. = Sta. 128+78.63 Δ = 43°17'08" R = 330.00' T = 130.94' L = 249.31' E = 25.03' P.R.C. = Sta. 127+47.69 P.T. = Sta. 129+97.00 | P.I. = Sta. 304+65.12 Δ = 12°49'00" R = 3819.72' T = 429.01' L = 854.45' E = 24.02' P.C. = Sta. 26+36.11 P.T. = Sta. 34+90.56 |

| PROJECT COORDINATES Illinois Coordinate System NAD 83(2011) East Zone | | |
|--|---------------|---------------|
| STATION | NORTH | EAST |
| 10+00.00 | 1,889,213.769 | 1,094,261.645 |
| 26+36.11 | 1,890,848.580 | 1,094,196.382 |
| 27+61.80 | 1,890,974.062 | 1,094,189.303 |
| 28+95.00 | 1,891,106.713 | 1,094,177.300 |
| 30+65.12 | 1,891,277.250 | 1,094,179.269 |
| 34+90.56 | 1,891,691.444 | 1,094,067.490 |
| 36+00.00 | 1,891,797.108 | 1,094,038.974 |
| 40+00.00 | 1,892,183.292 | 1,093,934.754 |
| 118+59.83 | 1,890,350.510 | 1,093,239.776 |
| 119+30.08 | 1,890,398.030 | 1,093,291.508 |
| 119+50.00 | 1,890,407.852 | 1,093,309.306 |
| 120+00.00 | 1,890,436.354 | 1,093,350.377 |
| 125+41.28 | 1,890,731.670 | 1,093,803.996 |
| 126+47.99 | 1,890,789.889 | 1,093,893.424 |
| 127+47.69 | 1,890,889.446 | 1,093,931.833 |
| 128+78.63 | 1,891,011.610 | 1,093,978.964 |
| 129+97.00 | 1,891,068.225 | 1,094,097.034 |
| 130+86.02 | 1,891,106.713 | 1,094,177.300 |



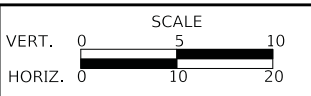
REVISION DATE REVISION



CIVILTECH
 Two Pierce Place, Suite 1400
 Itasca, Illinois 60143
 Tel: 630.773.3900 Fax: 630.773.3975
 www.civiltechinc.com

| | |
|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

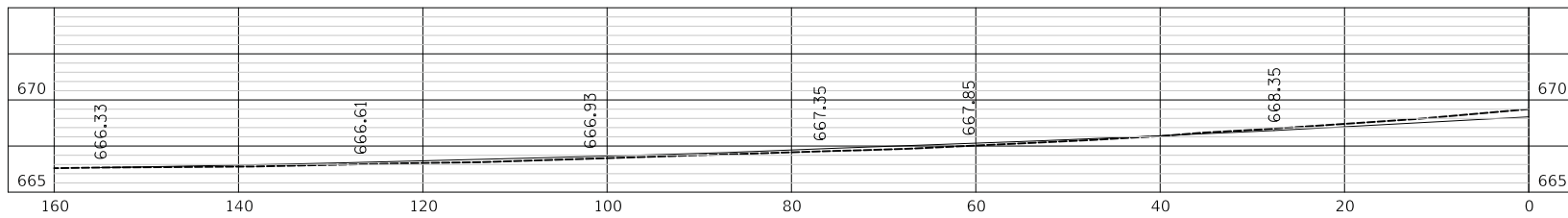
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



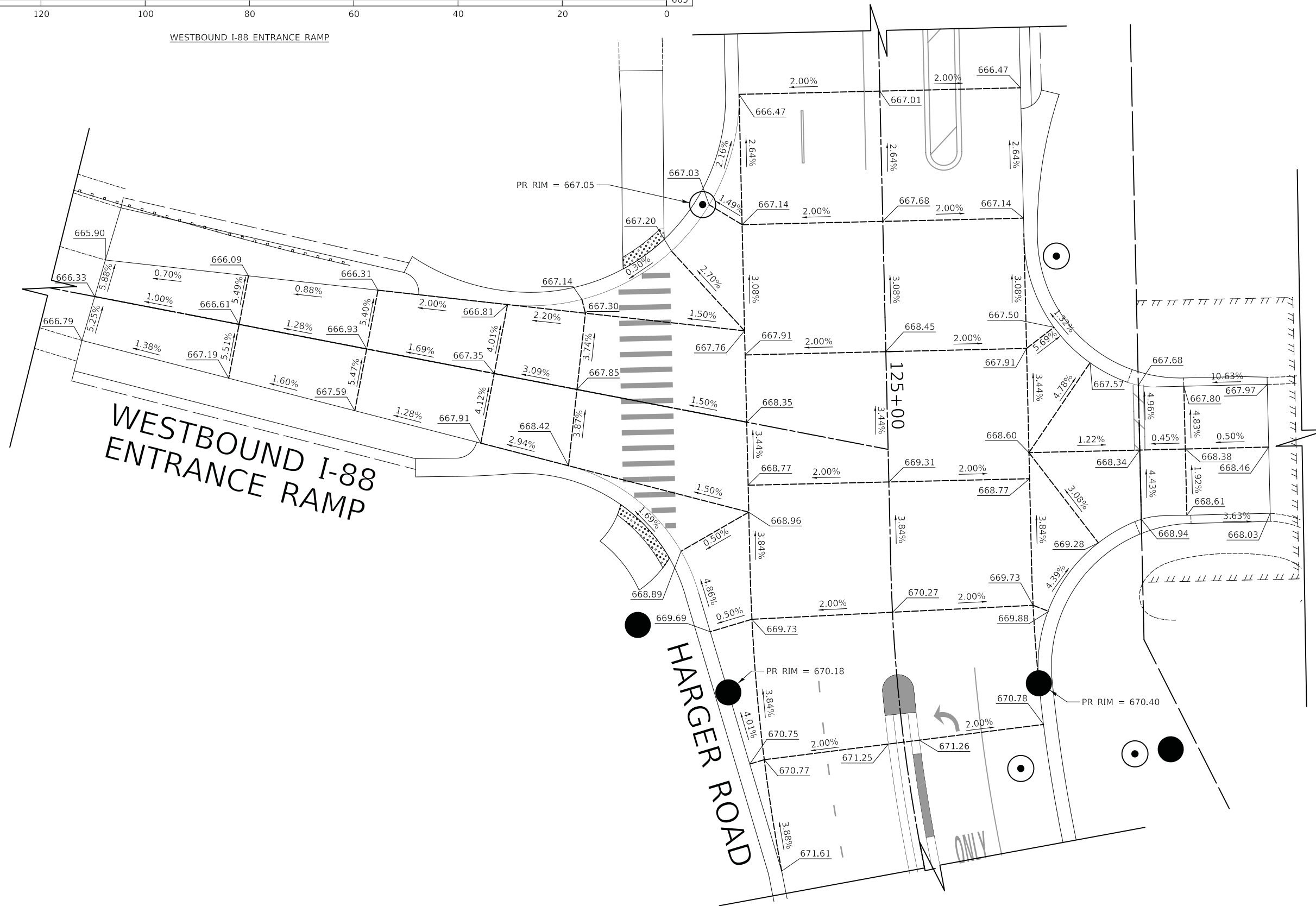
**INTERSECTION GRADING PLAN
 YORK ROAD & HARGER ROAD**

SHEET 1 OF 5 SHEETS STA. TO STA.

| | | | | |
|--------------------------------|------------------------|---------------|--------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 86 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 61G16 | |
| FED. AID PROJECT | | | | |



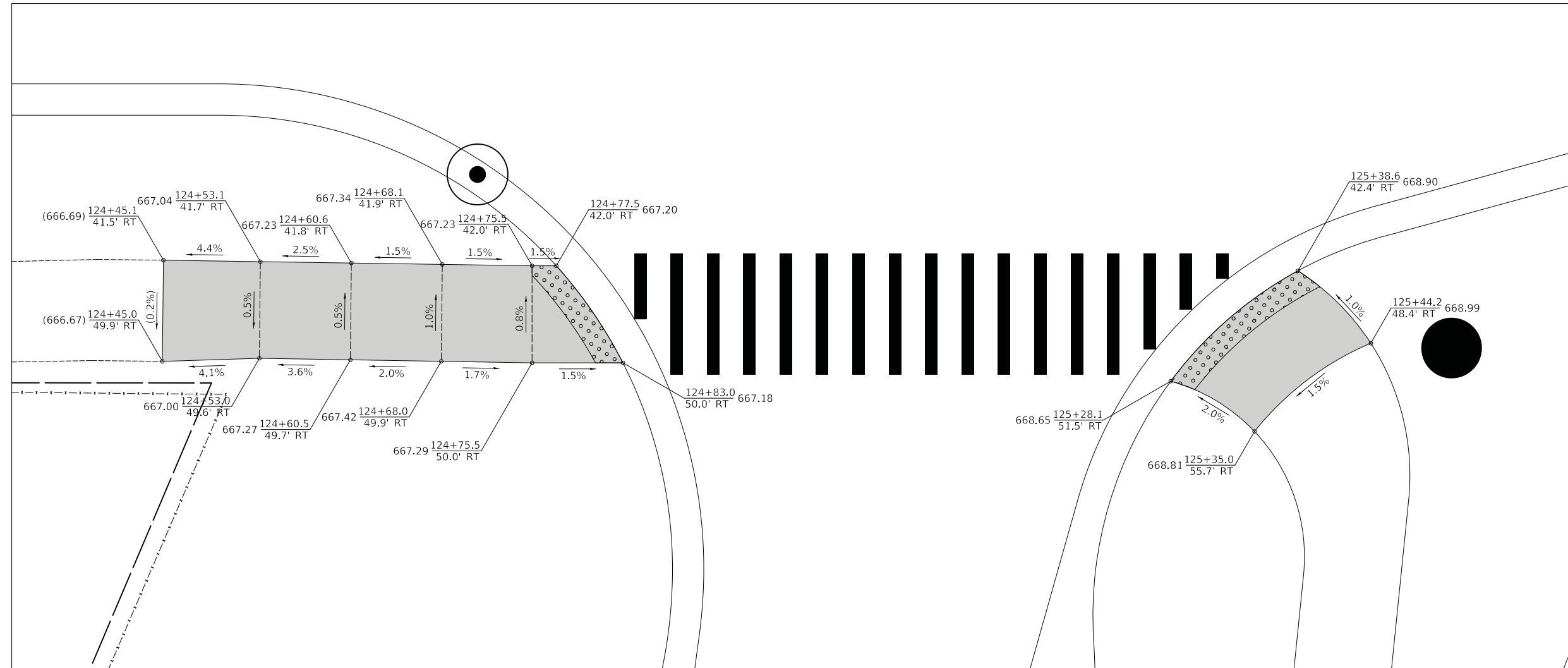
WESTBOUND I-88 ENTRANCE RAMP



| | |
|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |



| | | | | |
|---|------------------------|---------------|------------------|--------------|
| F.A.U. RTE. 2678/1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 88 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



HARGER ROAD AND WESTBOUND I-88 ENTRANCE RAMP INTERSECTION

| | |
|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

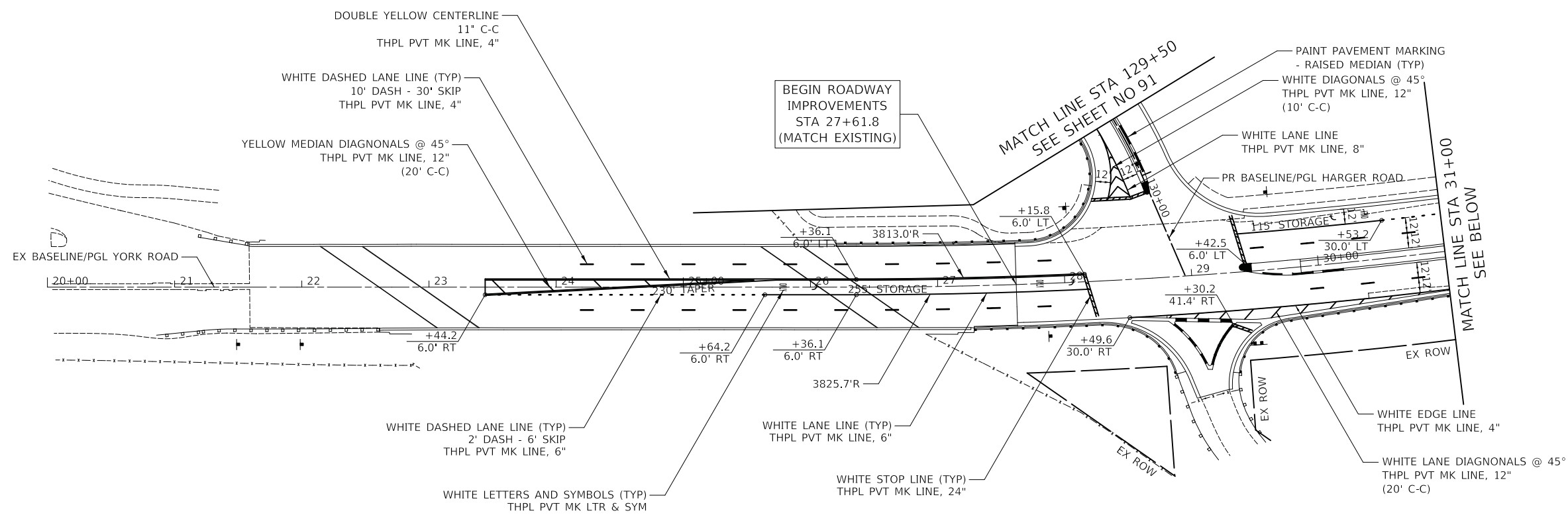
SCALE

| | | | |
|--------|---|---|----|
| VERT. | 0 | 5 | 10 |
| HORIZ. | 0 | 5 | 10 |

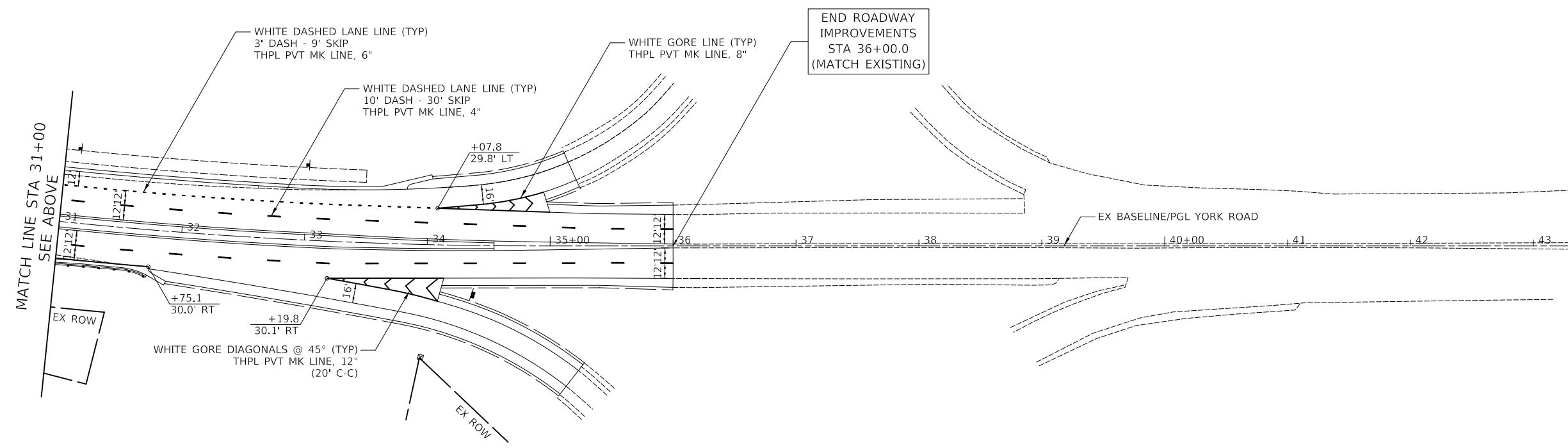
SIDEWALK RAMP DETAILS

SHEET 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------|-----------|
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 89 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |



YORK ROAD

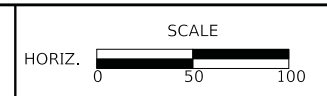


YORK ROAD

CIVILTECH
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| | |
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| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

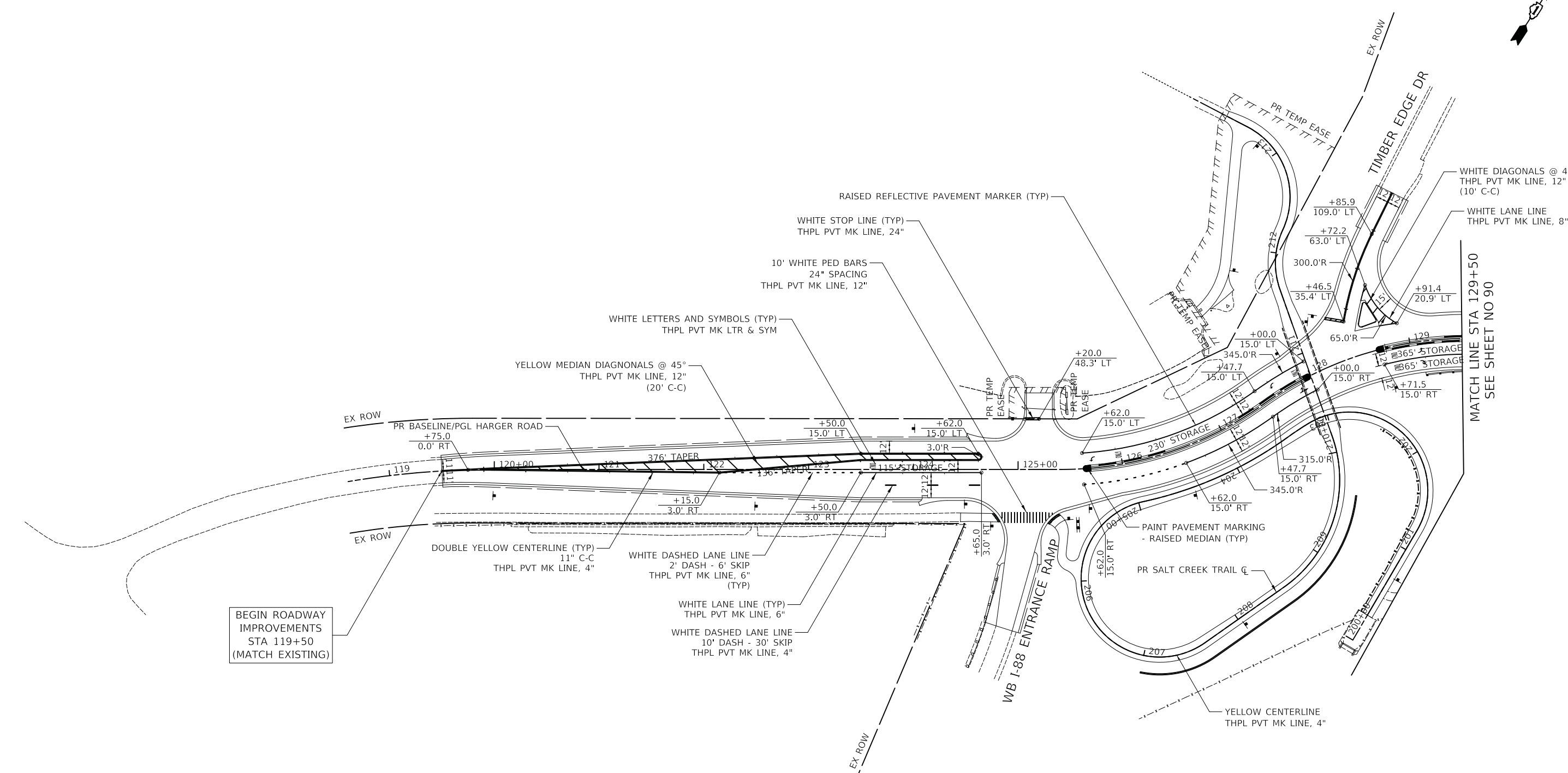
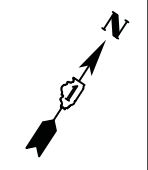
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



PAVEMENT MARKING PLAN

SHEET 1 OF 2 SHEETS STA. TO STA.

| | | | | |
|---|---------------------------|------------------|---------------------|-----------------|
| F.A.U. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 90 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



BEGIN ROADWAY IMPROVEMENTS STA 119+50 (MATCH EXISTING)

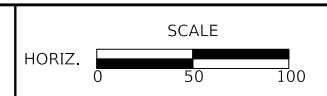
MATCH LINE STA 129+50 SEE SHEET NO 90

HARGER ROAD

CIVILTECH
 Two Pierce Place, Suite 1400
 Itasca, Illinois 60143
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 www.civiltechinc.com

| | |
|-------------------|-----------|
| DESIGNED - PK | REVISED - |
| DRAWN - MC | REVISED - |
| CHECKED - JRV | REVISED - |
| DATE - 12/27/2019 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



PAVEMENT MARKING PLAN

SHEET 2 OF 2 SHEETS STA. TO STA.

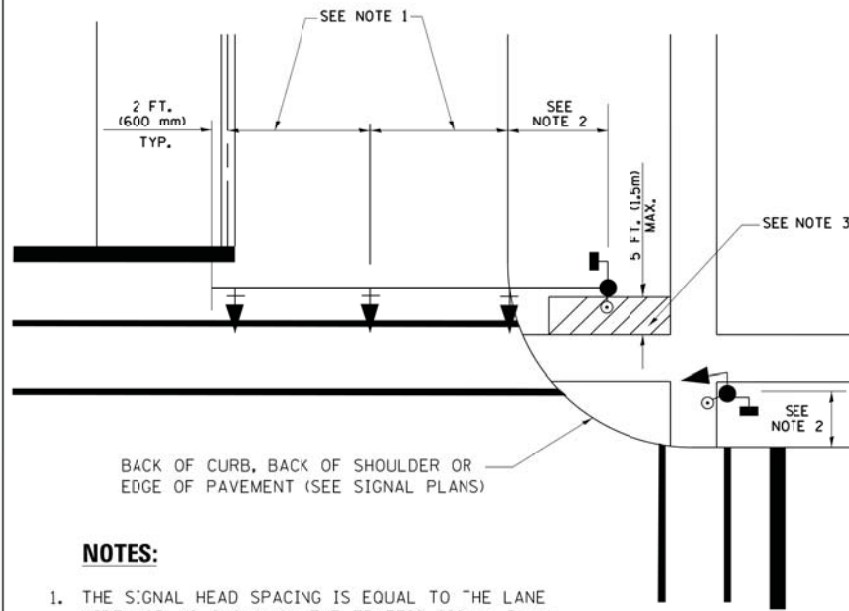
| | | | | |
|---|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2678 1446 | 17-00049-00-PV | DUPAGE | 208 | 91 |
| CONTRACT NO. 61G16 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

| ITEM | EXISTING | PROPOSED | ITEM | EXISTING | PROPOSED | ITEM | EXISTING | PROPOSED |
|---|----------|----------|--|----------|----------|---|----------|----------|
| CONTROLLER CABINET | | | HANDHOLE -SQUARE -ROUND | | | SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD | | |
| COMMUNICATION CABINET | | | HEAVY DUTY HANDHOLE -SQUARE -ROUND | | | SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE | | |
| MASTER CONTROLLER | | | DOUBLE HANDHOLE | | | PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS | | |
| MASTER MASTER CONTROLLER | | | JUNCTION BOX | | | PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER | | |
| UNINTERRUPTABLE POWER SUPPLY | | | RAILROAD CANTILEVER MAST ARM | | | ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN" | | |
| SERVICE INSTALLATION -(P) POLE MOUNTED | | | RAILROAD FLASHING SIGNAL | | | NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED | | |
| SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED | | | RAILROAD CROSSING GATE | | | GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN) | | |
| TELEPHONE CONNECTION | | | RAILROAD CROSSBUCK | | | ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C | | |
| STEEL MAST ARM ASSEMBLY AND POLE | | | RAILROAD CONTROLLER CABINET | | | COAXIAL CABLE | | |
| ALUMINUM MAST ARM ASSEMBLY AND POLE | | | UNDERGROUND CONDUIT (UC), GALVANIZED STEEL | | | VENDOR CABLE | | |
| STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE | | | TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE | | | COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED | | |
| SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY | | | SYSTEM ITEM | | | FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F | | |
| WOOD POLE | | | INTERSECTION ITEM | | | GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE | | |
| GUY WIRE | | | REMOVE ITEM | | | | | |
| SIGNAL HEAD | | | RELOCATE ITEM | | | | | |
| SIGNAL HEAD WITH BACKPLATE | | | ABANDON ITEM | | | | | |
| SIGNAL HEAD OPTICALLY PROGRAMMED | | | CONTROLLER CABINET AND FOUNDATION TO BE REMOVED | | | | | |
| FLASHER INSTALLATION -(FS) SOLAR POWERED | | | MAST ARM POLE AND FOUNDATION TO BE REMOVED | | | | | |
| PEDESTRIAN SIGNAL HEAD | | | SIGNAL POST AND FOUNDATION TO BE REMOVED | | | | | |
| PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON | | | DETECTOR LOOP, TYPE I | | | | | |
| RADAR DETECTION SENSOR | | | PREFORMED DETECTOR LOOP | | | | | |
| VIDEO DETECTION CAMERA | | | SAMPLING (SYSTEM) DETECTOR | | | | | |
| RADAR/VIDEO DETECTION ZONE | | | INTERSECTION AND SAMPLING (SYSTEM) DETECTOR | | | | | |
| PAN, TILT, ZOOM (PTZ) CAMERA | | | QUEUE AND SAMPLING (SYSTEM) DETECTOR | | | | | |
| EMERGENCY VEHICLE LIGHT DETECTOR | | | WIRELESS DETECTOR SENSOR | | | | | |
| CONFIRMATION BEACON | | | WIRELESS ACCESS POINT | | | | | |
| WIRELESS INTERCONNECT | | | | | | | | |
| WIRELESS INTERCONNECT RADIO REPEATER | | | | | | | | |

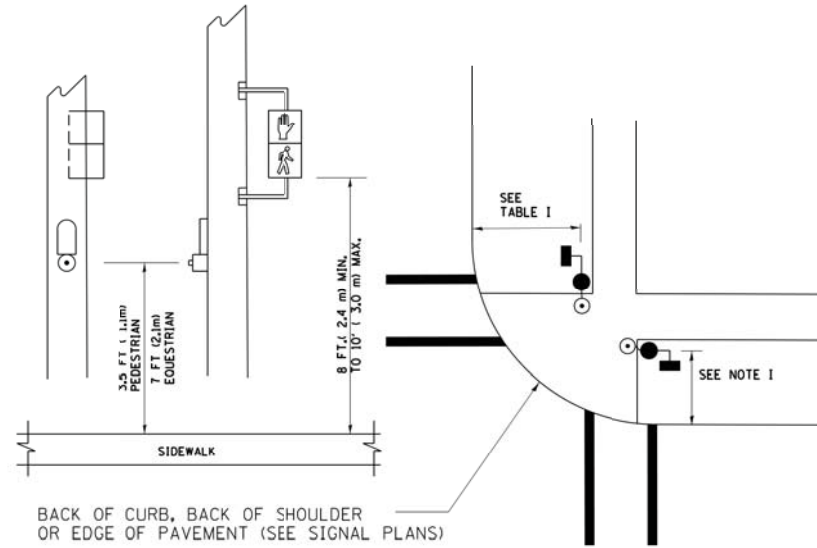
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

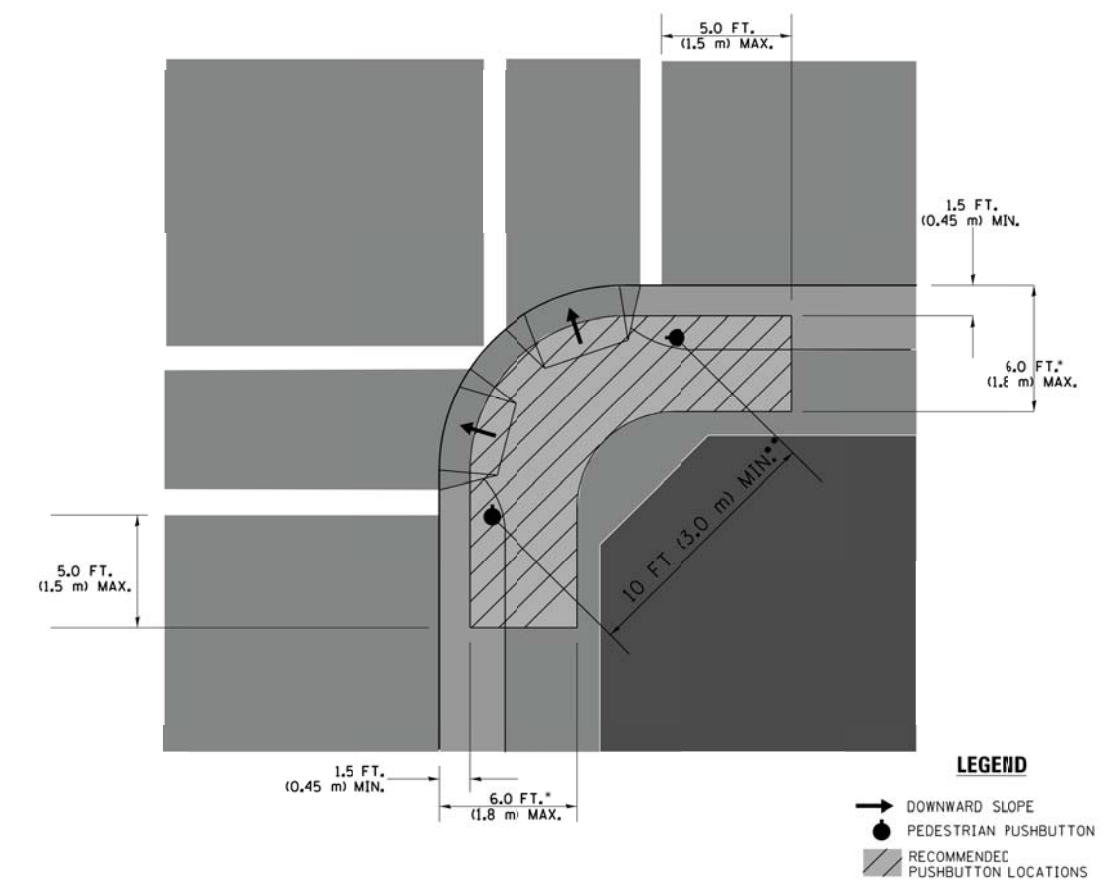
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

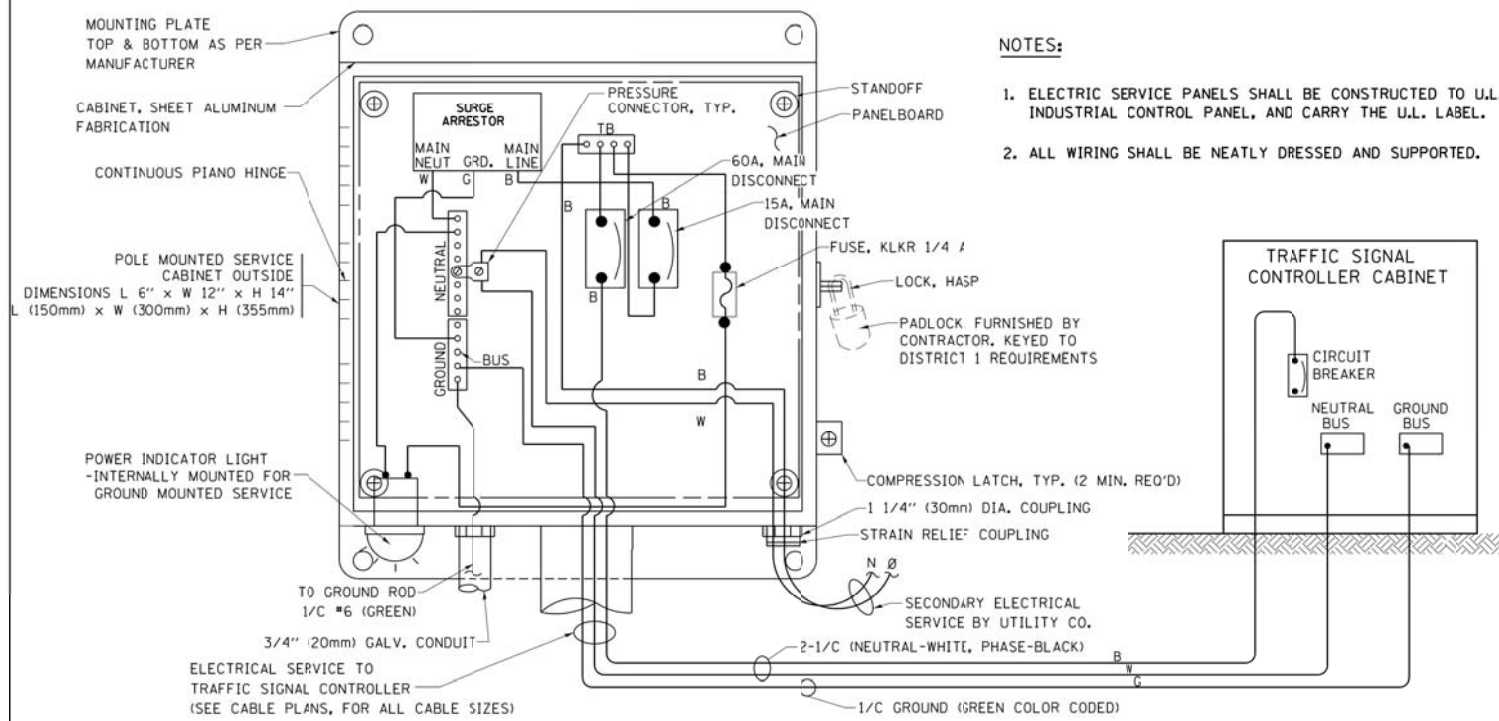
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

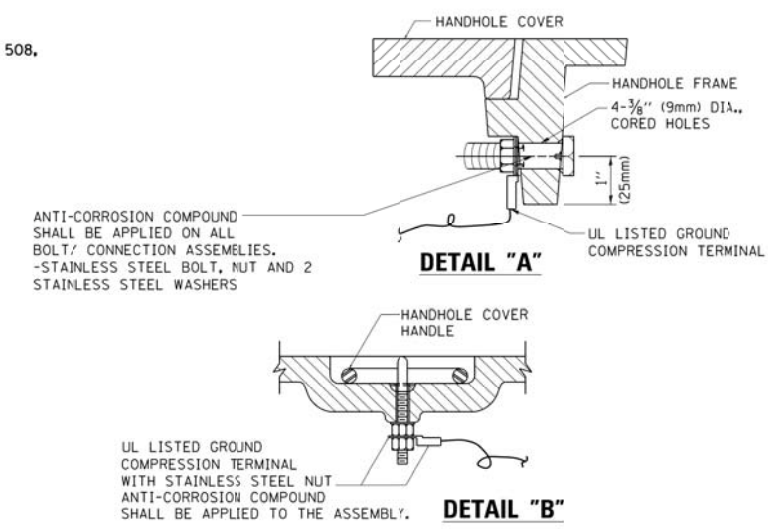
| TRAFFIC SIGNAL EQUIPMENT | COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION) | SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION) |
|------------------------------------|---|---|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TRAFFIC SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| PEDESTRIAN PUSHBUTTON POST | 4 FT (1.2m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| TEMPORARY WOOD POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m) |
| CONTROLLER CABINET | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |
| SERVICE INSTALLATION, GROUND MOUNT | 6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2 | SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3. |

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

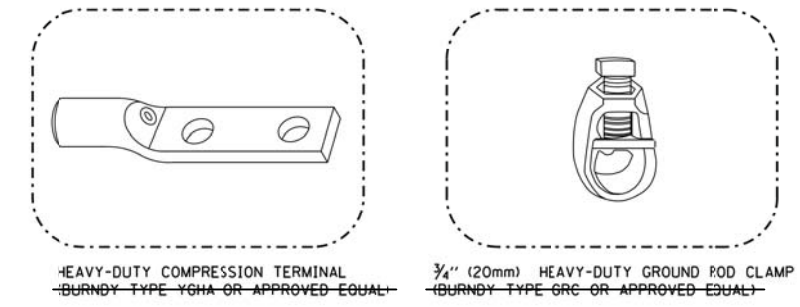
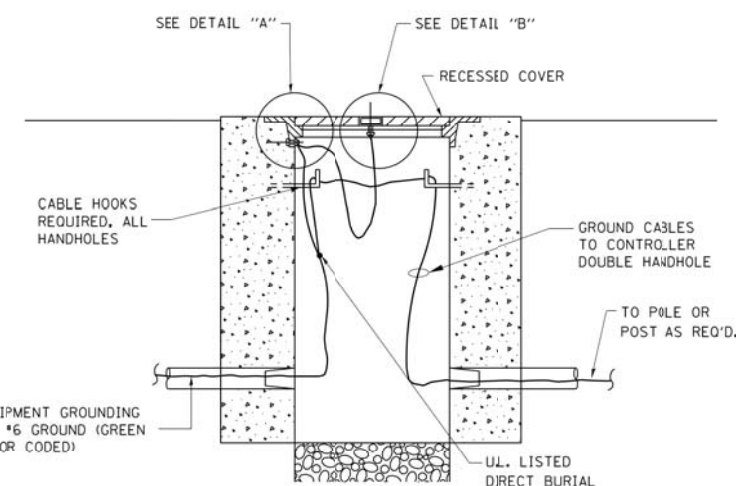


**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)**

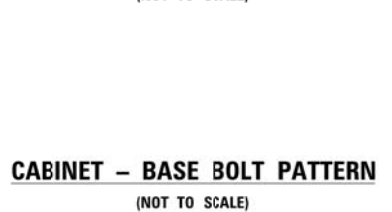
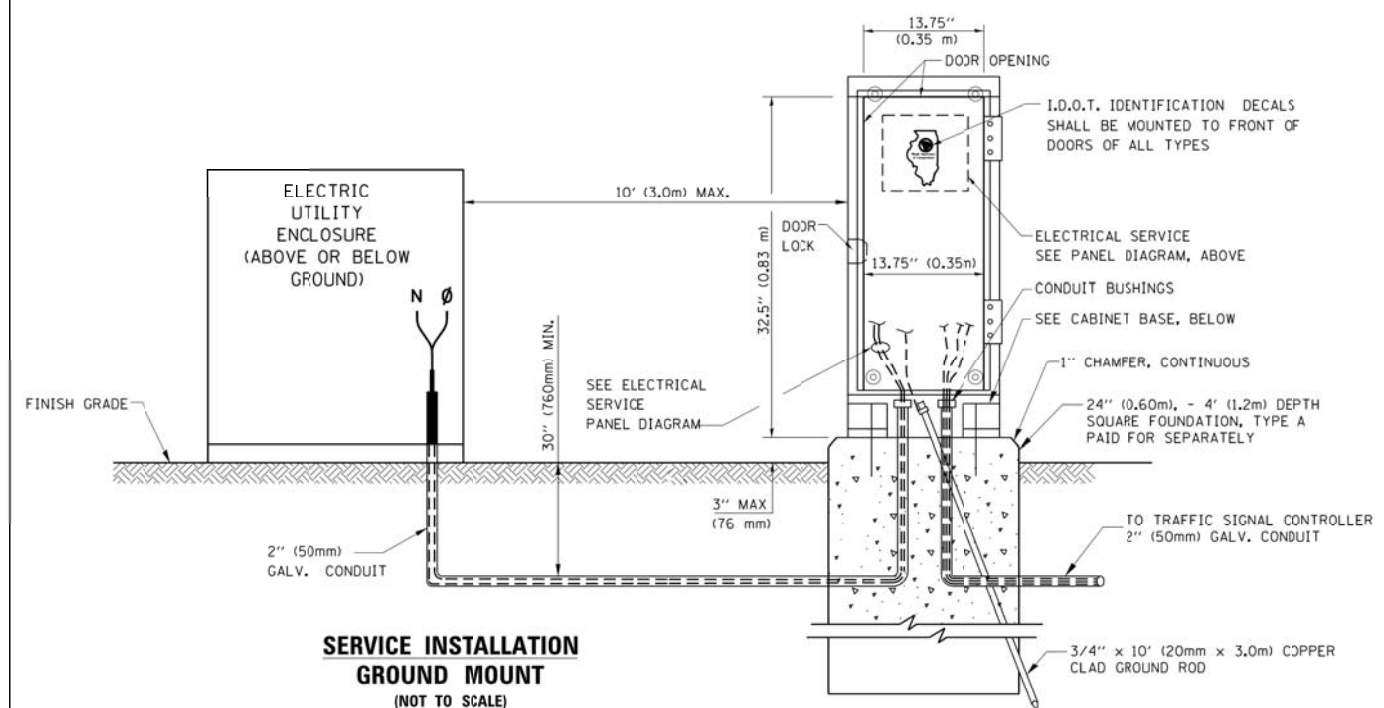
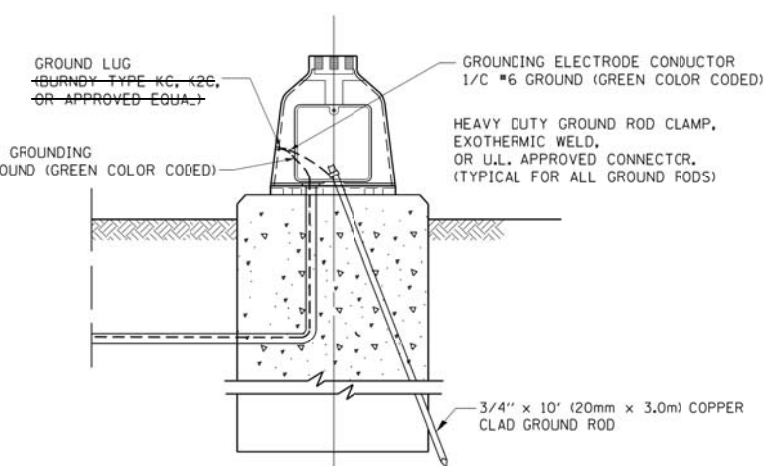
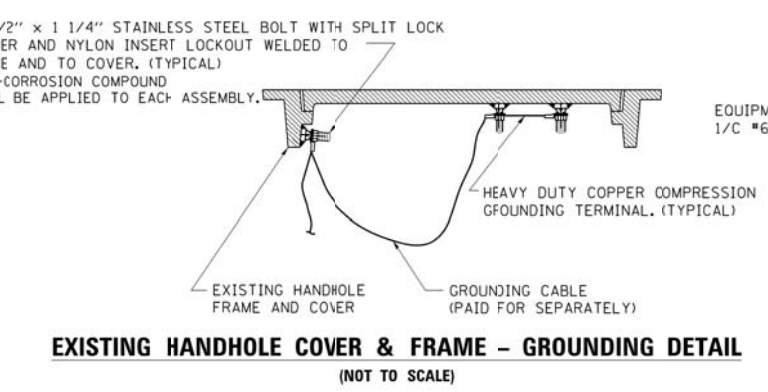


NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



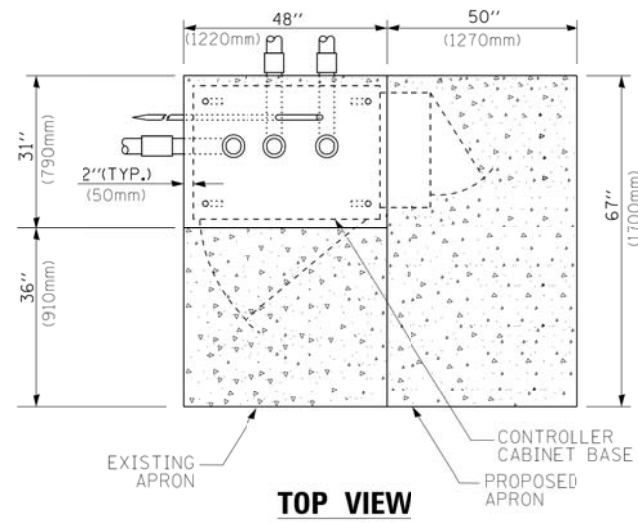
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| FILE NAME = | USER NAME = footemj | DESIGNED - DAD | REVISED - DAG 1-1-14 |
| ca:\pw\work\p1dot\footemj\d0108315\ts05.dgn | | DRAWN - BCK | REVISED - |
| | | CHECKED - DAD | REVISED - |
| | | DATE - 10-28-09 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

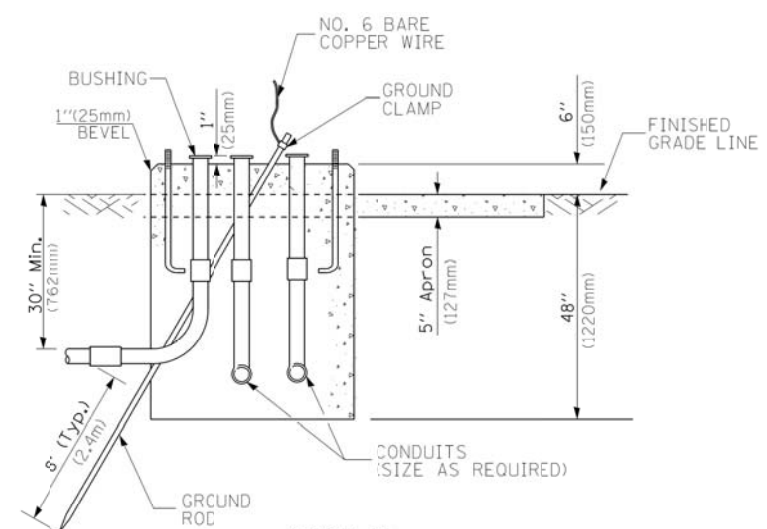
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 4 OF 7 SHEETS STA. TO STA.

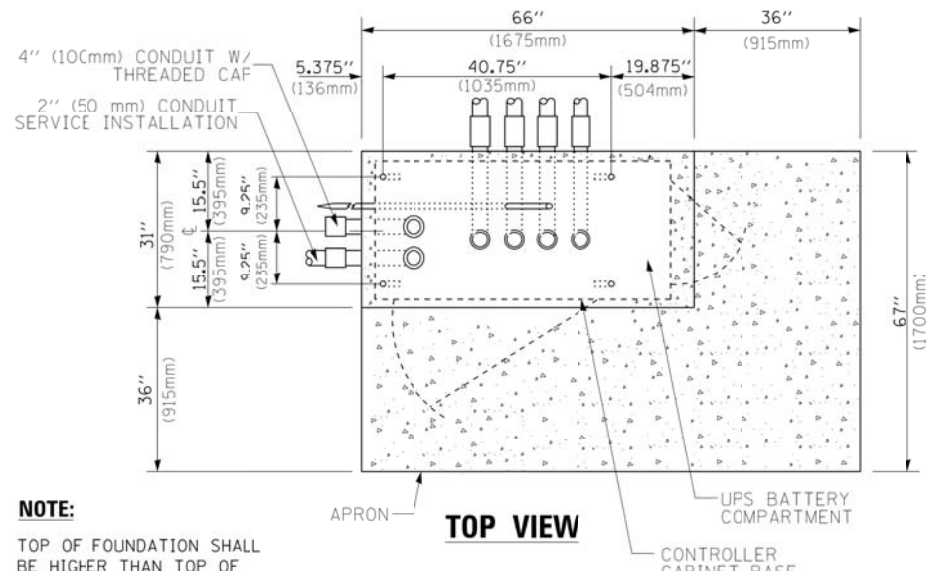
| | | | | |
|---|------------------------|---------------------------|------------------|--------------|
| F.A.J. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 98 |
| TS-05 | | CONTRACT NO. 61G16 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT | | | | |



TOP VIEW

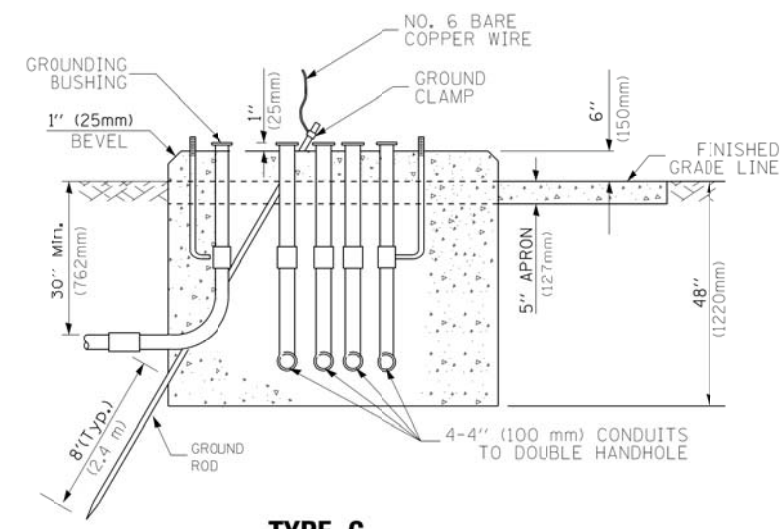


**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

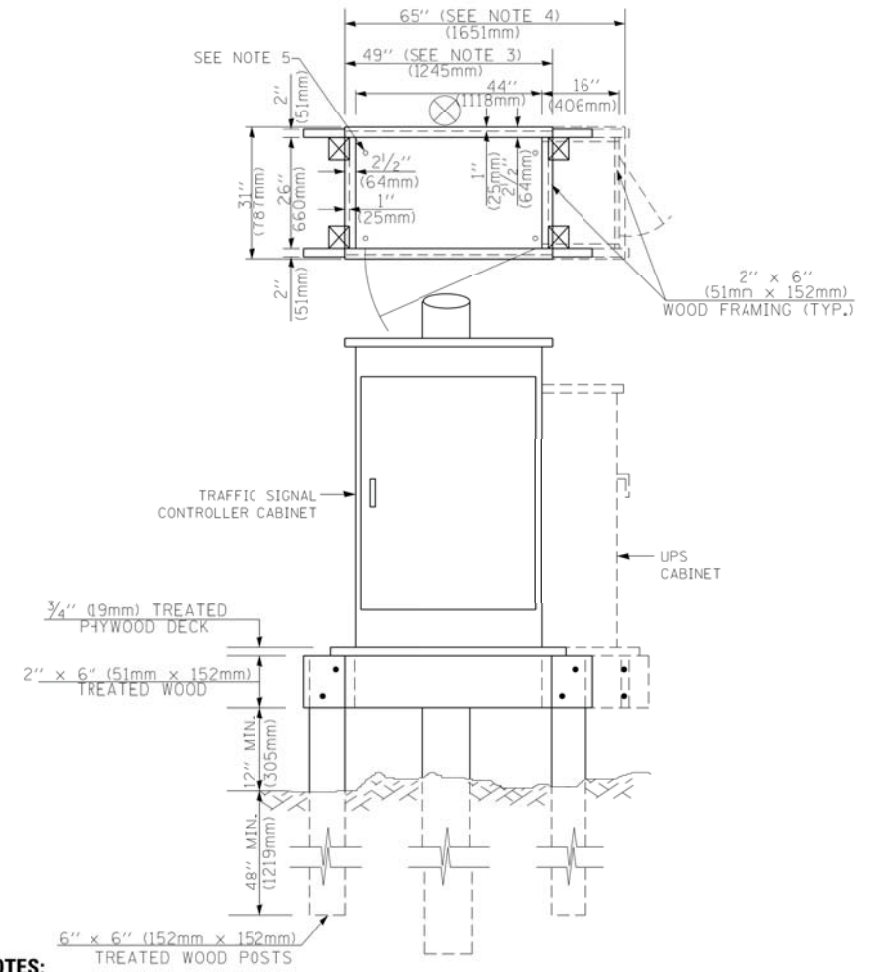


TOP VIEW

NOTE:
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

| CABLE SLACK LENGTH | FEET | METER |
|---|------|-------|
| HANDHOLE | 6.5 | 2.0 |
| DOUBLE HANDHOLE | 13.0 | 4.0 |
| SIGNAL POST | 2.0 | 0.6 |
| MAST ARM | 2.0 | 0.6 |
| CONTROLLER CABINET | 1.5 | 0.5 |
| FIBER OPTIC AT CABINET | 13.0 | 4.0 |
| ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION) | 1.5 | 0.5 |
| GROUND CABLE (SIGNAL POST, MAST ARM, CABINET) | 1.5 | 0.5 |
| GROUND CABLE (BETWEEN FRAME AND COVER) | 5.0 | 1.6 |

CABLE SLACK

| VERTICAL CABLE LENGTH | FEET | METER |
|---|--------|-------|
| MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM) | 20.0+L | 6.0+L |
| BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE) | 13.0 | 4.0 |
| PEDESTRIAN PUSH BUTTON | 6.0 | 2.0 |
| SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP | 13.5 | 4.1 |
| SERVICE INSTALLATION POLE MOUNT TO GROUND | 13.5 | 4.1 |
| SERVICE INSTALLATION GROUND MOUNT | 6.0 | 2.0 |
| FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT) | 3.0 | 1.0 |

VERTICAL CABLE LENGTH

| FOUNDATION | DEPTH |
|---|--------------|
| TYPE A - Signal Post | 4'-0" (1.2m) |
| TYPE C - CONTROLLER W/ UPS | 4'-0" (1.2m) |
| TYPE D - CONTROLLER | 4'-0" (1.2m) |
| SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE | 4'-0" (1.2m) |

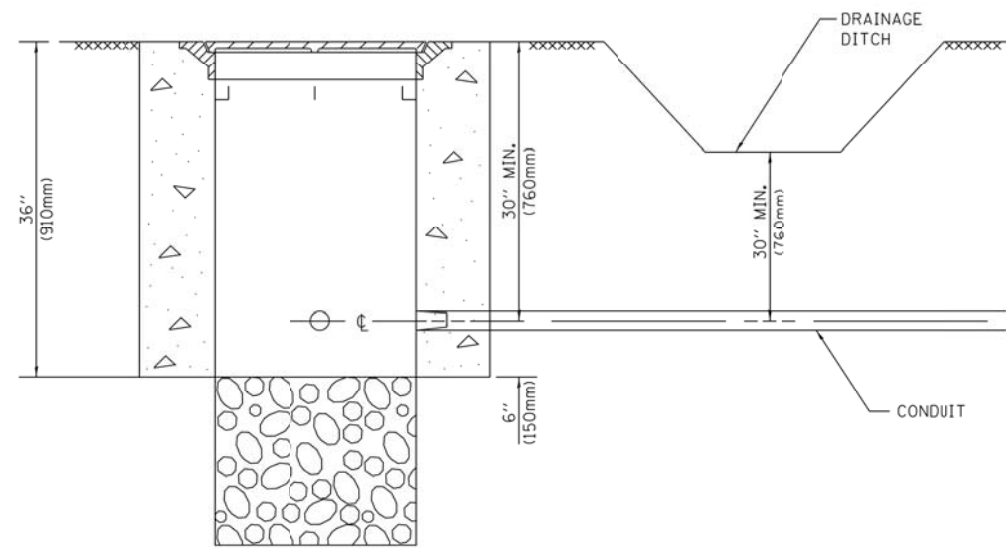
DEPTH OF FOUNDATION

| MAST ARM LENGTH | FOUNDATION DEPTH | FOUNDATION DIAMETER | SPIRAL DIAMETER | QUANTITY OF REBARS | SIZE OF REBARS |
|--|------------------|---------------------|-----------------|--------------------|----------------|
| Less than 30' (9.1 m) | 10'-0" (3.0 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m) | 13'-6" (4.1 m) | 30" (750mm) | 24" (600mm) | 8 | 6(19) |
| Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m) | 11'-0" (3.4 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m) | 13'-0" (4.0 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m) | 15'-0" (4.6 m) | 36" (900mm) | 30" (750mm) | 12 | 7(22) |
| Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m) | 21'-0" (6.4 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |
| Greater than or equal to 75' (22.9 m) | 25'-0" (7.6 m) | 42" (1060mm) | 36" (900mm) | 16 | 8(25) |

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

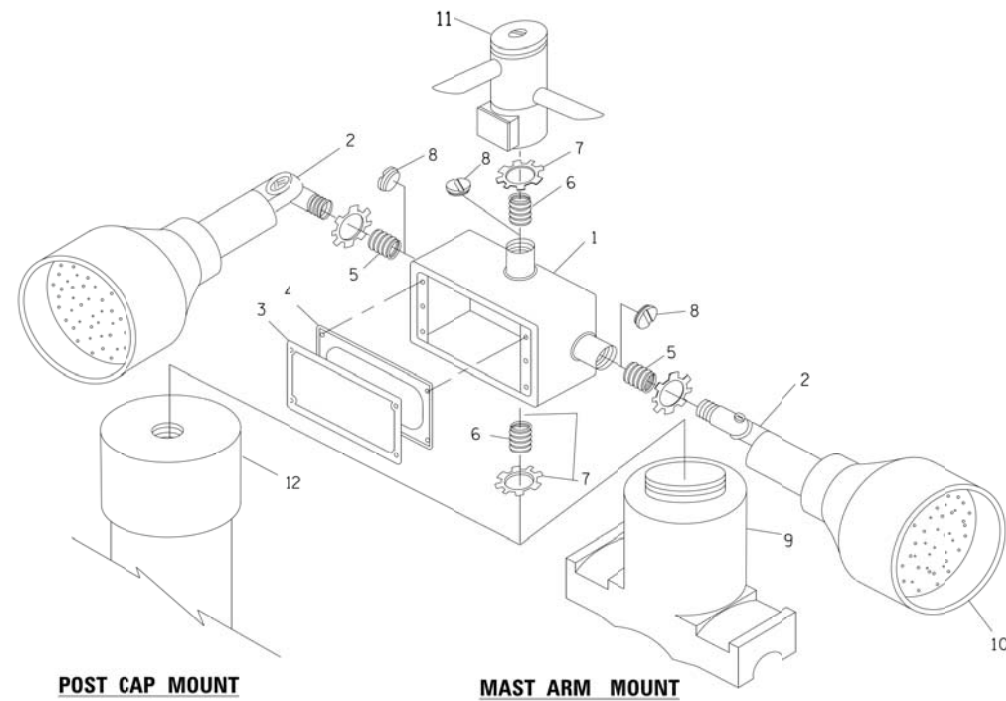
DEPTH OF MAST ARM FOUNDATIONS, TYPE E



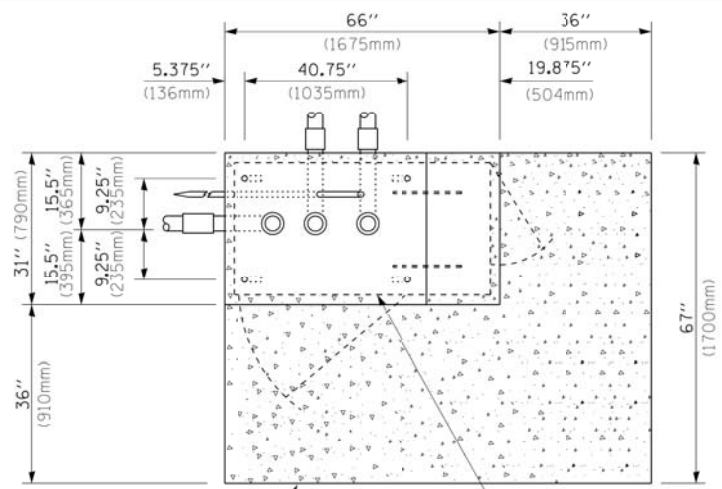
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

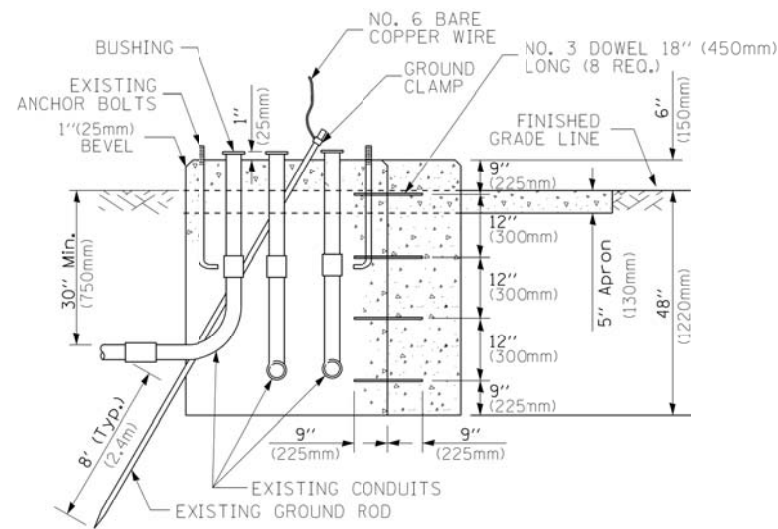
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW
(NOT TO SCALE)

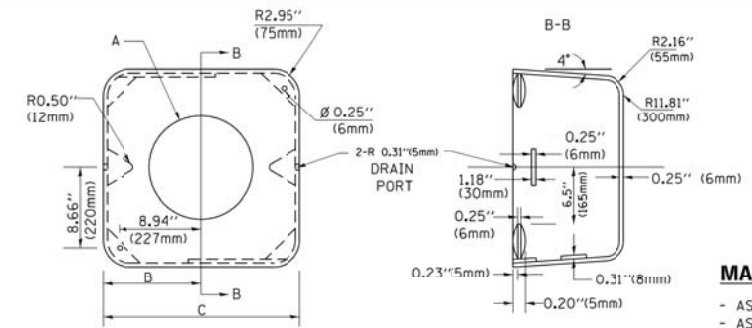


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

| ITEM NO. | IDENTIFICATION |
|----------|---|
| 1 | OUTLET BOX- GALV. 21 CU.IN. (0,000344 CU-M) |
| 2 | LAMP HOLDER AND COVER |
| 3 | OUTLET BOX COVER |
| 4 | RUBBER COVER GASKET |
| 5 | REDUCING BUSHING |
| 6 | 3/4" (19 mm) CLOSE NIPPLE |
| 7 | 3/4" (19 mm) LOCKNUT |
| 8 | 3/4" (19 mm) HOLE PLUG |
| 9 | SADDLE BRACKET - GALV. |
| 10 | 6 WATT PAR 38 LED FLOOD LAMP |
| 11 | DETECTOR UNIT |
| 12 | POST CAP [18 FT. (5.4 m) POST MIN.] |

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1 - GZ/GEDNEY FSX 150 OR EQUIVALENT
ITEM #2 - MULBERRY CON 0 SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9 - "BAND IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

| A | B | C | HEIGHT | WEIGHT |
|-----------|----------------|---------------|--------------------------|-----------------|
| VARIABLES | 9.5" (241mm) | 19" (483mm) | 7" (178mm) - 12" (300mm) | 53 lbs (24kg) |
| VARIABLES | 10.75" (273mm) | 21.5" (546mm) | 7" (178mm) - 12" (300mm) | 68 lbs (31 kg) |
| VARIABLES | 13.0" (330mm) | 26" (660mm) | 7" (178mm) - 12" (300mm) | 81 lbs (37 kg) |
| VARIABLES | 18.5" (470mm) | 37" (940mm) | 7" (178mm) - 12" (300mm) | 126 lbs (57 kg) |

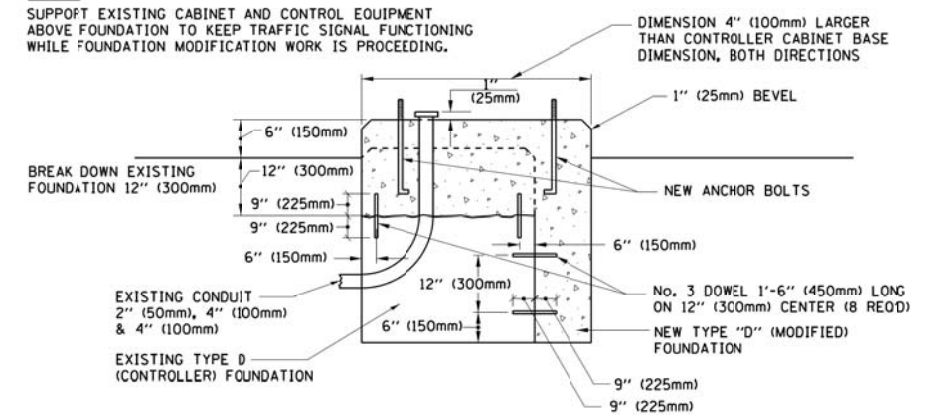
SHROUD

NOTES:

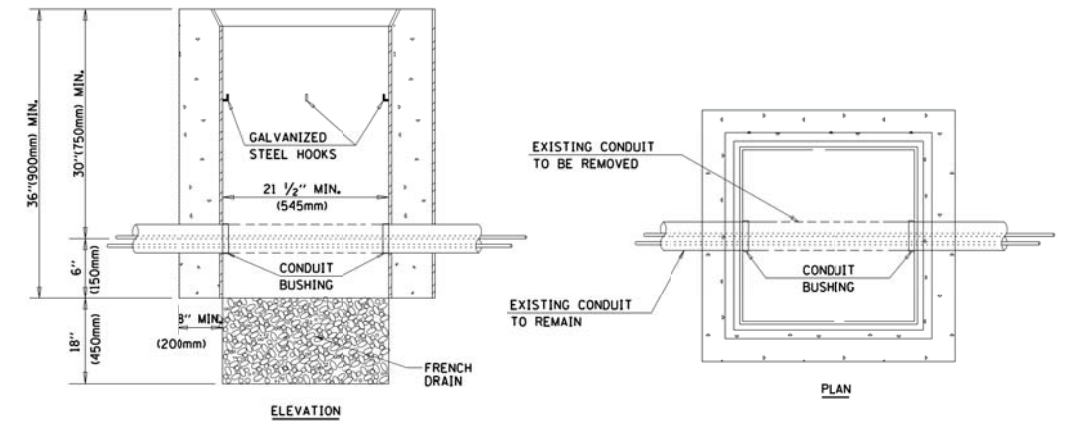
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

| | | | |
|--|----------------------------|-----------------|----------------------|
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| ca:\pw\work\pwidot\footemj\d0108315\ts05.dgn | | DRAWN - BCK | REVISED - |
| | PLOT SCALE = 50.0000' / 1" | CHECKED - DAD | REVISED - |
| | PLOT DATE = 1/13/2014 | DATE - 10-28-09 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

| | | | | |
|---|------------------------|--------------------|------------------|---------------|
| F.A.J. RTE. 2678 1446 | SECTION 17-00049-00-PV | COUNTY DUPAGE | TOTAL SHEETS 208 | SHEET NO. 100 |
| TS-05 | | CONTRACT NO. 61G16 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT | | | | |

SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.