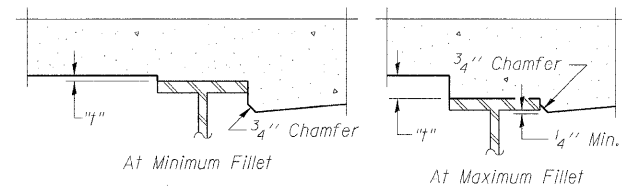
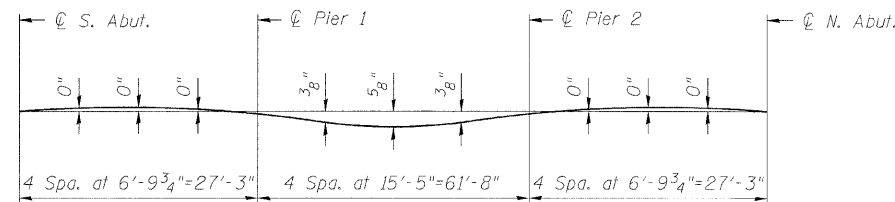
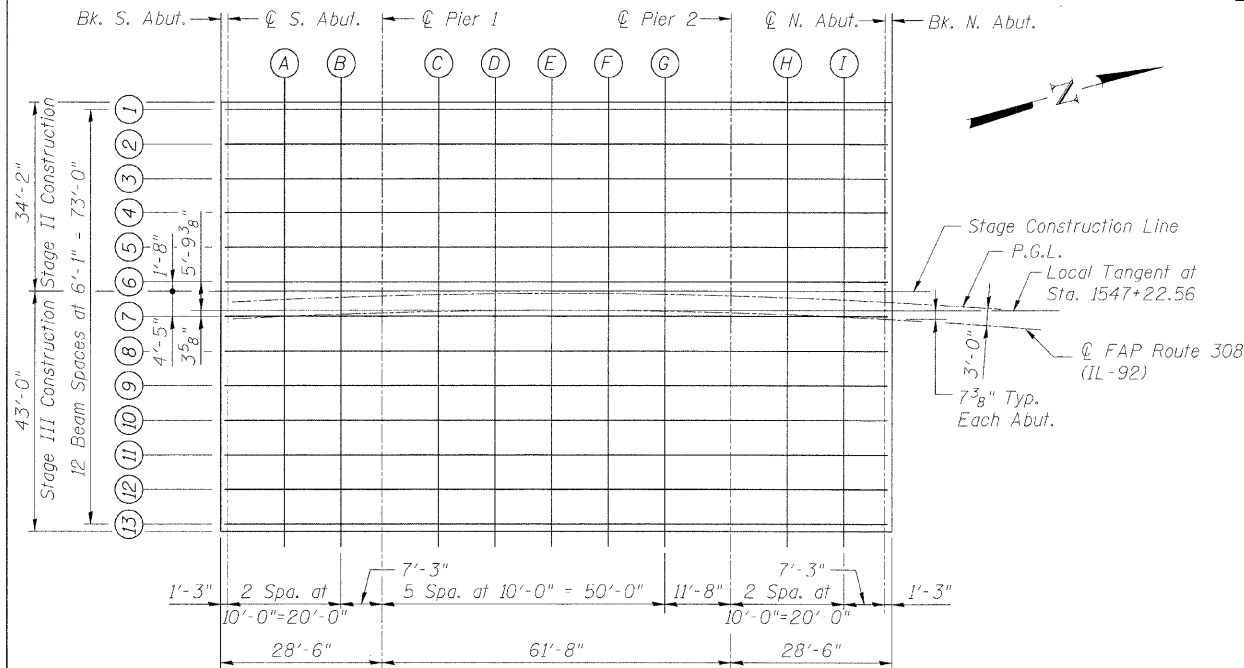


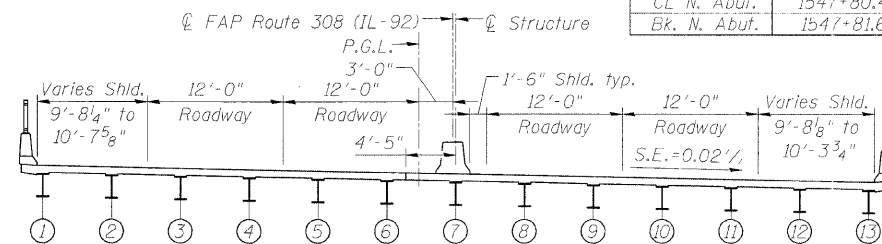
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|                       |          |                  |  |       |                          |
|-----------------------|----------|------------------|--|-------|--------------------------|
| ROUTE NO.             | SECTION  | COUNTY           | POST MILES                               | SHEET | SHEET NO. 5<br>27 SHEETS |
| F.A.P.<br>308         | *        | ROCK ISLAND      | 210                                      | 180   |                          |
| FED. ROAD DIST. NO. 7 | ILLINOIS | FED. AID PROJECT | Contract #64814 * (1HB, HB-1, VB, HB-2)R |       |                          |



To determine "f": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown herein. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown herein, minus slab thickness, equals the fillet heights "f" above top flange of beams.

FILLET HEIGHTS



|          |     |
|----------|-----|
| DESIGNED | CMM |
| CHECKED  | AMK |
| DRAWN    | OS  |
| CHECKED  | RWC |

**LOCHNER**  
H.W. LOCHNER, INC., CHICAGO, ILLINOIS

BEAM 1

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. S. Abut. | 1546+63.97 | -36.80 | 572.43                       | 572.43   |
| CL S. Abut.  | 1546+65.21 | -36.78 | 572.44                       | 572.44   |
| A            | 1546+75.08 | -36.60 | 572.51                       | 572.51   |
| B            | 1546+84.95 | -36.45 | 572.57                       | 572.57   |
| CL Pier 1    | 1546+92.11 | -36.36 | 572.60                       | 572.60   |
| C            | 1547+01.99 | -36.27 | 572.64                       | 572.66   |
| D            | 1547+11.86 | -36.22 | 572.67                       | 572.71   |
| E            | 1547+21.74 | -36.20 | 572.68                       | 572.74   |
| F            | 1547+31.61 | -36.21 | 572.68                       | 572.73   |
| G            | 1547+41.49 | -36.26 | 572.67                       | 572.70   |
| CL Pier 2    | 1547+53.01 | -36.36 | 572.65                       | 572.65   |
| H            | 1547+62.88 | -36.49 | 572.61                       | 572.61   |
| I            | 1547+72.75 | -36.64 | 572.56                       | 572.56   |
| CL N. Abut.  | 1547+79.91 | -36.78 | 572.52                       | 572.52   |
| Bk. N. Abut. | 1547+81.15 | -36.80 | 572.51                       | 572.51   |

BEAM 3

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. S. Abut. | 1546+63.73 | -24.64 | 572.19                       | 572.19   |
| CL S. Abut.  | 1546+64.97 | -24.61 | 572.20                       | 572.20   |
| A            | 1546+74.88 | -24.43 | 572.27                       | 572.27   |
| B            | 1546+84.80 | -24.28 | 572.33                       | 572.33   |
| CL Pier 1    | 1546+91.98 | -24.20 | 572.36                       | 572.36   |
| C            | 1547+01.90 | -24.11 | 572.40                       | 572.42   |
| D            | 1547+11.82 | -24.05 | 572.42                       | 572.47   |
| E            | 1547+21.73 | -24.03 | 572.44                       | 572.49   |
| F            | 1547+31.65 | -24.05 | 572.44                       | 572.49   |
| G            | 1547+41.57 | -24.09 | 572.43                       | 572.45   |
| CL Pier 2    | 1547+53.14 | -24.20 | 572.40                       | 572.40   |
| H            | 1547+63.05 | -24.32 | 572.37                       | 572.37   |
| I            | 1547+72.97 | -24.48 | 572.32                       | 572.32   |
| CL N. Abut.  | 1547+80.15 | -24.61 | 572.28                       | 572.28   |
| Bk. N. Abut. | 1547+81.39 | -24.64 | 572.27                       | 572.27   |

BEAM 5

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. S. Abut. | 1546+63.48 | -12.48 | 571.94                       | 571.94   |
| CL S. Abut.  | 1546+64.72 | -12.45 | 571.95                       | 571.95   |
| A            | 1546+74.68 | -12.27 | 572.02                       | 572.02   |
| B            | 1546+84.64 | -12.12 | 572.08                       | 572.08   |
| CL Pier 1    | 1546+91.85 | -12.03 | 572.12                       | 572.12   |
| C            | 1547+01.81 | -11.94 | 572.15                       | 572.18   |
| D            | 1547+11.77 | -11.88 | 572.18                       | 572.22   |
| E            | 1547+21.73 | -11.86 | 572.19                       | 572.25   |
| F            | 1547+31.69 | -11.88 | 572.19                       | 572.24   |
| G            | 1547+41.65 | -11.93 | 572.18                       | 572.21   |
| CL Pier 2    | 1547+53.27 | -12.03 | 572.16                       | 572.16   |
| H            | 1547+63.22 | -12.15 | 572.12                       | 572.12   |
| I            | 1547+73.18 | -12.31 | 572.07                       | 572.07   |
| CL N. Abut.  | 1547+80.40 | -12.45 | 572.03                       | 572.03   |
| Bk. N. Abut. | 1547+81.64 | -12.48 | 572.02                       | 572.02   |

BEAM 2

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. S. Abut. | 1546+63.85 | -30.72 | 572.31                       | 572.31   |
| CL S. Abut.  | 1546+65.09 | -30.70 | 572.32                       | 572.32   |
| A            | 1546+74.98 | -30.51 | 572.39                       | 572.39   |
| B            | 1546+84.87 | -30.36 | 572.45                       | 572.45   |
| CL Pier 1    | 1546+92.05 | -30.28 | 572.48                       | 572.48   |
| C            | 1547+01.94 | -30.19 | 572.52                       | 572.54   |
| D            | 1547+11.84 | -30.13 | 572.54                       | 572.59   |
| E            | 1547+21.74 | -30.11 | 572.56                       | 572.61   |
| F            | 1547+31.63 | -30.13 | 572.56                       | 572.61   |
| G            | 1547+41.53 | -30.18 | 572.55                       | 572.58   |
| CL Pier 2    | 1547+53.07 | -30.28 | 572.52                       | 572.52   |
| H            | 1547+62.97 | -30.40 | 572.49                       | 572.49   |
| I            | 1547+72.86 | -30.56 | 572.44                       | 572.44   |
| CL N. Abut.  | 1547+80.03 | -30.70 | 572.40                       | 572.40   |
| Bk. N. Abut. | 1547+81.27 | -30.72 | 572.39                       | 572.39   |

BEAM 4

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. S. Abut. | 1546+63.60 | -18.56 | 572.07                       | 572.07   |
| CL S. Abut.  | 1546+64.85 | -18.53 | 572.08                       | 572.08   |
| A            | 1546+74.78 | -18.35 | 572.15                       | 572.15   |
| B            | 1546+84.72 | -18.20 | 572.20                       | 572.20   |
| CL Pier 1    | 1546+91.92 | -18.11 | 572.24                       | 572.24   |
| C            | 1547+01.86 | -18.02 | 572.28                       | 572.30   |
| D            | 1547+11.79 | -17.97 | 572.30                       | 572.35   |
| E            | 1547+21.73 | -17.95 | 572.31                       | 572.37   |
| F            | 1547+31.67 | -17.96 | 572.32                       | 572.36   |
| G            | 1547+41.61 | -18.01 | 572.31                       | 572.33   |
| CL Pier 2    | 1547+53.20 | -18.11 | 572.28                       | 572.28   |
| H            | 1547+63.14 | -18.24 | 572.24                       | 572.24   |
| I            | 1547+73.07 | -18.40 | 572.20                       | 572.20   |
| CL N. Abut.  | 1547+80.27 | -18.53 | 572.15                       | 572.15   |
| Bk. N. Abut. | 1547+81.52 | -18.56 | 572.15                       | 572.15   |

BEAM 6

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. S. Abut. | 1546+63.35 | -6.39  | 571.82                       | 571.82   |
| CL S. Abut.  | 1546+64.60 | -6.37  | 571.83                       | 571.83   |
| A            | 1546+74.58 | -6.18  | 571.90                       | 571.90   |
| B            | 1546+84.56 | -6.03  | 571.96                       | 571.96   |
| CL Pier 1    | 1546+91.79 | -5.95  | 571.99                       | 571.99   |
| C            | 1547+01.77 | -5.86  | 572.03                       | 572.05   |
| D            | 1547+11.75 | -5.80  | 572.06                       | 572.10   |
| E            | 1547+21.73 | -5.78  | 572.07                       | 572.13   |
| F            | 1547+31.71 | -5.80  | 572.07                       | 572.12   |
| G            | 1547+41.69 | -5.85  | 572.06                       | 572.09   |
| CL Pier 2    | 1547+53.33 | -5.95  | 572.04                       | 572.04   |
| H            | 1547+63.31 | -6.07  | 572.00                       | 572.00   |
| I            | 1547+73.29 | -6.23  | 571.95                       | 571.95   |
| CL N. Abut.  | 1547+80.52 | -6.37  | 571.91                       | 571.91   |
| Bk. N. Abut. | 1547+81.77 | -6.39  | 571.90                       | 571.90   |

NOTE

Work this Sheet with Sheet No. 6

TOP OF SLAB ELEVATIONS 1  
IL ROUTE 92 (CENTENNIAL EXPRESSWAY)  
OVER 7th AVENUE  
FAP ROUTE 308 SEC. 1(HB)R  
ROCK ISLAND COUNTY  
STATION 1547+22.56  
STRUCTURE NO. 081-0170