FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF STATE AND LOCAL STANDARDS. SEE SHEET NO. 2

PROJECT LOCATED WITHIN THE VILLAGE OF BURR RIDGE

TRAFFIC DATA - 83RD STREET **POSTED SPEED: 30MPH DESIGN SPEED: 30MPH CURRENT ADT (2005): 4,420 VPD**

 \circ

JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1-800-892-0123 OR 811

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU RTE. 1552 (83rd STREET)

SECTION NO.: 07-00037-00-SW

SOUTH FRONTAGE ROAD TO COUNTY LINE ROAD (FAU 2684)

SIDEWALK CONSTRUCTION

PROJECT NO.: M-8003 (930)

DUPAGE COUNTY C-91-112-08

RANGE 11E 79th St Q.

PROJECT ENDS **STATION 87+13**

TOWNSHIP: 36 HORTH RANGE: 11 EAST

GROSS LENGTH OF PROJECT = 7,736 LINEAL FEET (1.47 MILES) NET LENGTH OF PROJECT = 7,736 LINEAL FEET (1.47 MILES)

07-00037-00-SW DUPAGE





DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

ANTHONY J. BRYANT, P.E.

Dec. 24 20 07

ILLINOIS REGISTRATION No. 062-056612 EXPIRATION DATE: 11/30/2009

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO. 83996

IL DESIGN FIRM NO. 184-001310 Burns &

PROJECT BEGINS

EXP. 04/30/09

	X OF SHEETS	
1	COVER SHEET	
2	LEGEND, GENERAL NOTES, I	EARTHWORK SUMMARY, AND SUMMARY OF QUANTITIES
3	PLAN SHEET 83RD STREET (STA. 9+77 TO STA. 20+00)
4	PLAN SHEET 83RD STREET (STA. 20+00 TO STA. 30+00)
5	PLAN SHEET 83RD STREET (STA. 30+00 TO STA. 40+00)
6	PLAN SHEET 83RD STREET (STA. 40+00 TO STA. 50+00)
7	PLAN SHEET 83RD STREET (STA. 50+00 TO STA. 60+00)
8	PLAN SHEET 83RD STREET (STA. 60+00 TO STA. 70+00)
9	PLAN SHEET 83RD STREET (STA. 70+00 TO STA. 80+00)
10	PLAN SHEET 83RD STREET (STA. 80+00 TO STA. 87+13)
11	MISCELLANEOUS DETAILS (1	OF 3)
12	MISCELLANEOUS DETAILS (2	2 OF 3)
13	MISCELLANEOUS DETAILS (3	3 OF 3)
14	BD01- DRIVEWAY DETAILS - EQUAL TO 4.5 M (15 FT)	DISTANCE BETWEEN ROW AND CURB OR EDGE GREATER THAN OR
15	BD02- DRIVEWAY DETAILS- D 4.5 M (15 FT)	DISTANCE BETWEEN ROW AND FACE OF CURB IS GREATER THAN
16	TC10- TRAFFIC CONTROL AN	ND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
17	TC13- DISTRICT ONE TYPICA	AL PAVEMENT MARKINGS
NA. 22 1004	STANDARD DETAILS	ABBREVIATIONS AND PATTERNS
	01-05 CURB RAMPS FOR SID	
	01-02 LANE CLOSURE, 2L, 2V	
		ILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
7019	* # P	
7019	LEGEND	
	GEORGEOGRA CON CON COMMUNICADO	EXISTING R.O.W.
		TREE LINE
	¤	LIGHT POLE
	DET	ELECTRIC TRANSFORMER
	□€т	ELECTRICAL TRANSFORMER W/CONC, PAD
	.	TRAFFIC SIGNAL
	-口-	POWER POLE
	\$	GUY WIRE

TELEPHONE PEDESTAL

HANDHOLE

SIGN

BENCHMARK

(FOUND) O

PK1 A

FIRE HYDRANT

LANDSCAPE STONE STORM SEWER INLET TREE STUMP TREE OR SHRUB

PROPERTY PIPE (FOUND)

CONTROL POINT (PK-NAIL)

*	INDICATES SPECIALTY ITEM
٨	DENOTES SPECIAL PROVISION

Number

20101000 TEMPORARY FENCE

20400800 FURNISHED EXCAVATION

21101615 TOPSOIL FURNISH AND PLACE, 4"

* 25000400 NITROGEN FERTILIZER NUTRIENT

* 25000600 POTASSIUM FERTILIZER NUTRIENT

* 25200110 SODDING, SALT TOLERANI

* 25200200 SUPPLEMENTAL WATERING

42400800 DETECTABLE WARNINGS

44000300 CURB REMOVAL

67100100 MOBILIZATION

B2000666

B2001466

B2002220

44000200 DRIVEWAY PAVEMENT REMOVAL

60255500 MANHOLES TO BE ADJUSTED

70102640 TRAFFIC CONTROL AND PROTECTION, STD

78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6'

* 78000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24"

* 25000500 PHOSPHORUS FERTILIZER NUTRIENT

40201000 AGGREGATE FOR TEMPORARY ACCESS

42300300 PORTLAND CEMENT CONCRETE DRIVEWAY, 7 INCH

42400200 PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH

42400400 PORTLAND CEMENT CONCRETE SIDEWALK, 7 INCH

^ 44001700 COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT FOOT 106

TREE, AMELANCHIER X GRANDIFLORA (APPLE SERVICEBERRY), 6"

REE, CORNUS MAS (CORNELIAN CHERRY DOG WOOD), 6' HEIGHT,

HAWTHORN), 2-1/2" CALIPER, TREE FORM, BALLED AND BURLAPED

TREE CRATAEGUS VIRDIS WINTER KING (WINTER KING GREEN

TREE, MALUS BRANDYWINE (BRANDYWINE CRAB APPLE), 2

HEIGHT, SHRUB FORM, BALLED AND BURLAPPED

SHRUB FORM, BALLED AND BURLAPPED

B2003016 TREE, MALUS DRANGT WINE CO. CALIPER, TREE FORM, BALLED AND BURLAPPED

^ X0322923 SEGMENTAL CONCRETE BLOCK WALL

^ XX004393 BITUMINOUS DRIVEWAY REPLACEMENT

^ X0324450 SEGMENTAL CONCRETE BLOCK WALL, SPECIAL

54213657 PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12" EACH 1

* 20101300 TREE PRUNING (1 TO 10 INCH DIAMETER)

* 20101350 TREE PRUNING (OVER 10 INCH DIAMETER)

20201200 REMOVAL & DISPOSAL OF UNSUITABLE MAT

* 20101200 TREE ROOT PRUNING

•		EARTHWORK SCHEDU	LE	
LOCATION (SOUTH FRONTAGE ROA	AD)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (CU. YD.) 20201200	FURNISHED EXCAVATION (CU. YD.) 20400800	TOPSOIL FURNISH AND PLACE, 4" (SQ. YD.) 21101615
STA. 0+00 TO STA. 12+	73	160		220
(83rd STREET) STA. 9+77 TO STA. 87+	13	1145	-	4220 ·
STA. 15+22 TO STA. 16+	43	pr.	20	-
STA. 40+23 TO STA. 45+	00	-	95	-
STA. 61+63 TO STA. 63+	00	-	25	-
STA. 70+57 TO STA. 72+	25	-	40	-
* TOPSOIL REMOVAL DEPTH ESTIMATED AT 7"	TOTAL	1305	180	4440

GENERAL NOTES

SFTY-1B

Quantity

165

Unit

FOOT

EACH 22

EACH 3

EACH 12

CU YD 1075

CU YD 130

SQ YD 3550

POUND 44

POUND 44

POUND 44

SQ YD 3550

UNIT 80

TON 195

SQ YD 210

SQ FT 31240

SQ FT 2850

SQ FT 402

SQ YD 1275

FOOT 91

EACH

LSUM 1

FOOT 100

10

10

10

10

LSUM FOOT 430

EACH

EACH

EACH

EACH

SQ FT 1482

SQ FT 150

SQ YD 680

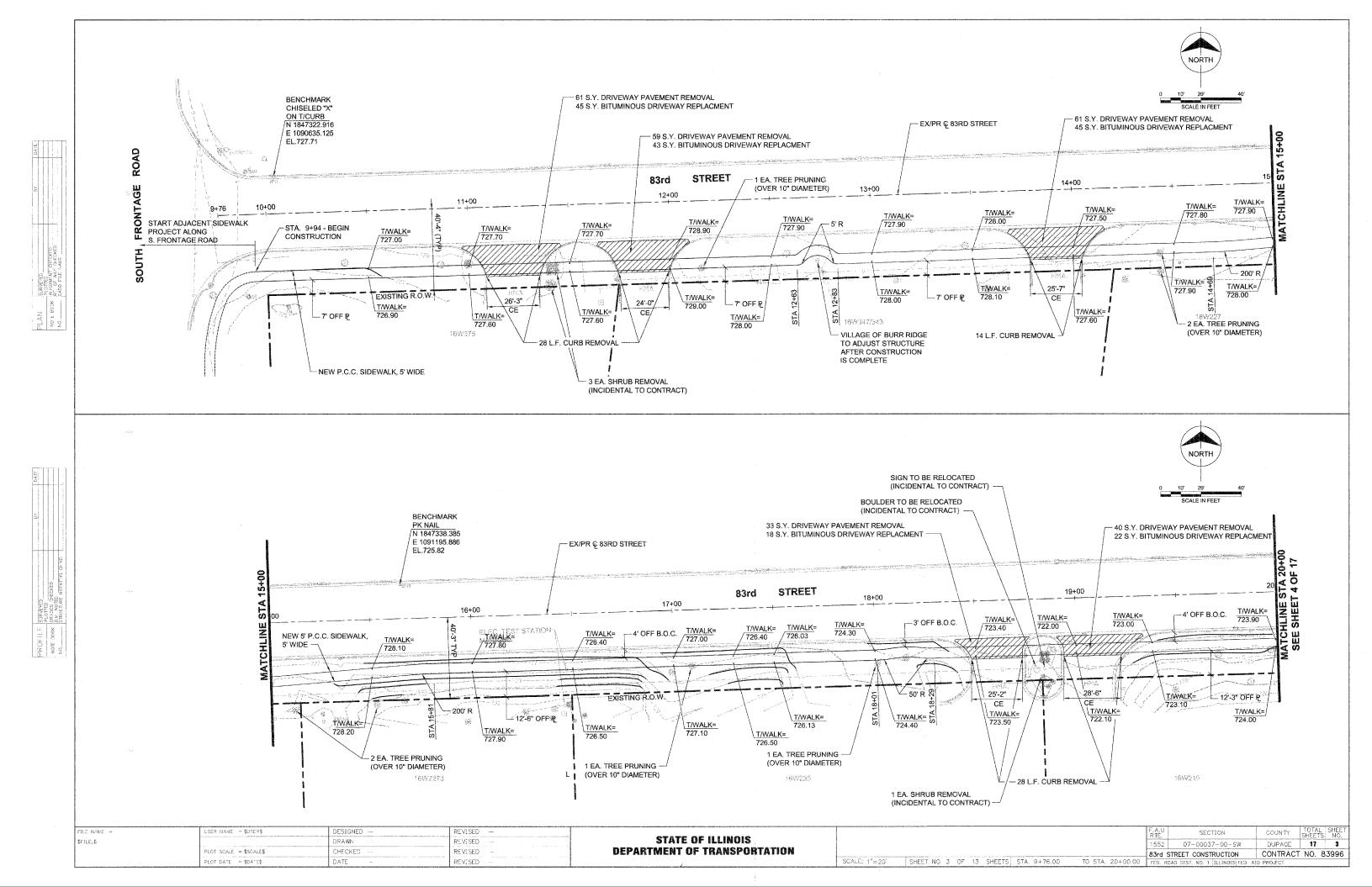
- 1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2007, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2008, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (MUTCD), THE DETAILS IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENT.
- 2. THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. (800-892-0123) AT LEAST 48 HOURS PRIOR TO CONSTRUCTION FOR LOCATIONS OF UTILITIES, AND SHALL BE RESPONSIBLE FOR THE PROTECTION OF THE SAME.
- 3. CONTRACTORS ARE CAUTIONED AGAINST INSTITUTING AND CHANGES AND/OR SUBSTITUTIONS TO THE PROJECT REPRESENTED BY THESE DRAWINGS WITHOUT PRIOR WRITTEN APPROVAL BY THE ENGINEER VISUAL CONTACT WITH THESE DRAWINGS SHALL CONSTITUTE CONCLUSIVE EVIDENCE OF ACCEPTANCE OF THESE
- 4. THE CONTRACTOR SHALL NOTICY THE ENGINEER AND THE VILLAGE OF BURR RIDGE AT LEAST 48 HOURS IN ADVANCE OF CONSTRUCTION, NO UNDERGROUND WORK SHALL BE COVERED UNTIL IT HAS BEEN INSPECTED. APPROVAL TO PROCEED MUST BE OBTAINED FROM THE ENGINEER PRIOR TO POURING ANY CONCRETE AFTER
- 5 WRITTEN DIMENSIONS ON THESE DRAWINGS SHALL TAKE PRECEDENCE OVER SCALED DIMENSIONS. CONTRACTORS SHALL VERIFY AND BE RESPONSIBLE FOR ALL ELEVATIONS, DIMENSIONS, AND CONDITIONS ON THE JOB. THE ENGINEER SHALL BE NOTIFIED AS SOON AS POSSIBLE OF ANY VARIATIONS FROM THE DIMENSIONS AND CONDITIONS SHOWN ON THESE DRAWINGS.
- 6. ANY DELAY OR EXTRA COST TO THE CONTRACTOR CAUSED BY PIPELINES OR OTHER UNDERGROUND STRUCTURES OR OBSTRUCTIONS WHICH ARE NOT SHOWN ON THE DRAWINGS OR FOUND IN LOCATIONS DIFFERENT FROM THOSE INDICATED, SHALL NOT CONSTITUTE A CLAIM FOR EXTRA WORK, ADDITIONAL PAYMENT, OR DAMAGES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL PUBLIC AND PRIVATE UTILITIES, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE DRAWINGS. ANY UTILITY DAMAGES DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING IMMEDIATELY ANY MUD. DIRT, OR DEBRIS DEPOSITED ON ROADWAYS FROM ANY VEHICLES EXITING OR
- 9, IT IS THE CONTRACTORS RESPONSIBILITY TO GUARD AND PROTECT NEWLY POURED CONCRETE AGAINST ANY FOREIGN MARKINGS, GRAFFITI OR OTHER DAMAGE, AND AT HIS EXPENSE, REPLACE ANY SIDEWALK NOT ACCEPTABLE TO THE ENGINEER.
- 10. TRAFFIC CONTROL SHALL CONFORM TO IDOT'S HIGHWAY STANDARDS AND THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AT ALL TIMES DURING CONSTRUCTION, TRAFFIC CONTROL SHALL INCLUDE ADVANCE WARNING LANE CLOSURE SIGNAGE. AN ARROW BOARD AND TYPE I BARRICADES WITH SANDBAGS.
- 11. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF BURR RIDGE.
- 12. SIGN RELOCATION SHALL BE INCIDENTAL TO THE CONTRACT, INVENTORY OF SIGNS SHALL OCCUR BY CONTRACTOR AND ENGINEER PRIOR TO THE WORK.
- 13. PRIVATE SPRINKLER HEADS AND LINES LOCATED ON PRIVATE PROPERTY THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT NO EXPENSE TO THE CONTRACT. PRIVATE SPRINKLER HEADS AND LINES LOCATED WITHIN THE RIGHT-OF-WAY SHALL BE RELOCATED ONTO PRIVATE PROPERTY BY THE HOMEOWNER IF IT INTERFERES WITH CONSTRUCTION OPERATIONS.
- 14. IF A PROPERTY HAS MULTIPLE DRIVEWAYS, AT LEAST ONE DRIVEWAY SHALL REMAIN OPEN AND ACCESSIBLE AT ALL TIMES.
- 15. THE ENGINEER AND THE VILLAGE OF BURR RIDGE SHALL DETERMINE THE PROPOSED TREE LOCATIONS IN THE FIELD DURING CONSTRUCTION.
- 16. THE PROPOSED BITUMINOUS DRIVEWAYS SHALL BE PAID FOR AS BITUMINOUS DRIVEWAY REPLACEMENT. THE DETAILS SHOWING BITUMINOUS DRIVEWAYS SHALL BE FOR REFERENCE ONLY

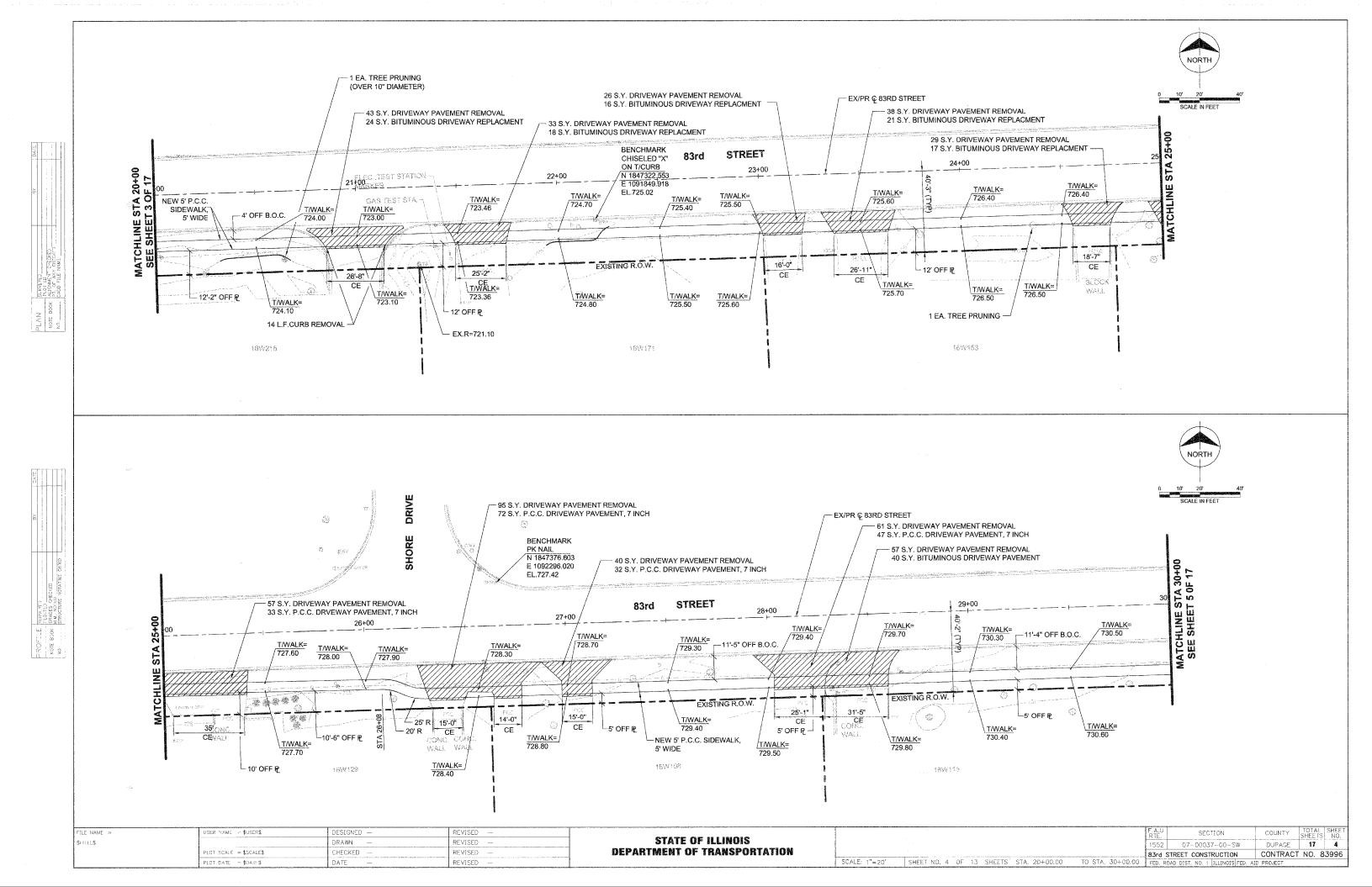
HMA MIXTURE RE	HMA MIXTURE REQUIREMENTS				
PAY ITEM	AC TYPE	VOIDS			
BITUMINOUS DRIVEWAY PAVEMENT HOT-MIX ASPHALT SURFACE COURSE, MIX 'C', N50 (IL 9.5 mm)	PG 64-22	4% @ 50 GYR			
PE: HOT-MIX ASPHALT BASE COURSE, 6" CE: HOT-MIX ASPHALT BASE COURSE, 8"	PG 64-22*	4% @ 50 GYR			

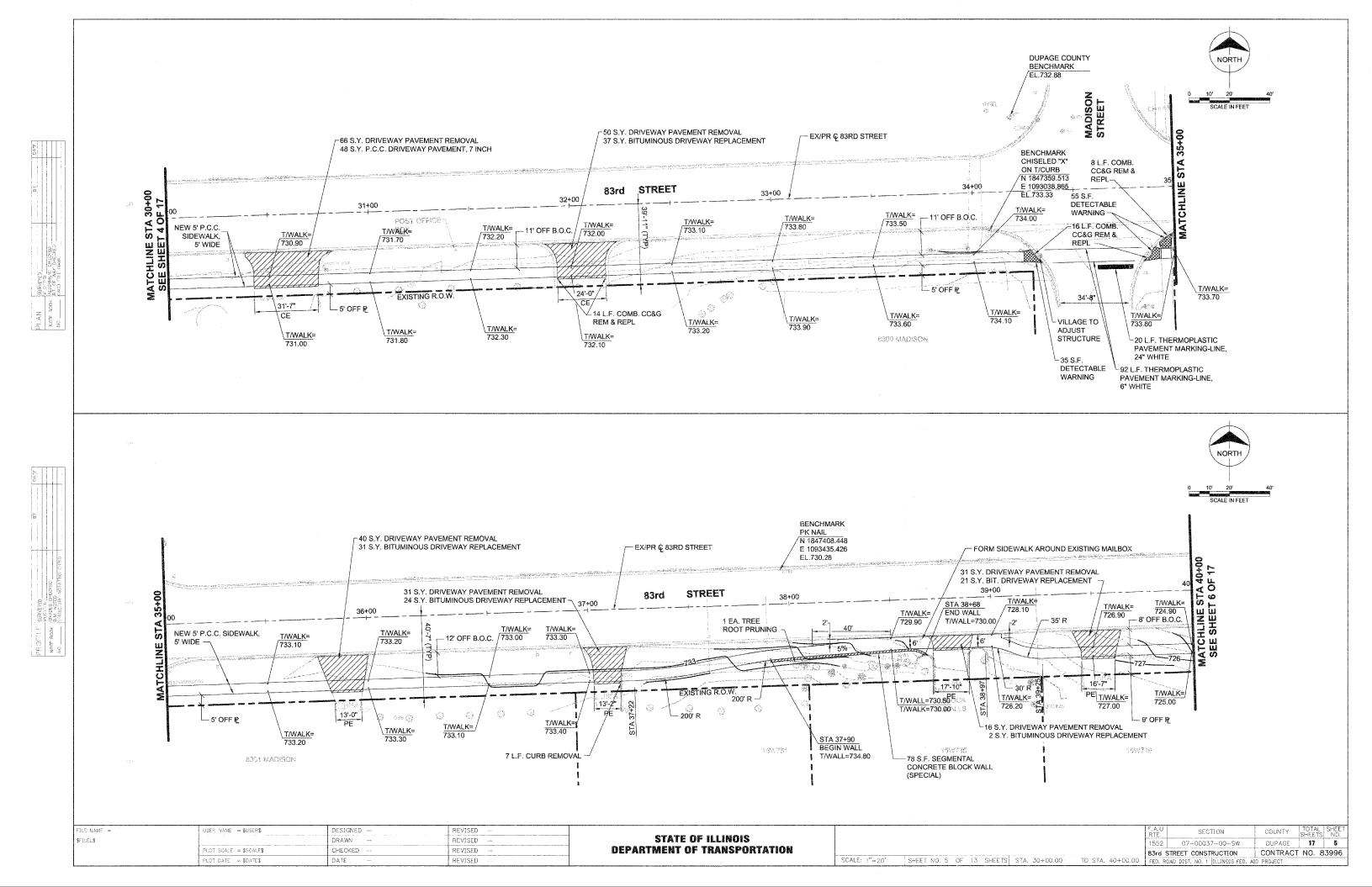
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

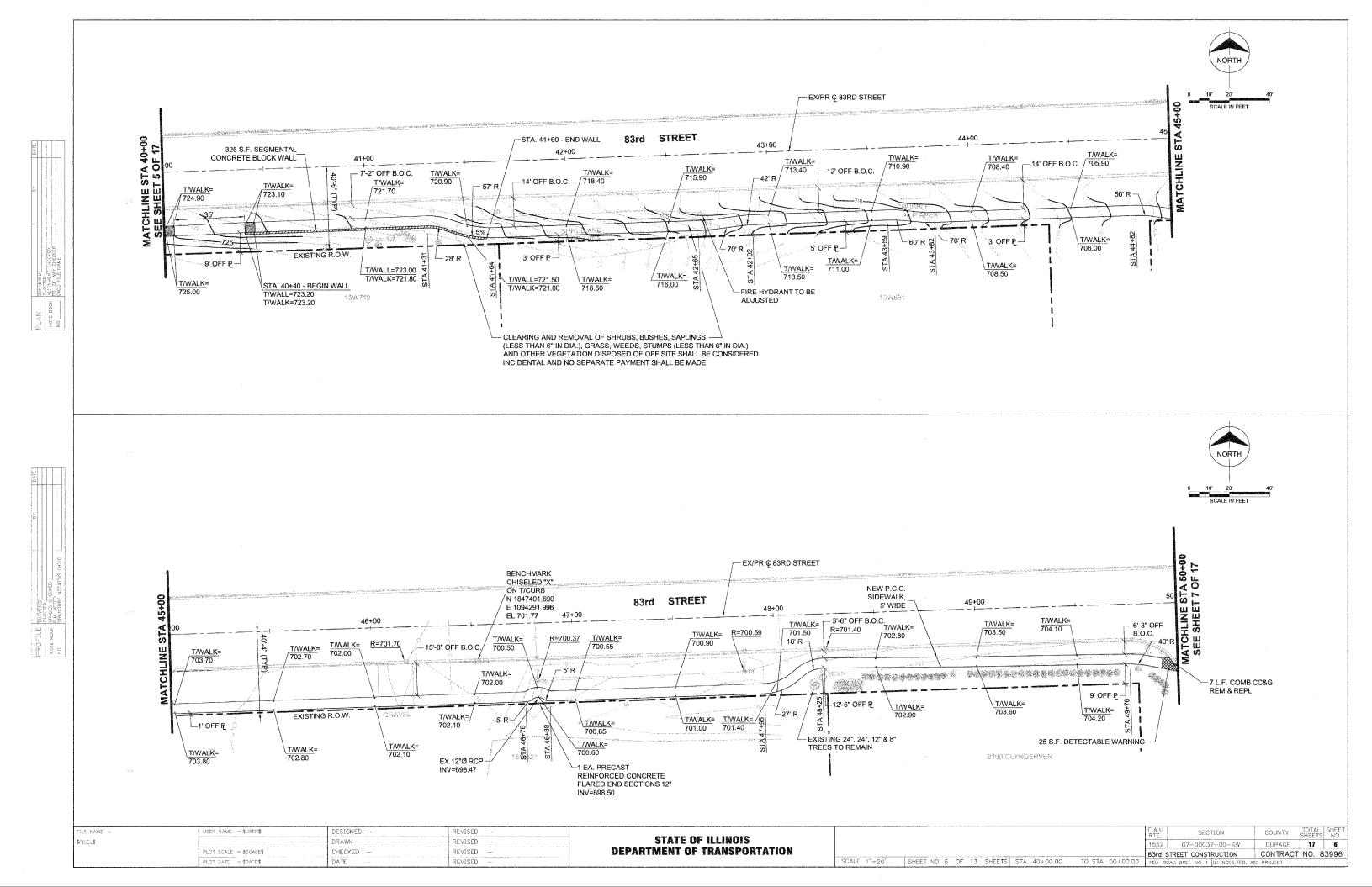
* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

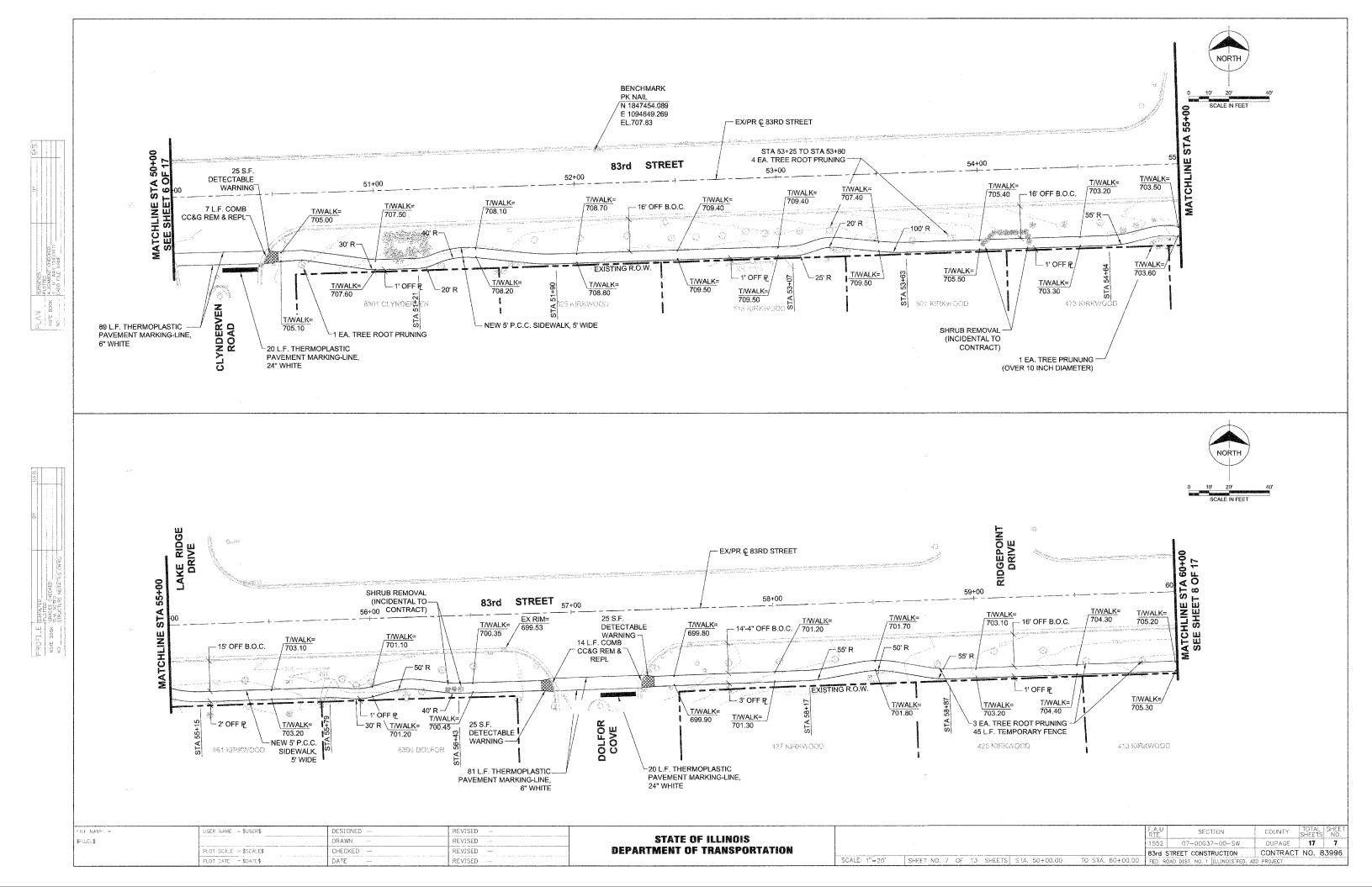
FILE NAME =	USER NAME = \$USER\$	DESIGNED	REVISED			F.A.U RTF	SECTION	COUNTY	TOTAL SHEET
\$60.61\$	·	DRAWN	REVISED	STATE OF ILLINOIS		1552	07-00037-00-SW	DUPAGE	17 2
	PLOT SCA.F = \$SCALE\$	CHECKED	REVISED	DEPARTMENT OF TRANSPORTATION	Man Artista of Man Ar	83rd STRE	ET CONSTRUCTION	CONTRACT	NO. 83996
	PLOT DATE = \$DAIL\$	DATE -	REVISED		SCALE: 1"-20" SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD D	DIST. NO. 1 ILLINOIS FED. A	JO PROJECT	

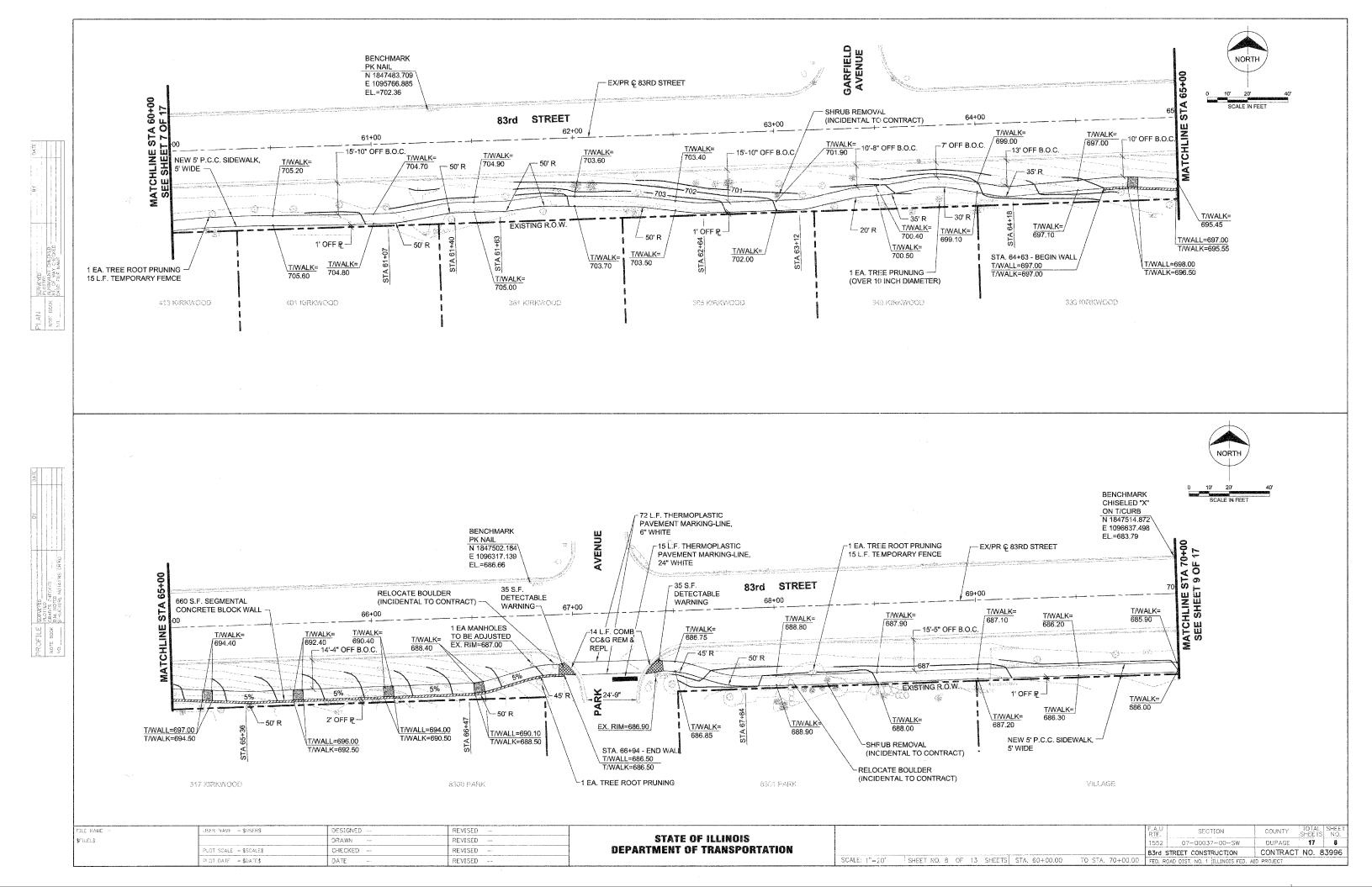


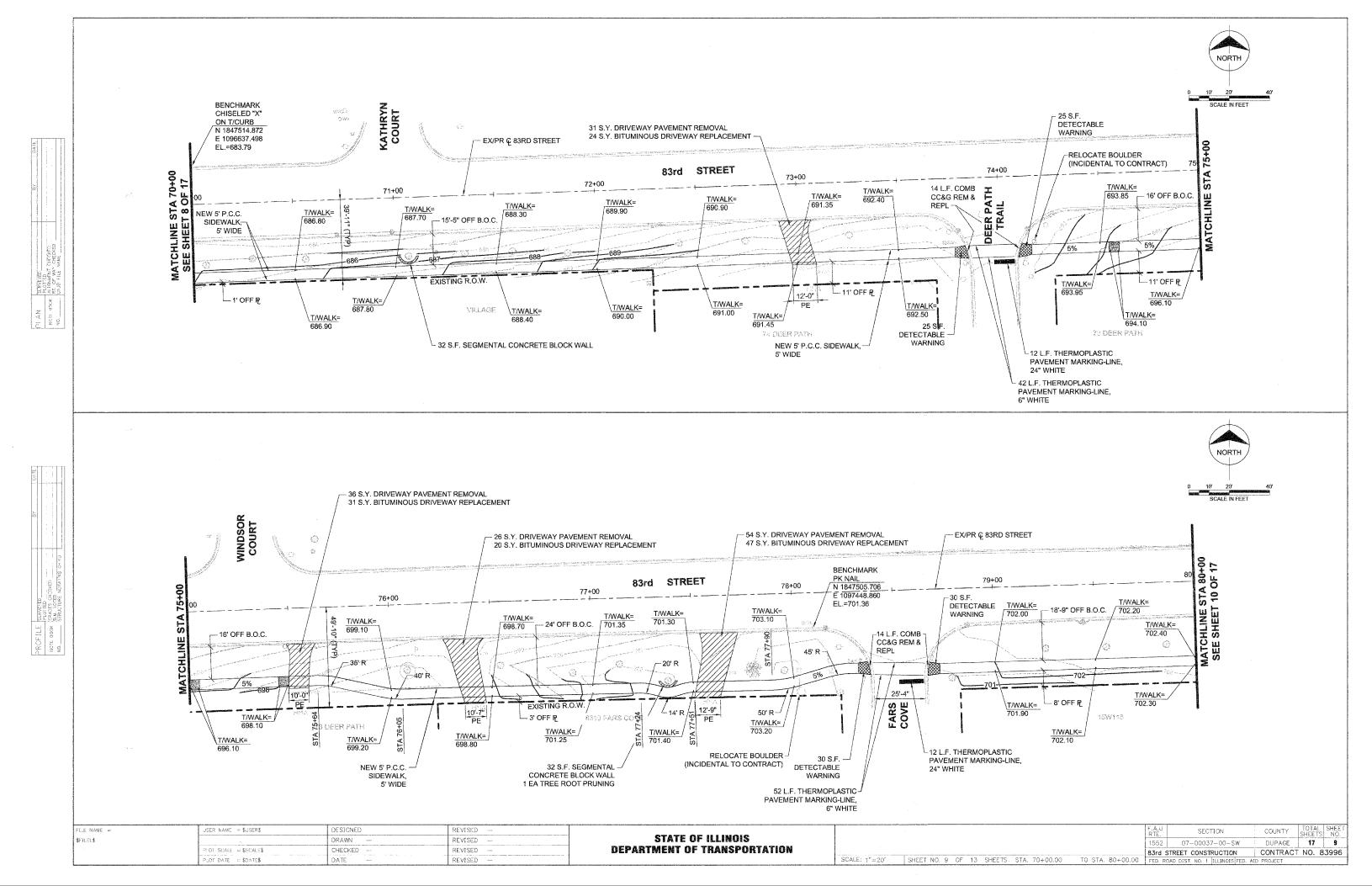


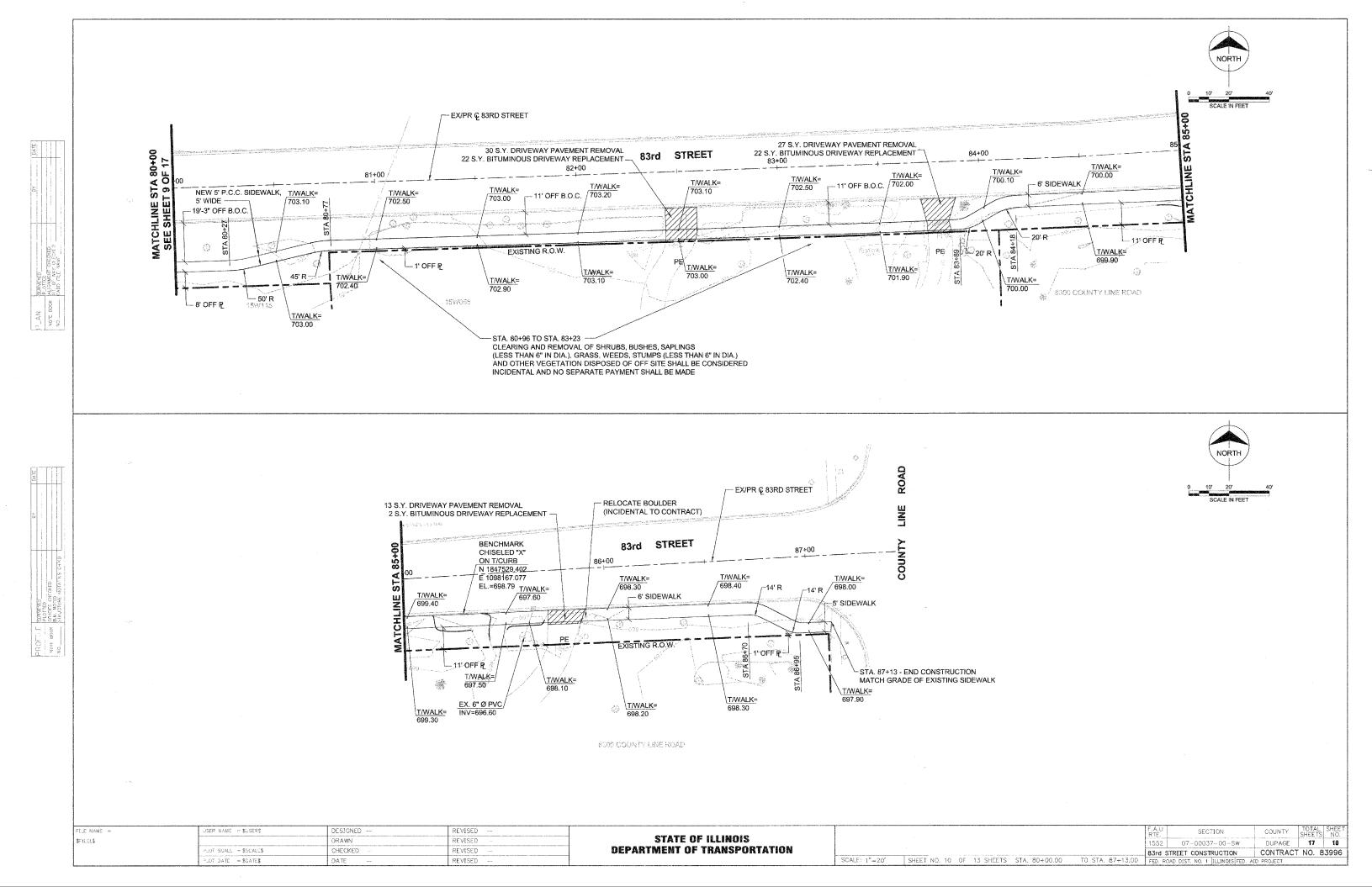


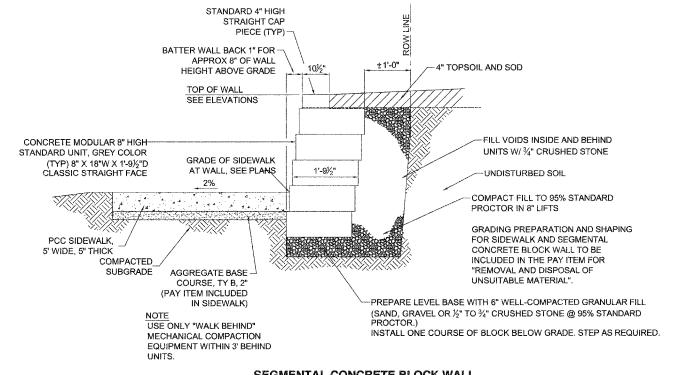






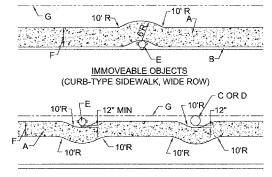






SEGMENTAL CONCRETE BLOCK WALL TYPICAL SECTION

NOT TO SCALE



IMMOVEABLE OBJECTS

SIDEWALK OBSTRUCTIONS

NOT TO SCALE

- A. SIDEWALK
- B. CURB
- C. SERVICE POLE, SEE NOTES 1 AND 2. D. TREE TRUNK, SEE NOTE 1 E. FIRE HYDRANT, SEE NOTES 1 AND 2.
- F. SIDEWALK WIDTH. G. RIGHT-OF-WAY (ROW)

- 1. A FIRE HYDRANT, SERVICE POLE OR TREE TRUNK SHALL BE CONSIDERED IMMOVEABLE ONLY WHEN DIRECTED BY THE VILLAGE ENGINEER.
- 2. WHEN A SERVICE POLE OR FIRE HYDRANT IS LOCATED WITHIN THE SIDEWALK AREA, AND IS CONSIDERED IMMOVEABLE, A 1/2" EXPANSION JOINT SHALL BE INSTALLED ENCIRCLING THE OBJECT. THIS WORK SHALL BE INCIDENTAL TO THE SIDEWALK COST.

PLAN PROFILE

SIDEWALK DETAIL

CONSTRUCTION NOTES

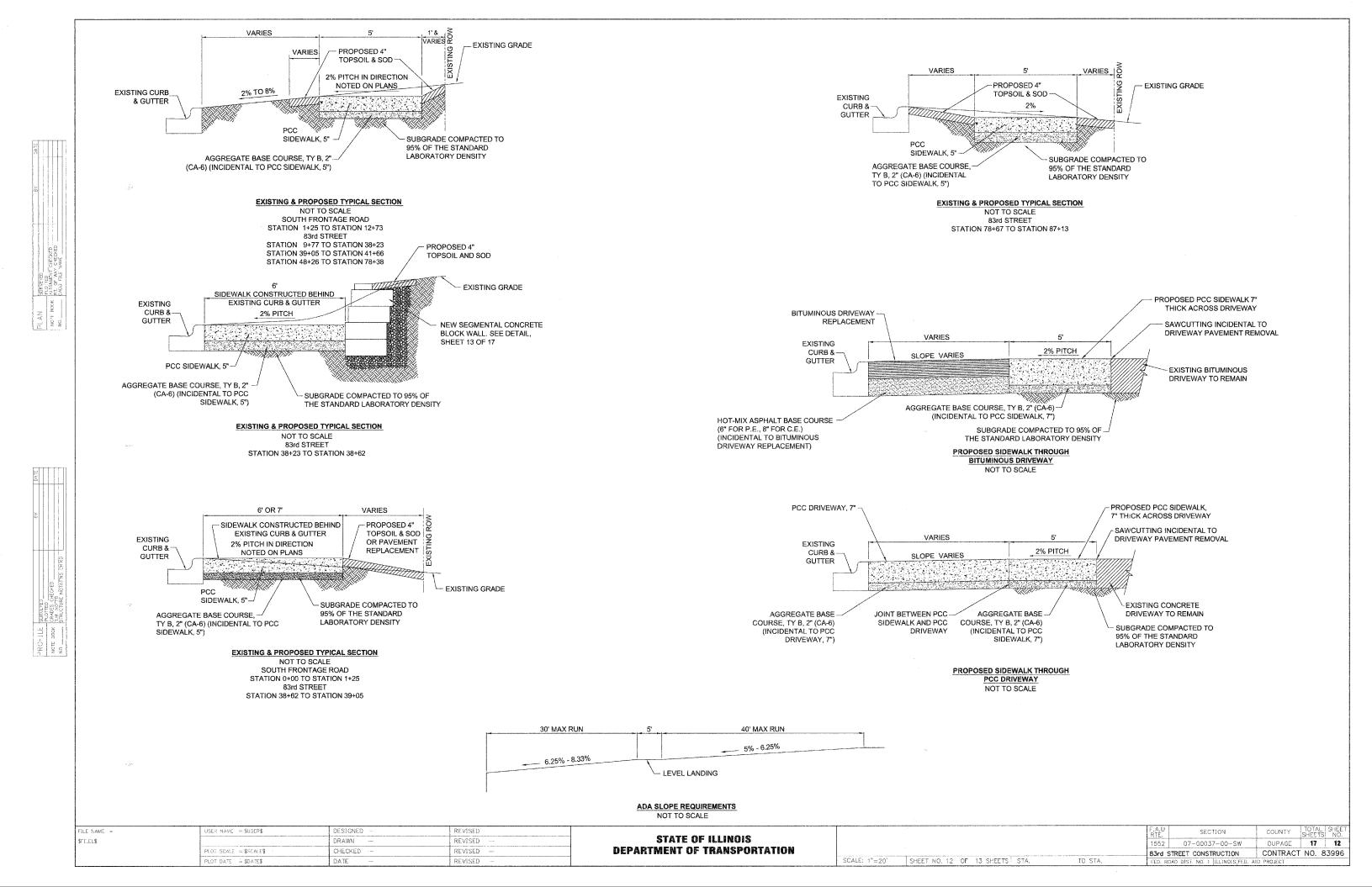
- A. 5' NORMAL WIDTH OR AS INDICATED ON PLANS
- B. AGGREGATE BASE COURSE, TY. B, 2" THICK, (CA-6) (PAY ITEM INCLUDED IN SIDEWALK)
- C. P.C.C. SIDEWALK
- D. SLOPE $\frac{1}{4}$ " PER FOOT TO STREET SIDE, OR AS NOTED ON PLAN
- E. JOINTS, SEE NOTES 3 AND 4
- F. SLOPE 2% TO 8% TOWARDS STREET
- G. CURB AND GUTTER
- H. CONCRETE THICKNESS, SEE NOTE 1
- J. COMPACTED SUBGRADE TO 95% OF THE STANDARD LABORATORY DENSITY

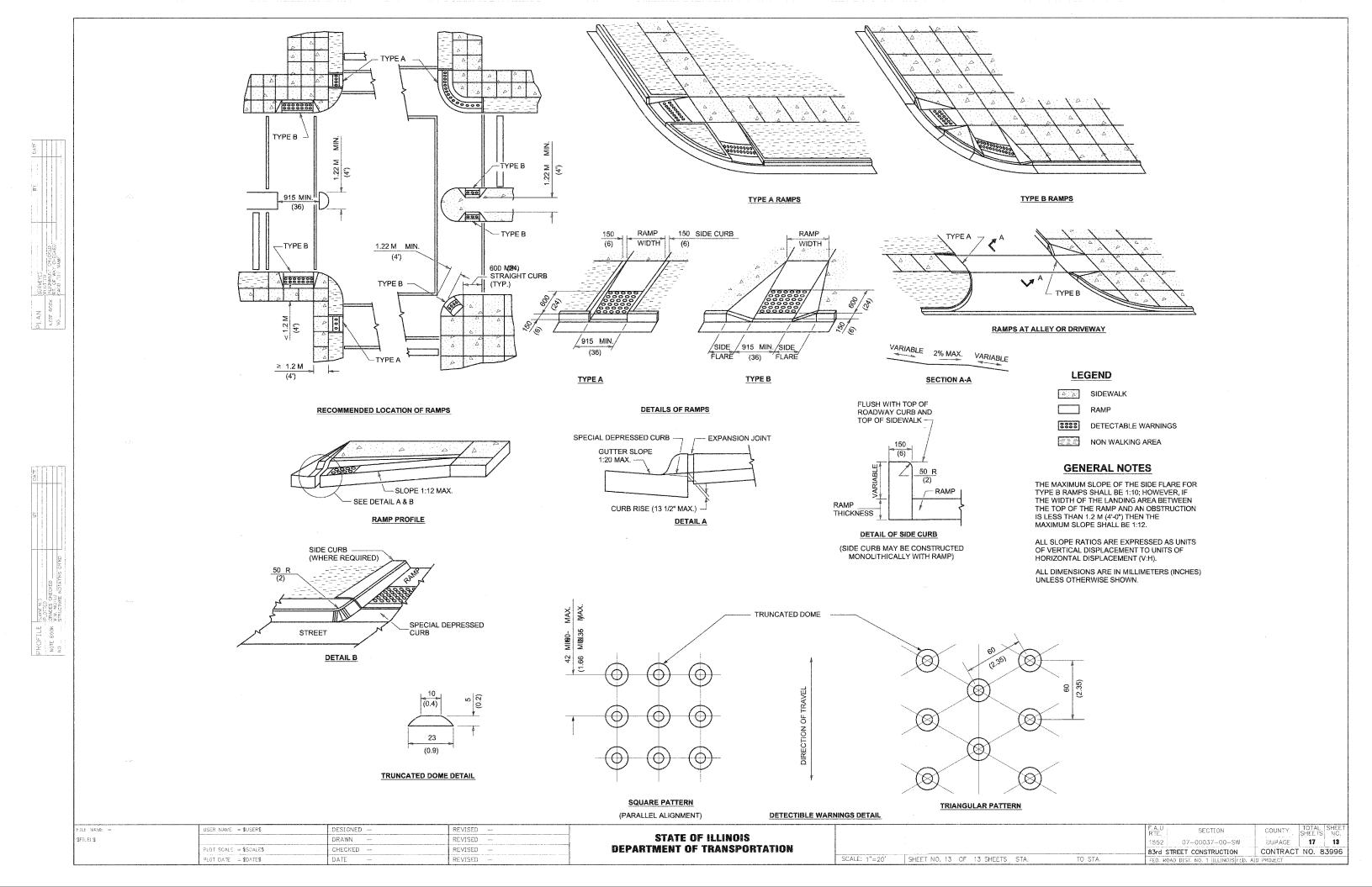
GENERAL NOTES

- STANDARD THICKNESS OF PCC SIDEWALK SHALL BE A MINIMUM OF 5". THICKNESS SHALL BE 7" ACROSS DRIVEWAY ENTRANCE.
- WHERE WOODEN FORMS ARE USED, 2x4's ARE NOT ALLOWED. ONLY 2x6's SHALL BE USED.
- 3. PREFORMED 3/4" EXPANSION JOINTS SHALL BE CONSTRUCTED AT MAX. SPACING OF 40'.
- 4. FORMED OR SAW-CUT CONTRACTION JOINTS SHALL BE SPACED GENERALLY 5' APART, BUT NO LESS THAN 4' AND NO MORE THAN 6'.
- 5. ½" FOAM EXPANSION JOINTS SHALL BE INSTALLED WHERE SIDEWALK ABUTS BUILDINGS, TRAFFIC SIGNAL FOUNDATIONS, SERVICE POLES, AND OTHER IMMOVABLE STRUCTURES.
- 6. CONCRETE TO BE MINIMUM 6-BAG MIX.

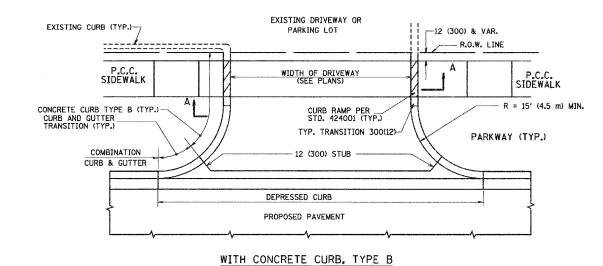
P.C.C. SIDEWALK 5" & 7" NOT TO SCALE

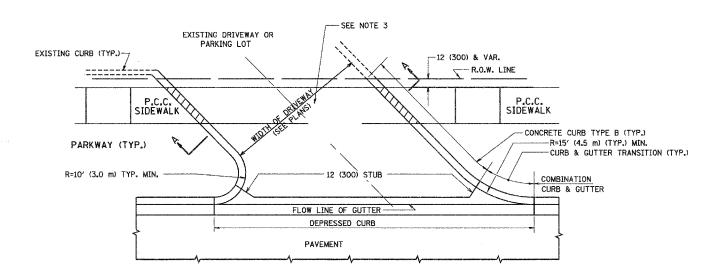
ESIGNED REVISED SECTION COUNTY SHEETS NO STATE OF ILLINOIS DRAWN REVISED \$FILEL\$ 07-00037-00-SW DUPAGE 17 11 **DEPARTMENT OF TRANSPORTATION** CHECKED REVISED 83rd STREET CONSTRUCTION CONTRACT NO. 83996 SHEET NO. 11 OF 13 SHEETS STA. REVISED ST DATE - \$DATE\$



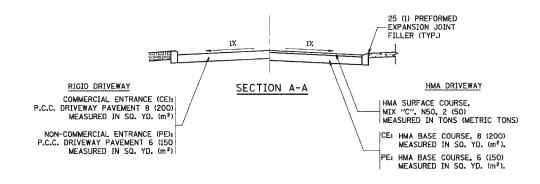


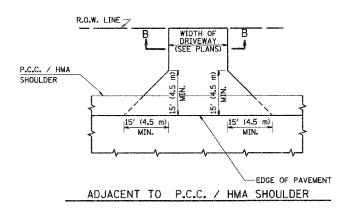


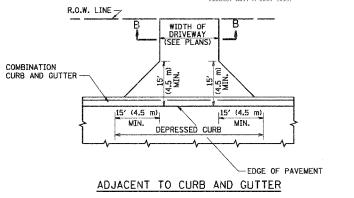


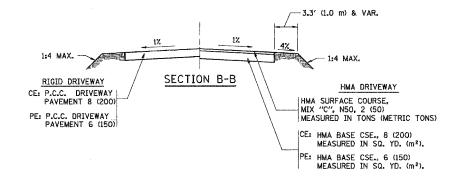


WITH CONCRETE CURB, TYPE B









RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "C", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE A 8 (200) MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

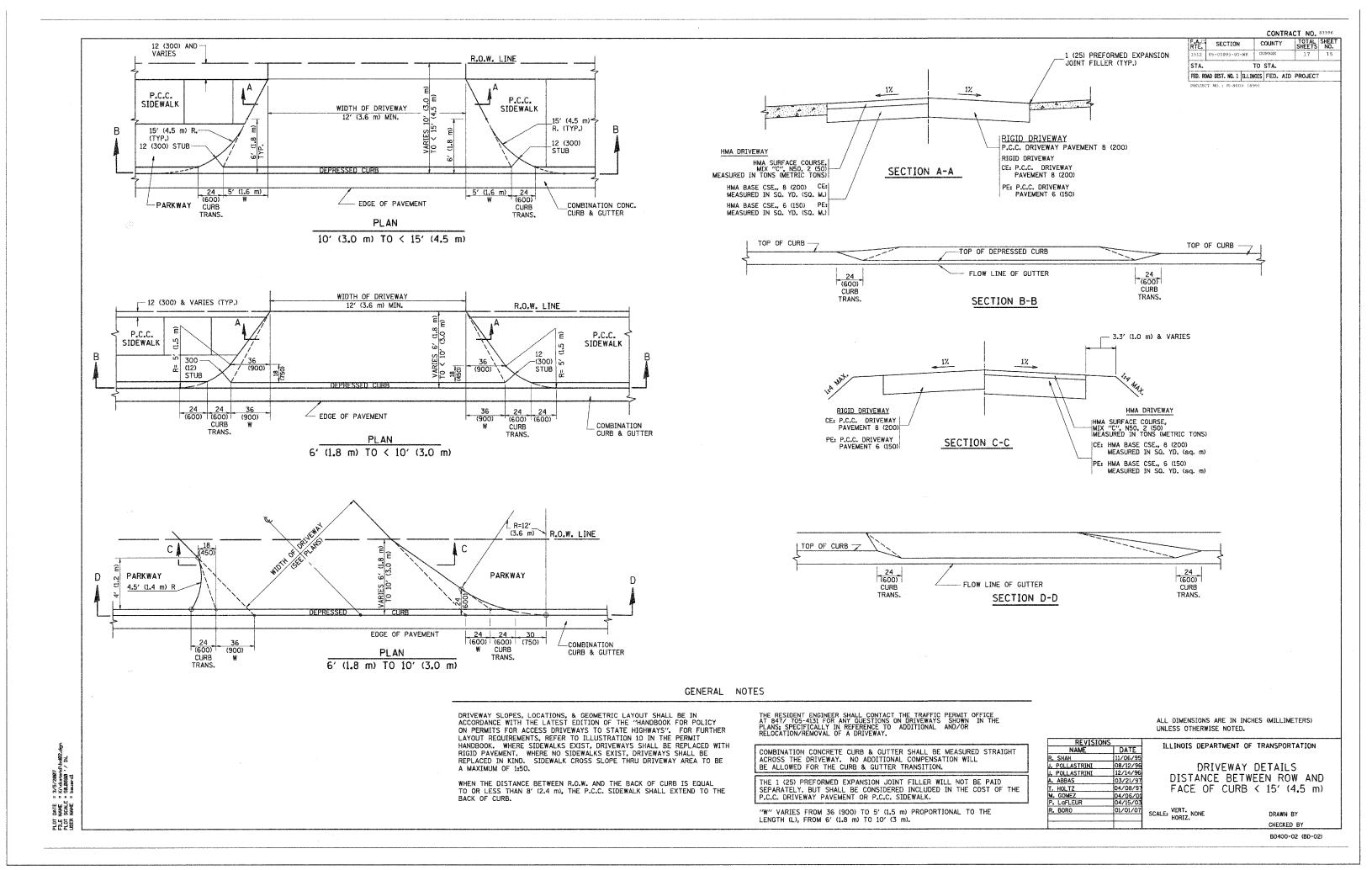
1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

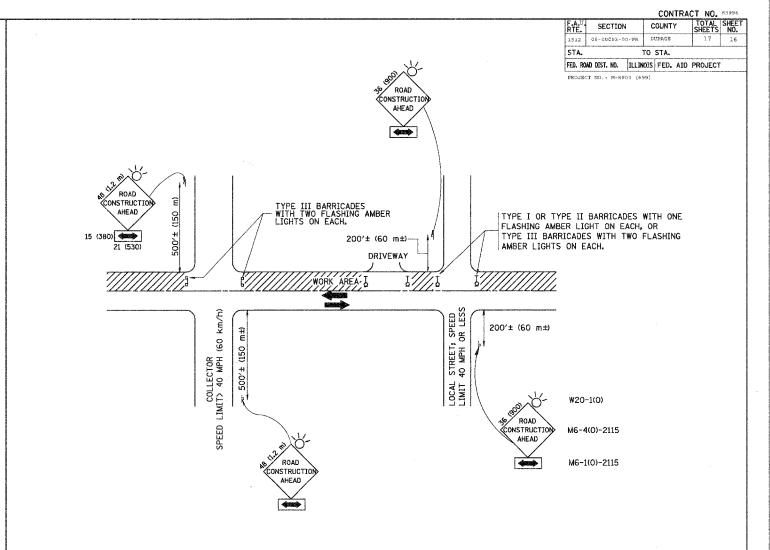
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

		OTTEMOO OTTEMOTOR			
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION			
NAME	DATE	ILLINOIS DEL ANTIGENI	OF TRANSFORTATION		
R. SHAH	11-04-95	DDIVEWAY	/ DETAILS		
J. POLLASTRINI	08-12-96	DUITEMA	L_UE I ALLS		
J. POLLASTRINI	12-14-96	DISTANCE BETW	VEEN R.O.W. AND		
A. ABBAS	03-21-97	FACE OF CUR			
T. HOLTZ	04-08-97				
M. GOMEZ	04-06-01	SHOULDER >	= 15′ (4.5 m)		
P. LaFLEUR	04-15-03				
R, BORO	01-01-07	SCALE: VERT. NONE	DD 1991 DV		
		HORIZ. NORE	DRAWN BY		
			CHECKED BY		
	DD0156 07 (DD 0)				

DATE SCALE NAME PLOT PLOT USER USER





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- d) one road construction ahead sign 36 \times 36 (900 \times 900) With a flasher AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE road construction ahead Sign 48 \times 48 (1.2 m \times 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS		
NAME	DATE	ŀ
LHA	6/89	Ī٦
T. RAMMACHER	09/08/94	'
J. OBERLE	10/18/95	1
A. HOUSEH	03/06/96	
A. HOUSEH	10/15/96	
T. RAMMACHER	01/06/00	
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		s
		-

ILLINOIS DEPARTMENT OF TRANSPORTATION

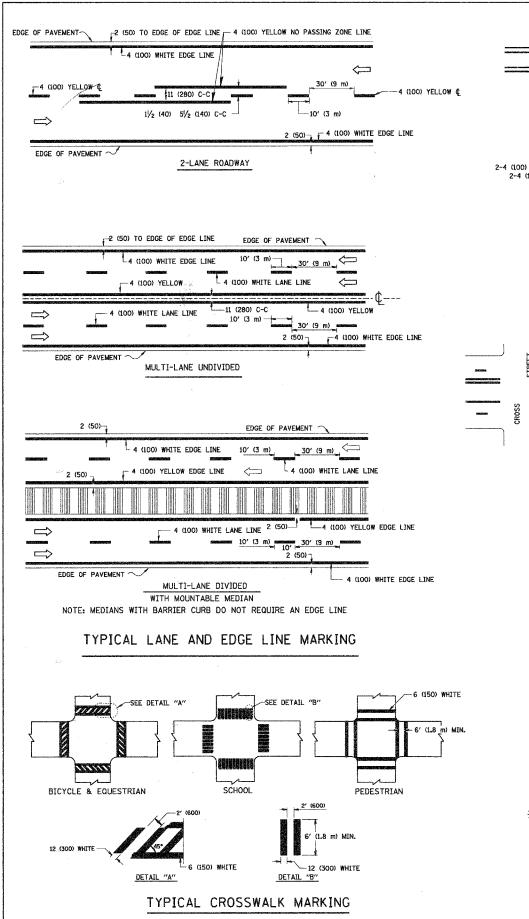
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

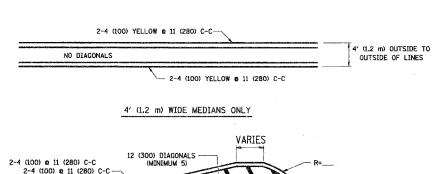
SCALE: NONE

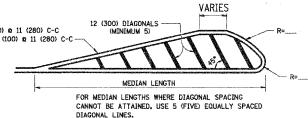
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TC-10

PLOT PLOT USER

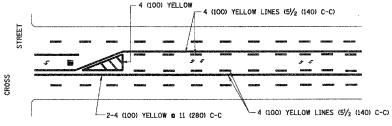




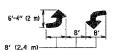


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

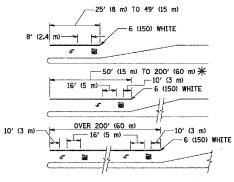


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

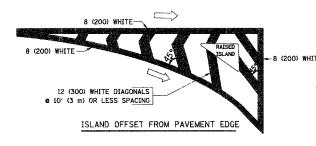


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \P AREA = 15.6 SQ. FT. (1.5 m²) \P AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



CONTRACT NO. 83998 SECTION COUNTY STA. TO STA. FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

8 (200) WHITE - 2 (50) RAISED ISLAND - 2 (50) ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVEDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	AETTOM AETTOM	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 æ 6 (150) 12 (300) æ 45° 12 (300) æ 90°	SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (500) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) e 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) T0 45MPH (70 km/h)) 1150' (45 m) C-C (0VER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

ILLINOIS		REVISIONS
ILL INOIS	DATE	NAME
	03-19-90	EVERS
	10-27-94	T. RAMMACHER
_	10-09-96	ALEX HOUSEH
T	10-17-96	ALEX HOUSEH
	01-06-00	T. RAMMACHER
	I	
SCALE: NONE	I	

S DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

DRAWN BY CADD CHECKED BY

TC-13

NAME SCALE NAME