

VILLAGE OF SCHAUMBURG SCHAUMBURG, ILLINOIS

CONSTRUCTION PLANS FOR SCHAUMBURG REGIONAL AIRPORT

AIRFIELD LIGHTING REHABILITATION: MIRLs, PAPIs,
REILs

ILLINOIS PROJECT: 06C-4837
AIP PROJECT: 3-17-SBGP-184/TBD

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SUMMARY OF QUANTITIES				
ITEM	DESCRIPTION	QUANTITY	UNIT	FIELD QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	10690	FOOT	
AR108706	1/C #6 COUNTERPOISE	9110	FOOT	
AR109210	VAULT MODIFICATIONS	1	L SUM	
AR109331	15 KW REGULATOR, STYLE 1	1	EACH	
AR125400	REPLACE ISOLATION TRANSFORMER	11	EACH	
AR125511	MIRL, BASE MOUNTED - LED	36	EACH	
AR125546	MI THRESHOLD LIGHT BASE MTD-LED	12	EACH	
AR125565	SPLICE CAN	4	EACH	
AR125620	ABBREVIATED PAPI (L-881 SYSTEM)	2	EACH	
AR125908	REMOVE PAPI	2	EACH	
AR150510	ENGINEER'S FIELD OFFICE	1	L SUM	
AR150520	MOBILIZATION	1	L SUM	
AR800080	3 1/2" #4 XLP-USE, 1/C #4 GND IN UD	285	FOOT	
AR800140	2 1/2" #6 XLP-USE, 1/C #10 GND - 1" UD	2220	FOOT	
AR800145	2 1/2" #4 XLP-USE, 1/C #10 GND IN 1-3/4" UD	3200	FOOT	
AR800192	INSTALL ALCMS L-890	1	L SUM	



Know what's below.
Call before you dig.

J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN AIRCRAFT APPROACH CATEGORY: B

DESIGN AIRCRAFT GROUP: I

SCHAUMBURG REGIONAL AIRPORT

TOWNSHIP: 40 NORTH TOWNSHIP: 41 NORTH
RANGE: 10 EAST RANGE: 10 EAST
DUPAGE COUNTY COOK COUNTY
BLOOMINGDALE TOWNSHIP SCHAUMBURG TOWNSHIP
(SECTION: 4) (SECTION: 33)

UNICOM RADIO FREQUENCY: 123.00 MHz

JANUARY 10, 2025

CMT 200256-02
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

DOUGLAS J. KUCYKOWSKI
062-080464
STATE OF ILLINOIS
LICENSED PROFESSIONAL ENGINEER

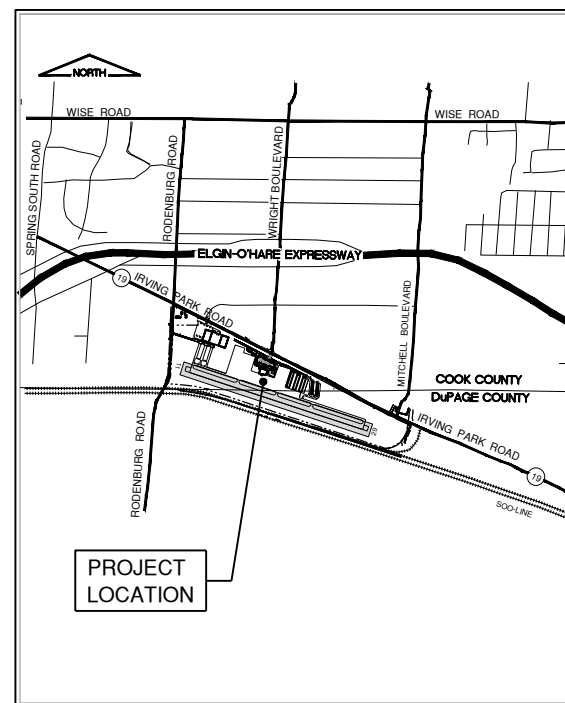
SUBMITTED BY *Dan Kucykowski, PE* EXP. 11/30/25

DATE JANUARY 10, 2025

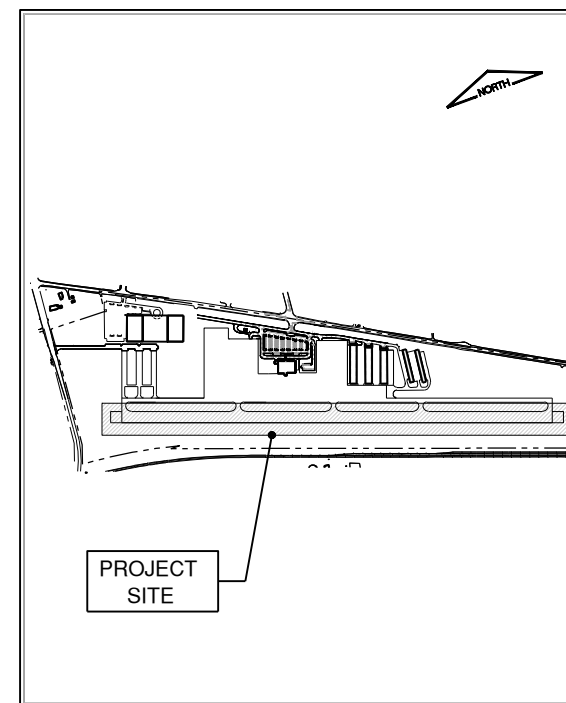
VILLAGE OF SCHAUMBURG
SCHAUMBURG REGIONAL AIRPORT

APPROVED BY *Erik Trydal*
ERIK TRYDAL, AIRPORT MANAGER

DATE JANUARY 10, 2025



LOCATION MAP

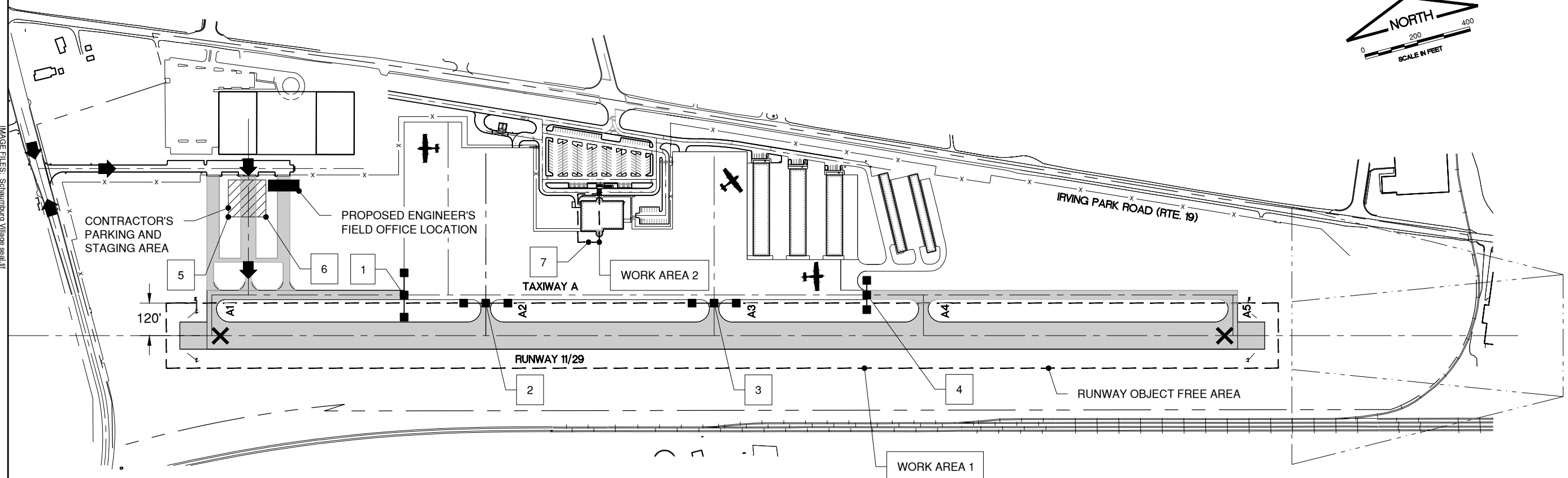
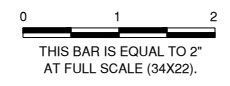


SITE PLAN

DATE: Thursday, January 9, 2025 7:08:27 AM
 FILE: K:\schaumburg\20250250-airfieldlighting\DrawSheets\202502 - Sequence of Construction Plans.dwg
 UPDATE BY: Jim Chise
 LAYOUT: Layout1
 XREF DWG: 01-airfield.dwg
 IMAGE FILES: Schauburg Village seal.tif

IL CONTRACT: **SH030**
 IL LETTING ITEM: **01A**
 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

REVISIONS		
NUMBER	BY	DATE



AIRFIELD IMPACT MATRIX		
WORK AREA	LIMITS OF CLOSURE	WORK ITEMS
1	RUNWAY 11/29 - CLOSED TAXIWAY A EAST OF TERMINAL APRON - CLOSED	AIRFIELD CABLING RUNWAY LIGHT MODIFICATION PAPI INSTALLATION REIL CABLING
2	RUNWAY 11/29 - OPEN TAXIWAY A - OPEN	VAULT IMPROVEMENTS

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS	
RUNWAY	11-29
APPROACH CATEGORY	B
DESIGN GROUP	I
RUNWAY SAFETY AREA WIDTH (RSA)	240'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	240'
TAXIWAY SAFETY AREA WIDTH (TSA)	49'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	89'
RUNWAY OBSTACLE FREE ZONE (ROFZ)	250'

FAA CRITICAL POINT TABLE								
POINT	WORK AREA	NORTHING	EASTING	LATITUDE	LONGITUDE	GROUND ELEVATION	OBSTRUCTION HEIGHT (FT)	ABOVE GROUND ELEVATION
1	1	1939239.743	1046250.83	N 041°59'26.0"	W088°06'19.0"	788'	25'	813'
2	1	1939132.75	1046534.38	N 041°59'25.0"	W088°06'15.2"	788'	25'	813'
3	1	1938914.93	1047347.72	N 041°59'22.9"	W088°06'04.4"	788'	25'	813'
4	1	1938798.00	1047900.47	N 041°59'21.7"	W088°05'57.1"	788'	25'	813'
5	1	1939685.16	1045699.57	N 041°59'30.5"	W088°06'26.2"			
6	1	1939649.23	1045833.73	N 041°59'30.1"	W088°06'24.5"			
7	2	1939251.72	1046958.62	N 041°59'26.2"	W088°06'9.6"			

SUGGESTED SEQUENCE OF CONSTRUCTION

WORK AREA 1

COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.

PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.

COVER AND/OR DE-ENERGIZE SIGNS AND LIGHTS FOR CLOSED TAXIWAYS AND RUNWAYS.

COMPLETE LIGHTING IMPROVEMENTS AND RESTORATION.

CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.

REMOVE BARRICADES AND PAVEMENT CLOSURE MARKERS.

WORK AREA 2

COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.

COMPLETE LIGHTING AND VAULT IMPROVEMENTS

CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.

LEGEND

- WORK AREA LIMITS
- AIRPORT PROPERTY LINE
- EXISTING FENCE
- CONTRACTOR'S ACCESS ROUTE
- RUNWAY CLOSED MARKER
- LOW PROFILE BARRICADES
- AIRFIELD PAVEMENT CLOSED TO TRAFFIC
- CONTRACTOR STAGING AND STORAGE AREA

NOTES

- WORK WITHIN RUNWAY 11/29 RSA AND ROFZ SHALL BE LIMITED TO DAILY CLOSURES. DAILY CLOSURES SHALL BE ANTICIPATED AS 7:00 AM - 4:00 PM, UNLESS NOTED OTHERWISE BY THE AIRPORT.
- RUNWAY AND TAXIWAY CIRCUITS SHALL BE RECONNECTED AT THE END OF THE WORK DAY BY THE CONTRACTOR FOR LIGHTS AND SIGNS TO REMAIN OPERATIONAL.
- SEE SEQUENCE OF CONSTRUCTION PLAN NOTES AND DETAIL SHEETS FOR CONTRACTOR RESPONSIBILITIES FOR CONTRACTOR ACCESS, INCLUDING CLEANING OF PAVEMENTS USED AS HAUL ROUTES AND RESTORATION OF STAGING AND STORAGE AREAS
- CONCRETE WASHOUT AREA SHALL BE LOCATED IN THE STAGING AND STORAGE AREA AND SHALL BE MAINTAINED BY THE CONTRACTOR. COST INCIDENTAL TO THE CONTRACT.

SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs
SEQUENCE OF CONSTRUCTION PLAN

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	MFZ
DRAWN BY:	JRO
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	1/10/2025
JOB No:	200256-02

FINAL

DATE: Thursday, January 9, 2025 7:08:30 AM
FILE: K:\schaumburg\2025\25025602 - Airfield Lighting Draw Sheets\20250202 - Seq Notes And Details - 1.dwg
UPDATE BY: Jim Ohee
LAYOUT: Sequence Of Construction General Notes And Details
MANAGE FILED IN: S:\aeroburg\Drawings\Drawings
XREF DWG: 040101.dwg

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL.
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL EXISTING AND PROPOSED FENCE LINES, EXCEPT AS OTHERWISE NOTED, SHALL BE MAINTAINED AND SHALL SERVE AS CONSTRUCTION AROUND THE PERIMETER OF THE PROJECT. ALL EXISTING GATES SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT OWNER'S REPRESENTATIVE. SHOULD THE CONTRACTOR CHOOSE TO KEEP A GATE OPEN FOR CONSTRUCTION OPERATIONS, A COMPETENT SECURITY GUARD SHALL MONITOR THE OPEN GATE. ANY COST SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT. PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, FLAGGER, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT OR HIS/HER DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD CIRCUITS IMPACTED BY CONSTRUCTION ACTIVITY.

2. PHASING

- TOTAL BASE BID CONTRACT TIME SHALL BE 49 CALENDAR DAYS.
- PHASING SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
- SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING GATE SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS IF APPLICABLE AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
- SUPERVISORY PERSONNEL SHALL DEMONSTRATE IN THE PRESENCE OF THE AIRPORT MANAGER THAT THEY ARE FAMILIAR WITH AIRPORT RADIO AND AIRPORT DRIVING PROCEDURES IN ORDER TO PERFORM WORK. OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. KNOWLEDGE OF THE AIRPORTS PROCEDURES BY THE SUPERVISORY PERSONNEL MUST BE DEMONSTRATED PRIOR TO THE START OF CONSTRUCTION.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFER SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT MONITORING AIRPORT TRAFFIC ON THE RADIO. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT WILL BE DETERMINED AT THE PRECONSTRUCTION MEETING PRIOR TO THE PROJECT START.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2 MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. SEE SECTION 70-17 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-16 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.
- SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- RUNWAY OR TAXIWAY CLOSURES (IF REQUIRED) ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- MARKING AND SIGNAGE FOR THE ACCESS ROUTE SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2 AND 150/5210-5 AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
- ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON TYPE II BARRICADES (2 EACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "UNAUTHORIZED ACCESS NOT ALLOWED".

17. PROTECTION

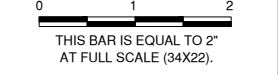
- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.

18. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS.
- THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- THE CONTRACTOR SHALL SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO. IN THE EVENT THAT THE AIRPORT MANAGER NEEDS TO CONTACT THE CONTRACTOR DIRECTLY, THE OPERATOR OF SAID RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND TUNED INTO THE GROUND CONTROL FREQUENCY.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

IL CONTRACT: **SH030**
IL LETTING ITEM: **01A**
IL PROJECT: **06C-4837**
S.B.G. PROJECT: 3-17-SBGP-184/TBD

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**SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILS**

**SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS - 1**

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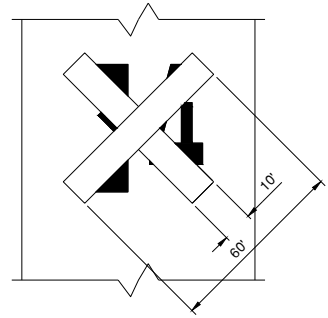
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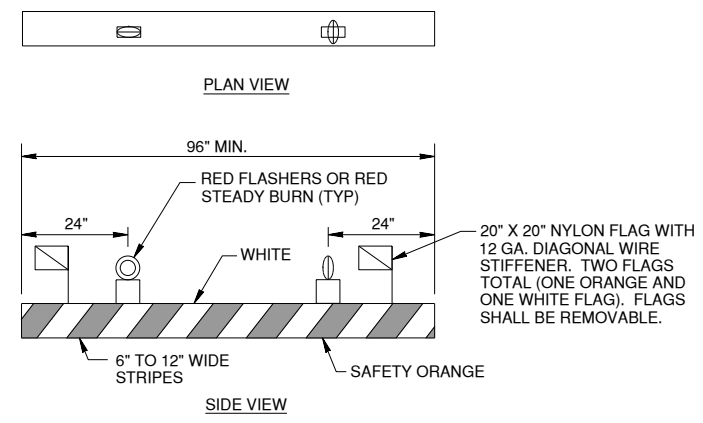
SHEET 4 OF 15 SHEETS

CLOSED RUNWAY MARKER NOTES

1. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING YELLOW CROSSES AT THE LOCATION AND DIMENSIONS DETAILED ON THE SEQUENCE OF CONSTRUCTION AND PER AC 150/5370-2 (LATEST EDITION). THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.
2. TEMPORARY CLOSED RUNWAY MARKERS SHALL BE YELLOW.
3. TEMPORARY MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
4. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
6. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**TEMPORARY
CLOSED RUNWAY MARKER DETAIL**
ON PAVEMENT - NO SCALE



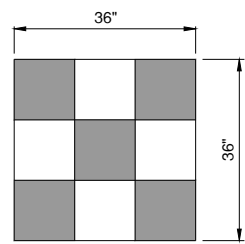
AIRSIDE LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

AIRFIELD LIGHTS AND SIGNS NOTES

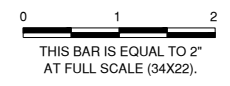
1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.



**CONSTRUCTION EQUIPMENT
AND TRUCK SIGNAL FLAG**
NOT TO SCALE

IL CONTRACT: **SH030**
IL LETTING ITEM: **01A**
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**SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs**

**SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS - 2**

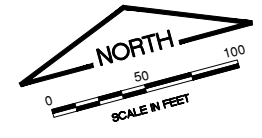
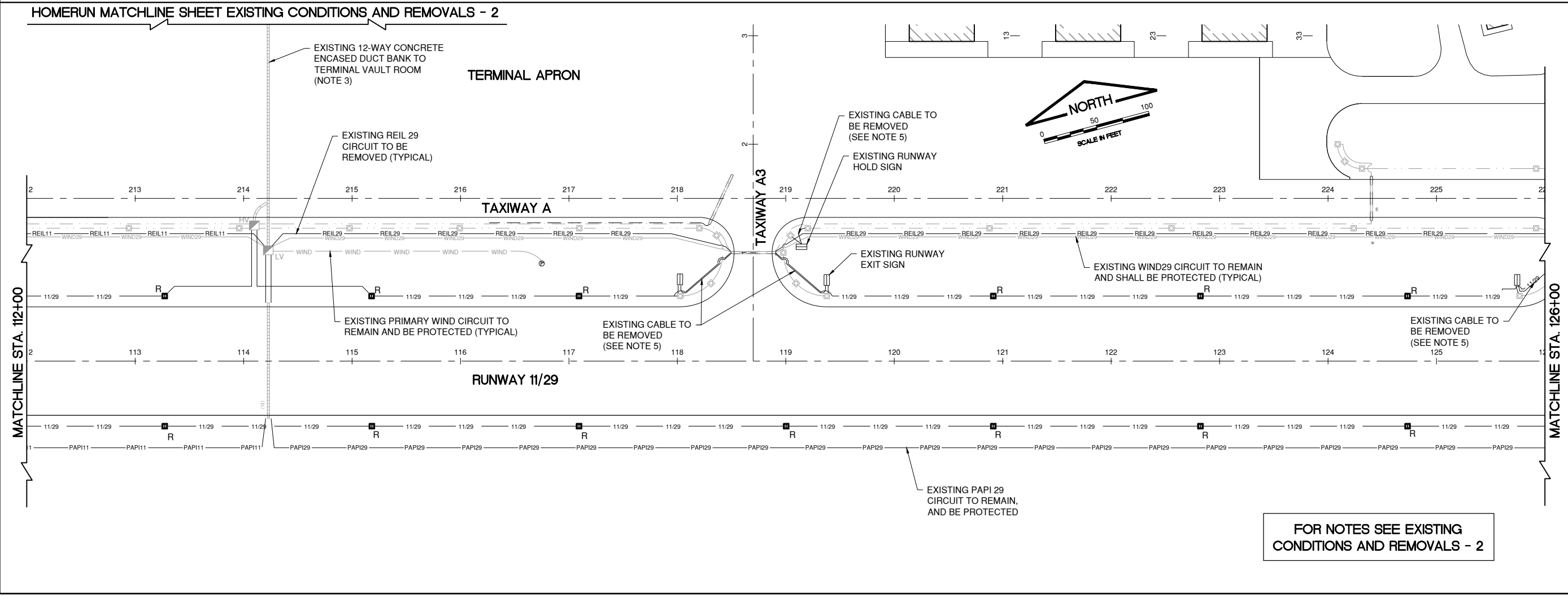
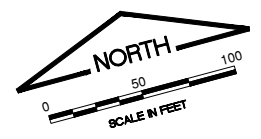
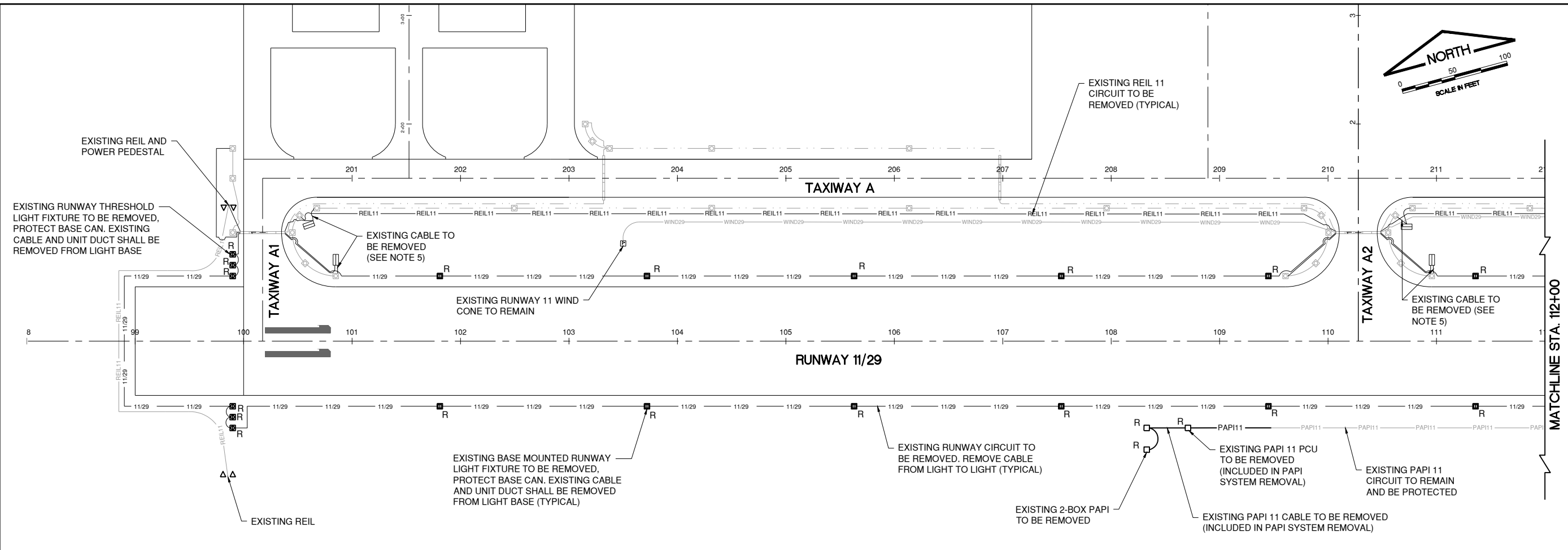
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DATE: Thursday, January 9, 2025 7:08:41 AM
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 UPDATE BY: Jim Chise
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 BASE: runway-airfield.dwg



FOR NOTES SEE EXISTING CONDITIONS AND REMOVALS - 2

IL CONTRACT: **SH030**
 IL LETTING ITEM: **01A**
 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

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NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs**

EXISTING CONDITIONS AND REMOVALS - 1

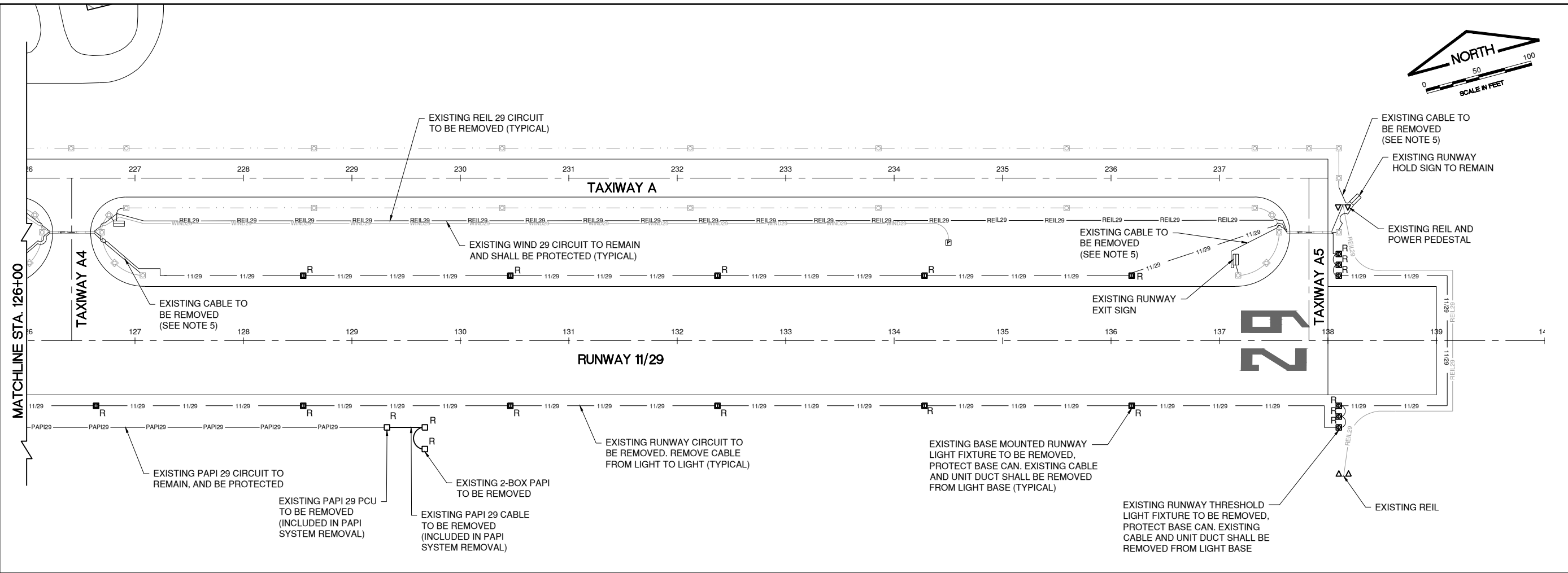
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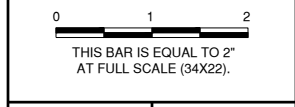
SHEET 6 OF 15 SHEETS

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IL CONTRACT: **SH030**
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 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

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NUMBER	BY	DATE

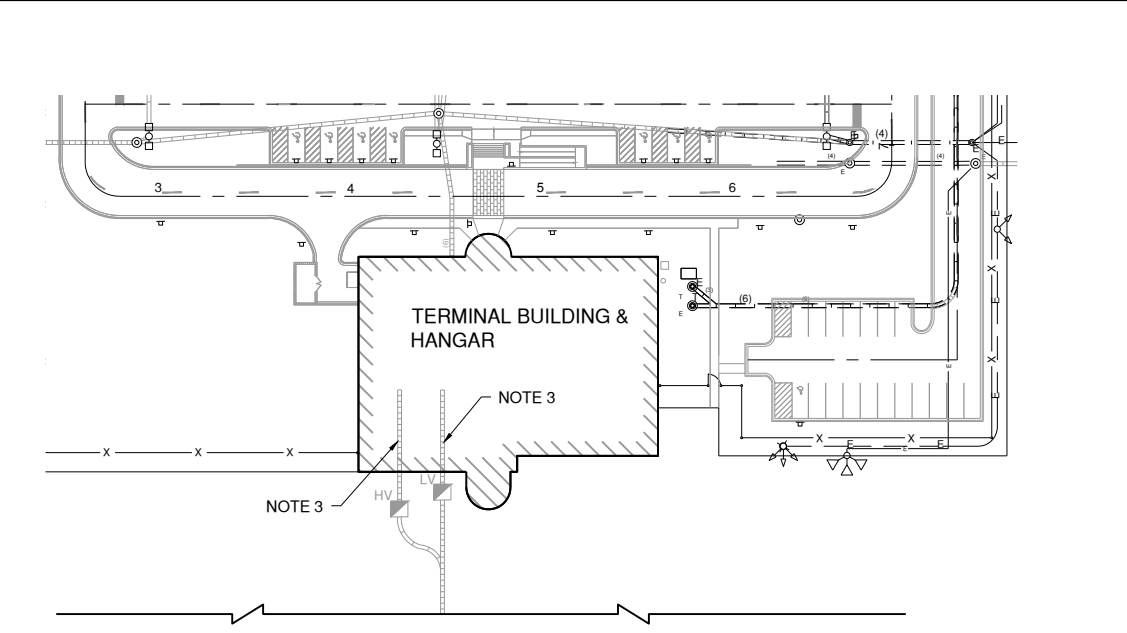


SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs
EXISTING CONDITIONS AND REMOVALS - 2

LEGEND:

	EXISTING BASE MOUNTED RUNWAY EDGE LIGHT
	EXISTING BASE MOUNTED THRESHOLD LIGHT
	EXISTING TAXIWAY EDGE LIGHT
	EXISTING HANDHOLE
	EXISTING AIRFIELD SIGN
	EXISTING SUPPLEMENTAL WINDCONE
	EXISTING REIL
	EXISTING PAPI 2-BOX
	EXISTING RUNWAY CIRCUIT TO BE REMOVED
	EXISTING TAXIWAY CIRCUIT
	EXISTING WINDCONE CIRCUIT
	EXISTING REIL CIRCUIT TO BE REMOVED
	EXISTING UNDERDRAIN
	EXISTING ELECTRICAL DUCT
	EXISTING HOME RUN ELECTRICAL DUCT
	ITEM TO BE REMOVED
	EXISTING PAPI CIRCUIT

- GENERAL NOTES (ALL SHEETS):**
- EXISTING LIGHTS, AND AIRFIELD CIRCUITS, OTHER THAN THOSE INDICATED FOR REMOVAL, ARE TO BE PROTECTED FROM DAMAGE AND REMAIN OPERATIONAL. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT THEIR EXPENSE.
 - LIGHT FIXTURES, LAMPS, PAPIs AND TRANSFORMERS TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT. LIGHT BASES SHALL REMAIN AT EXISTING LOCATION.
 - EXISTING CABLE FOR RUNWAY 11/29 LIGHTING, REILs AND PAPIs INCLUDING HOMERUN CIRCUITS (FOR LIGHTING AND REILs) SHALL BE REMOVED BACK TO SOURCE/VAULT. PROTECT ALL EXISTING CIRCUITS TO REMAIN. CONTRACTOR SHALL REMOVE EXISTING CABLES FROM ALL ITEMS TO BE REMOVED. COST SHALL BE INCIDENTAL TO THE CONTRACT.
 - ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
 - RUNWAY HOLD AND EXIT SIGNS SHALL BE RECABLED AND CONNECTED TO RUNWAY CIRCUIT. CONTRACTOR SHALL DISCONNECT THE SIGN FROM THE TAXIWAY CIRCUIT AND REMOVE CABLE FROM EXISTING SIGN TO ADJACENT LIGHT ON EITHER SIDE OF SIGN. COST OF REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.
 - SYMBOLS FOR PROPOSED LIGHTS AND TAXIWAY GUIDANCE SIGNS ARE ENLARGED FOR CLARITY.



HOMERUN MATCHLINE SHEET EXISTING CONDITIONS AND REMOVALS - 1

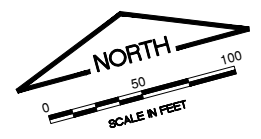
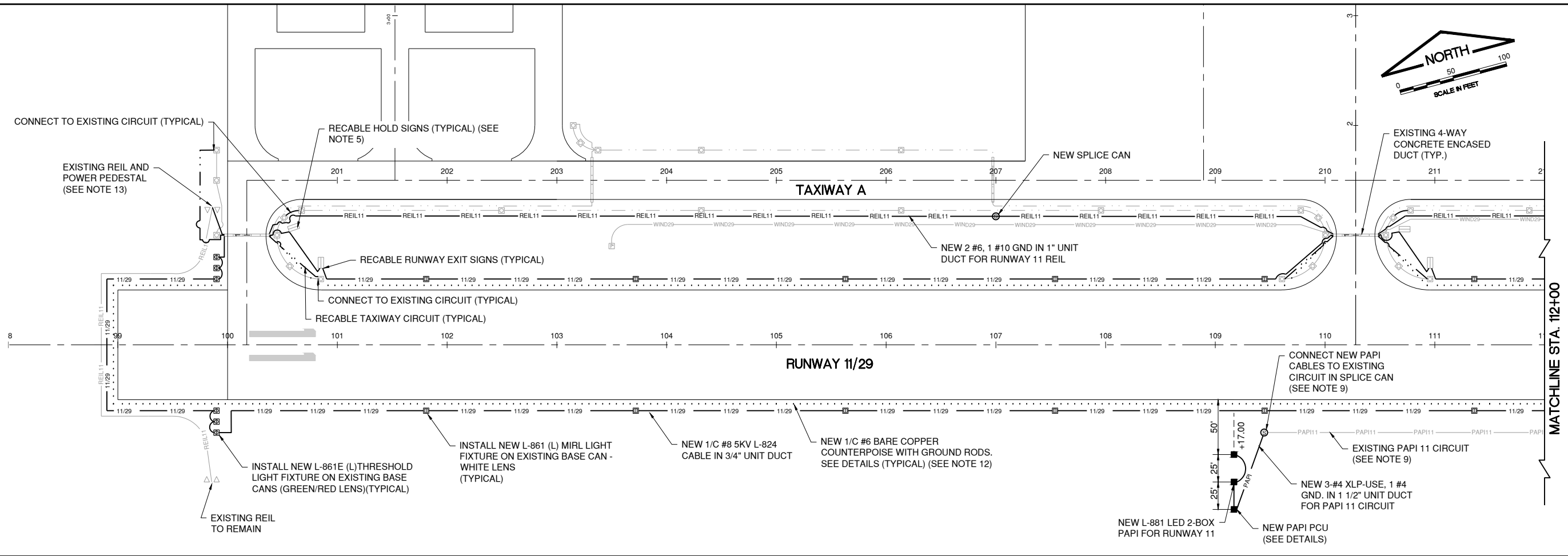
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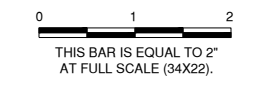
SHEET 7 OF 15 SHEETS

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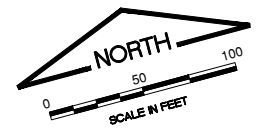
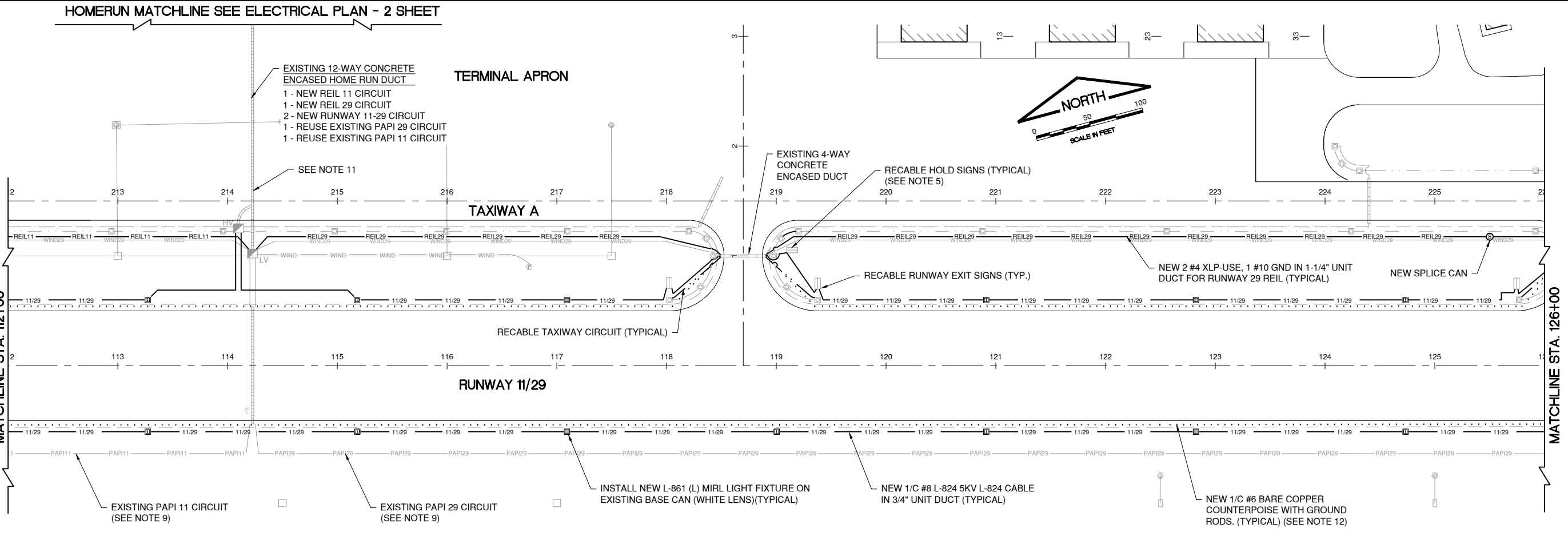


IL CONTRACT: **SH030**
 IL LETTING ITEM: **01A**
 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

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NUMBER	BY	DATE



SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs
ELECTRICAL PLAN - 1

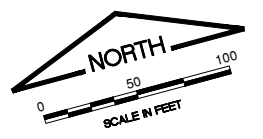
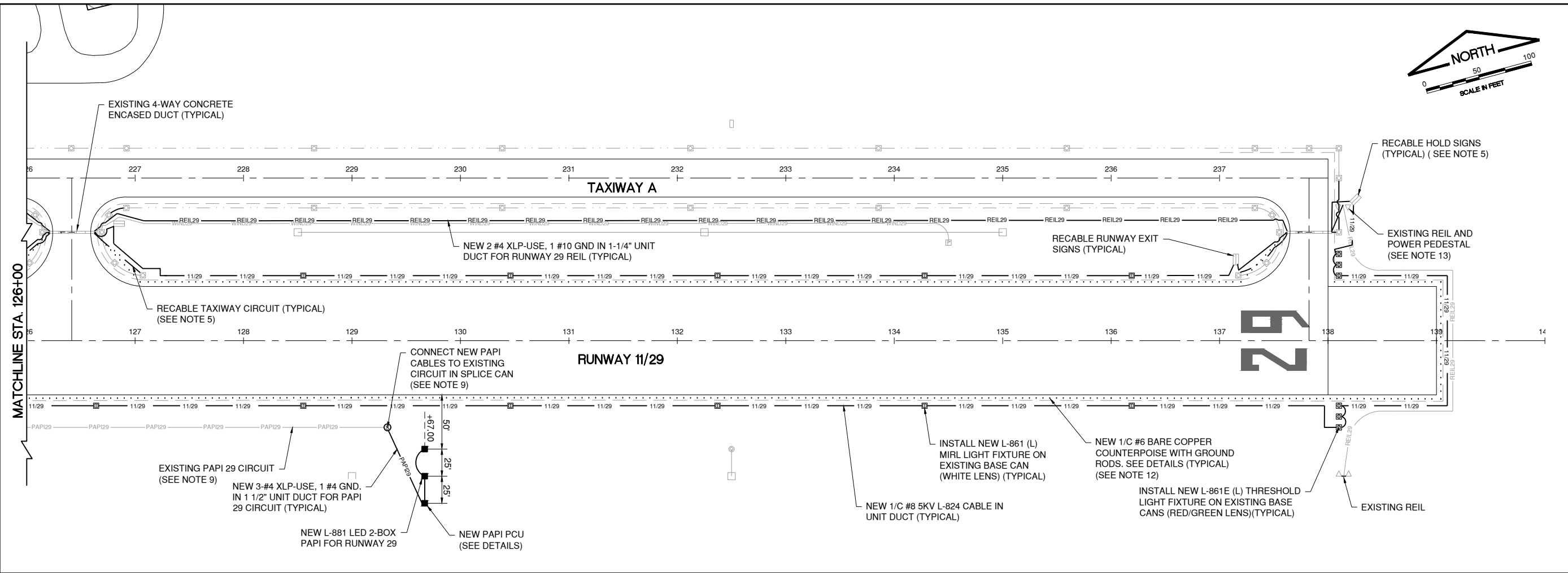


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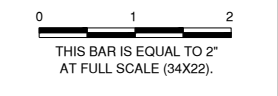
FOR NOTES SEE ELECTRICAL PLAN - 2

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 UPDATE BY: Jim Ohee
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I.L. CONTRACT: SH030
 I.L. LETTING ITEM: 01A
 I.L. PROJECT: 06C-4837
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

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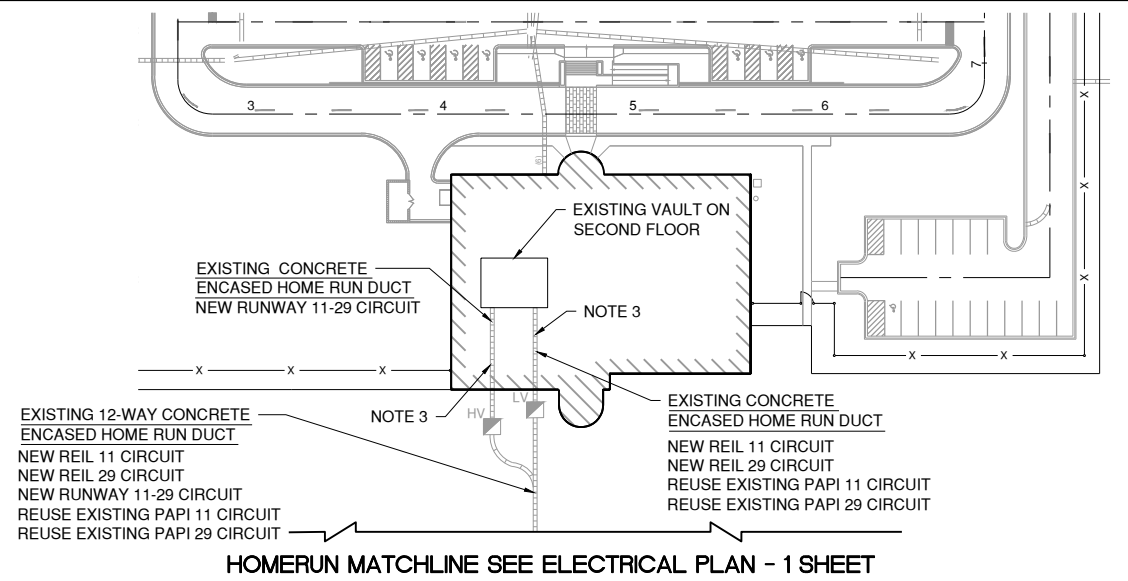


**SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs**
ELECTRICAL PLAN - 2

LEGEND:

	NEW BASE MOUNTED RUNWAY EDGE LIGHT (LED)
	NEW BASE MOUNTED THRESHOLD LIGHT (LED)
	EXISTING TAXIWAY EDGE LIGHT
	EXISTING HANDHOLE
	EXISTING AIRFIELD SIGN
	EXISTING SUPPLEMENTAL WINDCONE
	EXISTING REIL
	NEW LED PAPI 2-BOX
	NEW RUNWAY 11-29 CIRCUIT 1 - 1/C #8 L-824 5KV UG CABLE IN UNIT DUCT
	EXISTING TAXIWAY CIRCUIT
	NEW COUNTERPOISE BARE #6 WITH GROUND RODS
	EXISTING WINDCONE CIRCUIT
	NEW REIL 11 CIRCUIT
	NEW REIL 29 CIRCUIT
	NEW PAPI 29 CIRCUIT
	NEW PAPI 11 CIRCUIT
	EXISTING ELECTRICAL DUCT
	EXISTING HOME RUN ELECTRICAL DUCT
	NEW SPLICE CAN
	NEW TAXIWAY CIRCUIT 1 - 1/C #8 L-824 5KV UG CABLE IN UNIT DUCT

- GENERAL NOTES (ALL SHEETS):**
- EXISTING LIGHTS, AND AIRFIELD CIRCUITS, OTHER THAN THOSE INDICATED FOR REMOVAL, ARE TO BE PROTECTED FROM DAMAGE AND REMAIN OPERATIONAL. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT THEIR EXPENSE.
 - ALL SALVAGED ITEMS SHALL BE REMOVED AND TURNED OVER TO THE AIRPORT. ITEMS SHALL BE TRANSPORTED BY THE CONTRACTOR TO THE LOCATION DESIGNATED BY THE AIRPORT. IF THE AIRPORT DOES NOT WANT ANY OF THE REMOVED MATERIALS THEN THE CONTRACTOR SHALL DISPOSE OF OFF AIRPORT PROPERTY AT NO ADDITIONAL COST TO THE CONTRACTOR.
 - CONTRACTOR SHALL REMOVE EXISTING CABLES FROM ALL ITEMS TO BE REMOVED. COST SHALL BE INCIDENTAL TO THE CONTRACT.
 - ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
 - GUIDANCE AND HOLD SIGNS SHALL BE RECABLED AND CONNECTED TO THE RUNWAY CIRCUIT USING 1/C #8 L-824 5KV CABLE IN UD UNLESS NOTED OTHERWISE. CIRCUITS ROUTED THROUGH EXISTING CONDUITS SHALL BE CABLE IN UNIT DUCT UNLESS OTHERWISE NOTED. INSTALL NEW 1/C #6 COUNTERPOISE. REPLACE EXISTING L-830 ISOLATION TRANSFORMERS AND L-823 SPLICES. MATCH EXISTING SIZE TRANSFORMERS.
 - COUNTERPOISE WITH GROUND RODS SHALL BE INSTALLED ON ALL NEW 5KV ELECTRICAL CABLING UNLESS OTHERWISE NOTED. SEE ELECTRICAL DETAIL SHEETS FOR DETAILS.
 - FOR CLARITY, UNDERGROUND UTILITIES ARE NOT SHOWN. REFER TO EXISTING CONDITION SHEETS FOR UTILITIES LOCATION. CONTRACTOR IS RESPONSIBLE TO LOCATE ALL UNDERGROUND UTILITIES. ANY DAMAGE TO EXISTING EQUIPMENT OR UTILITIES DUE TO CONTRACTOR NEGLIGENCE SHALL BE REPAIRED IMMEDIATELY AT THEIR EXPENSE.
 - SYMBOLS FOR PROPOSED LIGHTS AND TAXIWAY GUIDANCE SIGNS ARE ENLARGED FOR CLARITY.
 - SPLICE NEW PAPI POWER CABLES TO EXISTING PAPI CIRCUIT INSIDE NEW SPLICE CAN. ALL SPLICES SHALL BE WATERPROOF AND RATED FOR 600V. EXISTING PAPI CABLES TO REMAIN AND BE REUTILIZED.
 - REPLACE BASE MOUNTED RUNWAY EDGE AND THRESHOLD LIGHTS. REUSE EXISTING LIGHT BASE CAN AND CONNECTING CONDUIT AT CURRENT LOCATION. ANY BASE CAN OR CONDUIT DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
 - REMOVE EXISTING CIRCUITS AND INSTALL NEW RUNWAY 11/29 CIRCUIT IN HIGH VOLTAGE DUCT BANK AND REIL 11 AND REIL 29 CIRCUITS IN LOW VOLTAGE DUCT BANK TO EXISTING VAULT. LABEL ALL CIRCUITS INSIDE LOW VOLTAGE AND HIGH VOLTAGE HANDHOLES
 - COUNTERPOISE SHALL BE INSTALLED FREE OF OBSTRUCTIONS FROM EXISTING UTILITIES. IF A CONFLICT OF UTILITIES WERE TO ARISE, THE PLACEMENT OF THE COUNTERPOISE SHALL BE APPROVED BY THE ENGINEER IN THE FIELD.
 - TERMINATE NEW REIL CIRCUIT AT EXISTING REIL MASTER UNIT. UTILIZE EXISTING CONDUIT STUBS. COST OF CABLE TERMINATION AND TESTING OF REIL'S SHALL BE INCIDENTAL TO INSTALLATION OF NEW CIRCUIT.

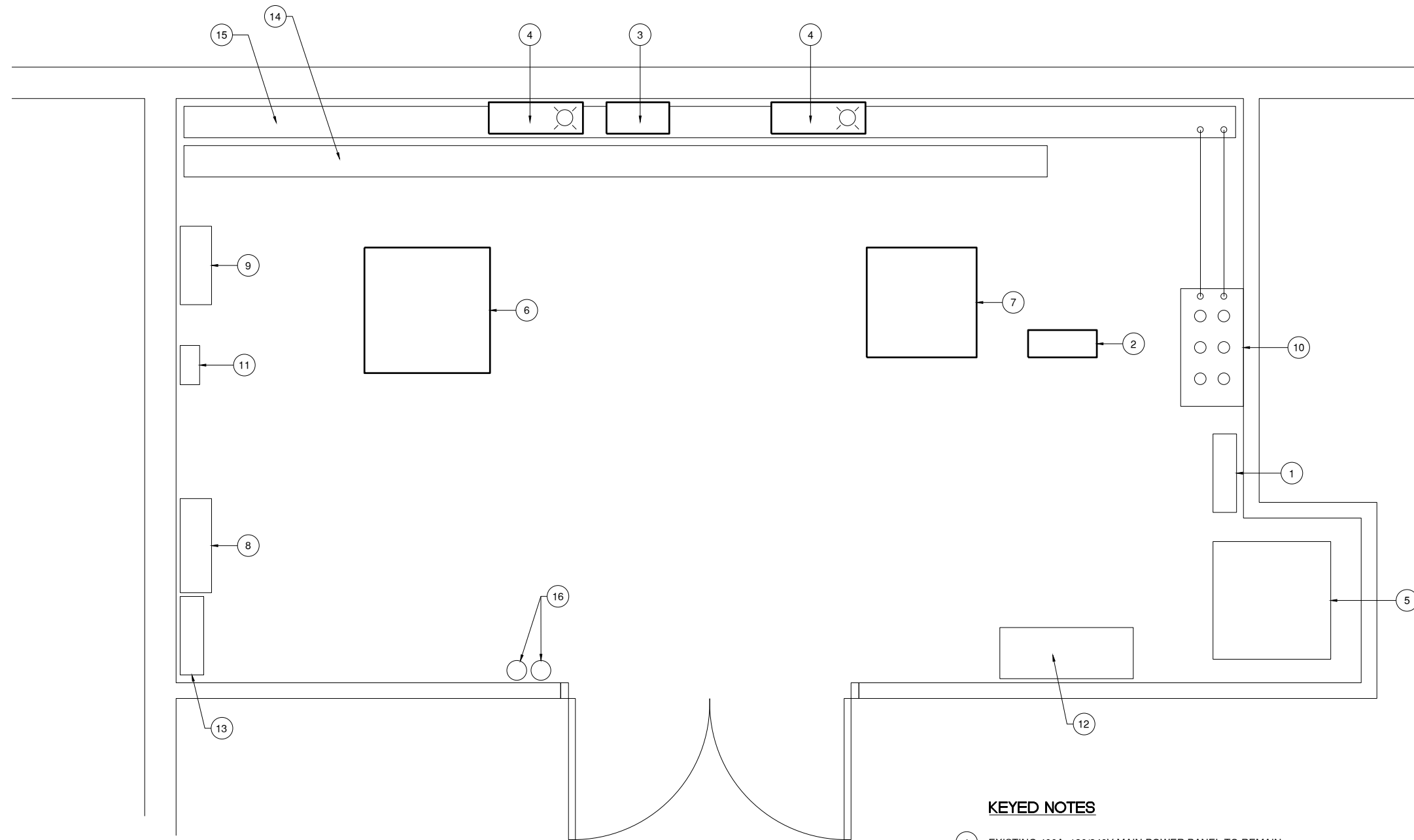


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 SHEET 9 OF 15 SHEETS

DATE: Thursday, January 9, 2025 7:08:57 AM
 FILE: K:\schaumburg\2025025600_airsfieldlighting\DrawSheets\2025025602_Vault_Plan_1.dwg
 UPDATE BY: Jim Chese
 LAYOUT: Vault Existing Conditions
 IMAGE FILES: Schaumburg_Village_scault.dwg
 XREF DWG: scault.dwg



EXISTING FLOOR PLAN FOR VAULT
NOT TO SCALE

NOTES

1. EQUIPMENT NOT CALLED OUT TO BE REMOVED SHALL REMAIN UNLESS NOTED OTHERWISE.
2. EXISTING EQUIPMENT, CONDUCTORS AND CONDUIT TO BE REMOVED SHALL BE DISPOSED OF OFF-SITE.
3. EXISTING CONDUCTORS AND CONDUIT SHALL BE REMOVED WITH EXISTING EQUIPMENT TO BE REMOVED. CONDUITS SHALL BE RE-USED UNLESS NOTED OTHERWISE.
4. ALL SALVAGED ITEMS SHALL BE REMOVED AND TURNED OVER TO THE AIRPORT. ITEMS SHALL BE TRANSPORTED BY THE CONTRACTOR TO THE LOCATION DESIGNATED BY THE AIRPORT. IF THE AIRPORT DOES NOT WANT ANY OF THE REMOVED MATERIALS THEN THE CONTRACTOR SHALL DISPOSE OF OFF AIRPORT PROPERTY AT NO ADDITIONAL COST TO THE CONTRACT.
5. EXISTING VAULT POWER DISTRIBUTIONS AND GROUNDING SYSTEM SHALL REMAIN AND SHALL BE PROTECTED FROM ANY DAMAGE DURING CONSTRUCTION.

KEYED NOTES

- 1 EXISTING 400A, 120/240V MAIN POWER PANEL TO REMAIN.
- 2 EXISTING REGULATOR DOUBLE THROW SAFETY SWITCH TO BE REMOVED.
- 3 EXISTING CUTOUPS ENCLOSURE TO BE REMOVED.
- 4 EXISTING CUTOOUT AND INDICATING LIGHT FOR REGULATOR TO BE REMOVED.
- 5 EXISTING 100KVA, 208-120/240VAC, TRANSFORMER TO REMAIN.
- 6 EXISTING 15KW RUNWAY 11/29, TAXIWAY A REGULATOR TO BE RELOCATED AND USED AS TAXIWAY A REGULATOR.
- 7 EXISTING 15KW SPARE REGULATOR TO REMAIN.
- 8 EXISTING LIGHTING CONTACTOR CONTROL PANEL TO REMAIN AND REWIRED TO NEW ALCMS.
- 9 EXISTING RADIO CONTROL INTERFACE TO BE REMOVED.
- 10 EXISTING PULL BOX TO REMAIN.
- 11 EXISTING L-854 RADIO CONTROLLER TO REMAIN.
- 12 EXISTING VAULT MAIN DISCONNECT TO REMAIN.
- 13 EXISTING DISTRIBUTION PANELBOARD FOR NAVAIDS TO REMAIN.
- 14 HIGH VOLTAGE WIREWAY TO REMAIN.
- 15 EXISTING LOW VOLTAGE WIREWAY TO REMAIN.
- 16 EXISTING FIRE EXTINGUISHER TO REMAIN.

IL CONTRACT: **SH030**
 IL LETTING ITEM: **01A**
 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

REVISIONS

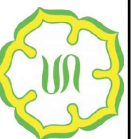
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THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILS
VAULT EXISTING CONDITIONS

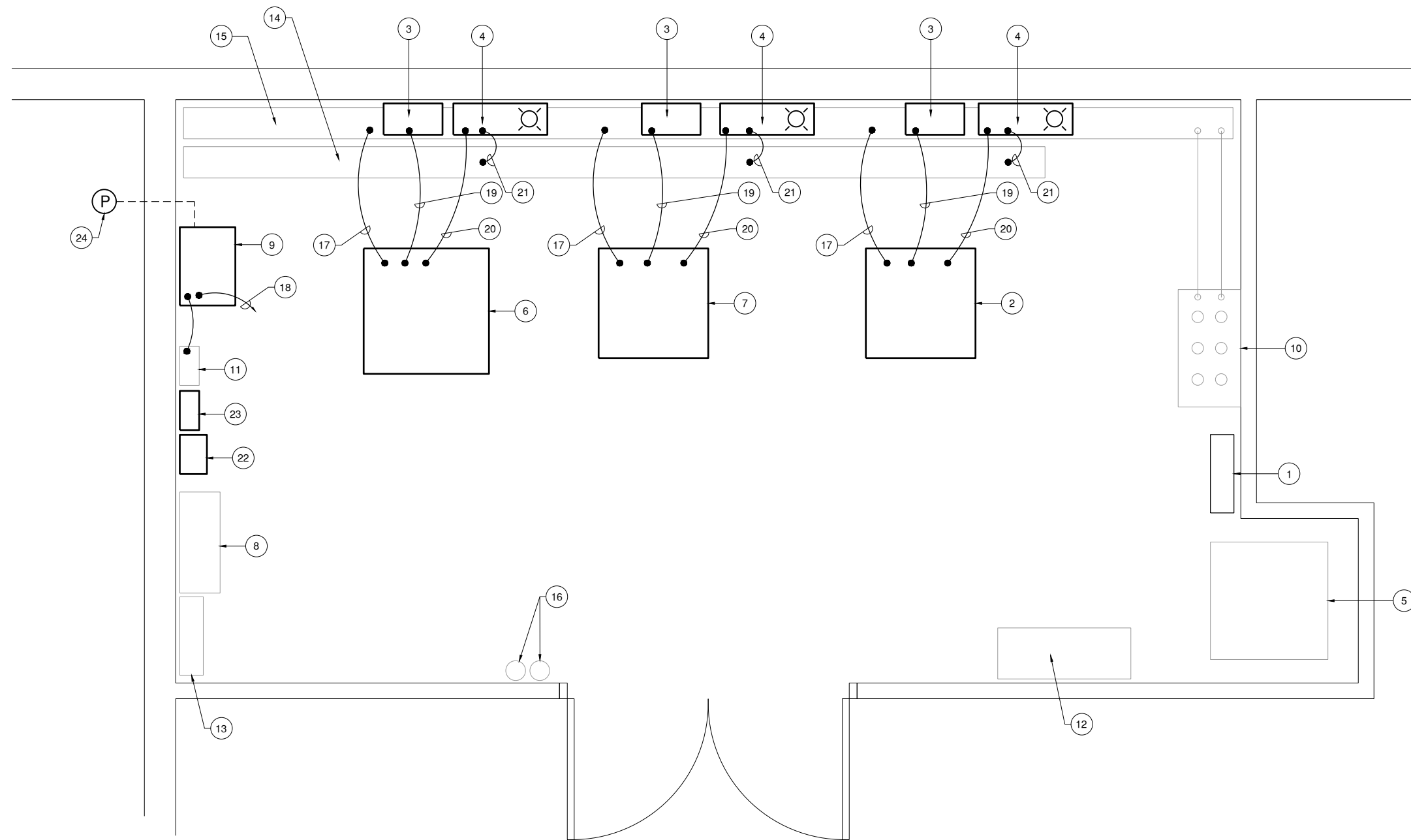
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 DRAWN BY: JRO
 CHECKED BY: AB
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 DATE: 1/10/2025
 JOB No: 200256-02

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NEW FLOOR PLAN FOR VAULT
NOT TO SCALE

KEYED NOTES

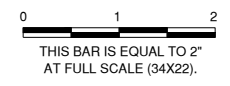
- 1 EXISTING 400A, 120/240V MAIN POWER PANEL.
- 2 EXISTING 15KW SPARE REGULATOR.
- 3 NEW WALL MOUNTED INTERFACE UNIT FOR ALCMS, AS REQUIRED BY ALCMS MANUFACTURER.
- 4 NEW CUTOUT AND INDICATOR LIGHT FOR REGULATOR. MOUNTED IN 12" x 12" x 8" ENCLOSURE (SEE DETAIL).
- 5 EXISTING 100KVA, 208-120/240VAC, TRANSFORMER.
- 6 NEW 15KW, 240V, 3-STEP REGULATOR FOR RUNWAY 11/29.
- 7 RELOCATED 15KW 240V, 3-STEP REGULATOR FOR TAXIWAY A.
- 8 EXISTING LIGHTING CONTACTOR CONTROL PANEL (NOTE 5).
- 9 NEW L-890 AIRFIELD LIGHTING CONTROL AND MONITORING (ALCMS) CABINET AND TOUCH SCREEN.
- 10 EXISTING PULL BOX.
- 11 EXISTING L-854 RADIO CONTROLLER (NOTE 6).
- 12 EXISTING VAULT MAIN DISCONNECT.
- 13 EXISTING DISTRIBUTION PANELBOARD FOR NAVAIDS.
- 14 EXISTING HIGH VOLTAGE WIREWAY.
- 15 EXISTING LOW VOLTAGE WIREWAY.
- 16 EXISTING FIRE EXTINGUISHER.
- 17 NEW 2 #2 THWN, 1 #6 GND. IN 1" FLEXIBLE METALLIC CONDUIT TO 120/240V POWER PANEL.
- 18 NEW 2 #12 THWN, 1 #12 GND. IN 1" CONDUIT TO 120/240V POWER PANEL.
- 19 NEW CONTROL AND COMMUNICATION CABLES IN FLEXIBLE CONDUIT AS RECOMMENDED BY ALCMS MANUFACTURER.
- 20 NEW (2) 1/C #8, 5KV, TYPE C CABLES IN 1" FLEXIBLE METALLIC CONDUIT TO INDICATING LIGHT EQUIPMENT.
- 21 NEW (2) 1/C #8, 5KV, TYPE C CABLES IN 1" FLEXIBLE METALLIC CONDUIT TO EDGE LIGHT (NOTE 7).
- 22 NEW WALL MOUNTED INTERFACE UNITS TO INTERFACE EXISTING CONTACTORS FOR WINDCONE, PAPI, REIL AND BEACON CIRCUITS WITH NEW ALCMS.
- 23 NEW WALL MOUNTED INTERFACE UNIT TO INTERFACE EXISTING L-854 RADIO CONTROLLER WITH ALCMS.
- 24 NEW PHOTCELL AND INTERFACE WITH ALCMS.

NOTES

1. EXISTING EQUIPMENT TO REMAIN IS SHOWN FOR INFORMATION ONLY. NEW WORK IS SHOWN IN BOLD.
2. INSTALL LAMACOID NAMEPLATES ON ALL EQUIPMENT.
3. INSTALL NEW CIRCUIT BREAKERS AS SHOWN ON PANELBOARD SCHEDULE.
4. CONTRACTOR SHALL VERIFY EXISTING AIRFIELD LIGHTING CIRCUITS AND LABEL ALL NEW AND EXISTING CIRCUITS IN HANDHOLE, PULL BOX AND WIREWAY.
5. CONNECT EXISTING "AUTO" SELECTOR SWITCHES TO ALCMS INTERFACE FOR ALL EXISTING CIRCUITS.
6. CONNECT EXISTING OUTPUTS OF L-854 RADIO CONTROLLER TO ALCMS INTERFACE.
7. CONNECT RUNWAY 11/29 AND TAXIWAY CIRCUITS TO HOMERUNS IN HV WIREWAY WITH L-823 CONNECTORS. INSTALL L-823 CONNECTORS FOR SPARE CIRCUITS. PROVIDE SLACK FOR SPARE REGULATOR TO BE CONNECTED TO AIRFIELD LIGHTING CIRCUITS. LABEL ALL CIRCUITS.
8. INSTALL NEW PHOTOCCELL FOR DAY/NIGHT MODE OF ALCMS.
9. CONNECT NEW REIL CIRCUITS TO EXISTING CONTACTORS.

IL. CONTRACT: **SH030**
IL. LETTING ITEM: **01A**
IL. PROJECT: **06C-4837**
S.B.G. PROJECT: 3-17-SBGP-184/TBD

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NUMBER	BY	DATE



**SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs
VAULT IMPROVEMENTS**

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APPROVED BY:	DJK
DATE:	1/10/2025
JOB No:	200256-02

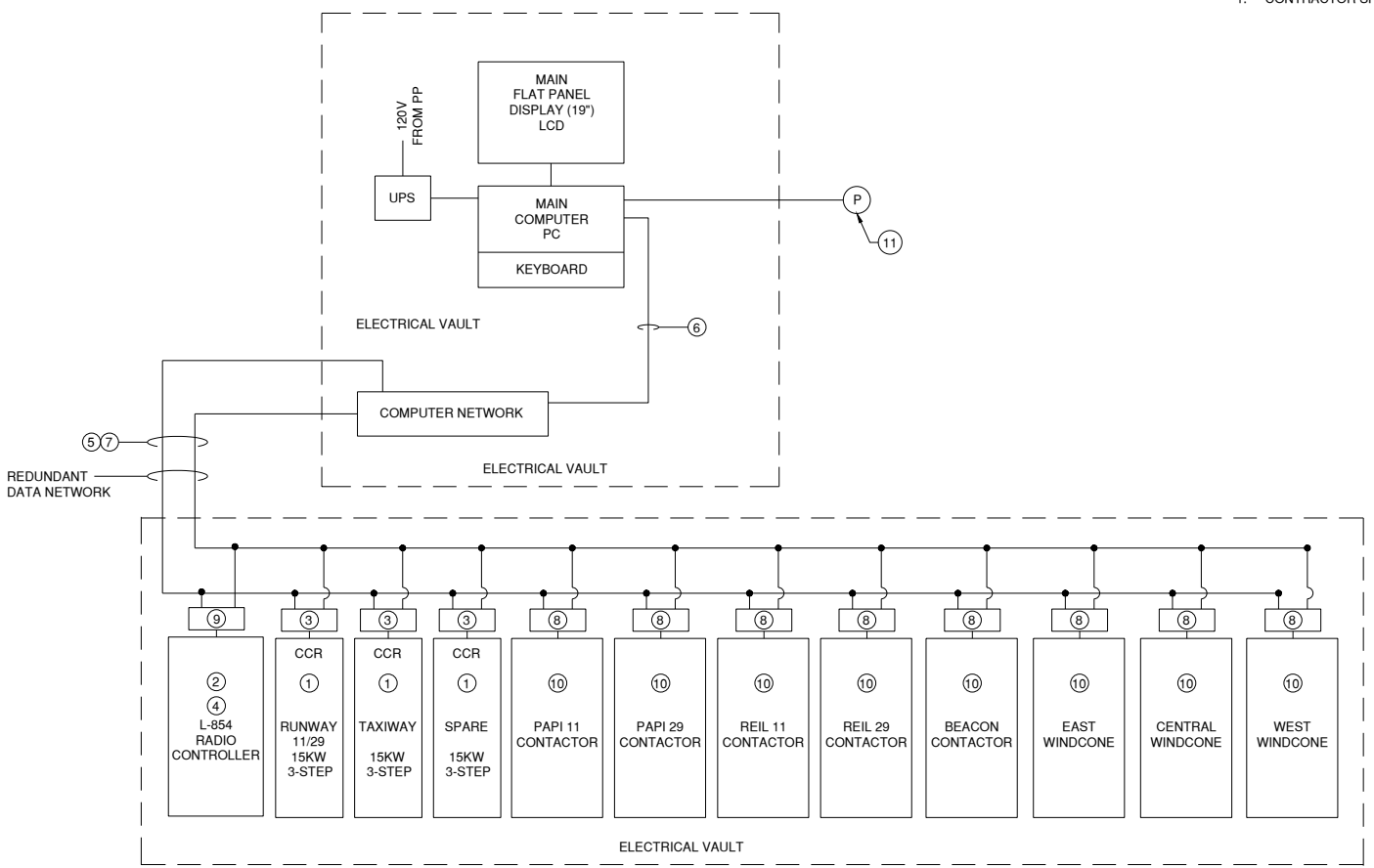
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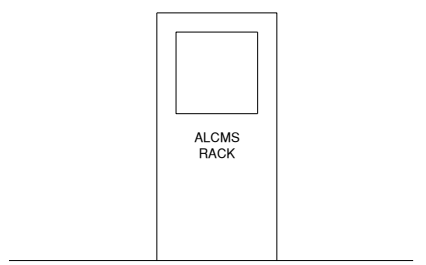
- 1 NEW AND RELOCATED CONSTANT CURRENT REGULATOR. MANUFACTURER AND CONTRACTOR SHALL PROVIDE ALL REQUIRED CT'S AND INTERFACE MODULES FOR COMPLETE L-829 AND L-890 SYSTEM.
- 2 PROVIDE ALL REQUIRED TRANSCEIVER AND INTERFACING PANEL FOR BEACON AND RADIO CONTROLLER CONTROL EQUIPMENT.
- 3 CONSTANT CURRENT REGULATOR CCR MICROPROCESSOR BASED CONTROL/MONITOR PANEL WITH REMOTE AND LOCAL CONTROL FOR CCR STATUS, DIMMING CONTROL AND CABLE INSULATION MONITOR.
- 4 COORDINATE ALL RADIO INTERFACING EQUIPMENT WITH OWNER. PROVIDE ALL NETWORK INTERFACING HARDWARE AND EQUIPMENT. INCLUDING SOFTWARE AND PROGRAMMING.
- 5 PROVIDE UPS POWER FOR DCME UNITS.
- 6 CAT 6 DATA LINE IN CONDUIT TYPICAL U.O.N. COORDINATE ALL WORK WITH ALCMS MANUFACTURER (CONSTANT CURRENT REGULATOR MANUFACTURER). PROVIDE ALL REQUIRED DATA CONNECTION LINKS.
- 7 24 AWG. SHIELDED TWISTED PAIR WITH A COMMON, MEETING EIARS-485 APPLICATIONS (BELDEN 9842) OR AS REQUIRED BY ALCMS MANUFACTURER.
- 8 MICROPROCESSOR BASED CONTROL/MONITOR PANEL WITH REMOTE AND LOCAL CONTROL (DCME) FOR EXISTING CONTACTORS.
- 9 MICROPROCESSOR BASED CONTROL/MONITOR PANEL WITH REMOTE AND LOCAL CONTROL (DCME) FOR L-854 RADIO CONTROLLER.
- 10 EXISTING CONTACTORS WIRED TO HAND-OFF-AUTO SELECTOR SWITCH INTERFACE "AUTO" MODE TO ALCMS.
- 11 NEW PHOTOCELL FOR DAY AND NIGHT OPERATIONS.

PANELBOARD SCHEDULE															
PANEL DESIGNATION: PP				BOND NEUTRAL AND GROUND BAR: NO				POLE: 42							
LOCATION: VAULT				NEUTRAL BUS RATING: N/A				SHORT CIRCUIT RATING: 42KA							
MFR & TYPE: SQUARE D				SERVICE ENTRANCE RATED: NO				SERIES OR FULLY RATED: SERIES							
				TVSS & DISCONNECT REQUIRED: NO											
VOLTS: 120/240V				MOUNTING: SURFACE				BUS RATING (AMPS): 400							
PHASE: 1				ENCL RATING: NEMA 1				BUS: COPPER							
WIRE: 3								MAIN CIRCUIT BREAKER: 400/2							
CKT NO	LOAD	BREAKER SIZE	LOAD AMPS	USAGE FACTOR	PHASE AMPS A	PHASE AMPS B	POLE NO	PHASE AMPS A	PHASE AMPS B	USAGE FACTOR	LOAD AMPS	BREAKER SIZE	LOAD	CKT NO	
1	NEW 15KW REGULATOR RWY 11-29	100A/2P	63	0.5	31.5	31.5	1 2	31.5	31.5	0.5	63	100A/2P	RELOCATED 15 KW REGULATOR TWYA	2	
3			63	0.5			3 4			0.5	63			4	
5	RELOCATED 15KW REGULATOR SPARE	100A/2P	63	0.5	31.5	31.5	5 6	0	0			15A/2P	REIL 11	6	
7			63	0.5			7 8							8	
9	REIL 29	15A/2P			0	0	9 10	0	0			15A/2P	EAST WINDCONE	10	
11							11 12	0	0					12	
13	CENTRAL WINDCONE	15A/2P			0	0	13 14	0	0			15A/2P	WEST WINDCONE	14	
15							15 16	0	0					16	
17	L-821 PANEL	15A/1P			0	0	17 18	0	0			45A/2P	FUTURE 15KW REGULATOR	18	
19	FUTURE 15KW REGULATOR	45A/2P					19 20	0	0					20	
21					0	0	21 22	0	0			20A/2P	BEACON LIGHT	22	
23	EAST GATE	20A/2P			0	0	23 24	0	0					24	
25							25 26	0	0			20A/2P	WEST GATE	26	
27	L-890 ALCMS	20A/1P	5	1		5	27 28	0	0					28	
29	PAPI 11	20A/2P			0	0	29 30	0	0			20A/1P	SPARE	30	
31							31 32	0	0			20A/1P	SPARE	32	
33	PAPI 29	20A/2P			0	0	33 34	0	0			20A/1P	SPARE	34	
35							35 36	0	0			20A/1P	SPARE	36	
37	SPARE	20A/2P			0	0	37 38	0	0			20A/1P	SPARE	38	
39							39 40	0	0			20A/1P	SPARE	40	
41	SPARE	20A/1P			0	0	41 42	0	0			20A/1P	SPARE	42	
SECTION TOTAL:			63		68			31.5	31.5			TOTAL USAGE LOAD: 23280 VA			
								PHASE TOTAL AMPS:		A B					
										94.5 99.5					
								PHASE TOTAL VA:		A B					
										11340 11940					

- NOTES:**
1. CONTRACTOR SHALL VERIFY ALL EXISTING CIRCUITS.

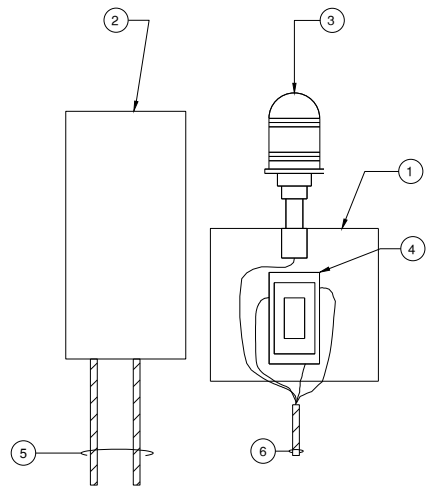


AIRFIELD LIGHTING AND EQUIPMENT CONTROL DIAGRAM
NOT TO SCALE



VAULT ALCMS RACK ELEVATION
NOT TO SCALE

NOTE: INSTALL POWER AND COMMUNICATION CONDUITS AS REQUIRED.

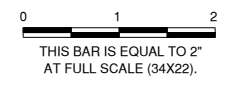


NEW EQUIPMENT MOUNTING ENCLOSURE DETAIL
NOT TO SCALE

- EQUIPMENT NOMENCLATURE**
- 1 EQUIPMENT MOUNTING ENCLOSURE, 12" x 12" x 8", MOUNTED ON WALL.
 - 2 ALCMS INTERFACE UNIT, REQUIRED BY ALCMS MANUFACTURER.
 - 3 RUNWAY/TAXIWAY INDICATOR LIGHT (LED).
 - 4 S-1 CUT-OUT.
 - 5 DATA/CONTROL CABLES (AS REQUIRED BY ALCMS MANUFACTURER) IN 1" CONDUIT TO REGULATOR.
 - 6 4 #8 5KV AIRFIELD LIGHTING CABLES IN FLEX CONDUIT TO HIGH VOLTAGE WIREWAY.

IL CONTRACT: **SH030**
IL LETTING ITEM: **01A**
IL PROJECT: **06C-4837**
S.B.G. PROJECT: 3-17-SBGP-184/TBD

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**SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILs
VAULT DETAILS**

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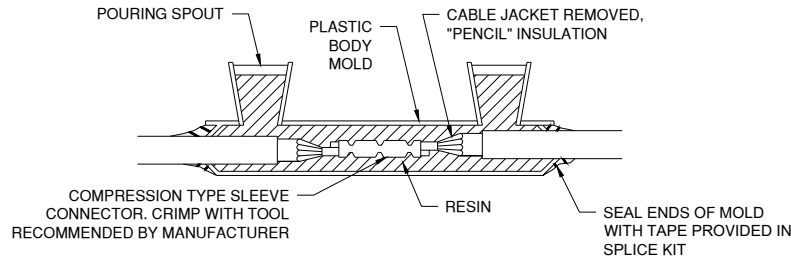
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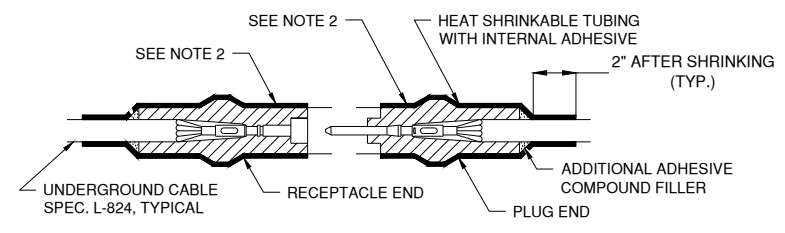
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CHECKED BY:	AB
APPROVED BY:	DJK
DATE:	1/10/2025
JOB No:	200256-02

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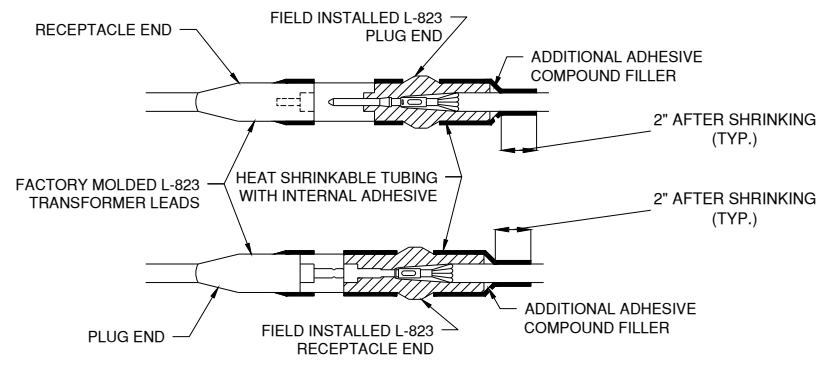
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 UPDATE BY: Jim Ohee
 LAYOUT: Electrical Details - Sheet 1
 IMAGE FILES: Schauburg Village aerial.tif
 REF: DWG: 2025-01-09



TYPE A - CABLE SPLICE
 N.T.S.
 FOR SPLICES IN HOMERUN AND FOR EXTENSIONS TO EXISTING CABLES ONLY

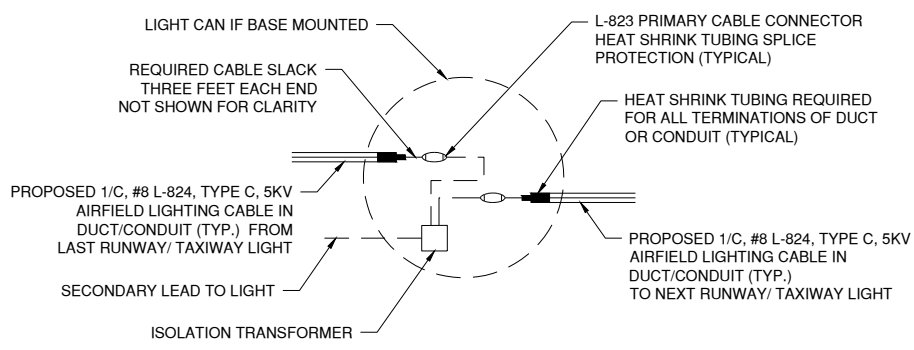


TYPE B - CABLE SPLICE
 N.T.S.
 FOR SPLICES AT HANDHOLE AND SPLICE CANS FOR EXTENSION OR JUNCTION OF HOMERUNS WITH LOOP CIRCUITS

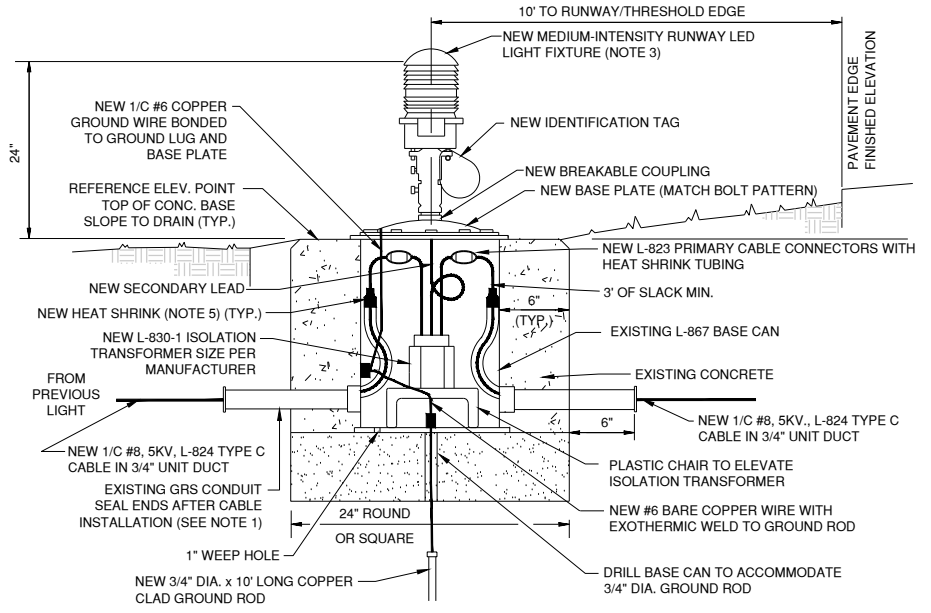


TYPE C & D - CABLE SPLICE
 N.T.S.
 FOR SPLICES AT RUNWAY / TAXIWAY LIGHTS AND SURFACE MOVEMENT GUIDANCE SIGNS

- NOTES:**
- THE INSIDE DIAMETER OF THE SPLICE CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
 - HEAT SHRINK THE ENTIRE SPLICE USING PROPERLY SIZED HEAT SHRINK TUBING WITH AN INTEGRAL INTERNAL ADHESIVE SEALANT. AN ACCEPTABLE COMPLETED SPLICE SHALL HAVE A CONTINUOUS SEALED HEAT SHRINK TUBE OVERLAPPING THE CABLE A MINIMUM OF 2-INCHES LONGER THAN THE SPLICE ON BOTH ENDS.
 - THE CONTRACTOR SHALL MAINTAIN ON SITE A MINIMUM OF TWO (2) TYPE A AND TYPE B SPLICE KITS AT ALL TIMES FOR EMERGENCY REPAIR. UPON COMPLETION OF THE PROJECT THE CONTRACTOR SHALL TRANSFER TO THE AIRPORT TWO COMPLETE SPLICE KITS OF EACH TYPE.
 - CONTRACTOR MAY INSTALL FAA APPROVED "COMPLETE KIT" IN LIEU OF L-823 SPLICE WITH HEAT SHRINK.

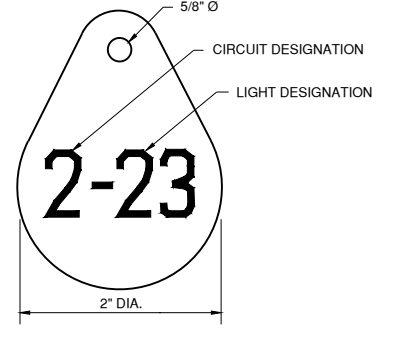


TYPICAL LIGHTING CIRCUIT CONNECTION DETAIL
 N.T.S.



REPLACE BASE MOUNTED MEDIUM INTENSITY RUNWAY/THRESHOLD EDGE LIGHT (LED)
 NOT TO SCALE

- NOTES:**
- CONTRACTOR SHALL VERIFY THE CONDITION AND SIZE OF GRS CONDUIT ENTRANCES/STUBS PER THE CABLING PLAN.
 - FOR PAY ITEM MIRL, BASE MOUNTED-LED AND MI THRESHOLD LIGHT, BASE MOUNTED - LED, CONTRACTOR SHALL REUSE EXISTING BASE CAN AND CONDUITS. PAY ITEM SHALL INCLUDE NEW FIXTURE, TRANSFORMER, GROUNDING, BASE PLATE, ID TAGS AND CONNECTORS.
 - LED LIGHT FIXTURES SHALL BE L-861(L) FOR EDGE LIGHT, OR L-861E(L) FOR THRESHOLD/END LIGHT.
 - BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
 - TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
 - AT THE CONTRACTOR'S OPTION, IN LIEU OF TAPE AND HEAT SHRINKABLE TUBING, A SELF-SEALING STYLE CONNECTOR L-823 "COMPLETE KITS" OR FAA APPROVED EQUAL MAY BE USED.

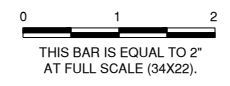


LIGHT IDENTIFICATION DETAIL
 NOT TO SCALE

- NOTES:**
- ON NEW LIGHTS, INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
 - NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
 - COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

IL CONTRACT: **SH030**
 IL LETTING ITEM: **01A**
 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

REVISIONS		
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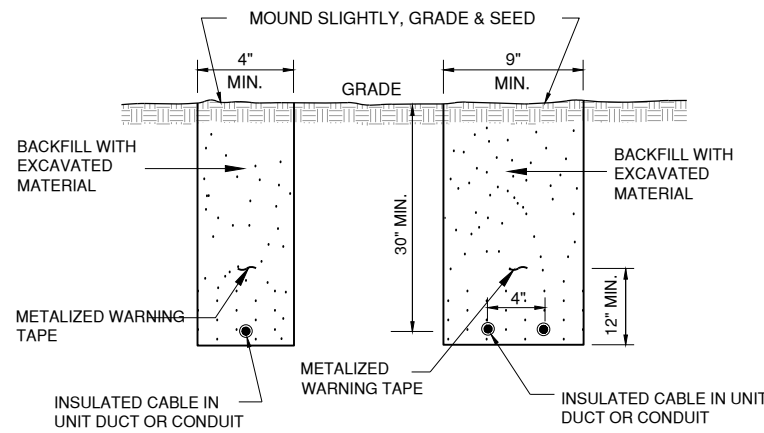
SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILS
ELECTRICAL DETAILS - 1

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APPROVED BY:	DJK
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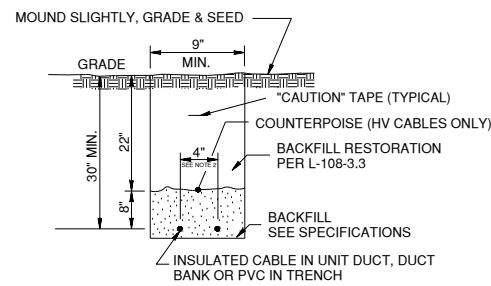
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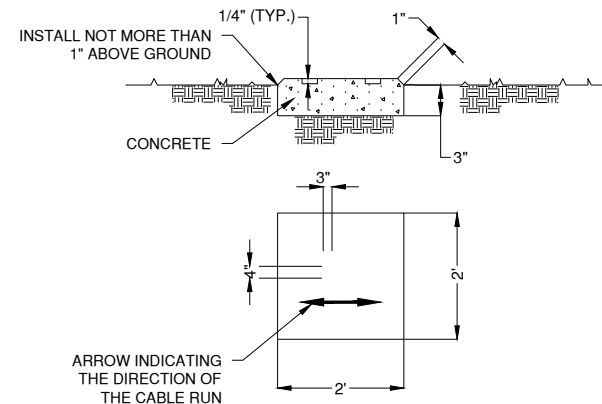
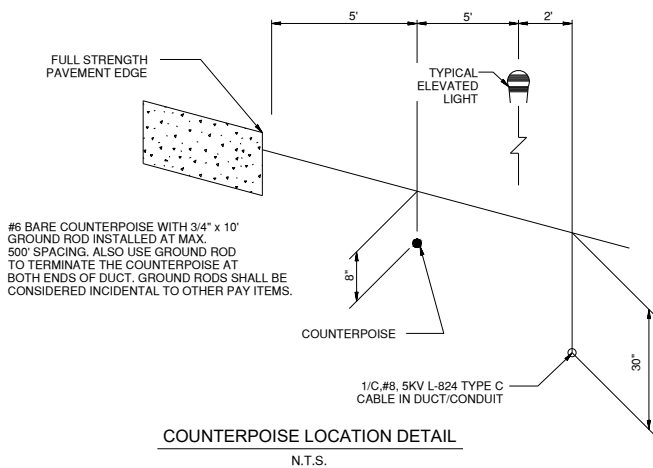
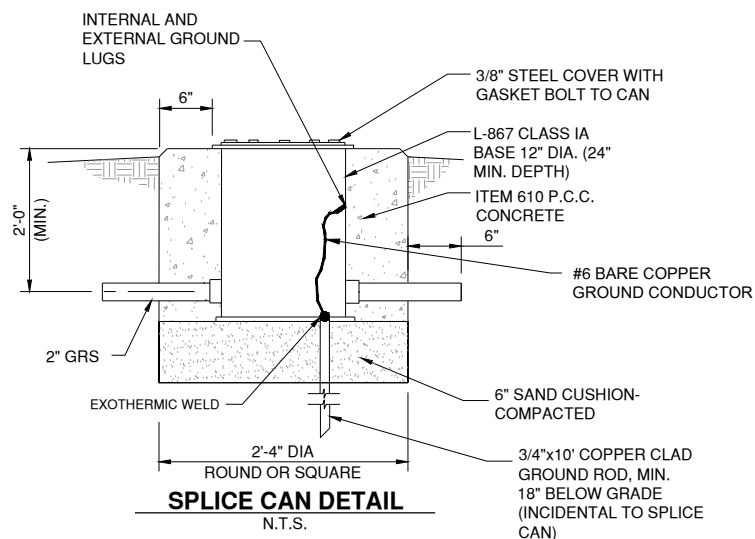
NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.



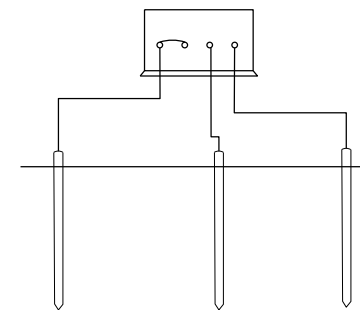
COUNTERPOISE NOTES:

- CABLES SHALL NOT BE PLACED LESS THAN 30" DEEP IN ANY ONE TRENCH. CABLES WITH DIFFERENT VOLTAGE RATINGS SHALL NOT BE INSTALLED IN THE SAME TRENCH.
- LOW AND HIGH VOLTAGE CABLES MAY BE COLOCATED IN A COMMON TRENCH. SEPARATION BETWEEN HIGH AND LOW VOLTAGE CABLES SHALL BE INCREASED TO 12".
- GROUND RODS SHALL BE INSTALLED AT 500' MAXIMUM INTERVALS. A NEW GROUND ROD SHALL BE INSTALLED AT THE COUNTERPOISE START/END POINTS AND AT AT POINTS WHERE COUNTERPOISE INTERSECT. COST OF GROUND RODS SHALL BE INCIDENTAL TO THE COUNTERPOISE PAY ITEM.



NOTES

- DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT AND WHERE PROPOSED ELECTRICAL DUCTS CROSS BOTH NEW AND EXISTING PAVEMENTS.
- CABLE MARKERS SHALL BE INSTALLED AT ANY CHANGE OF DIRECTION AND EVERY 200' ALONG THE CABLE RUN.
- SPLICE MARKERS SHALL BE INSTALLED DIRECTLY OVER ANY HOME RUN DIRECT BURY SPLICES.
- THE COST OF FURNISHING AND INSTALLING MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.



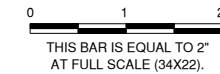
NOTES

- ALL RODS SHALL BE IN A STRAIGHT LINE.
- GROUND ROD TEST: BEFORE ANY WIRE IS CONNECTED TO THE GROUND RODS EACH ROD SHALL BE TESTED IN THE PRESENCE OF THE ENGINEER.. A WRITTEN RECORD OF THE RESULTS OF EACH INDIVIDUAL TEST SHALL BE PREPARED AND SIGNED BY THE CONTRACTOR AND THE ENGINEER. A DIRECT READING, SINGLE TEST, PORTABLE GROUND TESTING MEGGER SHALL BE USED TO TEST EACH GROUND ROD. THE AUXILIARY OR REFERENCED GROUND RODS SHALL BE 3/4" COPPER CLAD STEEL, NOT LESS THAN 4'-0" IN LENGTH, DRIVEN IN 3'-6" DEEP, AND SHALL BE INSTALLED IN A STRAIGHT LINE FROM THE GROUND ROD BEING TESTED. NO. 12 STRANDED WIRE LEADS WITH HIGH GRADE INSULATION SHALL BE CONNECTED TO THE ROD BEING TESTED. THE TWO REFERENCE RODS, AND TO THE PROPER GROUNDING POST ON THE INSTRUMENT. THE MEEGER INDICATES THE RESISTANCE TO EARTH IN OHMS. THESE TESTS SHALL NOT BE MADE WITHIN 48 HOURS AFTER RAINFALL OR DURING RAINY OR FOGGY WEATHER. IN ADDITION, AFTER GROUND RODS ARE TESTED AND CONNECTED, A COMPLETE SYSTEM TEST SHALL BE MADE IN A SIMILAR MANNER, USING THE SAME METHOD, AND THE SYSTEM TEST SHALL NOT EXCEED 1 OHM.

IL CONTRACT: **SH030**
 IL LETTING ITEM: **01A**
 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

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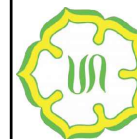


**SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILS**

ELECTRICAL DETAILS - 2

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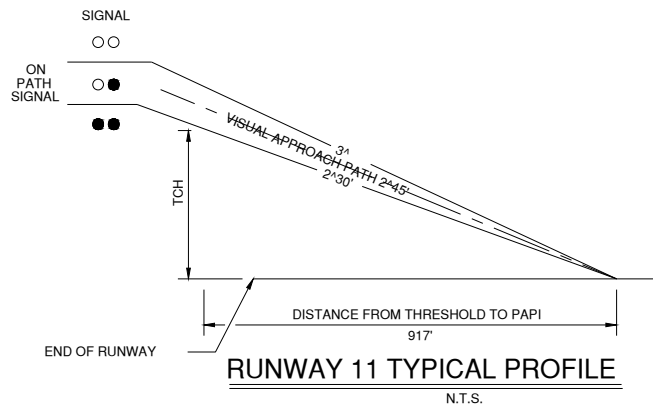
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DRAWN BY:	JRO
CHECKED BY:	AB
APPROVED BY:	DJK
DATE:	1/10/2025
JOB No:	200256-02

FINAL

DATE: Thursday, January 9, 2025 7:09:12 AM
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 IMAGE FILES: Schaumburg Village seal.tif
 XREF DWG: Detail.dwg

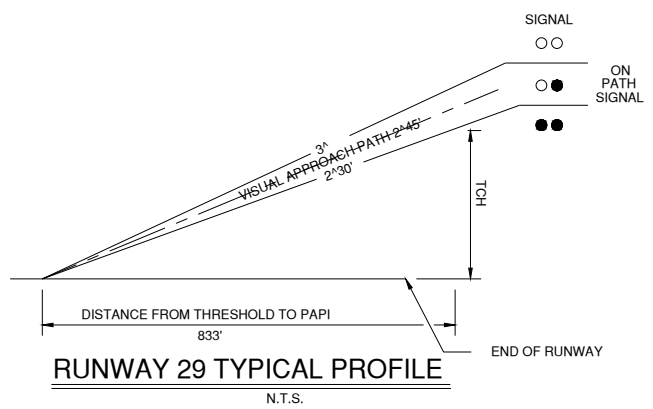


AIMING OF TYPE L-881 (2-BOX) PAPI
 RELATIVE TO PRESLECTED GLIDE PATH (3°00')

LIGHT UNIT	AIMING ANGLE (IN MINUTES OF ARC)	
	STANDARD INSTALLATION	
UNIT NEAREST RUNWAY	15° ABOVE GLIDE PATH	
NEXT ADJACENT UNIT	15° BELOW GLIDE PATH	

RUNWAY	11	29
HEIGHT GROUP USED FOR SITING	1	1
THRESHOLD STATIONING	100+00	138+00
THRESHOLD ELEVATION	801.0'	801.0'
THRESHOLD CROSSING HEIGHT	40'	40'
STATION FOR PAPIS	108+32.75	129+67.00
GLIDE PATH ANGLE °	3.0°	3.0°
REFERENCE POINT ELEVATION **	801.0'	801.0'

* THE VISUAL GLIDE PATH ANGLE IS THE CENTER OF THE ON COURSE ZONE AND IS MEASURED FROM THE HORIZONTAL
 ** ELEVATION OF CENTER OF PAPI LIGHTS



FOUNDATIONS:
 FOUNDATIONS FOR MOUNTING LIGHT BOXES SHALL BE MADE OF ITEM 610 CONCRETE. ALL LIGHT BOXES SHALL BE FRANGIBLY MOUNTED TO THE FOUNDATION.

AZIMUTHAL AIMING:
 EACH LIGHT UNIT SHALL BE AIMED OUTWARD INTO THE APPROACH ZONE ON A LINE PARALLEL TO THE RUNWAY CENTERLINE WITHIN A TOLERANCE OF ±1/2 DEGREE.

MOUNTING HEIGHT TOLERANCES:
 THE BEAM CENTERS OF ALL LIGHT UNITS SHALL BE WITHIN ±1 INCH OF A HORIZONTAL PLANE AT THE REFERENCE ELEVATION GIVEN IN THE TABLE.

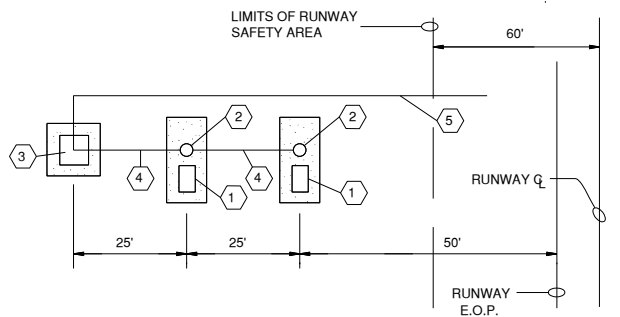
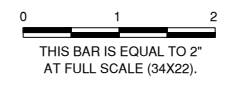
TOLERANCE ALONG LINE PERPENDICULAR TO RUNWAY:
 THE FRONT FACE OF EACH LIGHT UNIT IN A BAR SHALL BE LOCATED ON A LINE PERPENDICULAR TO THE RUNWAY CENTERLINE WITHIN ±6 INCHES.

LATERAL SPACING
 THE DIFFERENCE IN LATERAL SPACING BETWEEN LIGHT UNITS SHALL NOT EXCEED 1'-0".

IL CONTRACT: **SH030**
 IL LETTING ITEM: **01A**
 IL PROJECT: **06C-4837**
 S.B.G. PROJECT: 3-17-SBGP-184/TBD

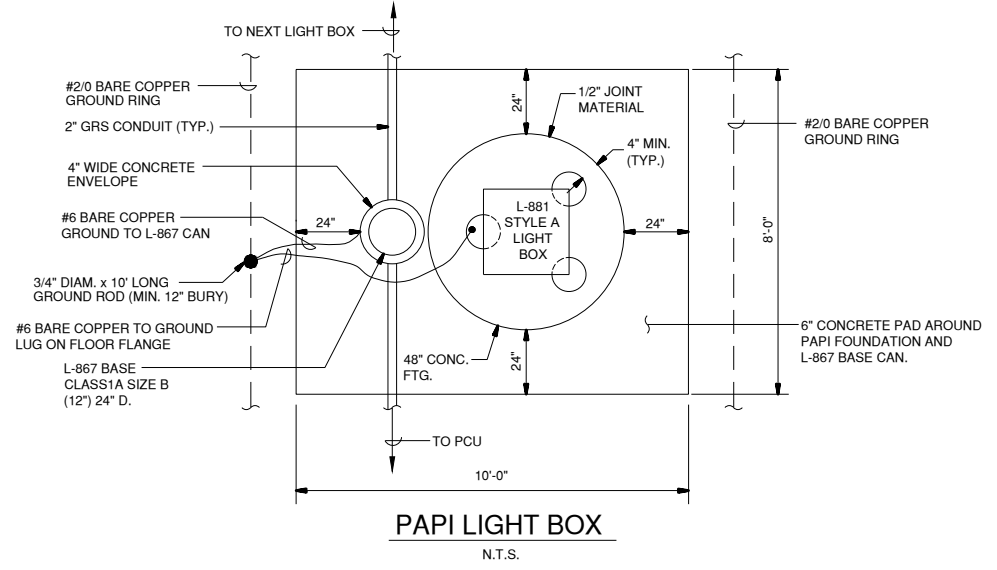
REVISIONS

NUMBER	BY	DATE

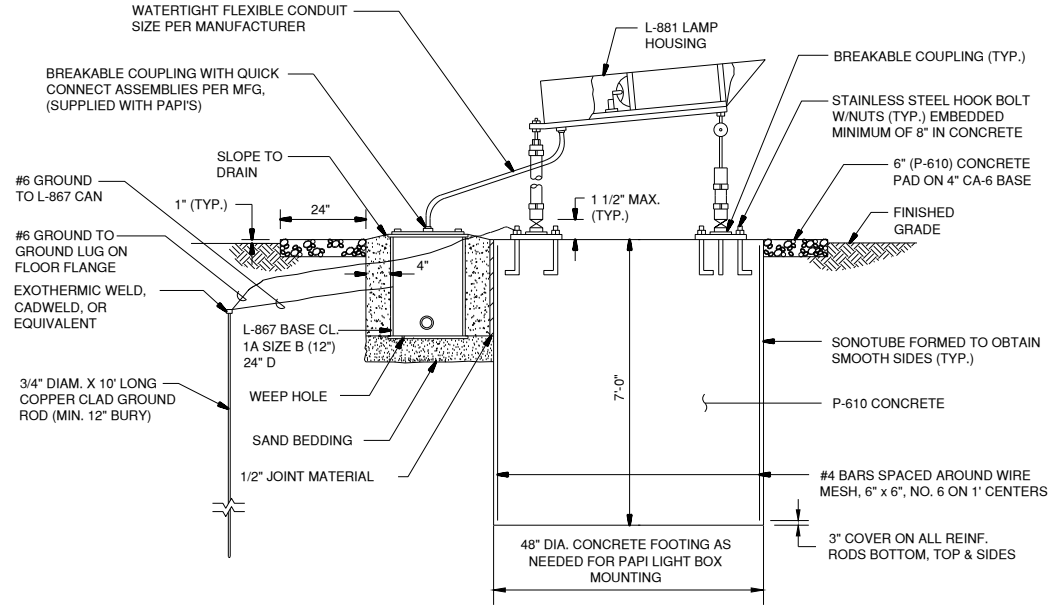


RUNWAY 29 PAPI PLAN
 (RWY. 11 MIRROR IMAGE)
 N.T.S.

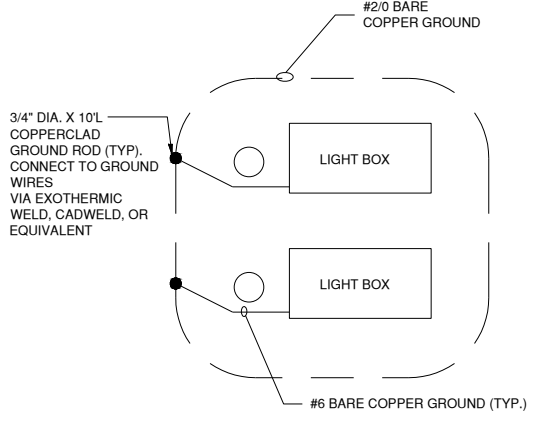
- 1 L-881 (L) PAPI LIGHT BOX.
- 2 L-867 CAN WITH SOLID LID.
- 3 PAPI PCU. SEE DETAIL, THIS SHEET.
- 4 PAPI LIGHT BOX POWER & TILT SWITCH WIRING.
- 5 PAPI POWER WIRING TO ELECTRICAL VAULT.



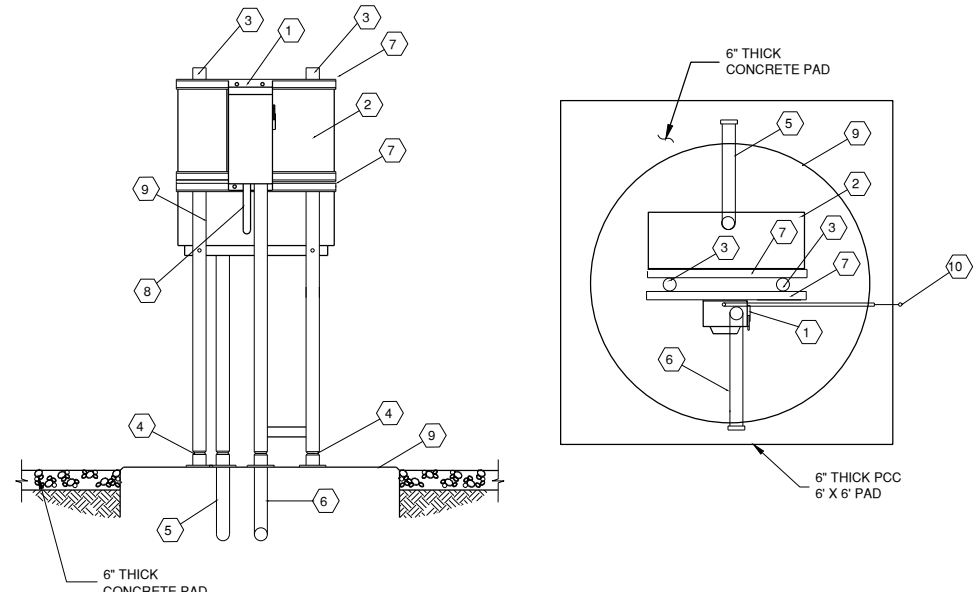
PAPI LIGHT BOX
 N.T.S.



PAPI SIDE ELEV.
PAPI DETAIL
 N.T.S.



PAPI GROUND RING DETAIL
 N.T.S.



PAPI PCU PLAN AND ELEVATION

- 1 HEAVY DUTY FUSIBLE DISCONNECT, 600VAC, 30A, NEMA 3R, SQUARE D H361AWK OR EQUAL, WITH TWO 15A FUSES.
- 2 120/240V PAPI POWER & CONTROL UNIT, WITH PHOTOCELL.
- 3 2" GALVANIZED EMT LEGS WITH TOPS CAPPED.
- 4 FRANGIBLE COUPLINGS & FLOOR FLANGES. ANCHOR TO CONCRETE FOUNDATION (TYP.).
- 5 2" GRS CONDUIT WITH PAPI LIGHT HOUSING POWER & PAPI TILT CONTROL CABLES, PER PAPI MANUFACTURER.
- 6 CONDUIT AND WIRING TO VAULT AS FOLLOWS:
 PAPI 11: 3 #4 XLP-USE, ONE #4 GND IN 1-1/2" UNIT DUCT
 PAPI 29: 3#4 XLP-USE, ONE #4 GND IN 1-1/2" UNIT DUCT
- 7 1-5/8" X 1-5/8" GALVANIZED STRUT.
- 8 TWO #12 THWN, ONE #12 NEUTRAL, ONE #12 GND IN 3/4" CONDUIT, TO PAPI POWER & CONTROL UNIT.
- 9 CONCRETE FOOTING, 36" DIAMETER X 48" DEEP (MIN.). SEE PAPI INSTALLATION FOR REBAR AND WIRE MESH INFO.
- 10 3/4" DIA. BY 10 FT. LONG COPPER CLAD GROUND ROD WITH #6 SOLID BARE COPPER GROUND CABLE ATTACHED BY EXOTHERMIC WELDING. OTHER END OF CABLE TERMINATES ON GROUND LUG IN DISCONNECT. (NOT SHOWN IN ELEVATION VIEW).

SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
AIRFIELD LIGHTING REHABILITATION: MIRLS, PAPIs, REILS
ELECTRICAL DETAILS - 3

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DESIGN BY:	LN
DRAWN BY:	JRO
CHECKED BY:	AB
APPROVED BY:	DJK
DATE:	1/10/2025
JOB No:	200256-02

FINAL

SHEET 15 OF 15 SHEETS