

03-08-13 LETTING ITEM 058

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	130S-1(12)	LAKE ILLINOIS	15	1

\*15+2=17

D-91-414-12



FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE CITY OF LAKE FOREST.

PROPOSED  
HIGHWAY PLANS

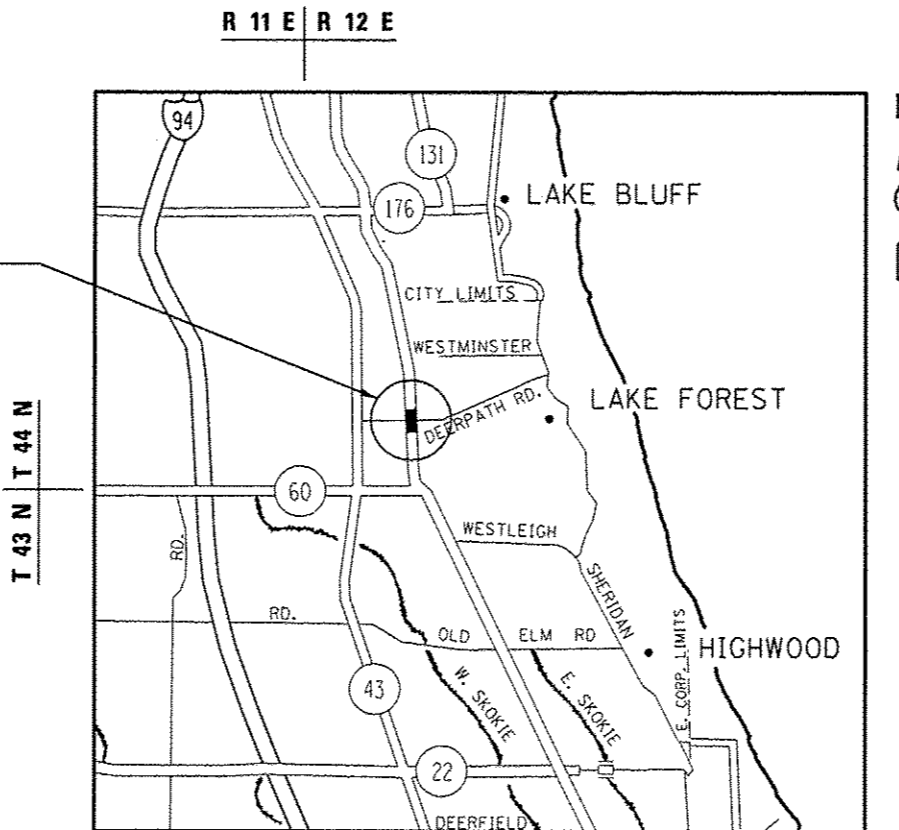
F.A.P. ROUTE 346: US 41 (SKOKIE HIGHWAY)  
OVER DEERPATH RD. (SN 049-0033)  
SECTION: 130S-1(12)  
BRIDGE JOINT & APPROACH PAVEMENT REPAIRS  
LAKE COUNTY

C-91-414-12

TRAFFIC DATA

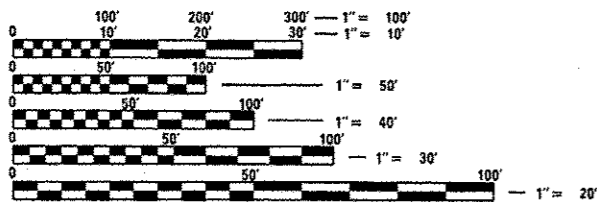
2011 ADT = 44,900  
POSTED SPEED LIMIT = 55 MPH

IMPROVEMENT LOCATION  
SN 049-0033



SHIELDS TOWNSHIP

GROSS & NET LENGTH = 568.8 FT. = 0.108 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER J. ALAIN MIDY (847) 221-3057  
PROJECT MANAGER ISSAM RAYYAN (847) 705-4178

CONTRACT NO. 60T65

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED December 18, 2012  
John Costantino  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Feb 1, 2013  
John D. Baranzelli, P.E.  
ENGINEER OF DESIGN AND ENVIRONMENT

Feb 1, 2013  
Omer Osman, P.E.  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

## INDEX OF SHEETS

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- 11-13. STAGING PLANS
14. TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
15. ARTERIAL ROAD INFORMATION SIGN (TC-22)
- 15A. TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
- 15B. DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)

## STATE STANDARDS

STANDARD NO.	DESCRIPTION
420001-07	PAVEMENT JOINTS
442101-07	CLASS B PATCHES
701101-03	OFF ROAD OPERATIONS, MULTILANE, $\leq$ 15 FT TO EOP
701421-05	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS $\leq$ 45 MPH TO 55 MPH
701426-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS $\geq$ 45 MPH
701901-02	TRAFFIC CONTROL DEVICES

## GENERAL NOTES

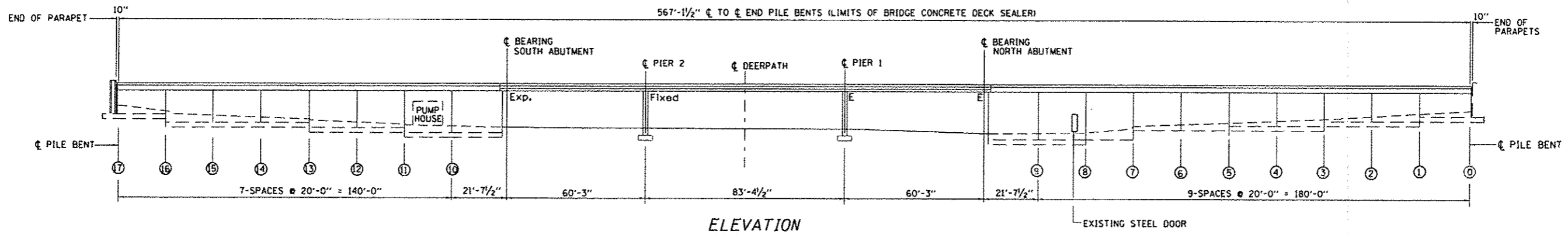
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED).
- THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF LAKE FOREST.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE BRIDGE INSPECTORS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- FOR ACCESS INTO THE VAULTED AREAS THE CONTRACTOR SHALL CONTACT THE BUREAU OF MAINTENANCE AT (847) 705-4181 FOR WORK IN THE NORTH VAULTED ABUTMENT AND ELECTRICAL MAINTENANCE OPERATION AT (708) 524-2145 FOR WORK IN THE SOUTH VAULTED ABUTMENT.
- USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS. USE THE "LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE)" DETAIL SHOWN ON HIGHWAY STANDARD 420001 FOR ALL LONGITUDINAL JOINTS.
- CONCRETE SUPERSTRUCTURE SHALL HAVE A SEVEN DAY MINIMUM CURE.

FILE NAME =	USER NAME = pyzanoskiqb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\p1010\pyzanoskiqb\031068	0141412-shit-plan.dgn	DRAWN -	REVISED -			346	1305-11(2)	LAKE	15	2	
*MODELNAME*	PLOT SCALE = 1/8" = 1' / in.	CHECKED -	REVISED -			<b>CONTRACT NO. 60T65</b>					
	PLOT DATE = 1/28/2013	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

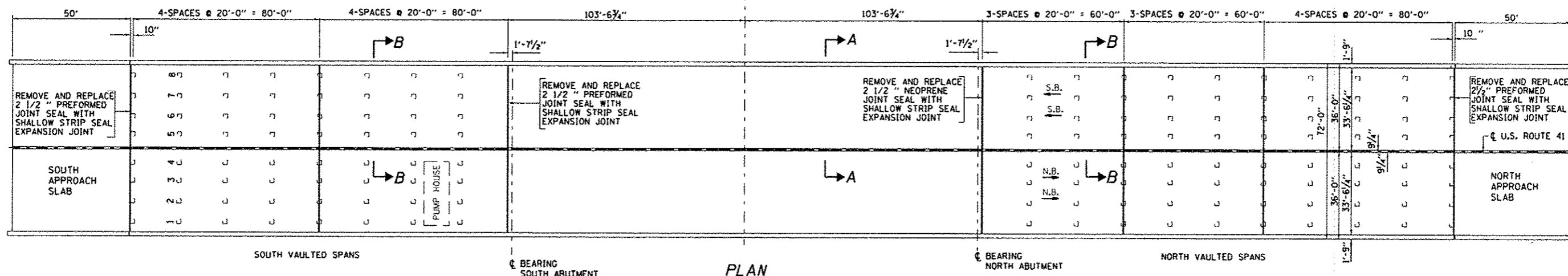
SUMMARY OF QUANTITIES				URBAN 100% STATE TOTAL QUANTITIES					CONSTRUCTION TYPE CODE 0014					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0014					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0014									
44201019	CLASS B PATCHES, TYPE II, 14 INCH	SO YD	39	39					*78008210	POLYUREA PAVEMENT MARKING TYPE I 4 INCH	FOOT	1135	1135									
44213202	TIE BARS 1"	EACH	25	25					78300100	PAVEMENT MARKING REMOVAL	SQ FT	612	612									
50102400	CONCRETE REMOVAL	CU YD	12.1	12.1					X5870015	BRIDGE DECK CONCRETE SEALER	SQ FT	45,030	45,030									
50300255	CONCRETE SUPERSTRUCTURE	CU YD	11.9	11.9					X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	8180	8180									
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1454	1454					Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4									
50800515	BAR SPLICERS	EACH	32	32																		
52000110	PREFORMED JOINT STRIP SEAL	FOOT	276	276																		
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	233	233																		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3																		
67100100	MOBILIZATION	L SUM	1	1																		
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1																		
Z0016200	DECK SLAB REPAIR (PARTIAL)	SO YD	27	27																		
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2727	2727																		
*78000200	THERMOPLASTIC PAVEMENT MARKING LINE 4"	FOOT	700	700																		

\* Specialty Items

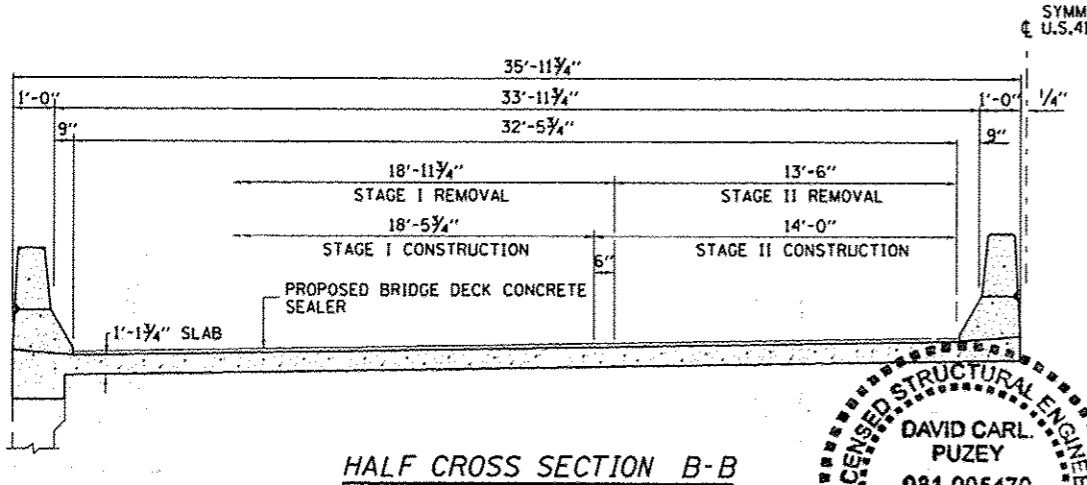
FILE NAME *	USER NAME *	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -						346	1305-1(12)	LAKE	15	3
		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 60T65				
		DATE	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



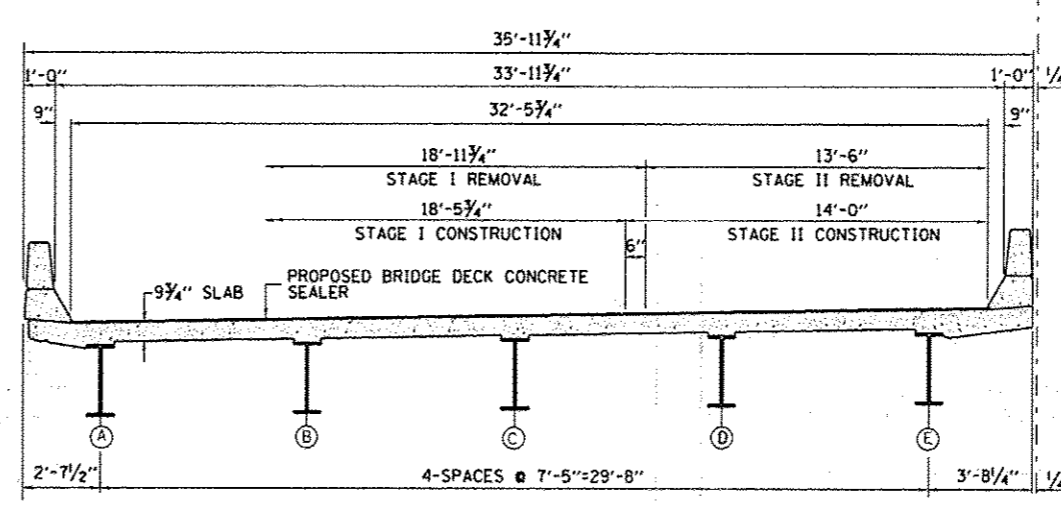
ELEVATION



PLAN



HALF CROSS SECTION B-B



HALF CROSS SECTION A-A

GENERAL NOTES:

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENT PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.

ALL STRUCTURAL STEEL SHALL CONFORM TO AASHTO CLASSIFICATION M-270 GR. 36, UNLESS OTHERWISE NOTED.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

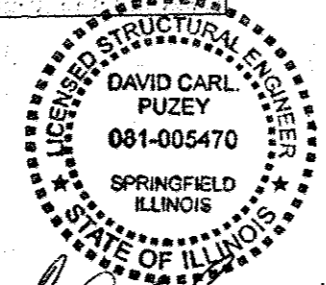
JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS, WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50° F.

THE DECK SURFACE SHALL HAVE ITS FINAL FINISH FINED ACCORDING TO ARTICLE 420.09(a)(1) OF THE STANDARD SPECIFICATIONS. COST INCLUDED WITH CONCRETE SUPERSTRUCTURES.

TOTAL - BILL OF MATERIALS

ITEM	UNIT	QUANTITY
REINFORCEMENT BARS (EPOXY COATED)	POUND	1454
CONCRETE REMOVAL	CU.YD.	12.1
CONCRETE SUPERSTRUCTURE	CU.YD.	11.9
DECK SLAB REPAIR (PARTIAL)	SO.YD.	27
PREFORMED JOINT STRIP SEAL	FOOT	276
BRIDGE DECK CONCRETE SEALER	SQ.FT.	45,030
BAR SPLICERS	EACH	32

NOTE:  
SEE ATTACHED SPECIAL PROVISION BRIDGE DECK CONCRETE SEALER IS NORMALLY ALSO APPLIED TO PARAPET FACES AND TOP.



*David Carl Puzey* 1/29/13  
Expires 1/30/14

FILE NAME: 0141412-shr-plan.dgn	USER NAME: pyzzenovskirb	DESIGNED - JAM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 41 OVER DEERPETH ROAD GENERAL PLAN, ELEVATION AND CROSS SECTION S.N. 049-0033			F.A.P. RTE. 346	SECTION 1305-1(12)	COUNTY LAKE	TOTAL SHEETS 15	SHEET NO. 4	
MODELNAME:	PLOT SCALE: 1/8" = 1'-0"	CHECKED - SMR	REVISED -		SCALE:	SHEET OF	SHEETS	STA. TO STA.	CONTRACT NO. 60T65				
	PLOT DATE: 1/28/2013	DATE:	REVISED -		ILLINOIS FED. AID PROJECT								

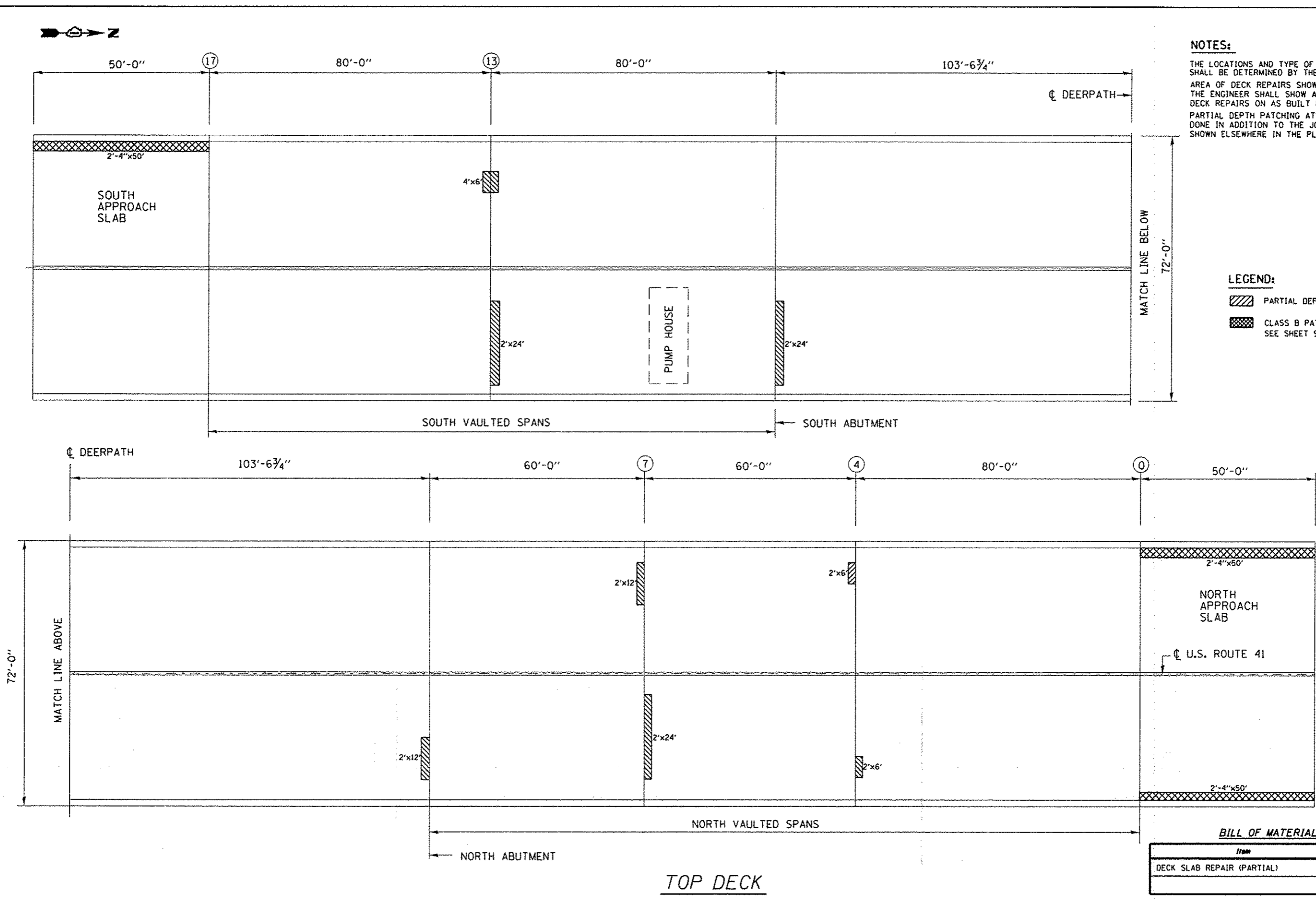


**NOTES:**

THE LOCATIONS AND TYPE OF THE DECK SLAB REPAIR SHALL BE DETERMINED BY THE ENGINEER.  
 AREA OF DECK REPAIRS SHOWN ARE ESTIMATED. THE ENGINEER SHALL SHOW ACTUAL LOCATIONS OF DECK REPAIRS ON AS BUILT PLANS.  
 PARTIAL DEPTH PATCHING AT JOINTS SHALL BE DONE IN ADDITION TO THE JOINT REPLACEMENT SHOWN ELSEWHERE IN THE PLANS.

**LEGEND:**

- PARTIAL DEPTH PATCHING
- CLASS B PATCHING (FULL DEPTH)  
SEE SHEET 9 OF 15

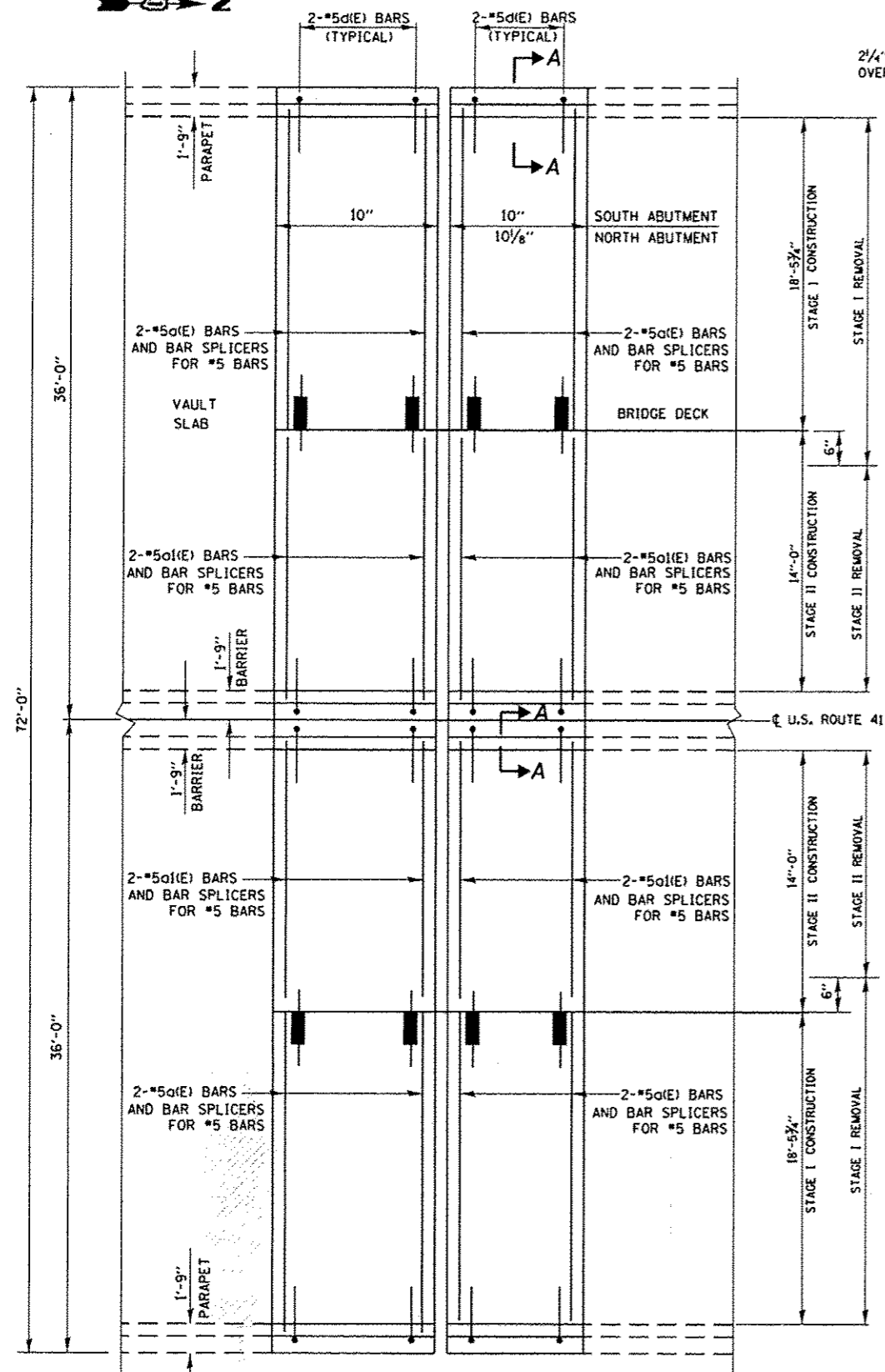


**BILL OF MATERIALS**

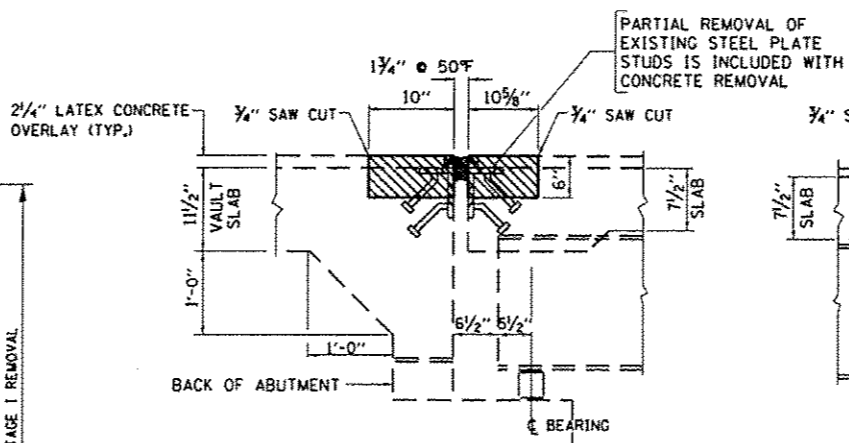
Item	Unit	Total
DECK SLAB REPAIR (PARTIAL)	SQ.YD.	27

TOP DECK

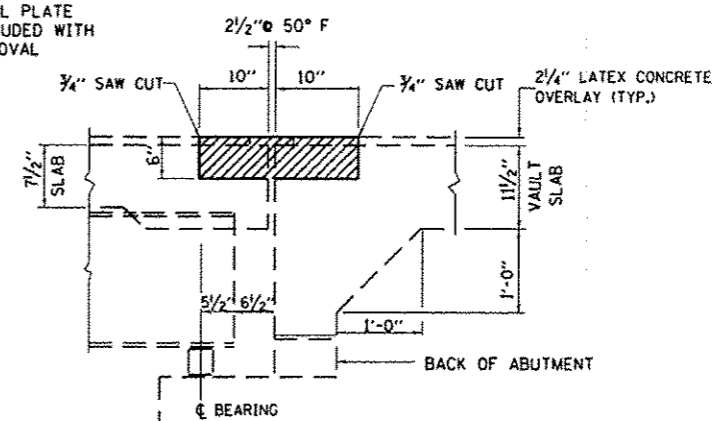
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	0141412-shr-plan.dgn	DRAWN: -	REVISED: -			346	1305-11121	LAKE	15	5	
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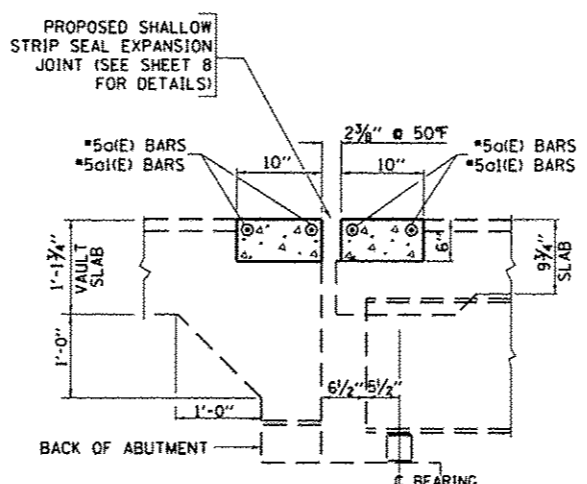
**PLAN**  
**SOUTH ABUTMENT**  
(NORTH ABUTMENT SLAB SIMILAR)



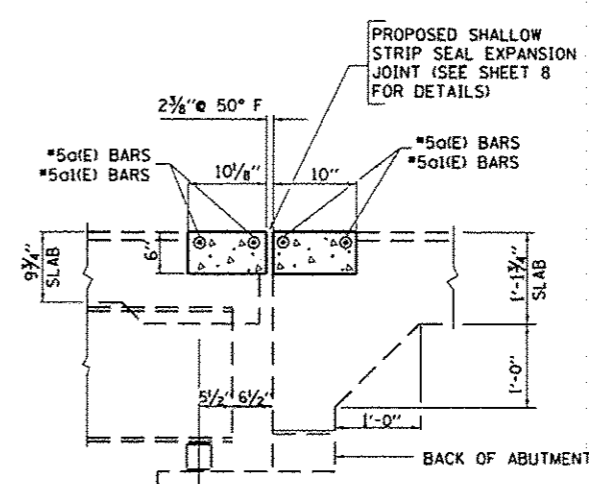
**EXISTING EXPANSION JOINT AT SOUTH ABUTMENT**



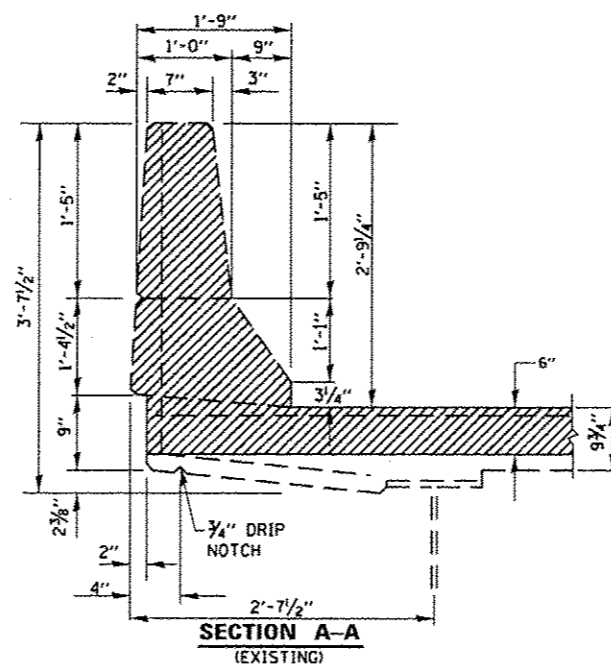
**EXISTING EXPANSION JOINT AT NORTH ABUTMENT**



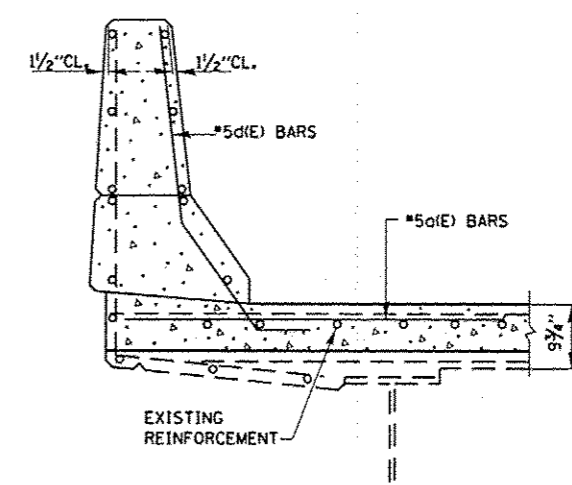
**PROPOSED EXPANSION JOINT AT SOUTH ABUTMENT**



**PROPOSED EXPANSION JOINT AT NORTH ABUTMENT**



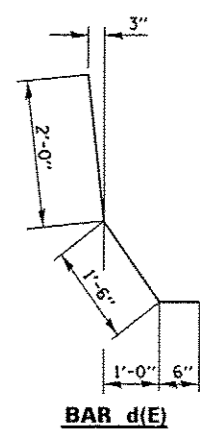
**SECTION A-A**  
(EXISTING)



**SECTION A-A**  
(PROPOSED)

**BILL OF MATERIALS**

BAR	NO.	SIZE	LENGTH	SHAPE	
d(E)	16	#5	19'-10"		
d(E)	16	#5	15'-4"		
d(E)	32	#5	4'-0"		
REINFORCEMENT BARS (EPOXY COATED)				POUND	727
CONCRETE REMOVAL				CU.YD.	6.0
CONCRETE SUPERSTRUCTURE				CU.YD.	6.0
BAR SPLICERS				EACH	16



**BAR d(E)**

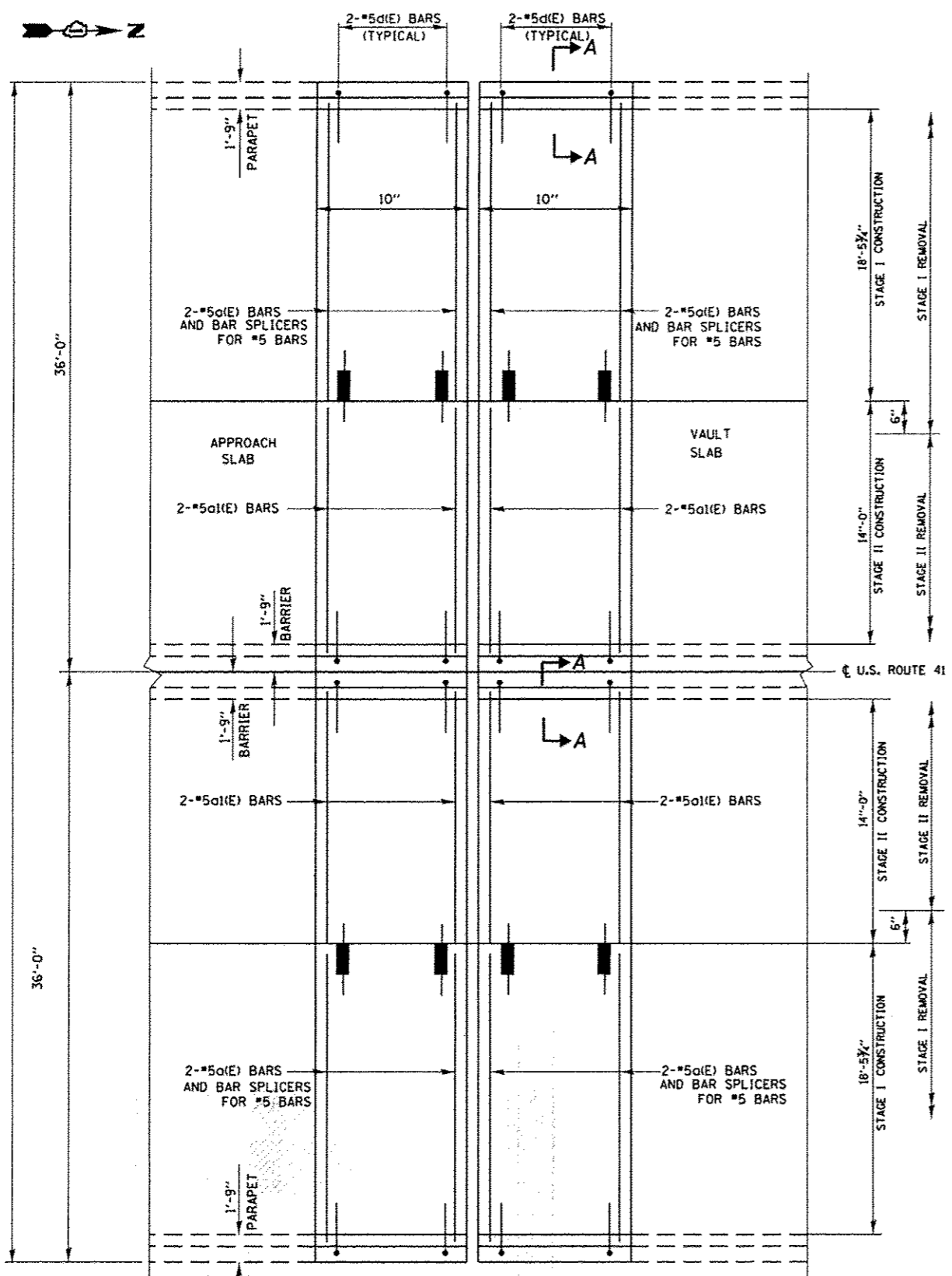
**LEGEND:**  
 CONCRETE REMOVAL

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0141412-eh1.p21en.dgn	LOT SCALE: 99.998 / in.	DRAWN: -	REVISED: -
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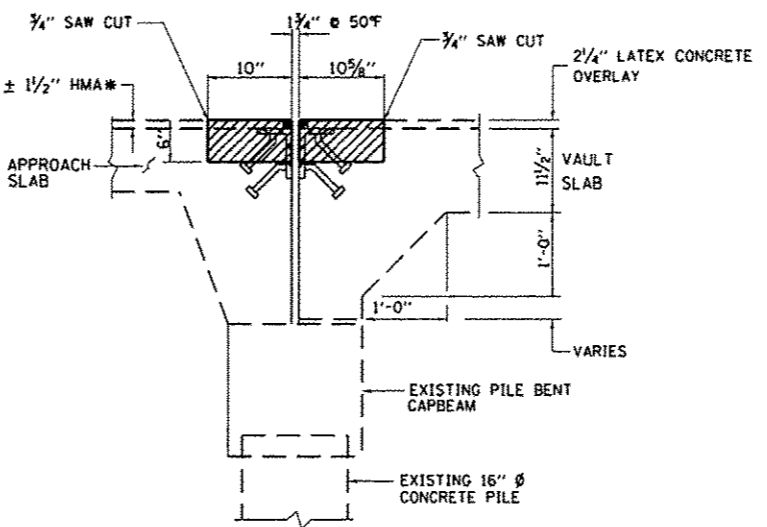
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**U.S. 41 OVER DEERPATH ROAD**  
**TRANSVERSE EXPANSION JOINT DETAILS**  
S.N. 049-0033

F.A.P. RTE. 346	SECTION 1305-(112)	COUNTY LAKE	TOTAL SHEETS 15	SHEET NO. 6
SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 60T65
ILLINOIS FED. AID PROJECT				

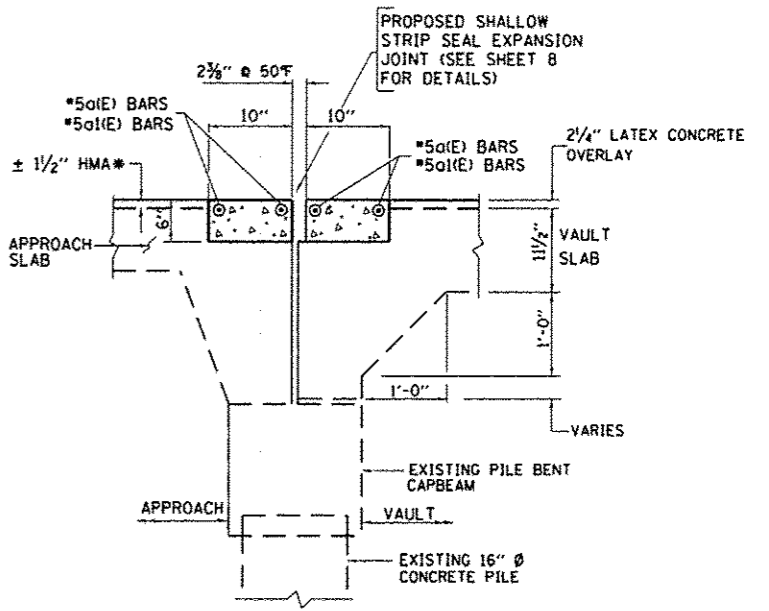


**PLAN**  
**SOUTH APPROACH SLAB**  
(NORTH APPROACH SLAB SIMILAR)

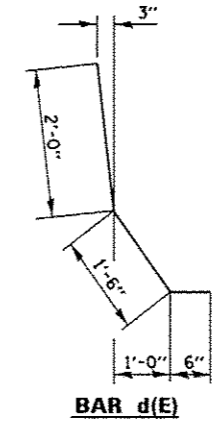


**EXISTING EXPANSION JOINT**  
**AT APPROACH SLAB**  
**JOINTS 0 AND 17**

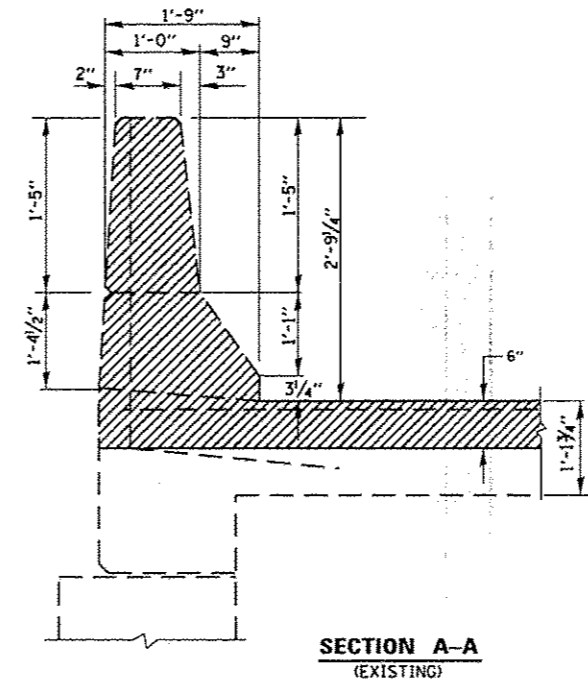
\* REMOVAL OF HMA IS INCLUDED IN THE COST OF CONCRETE REMOVAL.



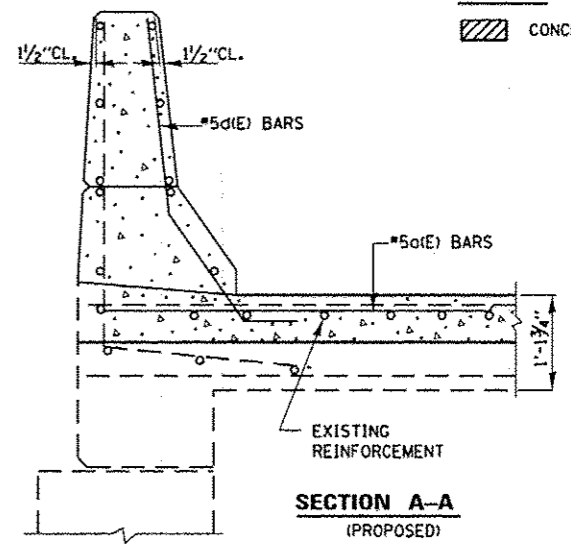
**PROPOSED EXPANSION JOINT**  
**AT APPROACH SLAB**  
**JOINTS 0 AND 17**



**BAR d(E)**



**SECTION A-A**  
(EXISTING)



**SECTION A-A**  
(PROPOSED)

**BILL OF MATERIALS**

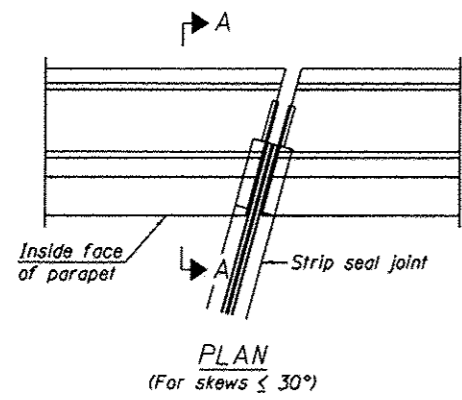
BAR NO.	SIZE	LENGTH	SHAPE
a(E)	#5	20'-0"	—
a(E)	#5	15'-4"	—
d(E)	#5	4'-0"	—
REINFORCEMENT BARS (EPOXY COATED)			POUND 727
CONCRETE REMOVAL			CU.YD. 6.1
CONCRETE SUPERSTRUCTURE			CU.YD. 5.9
BAR SPLICERS			EACH 16

**LEGEND:**

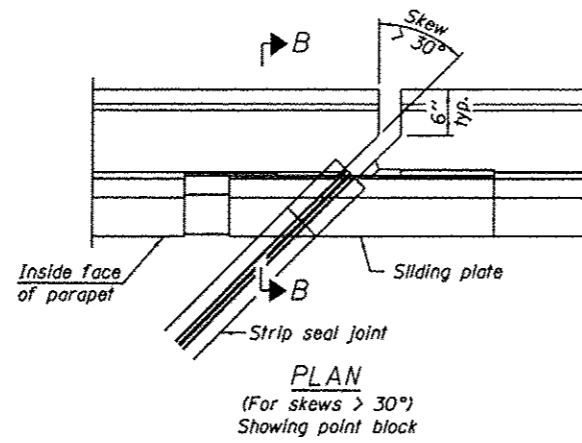
CONCRETE REMOVAL

FILE NAME: c:\p\work\psd\dot\pyrzanovskirb\d0310668	USER NAME: pyrzanovskirb	DESIGNED: -	REVISED: -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>U.S. 41 OVER DEERPATH ROAD</b> <b>TRANSVERSE EXPANSION JOINT DETAILS</b> <b>S.N. 049-0033</b>		F.A.P. RTE. 346	SECTION 1305-1(I)2	COUNTY LAKE	TOTAL SHEETS 15	SHEET NO. 7	
MODEL NAME:	0141412-shr-plan.dgn	DRAWN: -	REVISED: -		SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				
	PLOT SCALE: 1/8" = 1'-0"	CHECKED: -	REVISED: -					CONTRACT NO. 60T65				
	PLOT DATE: 1/28/2013	DATE: -	REVISED: -					CONTRACT NO. 60T65				

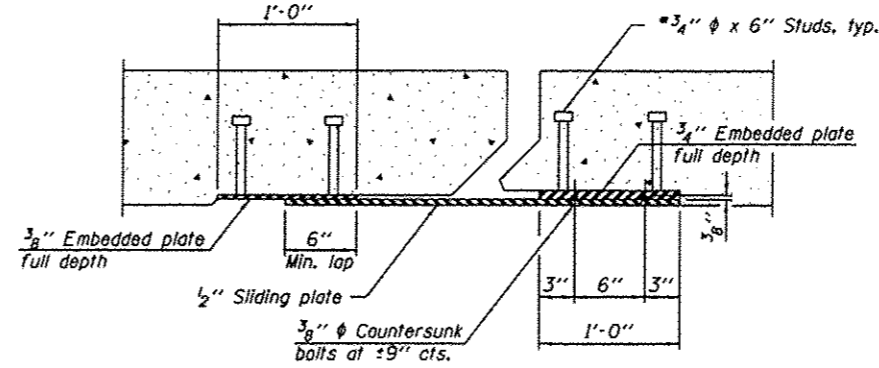




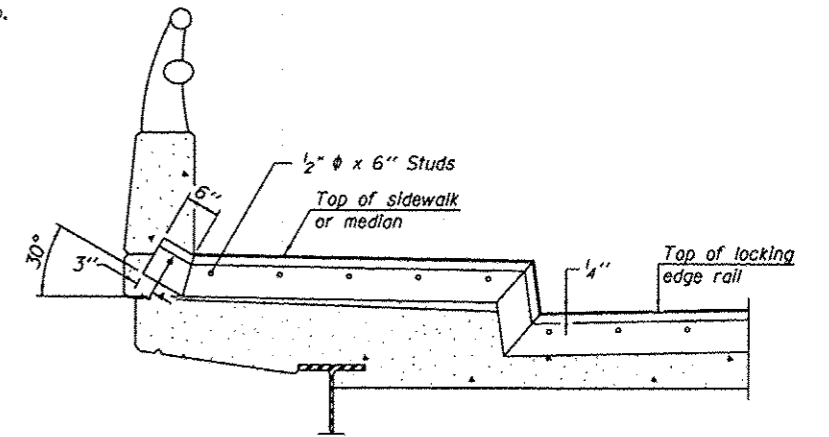
PLAN  
(For skews  $\leq 30^\circ$ )



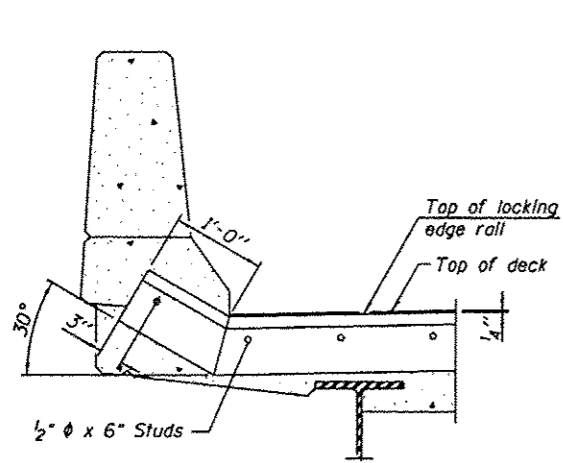
PLAN  
(For skews  $> 30^\circ$ )  
Showing point block



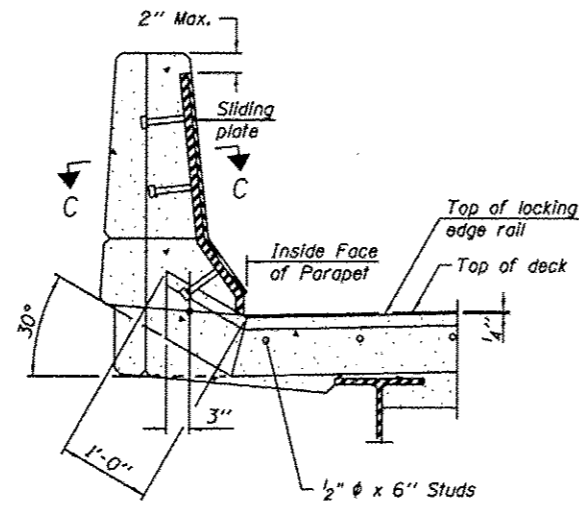
SECTION C-C



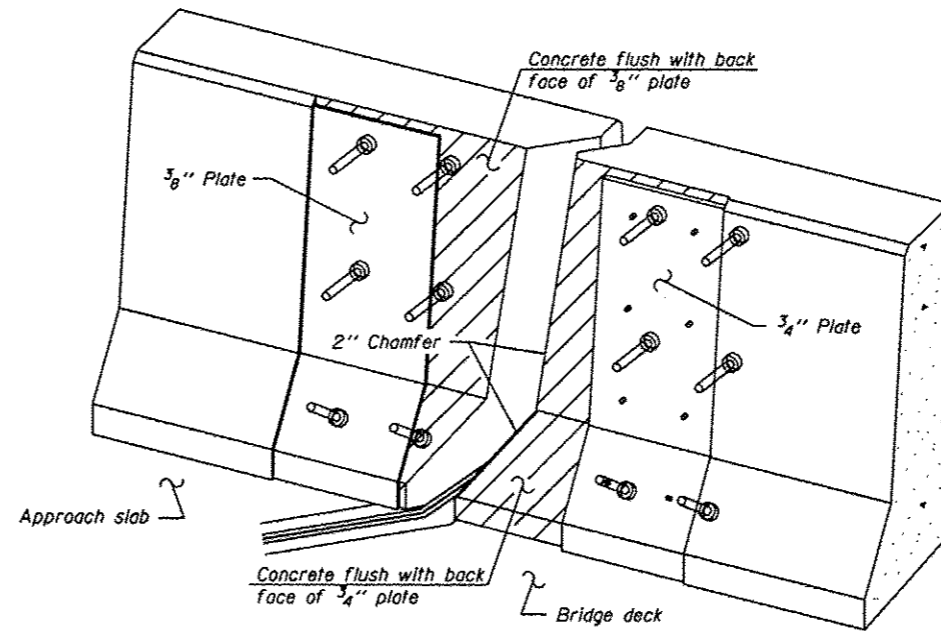
TYPICAL END TREATMENT  
AT SIDEWALK OR MEDIAN  
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



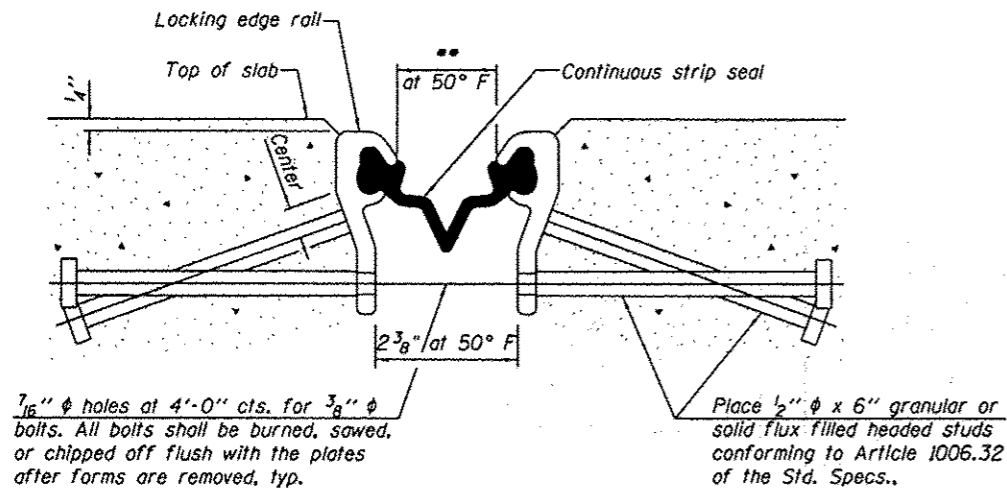
SECTION A-A



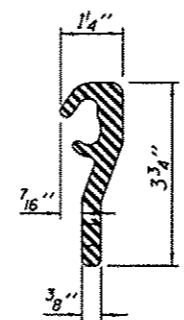
SECTION B-B



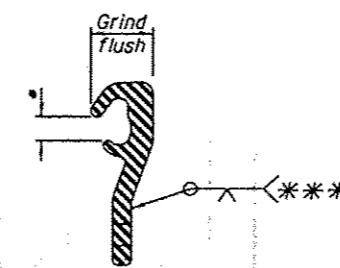
TRIMETRIC VIEW  
(Showing back plates only)



SECTION THRU STRIP SEAL JOINT  
FOR OVERLAY OVER DECK BEAMS



ROLLED  
(EXTRUDED) RAIL



LOCKING EDGE  
RAIL SPLICE

NOTES:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "Welded" strip seal gland configurations are not permitted. The gland shall be sized for maximum rated movement of 4 inches.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.

The inside of the Locking Edge Rail groove shall be free of weld residue. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant. joints in Rails within 10 Ft. of curbs shall be welded.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	276

- \* Omit weld at seal opening.
- \*\* The minimum dimension shall be 1/2" for installation purposes.
- \*\*\* Back gouge not required if complete joint penetration is verified by mock-up.

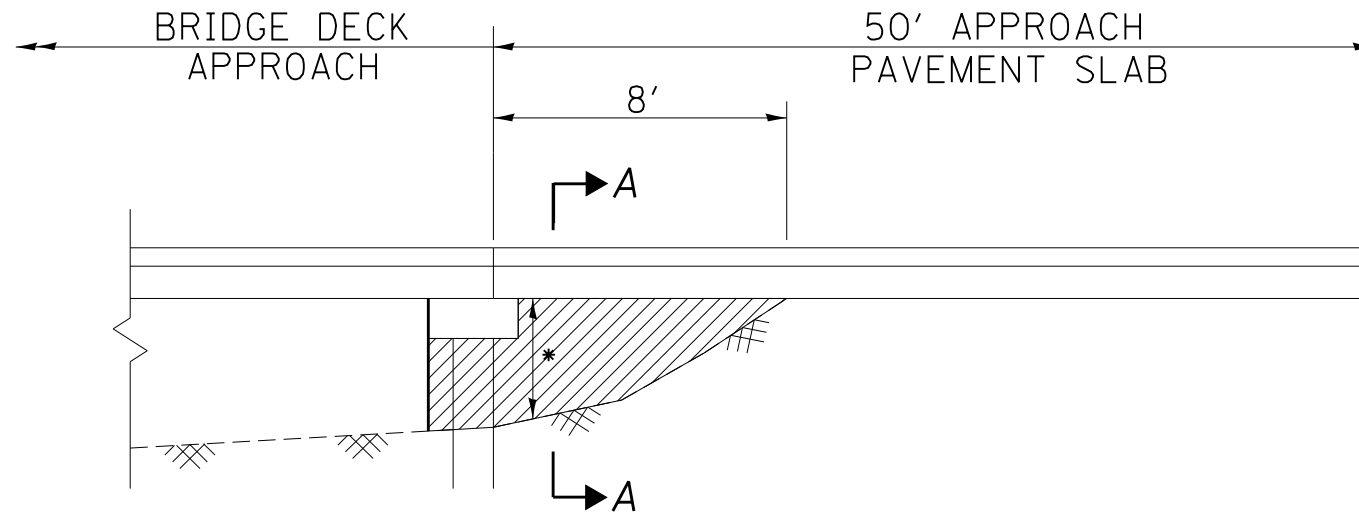
FILE NAME :	USER NAME : pyrzanawskib	DESIGNED - MVT	REVISED -
o:\p\work\p\stdat\pyrzanawskib\08318685	D:\41412-sht\plan.dgn	DRAWN - MVT	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/28/2013	DATE - 12-20-2011	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

U.S. 41 OVER DEERPATH ROAD  
PREFORMED JOINT STRIP SEAL  
S.N. 049-0033

SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A.P. RTE. 346	SECTION 1305-1(12)	COUNTY LAKE	TOTAL SHEETS 15	SHEET NO. 8
						CONTRACT NO. 60T65	
ILLINOIS FED. AID PROJECT							



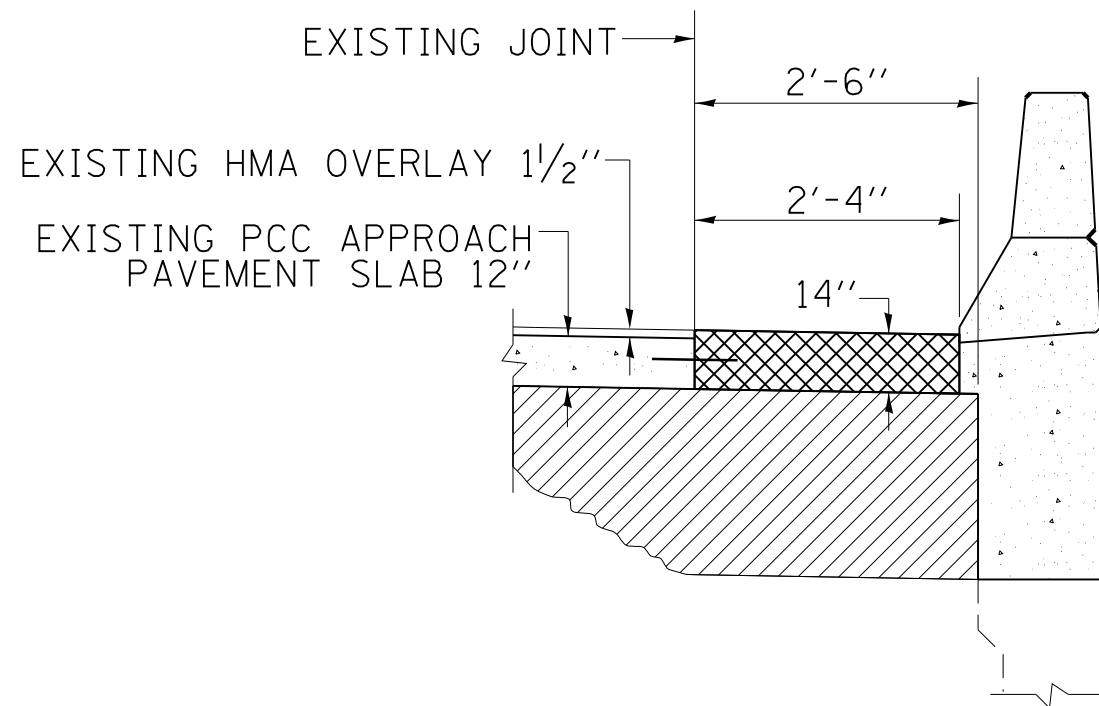


APPROACH SLAB ELEVATION

NOTE:

CONTROLLED LOW-STRENGTH MATERIAL SHALL BE USED TO FILL THE ENTIRE WIDTH OF THE APPROACHES. 70 FT FOR THE COMBINED NB & SB APPROACHES.

\* AVERAGE DEPTH OF FILL AREA = 4'-6"

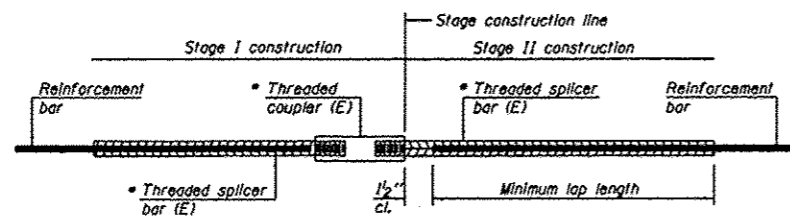


SECTION A-A

LEGEND:

- CONTROLLED LOW STRENGTH MATERIAL
- CLASS B PATCH (FULL DEPTH)

FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>U.S. 41 OVER DEERPATH ROAD APPROACH SLAB CROSS SECTION S.N. 049-0033</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\midot\midyja\0318685\0141412	-sht-plan.dgn	DRAWN -	REVISED -		346	1305-1(12)	LAKE	15	9			
#MODELNAME#	PLOT SCALE = 99.9998' / in.	CHECKED -	REVISED -		CONTRACT NO. 60T65			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/26/2012	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	



STANDARD BAR SPLICER ASSEMBLY

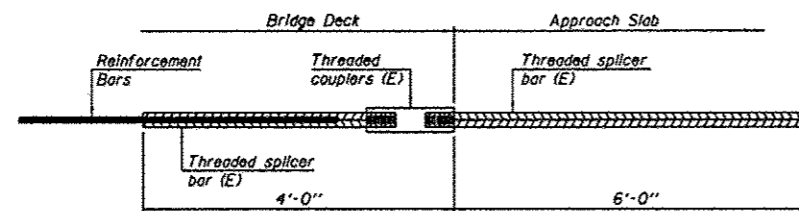
Bar size to be spliced	Minimum Lap Lengths					
	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, O.B Class C
- Table 2: Black bar, Top bar lap, O.B Class C
- Table 3: Epoxy bar, O.B Class C
- Table 4: Epoxy bar, Top bar lap, O.B Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

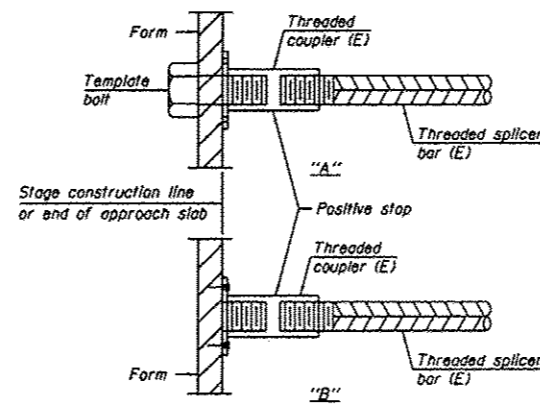
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Abutment, Joint O and 17	#5	32	4



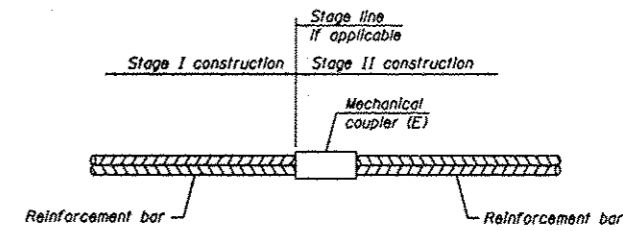
BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



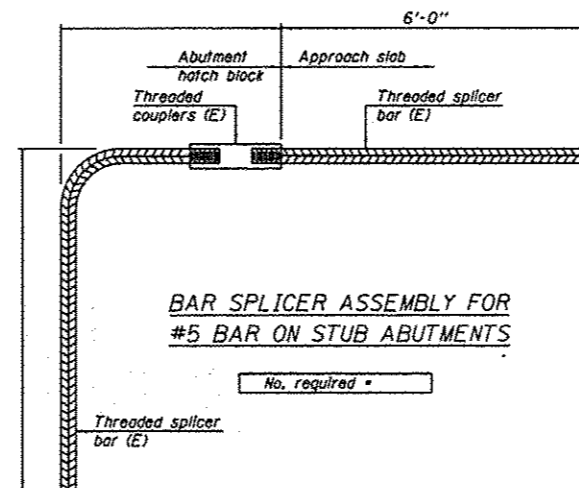
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

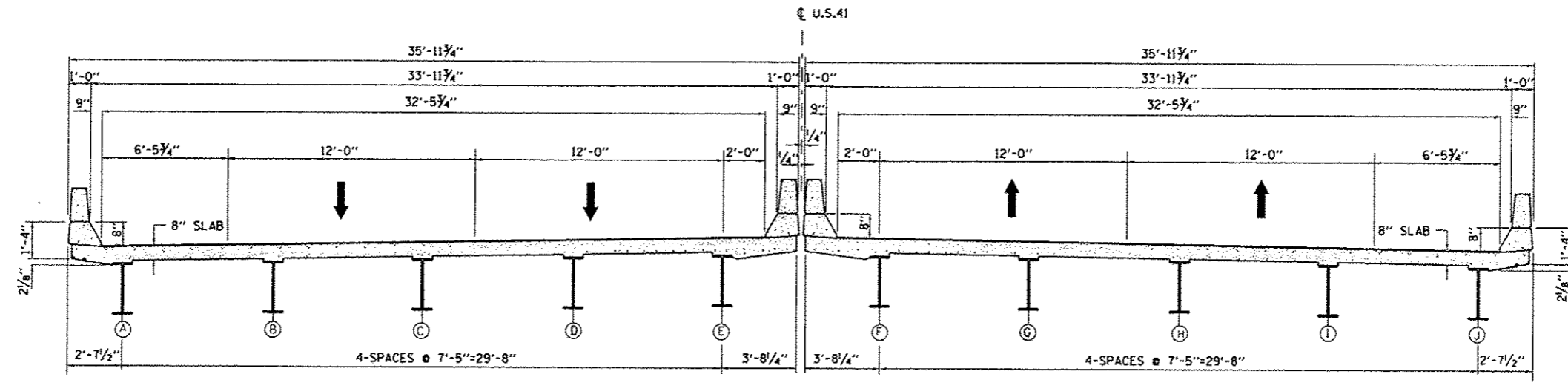
No. required =

**NOTES**  
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

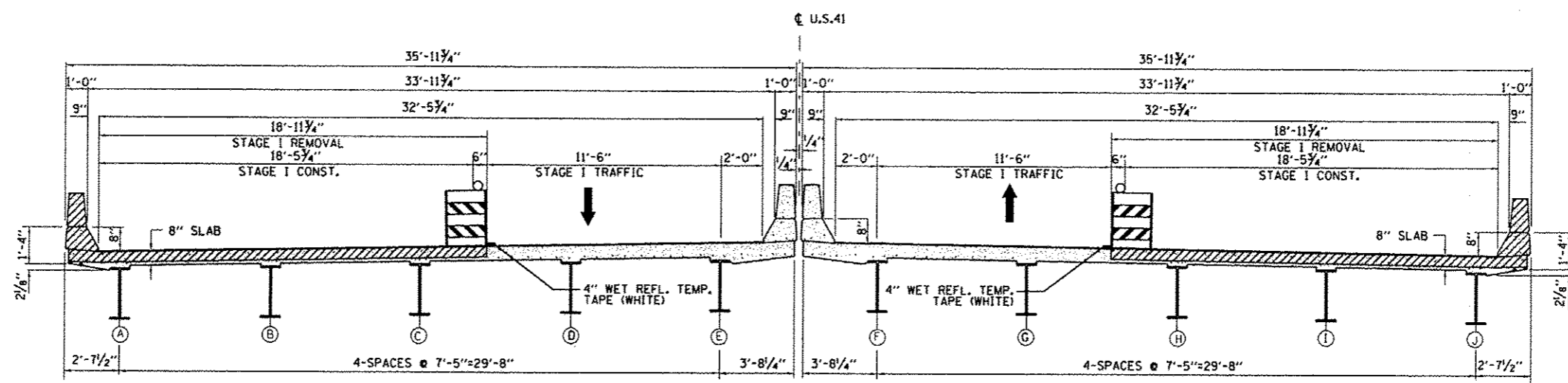
**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
 STRUCTURE NO. 049-0033**

BSD-1 1-27-12

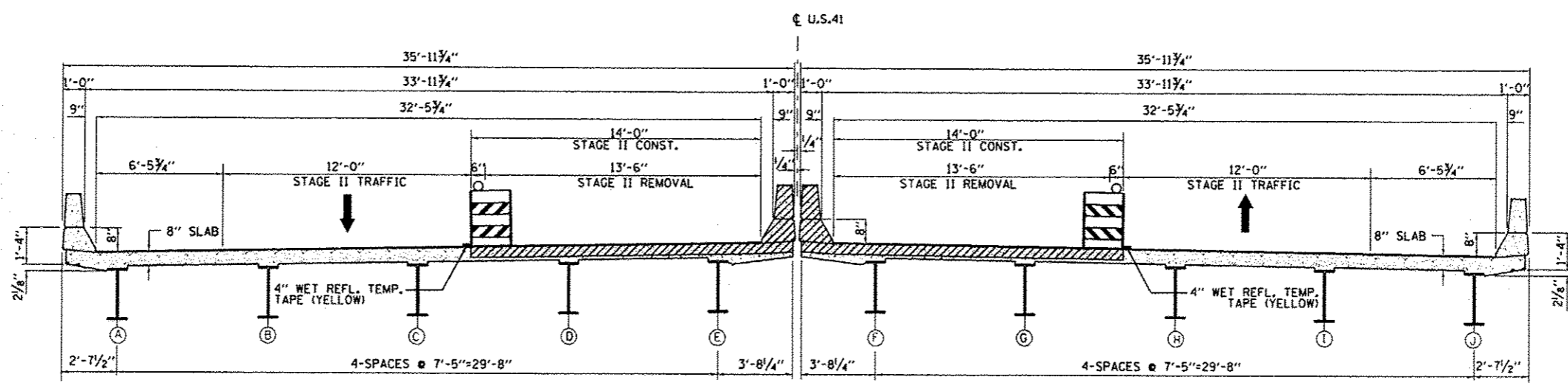
FILE NAME : c:\p_w\work\p\widet\pyrzanowski\0810608	USER NAME : pyrzanowski	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. 41 OVER DEERPATH ROAD BAR SPLICER ASSEMBLY S.N. 049-0033	F.A.P. RTE. 346	SECTION 130S-1(12)	COUNTY LAKE	TOTAL SHEETS 15	SHEET NO. 10	CONTRACT NO. 60T65 ILLINOIS FED. AID PROJECT
#MODELNAME:	PLOT SCALE : 99.9998 / 1 in.	CHECKED - DATE -	REVISED - REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.			
	PLOT DATE : 1/28/2013										



**EXISTING CROSS SECTION**



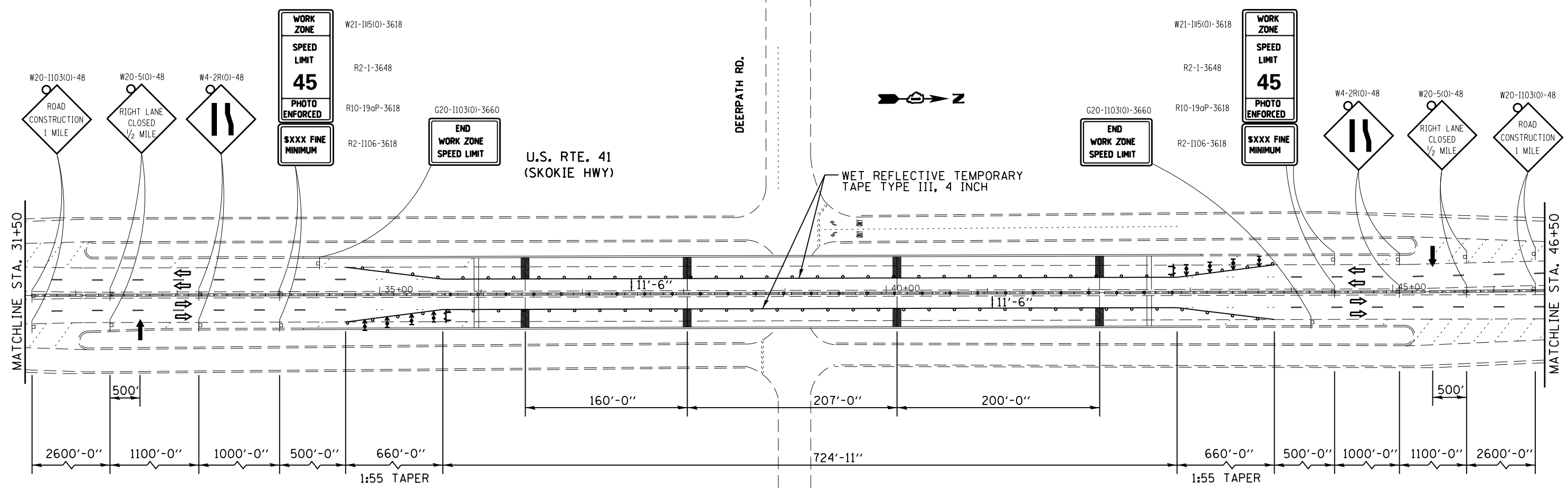
**STAGE I**



**STAGE II**

**LEGEND:**  
 CONCRETE REMOVAL

FILE NAME =	USER NAME = pyznowski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>U.S. 41 OVER DEERPATH ROAD STAGING CROSS SECTIONS S.N. 049-0033</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwork\p\eridat\pyznowski\b\0318685\0141412-sht-pln.dgn	0141412-sht-pln.dgn	DRAWN -	REVISED -		346	1305-1(12)	LAKE	15	11			
MODEL NAME =	PLOT SCALE = 99.9998 / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60T65				
	PLOT DATE = 1/28/2013	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				



**LEGEND**

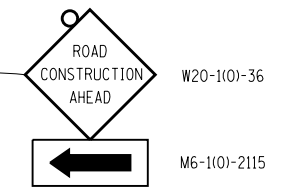
- ARROW BOARD
- TYPE III BARRICADE
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- EXPANSION JOINT RECONSTRUCTION AREA
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT @ 25' C-C

**NOTES:**

- TYPE III BARRICADES TO BE PLACED WHEN NO WORK IS BEING PERFORMED
- ADVANCE WARNING SIGNS & TRAFFIC CONTROL ON IL 41 (SKOKIE HWY.) SHALL BE ACCORDING TO STANDARD 701421 EXCEPT AS SHOWN. COST INCLUDED WITH "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".

U.S. RTE. 41  
(SKOKIE HWY)

DEERPATH RD.

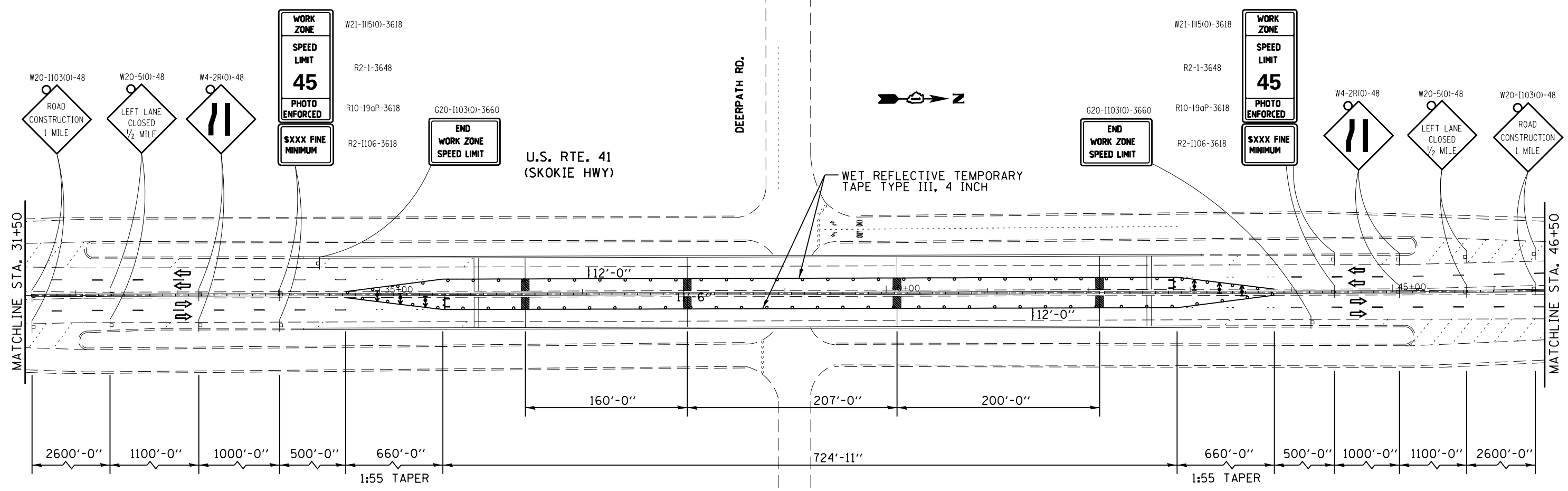


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\$MODELNAME\$	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/28/2013	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROPOSED STAGE I PLAN			
U.S. 41 (SKOKIE HWY) OVER DEERPATH RD.			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	1305-1(12)	LAKE	15	12
ILLINOIS FED. AID PROJECT				
CONTRACT NO. 60T65				



**LEGEND**

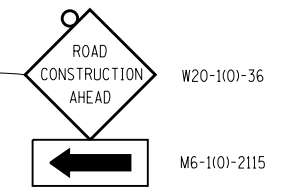
- ARROW BOARD
- TYPE III BARRICADE
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- EXPANSION JOINT RECONSTRUCTION AREA
- DIRECTION INDICATOR BARRICADE W/ STEADY BURN MONODIRECTIONAL LIGHT @ 25' C-C

**NOTES:**

- TYPE III BARRICADES TO BE PLACED WHEN NO WORK IS BEING PERFORMED
- ADVANCE WARNING SIGNS & TRAFFIC CONTROL ON US 41 (SKOKIE HWY.) SHALL BE ACCORDING TO STANDARD 701421 EXCEPT AS SHOWN. COST INCLUDED WITH "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".

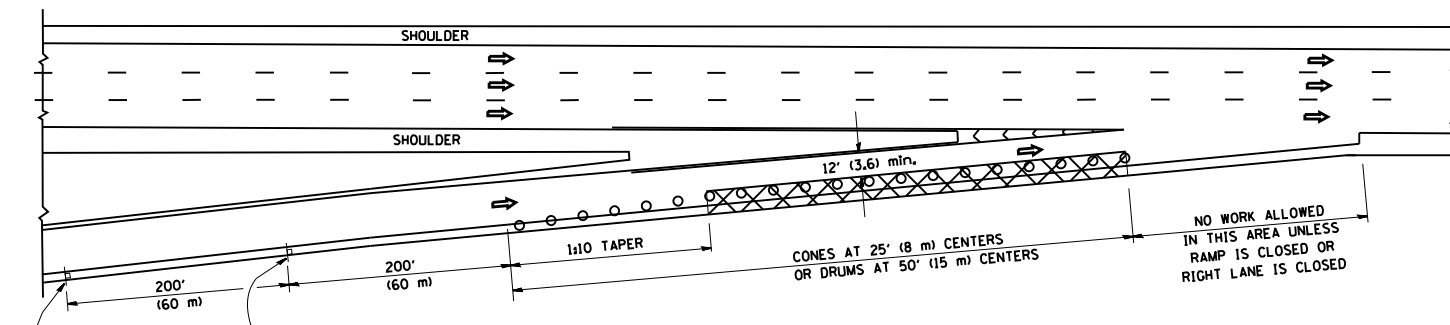
U.S. RTE. 41  
(SKOKIE HWY)

DEERPATH RD.

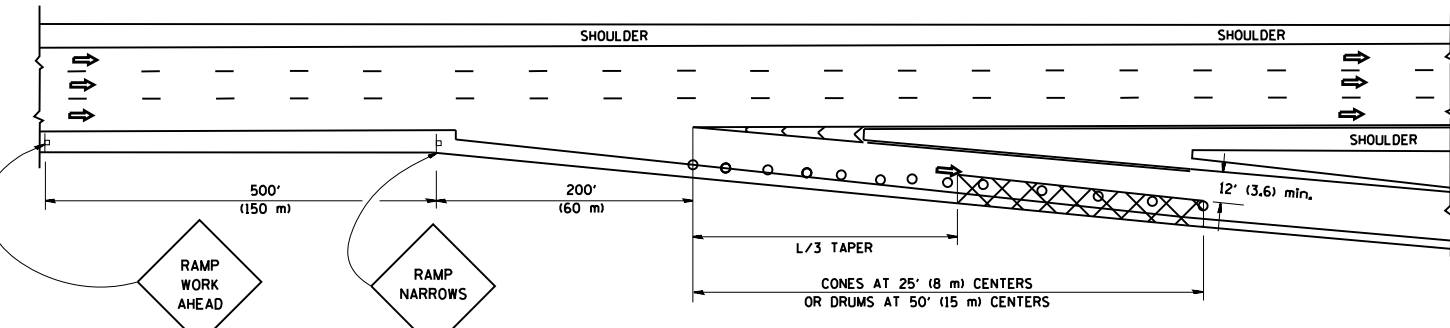


FILE NAME =	USER NAME = pyrzanowski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PROPOSED STAGE II PLAN U.S. 41 (SKOKIE HWY) OVER DEERPATH RD.</b>				F.A.P. RTE. 346	SECTION 1305-1(12)	COUNTY LAKE	TOTAL SHEETS 15	SHEET NO. 13
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	PLOT DATE = 1/28/2013	DATE -	REVISED -										

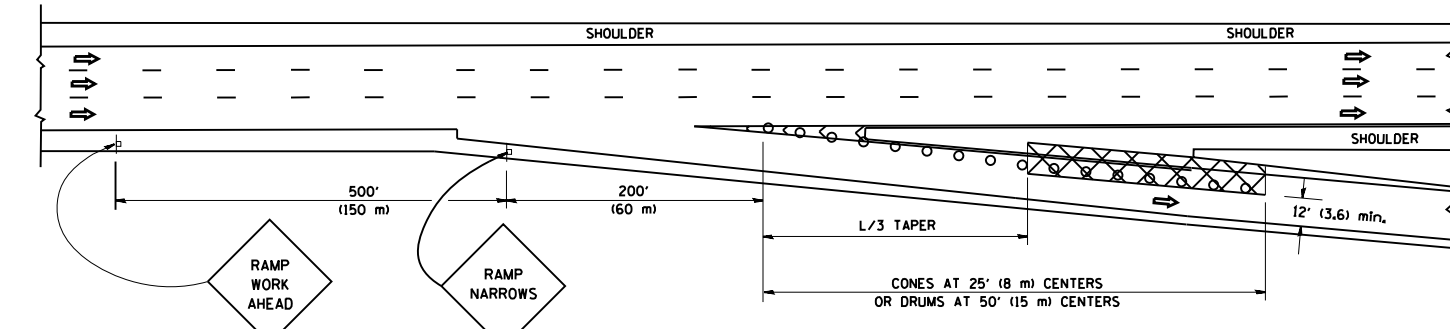
**PARTIAL RAMP CLOSURE DETAILS**



**TYPICAL ENTRANCE RAMP**



**TYPICAL EXIT RAMP**



**TYPICAL EXIT RAMP**

**SYMBOLS**

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

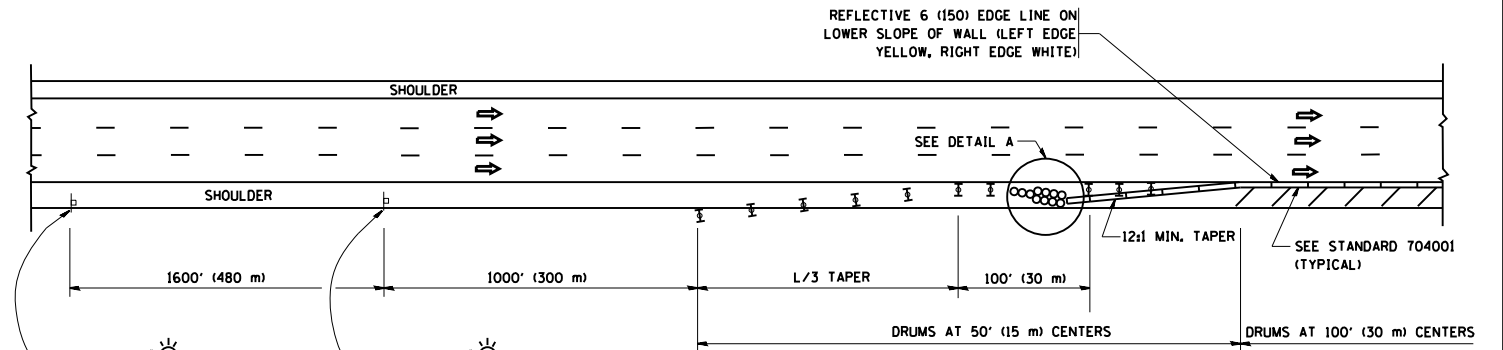
**GENERAL NOTES**

1. THE "L" DISTANCE EQUALS:
 

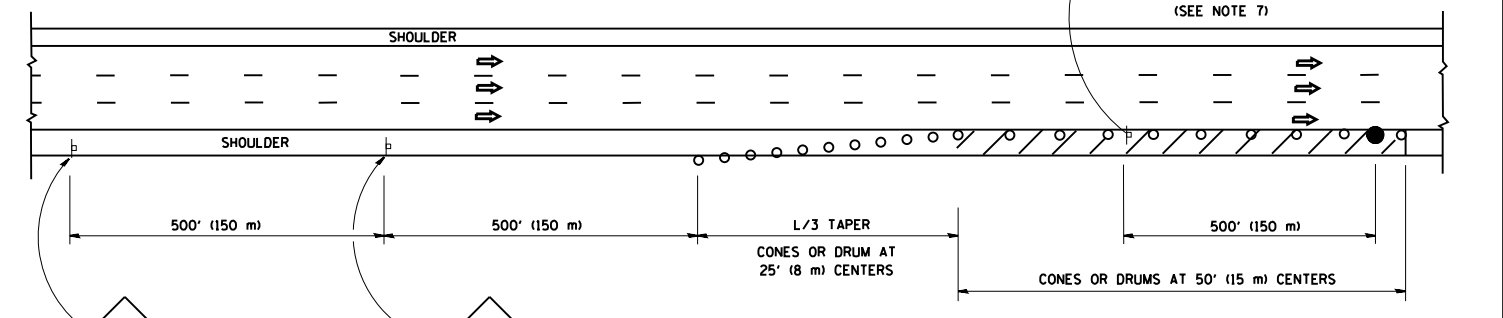
SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC    ENGLISH
	$L=0.65(WXS)$ $L=(WXS)$

W = WIDTH OF OFFSET IN FEET (METERS)  
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

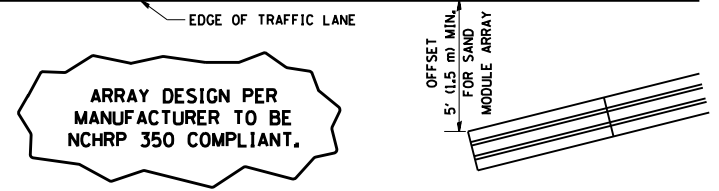
**SHOULDER CLOSURE DETAILS**



**PERMANENT SHOULDER CLOSURE**



**DAYTIME SHOULDER CLOSURE**



**DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)**

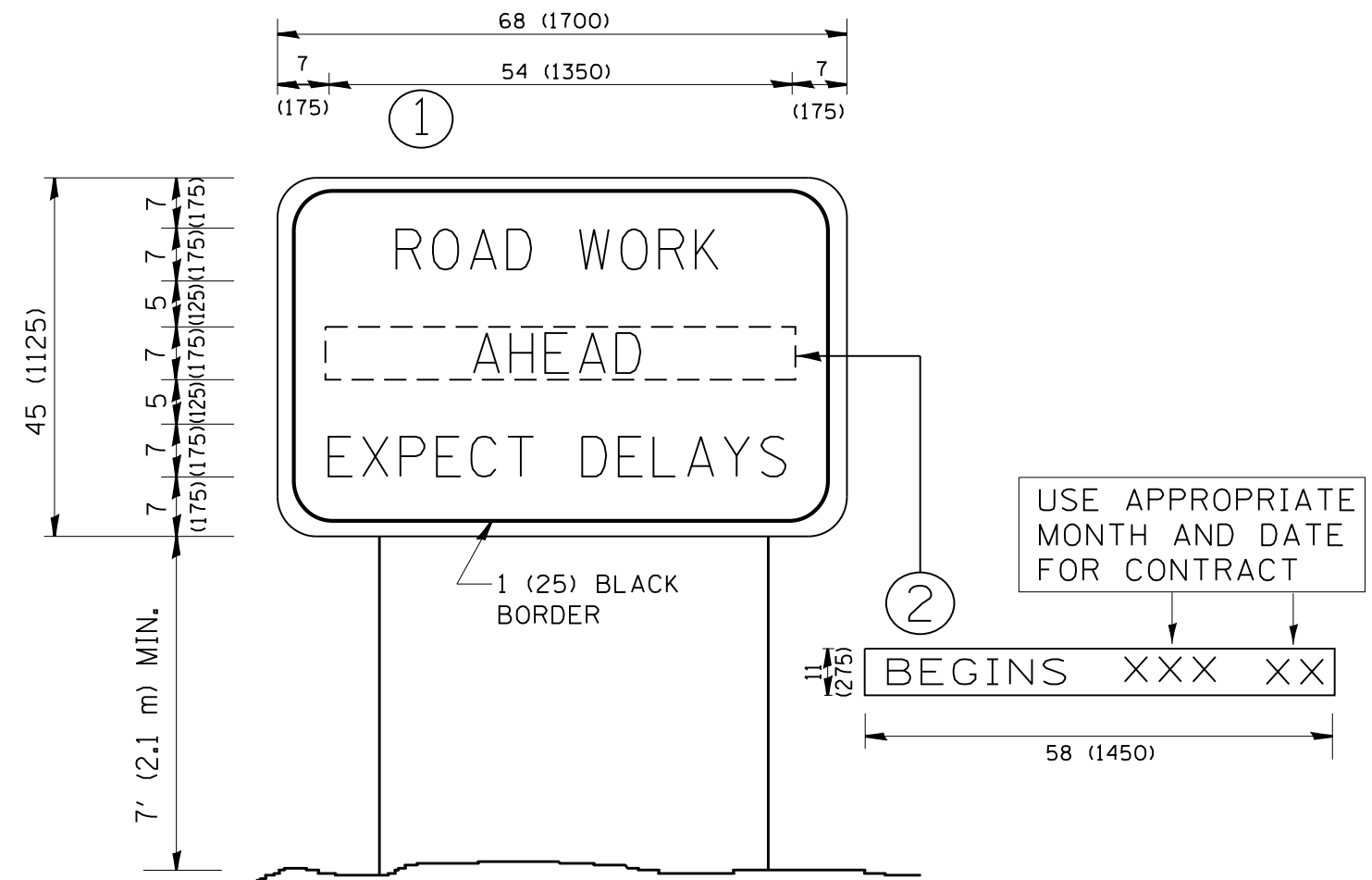
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.

THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = pyzranowskirb	DESIGNED -	REVISED - 04-03	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.P. RTE. 346	SECTION 1305-1(12)	COUNTY LAKE	TOTAL SHEETS 15	SHEET NO. 14
et:\pw_work\p1dot\pyzranowskirb\d0318688	D141412-sht-plan.dgn	DRAWN - D.W.S.	REVISED - J.A.F. 12-06			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60T65		
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - S.P.B. 01-07										
	PLOT DATE = 12/20/2012	DATE - 11-96	REVISED - S.P.B. 12-09										





**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

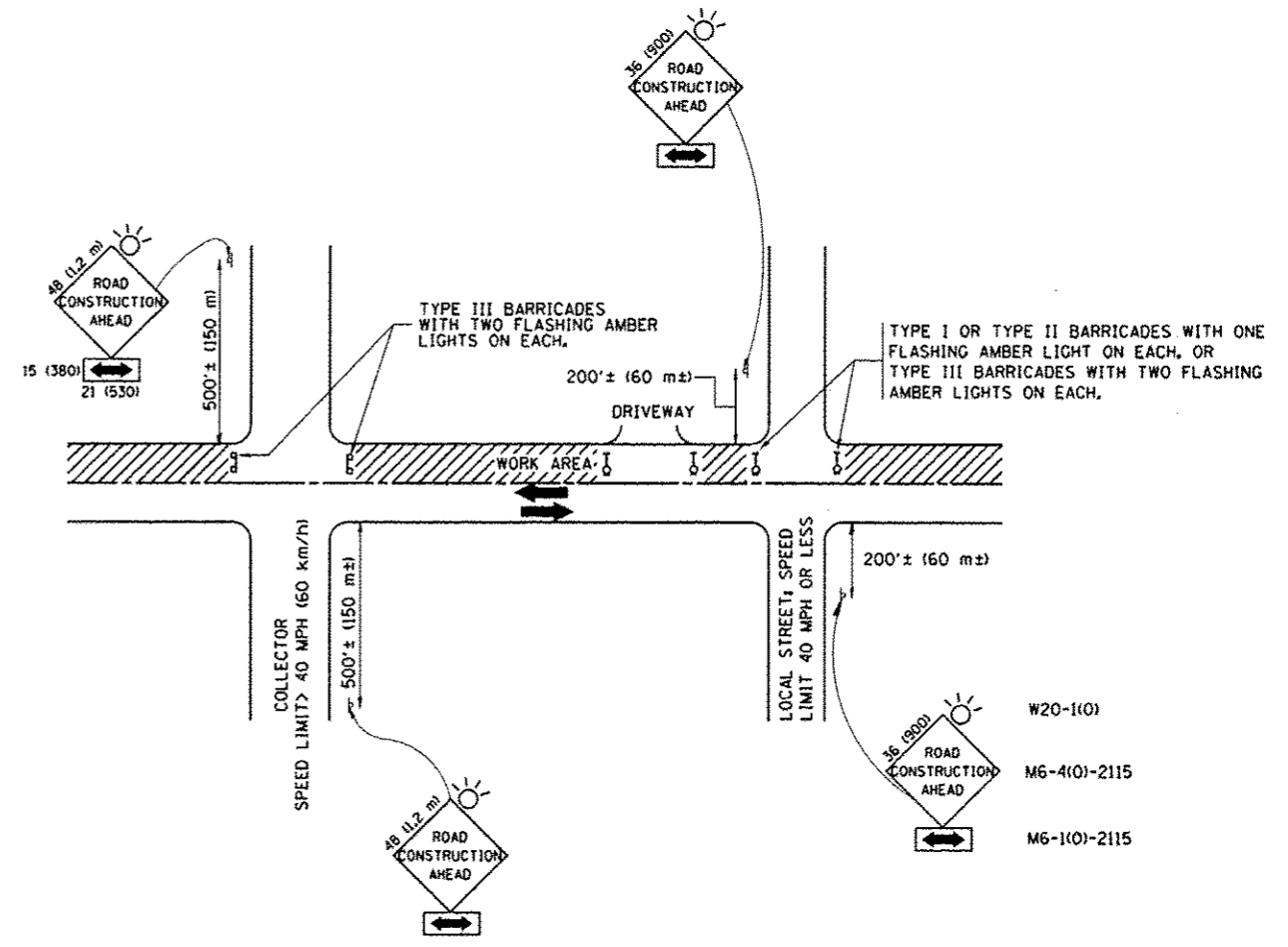
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 12/20/2012	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	1305-1(12)	LAKE	15	15
TC-22		CONTRACT NO. 60T65		
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

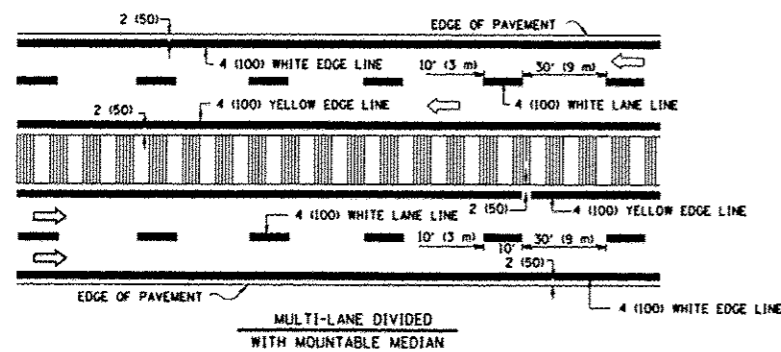
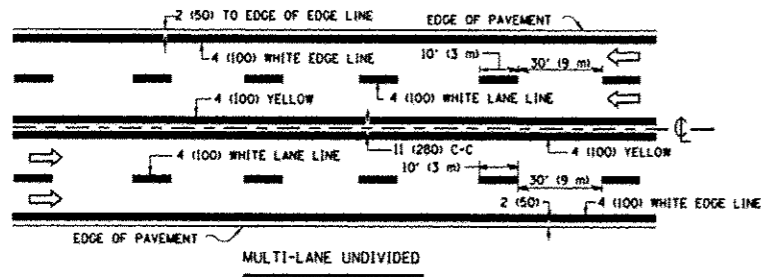
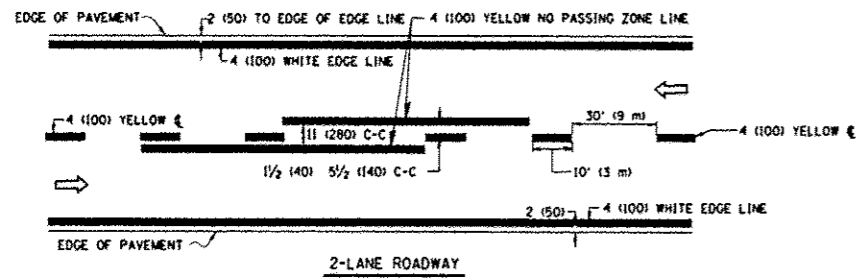
USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = pyzanoskiyb	DESIGNED - LMA	REVISED - J. OBERLE 10-18-95
ci:\pwwork\pwwid\pyzanoski-b\0318685	0141412-shr-plan.dgn	DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 100.0000 / 1"	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/28/2013	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

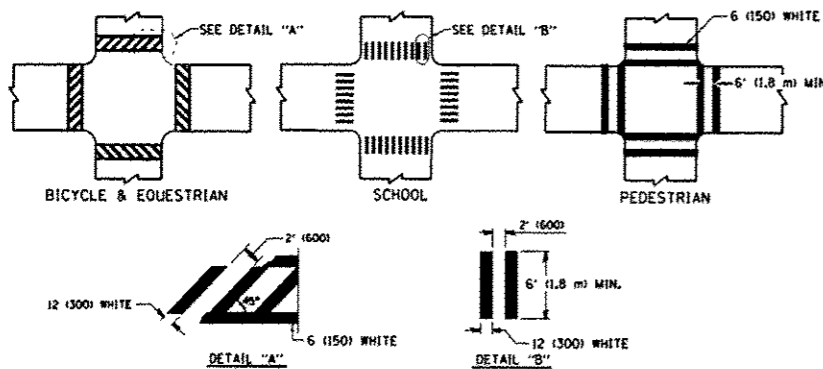
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		P.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	346	1305-(112)	LAKE	15	16A
STA. TO STA.		TC-10		CONTRACT NO. 60T65		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

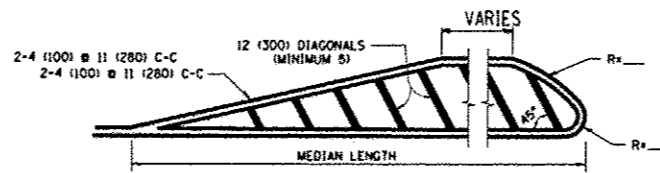
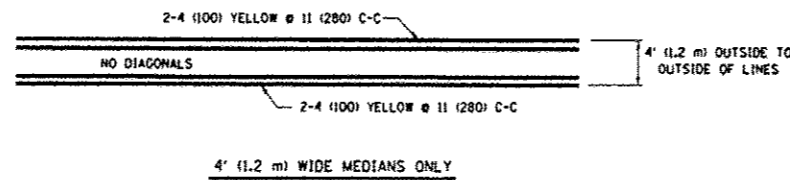


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**

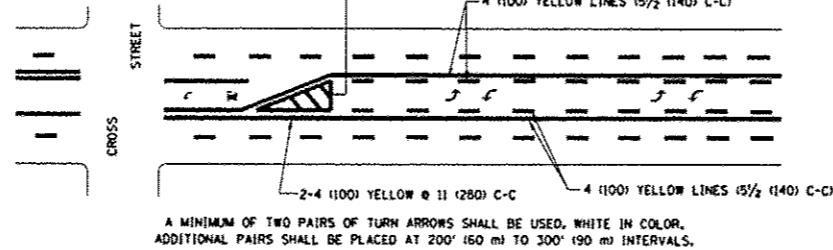


**TYPICAL CROSSWALK MARKING**



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**

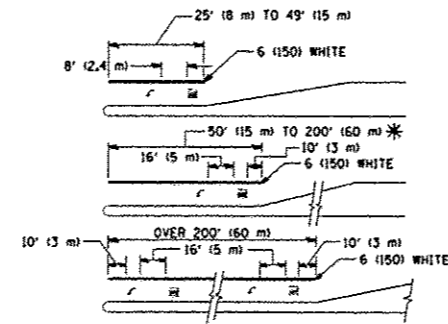


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

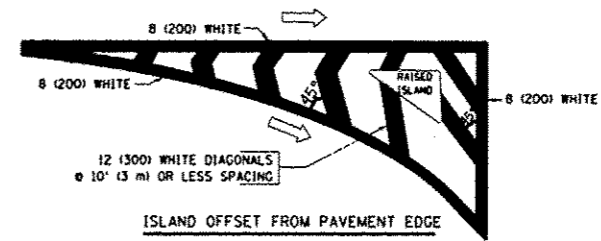
**TYPICAL PAINTED MEDIAN MARKING**



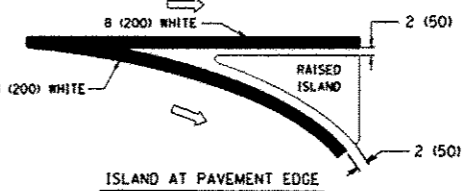
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m²) | AREA = 20.8 SQ. FT. (1.9 m²)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; IS 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R" = 3.6 SQ. FT. (0.33 m²) EACH "X" = 54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME: c:\p\work\p1dot\puzzenowskirb\0318695	USER NAME: puzzenowskirb	DESIGNED: EVERS	REVISED: T. RAMMACHER 10-27-94
	0141412-shs-plan.dgn	DRAWN:	REVISED: C. JUCIUS 09-09-09
	PLOT SCALE: 1/8" = 1'-0"	CHECKED:	REVISED:
	PLOT DATE: 1/29/2013	DATE: 03-19-90	REVISED:

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	130S-1121	LAKE	15	15B
TC-13		CONTRACT NO. 60T65		
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				