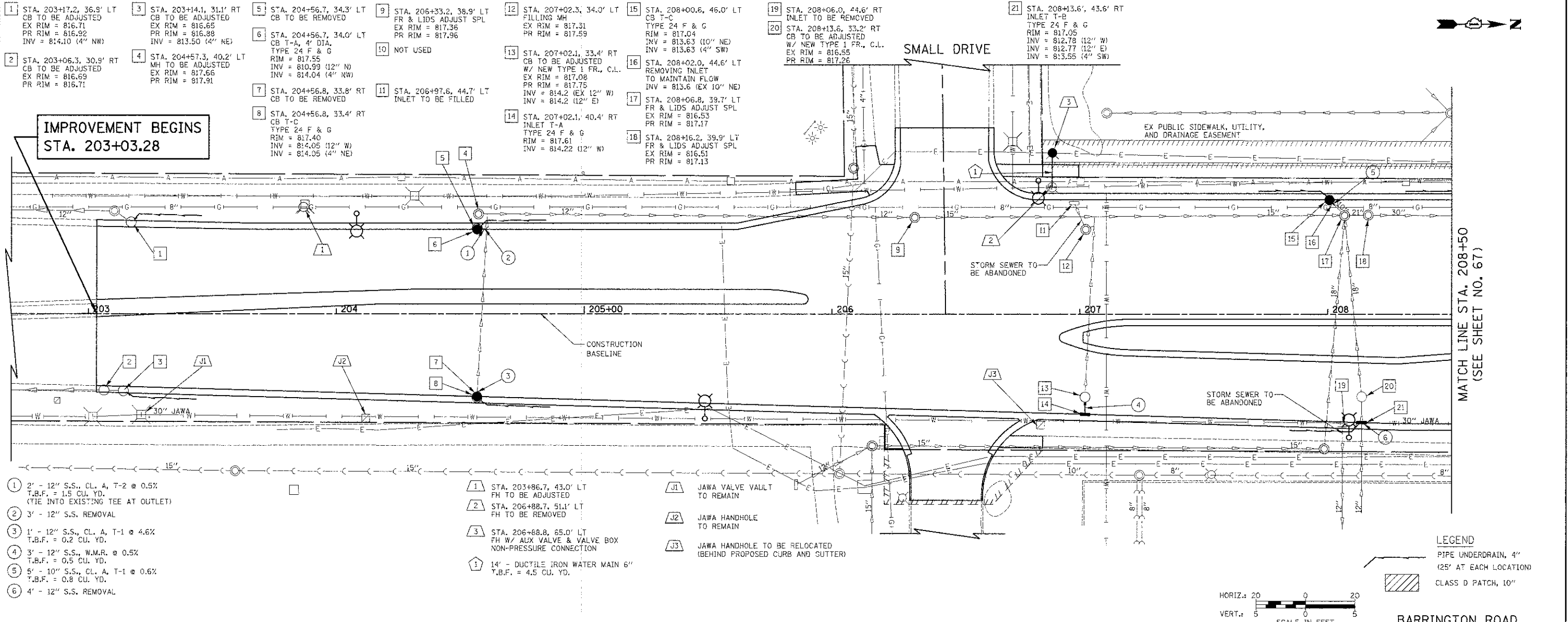


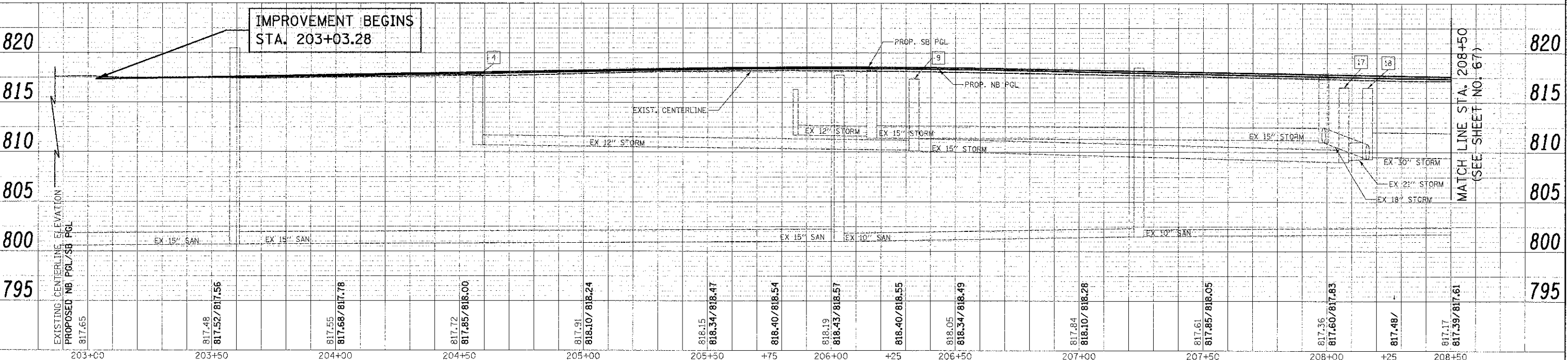
DATE: _____ BY: _____
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- 1 2' - 12" S.S., CL. A, T-2 @ 0.5% T.B.F. = 1.5 CU. YD. (TIE INTO EXISTING TEE AT OUTLET)
- 2 3' - 12" S.S. REMOVAL
- 3 1' - 12" S.S., CL. A, T-1 @ 4.6% T.B.F. = 0.2 CU. YD.
- 4 3' - 12" S.S., W.M.R. @ 0.5% T.B.F. = 0.5 CU. YD.
- 5 5' - 10" S.S., CL. A, T-1 @ 0.6% T.B.F. = 0.8 CU. YD.
- 6 4' - 12" S.S. REMOVAL
- 1 STA. 203+86.7, 43.0' LT FH TO BE ADJUSTED
- 2 STA. 206+88.7, 51.1' LT FH TO BE REMOVED
- 3 STA. 206+88.8, 65.0' LT FH W/ AUX VALVE & VALVE BOX NON-PRESSURE CONNECTION
- 4 14" - DUCTILE IRON WATER MAIN 6" T.B.F. = 4.5 CU. YD.
- J1 JAWA VALVE VAULT TO REMAIN
- J2 JAWA HANDHOLE TO REMAIN
- J3 JAWA HANDHOLE TO BE RELOCATED (BEHIND PROPOSED CURB AND GUTTER)

- 5 STA. 203+17.2, 36.9' LT CB TO BE ADJUSTED EX RIM = 816.71 PR RIM = 816.92 INV = 814.10 (4" NW)
- 6 STA. 203+06.3, 30.9' RT CB TO BE ADJUSTED EX RIM = 816.69 PR RIM = 816.71
- 7 STA. 204+14.1, 31.1' RT CB TO BE ADJUSTED EX RIM = 816.65 PR RIM = 816.88 INV = 813.50 (4" NE)
- 8 STA. 204+57.3, 40.2' LT MH TO BE ADJUSTED EX RIM = 817.66 PR RIM = 817.91
- 9 STA. 204+56.7, 34.0' LT CB TO BE REMOVED RIM = 817.55 INV = 810.99 (12" N)
- 10 STA. 204+56.7, 34.0' LT CB T-A, 4" DIA. TYPE 24 F & G RIM = 817.55 INV = 814.04 (4" NW)
- 11 STA. 204+56.8, 33.8' RT CB TO BE REMOVED
- 12 STA. 204+56.8, 33.4' RT CB T-C TYPE 24 F & G RIM = 817.40 INV = 814.05 (12" W)
- 13 STA. 204+56.8, 33.8' RT CB TO BE REMOVED
- 14 STA. 206+97.6, 44.7' LT INLET TO BE FILLED
- 15 STA. 207+02.3, 34.0' LT FILLING MH EX RIM = 817.31 PR RIM = 817.59
- 16 STA. 207+02.1, 33.4' RT CB TO BE ADJUSTED W/ NEW TYPE 1 FR., C.L. EX RIM = 817.08 PR RIM = 817.75 INV = 814.2 (EX 12" W)
- 17 STA. 207+02.1, 40.4' RT INLET T-A TYPE 24 F & G RIM = 817.61 INV = 814.22 (12" W)
- 18 STA. 208+00.6, 46.0' LT CS T-C TYPE 24 F & G RIM = 817.04 INV = 813.63 (10" NE)
- 19 STA. 208+02.0, 44.6' LT REMOVING INLET TO MAINTAIN FLOW INV = 813.6 (EX 10" NE)
- 20 STA. 208+06.8, 39.7' LT FR & LIDS ADJUST SPL EX RIM = 816.53 PR RIM = 817.17
- 21 STA. 208+16.2, 39.9' LT FR & LIDS ADJUST SPL EX RIM = 816.51 PR RIM = 817.13
- 22 STA. 208+06.0, 44.6' RT INLET TO BE REMOVED TYPE 24 F & G RIM = 817.05 INV = 812.78 (12" W)
- 23 STA. 208+13.6, 33.2' RT CB TO BE ADJUSTED W/ NEW TYPE 1 FR., C.L. EX RIM = 816.55 PR RIM = 817.26 INV = 812.77 (12" E)
- 24 STA. 208+13.6, 43.6' RT INLET T-B TYPE 24 F & G RIM = 817.05 INV = 812.78 (12" W)
- 25 STA. 208+13.6, 43.6' RT CB TO BE ADJUSTED W/ NEW TYPE 1 FR., C.L. EX RIM = 816.55 PR RIM = 817.26 INV = 812.77 (12" E)
- 26 STA. 208+13.6, 43.6' RT INV = 813.55 (4" SW)



FILE NAME: ... \2329\road\sheet\2325_DU_01.dwg	USER NAME: dj	DESIGNED: JAT	REVISED:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BARRINGTON ROAD DRAINAGE AND UTILITIES	F.A.U. SITE: 1322	SECTION: 01-00074-00-CH	COUNTY: COOK	TOTAL SHEETS: 66
PLOT SCALE: 28,0000' / 1"	CHECKED: DJK	REVISOR:	SHEET NO. 1 OF 15 SHEETS			STA. 203+03.28 TO STA. 208+50	CONTRACT NO. 63629		
PLOT DATE: 8/17/2012	DATE: 8-20-12	REVISOR:	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-CM-8003651						