

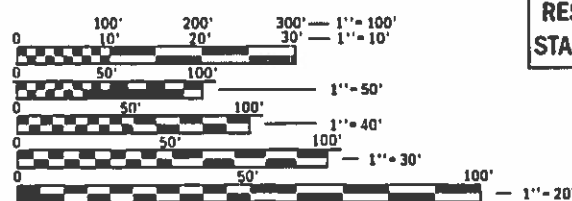
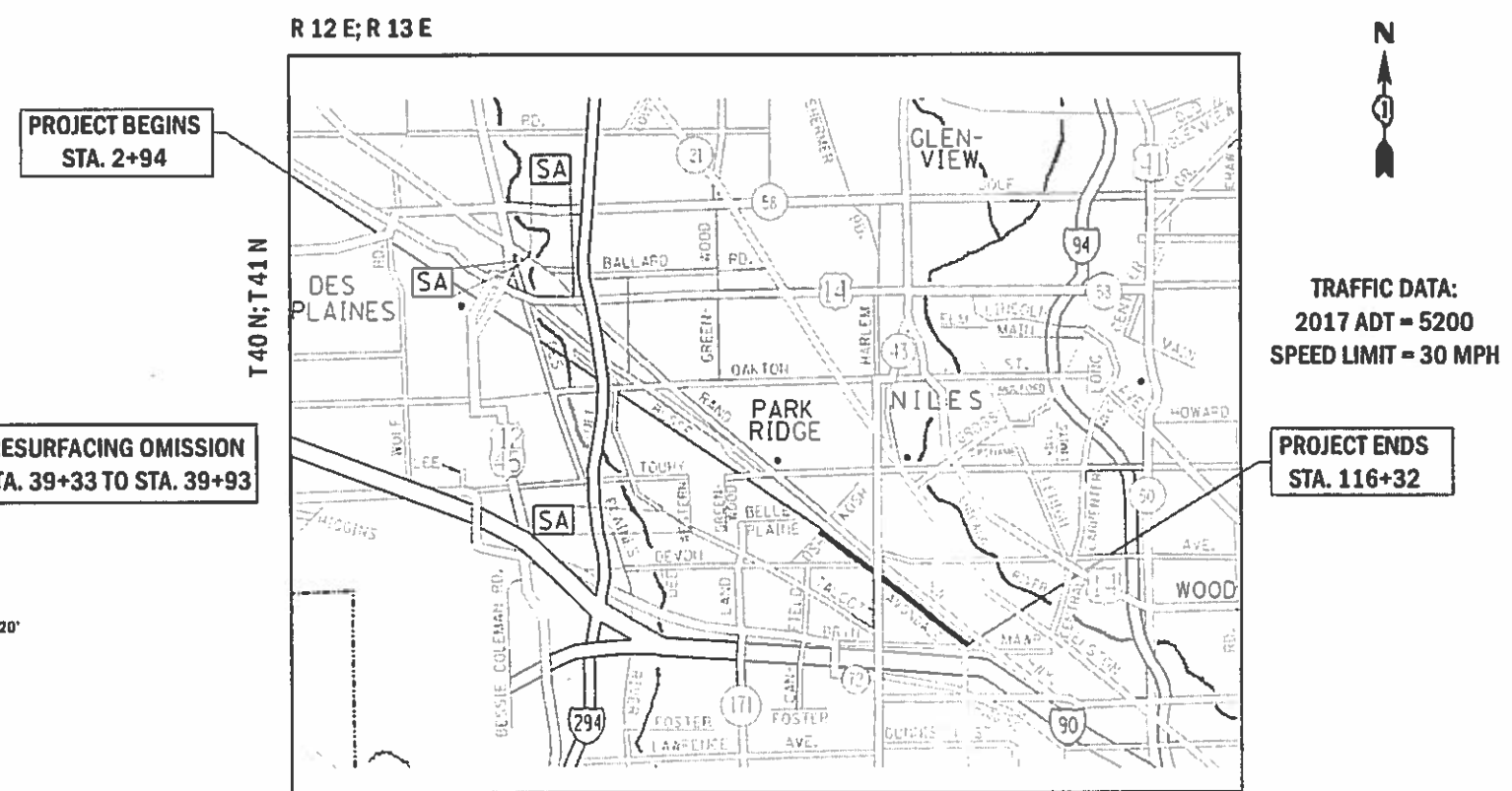
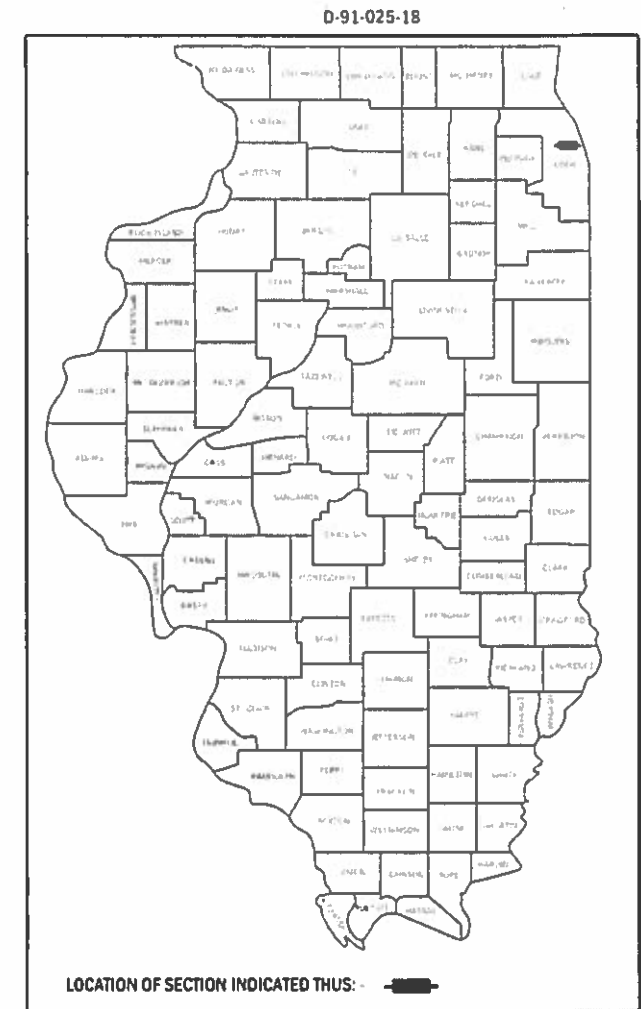
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OR ATE	SECTION	COUNT	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	1
CONTRACT NO. 62F66				

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN THE CITY OF CHICAGO

**PROPOSED
HIGHWAY PLANS**
ROUTE (OR 130) AVONDALE AVE.
OLIPHANT AVE. TO BRYN MAWR AVE.
SECTION: 2017-037RS
PROJECT: STP-8TYD(252)
RESURFACING (3P), PEDESTRIAN RAMPS
COOK COUNTY
C-91-035-18



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER DAN WILGREEN (847) 705-4240
PROJECT MANAGER FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62F66

PARK RIDGE AND RIVER FOREST TOWNSHIP

GROSS LENGTH = 11,338 FT. = 2.15 MILE
NET LENGTH = 11,278 FT. = 2.14 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED ~~December 4~~ 2018

Anthony A. Rungt REGIONAL ENGINEER
Feb 19 2019
EA. ETK
ENGINEER OF DESIGN AND ENVIRONMENT

Paul J. [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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INDEX OF SHEETS

SHEET NO.	DESCRIPTION	STATE STANDARDS	GENERAL NOTES:
1	TITLE SHEET	000001-07 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS	
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES	424001-11 PERPENDICULAR CURB RAMPS FOR SIDEWALKS	① BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
3-5	SUMMARY OF QUANTITIES	424006-04 DIAGONAL CURB RAMPS FOR SIDEWALKS	
6-7	TYPICAL SECTIONS	424011-04 CORNER PARALLEL CURB RAMPS FOR SIDEWALKS	② THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF CHICAGO.
8-12	ROADWAY & PAVEMENT MARKING PLANS	424016-05 MID-BLOCK CURB RAMPS FOR SIDEWALKS	
13-26	ADA SIDEWALK DETAIL SHEETS	424021-05 DEPRESSED CORNER FOR SIDEWALKS	③ THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
27	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER > = 15' (BD-01)	424026-03 ENTRANCE/ALLEY PEDSTRIAN CROSSINGS	④ ALL HOT-MIX ASPHALT PAVEMENT PATCHING SHALL BE CLASS D, LOCATIONS TO BE DETERMINED IN FIELD BY RESIDENT ENGINEER.
28	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB < 15' (BD-02)	442201-03 CLASS C AND D PATCHES	⑤ BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALLL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
29	DETAILS FOR FRAMES AND LIDS WITH MILLING (BD-08)	604001-04 FRAMES AND LIDS TYPE 1	
30	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT (BD-22)	604091-03 FRAME AND GRATE TYPE 24	⑥ WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 M/M) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/HR), WITH WRITTEN APPROVAL FROM THE ENINGEER, A MAXIMUM GRADE DIFFERNTIAL OF 3 INCHES (75 MM) MAYBE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
31	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701011-04 OFF-ROAD MOVING OPERATIONS 2L, 2W, DAY ONLY	⑦ 10 FEET (3 METER) TRANSITIONAL SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNITY PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
32	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)	701101-05 OFF ROAD OPERATIONS MULTILANE, 15' TO 24" FROM PAVEMENT EDGE	
33	CITY OF CHICAGO DETECTABLE WARNINGS (BD-58)	701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS	⑧ THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.
34	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)	701311-03 LANE CLOSURE, 2L, 2W MOVING OPERATIONS DAY ONLY	⑨ THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER VIA EMAIL AT DON.CHIARUGI@ILLINOIS.GOV A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
35	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)	701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≤ 40 MPH	⑩ WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
36	ARTERIAL ROAD INFORMATION SIGNING (TC-22)	701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVDED	⑪ THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
37-39	CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS (TC-24)	701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN	⑫ ALL SIDEWALK RAMPS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO CURRENT ADA REQUIRMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY ENGINEER.
		701611-01 URBAN HALF CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN	⑬ SIDEWALK REMOVAL AND P.C.C. SIDEWALK LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
		701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE	⑭ NIGHTTIME FLAGGERS OR WORKERS SHALL BE EQUIPPED WITH A FLUORESCENT ORANGE OR FLUORESCENT YELLOW/GREEN VEST MEETING THE REQUIREMENTS OF ANSI/ISEA 107-2004 FOR CONSPICUITY CLASS 3 GARMENTS.
		701901-08 TRAFFIC CONTROL DEVICES	⑮ PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
		780001-05 TYPICAL PAVEMENT MARKING	⑯ THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
		814001-03 HANDHOLES	⑰ CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT OF PROPOSED TREE REMOVAL ITEMS.

CITY OF CHICAGO NOTES

- ① BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED).
- ② THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- ③ ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWERS' STANDARDS.
- ④ PERFORATED LIDS SHALL BE PLACED ON ALL MANHOLES AND CATCH BASINS.
- ⑤ BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF THE IMPROVEMENT CAN BE OBTAINED IN SUITE 410 - 333 SOUTH STATE STREET, CHICAGO, IL 60604. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPLACEMENT OF ANY BENCH MONUMENT IF DAMAGED OR DESTROYED DURING CONSTRUCTION.
- ⑥ ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND LIDS ON SEWER STRUCTURES, SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND LIDS.
- ⑦ OPEN LID DRAINAGE STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.
- ⑧ CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- ⑨ PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR RESURFACING WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF SEWERS' PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION.
- ⑩ THE CONTRACTOR SHALL TAKE EXTRA CARE AND PRECAUTION WHEN REPAIRING THE CONCRETE CURB AND GUTTER AT LOCATIONS WHERE EXISTING TREES ARE PRESENT.
- ⑪ THE DEPARTMENT OF WATER MANAGEMENT CITY OF CHICAGO HAS IDENTIFIED FIVE (5) FEEDER MAIN ISOLATION VALVES THAT MUST BE TEMPORARILY RAMPED. IT IS REQUIRED THAT THIS DEPARTMENT HAS UNRESTRICTED ACCESS TO THESE FACILITIES AT ALL TIMES. THESE VALVE BASIN LIDS SHALL NOT BE INCLUDED IN THE "FRAMES AND LIDS TO BE ADJUSTED SPECIAL" AND CAN NOT BE COVERED BY A METAL PLATE DURING CONSTRUCTION. CONTACT BRIAN MCGAHAN AT BRIAN.MCGAHAN@CTRWATER.NET FROM THE DEPARTMENT OF WATER MANAGEMENT CITY OF CHICAGO FOR STRUCTURE LOCATIONS.
- ⑫ CURB RAMPS AND LANDING (KEYSTONE) TO BE CONSTRUCTED WITH 8" THICK CONCRETE AT ALL TRAFFIC SIGNALIZED INTERSECTIONS AND INDUSTRIAL STREET INTERSECTIONS. AT ALL OTHER LOCATIONS, 5" THICK CONCRETE TO BE USED.

FILE NAME =	USER NAME = valledolidv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.) INDEX OF SHEETS STATE STANDARDS AND GENERAL NOTES	OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI0258\Drawings\Design\DI02518-sht-gennote.dgn	PLLOT SCALE = 60000.0000' / in.	CHECKED -	REVISED -			130	2017-037RS	COOK	39	2	
Default	PLLOT DATE = 12/13/2018	DATE -	REVISED -			CONTRACT NO. 62F66					
						SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 ROADWAY				
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	166	166				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	30	30				
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	UNIT	16	16				
20200100	EARTH EXCAVATION	CU YD	44	44				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	2600	2600				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	49	49				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	49	49				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	49	49				
25100115	MULCH, METHOD 2	ACRE	0.2	0.2				
25200100	SODDING	SO YD	2600	2600				
25200200	SUPPLEMENTAL WATERING	UNIT	65	65				
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	100	100				
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	20	20				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	36905	36905				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	82	82				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 ROADWAY				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2232	2232				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	573	573				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	417	417				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	4545	4545				
42001300	PROTECTIVE COAT	SO YD	4047	4047				
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	150	150				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	20	20				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	3384	3384				
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SO FT	718	718				
44000100	PAVEMENT REMOVAL	SO YD	32	32				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	54101	54101				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	290	290				
44000600	SIDEWALK REMOVAL	SO FT	3812	3812				

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		DATE -	REVISED -			CONTRACT NO. 62F66					
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					URBAN TOTAL QUANTITIES	80% FED 20% STATE 0005 ROADWAY	CONSTRUCTION TYPE CODE								
CODE NO	ITEM	UNIT									CODE NO	ITEM	UNIT						
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	1860	1860						* 66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3					
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SO YD	705	705						* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1					
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SO YD	374	374						* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	15	15					
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SO YD	338	338						* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	100	100						67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	80	80						67100100	MOBILIZATION	LSUM	1	1					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	54	54						70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	10	10						70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	1					
60255500	MANHOLES TO BE ADJUSTED	EACH	2	2						70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	LSUM	1	1					
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2						70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	10	10						70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	65	65						70300100	SHORT TERM PAVEMENT MARKING	FOOT	7376	7376					
60404940	FRAMES AND GRATES TYPE 23	EACH	4	4															
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	40	40															
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	35	35															
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	44	44															

FILE NAME =	USER NAME = valladariv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.) SUMMARY OF QUANTITIES	OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED -			CONTRACT NO. 62F66					
		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

*= SPECIALTY ITEM

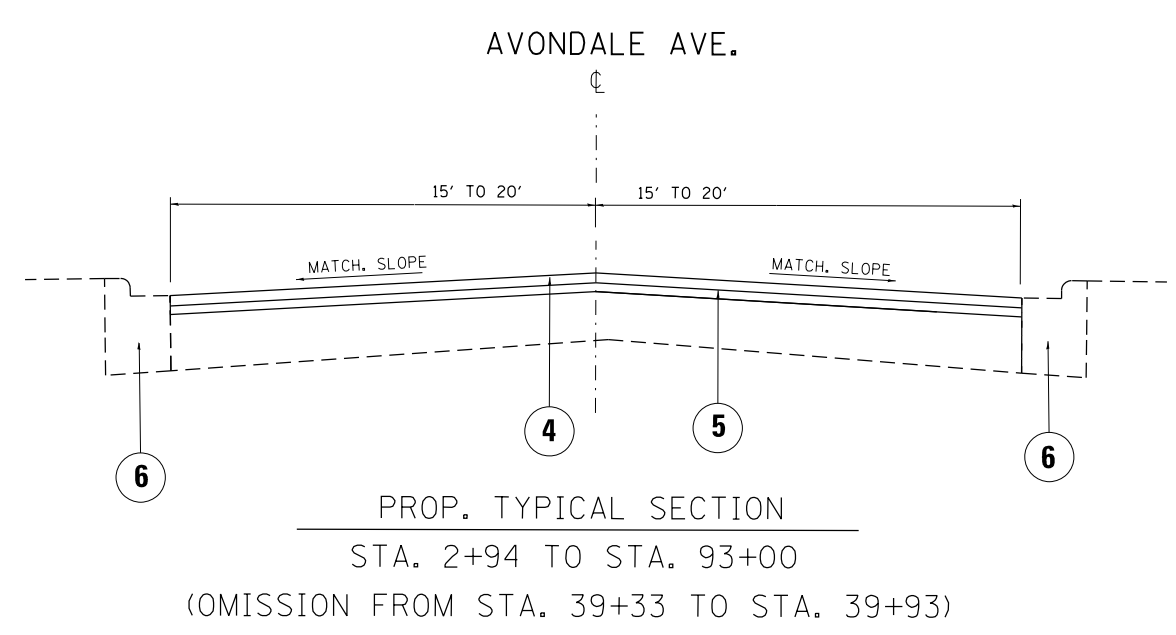
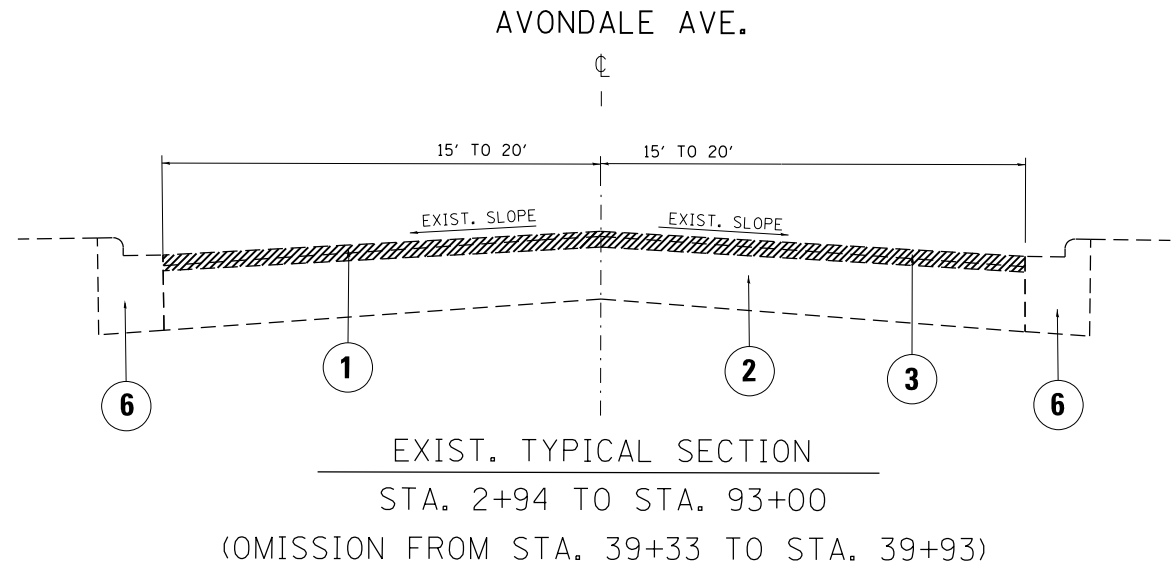
SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005 ROADWAY					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	2459	2459					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	78	78					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	5254	5254					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2119	2119					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	61	61					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1769	1769					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	348	348					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	1844	1844					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	78	78					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5254	5254					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2119	2119					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	61	61					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1769	1769					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	348	348					
* SPECIALTY ITEMS									

SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005 ROADWAY					
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1					
K0029618	WEED CONTROL, BROADLEAF IN TURF	GALLON	1	1					
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1					
X0327036	BIKE PATH REMOVAL	SO YD	65	65					
* X4240800	DETECTABLE WARNINGS (SPECIAL)	SO FT	423	423					
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL VARIABLE DEPTH	SO YD	1600	1600					
△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	350	350					
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	67	67					
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	5198	5198					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	15089	15089					
△ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	65	65					
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52					
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1					
Z0064800	SELECTIVE CLEARING	UNIT	5	5					

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pw:\N\LOB\4E\BID\INT\Illinois.gov\PI\DOT\Documents\DOT	Offices\District\Projects\DI025\B\CAD\Drawn	BY	REVISOR			130	2017-037RS	COOK	39	5	
	PLOT SCALE = 100,000' / 1in.	CHECKED -	REVISOR			CONTRACT NO. 62F66					
	PLOT DATE = 12/13/2018	DATE -	REVISOR			SCALE:	SHEET NO.	OF	SHEETS	STA.	TO

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LEGEND

- ① EXISTING HMA PAVEMENT, VARIES 4" (±)
- ② EXISTING PCC PAVEMENT, 8" (±)
- ③ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ④ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- ⑤ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50 3/4"
- ⑥ EXISTING COMB. CURB AND GUTTER

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
<u>MAINLINE RESURFACING</u>		
HOT-MIX ASPHALT, SURFACE COURSE, MIX "D", N50, (IL-9.5 mm), 1 1/2"	4.0% @ 50 GYR.	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	OCP
<u>DRIVEWAYS</u>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL-9.5 mm), 2"	4% @ 50 GYR.	OCP
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm); PE - 6" & CE - 8"	4% @ 50 GYR.	OC/OA
<u>PATCHING</u>		
CLASS D PATCH (HMA BINDER IL-19 mm)	4% @ 70 GYR.	OC/OA
HMA REPLACEMENT OVER PATCHES (HMA Binder IL-19 mm)	4% @ 70 GYR.	OC/OA
OMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (OC/OA); Quality Control for Performance (OCP)		

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SO YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

THE CONTRACTOR SHALL PATCH THE ROADWAY FIRST, THEN MILL AS PER BD-22 DETAIL.

USE HMA SURFACE REMOVAL VARIABLE DEPTH QUANTITY WHERE EVER AVONDALE AVE. GUTTER SEGMENTS ARE BURIED/OVERLAID WITH ASPHALT.

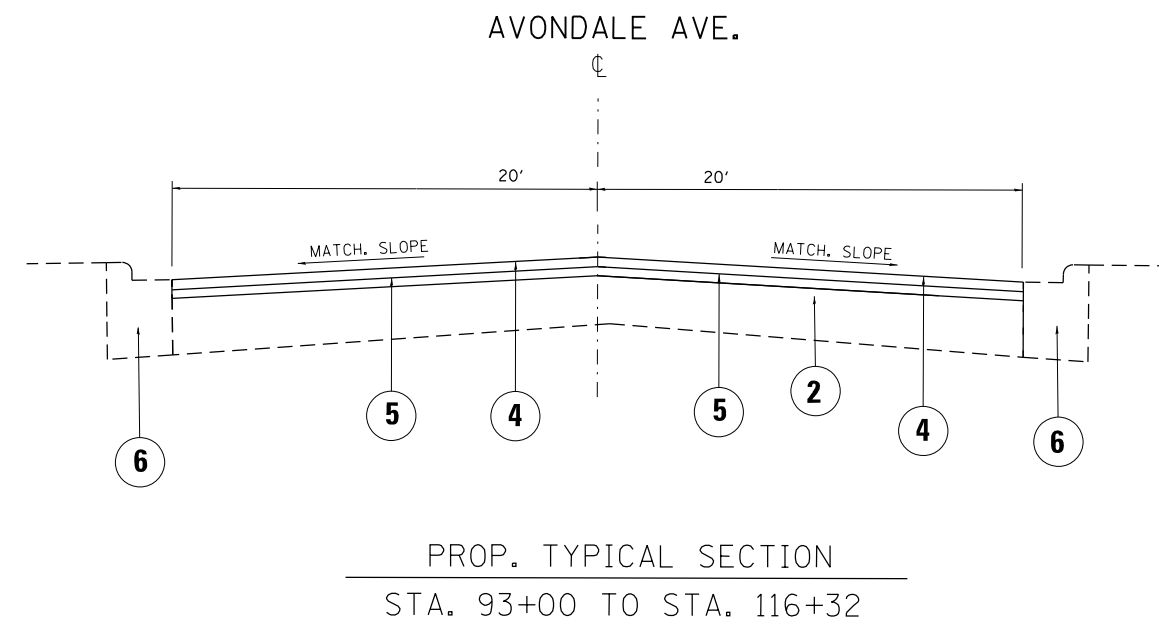
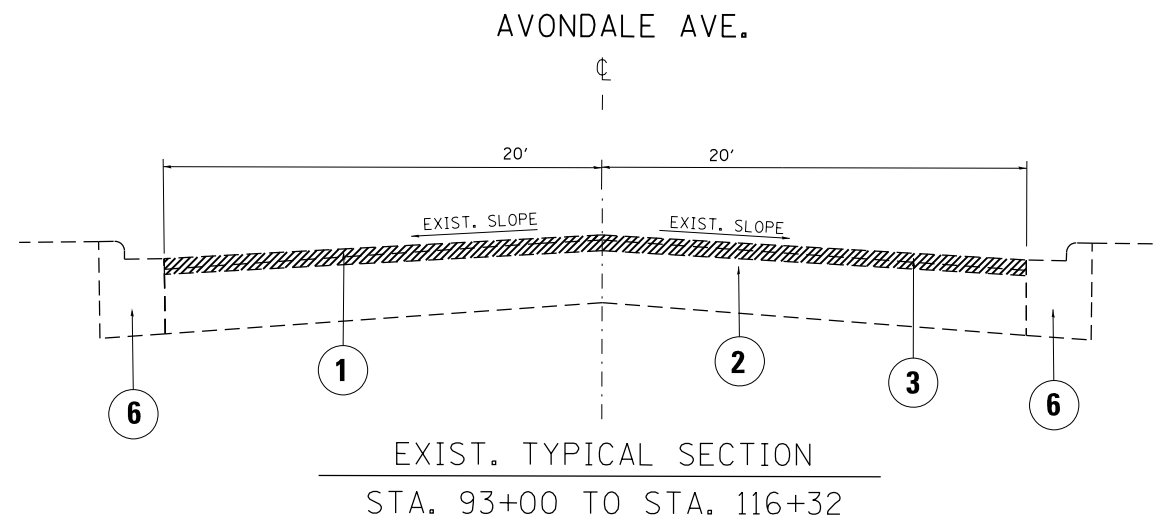
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.) TYPICAL SECTIONS		OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		130	2017-037RS	COOK	39	6
CONTRACT NO. 62F66						
SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.

LEGEND

- ① EXISTING HMA PAVEMENT, VARIES 4" (±)
- ② EXISTING PCC PAVEMENT, 10" (±)
- ③ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ④ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- ⑤ PROPOSED POLYMERIZED LEVLEING BINDER (MACHINE METHOD) IL-4.75, N50 3/4"
- ⑥ EXISTING COMB. CURB AND GUTTER



NOTES:

THE CONTRACTOR SHALL PATCH THE ROADWAY FIRST, THEN MILL AS PER BD-22 DETAIL.

USE HMA SURFACE REMOVAL VARIABLE DEPTH QUANTITY WHERE EVER AVONDALE AVE. GUTTER SEGMENTS ARE BURIED/OVERLAID WITH ASPHALT.

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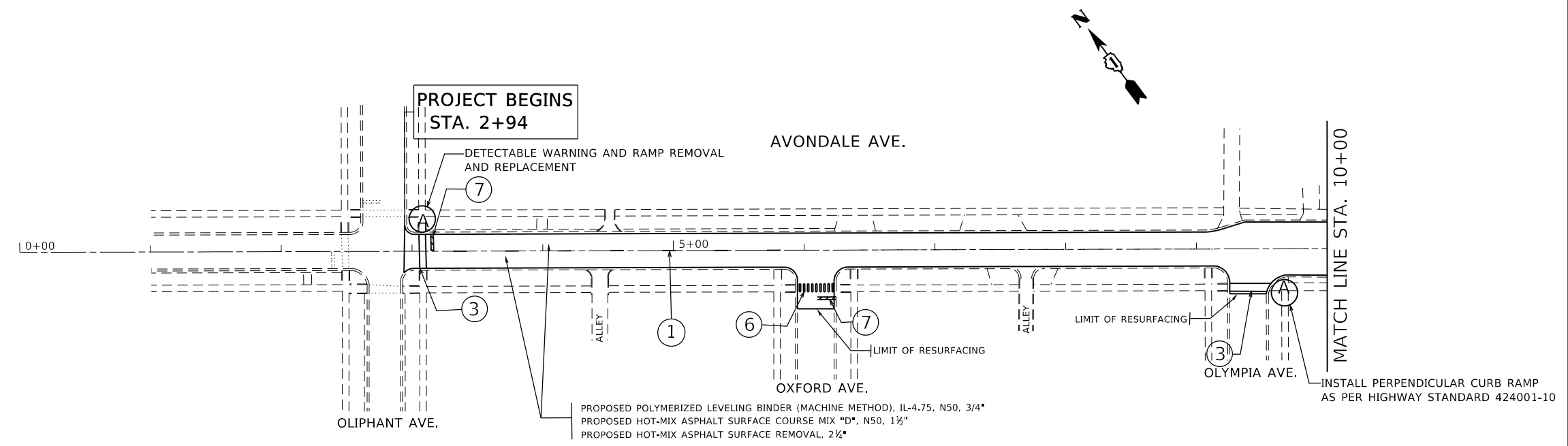
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.)
TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	7
CONTRACT NO. 62F66				
		ILLINOIS	FED. AID PROJECT	

- NOTES:
- ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS" DETAIL STANDARDS TC-24
 - THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.
 - SEE PEDESTRIAN RAMP (ADA) SHEETS FOR SIDEWALK DETAILS.
 - CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT OF PROPOSED TREE REMOVAL ITEMS.



(A) PROPOSED PEDESTRIAN RAMP (ADA) IMPROVEMENT

PROPOSED PAVEMENT MARKINGS LEGEND

- | | | |
|--|--|--|
| (1) THERMOPLASTIC PVMT. MARKING 4", 6' DASH - 18' SPACE, YELLOW (TYP.) | (5) THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.) - SEE DETAIL TC-24 FOR SPACING | (9) THERMOPLASTIC PVMT. MARKING 6", WHITE - DOTTED LINE (TYP.) |
| (2) THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.) | (6) THERMOPLASTIC PVMT. MARKING 24", CROSSWALK-SCHOOL, WHITE (TYP.) - SEE DETAIL TC-24 FOR SPACING | (10) THERMOPLASTIC PVMT. MARKING 12", LINE, WHITE (TYP.) |
| (3) THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.) | (7) THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.) | |
| (4) THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.) | (8) THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS | |

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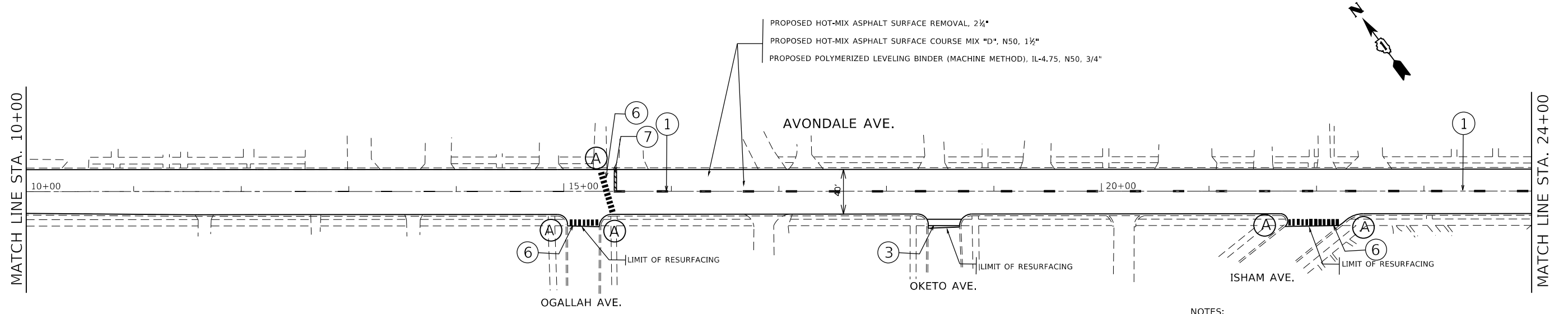
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.)

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	8
ILLINOIS FED. AID PROJECT				
CONTRACT NO. 62F66				

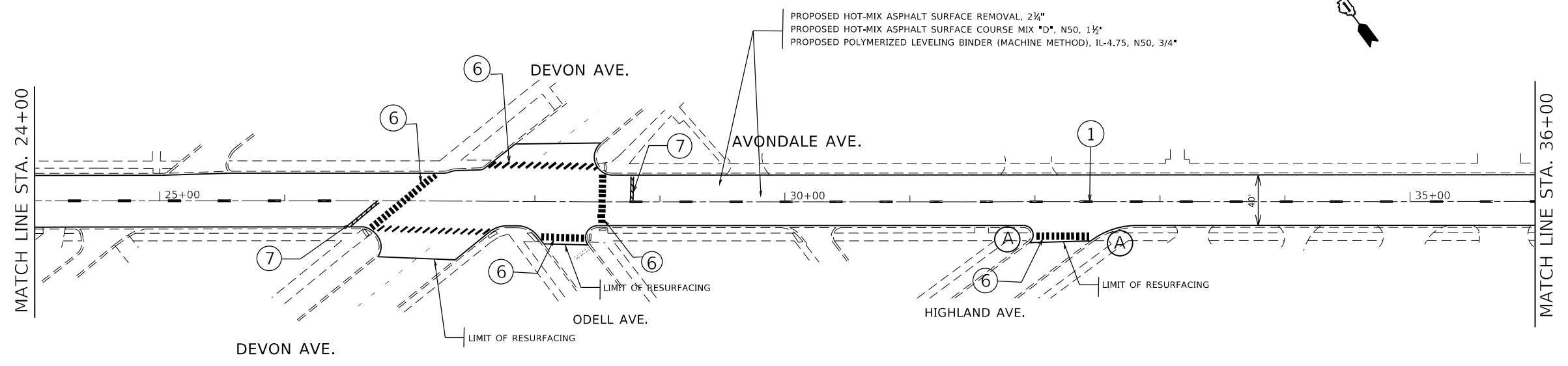


(A) PROPOSED PEDESTRIAN RAMP (ADA) IMPROVEMENT

PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", 6" DASH - 18" SPACE, YELLOW (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ④ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.)
- ⑤ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.) - SEE DETAIL TC-24 FOR SPACING
- ⑥ THERMOPLASTIC PVMT. MARKING 24", CROSSWALK-SCHOOL, WHITE (TYP.) - SEE DETAIL TC-24 FOR SPACING
- ⑦ THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑧ THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS
- ⑨ THERMOPLASTIC PVMT. MARKING 6", WHITE - DOTTED LINE (TYP.)
- ⑩ THERMOPLASTIC PVMT. MARKING 12", LINE, WHITE (TYP.)

- NOTES:
- 1) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS" DETAIL STANDARDS TC-24
 - 2) THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.
 - 3) SEE PEDESTRIAN RAMP (ADA) SHEETS FOR SIDEWALK DETAILS.
 - 4) CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT OF PROPOSED TREE REMOVAL ITEMS.



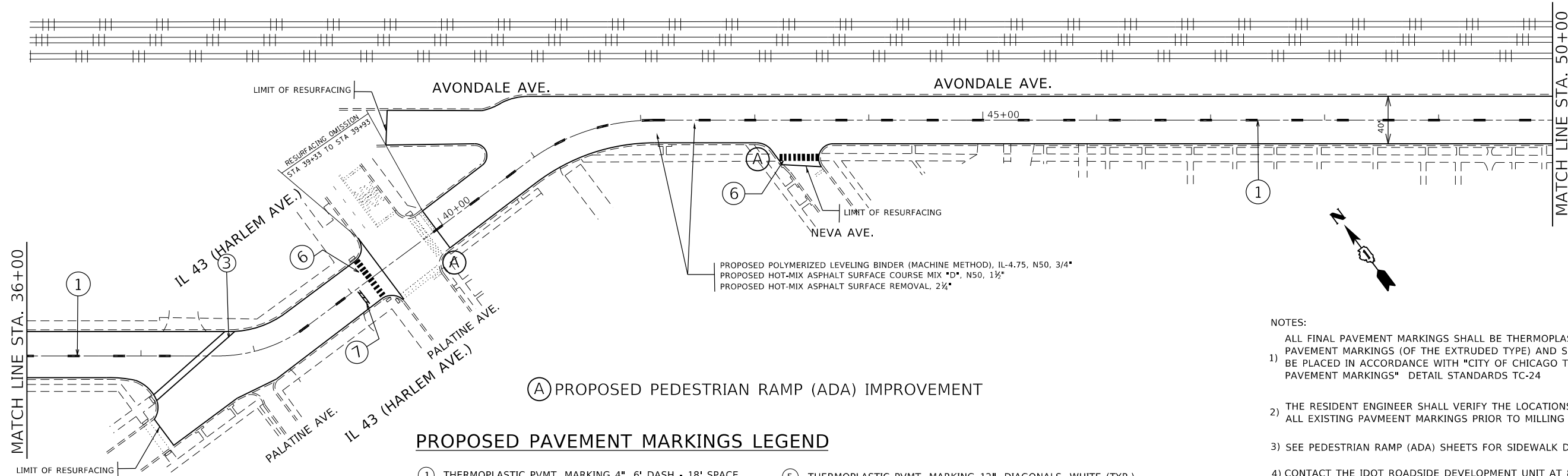
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLAN AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.)			
SCALE: 1" = 50'	SHEET	OF	SHEETS
	STA.	TO	STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037R5	COOK	39	9
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				

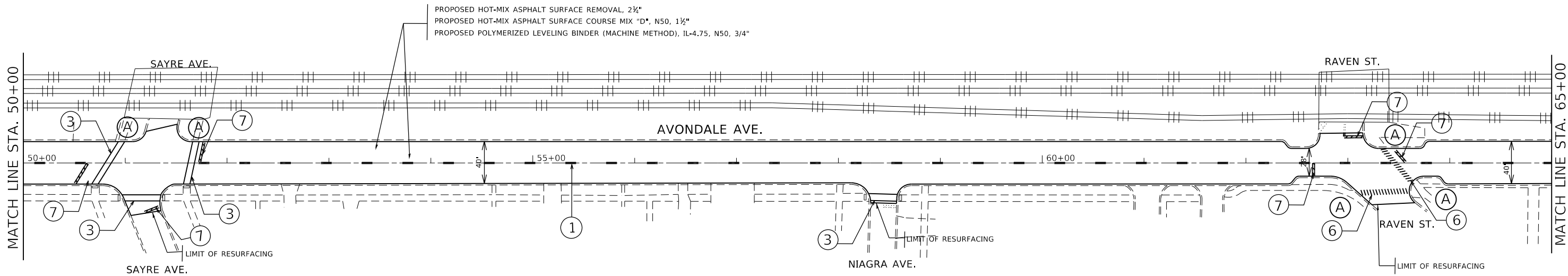


(A) PROPOSED PEDESTRIAN RAMP (ADA) IMPROVEMENT

PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", 6' DASH - 18' SPACE, YELLOW (TYP.)
- ② THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ④ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.)
- ⑤ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- SEE DETAIL TC-24 FOR SPACING
- ⑥ THERMOPLASTIC PVMT. MARKING 24", CROSSWALK-SCHOOL, WHITE (TYP.)
- SEE DETAIL TC-24 FOR SPACING
- ⑦ THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑧ THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS
- ⑨ THERMOPLASTIC PVMT. MARKING 6", WHITE - DOTTED LINE (TYP.)
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- NOTES:**
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 - 3) SEE PEDESTRIAN RAMP (ADA) SHEETS FOR SIDEWALK DETAILS.
 - 4) CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT OF PROPOSED TREE REMOVAL ITEMS.



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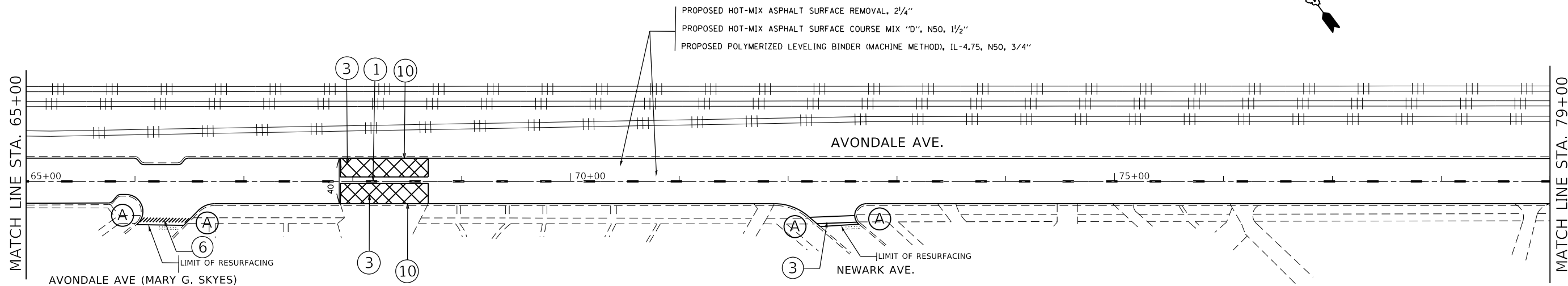
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	10
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				

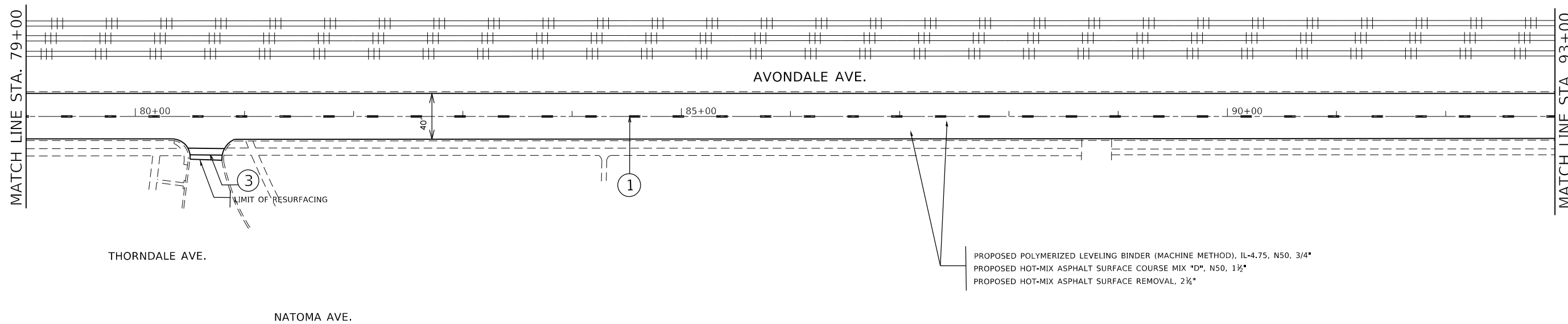


(A) PROPOSED PEDESTRIAN RAMP (ADA) IMPROVEMENT

PROPOSED PAVEMENT MARKINGS LEGEND

- | | | |
|--|--|--|
| ① THERMOPLASTIC PVMT. MARKING 4", 6' DASH - 18' SPACE, YELLOW (TYP.) | ⑤ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.) - SEE DETAIL TC-24 FOR SPACING | ⑨ THERMOPLASTIC PVMT. MARKING 6", WHITE - DOTTED LINE (TYP.) |
| ② THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.) | ⑥ THERMOPLASTIC PVMT. MARKING 24", CROSSWALK-SCHOOL, WHITE (TYP.) - SEE DETAIL TC-24 FOR SPACING | ⑩ THERMOPLASTIC PVMT. MARKING 12", LINE, WHITE (TYP.) |
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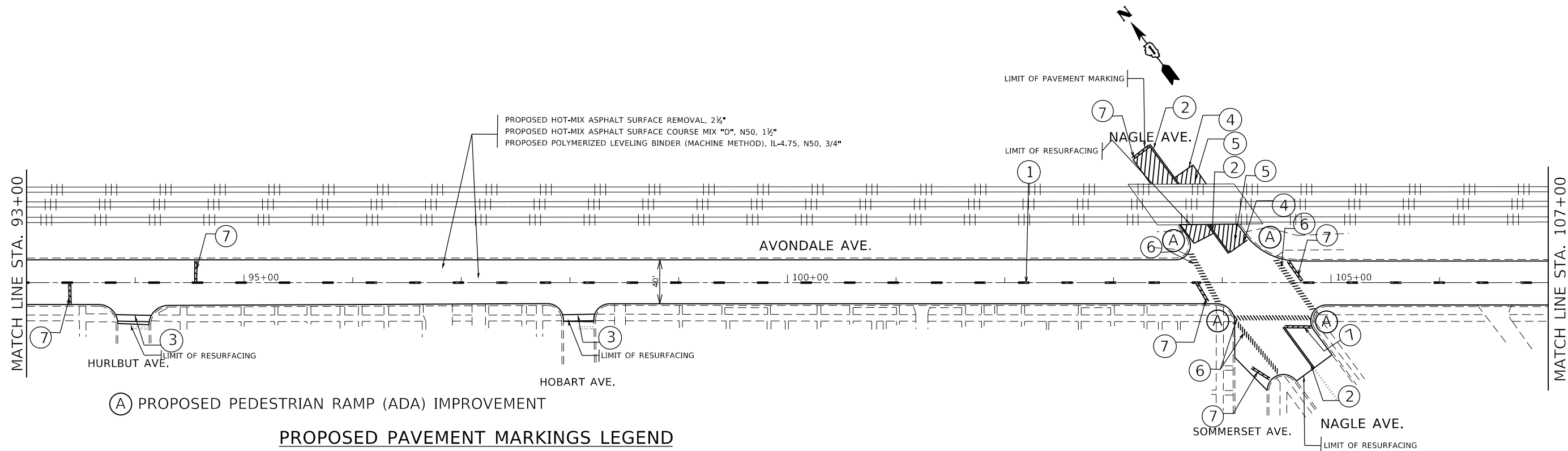
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.)**

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	11
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				

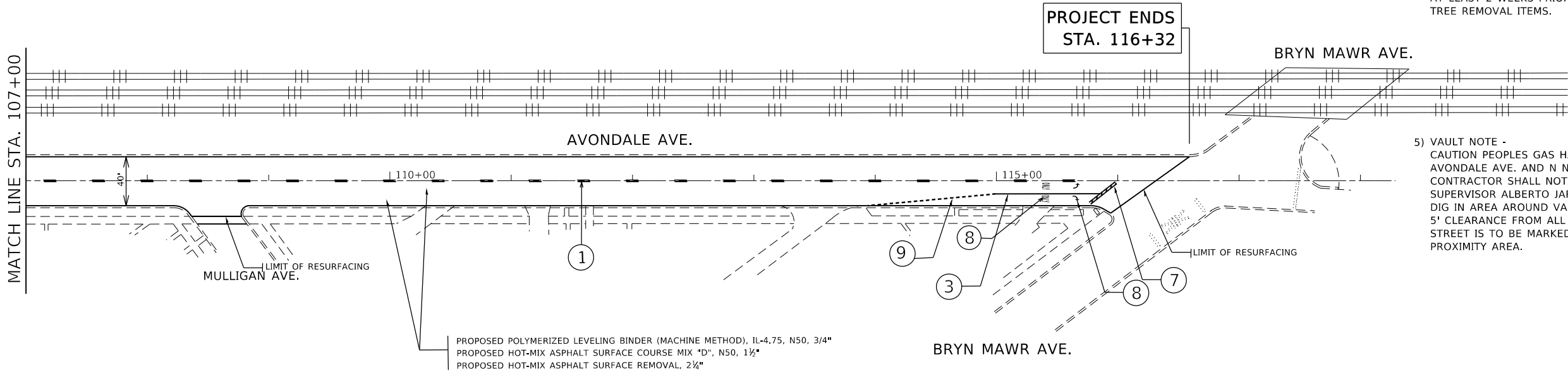


(A) PROPOSED PEDESTRIAN RAMP (ADA) IMPROVEMENT

PROPOSED PAVEMENT MARKINGS LEGEND

- ① THERMOPLASTIC PVMT. MARKING 4", 6' DASH - 18' SPACE, YELLOW (TYP.)
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 - 3) SEE PEDESTRIAN RAMP (ADA) SHEETS FOR SIDEWALK DETAILS.
 - 4) CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT OF PROPOSED TREE REMOVAL ITEMS.
 - 5) VAULT NOTE - CAUTION PEOPLES GAS HAS CRITICAL FACILITIES AT THE INTERSECTION OF N AVONDALE AVE. AND N NAGLE AVE. PRIOR TO ANY EXCAVATION NEAR VAULT CONTRACTOR SHALL NOTIFY PIPELINE & SYSTEM OPERATIONS ENGINEERING SUPERVISOR ALBERTO JARA, AT 773-658-2954. CONTRACTOR TO HAND DIG IN AREA AROUND VAULT DUE TO CONTROL LINES AND SHALL MAINTAIN 5' CLEARANCE FROM ALL VAULT FACILITIES. LOCATION OF THE VAULT IN THE STREET IS TO BE MARKED PRIOR TO ANY EXCAVATION COMMENCING WITHIN PROXIMITY AREA.



**PROJECT ENDS
STA. 116+32**

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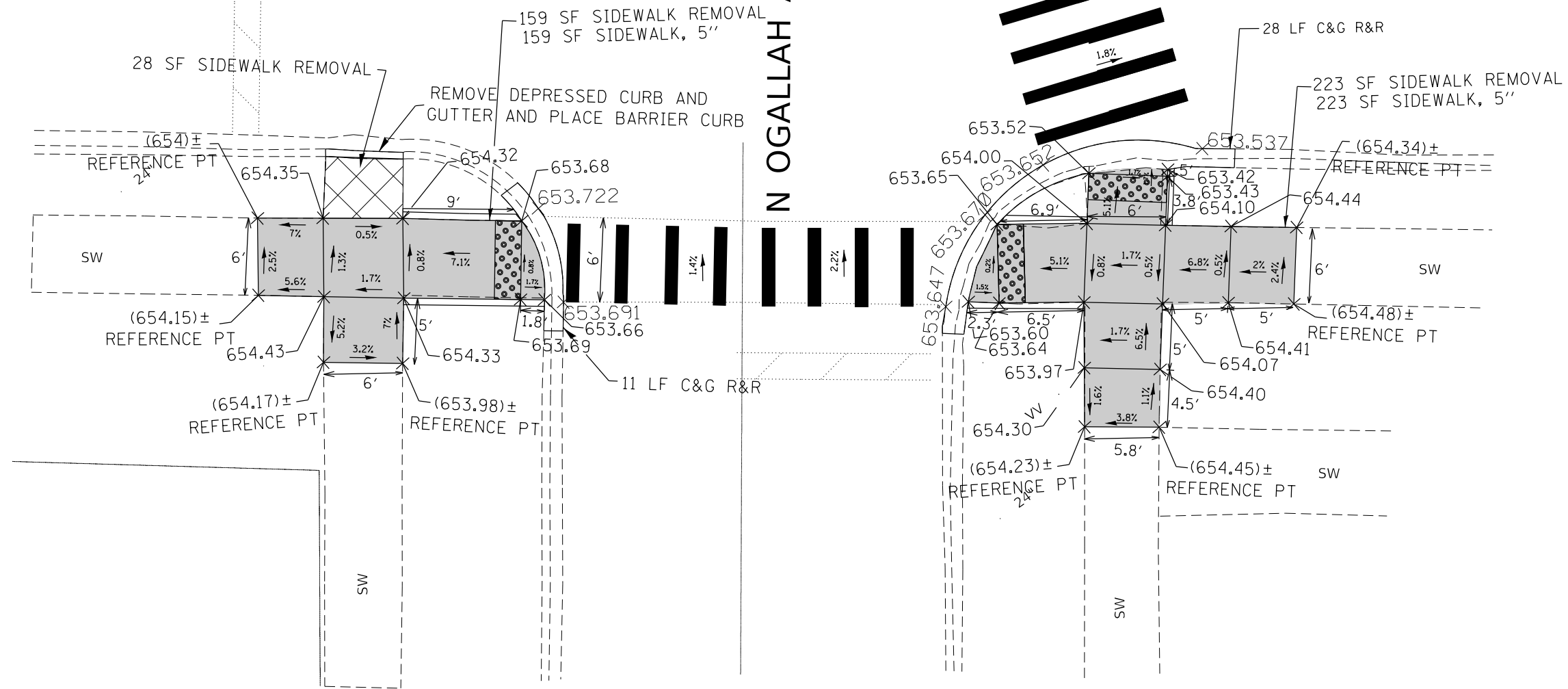
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLAN AVONDALE AVE. (OLIPHANT AVE. TO BRYN MAWR AVE.)			
SCALE: 1"= 50'	SHEET	OF SHEETS	STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	12
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				

N AVONDALE AVENUE

N OGALLAH AVENUE



LEGEND

xx.xx'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 655.699

BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH EAST CORNER

LOCATION : SOUTH EAST CORNER OF AVONDALE AVE. AND OGALLAH AVE.

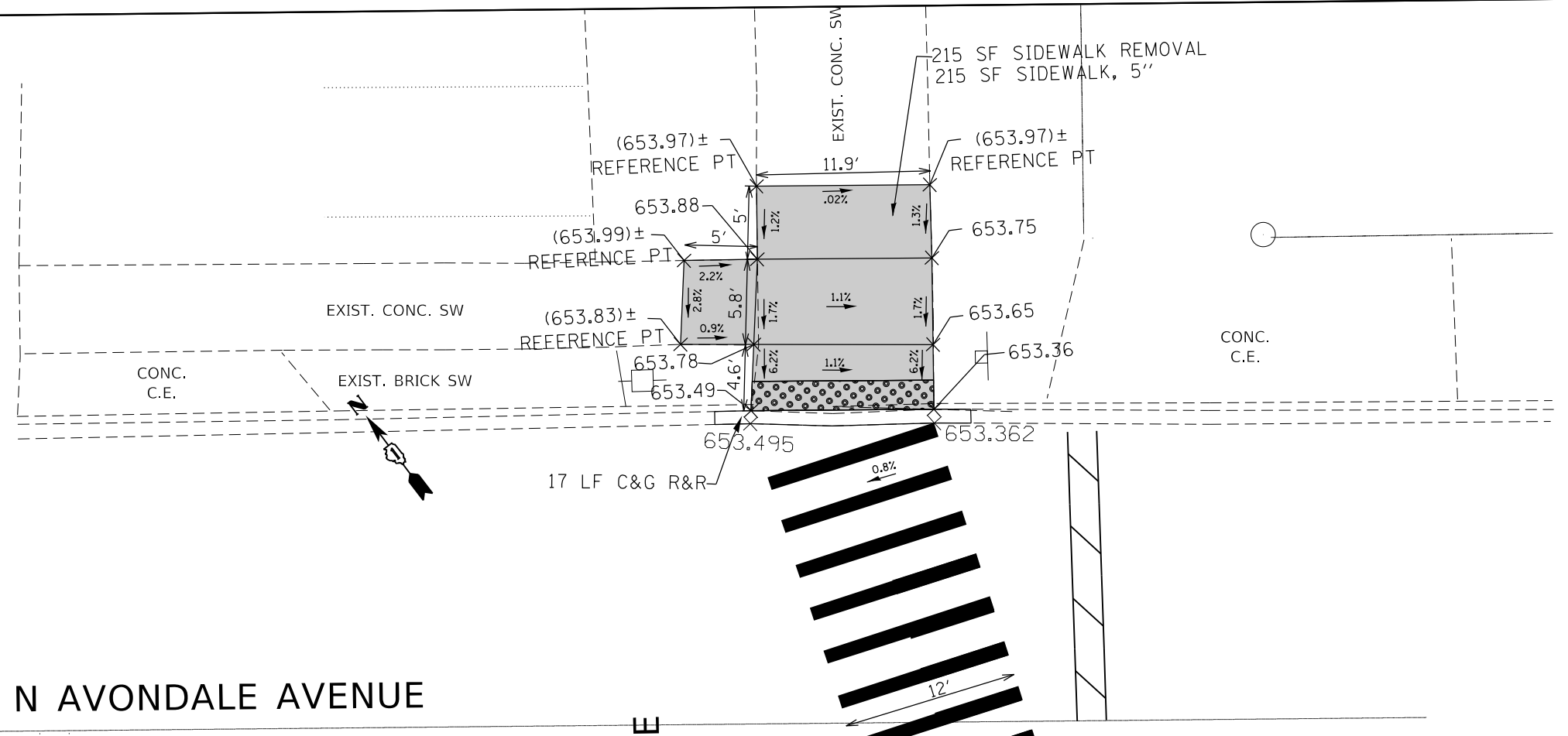
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

AVONDALE AVE. AT OGALLAH AVE. SWC AND SEC
ADA SIDEWALK DETAIL SHEETS

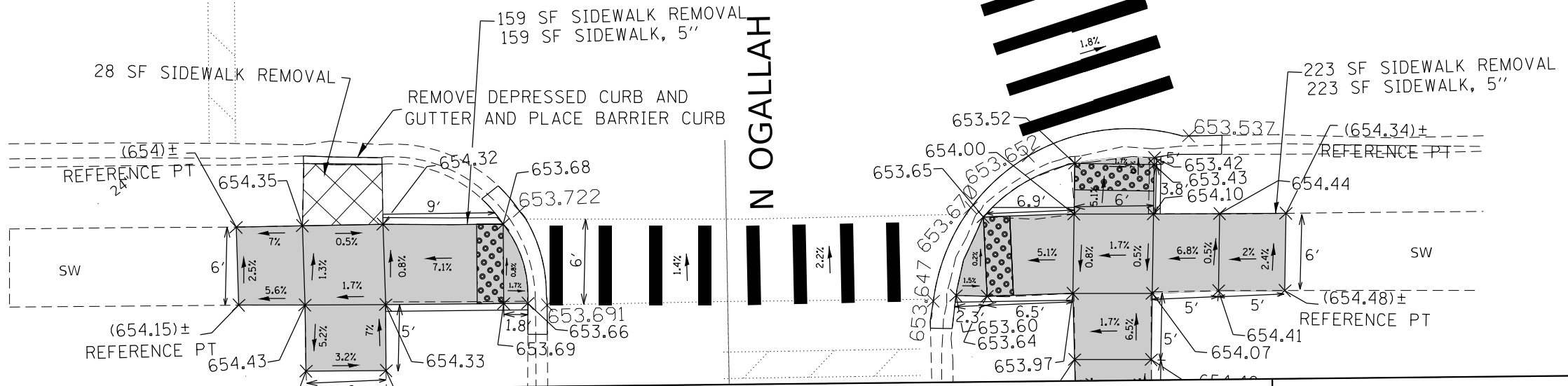
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OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	13
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				



N AVONDALE AVENUE

N OGALLAH AVENUE



LEGEND

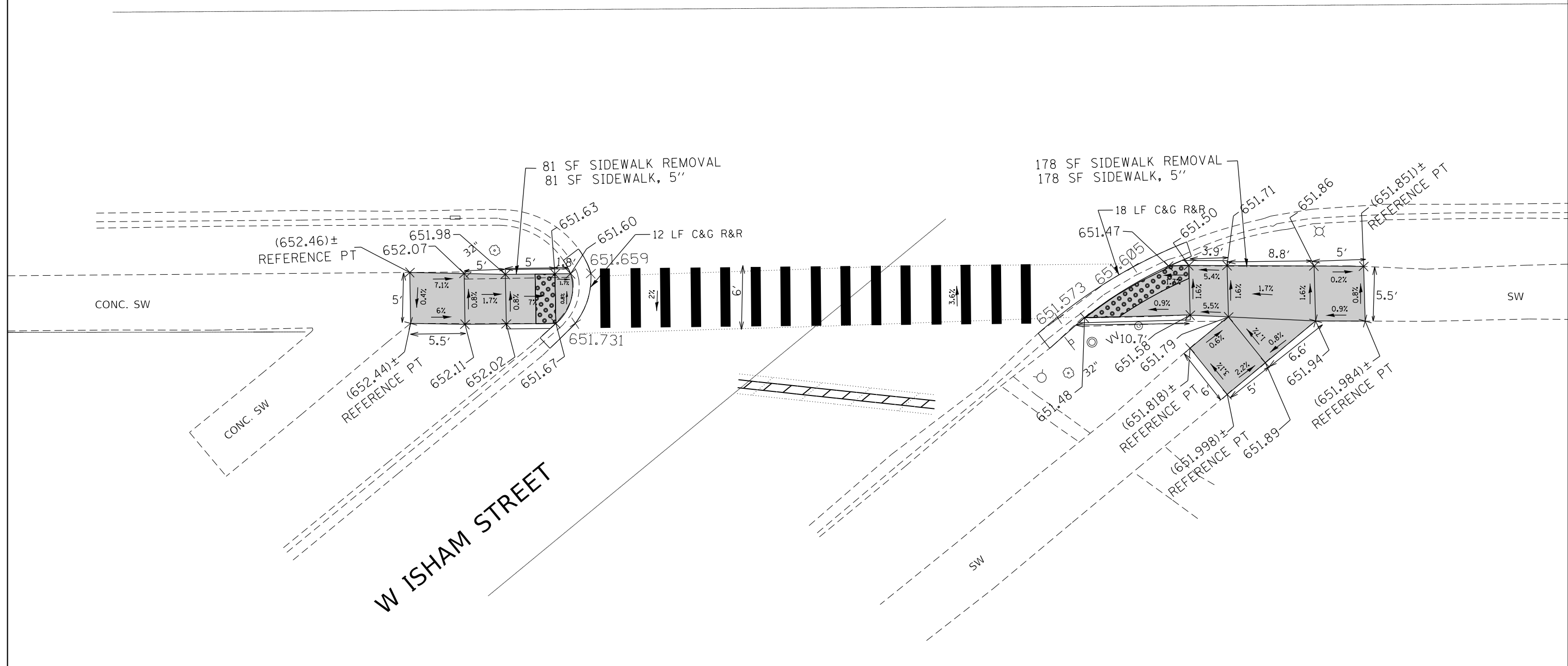
- xx.xx' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 655.699
 BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH EAST CORNER
 LOCATION : SOUTH EAST CORNER OF AVONDALE AVE. AND OGALLAH AVE.

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N AVONDALE AVENUE



LEGEND



EXISTING LENGTH



EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 653.839

BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH EAST CORNER

LOCATION : SOUTH EAST CORNER OF AVONDALE AVE. AND ISHAM AVE.

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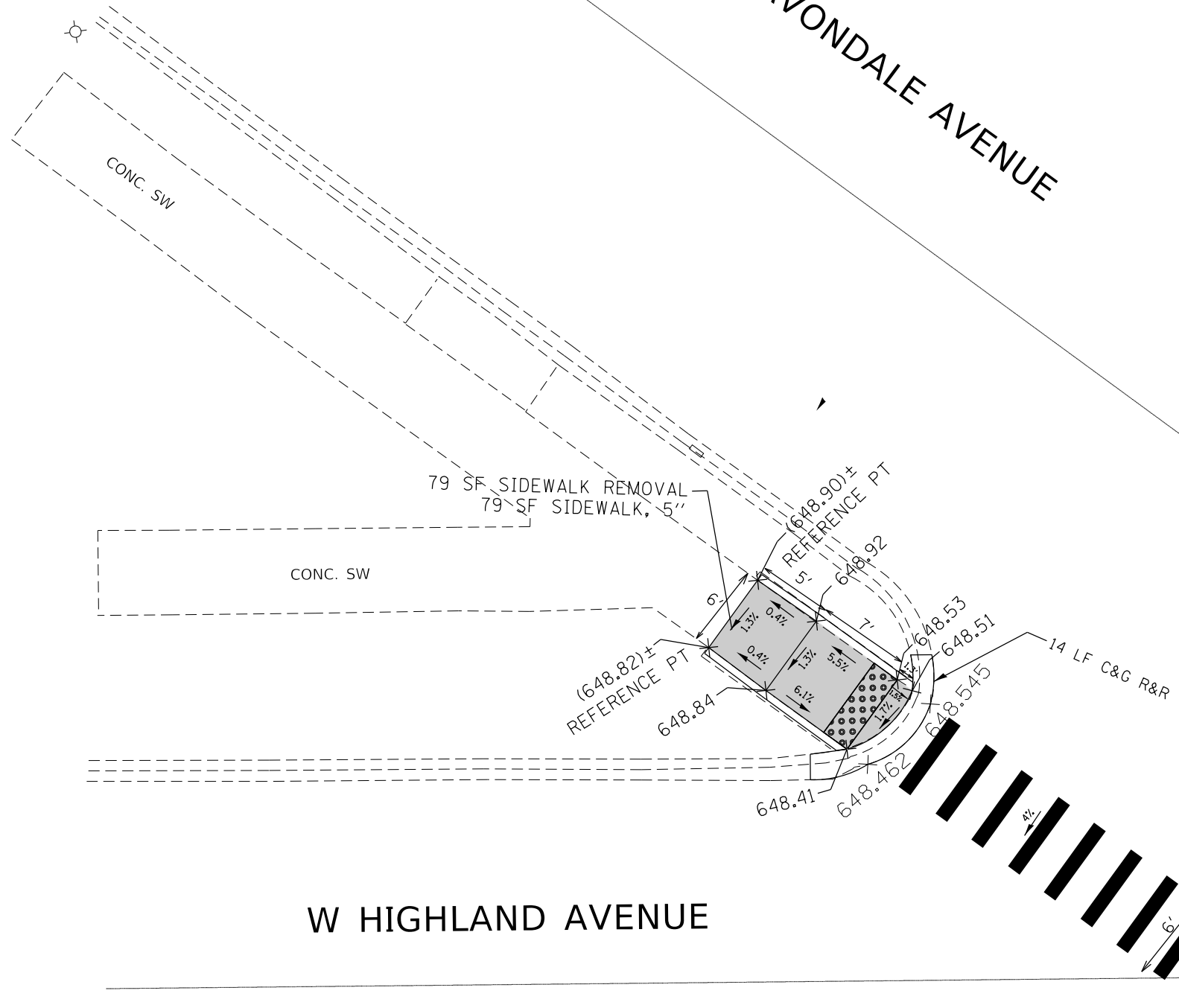
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

AVONDALE AVE. AT ISHAM ST.
ADA SIDEWALK DETAIL SHEETS

SCALE: SHEET OF SHEETS STA. TO STA.

OR R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	15
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62F66	

N AVONDALE AVENUE



W HIGHLAND AVENUE

LEGEND

- xx.xx EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 650.203
 BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH EAST CORNER
 LOCATION : SOUTH EAST CORNER OF AVONDALE AVE. AND HIGHLAND AVE.

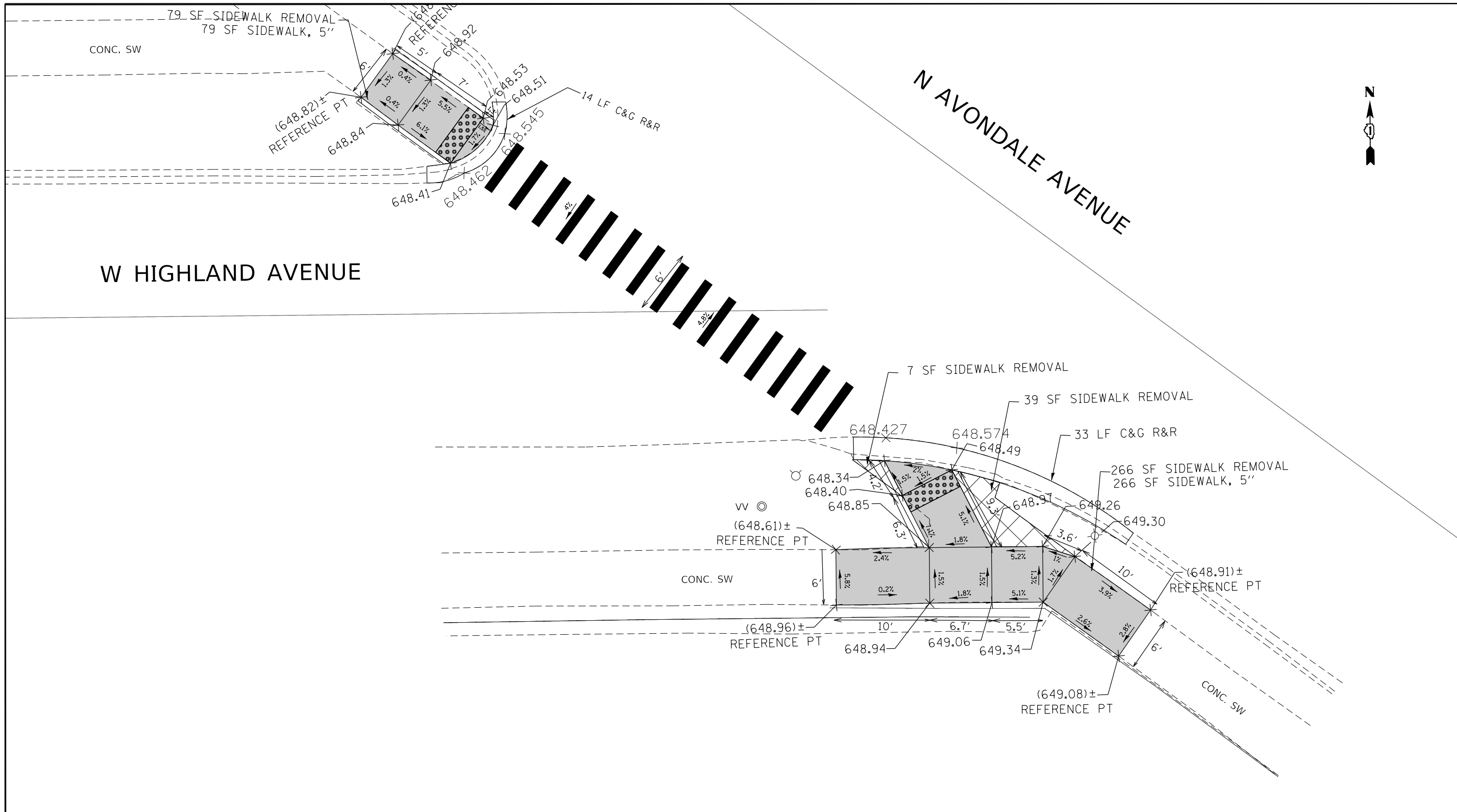
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT HIGHLAND AVE SWC.
 ADA SIDEWALK DETAIL SHEETS**

SCALE: SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	16
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				



LEGEND

- xx.xx' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 650.203
 BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH EAST CORNER
 LOCATION : SOUTH EAST CORNER OF AVONDALE AVE. AND HIGHLAND AVE.

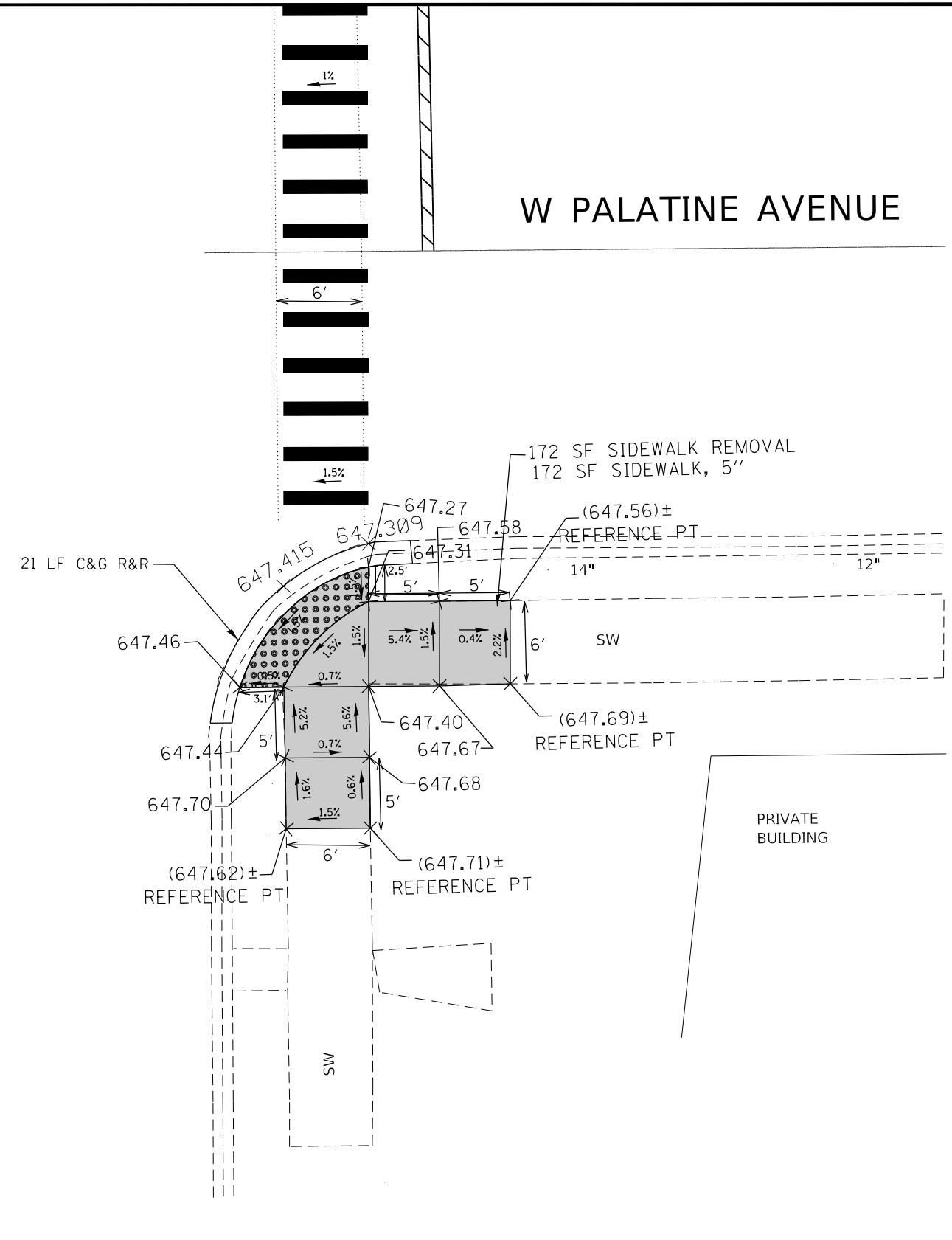
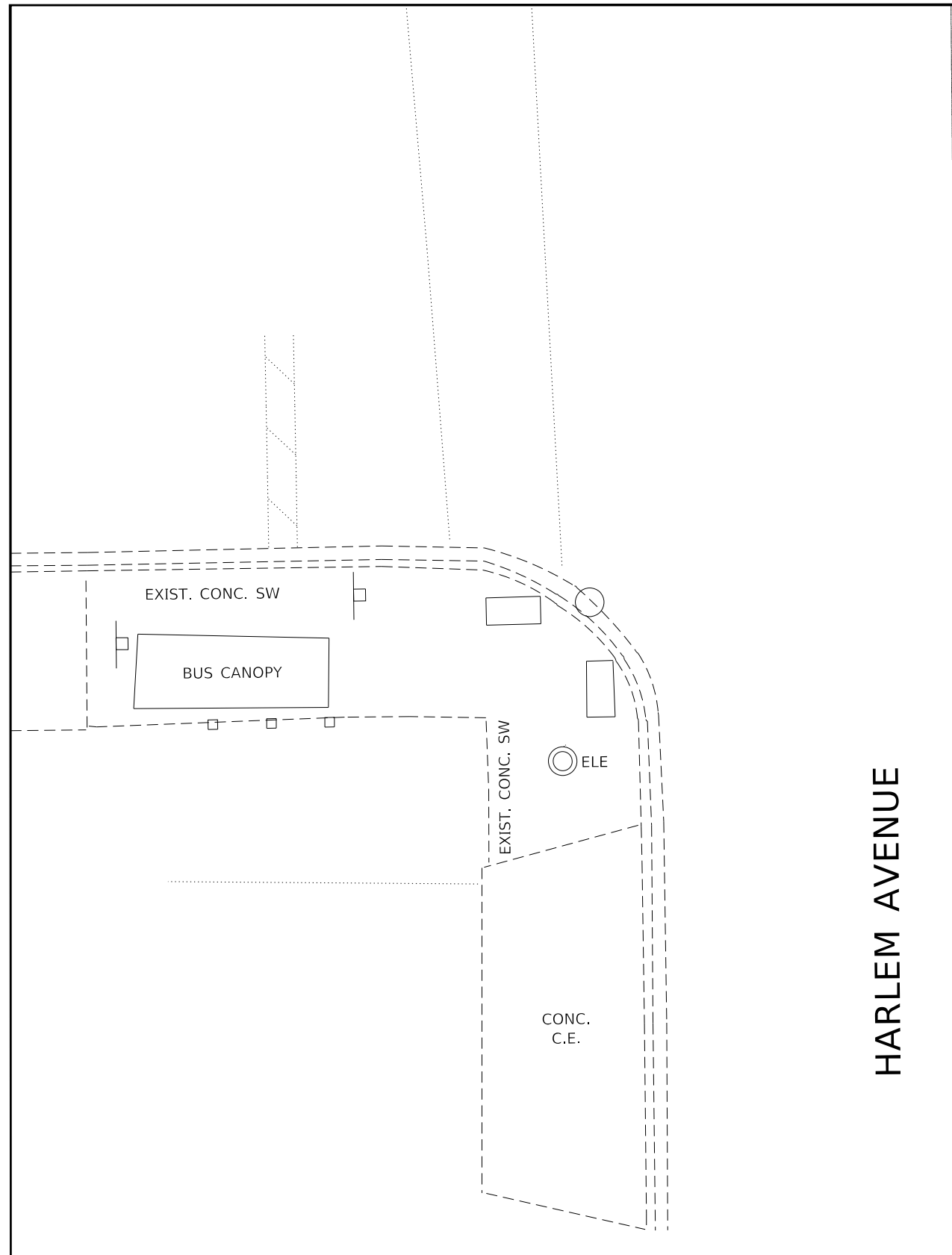
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Default	PLOT SCALE = 10.0007' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/13/2018	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT HIGHLAND AVE SEC
 ADA SIDEWALK DETAIL SHEETS**

SCALE: SHEET OF SHEETS STA. TO STA.

OR R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	17
CONTRACT NO.			62F66	
<small>ILLINOIS FED. AID PROJECT</small>				



LEGEND

- xx.xx' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE

- PROPOSED SIDEWALK
- PROPOSED SIDE CURB

- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 649.427

BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH EAST CORNER

LOCATION : SOUTH EAST CORNER OF AVONDALE AVE. AND IL. 43' (HARLEM AVE.)

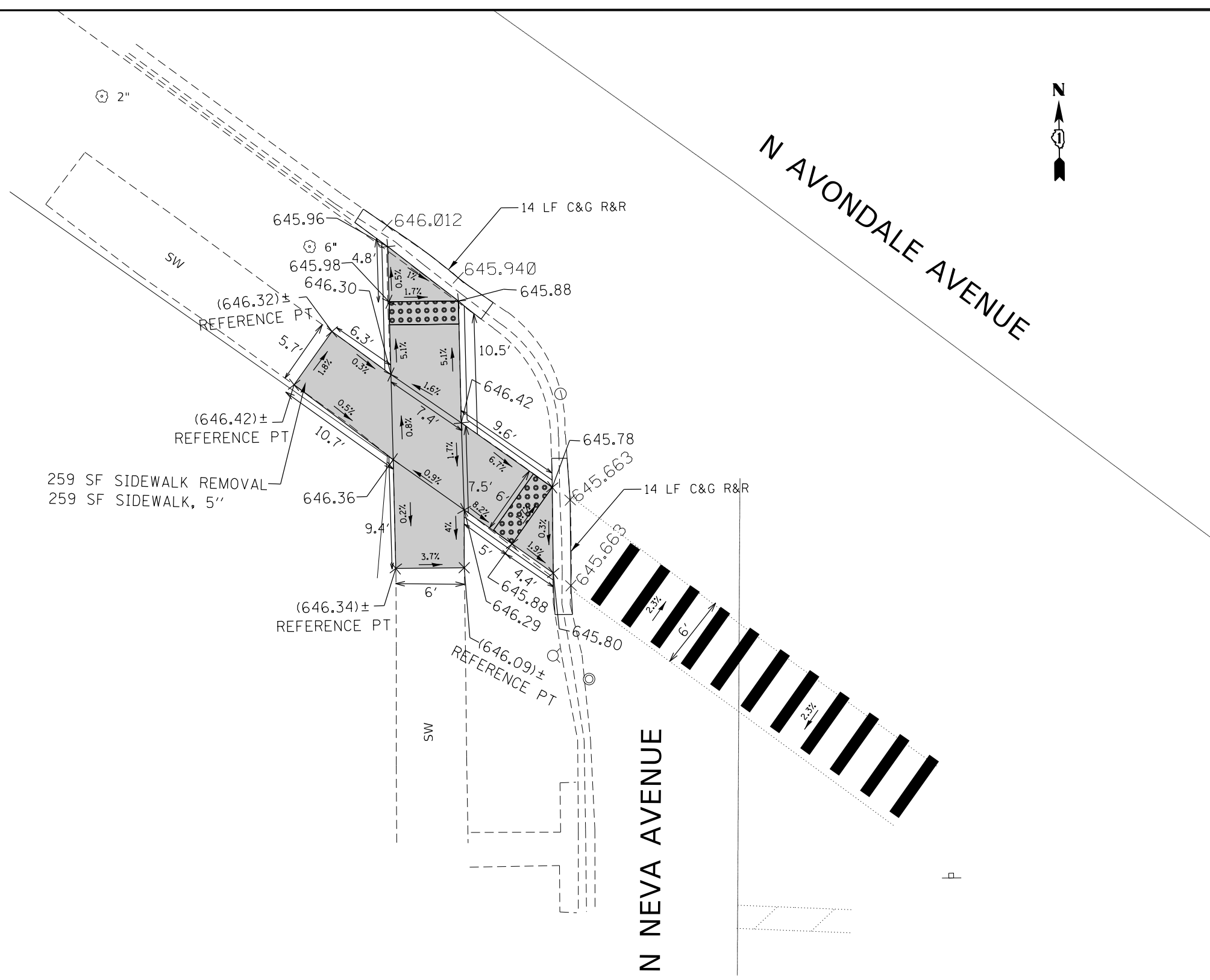
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Default	PLOT SCALE = 10.0005' / in.	DATE -	REVISED -
	PLOT DATE = 12/13/2018		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT W. PALATINE AVE./
AVONDALE AVE. SEC ADA SIDEWALK DETAIL SHEETS**

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037R5	COOK	39	18
CONTRACT NO.			62F66	
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



LEGEND

xx.xx'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 647.438

BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH WEST CORNER

LOCATION : SOUTH WEST CORNER OF AVONDALE AVE. AND NEVA AVE.

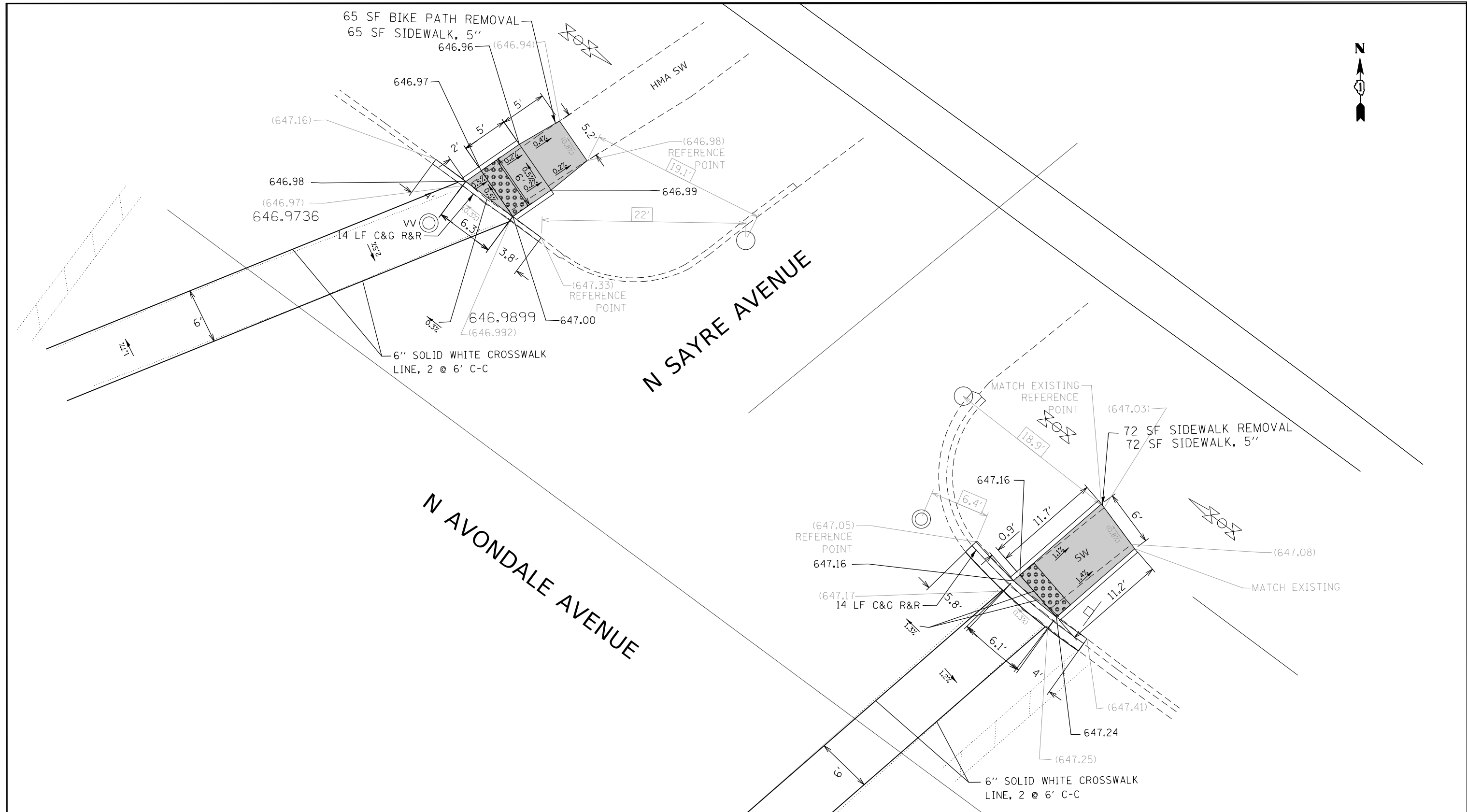
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Default	PLOT DATE = 12/13/2018	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT NEVA AVE. SWC
ADA SIDEWALK DETAIL SHEETS**

SCALE: SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	19
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				



LEGEND

xx.xx' EXISTING LENGTH
() EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK
PROPOSED SIDE CURB

CLASS D PATCH
DETECTABLE WARNINGS
SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 649.094
BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH WEST CORNER
LOCATION : SOUTH WEST CORNER OF AVONDALE AVE. AND NICKERSON AVE./SAYRE AVE.

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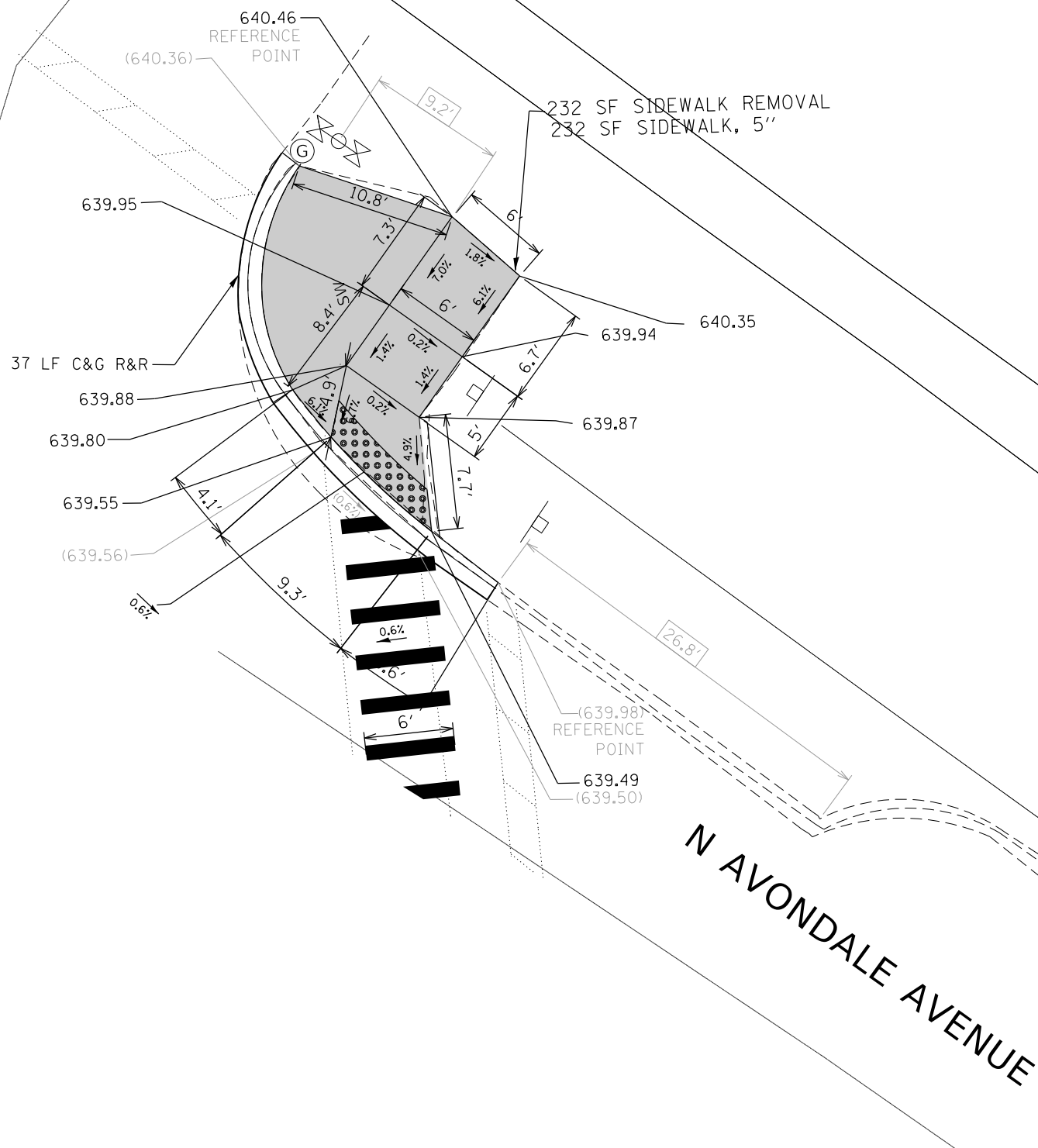
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT N. NICKERSON AVE./N. SAYRE AVE.
ADA SIDEWALK DETAIL SHEETS**

SCALE: SHEET OF SHEETS STA. TO STA.

OR R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	20
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				

RAVEN ST



LEGEND

xx.xx'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 641.271

BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH SIDE OF STREET

LOCATION : SOUTH SIDE OF AVONDALE AVE. AND RAVEN ST.

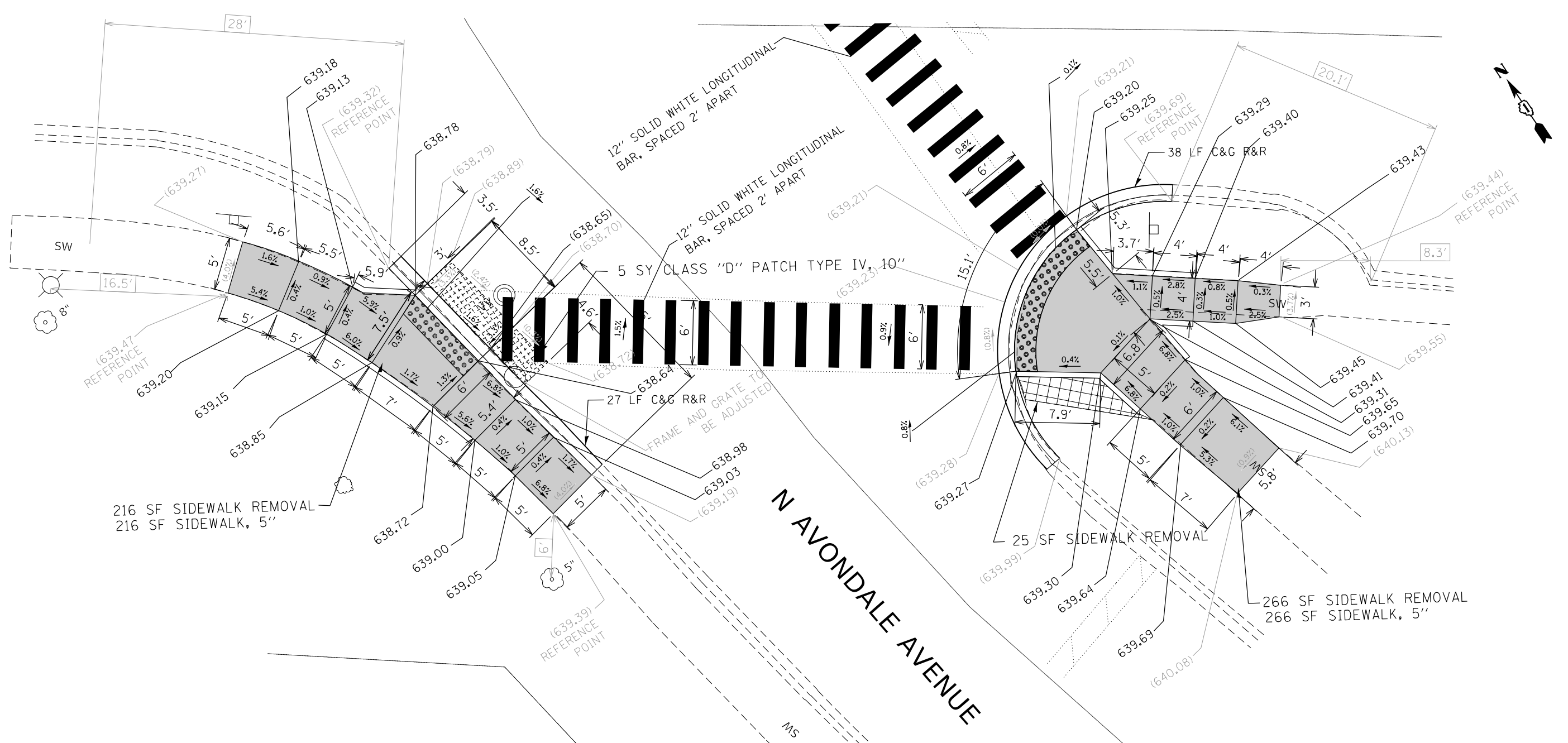
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	PLOT DATE = 12/13/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT RAVEN ST. NORTH
ADA SIDEWALK DETAIL SHEETS**

SCALE: SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	21
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				



LEGEND

- xx.xx' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 641.271
 BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH SIDE OF STREET
 LOCATION : SOUTH SIDE OF AVONDALE AVE. AND RAVEN ST.

FILE NAME =	USER NAME = valledolidv	DESIGNED -	REVISED -
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	PLOT DATE = 12/13/2018	DATE -	REVISED -

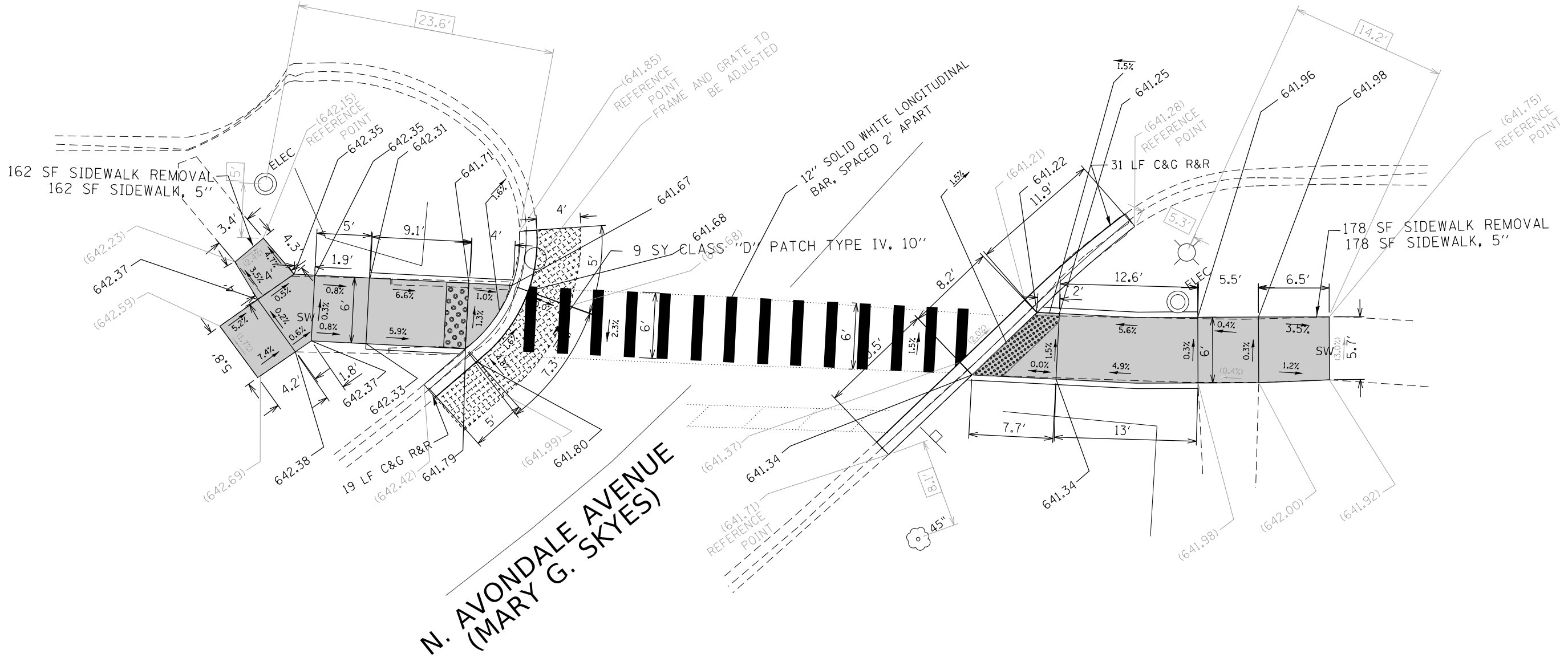
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT RAVEN SOUTH
 ADA SIDEWALK DETAIL SHEETS**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
--------	-------	----	--------	------	----	------

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037R5	COOK	39	22
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				

N AVONDALE AVENUE

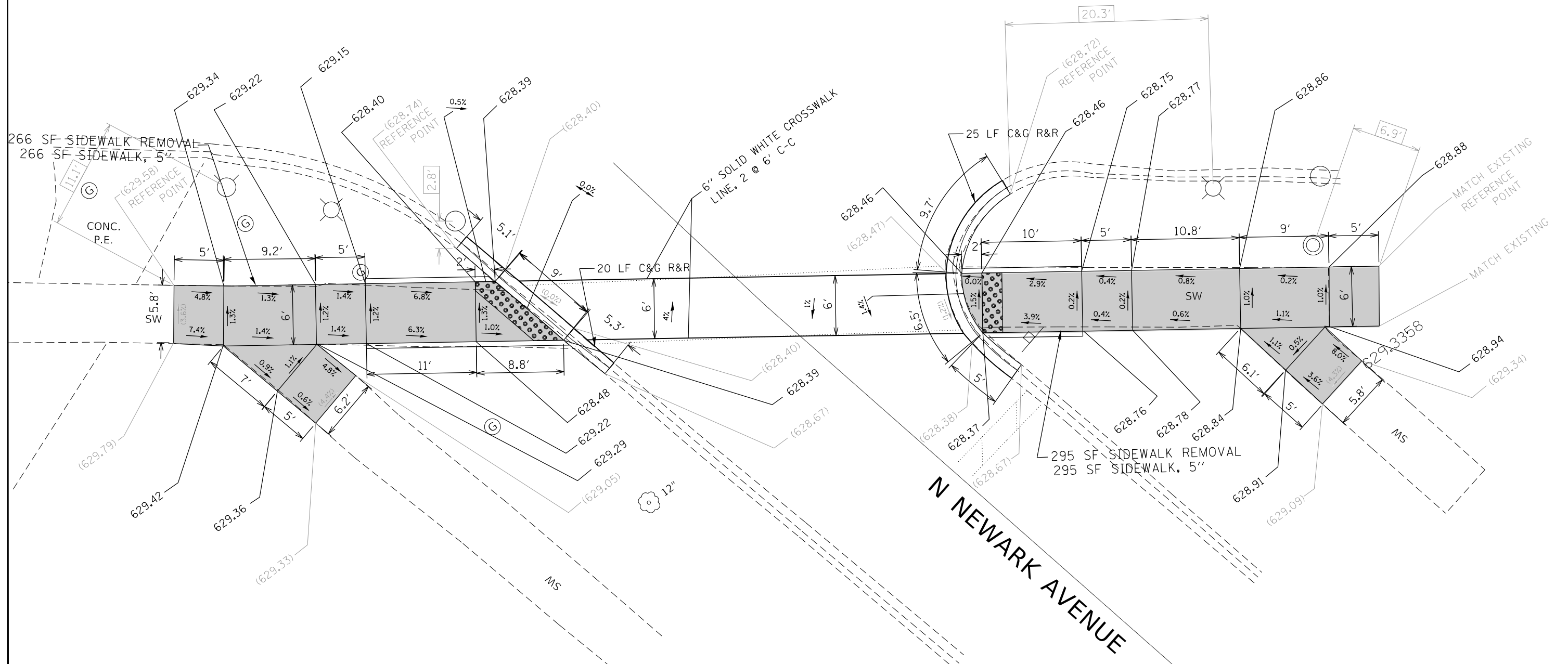
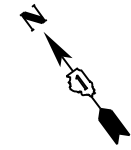


LEGEND

- xx.xx' EXISTING LENGTH
- () EXISTING ELEVATION/SLOPE
- [Solid Gray] PROPOSED SIDEWALK
- [Double Line] PROPOSED SIDE CURB
- [Cross-hatch] CLASS D PATCH
- [Grid of Dots] DETECTABLE WARNINGS
- [Cross-hatch] SIDEWALK REMOVAL REPLACE W/ TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 642.370
 BENCHMARK : TOP OF FIRE HYDRANT AT SOUTH EAST CORNER OF AVONDALE AVE. AND MARY G. SKYES
 LOCATION : SOUTH SIDE OF AVONDALE AVE. AND MARY G. SKYES

N AVONDALE AVENUE



LEGEND

xx.xx'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 630.444

BENCHMARK : SOUTH EAST BOLT AT SOUTH EAST CORNER OF AVONDALE AVE. AND NEWARK AVE.

LOCATION : SOUTH EAST CORNER OF AVONDALE AVE. AND NEWARK AVE.

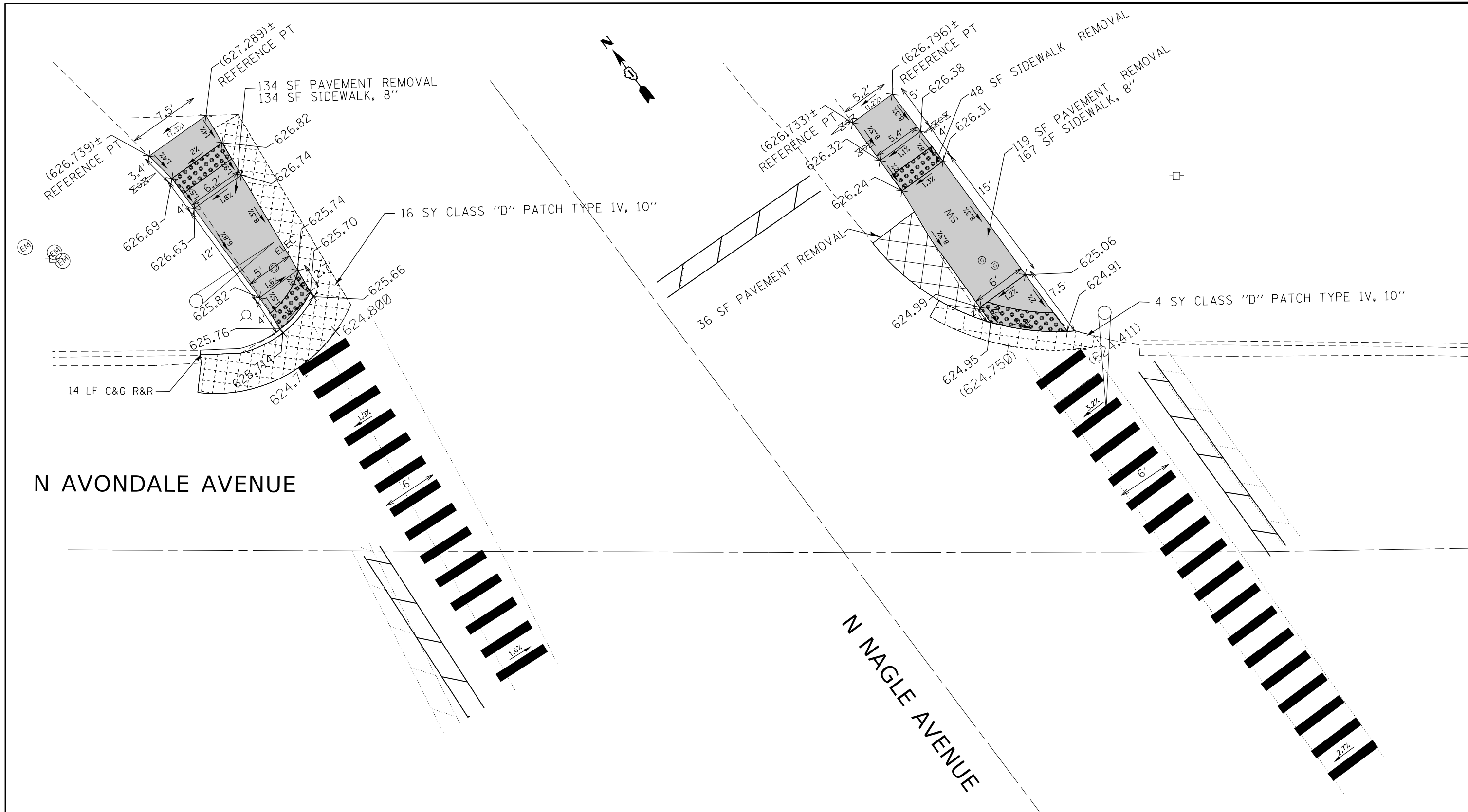
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Default	PLOT SCALE = 10.0005' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/13/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

AVONDALE AVE AT NEWARK AVE.
ADA SIDEWALK DETAIL SHEETS

SCALE: SHEET OF SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	24
CONTRACT NO. 62F66				
ILLINOIS FED. AID PROJECT				



LEGEND

xx.xx'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 625.363

BENCHMARK : TRAFFIC CONTROL BOX AT SOUTH WEST OF AVONDALE AVE. AND NAGLE AVE./SUMMERSET AVE.

LOCATION : SOUTHWEST CORNER OF AVONDALE AVE. AND NAGLE AVE./SUMMERSET AVE.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**AVONDALE AVE. AT NAGLE AVE./ SUMMERSET AVE. NORTH
ADA SIDEWALK DETAIL SHEETS**

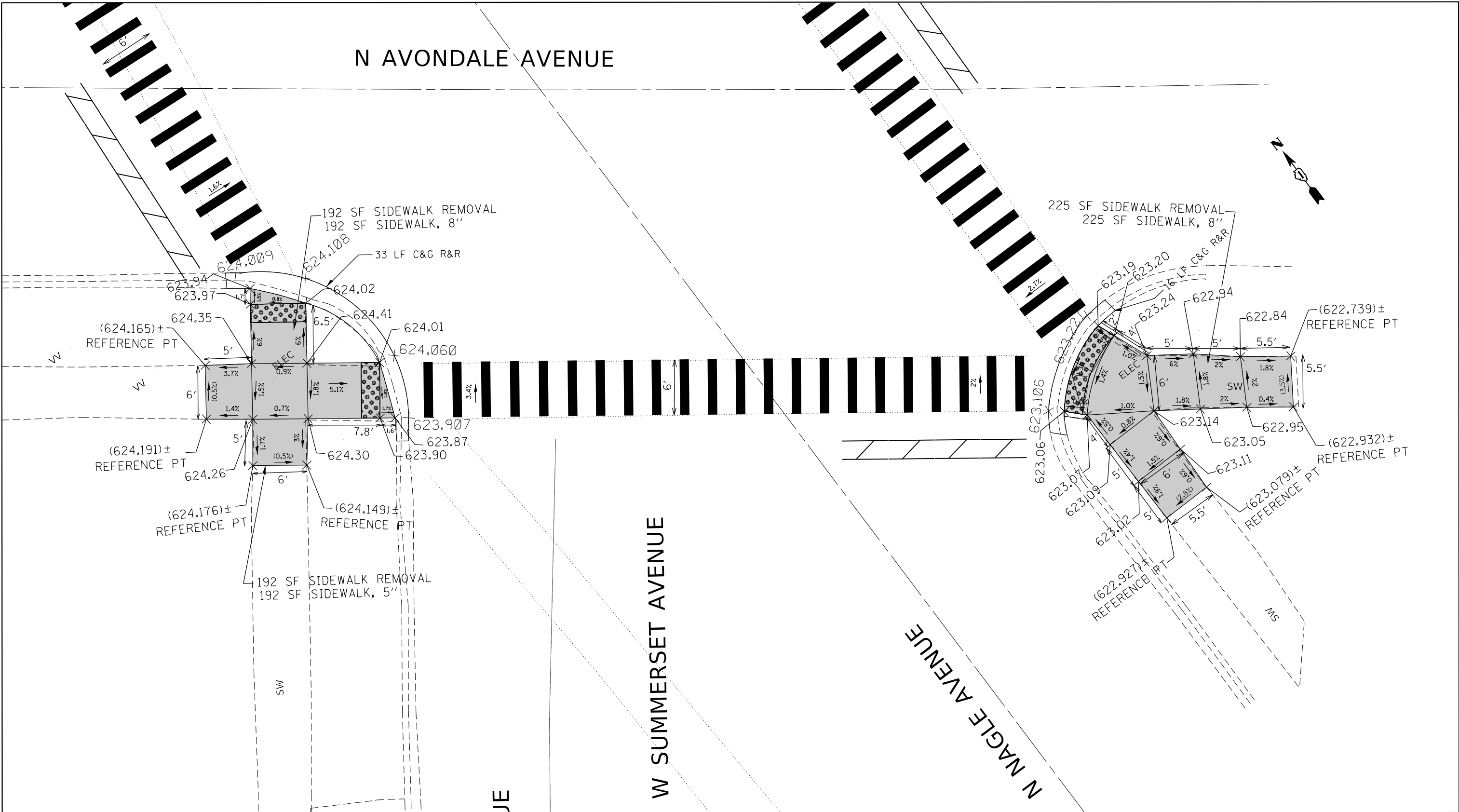
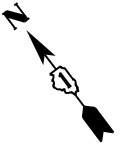
SCALE: SHEET OF SHEETS STA. TO STA.

OR R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	25
CONTRACT NO.			62F66	
ILLINOIS FED. AID PROJECT				

N AVONDALE AVENUE

W SUMMERSET AVENUE

N NAGLE AVENUE



LEGEND

xx.xx'

EXISTING LENGTH

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 625.363

BENCHMARK : TRAFFIC CONTROL BOX AT SOUTH WEST OF AVONDALE AVE. AND NAGLE AVE./SUMMERSET AVE.

LOCATION : SOUTH WEST CORNER OF AVONDALE AVE. AND NAGLE AVE./SUMMERSET AVE.

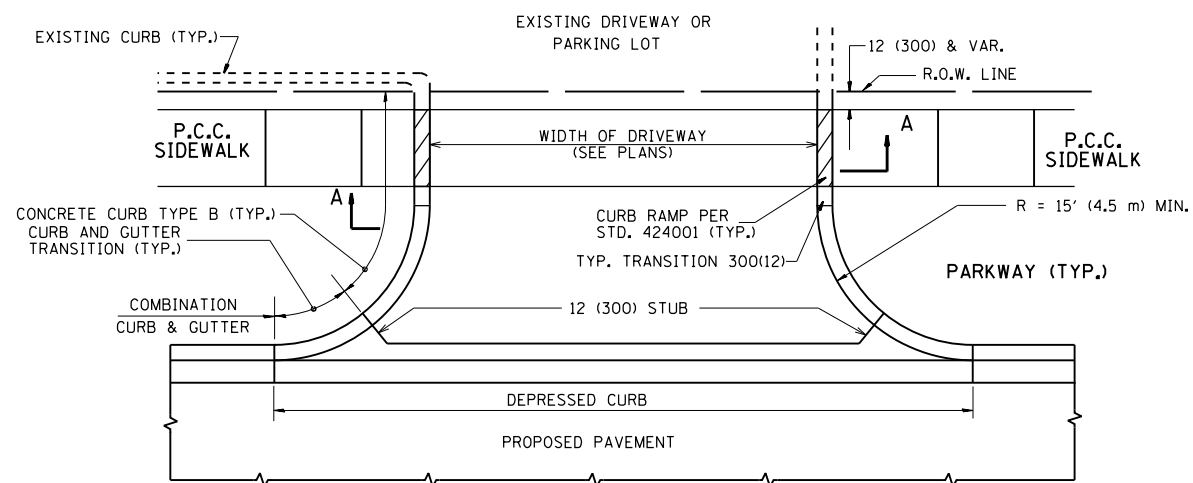
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

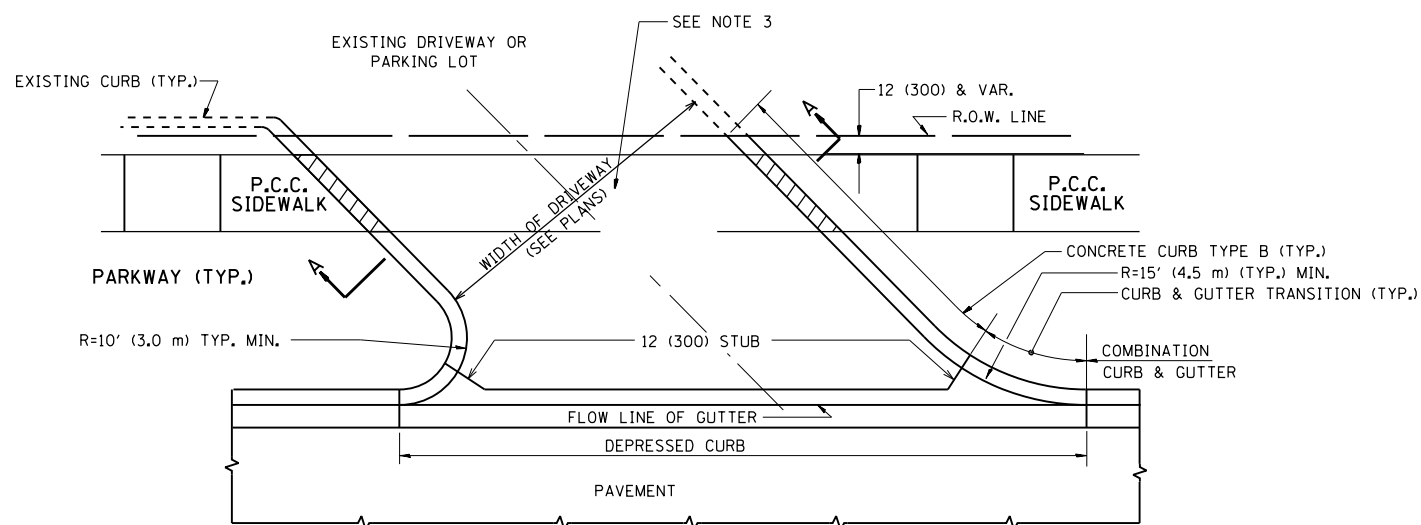
AVONDALE AVE AT NAGLE AVE./SUMMERSET AVE. SOUTH
ADA SIDEWALK DETAIL SHEETS

SCALE: SHEET OF SHEETS STA. TO STA.

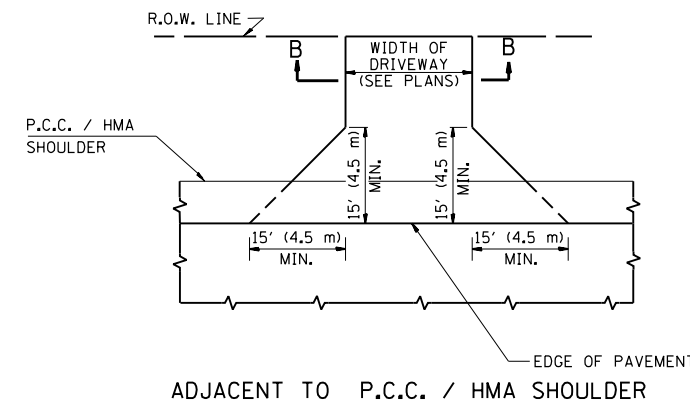
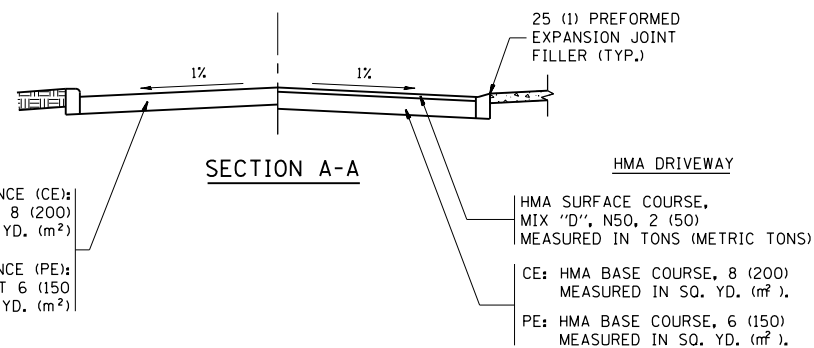
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130	2017-037RS	COOK	39	26
CONTRACT NO. 62F66			ILLINOIS FED. AID PROJECT	



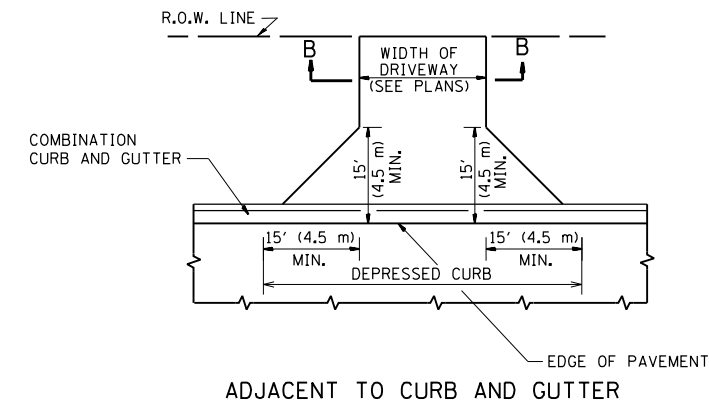
WITH CONCRETE CURB, TYPE B



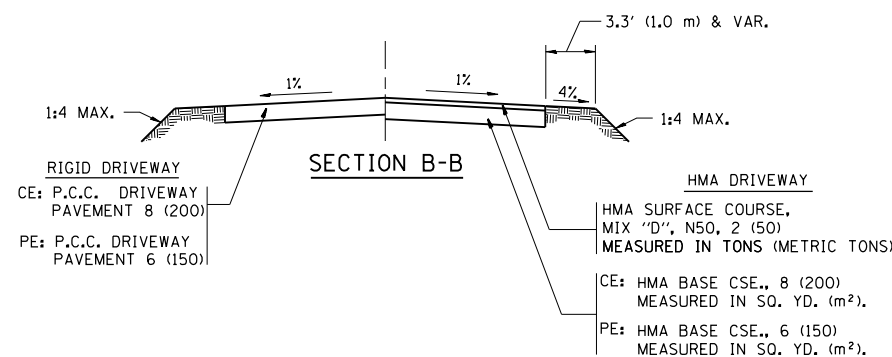
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

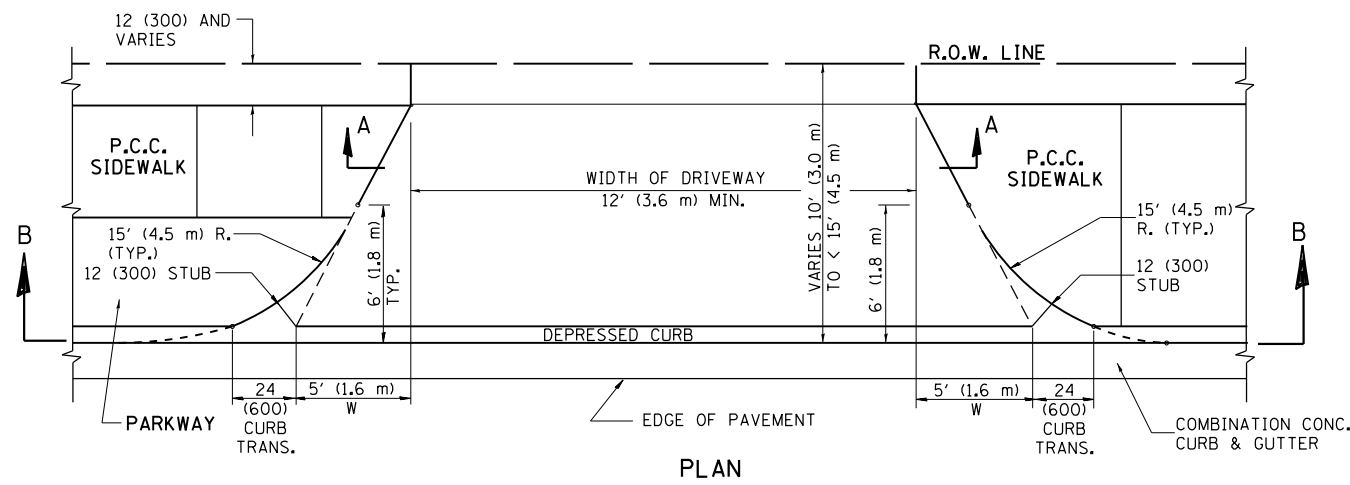
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = valledolidv	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI025\DRW\12132018\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 11-04-95	REVISED - R. BORO 06-11-08
			REVISED - R. BORO 09-06-11

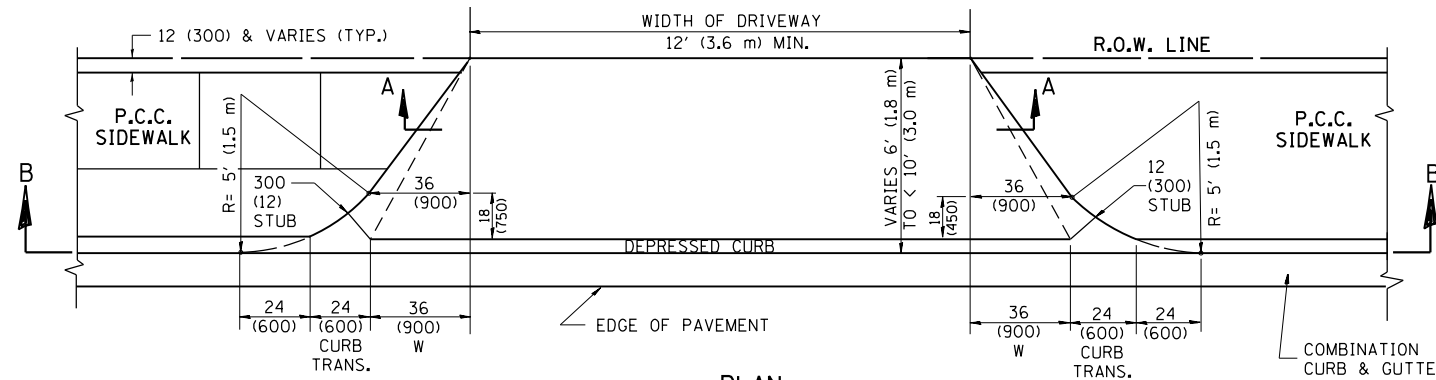
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

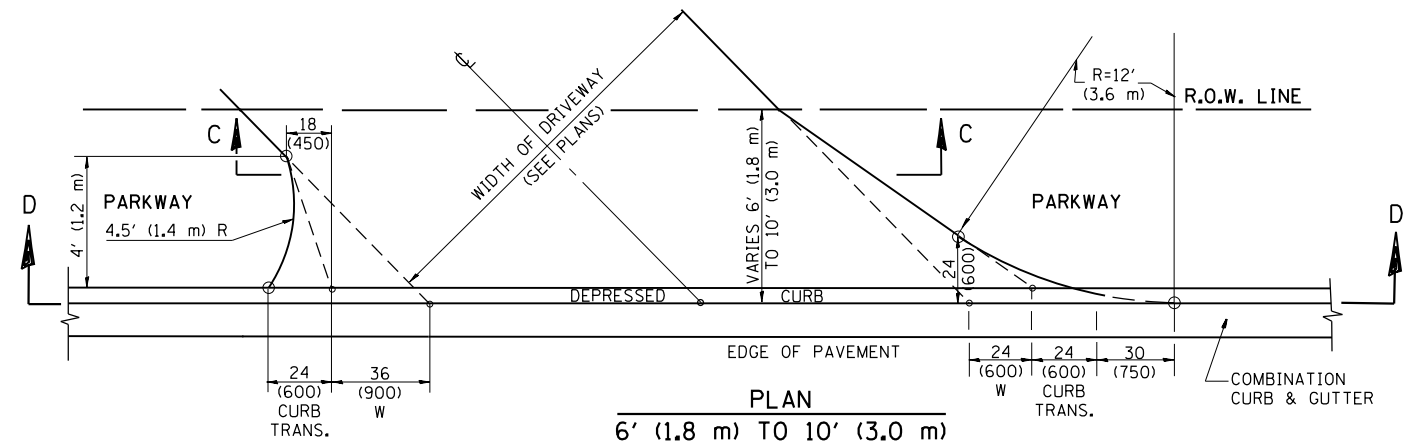
OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	27
BD0156-07 (BD-01)		CONTRACT NO.	62F66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



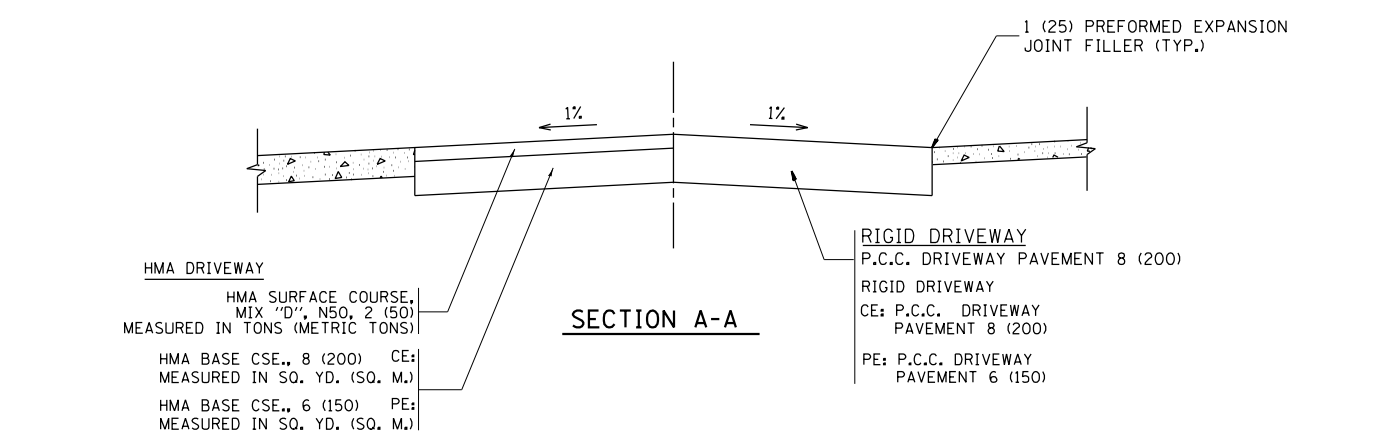
PLAN
10' (3.0 m) TO < 15' (4.5 m)



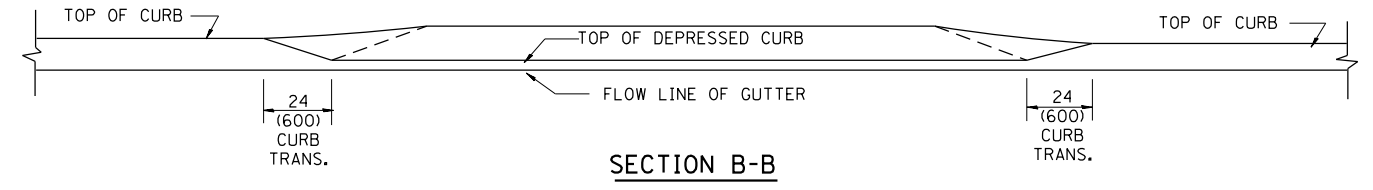
PLAN
6' (1.8 m) TO < 10' (3.0 m)



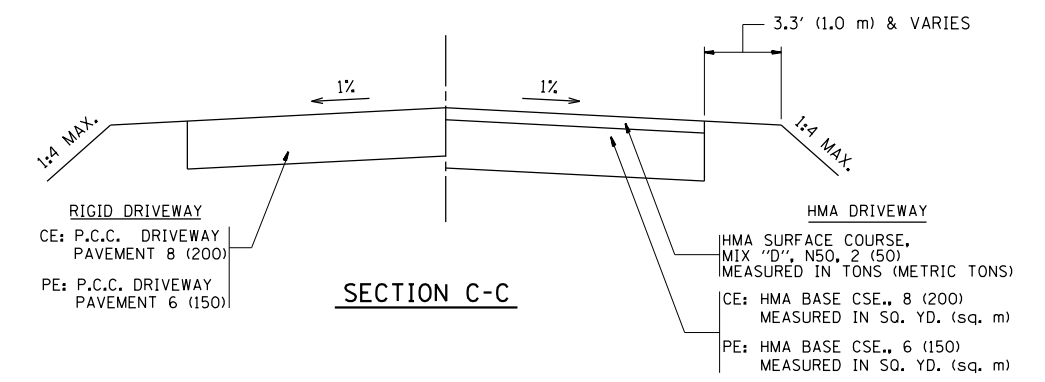
PLAN
6' (1.8 m) TO 10' (3.0 m)



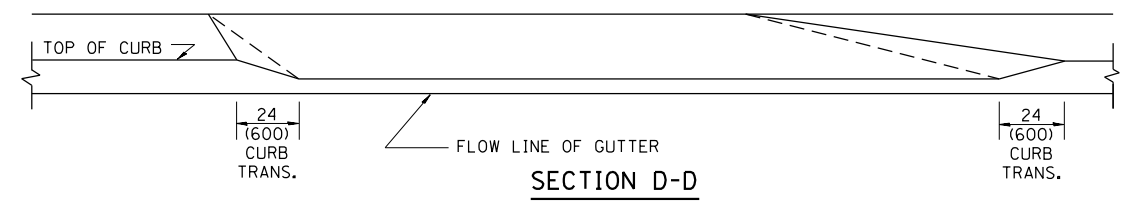
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

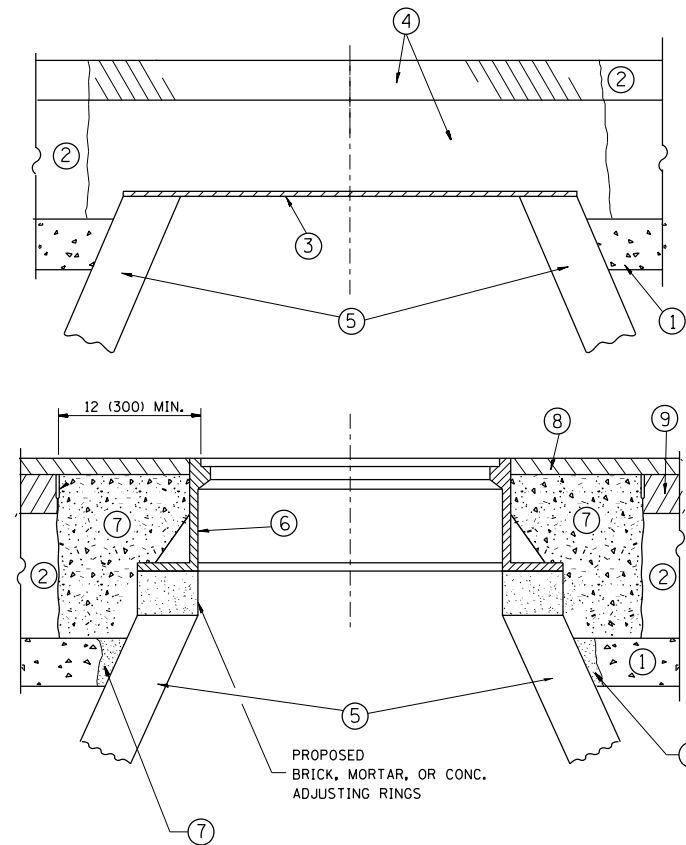
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = valledolidv	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
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PLOT SCALE = 100.0000' / 1"		DATE - 11-06-95	REVISED - R. BORO 01-01-07
PLOT DATE = 12/13/2018			REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	28
BD400-02 (BD-02)		CONTRACT NO.	62F66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

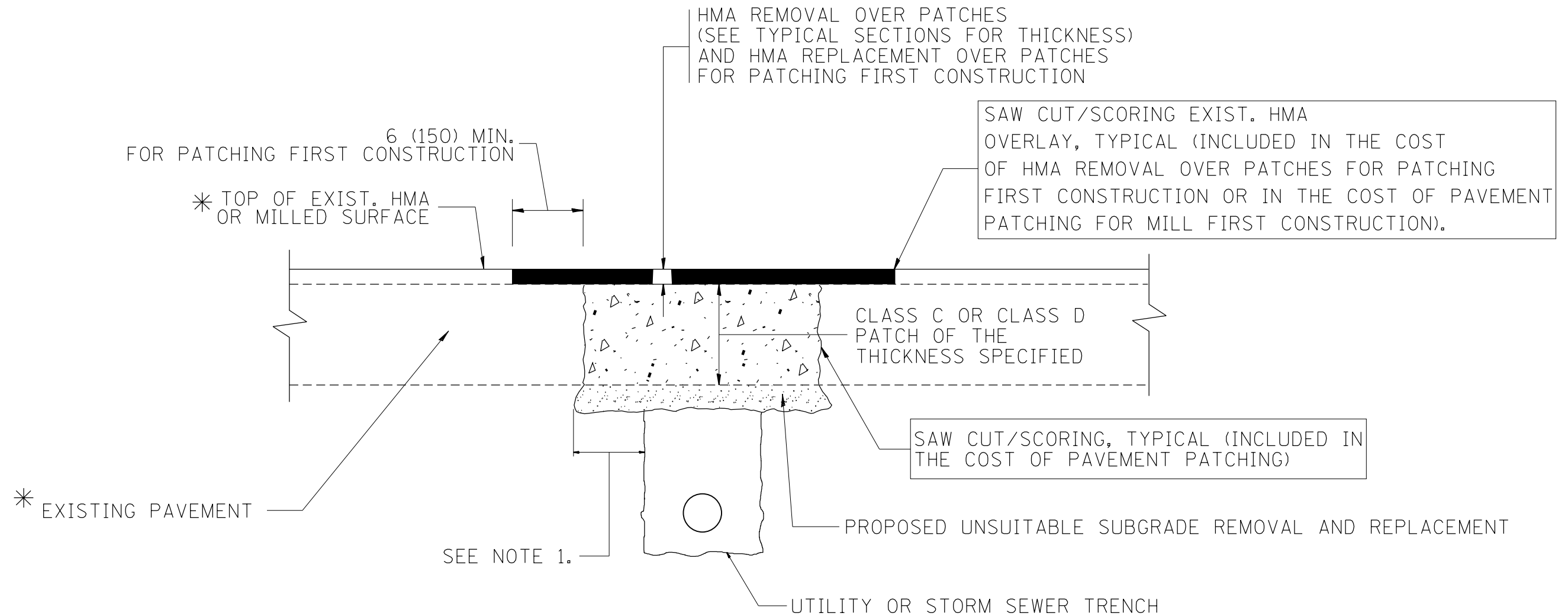
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = valledolidv	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI025\Drawings\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/13/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037R5	COOK	39	29
BD600-03 (BD-8)		CONTRACT NO.	62F66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = valledolidv	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	OR	SECTION	COUNTY	TOTAL	SHEET
pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\1025\Drawings\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07			130	2017-037RS	COOK	39	30
	PLOT DATE = 12/13/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07			BD400-04 (BD-22)		CONTRACT NO. 62F66		
			REVISED - K. ENG 10-27-08			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

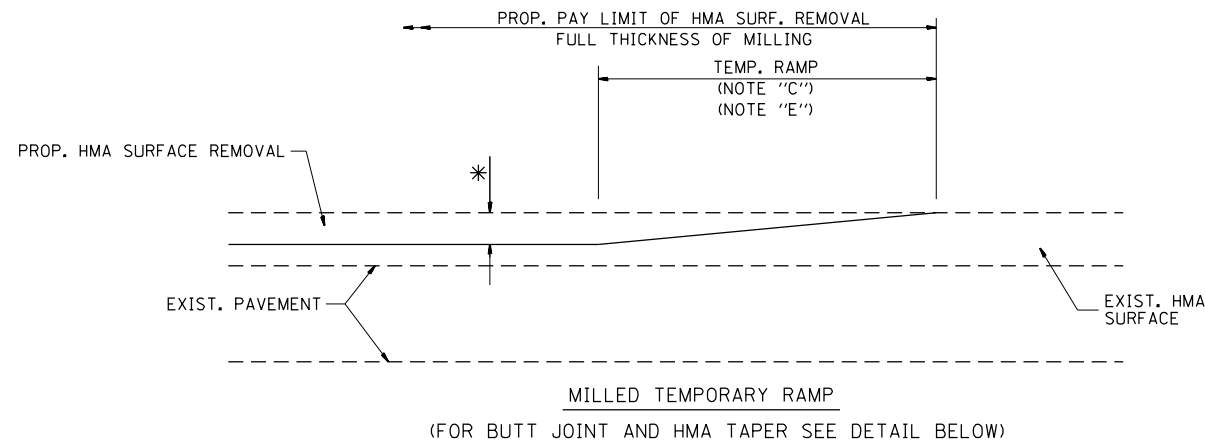
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

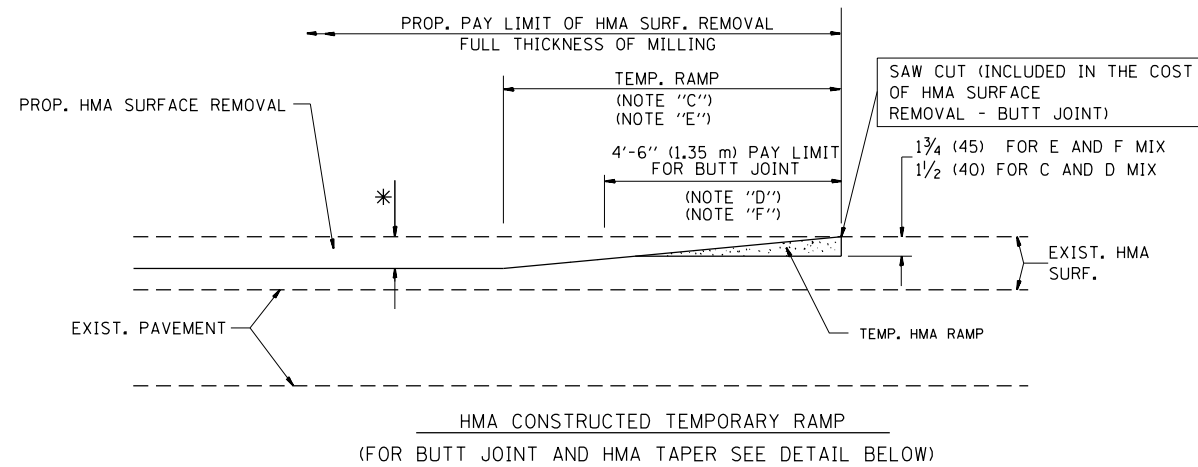
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = valledolidv	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	OR R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\1025\Drawings\Design\DistStd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			130	2017-037R5	COOK	39	31
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09			BD600-06 (BD-24)		CONTRACT NO. 62F66		
PLOT DATE = 12/13/2018	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

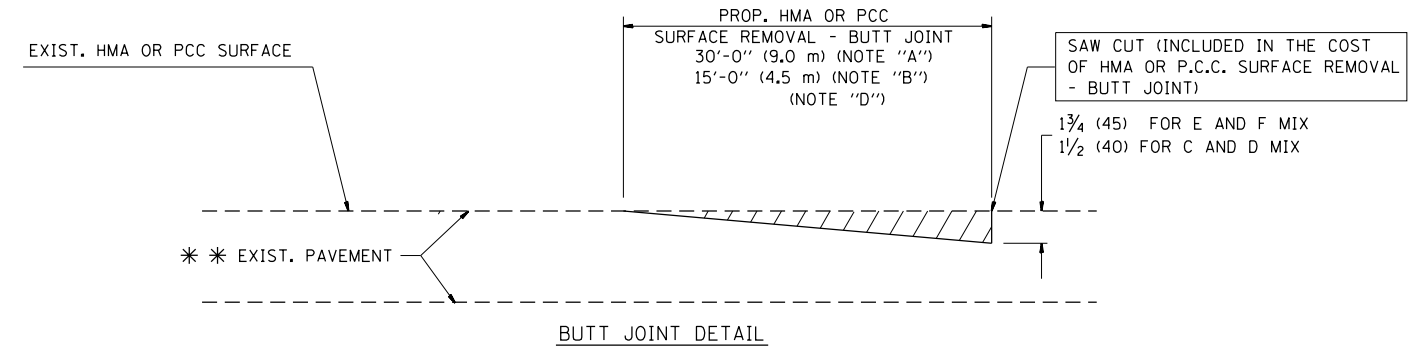


OPTION 1

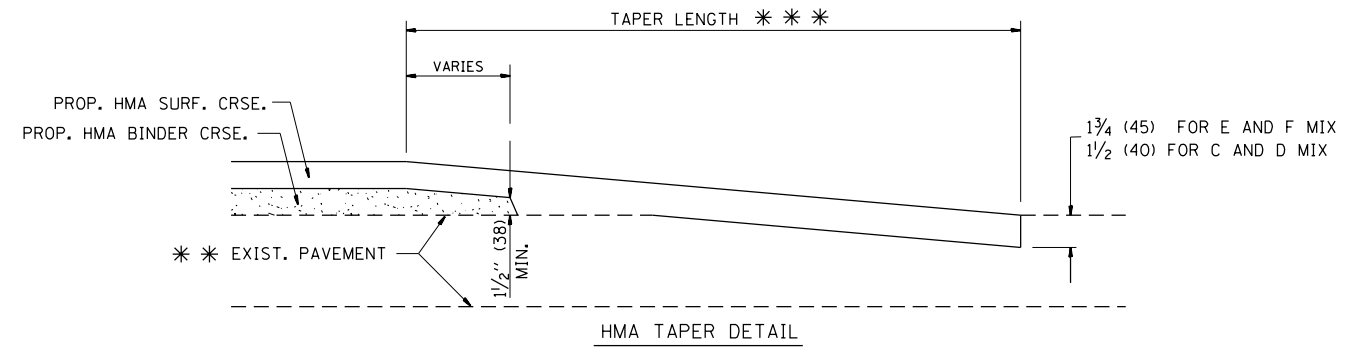


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

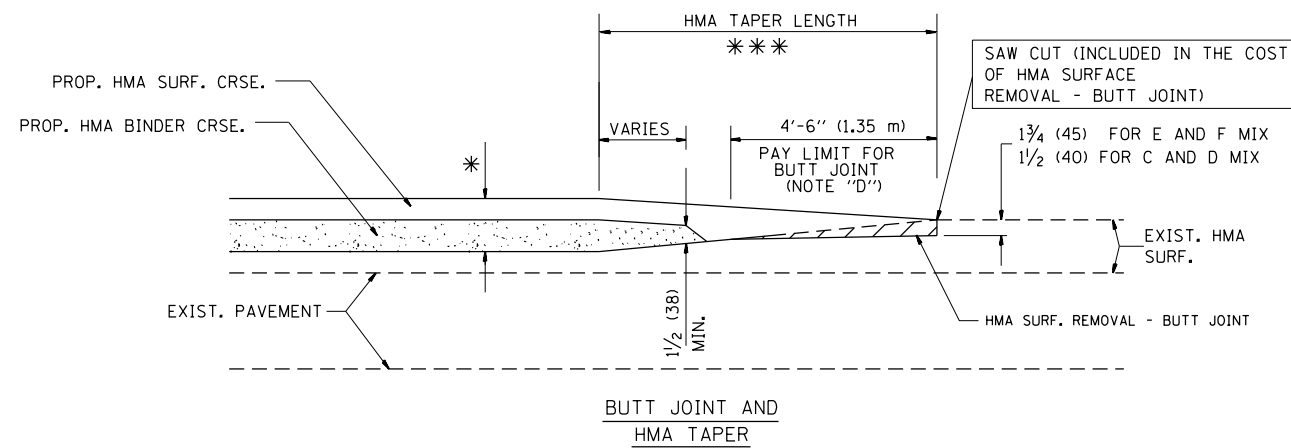
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

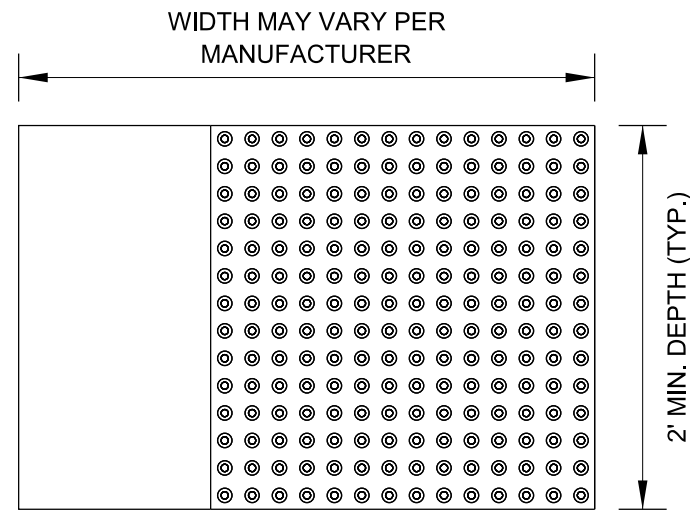
FILE NAME =	USER NAME = valledolidv	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI025\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 12/13/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

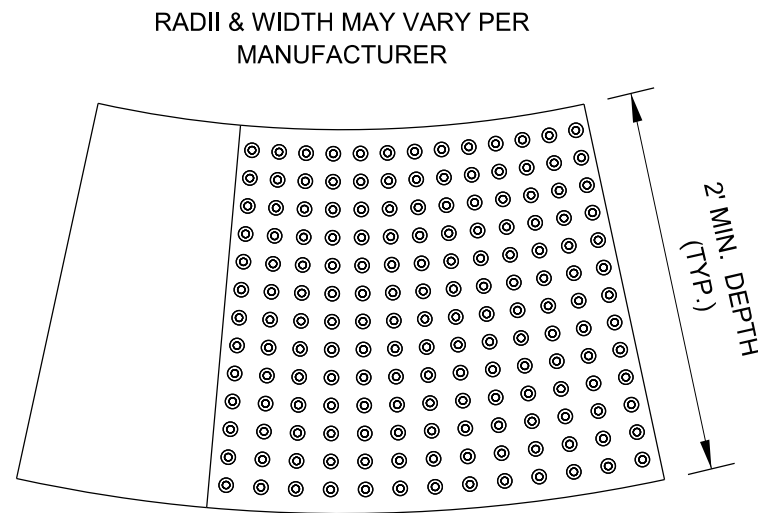
BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	32
BD400-05 BD32		CONTRACT NO.	62F66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

STRAIGHT DETECTABLE WARNING UNITS



RADIAL DETECTABLE WARNING UNITS

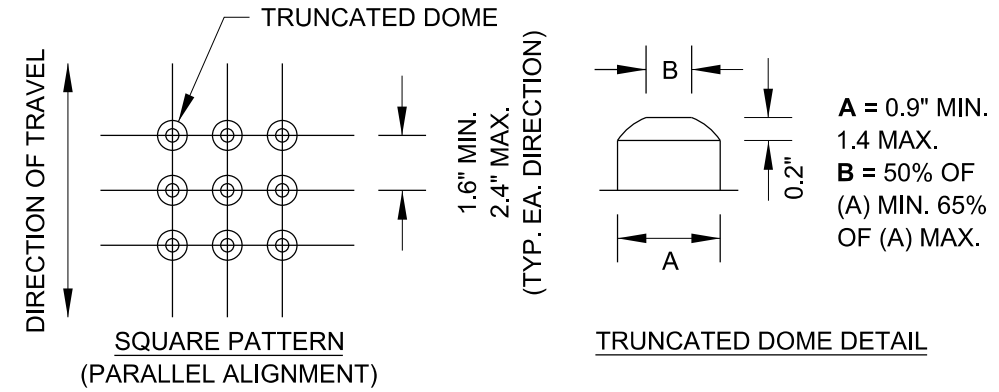


DETECTABLE WARNING UNIT SIZES

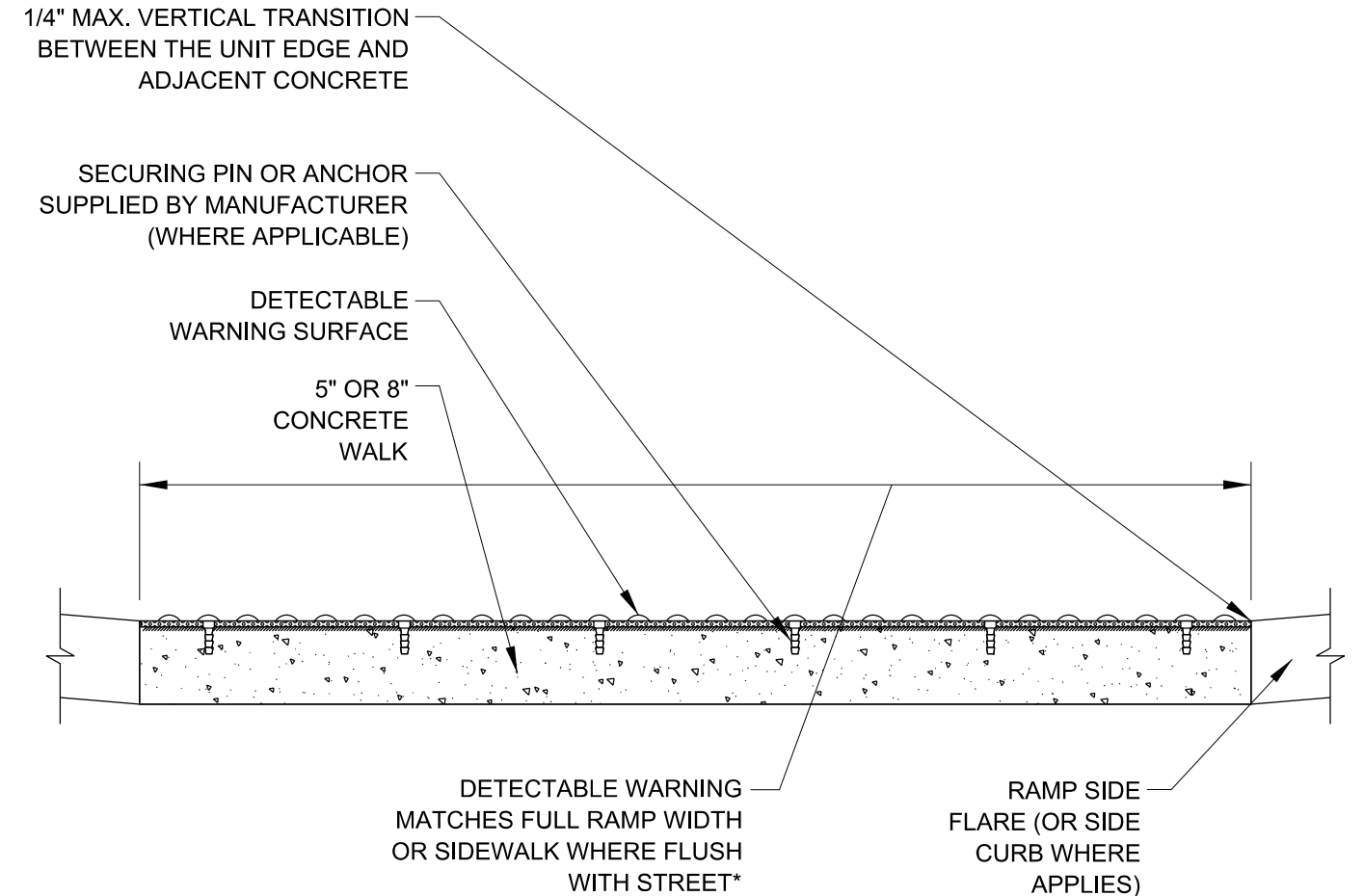
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



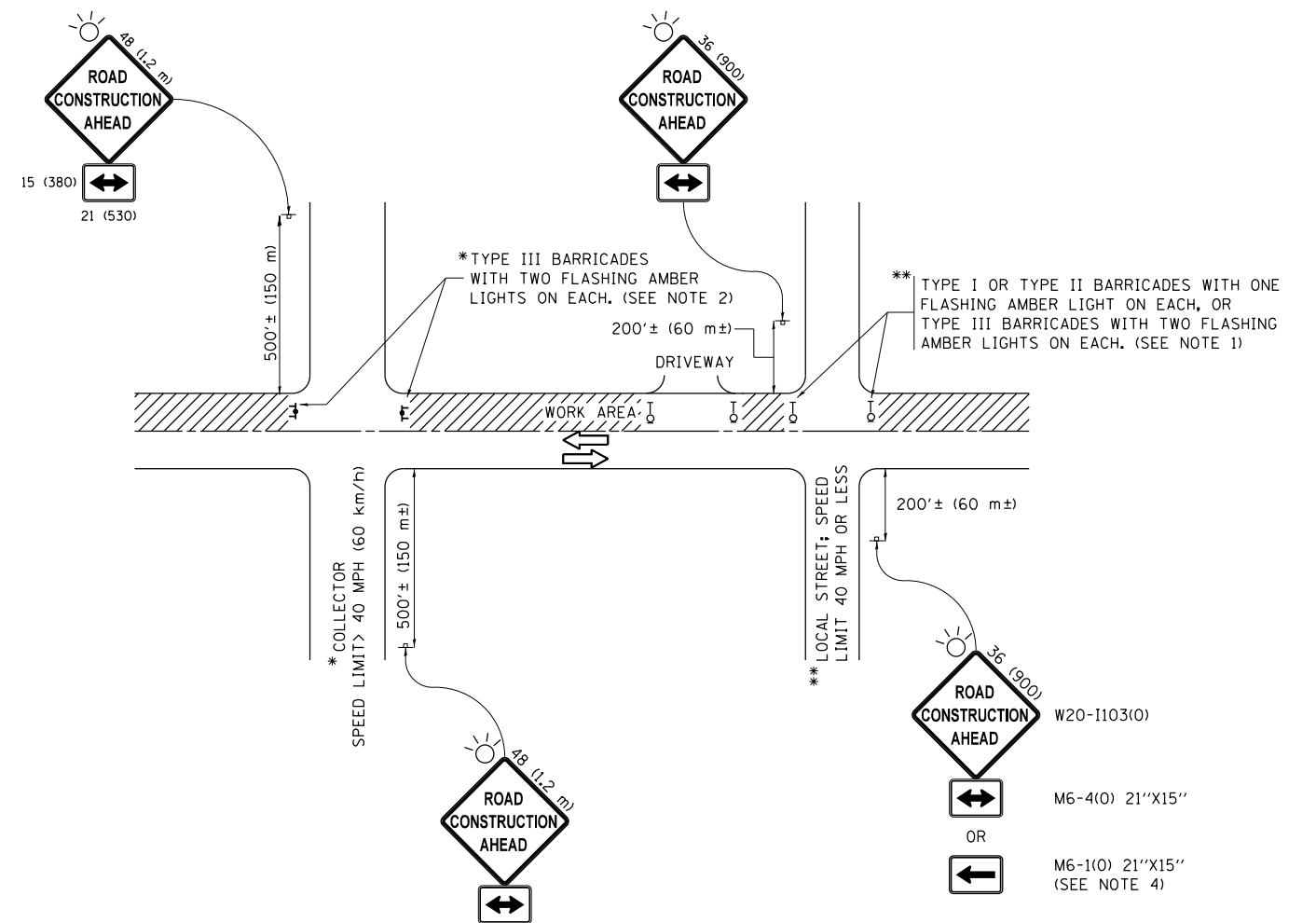
UNIT PATTERN & DOME DETAIL



*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

DETECTABLE WARNING UNIT SECTION

FILE NAME =	USER NAME = valledolidv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CITY OF CHICAGO DETECTABLE WARNINGS	OR	SECTION	COUNTY	TOTAL	SHEET
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI025\Drawings\Design\DistStd.dgn		CHECKED -	REVISED -			130	2017-037RS	COOK	39	33
Default	PLOT SCALE = 100.0000' / in.	DATE - 06-20-2017	REVISED -			BD 58		CONTRACT NO. 62F66		
	PLOT DATE = 12/13/2018	REVISED -				SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

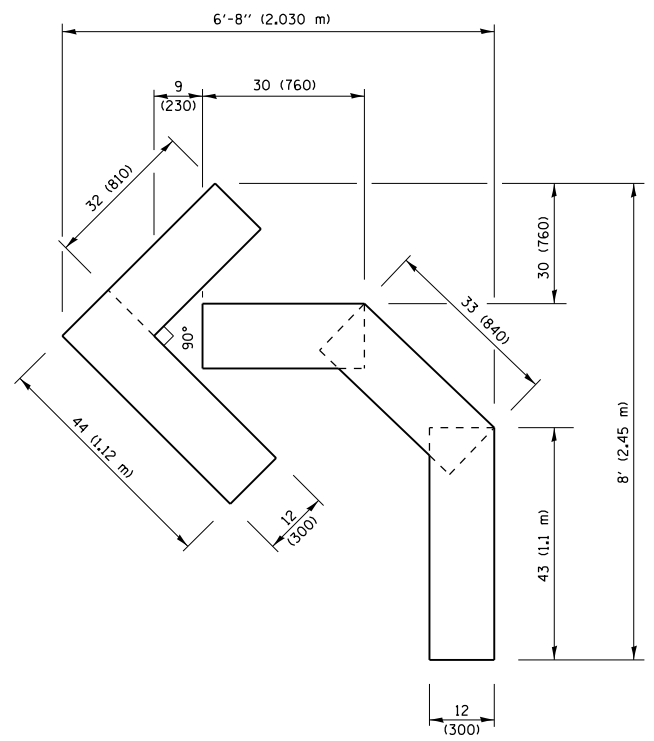
FILE NAME =	USER NAME = valledolidv	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI025\Drawings\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 12/13/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

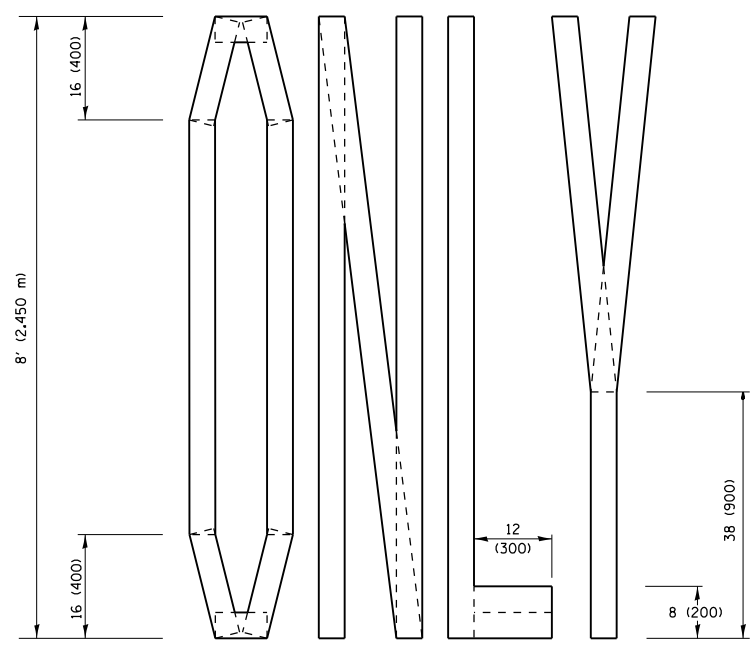
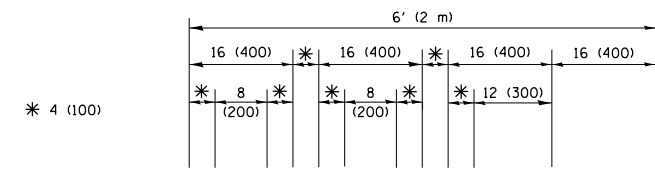
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

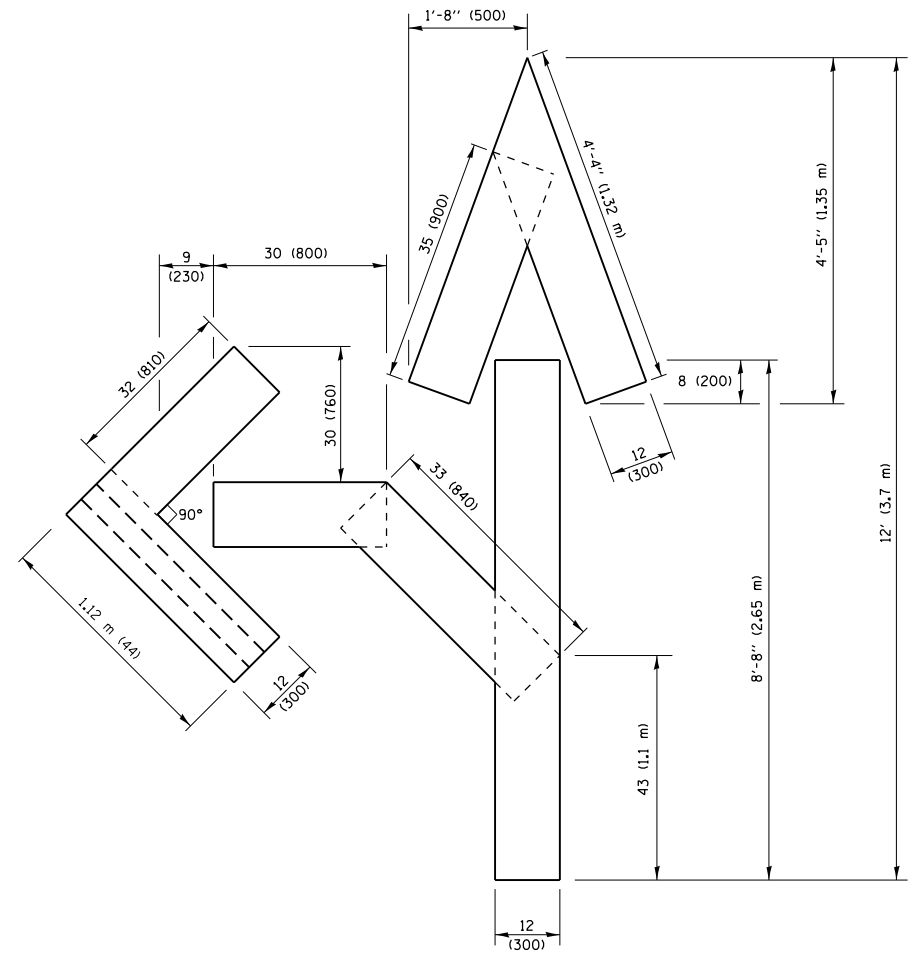
OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	34
TC-10		CONTRACT NO. 62F66		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

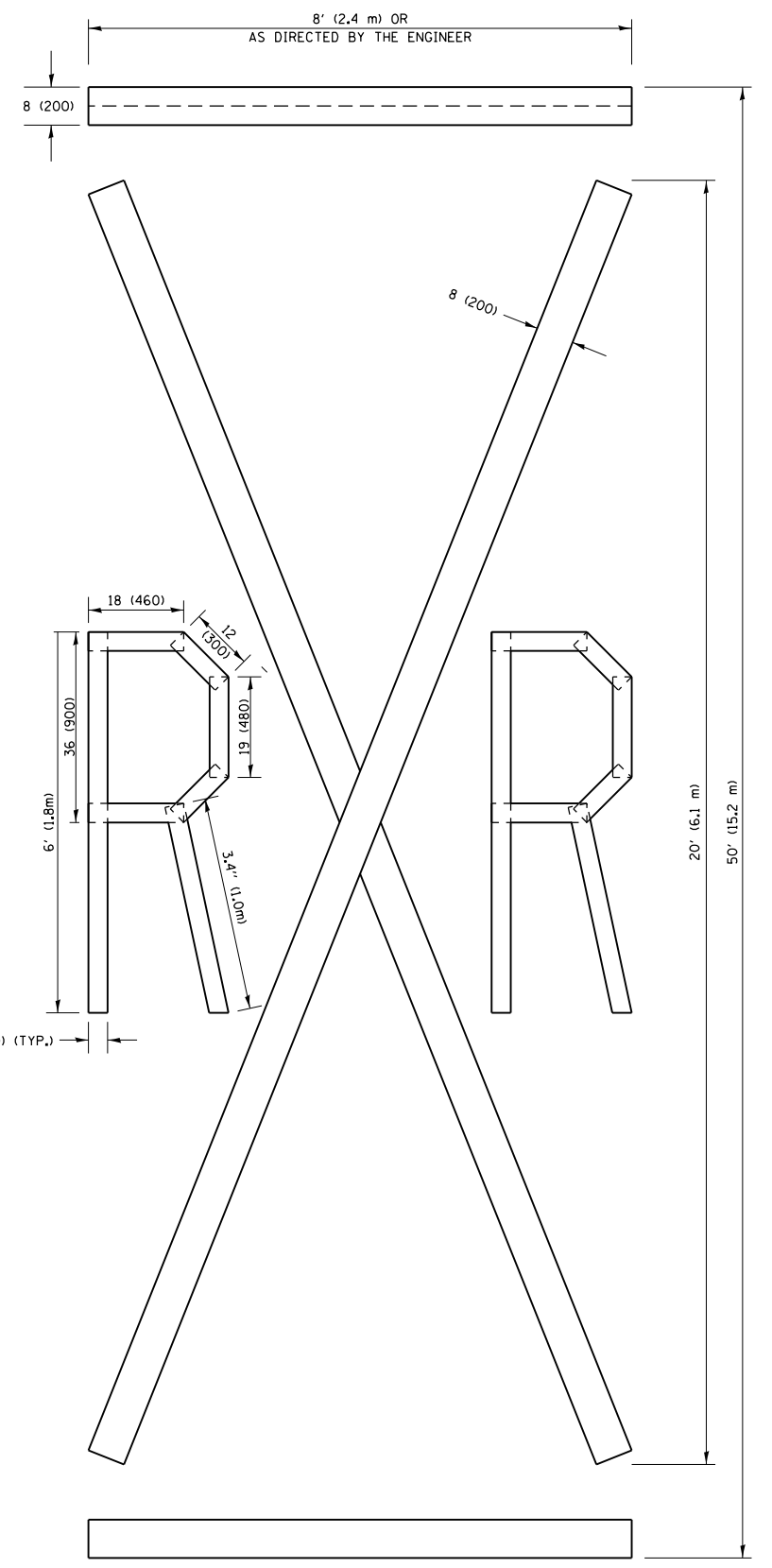


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

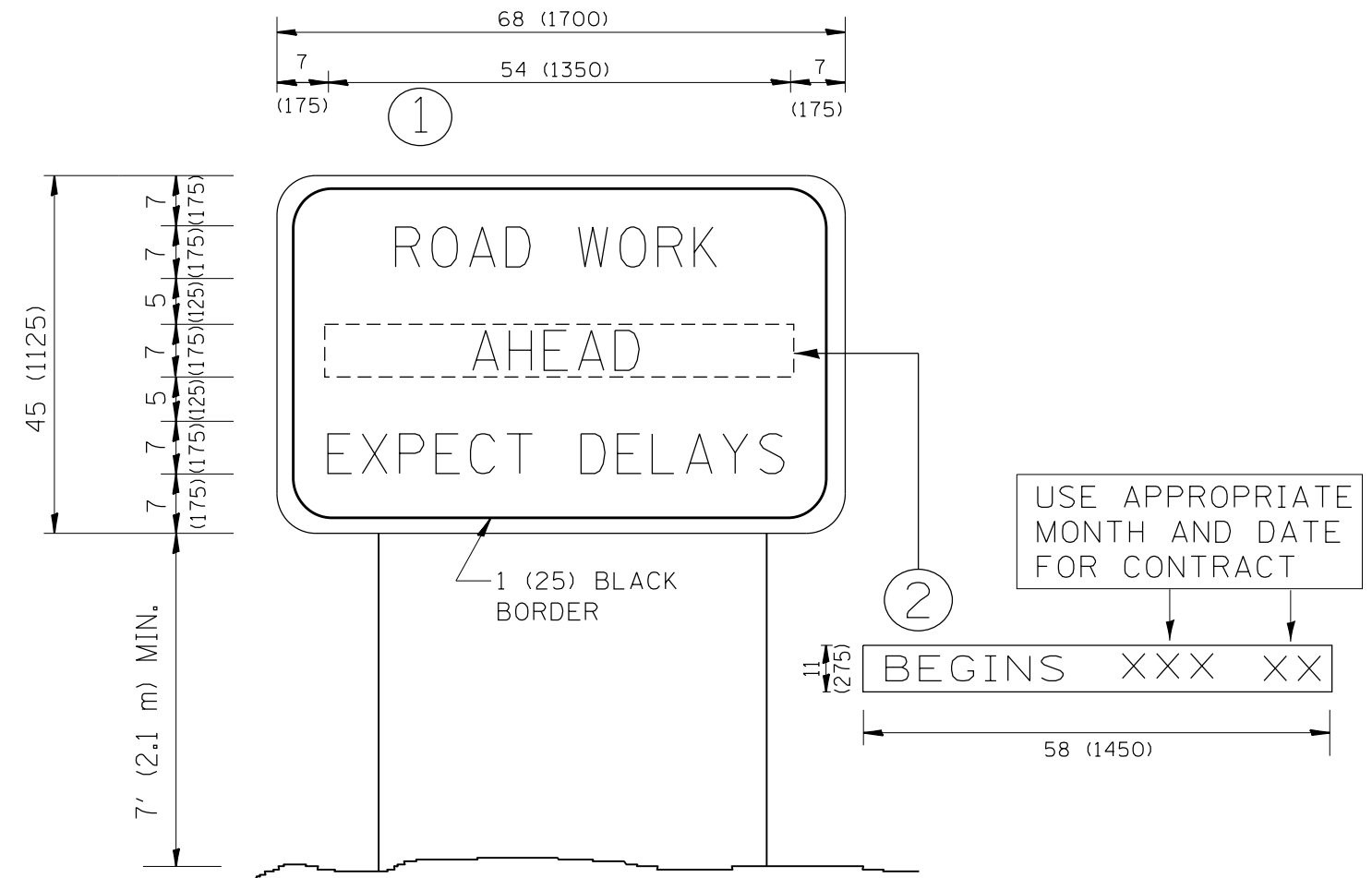
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = valledolidv	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01025\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	35
TC-16		CONTRACT NO. 62F66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

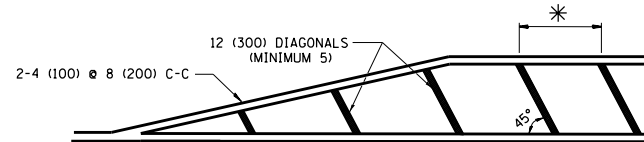
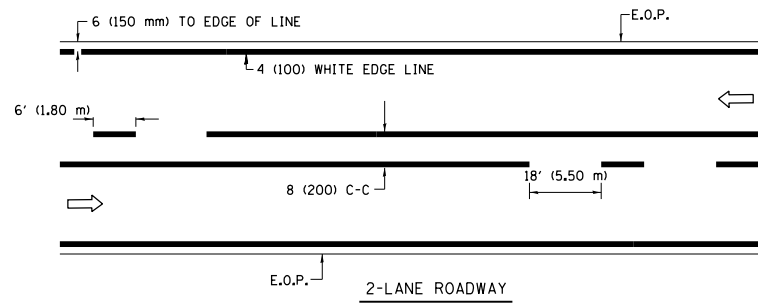


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

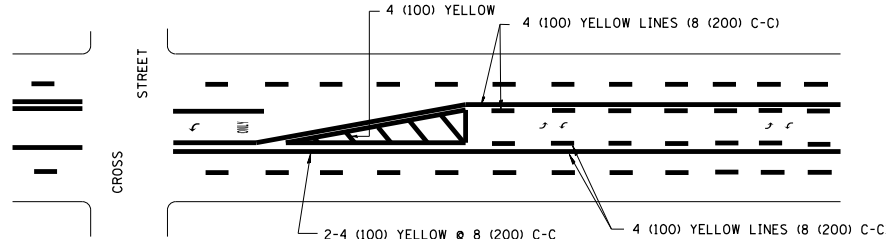
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p:\11\084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI025\Drawings\Design\DistStd.dgn		REVISED - R. MIRS 12-11-97	REVISED - R. MIRS 12-11-97			130	2017-037RS	COOK	39	36	
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 62F66			
	PLOT DATE = 12/13/2018	DATE -	REVISED - C. JUCIUS 01-31-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



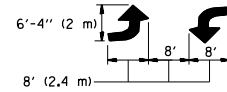
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

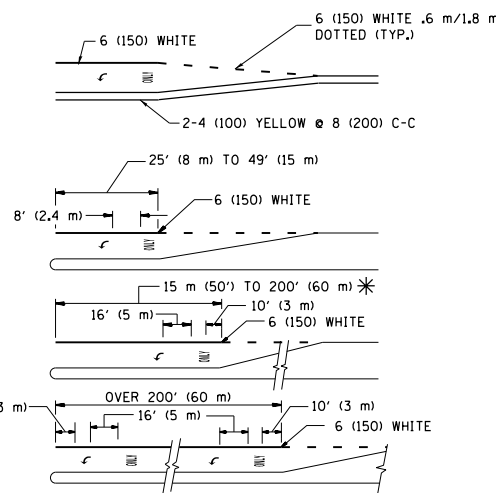


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

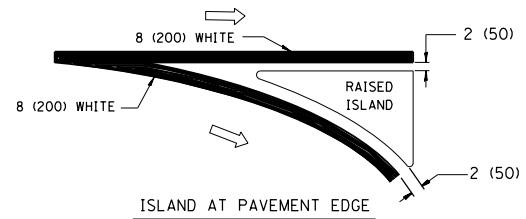
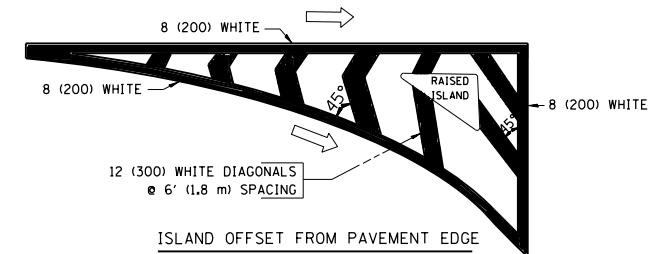


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

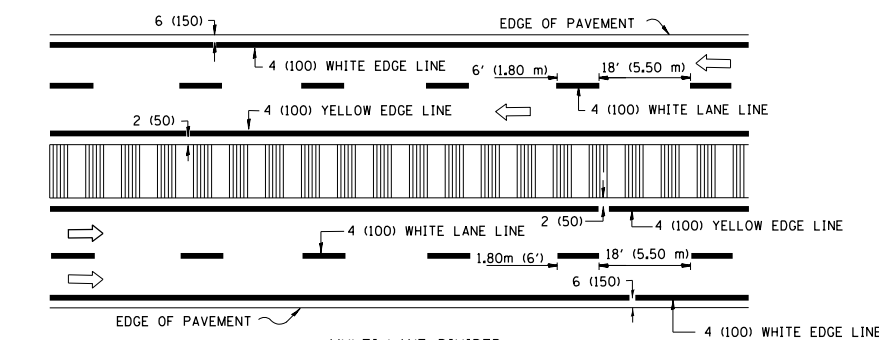
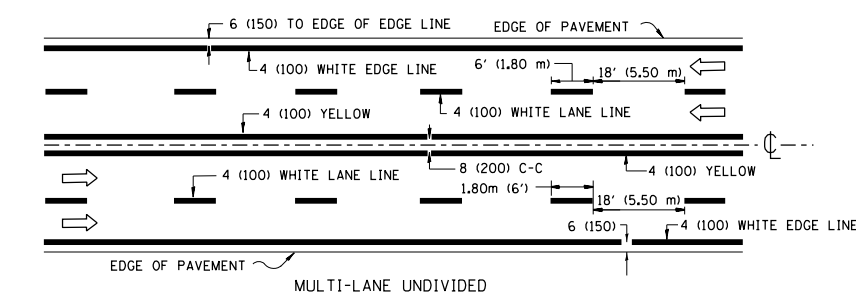


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

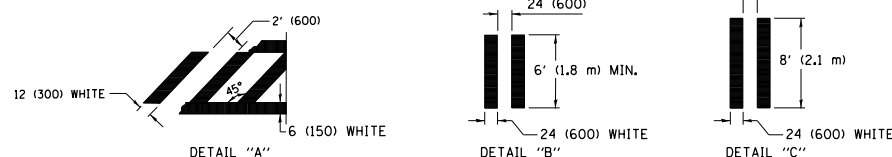
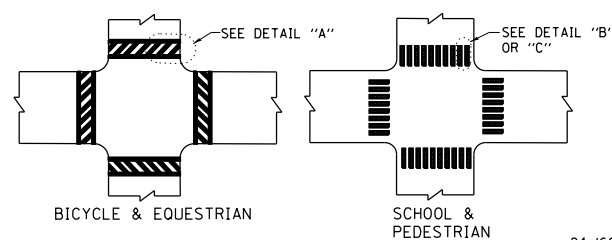
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



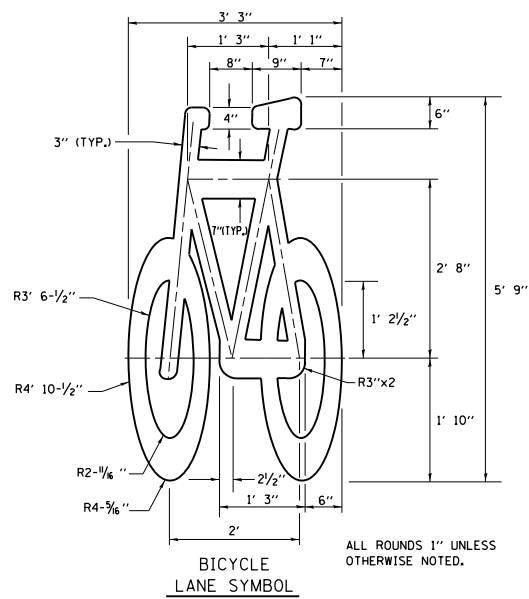
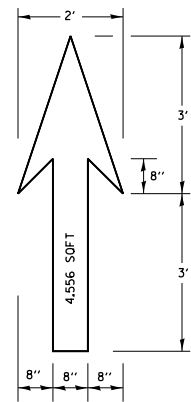
TYPICAL CROSSWALK MARKING

FILE NAME =	USER NAME = valledolidv	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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		DATE -	REVISED -
			REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

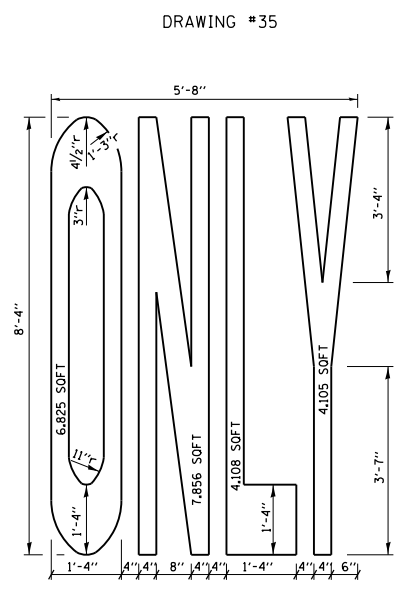
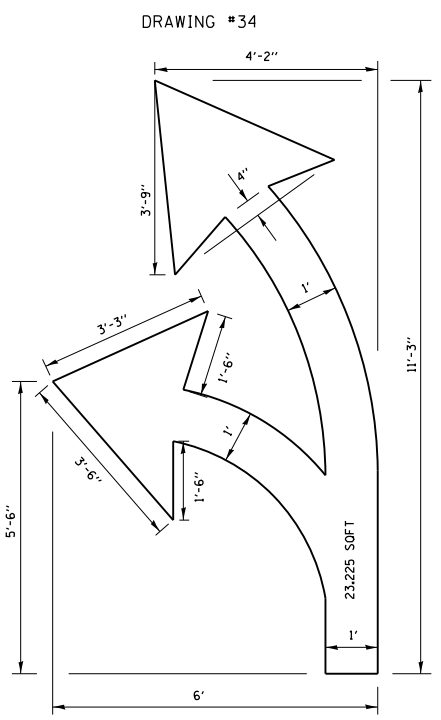
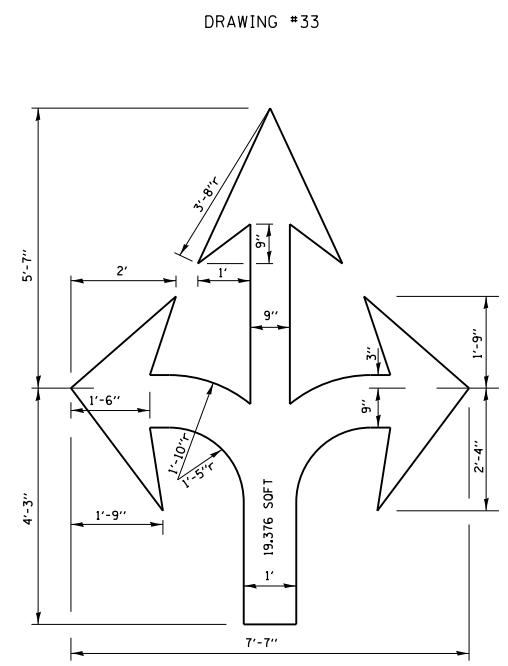
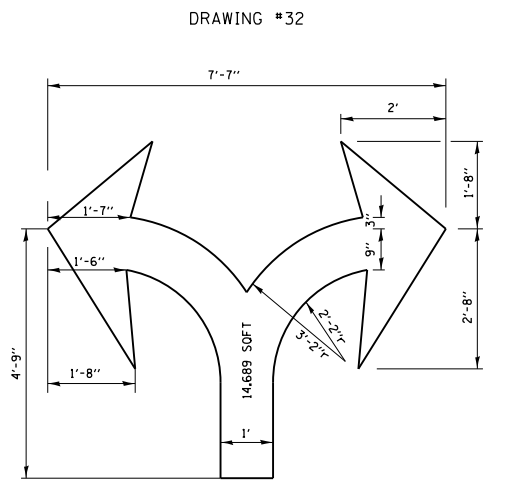
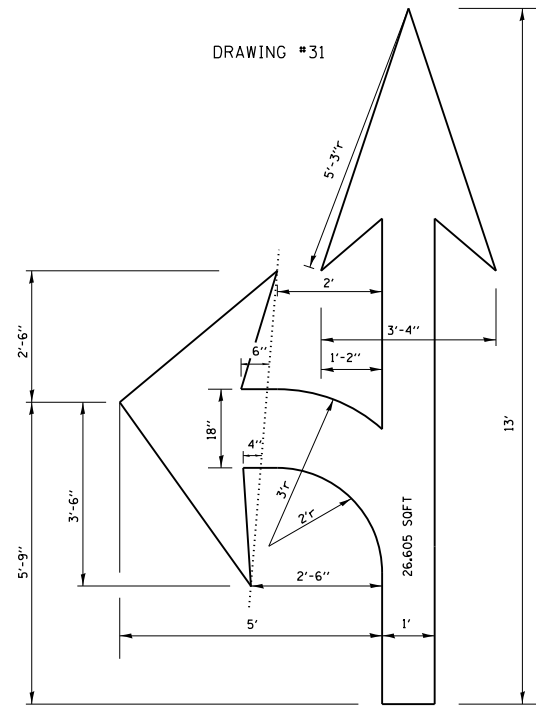
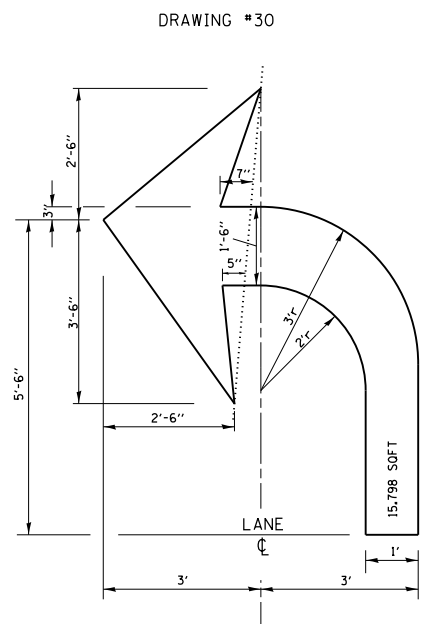
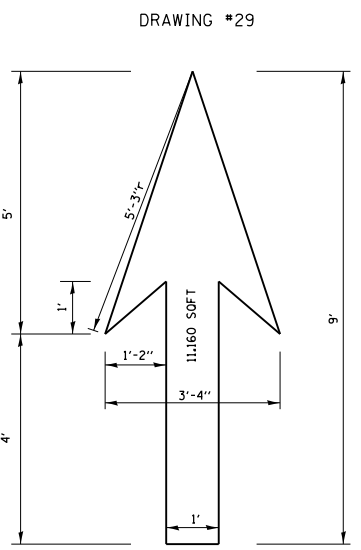
CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS
SCALE: NONE SHEET NO. 1 OF 3 SHEETS STA. TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	37
TC-24		CONTRACT NO. 62F66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTE:
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
 DRAWING #28



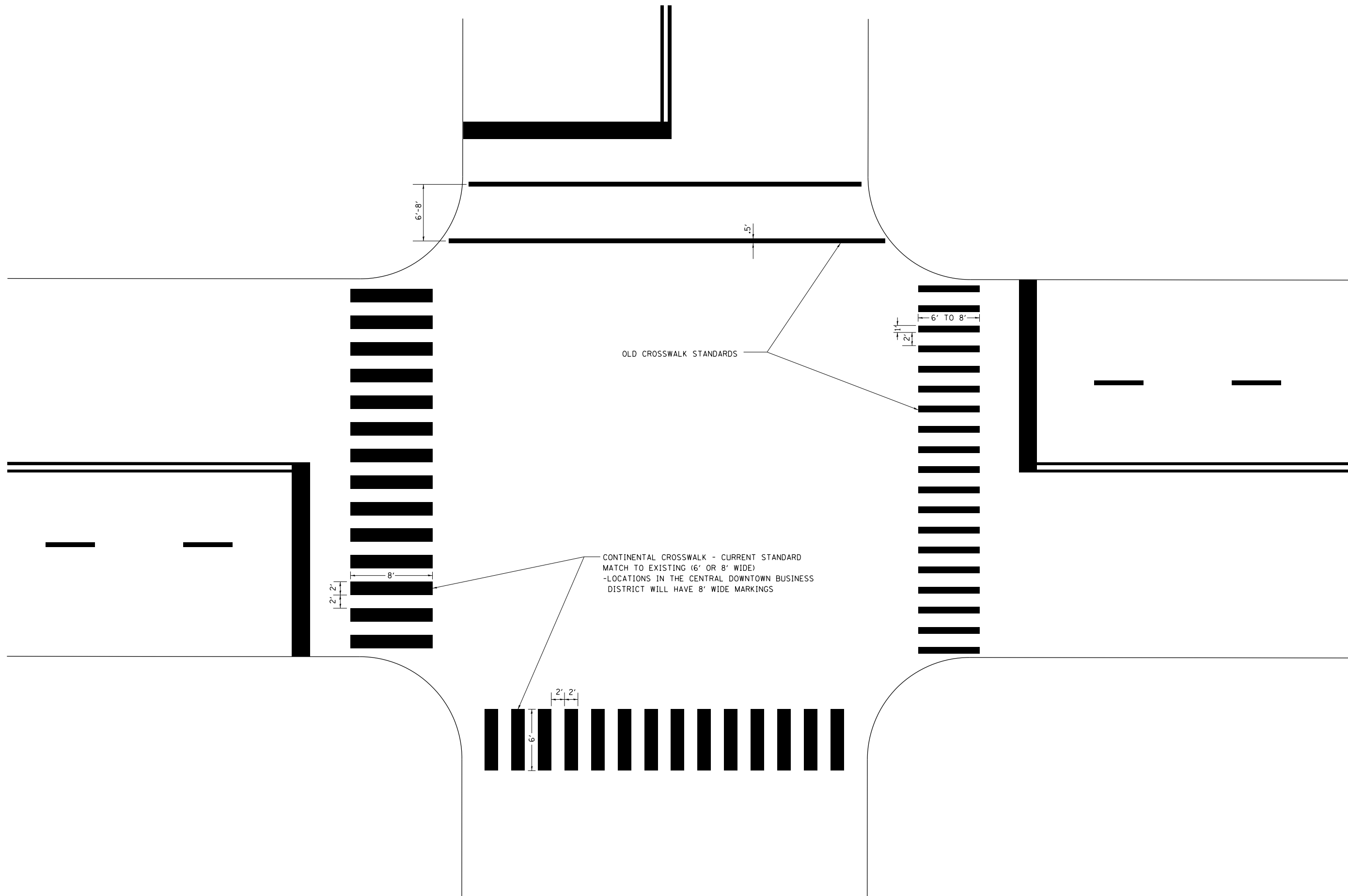
NOTE:
 ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = valledolidv	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
pw\11084EBIDINTEG.illinois.gov\PIWDDTD\Documents\DOT Offices\District 1\Projects\DI025\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - K. ENG 01-12-12
		DATE -	REVISED -
			REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.

OR R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	38
TC-24		CONTRACT NO. 62F66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = valledolidv	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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		PLOT SCALE = 100.0000' / in.	REVISED -
		PLOT DATE = 12/13/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

OR RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
130	2017-037RS	COOK	39	39
TC-24		CONTRACT NO. 62F66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				