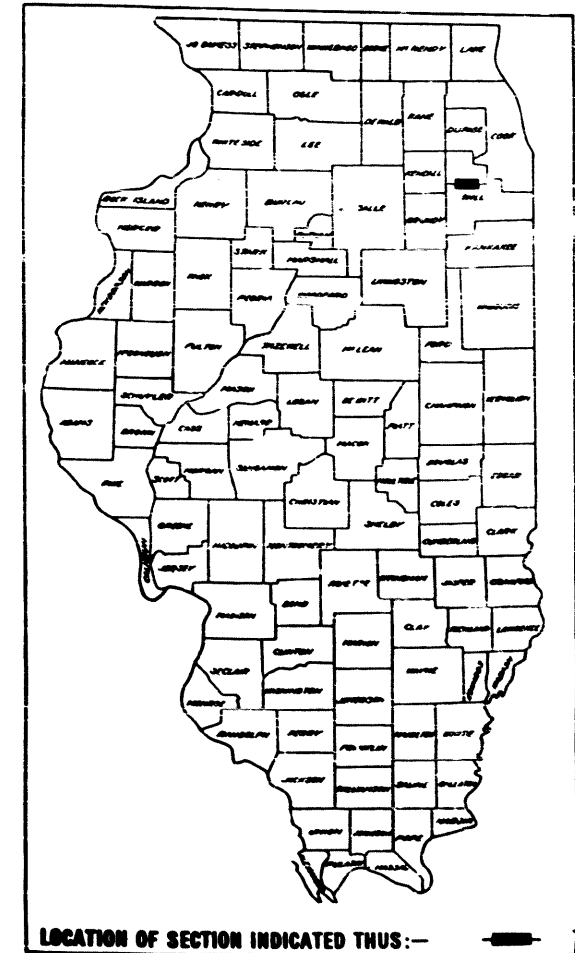


STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID INTERSTATE HIGHWAY

FEDERAL AID ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
EA-80	99-3B	WILL	71	1
FED. ROAD DIV. NO. 4		ILLINOIS PROJECT I-80-4(13)133		

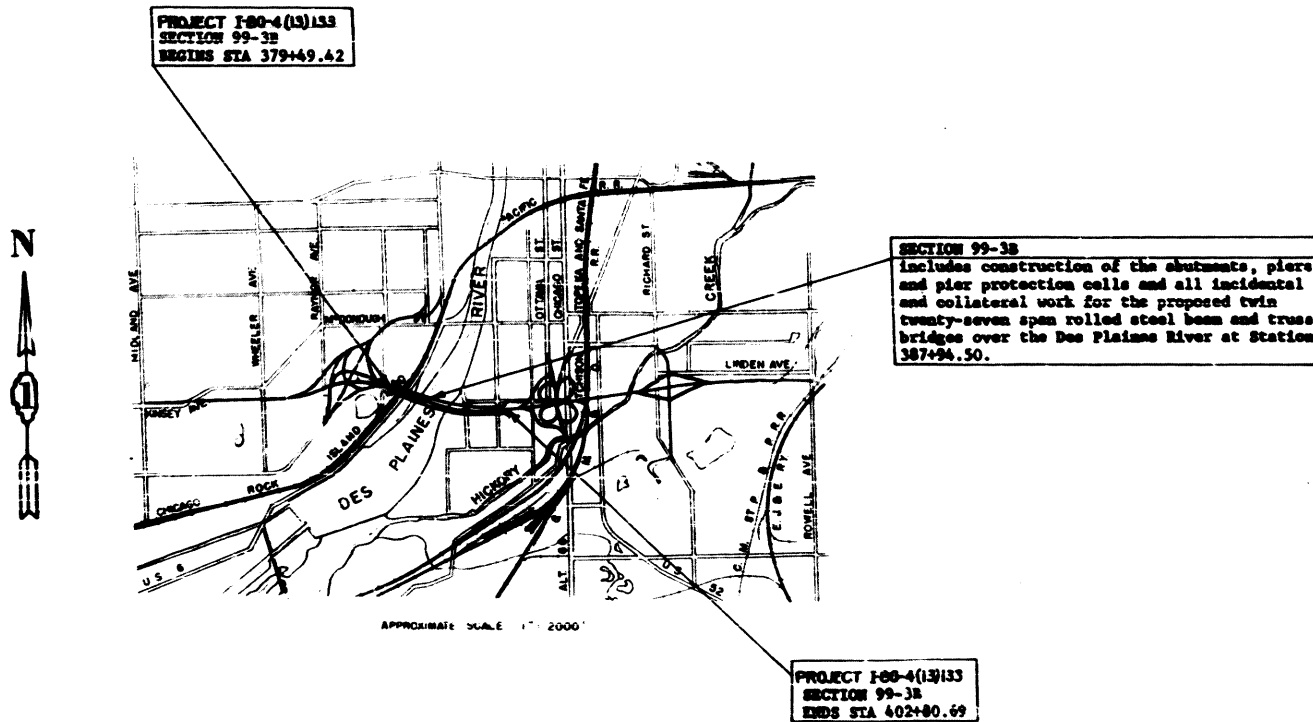
SCALES
 PLAN 1 INCH = 50 FT.
 PROFILE, HOR. 1 INCH = 50 FT.
 PROFILE, VERT. 1 INCH = 5 FT.
 CROSS-SECTIONS 1 INCH = 10 FT.

F.A.I. ROUTE 80 SECTION 99-3B
 PROJECT I-80-4(13)133
 WILL COUNTY



INDEX TO SHEETS

1. TITLE SHEET, INDEX OF SHEETS
2. TYPICAL SECTIONS
3. GENERAL NOTES & SUMMARY OF QUANTITIES
4. PLANS & PROFILES
- 4-12. EXISTING TOPOGRAPHY & REMOVAL QUANTITIES
- 14-16. EXISTING UTILITIES
- 17-19. GENERAL PLAN
- 20-21. BORING LAYOUT & BORING LOGS
- 22-23. APPROACH SPANS - PIER KEY PLANS
- 24-28. PIER 1 N
29. PIER 1 S AND 2 N/S
30. PIER 3 N/S
31. PIER 4 N/S AND 19 N
32. PIER 5 N/S
33. PIER 6 N/S
34. PIER 7 N/S AND 8 N/S
35. PIER 9 N/S
36. PIER 10 N/S, 11 N/S, 12 N/S, 13 N/S, and 14 N/S
37. PIER 15 N/S AND 16 N/S
38. PIER 17 N/S, 18 N/S AND 19 S
39. PIER 20 N AND 21 N
40. PIER 20 S AND 21 S
41. PIER 22 N, 23 N, 24 N AND 26 N
42. PIER 22 S AND 23 S
43. PIER 24 S AND 26 S
44. PIER 25 N
45. PIER 25 S
46. ABUTMENTS
- 47-51. PIER PROTECTION CELLS
52. BAR LISTS
- 53-59. BILLS OF MATERIALS
60. APPROACH SLABS
61. CROSS-SECTIONS
- 62-68. STANDARD DRAWINGS 1604-1, 2113
69. STANDARD DRAWINGS 2138-1, 2153-2
- 70, 70A. STANDARD DRAWINGS 1972-1, 2114, 1973
71. GENERAL LAYOUT SURVEY BASE LINES
- 13.



SECTION 99-3B
 Includes construction of the abutments, piers and pier protection cells and all incidental and collateral work for the proposed twin twenty-seven span rolled steel beam and truss bridges over the Des Plaines River at Station 387+94.50.

W. Benman 5/3/62

STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS	
DESIGNED BY	<i>W. Benman</i>
CHECKED BY	<i>W. Benman</i>
DRAWN BY	<i>W. Benman</i>
APPROVED BY	<i>W. Benman</i>
DATE	July 1962

PLANS PREPARED AND RECOMMENDED BY
BLISSVET ENGINEERING COMPANY
 CONSULTING ENGINEERS
 CRYSTAL LAKE, ILL.—NEW YORK, N.Y.—WOODBURY, N.J.

DEPARTMENT OF COMMERCE
 BUREAU OF PUBLIC ROADS

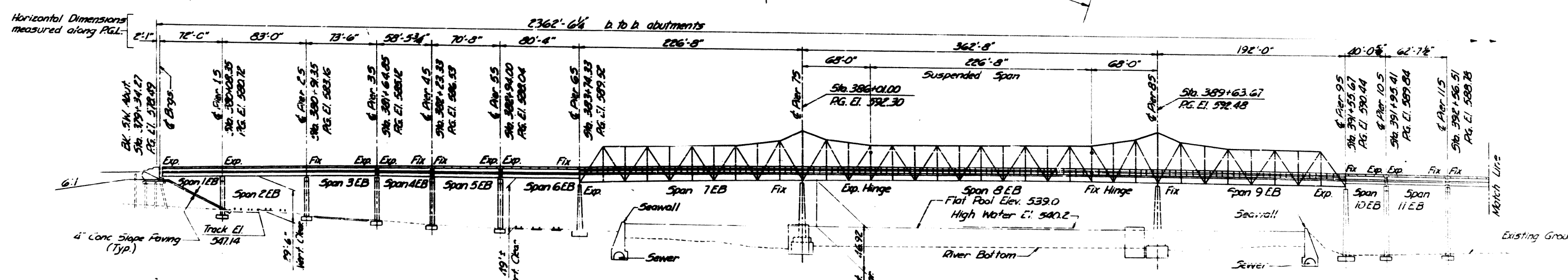
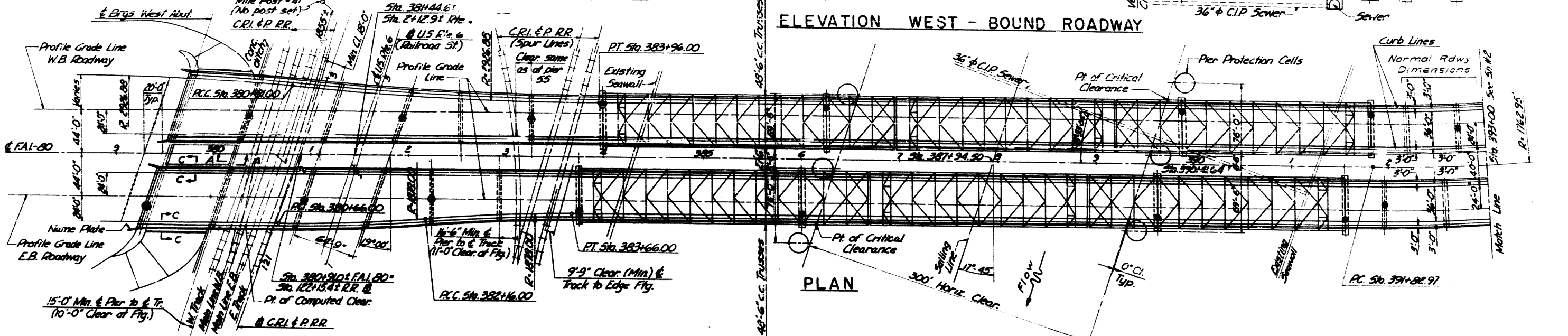
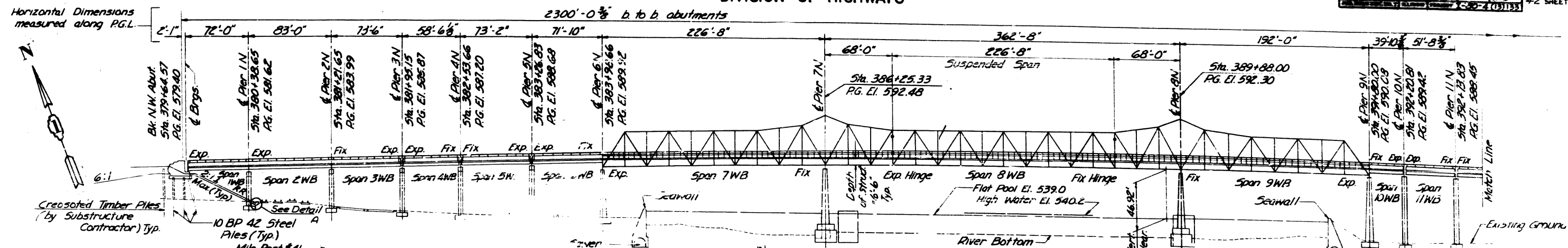
APPROVED _____

DIVISION ENGINEER _____ DATE _____

Bench Mark No. 546A: Brass station marker on West Seawall, Illinois Waterway Sta. 52+69.34 Elev. marked 542.61

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PROJECT NO.	WILL	71	20	SHEET NO. 1 OF 42 SHEETS
DATE				

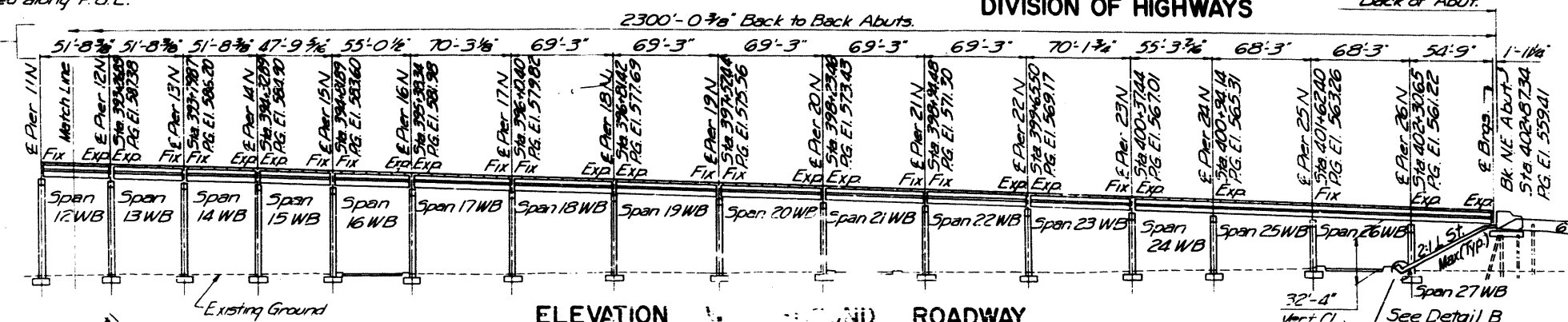


DESIGNED	HCM
CHECKED	JTL
DRAWN	GP RDF
CHECKED	HCM TLU

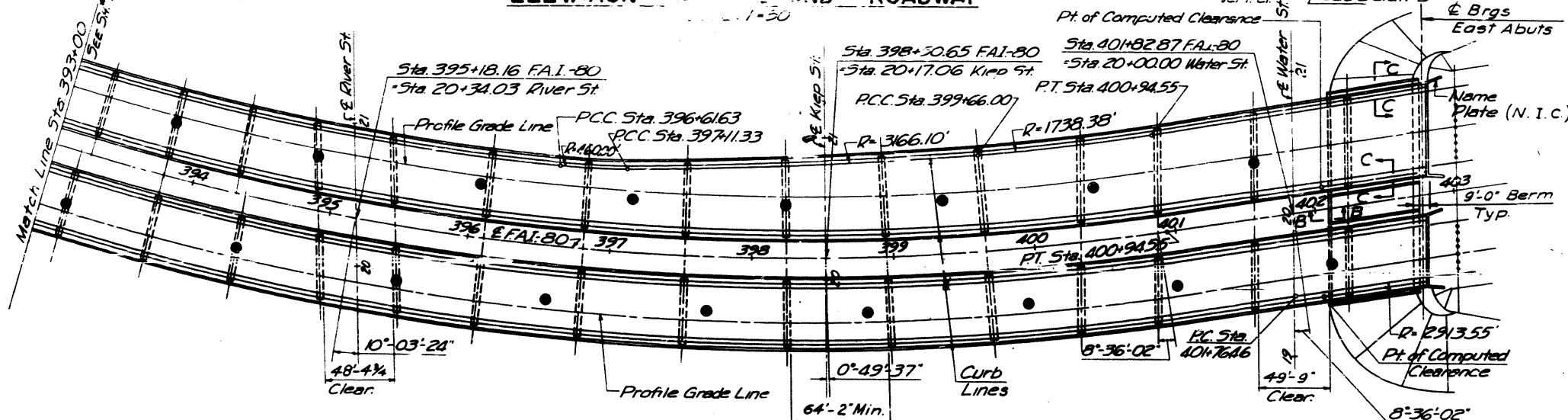
Note:
West Abutments and Piers 1N & 2N are parallel to Piers 3N
Piers 15N & 16N are parallel to River St.
Piers 23N, 24N, 25N & 26N and East Abutments are parallel to Water St.
All other Piers are normal to & FAI-80
© Dentes Boring

ELEVATION EAST - BOUND ROADWAY

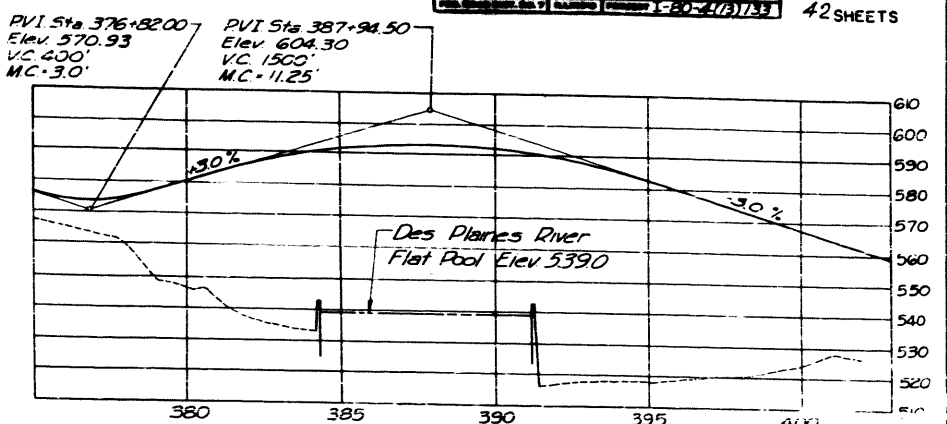
GENERAL PLAN & ELEVATION-I
TWIN BRIDGES OVER DES PLAINES RIVER
FAI-80 STA 387+94.50
PROJECT I-80-4(13)133
SECTION 99-3B
SCALE 1" = 50'
DATE Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.



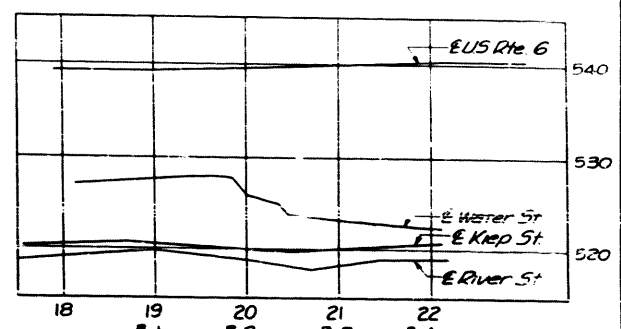
ELEVATION WEST-BOUND ROADWAY
Scale: 1"=50'



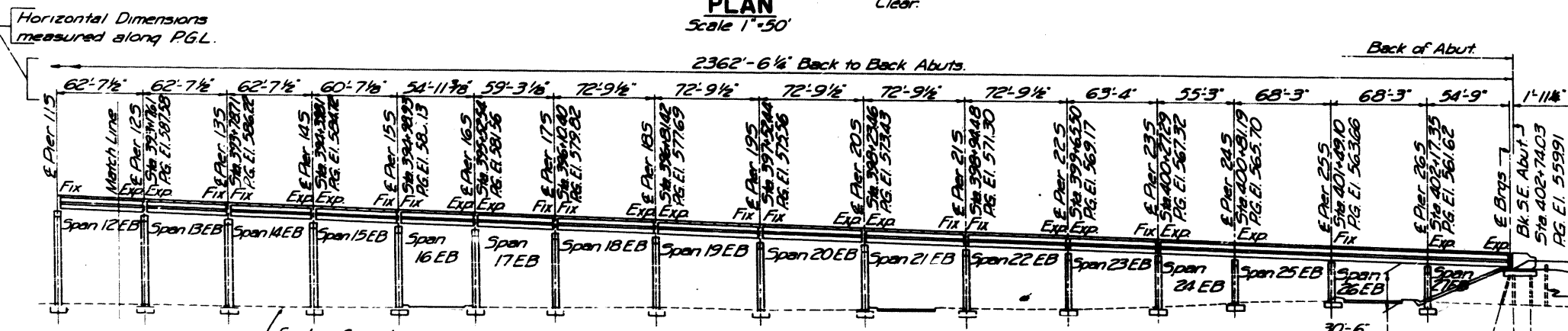
PLAN
Scale 1"=50'



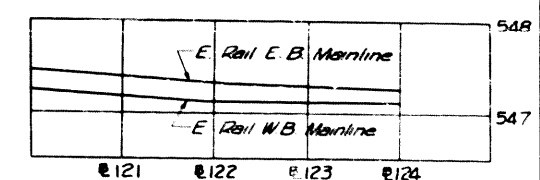
PROFILE GRADE FA.I-80
Scale: Horiz 1"=300'-Vert 1"=30'



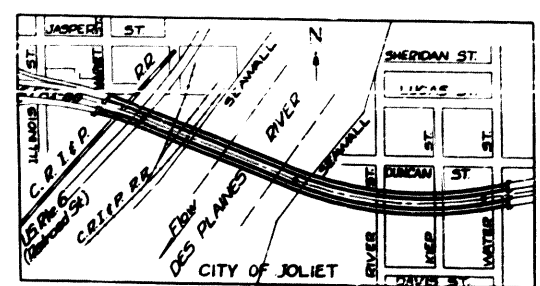
EXISTING PROFILE GRADES
U.S. ROUTE 6, RIVER ST, KIEP ST, & WATER ST.
Scale: Horiz 1"=100'-Vert 1"=10'



ELEVATION EAST-BOUND ROADWAY
Scale: 1"=50'



C.R.I.&P.R.R. MAINLINE PROFILE
Scale: Horiz 1"=100'-Vert 1"=1'



LOCATION PLAN
No Scale

DESIGNED	H.M.
CHECKED	J.T.L.
DRAWN	G.P. TLU
CHECKED	H.M. R.D.F.

Design Stresses
Reinforced Concrete
f_c = 3500 psi (Piers)
f_c = 1400 psi (except footings)
f_c = 1000 psi (Footings)
v_c = 90 psi (except footings)
v_c = 75 psi (Footings)
Design Loading
H20-S16-44 & Alternate

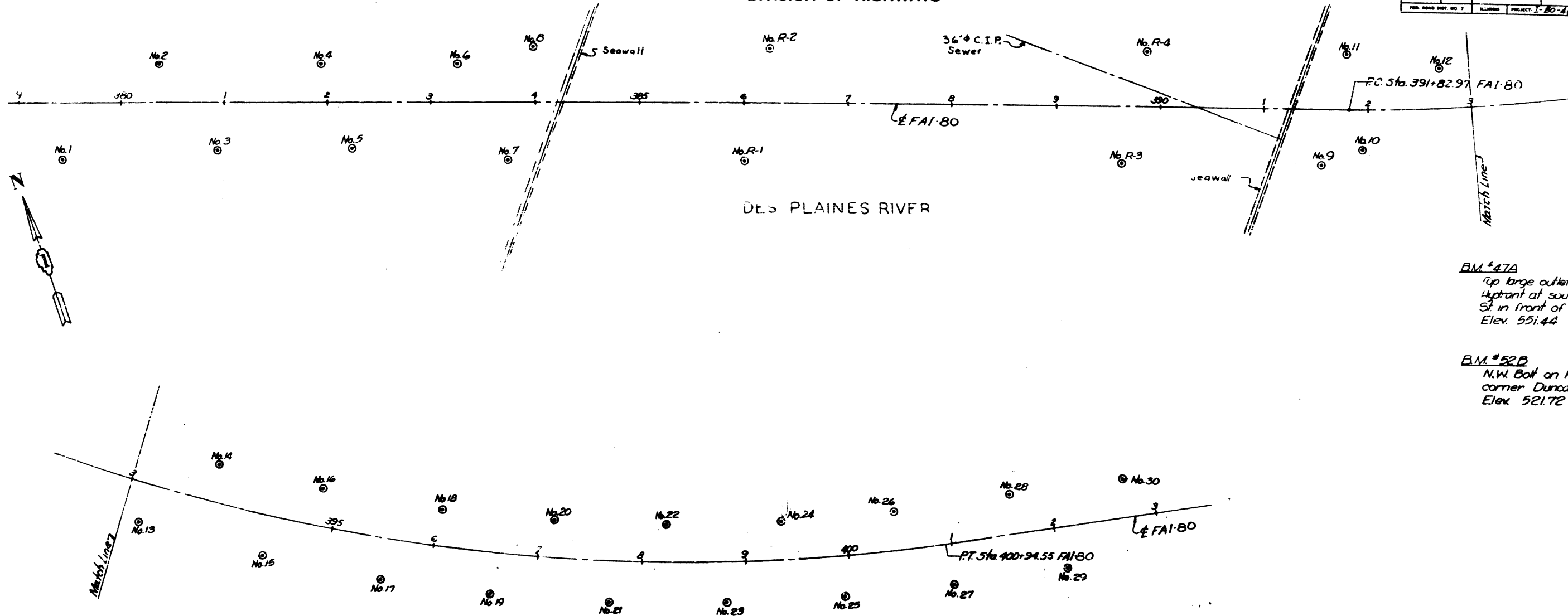
Note: For Design Foundation Pressures, see Pier Drawings.

Note
For Slopewall & Ditch Details and Sections, see Typical Sections, Sh. 2
For Name Plate Lettering, see Sh. 51

GENERAL PLAN & ELEVATION-2
TWIN BRIDGES OVER DES PLAINES RIVER
FA.I.-80 STA. 387+94.50
SECTION 99-38 PROJECT 1-80-4(13)133
SCALE As Noted WILL COUNTY
DATE Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	22
PROJECT I-80-4(13)133		SHEET NO. 3 42 SHEETS		



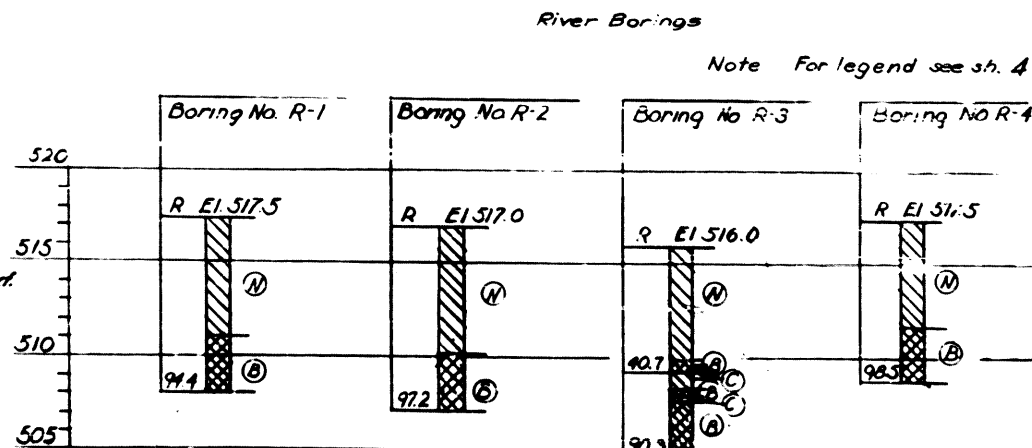
B.M. #47A
Top large outlet east side of Fire Hydrant at south end of Market St. in front of House #561
Elev. 551.44

B.M. #52B
N.W. Bolt on Fire Hydrant S.E. corner Duncan St & River St
Elev. 521.72

⊙ Indicates boring location

Boring No.	Station	Offset	Surf. Elev.	Boring No.	Station	Offset	Surf. Elev.
1	379+31.0	54' R	548.4	16	391+85.0	35' L	512.9
2	380+36.3	34' L	548.9	17	384+35.0	35' R	518.7
3	380+32.0	35' R	548.0	18	388+28.0	35' L	518.9
4	380+34.0	35' L	537.2	19	393+61.0	35' R	519.7
5	382+23.3	35' R	535.8	20	370+15.0	35' L	519.8
6	383+76.0	35' L	534.3	21	370+77.0	35' R	520.6
7	383+72.0	35' R	537.9	22	380+55.0	35' L	520.0
8	383+99.0	34' L	533.0	23	384+55.0	34' R	521.5
9	391+35.0	34' R	516.1	24	384+55.0	35' L	521.5
10	391+25.0	35' R	516.7	25	389+91.0	34' R	522.0
11	391+42.0	34' L	517.1	26	400+48.0	34' R	522.8
12	392+72.0	34' L	517.6	27	401+95.0	34' R	523.3
13	393+76.0	35' R	517.9	28	401+85.0	34' L	524.1
14	393+78.0	35' L	517.8	29	402+78.0	34' R	523.3
15	394+40.0	35' R	516.0	30	402+99.0	35' L	523.0
				R-1	386+00.0	34' R	529.0
				R-2	386+25.0	34' L	529.0
				R-3	386+50.0	34' R	529.0
				R-4	389+50.0	34' L	529.0

DESIGNED	H.C.M.
CHECKED	H.S.N.
DRAWN	F.A.
CHECKED	H.D.



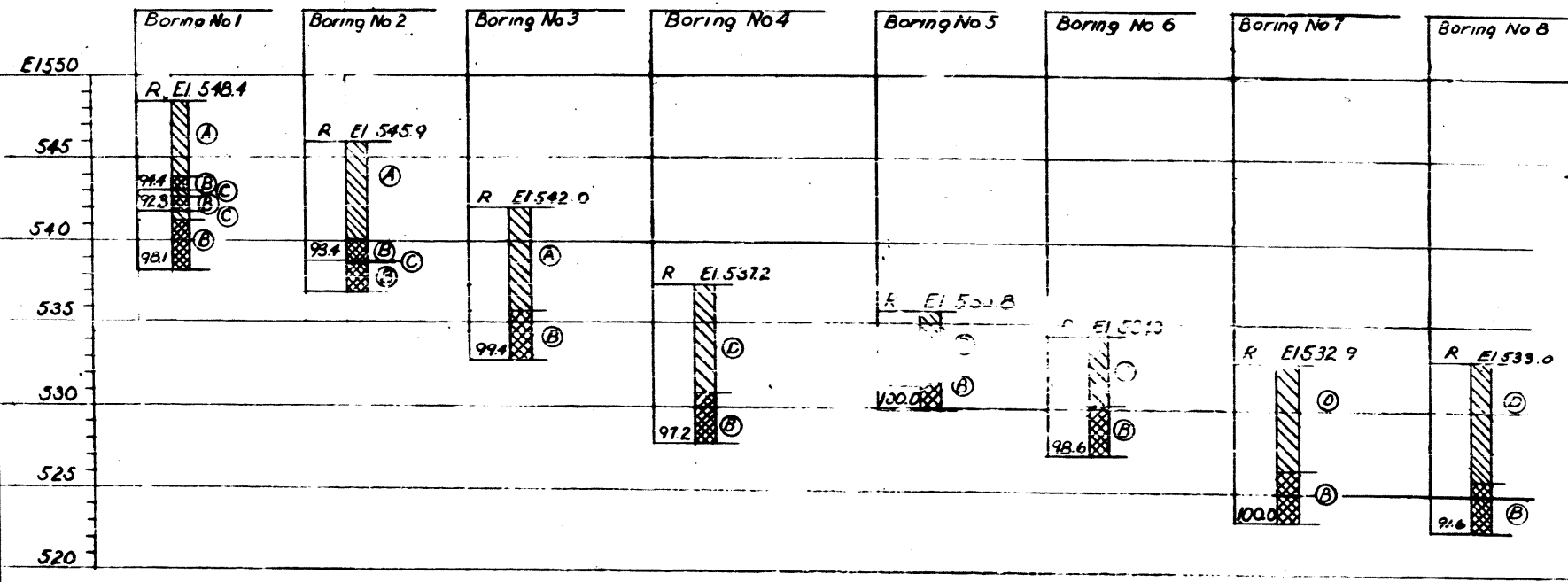
BORING LAYOUT & RIVER BORINGS
FAI-80 OVER DES PLAINES RIVER

STA 387 + 94 50
FAI ROUTE 80 SECTION 99-38
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

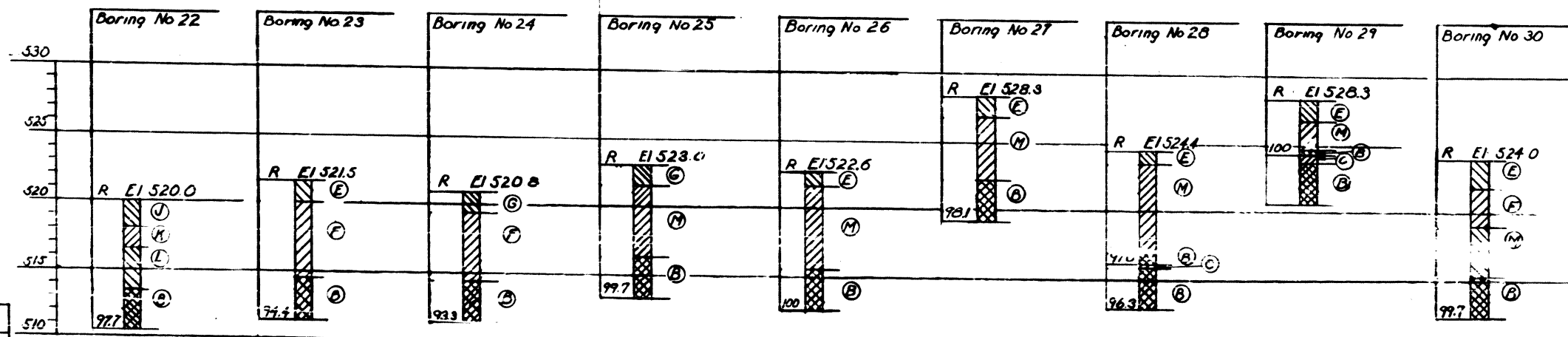
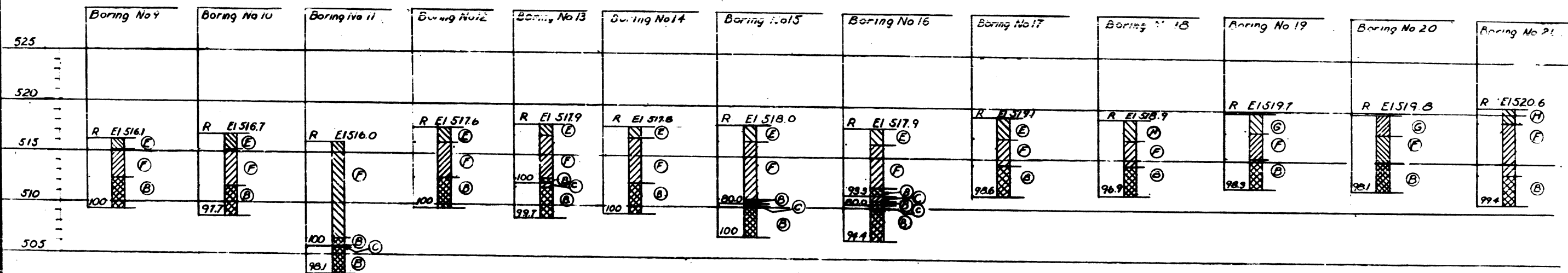
PROJECT I-80-4(13)133
WILL COUNTY
Date: Jan 25, 1961

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 4 42 SHEETS
FAI-80	99-38	WILL	71	23	
FBI ROAD DIST. DIV. 7		BLAUVELT	PROJECT: I-80-4(13)195		



Legend
R Rock Core % Recovered
Soil Code
A - Loose black top soil & large gravel (Moist)
B - Medium hard stratified lime stone, shown thus [hatched pattern]
C - Fine grain sand
D - Loose black under fill (Moist)
E - Black top soil (Moist)
F - Dense brown sand & gravel (Moist)
G - Black-brown topsoil
H - Stone fill
I - Cinder & stone fill
J - Black & brown clay (Moist)
K - Dense gray sand & gravel (Moist)
L - Very dense brown sand & gravel (Moist)
M - Sedimentary Sludge



Note:
For location of borings see sh. 3

DESIGNED	H.C.M.
CHECKED	H.S.N.
DRAWN	J.P.S.
CHECKED	H.C.M.

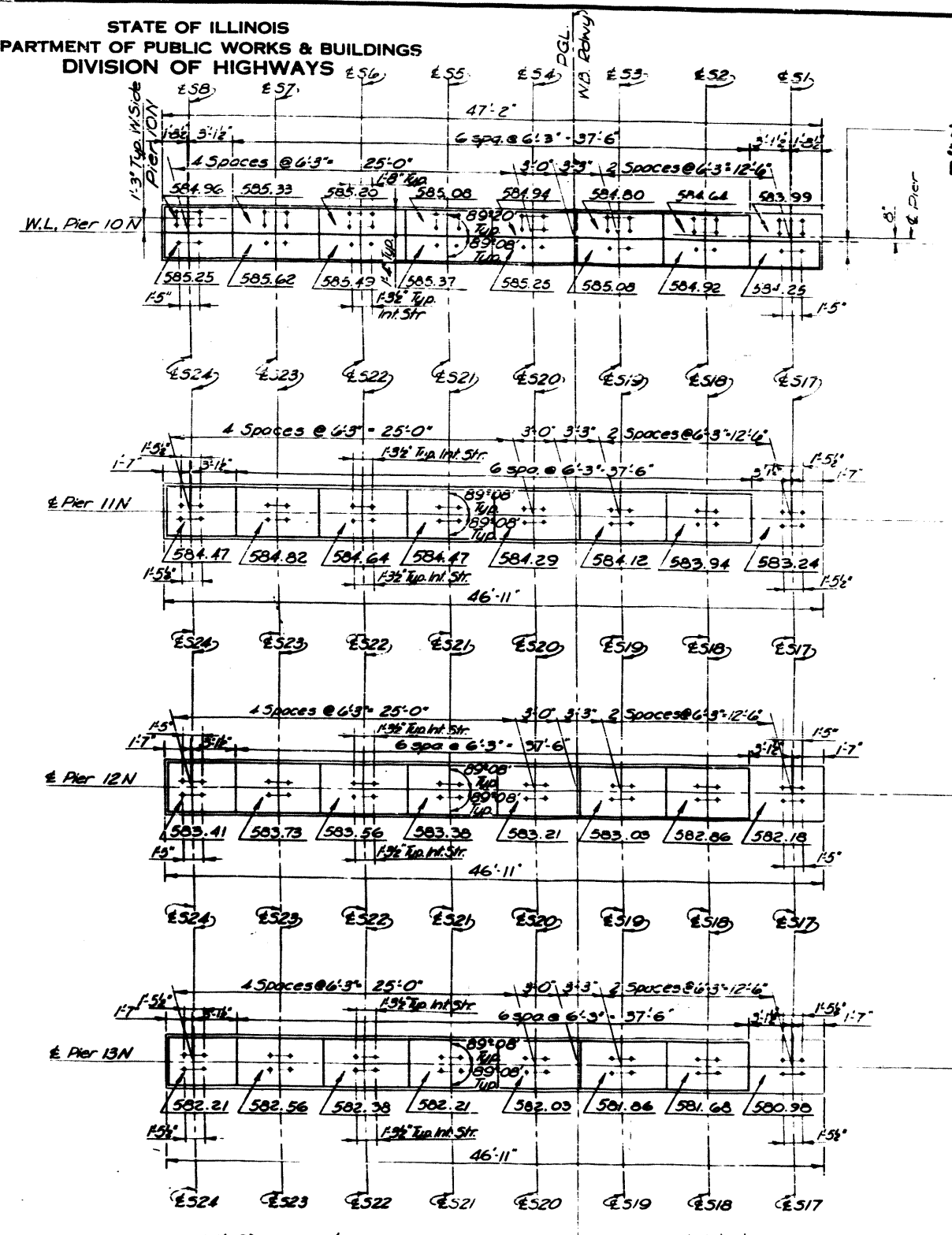
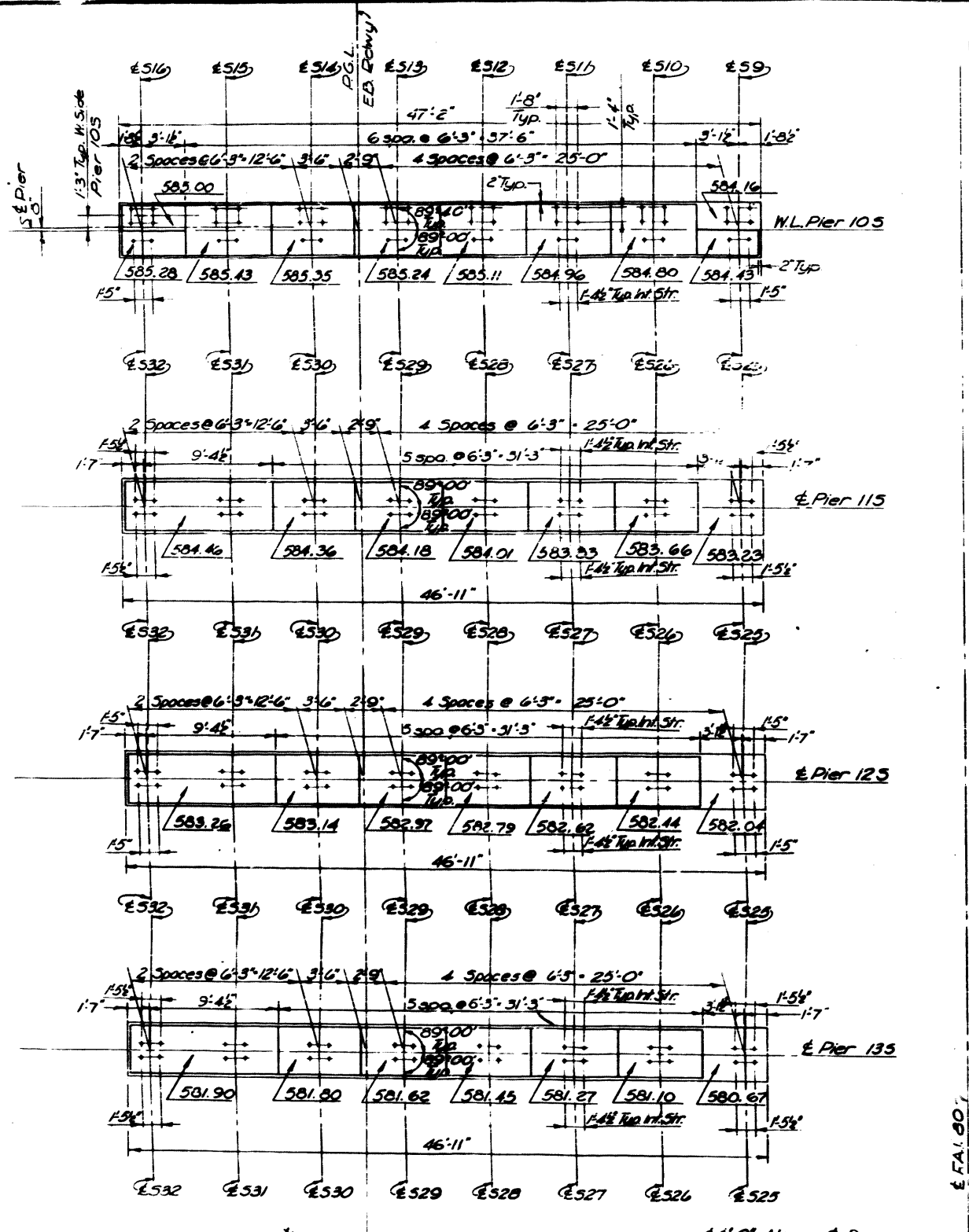
BORING LOGS
FAI-80 OVER DES PLAINES RIVER

STA 387 + 94.50
FAI ROUTE 80 SECTION 99-38 PROJECT I-80-4(13)195
WILL COUNTY Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

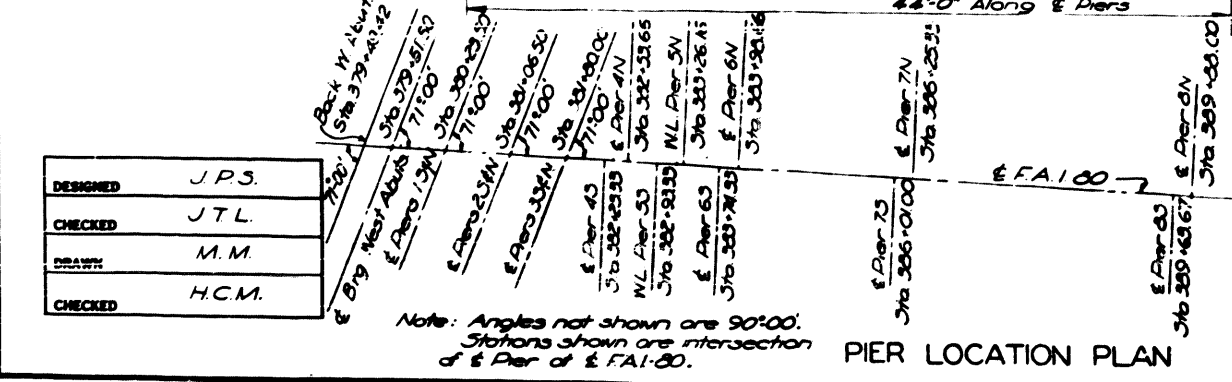
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	25
DESIGNED BY			DATE	1-25-1957

SHEET NO. 6
42 SHEETS



7' MEASURED ALONG & STRINGER TYPICAL, EXCEPT AS NOTED



DESIGNED	J.P.S.
CHECKED	J.T.L.
DRAWN	M.M.
CHECKED	H.C.M.

APPROACH SPANS - PIER KEY PLANS - 2
FAI-80 OVER DES PLAINES RIVER

STA 387 + 94.50

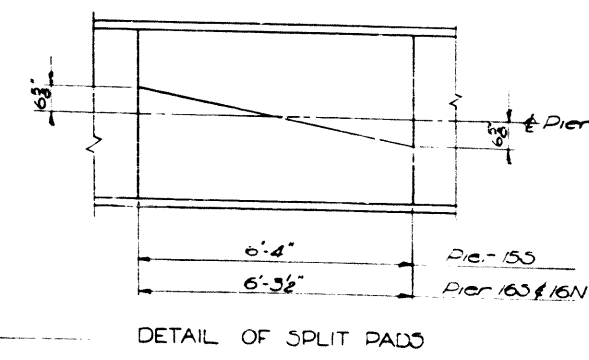
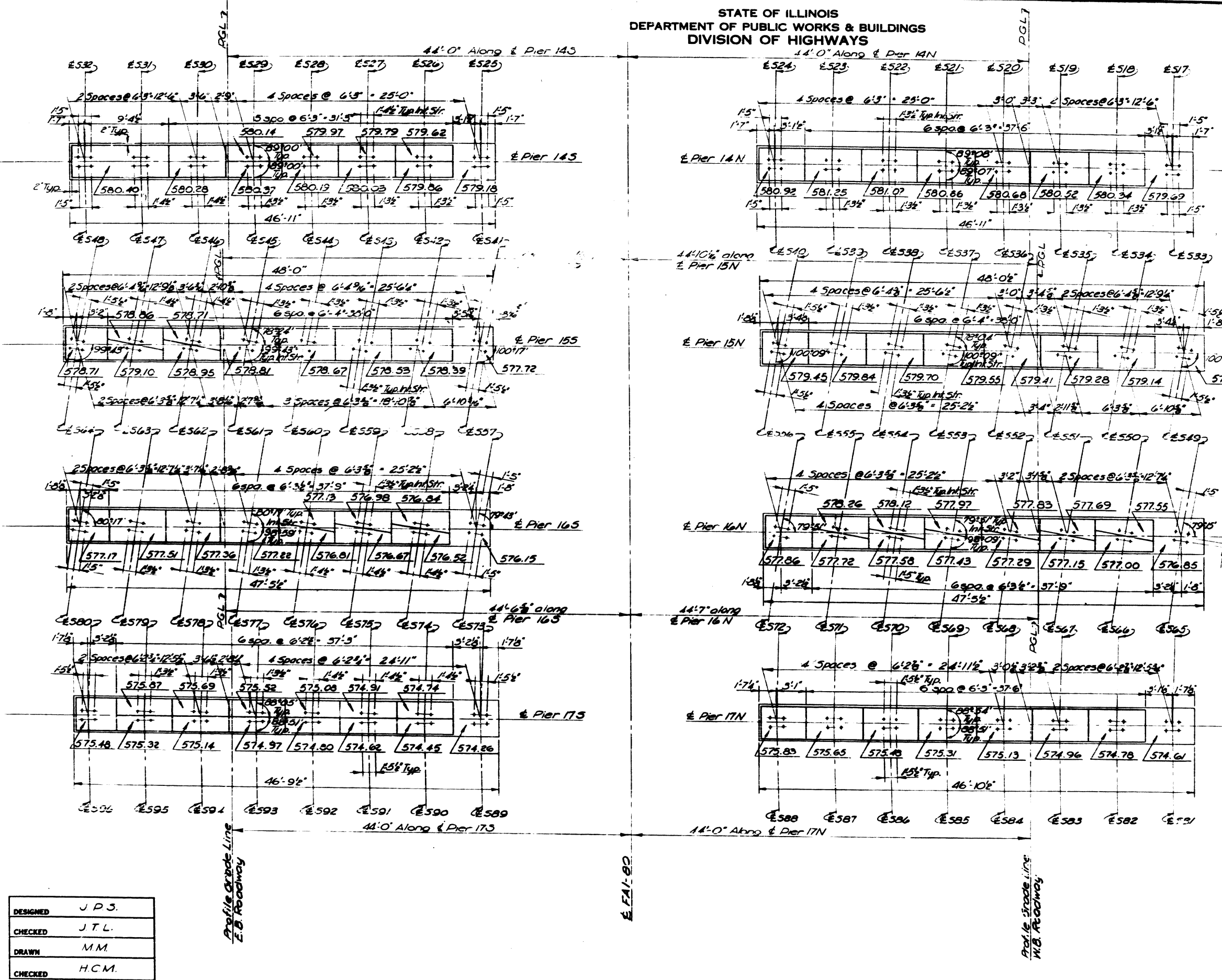
FAI ROUTE 80
SECTION 99-38
Scale: No Scale

PROJECT 3-00-40153
WILL COUNTY
Date: Jan. 25, 1957

BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N. J. NEW YORK, N. Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO	SECTION	COUNTY	TOTAL SHEETS	SHEET NO	SHEET NO. 7 47 SHEETS
FAI-80	99-38	WILL	71	26	
PROJECT: 1-80-473135					



7" MEASURED ALONG & STRINGER TYPICAL, EXCEPT AS NOTED

DESIGNED	J.P.S.
CHECKED	J.T.L.
DRAWN	M.M.
CHECKED	H.C.M.

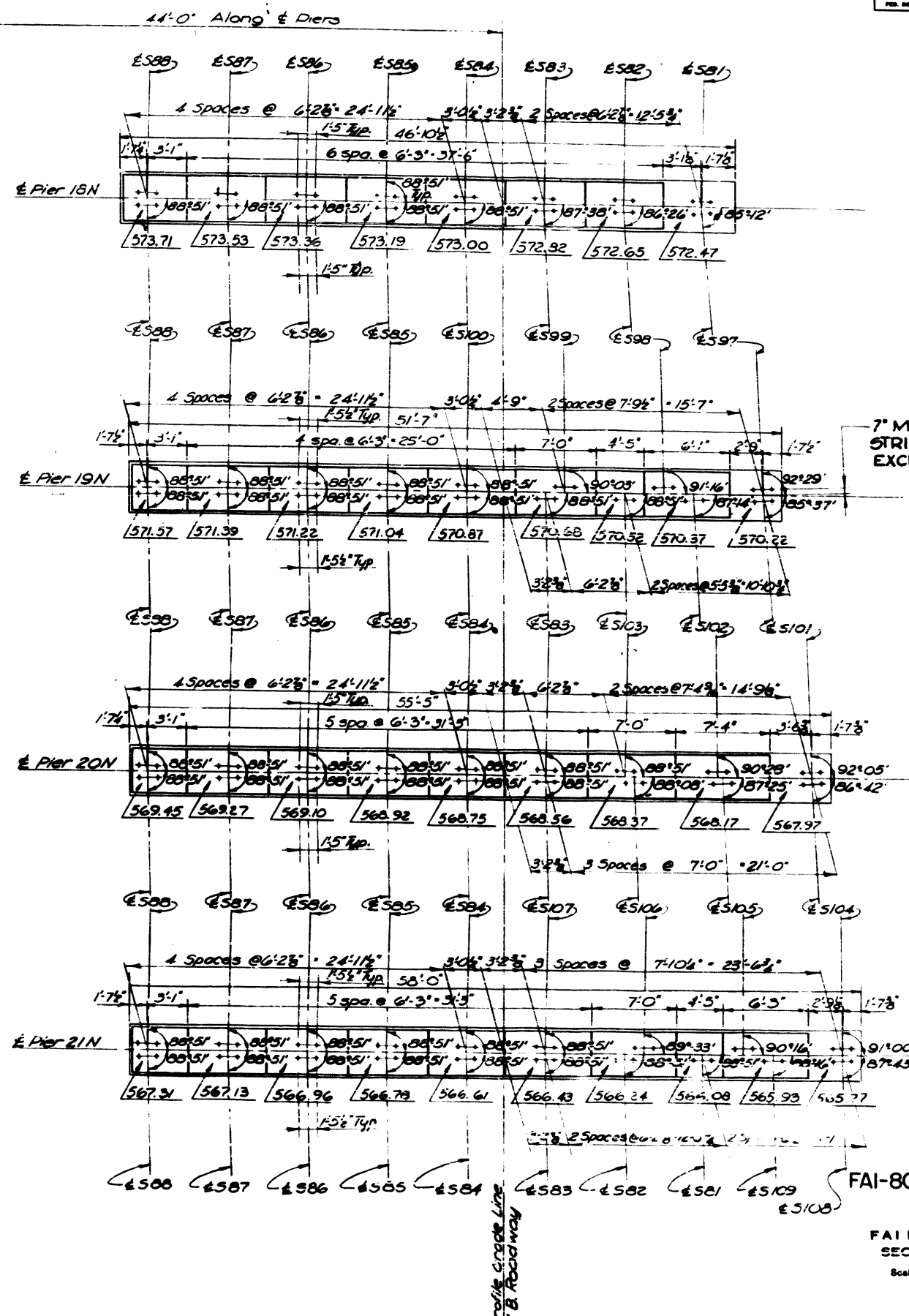
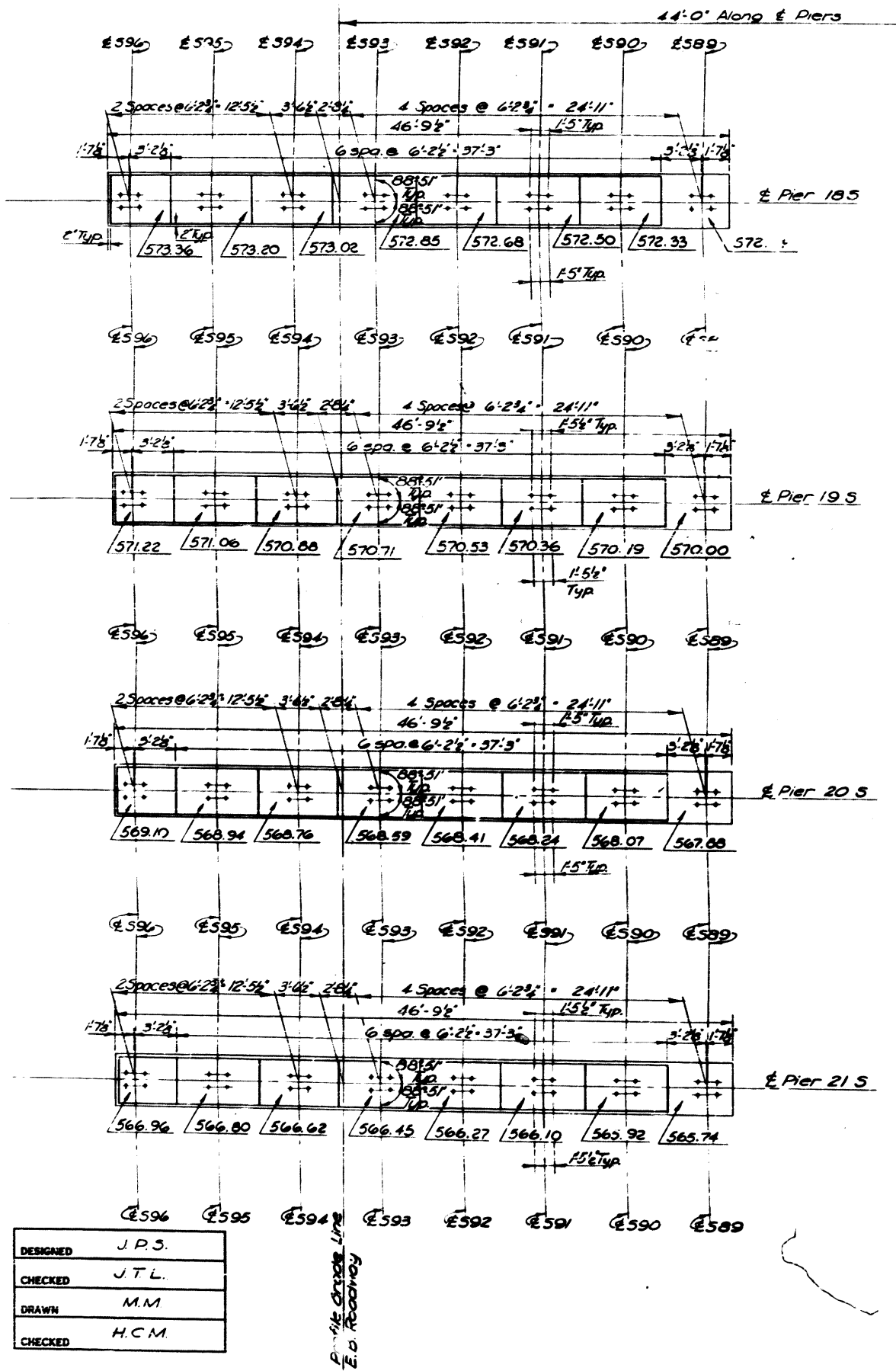
APPROACH SPANS - PIER KEY PLANS - 3
FAI-80 OVER DES PLAINES RIVER

STA 387 + 94.50
FAI ROUTE 80 SECTION 99-38 PROJECT 1-80-473135
WILL COUNTY
Scale: No Scale Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	27
PROJECT		I-80-4(13)133		

SHEET NO. 8
42 SHEETS



DESIGNED	J.P.S.
CHECKED	J.T.L.
DRAWN	M.M.
CHECKED	H.C.M.

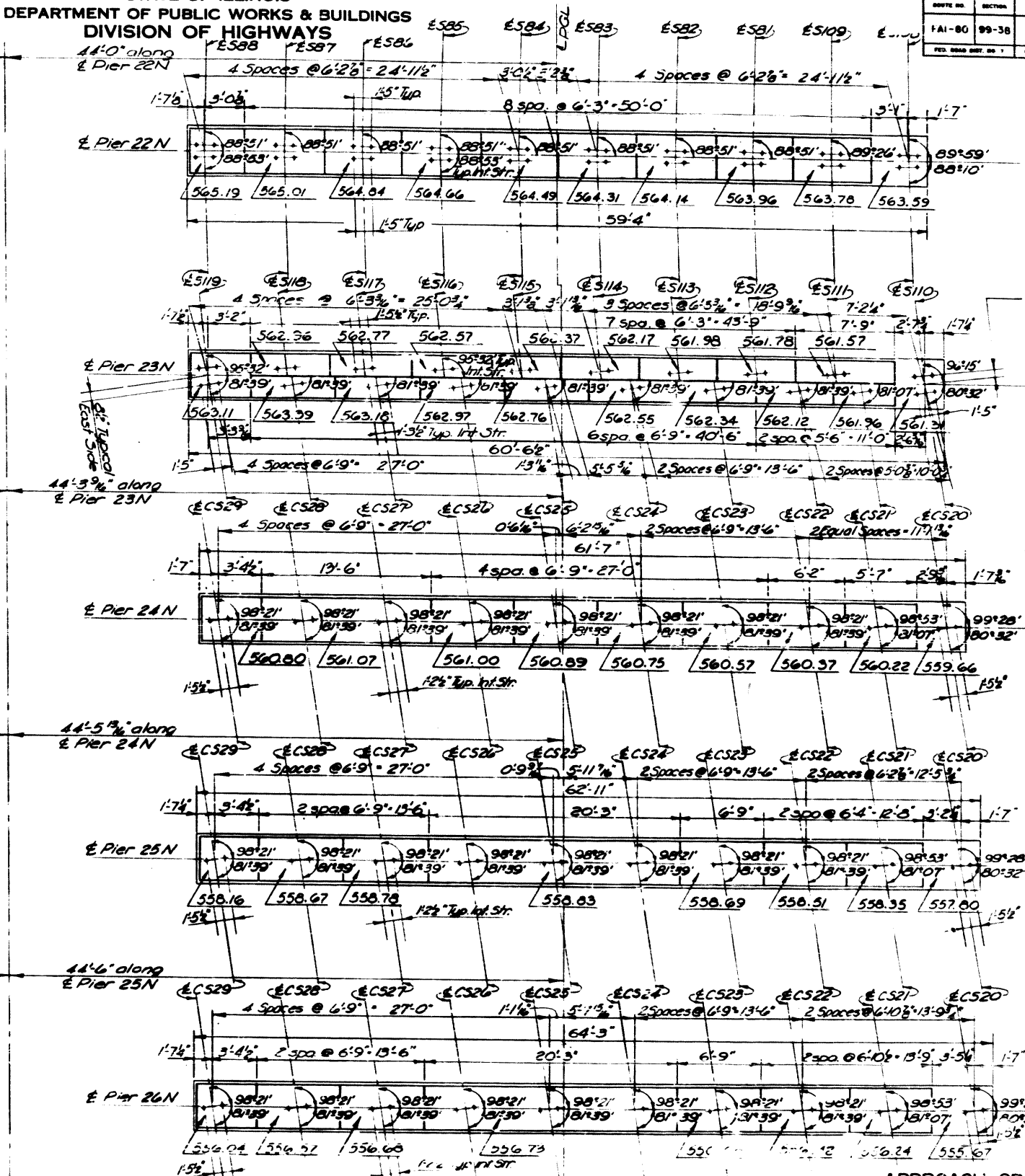
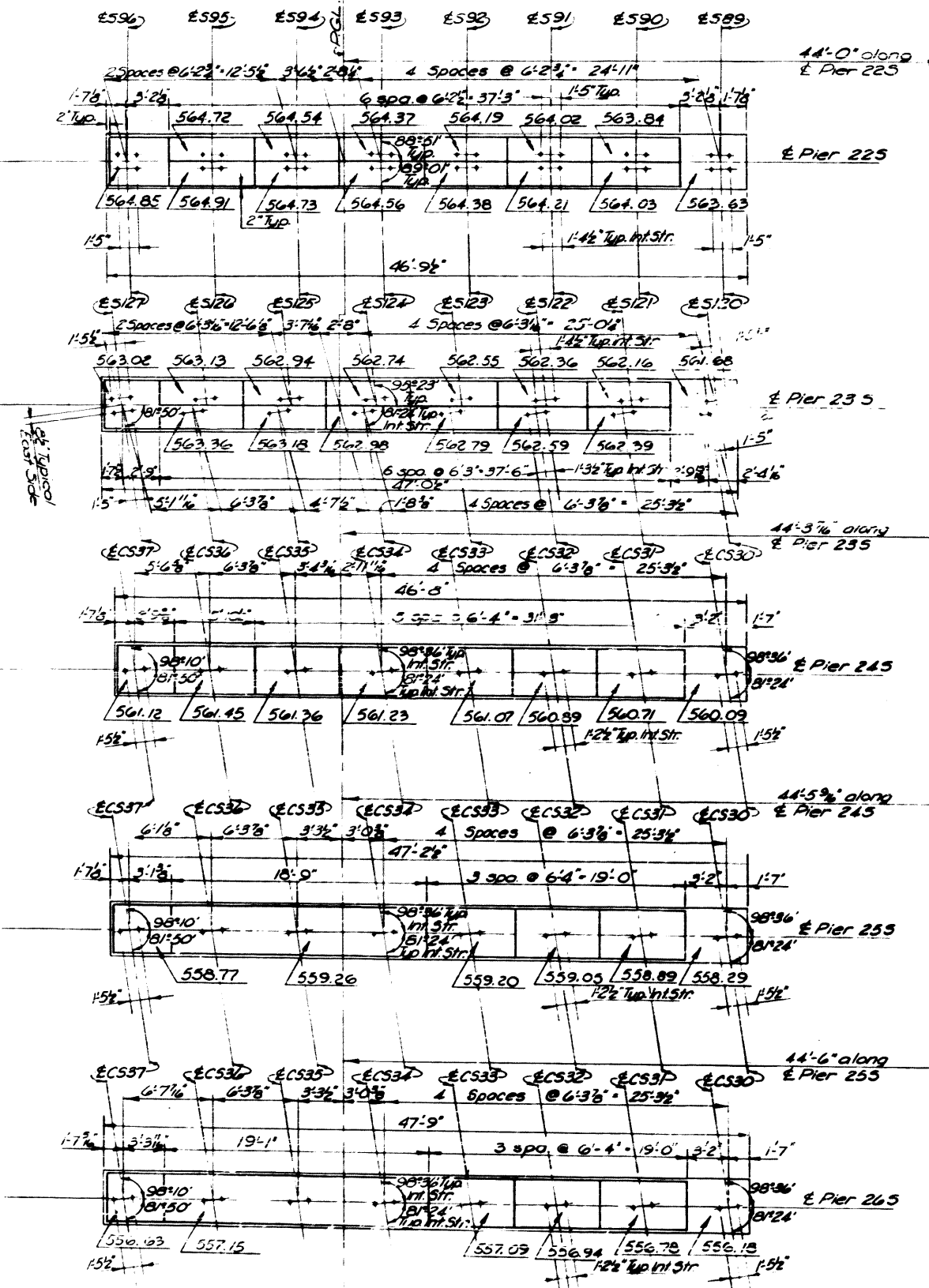
APPROACH SPANS
PIER KEY PLANS-4
FAI-80 OVER DES PLAINES RIVER
STA 387+94.50

FAI ROUTE 80
SECTION 99-38
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE ILL.

PROJECT I-80-4(13)133
WILL COUNTY
Date: Jan 25, 1961

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

DATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	28
PROJECT: I-80-4(13)33		SHEET NO. 9 42 SHEETS		



7" MEASURED ALONG & STRINGER TYPICAL, EXCEPT AS NOTED

DESIGNED	J.P.S.
CHECKED	J.T.L.
DRAWN	M.M.
CHECKED	H.C.M.

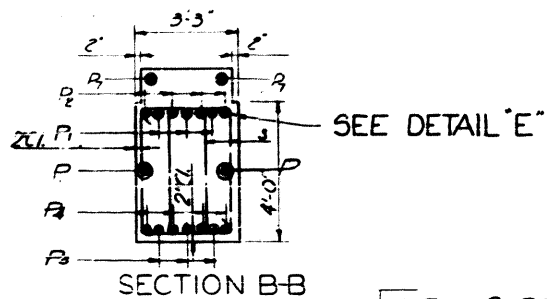
APPROACH SPANS - PIER KEY PLANS - 5
FAI-80 OVER DES PLAINES RIVER

STA 387+94.50

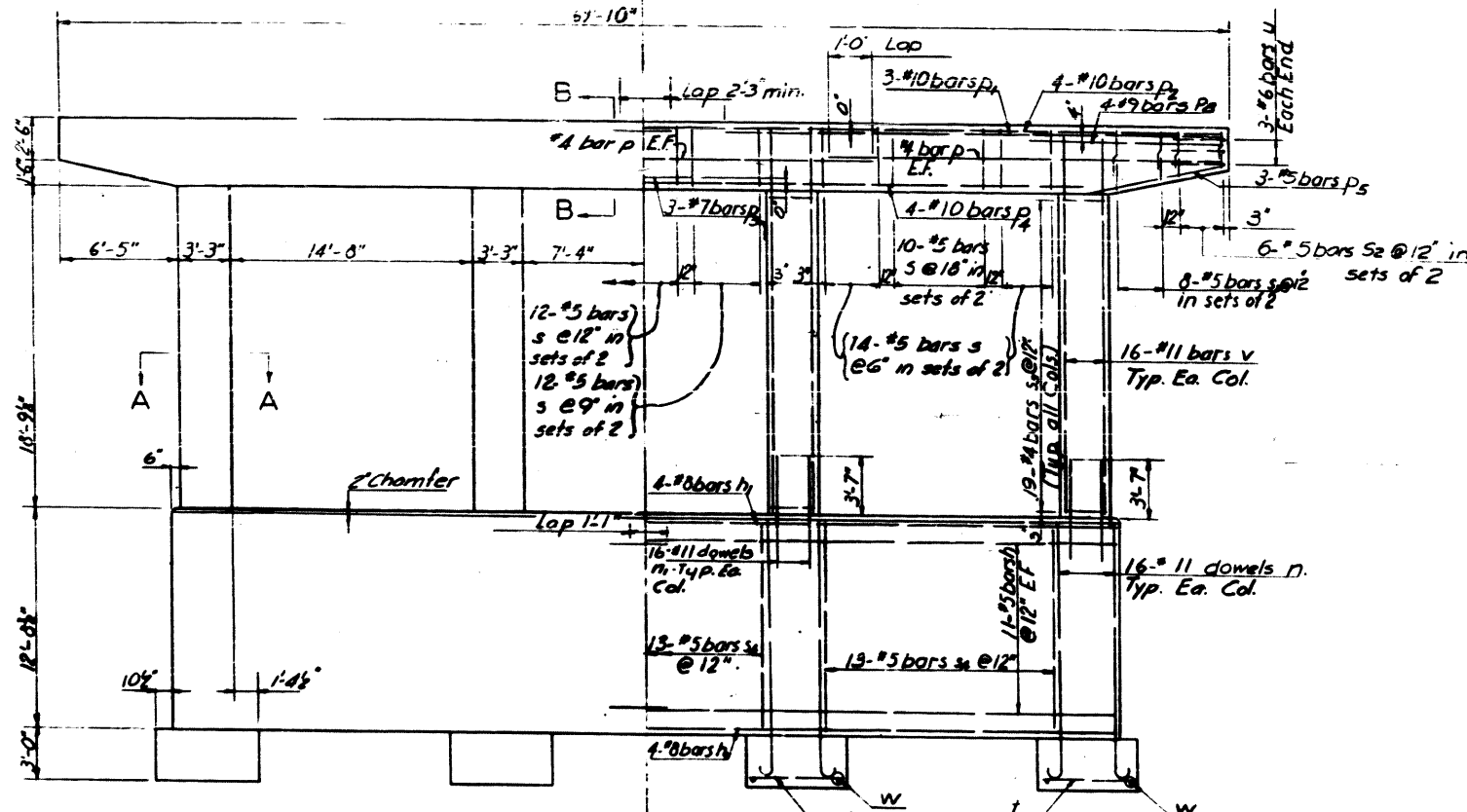
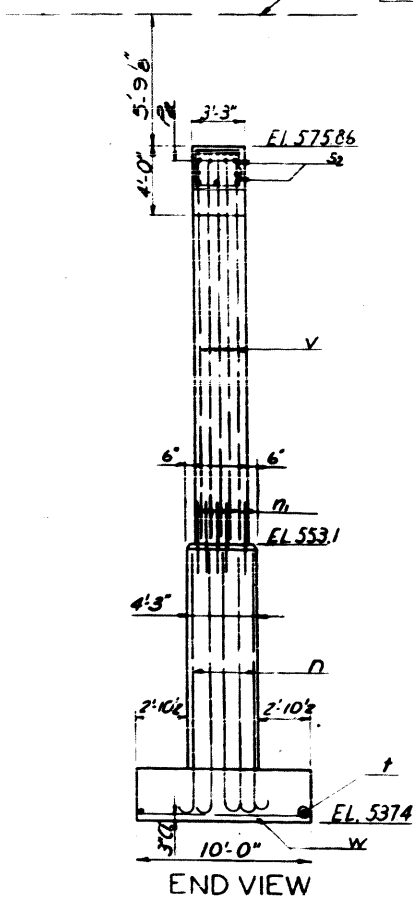
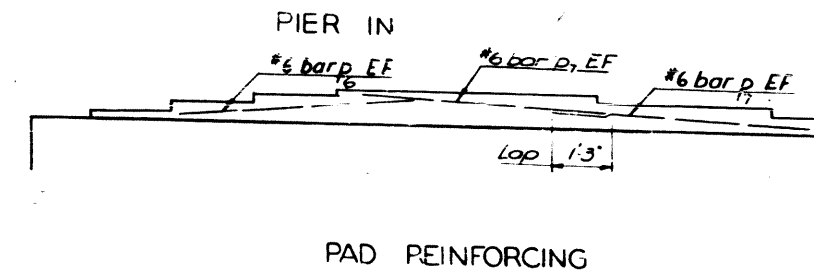
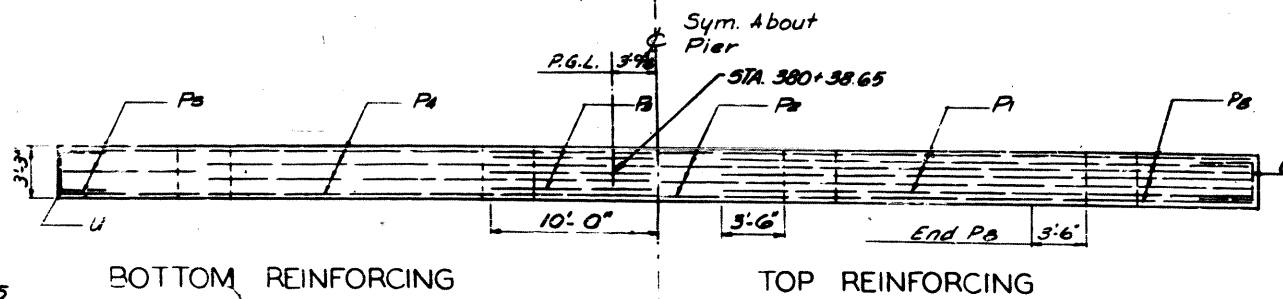
FAI ROUTE 80 PROJECT I-80-4(13)33
SECTION 99-38 WILL COUNTY
Scale: No Scale Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

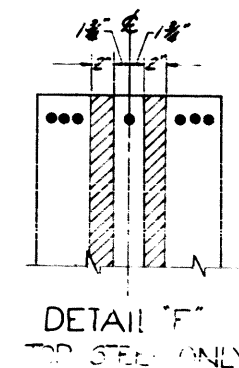
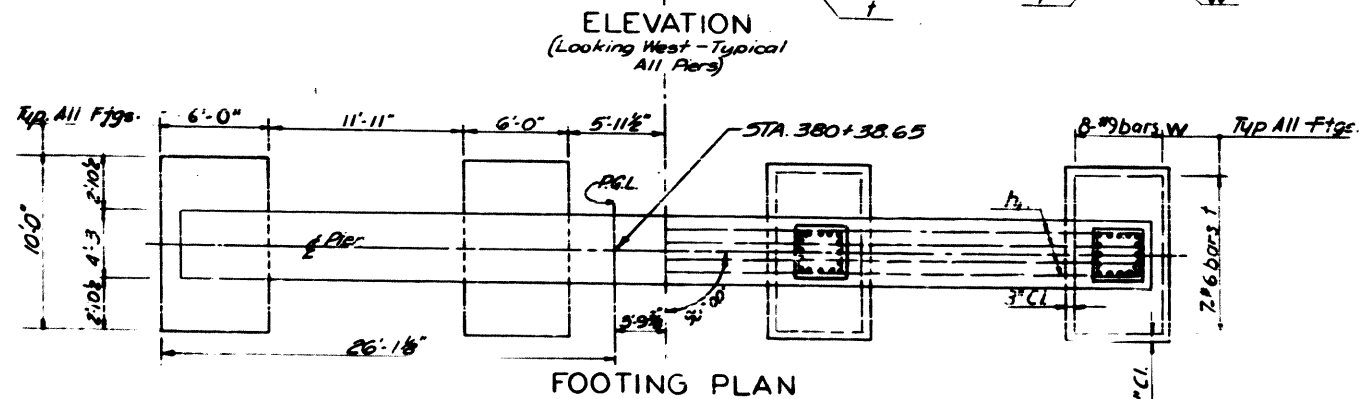
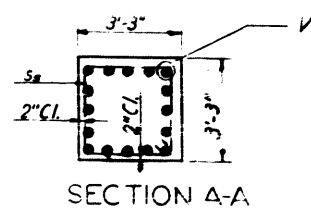
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	29
PROJECT: I-80-4(13)33			SHEET NO. 10	
			42 SHEETS	



4 Pier @ P.G.L.
Sta. 380+38.65
P.G. Pier. 581.62



Notes:
For Bill of Materials see sh 60
For Pad Elevations and Dimensions, see Key Plan, sh. 24
For Bar List see sh. 53
For Location Plan, see sh. 25
Maximum Foundation Pressure = 5 1/2



Notes:
Keep reinforcement clear of shaded area.
Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.

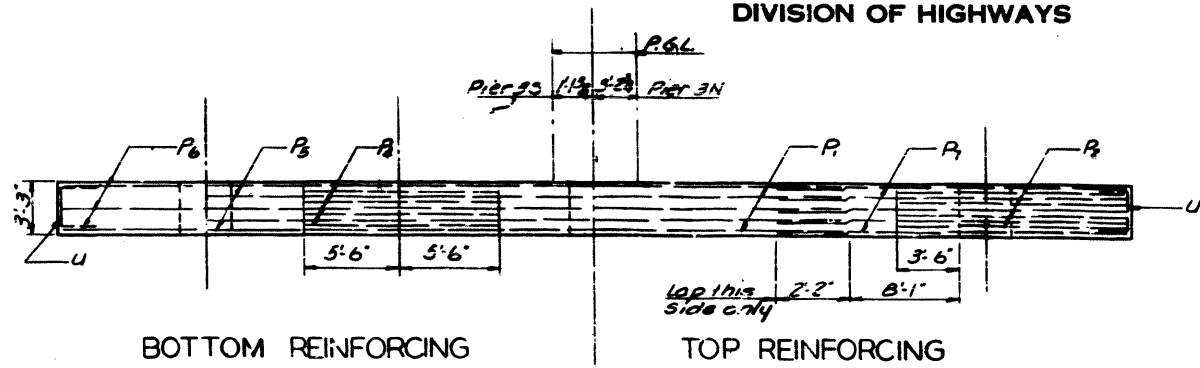
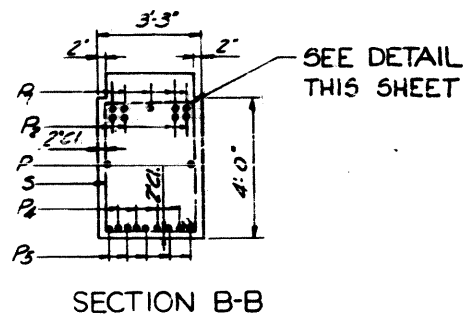
DESIGNED	RH
CHECKED	JTL
DRAWN	PvdM R.S.
CHECKED	HCM IG

PIER IN
FAI-80 OVER DES PLAINES RIVER
STA. 387+94.50

FAI ROUTE 80 PROJECT I-80-4(13)33
SECTION 99-38 WILL COUNTY
Scale: No Scale Date: Jan. 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

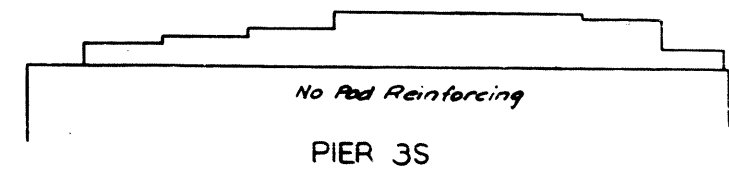
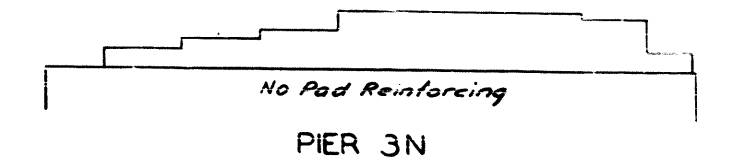
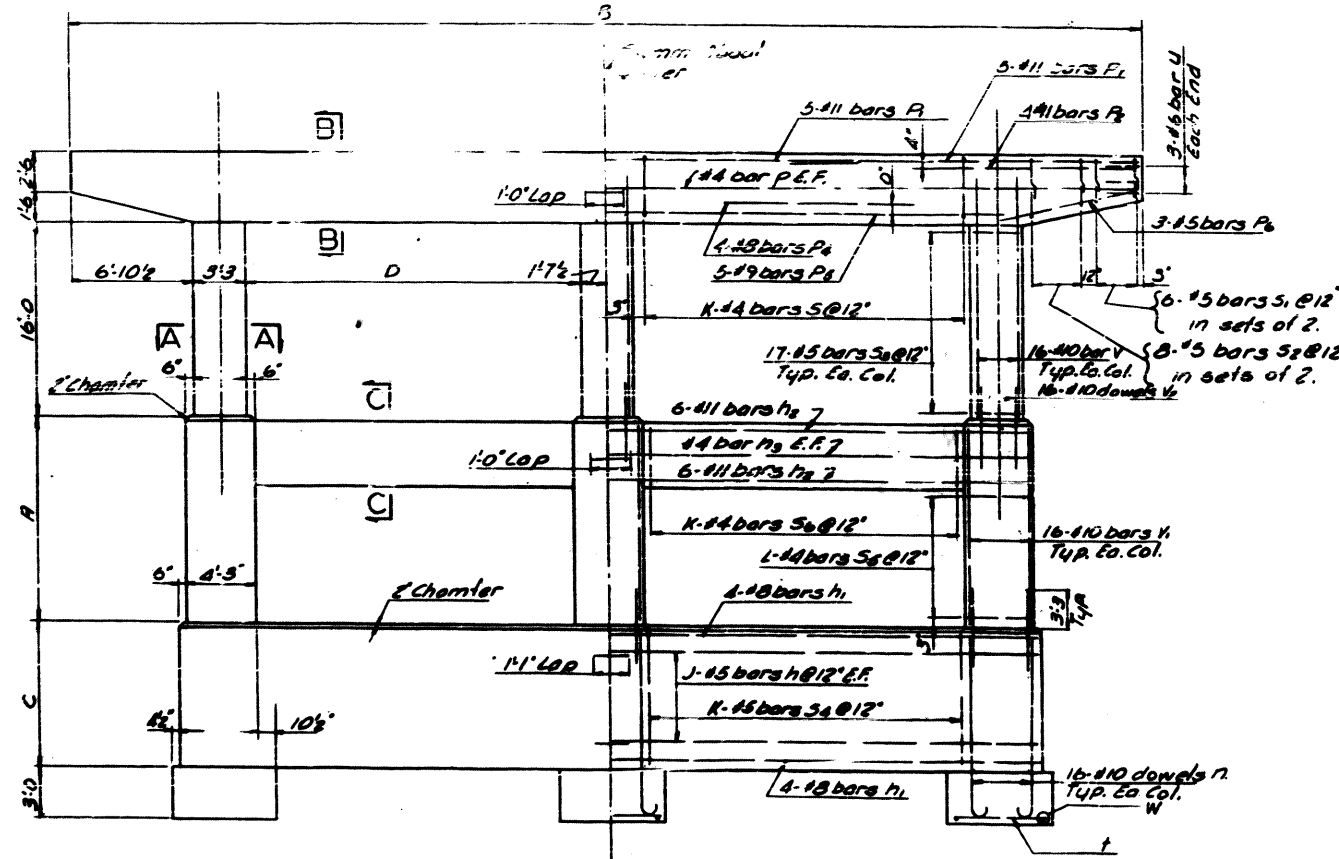
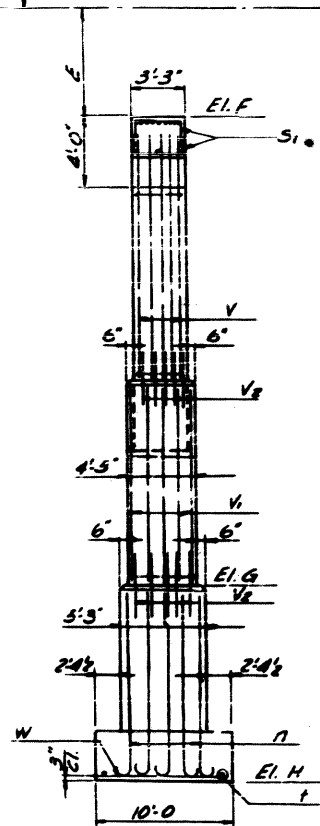
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

DESIGN NO.	SECTION	QUALITY	TOTAL SHEETS	SHEET NO.
F.A.I.-80	99-38	WILL	71	31
PROJECT: 80-4(13)133		SHEET NO. 12		
DATE: Jan 25, 1961		42 SHEETS		

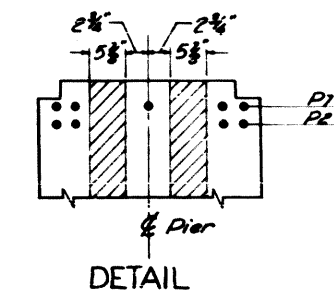


Pier No.	Dimensions					Elevations			No. of Bars			Station & Pier @ P.G.L.	P.G.L. Elev.
	A	B	C	D	E	F	G	H	J	K	L		
3N	18'-7 1/2"	56'-3 1/2"	9'-2 1/2"	16'-4 1/2"	5'-3 1/2"	580.41	541.80	529.60	8	17	15	381+95.15	585.87
3S	18'-5"	60'-6"	5'-2 3/4"	18'-6"	5'-4 1/4"	579.72	541.30	533.10	4	19	15	381+64.85	585.12

P.G. Elev. of Pier @ P.G.L. } See Table

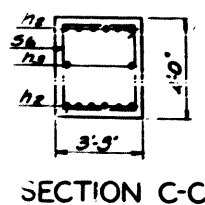
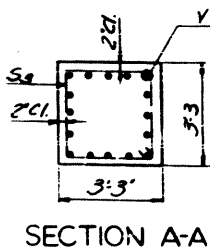
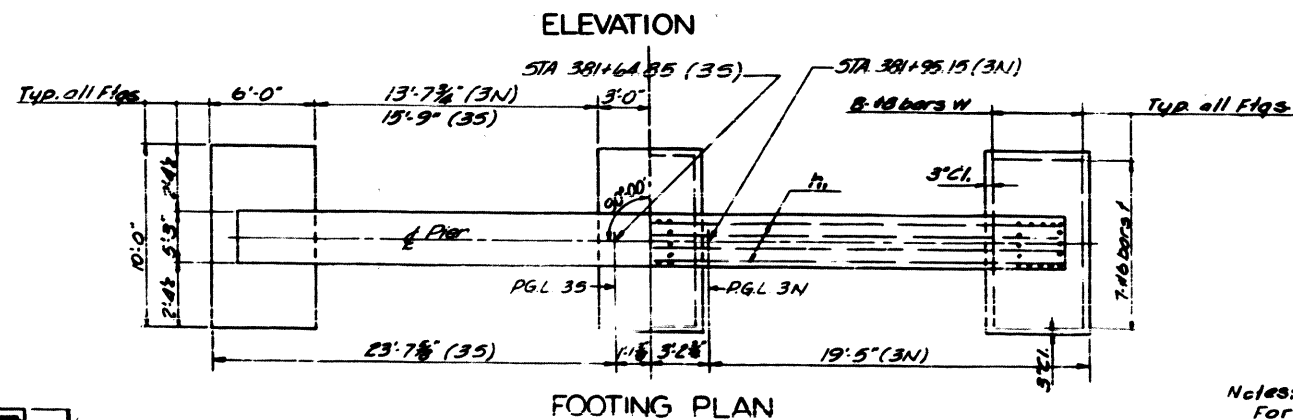


PAD ARRANGEMENT



Note:
Keep reinforcement clear of shaded area.

Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.



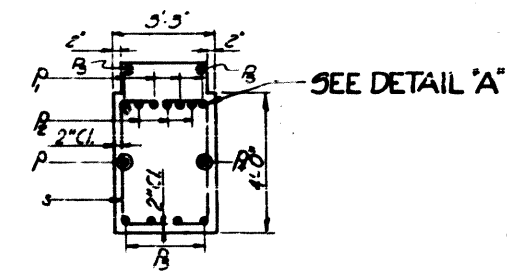
DESIGNED	R.H.
CHECKED	H.C.M.
DRAWN	R.S.
CHECKED	I.G.

Notes:
For Bill of Materials, see sh. 60
For Pad Elevations and Dimensions, see Plans, sh. 24
For Bar Lists, see sh. 53
For Location Plan, see sh. 25
Maximum Foundation Pressure - 37/ks

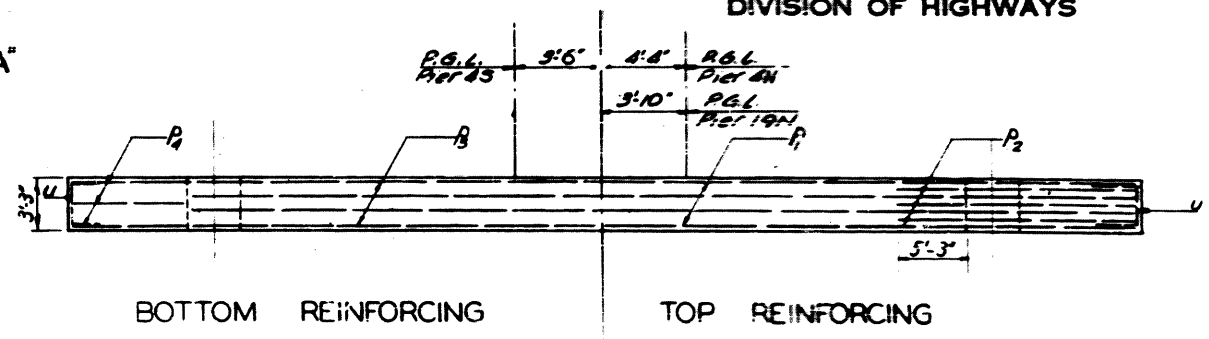
PIERS 3N/S
FAI-80 OVER DES PLAINES RIVER
STA 387+94.50
F A I ROUTE 80
SECTION 99-38
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N. J. NEW YORK, N. Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

DATE	DESIGN	CHECKED	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	32
PROJECT: I-80-4(13)33		SHEET NO. 13		
		42 SHEETS		

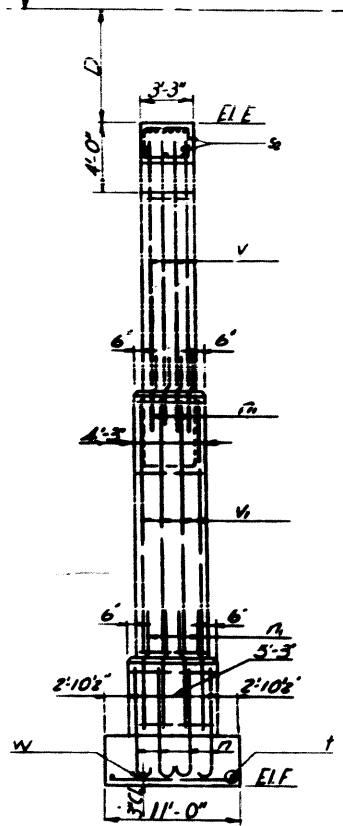


SECTION B-B

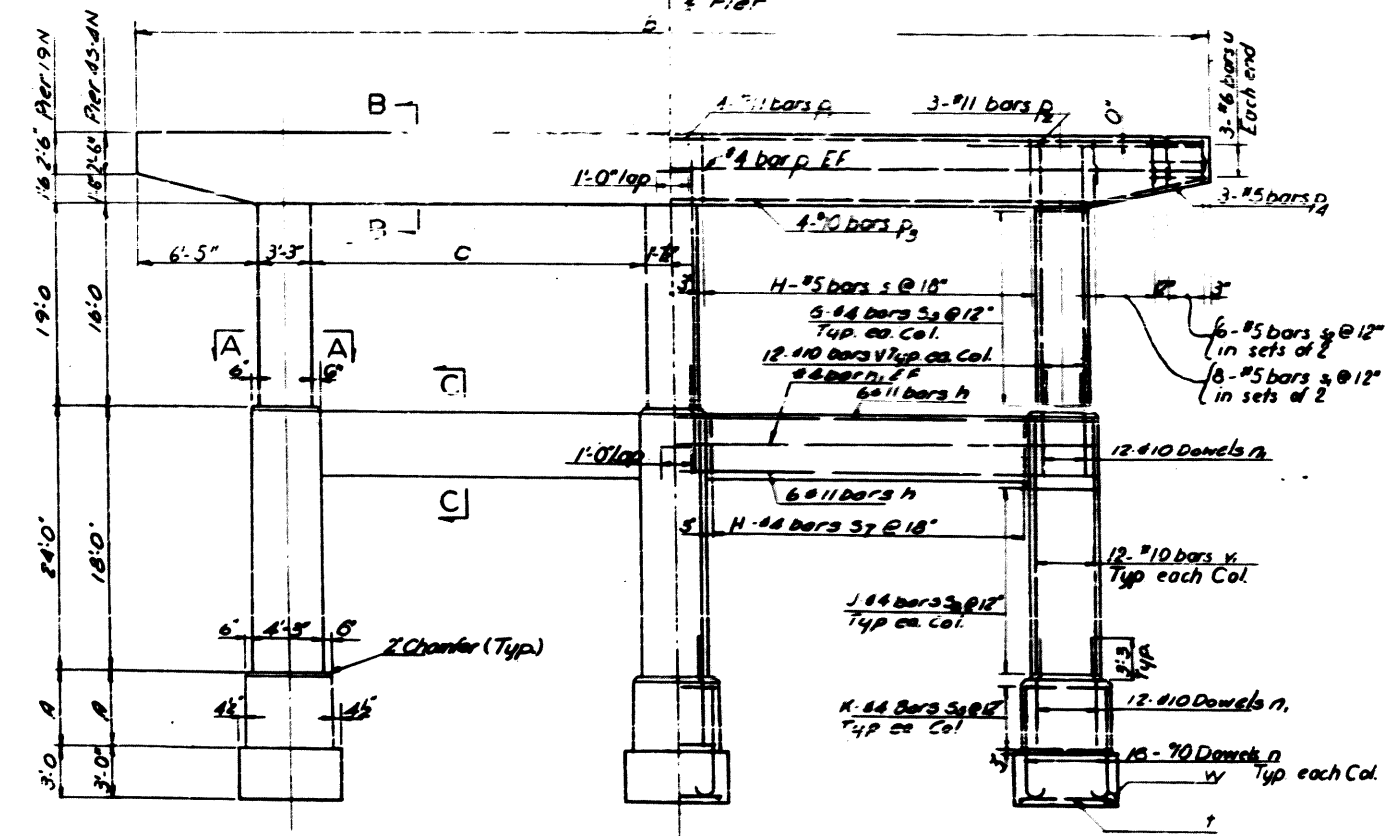


BOTTOM REINFORCING TOP REINFORCING

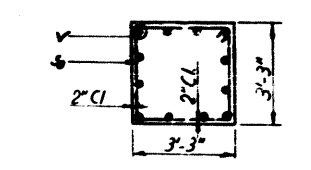
P.G. Elev. of Pier @ P.G.L. see table



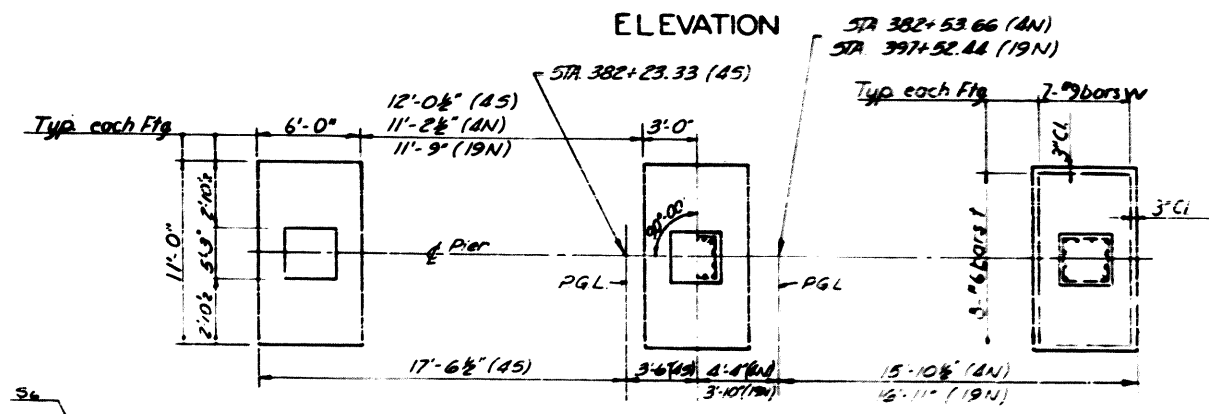
END VIEW



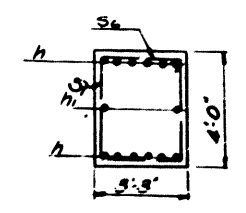
ELEVATION



SECTION A-A



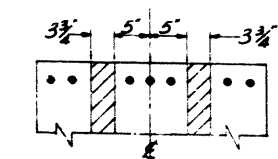
FOOTING PLAN



SECTION C-C

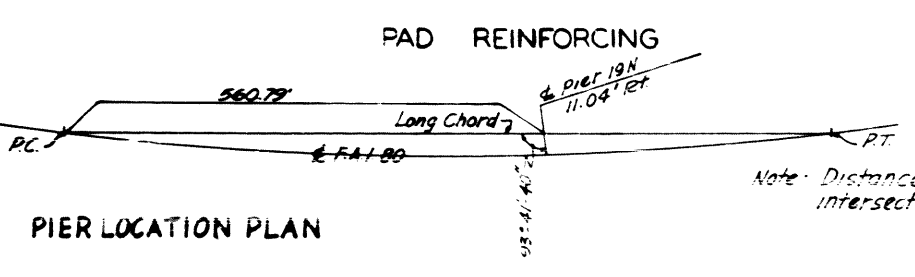
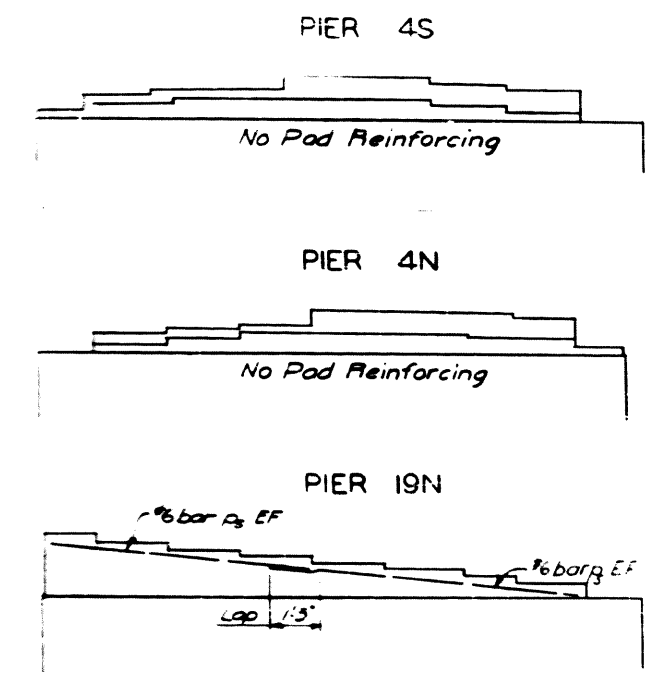
Pier No.	Station @ P.G.L.	P.G.L. El.
A	8'-8 1/2	11'-8
B	52'-2	50'-6
C	14'-9 1/2	13'-11 1/2
D	5'-2 1/2	5'-2 1/2
E	58'-29	58'-97
F	53'-6	52'-93
G	17	17
H	10	9
J	15	13
K	9	12

Pier No.	Station @ P.G.L.	P.G.L. El.
4S	382+23.33	586.53
4N	382+53.66	587.20
19N	397+52.44	575.56



DETAIL A TOP STEEL ONLY

Note: Keep reinforcement clear of shaded area. Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.



PIER LOCATION PLAN

PIERS 4N/S & 19N
FAI-80 OVER DES PLAINES RIVER

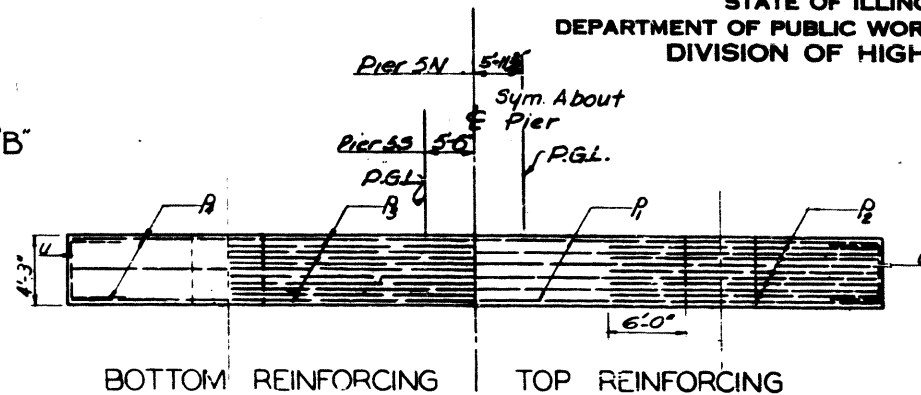
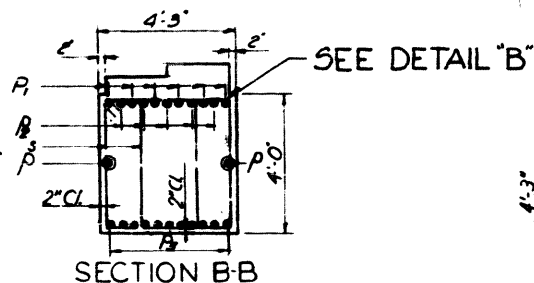
STA 387+94.50
FAI ROUTE 80
SECTION 99-38
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

Note: For Bill of Materials, See Sht. 60. For Pad Elevations and Dimensions see Key Plans, Sht. 1A & 27. For Bar Lists See Sht. 53 & 57. For Location Plan, see Sht. 25. Maximum Foundation Pressure: 2.5'.

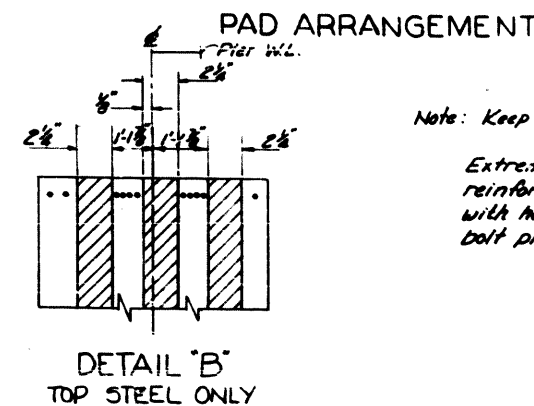
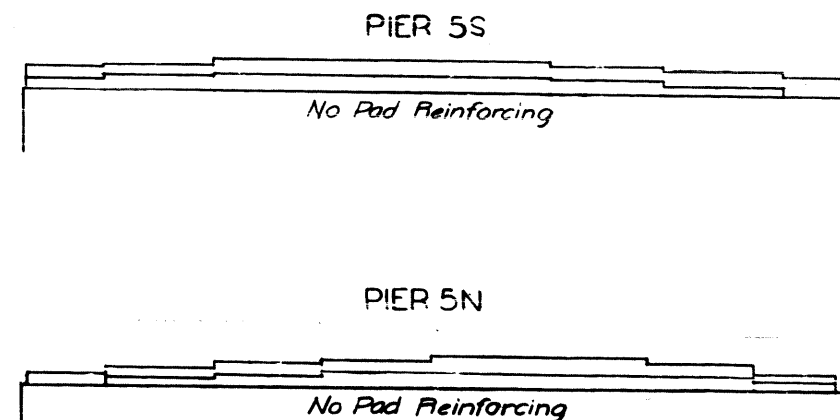
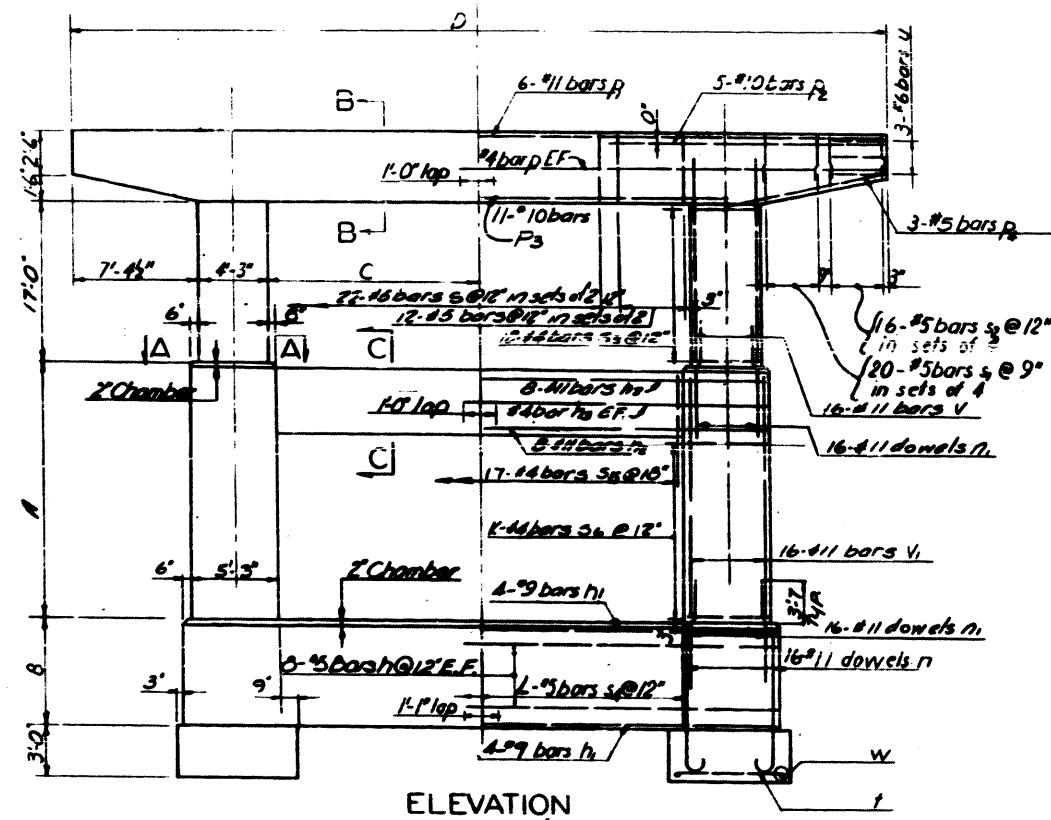
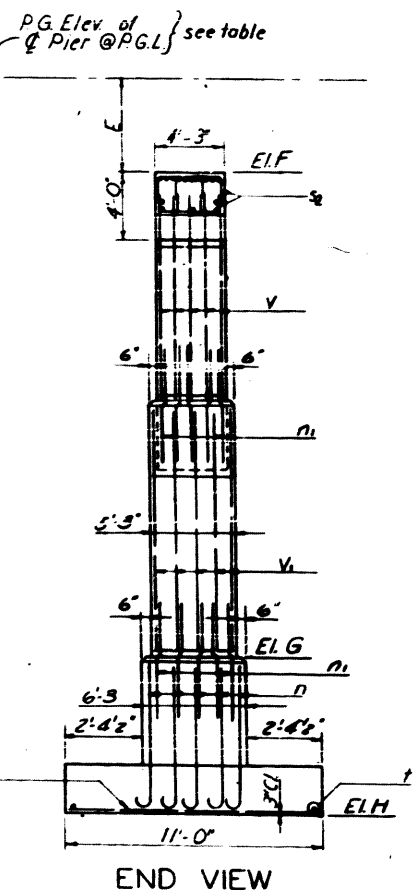
DESIGNED	R.H.
CHECKED	H.C.M.
DRAWN	P.v.d.M. R.S.
CHECKED	H.C.M. I.G.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

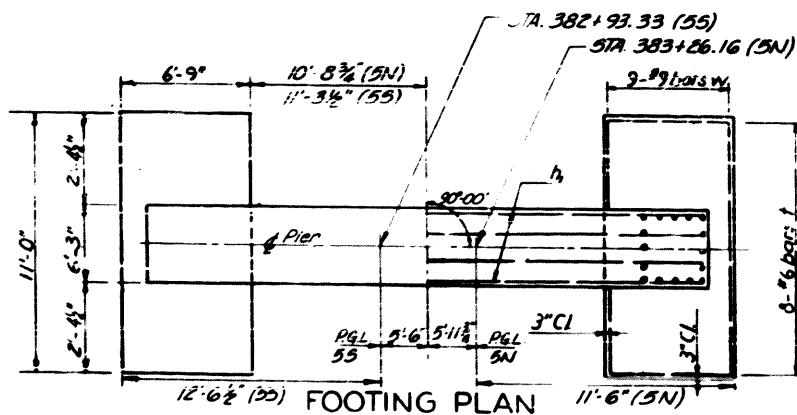
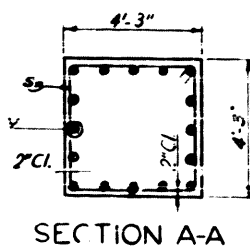
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 14
FAI-80	99-30	WILL	71	33	42 SHEETS
FED. ROAD DIST. NO. 7		PLANS	PROJECT: 1-80-4(12)33		



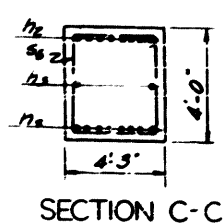
	Dimensions					Elevations			Bars	Station	P.G.L.	Elev.
	A	B	C	D	E	F	G	H				
Pier No 5N	21'-2"	9'-0"	11'-11 1/2"	47'-2 1/2"	5'-5 1/2"	583.17	541.0	529.0	18-24	383+26.16	588.66	
Pier No 5S	20'-5 1/2"	9'-3 1/2"	12'-6 1/2"	48'-4"	5'-6 1/2"	582.49	541.0	528.7	17-24	382+93.33	588.03	



Note: Keep reinforcement clear of shaded area.
Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.



DESIGNED	R.H.
CHECKED	H.C.M.
DRAWN	F.v.d.M. RS
CHECKED	H.C.M. IG



Notes:
For Bill of Materials See Sht. 60
For Pad Elevations and Dimensions, see Key Plans, Sht. 24
For Bar Lists, See Sht. 54
For Location Plan, see Sht. 25
Maximum Foundation Pressure: 5 1/2 t.

PIERS 5 N/S
FAI-80 OVER DES PLAINES RIVER

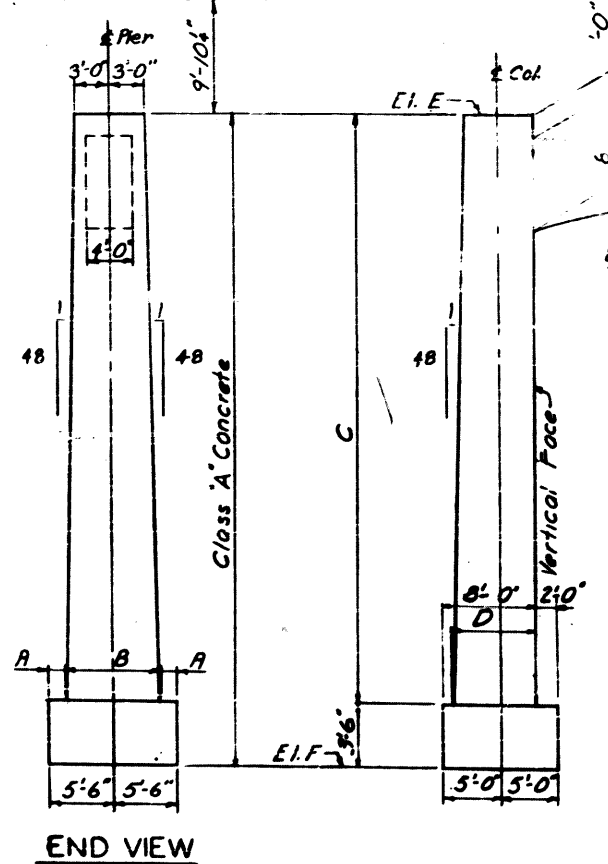
STA 387 + 94.50
FAI ROUTE 80 SECTION 99-30 PROJECT 1-80-4(12)33
Scale: No Scale WILL COUNTY Date: Jan. 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

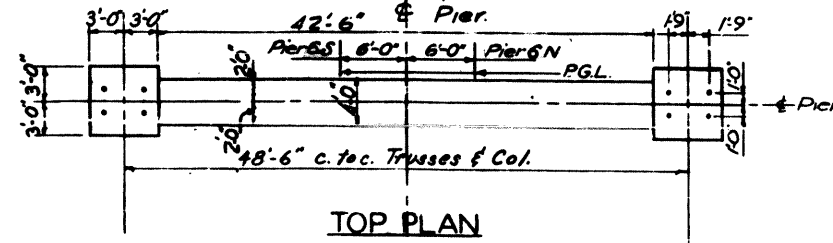
DATE	SECTION	COUNTY	SHEET NO.	SHEET NO.
FAI-80	99-38	WILL	71	34
PROJECT I-80-4(13)153		42 SHEETS		

Pier #6N Sta. 383+98.66 El. 569.92
Pier #6S Sta. 383+74.33 El. 569.52
P.G.L. 7

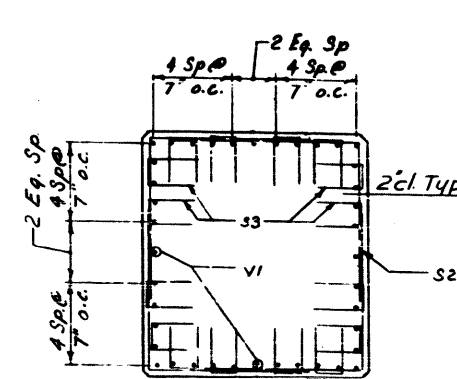
Dim.	Pier No 6N	Pier No 6S
A	15'	15 1/4'
B	8'-2"	8'-1 1/2"
C	52'-0 3/4"	50'-11"
D	7'-1"	7'-0 3/4"
E	580.08	579.68
F	524.50	525.25



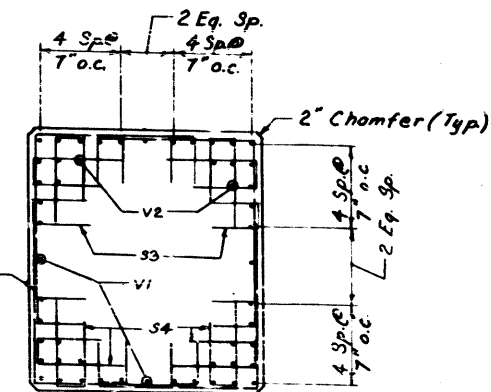
ELEVATION



TOP PLAN

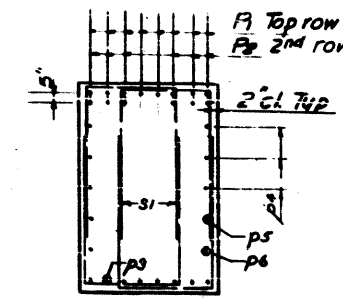


SECTION BB

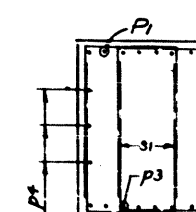


SECTION A-A

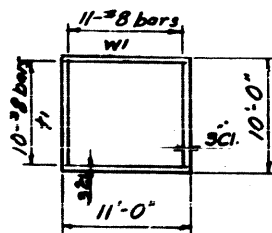
Pier #6S - 19 sets * 5 Ties
76-52 & 304-52 Each Col.
Pier #6N - 20 Sets * 5 Ties
80-52 & 320-52 Each Col.



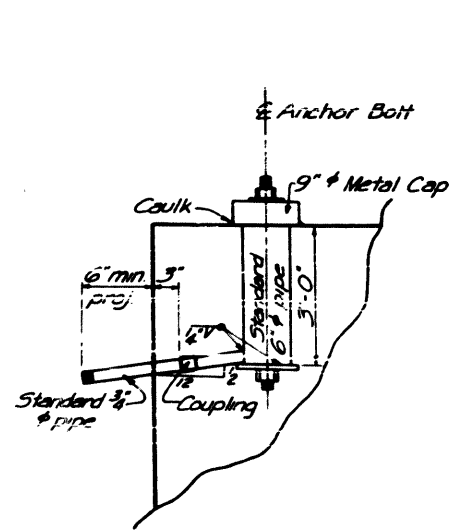
SECTION C-C



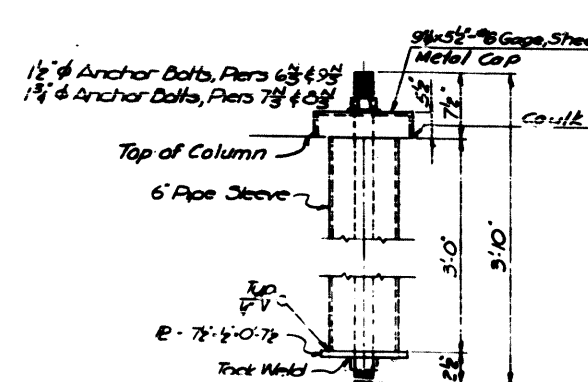
SECTION D-D



FOOTING PLAN



ANCHOR BOLT WELL WITH DRAIN



ANCHOR BOLT DETAIL

Anchor bolt assemblies for piers 6 thru 9 inclusive to be provided, set and grouted under this contract (Sec. 99-38) 96 Required (See special provisions)

Notes:
For Bill of Materials, see sh. 60
For Bar Lists, see sh. 54
For Location Plan, see sh. 25
Maximum Foundation Pressure = 10T/ft

PIERS 6N/S
FAI-80 OVER DES PLAINES RIVER

STA 387 + 94.50

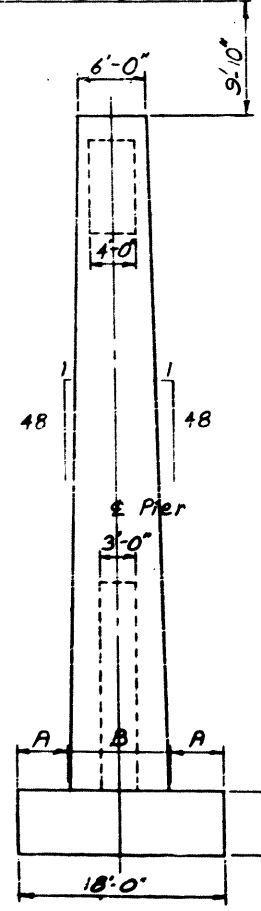
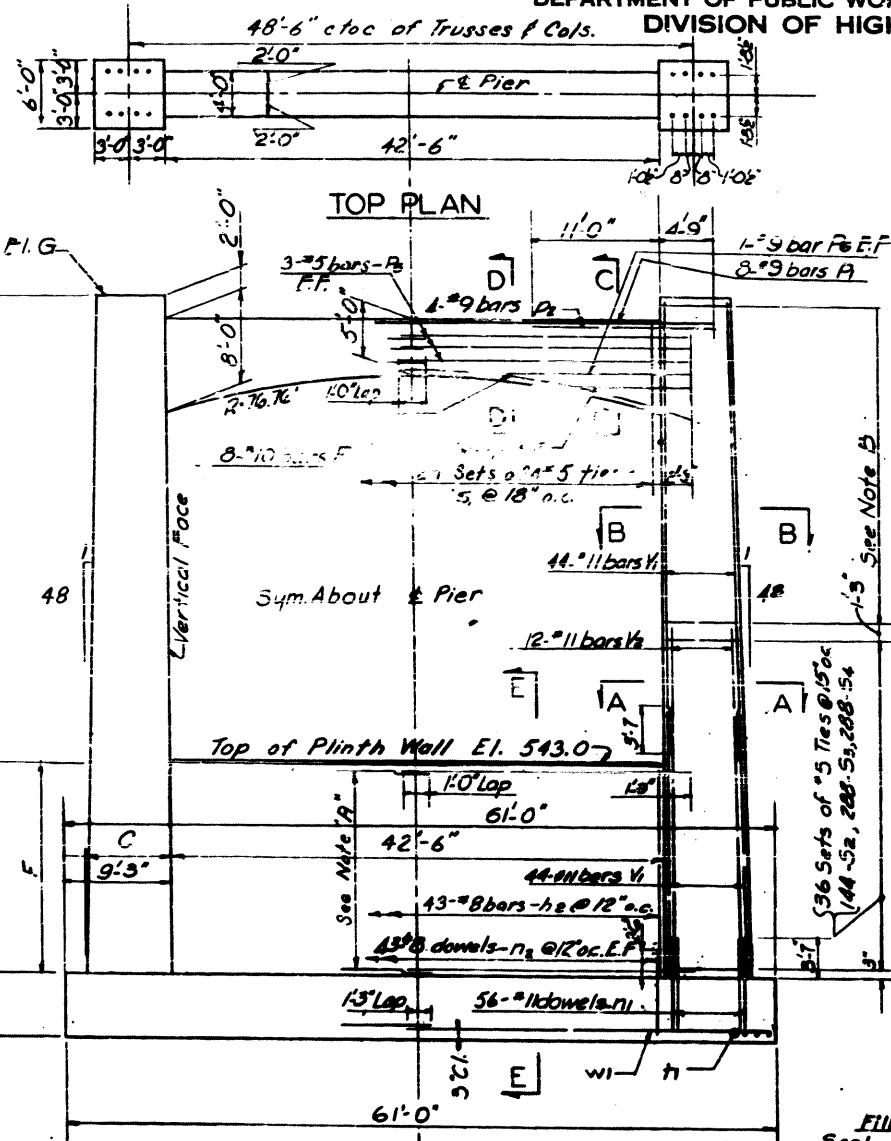
FAI ROUTE 99-38 SECTION 99-38
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N. J. NEW YORK, N. Y. CRYSTAL LAKE, ILL.

DESIGNED	L.D.
CHECKED	A.H.
DRAWN	L.D.
CHECKED	H.C.M.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 16 42 SHEETS
FAI-80	99-38	WILL	71	55	
Pier Nos. 7N-8N		PROJECT I-80-4(13)133			

P.G. Elevations.
Pier 7N Sta. 386+25.33 El. 592.48
Pier 7S Sta. 386+01.00 El. 592.30
Pier 8N Sta. 389+88.00 El. 592.30
Pier 8S Sta. 389+63.67 El. 592.48
P.G.L. →

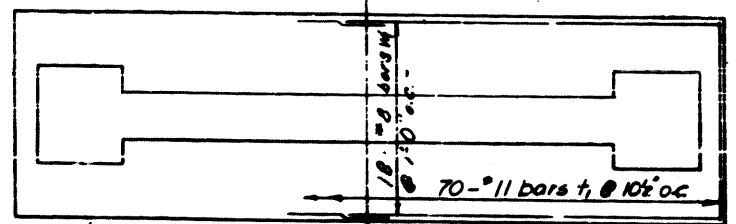


END-VIEW

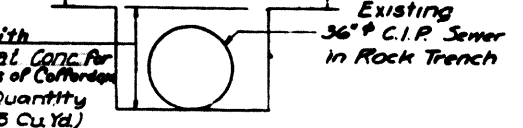
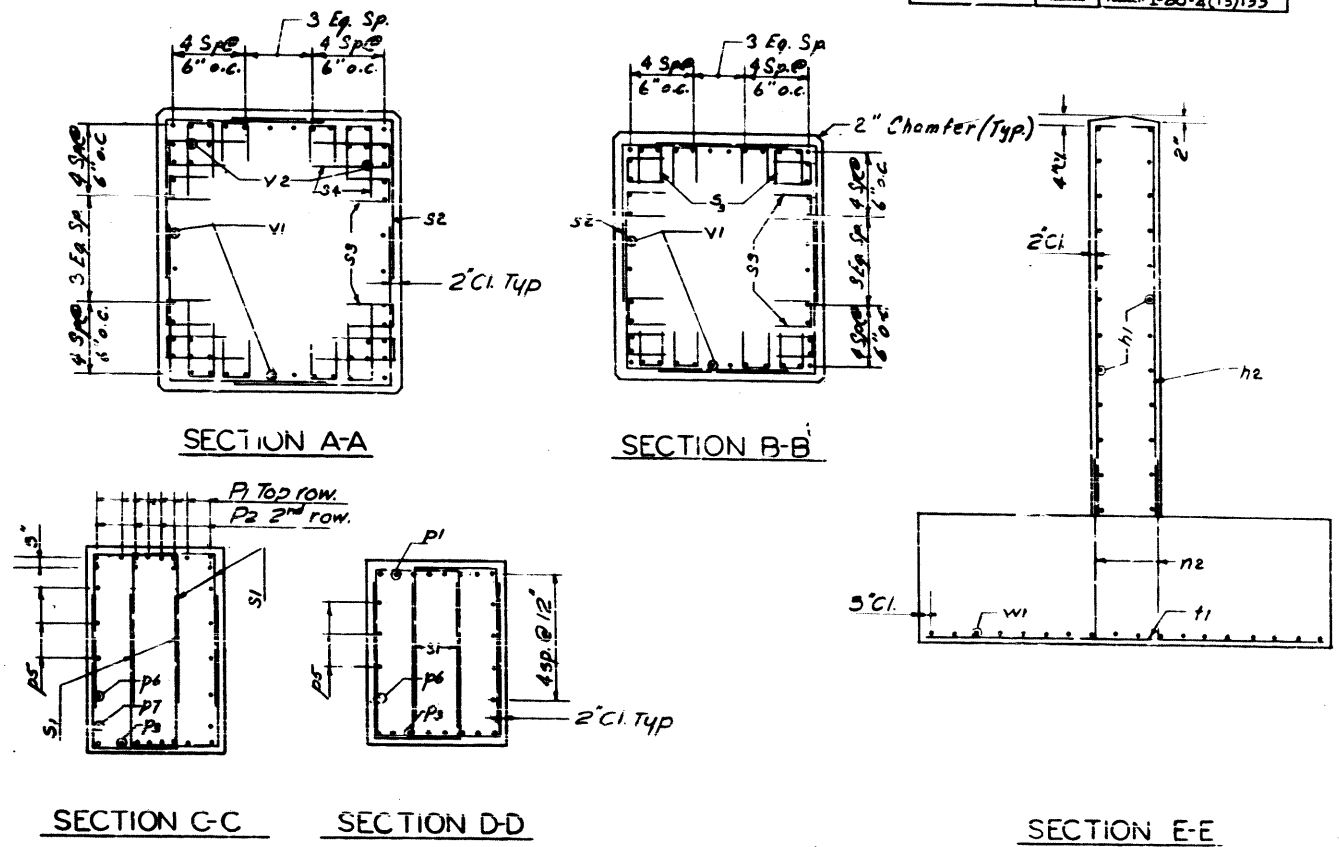
Note "A" (No. Bars E.F.)
7S-20-#5 bars-h₁ @ 18" o.c.
7N-19-#5 bars-h₁ @ 18" o.c.
8S-21-#5 bars-h₁ @ 18" o.c.
8N-18-#5 bars-h₁ @ 18" o.c.

ELEVATION

Note "B" (No. Ties per Col.)
7S-18 Sets of #5 ties @ 15" o.c.
(72-32; 288-33)
7N-19 Sets of #5 ties @ 15" o.c.
(76-32; 304-33)
8S-21 Sets of #5 ties @ 15" o.c.
(84-32; 336-33)
8N-17 Sets of #5 ties @ 15" o.c.
(68-32; 272-33)



FOOTING PLAN



DETAIL A
Showing construction of footing for Pier 8N. Contractor to determine dimension of trench in field.

Notes:
For Bar Lists, see sh. 54
For Bills of Materials see sh. 60
For Location Plan, see sh. 25
For Anchor Bolt Detail, see sh. 34
Maximum foundation Pressure = 10 T/SF

	Pier No. 7N	Pier No. 7S	Pier No. 8N	Pier No. 8S
A	4'-7"	4'-7 1/2"	4'-7 1/2"	4'-6 3/4"
B	8'-10"	8'-9 1/2"	8'-9"	8'-11 1/4"
C	7'-5"	7'-4 1/2"	7'-4 1/2"	7'-3 3/4"
D	68'-1 3/4"	66'-10 1/2"	66'-2"	70'-7 1/4"
F	28'-6"	27'-4 1/4"	26'-8 3/4"	31'-0"
G	582.64	582.47	582.47	582.64
K	509.00	510.10	510.80	506.50

DESIGNED	P.S.S.
CHECKED	J.T.L.
DRAWN	L.D.
CHECKED	M.C.M.

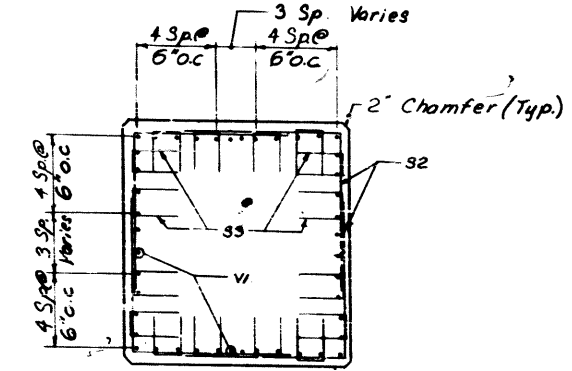
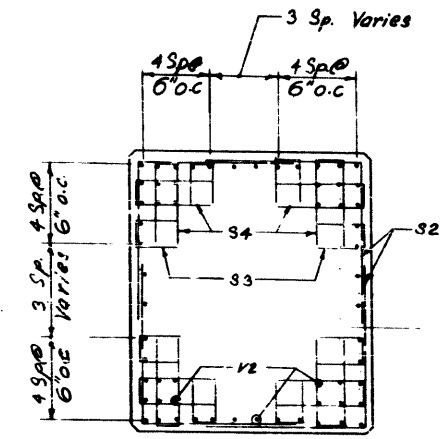
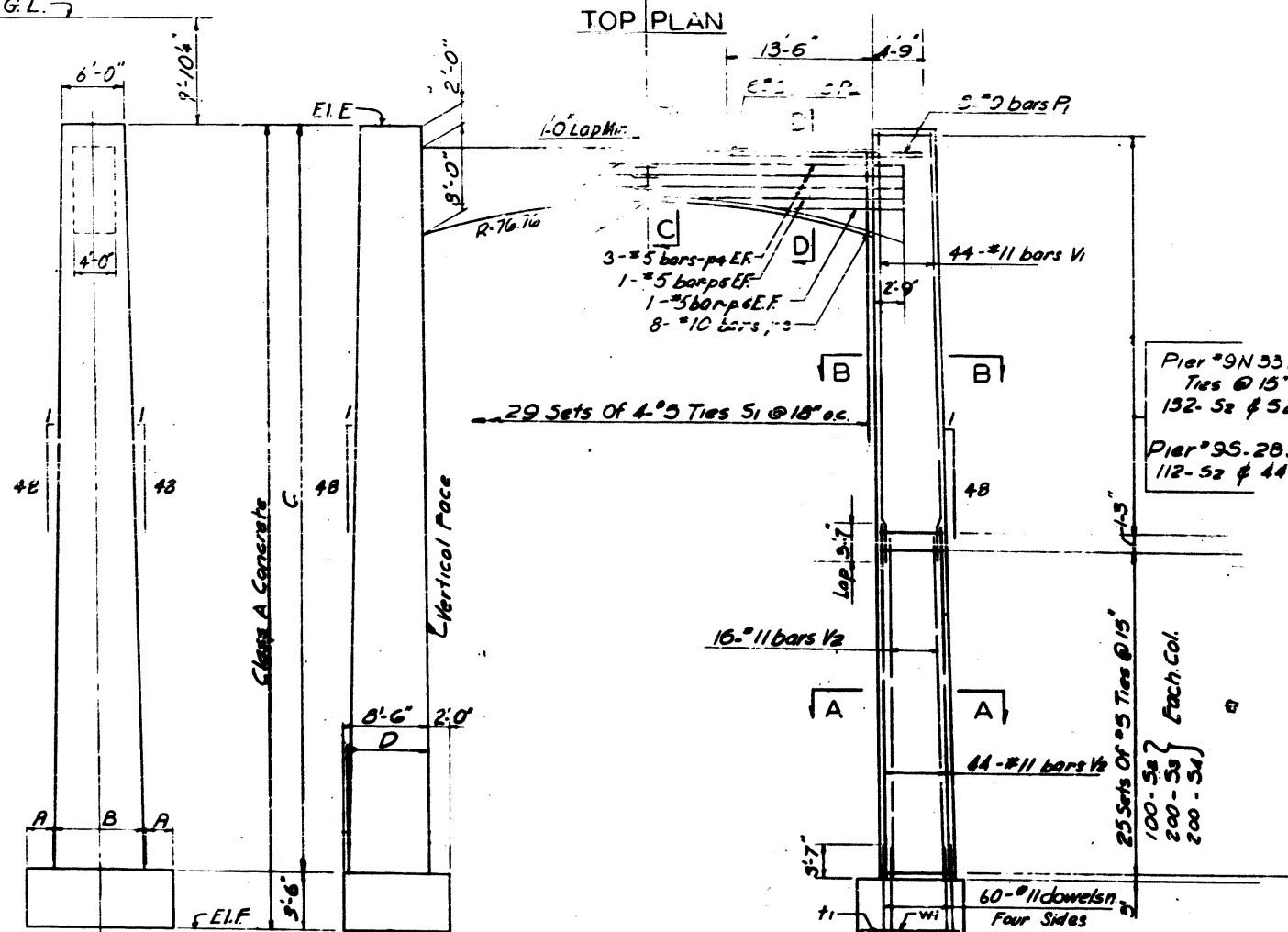
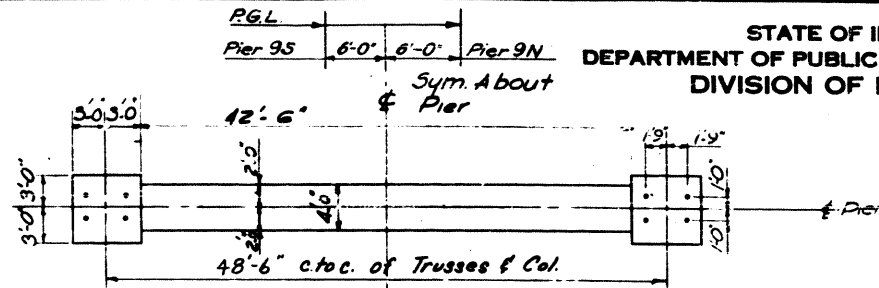
PIERS 7N/S & 8N/S
FAI-80 OVER DES PLAINES RIVER
STA 387 + 94.50
FAI ROUTE 80 SECTION 99-38
WILL COUNTY
PROJECT I-80-4(13)133
DATE Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SHEET NO.	SECTION	COUNTY	WILL	PROJECT	DATE	SHEET NO.
F.A.I.-80	99-38	WILL	71	36		42 SHEETS
F.A.I. ROUTE 80		PROJECT I-80-4(15)133				

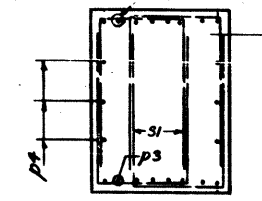
Pier #9N Sta. 391+80.00 Elev. 590.08
Pier #9S Sta. 391+55.67 Elev. 590.44
P.G.L.

Elev. Dim	Pier No.	
	9N	9S
A	2'-6"	2'-7 1/2"
B	9'-0"	8'-9"
C	72'-2 3/8"	65'-10 1/8"
D	7'-6"	7'-4 1/2"
E	580.22	580.59
F	504.50	511.25

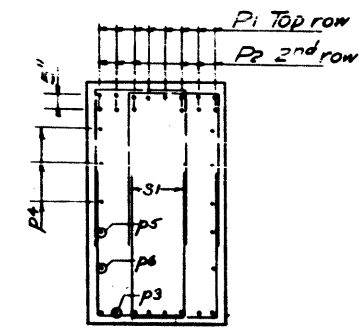


SECTION A-A

SECTION B-B



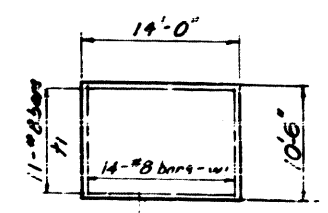
SECTION C-C



SECTION D-D

END VIEW

ELEVATION



FOOTING PLAN

Notes
For Bill of Materials, see sh 60
For Bar Lists, see sh 55
For Location Plan, see sh 25
For Anchor Bolt Detail, see sh 34
Maximum Foundation Pressure - 10 1/2 ksf

DESIGNED	L.D.
CHECKED	A.H.
DRAWN	L.D.
CHECKED	H.C.M.

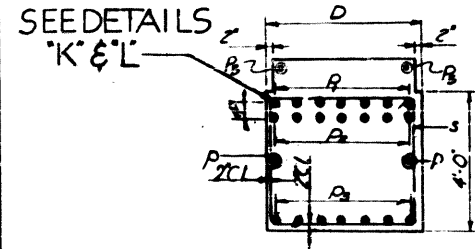
PIERS 9N/S
FAI-80 OVER DES PLAINES RIVER
STA 387 + 94.50
FAI ROUTE 80 PROJECT I-80-4(15)133
SECTION 99-38 WILL COUNTY
Scale: No Scale Date: Jan 25, 1961
REAVELT ENGINEERING CO
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

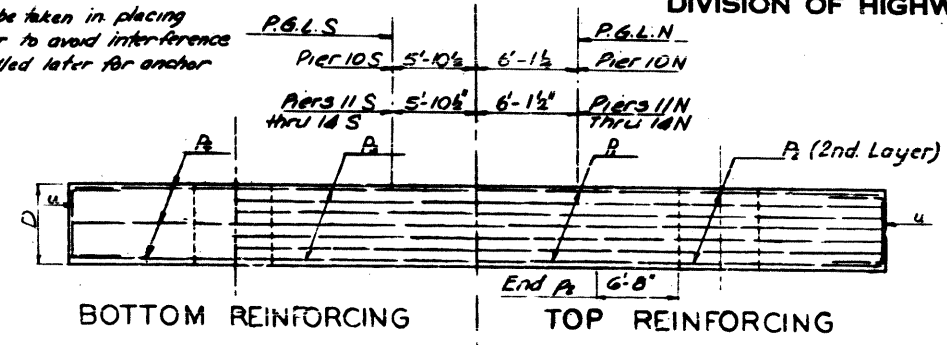
PROJECT NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	37
PROJECT 1-80-413/133				

SHEET NO. 18
42 SHEETS

Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.



SECTION B-B



BOTTOM REINFORCING TOP REINFORCING

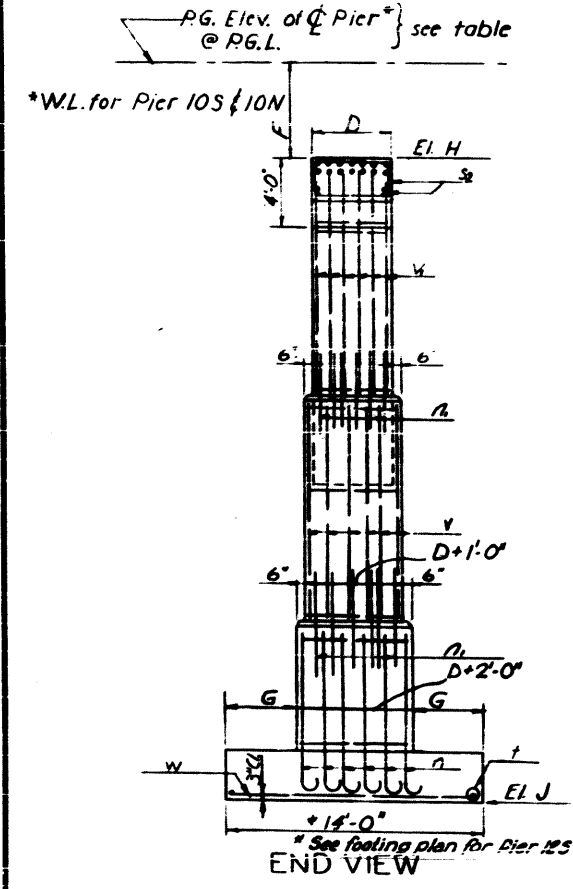
Pier No.	*Station @ Pier @ P.G.L. Elev.	R.G. Elev.
10S	391 + 95.41	589.84
10N	392 + 20.81	589.42
11S	392 + 56.51	588.78
11N	392 + 73.83	588.43
12S	393 + 17.61	587.58
12N	393 + 26.85	587.98
13S	393 + 78.71	586.22
13N	393 + 79.87	586.20
14S	394 + 39.81	584.72
14N	394 + 32.89	584.90

*Station W.L. Pier @ P.G.L. for Piers 10S & 10N

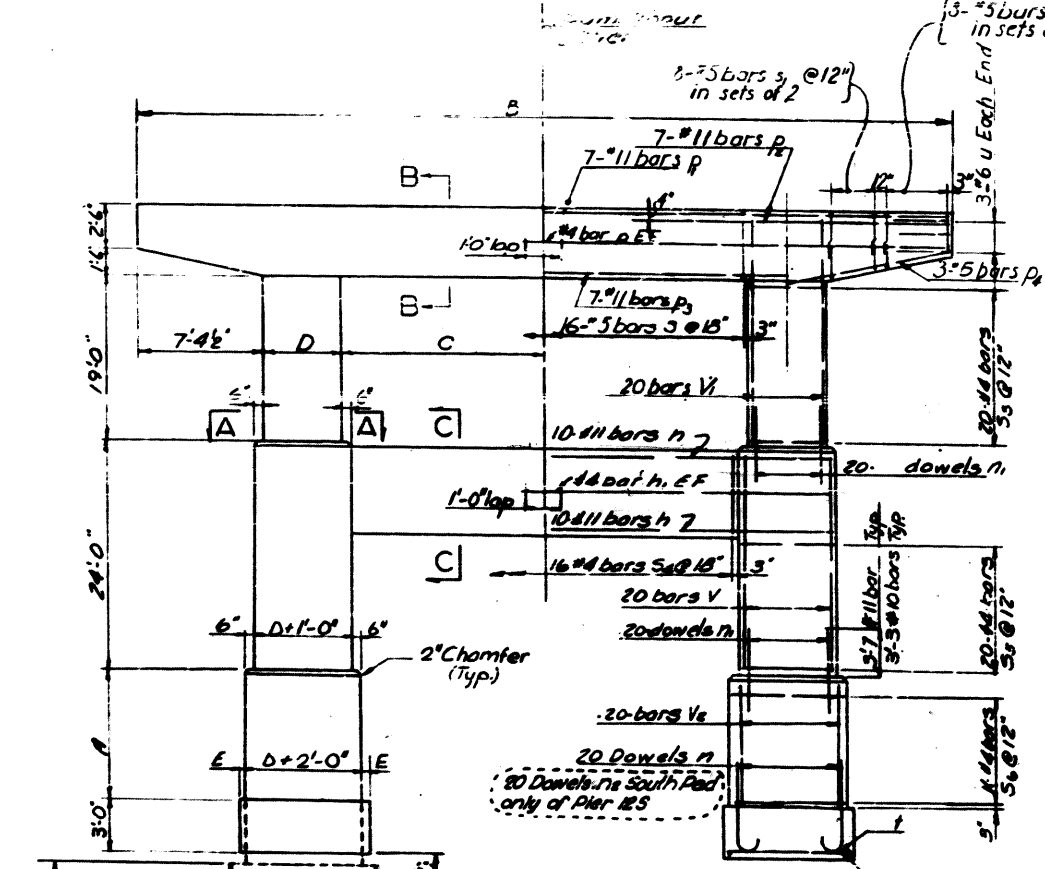
Pier No.	Pier No. 10		Pier No. 11		Pier No. 12		Pier No. 13		Pier No. 14	
	S	N	S	N	S	N	S	N	S	N
A	24'-6"	26'-2"	22'-6"	21'-6"	21'-2"	20'-8"	20'-8"	19'-8"	19'-11"	19'-7"
B	47'-2"	47'-2"	46'-11"	46'-11"	46'-11"	46'-11"	46'-11"	46'-11"	46'-11"	46'-11"
C	11'-8"	11'-8"	11'-7"	11'-7"	11'-7"	11'-7"	11'-10"	11'-10"	11'-10"	11'-10"
D	4'-6"	4'-6"	4'-6"	4'-6"	4'-6"	4'-6"	4'-3"	4'-3"	4'-3"	4'-3"
E	3"	3"	3"	3"	3"	3"	4"	4"	4"	4"
F	5'-8"	5'-5"	5'-6"	5'-2"	5'-6"	5'-2"	5'-6"	5'-2"	5'-6"	5'-2"
G	3'-9"	3'-9"	3'-9"	3'-9"	3'-9"	3'-9"	3'-10"	3'-10"	3'-10"	3'-10"
H	584.16	583.99	583.23	583.24	582.04	582.18	580.67	580.98	579.18	579.69
J	510.6	507.8	510.7	511.7	510.8	511.5	510.0	511.3	509.2	510.1
K	25	27	23	22	22	21	21	20	20	20

PIER 12S SOUTH PAD ONLY

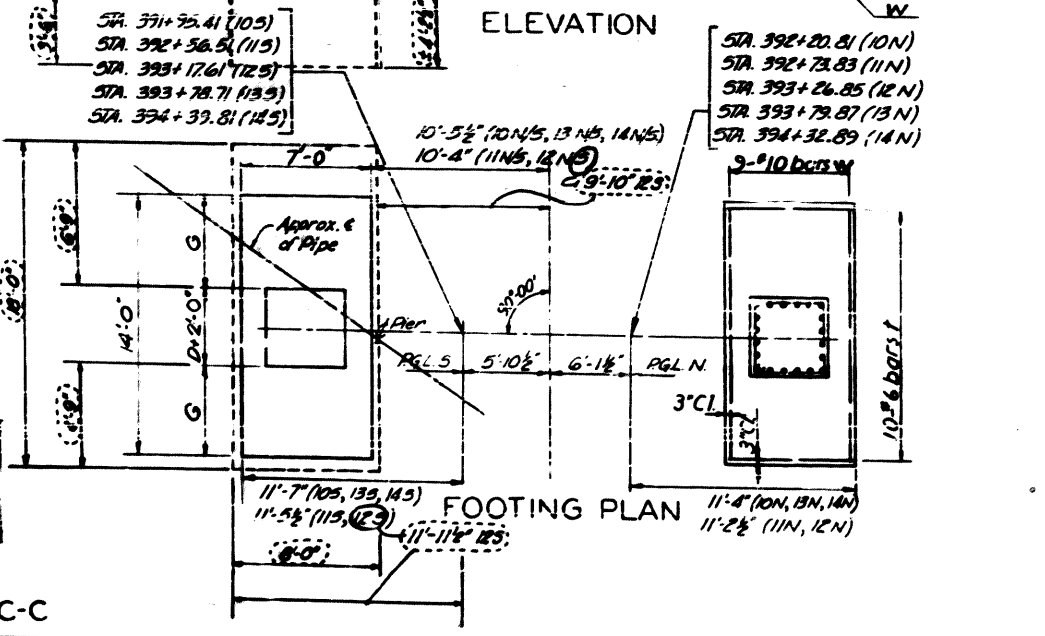
A	24'-11"
B	46'-11"
C	11'-7"
D	4'-6"
E	9"
F	5'-6"
G	3'-10"
H	582.04
J	506.5
BASE	41



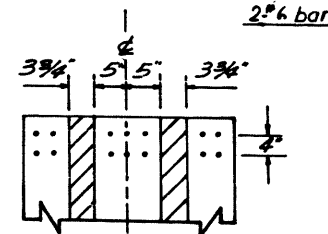
END VIEW



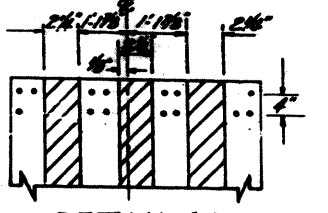
ELEVATION



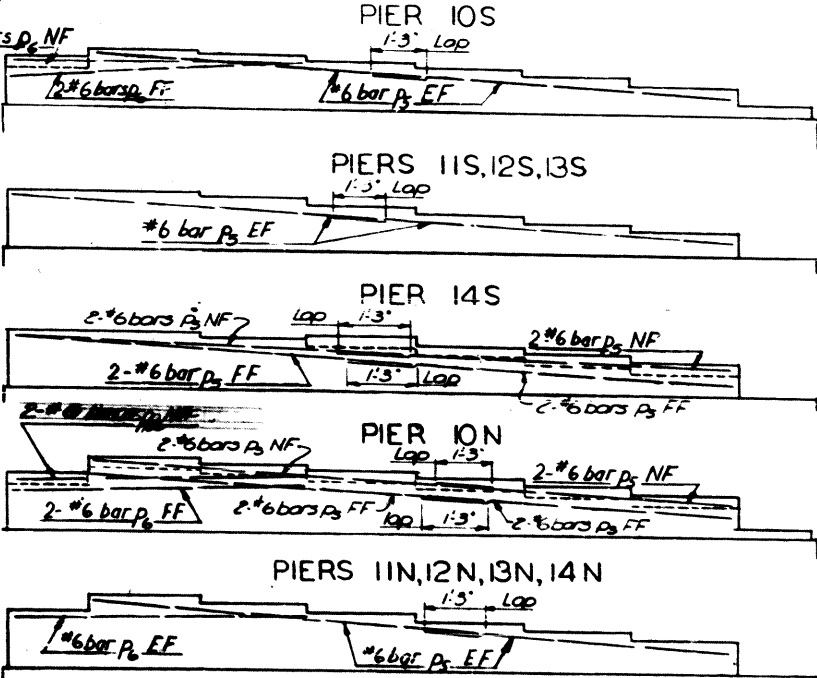
FOOTING PLAN



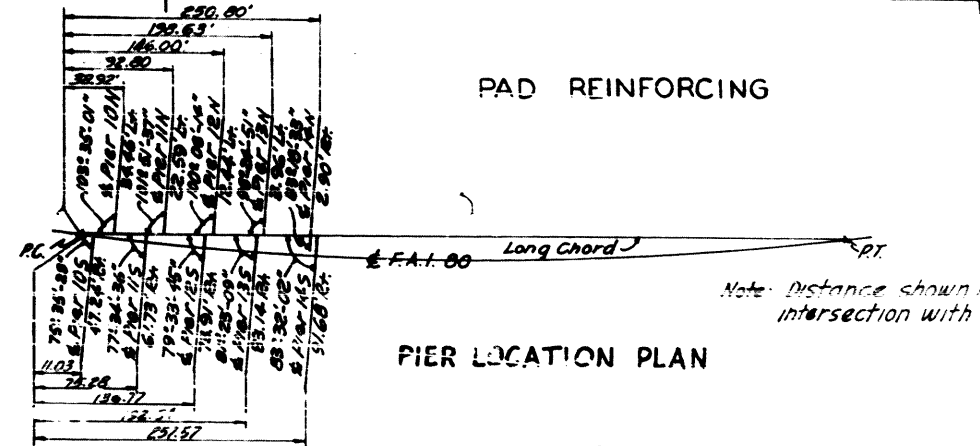
DETAIL 'K' Piers 11N, 12N, 13N & 14N TOP STEEL ONLY



DETAIL 'L' Piers 10N & 12N TOP STEEL ONLY



PAD REINFORCING



PIER LOCATION PLAN

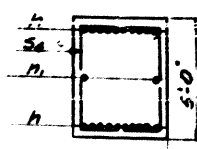
Note: For Bill of materials see sh. 60
For Pad Elevations and Dimensions, see Key Plans, sh. 25 & 26
For Bar Lists see sh. 55 & 56
For Location Plan, see sh. 25
Maximum Foundation Pressure = 5 1/2 t/c

PIERS 10 N/S, 11 N/S, 12 N/S, 13 N/S & 14 N/S
FAI-80 OVER DES PLAINES RIVER

STA 387 + 94.50

FAI ROUTE 80 SECTION 99-38 PROJECT 1-80-413/133
WILL COUNTY Date: Jan 25, 1961
BLAUVELT ENGINEERING CO. CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

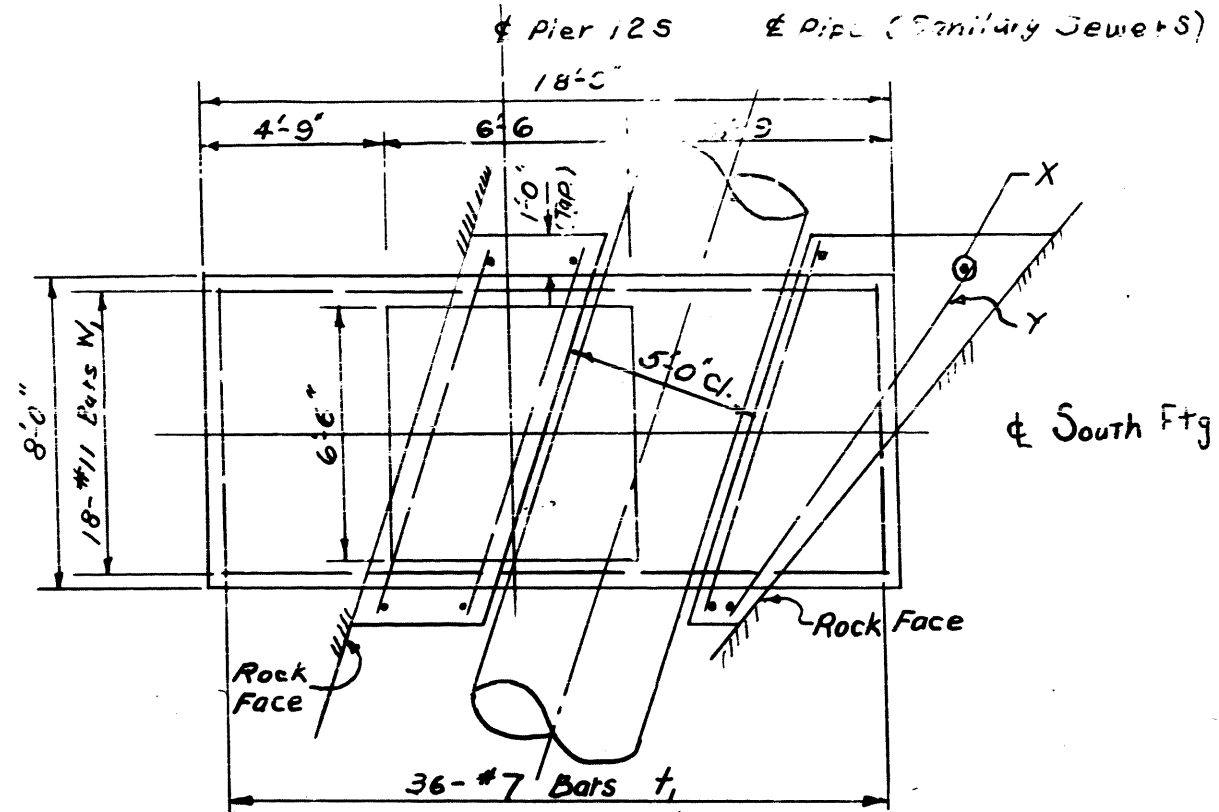
DESIGNED	R.H.
CHECKED	H.C.M.
DRAWN	P.v.d.M. R.S.
CHECKED	H.C.M. I.G.



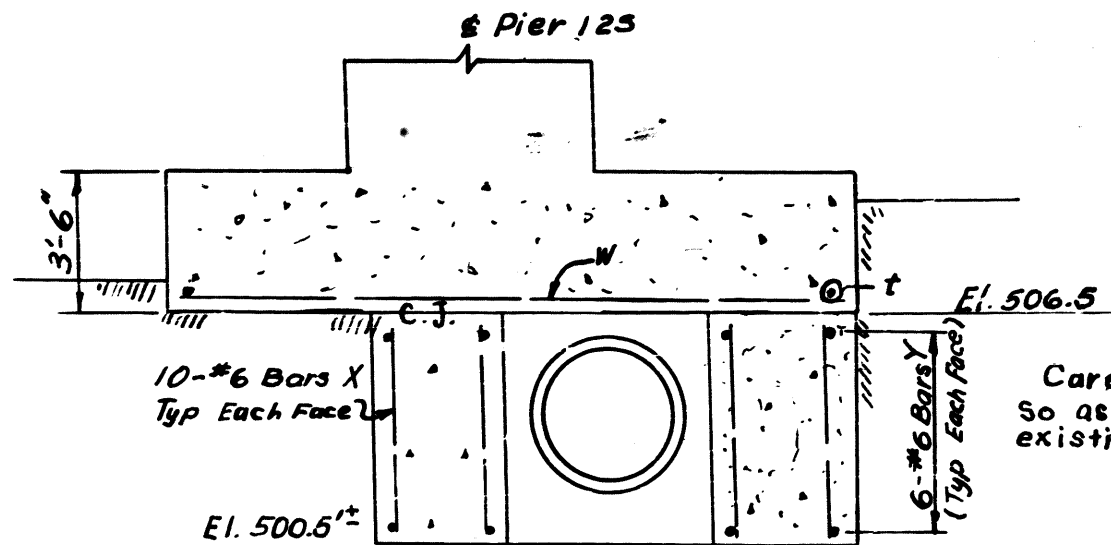
SECTION C-C

Revision 1-90-60 South pad of pier 12S revised. W.C.K.

FOR REINFORCEMENT DETAILS AND OTHER FOOTING DETAILS AND ADDITIONAL QUANTITIES FOR THE SOUTH PAD OF PIER 12S SEE SHEET 37A.



FOOTING PLAN



SECTION

ADDITIONAL QUANTITIES

CLASS X Concrete	Cu. YDS.	31.8
REINFORCEMENT BARS	LBS.	4320

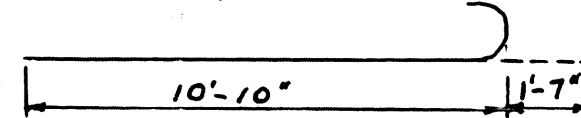
BAR LIST (NEW BARS)

Mark	Size	No.	Length	Type
t ₁	#7	36	7'-6"	—
W ₁	#11	18	17'-6"	—
X	#6	40	5'-6"	—
Y	#6	24	10'-0"	—
n ₂	#11	20	12'-5"	—
S ₆	#4	5	*	*

*See Sheet 55 of plans.

Column Dowels as per Contract Plan

Rock & Sewer locations from P.E. Tanner's letter of 12/20/62



BAR n₂

FAI-80 OVER DES-PLAINES RIVER

Proposed Footing Revision
Pier # 12 S

DWG NO. 174 X 1	SCALE 4" = 1'-0"	DATE 11/7/63	BLAUVELT ENGINEERING CO. CONSULTING ENGINEERS
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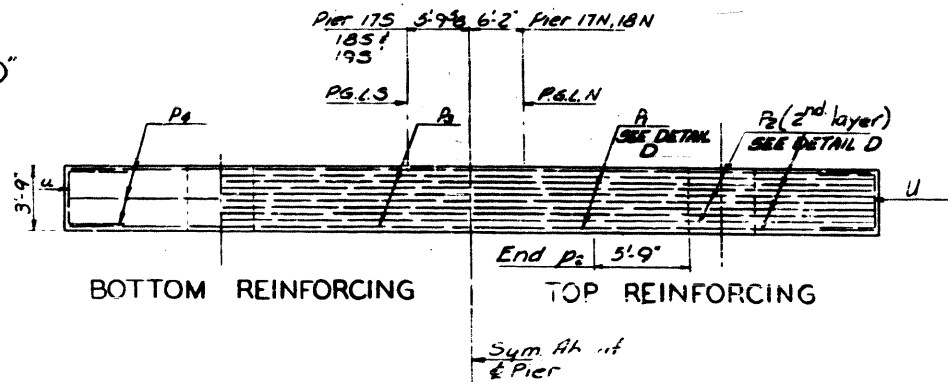
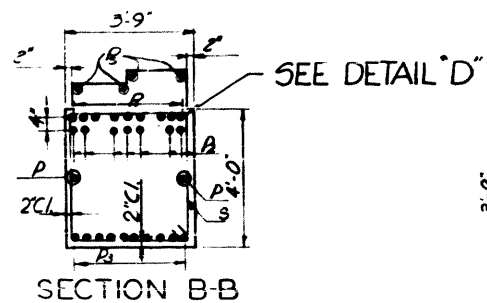
SFM

FAI ROUTE 80 PROJECT 1-80-4(13) 133
SECTION 99-3B WILL COUNTY
STATION 387 + 94.50

This sheet added to plans 1-30-63 W.C.K.
Construction revisions south pad pier 123.

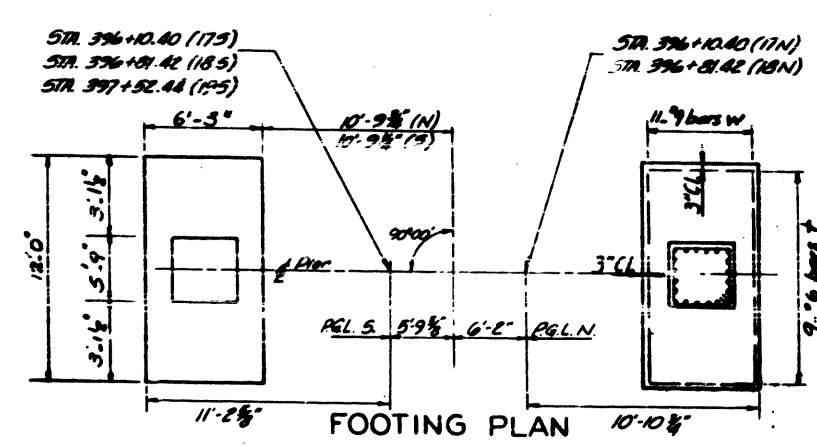
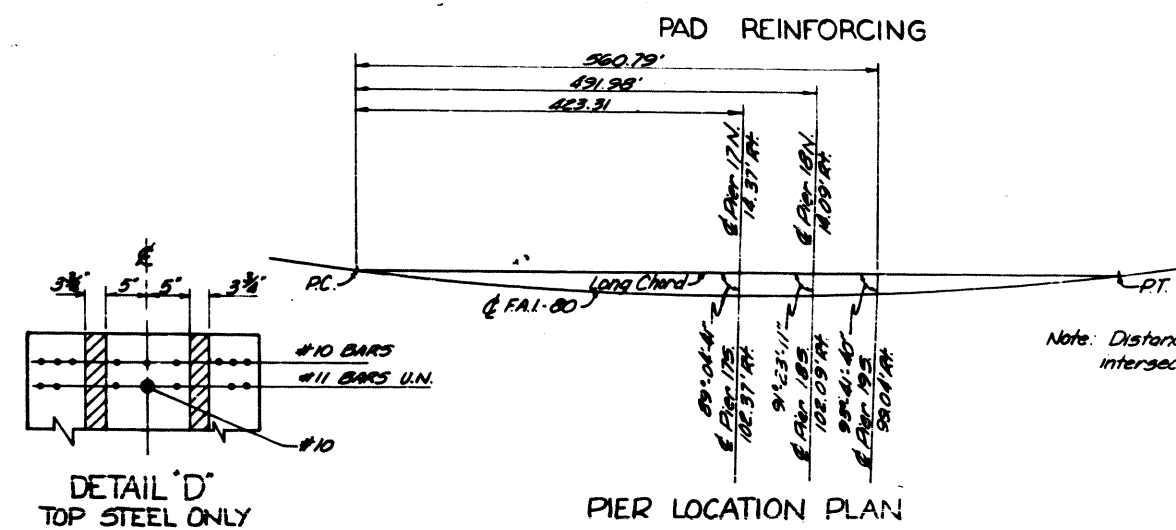
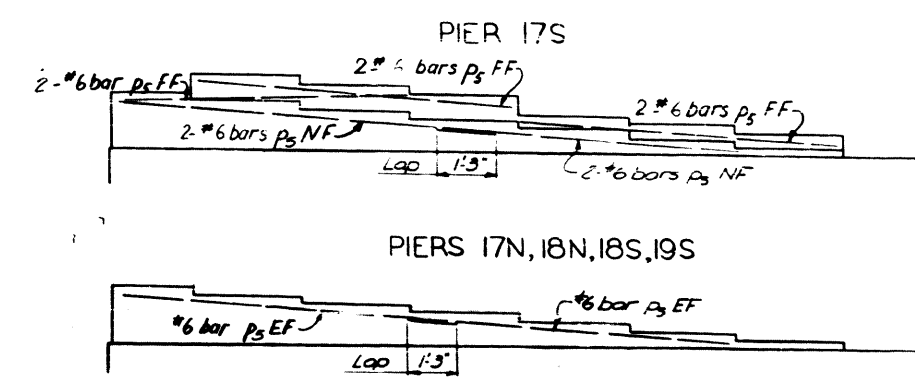
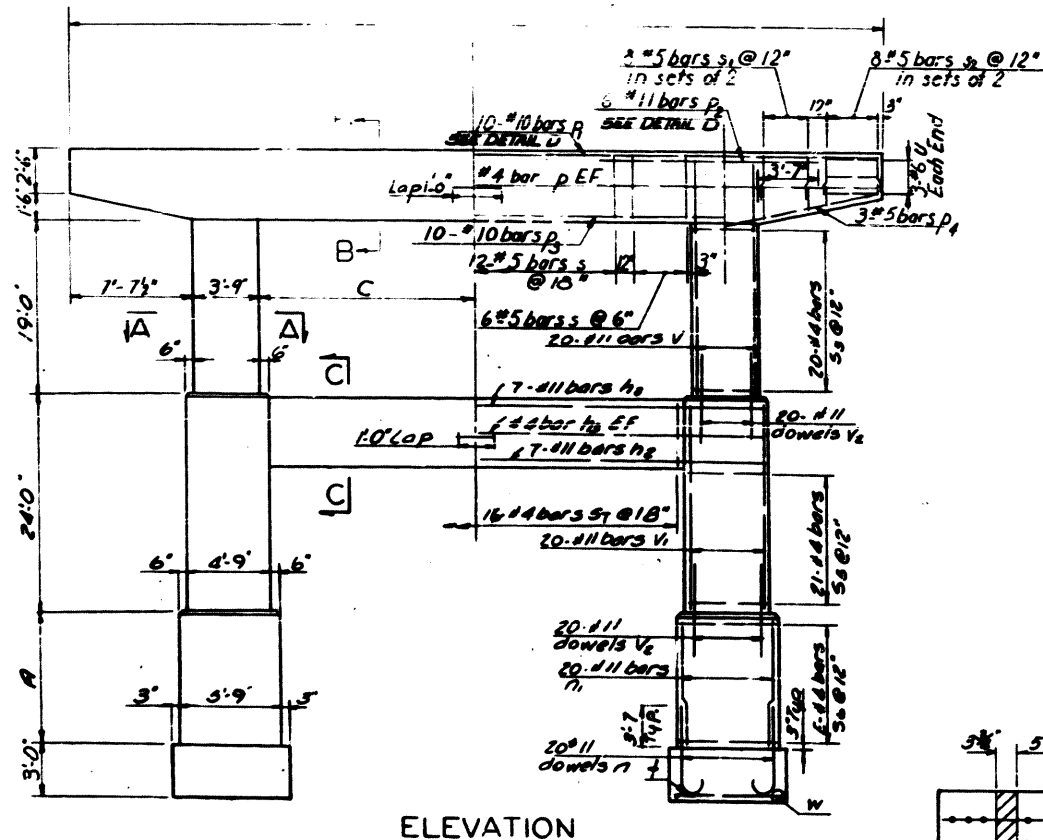
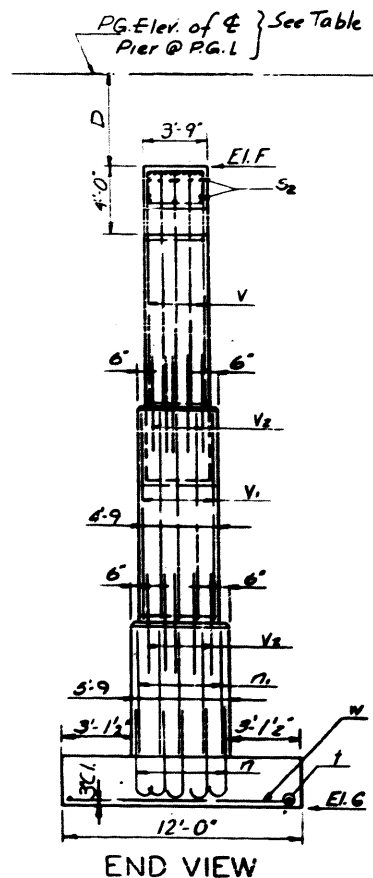
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE OR SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80 99-3B	WILL	71	30
PROJECT: 7-80-d(13)-53		SHEET NO. 20 42 SHEETS	



Dimen.	Pier No. 17		Pier No. 18		Pier No. 19
	S	N	S	N	S
A	10'-3"	11'-1 1/2"	8'-1 1/2"	8'-5 1/2"	7'-0"
B	46'-9 1/2"	46'-10 1/2"	46'-9 1/2"	46'-10 1/2"	46'-9 1/2"
C	12'-0 1/2"	12'-0 1/2"	12'-0 1/2"	12'-0 1/2"	12'-0 1/2"
D	5'-6 1/2"	5'-7 1/2"	5'-6 1/2"	5'-7 1/2"	5'-6 1/2"
Elev.	11	12	2	2	2
F	574.26	574.61	572.14	572.47	570.00
G	514.0	513.5	514.0	514.0	513.0

Pier No.	Station @ Pier @ P.G.L.	P.G. Elev.
17S	396+10.40	574.82
17N	396+10.40	574.82
18S	396+81.42	577.69
18N	396+81.42	577.69
19S	397+52.44	575.56

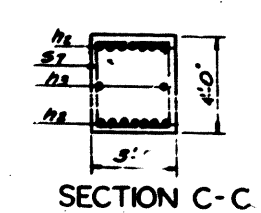


Note: Keep reinforcement free of shaded area.
Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.

Notes:
For Bill of Materials, see sh. 60
For Pad Elevations and Dimensions, see Key Plans, sh. 26 & 27
For Bar Lists, see sh. 57
For Location Plan, see sh. 25
Maximum Foundation Pressure: 5 1/2 ksc

PIERS 17 N/S, 18 N/S, & 19 S
FAI-80 OVER DES PLAINES RIVER
STA 387+94.50
FAI ROUTE 80 SECTION 99-3B
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE ILL.

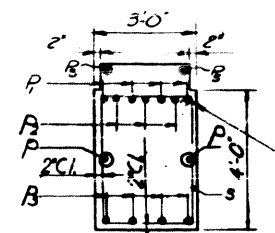
DESIGNED R.H.
CHECKED H.C.M.
DRAWN P.v.d.M. R.S.
CHECKED H.C.M. I.G.



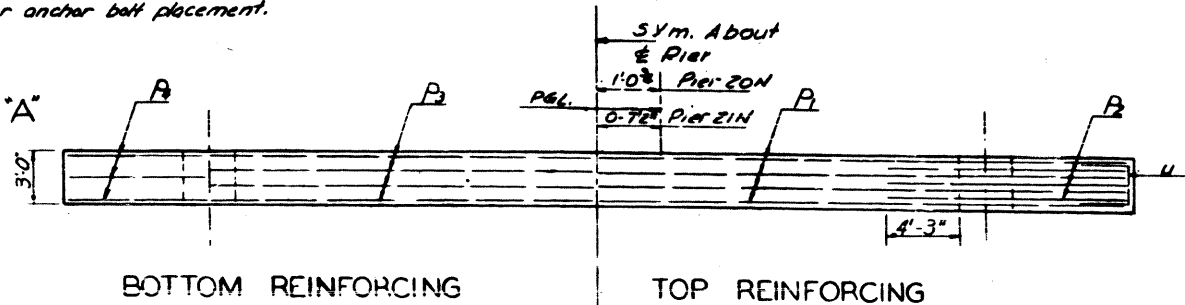
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 21
FAI-80	99-38	WILL	71	40	40 SHEETS
FILE ROAD DIST. NO. 7	ALIGNED	PROJECT 1-80-473/133			

Note:
Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.



SECTION B-B



BOTTOM REINFORCING

TOP REINFORCING

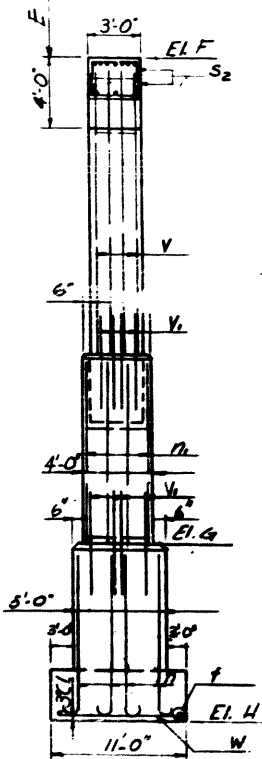
Pier No.	Dimensions					Elevations			No. of bars			Station & Pier @ P.G.L.	D.G. Elev.
	A	B	C	D	E	F	G	H	J	K	L		
20N	23'5 1/2"	6'1 1/2"	55'5"	16'8 1/2"	5'5 1/2"	567.97	521.5	512.4	12	17	20	398+23.46	573.43
21N	21'3 1/4"	5'6"	58'0"	16'0"	5'6 1/2"	565.77	521.5	513.0	13	18	18	398+94.48	571.30

PIER 20N & 21N

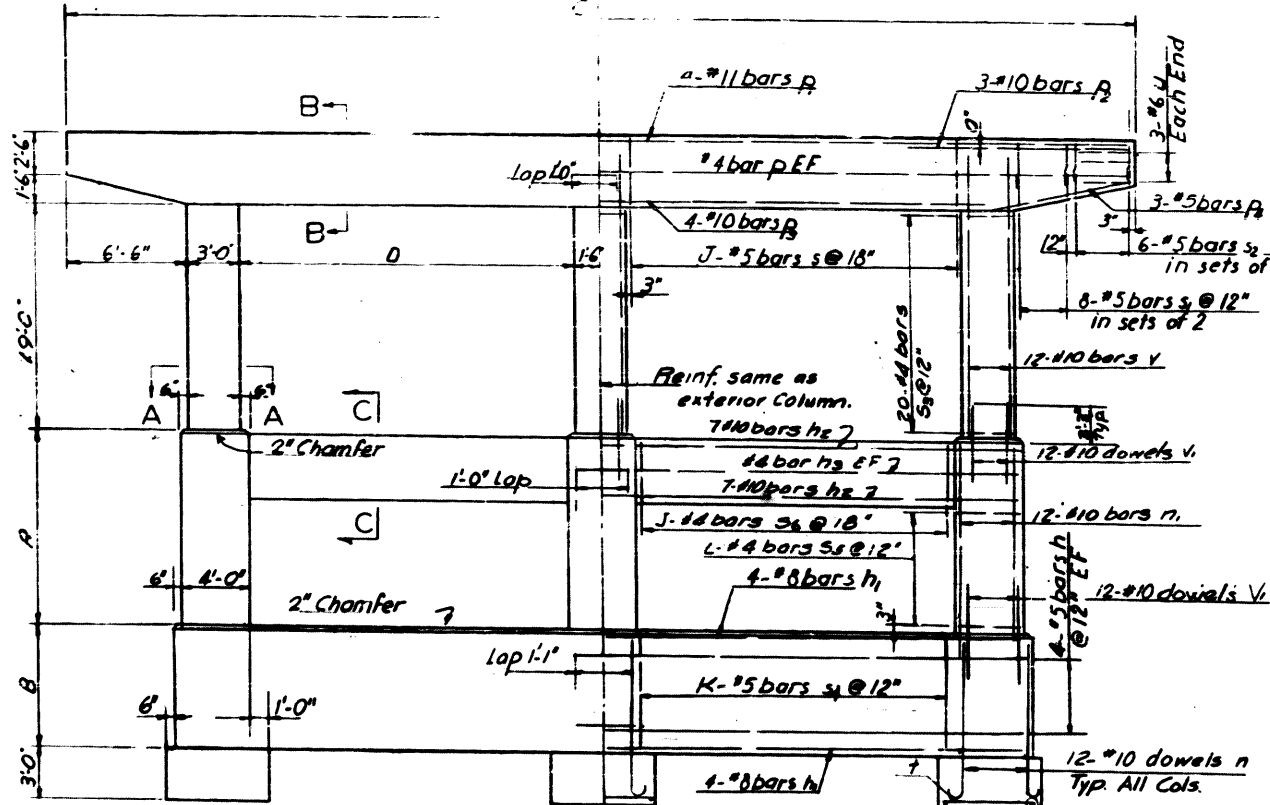


PAD REINFORCING

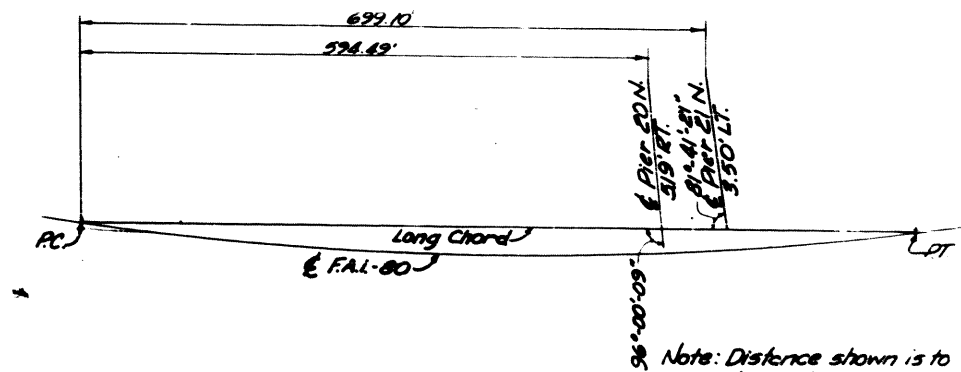
PG Elev. of Pier @ P.G.L. See Table



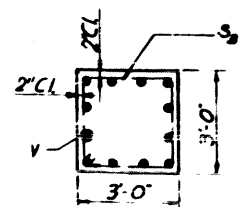
END VIEW



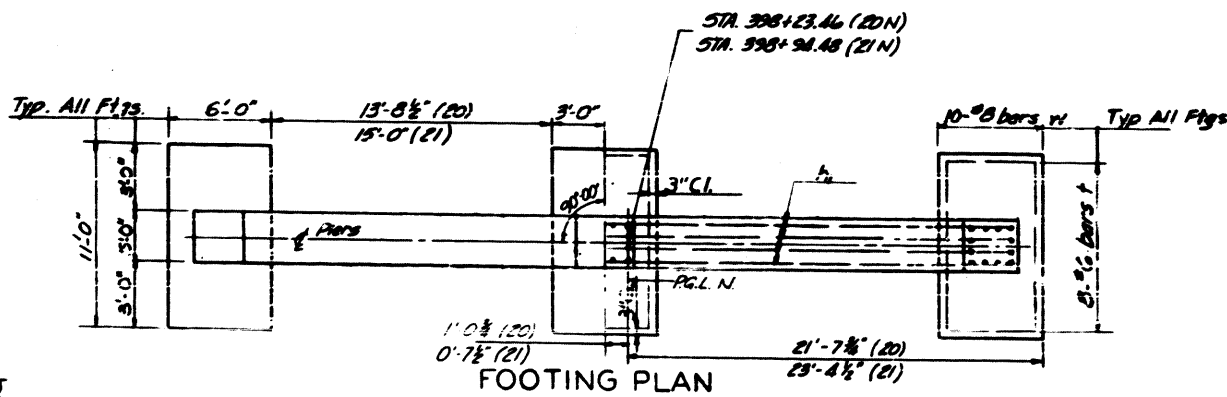
ELEVATION



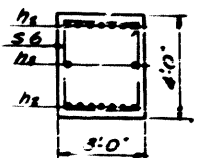
PIER LOCATION PLAN



SECTION A-A



FOOTING PLAN



SECTION C-C

DESIGNED	F.H.
CHECKED	F.T.G.
DRAWN	P.v.d.M. R.S.
CHECKED	H.C.M. I.G.

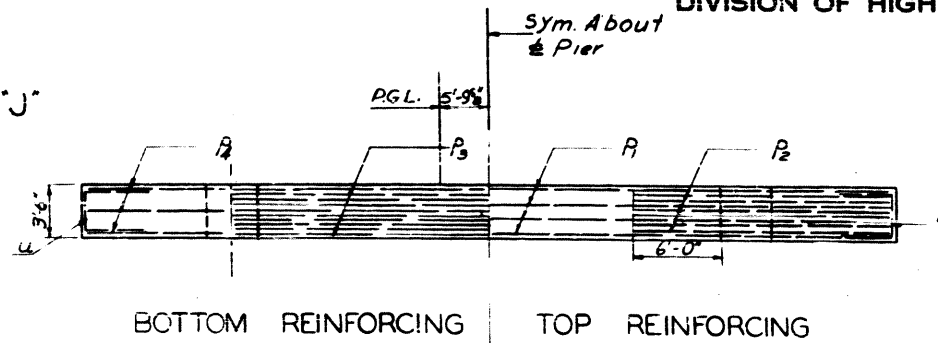
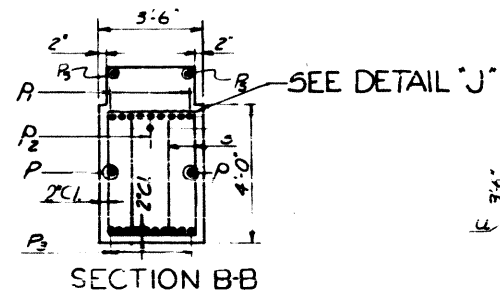
Note:
For Bill of Materials see sh. 60
For Pad Elevations and Dimensions see Key Plans, sh. 26 & 27
For Bar Lists see sh. 57 & 58
For Location Plan see sh. 25
Maximum Foundation Pressure: 5.75t

PIERS 20N & 21N
FAI-80 OVER DES PLAINES RIVER

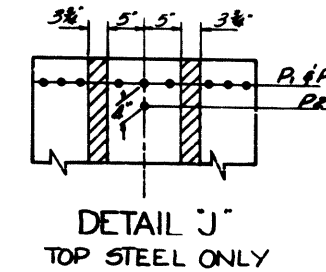
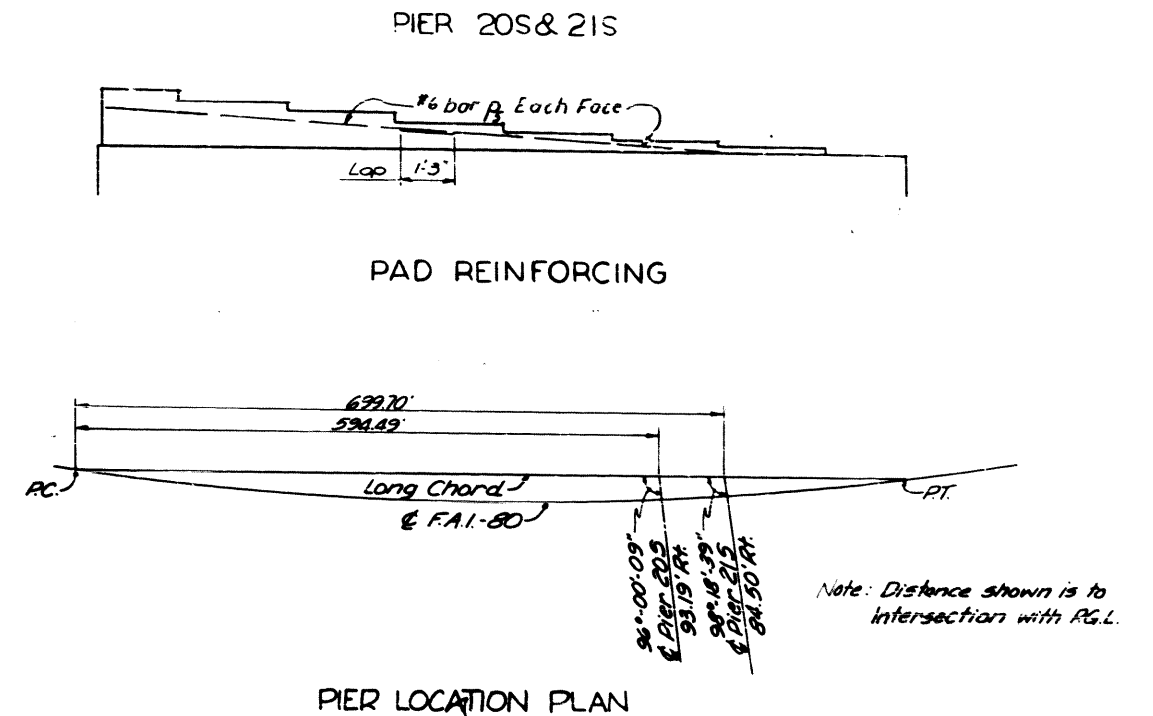
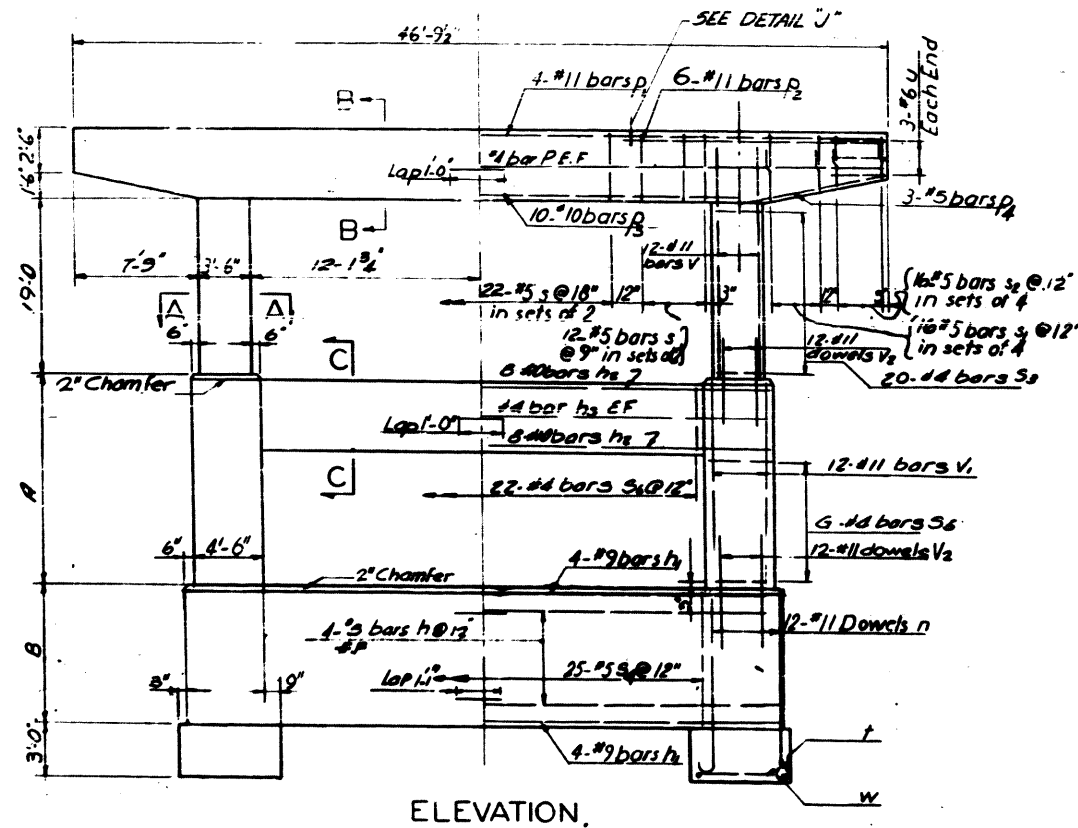
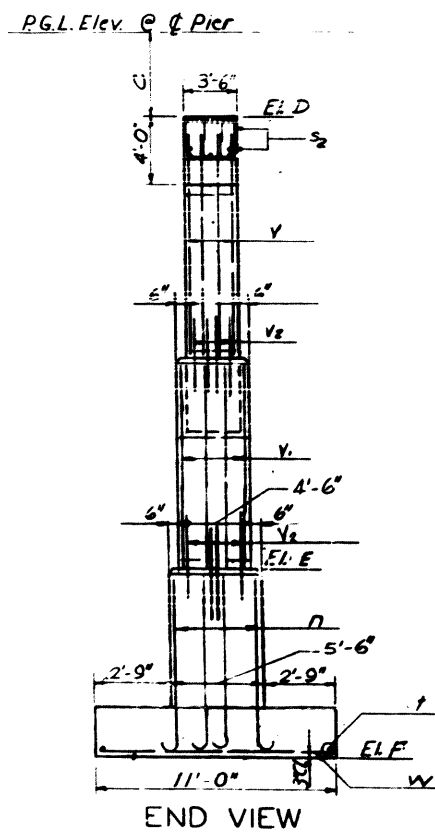
STA. 387 + 94.50
FAI ROUTE 80 PROJECT 1-80-473/133
SECTION 99-38 WILL COUNTY
Scale: No Scale Date: Jan 25, 1951
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

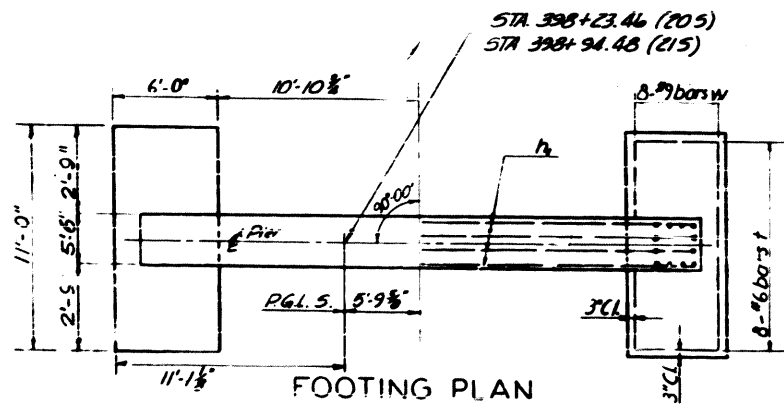
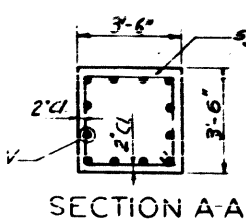
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	41
PROJECT: I-80-4(13)133		SHEET NO. 22 42 SHEETS		



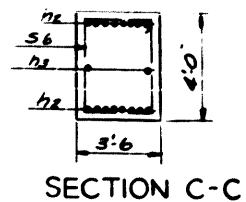
Pier NO	Dimensions			Elevations			Bars	Station @ P.G.L.	P.G. Elev.
	A	B	C	D	E	F			
20S	22'-10 1/2"	5'-10 3/4"	5'-6 3/4"	567.00	522.0	513.1	19	398+23.46	573.43
21S	20'-8 3/4"	5'-3 3/4"	5'-6 3/4"	565.74	522.0	513.7	18	398+94.48	571.30



Note: Keep reinforcement free of shaded area.
Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.



DESIGNED	F.T.G
CHECKED	R.H
DRAWN	P.v.d.M R.S
CHECKED	H.C.M I.G



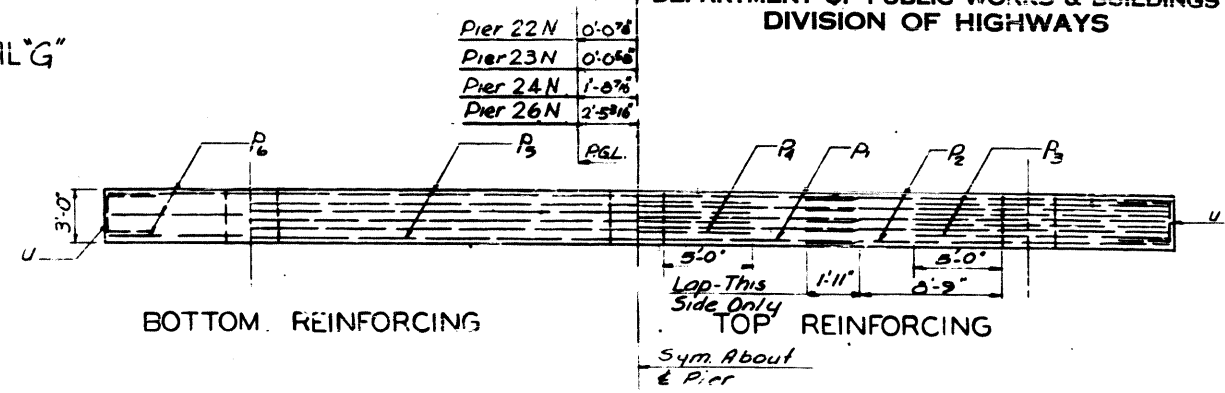
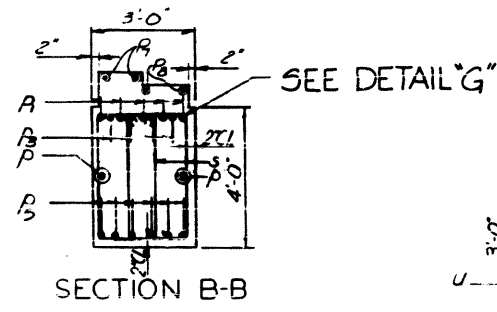
Notes:
For Bill of Materials see sh. 60
For Pad Elevations and Dimensions, see Key Plans, sh. 27
For Bar Lists see sh 57 & 58
For Location Plan, see sh. 25
Maximum foundation Pressure: 5 T/SF

PIERS 20 S & 21 S
FAI-80 OVER DES PLAINES RIVER
STA 387 + 94.50
FAI ROUTE 80
SECTION 99-38
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

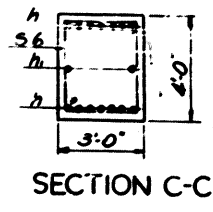
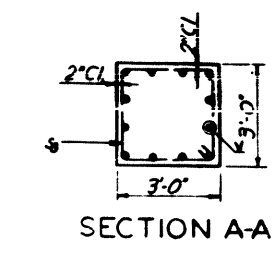
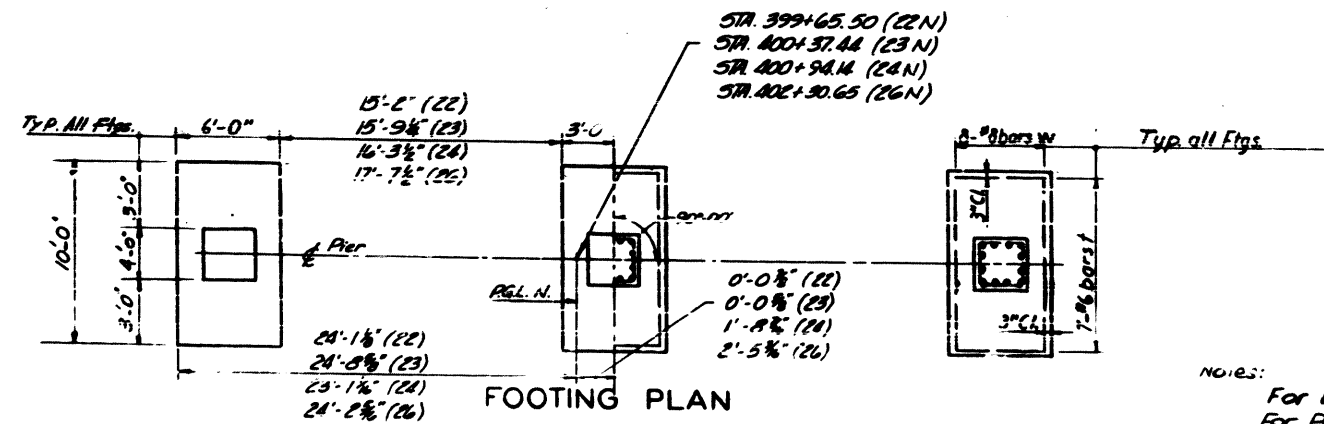
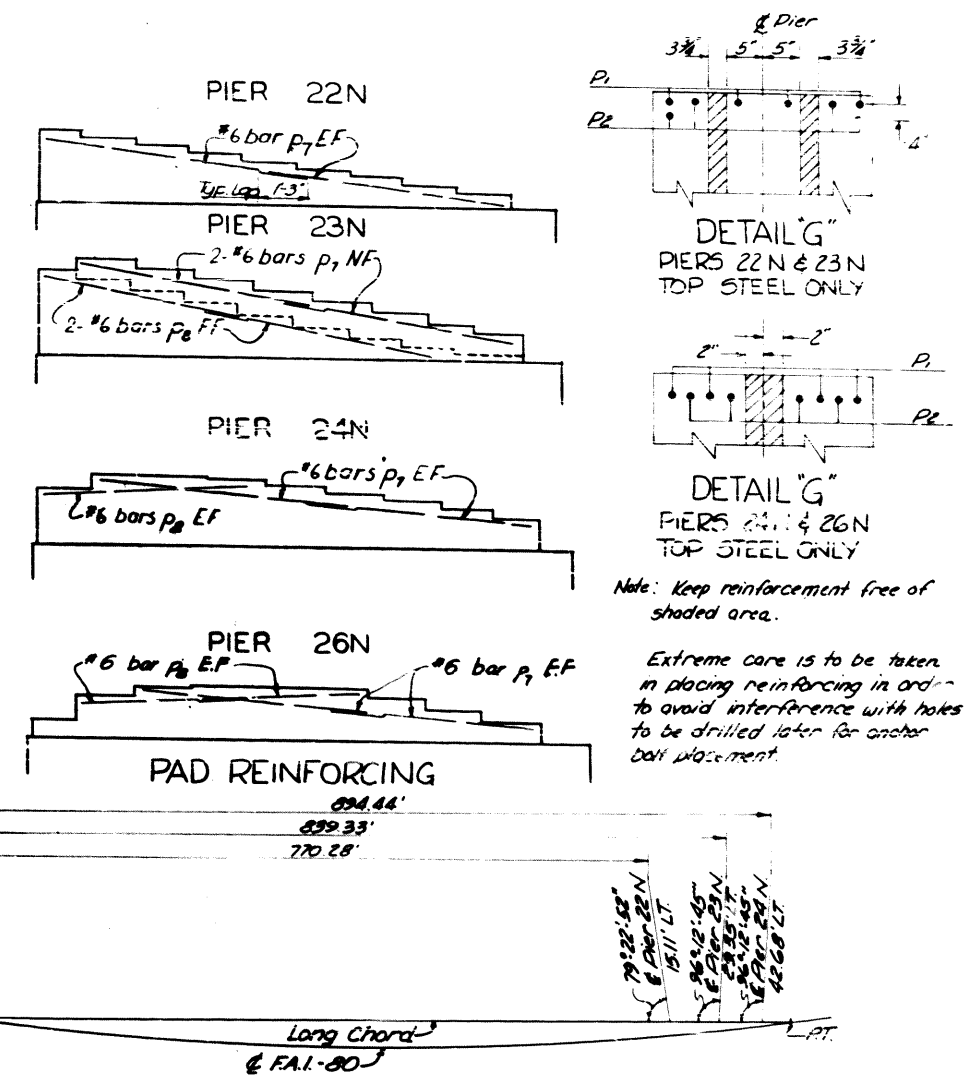
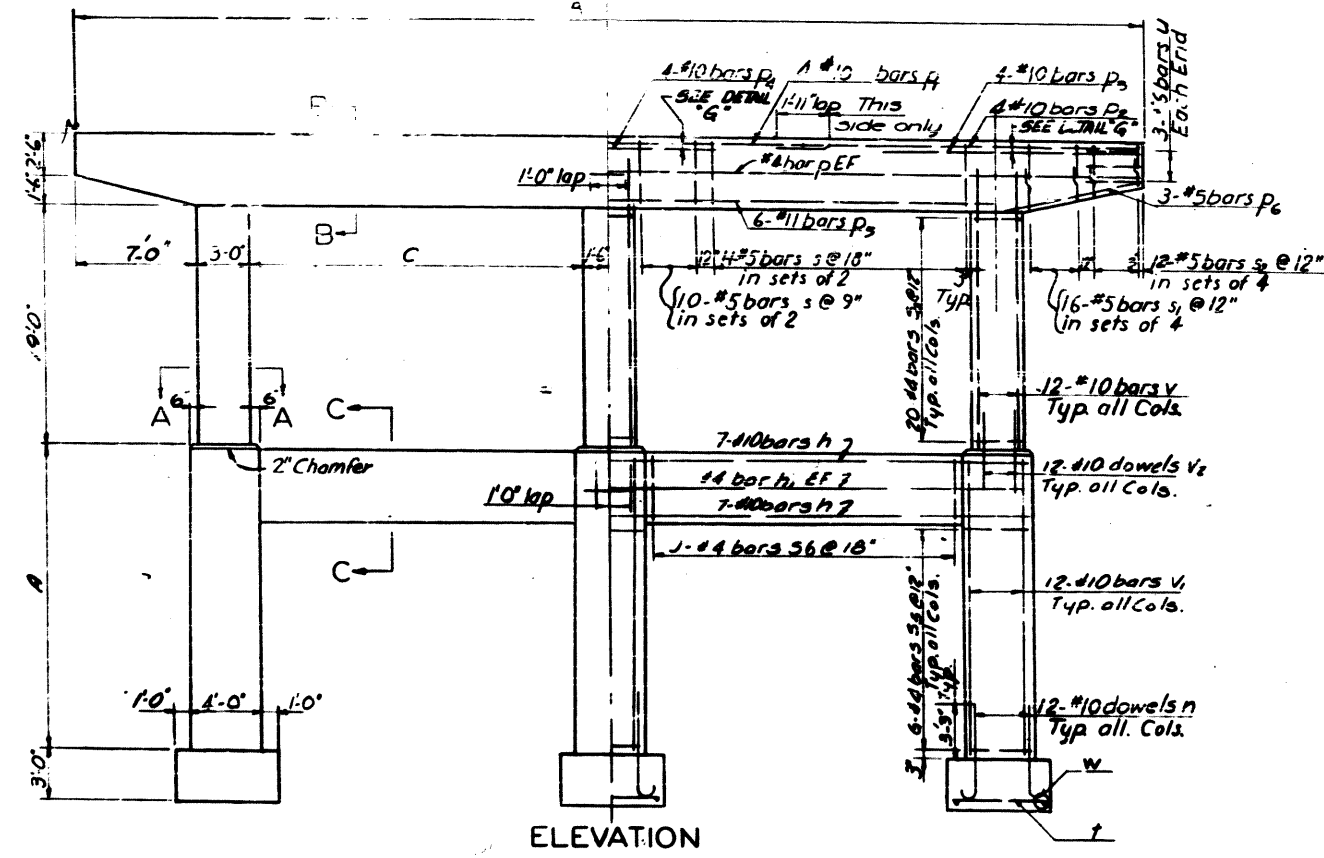
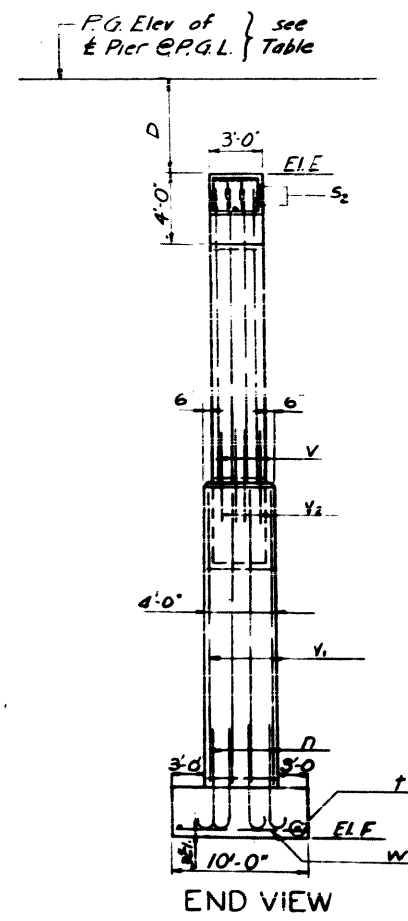
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	42
PROJECT		I-80-4(13)/33		

SHEET NO. 23
42 SHEETS



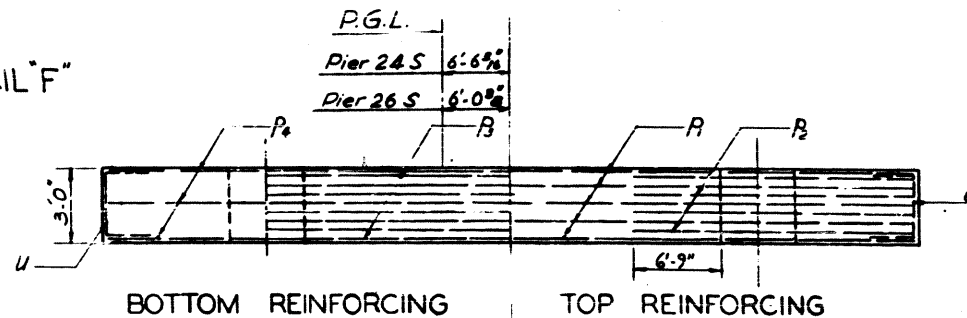
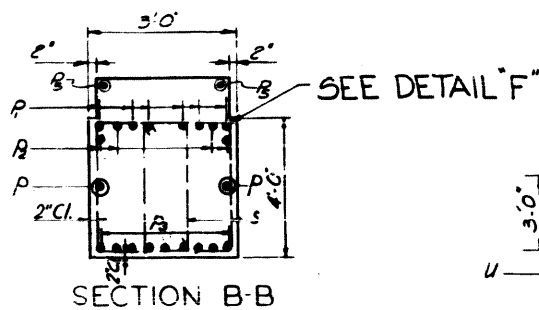
Pier No.	Dimensions				Elevations		NO. Of Bars				Station & Pier @ P.G.L. Elev.	P.G. Elev.
	A	B	C	D	E	F	G	H	I	J		
22N	23'-10"	59'-4"	18'-2"	5'-7"	563.59	513.7	21	12	20	399+65.50	569.17	
23N	20'-10"	60'-6"	18'-9"	5'-8"	561.31	514.4	18	13	22	400+37.44	567.01	
24N	19'-0"	61'-7"	19'-3"	5'-7"	559.66	514.6	16	13	22	400+94.14	565.31	
26N	14'-11"	64'-3"	20'-7"	5'-6"	555.67	514.7	12	14	24	402+30.65	561.22	



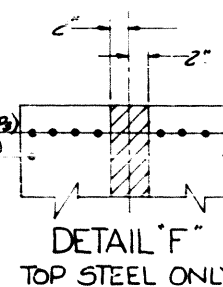
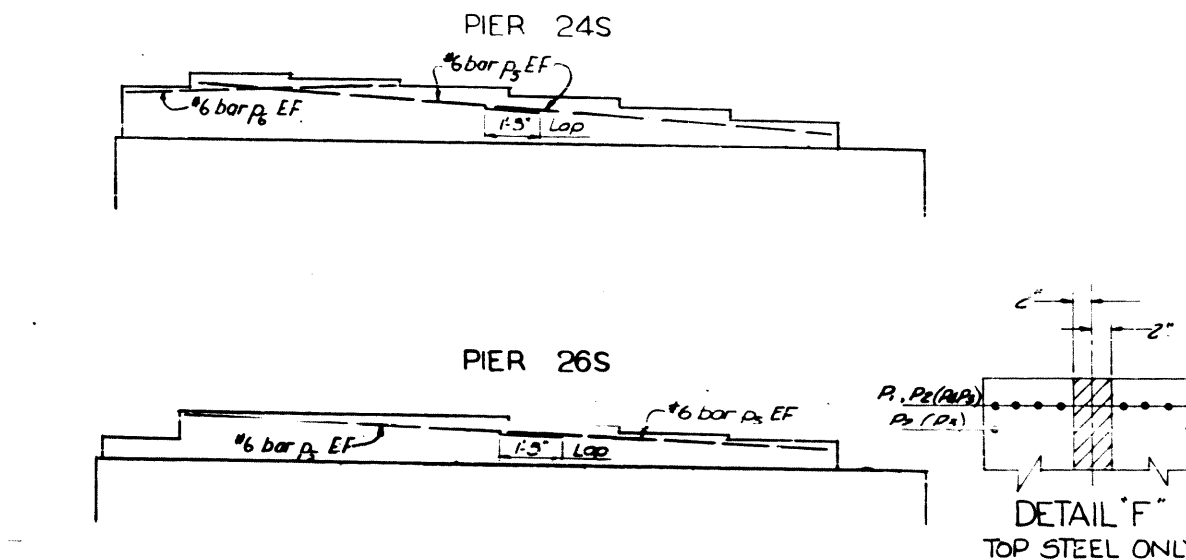
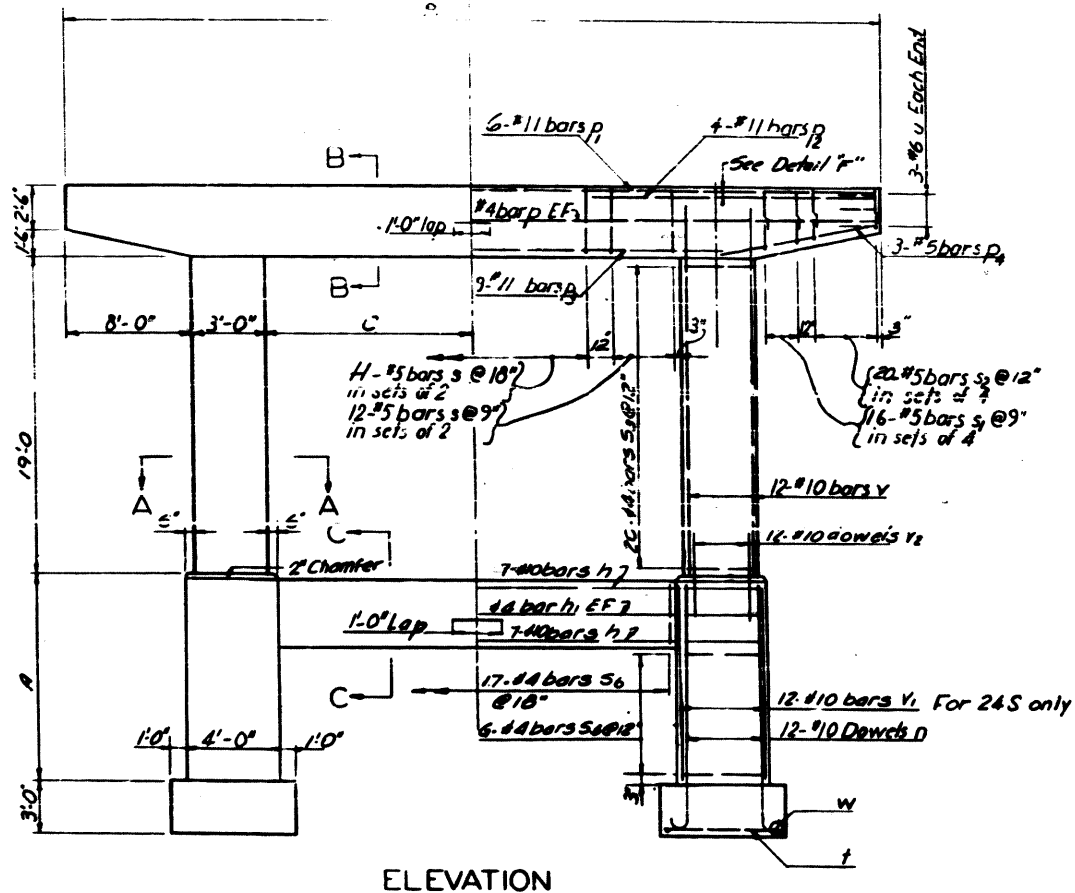
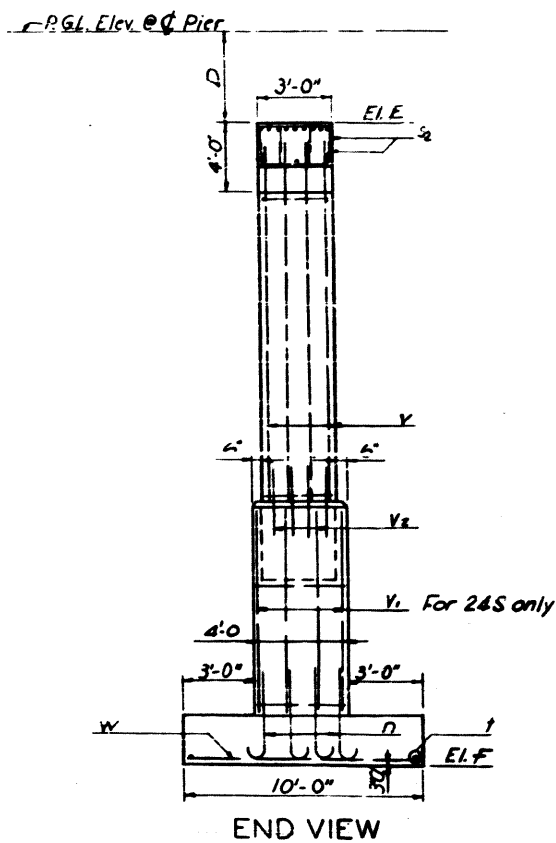
DESIGNED	F.M.
CHECKED	F.T.G.
DRAWN	Pvd.M. R.S.
CHECKED	H.C.M. I.G.

NOTES:
For Bills of materials see sh. 60
For Pad Elevations and Dimensions, see Key Plans, sh. 28
For Bar Lists, see sh. 58 & 59
For Location Plan, see sh. 25
Maximum Foundation Pressure = 4 1/2 t.

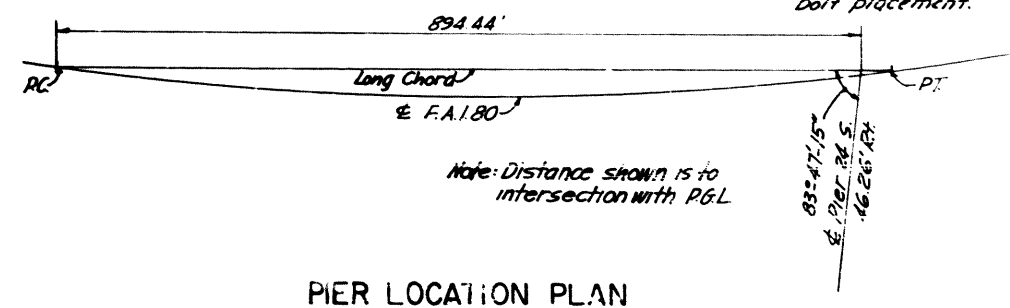
PIERS 22N, 23N, 24N & 26N
FAI-80 OVER DES PLAINES RIVER
STA 387 + 94.50
FAI ROUTE 80 SECTION 99-38
Scale: No Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK N.Y. CRYSTAL LAKE, ILL.



Pier NO.	Dimension				Elevation		No. of Bar		Station & Pier @ P.G.L.	PG Elev.
	A	B	C	D	E	F	G	H		
24S	13'-7 1/2"	46'-0"	12'-4"	5'-7 3/8"	560.09	520.5	10	24	400+81.19	565.70
26S	7'-4 1/2"	47'-9"	12'-10 1/2"	5'-5 1/8"	556.10	522.8	4	22	402+17.35	561.62



Note: Keep reinforcement free of shaded area.
Extreme care is to be taken in placing reinforcing in order to avoid interference with holes to be drilled later for anchor bolt placement.

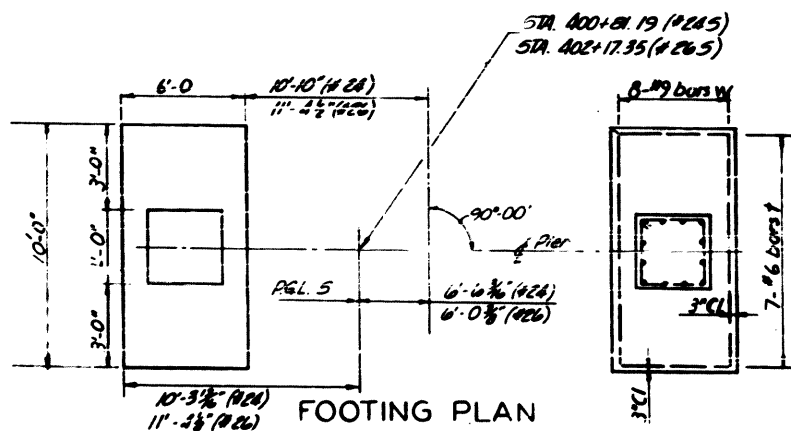
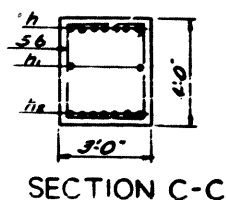


Notes:
For Bill of Materials see sh. 60
For Pad Elevations and Dimensions, see Key Plans, sh. 28
For Bar Lists see sh. 58 & 59
For Location Plan, see sh. 25
Maximum Foundation Pressure - 5 1/2%.

PIERS 24 S & 26 S
FAI-80 OVER DES PLAINES RIVER

STA 387 + 94.50
FAI ROUTE 80 SECTION 99-38
Scale: No Scale
PROJECT I-80-4(13)133
WILL COUNTY
Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

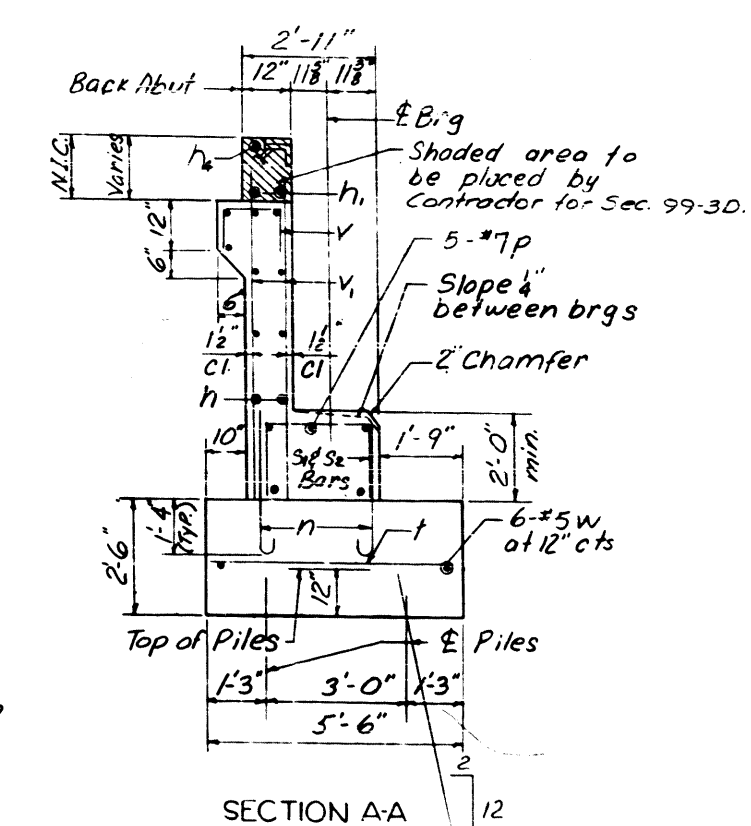
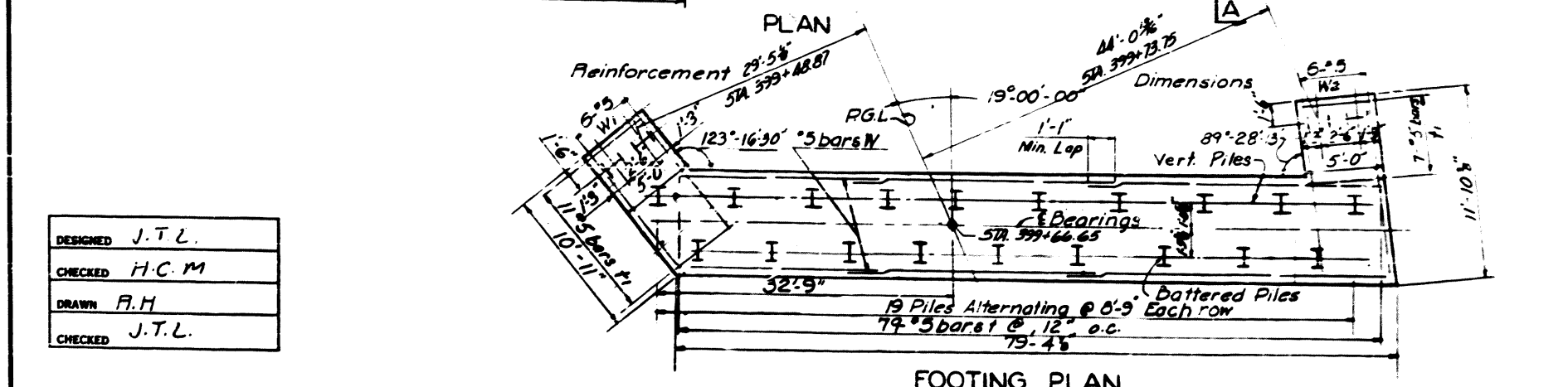
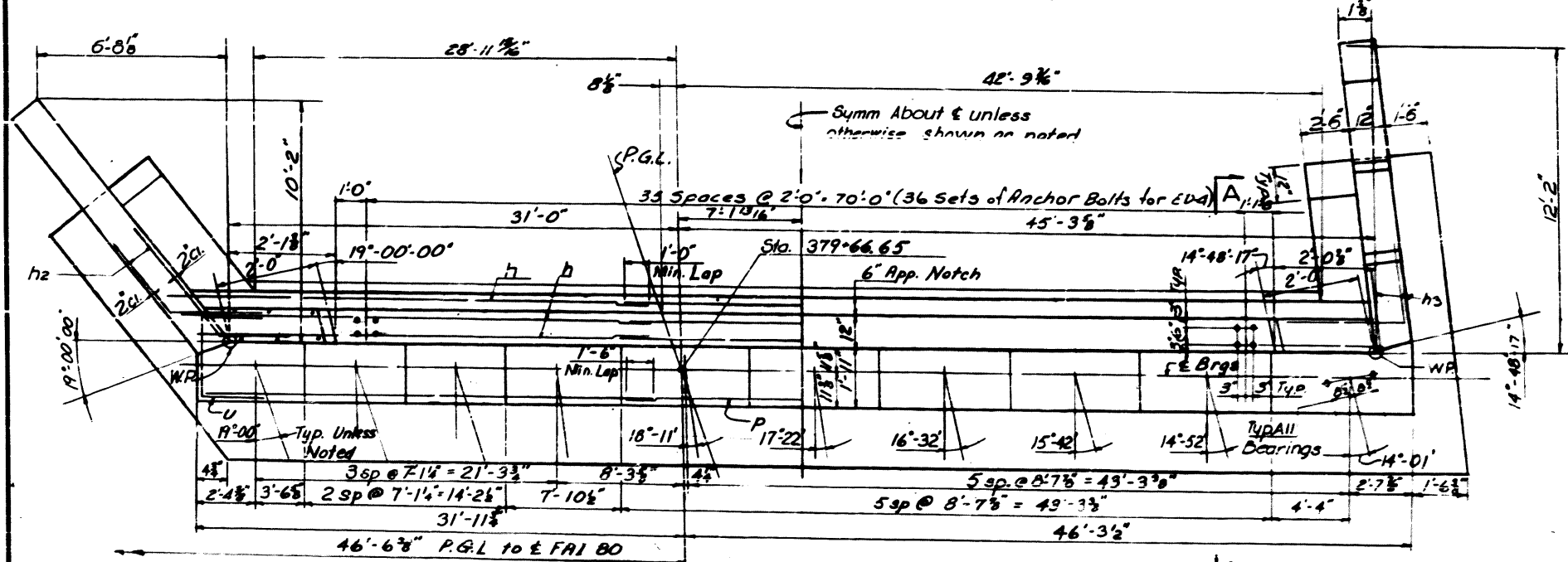
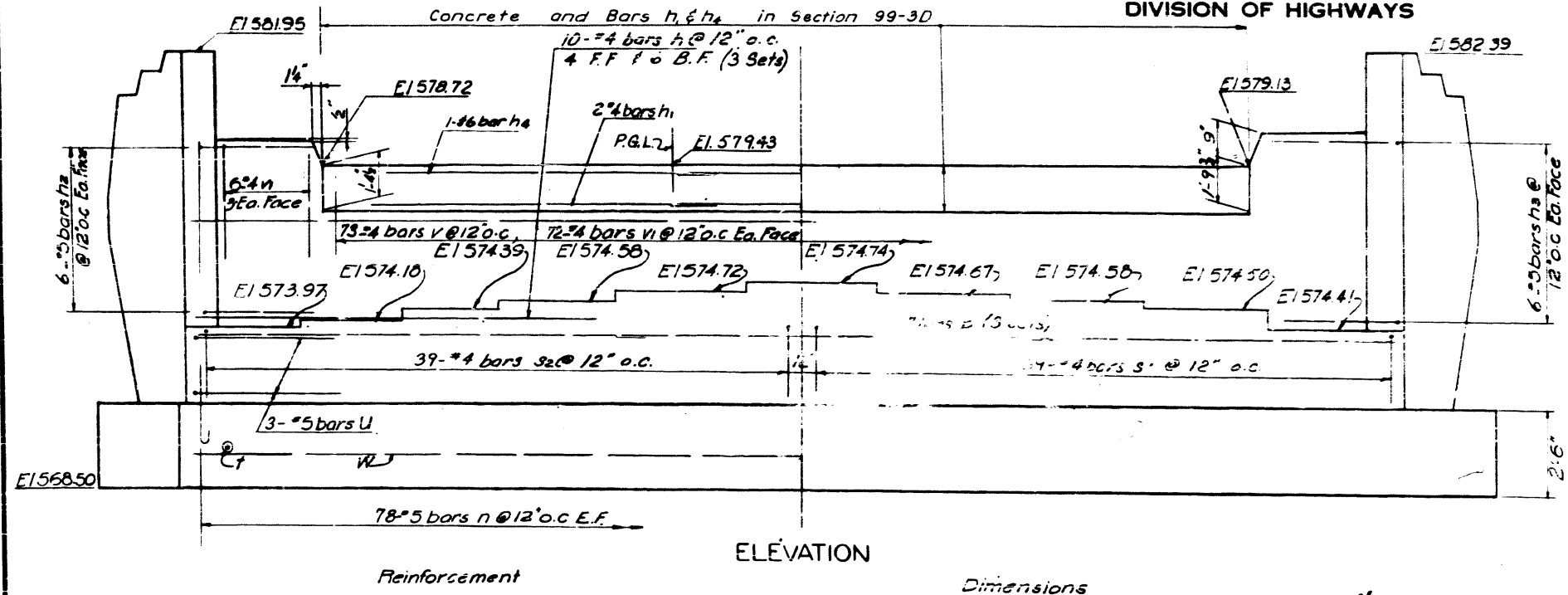
DESIGNED	F.T.G.
CHECKED	R.H.
DRAWN	Prd.M. R.S.
CHECKED	H.C.M. I.G.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	47
FED. ROAD DIST. NO. 7	ILLINOIS PROJECT	I-80-4(15)133		

SHEET NO. 28
42 SHEETS



PILE DATA
Type - 10BP42
Capacity - 37 Ton Max.
Est. Length - 30 Feet
No. Req'd - 22
One Test Pile

Note:
For Bar List, see sh 59
For Bill of Materials, see sh 60
For Abutment Details, see sh 51
All material to be furnished and placed by Contractor for Sec. 99-38, except as noted.
Expansion Bar ED-4, including anchor bolts are in Section 99-3D

DESIGNED	J.T.L.
CHECKED	H.C.M.
DRAWN	F.H.
CHECKED	J.T.L.

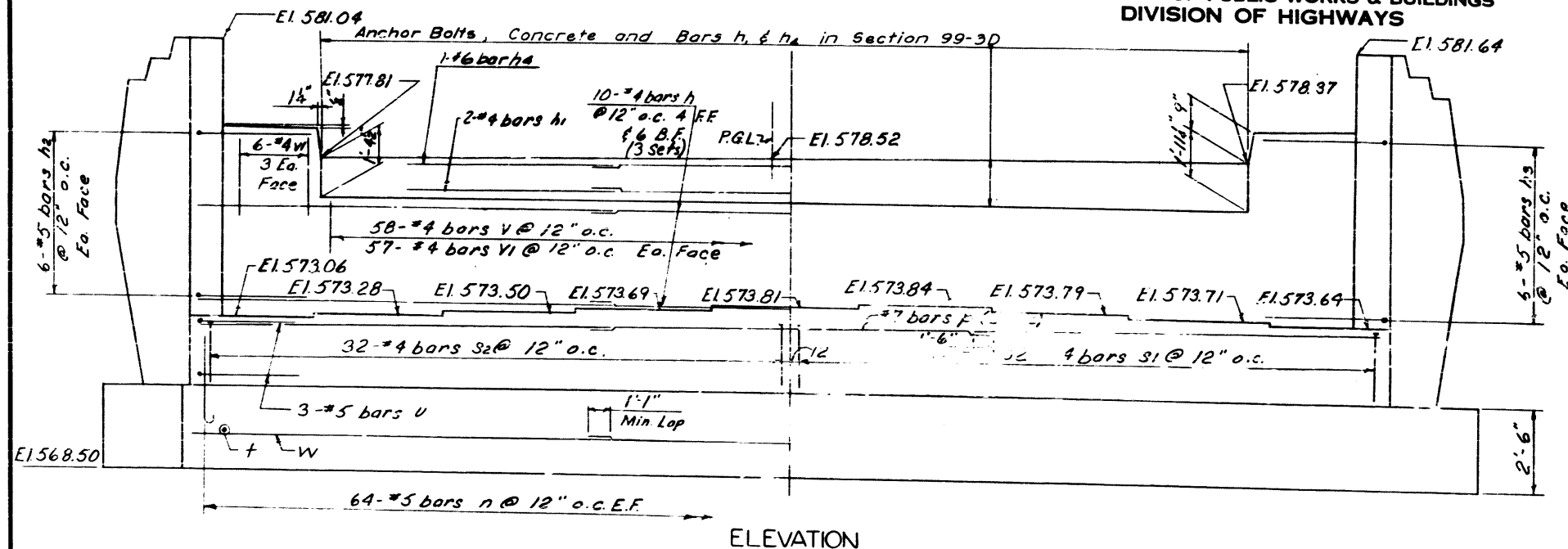
NORTHWEST ABUTMENT
FAI-80 OVER DES PLAINES RIVER
STA. 387 + 94.50
FAI ROUTE 80
SECTION 99-38
Scale: NO Scale
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

PROJECT I-80-4(15)133
WILL COUNTY
Date: Jan 25, 1961

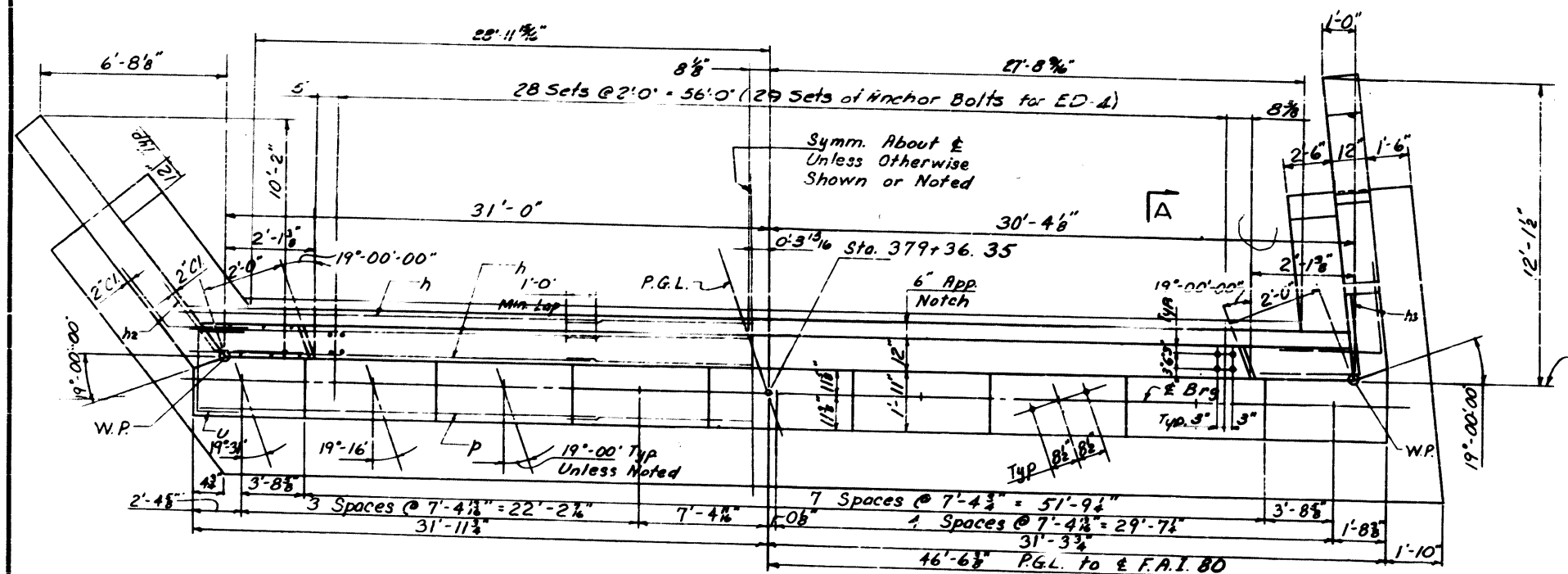
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-3B	WILL	71	48
PER ROAD DIST. NO. 7		ALLOTTED	PROJECT	I-80-4(13)133

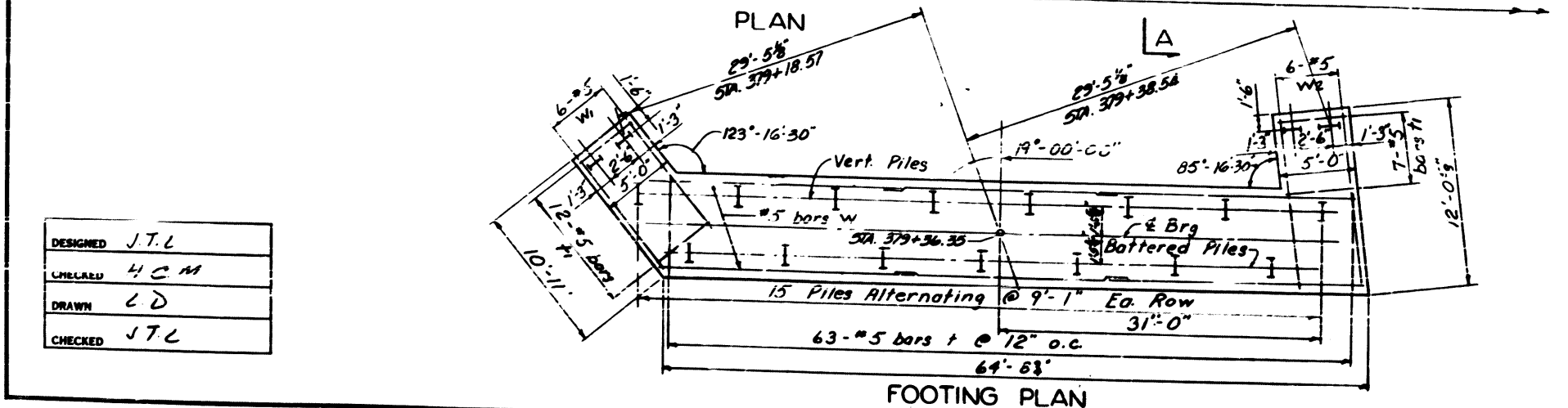
SHEET NO. 29
42 SHEETS



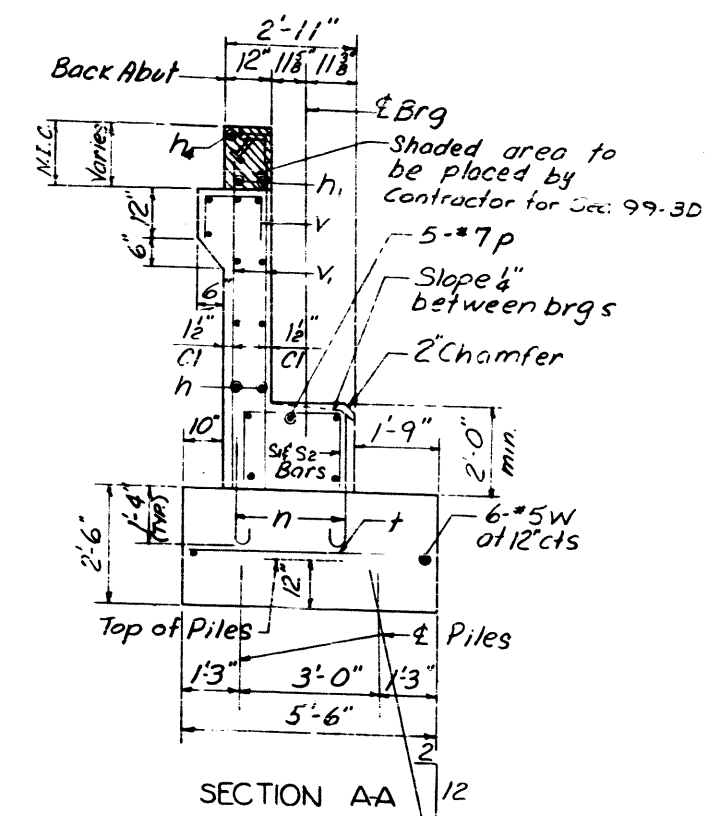
ELEVATION



PLAN



FOOTING PLAN



SECTION AA

PILE DATA
Type - 10BP42
Capacity - 37 Ton Max.
Est. Length - 30 Feet
No. Req'd - 19

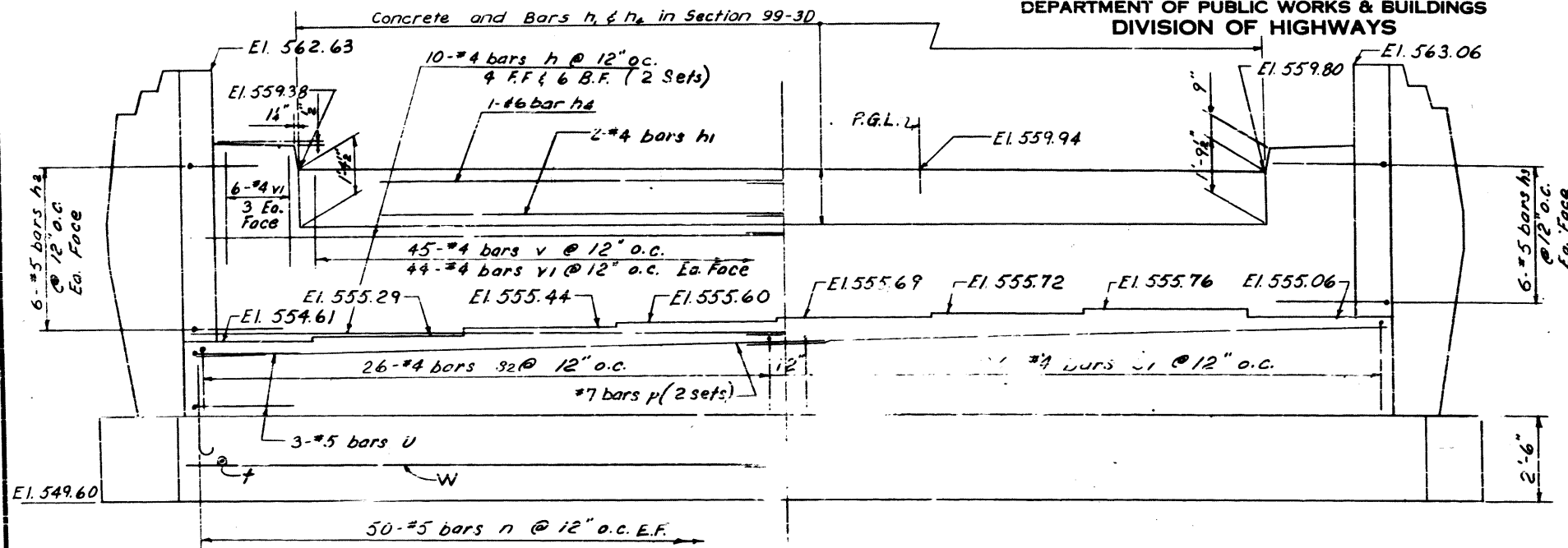
Note:
For Bar Lists, see sh. 53
For Bill of Materials, see sh. 60
For Abutment Details, see sh. 51
All material to be furnished and placed by Contractor for Sec. 99-3B, except as noted.
Expansion Dam ED-4 including anchor bolts are in Section 99-3D

DESIGNED	J.T.L.
CHECKED	H.C.M.
DRAWN	L.D.
CHECKED	J.T.L.

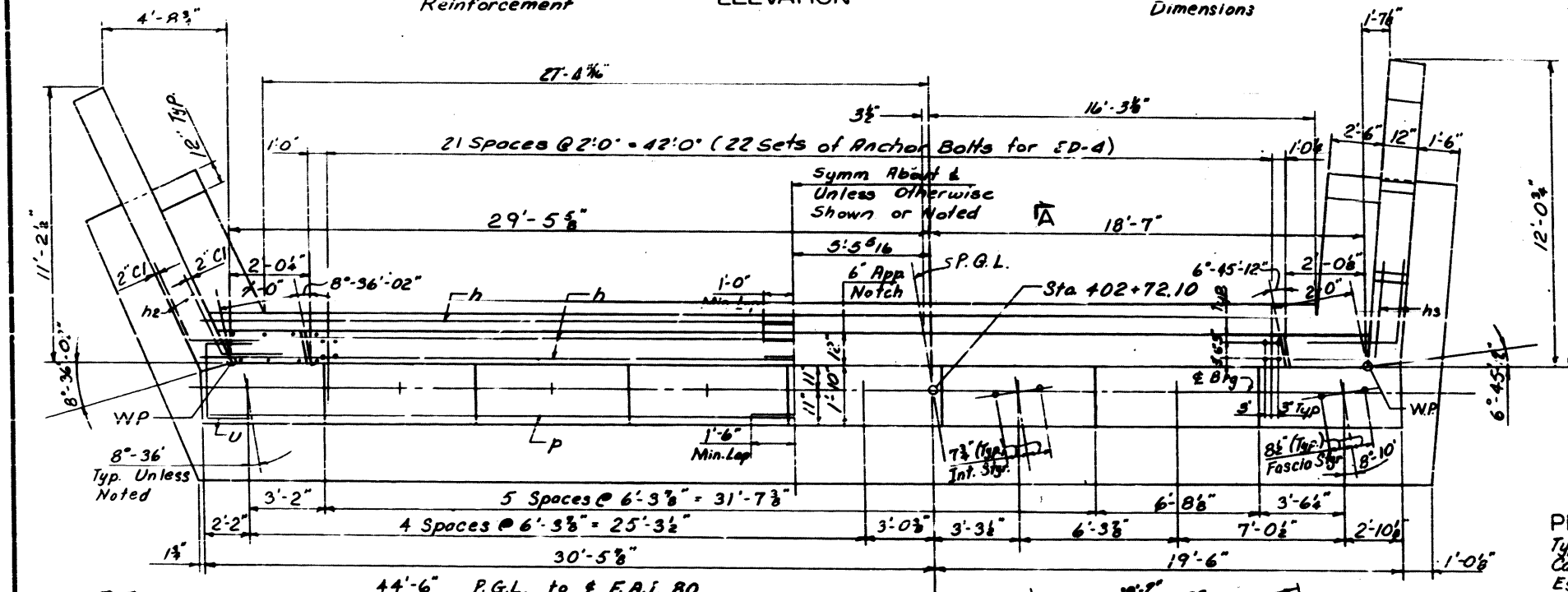
SOUTHWEST ABUTMENT
FAI-80 OVER DES PLAINES RIVER
STA. 387 + 94.50
FAI ROUTE 80 SECTION 99 3B PROJECT I-80-4(13)133
WILL COUNTY
Scale: No Scale Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

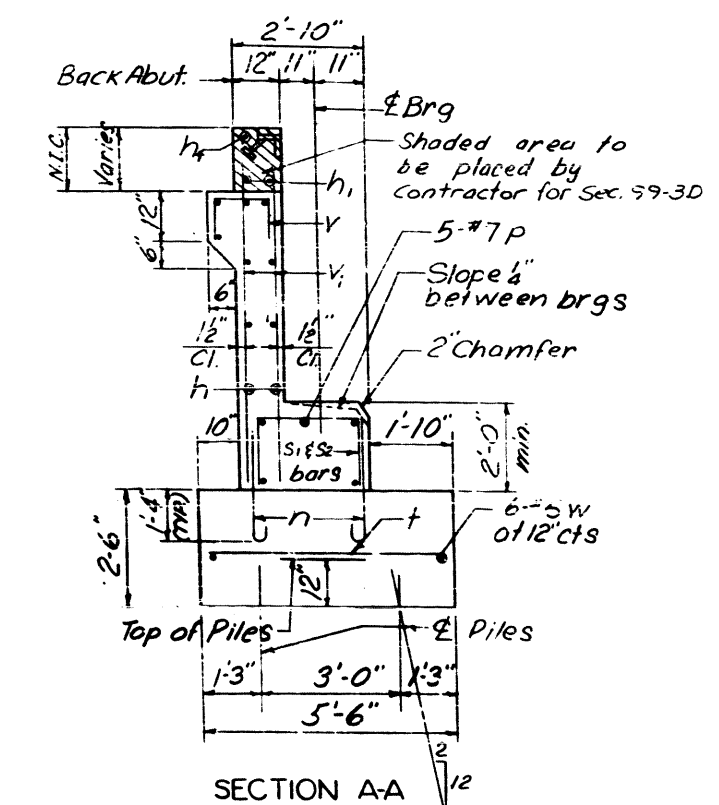
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 31 42 SHEETS
FAI-80	99-38	WILL	71	50	
P.L.A. ROAD DIST. NO. 7		W. NUMBER	PROJECT: I-80-4(13)/53		



Reinforcement ELEVATION Dimensions



Reinforcement PLAN Dimensions



SECTION A-A

PILE DATA
Type-10BP42
Capacity-37 Ton Max.
Est. Length-28 Feet
No Req'd-15
One Test Pile

Note:
For Bar List, see sh 59
For Bill of Material, see sh 50
For Abutment Details, see sh 51
All material to be furnished and placed by Contractor for 99-38, except as noted
Expansion Dam EP-4 including anchor bolts are in Section 99-3D

SOUTHEAST ABUTMENT
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50

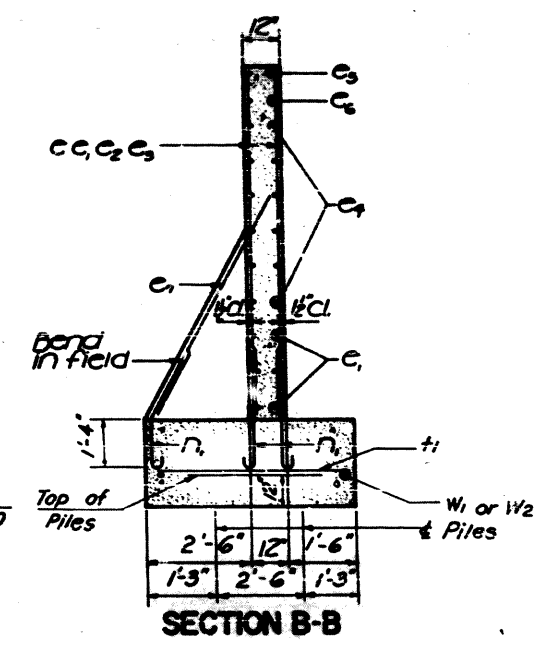
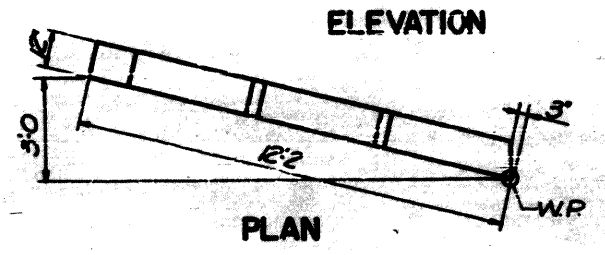
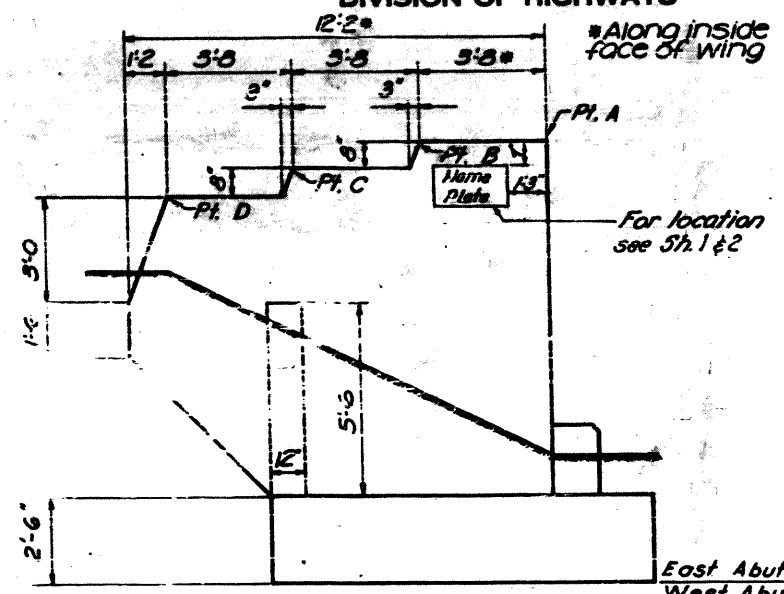
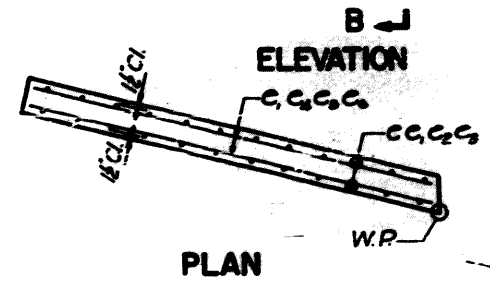
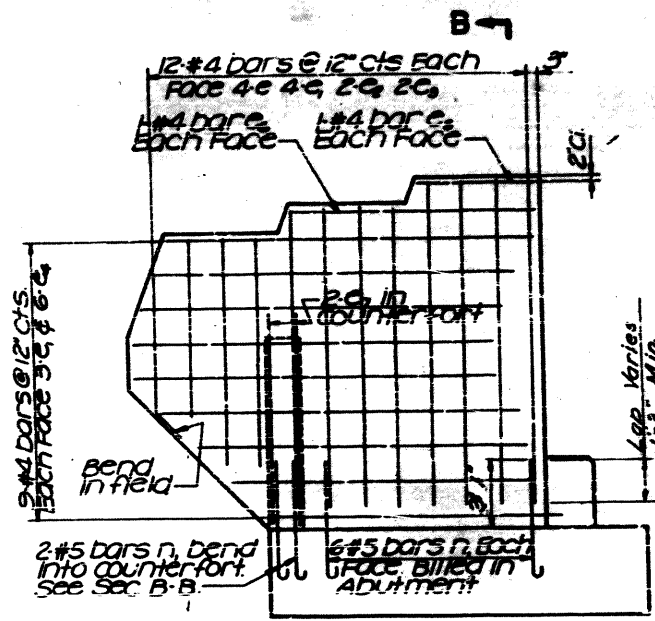
FAI ROUTE 80 SECTION 99-38 PROJECT I-80-4(13)/53
WILL COUNTY
Scale: NO Scale Date: Jan 25, 1961

BLAUVELT ENGINEERING CO
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

DESIGNED	J.T.L.
CHECKED	H.C.M.
DRAWN	L.D.
CHECKED	J.T.L.

FOOTING PLAN

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS



ONE END POST
BILL OF MATERIAL *

Bar	No.	Size	Length	Shape
C	8	#4	9-3	—
C	14	#4	8-6	—
C	4	#4	6-6	—
C	4	#4	5-0	—
C	12	#4	10-6	—
C	2	#4	3-8	—
C	2	#4	7-0	—
C	2	#5	7-0	—
Reinforcement bars lbs 270				

* Concrete, Reinforcement Bars and Piles are included in Bills of Material for Abutments, s.h. 41

East Abuts. El. 549.60
West Abuts. El. 568.50

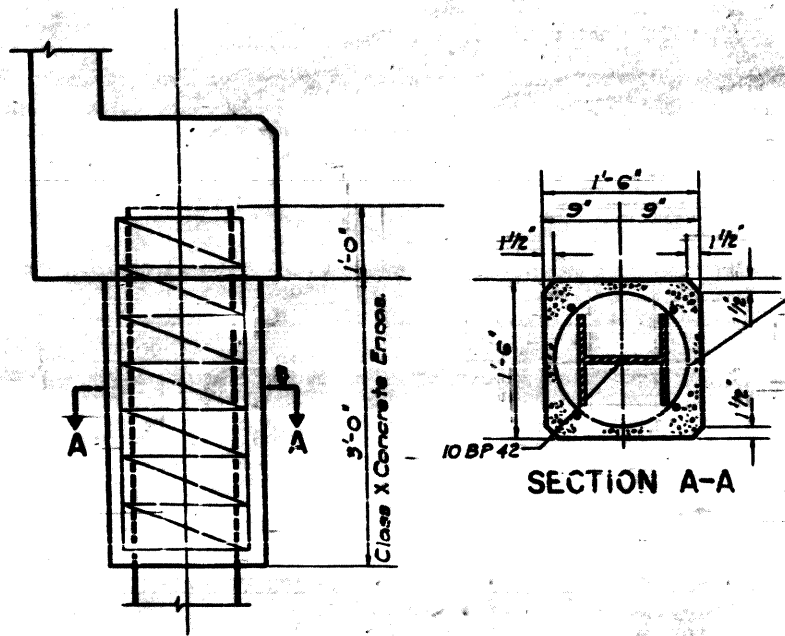
Pt.	North. West Abut.		South West Abut.		North East Abut.		South East Abut.	
	South W.W.	North W.W.	South W.W.	North W.W.	South W.W.	North W.W.	South W.W.	North W.W.
A	581.95	582.39	581.04	581.64	562.49	562.09	563.06	562.63
B	581.84	582.28	580.93	581.53	562.38	561.98	562.95	562.52
C	581.08	581.53	580.17	580.77	561.62	561.22	562.19	561.76
D	580.32	580.77	579.41	580.01	560.87	560.46	561.43	561.00

STATION 387+94.50
BUILT 196 BY
STATE OF ILLINOIS
F.A.I. RT. 80 SEC. 99-38-D-E-F-P
F.A. PROJ. 1-80-4
LOADING H20-S16 SALT

See Standard 2113

LETTERING FOR NAME PLATES

DESIGNED	JTL
CHECKED	HCM
DRAWN	SEF
CHECKED	HCM



ENCASEMENT DETAIL
FOR STEEL H PILES

15" Dia. spiral #2 Wire, 6" pitch.
2 extra turns top and bottom.
4-#4 tie bars. The cost of Class X Concrete Encasement and Reinforcement is incidental to the cost of furnishing piles.

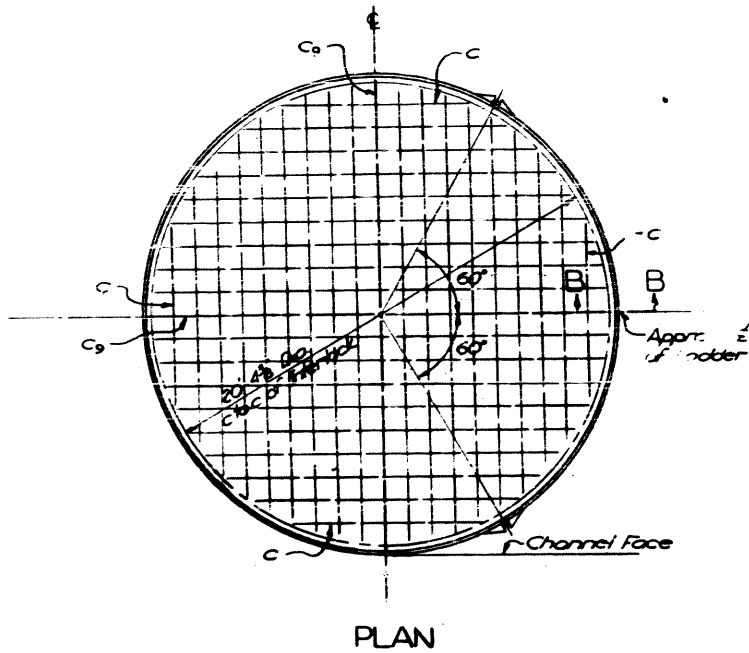
Note:
Driving and bearing ends of pile shall be cut square.

ABUTMENT DETAILS
FAI-80 OVER DES PLAINES RIVER

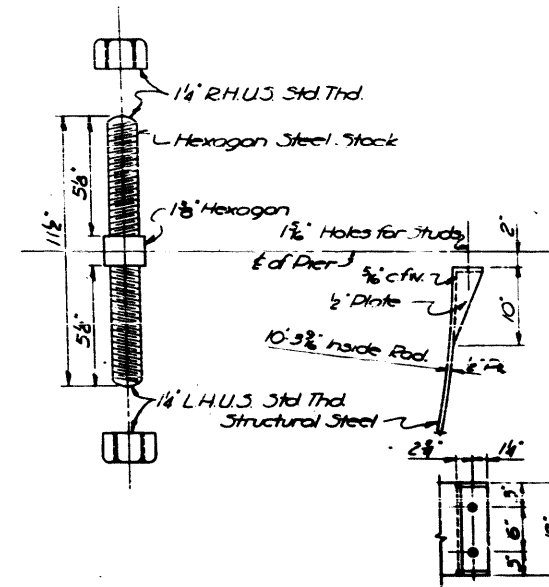
STA. 387 + 94.50
F A I ROUTE 80 PROJECT I-80-4(13) 133
SECTION 99-38 WILL COUNTY
Scale: No Scale Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.Y. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

DESIGN NO.	SECTION	COUNTY	SHEET NO.	TOTAL SHEETS
FAI-80	99-38	WILL	71	52
PROJECT: I-80-403/33		SHEET NO. 33 42 SHEETS		



PLAN

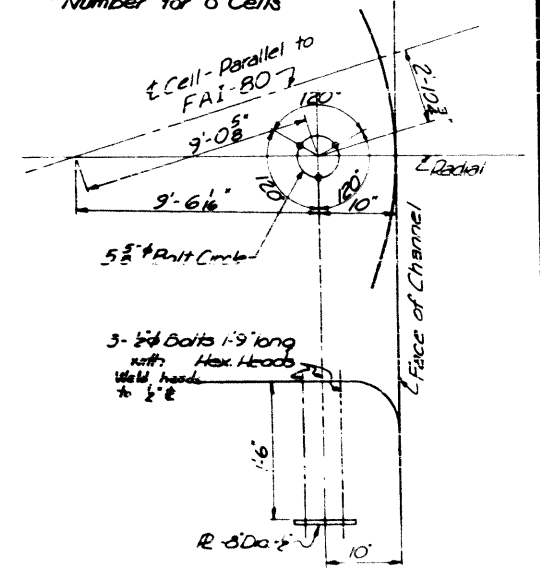


DETAIL OF STEEL BAND

BAR LIST

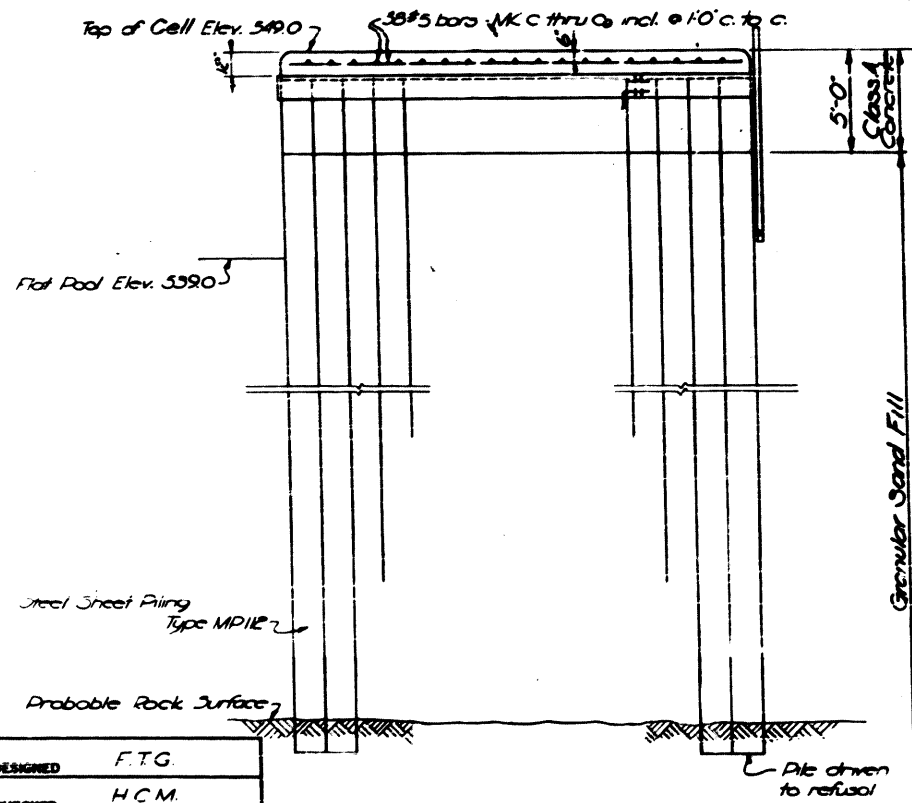
Bar	No.	Size	Length	Shape
C	24	#5	8.8'	---
C	24	#5	2.0'	---
C ₂	24	#5	14.3'	---
C ₃	24	#5	6.0'	---
C ₄	24	#5	17.4'	---
C ₅	24	#5	13.4'	---
C ₆	24	#5	19.1'	---
C ₇	24	#5	19.8'	---
C ₈	24	#5	20.0'	---
C ₉	12	#5	20.0'	---

* Number for 6 Cells

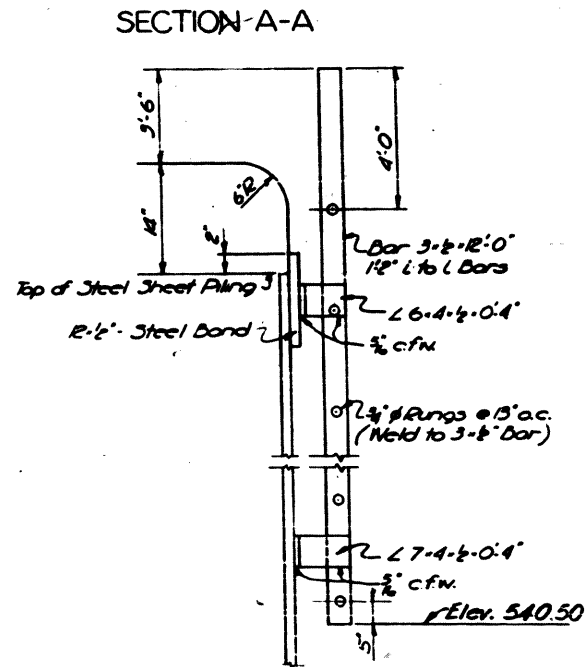


ANCHOR BOLT ASSEMBLY

For Navigation Lights on Cells Nos. 1, 3, 4, 6
Note: See Special Provisions for Navigation Lights.

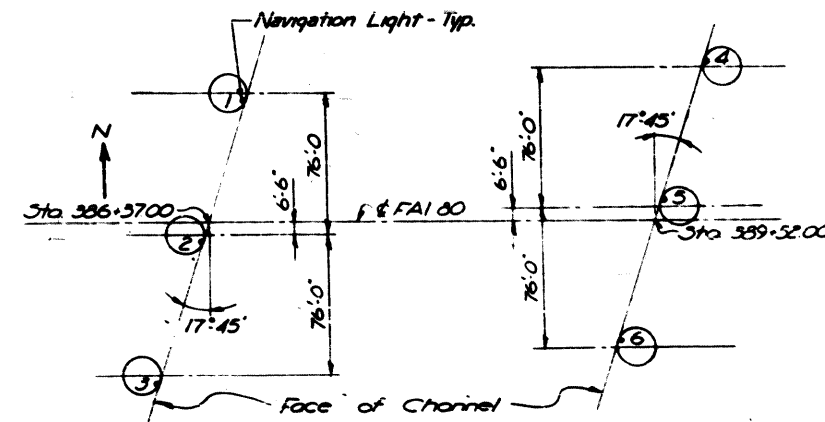


ELEVATION



SECTION A-A

Ladder to be furnished for Cells Nos. 1, 3, 4, 6



LOCATION PLAN

DESIGNED	FTG.
CHECKED	H.C.M.
DRAWN	FTG.
CHECKED	H.C.M.

PIER PROTECTION CELLS
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50

FAI ROUTE 80 PROJECT I-80-403/33
SECTION 99-38 WILL COUNTY
Scale: No Scale Date: Jan. 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

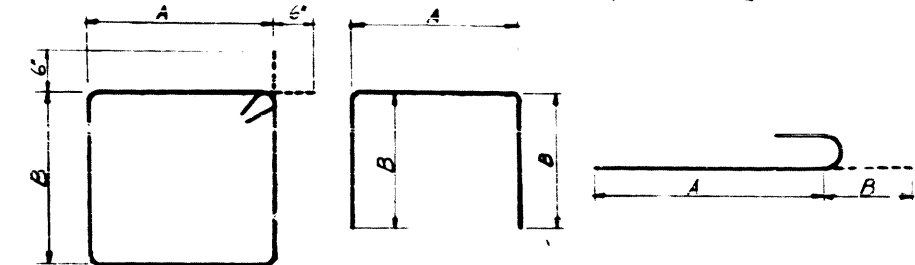
PIER NO 1N				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	6	23'-10"	
P1	#10	6	30'-11"	
P2	#10	6	35'-10"	
P3	#7	3	20'-0"	
P4	#10	4	53'-9"	
P5	#5	6	7'-10"	
P6	#6	2	23'-6"	
P7	#6	4	21'-0"	
P8	#9	8	13'-1"	
V	#11	64	22'-6"	
S	#5	112	12'-6"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	76	12'-8"	
S4	#5	39	28'-7"	
h	#5	44	29'-4"	
h1	#8	8	37'-2"	
t	#6	28	5'-6"	
w	#9	32	9'-6"	
U	#6	6	8'-11"	
n	#11	64	16'-10"	
n1	#11	64	7'-2"	

PIER NO 2N				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	31'-6"	
P1	#11	5	44'-0"	
P2	#11	8	13'-6"	
P3	#8	4	10'-3"	
P4	#9	8	11'-0"	
P5	#9	5	45'-4"	
P6	#5	6	8'-4"	
P7	#11	5	20'-5"	
V	#11	48	24'-10"	
S	#5	40	14'-2"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	63	12'-8"	
S4	#5	36	37'-5"	
h	#5	64	25'-2"	
h1	#8	8	49'-3"	
t	#6	21	5'-6"	
w	#9	24	9'-6"	
U	#6	6	8'-11"	
n	#11	48	21'-3"	
n1	#11	48	7'-2"	

PIER NO 3N				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	28'-6"	
P1	#11	5	38'-0"	
P2	#11	8	13'-6"	
P3	#8	8	11'-0"	
P4	#9	5	39'-3"	
P5	#5	6	8'-4"	
P7	#11	5	20'-2"	
V	#10	48	17'-5"	
V1	#10	20	18'-5"	
V2	#10	28	6'-6"	
S	#5	32	14'-2"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	51	12'-8"	
S4	#5	34	22'-8"	
S5	#4	45	16'-8"	
S6	#4	34	14'-2"	
h	#5	32	22'-7"	
h1	#8	8	44'-2"	
h2	#11	12	42'-5"	
h3	#4	4	21'-9"	
t	#6	21	5'-6"	
w	#9	24	9'-6"	
U	#6	6	8'-11"	
n	#10	48	19'-1"	

PIER NO 4N				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	25'-7"	
P1	#11	4	50'-2"	
P2	#11	6	14'-9"	
P3	#10	4	54'-5"	
P4	#5	6	7'-10"	
h	#11	12	37'-8"	
h1	#4	4	19'-4"	
V	#10	36	19'-3"	
V1	#10	36	17'-10"	
S7	#4	18	14'-2"	
S	#5	20	14'-2"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	51	12'-8"	
S4	#4	26	20'-8"	
S5	#4	45	16'-8"	
t	#6	24	5'-6"	
w	#9	21	10'-6"	
U	#6	6	8'-11"	
n	#10	48	15'-8"	
n1	#10	72	6'-6"	

Pier No.	IN	1S	2N	2S	3N	3S	4N	4S
S	A	2'-1"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"
S	B	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"
S1	A	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"
S1	B	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"
S2	A	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"
S2	B	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"
S3	A	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"
S3	B	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"
S4	A	3'-11"	3'-11"	3'-11"	3'-11"	4'-11"	4'-11"	4'-11"
S4	B	12'-4"	12'-3 1/2"	16'-9"	15'-3"	8'-10 1/2"	4'-10 1/2"	4'-10 1/2"
U	A	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"	2'-11"
U	B	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"
n	A	15'-3"	15'-3"	19'-8"	18'-2"	11'-8"	7'-8"	14'-3"
n	B	1'-7"	1'-7"	1'-7"	1'-7"	1'-5"	1'-5"	1'-5"
S5	A					3'-11"	3'-11"	4'-11"
S5	B					3'-11"	3'-11"	4'-11"
S6	A					2'-11"	2'-11"	3'-11"
S6	B					3'-8"	3'-8"	3'-11"
S7	A						2'-11"	2'-11"
S7	B						3'-8"	3'-8"



S, S3, S5, S6, S7 S1, S2, S4, U n.

PIER NO 1S				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	31'-1"	
P1	#11	5	43'-2"	
P2	#11	8	13'-6"	
P3	#8	4	10'-3"	
P4	#9	8	11'-0"	
P5	#9	5	44'-6"	
P6	#5	6	8'-4"	
P7	#11	5	20'-5"	
V	#11	48	21'-6"	
S	#5	38	14'-2"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	54	12'-8"	
S4	#5	36	28'-6"	
h	#5	44	24'-9"	
h1	#8	8	48'-4"	
t	#6	21	5'-6"	
w	#9	24	9'-6"	
U	#6	6	8'-11"	
n	#11	48	16'-10"	
n1	#11	48	7'-2"	

PIER NO 2S				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	30'-8"	
P1	#11	5	42'-4"	
P2	#11	8	13'-6"	
P3	#8	4	10'-3"	
P4	#9	8	11'-0"	
P5	#9	5	43'-9"	
P6	#5	6	8'-4"	
P7	#11	5	20'-5"	
V	#11	48	23'-10"	
S	#5	38	14'-2"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	60	12'-8"	
S4	#5	34	34'-5"	
h	#5	56	24'-4"	
h1	#8	8	47'-8"	
t	#6	21	5'-6"	
w	#9	24	9'-6"	
U	#6	6	8'-11"	
n	#11	48	17'-9"	
n1	#11	48	7'-2"	

PIER NO 3S				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	30'-7"	
P1	#11	5	42'-2"	
P2	#11	8	13'-6"	
P3	#8	8	11'-0"	
P4	#9	5	43'-6"	
P5	#5	6	8'-4"	
P7	#11	5	20'-2"	
V	#10	48	19'-3"	
V1	#10	48	18'-5"	
V2	#10	96	6'-6"	
S	#5	38	14'-2"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	51	12'-8"	
S4	#5	38	14'-8"	
S5	#4	45	16'-8"	
S6	#4	38	14'-2"	
h	#5	16	24'-9"	
h1	#8	8	48'-5"	
h2	#11	12	46'-9"	
h3	#4	4	23'-11"	
t	#6	21	5'-6"	
w	#9	24	9'-6"	
U	#6	6	8'-11"	
n	#10	48	9'-1"	

PIER NO 4S				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	26'-5"	
P1	#11	4	51'-10"	
P2	#11	6	14'-9"	
P3	#10	4	56'-7"	
P4	#5	6	7'-10"	
h	#11	12	39'-4"	
h1	#4	4	20'-2"	
V	#10	36	19'-3"	
V1	#10	36	17'-10"	
S7	#4	20	14'-2"	
S	#5	22	14'-2"	
S1	#5	16	7'-8"	
S2	#5	12	6'-11"	
S3	#4	51	12'-8"	
S4	#4	27	20'-8"	
S5	#4	45	16'-8"	
t	#6	24	5'-6"	
w	#9	21	10'-6"	
U	#6	6	8'-11"	
n	#10	48	12'-8"	
n1	#10	72	6'-6"	

Note:
Fabricator shall identify bars by location
and bar mark, i.e. Pier 1S mk p1.

DESIGNED	FTG
CHECKED	JTL
DRAWN	DvdM
CHECKED	HCM

BAR LISTS-PIERS 1 - 4
FAI-80 OVER DES PLAINES RIVER
STA. 387 + 94.50
FAI ROUTE 80
SECTION 99-38
WILL COUNTY
PROJECT 7-80-4(3)133
Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PIER NO. 5N				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	23'-11"	
P1	#11	6	46'-10"	
P2	#10	10	17'-5"	
P3	#11	9	28'-3"	
P4	#5	6	9'-4"	
V	#11	32	20'-7"	
V1	#11	32	21'-0"	
S	#5	46	16'-2"	
S1	#5	40	8'-8"	
S2	#5	32	7'-11"	
S3	#4	36	16'-8"	
S4	#5	25	23'-1"	
S5	#4	24	16'-2"	
S6	#4	36	20'-8"	
T	#6	16	6'-3"	
W	#9	18	10'-6"	
n	#5	32	17'-7"	
h1	#9	8	34'-1"	
h2	#11	16	33'-2"	
h3	#4	4	17'-1"	
n	#11	32	13'-2"	
n1	#11	64	7'-2"	

PIER NO. 6N				
MARK	SIZE	NO	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	12	21'-3"	
P3	#10	8	53'-6"	
P4	#5	12	24'-9"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	80	51'-10"	
V2	#11	24	27'-0"	
S1	#5	116	11'-4"	
S2	#5	336	9'-0"	
S3	#5	992	3'-1"	
S4	#5	352	4'-10"	
n	#11	104	8'-5"	
W1	#8	22	9'-6"	
T1	#8	20	10'-6"	

PIER NO. 7N				
MARK	SIZE	NO	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	8	15'-9"	
P3	#10	8	53'-6"	
P4	#5	12	23'-6"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	176	35'-9"	
V2	#11	24	45'-0"	
n	#11	112	8'-10"	
n2	#8	86	7'-9"	
h1	#5	40	23'-6"	
h2	#8	43	33'-0"	
S4	#5	576	4'-2"	
S1	#5	116	11'-4"	
S2	#5	440	9'-8"	
S3	#5	1184	2'-8"	

PIER NO. 8N				
MARK	SIZE	NO	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	8	15'-9"	
P3	#10	8	53'-6"	
P4	#5	12	23'-6"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	176	34'-9"	
V2	#11	24	45'-0"	
W1	#8	36	31'-6"	
T1	#11	70	17'-6"	
n1	#11	112	8'-10"	
n2	#8	86	7'-9"	
h1	#5	40	23'-6"	
h2	#8	43	30'-8"	
S4	#5	576	4'-2"	
S1	#5	116	11'-4"	
S2	#5	424	9'-8"	
S3	#5	1120	2'-8"	

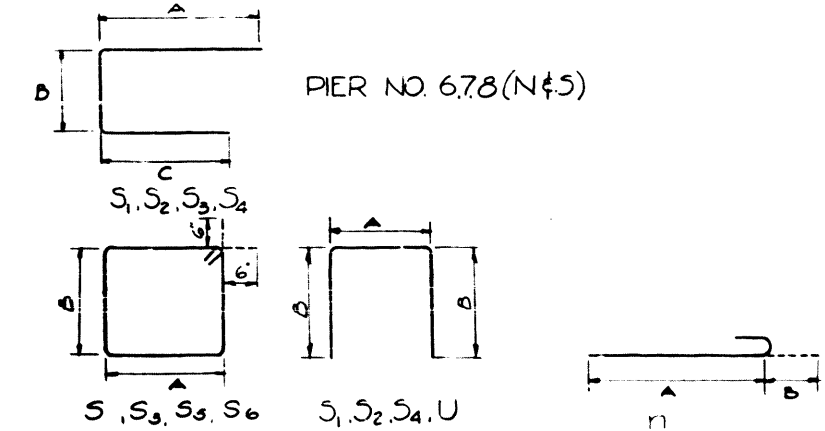
PIER NO	5N	5S	6N	6S	7N	7S	8N	8S
S1	A 2'-9"	2'-9"	4'-5"	4'-3"	4'-3"	4'-3"	4'-3"	4'-3"
	B 2'-4"	2'-4"	2'-10"	2'-10"	2'-10"	2'-10"	2'-10"	2'-10"
	C		4'-3"	4'-3"	4'-3"	4'-3"	4'-3"	4'-3"
S2	A 2'-9"	2'-9"	4'-6"	4'-6"	4'-10"	4'-10"	4'-10"	4'-10"
	B 2'-0"	2'-0"	4'-6"	4'-6"	4'-10"	4'-10"	4'-10"	4'-10"
	C		0'-0"	0'-0"	0'-0"	0'-0"	0'-0"	0'-0"
S3	A 3'-11"	3'-11"	1'-2"	1'-2"	1'-0"	1'-0"	1'-0"	1'-0"
	B 3'-11"	3'-11"	0'-9"	0'-9"	0'-8"	0'-8"	0'-8"	0'-8"
	C		1'-2"	1'-2"	1'-0"	1'-0"	1'-0"	1'-0"
S4	A 5'-11"	5'-11"	2'-4"	2'-4"	2'-0"	2'-0"	2'-0"	2'-0"
	B 2'-7"	2'-7"	0'-9"	0'-9"	0'-8"	0'-8"	0'-8"	0'-8"
	C		1'-9"	1'-9"	1'-6"	1'-6"	1'-6"	1'-6"
S	A 2'-9"	2'-9"						
	B 3'-8"	3'-8"						
U	A 3'-11"	3'-11"						
	B 3'-0"	3'-0"						
N	A 11'-7"	11'-11"	6'-10"	6'-10"				
	B 1'-7"	1'-7"	1'-7"	1'-7"				
S5	A 3'-11"	3'-11"						
	B 3'-8"	3'-8"						
S6	A 4'-11"	4'-11"						
	B 4'-11"	4'-11"						

PIER NO. 5S				
MARK	SIZE	NO	LENGTH	TYPE
P	#4	4	24'-6"	
P1	#11	6	48'-0"	
P2	#10	10	17'-5"	
P3	#11	9	29'-4"	
P4	#5	6	9'-4"	
V	#11	32	20'-7"	
V1	#11	32	20'-4"	
S	#5	46	15'-10"	
S1	#5	40	7'-6"	
S2	#5	32	6'-9"	
S3	#4	36	16'-8"	
S4	#5	26	23'-9"	
S5	#4	17	16'-2"	
S6	#4	34	20'-8"	
T	#6	16	6'-3"	
W	#9	18	10'-6"	
h	#5	32	18'-2"	
h1	#9	8	35'-3"	
h2	#11	16	34'-3"	
h3	#4	4	17'-8"	
n	#11	32	13'-6"	
n1	#11	64	7'-2"	

PIER NO. 6S				
MARK	SIZE	NO	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	12	21'-3"	
P3	#10	8	53'-6"	
P4	#5	12	24'-9"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	80	50'-9"	
V2	#11	24	27'-0"	
S1	#5	116	11'-4"	
S2	#5	328	9'-0"	
S3	#5	960	3'-1"	
S4	#5	352	4'-10"	
n	#11	104	8'-5"	
W1	#8	22	9'-6"	
T1	#8	20	10'-6"	

PIER NO. 7S				
MARK	SIZE	NO	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	8	15'-9"	
P3	#10	8	53'-6"	
P4	#5	12	23'-6"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	176	35'-2"	
V2	#11	24	45'-0"	
W1	#8	36	31'-6"	
T1	#11	70	17'-6"	
n1	#11	112	8'-10"	
n2	#8	86	7'-9"	
h1	#5	44	23'-6"	
h2	#8	43	31'-8"	
S1	#5	116	11'-4"	
S2	#5	432	9'-8"	
S3	#5	1182	2'-8"	
S4	#5	576	4'-2"	

PIER NO. 8S				
MARK	SIZE	NO	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	8	15'-9"	
P3	#10	8	53'-6"	
P4	#5	12	23'-6"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	176	37'-0"	
V2	#11	24	45'-0"	
W1	#8	36	31'-6"	
T1	#11	70	17'-6"	
n1	#11	112	8'-10"	
n2	#8	86	7'-9"	
h1	#5	44	23'-6"	
h2	#8	43	33'-0"	
S1	#5	116	11'-4"	
S2	#5	456	9'-8"	
S3	#5	1248	2'-8"	
S4	#5	576	4'-2"	



Note:
Fabricator shall identify bars by location
and bar mark, i.e. Pier 15 mk P1.

DESIGNED	F.T.G.
CHECKED	J.T.L.
DRAWN	P.v.d.M.
CHECKED	H.C.M.

* Drop P3 bars in field.
(Piers 6, 7 and 8)

BAR LISTS-PIERS 5-8
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50
FAI ROUTE 80 SECTION 99-38 PROJECT I-80-4(3)33
WILL COUNTY
Date: Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PIER NO. 9N				
MARK	SIZE	NO.	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	12	18'-3"	
P3	#10	8	53'-6"	
P4	#5	12	24'-9"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	88	43'-7"	
V2	#11	120	32'-0"	
S1	#5	116	11'-4"	
S2	#5	464	9'-6"	
S3	#5	1456	2'-8"	
S4	#5	400	4'-2"	
n	#11	120	8'-5"	
W1	#8	28	10'-0"	
T1	#8	22	13'-6"	

PIER NO. 10N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-11"	
P1	#11	7	46'-10"	
P2	#11	14	18'-4"	
P3	#11	7	27'-11"	
P4	#5	6	9'-6"	
P5	#6	4	19'-2"	
P6	#6	4	17'-0"	
h	#11	20	32'-10"	
h1	#4	4	17'-1"	
h2	#4	4	16'-11"	
V	#11	40	23'-10"	
V1	#11	40	22'-7"	
V2	#11	40	22'-0"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	40	21'-8"	
S6	#4	44	25'-8"	
s	#5	16	16'-8"	
S1	#5	16	8'-11"	
S2	#5	16	8'-2"	
S3	#4	40	17'-8"	
S4	#4	16	18'-8"	
u	#6	6	10'-2"	
n1	#11	80	7'-2"	
n	#11	40	7'-11"	

PIER NO. 11N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-2"	
P3	#11	7	27'-8"	
P4	#5	6	9'-6"	
P5	#6	4	19'-2"	
P6	#6	2	17'-0"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
h2	#4	4	16'-11"	
V	#11	40	23'-10"	
V1	#11	40	22'-7"	
V2	#11	40	21'-4"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	40	21'-8"	
S6	#4	44	25'-8"	
s	#5	16	16'-8"	
S1	#5	16	8'-11"	
S2	#5	16	8'-2"	
S3	#4	40	17'-8"	
S4	#4	16	18'-8"	
u	#6	6	10'-2"	
n1	#11	80	7'-2"	
n	#11	40	7'-11"	

PIER NO. 12N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-2"	
P3	#11	7	27'-8"	
P4	#5	6	9'-6"	
P5	#6	4	19'-2"	
P6	#6	2	17'-0"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
h2	#4	4	16'-11"	
V	#11	40	23'-10"	
V1	#11	40	22'-7"	
V2	#11	40	20'-6"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	40	21'-8"	
S6	#4	42	25'-8"	
s	#5	16	16'-8"	
S1	#5	16	8'-11"	
S2	#5	16	8'-2"	
S3	#4	40	17'-8"	
S4	#4	16	18'-8"	
u	#6	6	10'-2"	
n1	#11	80	7'-2"	
n	#11	40	7'-11"	

Pier No	9N	9S	10N	10S	11N	11S	12N	12S
S1	A	4'-3"	4'-3"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
	B	2'-10"	2'-10"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"
	C	4'-3"	4'-3"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
S2	A	4'-9"	4'-9"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
	B	4'-9"	4'-9"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"
	C	0'-0"	0'-0"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
S3	A	1'-0"	1'-0"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
	B	0'-8"	0'-8"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
	C	1'-0"	1'-0"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
S4	A	2'-0"	2'-0"	4'-8"	4'-8"	4'-8"	4'-8"	4'-8"
	B	0'-8"	0'-8"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
	C	1'-6"	1'-6"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
S	A		4'-2"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
	B		3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"
U	A		4'-2"	4'-2"	4'-2"	4'-2"	4'-2"	4'-2"
	B		3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"
n	A	6'-10"	6'-10"	6'-4"	6'-4"	6'-4"	6'-4"	6'-4"
	B	1'-7"	1'-7"	1'-7"	1'-7"	1'-7"	1'-7"	1'-7"
S6	A		5'-2"	5'-2"	5'-2"	5'-2"	5'-2"	5'-2"
	B		5'-2"	5'-2"	5'-2"	5'-2"	5'-2"	5'-2"
S6	A		6'-2"	6'-2"	6'-2"	6'-2"	6'-2"	6'-2"
	B		6'-2"	6'-2"	6'-2"	6'-2"	6'-2"	6'-2"

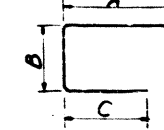
PIER NO. 9S				
MARK	SIZE	NO.	LENGTH	TYPE
P1	#9	8	52'-0"	
P2	#9	12	18'-3"	
P3	#10	8	53'-6"	
P4	#5	12	24'-9"	
P5	#5	4	19'-6"	
P6	#5	4	12'-0"	
V1	#11	88	37'-2"	
V2	#11	120	32'-0"	
S1	#5	116	11'-4"	
S2	#5	424	9'-6"	
S3	#5	1296	2'-8"	
S4	#5	400	4'-2"	
n	#11	120	8'-5"	
W1	#8	28	10'-0"	
T1	#8	22	13'-6"	

PIER NO. 10S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-11"	
P1	#11	7	46'-10"	
P2	#11	14	18'-4"	
P3	#11	7	27'-11"	
P4	#5	6	9'-6"	
P5	#6	4	19'-2"	
P6	#6	4	17'-0"	
h	#11	20	32'-10"	
h1	#4	4	17'-1"	
h2	#4	4	16'-11"	
V	#11	40	23'-10"	
V1	#11	40	22'-7"	
V2	#11	40	24'-4"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	40	25'-8"	
S	#5	16	16'-8"	
S1	#5	16	8'-11"	
S2	#5	16	8'-2"	
S3	#4	40	17'-8"	
S4	#4	16	18'-8"	
S5	#4	40	21'-8"	
u	#6	6	10'-2"	
n1	#11	80	7'-2"	
n	#11	40	7'-11"	

PIER NO. 11S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-2"	
P3	#11	7	27'-8"	
P4	#5	6	9'-6"	
P5	#6	4	21'-6"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
h2	#4	4	16'-11"	
V	#11	40	23'-10"	
V1	#11	40	22'-7"	
V2	#11	40	22'-4"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	46	25'-8"	
S	#5	16	16'-8"	
S1	#5	16	8'-11"	
S2	#5	16	8'-2"	
S3	#4	40	17'-8"	
S4	#4	16	18'-8"	
S5	#4	40	21'-8"	
u	#6	6	10'-2"	
n1	#11	80	7'-2"	
n	#11	40	7'-11"	

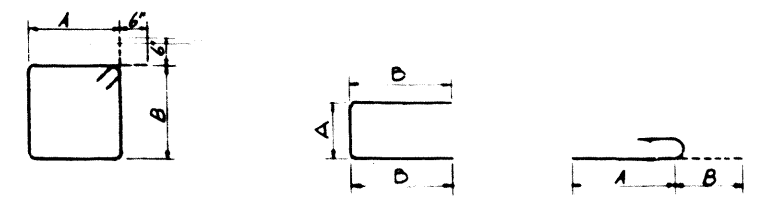
PIER NO. 12S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-2"	
P3	#11	7	27'-8"	
P4	#5	6	9'-6"	
P5	#6	4	21'-6"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
h2	#4	4	16'-11"	
V	#11	40	23'-10"	
V1	#11	40	22'-7"	
V2	#11	40	21'-0"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	44	25'-8"	
S	#5	16	16'-8"	
S1	#5	16	8'-11"	
S2	#5	16	8'-2"	
S3	#4	40	17'-8"	
S4	#4	16	18'-8"	
S5	#4	40	21'-8"	
u	#6	6	10'-2"	
n1	#11	80	7'-2"	
n	#11	40	7'-11"	

PIER 9N & 9S



S1, S2, S3, S4

PIER 10N, 10S, 11N, 11S, 12N, 12S



S, S3, S4, S6, S6

S1, S2, U

n

Note:
Fabricator shall identify bars by location and bar mark, i.e. Pier 1S mk p1.

* Drop #3 bars in field (Piers 9)

DESIGNED	FTG
CHECKED	J.T.L.
DRAWN	Pvd.M.
CHECKED	H.C.M.

BAR LISTS-PIERS 9-12
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50

FAI ROUTE 80
SECTION 00 22

PROJECT 7-80-4(3)33

WILL COUNTY

Scale:

Date: JAN 25, 1961

BLAUVE ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

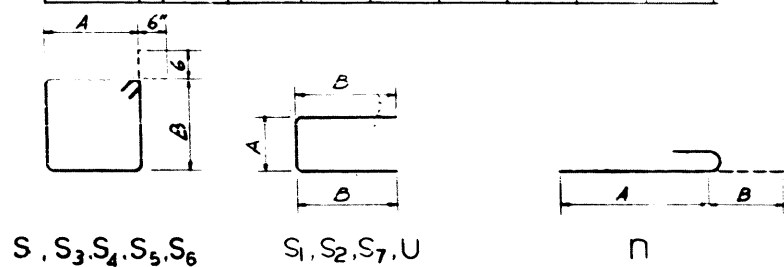
PIER NO. 13N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-1"	
P3	#11	7	27'-11"	
P4	#5	6	9'-4"	
P5	#6	4	19'-2"	
P6	#6	2	17'-0"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
v	#10	40	23'-10"	
v1	#10	40	22'-3"	
v2	#10	40	19'-6"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	40	24'-8"	
s	#5	16	16'-2"	
s1	#5	16	8'-8"	
s2	#5	16	7'-11"	
s3	#4	40	16'-8"	
s4	#4	16	18'-2"	
s5	#4	40	20'-8"	
u	#6	6	9'-11"	
n1	#10	80	6'-6"	
n	#10	40	7'-5"	

PIER NO. 14N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-1"	
P3	#11	7	27'-11"	
P4	#5	6	9'-4"	
P5	#6	4	19'-2"	
P6	#6	2	17'-0"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
v	#10	40	23'-10"	
v1	#10	40	22'-3"	
v2	#10	40	19'-5"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	40	24'-8"	
s	#5	16	16'-2"	
s1	#5	16	8'-8"	
s2	#5	16	7'-11"	
s3	#4	40	16'-8"	
s4	#4	16	18'-2"	
s5	#4	40	20'-8"	
u	#6	6	9'-11"	
n1	#10	80	6'-6"	
n	#10	40	7'-5"	

PIER NO. 15N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	24'-4"	
P1	#11	5	47'-8"	
P2	#10	8	17'-11"	
P3	#10	5	29'-0"	
P4	#9	4	29'-0"	
P5	#5	6	9'-4"	
P6	#6	2	16'-9"	
P7	#6	4	19'-8"	
v	#10	32	23'-3"	
v1	#10	32	23'-10"	
v2	#10	32	6'-6"	
t	#6	20	6'-6"	
w	#10	20	13'-6"	
h2	#4	4	17'-2"	
h	#5	24	17'-9"	
h1	#8	10	34'-5"	
h2	#11	14	33'-5"	
S7	#5	23	19'-9"	
s	#5	29	15'-2"	
s1	#5	20	8'-2"	
s2	#5	20	7'-5"	
s3	#4	40	14'-8"	
s4	#4	26	22'-8"	
s5	#4	16	15'-2"	
s6	#4	40	18'-8"	
u	#6	6	9'-5"	
n1	#11	32	19'-4"	
n	#11	32	7'-9"	

PIER NO. 16N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	24'-0"	
P1	#11	5	47'-1"	
P2	#10	8	17'-11"	
P3	#10	5	28'-6"	
P4	#9	4	28'-6"	
P5	#5	6	9'-4"	
P6	#6	2	16'-9"	
P7	#6	4	19'-8"	
v	#10	32	23'-3"	
v1	#10	32	23'-10"	
v2	#10	32	6'-6"	
t	#6	20	6'-6"	
w	#10	20	13'-6"	
h2	#4	4	16'-11"	
h	#5	16	17'-5"	
h1	#8	10	33'-8"	
h2	#11	14	32'-8"	
S7	#5	22	15'-3"	
s	#5	29	15'-2"	
s1	#5	20	8'-2"	
s2	#5	20	7'-5"	
s3	#4	40	14'-8"	
s4	#4	22	22'-8"	
s5	#4	16	15'-2"	
s6	#4	40	18'-8"	
u	#6	6	9'-5"	
n1	#11	32	15'-6"	
n	#11	32	7'-9"	

PIER NO.	13N	13S	14N	14S	15N	15S	16N	16S
S	A 3'-11"	3'-11"	3'-11"	3'-11"	3'-5"	3'-5"	3'-5"	3'-5"
	B 3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"
S1	A 3'-11"	3'-11"	3'-11"	3'-11"	3'-5"	3'-5"	3'-5"	3'-5"
	B 2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"	2'-4 1/2"
S2	A 3'-11"	3'-11"	3'-11"	3'-11"	3'-5"	3'-5"	3'-5"	3'-5"
	B 2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"
S3	A 3'-11"	3'-11"	3'-11"	3'-11"	3'-5"	3'-5"	3'-5"	3'-5"
	B 3'-11"	3'-11"	3'-11"	3'-11"	3'-5"	3'-5"	3'-5"	3'-5"
S4	A 3'-11"	3'-11"	3'-11"	3'-11"	3'-5"	3'-5"	3'-5"	3'-5"
	B 4'-8"	4'-8"	4'-8"	4'-8"	3'-5"	3'-5"	3'-5"	3'-5"
U	A 3'-11"	3'-11"	3'-11"	3'-11"	3'-5"	3'-5"	3'-5"	3'-5"
	B 3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"
N	A 6'-0"	6'-0"	6'-0"	6'-0"	6'-4"	6'-4"	6'-4"	6'-4"
	B 1'-5"	1'-5"	1'-5"	1'-5"	1'-5"	1'-5"	1'-5"	1'-5"
S5	A 4'-11"	4'-11"	4'-11"	4'-11"	3'-8"	3'-8"	3'-8"	3'-8"
	B 4'-11"	4'-11"	4'-11"	4'-11"	3'-5"	3'-5"	3'-5"	3'-5"
S6	A 5'-11"	5'-11"	5'-11"	5'-11"	4'-5"	4'-5"	4'-5"	4'-5"
	B 3'-11"	3'-11"	3'-11"	3'-11"	4'-5"	4'-5"	4'-5"	4'-5"
S7	A				3'-5"	3'-5"	3'-5"	3'-5"
	B				7'-2"	5'-10"	4'-11"	3'-9"



PIER NO. 13S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-1"	
P3	#11	7	27'-11"	
P4	#5	6	9'-4"	
P5	#6	4	21'-6"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
v	#10	40	23'-10"	
v1	#10	40	22'-3"	
v2	#10	40	20'-6"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	42	24'-8"	
s	#5	16	16'-2"	
s1	#5	16	8'-8"	
s2	#5	16	7'-11"	
s3	#4	40	16'-8"	
s4	#4	16	18'-2"	
s5	#4	40	20'-8"	
u	#6	6	9'-11"	
n1	#10	80	6'-6"	
n	#10	40	7'-5"	

PIER NO. 14S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#11	7	46'-7"	
P2	#11	14	18'-1"	
P3	#11	7	27'-11"	
P4	#5	6	9'-4"	
P5	#6	4	21'-6"	
h	#11	20	32'-10"	
h1	#4	4	16'-11"	
v	#10	40	23'-10"	
v1	#10	40	22'-3"	
v2	#10	40	19'-10"	
t	#6	20	6'-6"	
w	#10	18	13'-6"	
S6	#4	40	24'-8"	
s	#5	16	16'-2"	
s1	#5	16	8'-8"	
s2	#5	16	7'-11"	
s3	#4	40	16'-8"	
s4	#4	16	18'-2"	
s5	#4	40	20'-8"	
u	#6	6	9'-11"	
n1	#10	80	6'-6"	
n	#10	40	7'-5"	

PIER NO. 15S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	24'-4"	
P1	#11	5	47'-8"	
P2	#10	8	17'-11"	
P3	#10	5	29'-0"	
P4	#9	4	29'-0"	
P5	#5	6	9'-4"	
P6	#6	2	16'-9"	
P7	#6	4	19'-8"	
v	#10	32	23'-3"	
v1	#10	32	23'-10"	
v2	#10	64	6'-6"	
t	#6	20	6'-6"	
w	#10	20	13'-6"	
h2	#4	4	17'-2"	
h	#5	20	17'-9"	
h1	#8	10	34'-5"	
h2	#11	14	33'-5"	
S7	#5	23	17'-1"	
s	#5	29	15'-2"	
s1	#5	20	8'-2"	
s2	#5	20	7'-5"	
s3	#4	40	14'-8"	
s4	#4	22	22'-8"	
s5	#4	16	15'-2"	
s6	#4	40	18'-8"	
u	#6	6	9'-5"	
n1	#11	32	16'-3"	
n	#11	32	7'-9"	

PIER NO. 16S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	24'-0"	
P1	#11	5	47'-1"	
P2	#10	8	17'-11"	
P3	#10	5	28'-6"	
P4	#9	4	28'-6"	
P5	#5	6	9'-4"	
P6	#6	2	16'-9"	
P7	#6	4	19'-8"	
v	#10	32	23'-3"	
v1	#10	32	23'-10"	
v2	#10	64	6'-6"	
t	#6	20	6'-6"	
w	#10	20	13'-6"	
h2	#4	4	16'-11"	
h	#5	16	17'-5"	
h1	#8	10	33'-8"	
h2	#11	14	32'-8"	
S7	#5	22	12'-11"	
s	#5	29	15'-2"	
s1	#5	20	8'-2"	
s2	#5	20	7'-5"	
s3	#4	40	14'-8"	
s4	#4	22	22'-8"	
s5	#4	16	15'-2"	
s6	#4	40	18'-8"	
u	#6	6	9'-5"	
n1	#11	32	12'-6"	
n	#11	32	7'-9"	

Note: Fabricator shall identify bars by location and bar mark, i.e. Pier 15 mt P.

BAR LISTS-PIERS 13-16
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50

FAI ROUTE 80 PROJECT: F-80-4(13)133
SECTION 99-38 WILL COUNTY
Date: Jan 25, 1961

BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

DESIGNED F.T.G.
CHECKED J.T.L.
DRAWN P.v.d.M.
CHECKED H.C.M.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	57
PROJECT: I-80-4(13)33			SHEET NO. 38	
			42 SHEETS	

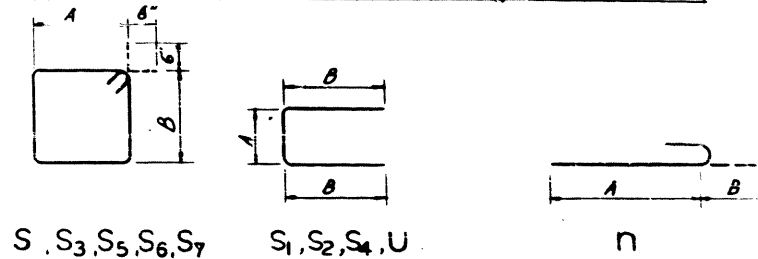
PIER NO. 17N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#10	10	46'-6"	
P2	#11	12	13'-1"	
P3	#10	10	27'-10"	
P4	#5	6	9'-4"	
P5	#6	4	18'-6"	
h2	#11	14	32'-2"	
h3	#4	4	16'-7"	
v	#11	40	22'-7"	
v1	#11	40	23'-10"	
f	#6	18	5'-9"	
w	#9	22	11'-6"	
s	#5	24	15'-2"	
s1	#5	16	8'-2"	
s2	#5	16	7'-5"	
s3	#4	40	14'-8"	
s4	#4	42	18'-8"	
s5	#4	24	22'-8"	
s6	#4	16	15'-2"	
u	#6	6	9'-5"	
n1	#11	40	10'-11"	
n	#11	40	7'-11"	

PIER NO. 18N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#10	10	46'-6"	
P2	#11	12	13'-1"	
P3	#10	10	27'-10"	
P4	#5	6	9'-4"	
P5	#6	4	18'-6"	
h2	#11	14	32'-2"	
h3	#4	4	16'-7"	
v	#11	40	22'-7"	
v1	#11	40	23'-10"	
f	#6	18	5'-9"	
w	#9	22	11'-6"	
s	#5	24	15'-2"	
s1	#5	16	8'-2"	
s2	#5	16	7'-5"	
s3	#4	40	14'-8"	
s4	#4	42	18'-8"	
s5	#4	24	22'-8"	
s6	#4	16	15'-2"	
u	#6	6	9'-5"	
n1	#11	40	8'-3"	
n	#11	40	7'-11"	

PIER NO. 19N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	26'-1"	
P1	#11	4	51'-3"	
P2	#11	6	14'-9"	
P3	#10	4	35'-4"	
P4	#5	6	7'-10"	
P5	#6	4	24'-4"	
v	#10	36	23'-10"	
v	#10	36	22'-3"	
h	#11	12	38'-9"	
i	#6	6	19'-11"	
i	#6	6	5'-6"	
i	#6	6	7'-6"	
s	#5	22	14'-2"	
s	#5	22	14'-2"	
s1	#5	16	7'-8"	
s2	#5	12	8'-11"	
s3	#4	60	12'-8"	
s4	#4	21	20'-8"	
s5	#4	63	16'-8"	
u	#6	6	8'-11"	
n1	#10	12	6'-6"	
n	#10	48	10'-8"	

PIER NO. 20N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	28'-0"	
P1	#11	4	55'-1"	
P2	#10	6	13'-7"	
P3	#10	4	39'-5"	
P4	#5	6	7'-10"	
P5	#6	4	22'-2"	
v	#10	36	22'-3"	
v1	#10	12	6'-6"	
f	#6	24	5'-6"	
w	#8	32	10'-6"	
h	#5	16	22'-8"	
h1	#8	8	44'-1"	
h2	#10	14	43'-1"	
h3	#4	4	22'-1"	
s	#5	24	13'-8"	
s1	#5	16	7'-5"	
s2	#5	12	6'-8"	
s3	#4	40	11'-8"	
s4	#5	34	16'-2"	
s5	#4	60	15'-8"	
s6	#4	24	13'-8"	
u	#6	6	8'-8"	
n	#10	36	23'-8"	
n	#10	36	10'-1"	

PIER NO.	17N	17S	18N	18S	19N	19S	20N	20S
S	A 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	2'-8"	2'-2"
	B 3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"
S1	A 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	2'-8"	2'-2"
	B 2'-4"	2'-4"	2'-4"	2'-4"	2'-4"	2'-4"	2'-4"	2'-4"
S2	A 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	2'-8"	2'-2"
	B 2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"
S3	A 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	2'-8"	2'-2"
	B 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	2'-8"	2'-2"
S4	A						4'-8"	5'-2"
	B						5'-9"	5'-6"
U	A 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	2'-8"	2'-2"
	B 3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"
n	A 6'-4"	6'-4"	6'-4"	6'-4"	9'-3"	6'-4"	8'-8"	8'-6"
	B 1'-7"	1'-7"	1'-7"	1'-7"	1'-5"	1'-7"	1'-5"	1'-7"
S5	A 4'-5"	4'-5"	4'-5"	4'-5"	4'-11"	4'-5"	3'-8"	4'-2"
	B 4'-5"	4'-5"	4'-5"	4'-5"	4'-11"	4'-5"	3'-8"	4'-2"
S6	A 5'-5"	5'-5"	5'-5"	5'-5"	3'-11"	5'-5"	3'-8"	3'-2"
	B 5'-5"	5'-5"	5'-5"	5'-5"	3'-11"	5'-5"	3'-8"	3'-2"
S7	A 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	3'-8"	3'-8"
	B 3'-5"	3'-5"	3'-5"	3'-5"	2'-11"	3'-5"	3'-8"	3'-8"



PIER NO. 17S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#10	10	46'-5"	
P2	#11	12	13'-1"	
P3	#10	10	27'-10"	
P4	#5	6	9'-4"	
P5	#6	10	18'-6"	
h2	#11	14	32'-2"	
h3	#4	4	16'-7"	
v	#11	40	22'-7"	
v1	#11	40	23'-7"	
f	#6	18	5'-9"	
w	#9	22	11'-6"	
s	#5	24	15'-2"	
s1	#5	16	8'-2"	
s2	#5	16	7'-5"	
s3	#4	40	14'-8"	
s4	#4	42	18'-8"	
s5	#4	22	22'-8"	
s6	#4	16	15'-2"	
u	#6	6	9'-5"	
n1	#11	40	10'-1"	
n	#11	40	7'-11"	

PIER NO. 18S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#10	10	46'-5"	
P2	#11	12	13'-1"	
P3	#10	10	27'-10"	
P4	#5	6	9'-4"	
P5	#6	4	18'-6"	
h2	#11	14	32'-2"	
h3	#4	4	16'-7"	
v	#11	40	22'-7"	
v1	#11	40	23'-10"	
f	#6	18	5'-9"	
w	#9	22	11'-6"	
s	#5	24	15'-2"	
s1	#5	16	8'-2"	
s2	#5	16	7'-5"	
s3	#4	40	14'-8"	
s4	#4	42	18'-8"	
s5	#4	18	22'-8"	
s6	#4	6	9'-5"	
n1	#11	40	7'-11"	
n	#11	40	7'-11"	

PIER NO. 19S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-9"	
P1	#10	10	46'-5"	
P2	#11	12	13'-1"	
P3	#10	10	27'-10"	
P4	#5	6	9'-4"	
P5	#6	4	18'-6"	
h2	#11	14	32'-2"	
h3	#4	4	16'-7"	
v	#11	40	22'-7"	
v1	#11	40	23'-10"	
f	#6	18	5'-9"	
w	#9	22	11'-6"	
s	#5	24	15'-2"	
s1	#5	16	8'-2"	
s2	#5	16	7'-5"	
s3	#4	40	14'-8"	
s4	#4	42	18'-8"	
s5	#4	16	22'-8"	
s6	#4	6	9'-5"	
n1	#11	40	6'-10"	
n	#11	40	7'-11"	

PIER NO. 20S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-10"	
P1	#11	4	46'-5"	
P2	#11	12	13'-1"	
P3	#10	10	27'-10"	
P4	#5	6	9'-4"	
P5	#6	4	18'-2"	
v	#11	24	22'-7"	
v1	#11	24	22'-8"	
v2	#11	48	7'-2"	
f	#6	16	5'-6"	
w	#9	16	10'-6"	
h	#5	16	17'-1"	
h1	#9	8	32'-11"	
h2	#10	16	31'-9"	
h3	#4	4	16'-8"	
s	#5	26	12'-8"	
s1	#5	32	6'-11"	
s2	#5	32	6'-2"	
s3	#4	40	13'-8"	
s4	#5	25	16'-2"	
s5	#4	38	17'-8"	
s6	#4	22	14'-8"	
u	#6	6	9'-2"	
n	#11	24	10'-1"	

DESIGNED	F.T.G.
CHECKED	J.T.L.
DRAWN	Rvd.M.
CHECKED	H.C.M.

Note:
Fabricator shall identify bars by location and bar mark, i.e. Pier 15, mt. P.

BAR LISTS-PIERS 17-20
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50
FAI ROUTE 80 SECTION 99-38 PROJECT I-80-4(13)33
WILL COUNTY
Date: Jan. 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.Y. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

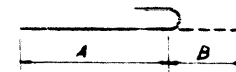
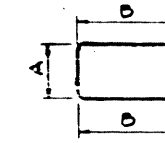
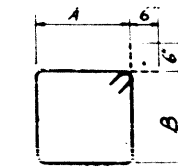
PIER NO. 21N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	29'-8"	
P1	#11	4	37'-8"	
P2	#10	6	13'-1"	
P3	#10	4	42'-0"	
P4	#5	6	7'-10"	
P5	#6	4	22'-2"	
V	#10	36	22'-3"	
V1	#10	72	6'-6"	
T	#6	24	5'-6"	
W	#8	30	10'-6"	
S	#4	4	23'-3"	
H	#5	16	23'-10"	
H1	#8	8	46'-6"	
H2	#10	14	43'-6"	
S6	#4	26	13'-8"	
S	#5	24	13'-8"	
S1	#5	16	7'-5"	
S2	#5	12	6'-8"	
S3	#4	30	11'-8"	
S4	#5	36	15'-0"	
S5	#4	54	13'-8"	
U	#6	6	8'-8"	
n	#10	36	7'-5"	
n	#10	36	9'-6"	

PIER NO. 22N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	30'-0"	
P1	#10	4	40'-5"	
P2	#10	4	20'-6"	
P3	#10	8	14'-10"	
P4	#10	4	13'-0"	
P5	#11	6	42'-4"	
P6	#5	6	8'-4"	
P7	#6	4	27'-5"	
V	#10	36	22'-3"	
V1	#10	36	23'-8"	
V2	#10	36	6'-6"	
T	#6	27	5'-6"	
W	#8	24	9'-6"	
S	#5	60	11'-8"	
S1	#5	32	6'-5"	
S2	#5	24	5'-8"	
S3	#4	60	11'-8"	
S6	#4	24	13'-8"	
U	#6	6	8'-8"	
H	#10	14	46'-0"	
H1	#4	4	23'-6"	
n	#10	36	7'-5"	

PIER NO. 23N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	30'-7"	
P1	#10	4	41'-7"	
P2	#10	4	20'-6"	
P3	#10	8	14'-10"	
P4	#10	4	13'-0"	
P5	#11	6	43'-7"	
P6	#5	6	8'-4"	
P7	#6	4	26'-2"	
P8	#4	4	23'-8"	
V	#10	36	22'-3"	
V1	#10	36	23'-8"	
V2	#10	36	6'-6"	
T	#6	27	5'-6"	
W	#8	24	9'-6"	
S	#5	60	11'-8"	
S1	#5	32	6'-5"	
S2	#5	24	5'-8"	
S3	#4	60	11'-8"	
S6	#4	24	13'-8"	
U	#6	6	8'-8"	
n	#10	36	7'-5"	

PIER NO. 24N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	31'-1"	
P1	#10	4	42'-8"	
P2	#10	4	20'-6"	
P3	#10	8	14'-10"	
P4	#10	4	13'-0"	
P5	#11	6	44'-7"	
P6	#5	6	8'-4"	
P7	#6	4	26'-2"	
P8	#6	2	25'-0"	
V	#10	36	22'-3"	
V1	#10	36	18'-11"	
V2	#10	36	6'-6"	
T	#6	27	5'-6"	
W	#8	24	9'-6"	
H	#10	14	48'-3"	
H1	#4	4	24'-1"	
H2	#4	4	24'-7"	
S	#5	64	11'-8"	
S1	#5	32	6'-5"	
S2	#5	24	5'-8"	
S3	#4	60	11'-8"	
S6	#4	24	13'-8"	
U	#6	6	8'-8"	
n	#10	36	7'-5"	

Pier No.	21N	21S	22N	22S	23N	23S	24N	24S
S	A 2'-8"	2'-2"	1'-8"	3'-2"	1'-8"	3'-2"	1'-8"	1'-8"
	B 3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"
S1	A 2'-8"	2'-2"	1'-8"	3'-2"	1'-8"	3'-2"	1'-8"	1'-8"
	B 2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"
S2	A 2'-8"	2'-2"	1'-8"	3'-2"	1'-8"	3'-2"	1'-8"	1'-8"
	B 2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"	2'-0"
S3	A 2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	2'-8"
	B 2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	2'-8"
S4	A 4'-8"	5'-2"						
	B 5'-2"	4'-11 1/2"						
U	A 2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	2'-8"
	B 3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"	3'-0"
n	A 8'-1"	7'-10"	6'-0"	6'-4"	6'-0"	6'-4"	6'-0"	6'-0"
	B 1'-5"	1'-7"	1'-5"	1'-7"	1'-5"	1'-7"	1'-5"	1'-5"
S5	A 3'-8"	4'-2"	3'-8"	4'-2"	3'-8"	4'-2"	3'-8"	3'-8"
	B 3'-8"	4'-2"	3'-8"	4'-2"	3'-8"	4'-2"	3'-8"	3'-8"
S6	A 2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	3'-2"	2'-8"	2'-8"
	B 3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"	3'-8"



S, S3, S5, S6, S1, S2, S4, U

n

PIER NO. 21S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-10"	
P1	#11	4	46'-5"	
P2	#11	12	17'-1"	
P3	#10	10	27'-10"	
P4	#5	6	9'-4"	
P5	#6	4	18'-2"	
V	#11	24	22'-7"	
V1	#11	24	20'-7"	
V2	#11	48	7'-2"	
T	#6	16	5'-6"	
W	#9	16	10'-6"	
S	#5	46	12'-8"	
S1	#5	32	6'-11"	
S2	#5	32	6'-2"	
S3	#4	40	13'-8"	
S4	#5	25	15'-1"	
S6	#4	36	17'-8"	
S6	#4	22	14'-8"	
H	#5	16	17'-1"	
H1	#9	8	32'-11"	
H2	#10	16	31'-9"	
H3	#4	4	16'-5"	
U	#6	6	9'-2"	
n	#11	24	9'-5"	

PIER NO. 22S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-8"	
P1	#10	5	46'-5"	
P2	#10	18	19'-4"	
P3	#11	9	27'-10"	
P4	#5	6	9'-4"	
P5	#6	4	19'-2"	
P6	#6	4	18'-2"	
V	#11	24	23'-7"	
V1	#11	24	22'-5"	
V2	#11	24	7'-2"	
T	#6	14	5'-6"	
W	#9	14	9'-6"	
H	#10	16	31'-10"	
H1	#4	4	16'-6"	
S	#5	24	14'-8"	
S1	#5	16	7'-11"	
S2	#5	16	7'-2"	
S3	#4	40	13'-8"	
S6	#4	38	17'-8"	
S6	#4	16	14'-8"	
U	#6	6	9'-2"	
n	#11	24	7'-11"	

PIER NO. 23S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-10"	
P1	#10	5	46'-8"	
P2	#10	18	19'-4"	
P3	#11	9	28'-1"	
P4	#5	6	9'-4"	
P5	#6	2	16'-0"	
P6	#6	3	19'-2"	
V	#11	24	23'-7"	
V1	#11	24	18'-2"	
V2	#11	24	7'-2"	
T	#6	14	5'-6"	
W	#9	14	9'-6"	
H	#10	16	32'-2"	
H1	#4	4	16'-6"	
S	#5	24	14'-8"	
S1	#5	16	7'-11"	
S2	#5	16	7'-2"	
S3	#4	40	13'-8"	
S6	#4	30	17'-8"	
S6	#4	16	14'-8"	
U	#6	6	9'-2"	
n	#11	24	7'-11"	

PIER NO. 24S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-8"	
P1	#11	6	46'-4"	
P2	#11	8	17'-7"	
P3	#11	9	27'-8"	
P4	#5	6	9'-6"	
P5	#6	4	18'-8"	
P6	#6	2	18'-0"	
V	#10	24	23'-3"	
V1	#10	24	13'-8"	
V2	#10	24	6'-6"	
S	#5	48	11'-8"	
S1	#5	32	6'-5"	
S2	#5	40	5'-8"	
S3	#4	40	11'-8"	
S6	#4	20	15'-8"	
S6	#4	17	13'-8"	
U	#6	6	8'-8"	
H	#10	14	31'-4"	
H1	#4	4	16'-2"	
n	#10	24	7'-5"	
T	#6	14	5'-6"	
W	#9	16	9'-6"	

Note:
Fabricator shall identify bars by location
and bar mark, i.e. Pier 15, M6 P1.

DESIGNED	FTG
CHECKED	J.T.L.
DRAWN	P.vd.M.
CHECKED	H.C.M.

BAR LISTS-PIERS 21-24
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50

FAI-ROUTE 80 PROJECT I-80-4(13)S3
SECTION 99-38 WILL COUNTY

Scale: Date: Jan 25, 1961

BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PIER NO. 25N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	31'-11"	—
P1	#9	6	43'-5"	—
P2	#9	6	21'-0"	—
P3	#10	8	14'-6"	—
P4	#10	4	11'-6"	—
P5	#11	6	45'-0"	—
P6	#5	6	8'-9"	—
P7	#6	4	23'-2"	—
P8	#6	2	29'-0"	—
V	#10	36	22'-3"	—
V1	#10	36	6'-6"	—
T	#6	21	5'-6"	—
W	#8	27	9'-5"	—
H	#5	28	25'-0"	—
H1	#8	8	48'-3"	—
S	#5	80	11'-8"	□
S1	#5	16	7'-5"	□
S2	#5	16	6'-8"	□
S3	#4	60	11'-8"	□
S4	#5	38	20'-2"	□
S5	#4	27	13'-8"	□
U	#6	6	8'-8"	□
N1	#10	36	16'-9"	—
N	#10	36	7'-5"	—

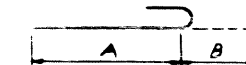
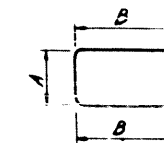
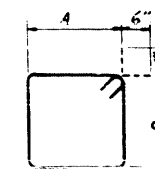
PIER NO. 25S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	23'-11"	—
P1	#11	6	46'-10"	—
P2	#11	8	16'-5"	—
P3	#10	5	28'-3"	—
P4	#10	4	28'-3"	—
P5	#5	6	9'-4"	—
P6	#6	2	29'-0"	—
P7	#6	4	19'-5"	—
V	#11	32	22'-7"	—
V1	#11	32	7'-2"	—
T	#6	14	5'-6"	—
W	#8	12	9'-6"	—
H	#5	12	16'-7"	—
H1	#8	8	32'-1"	—
S	#5	48	12'-0"	□
S1	#5	32	6'-7"	□
S2	#5	40	5'-10"	□
S3	#4	40	12'-8"	□
S4	#5	29	12'-3"	□
S5	#4	12	16'-8"	□
U	#6	6	8'-11"	□
N1	#11	32	10'-1"	—
N	#11	32	7'-11"	—

PIER NO. 26N				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	32'-5"	—
P1	#10	4	45'-4"	—
P2	#10	2	20'-6"	—
P3	#10	8	14'-10"	—
P4	#10	4	13'-0"	—
P5	#11	6	47'-3"	—
P6	#5	6	8'-4"	—
P7	#6	4	23'-11"	—
P8	#6	4	17'-2"	—
V2	#10	36	6'-6"	—
V	#10	36	22'-3"	—
V1	#10	36	14'-7"	—
T	#6	14	5'-6"	—
W	#8	12	9'-6"	—
H1	#4	4	21'-0"	—
S1	#5	32	6'-5"	□
S2	#5	28	5'-8"	□
S3	#4	60	11'-8"	□
S4	#4	36	13'-8"	□
S5	#4	28	13'-8"	□
U	#6	6	8'-8"	□
N	#10	36	7'-5"	—

PIER NO. 26S				
MARK	SIZE	NO.	LENGTH	TYPE
P	#4	4	24'-8"	—
P1	#11	6	47'-5"	—
P2	#11	8	17'-7"	—
P3	#11	9	28'-9"	—
P4	#5	6	9'-6"	—
P5	#6	4	13'-8"	—
H	#10	14	32'-5"	—
H1	#4	4	16'-8"	—
V	#10	24	22'-3"	—
V2	#10	24	6'-6"	—
T	#6	14	5'-6"	—
W	#9	16	9'-6"	—
S	#5	46	11'-8"	□
S1	#5	32	11'-5"	□
S2	#4	40	5'-8"	□
S3	#4	40	11'-8"	□
S4	#4	8	15'-8"	□
S5	#4	17	13'-8"	□
U	#6	6	8'-8"	□
N	#10	24	11'-4"	—

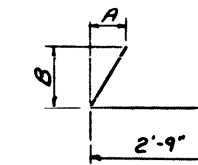
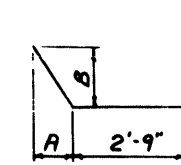
Pier No.	25N	25S	26N	26S
S	A 1'-8"	1'-10"	1'-8"	1'-8"
	B 3'-8"	3'-8"	3'-8"	3'-8"
S1	A 2'-8"	1'-10"	1'-8"	1'-8"
	B 2'-4"	2'-4"	2'-4"	2'-4"
S2	A 2'-8"	1'-10"	1'-8"	1'-8"
	B 2'-0"	2'-0"	2'-0"	2'-0"
S3	A 2'-8"	2'-11"	2'-8"	2'-8"
	B 2'-8"	2'-11"	2'-8"	2'-8"
S4	A 3'-8"	3'-11"		
	B 8'-3"	4'-8"		
U	A 2'-8"	2'-11"	2'-8"	2'-8"
	B 3'-0"	3'-0"	3'-0"	3'-0"
N	A 6'-0"	6'-4"	6'-0"	9'-11"
	B 1'-5"	1'-7"	1'-5"	1'-5"
S5	A 3'-8"	3'-11"	3'-8"	3'-8"
	B 3'-8"	3'-11"	3'-8"	3'-8"
S6	A		2'-8"	2'-8"
	B		3'-8"	3'-8"

Abut.	S.E.	N.E.	S.W.	N.W.
S1	A 2'-6"	2'-6"	2'-6"	2'-6"
	B 2'-6"	2'-0"	2'-0"	3'-0"
S2	A 2'-6"	2'-6"	2'-6"	2'-6"
	B 2'-3"	7'-9"	7'-9"	2'-9"
U	A 2'-6"	2'-6"	2'-6"	2'-6"
	B 1'-6"	1'-6"	1'-6"	1'-6"
V	A 2'-8"	2'-11"	2'-8"	2'-8"
	B 0'-9"	0'-9"	0'-9"	0'-9"
N	A 3'-2"	3'-2"	3'-2"	3'-2"
	B 0'-7"	0'-7"	0'-7"	0'-7"
N1	A 4'-5"	4'-5"	4'-5"	4'-5"
	B 0'-7"	0'-7"	0'-7"	0'-7"
h2	A 0'-9"	0'-9"	1'-14"	1'-14"
	B 1'-10"	1'-10"	1'-8"	1'-8"
h3	A 0'-3"	0'-2"	0'-2"	0
	B 2'-0"	2'-0"	2'-0"	2'-0"



S, S3, S5, S6, S1, S2, S4, U, V

N, N1



h2

h3 S.W. & N.W. Abut.

h3 S.E. & N.E. Abut.

S.E. ABUTMENT				
MARK	SIZE	NO.	LENGTH	TYPE
W	#5	12	24'-0"	—
W1	#5	6	10'-6"	—
W2	#5	6	11'-3"	—
T	#5	47	5'-0"	—
T1	#5	25	4'-6"	—
N	#5	100	3'-9"	—
N1	#5	28	5'-0"	—
P	#7	10	26'-0"	—
S1	#4	26	7'-6"	□
S2	#4	26	7'-0"	□
U	#5	6	5'-6"	□
V	#4	45	2'-9"	□
V1	#4	100	7'-0"	—
H	#4	20	26'-0"	—
H2	#5	12	4'-9"	—
H3	#5	12	4'-9"	—

N.E. ABUTMENT				
MARK	SIZE	NO.	LENGTH	TYPE
W	#5	18	21'-6"	—
W1	#5	6	10'-6"	—
W2	#5	6	11'-6"	—
T	#5	63	5'-0"	—
T1	#5	23	4'-6"	—
N	#5	132	3'-9"	—
N1	#5	28	5'-0"	—
P	#7	15	23'-0"	—
S1	#4	33	6'-6"	□
S2	#4	33	6'-0"	□
U	#5	6	5'-6"	□
V	#4	61	2'-9"	□
V1	#4	132	6'-6"	—
H	#4	37	23'-0"	—
H2	#5	12	4'-9"	—
H3	#5	12	4'-9"	—

S.W. ABUTMENT				
MARK	SIZE	NO.	LENGTH	TYPE
W	#5	18	21'-6"	—
W1	#5	6	10'-6"	—
W2	#5	6	11'-3"	—
T	#5	63	5'-0"	—
T1	#5	19	4'-6"	—
N	#5	128	3'-9"	—
N1	#5	28	5'-0"	—
P	#7	15	21'-0"	—
S1	#4	32	6'-6"	□
S2	#4	32	6'-0"	□
U	#5	6	5'-6"	□
V	#4	58	2'-9"	□
V1	#4	126	6'-6"	—
H	#4	30	22'-0"	—
H2	#5	12	4'-9"	—
H3	#5	12	4'-9"	—

N.W. ABUTMENT				
MARK	SIZE	NO.	LENGTH	TYPE
W	#5	18	26'-9"	—
W1	#5	6	10'-6"	—
W2	#5	6	11'-6"	—
T	#5	74	5'-0"	—
T1	#5	18	4'-6"	—
N	#5	156	3'-9"	—
N1	#5	28	5'-0"	—
P	#7	15	27'-0"	—
S1	#4	39	8'-6"	□
S2	#4	39	8'-0"	□
U	#5	6	5'-6"	□
V	#4	73	2'-9"	□
V1	#4	156	7'-6"	—
H	#4	30	27'-0"	—
H2	#5	12	4'-9"	—
H3	#5	12	4'-9"	—
Class X Concrete C.Y. 95.2				

Note:
Fabricator shall identify bars by location and bar mark, i.e. Pier 15 mk p.

BAR LISTS-PIERS 25-26
& ABUTMENTS
FAI-80 OVER DES PLAINES RIVER

STA. 387 + 94.50
FAI ROUTE 80 SECTION 99-38 PROJECT J-80-4(3)33
WILL COUNTY Date: Jan. 26, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

DESIGNED	F.T.G.
CHECKED	J.T.L.
DRAWN	Prd.M.
CHECKED	H.C.M.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO	SECTION	CO.	TOTAL SHEETS	SHEET NO	SHEET NO. 41 42 SHEETS
FAI-80	99-38	WILL	71	60	
PER ROAD DIST NO 1	ILLINOIS	PROJECT	I-80-4(13)133		

ITEM	UNIT	N.W. ABUT.	PIER 1N	PIER 2N	PIER 3N	PIER 4N	PIER 5N	PIER 6N	PIER 7N	PIER 8N	PIER 9N	PIER 10N	PIER 11N	PIER 12N	PIER 13N	PIER 14N	PIER 15N	PIER 16N	PIER 17N	PIER 18N	PIER 19N	PIER 20N	PIER 21N	PIER 22N	PIER 23N	PIER 24N	PIER 25N	PIER 26N	N.E. ABUT.
Class A Excav. for Structures	CU.YD.	—	190	145	135	93	67	115	—	—	201	122	72	78	62	106	156	111	53	55	88	140	134	103	110	118	183	135	—
Rock Excav. for Structures	CU.YD.	—	9	7	7	7	6	6	44	42	12	7	7	7	7	7	7	6	6	7	7	7	7	7	7	7	7	7	—
Cofferdams	EA.	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—
Cofferdam Excavation	CU.YD.	—	—	—	—	—	—	—	367	300	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—
Class A Concrete	CU.YD.	—	—	—	—	—	—	—	247.1	652.3	634.1	352.7	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—
Class X Concrete	CU.YD.	95.3	2084	210.6	199.5	150.7	199.5	—	—	—	2420	2268	224.0	205.8	205.3	210.7	187.9	144.7	138.3	153.4	171.4	171.0	126.6	124.4	121.6	147.9	115.5	76.7	
Reinforcement Bars	LB.	5316	26,992	23,519	24,394	18,832	23,826	45,297	76,023	71,681	62,885	32,342	30,963	30,752	26,468	26,454	24,114	23,116	26,802	26,144	19,568	20,454	20,324	18,826	18,613	18,318	17,474	17,923	4,156
Steel Piles 10BP 42	L.F.	660	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	740
Test Piles 10BP 42	EA.	1	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—

ITEM	UNIT	S.W. ABUT.	PIER 1S	PIER 2S	PIER 3S	PIER 4S	PIER 5S	PIER 6S	PIER 7S	PIER 8S	PIER 9S	PIER 10S	PIER 11S	PIER 12S	PIER 13S	PIER 14S	PIER 15S	PIER 16S	PIER 17S	PIER 18S	PIER 19S	PIER 20S	PIER 21S	PIER 22S	PIER 23S	PIER 24S	PIER 25S	PIER 26S	S.E. ABUT.
Class A Excav. for Structures	CU.YD.	—	142	127	90	53	77	103	—	—	74	78	84	90	103	114	115	86	53	60	81	111	107	67	77	67	73	48	—
Rock Excav. for Structures	CU.YD.	—	7	7	7	7	6	6	44	139	11	7	9	7	7	7	9	7	6	6	6	5	5	4	4	4	4	4	—
Cofferdams	EA.	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—
Cofferdam Excavation	CU.YD.	—	—	—	—	—	—	—	335	322	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—
Class A Concrete	CU.YD.	—	—	—	—	—	—	—	242.4	641.0	677.1	321.0	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—
Class X Concrete	CU.YD.	73.5	169.9	192.7	172.6	143.2	204.4	—	—	—	236.1	228.5	224.1	207.1	205.9	195.7	176.1	143.6	137.4	134.6	144.2	137.0	103.3	98.7	76.3	84.0	69.1	64.5	
Reinforcement Bars	LB.	4,308	20,501	22,216	23,649	18,311	23,800	44,659	75,453	78,059	60,044	31,805	31,173	30,856	26,636	26,617	23,439	22,278	26,753	26,067	25,806	19,079	18,677	16,437	15,896	13,406	15,181	12,279	3,719
Steel Piles 10BP 42	L.F.	570	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	420
Test Piles 10BP 42	EA.	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	—	1

Note: For Seal Coat Concrete under pier 8N see sheet 35.

DESIGNED	
CHECKED	
DRAWN	F.C.
CHECKED	R.S.

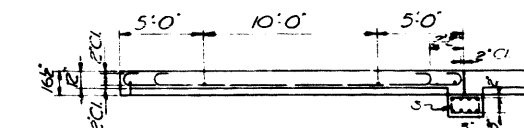
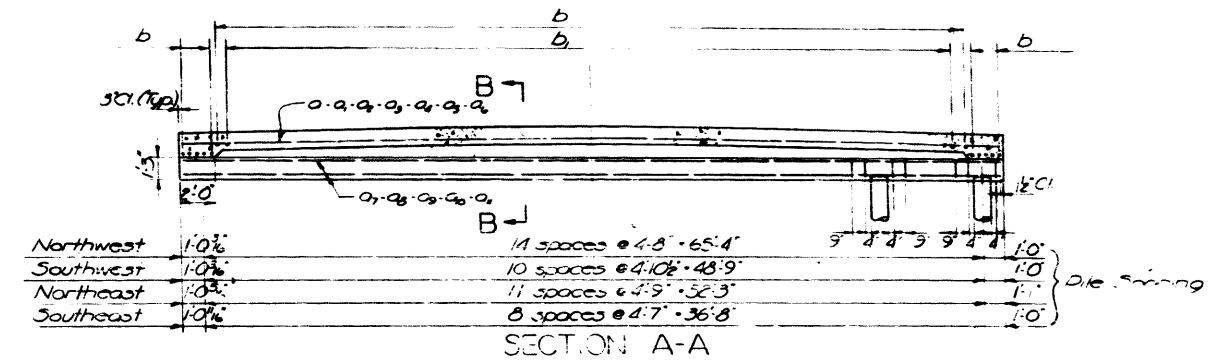
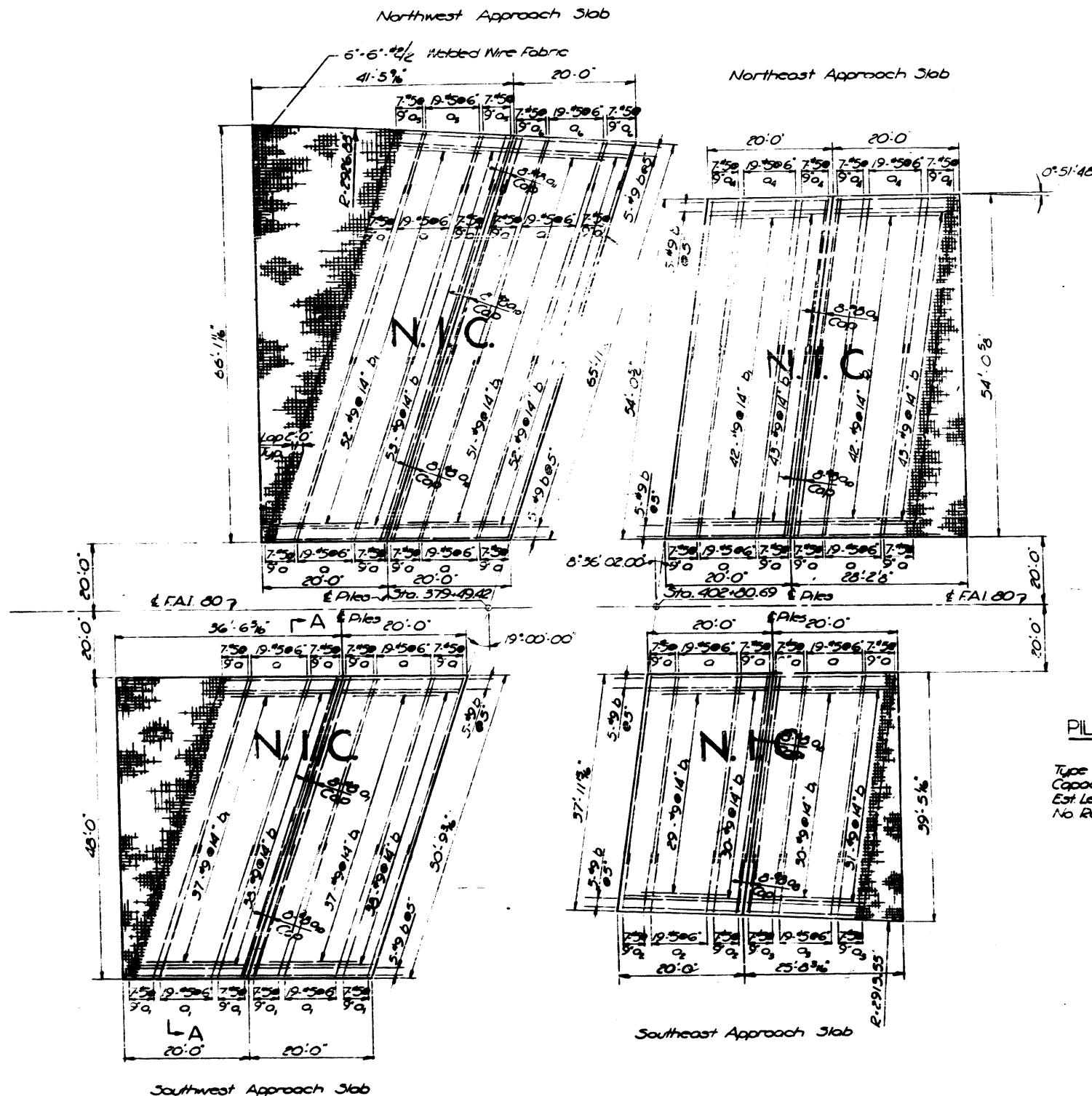
BILL OF MATERIAL
FAI-80 OVER DES PLAINES RIVER
STA. 387+ 94.50

FAI ROUTE 80 SECTION 99-38 PROJECT I-80-4(13)133
WILL COUNTY Date Jan 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N.J. NEW YORK, N.Y. CRYSTAL LAKE, ILL.

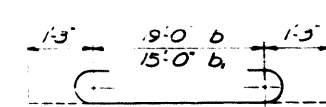
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	PILE SHEETS	SHEET NO.
FAI-80	99-38	WILL	71	61
PIL. ROAD DIST. NO. 7		ILLINOIS PROJECT	S-4(9) 133	

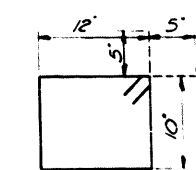
SHEET NO. 42
42 SHEETS



SECTION B-B



BARS b & b



BAR s

PILE DATA
Type: Creosoted Timber
Capacity: 15 Tons
Est Length: 25'
No Required: 47

Notes:
Minimum Laps: #5 bars: 1.1'
#8 bars: 1.8'
Provide E.C.I. from reinf. to face of concrete unless otherwise noted.
For additional notes and details see Illinois Standard 213B.

NOTE: PILES FOR APPROACH SLABS DRIVEN UNDER THIS CONTRACT.
APPROACH SLAB PAVING BY OTHERS.

BILL OF MATERIAL

Mark	Size	No.	Length	Type
0	#5	330	29.0'	
1	#5	66	22.6'	
2	#5	33	10.3'	
3	#5	33	11.3'	
4	#5	66	25.6'	
5	#5	33	12.3'	
6	#5	33	11.0'	
7	#8	8	23.9'	
8	#8	8	11.3'	
9	#8	8	26.6'	
10	#8	40	29.0'	
11	#8	8	12.6'	
b	#9	408	21.6'	C
b	#9	320	17.6'	C
s	#4	180	4.6'	

Item	Unit	Quan
Class X Concrete	CY	14.7
Reinforcement Bars	Lbs	68,910
Pavement 1 1/2" 12" 1 1/2"	3Y	1,068
Creosoted Piles 20" to 30"	LF	1,175

DETAILS OF APPROACH SLABS

DESIGNED	FTG
CHECKED	J.T.L.
DRAWN	FTG
CHECKED	H.C.M.

APPROACH SLABS
FAI-80 OVER DES PLAINES RIVER
STA. 387 + 94.50
FAI ROUTE 80 PROJECT
SECTION 99-38 WILL COUNTY
Scale: No Scale Date: JAN. 25, 1961
BLAUVELT ENGINEERING CO.
CONSULTING ENGINEERS
WOODBURY, N. J. NEW YORK, N. Y. CRYSTAL LAKE, ILL.