

- \divideontimes 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

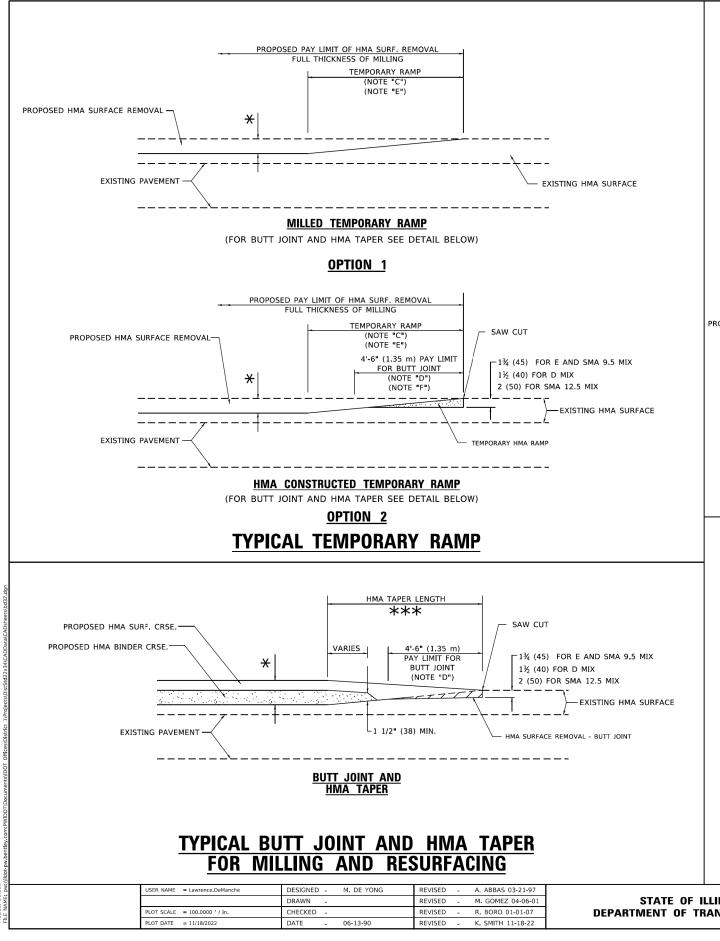
USER NAME = footemj DESIGNED - A. HOUSEH REVISED - A. ABBAS 03-21-97 CURB OR CURB AND GUTTER F.A. SECTION	COUNTY SHEETS NO.
DRAWN - REVISED - M. GOMEZ 01-22-01 STATE OF ILLINOIS REMOVAL AND REPLACEMENT	5.12.10
PLOT SCALE = 50,0000 '/ In. CHECKED - REVISED - R. BORO 12-15-09 DEPARTMENT OF TRANSPORTATION REWOVAL AND REPLACEMENT BD600—06 (BD-24)	CONTRACT NO.
PLOT DATE = 7/11/2019 DATE - 03-11-94 REVISED - K. SMITH 07-11-19 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.	FED. AID PROJECT

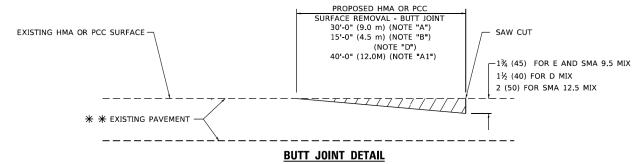
SER NAME = Ibolzenius DESIGNED - BAW REVISED DRAWN -BAW REVISED CHECKED - LB REVISED LOT DATE = 12/28/2023

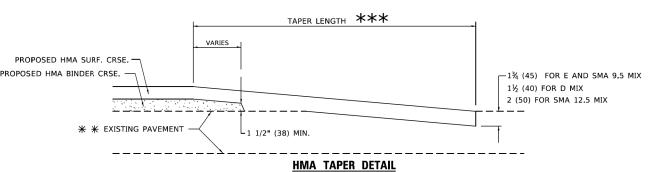
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

GENEVA ROAD IDOT DISTRICT 1 STANDARD DRAWINGS SHEET 3 OF 18 SHEETS STA.

SECTION 18-00206-10-BR DUPAGE 134 101 CONTRACT NO. 61J30







TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS. igstar SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97	·		BUTT JOINT	AND		F.A.	SECTION
	DRAWN -	REVISED - M. GOMEZ 04-06-01	STATE OF ILLINOIS						
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07	DEPARTMENT OF TRANSPORTATION		HMA TAPER	DETAILS			BD400-05 BD-32
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22		SCALE: NONE	SHEET 1 OF 1 SHEET	rs sta.	TO STA.		ILLINOIS

PATRICK	
ENGINEERING	

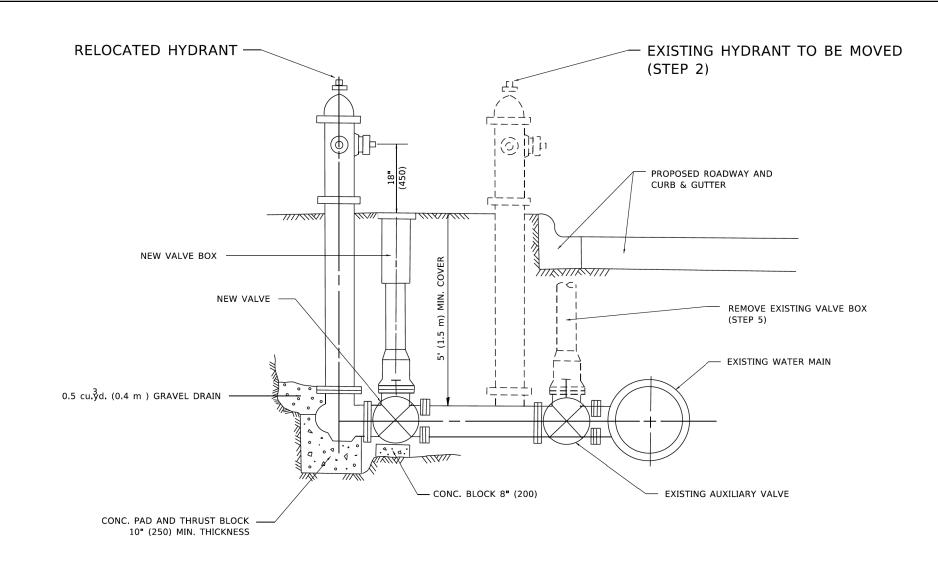
USER NAME = Ibolzenius	DESIGNED -	BAW	REVISED -
	DRAWN -	BAW	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	LB	REVISED -
PLOT DATE = 12/28/2023	DATE -	9/1/23	REVISED -

STATE	OF	ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

		GENEVA ROAD										
	IDOT DISTRICT 1 STANDARD DRAWINGS											
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SCALE: NTS	SHEET 4	OF 18	SHEETS	STA.	TO STA.							

F.A. U. RTE	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
1397	18-00206-10-BR	DUPAGE	134	102	
		CONTRACT	NO. 6	51J30	

COUNTY CONTRACT NO



SEQUENCE OF CONSTRUCTION:

- 1. CLOSE EXISTING VALVE.
- 2. REMOVE EXISTING HYDRANT.
- 3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
- 4. RELOCATE EXISTING HYDRANT.
- 5. OPEN EXISTING VALVE, REMOVE BOX.
- 6. BACKFILL.
- FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

NOTE:

ALL WORK TO BE DONE IN ACCORDANCE WITH SECTION 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = Lawrence.DeManche		REVISED - R. SHAH 09-09-94							F.A. RTE.	SECTIO	N	COUNTY	TOTAL S SHEETS	NO.
	DRAWN -	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS		FIRE HYDRANT		BE MOVED							
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - K. SMITH 11-18-22	DEPARTMENT OF TRANSPORTATION							BD-36		CONTRACT	T NO.	
PLOT DATE = 11/18/2022	DATE -	REVISED -		SCALE: NONE	SHEET 1 OF 1 S	SHEETS	STA.	TO STA.		IL	LINOIS FED. A	D PROJECT		

PATRICK ENGINEERING STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENEVA ROAD

IDOT DISTRICT 1 STANDARD DRAWINGS

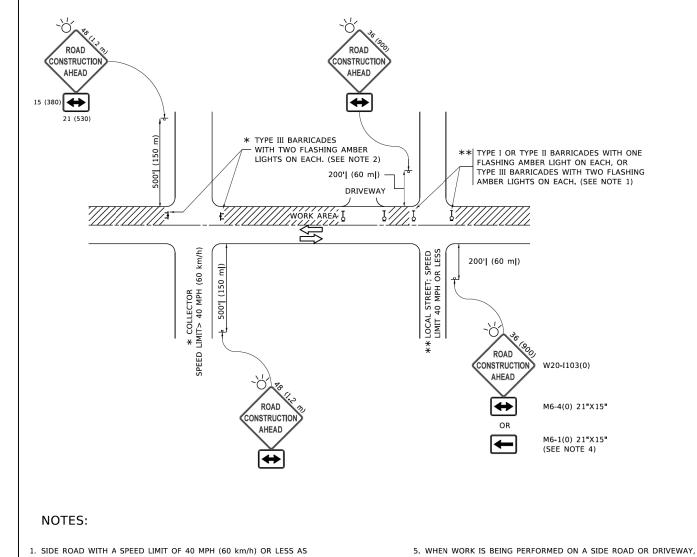
TS SHEET 5 OF 18 SHEETS STA. TO STA.

F.A. U. SECTION COUNTY TOTAL SHEET NO.

1397 18-00206-10-BR DUPAGE 134 103

CONTRACT NO. 61J30

| ILLINOIS | FED. AID | PROJECT



- SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710)
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE 4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

SCALE: I

- FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- 6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE
- 7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters)

USER NAME = footemj	DESIGNED	-	L.H.A.	REVISED	- A. HOUSEH 10-15-96
	DRAWN	-		REVISED	- T. RAMMACHER 01-06-0
PLOT SCALE = 50.0000 ' / in.	CHECKED	-		REVISED	- A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE	-	06-89	REVISED	_ A. SCHUETZE 09-15-16

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

	TRAFF	IC	CONT	RO	L AND F	PROTEC	TION FOR
S	IDE RO	ADS	S, INTI	ERS	SECTIONS	S, AND	DRIVEWAYS
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F.A. RTE.	SEC	ΠΟN		COUNTY	TOTAL SHEETS	SHE
	TC-10	CONTRACT	NO.			
		ILLINOIS	FED. A	ID PROJECT		

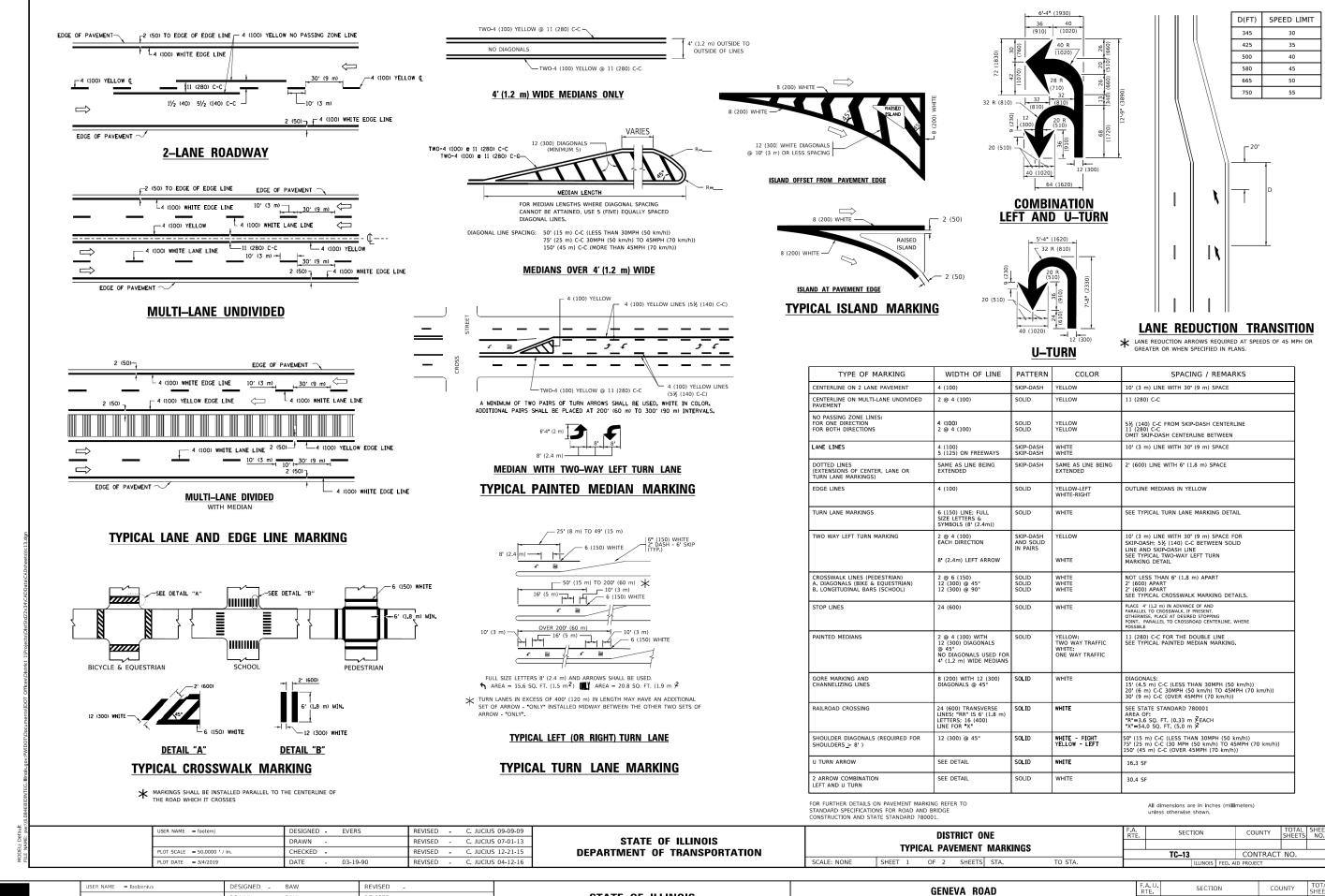
PATRICK	
ENGINEERING	
ENGINEERING	

USER NAME = Ibolzenius	DESIGNED -	BAW	REVISED -
	DRAWN -	BAW	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	LB	REVISED -
PLOT DATE = 12/28/2023	DATE -	9/1/23	REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

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	IDOT	DIST	RICT	1	STANDA	\RD	DRAWINGS
SCALE: NTS	SHEET	6	OF 1	8	SHEETS	STA.	TO STA.

F.A. U. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEE
1397	18-00206-10-BR	DUPAGE	134	104
		CONTRACT	NO. 6	51J30



DRAWN BAW REVISED HECKED -REVISED REVISED

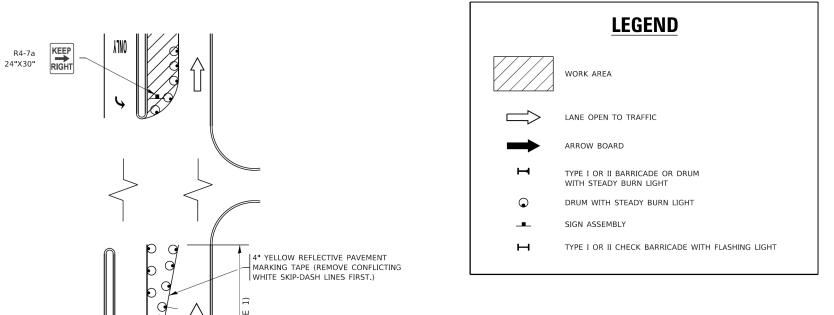
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** **IDOT DISTRICT 1 STANDARD DRAWINGS**

18-00206-10-BR DUPAGE 134 105 CONTRACT NO. 61J30

SHEET 7 OF 18 SHEETS STA.

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

SEE DETAIL "A"

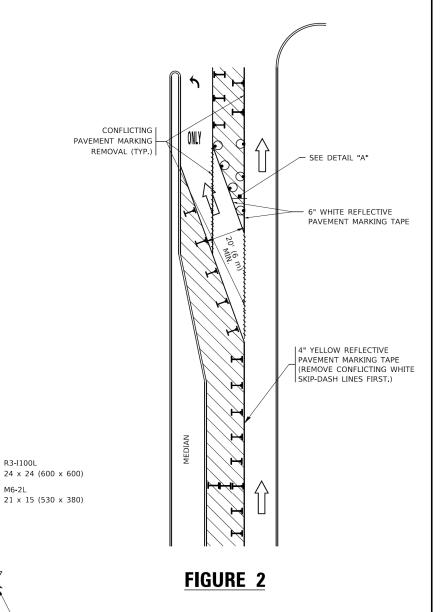


NOTES:

- ARROW BOARD

- 1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
- B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- 2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- 3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- 4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- 5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- 6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- 7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREOUIREMENTS.
- 8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE



DETAIL A

STABILIZE SIGN SUPPORT WITH

SANDBAGS AS

NECESSARY

LEFT

TURN

LANE

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj	DESIGNED -T. RAMMACHER 09-08-94	REVISED	- R. BORO 09-14-09		TRA	FFIC CONTR	RUI A	NID PR	RULEU	TION AT TU	RN RAVS	F.A.	SECTION	ON COUNTY	TOTAL	L SHEE
	DRAWN - A. HOUSEH 11-07-95	REVISED	- A. SCHUETZE 07-01-13	STATE OF ILLINOIS	'"						IIIV DAIS	IXIE.			- SHEET	3 110.
PLCT SCALE = 50.0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED	- A. SCHUETZE 09-15-16	DEPARTMENT OF TRANSPORTATION		(10 1	KEWA	IIN UP	'EN I	O TRAFFIC)			TC-14	CONTRA	ACT NO.	
PLCT DATE = 3/4/2019	DATE -T. RAMMACHER 01-06-00	REVISED	=		SCALE: NONE	SHEET 1	OF	1 SI	HEETS	STA.	TO STA.		II	LINOIS FED. AID PROJECT		

SCALE: NTS



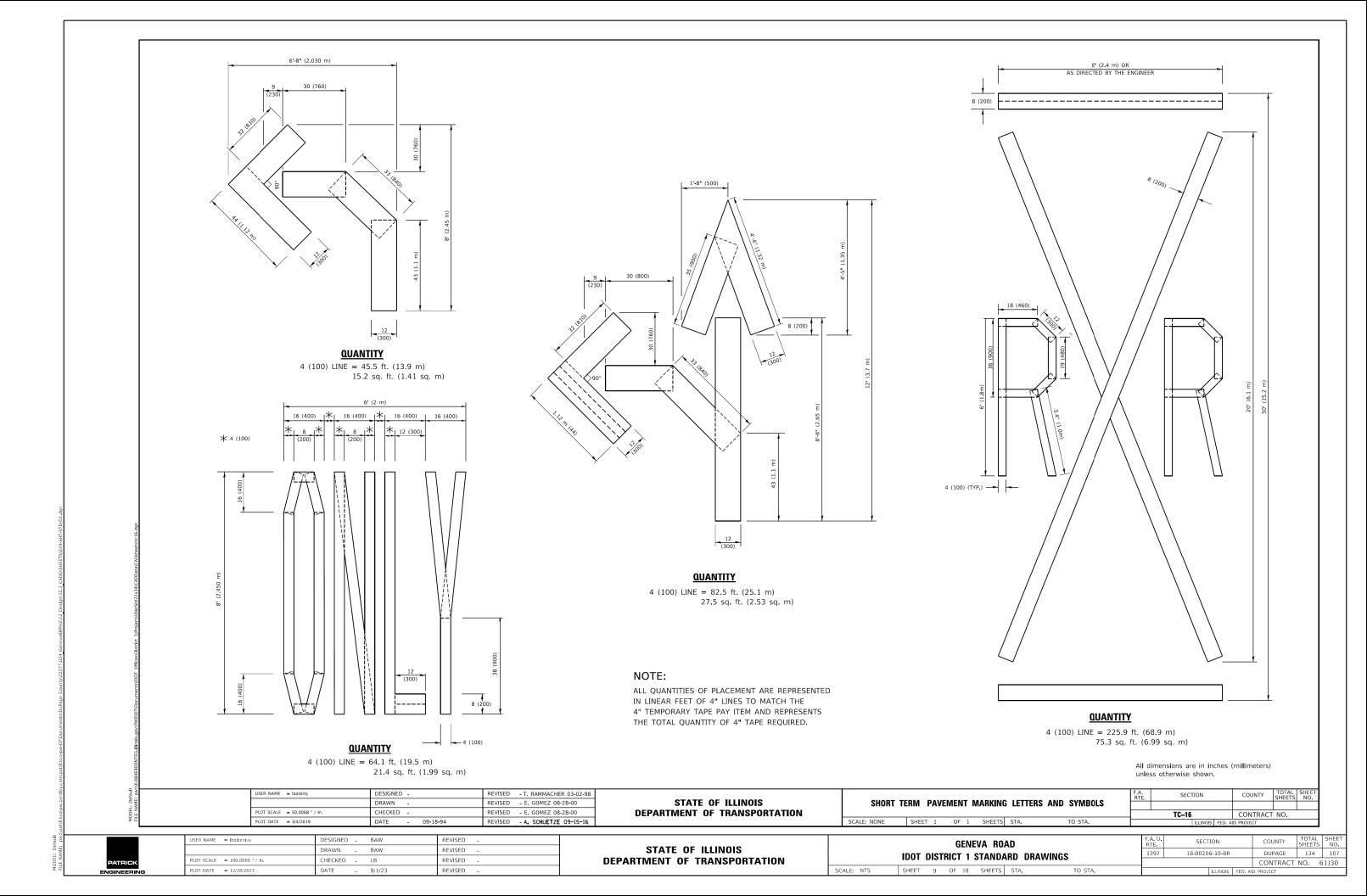
USER NAME = Ibolzenius	DESIGNED -	BAW	REVISED -
	DRAWN -	BAW	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	LB	REVISED -
PLOT DATE = 12/28/2023	DATE -	9/1/23	REVISED -

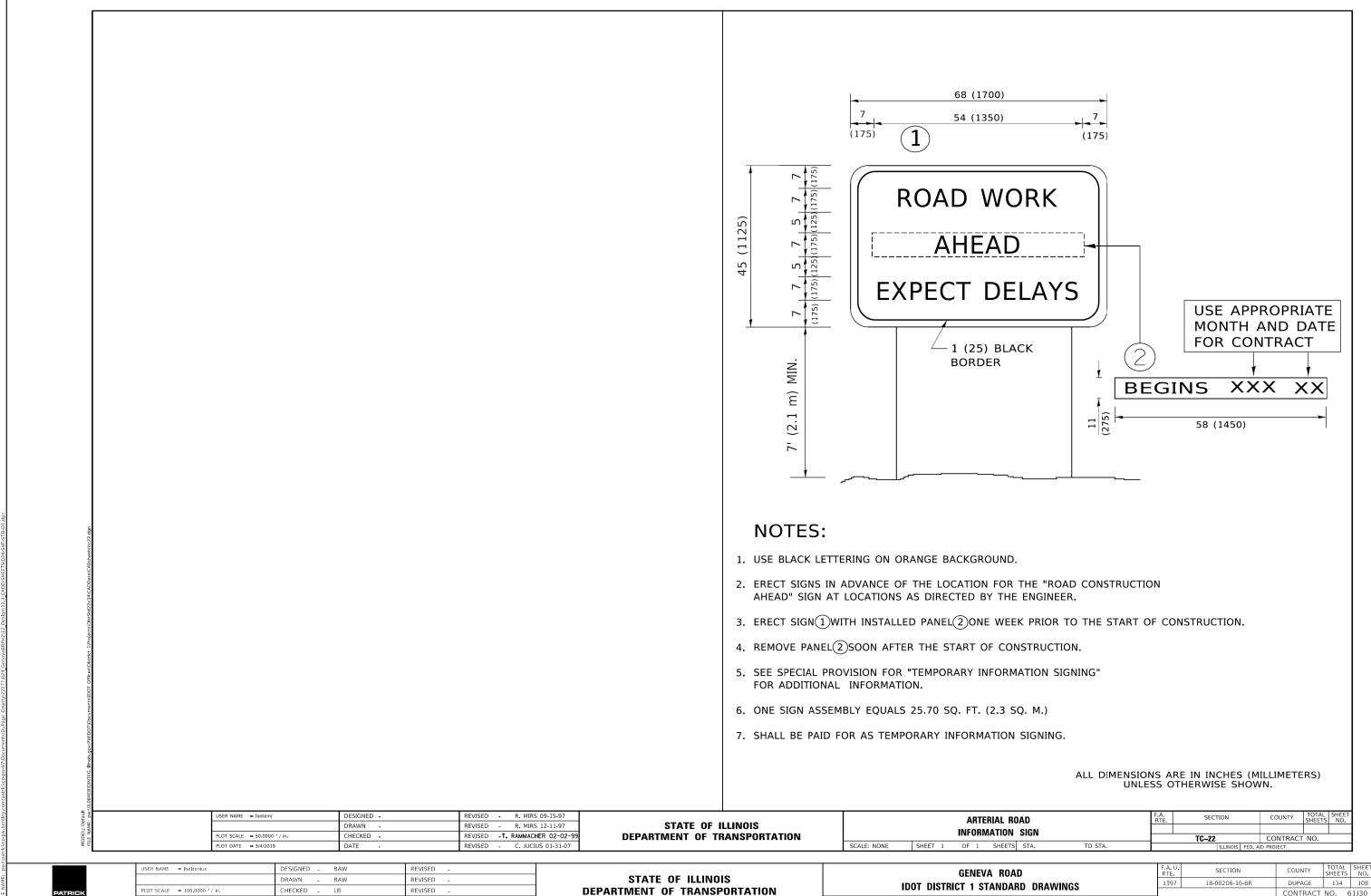
FIGURE 1

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SECTION **GENEVA ROAD** 1397 18-00206-10-BR **IDOT DISTRICT 1 STANDARD DRAWINGS** SHEET 8 OF 18 SHEETS STA.

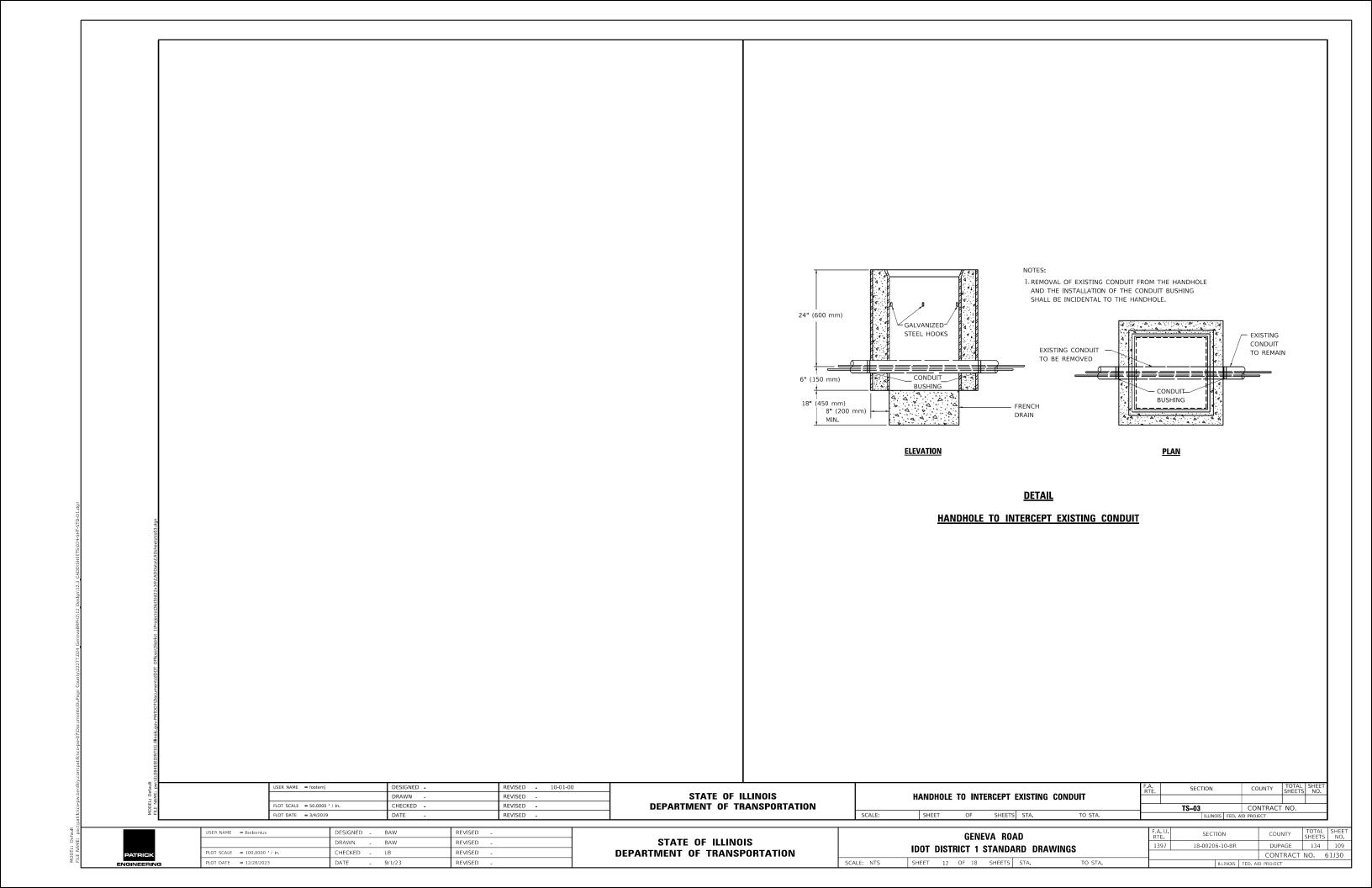
DUPAGE 134 106 CONTRACT NO. 61J30





CONTRACT NO. 61J30

SHEET 11 OF 18 SHEETS STA.



TRAFFIC SIGNAL LEGEND (NOT TO SCALE)

HANDHOLE -SQUARE -ROUND HEAVY DUTY HANDHOL -SQUARE -ROUND DOUBLE HANDHOLE JUNCTION BOX RAILROAD CANTILEVER RAILROAD FLASHING S RAILROAD CROSSING O RAILROAD CROSSING O RAILROAD CONTROLLE! UNDERGROUND CONDU GALVANIZED STEEL TEMPORARY SPAN WIR TETHER WIRE, AND CA SYSTEM ITEM INTERSECTION ITEM REMOVE ITEM ABANDON ITEM CONTROLLER CABINET FOUNDATION TO BE RE FS SIGNAL POST AND FOUNDATION TO BE RE DETECTOR LOOP, TYPE	R MAST ARM SIGNAL GATE K ER CABINET DUIT (UC), RE, ABLE		E B SP IP R R RL A RCF	SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN" NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN) ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		R Y G Y G Y G Y G Y G Y G Y G Y G Y G Y
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DETECTOR LOOP, TYPE	REMOVED		RPF	COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		(6#18)
.PS PREFORMED DETECTOR	ΕΙ			FIBER OPTIC CABLE -NO. 62.5/125, MM12F		—(12F)—
THE OHNED BETECTOR	R LOOP	РР	РР	-NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
SAMPLING (SYSTEM) DI	DETECTOR	s s	s s			—(36F)—
	AMPLING	IS (S)	IS (IS)		, —	
QUEUE AND SAMPLING	3	os os	QS QS	GROUND ROD -(C) CONTROLLER -(M) MAST ARM	<u>a</u> C <u>a</u> M <u>a</u> P <u>a</u> S	±C ±M ±P ±S
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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

GENEVA ROAD IDOT DISTRICT 1 STANDARD DRAWINGS SHEET 13 OF 18 SHEETS STA.

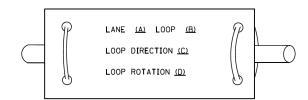
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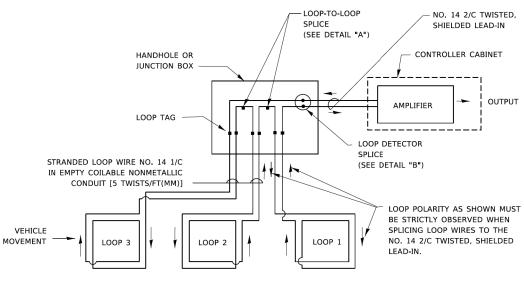
LOOP DETECTOR NOTES

- 1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- 2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- 6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

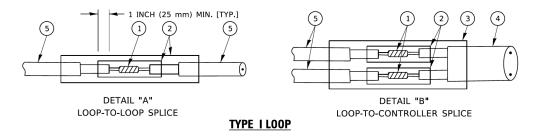


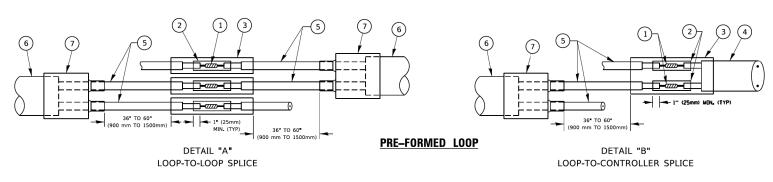
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.





LOOP DETECTOR SPLICE

- (1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- (4) NO. 14 2/C TWISTED, SHIELDED CABLE.

- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- (7) BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

GENEVA ROAD IDOT DISTRICT 1 STANDARD DRAWINGS SHEET 14 OF 18 SHEETS STA.

18-00206-10-BR DUPAGE 134 111 CONTRACT NO. 61J30

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND

PEDESTRIAN PUSHBUTTON DETECTORS.

SEE NOTE 1

ONTE 2

FT.

(600 mm)

TYP.

SEE NOTE 3

SEE NOTE 3

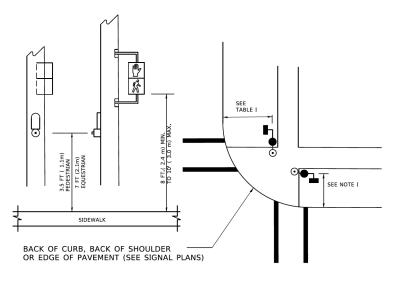
SEE NOTE 3

NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.

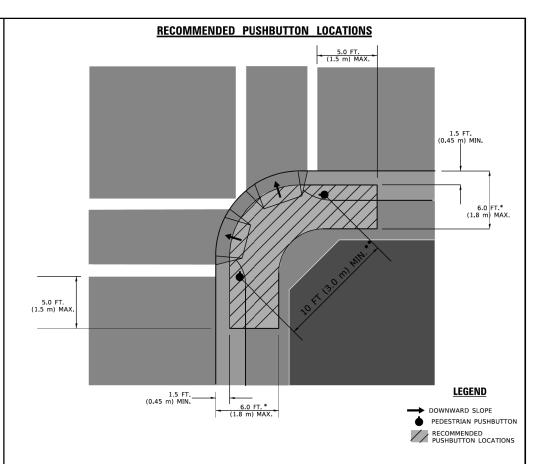
- 2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
- 4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES"

PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

- 1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
- 3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- . THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND EACH LITIES."



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

- PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
- THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY
- 3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
- THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

	THATTIC SIGNAL EQUITMENT OFFS	·-·
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

- 1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
- 2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
- 3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
- 4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLAN AND THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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PATRICK ENGINEERING

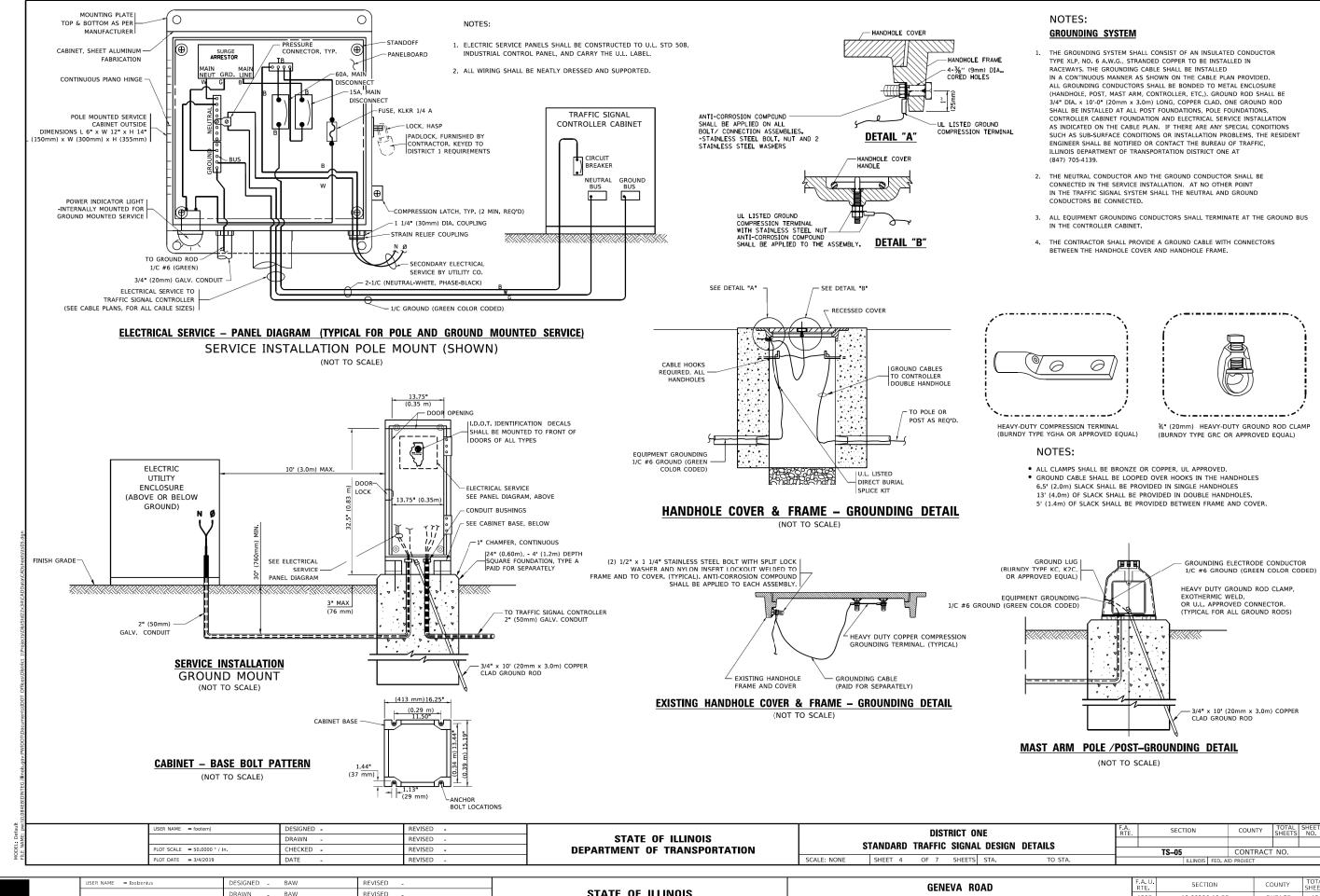
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

GENEVA ROAD	F.A. U. RTE	SECT	COUNTY	TOTAL SHEETS	SHEET NO.		
IDOT DISTRICT 1 STANDARD DRAWINGS	1397	18-0020	18-00206-10-BR		DUPAGE	134	112
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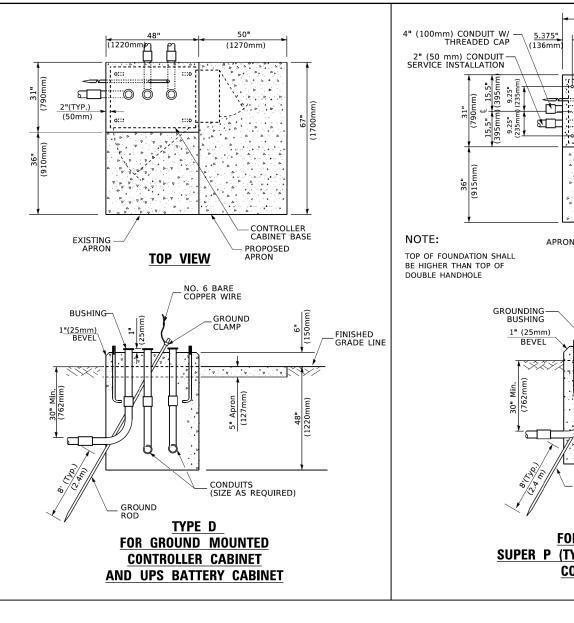


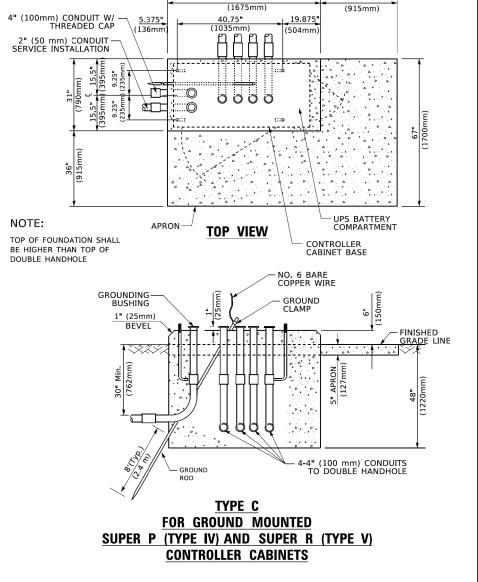
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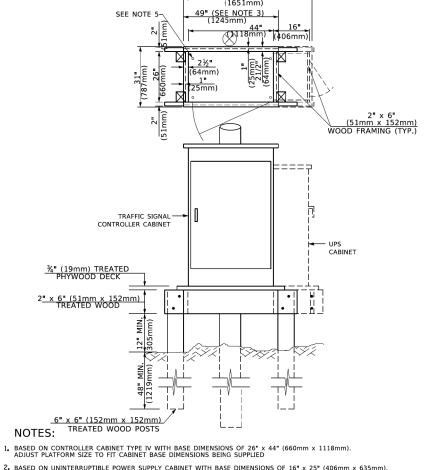
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENEVA ROAD
IDOT DISTRICT 1 STANDARD DRAWINGS

SHEET 16 OF 18 SHEETS STA. TO STANDARD DRAWINGS







- BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm).
 ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- 3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
- 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER **WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD)		
(L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SOUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30′ (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to	13'-6" (4,1 m)	30" (750mm)	24" (600mm)	8	6(19)
30' (9.1 m) and less than 40' (12.2 m)	11'-0'' (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4 . 0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0'' (4.6 m)	36" (900mm)	30'' (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36'' (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36'' (900mm)	16	8(25)

- 1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shoft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpc). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation of lilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
- 2. Combination mast arm assemblies under 55 feet (16.8 m) shall use $36^{\prime\prime}$ (900 mm) diameter foundations.
- 3. Combination most arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
- 4. For most arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

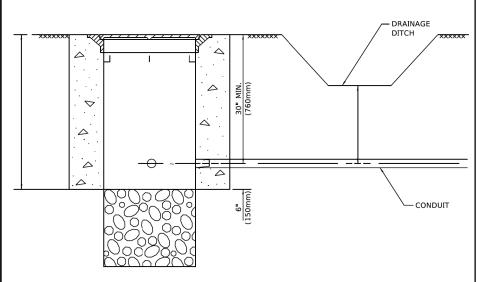
USER NAME = footemj	DESIGNED -	REVISED -	·		DISTRICT ONE		F.A.	SECTION	COUNTY	TOTAL S	HEET
	DRAWN -	REVISED -	STATE OF ILLINOIS	۱ .		KIE.		3	JHEE 13	١٥.	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	S	TANDARD TRAFFIC SIGNAL D		TS-05	CONTRACT	NO.	_	
PLOT DATE = 3/4/2019	DATE -	REVISED -		SCALE: NONE	SHEET 5 OF 7 SHEETS ST	TA. TO STA.		ILLINOIS FE	D. AID PROJECT		

BAW SER NAME = Ibolzenius DESIGNED -DRAWN BAW OT SCALE = 100.0000 ' / in. HECKED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

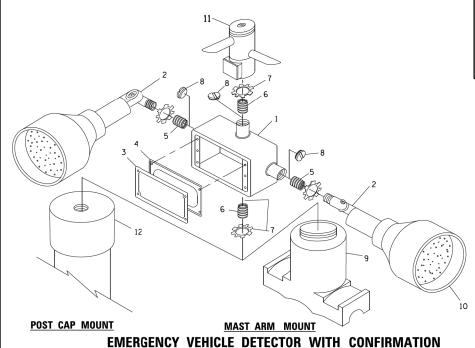
SECTION **GENEVA ROAD** 18-00206-10-BR DUPAGE 134 114 **IDOT DISTRICT 1 STANDARD DRAWINGS** CONTRACT NO. 61J30 SHEET 17 OF 18 SHEETS STA.

REVISED REVISED REVISED REVISED



- 1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM
- 2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- 3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH



(1675mm (915mm) 40.75" 19.875" (504mm) (1035mm) CONTROLLER CABINET BASE PROPOSED-**TOP VIEW** APRON -NO. 3 DOWEL 18" (450mm NO. 6 BARE COPPER WIRE LONG (8 REQ.) **BUSHING-**GROUND. EXISTING: ANCHOR BOLTS FINISHED GRADE LINE BEVEL (300mm) 12" (300mm) 12" (300mm) (225mm) (225mm) -EXISTING CONDUITS EXISTING GROUND ROD

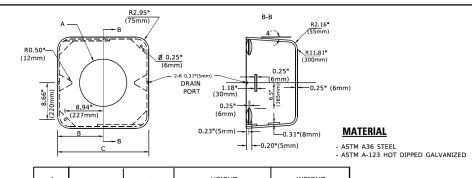
MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)

1 OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M) 2 LAMP HOLDER AND COVER 3 OUTLET BOX COVER RUBBER COVER GASKET REDUCING BUSHING ¾"(19 mm) CLOSE NIPPLI 7 %"(19 mm) LOCKNUT 8 %"(19 mm) HOLE PLUG 9 SADDLE BRACKET - GALV. 10 6 WATT PAR 38 LED FLOOD LAMP

NOTES:

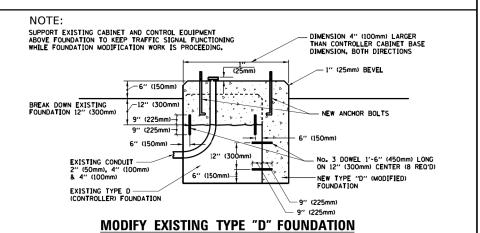
- 1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- 2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- 3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



А	В	С	HEIGHT	WEIGHT
VARIES	9.5*(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18 . 5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

- . DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- 2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- 3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



EXISTING CONDUIT PLAN ELEVATION

- 1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- 2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

BEACON MOU	NTING DETAIL												
JSER NAME = footemj	DESIGNED -	REVISED -			DISTRICT ONE						F.A. RTE.	SECTION	
	DRAWN -	REVISED -	STATE OF ILLINOIS							IXIE.			
PLOT SCALE = 50,0000 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		STANDARD TRAFFIC SIGNAL DESIGN DETAILS					: IAILS		TS-05	
PLOT DATE = 3/4/2019	DATE -	REVISED -		SCALE: NON	IE	SHEET 6	OF 7	SHEETS	STA.	TO STA.			IOIS FE

SER NAME = Ibolzenius BAW DESIGNED -REVISED DRAWN BAW REVISED HECKED -REVISED LOT DATE = 12/28/2023 REVISED

STATE OF ILLINOIS

GENEVA ROAD IDOT DISTRICT 1 STANDARD DRAWINGS

SHEET 18 OF 18 SHEETS STA.

SECTION 18-00206-10-BR DUPAGE 134 115 CONTRACT NO. 61J30

COUNTY CONTRACT NO

DEPARTMENT OF TRANSPORTATION

LOOPS NEXT TO SHOULDERS PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER. NON-PAVED SHOULDER (1.5 m) (1.8 m) (1.5 m) + 1" (25 mm) UNIT DUCT-TRENCHED (3.0 m) (3.0 m) TO F/P ** + = (600 mm) $\frak{ imes}$ Unit duct is to be shown on plan sheets BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS. ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)

* * = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF

AND HANDHOLE.

(TYP. FOR LOOPS

THAT TERMINATE

IN HANDHOLES OUTSIDE PAVEMENT)

DUCT IS RUN BETWEEN

PAVEMENT, 1" (25 mm) UNIT

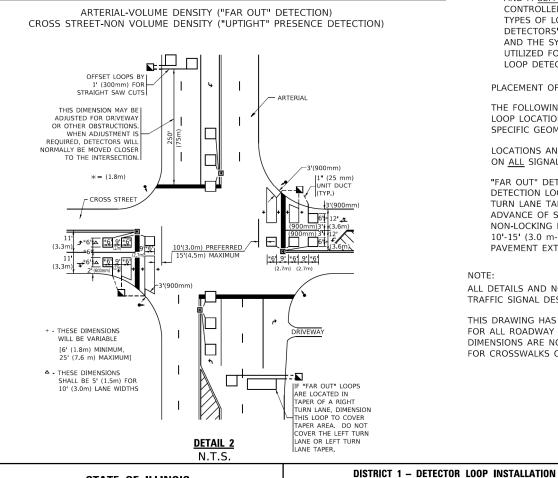
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PLOT SCALE = 50.0000 ' / in

CROSS STREET

LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING) HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN. TRENCHED 1" (25 mm) UNIT DUCT (3) * * + = (600 mm) (900 mn UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS, NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING) + = (600 mm) (900 (1.8 m STRAIGHT SAW CUT TO HEAVY DUTY HANDHOLE (TYP.) PLACE HEAVY DUTY HANDHOLE BETWEEN FIRST AND SECOND LOOP AS SHOWN. NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT



NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- st ONE DIMENSION OF <u>ALL</u> DETECTOR LOOPS SHALL BE SIX FEET
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- st When system loops are required on an approach of an INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A <u>SEPARATE</u> INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

PATRICK	
ENGINEERING	

SER NAME = Ibolzeniu BAW REVISED DESIGNED -RAWN BAW REVISED HECKED REVISED LOT DATE = 12/28/2023 9/1/23 REVISED

STRAIGHT SAW CUTS TO HEAVY-

IN PAVEMENT

DUTY HANDHOLE

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

GENEVA ROAD IDOT DISTRICT 1 STANDARD DRAWINGS SHEET 20 OF 18 SHEETS STA.

SCALE: NONE

DETAILS FOR ROADWAY RESURFACING

SECTION COUNTY 1397 18-00206-10-BR DUPAGE 134 116 CONTRACT NO. 61J30

CONTRACT NO

TS-07

DETAIL 1

N.T.S.

DESIGNED -

CHECKED - R.K.F.

DRAWN

- ARTERIAL

WAY LOCATION

(600mm

[TYP.-12' (3.6m) LANES]

IDO NOT INSTALL

CALLING LOOP IN RIGHT TURN LANE.

IOFF SET LOOPS BY

REVISED -

REVISED -

REVISED

REVISED

