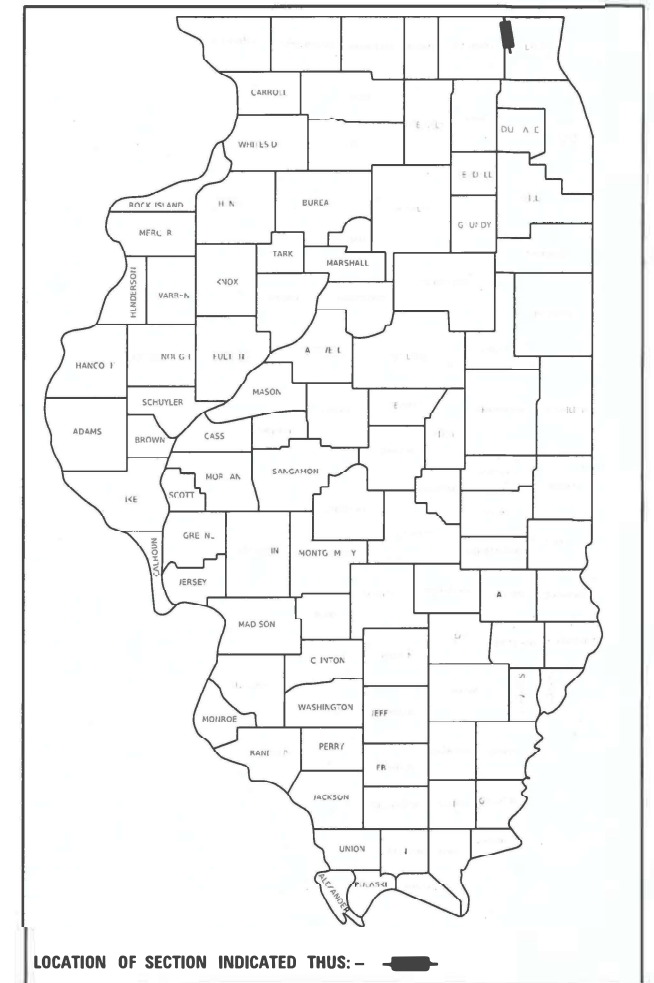


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	1
		ILLINOIS	CONTRACT NO. 62T59	

D-91-231-22



# PROPOSED HIGHWAY PLANS

FAP ROUTE 334: IL 59 /US 12 (RAND ROAD)  
STATE PARK RD /EAST ST TO NORTH OF IL 120

SECTION: FAP 0334 22 RS

PROJECT: NHPP-I8QQ(544)

SMART OVERLAY AND ADA RAMP IMPROVEMENTS

LAKE & MCHENRY COUNTY

C-91-284-22

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN  
THE VILLAGES OF SPRING GROVE, FOX  
LAKE AND VOLO

**TRAFFIC DATA:**

US 12:

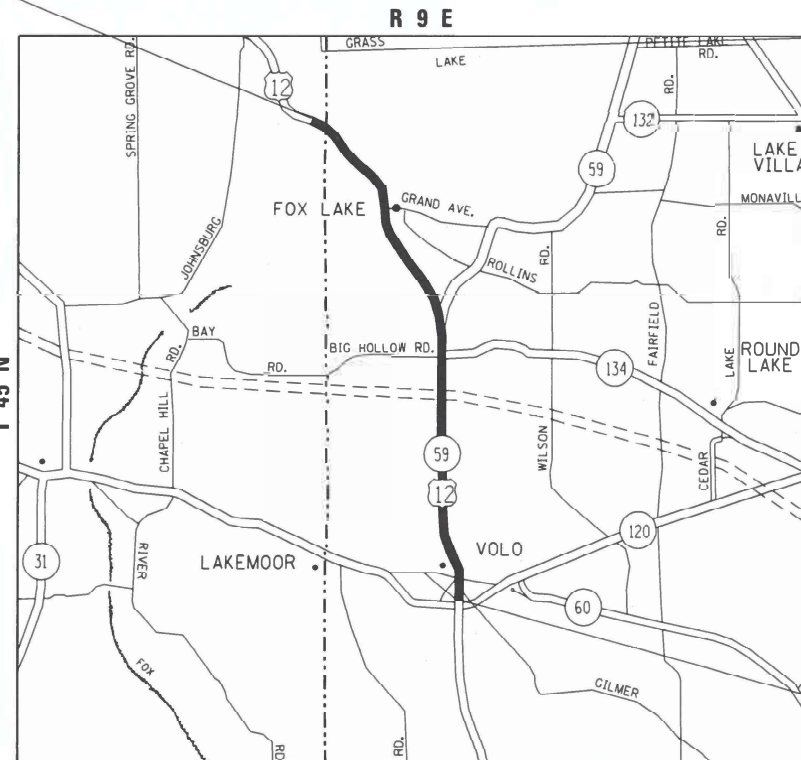
PROJECT BEGIN TO PROJECT END:

ADT (2021) = 29,600

SPEED LIMIT = 50 MPH ,  
45 MPH ,

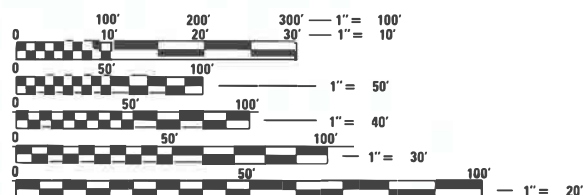
55 MPH

PROJECT STARTS  
STA 12+60



LOCATION MAP  
(NOT TO SCALE)

PROJECT ENDS  
STA 377+98



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

MCHENRY, GRANT & WAUCONDA TOWNSHIPS

NET LENGTH = 32,135 FEET. = 6.09 MILES  
GROSS LENGTH = 36,538 FEET. = 6.92 MILES  
OMMISSIONS = 4,403 FEET = 0.84 MILES

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240  
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62T59

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *August 18, 2023*  
REGIONAL ENGINEER

February 2, 2024  
ENGINEER OF DESIGN AND ENVIRONMENT

February 2, 2024  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

## INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES
3-6	SUMMARY OF QUANTITIES
7-8	TYPICAL SECTIONS
9-21	ROADWAY AND PAVEMENT MARKING PLANS
22-36	APS AND DETECTOR LOOP INSTALLATION PLAN / TRAFFIC SIGNAL CABLE PLANS
37-40	ADA RAMP DESIGNS
41-46	ADA RAMP STANDARDS
47	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)
48	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
49	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
50	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
51	ENTRANCE AND EXIT RAMP CLOSURES DETAILS (TC-08)
52	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
53	TYPICAL APPLICATIONS FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)
54	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
55	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TC-14)
56	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
57	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
58	ARTERIAL ROAD INFORMATION SIGN (TC-22)
59	DETECTOR LOOP INSTALLATION DETAILS FOR RESURFACING (TS-07)

## HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-05	FRAMES AND LIDS TYPE 1
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 M) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701101-05	OFF-RD MOVING OPERATIONS, MULTILANE 15' TO 24" FROM PAVEMENT
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE 2L, 2W, UNDIVIDED
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOPS INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

## GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF SPRING GROVE, FOX LAKE, VOLO, AND UNINCORPORATED INGLESIDE.
3. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KAPLANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
4. THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER FADI SULTAN AT FADI.SULTAN@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA).
6. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
7. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
8. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER
9. ALL FINAL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
10. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
11. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
12. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
13. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
14. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
15. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
16. TEMPORARY PAVEMENT MARKING, TYPE III TAPE SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
17. WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
18. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
19. ALL MILLED SURFACES SHALL BE AT A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO DEPARTMENT.
20. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
21. SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
22. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
334	FAP 0334 22 RS	LAKE & MCHENRY	59
			2
CONTRACT NO. 62T59			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

ILLINOIS	FED. AID PROJECT
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SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED	80% FED	100% STATE	80% FED	80% FED
				20% STATE LAKE 0005	20% STATE MCHENRY 0005	LAKE 0005	20% STATE LAKE PEDESTRIAN SIGNAL 0021	20% STATE MCHENRY PEDESTRIAN SIGNAL 0021
20200100	EARTH EXCAVATION	CU YD	25	25				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	139	139				
25200110	SODDING, SALT TOLERANT	SO YD	139	139				
25200200	SUPPLEMENTAL WATERING	UNIT	1.4	1.4				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	130035	128607	1428			
40600370	LONGITUDINAL JOINT SEALANT	FOOT	116050	114509	1541			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	434	429	5			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1213	1122	91			
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	273	273				
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	3509	3509				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	24572	24261	311			
42001300	PROTECTIVE COAT	SO YD	235	235				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2080	2080				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED	80% FED	100% STATE	80% FED	80% FED
				20% STATE LAKE 0005	20% STATE MCHENRY 0005	LAKE 0005	20% STATE LAKE PEDESTRIAN SIGNAL 0021	20% STATE MCHENRY PEDESTRIAN SIGNAL 0021
42400800	DETECTABLE WARNINGS	SO FT	190	190				
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SO YD	286530	283357	3173			
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	2436	2436				
44000600	SIDEWALK REMOVAL	SO FT	2080	2080				
44003100	MEDIAN REMOVAL	SO FT	5998	5998				
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SO YD	3300	3300				
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SO YD	1650	1650				
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SO YD	825	825				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1042	1042				
56109210	WATER VALVES TO BE ADJUSTED	EACH	5	5				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	10	10				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	10	10				
60255500	MANHOLES TO BE ADJUSTED	EACH	4	3	1			
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	30	30				

\* = SPECIALTY ITEMS  
△ = NON-PARTICIPATING WORK (100% STATE)

FILE NAME =	USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000 "/ In.	CHECKED -	REVISED -
	PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
<b>IL-59 /US 12 (RAND RD) FROM STATE PARK RDEAST ST TO N OF IL 120</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T59	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE LAKE 0005	80% FED 20% STATE MCHENRY 0005	100% STATE LAKE 0005	80% FED 20% STATE LAKE PEDESTRIAN SIGNAL 0021	80% FED 20% STATE MCHENRY PEDESTRIAN SIGNAL 0021
60256500	MANHOLES TO BE ADJUSTED WITH NEW TYPE 9	EACH	4	4				
	FRAME AND GRATE							
60256940	MANHOLES TO BE ADJUSTED WITH NEW TYPE	EACH	10	10				
	24 FRAME AND GRATE							
60260300	INLETS TO BE ADJUSTED WITH NEW TYPE 1	EACH	30	30				
	FRAME, OPEN LID							
60261100	INLETS TO BE ADJUSTED WITH NEW TYPE 9	EACH	4	4				
	FRAME AND GRATE							
60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24	EACH	10	10				
	FRAME AND GRATE							
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	60	60				
60619200	CONCRETE MEDIAN, TYPE SB-6.06	SQ FT	5998	5998				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	25	25				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION	L SUM	1	1				
	PLAN							
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION	L SUM	1	1				
	REPORT							
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	6	6				

SUMMARY OF QUANTITIES				TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	80% FED 20% STATE LAKE 0005		80% FED 20% STATE MCHENRY 0005	100% STATE LAKE 0005	80% FED 20% STATE LAKE PEDESTRIAN SIGNAL 0021	80% FED 20% STATE MCHENRY PEDESTRIAN SIGNAL 0021	
67100100	MOBILIZATION	L SUM	1	0.9	0.1				
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	0.9	0.1				
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	0.9	0.1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	0.9	0.1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	0.9	0.1				
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	0.9	0.1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.9	0.1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	0.9	0.1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	31150	29916	1234				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	5192	4986	206				
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	3164	2995	169				

\* = SPECIALTY ITEMS  
△ = NON-PARTICIPATING WORK (100% STATE)

FILE NAME =	USER NAME = Wendy.Andonayre	DESIGNED -	REVISOR -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b> IL-59 /US 12 (RAND RD) FROM STATE PARK RDEAST ST TO N OF IL 120			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
pw\N\dot-pw\benley.com\PW\DOT\Documents\1007	Offices\Dist\1\Projects\DI23122\CAD\dot\Des\gn\DI23122-sh-5	DRAWN -	REVISOR -					334	FAP 0334 22 RS	LAKE & MCHENRY	59	4			
	PLOT SCALE = 100,0000' / 1in	CHECKED -	REVISOR -					SCALE:			SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
	PLOT DATE = 12/15/2023	DATE -	REVISOR -												

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
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70300221	TEMPORARY PAVEMENT MARKING - LINE 4"-	FOOT	126565	125615	950			* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	8316	7889	427				
	PAINT								12"								
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"-	FOOT	13841	13426	415			* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	1543	1222	321				
	PAINT								24"								
70300251	TEMPORARY PAVEMENT MARKING - LINE 8"-	FOOT	4734	4637	97			* 78004635	PREFORMED PLASTIC PAVEMENT MARKING TYPE D -LINE 7"	FOOT	12698	12698					
	PAINT							* 78009004	MODIFIED URETHANE PAVEMENT MARKING -	FOOT	3842	3842					
									LINE 4"								
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"-	FOOT	8316	7889	427			* 78009005	MODIFIED URETHANE PAVEMENT MARKING -LINE 5"	FOOT	360	360					
	PAINT							* 78009012	MODIFIED URETHANE PAVEMENT MARKING -	FOOT	450	450					
									LINE 12"								
70300281	TEMPORARY PAVEMENT MARKING - LINE 24"-	FOOT	1543	1222	321			* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4110	4100	10				
	PAINT																
70306120	TEMPORARY PAVEMENT MARKING - LINE 4" -	FOOT	15575	14958	617												
	TYPE III TAPE																
* 78000100	THERMOPLASTIC PAVEMENT MARKING -	SO FT	3164	2995	169												
	LETTERS AND SYMBOLS																
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	126565	125615	950			* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL,	FOOT	180			180			
	4"								2" DIA.								
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	13841	13426	415			* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL	EACH	4			4			
	6"								INSTALLATION								
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	4734	4637	97			* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	1407			1407			
	8"								14 2C								
								* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	1108			1108			
									14 3C								

\* = SPECIALTY ITEMS  
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FILE NAME =	USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		<b>SUMMARY OF QUANTITIES IL-59 /US 12 (RAND RD) FROM STATE PARK RDEAST ST TO N OF IL 120</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
pw\N\dot-pw\benfrey.com\PW\DOT\Documents\1007	Office\dot-pw\benfrey.com\Projects\DI23122\CAD\dot-pw\benfrey.com\DOT\123122-sh-50	DRAWN -	REVISED -							334	FAP 0334 22 RS	LAKE & MCHENRY	59	5			
		CHECKED -	REVISED -							SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT
		DATE -	REVISED -														

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE														
CODE NO	ITEM	UNIT		80% FED 20% STATE LAKE 0005	80% FED 20% STATE MCHENRY 0005	100% STATE LAKE 0005	80% FED 20% STATE LAKE PEDESTRIAN SIGNAL 0021	80% FED 20% STATE MCHENRY PEDESTRIAN SIGNAL 0021	CODE NO	ITEM	UNIT		80% FED 20% STATE LAKE 0005	80% FED 20% STATE MCHENRY 0005	100% STATE LAKE 0005	80% FED 20% STATE LAKE PEDESTRIAN SIGNAL 0021	80% FED 20% STATE MCHENRY PEDESTRIAN SIGNAL 0021										
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1298				1298									X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	4200	4150	50						
* 87900200	DRILL EXISTING HANDHOLE	EACH	5				5									△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	2100				2100				
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	5				5									X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	60	60							
* X8860105	DETECTOR LOOP REPLACEMENT	FOOT	7264				7114	150								X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	10	2						
* 89500200	RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	4				4								*	X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	26					26			
* 89502200	MODIFY EXISTING CONTROLLER	EACH	4				4								*	X8780012	CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	44					44			
* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	17				17								△	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	220				220				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	0.9	0.1											Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	179.9	154.2	25.7						
* X1400367	PEDESTRIAN SIGNAL POST, 10 FT.	EACH	5				5								∅	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1							
* X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	3				3								∅	Z0076600	TRAINEES	HOURS	500	500							
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	660	660												Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500							
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	740	740																							

∅ 0042  
 \* = SPECIALTY ITEMS  
 △ = NON-PARTICIPATING WORK (100% STATE)

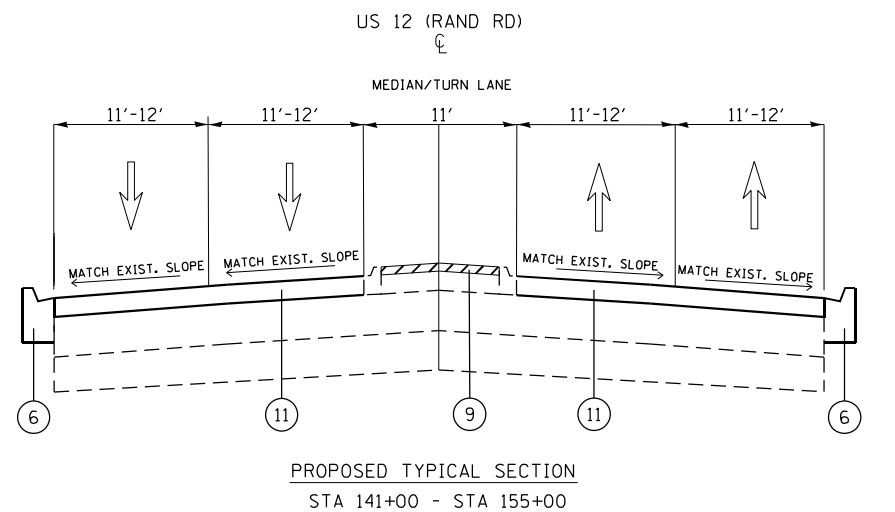
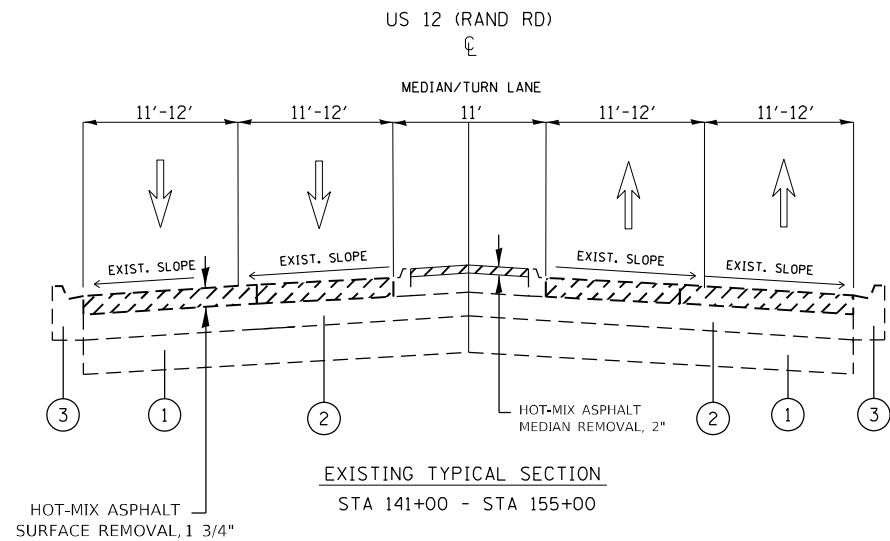
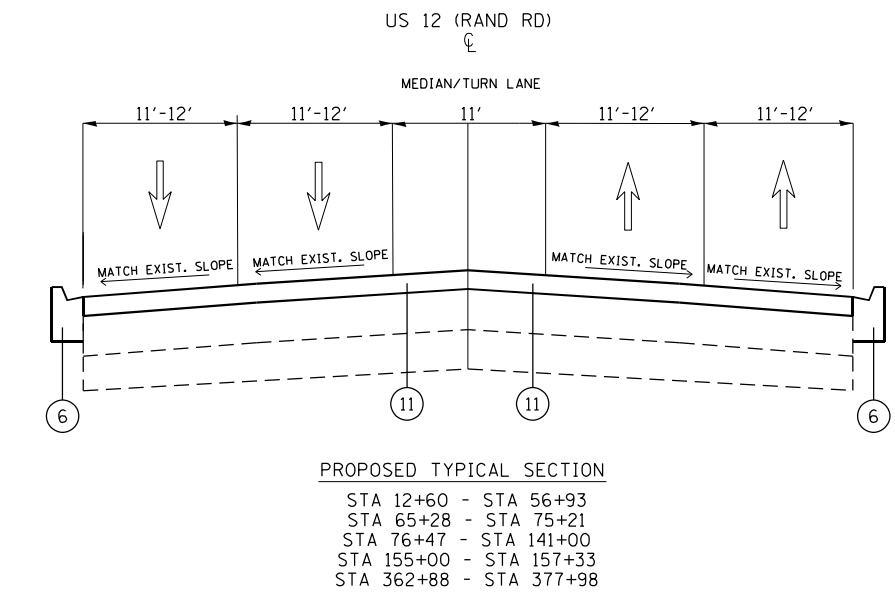
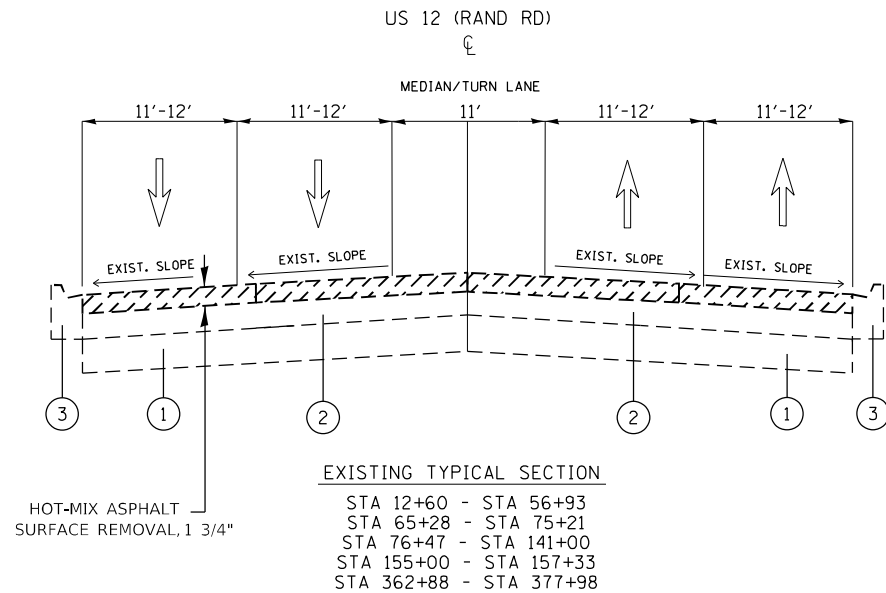


**LEGEND - EXISTING:**

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10"±
- ② HOT-MIX ASPHALT PAVEMENT 5.5"±
- ③ COMBINATION CONCRETE CURB AND GUTTER
- ④ EXISTING HOT-MIX ASPHALT SHOULDER 8"
- ⑤ EXISTING AGGREGATE SHOULDER

**LEGEND - PROPOSED**

- ⑥ COMBINATION CURB AND GUTTER (REMOVAL AND REPLACEMENT DETERMINED BY RE)
- ⑦ GRADING AND SHAPING SHOULDERS
- ⑧ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 2"
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1.75"
- ⑪ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT 9.5 MIX "F", N80 1.75"



LOCATION	HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
	MIXTURE TYPE	AIR VOIDS(%) @ Ndes	
	<b>PAVEMENT</b>		
US-12 (RAND RD)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5 MIX "F", N80 1.75 "	3.5% @ 80 GYR.	PPF
	<b>OUTSIDE SHOULDERS</b>		
US-12 (RAND RD)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1.75"	4.0% @ 70 GYR.	OCP
	<b>MEDIANS</b>		
US-12 (RAND RD)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 2"	4.0% @ 50 GYR.	OC/OA
	<b>PATCHING</b>		
US-12 (RAND RD)	CLASS D PATCHES (HMA BINDER IL-19 MM, 14")	4.0% @ 70 GYR.	OC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (OC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP); PAY FOR PERFORMANCE (PPF)			

**NOTE**

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.
- NOTE 3: THE CONTRACTOR SHALL MILL THEN PATCH
- NOTE 4: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACE OVER THE MILLED SURFACE

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTION**  
IL-59 /US 12 (RAND RD) FROM STATE PARK RDEAST ST TO NORTH OF IL 120

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 R5	LAKE & MCHENRY	59	7
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				

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 PLOT SCALE: 1/16" = 100.0002' / in.  
 USER: Wendy.Andonayre  
 DESIGNED: Wendy.Andonayre  
 DRAWN: Wendy.Andonayre  
 CHECKED: Wendy.Andonayre  
 DATE: 12/15/2023

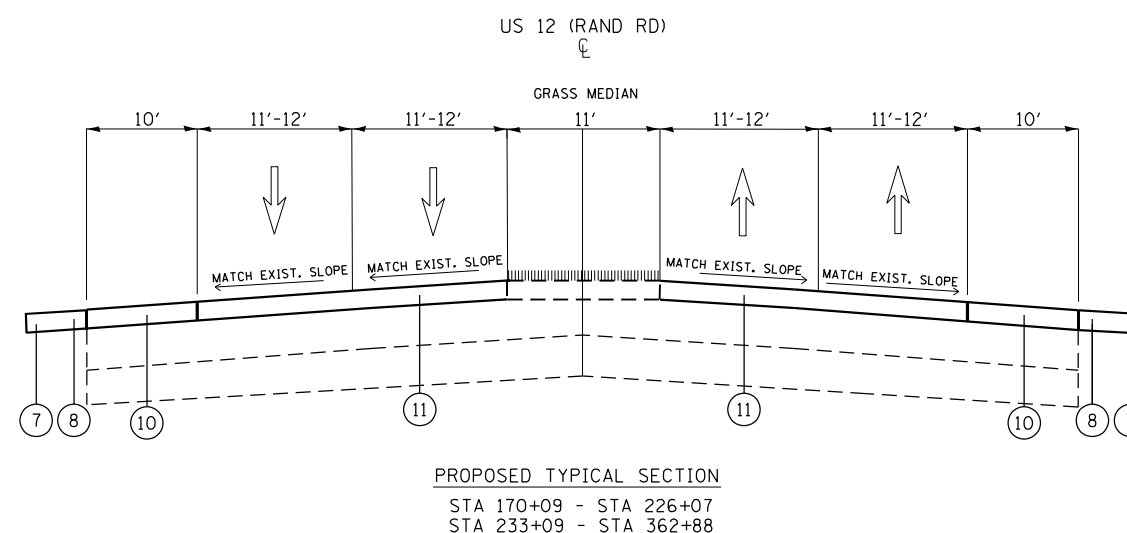
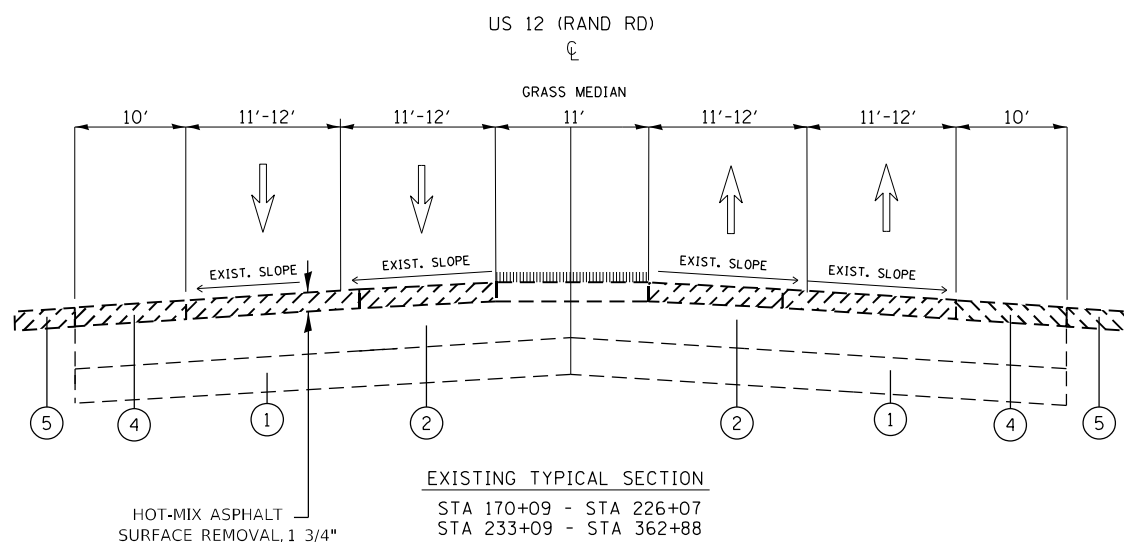
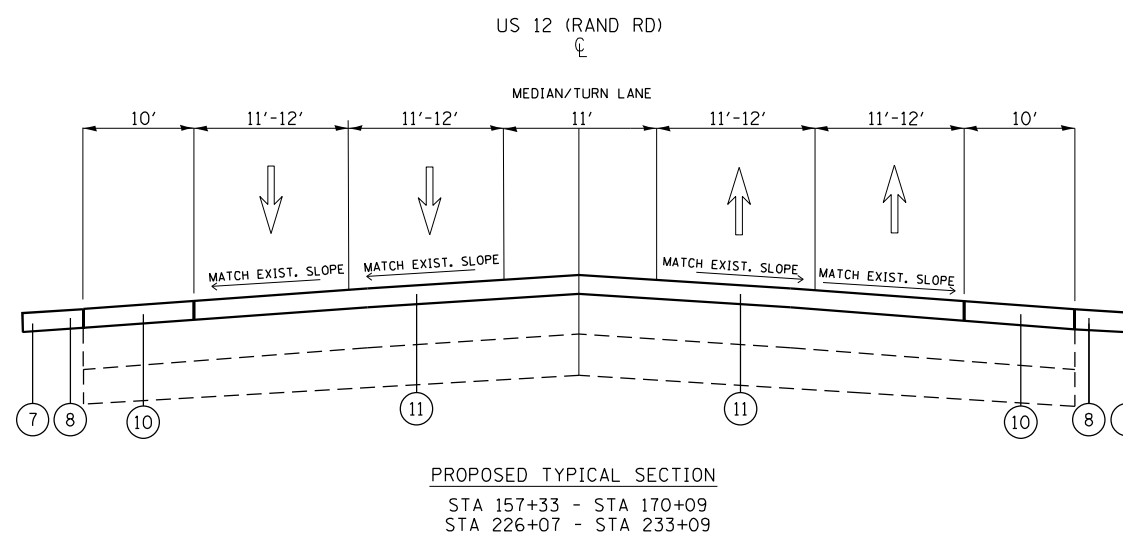
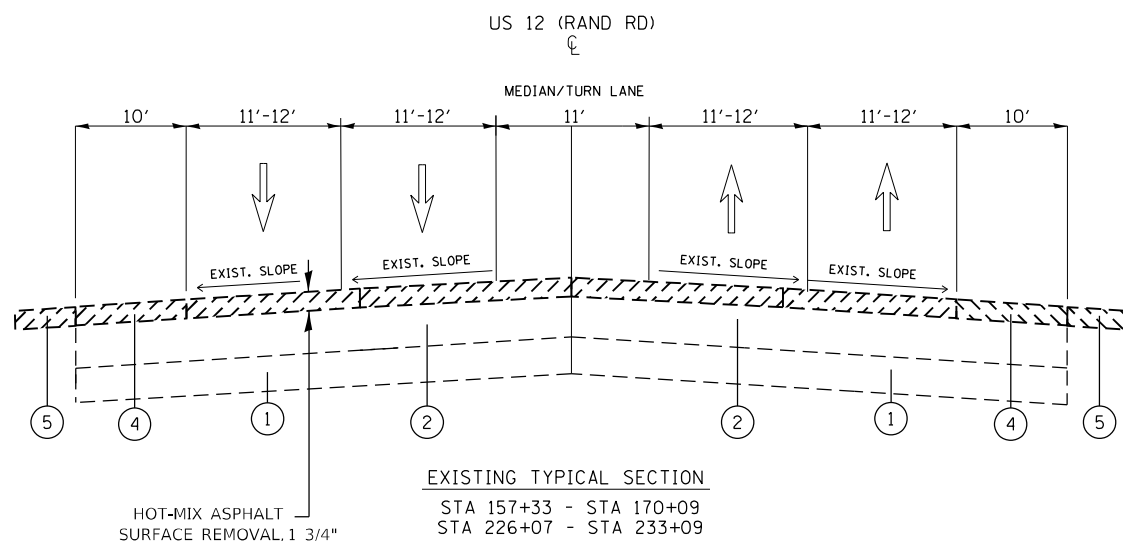
**LEGEND - EXISTING:**

- ① PORTLAND CEMENT CONCRETE PAVEMENT 10"±
- ② HOT-MIX ASPHALT PAVEMENT 5.5"±
- ③ COMBINATION CONCRETE CURB AND GUTTER
- ④ EXISTING HOT-MIX ASPHALT SHOULDER 8"
- ⑤ EXISTING AGGREGATE SHOULDER

- ⑥ COMBINATION CURB AND GUTTER (REMOVAL AND REPLACEMENT DETERMINED BY RE)
- ⑦ GRADING AND SHAPING SHOULDERS
- ⑧ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 2"

**LEGEND - PROPOSED**

- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1.75"
- ⑪ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT 9.5 MIX "F", N80 1.75"



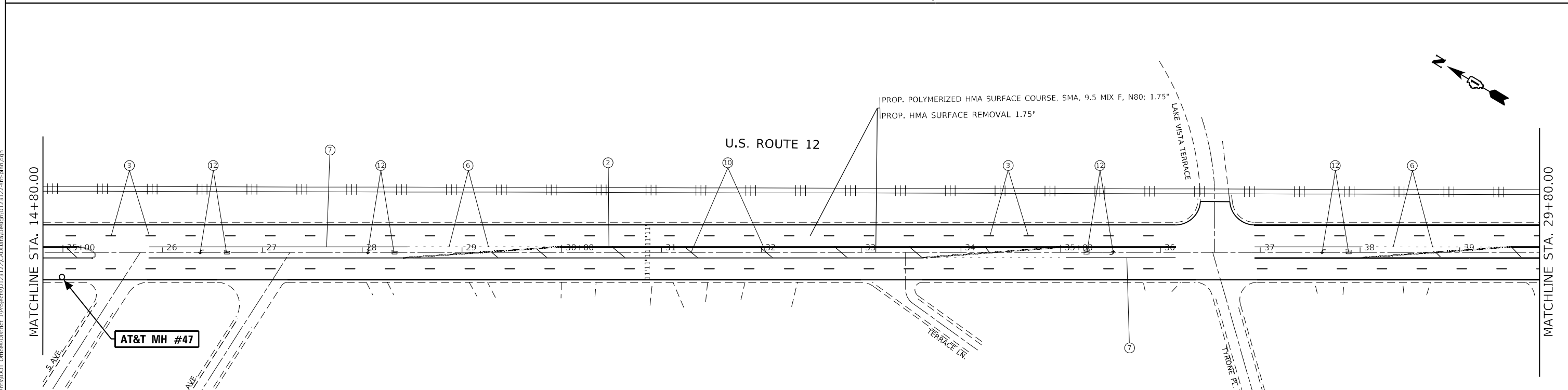
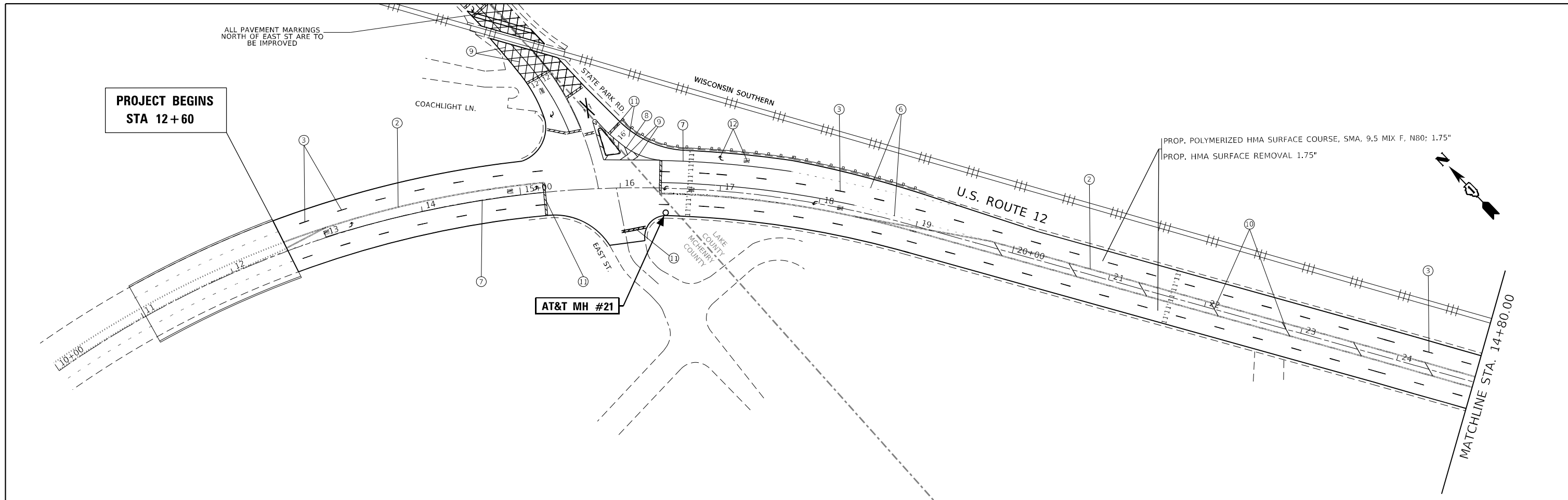
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PLOT SCALE = 100,0002 ' / in.	DRAWN -	REVISED -
PLOT DATE = 12/15/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTION			
IL-59 /US 12 (RAND RD) FROM STATE PARK RDEAST ST TO NORTH OF IL 120			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 R5	LAKE & MCHENRY	59	8
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

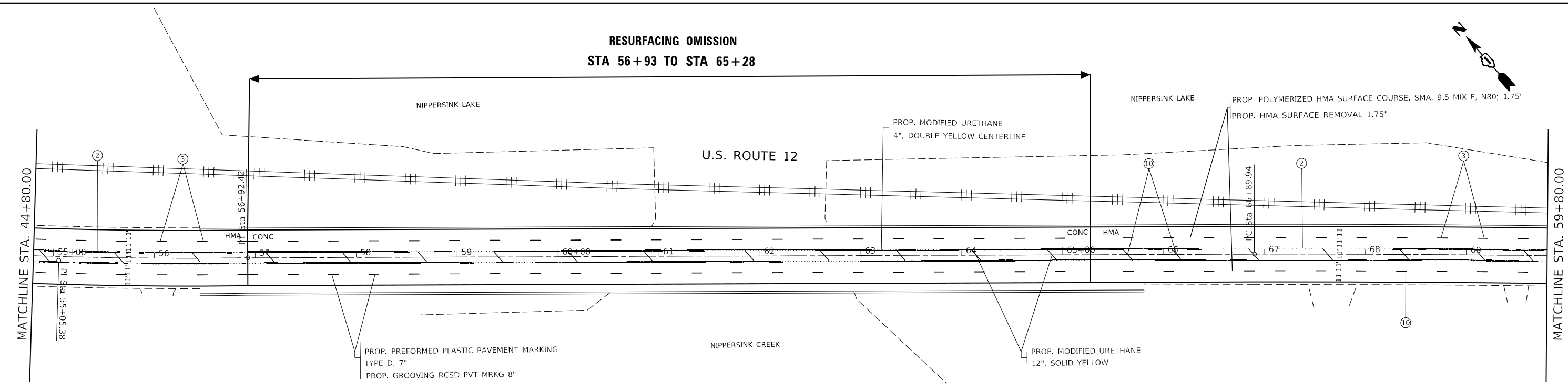
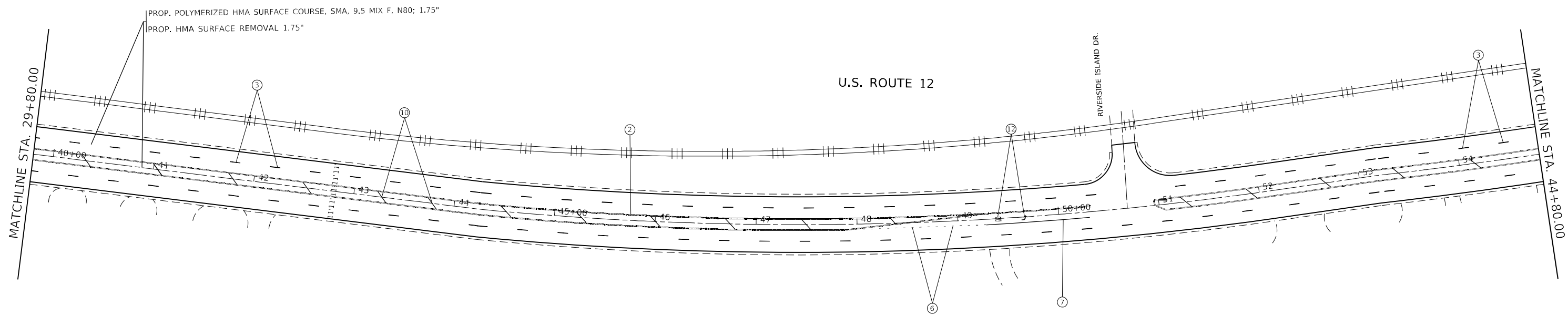
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PLOT DATE = 1/24/2024	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY PLAN</b>			
<b>IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	9
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

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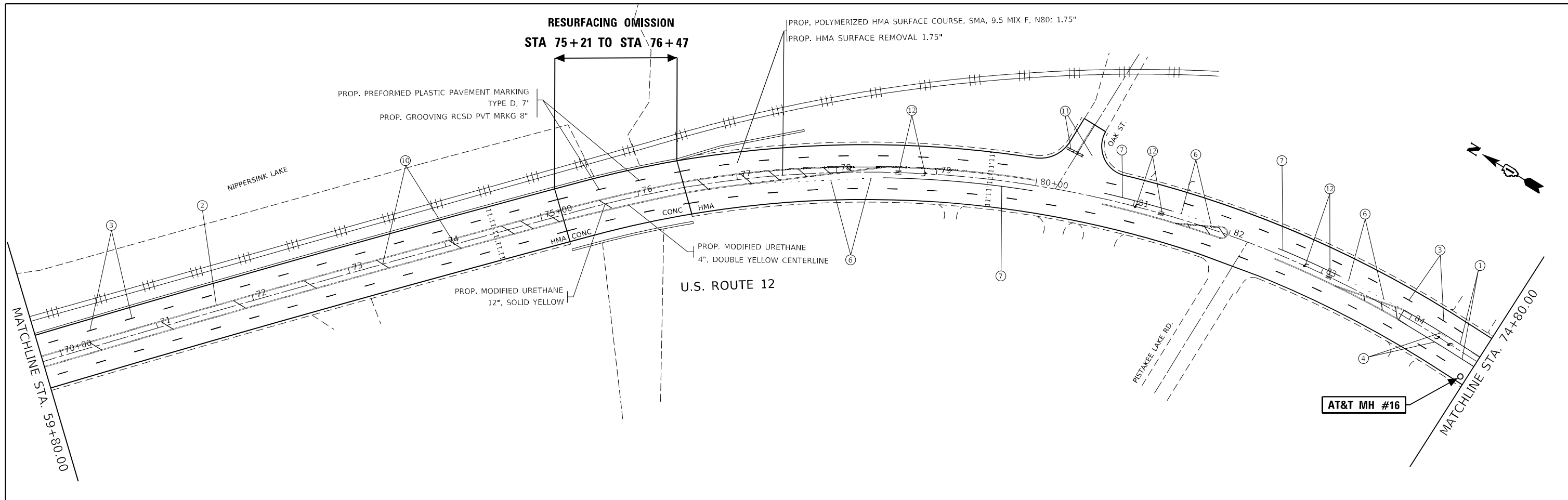
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PLOT DATE = 1/24/2024	CHECKED -	REVISOR -
	DATE -	REVISOR -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY PLAN</b>			
<b>IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

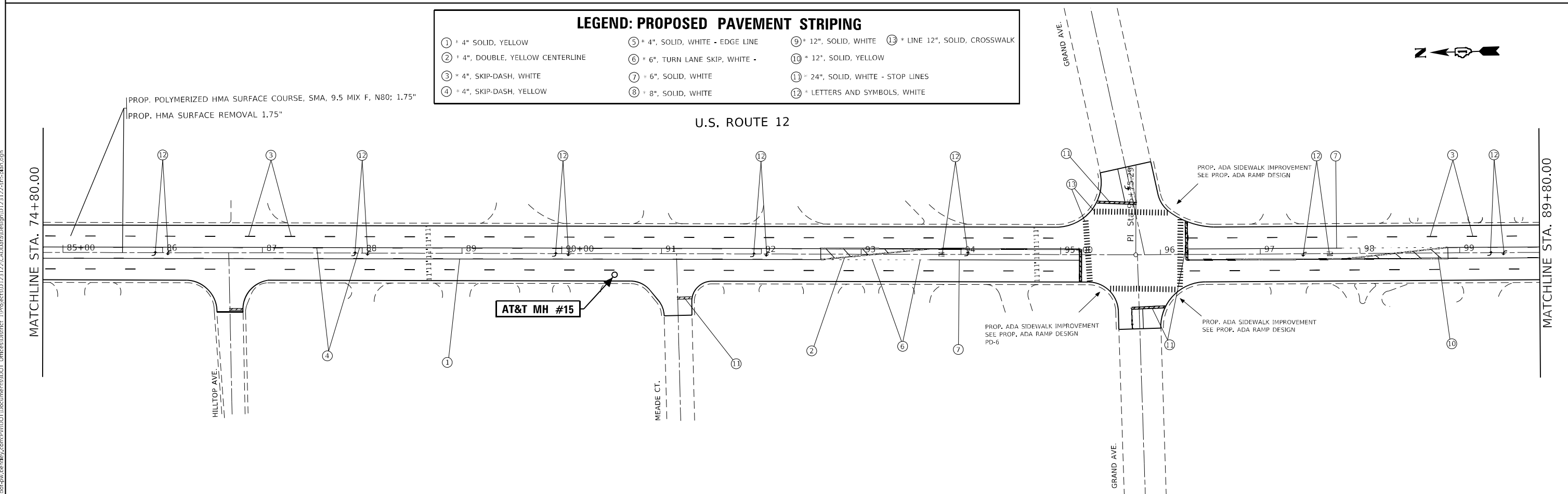
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CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				





**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4", SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	



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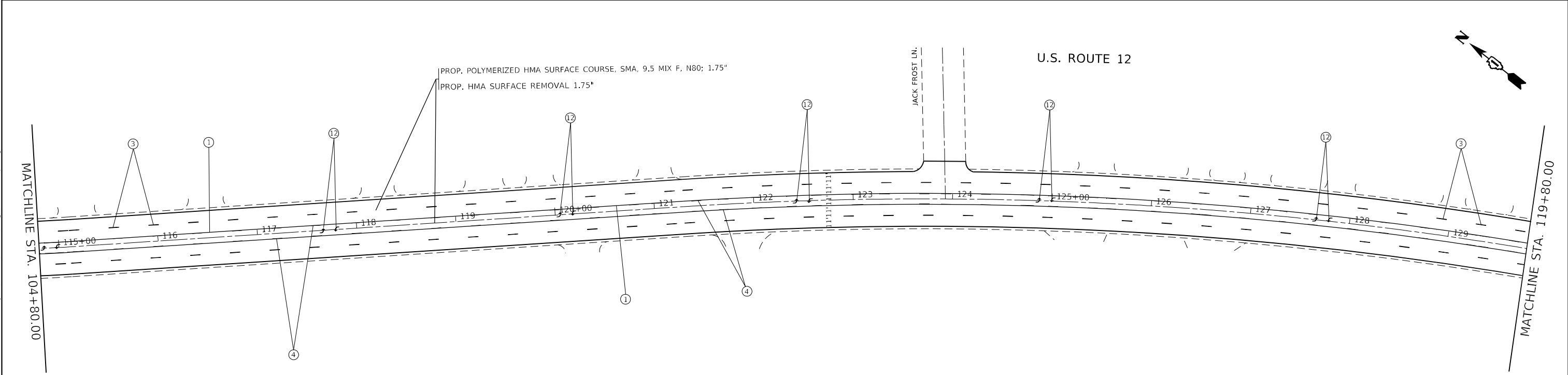
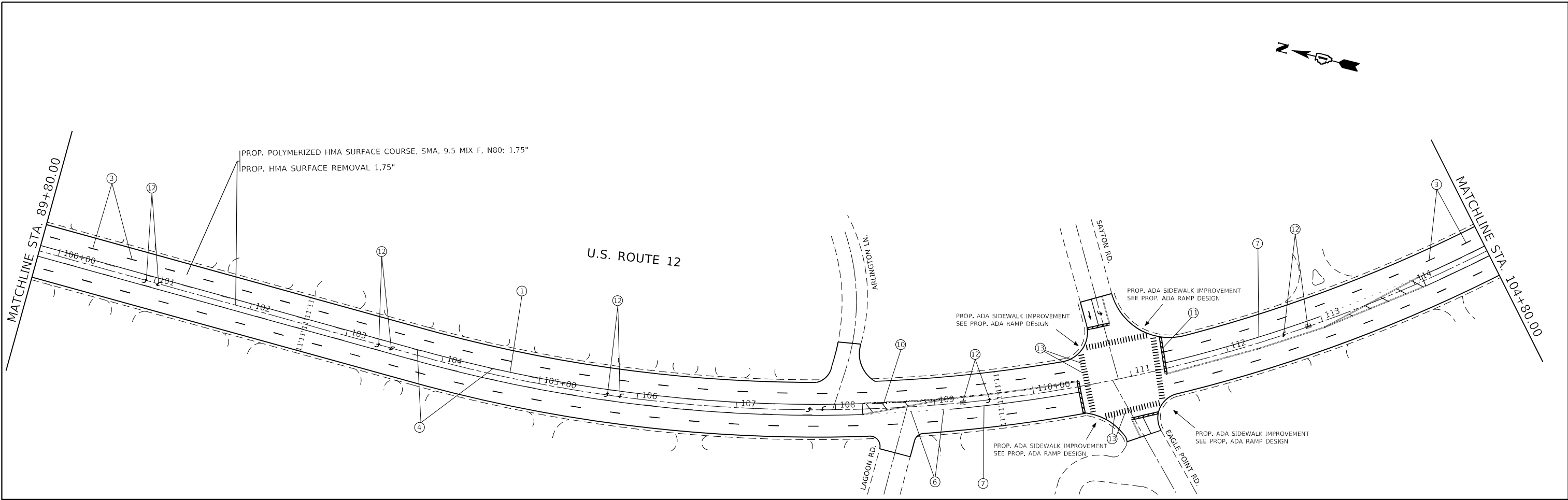
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**

**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	11
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

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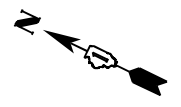
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**

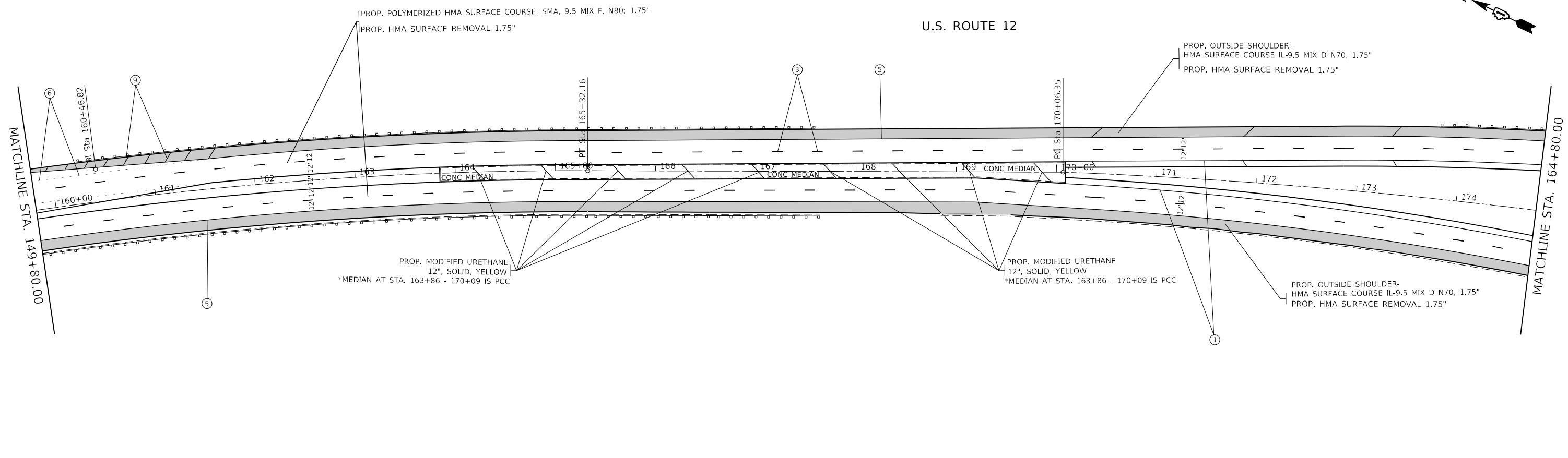
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	12
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				

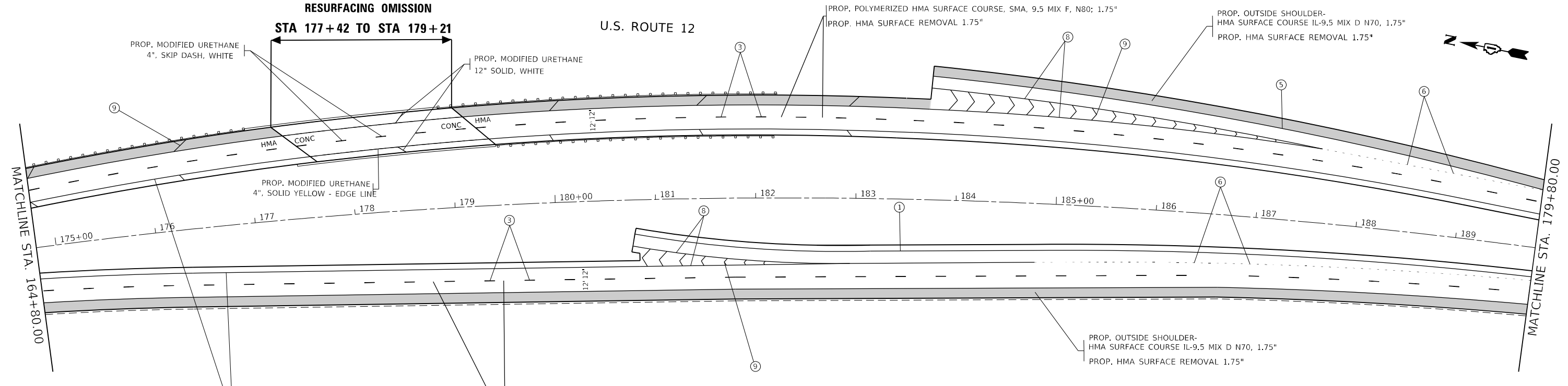


U.S. ROUTE 12



U.S. ROUTE 12

**RESURFACING OMISSION**  
STA 177+42 TO STA 179+21



**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

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PLOT DATE = 12/15/2023	DATE -	REVISED -

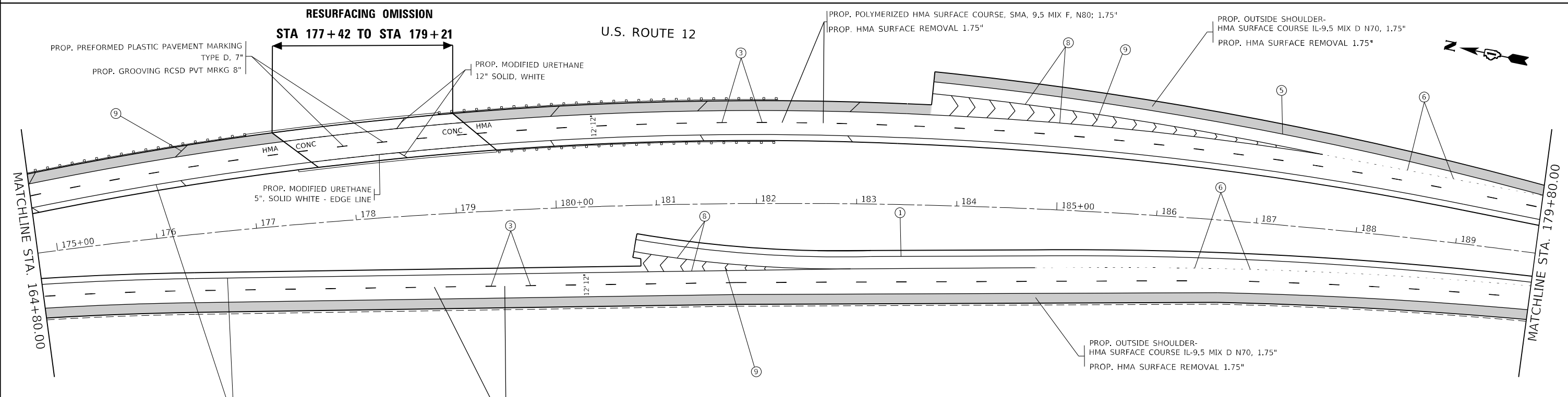
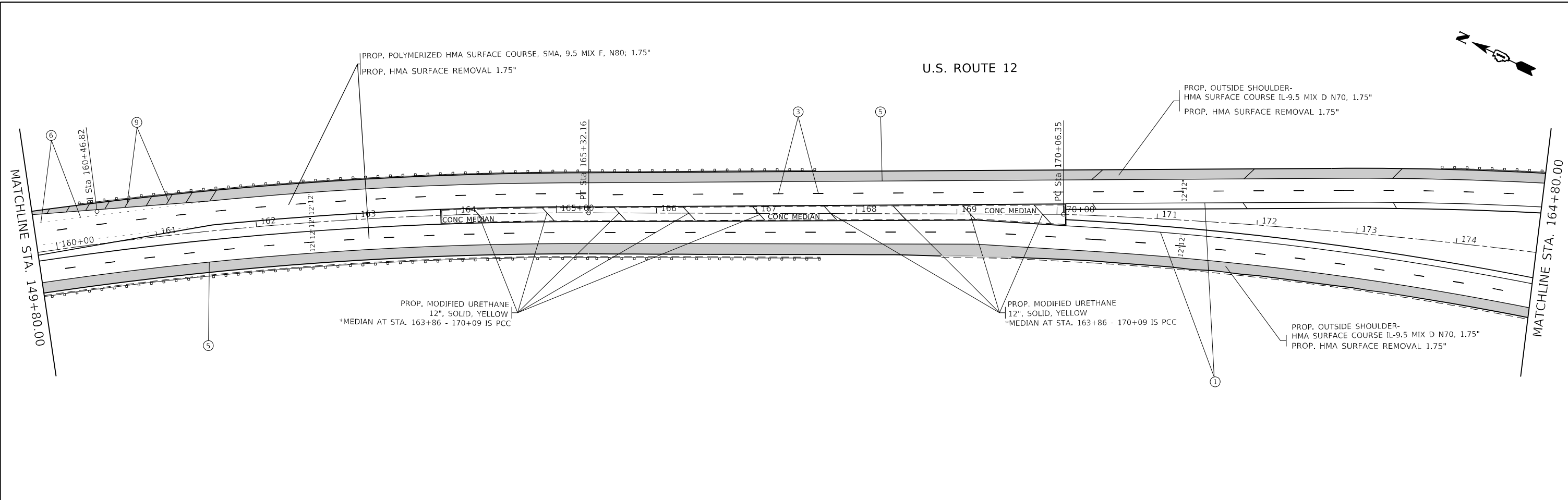
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 13
ILLINOIS FED. AID PROJECT				

CONTRACT NO. 62T59



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LEGEND: PROPOSED PAVEMENT STRIPING			
① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

USER NAME = Wendy.Andonayre	DESIGNED -	REVISED - 1/24/24 WA
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/24/2024	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

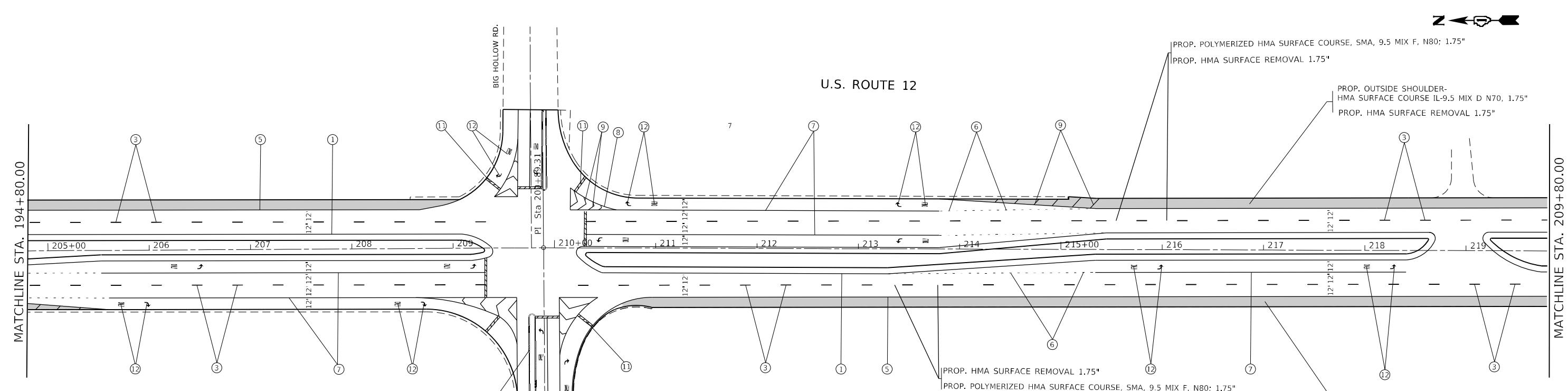
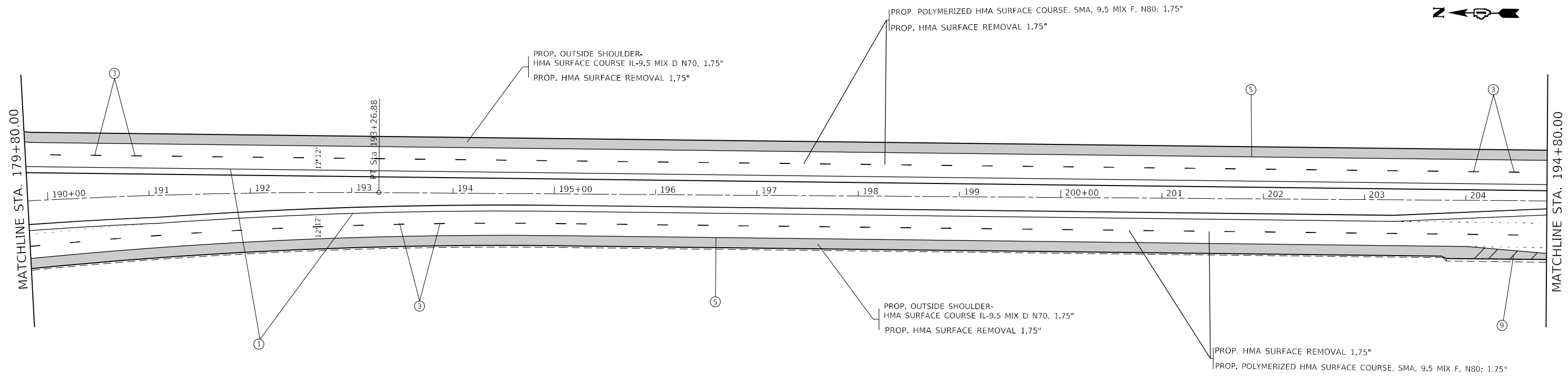
**ROADWAY PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 14
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



U.S. ROUTE 12



**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

MODEL: Default  
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 PLOT DATE: 12/15/2023

USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
DRAWN -	REVISOR -	REVISION -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/15/2023	DATE -	REVISION -

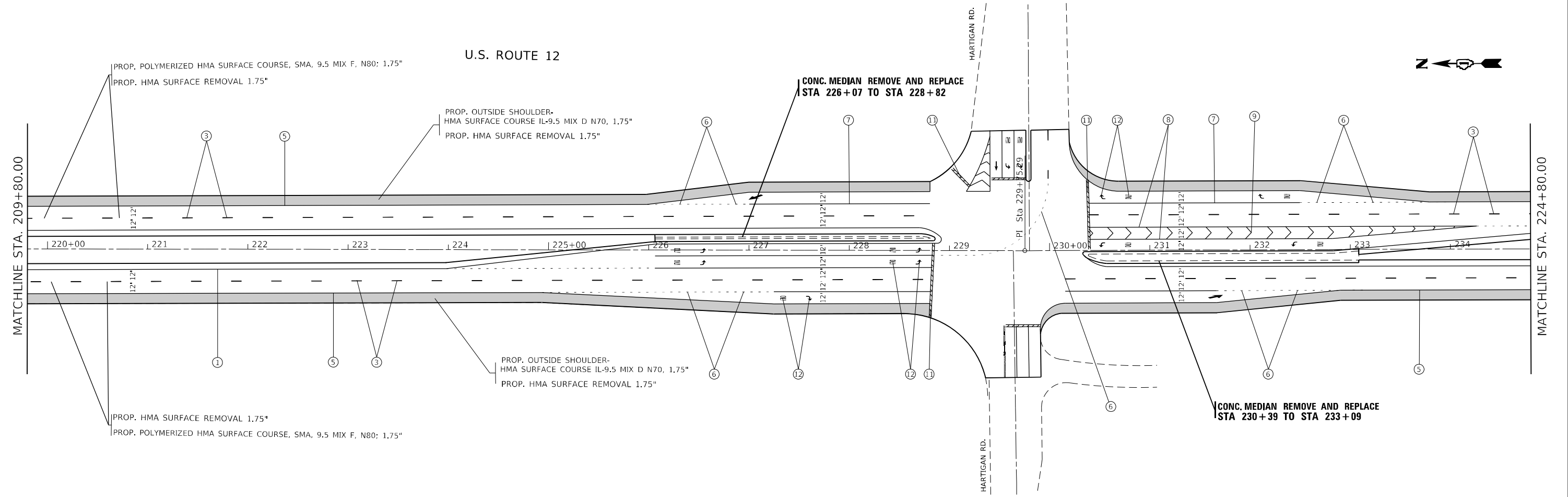
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

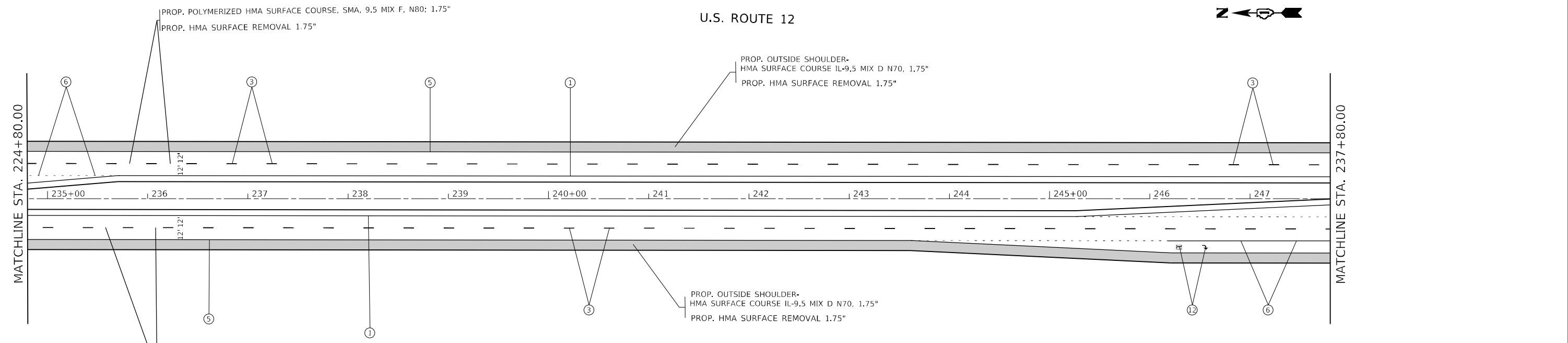
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	15
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				

U.S. ROUTE 12



U.S. ROUTE 12



LEGEND: PROPOSED PAVEMENT STRIPING			
1 * 4" SOLID, YELLOW	5 * 4", SOLID, WHITE - EDGE LINE	9 * 12", SOLID, WHITE	13 * LINE 12", SOLID, CROSSWALK
2 * 4", DOUBLE, YELLOW CENTERLINE	6 * 6", TURN LANE SKIP, WHITE -	10 * 12", SOLID, YELLOW	
3 * 4", SKIP-DASH, WHITE	7 * 6", SOLID, WHITE	11 * 24", SOLID, WHITE - STOP LINES	
4 * 4", SKIP-DASH, YELLOW	8 * 8", SOLID, WHITE	12 * LETTERS AND SYMBOLS, WHITE	

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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED - 1/24/24 WA
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PLOT DATE = 1/24/2024	CHECKED -	REVISED -
	DATE -	REVISED -

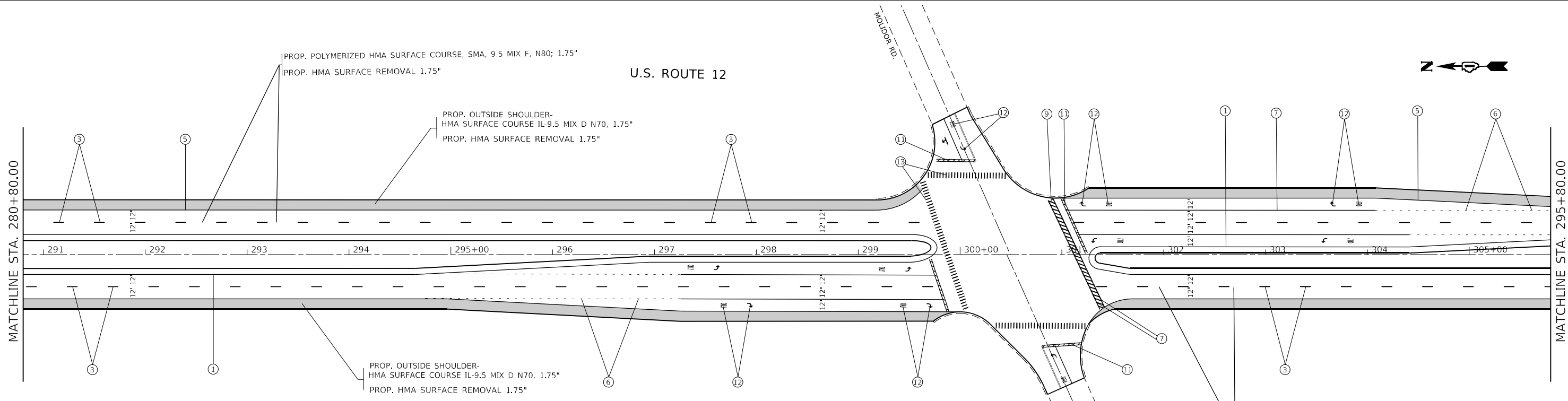
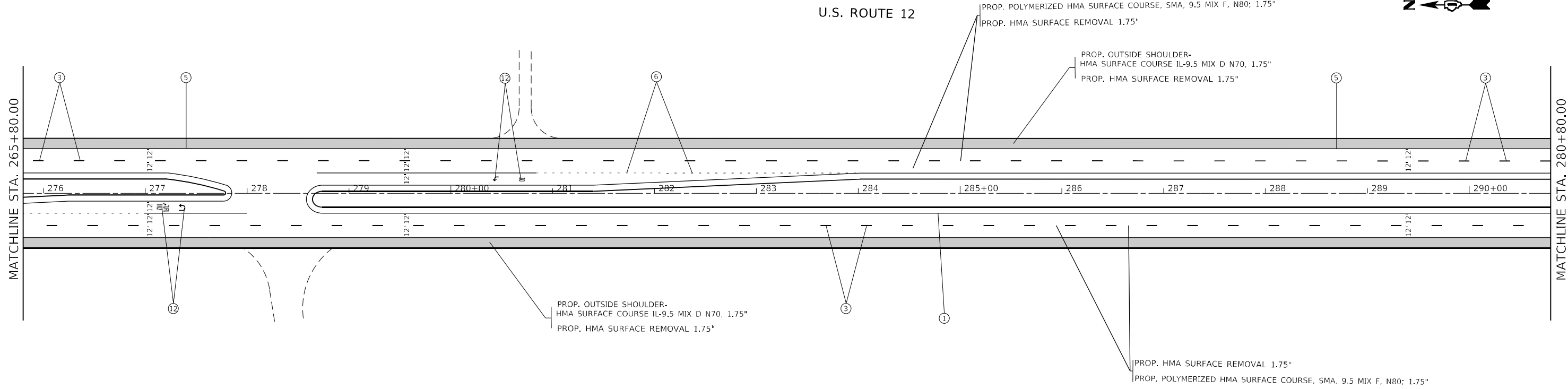
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	16
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



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LEGEND: PROPOSED PAVEMENT STRIPING			
① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/15/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

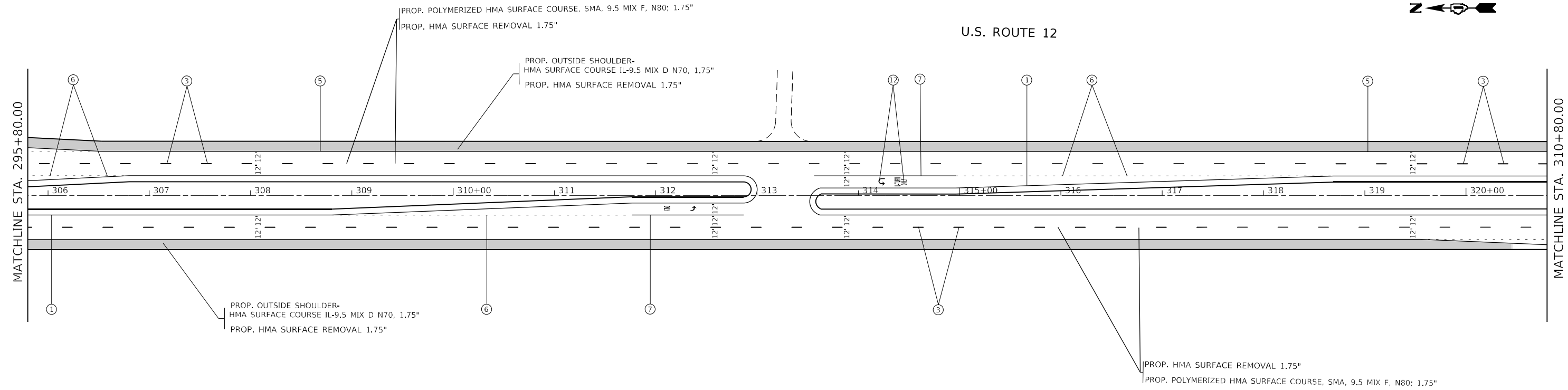
F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 18
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

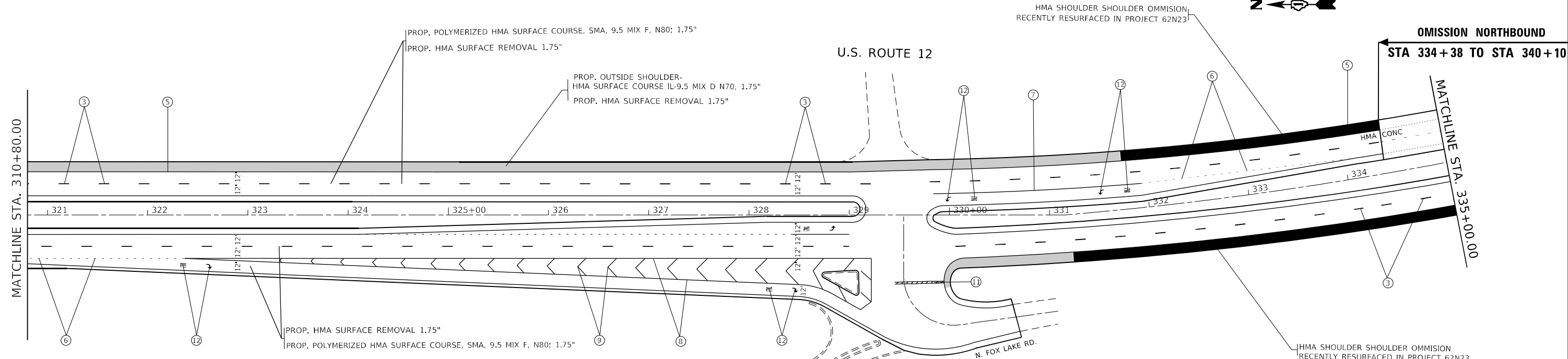




U.S. ROUTE 12



U.S. ROUTE 12



OMISSION NORTHBOUND  
STA 334+38 TO STA 340+10

N. FOX LAKE RD.

HMA SHOULDER SHOULDER OMISSION  
RECENTLY RESURFACED IN PROJECT 62N23

**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

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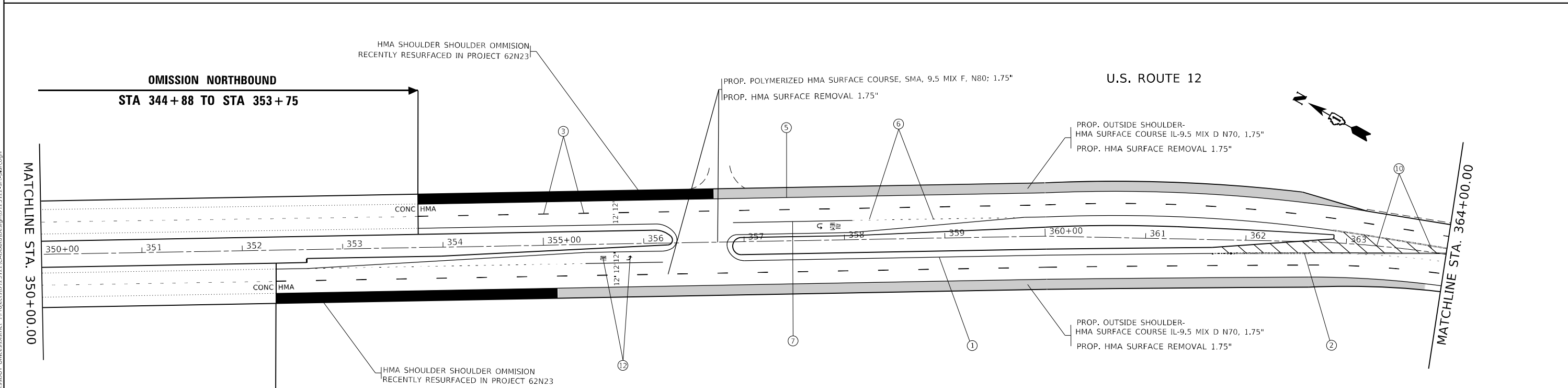
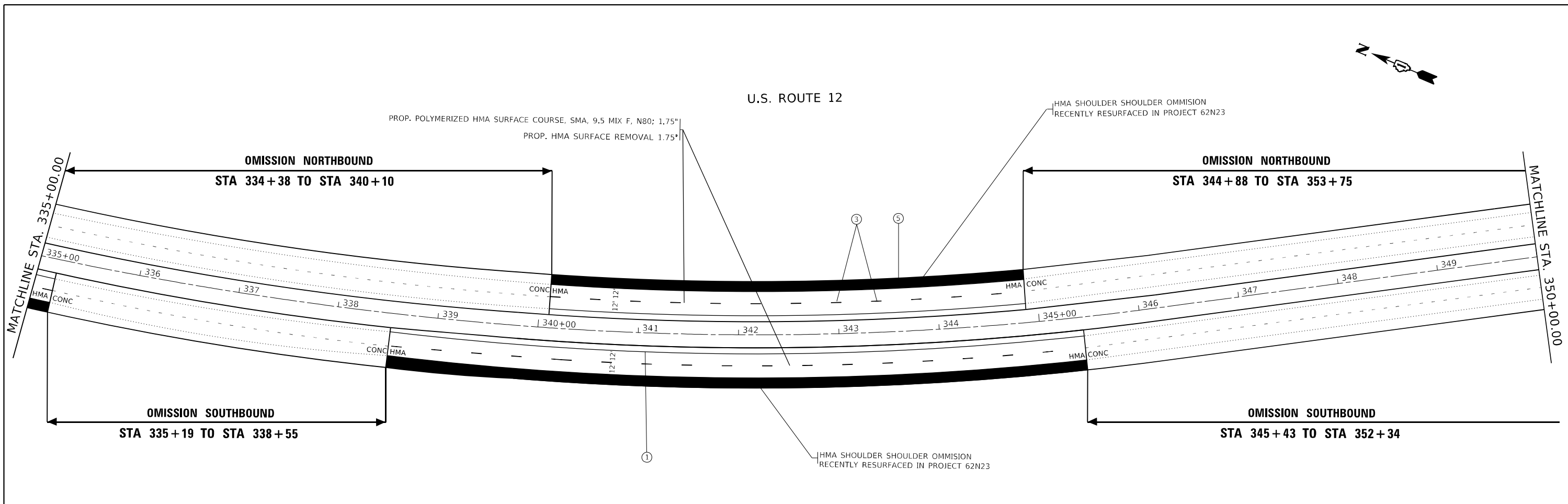
USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**ROADWAY PLAN**  
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	19
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

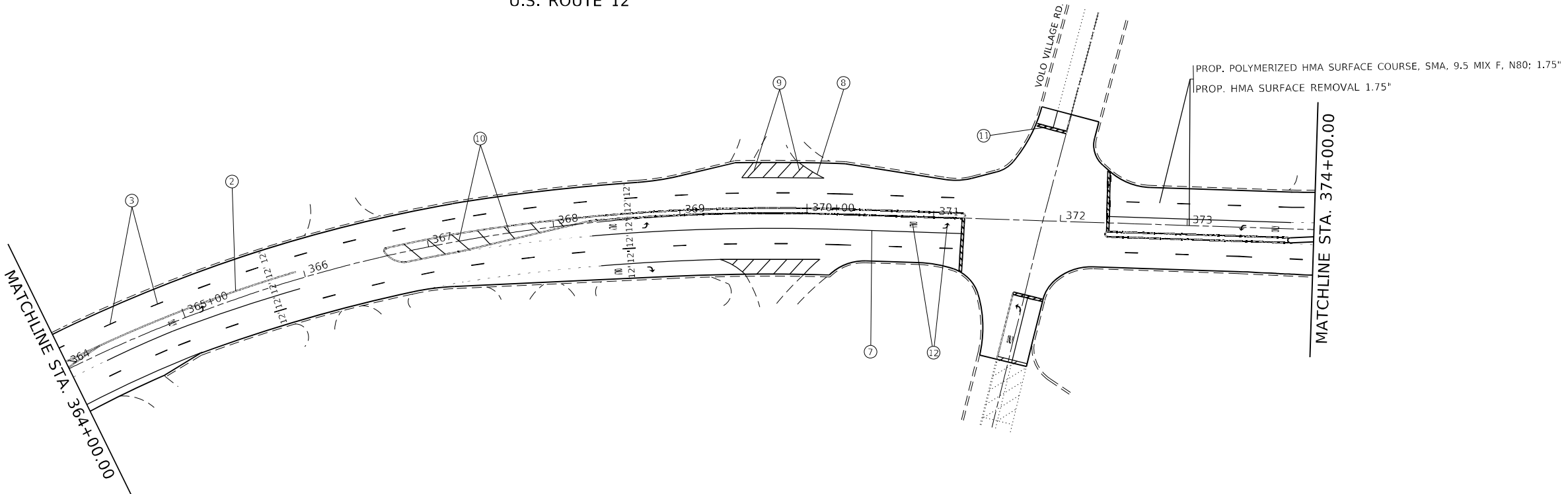
**ROADWAY PLAN**  
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: SHEET OF SHEETS STA. TO STA.

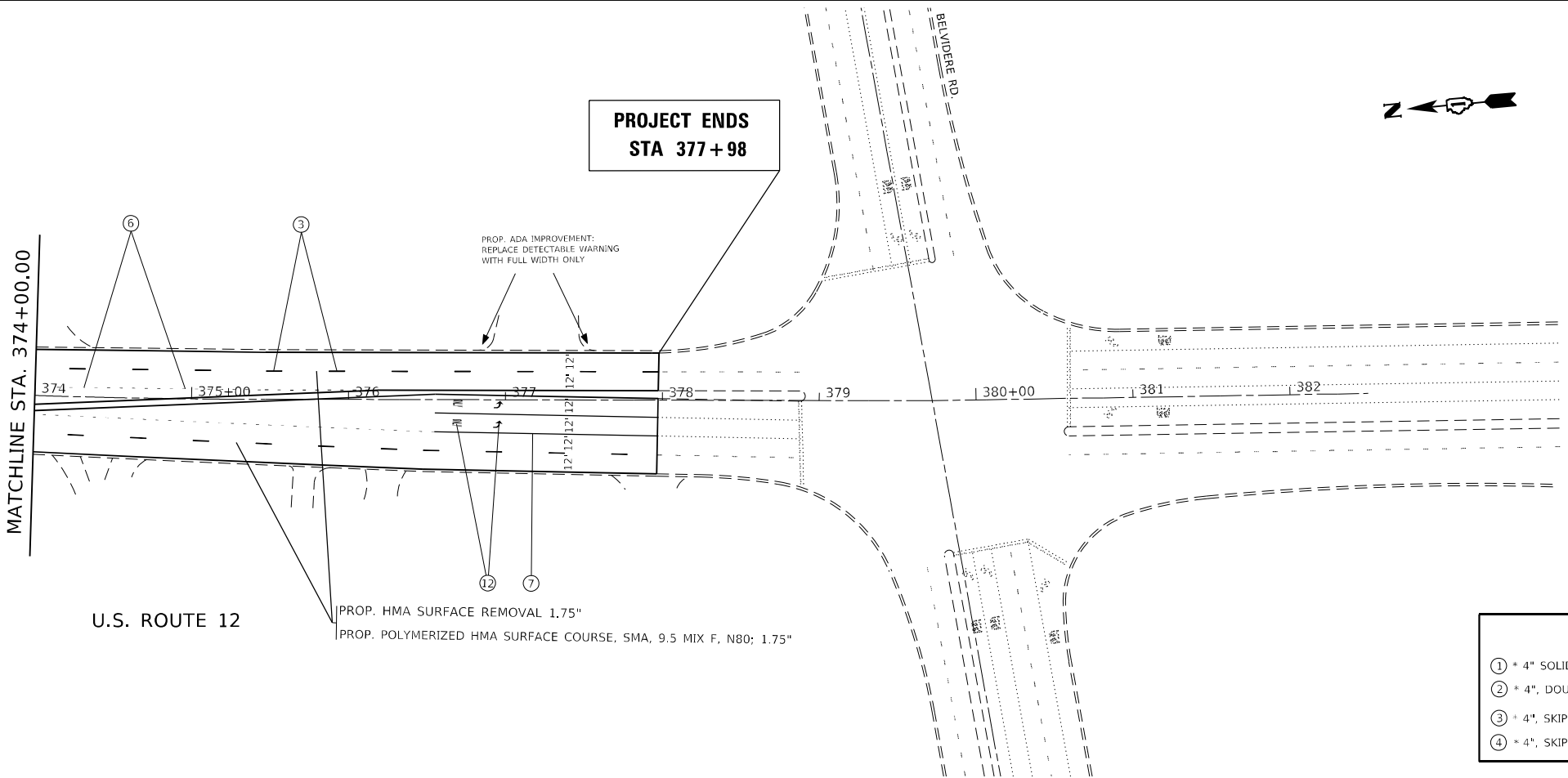
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	20
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



U.S. ROUTE 12



**PROJECT ENDS  
STA 377 + 98**



**LEGEND: PROPOSED PAVEMENT STRIPING**

① * 4" SOLID, YELLOW	⑤ * 4", SOLID, WHITE - EDGE LINE	⑨ * 12", SOLID, WHITE	⑬ * LINE 12", SOLID, CROSSWALK
② * 4", DOUBLE, YELLOW CENTERLINE	⑥ * 6", TURN LANE SKIP, WHITE -	⑩ * 12", SOLID, YELLOW	
③ * 4", SKIP-DASH, WHITE	⑦ * 6", SOLID, WHITE	⑪ * 24", SOLID, WHITE - STOP LINES	
④ * 4", SKIP-DASH, YELLOW	⑧ * 8", SOLID, WHITE	⑫ * LETTERS AND SYMBOLS, WHITE	

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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: SHEET OF SHEETS STA. TO STA.

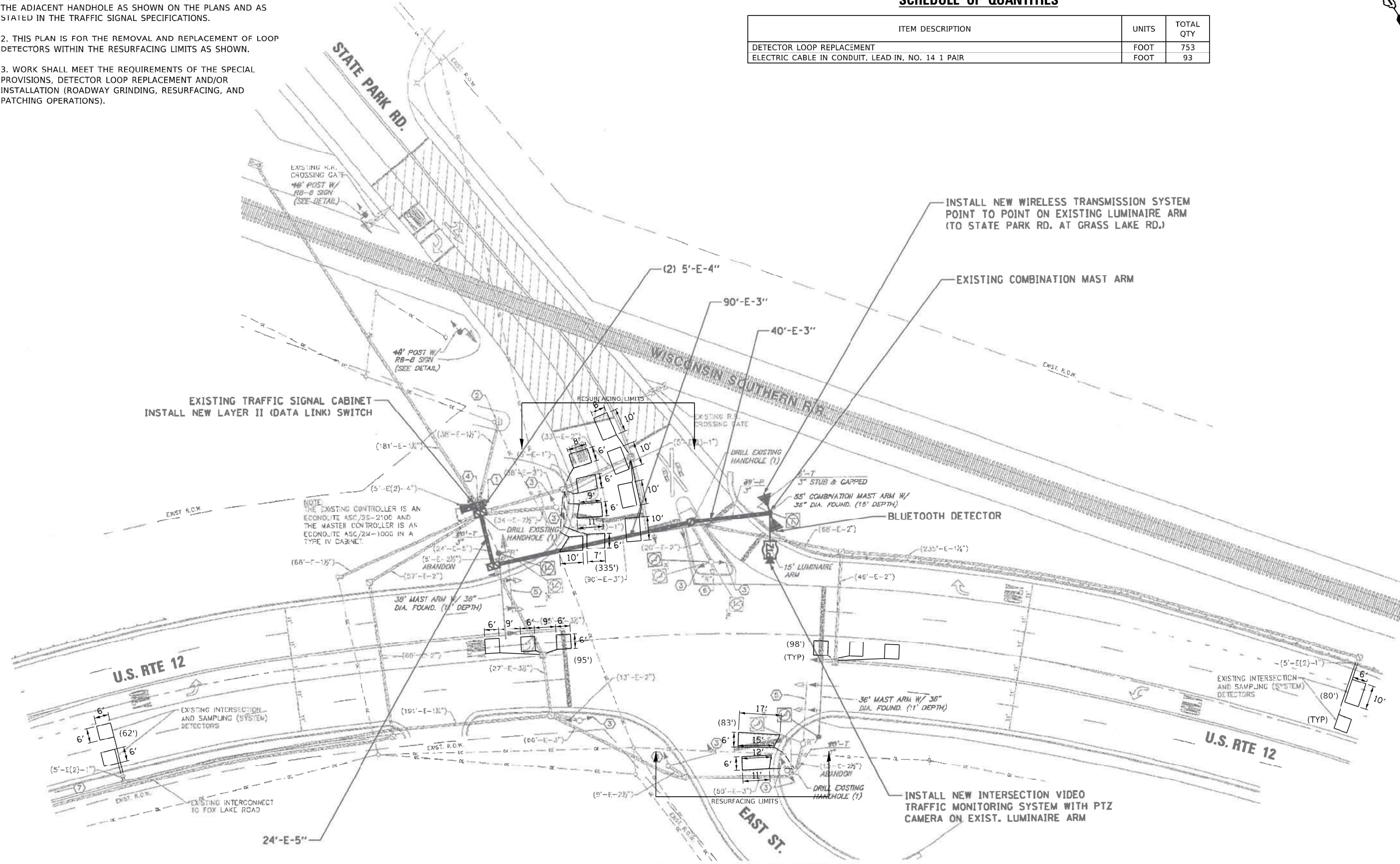
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	21
				CONTRACT NO. 62T59
ILLINOIS FED. AID PROJECT				

**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	753
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	93



MODEL: Default  
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 USER: mohammad.hamwi  
 DATE: 10/19/2023

**TS 11945**

USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2023	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

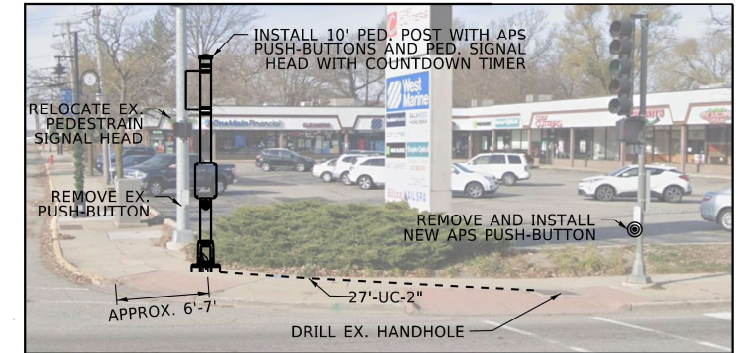
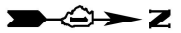
**APS AND DETECTOR LOOP INSTALLATION PLAN**  
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120)**  
 SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	22
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
4. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
5. APS SHALL BE PLACE PARALLEL TO THE CORRESPONDING CROSSWALK.
6. PEDESTRIAN POSTS SHALL HAVE A MINIMUM OF 4' SIDEWALK CLEARANCE EITHER IN FRONT OR BEHIND OF THE FOUNDATION.



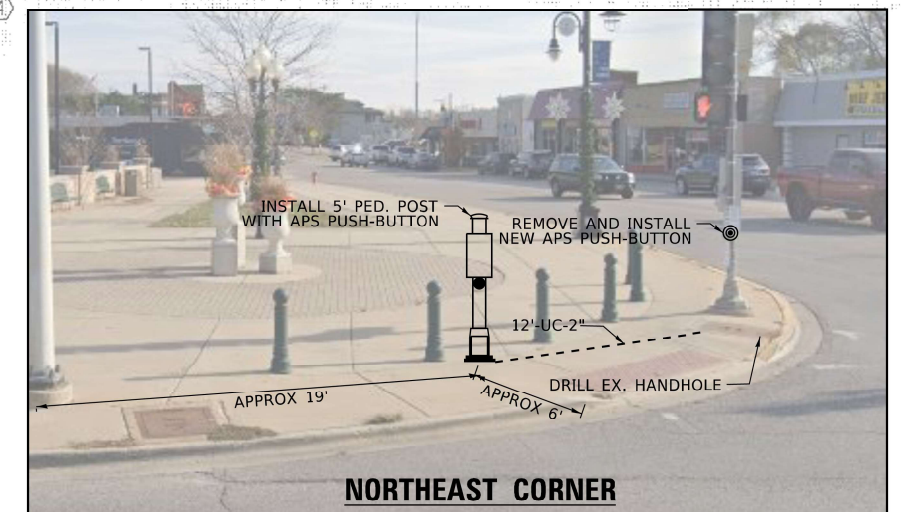
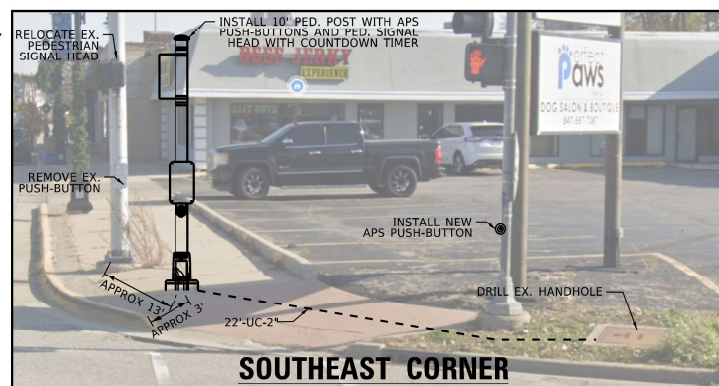
**REMOVAL AND RELOCATION NOTES:**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

5 EACH PEDESTRIAN PUSH-BUTTON

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR, SAFELY STORED AND RELOCATED TO THE PROPOSED POST AND TRAFFIC SIGNAL CONTROLLER:

2 EACH PEDESTRIAN SIGNAL HEAD



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120

TS 945

USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2023	DATE -	REVISED -

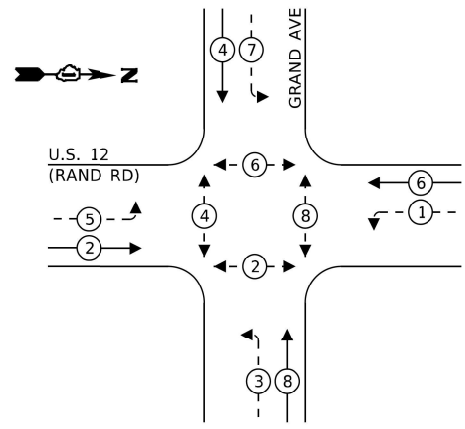
SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 23
ILLINOIS				FED. AID PROJECT

CONTRACT NO. 62T59

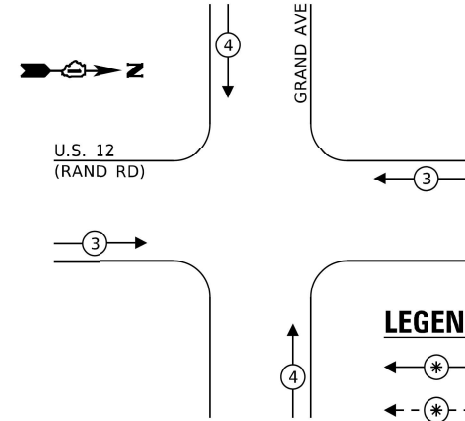


### EXISTING CONTROLLER SEQUENCE



EX. INTERCONNECT TO SAYTON RD  
EX. TRACER CABLE

### EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



#### LEGEND:

- ← (M) → PROTECTED PHASE
- ← (M) - PROTECTED/PERMITTED PHASE
- ← (M) > PEDESTRIAN PHASE

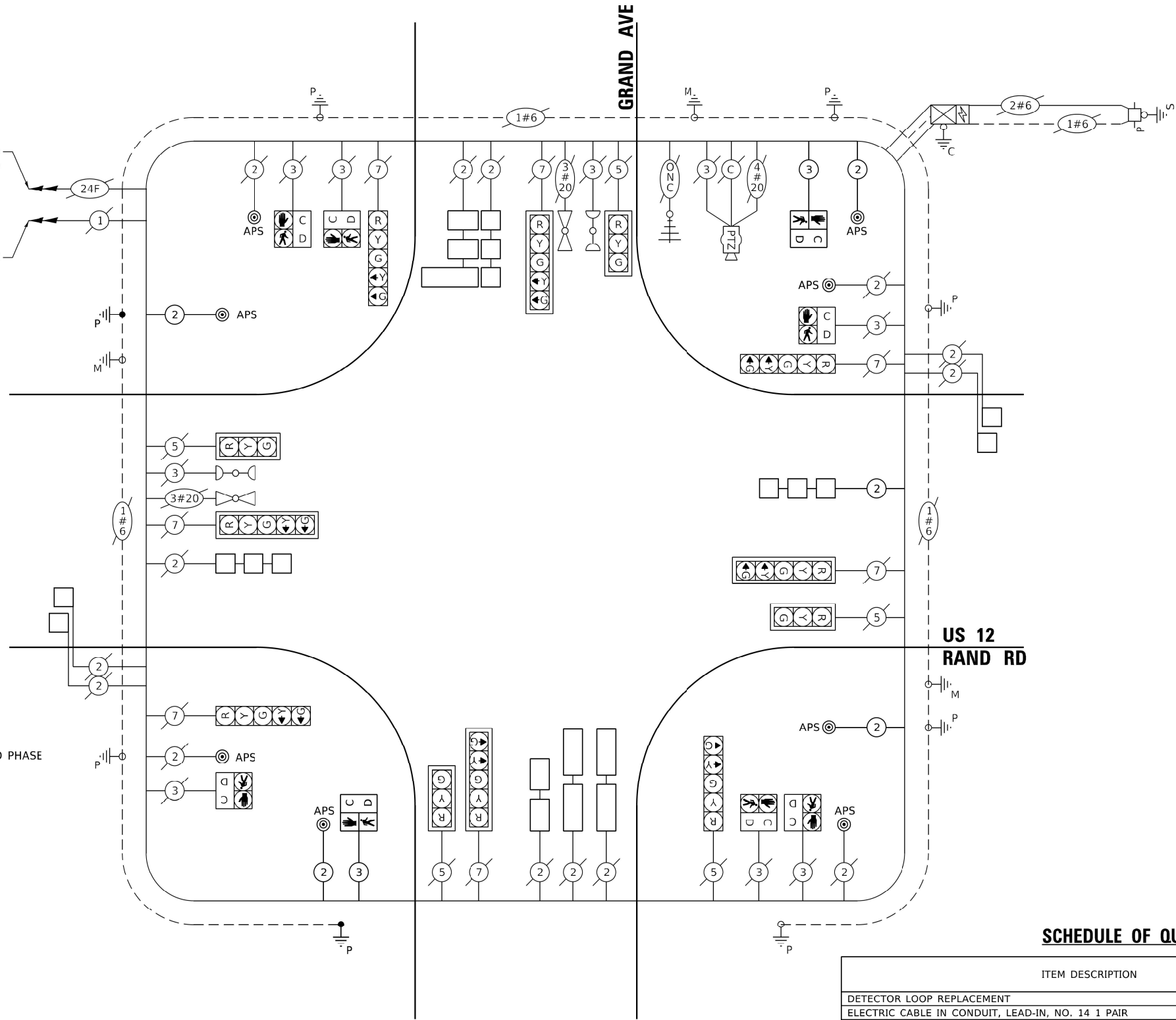
### TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66.0
(YELLOW)	12	20	5	12.0
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	16	10	10	16.0
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>443.8</b>

ENERGY COSTS TO:

**ILLINOIS DEPARTMENT OF TRANSPORTATION**  
201 W CENTER CT  
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: TERRI BLECK  
PHONE: (847) 816-5239  
COMPANY: COMMONWEALTH EDISON  
ACCOUNT NUMBER: ---



### CABLE PLAN

### SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	830
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	216
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	61
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	2
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	543.5
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	304
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	24
DRILL EXISTING HANDHOLE	EACH	3
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	5
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**APS AND DETECTOR LOOP INSTALLATION PLAN**  
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: 1"=50'

SHEET OF SHEETS STA. TO STA.

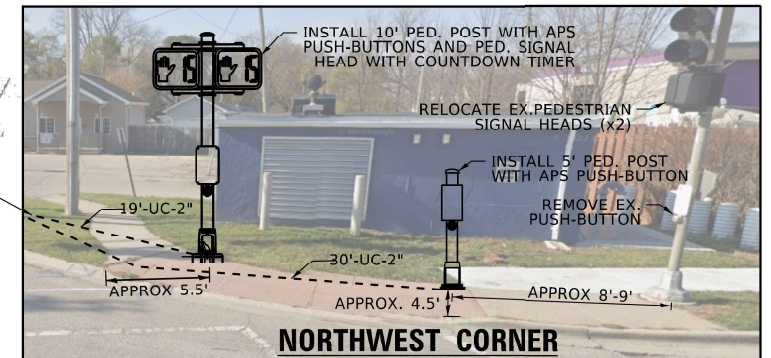
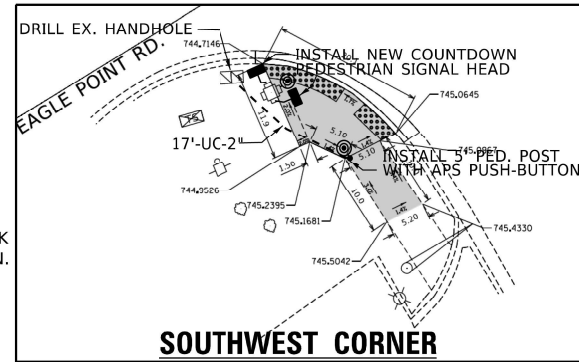
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	24
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				

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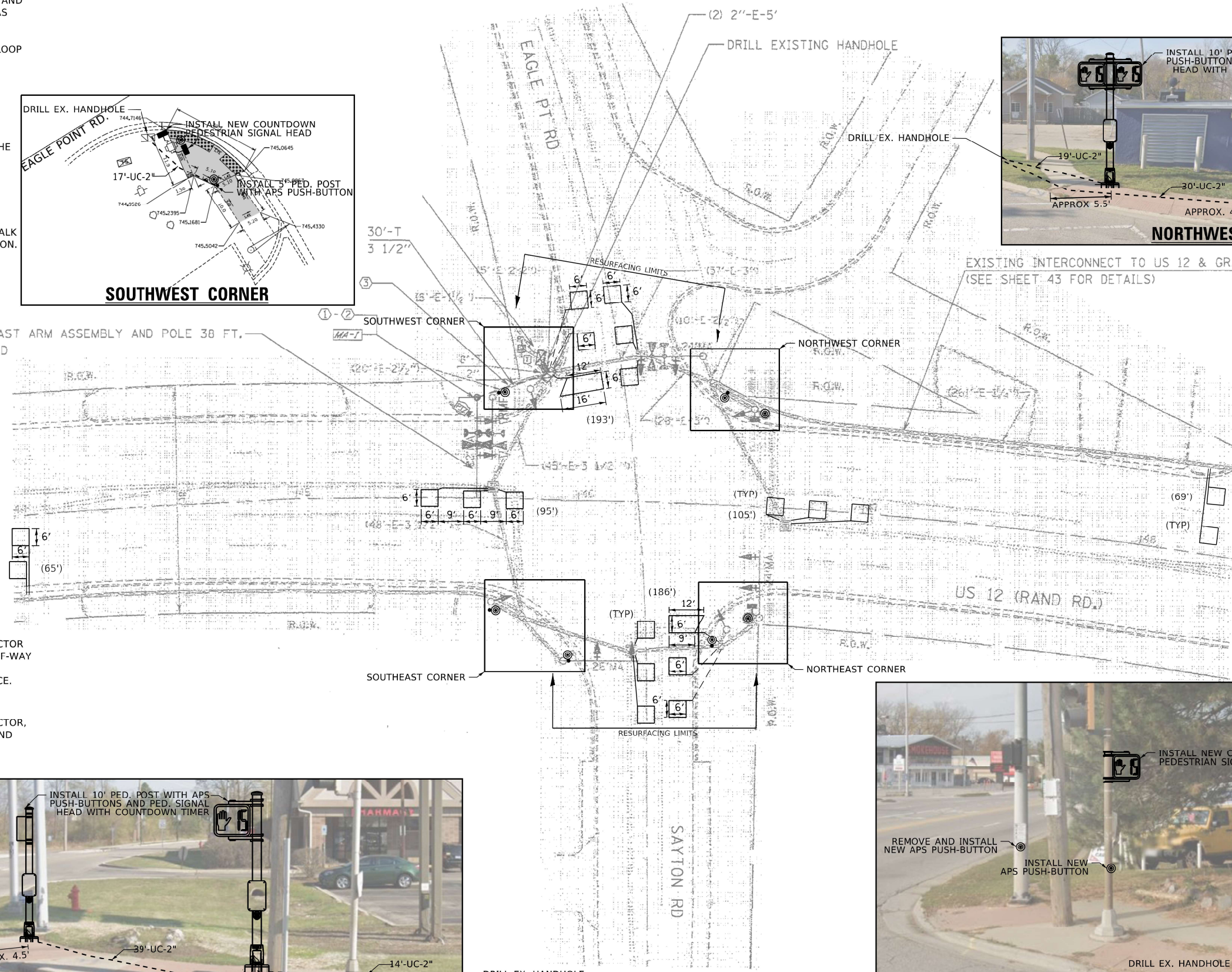


**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
4. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
5. APS SHALL BE PLACE PARALLEL TO THE CORRESPONDING CROSSWALK.
6. PEDESTRIAN POSTS SHALL HAVE A MINIMUM OF 4' SIDEWALK CLEARANCE EITHER IN FRONT OR BEHIND OF THE FOUNDATION.



STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT.  
NO LUMINAIRE REQUIRED



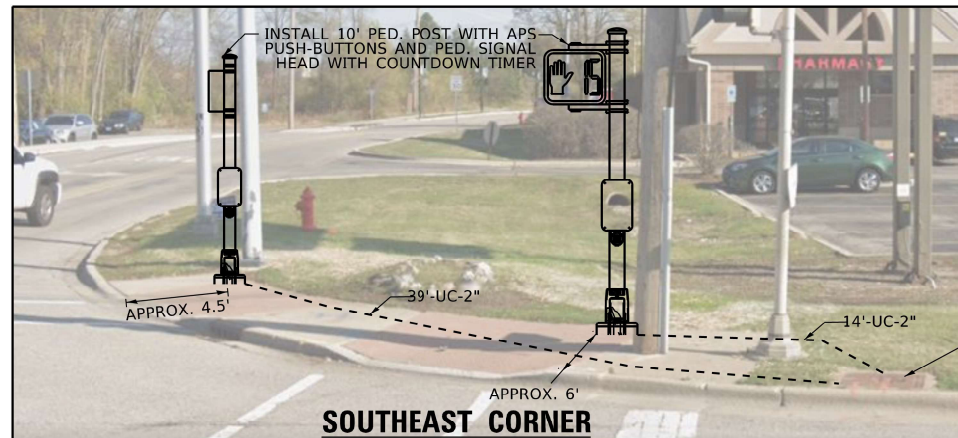
**REMOVAL AND RELOCATION NOTES:**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH PEDESTRIAN PUSH-BUTTON

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR, SAFELY STORED AND RELOCATED TO THE PROPOSED POST AND TRAFFIC SIGNAL CONTROLLER:

- 2 EACH PEDESTRIAN SIGNAL HEAD



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120

TS 12290

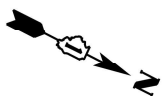
USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2023	DATE -	REVISED -

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

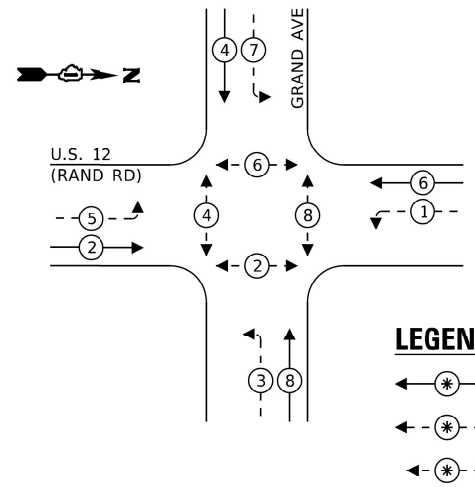
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				CONTRACT NO. 62T59
ILLINOIS FED. AID PROJECT				

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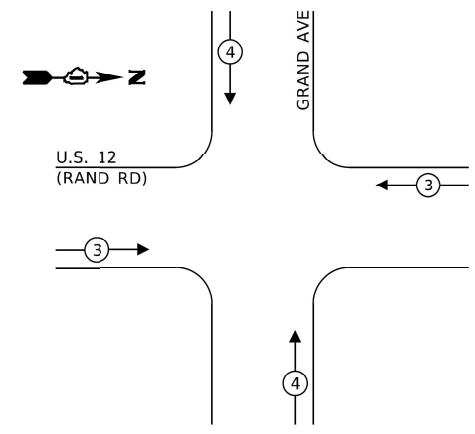


**EXISTING CONTROLLER SEQUENCE**



- LEGEND:**
- ⊛ PROTECTED PHASE
  - ⊛- - PROTECTED/PERMITTED PHASE
  - ⊛↔ PEDESTRIAN PHASE

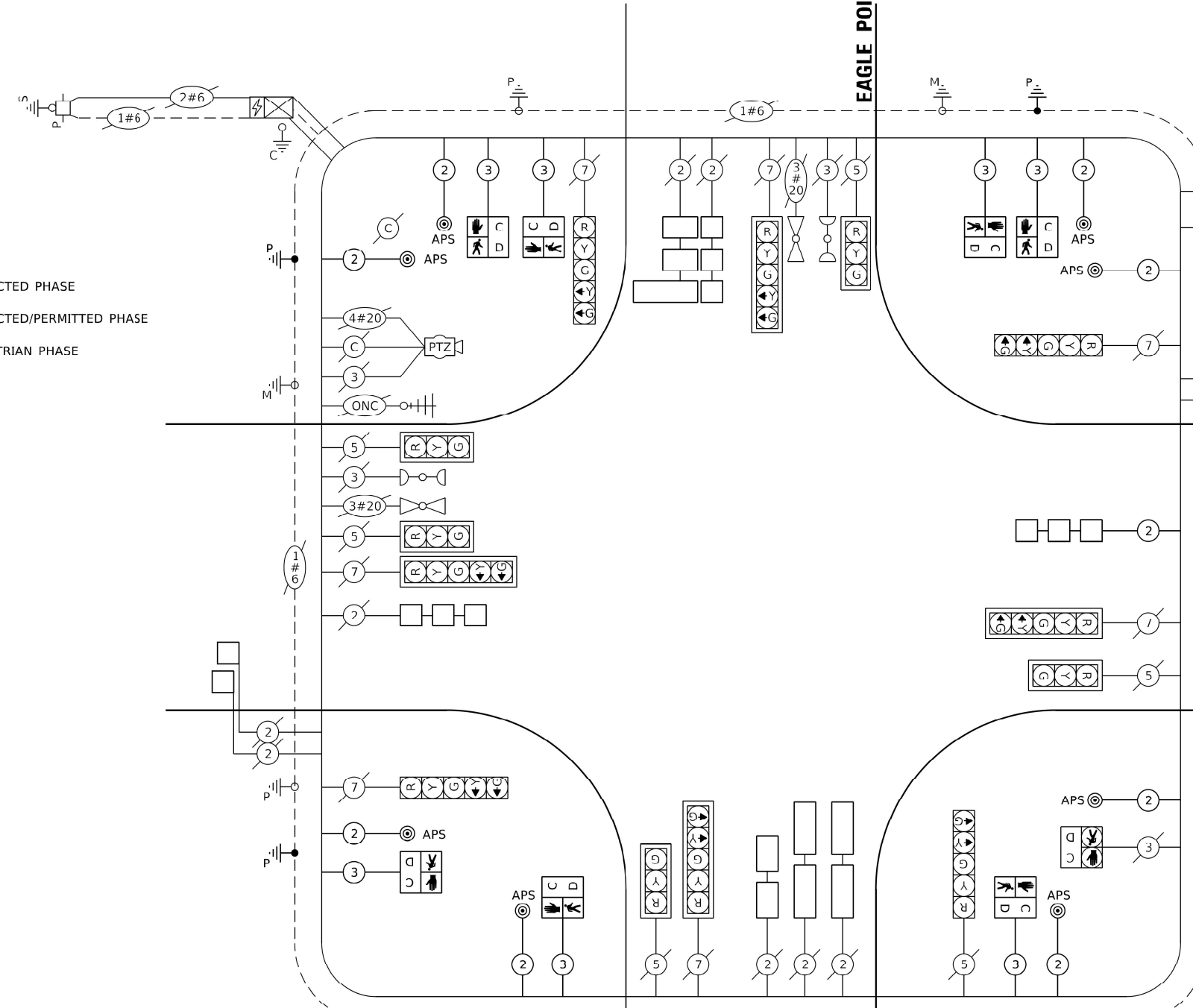
**EXISTING EMERGENCY VEHICLE  
PREEMPTION SEQUENCE**



**TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	13	11	50	71.5
(YELLOW)	13	20	5	13.0
(GREEN)	13	12	45	70.2
PERMISSIVE ARROW	16	10	10	16.0
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>455.7</b>

ENERGY COSTS TO:  
**ILLINOIS DEPARTMENT OF TRANSPORTATION**  
 201 W CENTER CT  
 SCHAUMBURG, IL 60196  
 ENERGY SUPPLY: CONTACT: TERRI BLECK  
 PHONE: (847) 816-5239  
 COMPANY: COMMONWEALTH EDISON  
 ACCOUNT NUMBER: ---



**CABLE PLAN**

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	713
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	92
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	119
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	2
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	3
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	5
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	863.5
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	804
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	20
DRILL EXISTING HANDHOLE	EACH	5
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2

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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

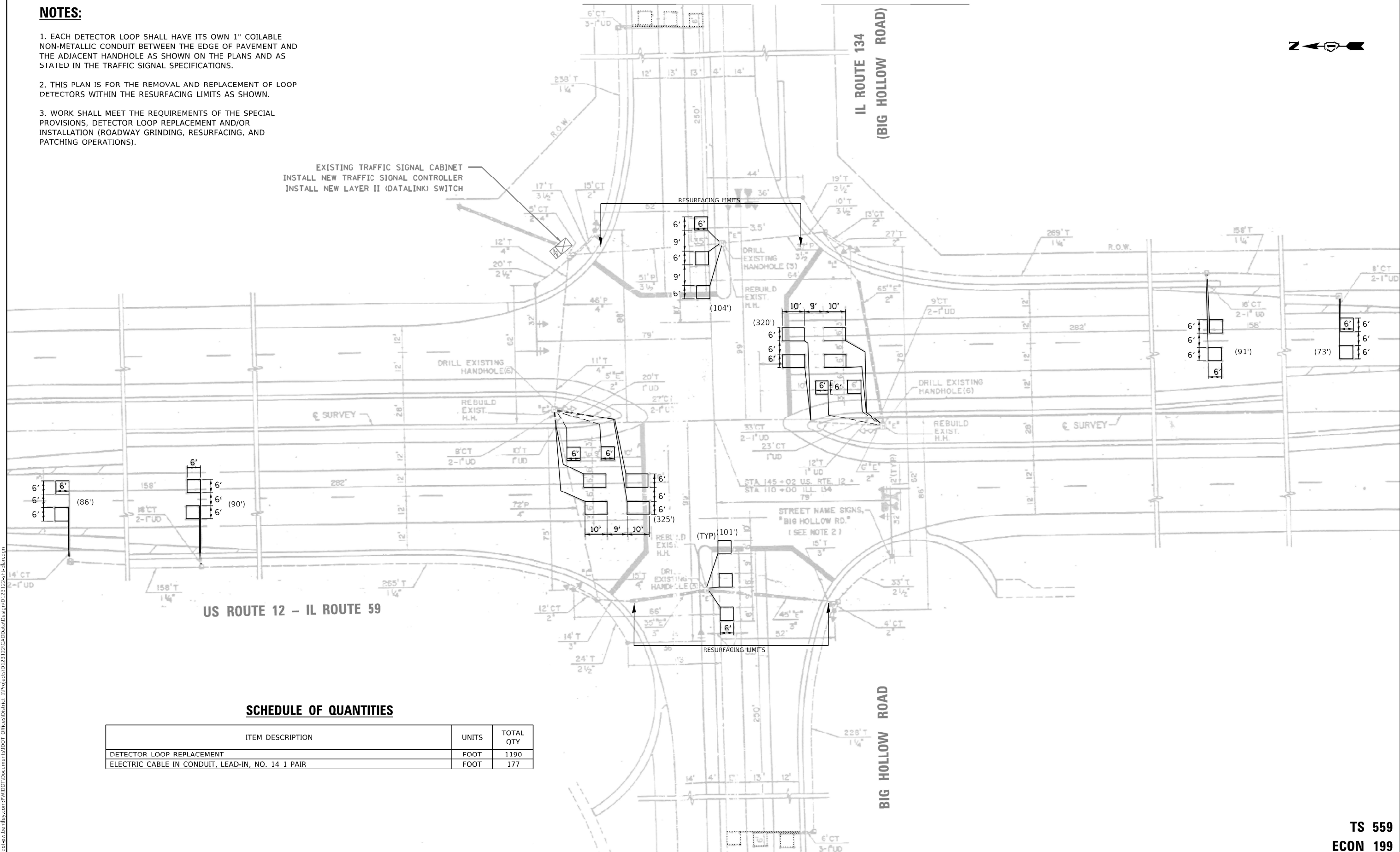
**APS AND DETECTOR LOOP INSTALLATION PLAN**  
**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**  
 SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 26
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T59	



**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).



**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	1190
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	177

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120

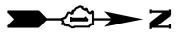
TS 559  
ECON 199

USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
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	DATE -	REVISED -

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

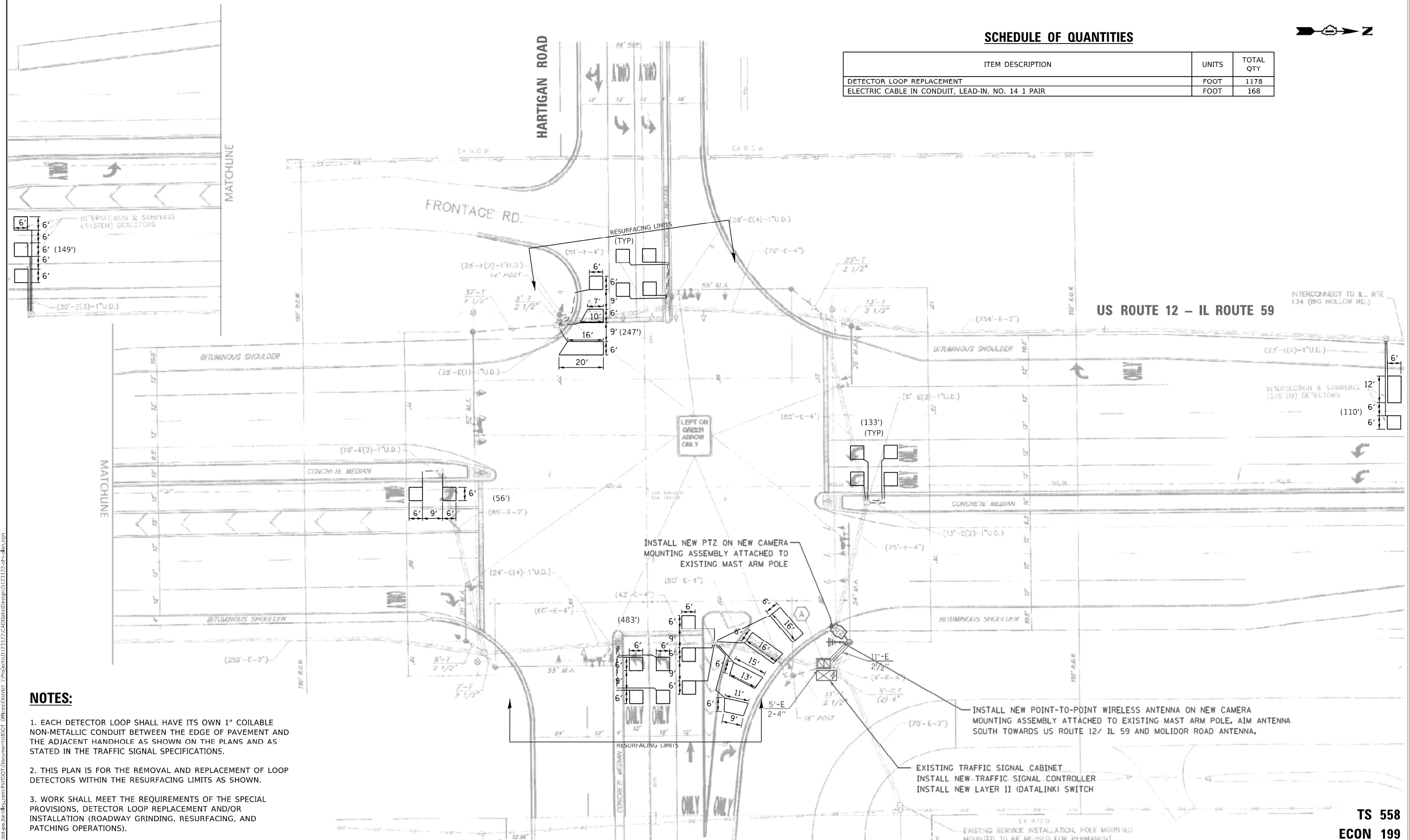
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	27
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				

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**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	1178
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	168



**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

INSTALL NEW POINT-TO-POINT WIRELESS ANTENNA ON NEW CAMERA MOUNTING ASSEMBLY ATTACHED TO EXISTING MAST ARM POLE. AIM ANTENNA SOUTH TOWARDS US ROUTE 12/ IL 59 AND MOLITOR ROAD ANTENNA.

EXISTING TRAFFIC SIGNAL CABINET  
INSTALL NEW TRAFFIC SIGNAL CONTROLLER  
INSTALL NEW LAYER II (DATALINK) SWITCH

**TS 558  
ECON 199**

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PLOT DATE = 10/19/2023	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

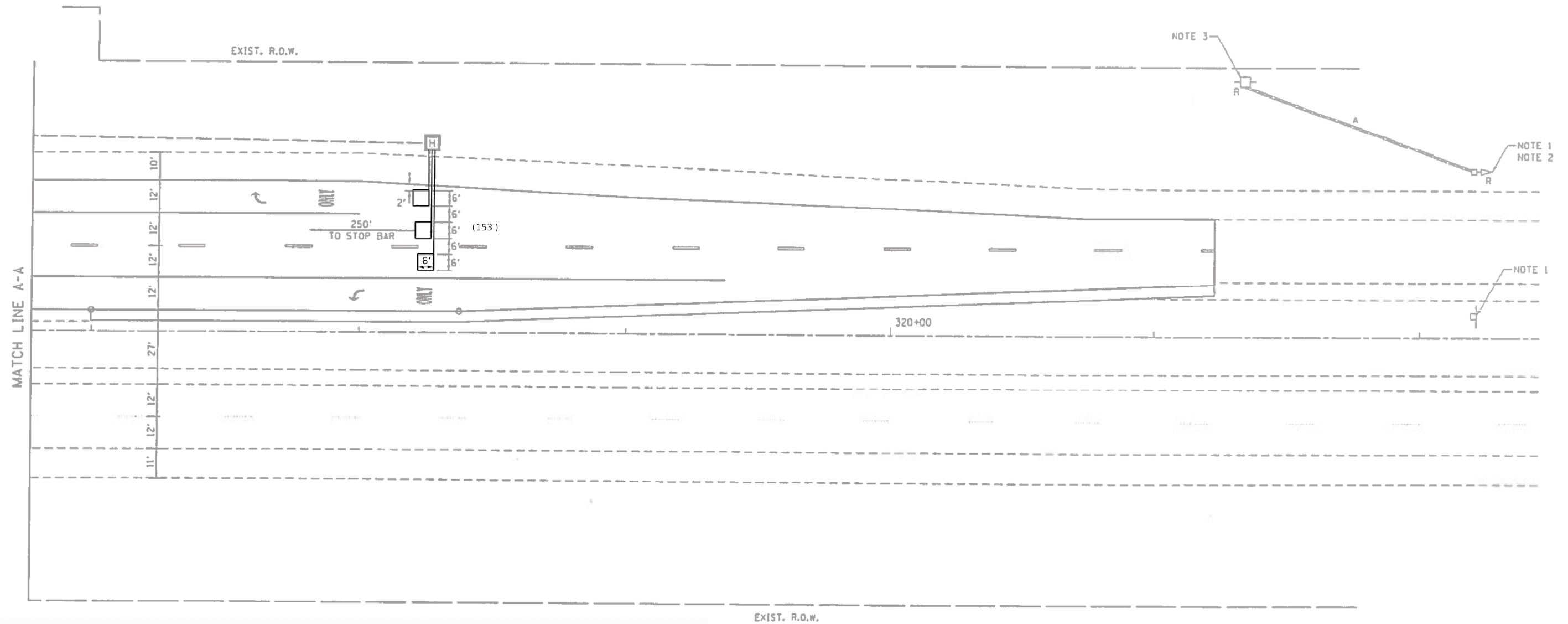
F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 28
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				





**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).



**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	153
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	10

**TS 6512**

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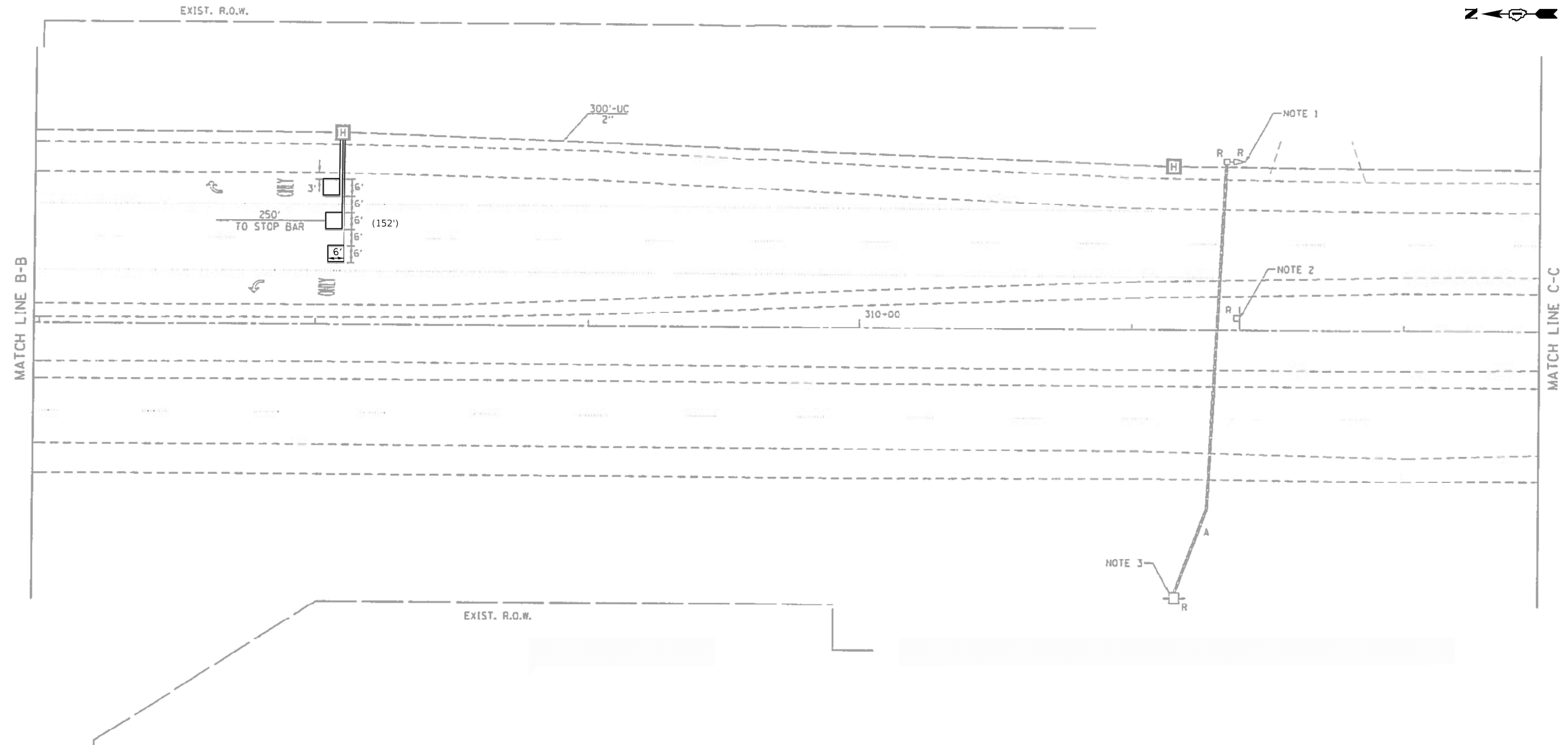
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PLOT DATE = 10/19/2023	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 30
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T59	



**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	152
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	10

**TS 6512**

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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/19/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

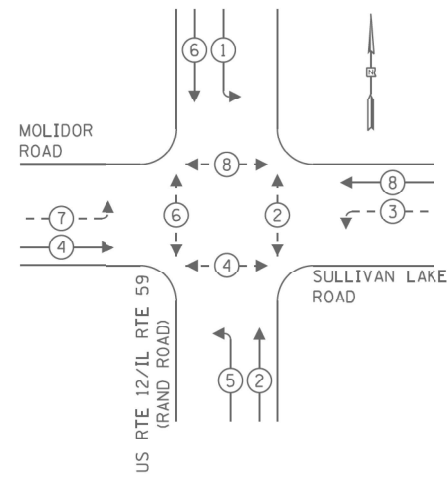
**APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 31
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T59	



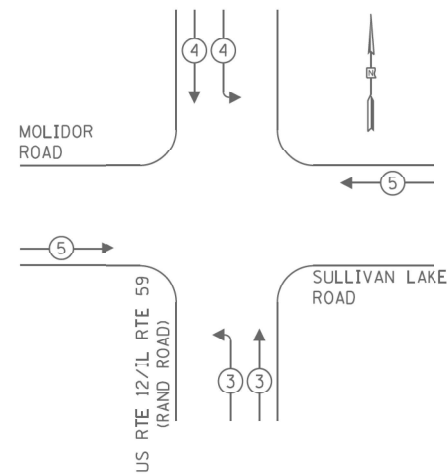
**EXISTING CONTROLLER SEQUENCE**



**LEGEND:**

- ← \* → PROTECTED PHASE
- ← \* - - → PROTECTED/PERMITTED PHASE
- ← \* → PEDESTRIAN PHASE
- ← \* OL → OVERLAP

**EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	18	11	50	99.0
(YELLOW)	18	20	5	18.0
(GREEN)	18	12	45	97.2
PERMISSIVE ARROW	8	10	10	8.0
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	1	150	100	150.0
BLANK-OUT SIGN	-	25	5	-
FLASHER	2	20	50	20.0
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>677.2</b>

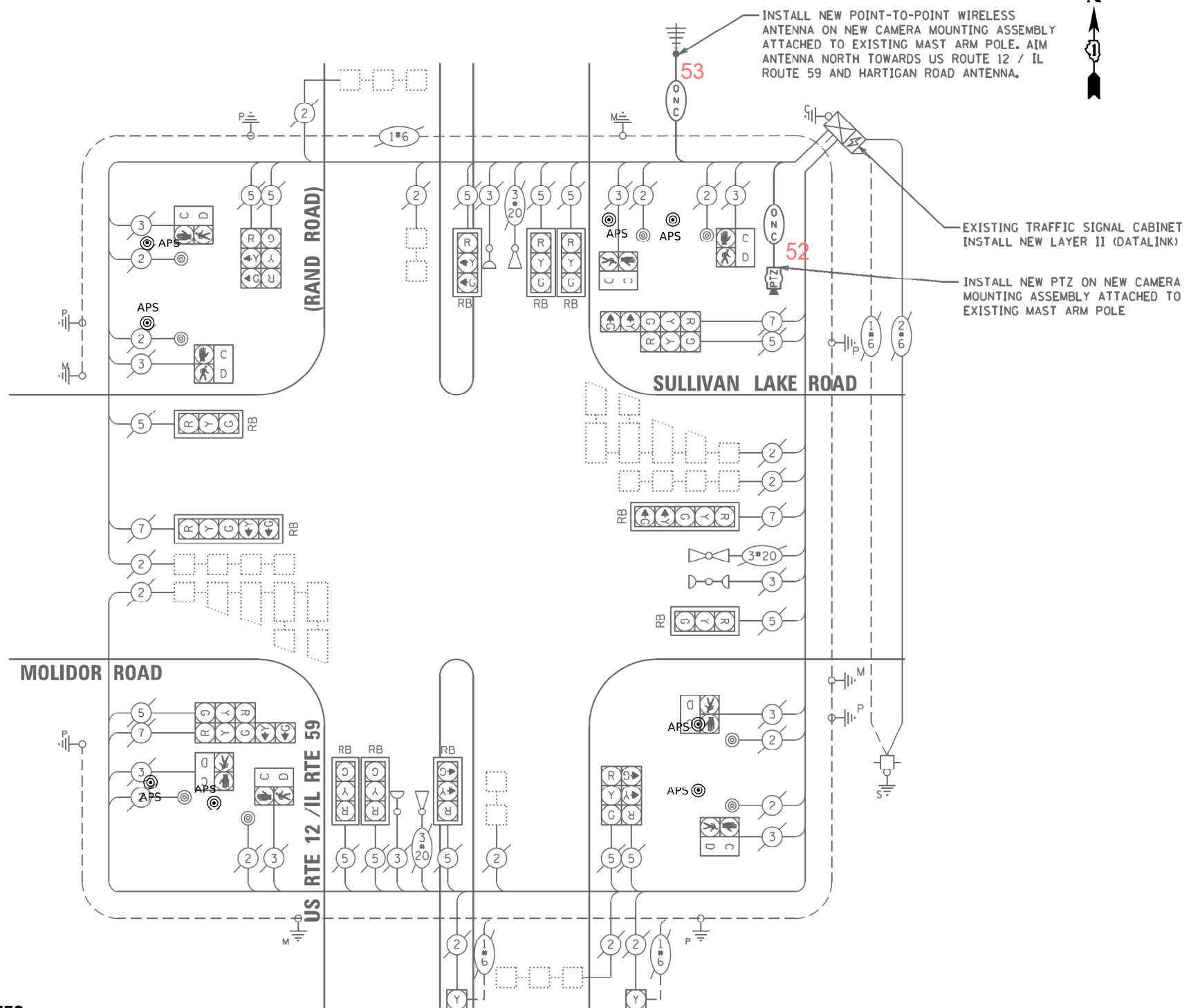
ENERGY COSTS TO:

VILLAGE OF VOLO  
500 S. FISH LAKE ROAD  
VOLO, ILLINOIS 60073

ENERGY SUPPLY: CONTACT: TERRI BLECK  
PHONE: 847-816-5239  
COMPANY: COMMONWEALTH EDISON  
ACCOUNT NUMBER: ---

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	1191
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	359
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	8



**CABLE PLAN**  
(NOT TO SCALE)

FLASHERS - WILL ACTIVATE DURING PHASE 2 YELLOW AND RED INDICATIONS

INSTALL NEW POINT-TO-POINT WIRELESS ANTENNA ON NEW CAMERA MOUNTING ASSEMBLY ATTACHED TO EXISTING MAST ARM POLE. AIM ANTENNA NORTH TOWARDS US ROUTE 12 / IL ROUTE 59 AND HARTIGAN ROAD ANTENNA.

EXISTING TRAFFIC SIGNAL CABINET  
INSTALL NEW LAYER II (DATALINK)

INSTALL NEW PTZ ON NEW CAMERA MOUNTING ASSEMBLY ATTACHED TO EXISTING MAST ARM POLE



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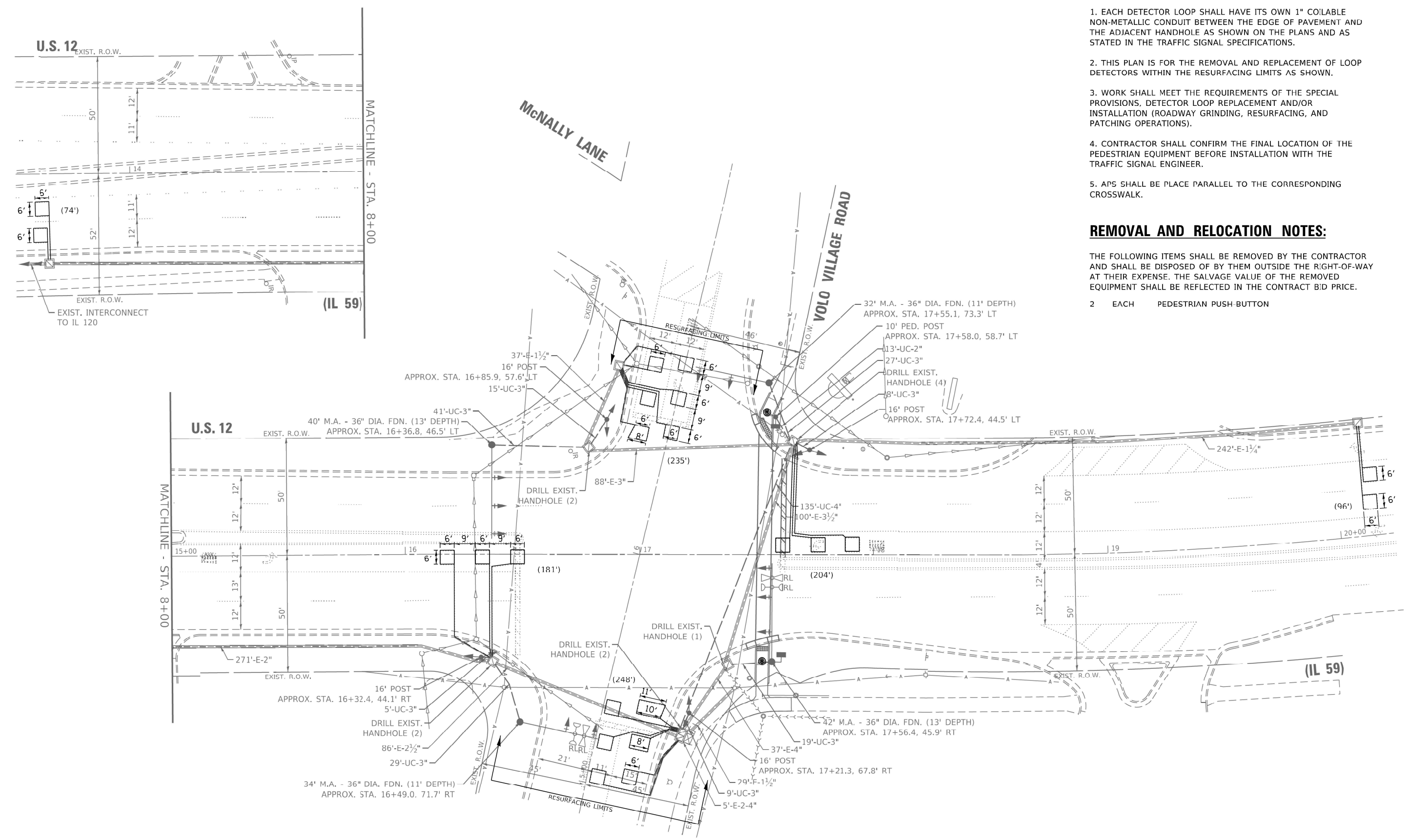
**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.
3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
4. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
5. APS SHALL BE PLACE PARALLEL TO THE CORRESPONDING CROSSWALK.

**REMOVAL AND RELOCATION NOTES:**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH PEDESTRIAN PUSH-BUTTON



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	DRAWN -	REVISED -
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PLOT DATE = 10/19/2023	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

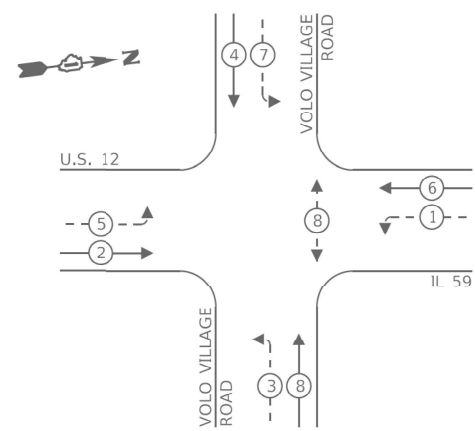
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**IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**  
 SCALE: 1"=50'      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 R5	LAKE & MCHENRY	59	33
CONTRACT NO. 62759				
ILLINOIS FED. AID PROJECT				

**TS 6511  
EAGLE 1C**



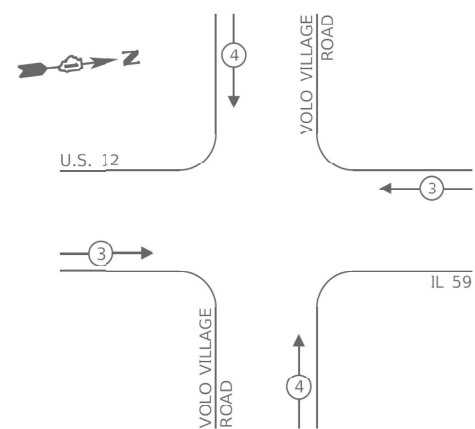
**PROPOSED CONTROLLER SEQUENCE**



**LEGEND:**

- ← ⊛ → PROTECTED PHASE
- ← ⊛ - - → PROTECTED/PERMITTED PHASE
- ← ⊛ → PEDESTRIAN PHASE
- ⊛ OL OVERLAP

**PROPOSED EMERGENCY VEHICLE  
PREEMPTION SEQUENCE**



**TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	18	11	50	99
(YELLOW)	18	20	5	18
(GREEN)	18	12	45	97.2
PERMISSIVE ARROW	16	10	10	16
PED. SIGNAL	2	20	100	40
CONTROLLER	1	100	100	100
UPS	1	25	100	25
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	4	120	50	240
LUMINAIRE	-	250	50	-
TOTAL =				635.2

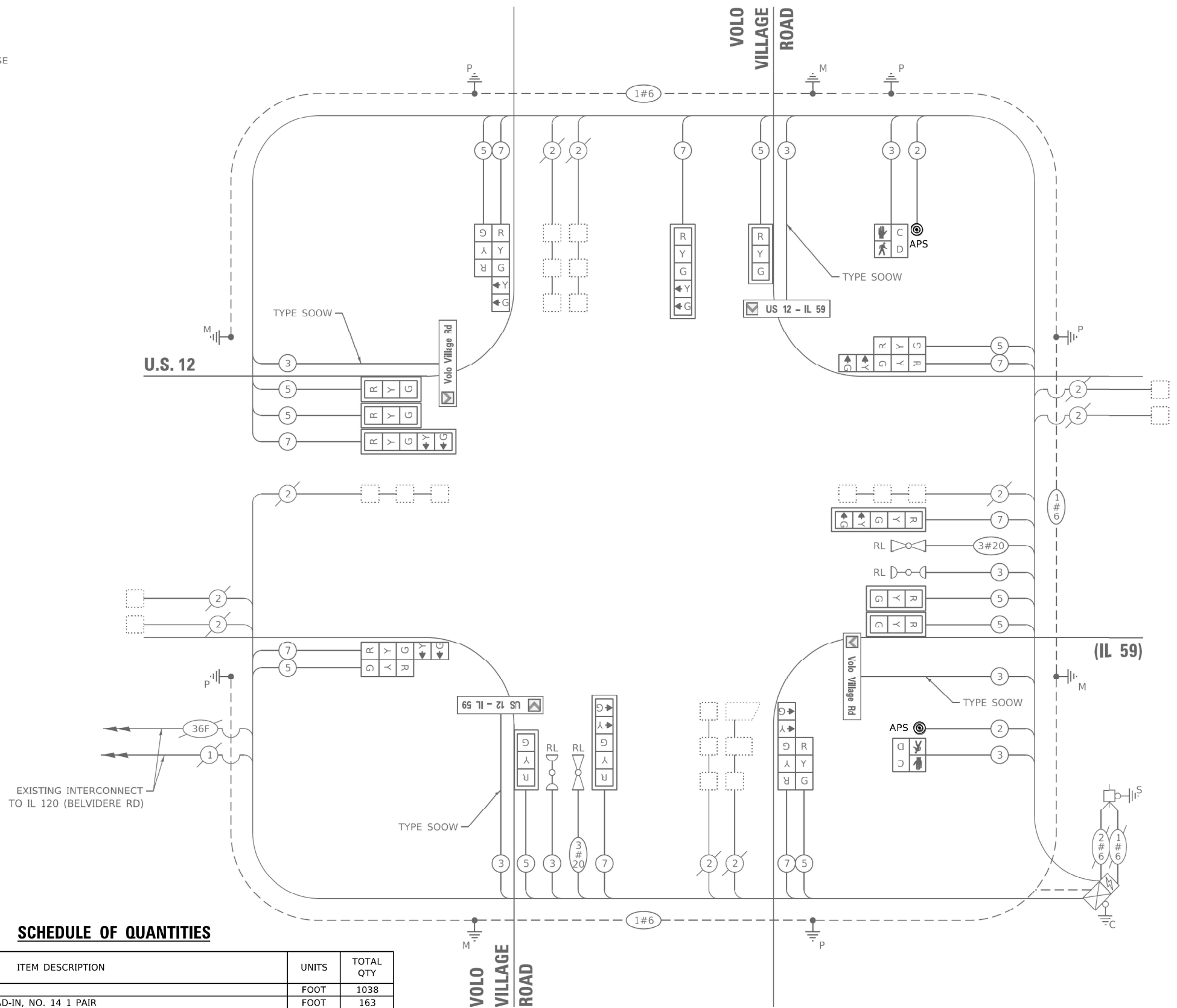
ENERGY COSTS TO:

VILLAGE OF VOLO  
500 SOUTH FISH LAKE ROAD  
VOLO, IL 60073

ENERGY SUPPLY: CONTACT: TERRI BLECK  
PHONE: (847) 816-5234  
COMPANY: COM-ED  
ACCOUNT NUMBER: -

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	1038
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	163
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2



**CABLE PLAN  
(NOT TO SCALE)**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

**TS 6511  
EAGLE 1C**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	34
CONTRACT NO. 62759				

ILLINOIS FED. AID PROJECT

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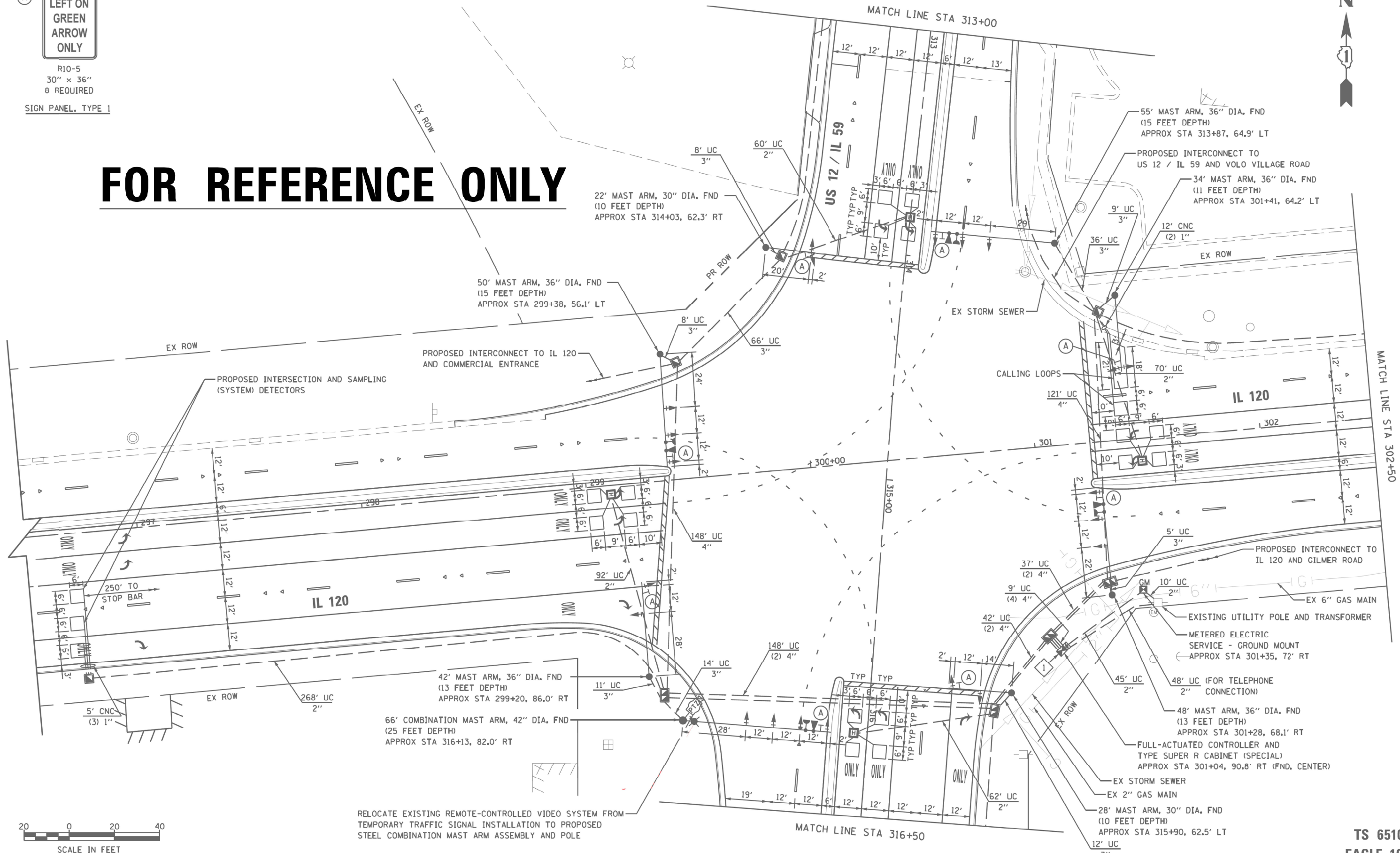


**LEFT ON GREEN  
ARROW  
ONLY**

R10-5  
30" x 36"  
8 REQUIRED

SIGN PANEL, TYPE 1

**FOR REFERENCE ONLY**



RELOCATE EXISTING REMOTE-CONTROLLED VIDEO SYSTEM FROM TEMPORARY TRAFFIC SIGNAL INSTALLATION TO PROPOSED STEEL COMBINATION MAST ARM ASSEMBLY AND POLE

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

**TS 6510  
EAGLE 1C**

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	DRAWN -	REVISED -
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PLOT DATE = 10/19/2023	DATE -	REVISED -

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 35
				CONTRACT NO. 62759
ILLINOIS FED. AID PROJECT				

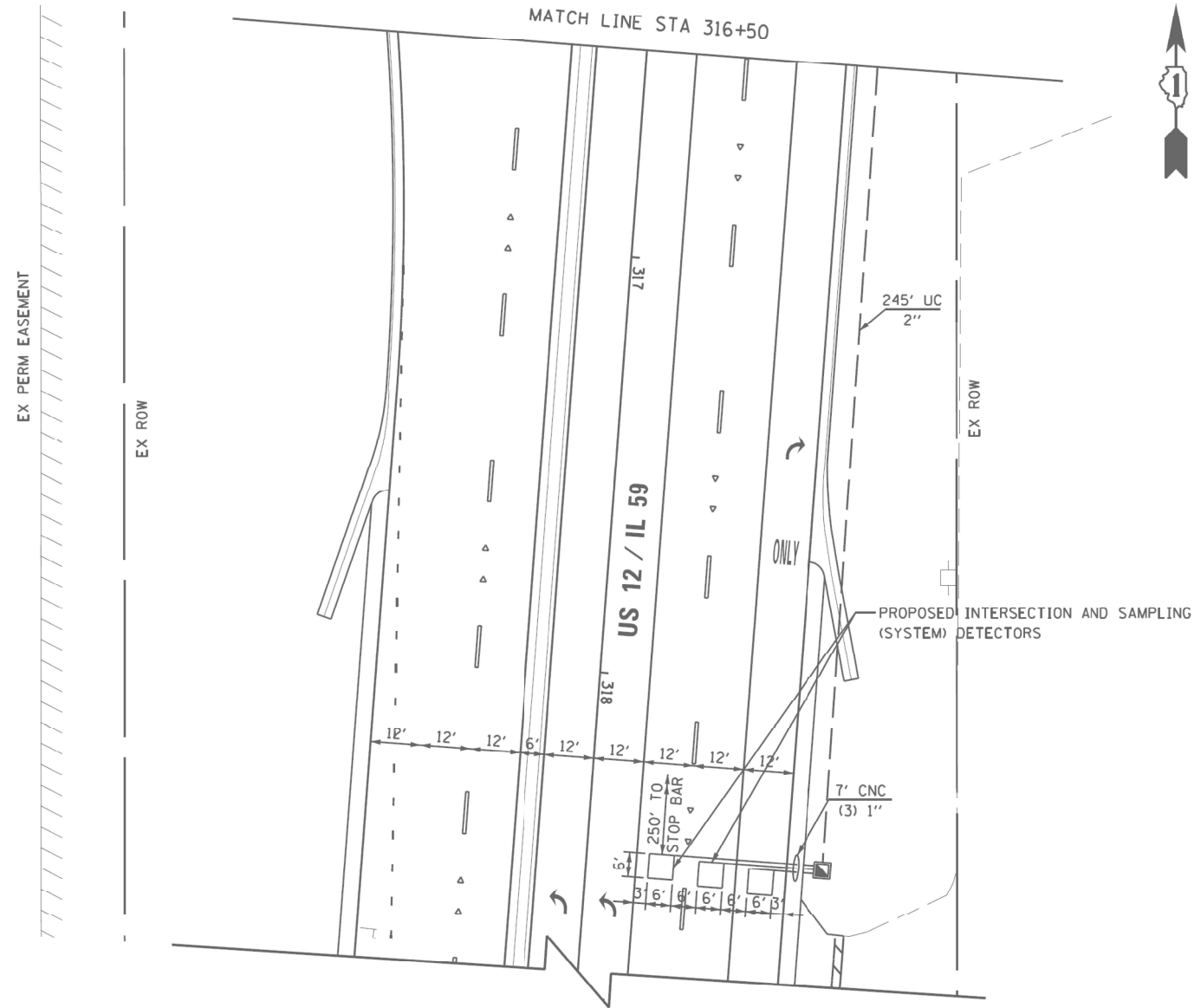
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 PLOT USER: mohammad.hamwi  
 PLOT DEVICE: HP DesignJet T1100ps  
 PLOT STATUS: Success  
 PLOT MESSAGE:

**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

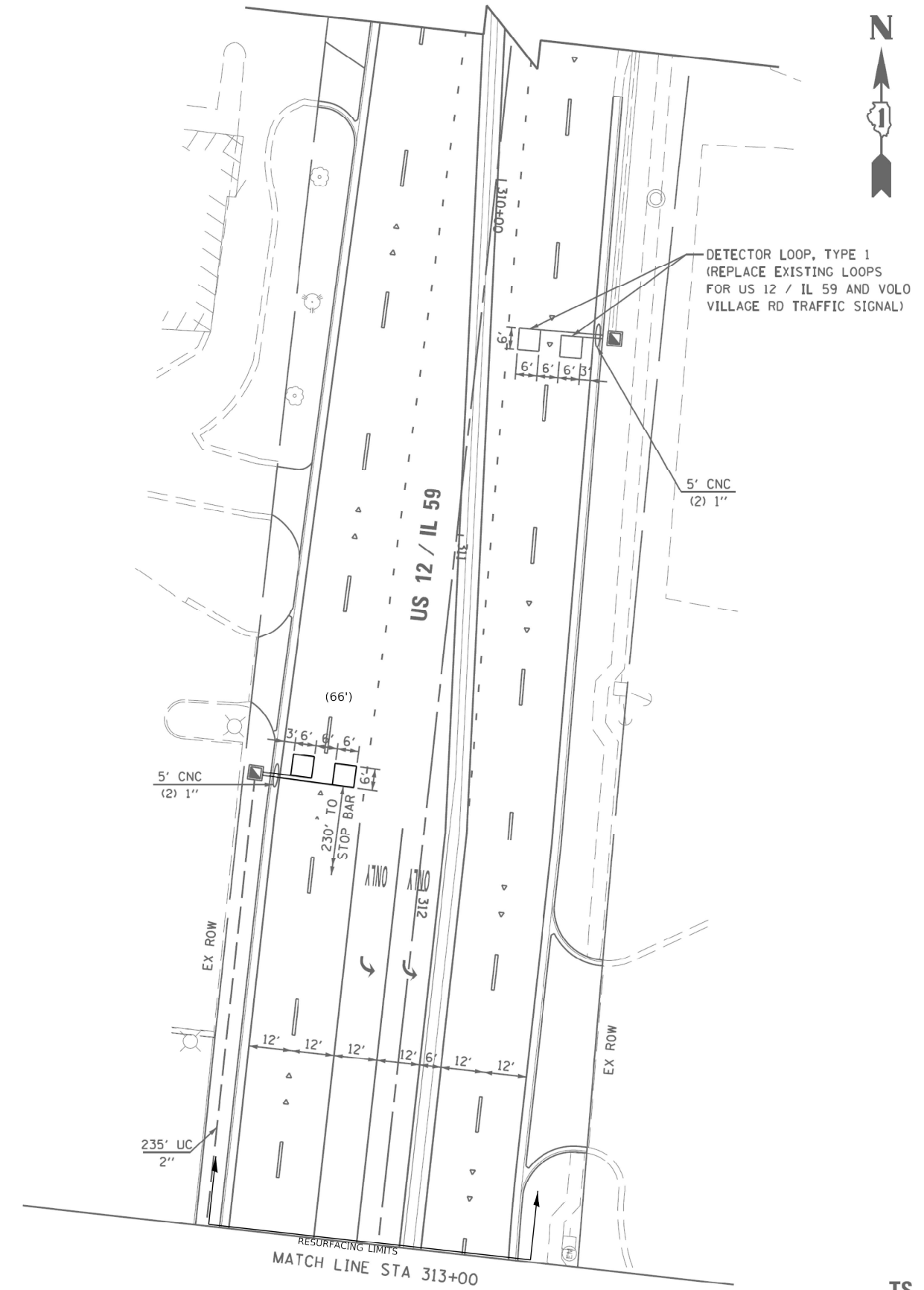
2. THIS PLAN IS FOR THE REMOVAL AND REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.

3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).



**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY
DETECTOR LOOP REPLACEMENT	FOOT	66
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	10



**TS 6510  
EAGLE 1C**

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PLOT DATE = 10/19/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**APS AND DETECTOR LOOP INSTALLATION PLAN  
IL-59 /US 12 (RAND RD) FROM STATE PARK /EAST ST. TO N. OF IL 120**

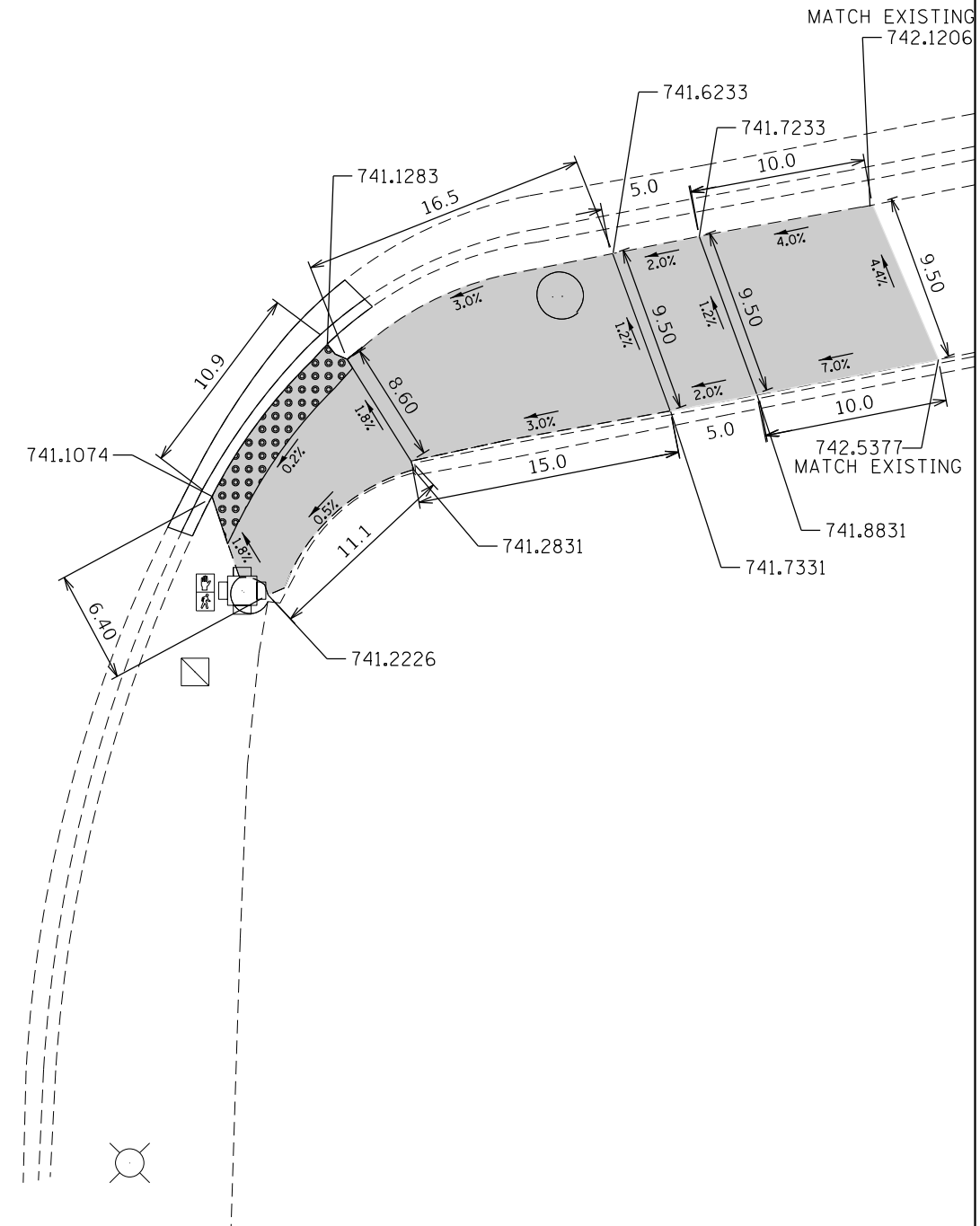
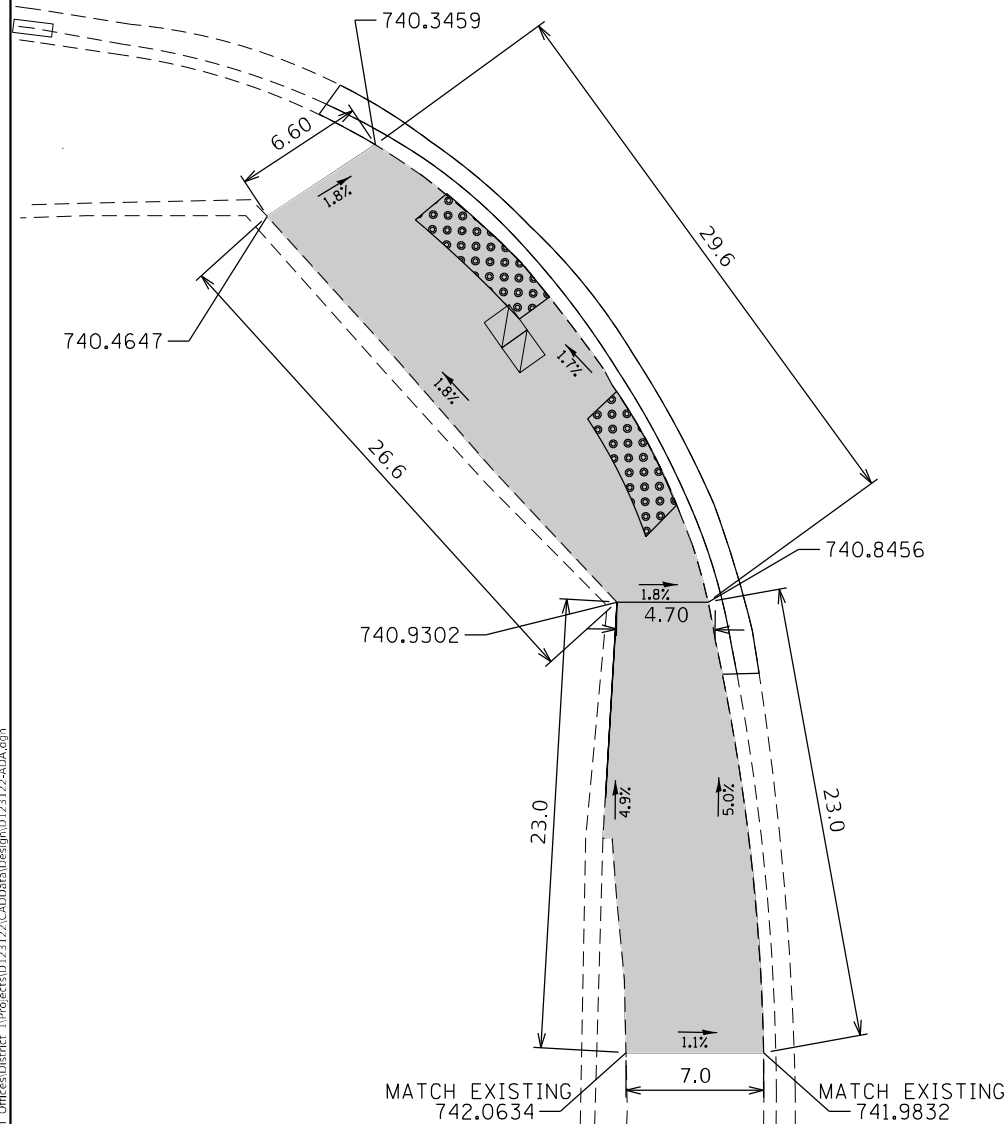
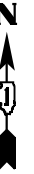
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F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 36
ILLINOIS				FED. AID PROJECT

CONTRACT NO. 62T59

# GRAND AVE.

# US 12



### DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: IN THE E. BASE OF T.S. W/ ARM IN THE N.W CORNER OF US 12 AND GRAND AVE. EL. 741.24

### LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

### CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

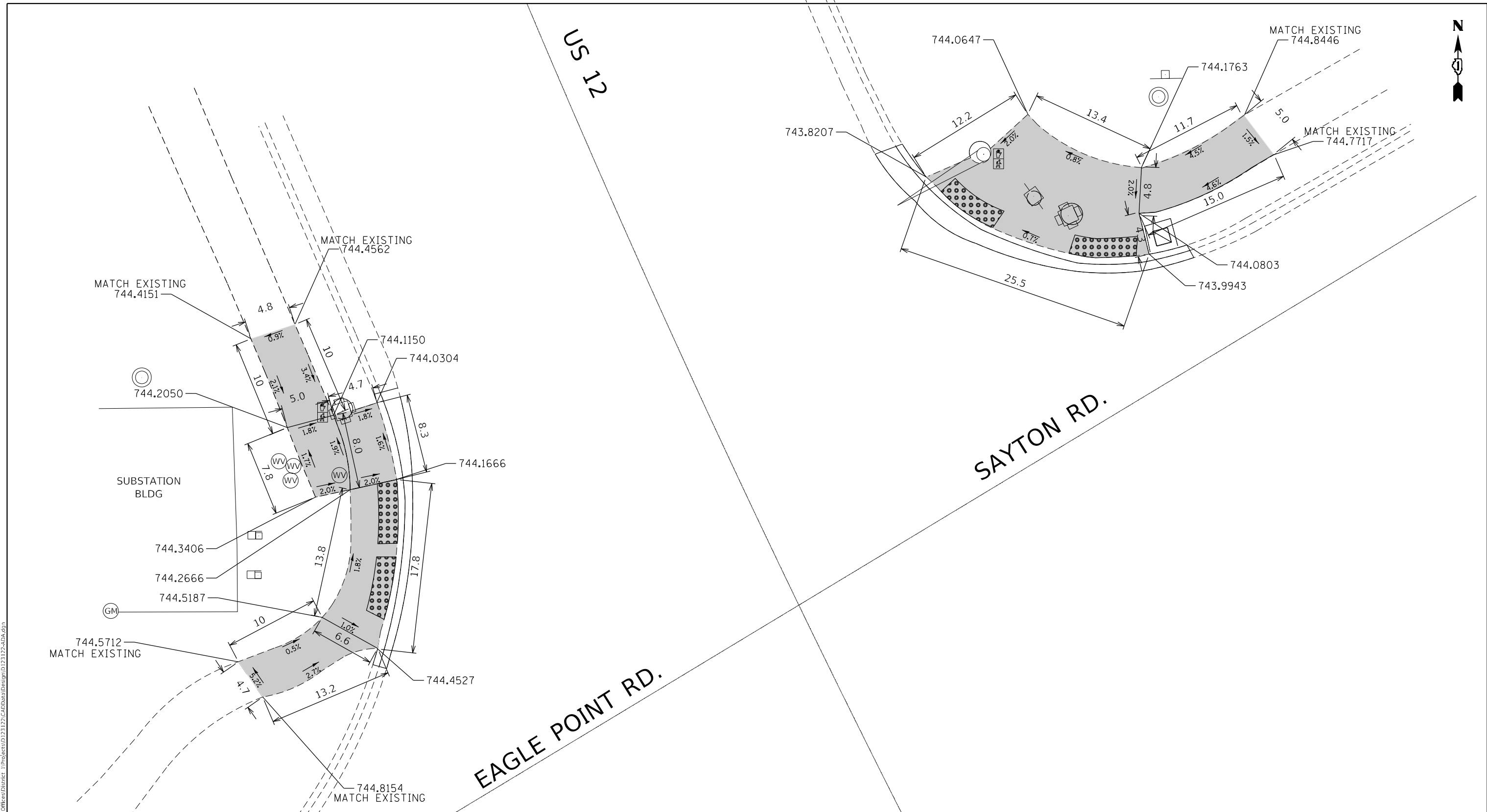
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

US 12 AND GRAND AVE  
CURB RAMP IMPROVEMENT

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	37
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T59	

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**DESIGNER NOTES**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
  - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
  - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
  - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS
- BENCHMARK: ARROW ON BOLT OF F.H. IN THE S.E. CORNER OF US 12 & SAYTON RD EL. 746.97

**LEGEND**

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

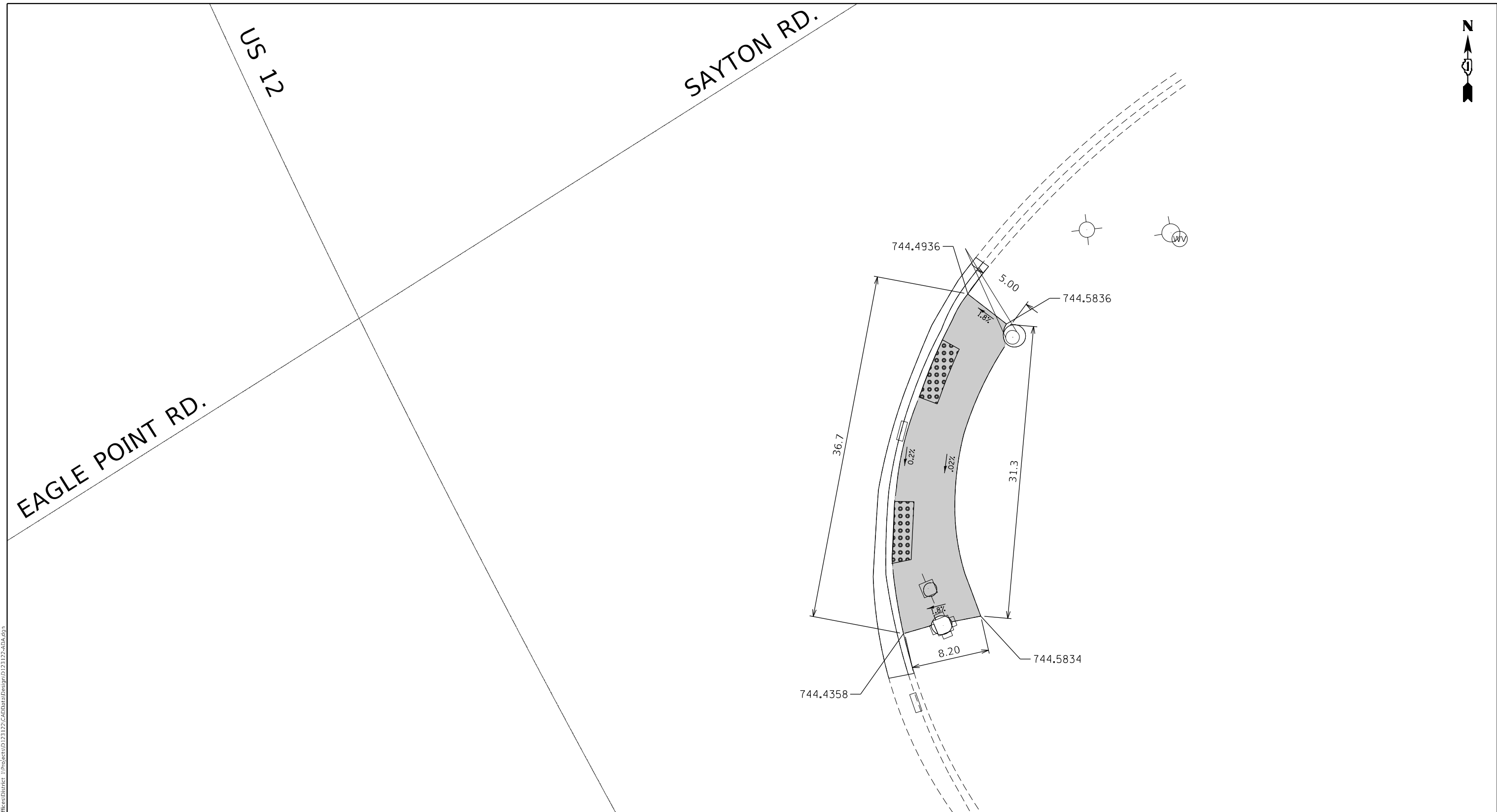
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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
DRAWN -	REVISIONS -	
PLOT SCALE = 9.7883' / in.	CHECKED -	REVISIONS -
PLOT DATE = 12/15/2023	DATE -	REVISIONS -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

US 12 AND SAYTON RD / EAGLE POINT RD			
CURB RAMP IMPROVEMENT			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 334	SECTION FAP 0334 22 RS	COUNTY LAKE & MCHENRY	TOTAL SHEETS 59	SHEET NO. 38
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				







**DESIGNER NOTES**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: ARROW ON BOLT OF F.H. IN THE S.E. CORNER OF US 12 & SAYTON RD EL. 746.97

**LEGEND**

-  PROPOSED SIDE CURB
-  EXIST. GRASS
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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	DRAWN -	REVISED -
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PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

US 12 AND SAYTON RD / EAGLE POINT RD			
CURB RAMP IMPROVEMENT			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

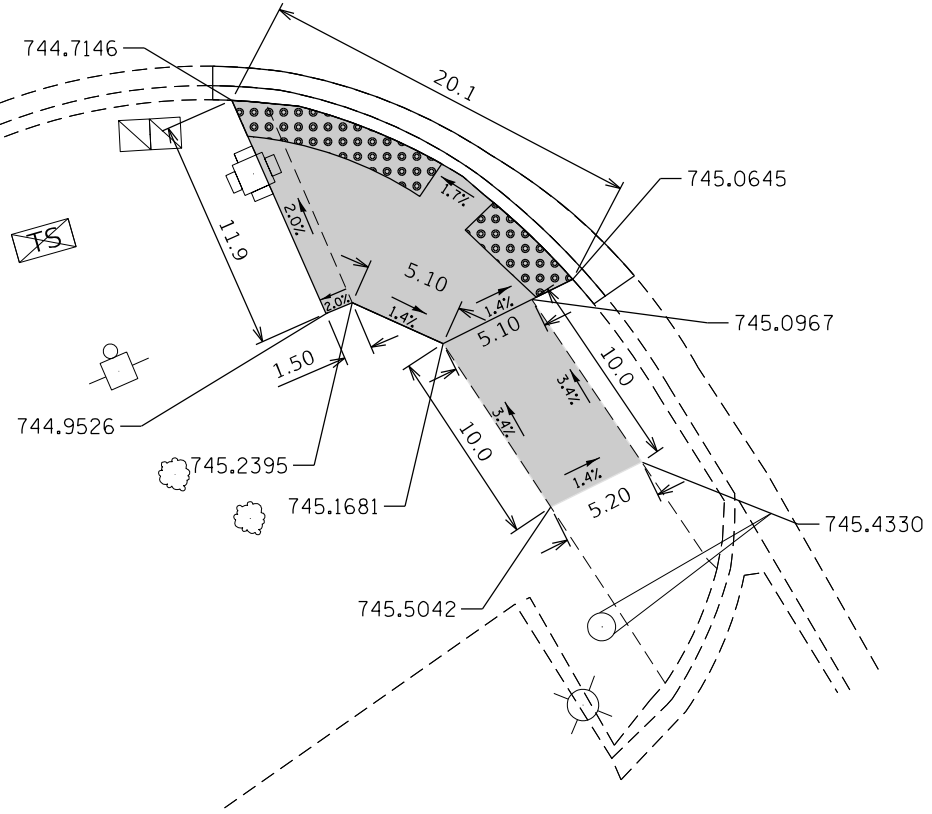
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	39
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				



SAYTON RD.

US 12

EAGLE POINT RD.







**DESIGNER NOTES**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

BENCHMARK: ARROW ON BOLT OF F.H. IN THE S.E. CORNER OF US 12 & SAYTON RD EL. 746.97

**LEGEND**

-  PROPOSED SIDE CURB
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS
-  EXIST. GRASS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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	DRAWN -	REVISED -
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PLOT DATE = 12/15/2023	DATE -	REVISED -

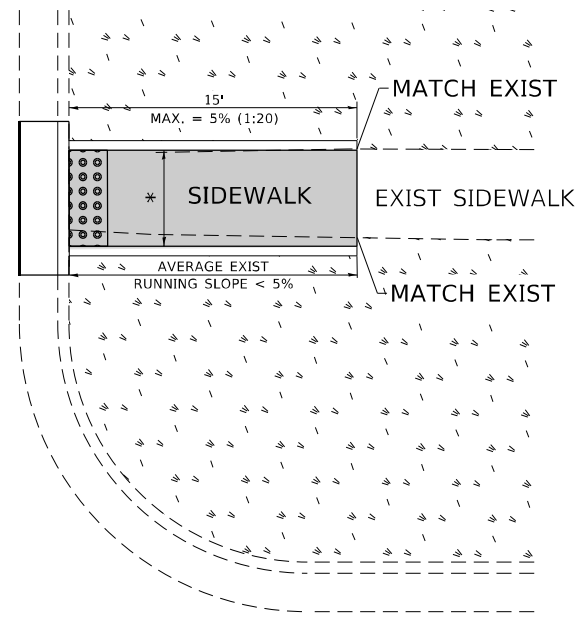
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

US 12 AND SAYTON RD / EAGLE POINT RD			
CURB RAMP IMPROVEMENT			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

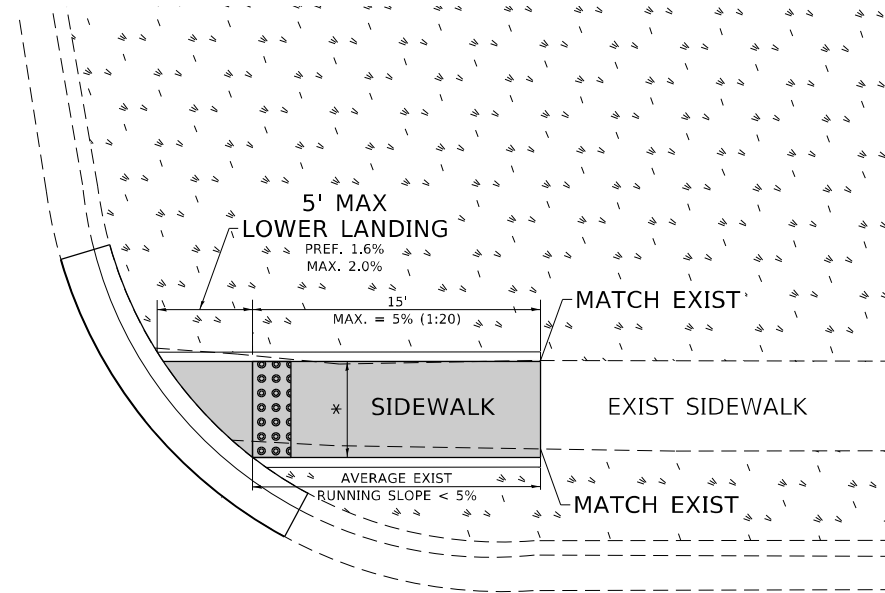
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	40
CONTRACT NO. 62T59				
ILLINOIS FED. AID PROJECT				

# ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

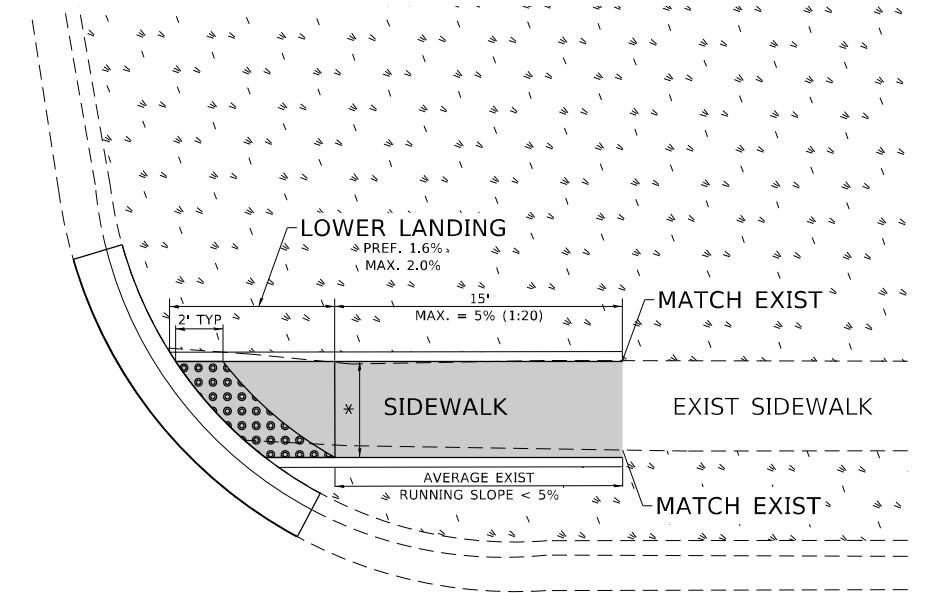
**PD-01A**



**PD-01B**



**PD-01C**



**LEGEND**

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

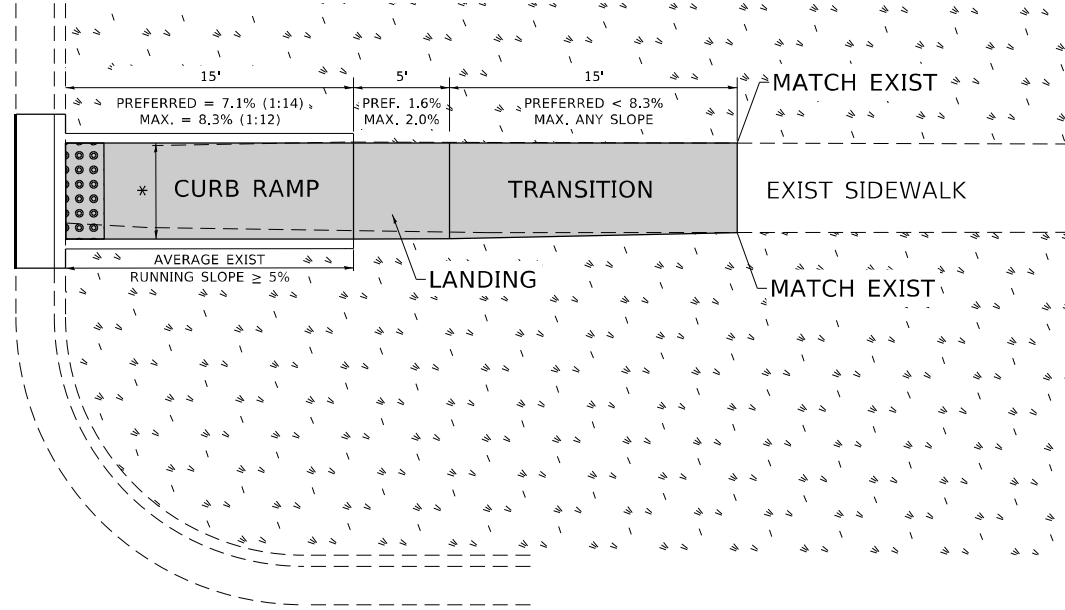
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS  
(PD-01)**

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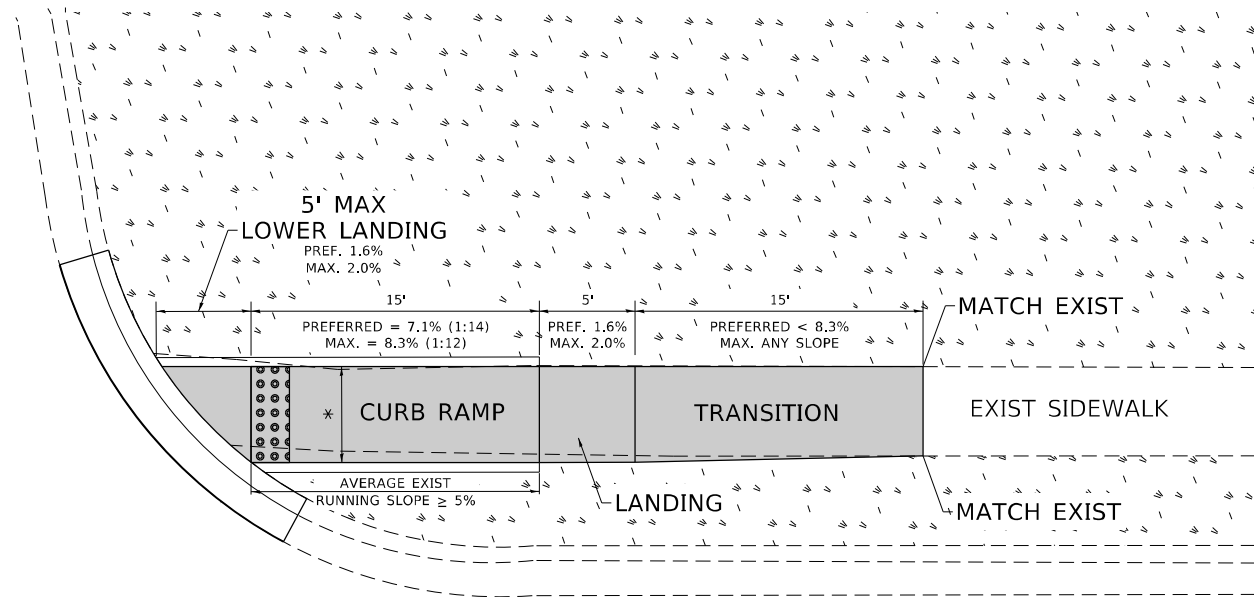
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	41
PD-01			CONTRACT NO. 62T59	
ILLINOIS FED. AID PROJECT				

# ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

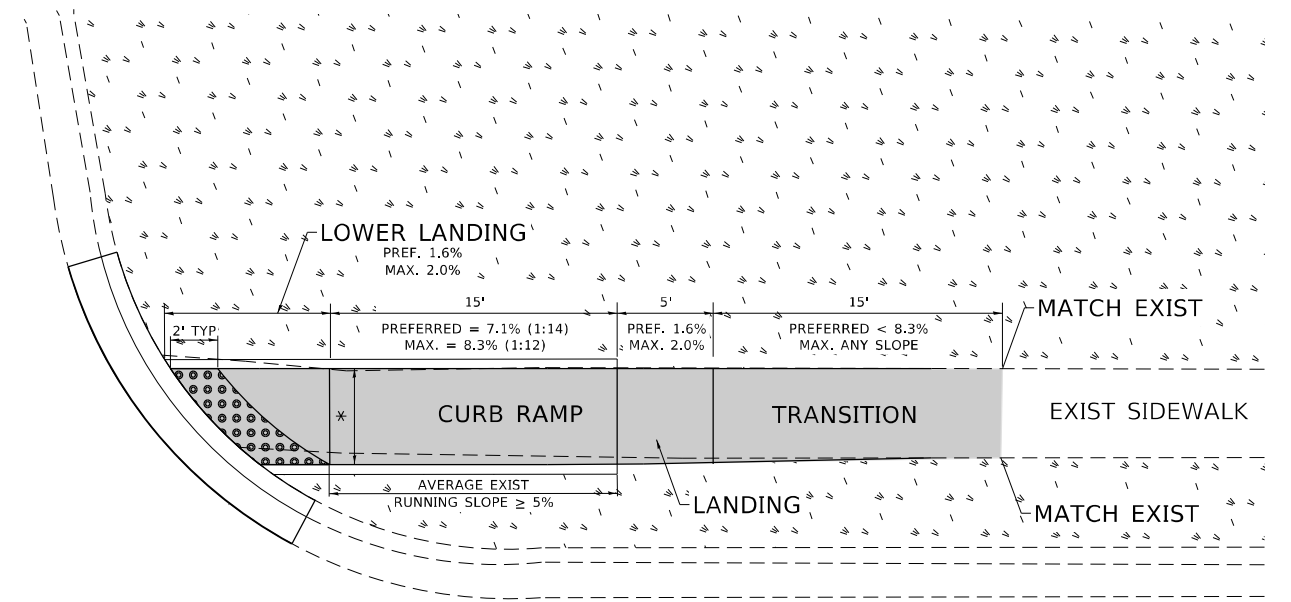
**PD-02A**







**PD-02B**



**PD-02C**



**LEGEND**

-  EXIST. GRASS
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS
-  PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE - 10/02/2019	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS  
(PD-02)

SCALE: NONE SHEET OF SHEETS STA. TO STA.

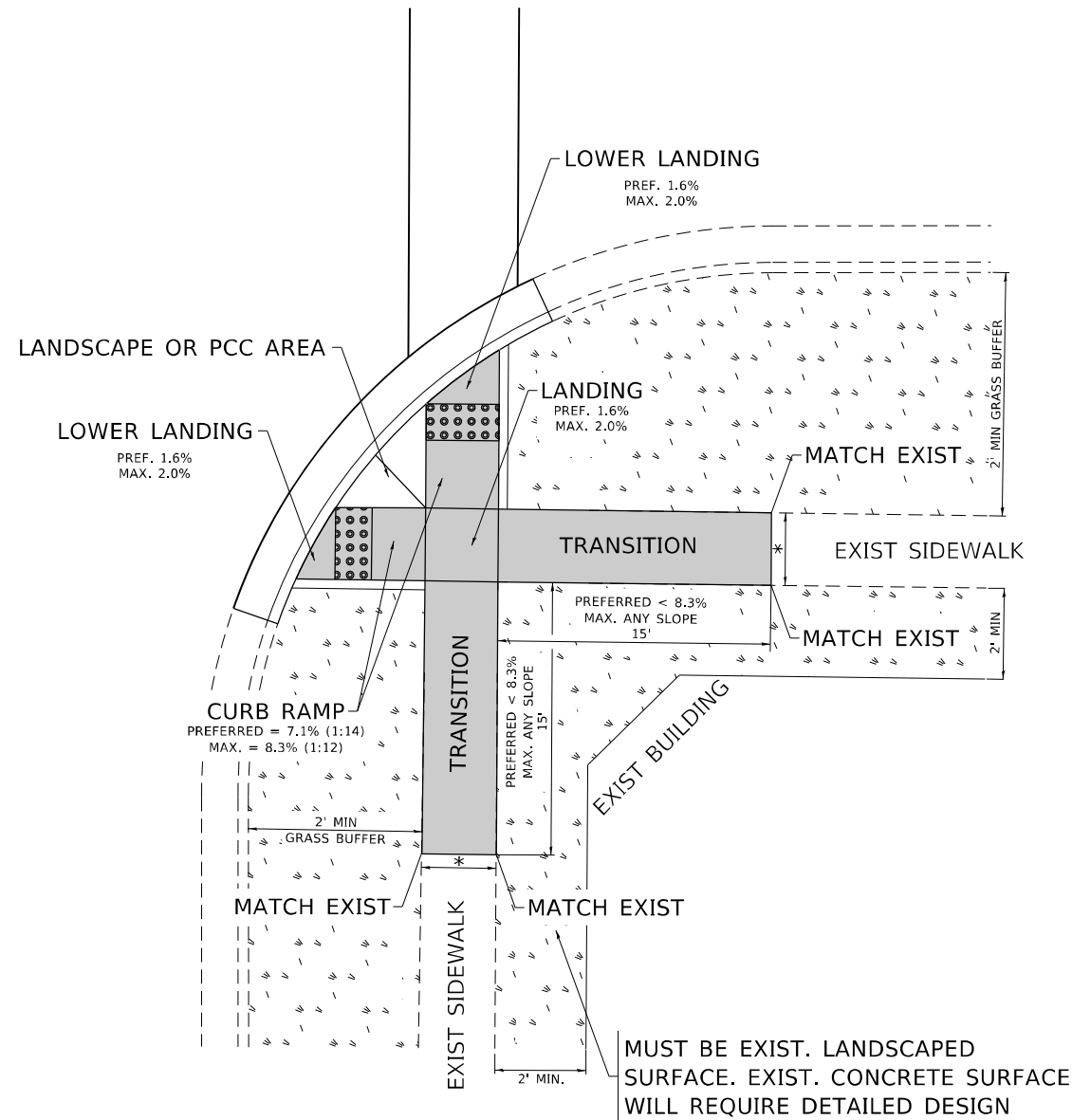
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	42
	PD-02			CONTRACT NO. 62T59

ILLINOIS FED. AID PROJECT

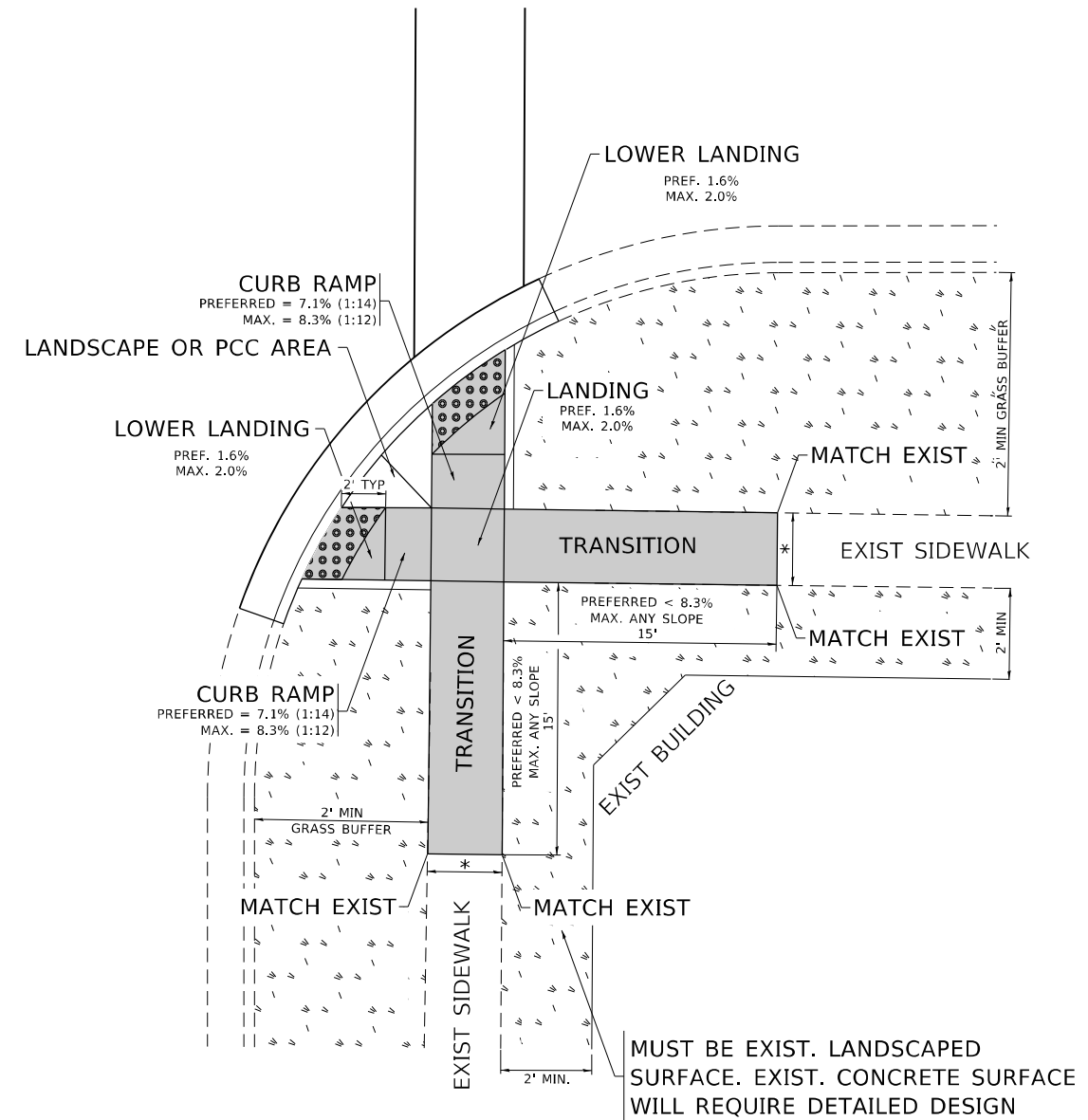


# ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

**PD-03A**



**PD-03B**



**LEGEND**

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

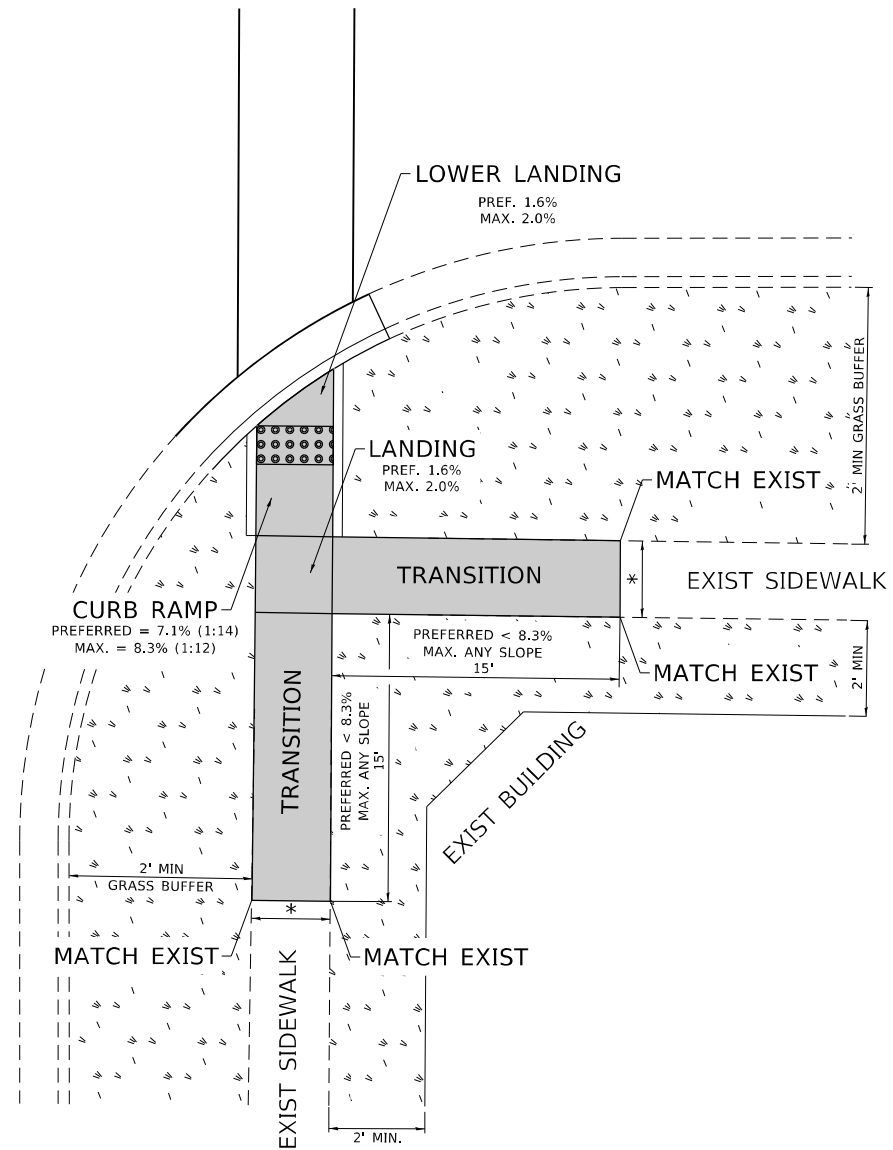
**PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS  
(PD-03)**

SCALE: NONE    SHEET    OF    SHEETS    STA.    TO STA.

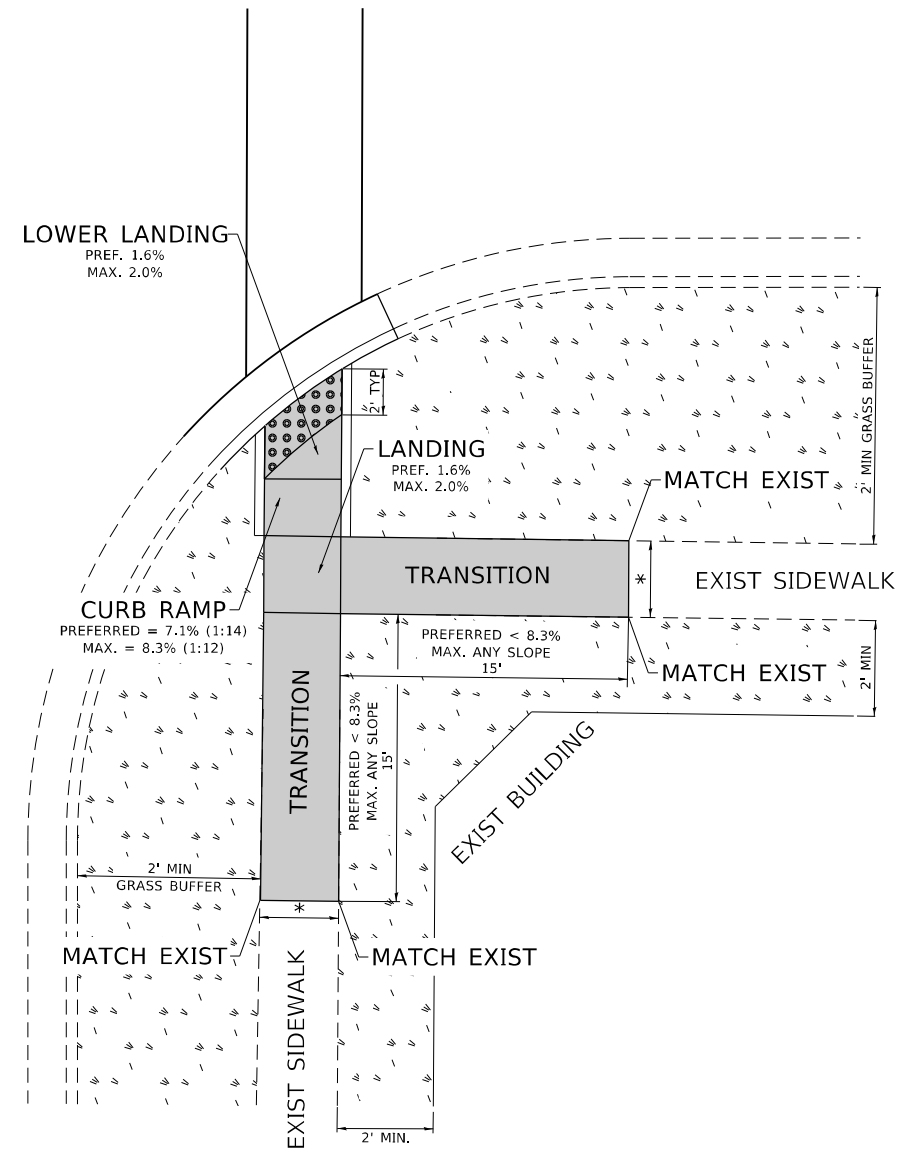
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	43
PD-03		CONTRACT NO. 62T59		
ILLINOIS FED. AID PROJECT				

# ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

**PD-04A**



**PD-04B**



**LEGEND**

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH  
TURNING SPACE (PD-04)**

SCALE: NONE    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	44
	PD-04			CONTRACT NO. 62T59

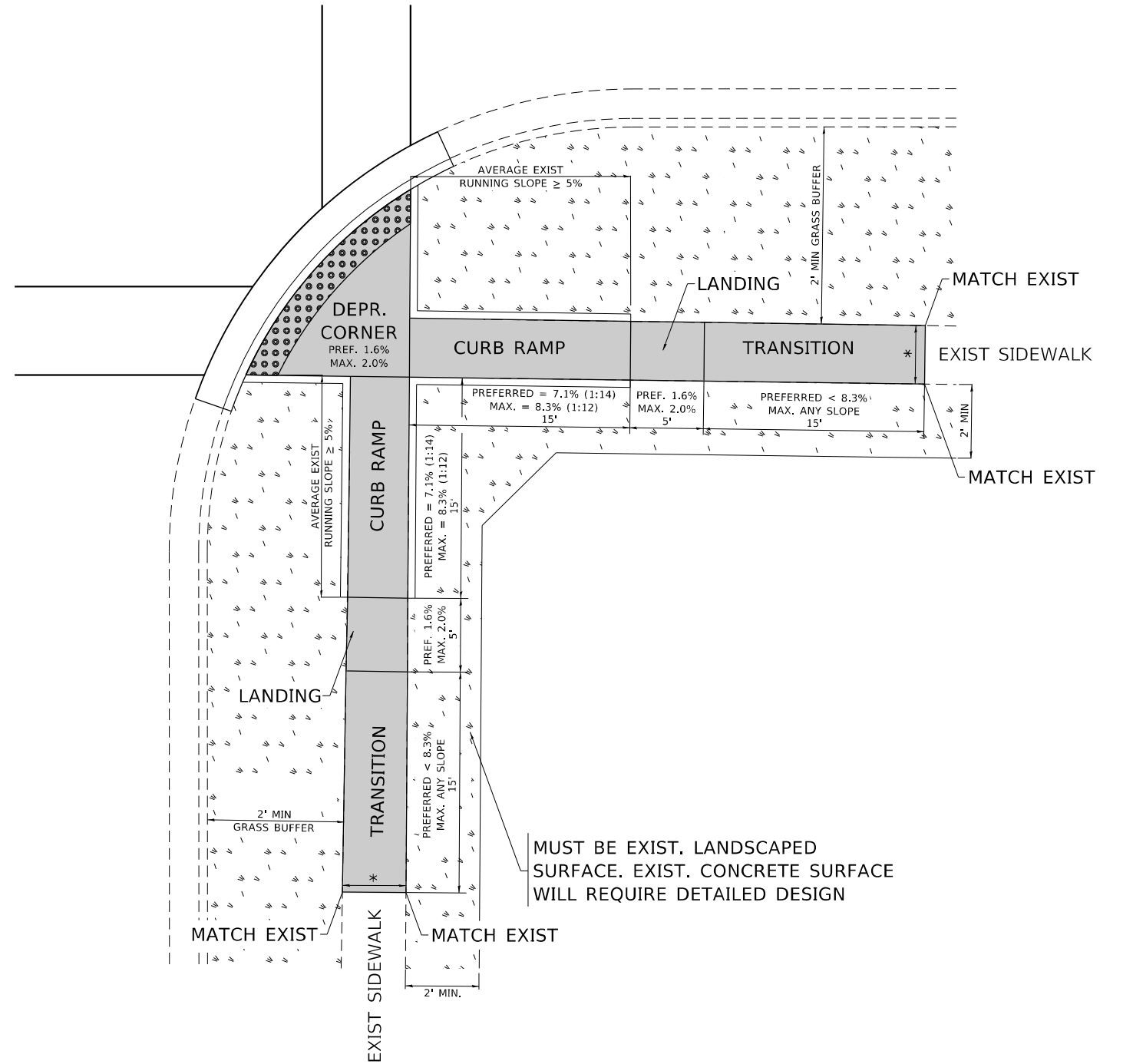
ILLINOIS FED. AID PROJECT

# ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

**PD-05A**



**PD-05B**



**LEGEND**

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED -
DRAWN - R. LEDEZMA	REVISOR -	REVISOR -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/15/2023	DATE - 10/02/2019	REVISOR -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

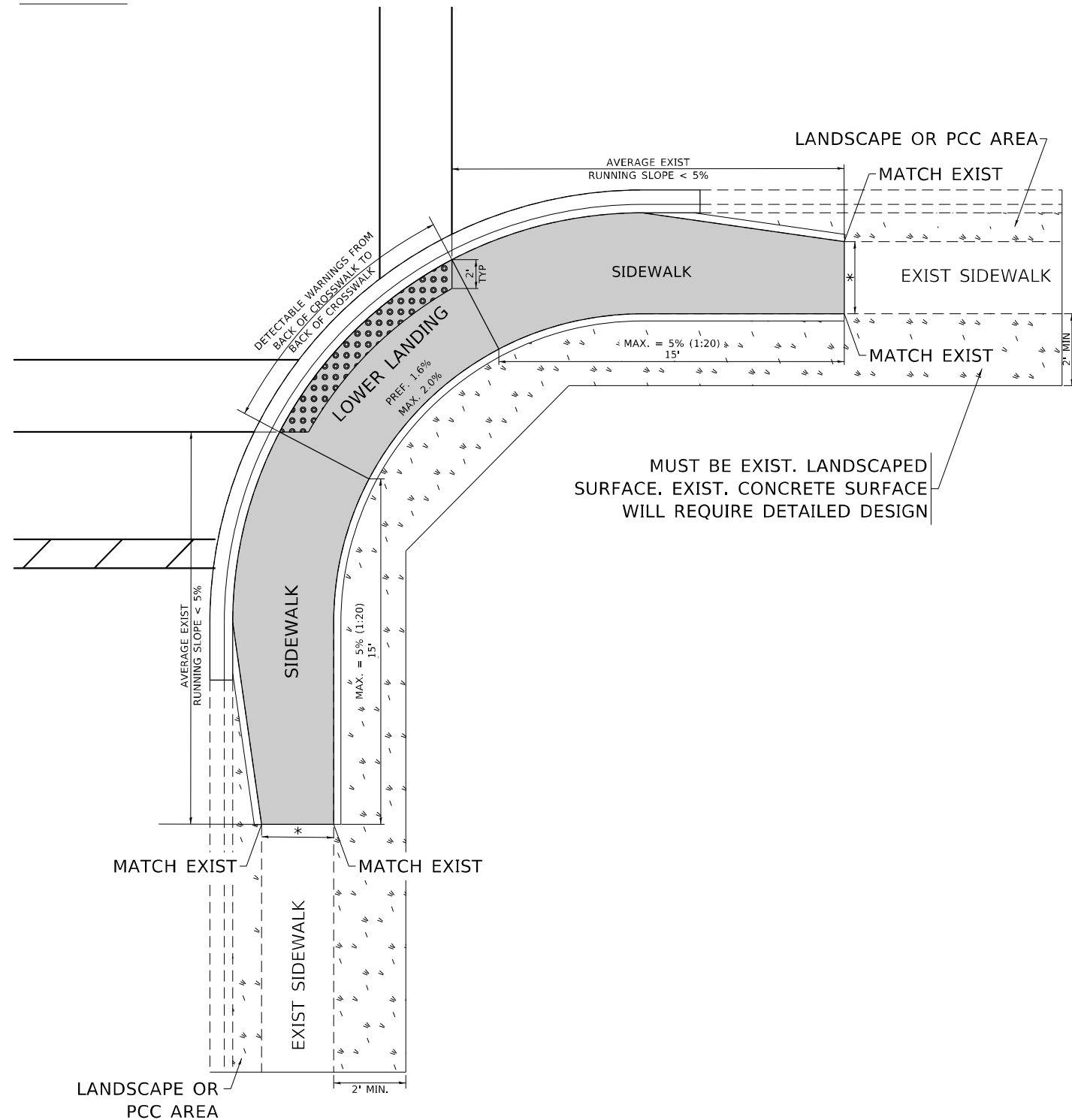
**PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS  
(PD-05)**

SCALE: NONE    SHEET    OF    SHEETS    STA.    TO    STA.

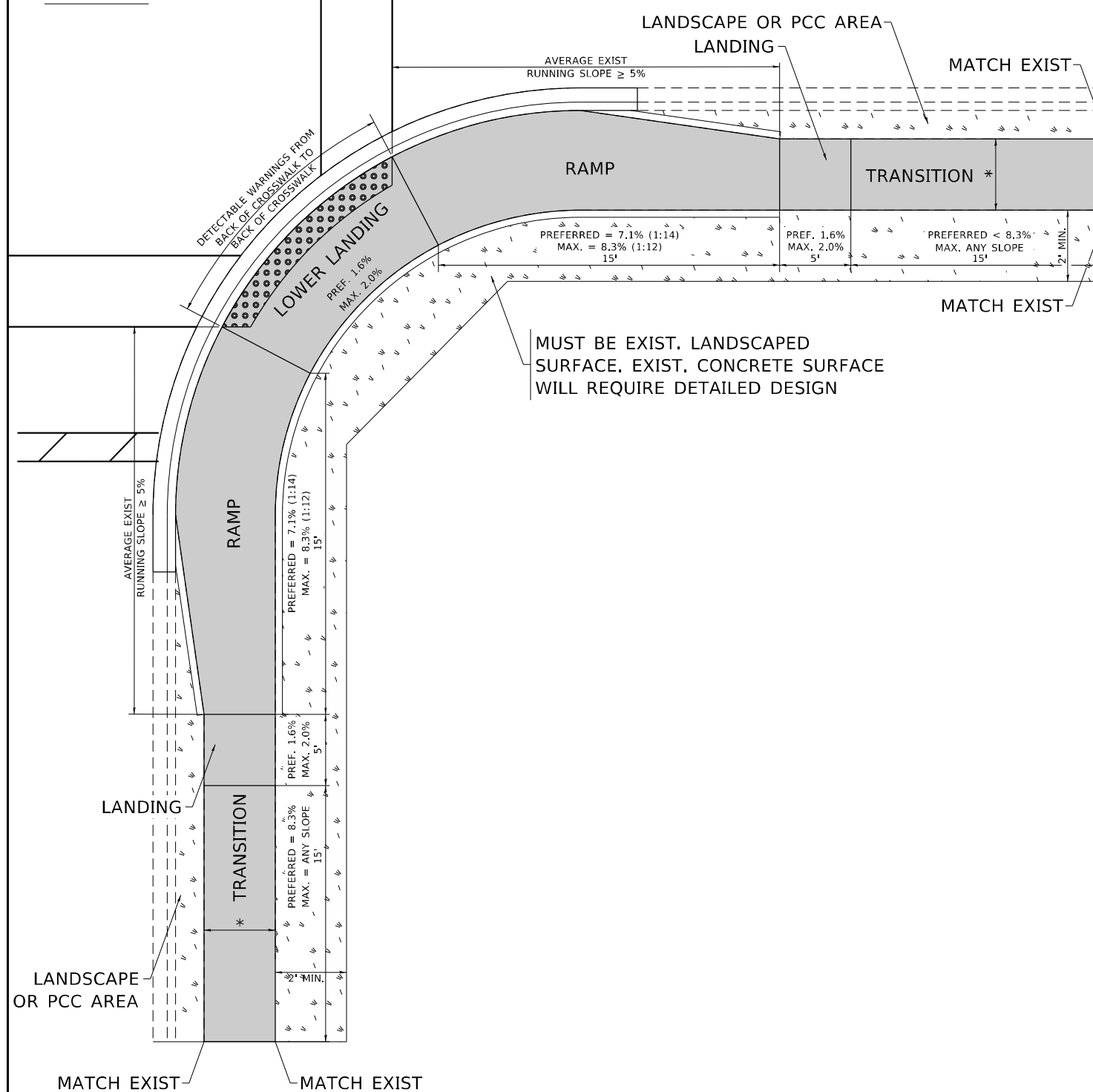
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	45
	PD-05		CONTRACT NO. 62T59	
		ILLINOIS	FED. AID PROJECT	

# ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

**PD-06A**



**PD-06B**



**LEGEND**

-  EXIST. GRASS
-  PROPOSED SIDE CURB
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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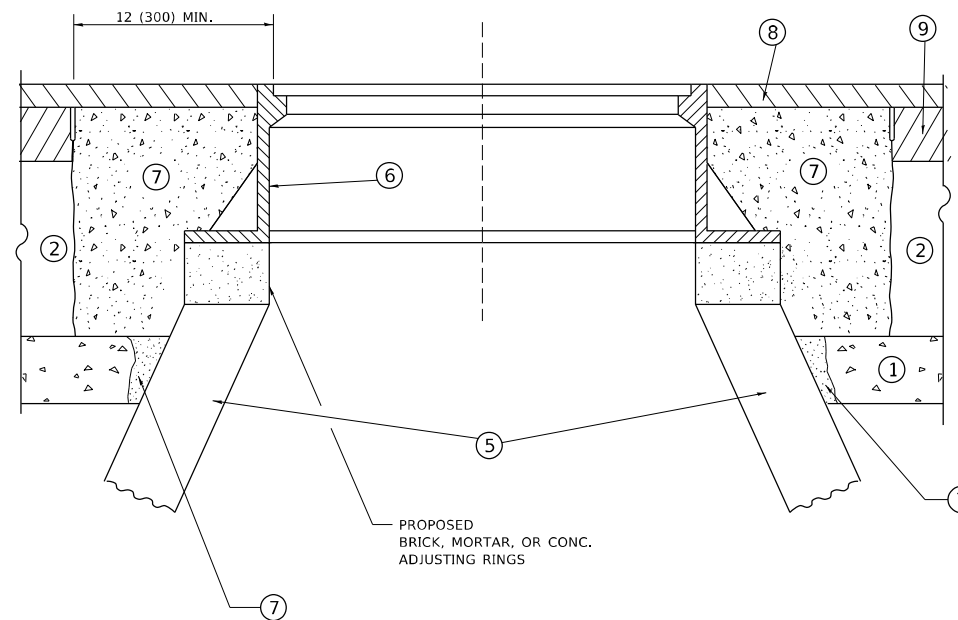
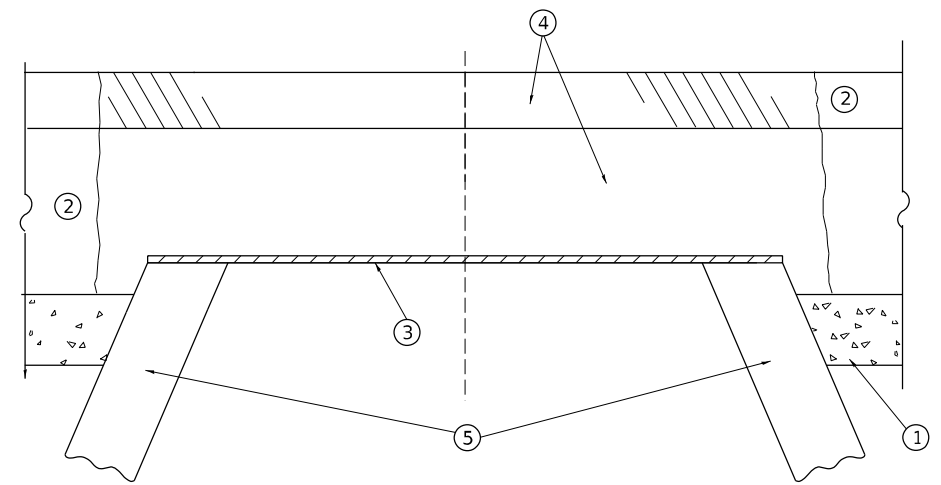
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	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR PARALLEL CURB RAMPS  
(PD-06)**

SCALE: NONE    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	46
	PD-06		CONTRACT NO. 62T59	
		ILLINOIS	FED. AID PROJECT	



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-2\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

- 1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- 2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- 3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- 4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

**NOTES**

- 1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- 3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- 4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- 5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

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USER NAME = Wendy.Andonayre	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED - R. BORO 12-06-11
PLOT DATE = 12/15/2023	CHECKED -	REVISED - K. SMITH 11-18-22
	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

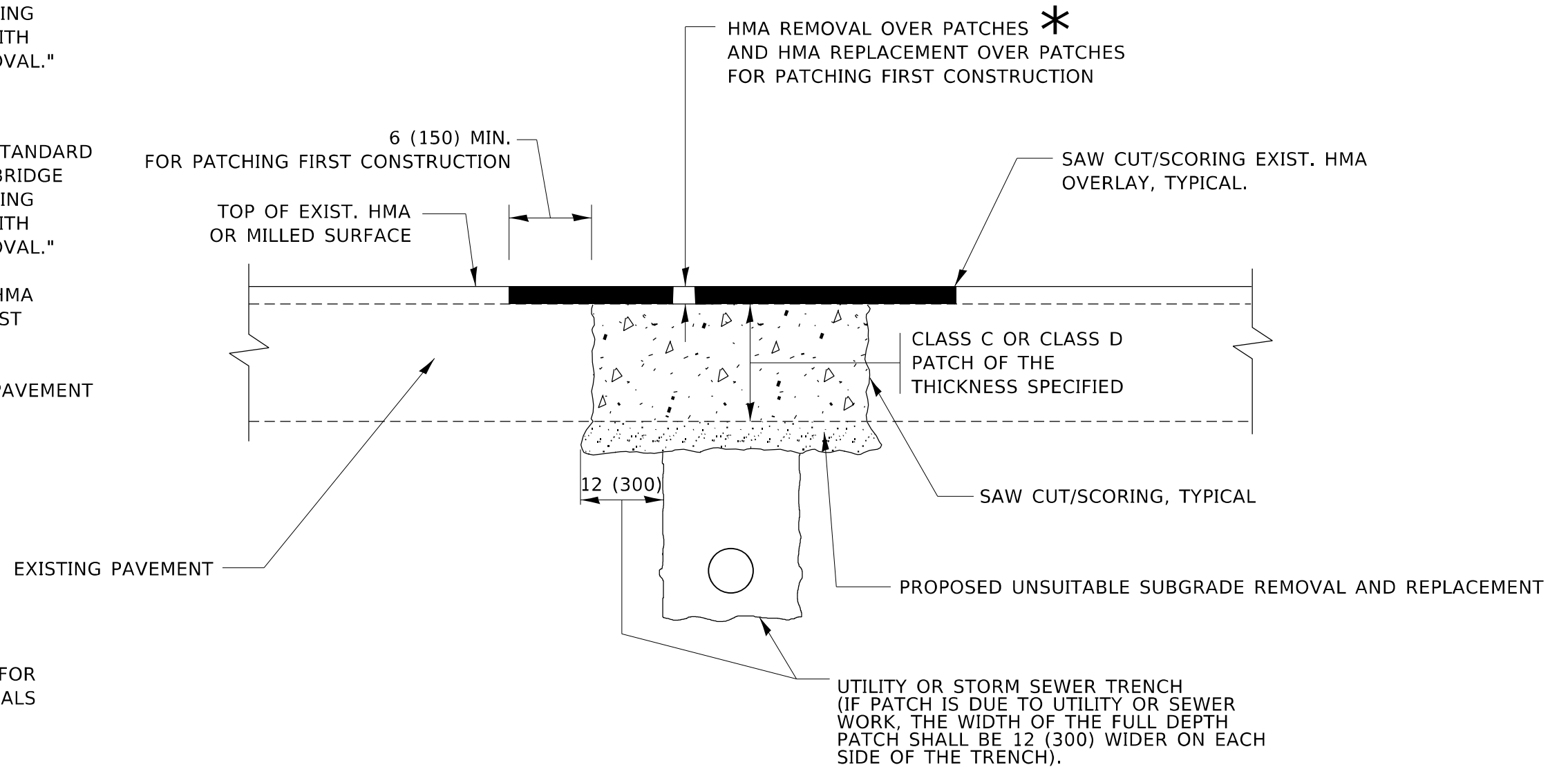
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	47
BD600-03 (BD-08)			CONTRACT NO. 62T59	
ILLINOIS FED. AID PROJECT				

**METHOD OF MEASUREMENT**

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

**BASIS OF PAYMENT**

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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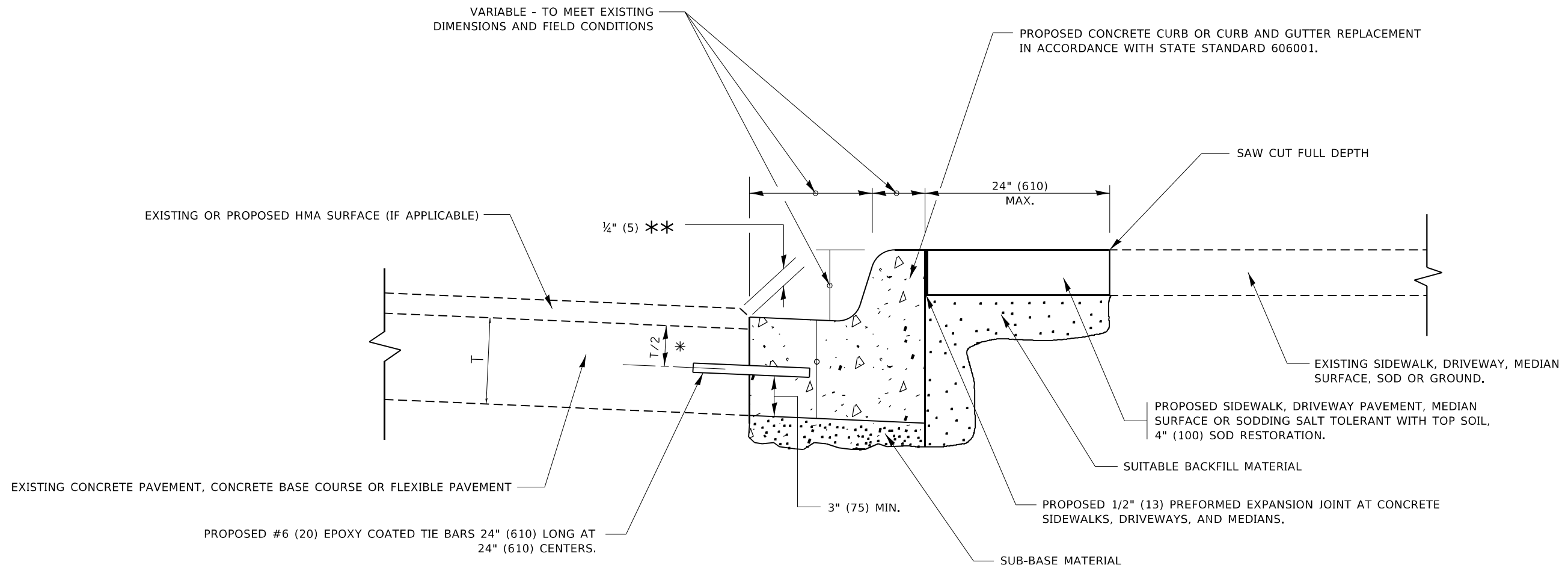
USER NAME = Wendy.Andonayre	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07
	DRAWN -	REVISED - R. BORO 09-04-07
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - K. ENG 10-27-08
PLOT DATE = 12/15/2023	DATE - 10-25-94	REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	48
<b>BD400-04 (BD-22)</b>			CONTRACT NO. 62T59	
ILLINOIS FED. AID PROJECT				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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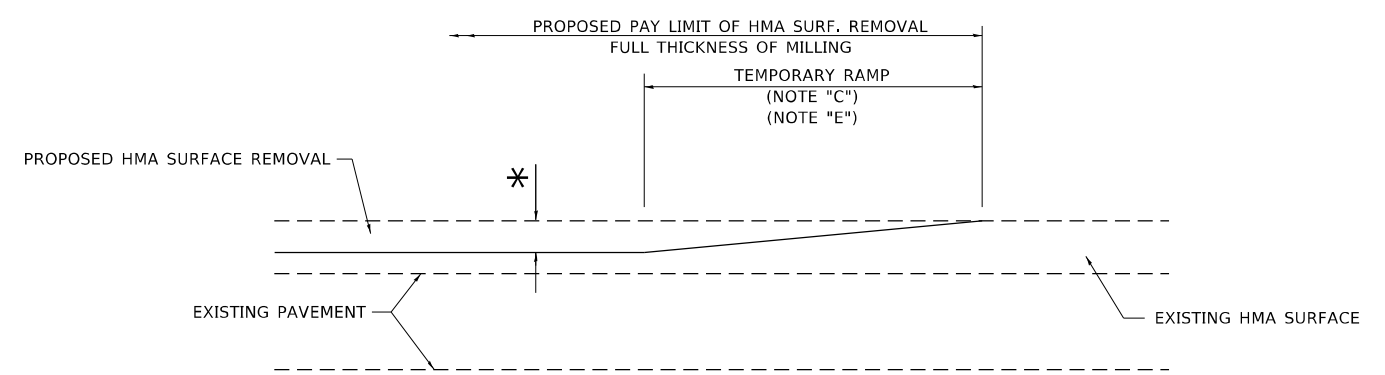
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	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 12/15/2023	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

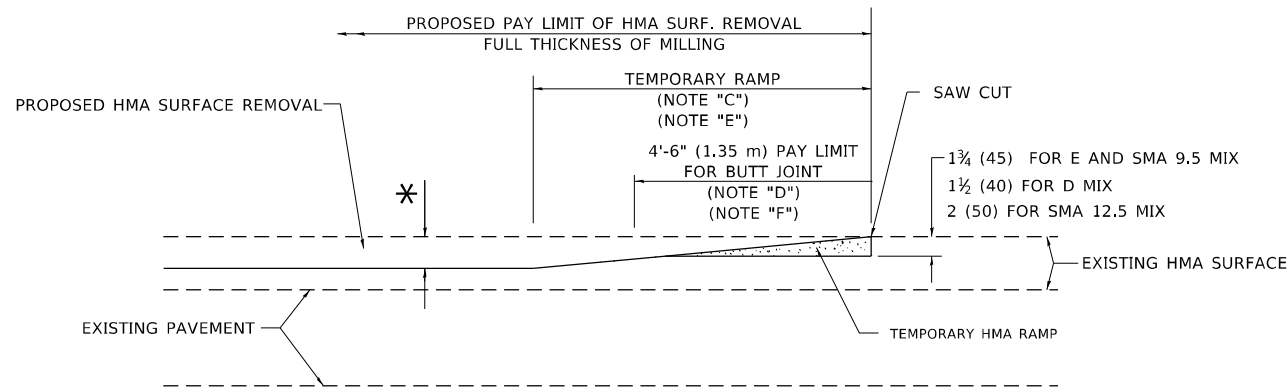
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	49
<b>BD600-06 (BD-24)</b>		CONTRACT NO. 62T59		
ILLINOIS FED. AID PROJECT				



**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

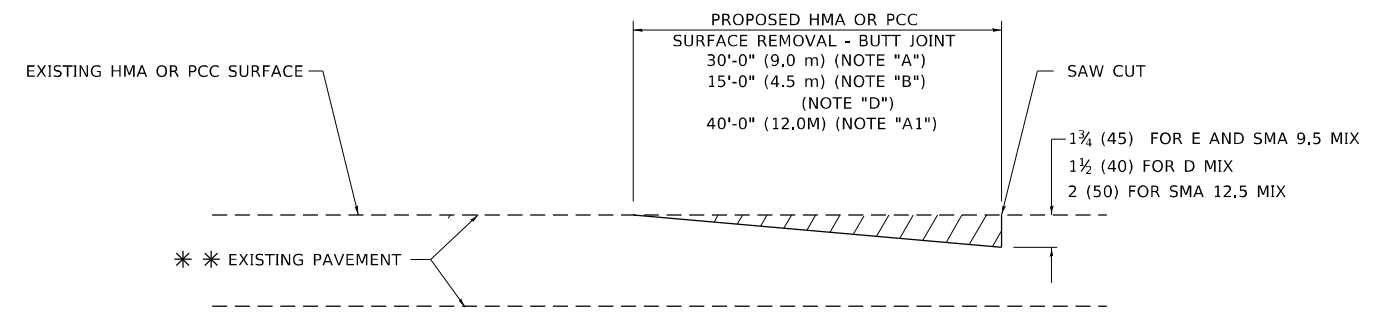


**HMA CONSTRUCTED TEMPORARY RAMP**

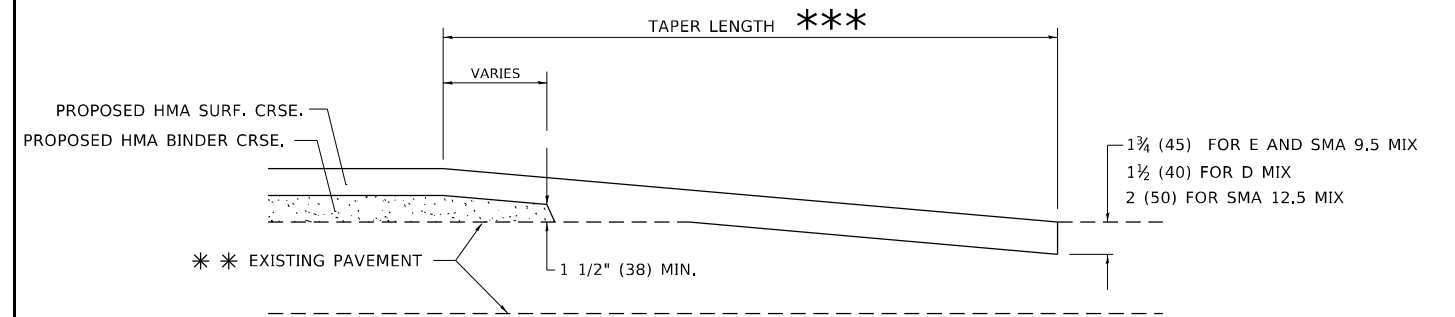
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

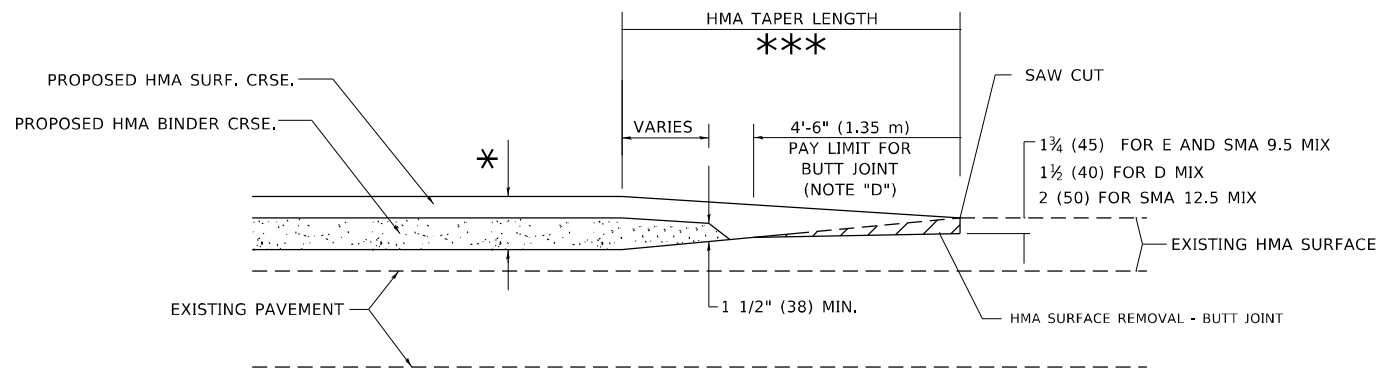
**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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USER NAME = Wendy.Andonayre	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 12/15/2023	DATE - 06-13-90	REVISED - R. BORO 01-01-07
		REVISED - K. SMITH 11-18-22

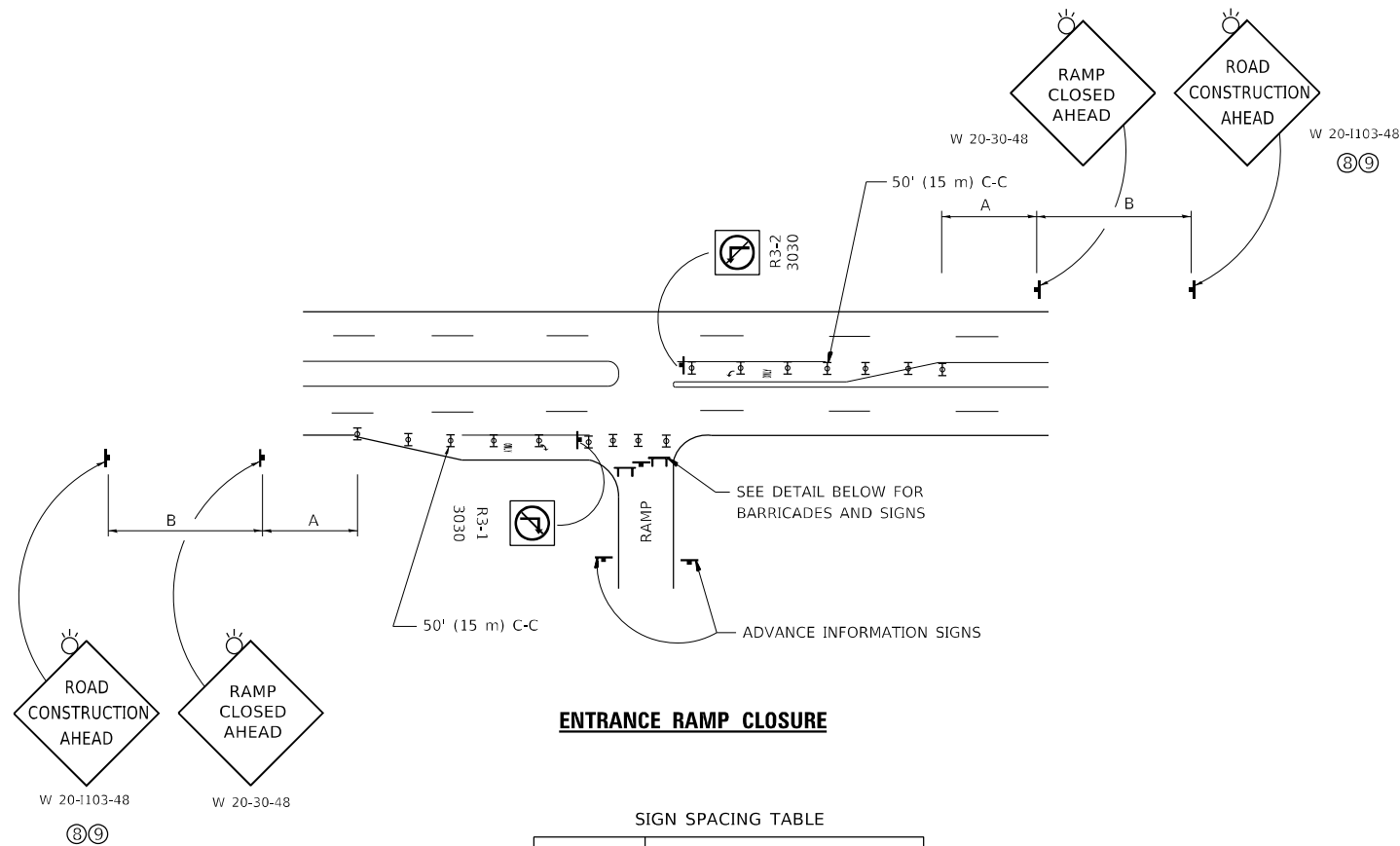
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	50
<b>BD400-05 BD-32</b>		CONTRACT NO. 62T59		
ILLINOIS FED. AID PROJECT				



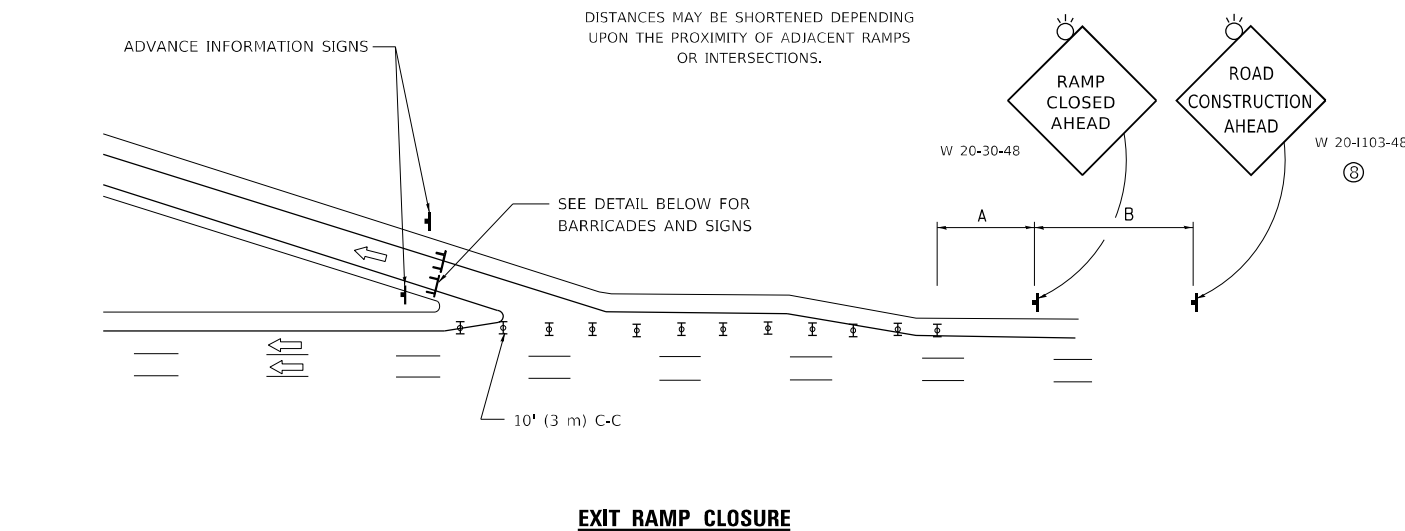


**ENTRANCE RAMP CLOSURE**

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

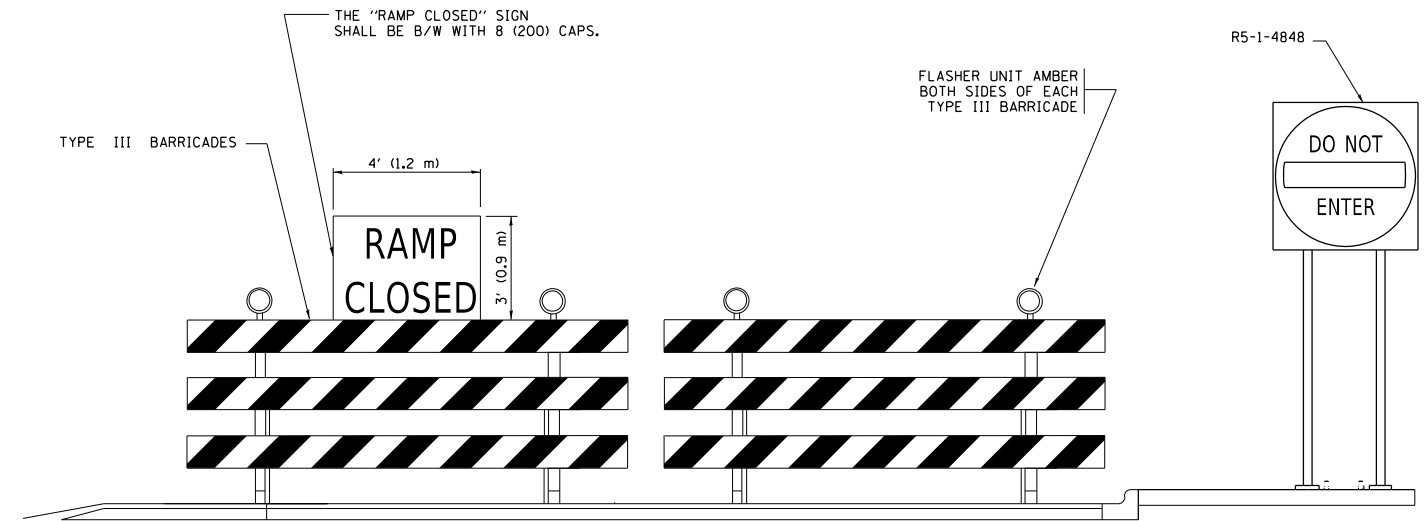
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



**EXIT RAMP CLOSURE**

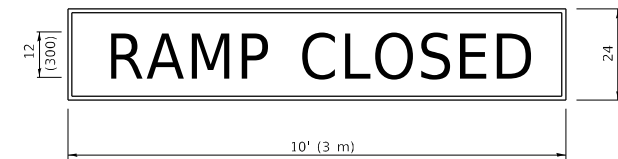
**SYMBOLS**

- ▬ TYPE II BARRICADE OR DRUM
- ▬ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



**DETAIL FOR REQUIRED BARRICADES & SIGNS**

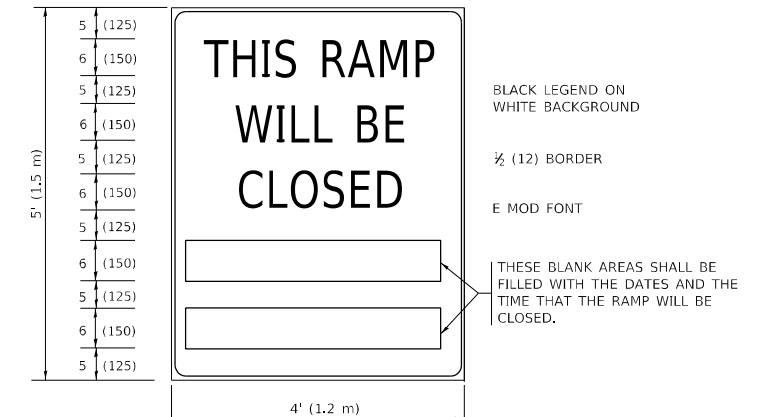
**RAMP CLOSURE ADVANCE WARNING SIGN**



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

**RAMP CLOSURE ADVANCE INFORMATION SIGN**



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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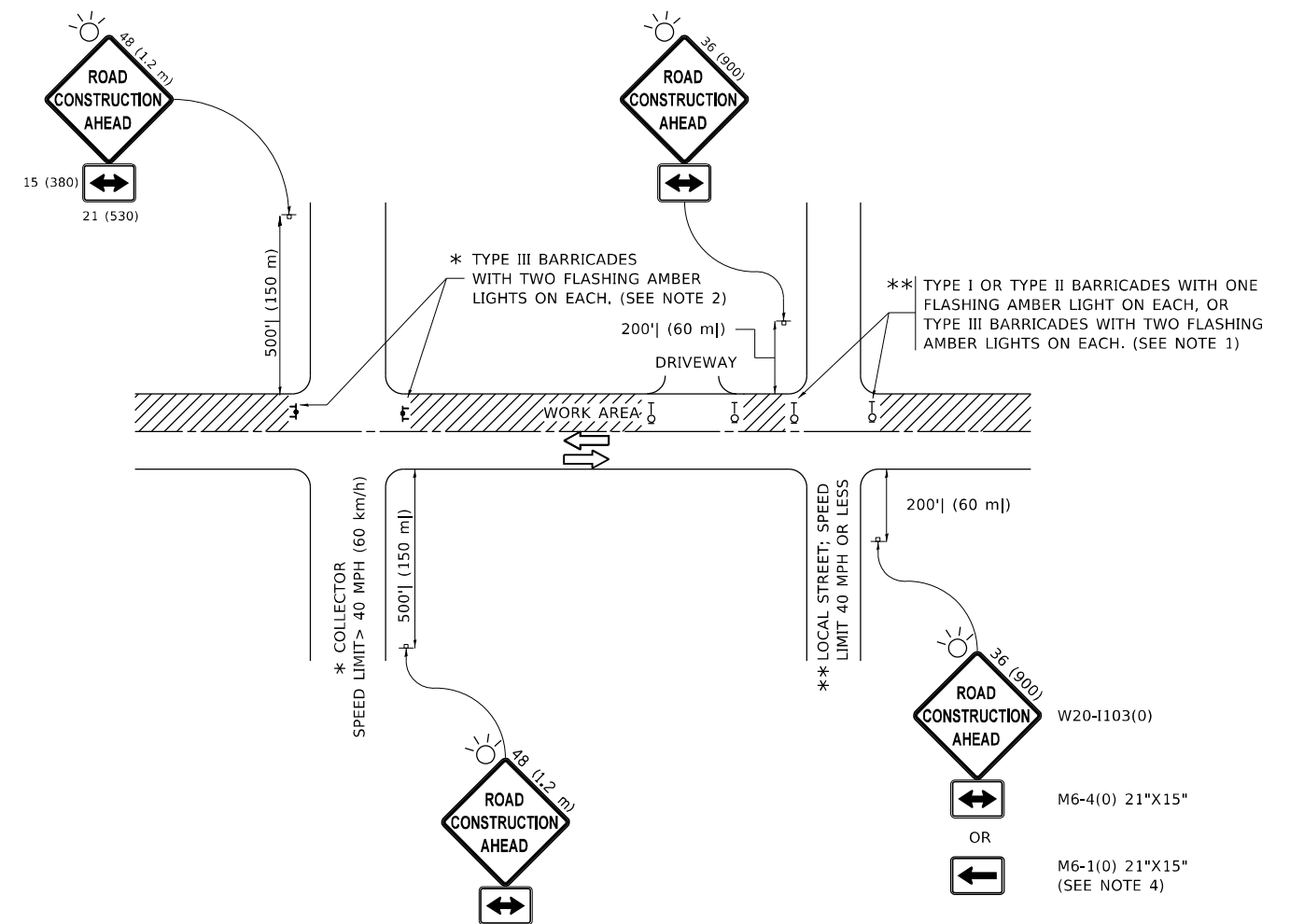
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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - S.P.B._12-09
PLOT DATE = 12/15/2023	DATE - 02-83	REVISED - M.D._06-13
		REVISED - M.D._01-18

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE\_AND\_EXIT\_RAMP  
CLOSURE\_DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHERRY	59	51
<b>TC-08</b>			CONTRACT NO. 62T59	
ILLINOIS		FED. AID PROJECT		



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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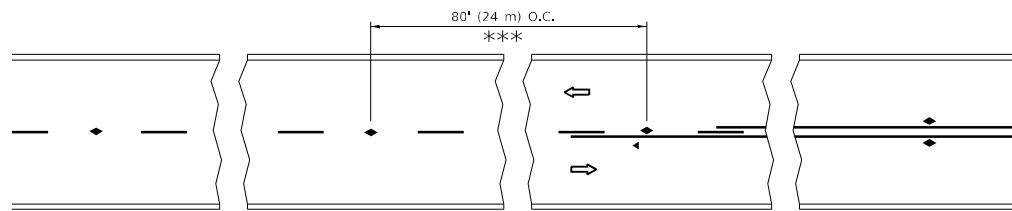
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 12/15/2023	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

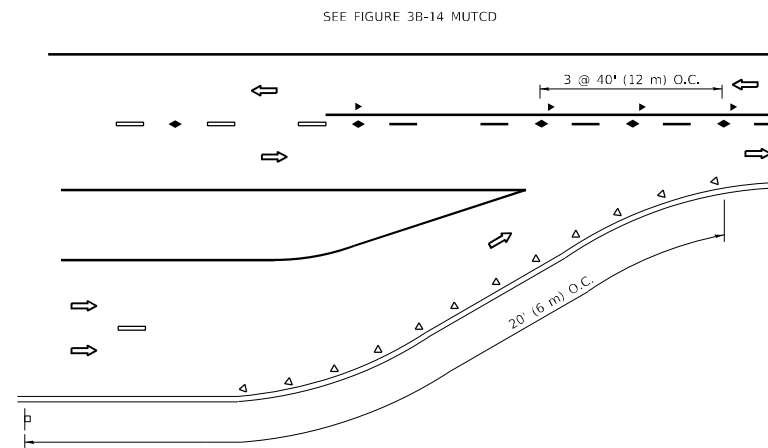
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-10</b>			CONTRACT NO. 62T59	
ILLINOIS FED. AID PROJECT				

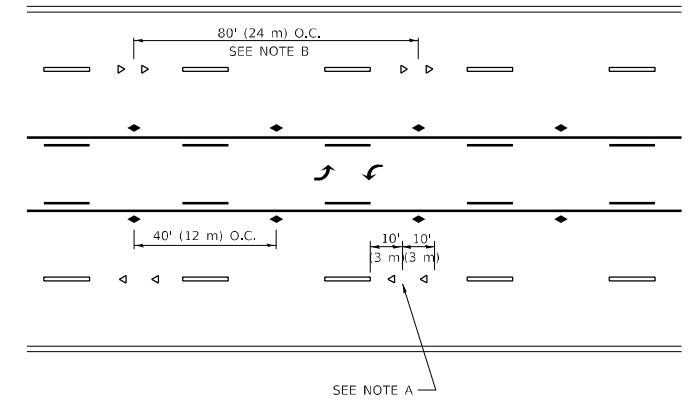


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

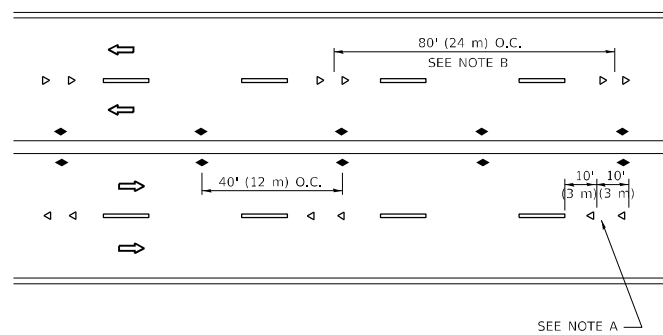
**TWO-LANE/TWO-WAY**



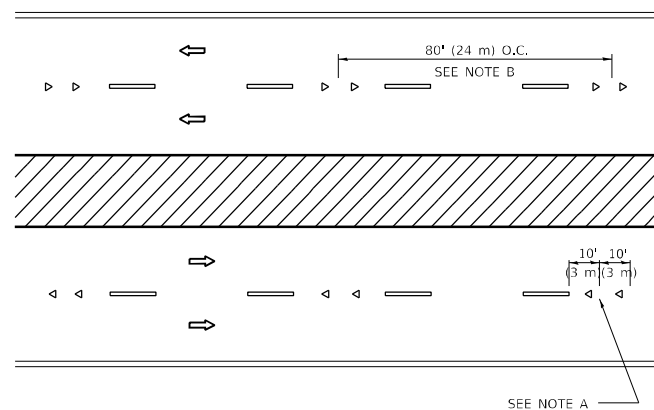
**LANE REDUCTION TRANSITION**



**TWO-WAY LEFT TURN**



**MULTI-LANE/UNDIVIDED**



**MULTI-LANE/DIVIDED**

**GENERAL NOTES**

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

**SYMBOLS**

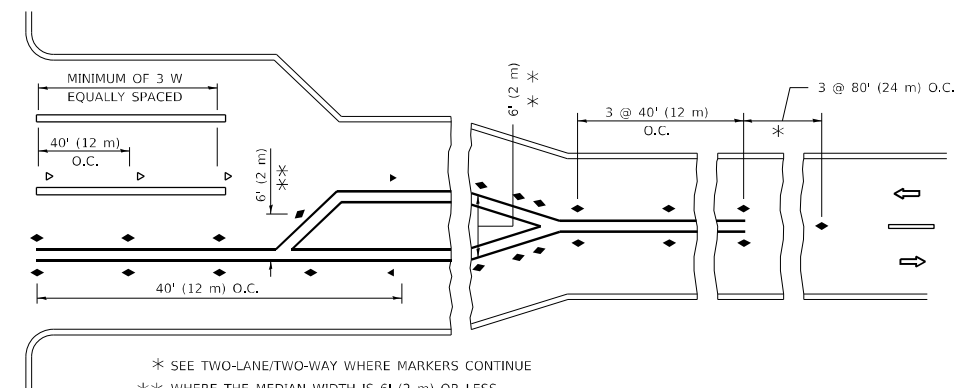
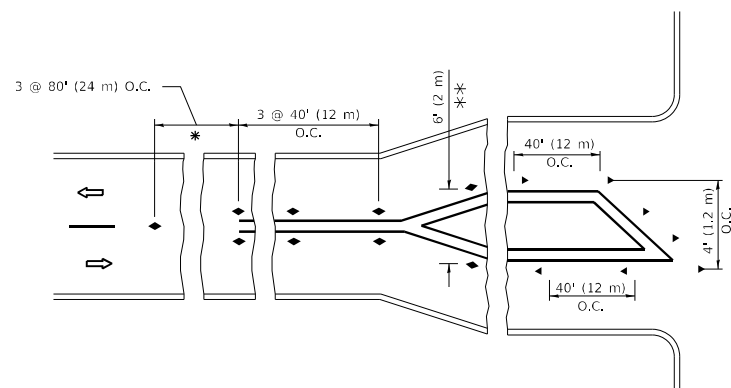
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

**LANE MARKER NOTES**

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

**DESIGN NOTES**

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



**TURN LANES**

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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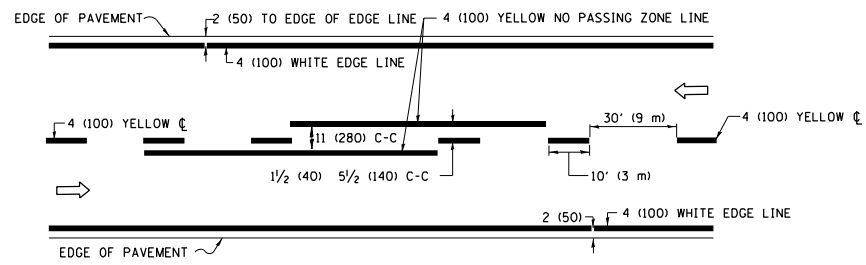
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 12/15/2023	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

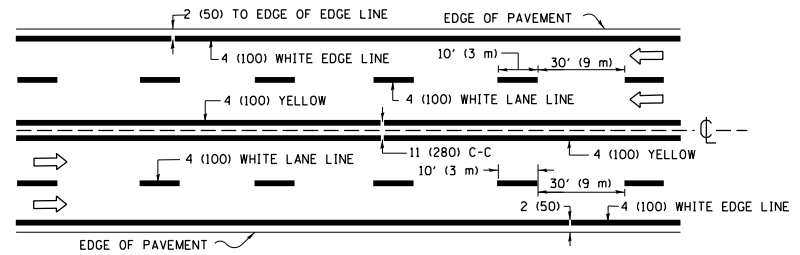
**TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

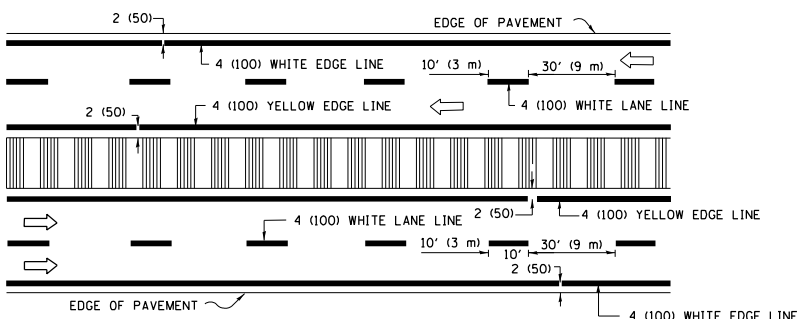
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	53
TC-11		CONTRACT NO. 62T59		
ILLINOIS		FED. AID PROJECT		



**2-LANE ROADWAY**

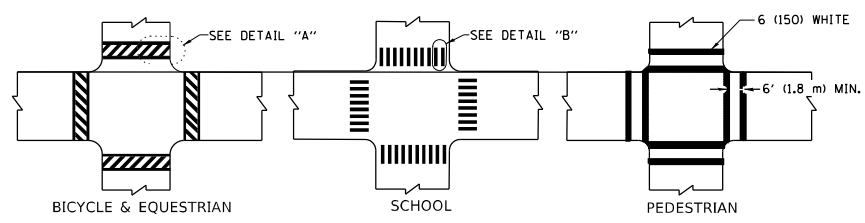


**MULTI-LANE UNDIVIDED**



**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

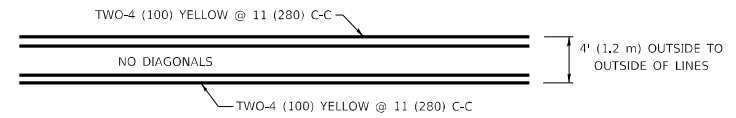


**DETAIL "A"**

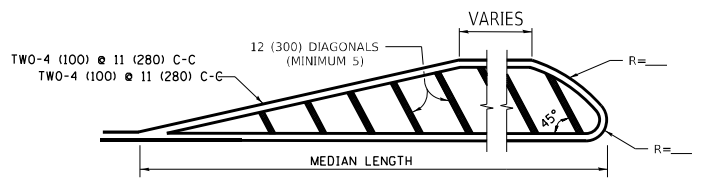
**DETAIL "B"**

**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

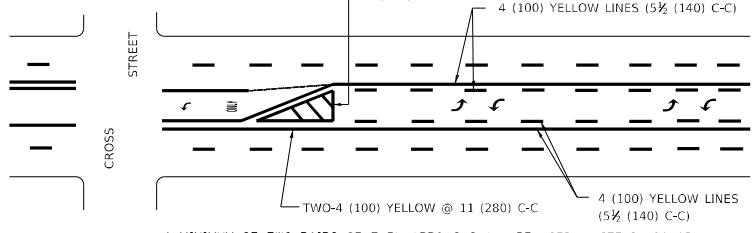


**4' (1.2 m) WIDE MEDIANS ONLY**



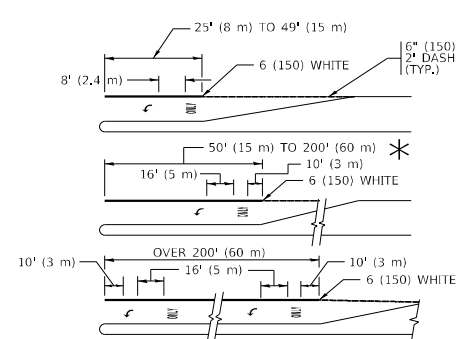
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

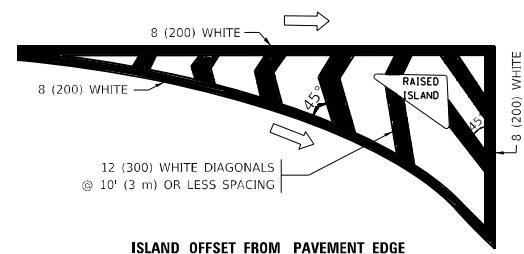
**TYPICAL PAINTED MEDIAN MARKING**



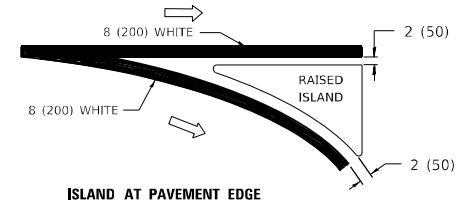
**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

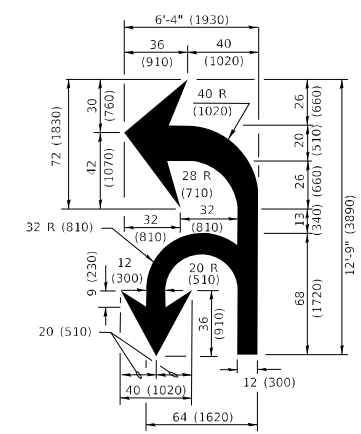


**ISLAND OFFSET FROM PAVEMENT EDGE**

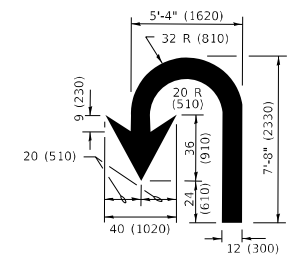


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 12/15/2023	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

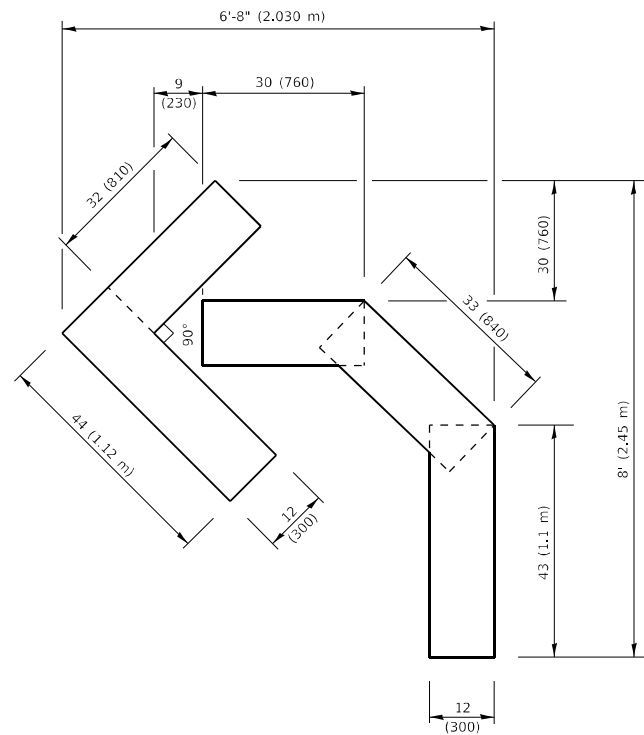
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 2 SHEETS	STA.	TO STA.
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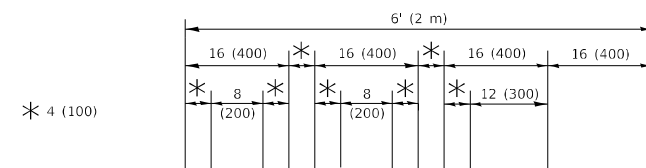
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-13</b>			CONTRACT NO. 62T59	
ILLINOIS FED. AID PROJECT				





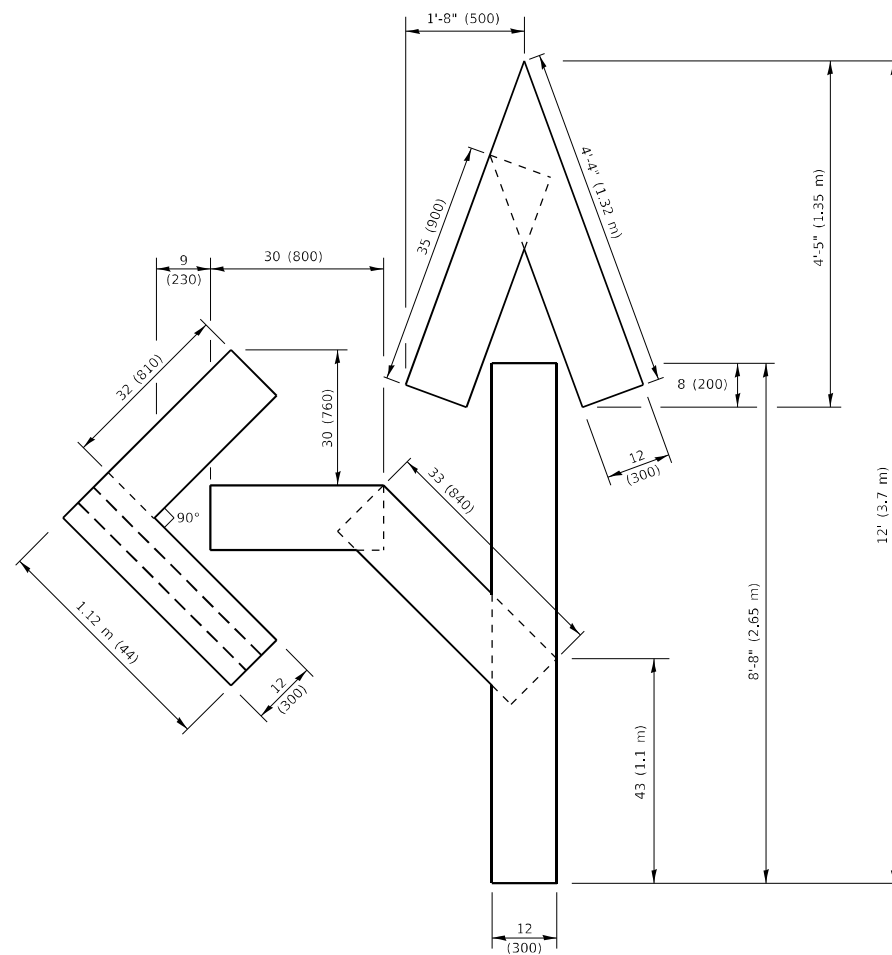
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

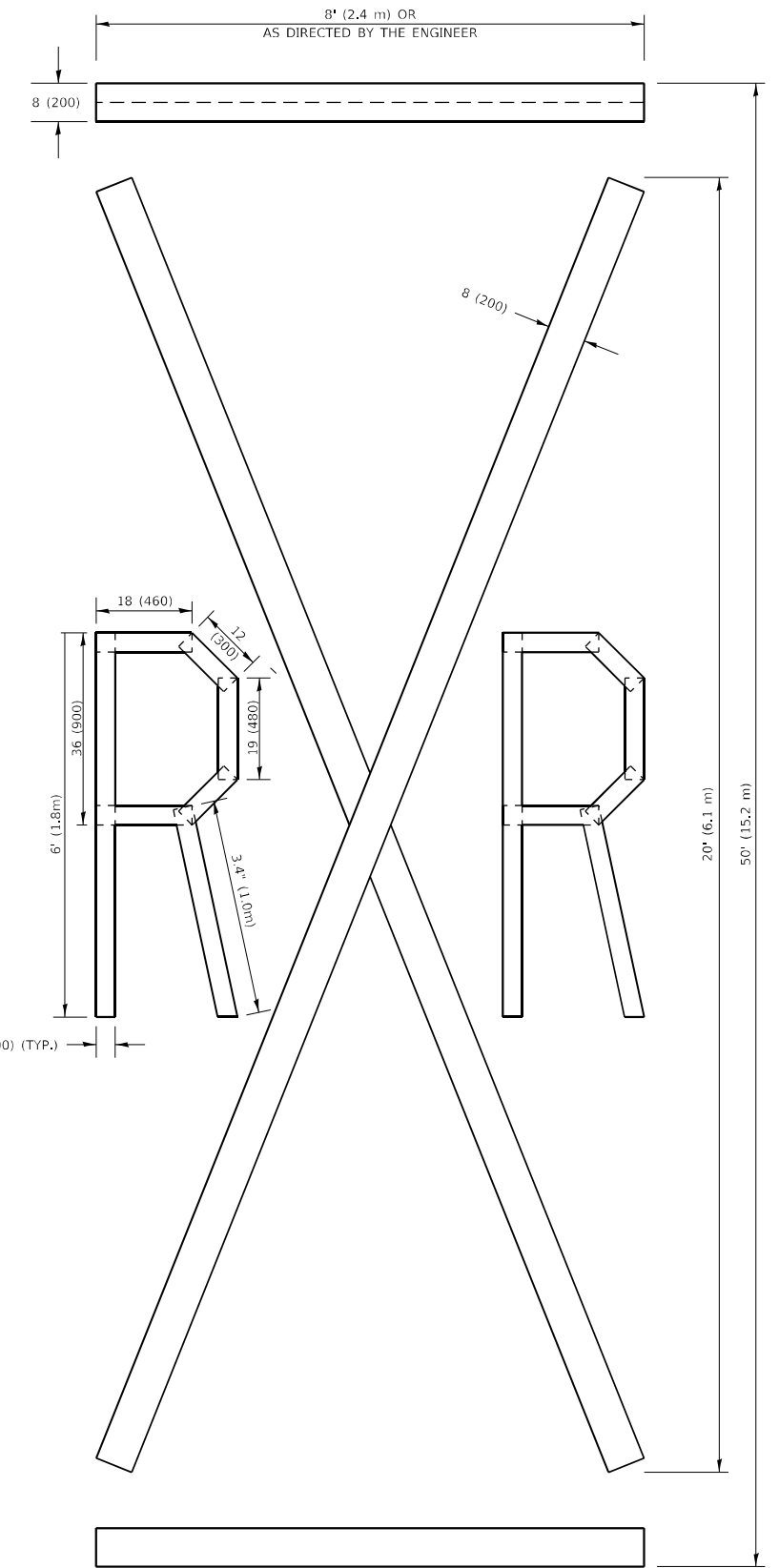


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 100,0000 ' / in.	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 12/15/2023	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

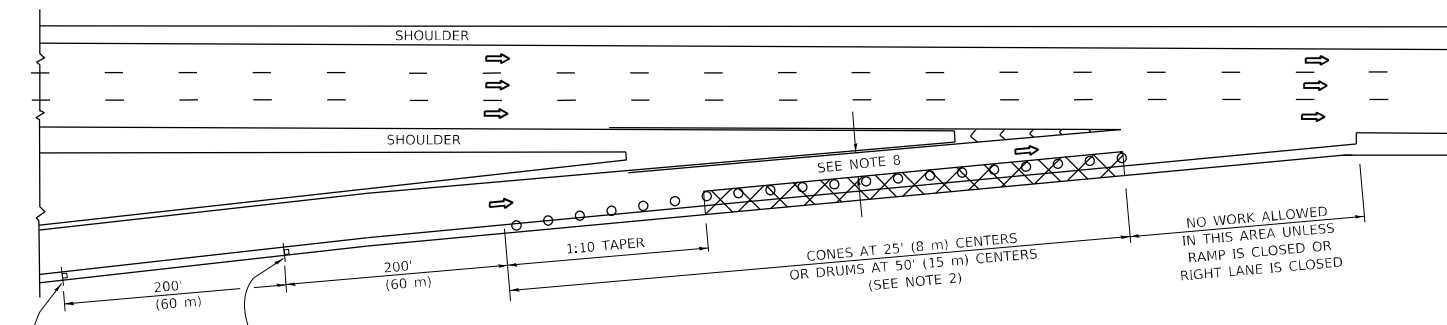
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

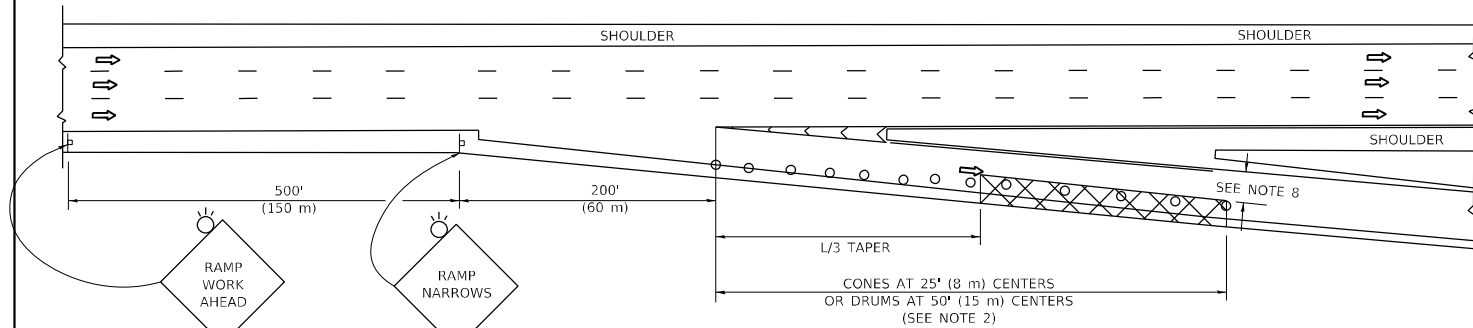
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-16</b>		CONTRACT NO. 62T59		
ILLINOIS FED. AID PROJECT				

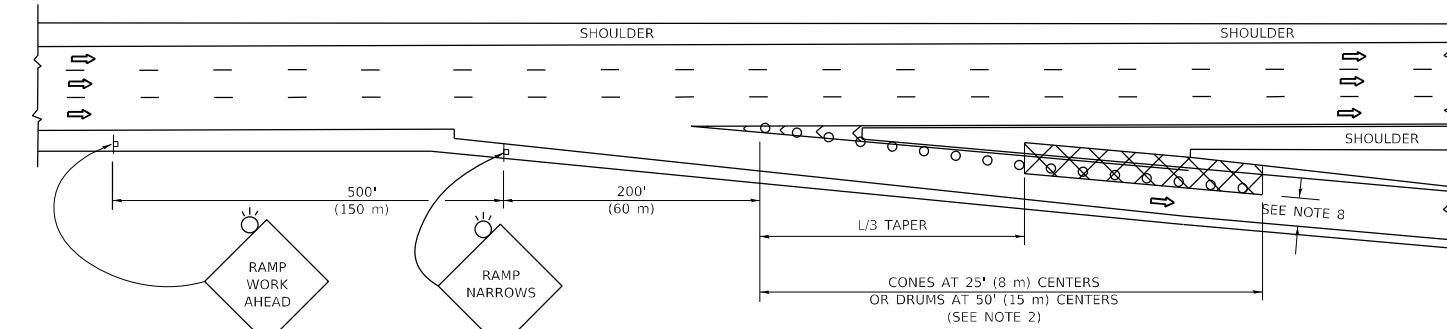
**PARTIAL RAMP CLOSURE DETAILS**



**TYPICAL ENTRANCE RAMP**



**TYPICAL EXIT RAMP**



**TYPICAL EXIT RAMP**

**SYMBOLS**

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

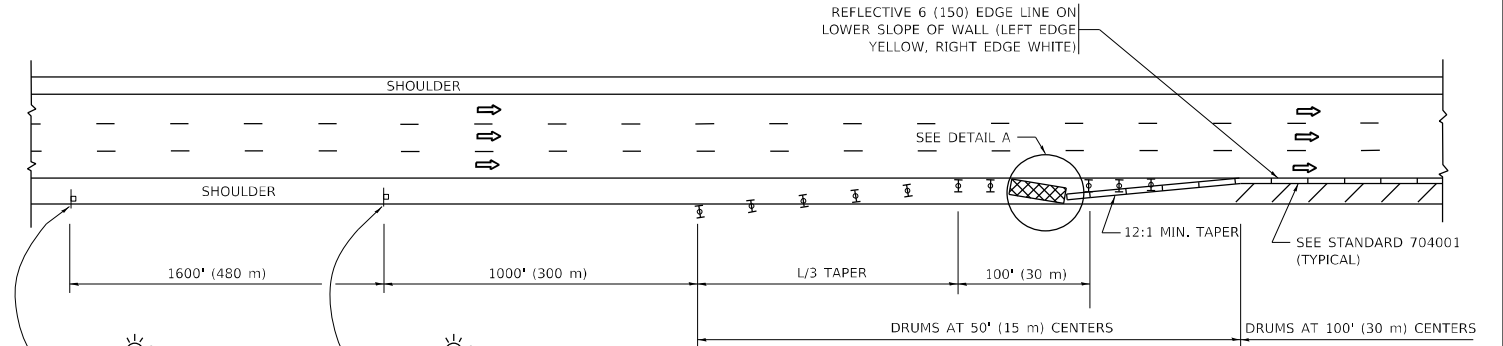
**GENERAL NOTES:**

- THE "L" DISTANCE EQUALS:  

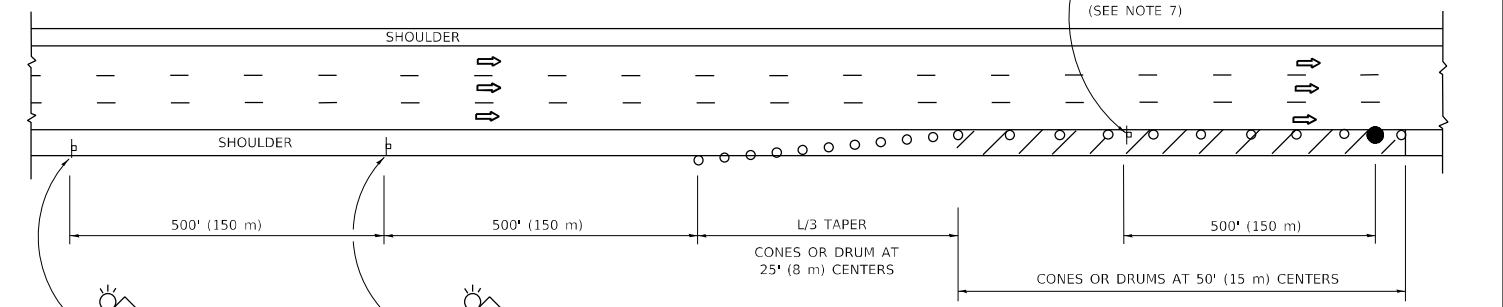
$L = \frac{S^2}{450}$	$L = \frac{S^2}{100}$
45 mph (80 km/h) OR GREATER	METRIC ENGLISH
$L = 0.65(W/S)$	$L = (W/S)$

W = WIDTH OF OFFSET IN FEET (METERS)  
 S = NORMAL POSTED SPEED MPH (KM/H)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

**SHOULDER CLOSURE DETAILS**



**PERMANENT SHOULDER CLOSURE**



**TEMPORARY SHOULDER CLOSURE**

THIS DETAIL IS USED WHERE:  
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCOACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)**

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - THE WORK ACTIVITY REQUIRES FREQUENT ENCOACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
- 12' MIN. WIDTH TANGENT SECTION  
16' MIN. WIDTH CURVE SECTION.
- TEMPORARY SHOULDER AND NON-SYSTEM INTERCHANGE PARTIAL RAMP CLOSURES ARE ALLOWED WEEKDAYS BETWEEN 9:00 A.M. AND 3:00 P.M. AND BETWEEN 7:00 P.M. AND 5:00 A.M. OR AS APPROVED BY THE EXPRESSWAY TRAFFIC OPERATIONS ENGINEER. PERMANENT SHOULDER AND PARTIAL RAMP CLOSURES WILL ONLY BE PERMITTED IF CALLED FOR IN THE PLANS OR AS APPROVED BY THE EXPRESSWAY OPERATIONS ENGINEER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

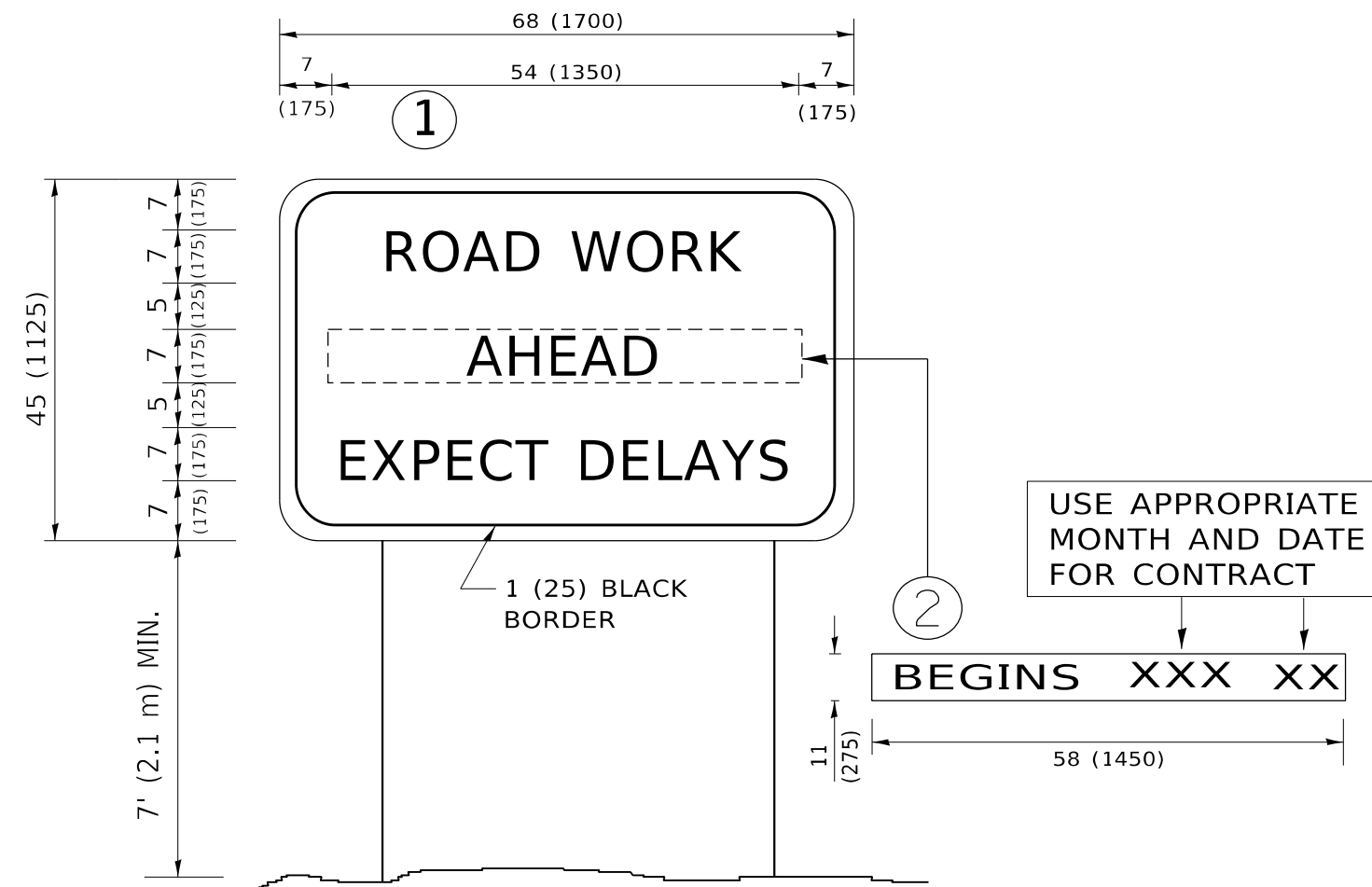
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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED - S.P.B. 12-09
PLOT SCALE = 100,0000' / in.	DRAWN - D.W.S.	REVISED - M.D. 06-13
PLOT DATE = 12/15/2023	CHECKED -	REVISED - M.D. 01-18
	DATE - 11-96	REVISED - M.D. 10-20

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHERRY	59	57
<b>TC-17</b>			CONTRACT NO. 62T59	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Wendy.Andonayre	DESIGNED -	REVISED - R. MIRS 09-15-97
PLOT SCALE = 100,0000 ' / in.	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT DATE = 12/15/2023	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

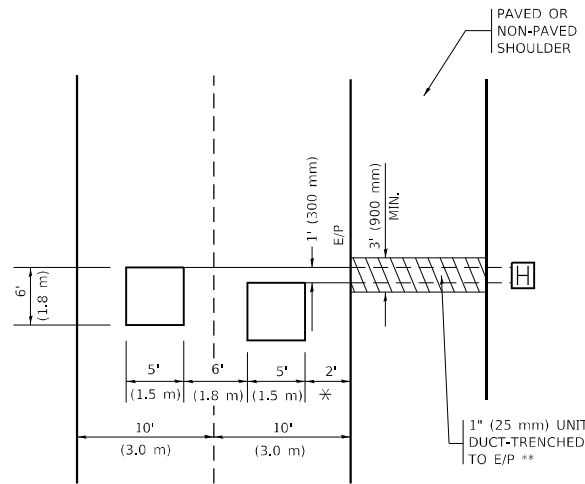
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	58
<b>TC-22</b>			CONTRACT NO. 62T59	
ILLINOIS FED. AID PROJECT				



**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

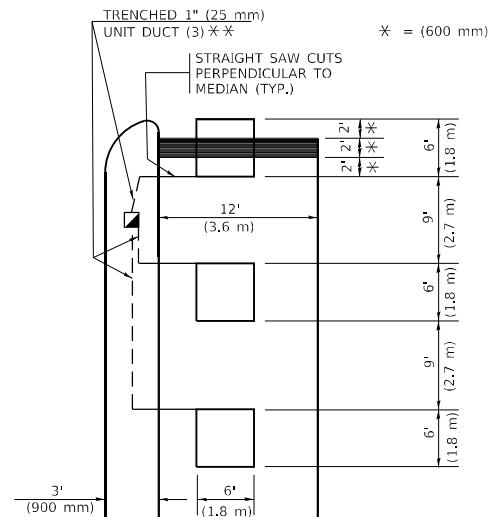


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

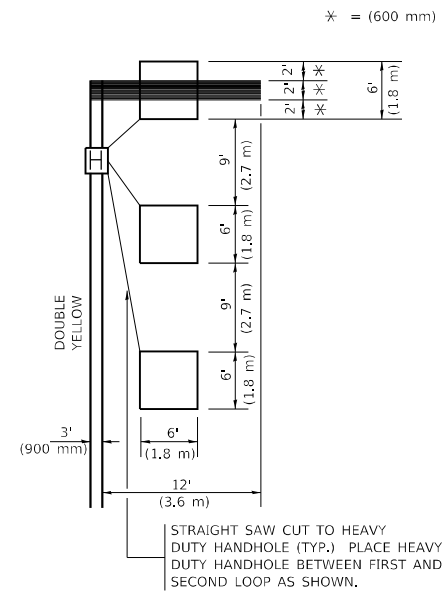


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**



\* = (600 mm)

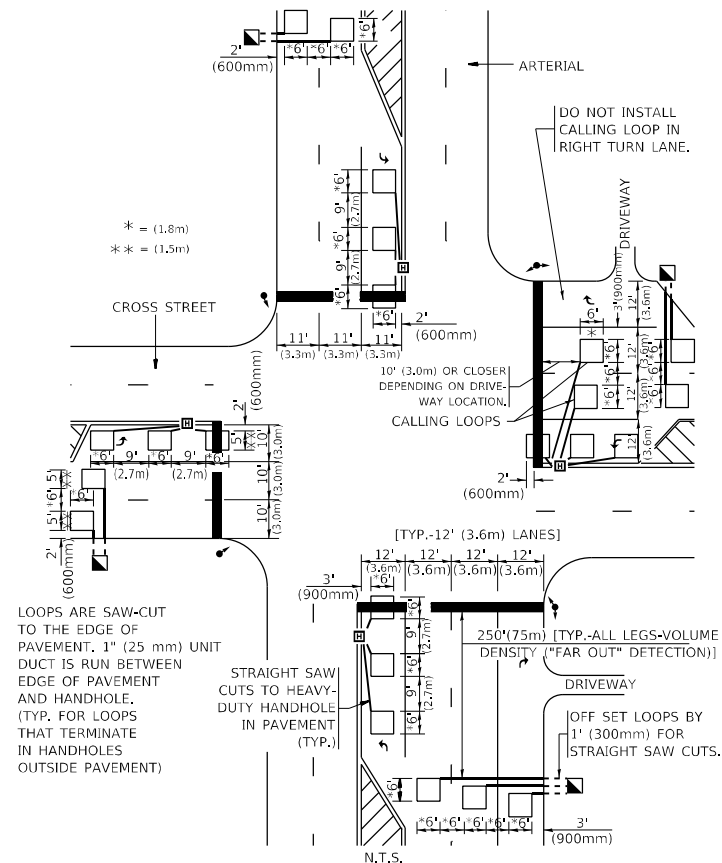
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)**



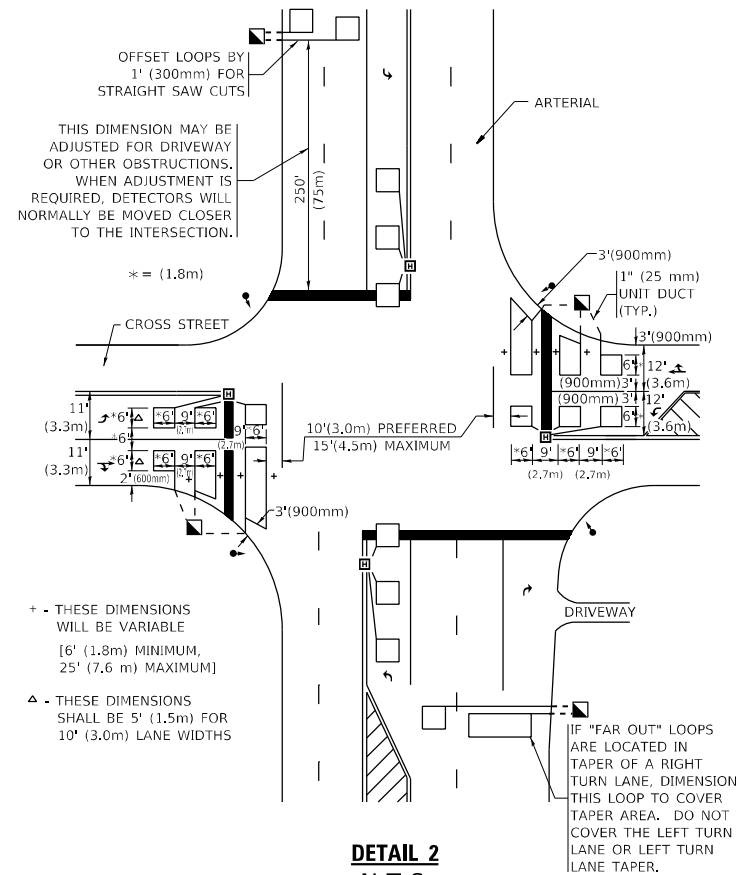
\* = (1.8 m)  
 \*\* = (1.5 m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

**DETAIL 1**  
 N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



OFFSET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS

THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

**DETAIL 2**  
 N.T.S.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/15/2023	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION</b>			
<b>DETAILS FOR ROADWAY RESURFACING</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	FAP 0334 22 RS	LAKE & MCHENRY	59	59
<b>TS-07</b>		CONTRACT NO. 62T59		
ILLINOIS FED. AID PROJECT				