

03-08-2024 LETTING ITEM 135

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

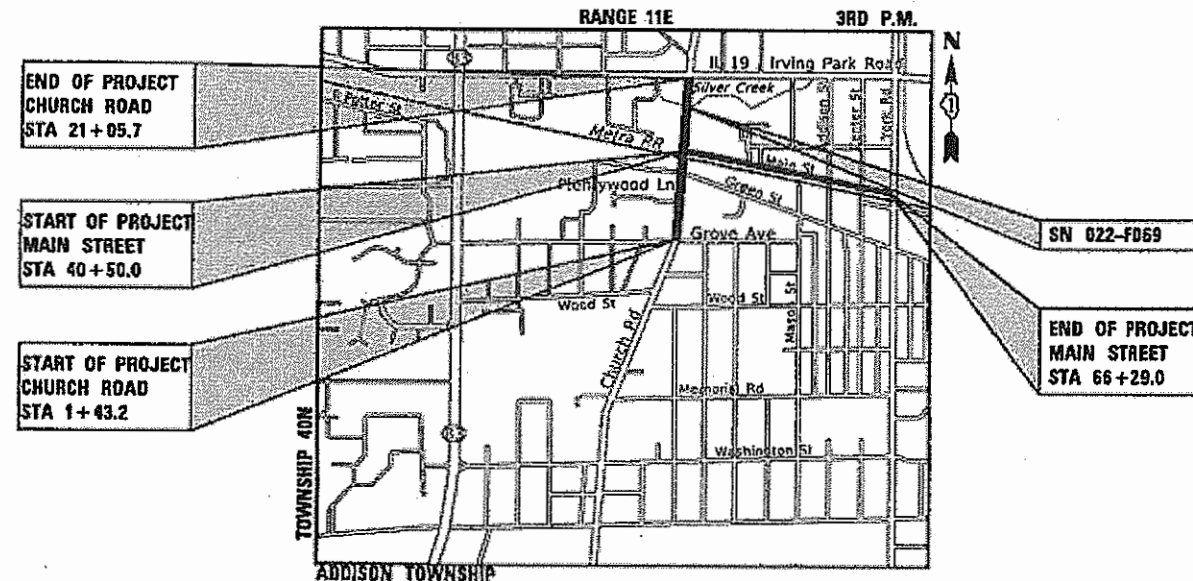
FAU RTE 2667 (CHURCH ROAD)-GROVE AVENUE TO IL ROUTE 19 (IRVING PARK ROAD)
FAU RTE 3001 (MAIN STREET)-CHURCH ROAD TO YORK ROAD
RESURFACING AND SIDEWALK CONSTRUCTION

SECTION 20-00100-00-RS
PROJECT NO. SNR8(969)
VILLAGE OF BENSENVILLE
DUPAGE COUNTY
JOB NO. C-91-093-21

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	1
3001 MAIN				
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 61K22	

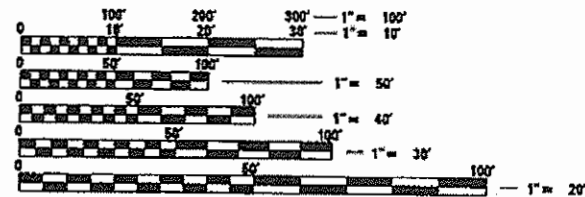


FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR INDEX OF HIGHWAY STANDARDS,
SEE SHEET NO. 2



LOCATION MAP
NOT TO SCALE

DESIGN DESIGNATION = MAJOR URBAN COLLECTOR
POSTED SPEED = 25 MPH
CHURCH ROAD - 2021 ADT = 10,800 VPD
MAIN STREET - 2021 ADT = 2,700 VPD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT LENGTH:

CHURCH ROAD GROSS LENGTH	-	1,962.5 FT (0.372 MILES)
MAIN STREET GROSS LENGTH	-	2,579.0 FT (0.488 MILES)
PROJECT GROSS LENGTH TOTAL	-	4,541.5 FT (0.860 MILES)
CHURCH ROAD RAILROAD OMISSION	-	37.7 FT (0.007 MILES)
CHURCH ROAD NET LENGTH	-	1,924.8 FT (0.365 MILES)
MAIN STREET NET LENGTH	-	2,579.0 FT (0.488 MILES)
PROJECT NET LENGTH TOTAL	-	4,503.8 FT (0.853 MILES)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Approved: *Joseph M. Casacci*
Village of Bensenville, Director of Public Works

Passed: *1-2-2024*
C. F. ...
District One Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: *January 3, 2024*
Jesse ...
Regional Engineer

APPLIES TO SHEETS 1 - 69

REGISTERED P.E., STATE OF ILLINOIS LICENSE 11/01/2025

PLANS PREPARED BY:

Two Pierce Place, Suite 1400 - Itasca, Illinois 60143
Tel: 630.773.3900 - Fax: 630.773.3975
www.civiltechinc.com

CONTRACT NO. 61K22

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FEDERAL AID PROGRAM DESIGN ENGINEER: CARMEN E. RAMOS, P.E., SCHALMBURG, IL

GENERAL

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS:

"STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2022 (REFERRED TO AS THE "STANDARD SPECIFICATIONS")

"SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2024

AND THE CURRENT EDITION OF THE FOLLOWING:
"MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS "

"MANUAL OF TEST PROCEDURES FOR MATERIALS"
"STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".

ANY REFERENCE TO "STANDARDS" THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST I.D.O.T. STANDARD.
- THE THICKNESSES OF HMA MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE COURSE UPON WHICH THE HMA MATERIALS ARE PLACED.
- ALL DIMENSIONS, INCLUDING RADII, ARE GIVEN TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER, THE VILLAGE OF BENSENVILLE (630) 350-3435, UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS, AND SHALL SUBMIT A TRAFFIC CONTROL AUTHORIZATION REQUEST TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION 72 HOURS PRIOR TO THE COMMENCEMENT OF WORK. SEE SHEET 3 FOR CONTACT INFORMATION.
- THE CONTRACTOR SHALL LIMIT ALL DROP- OFFS BETWEEN LANES TO 2" DURING ALL OVERNIGHT PERIODS.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET TO THE SATISFACTION OF THE ENGINEER.
- WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SEEDED/SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS" AND SPECIAL PROVISIONS.
- IN ADDITION TO THE REQUIREMENTS OF THE SPECIAL PROVISION FOR CONSTRUCTION LAYOUT STAKES (ILLINOIS DEPARTMENT OF TRANSPORTATION CHECK SHEET #10), THE CONTRACTOR SHALL REESTABLISH, MONUMENT, AND TIE ALL CONTROL POINTS USED TO COMPLETE THE WORK AS SPECIFIED INCLUDING ALL P'S, PC'S, PT'S, AND POT'S AFTER FINAL PAVING. THE TYPE OF MONUMENTATION USED WILL BE PK NAILS, IRON PIPES, RR SPIKES OR AS APPROVED BY THE ENGINEER.
- WHEN REMOVING CURB AND GUTTER, PAVEMENT, OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE PRECAUTIONS NECESSARY TO AVOID DAMAGE TO UNDERGROUND PUBLIC OR PRIVATE UTILITIES. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE ALLOWED.
- CONCRETE TRUCK WASHOUT FACILITY SHOULD BE PROVIDED IN THE FIELD BY THE CONTRACTOR AT A LOCATION OR LOCATIONS APPROVED BY THE ENGINEER IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS" AND SPECIAL PROVISIONS. WASHING CONCRETE TRUCKS AT CURB AND GUTTER IS STRICTLY PROHIBITED.
- CURB AND GUTTER JOINTS SHALL BE PLACED AS PER STANDARD 606001 AND IN ADDITION ONE INCH EXPANSION JOINTS SHALL BE PLACED EVERY 150 FT.
- THOSE SEEKING THE FULL GEOTECHNICAL REPORT SHOULD CONTACT THE OWNER OF RECORD. CONTACT THE VILLAGE OF BENSENVILLE ENGINEER, JEFF MACZKO, AT 630.594.1196 TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AND PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIME DURING CONSTRUCTION.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

STORM SEWERS, STRUCTURES, AND UTILITIES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND APPLICABLE ARTICLES INCLUDED IN THE "STANDARD SPECIFICATIONS" INCLUDING, BUT NOT LIMITED TO, ARTICLES 105.07 AND 107.31. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.
- ONLY PRECAST CONCRETE ADJUSTMENT RINGS, MAXIMUM OF 12" IN HEIGHT, WILL BE ALLOWED IN THE ADJUSTMENT OR RECONSTRUCTION OF CATCH BASIN, MANHOLE, INLET AND VALVE VAULT STRUCTURES, UNLESS INDICATED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE ENGINEER. COMMON BRICK WILL NOT BE ALLOWED. ALL TYPE 8 GRATES ON RESTRICTED DEPTH DRAINAGE STRUCTURES SHALL BE ADJUSTED TO PLAN GRADE WITH 4" MINIMUM CONCRETE ADJUSTMENT RINGS.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR STRUCTURES. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS, AND DISCHARGE THE SAME. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. THE CONTRACTOR SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE.

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VILLAGE OF BENSENVILLE CONSTRUCTION DETAILS

GENERAL MAIL BOX
SILT FENCE INSTALLATION STANDARD
CURB AND GUTTER STANDARD
DRIVEWAY STANDARD

IDOT DISTRICT ONE STANDARDS

BD-08	FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTION AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-23	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-05	STANDARD TRAFFIC SIGNAL DESIGN DETAILS
TS-07	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

IDOT HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5M) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-09	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
729001-01	APPLICATIONS OF TYPES A&B METAL POSTS (FOR SIGNS & MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS

PLAN	SURVEYED	DATE
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	CHECKED	
	BY	
	DATE	
	NO. OF WAY CHECKED	
	ADJ. FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	CHECKED	
	BY	
	DATE	
	STRUCTURE NOTATIONS CHECKED	



Two Pierce Place, Suite 1400
Itasca, Illinois 60143
Tel: 630.773.3900 Fax: 630.773.3975
www.civiltechinc.com

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DATE - 12/26/2023

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REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHURCH ROAD AND MAIN STREET RESURFACING
GENERAL NOTES AND INDEX

SHEET NO. 1 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	2
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING, STRIPING, AND LANDSCAPING

20. ALL WORK INVOLVING SIGNS SHALL BE GOVERNED BY THE FOLLOWING REQUIREMENTS:
- A) SIGNS SHALL NOT BE MOVED UNTIL PROGRESS OF WORK NECESSITATES IT.
 - B) EVERY SIGN TO BE RELOCATED MUST BE RE-ERECTED AT A TEMPORARY LOCATION AND BE VISIBLE TO TRAFFIC FOR WHICH IT IS INTENDED. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING.
 - C) ALL SIGNS TO BE RELOCATED SHALL BE RE-ERECTED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. HORIZONTAL LOCATION FROM THE EDGE OF PAVEMENT SHALL BE AS DESIGNATED BY THE ENGINEER.
 - D) ALL UNUSED SIGNS SHALL BE RETURNED TO THEIR OWNER (VILLAGE OF BENSENVILLE).
 - E) LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS.
21. ALL EXISTING SIGNS SHALL CONFORM TO THE CURRENT EDITION OF "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS." THOSE EXISTING SIGNS THAT DO NOT CONFORM SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
22. ITEMS UNDER THE GENERAL HEADING OF "THERMOPLASTIC PAVEMENT MARKING" PROVIDE FOR ONE APPLICATION.
23. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN, INCLUDING CONCRETE TRUCK WASHOUT FACILITIES. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR.
24. PER THE ILLINOIS LAWN CARE PRODUCTS APPLICATION NOTICE ACT 096-1005, PHOSPHORUS FERTILIZER NUTRIENT SHALL NOT BE USED.

RAILROAD COORDINATION

25. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH METRA RAILROAD (THE COMMUTER RAIL DIVISION OF THE REGIONAL TRANSPORTATION AUTHORITY) WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE METRA RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID ACCORDING TO ARTICLE 107.12 AND REIMBURSED ACCORDING TO ARTICLE 109.05. THE CONTRACTOR MUST CONTACT MILWAUKEE DISTRICT DIRECTOR OF ENGINEERING, AT (312) 322-4101 AT LEAST 72 HOURS PRIOR TO WORK START UP TO ARRANGE FOR RAILROAD FLAGGING, PROTECTION, ETC.

COMMITMENTS

1. TREES THREE (3) INCHES OR GREATER IN DIAMETER AT BREAST HEIGHT SHALL NOT BE CLEARED FROM APRIL 1ST THROUGH SEPTEMBER 30TH OF ANY GIVEN YEAR IN ORDER TO CONSERVE THE THREATENED AND ENDANGERED NORTHERN LONG EARED BAT (NLEB).

POINTS OF CONTACT

VILLAGE OF BENSENVILLE
 JOSEPH CARACCI,
 DIRECTOR OF PUBLIC WORKS
 717 E. JEFFERSON ST.
 BENSENVILLE, IL 60106
 jcaracci@bensenville.il.us
 (630) 350-3435

METRA
 DON WHISTLER
 RIGHT OF WAY ADMINISTRATOR
 547 WEST JACKSON BOULEVARD
 CHICAGO, IL 60661
 dwhistler@metrarr.com
 (312) 322-8016

VILLAGE OF BENSENVILLE
 JEFF MACZKO
 VILLAGE ENGINEER
 717 E. JEFFERSON ST.
 BENSENVILLE, IL 60106
 jmaczko@bensenville.il.us
 (630) 594-1196

BENSENVILLE SCHOOL DISTRICT #2
 210 S. CHURCH ROAD
 BENSENVILLE, IL 60106
 (630) 766-5940

VILLAGE OF BENSENVILLE
 POLICE DEPARTMENT
 345 E. GREEN STREET
 BENSENVILLE, IL 60106
 (630) 350-3455

VILLAGE OF BENSENVILLE
 FIRE DEPARTMENT
 500 S. YORK ROAD
 BENSENVILLE, IL 60106
 (630) 350-3441

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CHURCH ROAD AND MAIN STREET RESURFACING
 GENERAL NOTES**

SHEET NO. 2 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	3
3001 MAIN	CONTRACT NO. 61K22			

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

PLANNED	DATE
NOTED	BY
ALIGNED	CHECKED
NOTE BOOK	NO.
JOB	

PROFILE	DATE
NOTED	BY
GRADES	CHECKED
NOTE BOOK	NO.
JOB	

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
					STU FUNDS			100% Village
					70% Federal	30% Village		
					0005 Roadway	0021 Signals	0042 Trainees	
	20101000	TEMPORARY FENCE	FOOT	1400	1400			
	20101100	TREE TRUNK PROTECTION	EACH	35	35			
X	20101200	TREE ROOT PRUNING	EACH	35	35			
X	20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	28	28			
X	20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	7	7			
	20200100	EARTH EXCAVATION	CU YD	60	60			
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	30	30			
	20400800	FURNISHED EXCAVATION	CU YD	40	40			
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	649	649			
	21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	90	90			
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	50	50			
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	11	11			
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	11	11			
	25200110	SODDING, SALT TOLERANT	SQ YD	786	786			
	25200200	SUPPLEMENTAL WATERING	UNIT	4	4			



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHURCH ROAD AND MAIN STREET RESURFACING
SUMMARY OF QUANTITIES

SHEET NO. 1 OF 7 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	4
3001 MAIN		CONTRACT NO. 61K22		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	DATE
NO. _____	BY _____
NOTE BOOK	DESIGNED
NO. _____	CHECKED
	PLOTTED
	DATE

PROFILE	DATE
NO. _____	BY _____
NOTE BOOK	DESIGNED
NO. _____	CHECKED
	PLOTTED
	DATE

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
					STU FUNDS 70% Federal 30% Village			100% Village
					0005	0021	0042	Non- Participating
					Roadway	Signals	Trainees	
	28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	18	18			
	28000305	TEMPORARY DITCH CHECKS	FOOT	48	48			
	28000400	PERIMETER EROSION BARRIER	FOOT	355	355			
	28000510	INLET FILTERS	EACH	55	55			
	28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	788	788			
	35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	1056	1056			
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	15117	15117			
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	3558	3558			
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	23	23			
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	183	183			
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	624	624			
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	861	861			
	40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	797	797			
	42001300	PROTECTIVE COAT	SQ YD	1106	1106			
	42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	175	175			

CIVILTECH
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CHURCH ROAD AND MAIN STREET RESURFACING
 SUMMARY OF QUANTITIES**

SHEET NO. 2 OF 7 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	5
3001 MAIN				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

DATE	
BY	
APPROVED	
ALIGNMENT CHECKED	
PT. OF WAY CHECKED	
NO. _____	
PLAN	
NOTE BOOK	
NO. _____	

DATE	
BY	
APPROVED	
GRADES CHECKED	
STRUCTURE NOTE THE EXPD	
NO. _____	
PROFILE	
NOTE BOOK	
NO. _____	

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
					STU FUNDS 70% Federal 30% Village			100% Village Non- Participating
					0005	0021	0042	
					Roadway	Signals	Trainees	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	6061	6061			
	42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	580	580			
	42400800	DETECTABLE WARNINGS	SQ FT	122	122			
	44000100	PAVEMENT REMOVAL	SQ YD	99	99			
	44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	14792	14792			
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	218	218			
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1780	1780			
	44000600	SIDEWALK REMOVAL	SQ FT	2373	2373			
	44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	178	178			
	44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	178	178			
	44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	178	178			
	44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	178	178			
	44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	192	192			
	44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	192	192			
	44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	192	192			

CIVILTECH
Two Pierce Place, Suite 1400
Itasca, Illinois 60143
Tel: 630.773.3900 Fax: 630.773.3975
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DRAWN - JRR	REVISED -
CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHURCH ROAD AND MAIN STREET RESURFACING
SUMMARY OF QUANTITIES

SHEET NO. 3 OF 7 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	6
3001 MAIN				CONTRACT NO. 61K22
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
NOTE BOOK	ALIGNED		
NO.	CHECKED		
	BY		
	DATE		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	GRADES		
NO.	NOTED		
	CHECKED		
	BY		
	DATE		

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
					STU FUNDS 70% Federal 30% Village			100% Village
					0005	0021	0042	Non- Participating
					Roadway	Signals	Trainees	
	44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	192	192			
X	56400300	FIRE HYDRANTS TO BE ADJUSTED	EACH	1	1			
	60265700	VALVE VAULTS TO BE ADJUSTED	EACH	4	4			
	60603900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (ABUTTING EXISTING PAVEMENT)	FOOT	1780	1780			
X	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	12	12			
X	66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2			
X	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1			
X	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1			
X	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	15	15			
	67100100	MOBILIZATION	L SUM	1	1			
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1			
	70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1			
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1			
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1			
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	196	196			



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DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHURCH ROAD AND MAIN STREET RESURFACING
SUMMARY OF QUANTITIES

SHEET NO. 4 OF 7 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH 3001 MAIN	20-00100-00-R5	DUPAGE	71	7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61K22	

PLAN	DATE
NO. _____	BY _____
NOTE BOOK	CHECKED _____
NO. _____	DATE _____

PROFILE	DATE
NO. _____	BY _____
NOTE BOOK	CHECKED _____
NO. _____	DATE _____

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
					STU FUNDS 70% Federal 30% Village			100% Village
					0005	0021	0042	
					Roadway	Signals	Trainees	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2645	2645			
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	873	873			
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	402	402			
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	9227	9227			
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	1103	1103			
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	240	240			
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	250	250			
X	72000100	SIGN PANEL - TYPE 1	SQ FT	41	41			
	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	3	3			
	72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1			
X	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	1	1			
X	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	35	35			
X	73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	2	2			
X	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	402	402			
X	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9227	9227			



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DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHURCH ROAD AND MAIN STREET RESURFACING
SUMMARY OF QUANTITIES

SHEET NO. 5 OF 7 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	8
3001 MAIN			CONTRACT NO. 61K22	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	REVISIONS	DATE
NOTE BOOK NO.	BY	
NO.		
	REQUESTED	
	PLOTTED	
	ALIGNED	
	CHECKED	
	DATE	
	NAME	

PROFILE	REVISIONS	DATE
NOTE BOOK NO.	BY	
NO.		
	REQUESTED	
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE ADJUSTED	
	DATE	
	NAME	

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
					STU FUNDS 70% Federal 30% Village			100% Village
					0005	0021	0042	
					Roadway	Signals	Trainees	Non-Participating
X	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1103	1103			
X	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	240	240			
X	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	250	250			
	K0036120	MULCH PLACEMENT 4"	SQ YD	114	114			
	X0327552	TREE GRATE REMOVAL	EACH	4	4			
	X1700112	BRICK PAVER REMOVAL	SQ FT	2154	2154			
	X2010400	STUMP REMOVAL ONLY	UNIT	80	80			
	X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	100	100			
	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	2	2			
	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	4	4			
	X4023000	TEMPORARY ACCESS (ROAD)	EACH	1	1			
	X4405030	LONGITUDINAL PARTIAL DEPTH REMOVAL 3"	FOOT	2230	2230			
	X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	125	125			
	X6030205	FRAMES AND GRATES TO BE ADJUSTED (SPECIAL)	EACH	2	2			
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	47	47			



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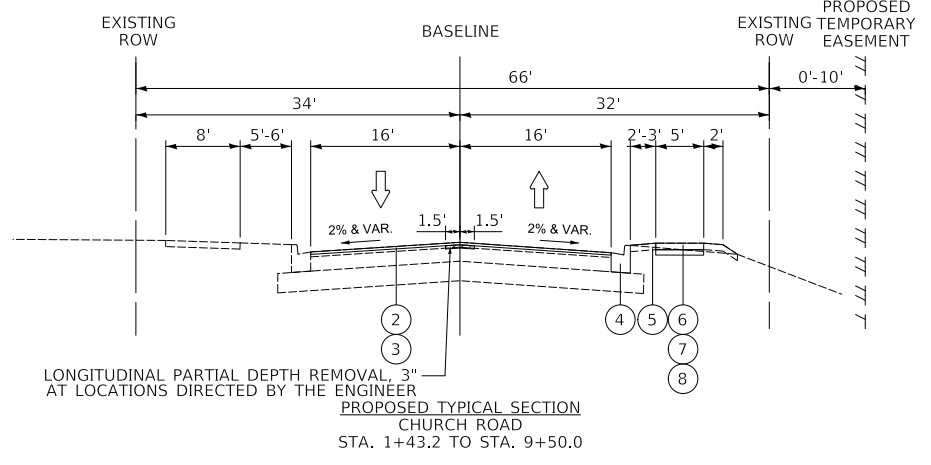
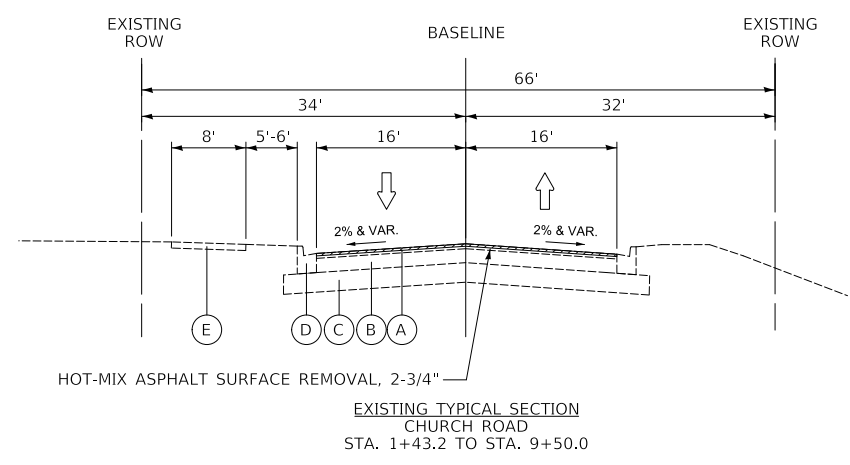
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CHURCH ROAD AND MAIN STREET RESURFACING
 SUMMARY OF QUANTITIES

SHEET NO. 6 OF 7 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	9
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

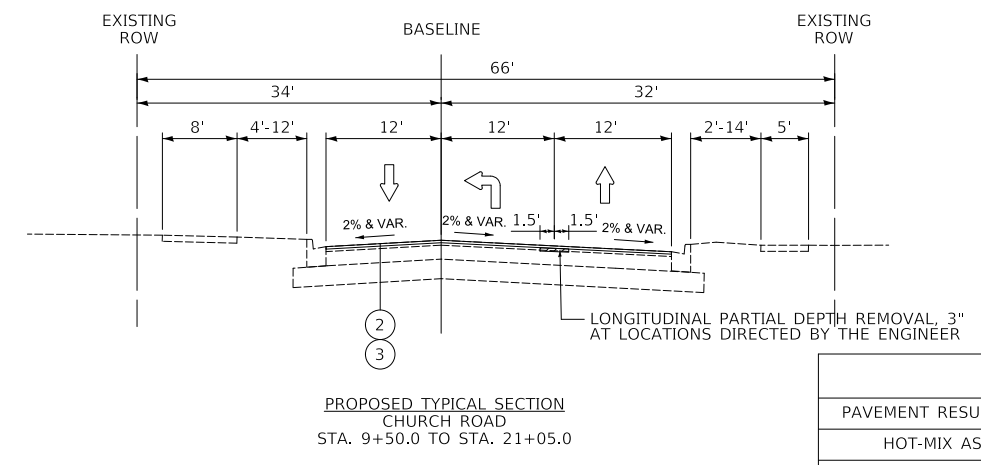
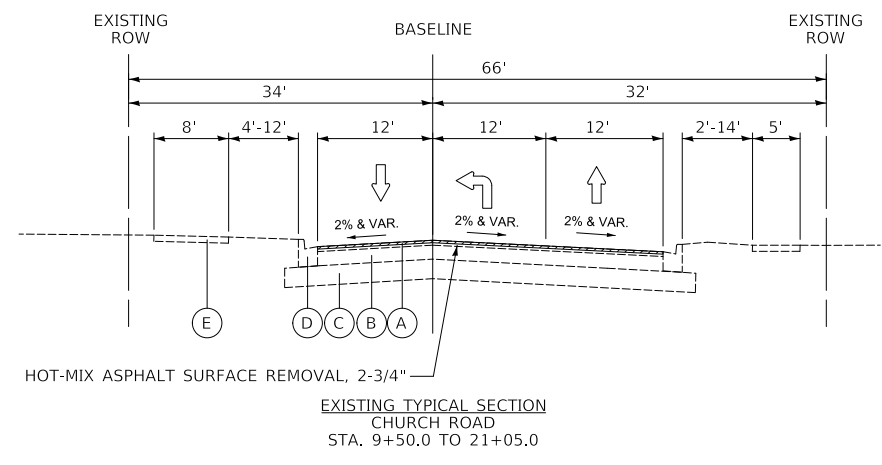
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BY	
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PLOTTED	
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NOTE BOOK	
NO.	



LEGEND

- (A) EXISTING HOT-MIX ASPHALT SURFACE THICKNESS VARIES FROM 2-1/4 INCHES TO 5-1/4 INCHES (REFERENCE ROADWAY PAVEMENT CORE REPORT PREPARED BY MSET DATED 12/11/2020)
- (B) EXISTING HOT-MIX ASPHALT BINDER THICKNESS VARIES FROM 2 INCHES TO 5-1/2 INCHES (REFERENCE ROADWAY PAVEMENT CORE REPORT PREPARED BY MSET DATED 12/11/2020)
- (C) EXISTING AGGREGATE SUB-BASE THICKNESS VARIES 6-1/2 INCHES TO 28-1/4 INCHES (REFERENCE ROADWAY PAVEMENT CORE REPORT PREPARED BY MSET DATED 12/11/2020)
- (D) EXISTING CURB AND GUTTER, TYPE B-6.12
- (E) EXISTING PCC SIDEWALK
- (1) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
- (2) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"
- (3) PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- (4) PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SEE ROADWAY PLANS FOR LIMITS OF CURB REPLACEMENT)
- (5) PROPOSED SODDING, SALT TOLERANT PROPOSED TOPSOIL FURNACE AND PLACE, 4"
- (6) PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (7) PROPOSED AGGREGATE BASE COURSE, TYP B, 4" MIN.
- (8) PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

DATE	
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NOTES:

1. ALL CURB AND GUTTER TO BE REMOVED WILL BE REPLACED IN KIND (REGARDLESS OF TYPE) AND PAID FOR AT THE SAME UNIT BID PRICE FOR "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (ABUTTING EXISTING PAVEMENT)". (SEE PLANS FOR LOCATIONS).
2. THE ROADWAY SOIL INVESTIGATION REPORT DATED DECEMBER 11, 2020, IS AVAILABLE FROM THE VILLAGE OF BENSENVILLE REPRESENTATIVE.



HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	PERCENT AIR VOIDS @ Ndes.	QMP
PAVEMENT RESURFACING - MAIN STREET		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"	4% @ 50 GYR.	LR 1030-2
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	LR 1030-2
PAVEMENT RESURFACING - CHURCH ROAD		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"	4% @ 70 GYR.	LR 1030-2
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	LR 1030-2
LONGITUDINAL PARTIAL DEPTH PATCHING, 3"		
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 3"	4% @ 70 GYR.	LR 1030-2
CLASS D PATCHES		
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	4% @ 70 GYR.	LR 1030-2
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LOCAL ROADS SPECIAL PROVISION LR 1030-2		

THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LB/SY-IN.

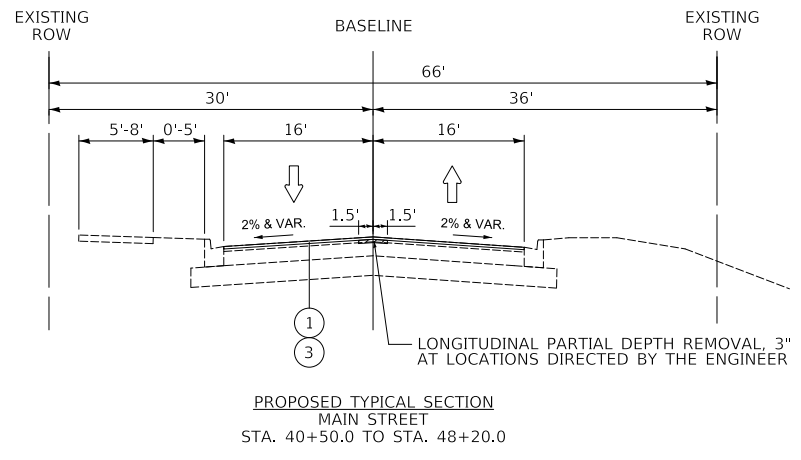
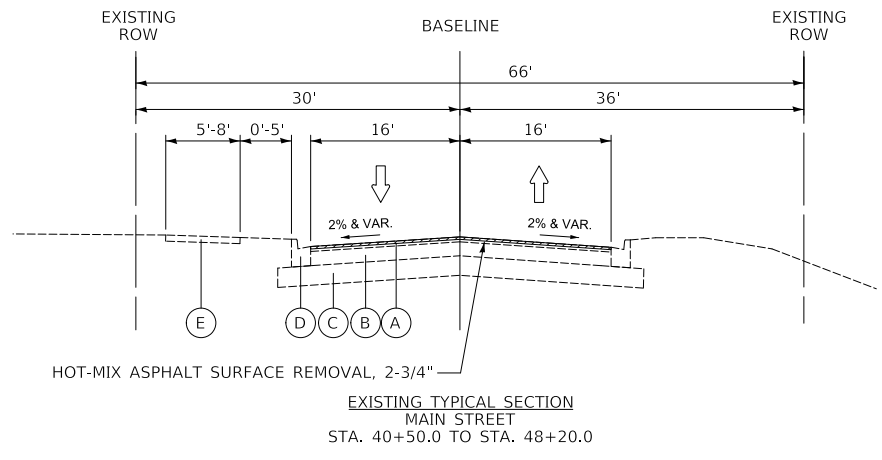
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE P HMA BC IL-4.75 N50.

MILLING FIRST SEQUENCE OF CONSTRUCTION TO BE FOLLOWED.

DATE	
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NO. OF WAY CHECKED	
NO. OF STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	
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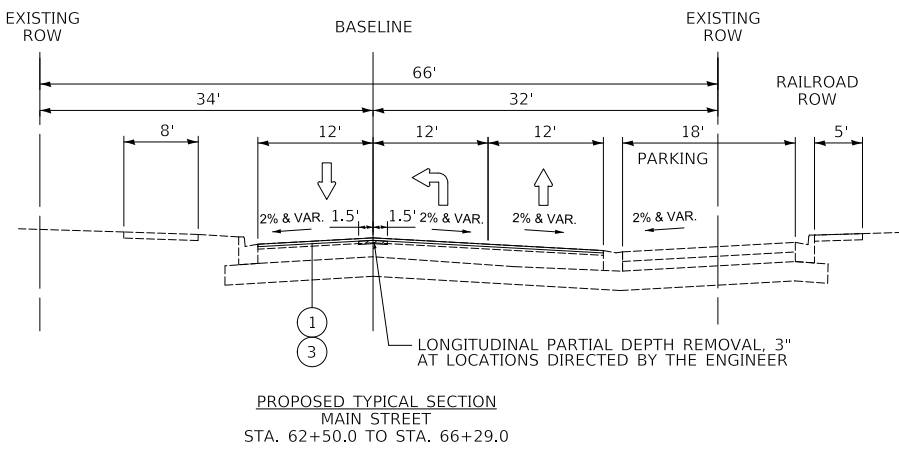
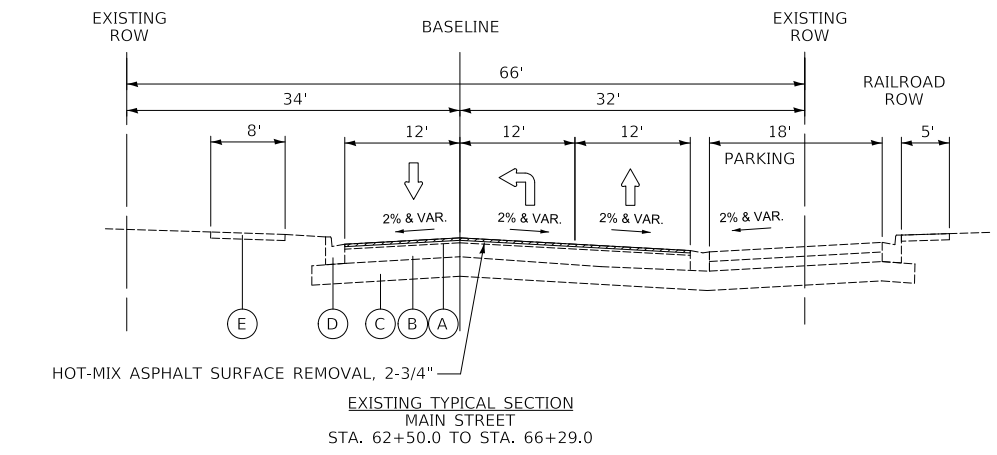
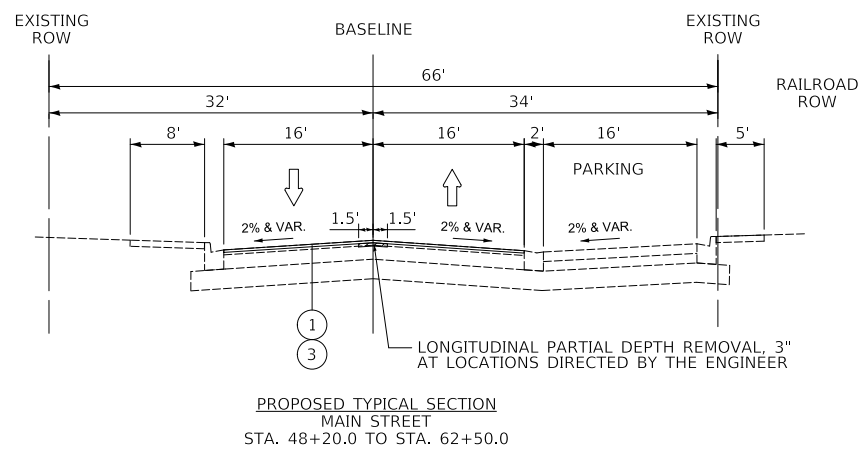
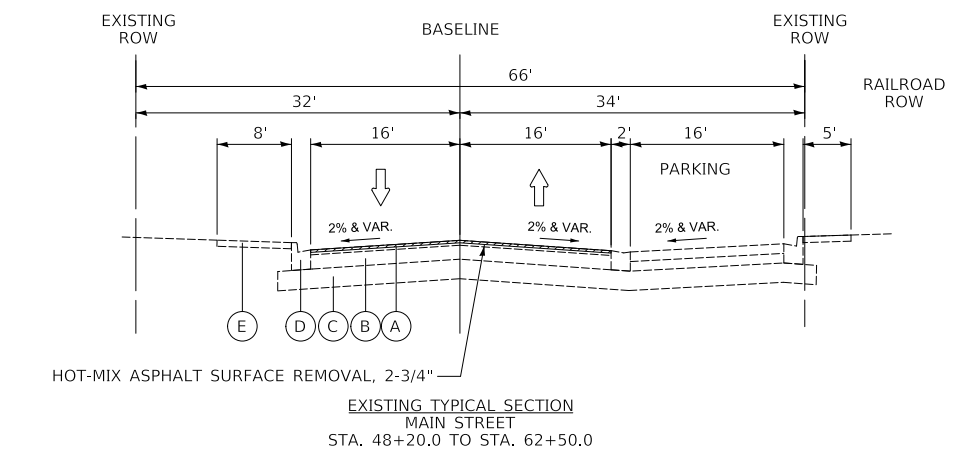
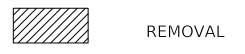


LEGEND

- (A) EXISTING HOT-MIX ASPHALT SURFACE THICKNESS VARIES FROM 2-1/4 INCHES TO 5-1/4 INCHES (REFERENCE ROADWAY PAVEMENT CORE REPORT PREPARED BY MSET DATED 12/11/2020)
- (B) EXISTING HOT-MIX ASPHALT BINDER THICKNESS VARIES FROM 2 INCHES TO 5-1/2 INCHES (REFERENCE ROADWAY PAVEMENT CORE REPORT PREPARED BY MSET DATED 12/11/2020)
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- (D) EXISTING CURB AND GUTTER, TYPE B-6.12
- (E) EXISTING PCC SIDEWALK
- (1) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
- (2) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2"
- (3) PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- (4) PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SEE ROADWAY PLANS FOR LIMITS OF CURB REPLACEMENT)
- (5) PROPOSED SODDING, SALT TOLERANT
PROPOSED TOPSOIL FURNISH AND PLACE, 4"
- (6) PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (7) PROPOSED AGGREGATE BASE COURSE, TYP B, 4" MIN.
- (8) PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

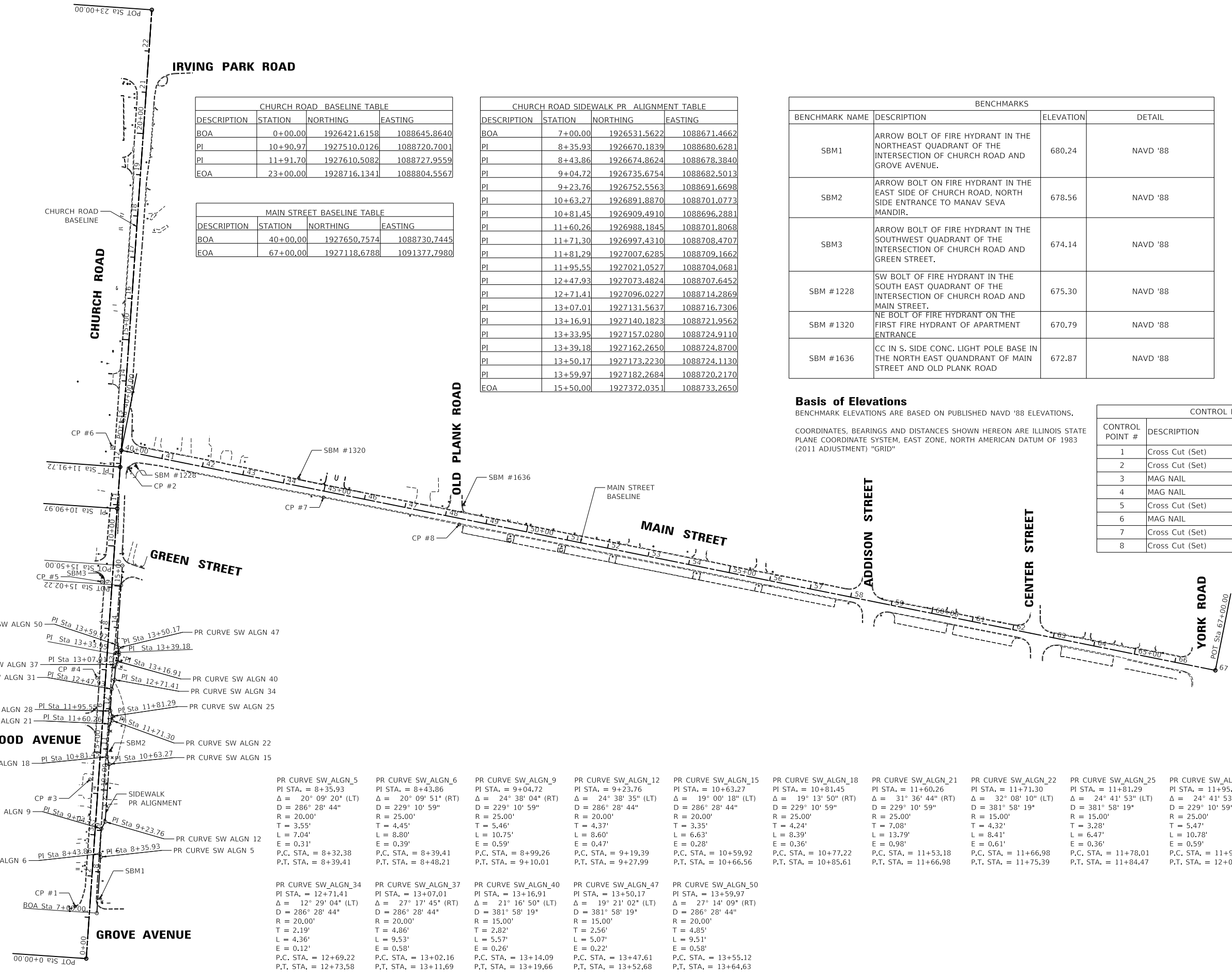
NOTES:

1. ALL CURB AND GUTTER TO BE REMOVED WILL BE REPLACED IN KIND (REGARDLESS OF TYPE) AND PAID FOR AT THE SAME UNIT BID PRICE FOR "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (ABUTTING EXISTING PAVEMENT)". (SEE PLANS FOR LOCATIONS).
2. THE ROADWAY SOIL INVESTIGATION REPORT DATED DECEMBER 11, 2020, IS AVAILABLE FROM THE VILLAGE OF BENSENVILLE REPRESENTATIVE.



PLAN	SURVEYED	DATE
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PROFILE	SURVEYED	DATE
	NOTED	
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	BY	
	NO. OF WAY CHECKED	
	NO.	



DESCRIPTION	STATION	NORTHING	EASTING
BOA	0+00.00	1926421.6158	1088645.8640
PI	10+90.97	1927510.0126	1088720.7001
PI	11+91.70	1927610.5082	1088727.9559
EOA	23+00.00	1928716.1341	1088804.5567

DESCRIPTION	STATION	NORTHING	EASTING
BOA	40+00.00	1927650.7574	1088730.7445
EOA	67+00.00	1927118.6788	1091377.7980

DESCRIPTION	STATION	NORTHING	EASTING
BOA	7+00.00	1926531.5622	1088671.4662
PI	8+35.93	1926670.1839	1088680.6281
PI	8+43.86	1926674.8624	1088678.3840
PI	9+04.72	1926735.6754	1088682.5013
PI	9+23.76	1926752.5563	1088691.6698
PI	10+63.27	1926891.8870	1088701.0773
PI	10+81.45	1926909.4910	1088696.2881
PI	11+60.26	1926988.1845	1088701.8068
PI	11+71.30	1926997.4310	1088708.4707
PI	11+81.29	1927007.6285	1088709.1662
PI	11+95.55	1927021.0527	1088704.0681
PI	12+47.93	1927073.4824	1088707.6452
PI	12+71.41	1927096.0227	1088714.2869
PI	13+07.01	1927131.5637	1088716.7306
PI	13+16.91	1927140.1823	1088721.9562
PI	13+33.95	1927157.0280	1088724.9110
PI	13+39.18	1927162.2650	1088724.8700
PI	13+50.17	1927173.2230	1088724.1130
PI	13+59.97	1927182.2684	1088720.2170
EOA	15+50.00	1927372.0351	1088733.2650

BENCHMARK NAME	DESCRIPTION	ELEVATION	DETAIL
SBM1	ARROW BOLT OF FIRE HYDRANT IN THE NORTHEAST QUADRANT OF THE INTERSECTION OF CHURCH ROAD AND GROVE AVENUE.	680.24	NAVD '88
SBM2	ARROW BOLT ON FIRE HYDRANT IN THE EAST SIDE OF CHURCH ROAD, NORTH SIDE ENTRANCE TO MANAV SEVA MANDIR.	678.56	NAVD '88
SBM3	ARROW BOLT OF FIRE HYDRANT IN THE SOUTHWEST QUADRANT OF THE INTERSECTION OF CHURCH ROAD AND GREEN STREET.	674.14	NAVD '88
SBM #1228	SW BOLT OF FIRE HYDRANT IN THE SOUTH EAST QUADRANT OF THE INTERSECTION OF CHURCH ROAD AND MAIN STREET.	675.30	NAVD '88
SBM #1320	NE BOLT OF FIRE HYDRANT ON THE FIRST FIRE HYDRANT OF APARTMENT ENTRANCE	670.79	NAVD '88
SBM #1636	CC IN S. SIDE CONC. LIGHT POLE BASE IN THE NORTH EAST QUADRANT OF MAIN STREET AND OLD PLANK ROAD	672.87	NAVD '88

Basis of Elevations
 BENCHMARK ELEVATIONS ARE BASED ON PUBLISHED NAVD '88 ELEVATIONS.
 COORDINATES, BEARINGS AND DISTANCES SHOWN HEREON ARE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983 (2011 ADJUSTMENT) "GRID"

CONTROL POINT #	DESCRIPTION	NORTHING	EASTING
1	Cross Cut (Set)	1926547.762	1088618.864
2	Cross Cut (Set)	1927607.581	1088748.055
3	MAG NAIL	1926854.806	1088654.960
4	MAG NAIL	1927101.017	1088671.758
5	Cross Cut (Set)	1927333.207	1088682.790
6	MAG NAIL	1927660.146	1088707.898
7	Cross Cut (Set)	1927536.984	1089219.801
8	Cross Cut (Set)	1927471.508	1089548.372

PR CURVE SW_ALGN_5 PI STA. = 8+35.93 Δ = 20° 09' 20" (LT) D = 286° 28' 44" R = 20.00' T = 3.55' L = 7.04' E = 0.31'P.C. STA. = 8+32.38 P.T. STA. = 8+39.41	PR CURVE SW_ALGN_6 PI STA. = 8+43.86 Δ = 20° 09' 51" (RT) D = 229° 10' 59" R = 25.00' T = 4.45' L = 8.80' E = 0.39'	PR CURVE SW_ALGN_9 PI STA. = 9+04.72 Δ = 24° 38' 04" (RT) D = 229° 10' 59" R = 25.00' T = 5.46' L = 10.75' E = 0.59'	PR CURVE SW_ALGN_12 PI STA. = 9+23.76 Δ = 24° 38' 35" (LT) D = 286° 28' 44" R = 20.00' T = 4.37' L = 8.60' E = 0.47'	PR CURVE SW_ALGN_15 PI STA. = 10+63.27 Δ = 19° 00' 18" (LT) D = 286° 28' 44" R = 20.00' T = 4.24' L = 8.39' E = 0.36'	PR CURVE SW_ALGN_18 PI STA. = 10+81.45 Δ = 19° 13' 50" (RT) D = 229° 10' 59" R = 25.00' T = 4.24' L = 8.39' E = 0.36'	PR CURVE SW_ALGN_21 PI STA. = 11+60.26 Δ = 31° 36' 44" (RT) D = 381° 58' 19" R = 15.00' T = 7.08' L = 13.79' E = 0.98'	PR CURVE SW_ALGN_22 PI STA. = 11+71.30 Δ = 32° 08' 10" (LT) D = 381° 58' 19" R = 15.00' T = 4.32' L = 8.41' E = 0.61'	PR CURVE SW_ALGN_25 PI STA. = 11+81.29 Δ = 24° 41' 53" (LT) D = 229° 10' 59" R = 25.00' T = 3.28' L = 6.47' E = 0.36'	PR CURVE SW_ALGN_28 PI STA. = 11+95.55 Δ = 24° 41' 53" (RT) D = 229° 10' 59" R = 25.00' T = 5.47' L = 10.78' E = 0.59'	PR CURVE SW_ALGN_31 PI STA. = 12+47.93 Δ = 12° 30' 53" (RT) D = 229° 10' 59" R = 25.00' T = 2.74' L = 5.46' E = 0.15'
PR CURVE SW_ALGN_34 PI STA. = 12+71.41 Δ = 12° 29' 04" (LT) D = 286° 28' 44" R = 20.00' T = 2.19' L = 4.36' E = 0.12'	PR CURVE SW_ALGN_37 PI STA. = 13+02.16 Δ = 27° 17' 45" (RT) D = 286° 28' 44" R = 20.00' T = 4.82' L = 9.53' E = 0.58'	PR CURVE SW_ALGN_40 PI STA. = 13+14.09 Δ = 21° 16' 50" (LT) D = 381° 58' 19" R = 15.00' T = 5.57' L = 10.75' E = 0.58'	PR CURVE SW_ALGN_47 PI STA. = 13+50.17 Δ = 19° 21' 02" (LT) D = 286° 28' 44" R = 15.00' T = 2.56' L = 5.07' E = 0.22'	PR CURVE SW_ALGN_50 PI STA. = 13+59.97 Δ = 27° 14' 09" (RT) D = 286° 28' 44" R = 20.00' T = 4.85' L = 9.51' E = 0.58'						

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



**CHURCH ROAD AND MAIN STREET RESURFACING
 ALIGNMENT, TIES AND BENCHMARKS**

SHEET NO. 1 OF 1 SHEETS

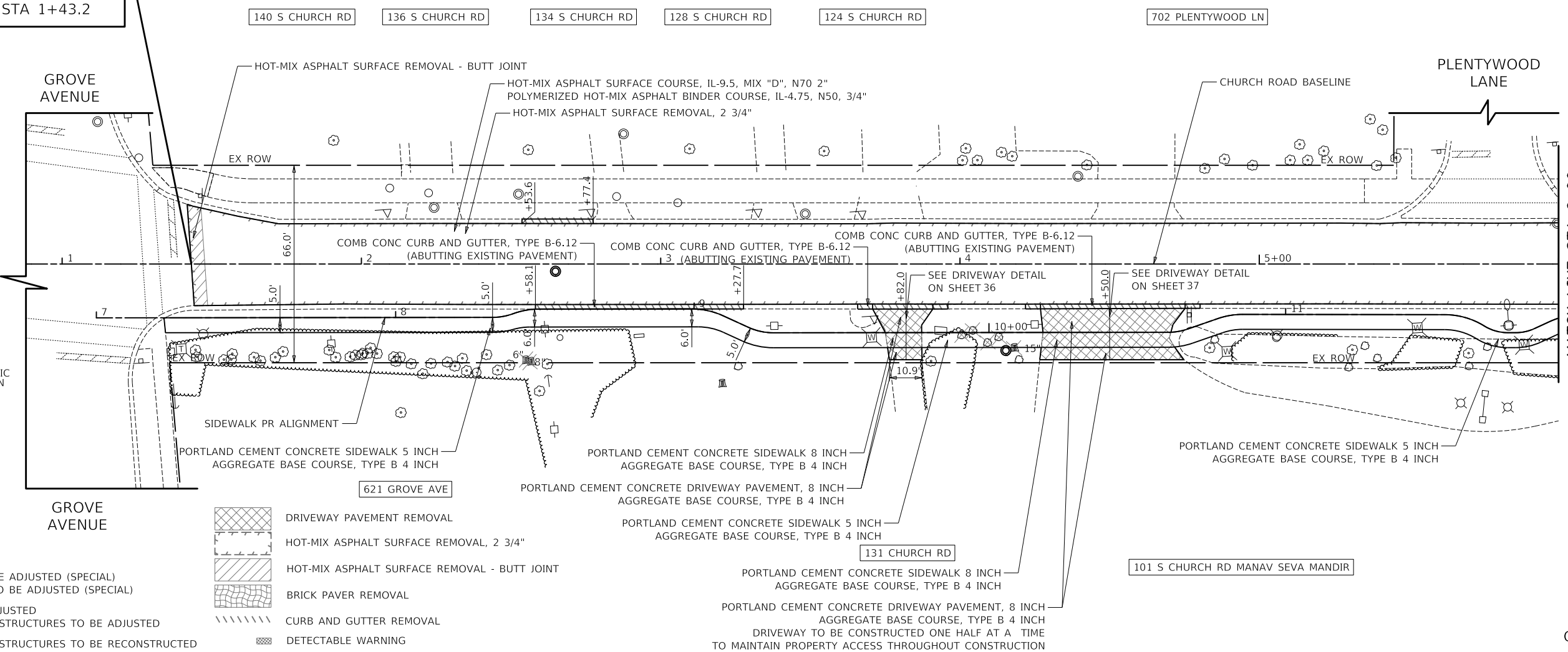
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	13
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



START OF PROJECT
CHURCH ROAD
STA 1+43.2

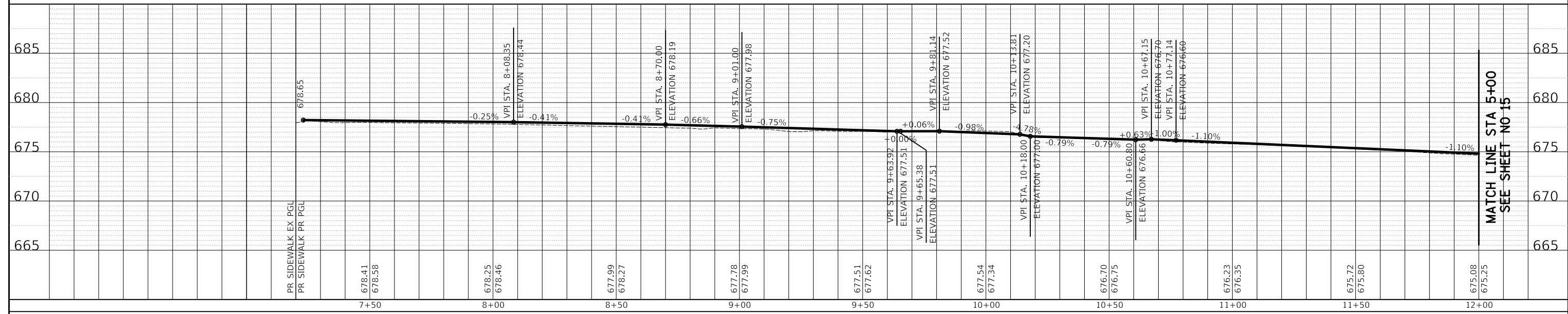
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NO.	
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- NOTES:
- PLACE GEOTECHNICAL FABRIC FOR GROUND STABILIZATION UNDER PCC SIDEWALK AGGREGATE BASE COURSE.
 - LOCATIONS OF CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE CONFIRMED BY THE ENGINEER PRIOR TO REMOVAL OPERATIONS.

- LEGEND
- XX" STUMP REMOVAL ONLY
 - SHRUB REMOVAL
 - SP FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
FRAMES AND GRATES TO BE ADJUSTED (SPECIAL)
 - ADJ VALVE VAULT TO BE ADJUSTED
DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
 - REC DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED
 - DRIVEWAY PAVEMENT REMOVAL
 - HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"
 - HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
 - BRICK PAVER REMOVAL
 - CURB AND GUTTER REMOVAL
 - DETECTABLE WARNING

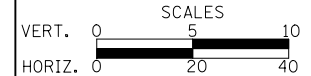


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DEPARTMENT OF TRANSPORTATION



**CHURCH ROAD AND MAIN STREET RESURFACING
AND CHURCH ROAD SIDEWALK
PLAN AND PROFILE**

SHEET NO. 1 OF 6 SHEETS STA. 1+43.2 TO STA. 6+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	14
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LEGEND

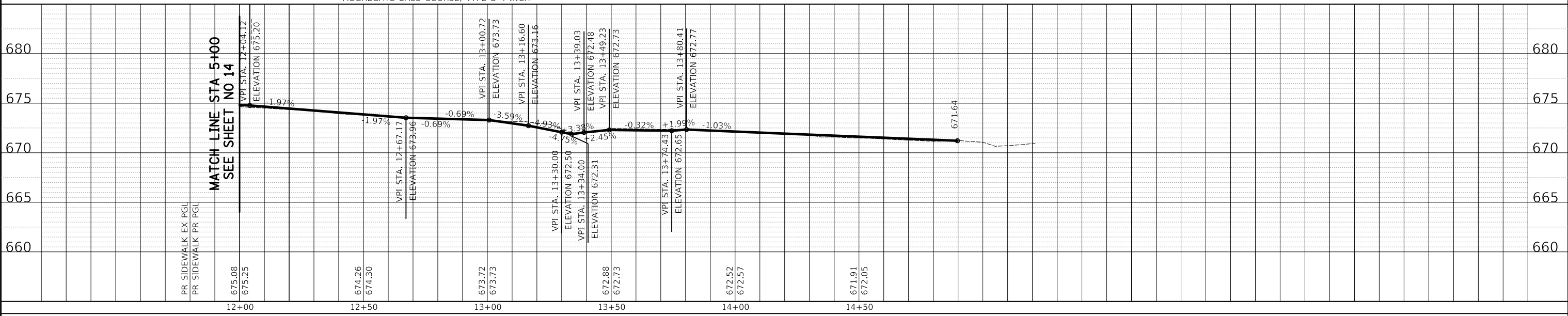
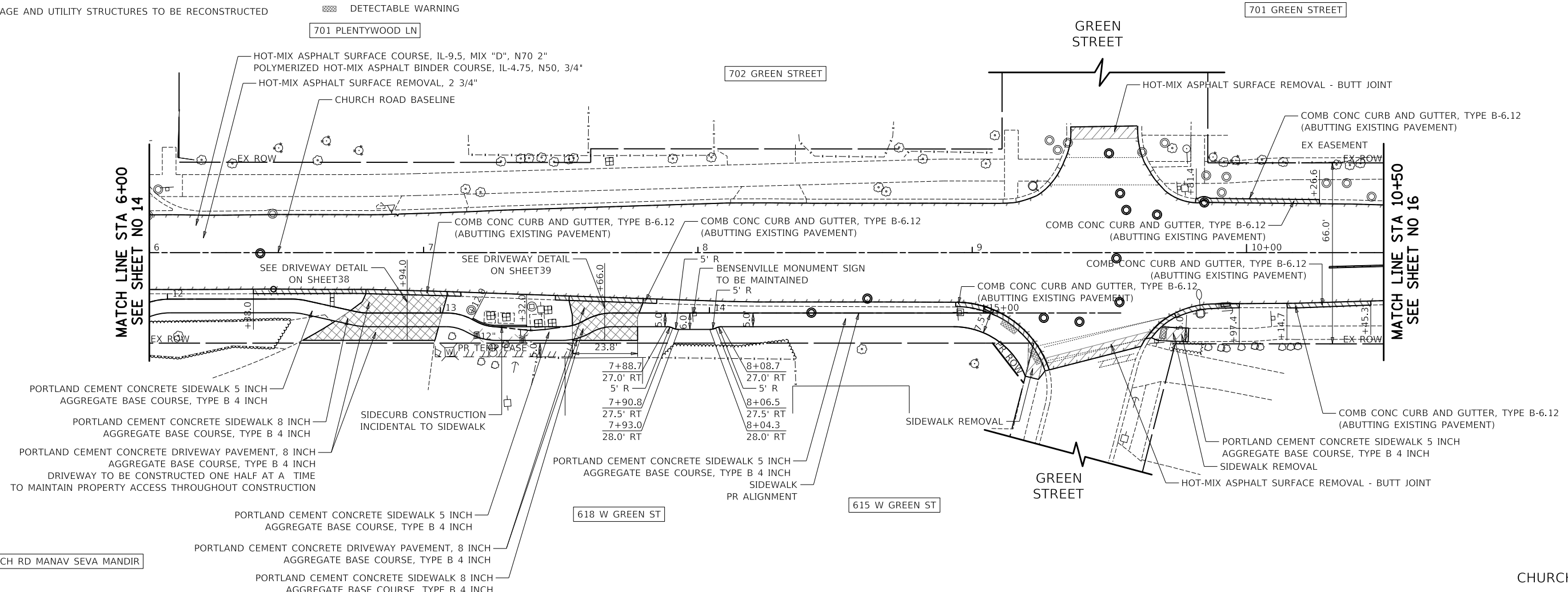
- XX" STUMP REMOVAL ONLY
- SHRUB REMOVAL
- SP FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
FRAMES AND GRATES TO BE ADJUSTED (SPECIAL)
- ADJ VALVE VAULT TO BE ADJUSTED
DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
- REC DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- BRICK PAVER REMOVAL
- CURB AND GUTTER REMOVAL
- DETECTABLE WARNING

- NOTES:
- PLACE GEOTECHNICAL FABRIC FOR GROUND STABILIZATION UNDER PCC SIDEWALK AGGREGATE BASE COURSE.
 - LOCATIONS OF CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE CONFIRMED BY THE ENGINEER PRIOR TO REMOVAL OPERATIONS.



PLAN	SURVEYED	DATE
	PLOTTED	
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	BY	
	NO. OF WAY CHECKED	
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PROFILE	SURVEYED	DATE
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	STRUCTURE NOTATIONS CHECKED	

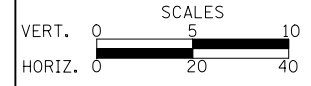


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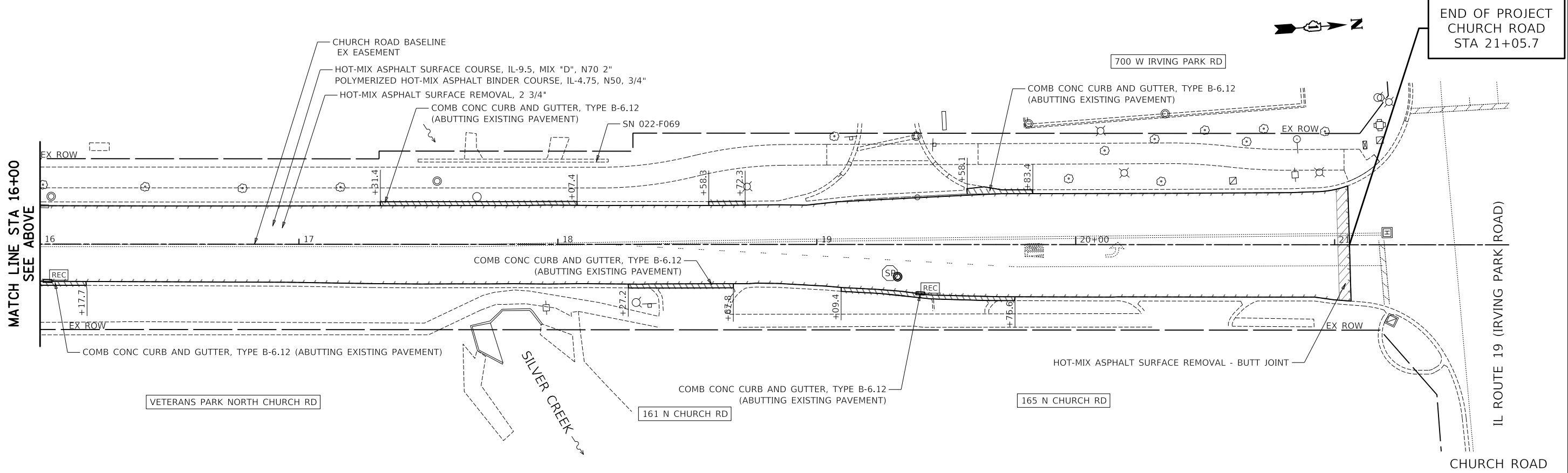
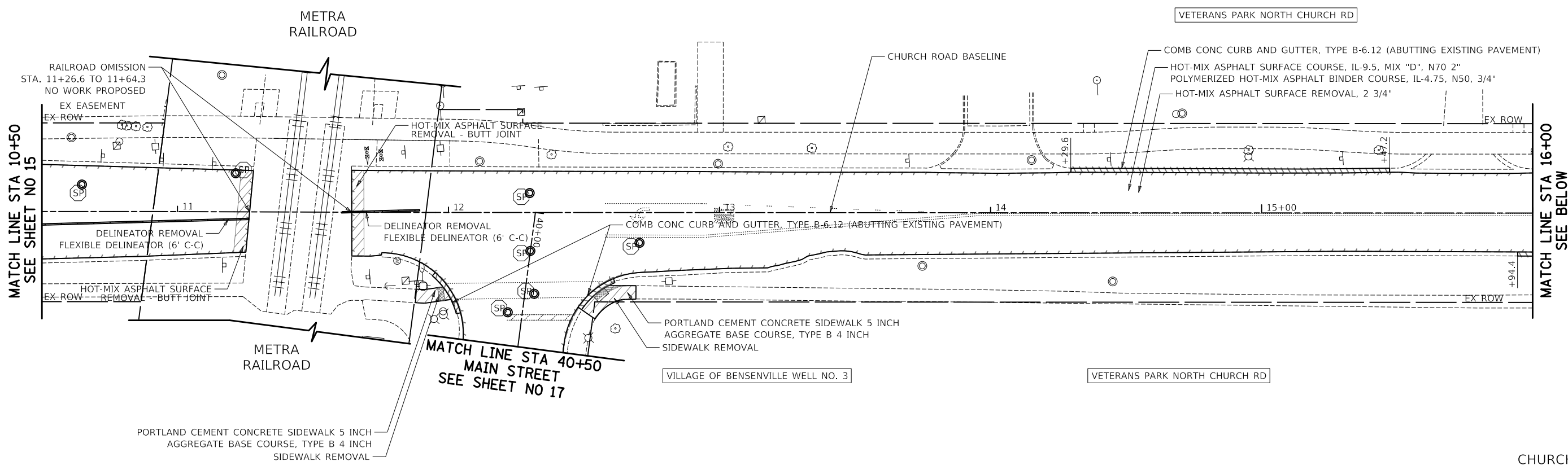
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CHURCH ROAD AND MAIN STREET RESURFACING AND CHURCH ROAD SIDEWALK PLAN AND PROFILE

SHEET NO. 2 OF 6 SHEETS STA. 6+00 TO STA. 10+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	15
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



END OF PROJECT
CHURCH ROAD
STA 21+05.7

PLAN	SURVEYED	DATE
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DEPARTMENT OF TRANSPORTATION



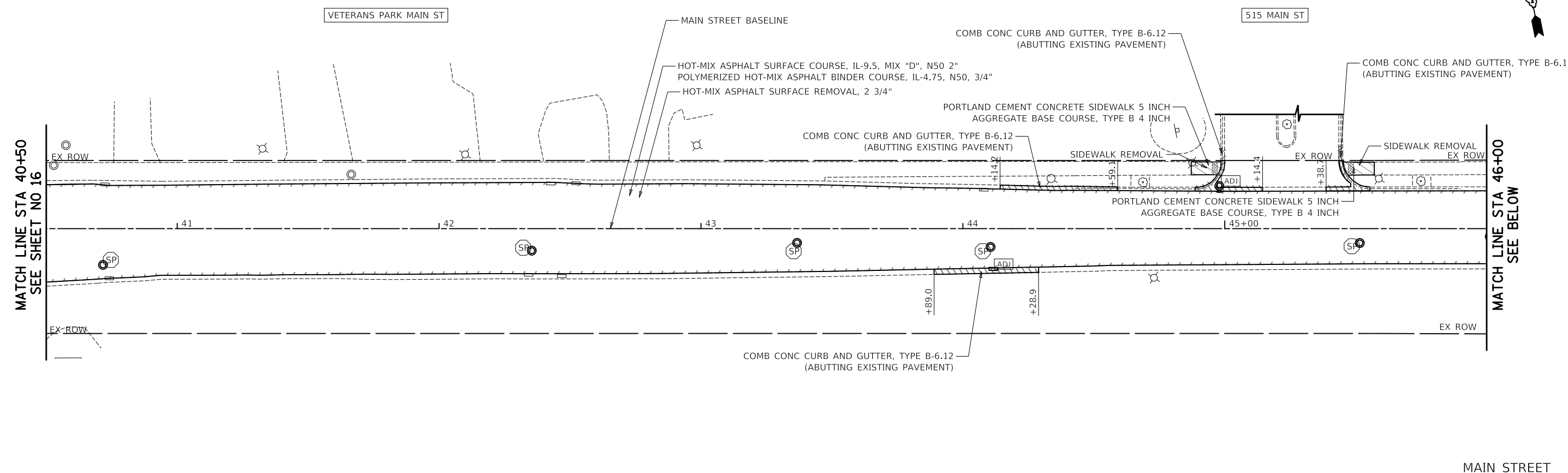
CHURCH ROAD AND MAIN STREET RESURFACING
PROPOSED PLAN

SHEET NO. 3 OF 6 SHEETS STA. 10+50 TO STA. 21+05.7

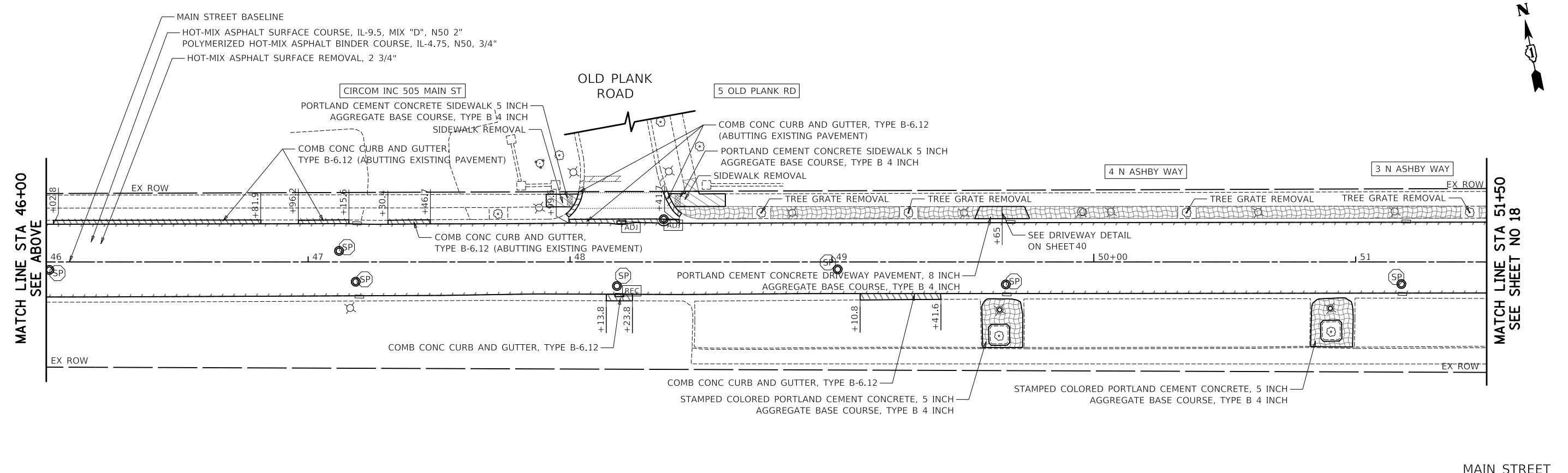
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	16
3001 MAIN				
CONTRACT NO. 61K22				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
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	NO. OF WAY CHECKED		
	STRUCTURE NOTATIONS CHKD		
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MAIN STREET



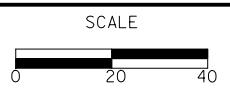
MAIN STREET

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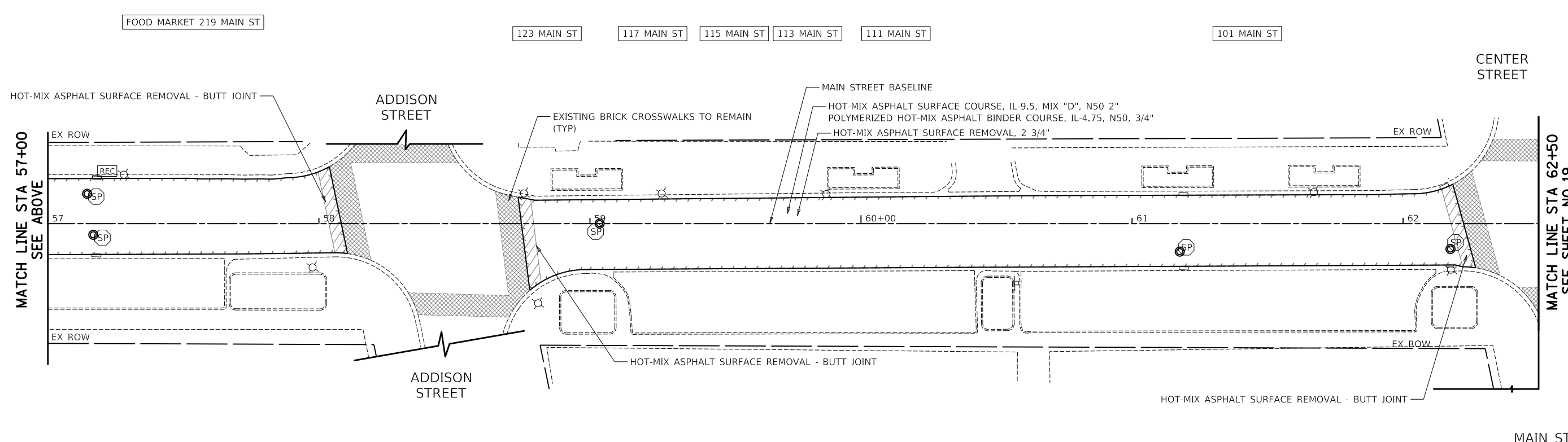
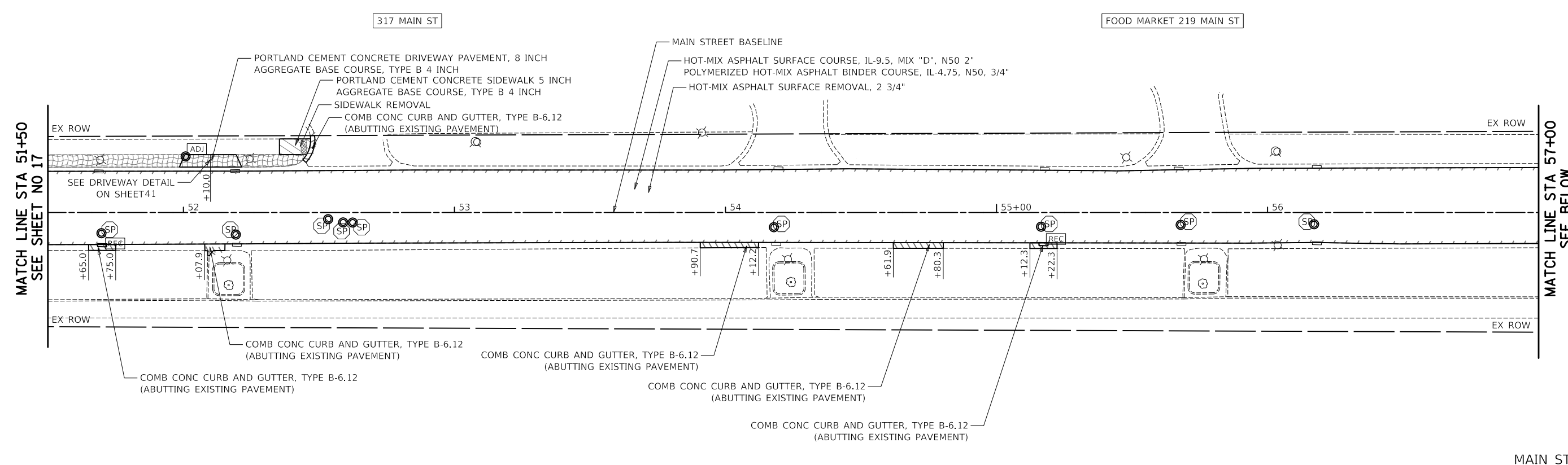
**CHURCH ROAD AND MAIN STREET RESURFACING
PROPOSED PLAN**

SHEET NO. 4 OF 6 SHEETS STA. 40+50 TO STA. 51+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	17
3001 MAIN			CONTRACT NO. 61K22	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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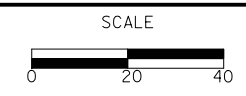
PROFILE	SURVEYED	BY	DATE
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	STRUCTURE NOTATIONS CHKD		



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CHURCH ROAD AND MAIN STREET RESURFACING
PROPOSED PLAN

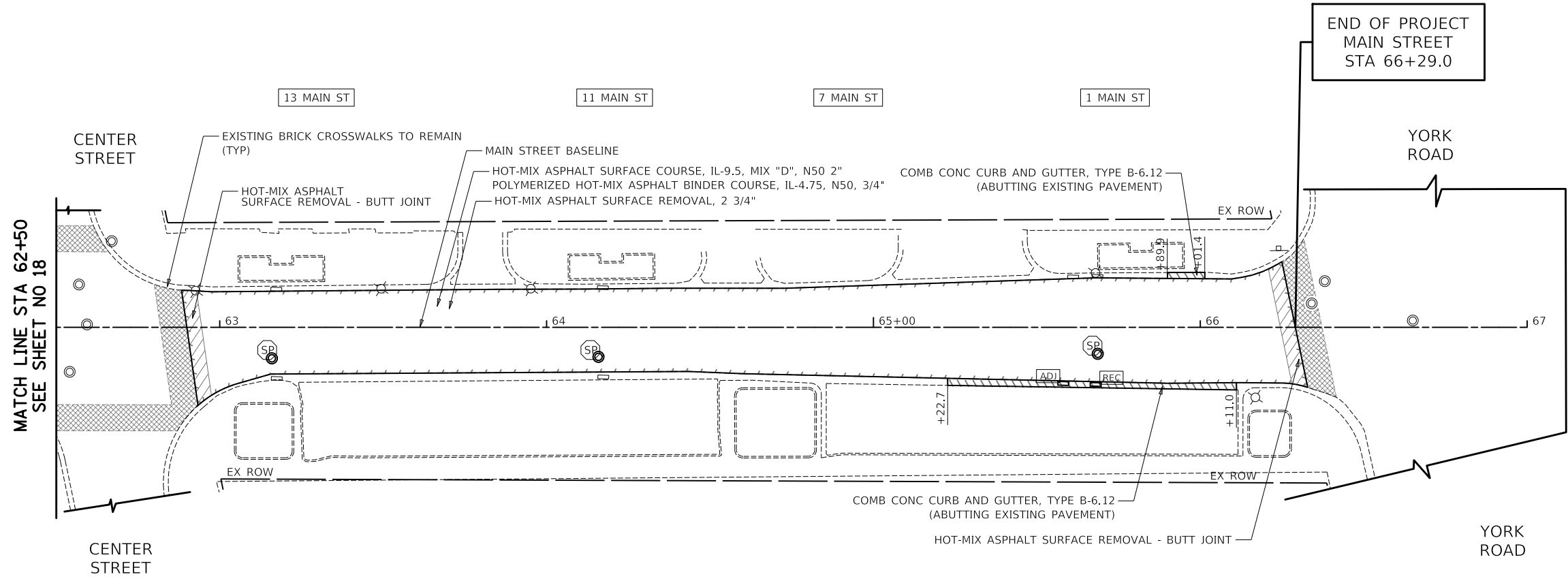
SHEET NO. 5 OF 6 SHEETS STA. 51+50 TO STA. 62+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	18
3001 MAIN			CONTRACT NO. 61K22	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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PROFILE	SURVEYED	DATE
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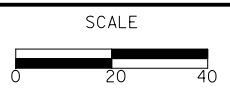


MAIN STREET

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CHURCH ROAD AND MAIN STREET RESURFACING
PROPOSED PLAN

SHEET NO. 6 OF 6 SHEETS STA. 62+50 TO STA. 66+29.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	19
3001 MAIN			CONTRACT NO. 61K22	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
2. DURING CONSTRUCTION OF PROPOSED IMPROVEMENTS, TWO-WAY TRAFFIC SHALL REMAIN OPEN FOR ALL TRAFFIC AT ALL TIMES. IF ANY ACTIVITY REQUIRES ENCROACHMENT INTO A LANE OPEN FOR TRAFFIC, THAT ACTIVITY SHALL BE RESTRICTED TO WITHIN THE HOURS OF 7:00 A.M. AND 6:00 P.M. FOLLOWING THE APPLICABLE IDOT AND IDOT-DISTRICT 1 TRAFFIC CONTROL STANDARDS INCLUDED IN THE PLANS FOR OFF-ROAD AND ON-ROAD APPLICATIONS.
3. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
4. ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
5. ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS. SIGNS THAT ARE REMOVED AND RE-ERECTED ON A DAILY BASIS ARE EXEMPT FROM THIS REQUIREMENT.
6. DRUMS WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE. AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS, SPACING SHALL BE AS SHOWN ON THE HIGHWAY STANDARDS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
7. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.
8. TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH APPLICABLE HIGHWAY STANDARDS UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.
9. THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.
10. EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER; ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED. ANY DAMAGE CAUSED BY THE CONTRACTOR'S WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
11. THE FIRST TWO WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
12. "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT FOR MORE THAN ONE HOUR. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
13. EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
14. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED AT A LOCATION APPROVED BY THE ENGINEER AT LEAST 7 DAYS IN ADVANCE OF THE COMMENCEMENT OF CONSTRUCTION ACTIVITIES FOR ADVANCED NOTIFICATION TO THE PUBLIC. THE MESSAGE SHALL BE DETERMINED BY THE ENGINEER. MESSAGES SHALL BE UPDATED AS REQUIRED BY THE ENGINEER.
15. THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL AND PROTECTION ITEMS.
16. QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS AND SHORT-TERM PAVEMENT MARKING REMOVAL SHALL BE MEASURED SEPARATELY FOR PAYMENT.
17. THE CONTRACTOR SHALL ENSURE THAT WORK OPERATIONS DO NOT OBSTRUCT LINES OF SIGHT REQUIRED FOR DRIVERS TO MAKE TURNING MOVEMENTS SAFELY. THE ENGINEER MAY REQUIRE ADDITIONAL FLAGGERS BE PRESENT TO SAFELY NAVIGATE DRIVERS THROUGH AND AROUND CONSTRUCTION ACTIVITIES AT NO ADDITIONAL COST TO THE CONTRACT.
18. LANE CLOSURES AND CONFIGURATIONS MUST NOT IMPEDE SAFE AND UNOBSTRUCTED TURNING MOVEMENTS AT INTERSECTIONS AND DRIVEWAYS. IF OPERATIONS CREATE UNSAFE TURNING CONATIONS AS DETERMINED BY THE ENGINEER THE ENGINEER MAY REQUIRE ADDITIONAL FLAGGERS BE PRESENT TO SAFELY NAVIGATE DRIVERS THROUGH AND AROUND CONSTRUCTION ACTIVITIES AT NO ADDITIONAL COST TO THE CONTRACT.

CONSTRUCTION REQUIREMENTS

1. ALL WORK SHALL BE IN ACCORDANCE WITH IDOT'S SAFETY ENGINEERING POLICY MEMORANDUM. SAFETY 4-21, "UNEVEN LANE" SIGNS (W8-1-4848) SHALL BE PLACED AT THE INTERVALS REQUIRED BY THE ENGINEER WHEN TRAFFIC IS ON PAVEMENT WITHOUT SURFACE COURSE AND ADJACENT TO SURFACED PAVEMENT. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL AND PROTECTION ITEMS.
2. "UNEVEN LANE" SIGNS (W8-1(O)-4848) SHALL BE PLACED AT THE INTERVALS REQUIRED BY THE ENGINEER WHEN TRAFFIC IS ADJACENT TO THE MILLED SURFACE. THESE SIGNS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL AND PROTECTION ITEMS.
3. "ROUGH GROOVED SURFACE" SIGNS (W8-1107(O)) SHALL BE USED WHEN THE ROAD HAS BEEN COLD MILLED AND OPEN TO TRAFFIC. THE SIGNS SHALL REMAIN IN PLACE UNTIL THE MILLED SURFACE CONDITIONS NO LONGER EXISTS. THESE SIGNS SHALL BE ERECTED A MINIMUM OF 500 FEET PRECEDING THE START OF THE MILLED PAVEMENT AND ON ALL SIDE ROADS WITHIN THE POSTED AREA. THE SIGNS ON THE SIDE ROADS SHALL BE POSTED 200 FEET FROM THE MAINLINE PAVEMENT. ALL SIGNS SHALL HAVE AN 18"X18" ORANGE FLAG AND AMBER FLASHING LIGHTS ATTACHED. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL AND PROTECTION ITEMS.

DRIVEWAYS AND PEDESTRIAN ACCESS

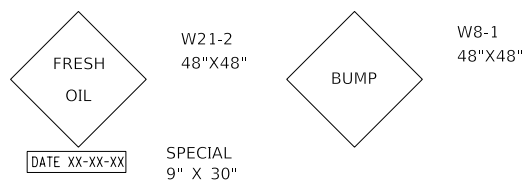
1. ALL DRIVEWAYS SHALL BE STAGE CONSTRUCTED TO ALLOW ACCESS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
2. SIDEWALK ON ONE SIDE OF THE ROAD SHALL REMAIN OPEN AT ALL TIMES TO PROVIDE PEDESTRIAN ACCESS. PEDESTRIAN ACCESS ACROSS DRIVEWAYS ON MAIN STREET WHERE THERE IS NO SIDEWALK ON THE OPPOSITE SIDE OF THE ROAD SHALL BE MAINTAINED THROUGH THE USE OF TEMPORARY ACCESS (ROAD) OR TEMPORARY ACCESS (COMMERCIAL ENTRANCE).

FLEXIBLE DELINEATORS

1. UPON REMOVAL OF FLEXIBLE DELINEATORS, CONTRACTOR SHALL INSTALL TUBULAR MARKERS MEETING STANDARD NO. 701901. MARKERS SHALL BE PLACED AT 6' SPACING AND LEFT IN PLACE UNTIL PROPOSED FLEXIBLE DELINEATORS ARE INSTALLED. THIS WORK SHALL BE PAID FOR AS "TUBULAR MARKER."

PLAN	DATE
SURVEYED PLOTTED CHECKED BY: _____	DATE
NOTE BOOK NO. _____	DATE
CADDY FILE NAME	DATE

PROFILE	DATE
SURVEYED PLOTTED CHECKED BY: _____	DATE
NOTE BOOK NO. _____	DATE
STRUCTURE NOTATIONS CHKD	DATE



THESE SIGNS SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THE COST SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL AND PROTECTION PAY ITEMS.



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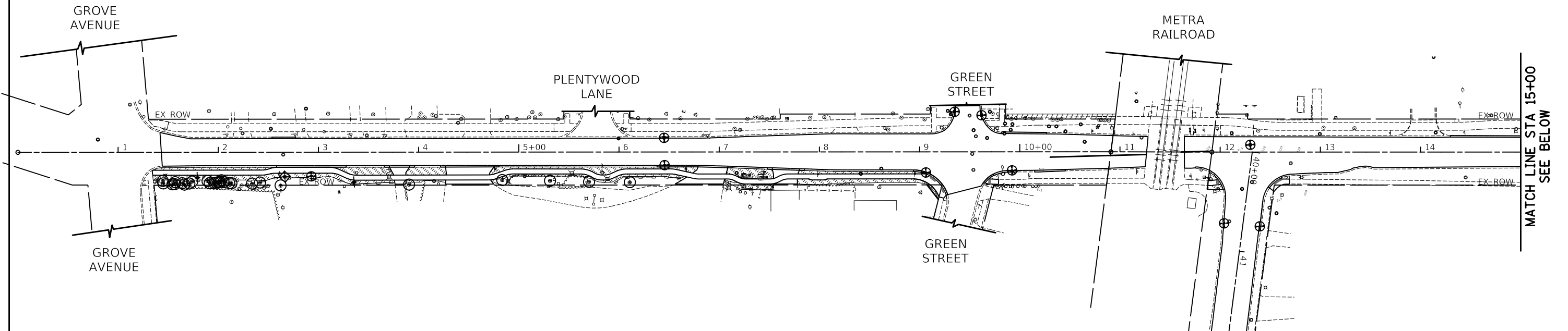
**CHURCH ROAD AND MAIN STREET RESURFACING
MAINTENANCE OF TRAFFIC GENERAL NOTES**

SHEET NO. 1 OF 1 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	20
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DATE	
BY	
SURVEYED	
PLOTTED	
CHECKED	
PT. OF WAY CHECKED	
NO. OF WAY CHECKED	
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ADD. FILE NAME	



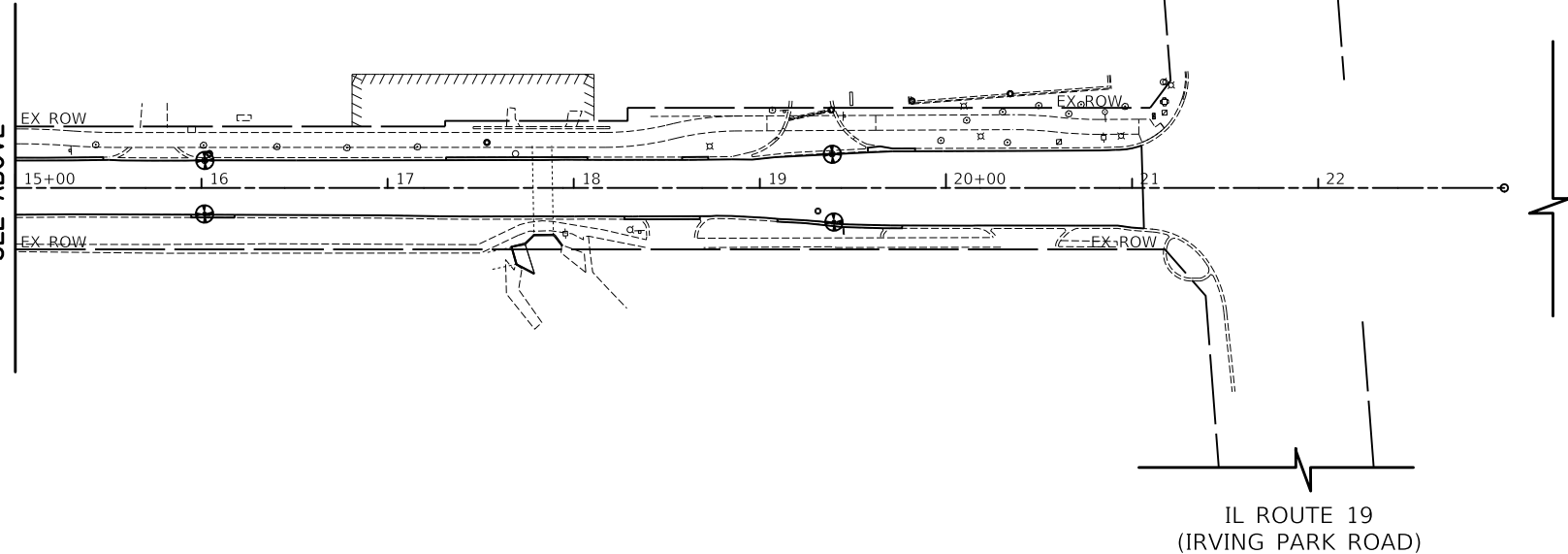
LEGEND

- TREE PROTECTION
(PAID FOR AS TREE ROOT PRUNING, TREE PRUNING, AND 40 FT OF TEMPORARY FENCE. TREE TRUNK PROTECTION ONLY TO BE UTILIZED DUE TO CONSTRUCTION PROXIMITY AND ENGINEER DIRECTION)
- INLET FILTER
- INLET AND PIPE PROTECTION
(PAID FOR AS 16 LF OF TEMPORARY DITCH CHECKS, 2 SY OF TEMPORARY EROSION CONTROL BLANKET, AND 1 INLET FILTER)
- PROPOSED TREE
- PROPOSED PERIMETER EROSION BARRIER
- PROPOSED TEMPORARY DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING
- TEMPORARY EROSION CONTROL BLANKET
- TOPSOIL PLACEMENT 4" (SEE NOTE 1)
- SODDING, SA LT TOLERANT

NOTES

1. TOPSOIL EXCAVATION AND PLACEMENT SHALL BE USED FIRST FOR PLACEMENT OF 4" OF TOPSOIL WHERE INDICATED. IF SUFFICIENT EXISTING TOPSOIL IS NOT AVAILABLE, THE REMAINING AREA SHALL BE COMPLETED USING THE PAY ITEM "TOPSOIL, FURNISH AND PLACE 4".
2. MULCH BED SHALL BE ADDED TO ANY EXISTING TREES TO REMAIN. MULCH BED SIZE WILL VARY UPON EXISTING TREE SIZES. THIS WORK SHALL BE PAID FOR AS "MULCH PLACEMENT 4".
3. CONTRACTOR SHALL PROVIDE SPADED EDGES FOR ALL SODDING AREA ABUTTING EXISTING TREES BY MAINTAINING THE MULCH AREA AROUND THE TREE.

MATCH LINE STA 15+00
SEE ABOVE



CHURCH ROAD

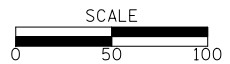


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PROFILE	
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PLOTTED	
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DESIGNED - JRR	REVISED -
DRAWN - JRR	REVISED -
CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



**CHURCH ROAD AND MAIN STREET RESURFACING
EROSION CONTROL AND LANDSCAPING PLAN**

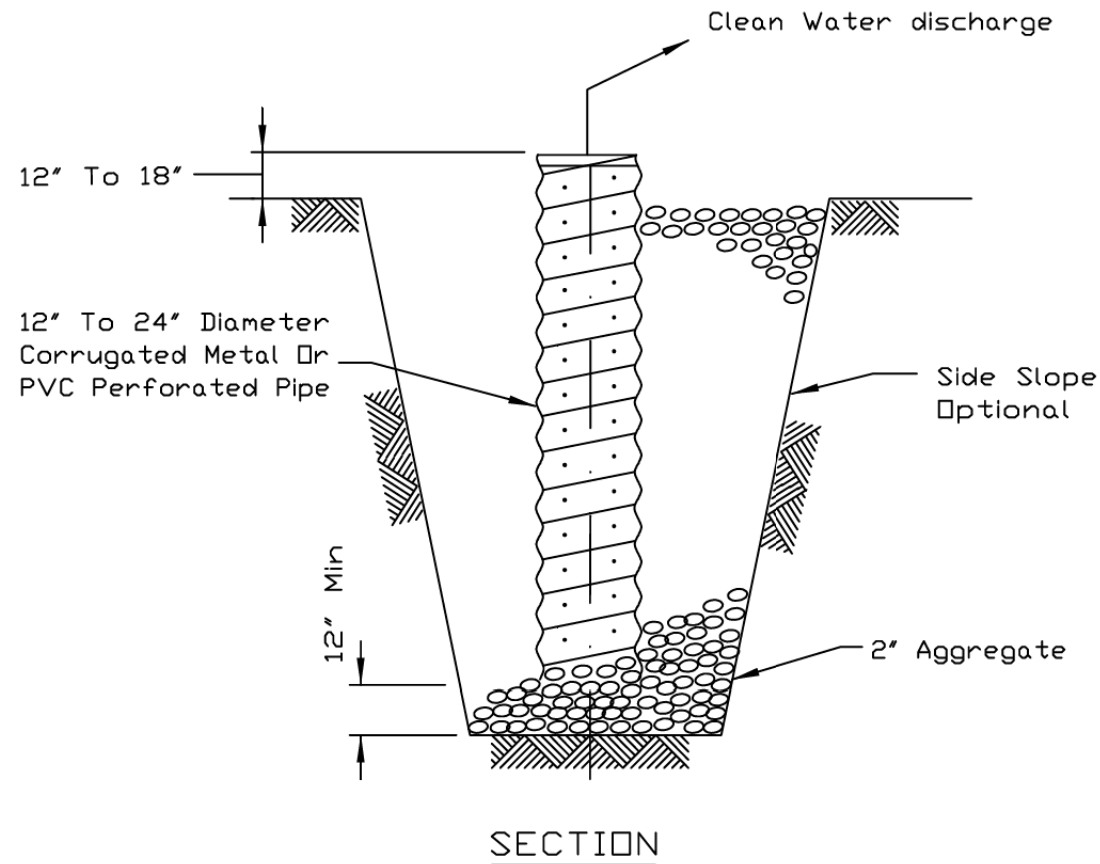
SHEET NO. 1 OF 2 SHEETS STA. 1+00 TO STA. 22+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	22
3001 MAIN				
CONTRACT NO. 61K22				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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PROF. FILE	
DESIGNED	
CHECKED	
DATE	
NO. OF WAY CHECKED	
NO.	
STRUCTURE NOTATIONS CHECKED	

SUMP PIT PLAN



NOTES:

1. Pit dimensions are optional.
2. The standpipe will be constructed by perforating a 12"-24" diameter corrugated metal or PVC pipe.
3. A base of 2" aggregate will be placed in the pit to a minimum depth of 12". After installing the standpipe, the pit surrounding the standpipe will then be backfilled with 2" aggregate.
4. The standpipe will extend 12" to 18" above the lip of the pit.
5. If discharge will be pumped directly to a storm drainage system, the standpipe will be wrapped with filter fabric before installation.
6. If desired, 1/4"-1/2" hardware cloth may be placed around the standpipe prior to attaching the filter fabric. This will increase the rate of water seepage into the pipe.

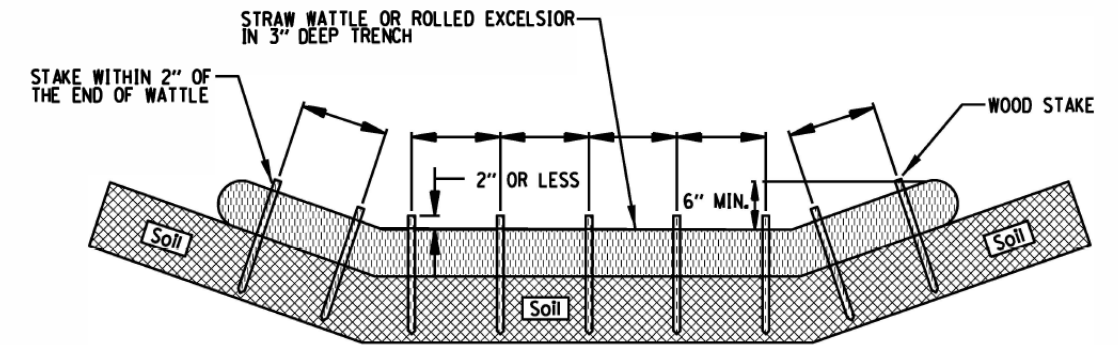
REFERENCE	Project	_____
	Designed	_____ Date _____
	Checked	_____ Date _____
	Approved	_____ Date _____



STANDARD DWG. NO.	IL-650
SHEET	1 OF 1
DATE	8-11-94

ROLLED EROSION CONTROL PRODUCTS

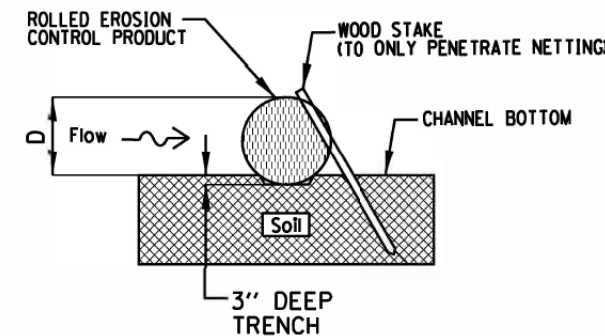
STAKING PATTERN GUIDE



NOTES:

1. OVERLAP MINIMUM IS THE DIAMETER OF THE ROLL.
2. 4' SPACING FOR WATTLES.
3. 2' SPACING FOR ROLLED EXCELSIOR.
4. OR SPACE ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

STAKE DETAIL



NOTES:

1. DRAWINGS ARE NOT TO SCALE.
2. ENDS OF WATTLES OR ROLLED EXCELSIOR SHALL BE TURNED AT LEAST 6" UPSLOPE.
3. RECOMMENDED STAKES ARE 1 1/8" WIDE x 1 1/8" THICK x 30" LONG.
4. STAKES SHALL NOT EXTEND ABOVE THE STRAW WATTLE MORE THAN 2".
5. SPACING: THE TOE OF THE UPSTREAM DITCH CHECK SHALL CREATE A HORIZONTAL LINE WITH THE TOP OF THE DOWNSTREAM DITCH CHECK.

REFERENCE	Project	_____
	Designed	_____ Date _____
	Checked	_____ Date _____
	Approved	_____ Date _____



STANDARD DWG. NO.	IUM-514
SHEET	1 OF 1
DATE	08-2-2019



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CHECKED	- KRK	REVISED	-
DATE	- 12/26/2023	REVISED	-

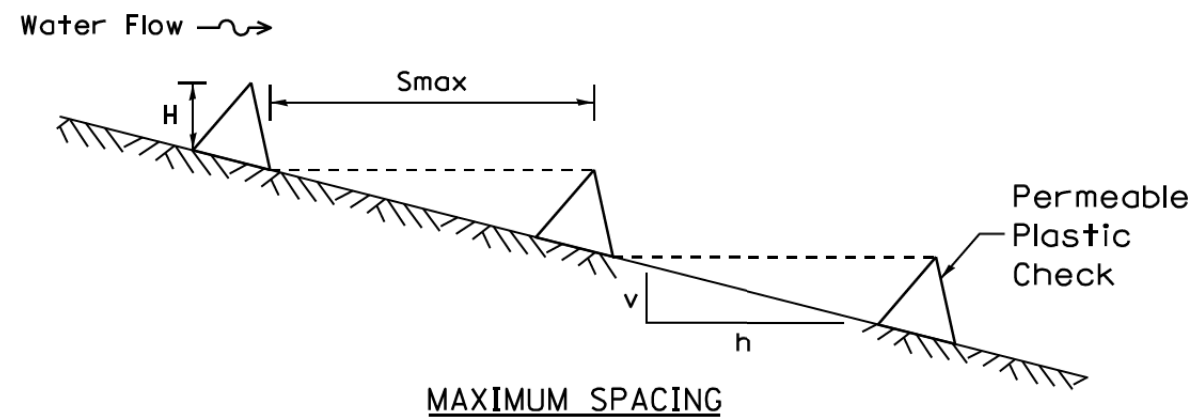
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHURCH ROAD AND MAIN STREET RESURFACING
EROSION CONTROL DETAILS

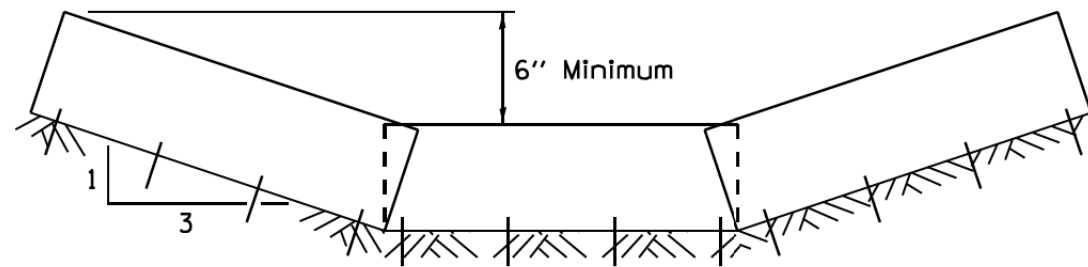
SHEET NO. 2 OF 6 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	25
3001 MAIN				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61K22	

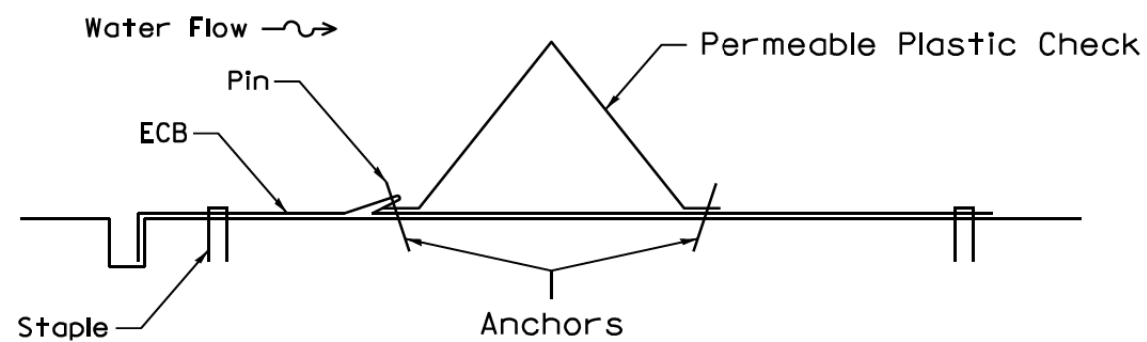
PLASTIC PERMEABLE CHECKS



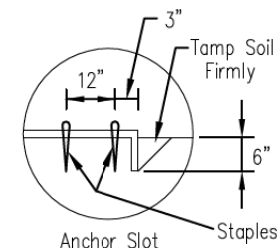
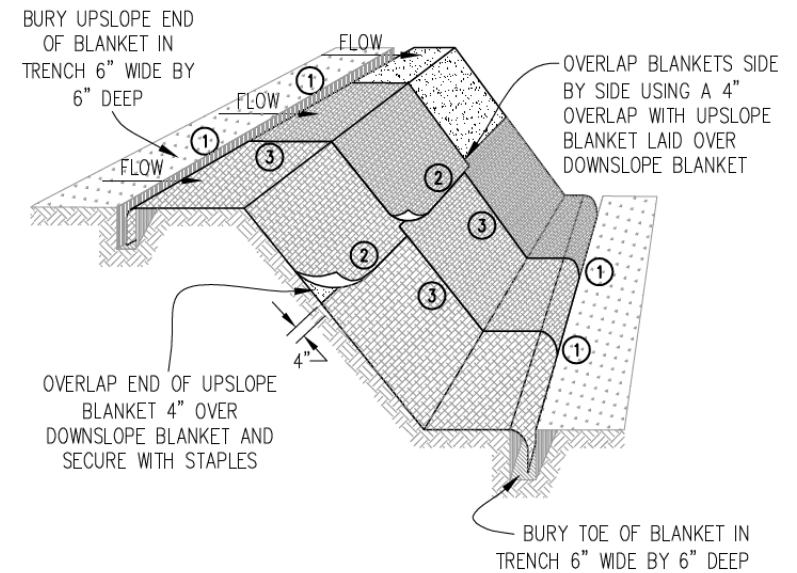
MAXIMUM SPACING



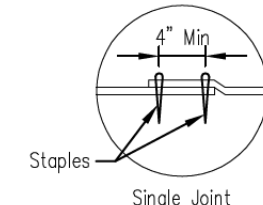
TYPICAL DITCH CROSS SECTION



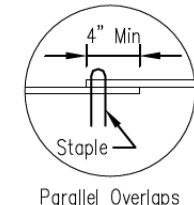
PLASTIC PERMEABLE CHECK CROSS SECTION



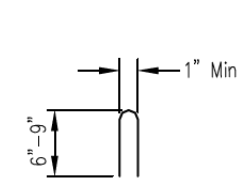
DETAIL 1



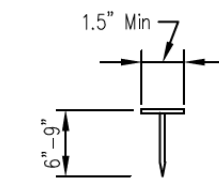
DETAIL 2



DETAIL 3



STAPLE DETAIL



PUSH PIN DETAIL

NOTES:

1. Staples shall be placed in a diamond pattern at 2 per s.y. for stiched blankets. Non-stiched shall use 4 staples per s.y. of material. This equates to 200 staples with stiched blanket and 400 stapels with non-stiched blanket per 100 s.y. of material.
2. Staple or push pin lengths shall be selected based on soil type and conditions. (minimum staple length is 6")
3. Erosion control material shall be placed in contact with the soil over a prepared seedbed.
4. All anchor slots shall be stapled at approximately 12" intervals.

REFERENCE	
Project	_____
Designed	_____ Date _____
Checked	_____ Date _____
Approved	_____ Date _____



STANDARD DWG. NO.
IUM-514
SHEET 1 OF 1
DATE 8-19-11

Sheet 1 of 1
Drawing No. IUM-530
File No. IUM-530

**EROSION CONTROL
BLANKET INSTALLATION DETAILS**

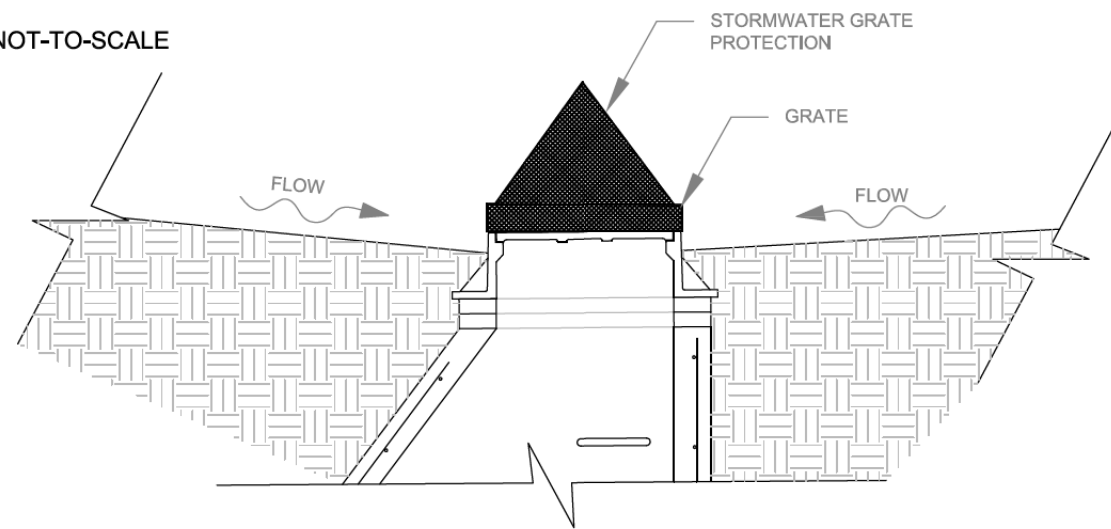
Designed	_____	Date	_____
Drawn	B. JOHNSON	11/08	
Checked	_____		
Approved	_____		

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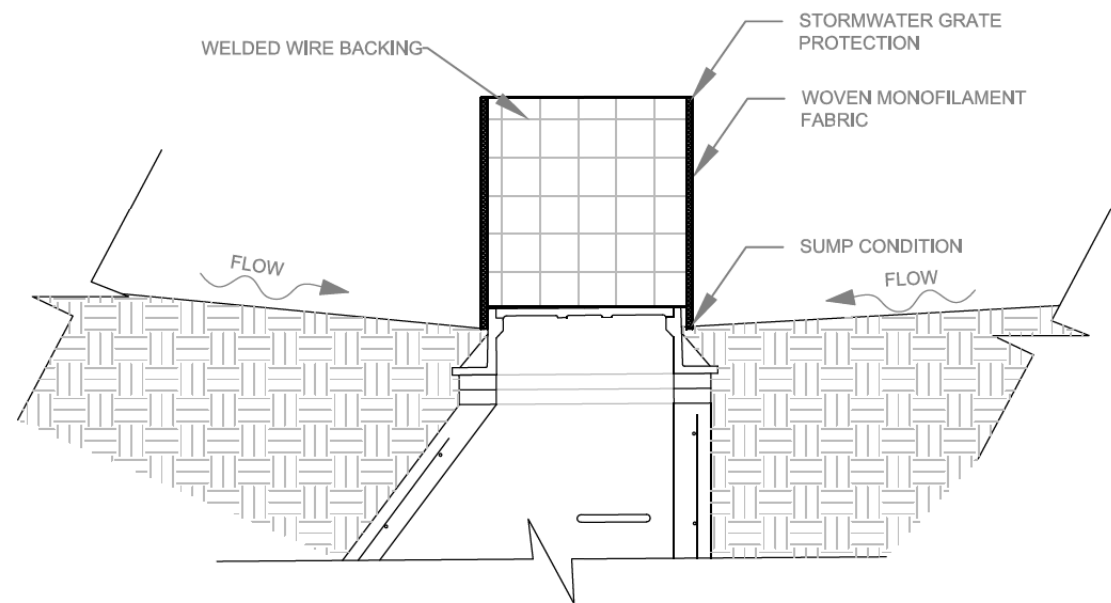
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STRUCTURE NOTATIONS CHKD	

MANUFACTURED ABOVE GRADE INLET PROTECTION

NOT-TO-SCALE



SECTION



SECTION

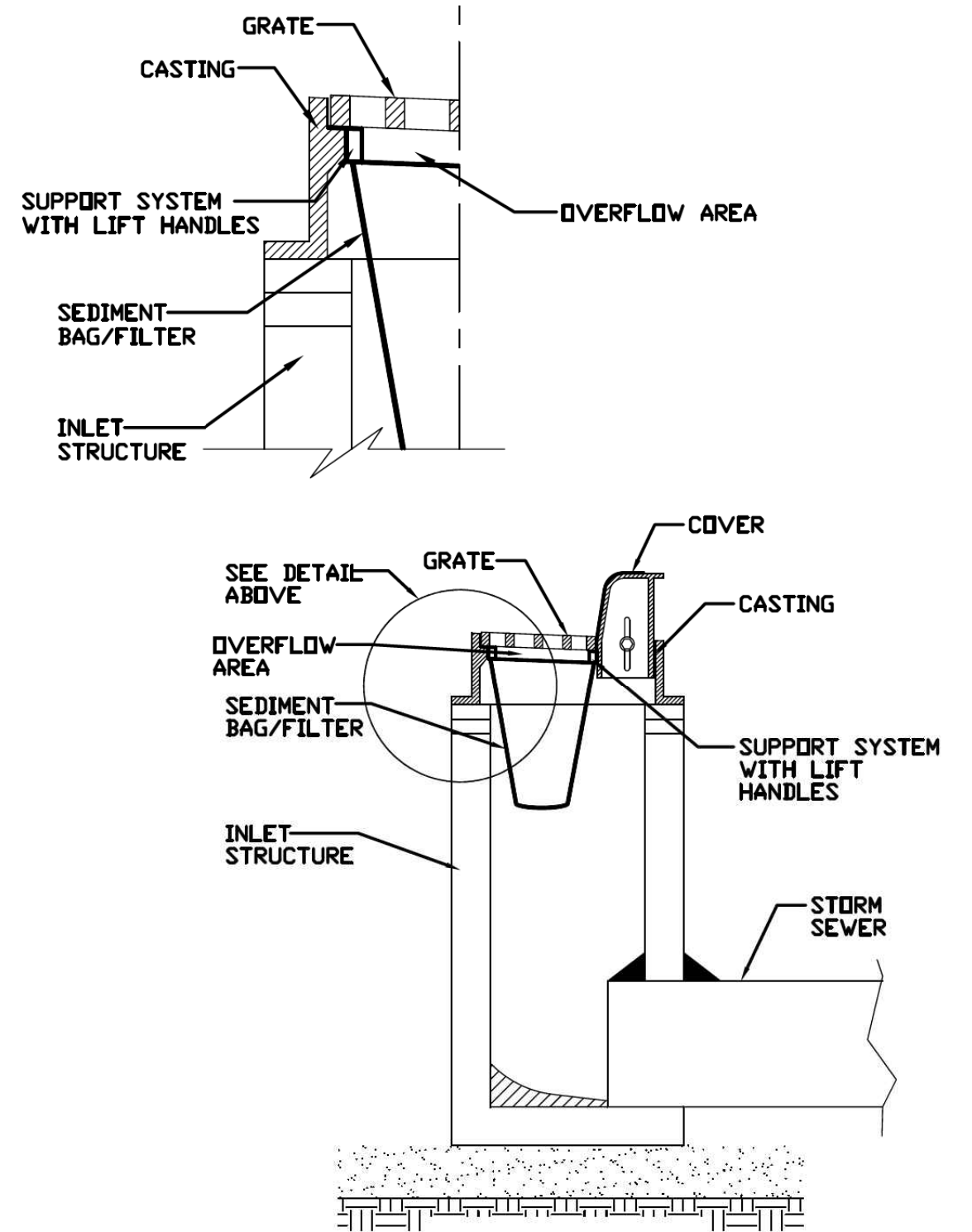
NOTES:

1. A maintenance schedule must maintain a sediment accumulation of less than 50% of the height of the inlet protection.
2. Device should be supported.
3. Fabric type should be IUM 592 Standard Table 1 Class 4

REFERENCE	
Project	_____
Designed	_____ Date _____
Checked	_____ Date _____
Approved	_____ Date _____

STANDARD DWG. NO.	IUM-531
SHEET	1 OF 1
DATE	03-27-17

INLET PROTECTION - PAVED AREAS DROP-IN PROTECTION



REFERENCE	
Project	_____
Designed	_____ Date _____
Checked	_____ Date _____
Approved	_____ Date _____



STANDARD DWG. NO.	IUM-561D
SHEET	1 OF 1
DATE	01-11-11



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CHECKED	- KRK	REVISED	-
DATE	- 12/26/2023	REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CHURCH ROAD AND MAIN STREET RESURFACING
EROSION CONTROL DETAILS**

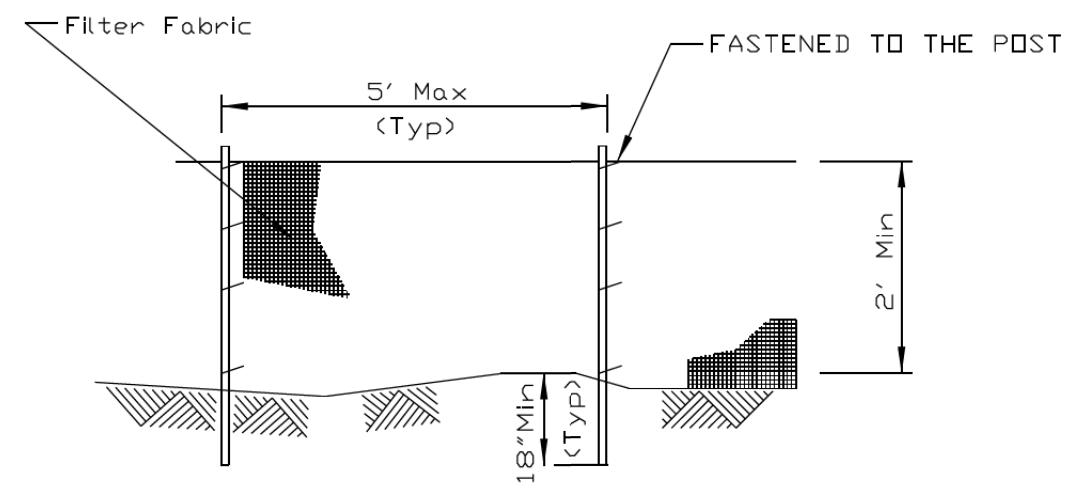
SHEET NO. 4 OF 6 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	27
3001 MAIN				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61K22	

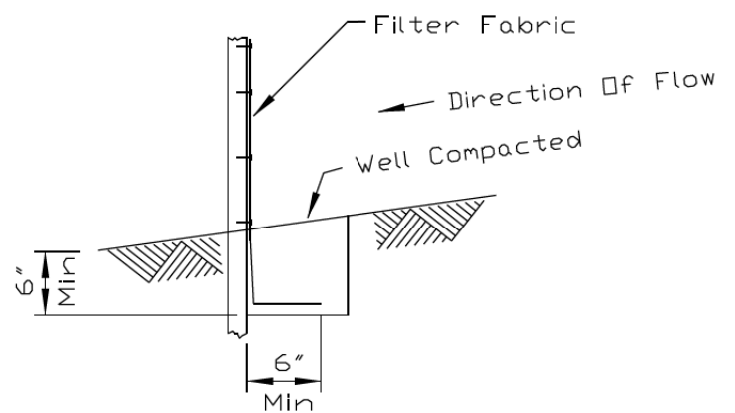
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STRUCTURE NOTATIONS CHKD	

SILT FENCE PLAN



ELEVATION



FABRIC ANCHOR DETAIL

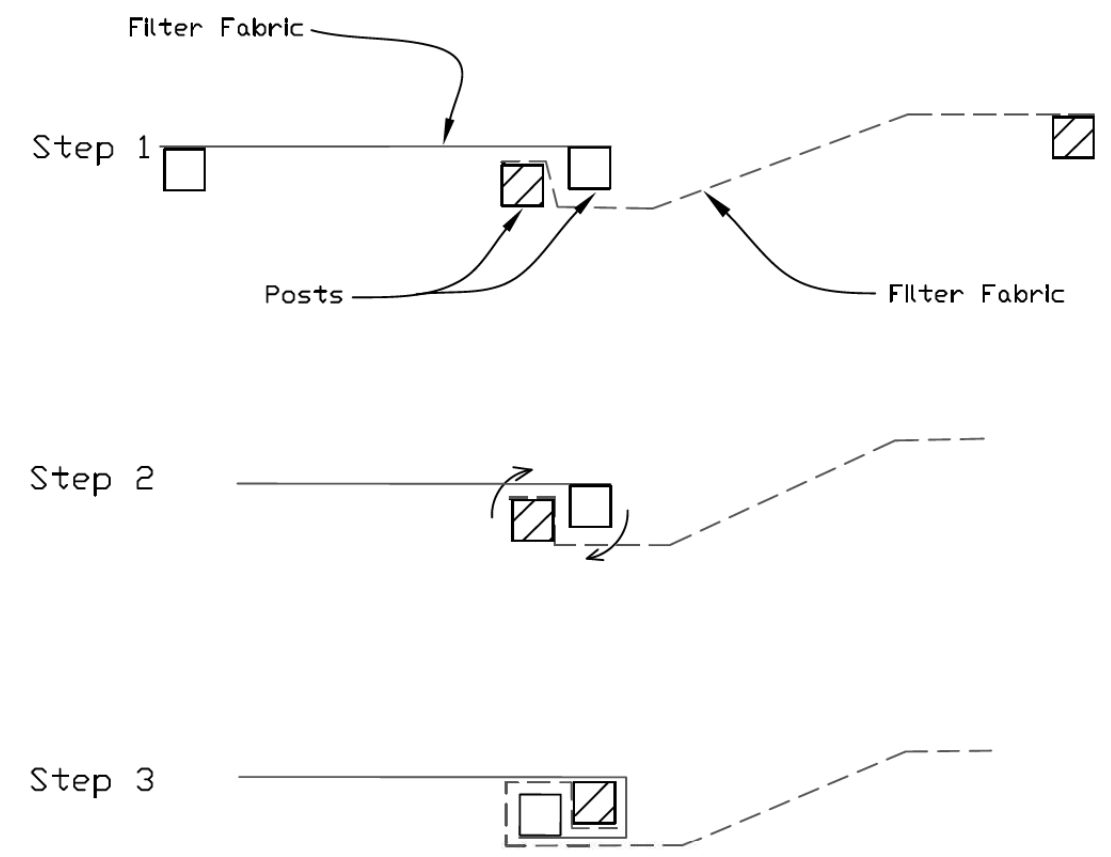
- NOTES:
1. Temporary sediment fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization.
 2. Filter fabric shall meet the requirements of material specification 592 Geotextile Table 1, Class 2.
 3. Fence posts shall be either standard steel post or wood post 2" X 2" nominal.

REFERENCE	
Project	_____
Designed	_____ Date _____
Checked	_____ Date _____
Approved	_____ Date _____



STANDARD DWG. NO.
IUM-620A
SHEET 1 OF 2
DATE 04-15-2021

SILT FENCE - SPLICING TWO FENCES



ATTACHING TWO SILT FENCES

1. Place the end post of the second fence inside the end post of the first fence.
2. Rotate both posts at least 180 degrees in a clockwise direction to create a tight seal with the fabric material.
3. Cut the fabric near the bottom of the stakes to accommodate the 6" flap.
4. Drive both posts a minimum of 18 inches into the ground and bury the flap.
5. Compact backfill (particularly at splices) completely to prevent stormwater piping.

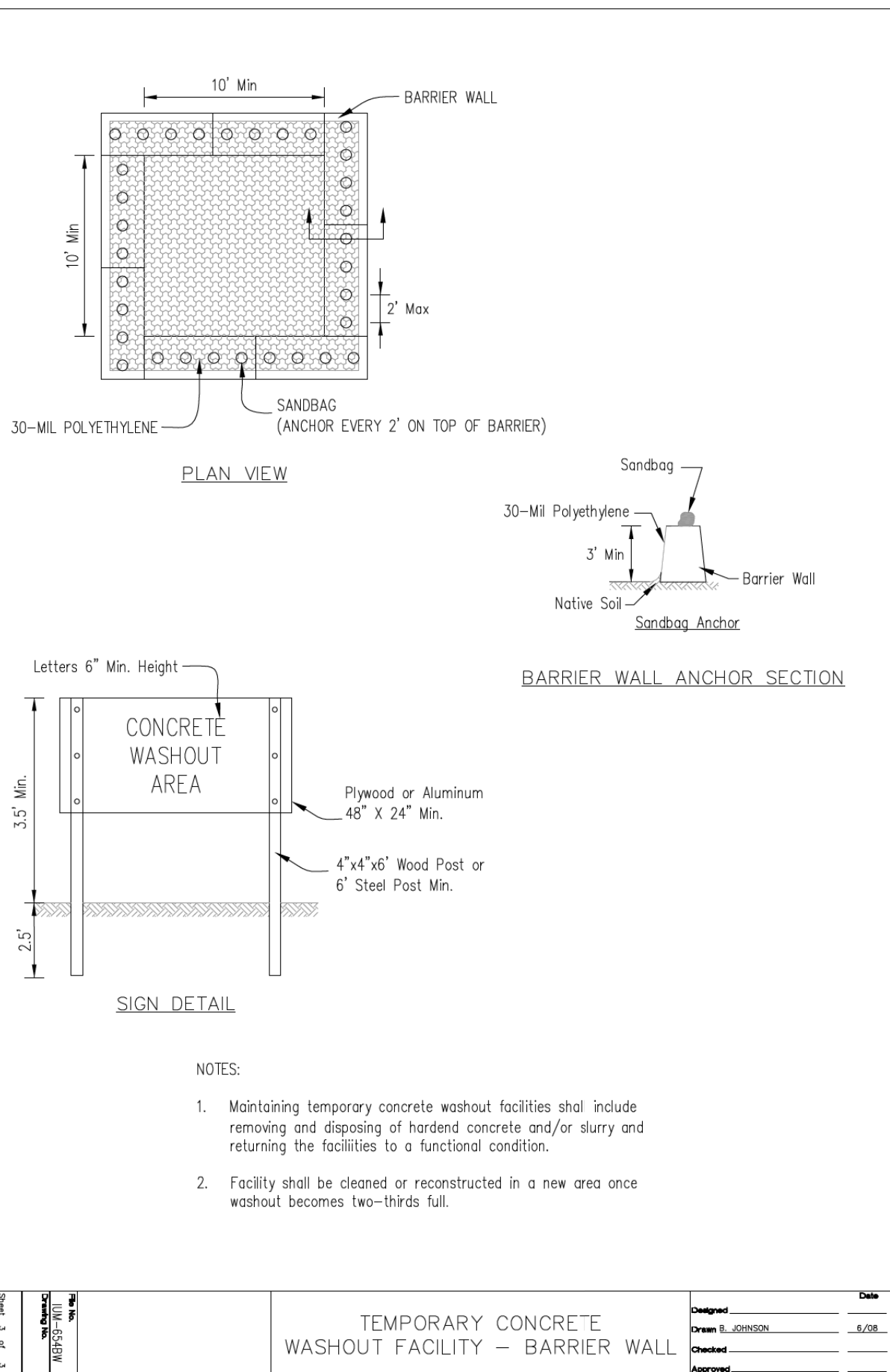
REFERENCE	
Project	_____
Designed	_____ Date _____
Checked	_____ Date _____
Approved	_____ Date _____



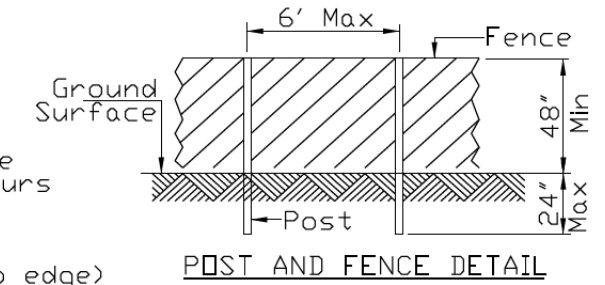
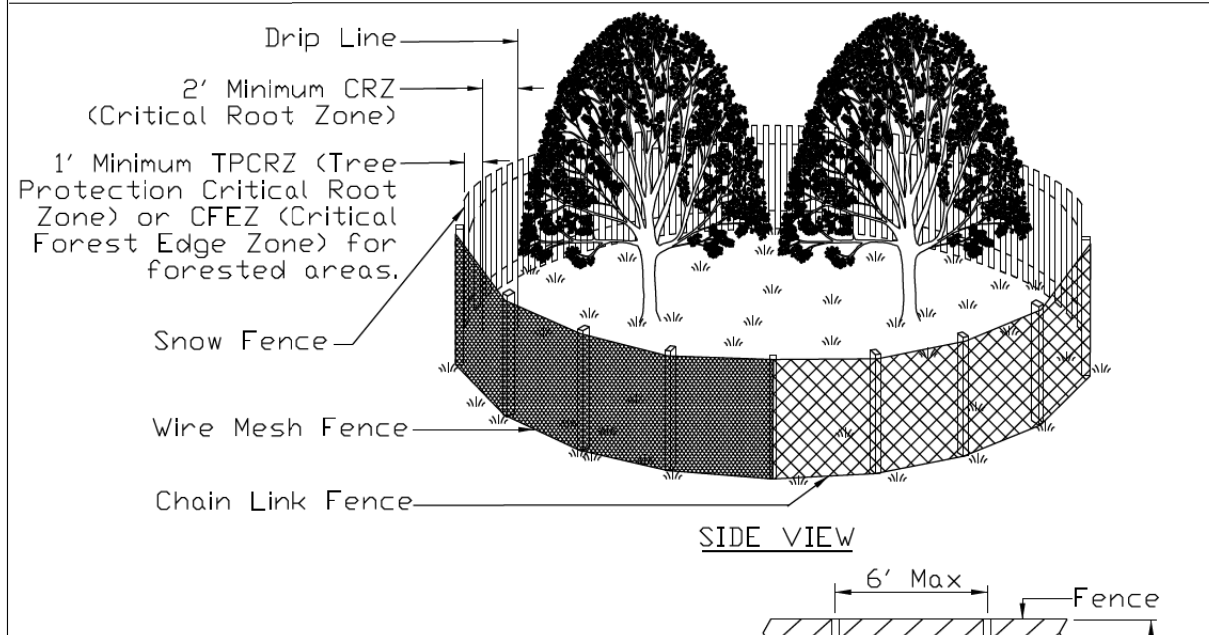
STANDARD DWG. NO.
IUM-620B(W)
SHEET 1 OF 1
DATE 3-16-2012

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TREE PROTECTION - FENCING (Moderately Urbanized to Open Space Areas)

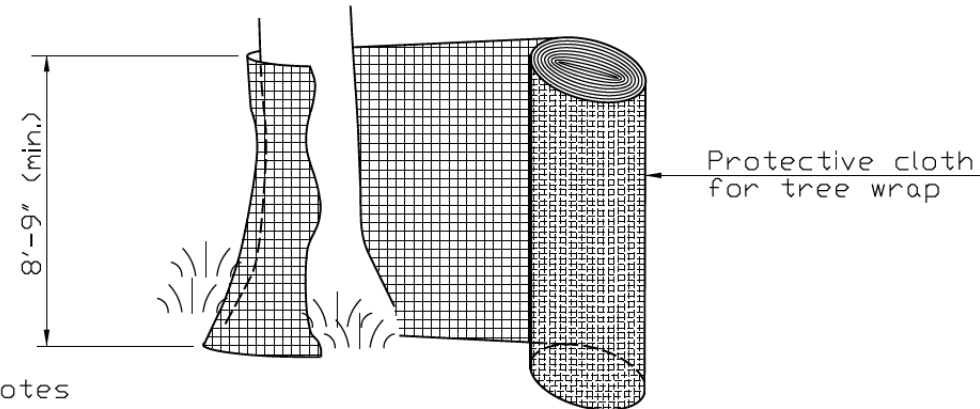
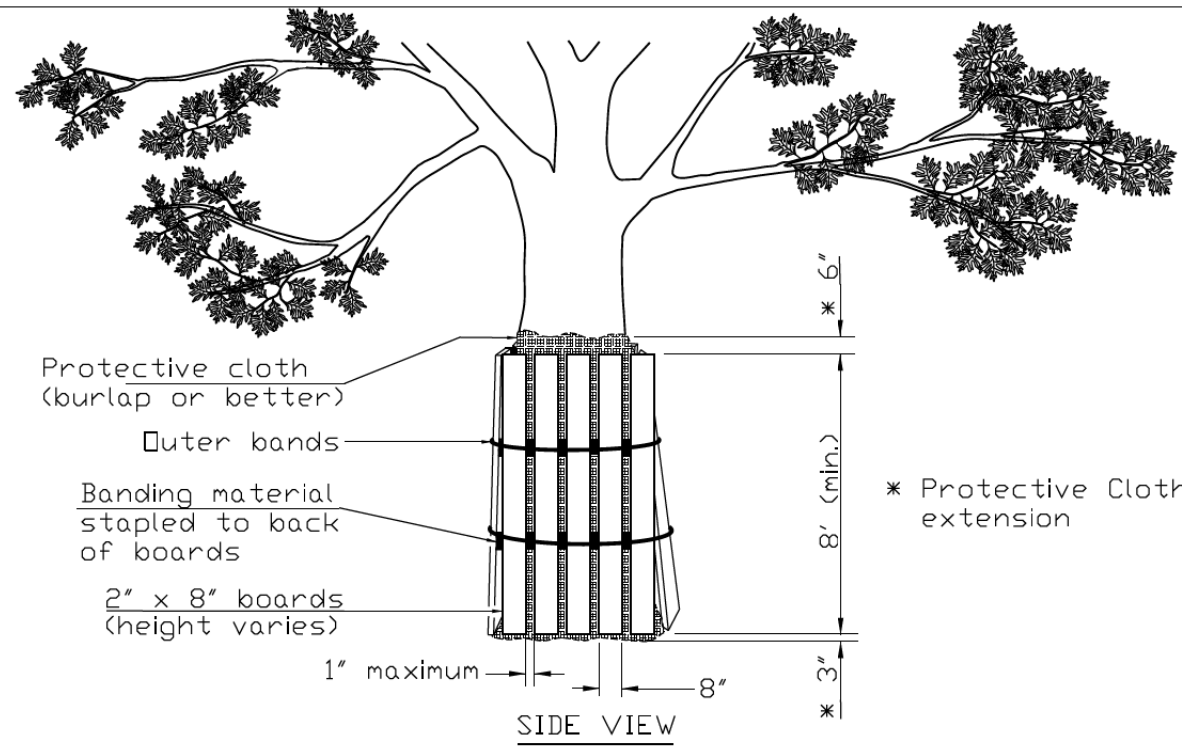


- NOTES:**
- Call J.U.L.I.E. (800-892-0123) for the location of existing utilities 48 hours prior to commencement of work.
 - The CRZ is located 2' from the farthest outreaching branch (drip edge) or the distance as determined by the ISA trunk diameter method or whichever is greater.
 - The fence shall be located 1' from the Critical Root Zone (CRZ) of the protected tree, thus creating the Tree Protection Critical Root Zone (TPCRZ) and the Critical Forest Edge Zone (CFEZ) for forested areas.
 - Fence Posts shall be either 6' steel posts @ 1.33Lbs./Ft. or 2" x 2" nominal wood posts.
 - For projects without highly significant or historical trees and that will last for less than 6 months duration, a non-treated wood lath snow fence or wire mesh fencing shall be used with appropriate posts that are securely anchored into the ground. For projects over 6 months in duration or trees considered significant or historical, a chain link fence with Construction Specification Chain Link Fence IUM 91 or better (as approved by the local Forester per local ordinances singularly or in tandem with the project Engineer) shall be used. Fencing shall be a minimum height of 4'. For chain link fencing, metal posts shall be placed 6' on center (OC) and the fencing securely anchored to the post.
 - Outside the TPCRZ or CFEZ, erosion and sediment control measures shall be installed to prevent sediment reaching the TPCRZ or the CFEZ. These measures shall extend out from the fence 10' and shall be continuous around the perimeter of the fence. These measures include, but are not limited to vegetative filter strip, rolled excelsior blankets and mulch with a 3" to 5" depth. Other measures may be used if approved by the Professional Forester, Certified Arborist or Horticulturalist. Installation shall cause no disturbance to soils.

REFERENCE	STANDARD DWG. NO.
Project _____	IUM-690-A
Designed _____ Date _____	SHEET 1 OF 1
Checked _____ Date _____	DATE 09-14-2017
Approved _____ Date _____	



TREE TRUNK PROTECTION



Notes

1. The contractor shall provide 2" x 8" boards banded continuously around each trunk with a protective cloth (such as burlap or better) placed between the boards and the tree to prevent scarring of the tree being protected. The height of the boards is variable due to height of tree being protected. Trees to be protected shall be shown in the plans or designated by the Professional Forester or Certified Arborist.
2. The protective cloth shall extend past both the top and bottom of the boards as shown in the detail. Width of wrap material varies. For fabric that does not meet the required height, fabric shall overlap a minimum of 6" and shall be spliced to avoid slippage.

REFERENCE
 Project _____
 Designed _____ Date _____
 Checked _____ Date _____
 Approved _____ Date _____



STANDARD DWG. NO.
 IUM-690-C
 SHEET 1 OF 1
 DATE 09-14-2017

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STRUCTURE NOTATIONS CHKD	

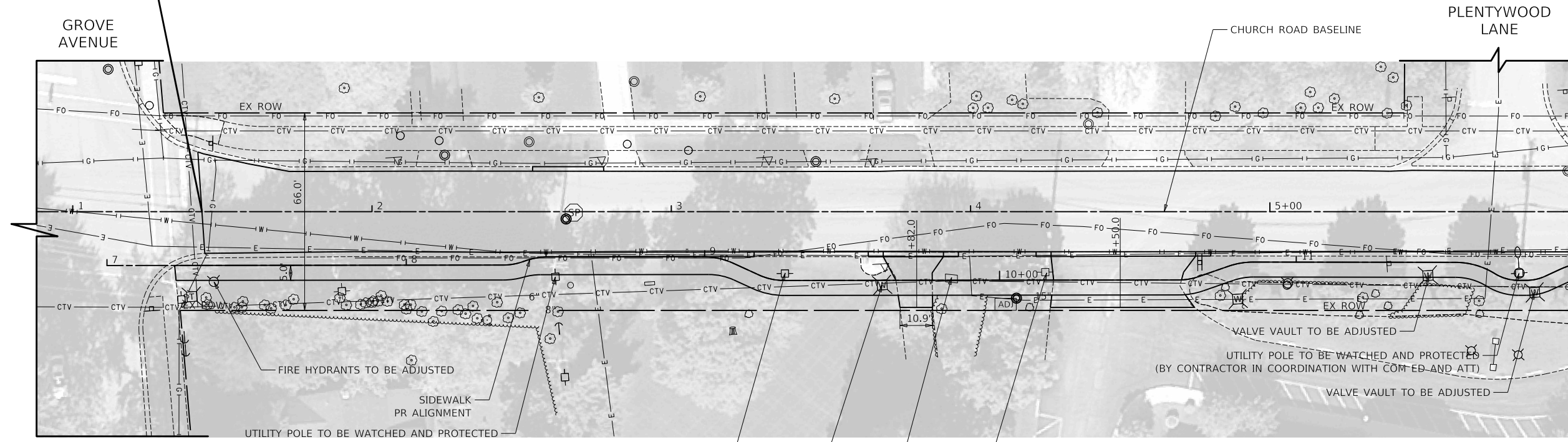


START OF PROJECT
CHURCH ROAD
STA 1+43.2

140 S CHURCH RD 136 S CHURCH RD 134 S CHURCH RD 128 S CHURCH RD 124 S CHURCH RD 702 PLENTYWOOD LN

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	CADD FILE NAME	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	



MATCH LINE STA 6+00
SEE SHEET NO 32

- LEGEND**
- SP FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
FRAMES AND GRATES TO BE ADJUSTED (SPECIAL)
 - ADJ VALVE VAULT TO BE ADJUSTED
DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
 - REC DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED
 - DETECTABLE WARNING

UTILITY POLE TO BE WATCHED AND PROTECTED (BY CONTRACTOR IN COORDINATION WITH COM ED AND ATT)

UTILITY POLE TO BE WATCHED AND PROTECTED (BY CONTRACTOR IN COORDINATION WITH COM ED AND ATT)

DOMESTIC WATER METER VAULT TO BE ADJUSTED (SPECIAL)

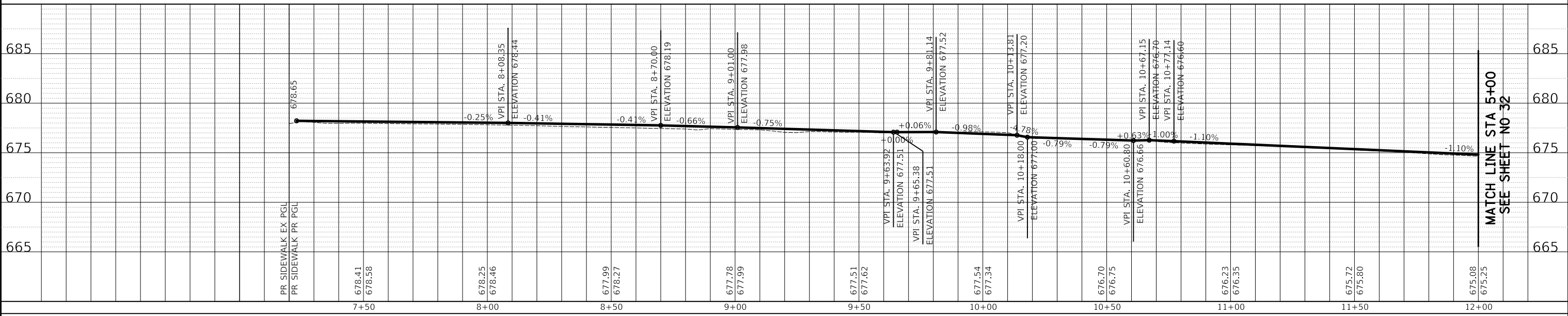
UTILITY TO BE RELOCATED (BY COMCAST)

UTILITY POLE TO BE WATCHED AND PROTECTED (BY CONTRACTOR IN COORDINATION WITH COM ED AND ATT)

101 S CHURCH RD MANAV SEVA MANDIR

131 S CHURCH RD

CHURCH ROAD



MATCH LINE STA 5+00
SEE SHEET NO 32

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DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



**CHURCH ROAD AND MAIN STREET RESURFACING
AND CHURCH ROAD SIDEWALK
DRAINAGE AND UTILITIES**

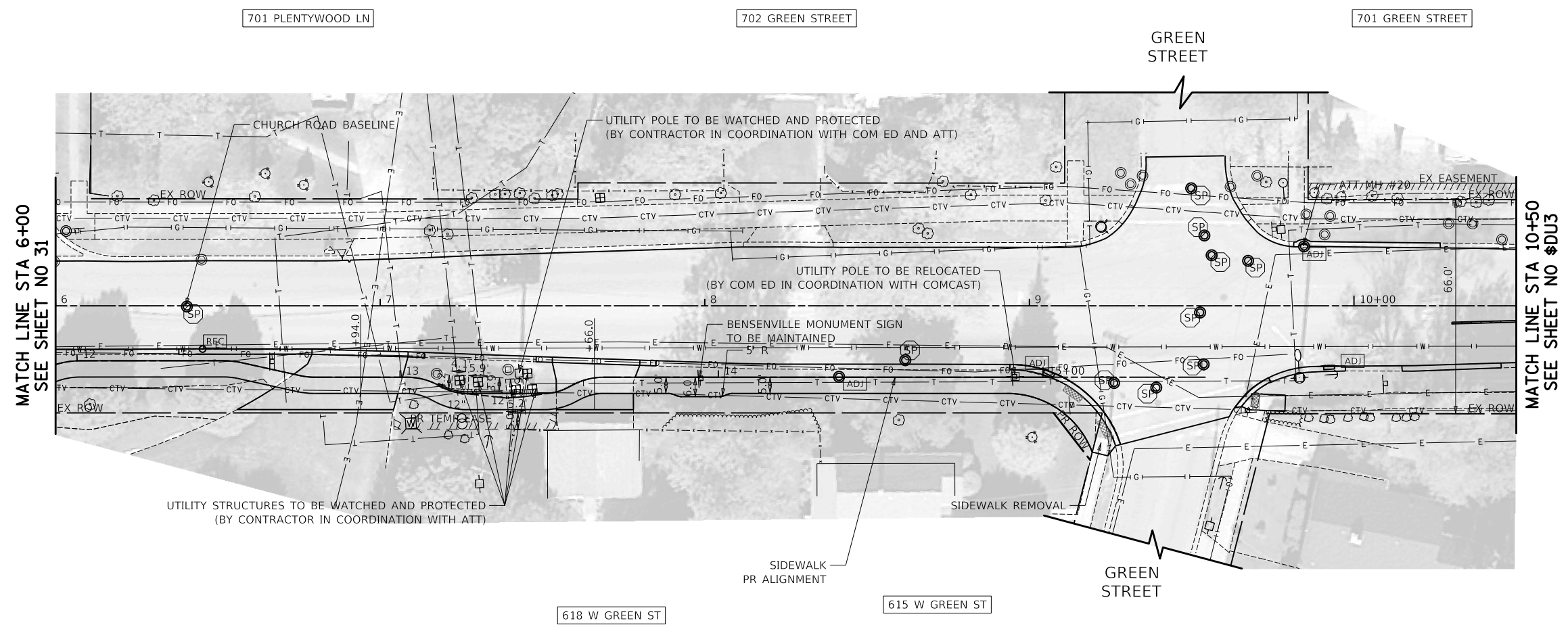
SHEET NO. 1 OF 6 SHEETS STA. 1+43.2 TO STA. 6+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	31
3001 MAIN				
CONTRACT NO. 61K22				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

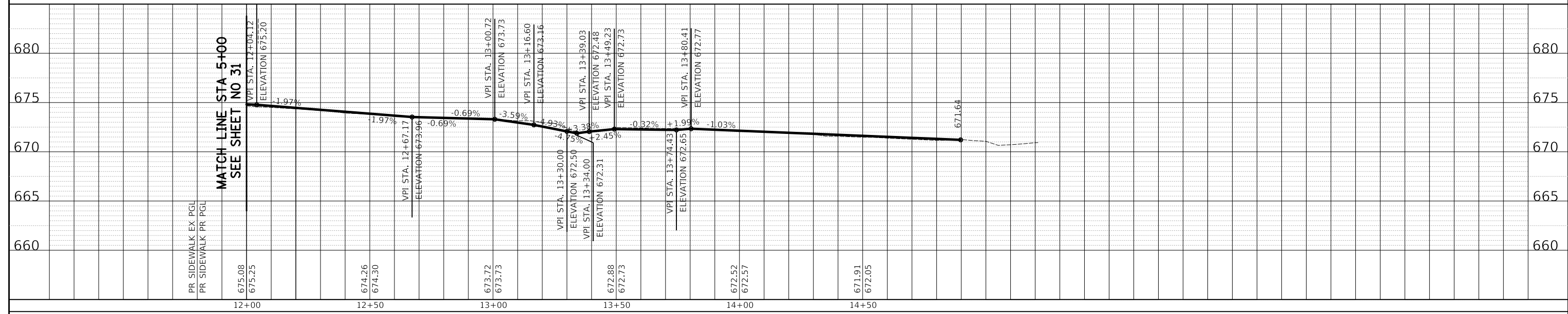


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	CHECKED	
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	NO. OF WAY CHECKED	
	STRUCTURE NOTATIONS CHECKED	



101 S CHURCH RD MANAV SEVA MANDIR CHURCH ROAD



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DESIGNED - JRR
 DRAWN - JRR
 CHECKED - KRK
 DATE - 12/26/2023

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



**CHURCH ROAD AND MAIN STREET RESURFACING
 AND CHURCH ROAD SIDEWALK
 DRAINAGE AND UTILITIES**

SHEET NO. 2 OF 6 SHEETS STA. 6+00 TO STA. 10+50

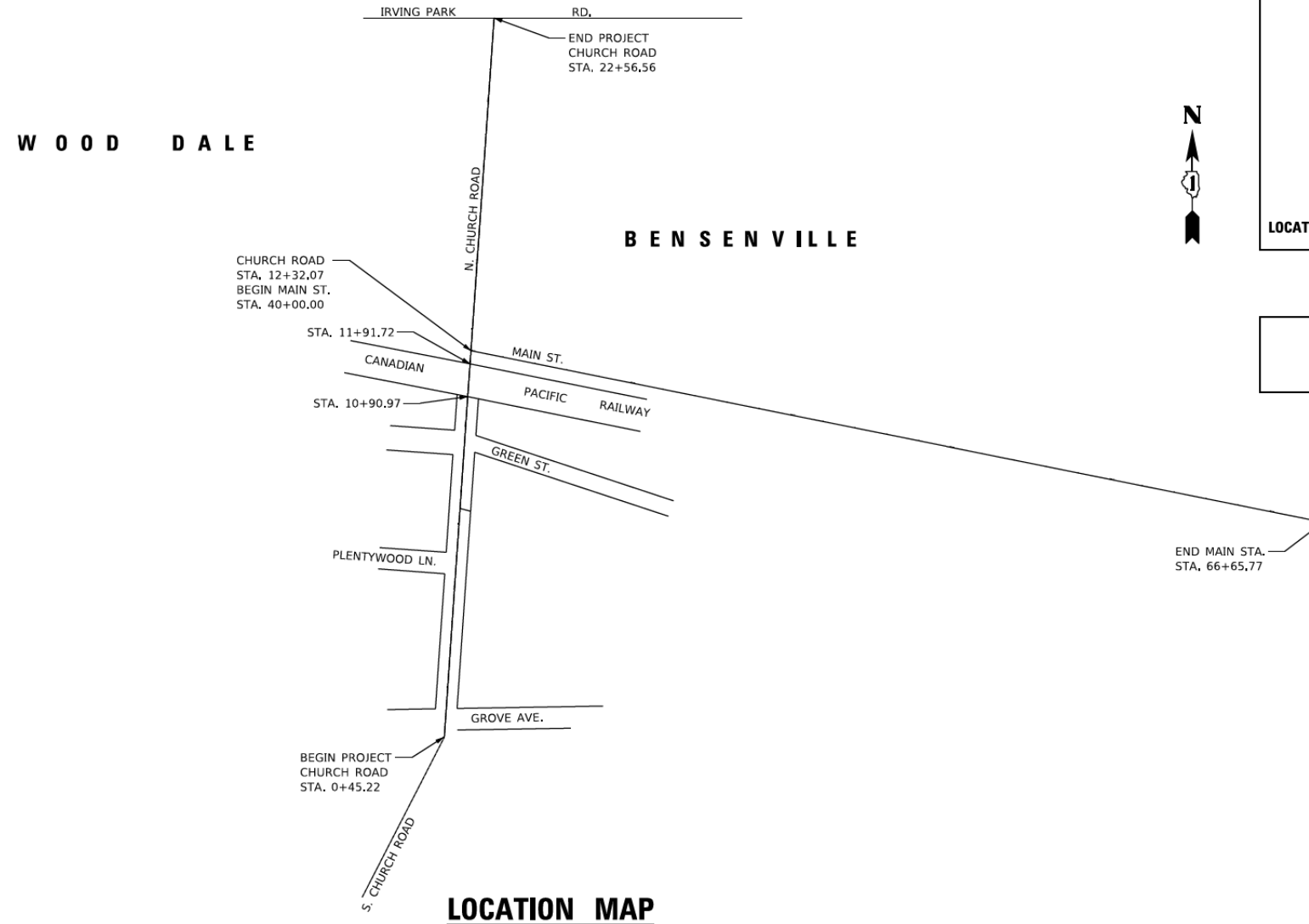
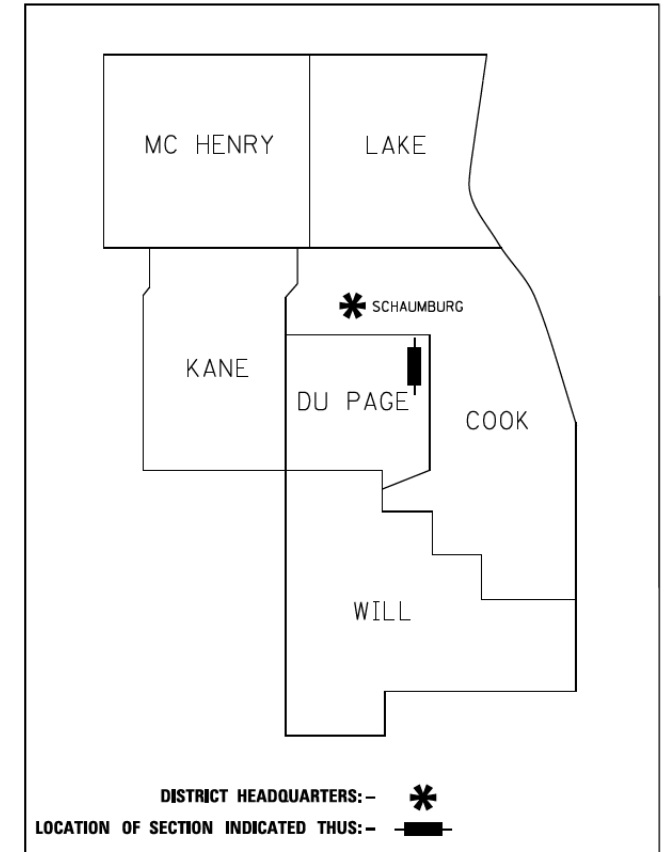
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	32
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLAT OF HIGHWAYS

CHURCH ROAD
SECTION 20-00100-00-RS
DUPAGE COUNTY
LIMITS: GROVE AVE. - IRVING PARK RD.
JOB NO.: C-91-093-21

PARCEL NUMBER	OWNER	SHEET NUMBER	PROPERTY ACQUIRED BY
0001TE	ROMEO D. PETROV	2	03-14-400-072
0002TE	CHICAGO TITLE LAND TRUST COMPANY AS SUCCESSOR TO BANK OF RAVENSWOOD AS TRUSTEE UNDER TRUST AGREEMENT DATED DECEMBER 10, 1987 KNOWN AS TRUST NO. 25-8926	3	03-14-400-065 03-14-400-071
0003 0003TE	MARIO KOVACEVIC, A SINGLE PERSON	3	03-14-400-001



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 129 CAPISTA DRIVE • SHOREWOOD, ILLINOIS 60404
 PH. (815) 744-6600 FAX (815) 744-0101
 website: www.ruettiger.com

R.T. & A. Dwg. No.: 2020-1256.00

IDOT USE ONLY

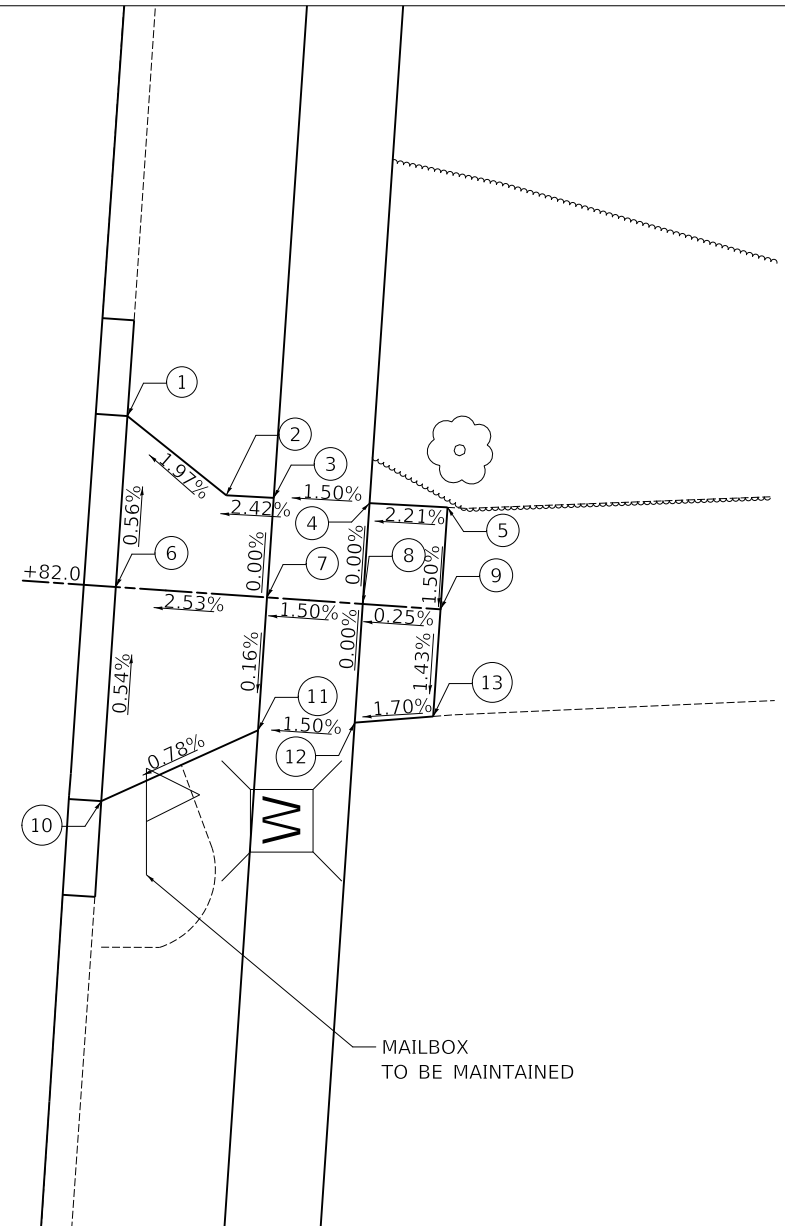
CHURCH ROAD BASELINE
STATION 3+82.0 DRIVEWAY GRADING PLAN

	STATION	OFFSET	ELEVATION
1	3+90.9	15.0' RT	(677.33)
2	3+87.2	20.5' RT	677.46
3	3+87.2	22.9' RT	677.52
4	3+87.3	27.9' RT	677.59
5	3+87.3	32.0' RT	(677.68)
6	3+82.0	15.0' RT	(677.38)
7	3+82.0	22.9' RT	677.52
8	3+82.0	27.9' RT	677.59
9	3+82.0	32.0' RT	(677.60)
10	3+70.8	15.1' RT	(677.44)
11	3+75.1	22.9' RT	677.51
12	3+75.8	27.9' RT	677.59
13	3+76.4	32.0' RT	(677.52)



PLAN	SURVEYED	DATE
NO. _____	BY _____	DATE _____
NOTE BOOK NO. _____	PLOTTED / CHECKED	
	BY _____	
	DATE _____	
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	BY _____	
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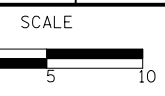
PROFILE	SURVEYED	DATE
NO. _____	BY _____	DATE _____
NOTE BOOK NO. _____	PLOTTED / CHECKED	
	BY _____	
	DATE _____	
	STRUCTURE NOTATIONS CHKD	



LEGEND

X% / XXX.XX PROPOSED SLOPE / ELEVATION
 (X%) / (XXX.XX) EXISTING SLOPE / ELEVATION

CHURCH ROAD



**CHURCH ROAD AND MAIN STREET RESURFACING
CHURCH ROAD DRIVEWAY LAYOUT STA 3 + 82.0**

SHEET NO. 1 OF 6 SHEETS

CIVILTECH
 Two Pierce Place, Suite 1400
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 Tel: 630.773.3900 Fax: 630.773.3975
 www.civiltechinc.com

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CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	36
3001 MAIN	CONTRACT NO. 61K22			

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

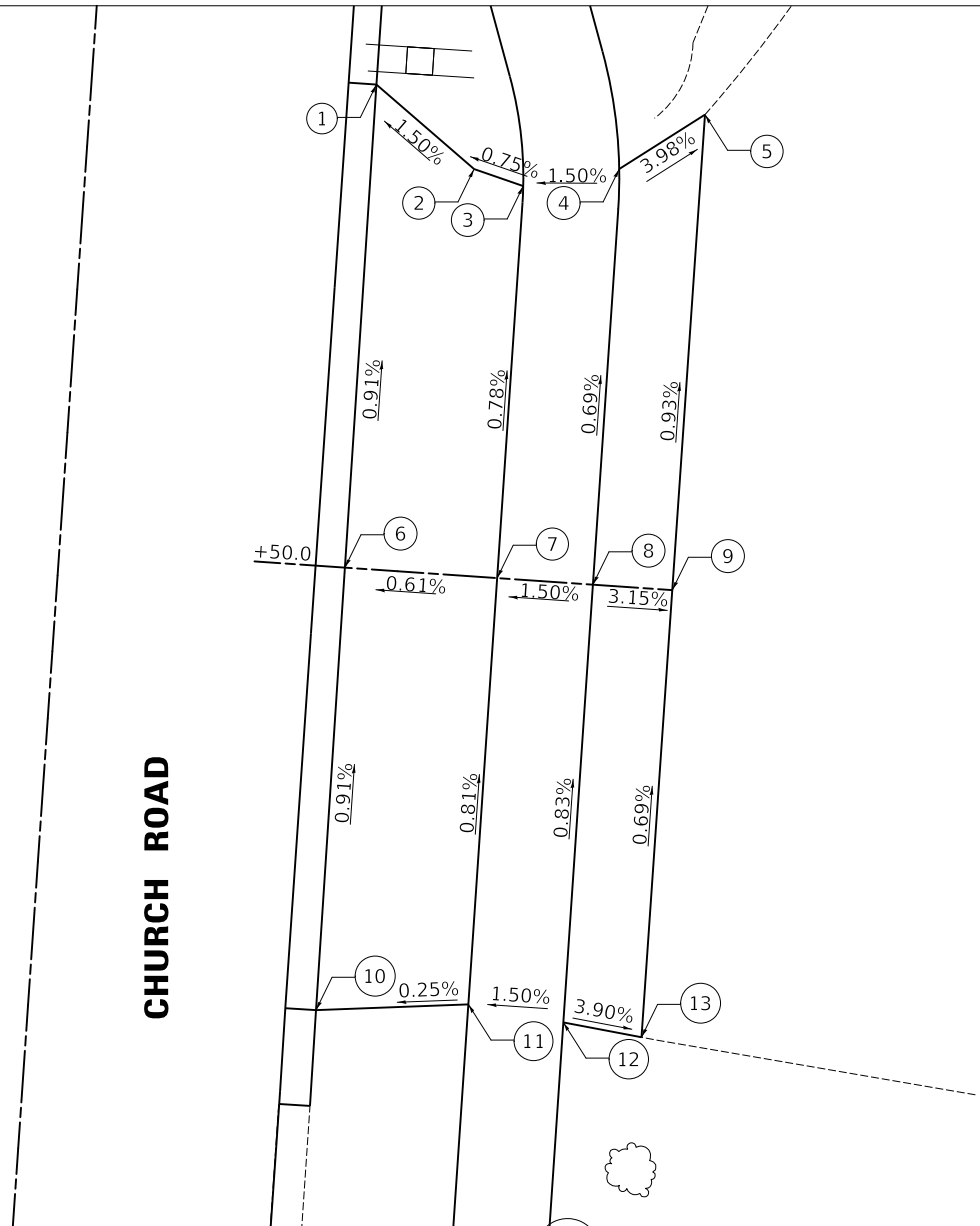


CHURCH ROAD BASELINE
STATION 4+50.0 DRIVEWAY GRADING PLAN

	STATION	OFFSET	ELEVATION
1	4+75.2	14.8' RT	(676.54)
2	4+71.2	20.2' RT	676.64
3	4+70.5	22.8' RT	676.66
4	4+71.7	27.7' RT	676.74
5	4+74.8	32.0' RT	(676.53)
6	4+50.0	14.9' RT	(676.77)
7	4+50.0	22.9' RT	676.82
8	4+50.0	27.9' RT	676.89
9	4+50.0	32.0' RT	(676.76)
10	4+26.9	15.0' RT	(676.98)
11	4+27.7	22.9' RT	677.00
12	4+27.1	27.9' RT	677.08
13	4+26.7	32.0' RT	(676.92)

PLAN	DATE
SURVEYED _____	
PLOTTED _____	
CHECKED _____	
BY _____	
NO. _____	

PROFILE	DATE
SURVEYED _____	
PLOTTED _____	
CHECKED _____	
BY _____	
NO. _____	



LEGEND

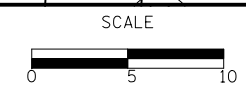
X% / XXX.XX PROPOSED SLOPE / ELEVATION
(X%) / (XXX.XX) EXISTING SLOPE / ELEVATION



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**CHURCH ROAD AND MAIN STREET RESURFACING
CHURCH ROAD DRIVEWAY LAYOUT STA 4 + 50.0**

SHEET NO. 2 OF 6 SHEETS

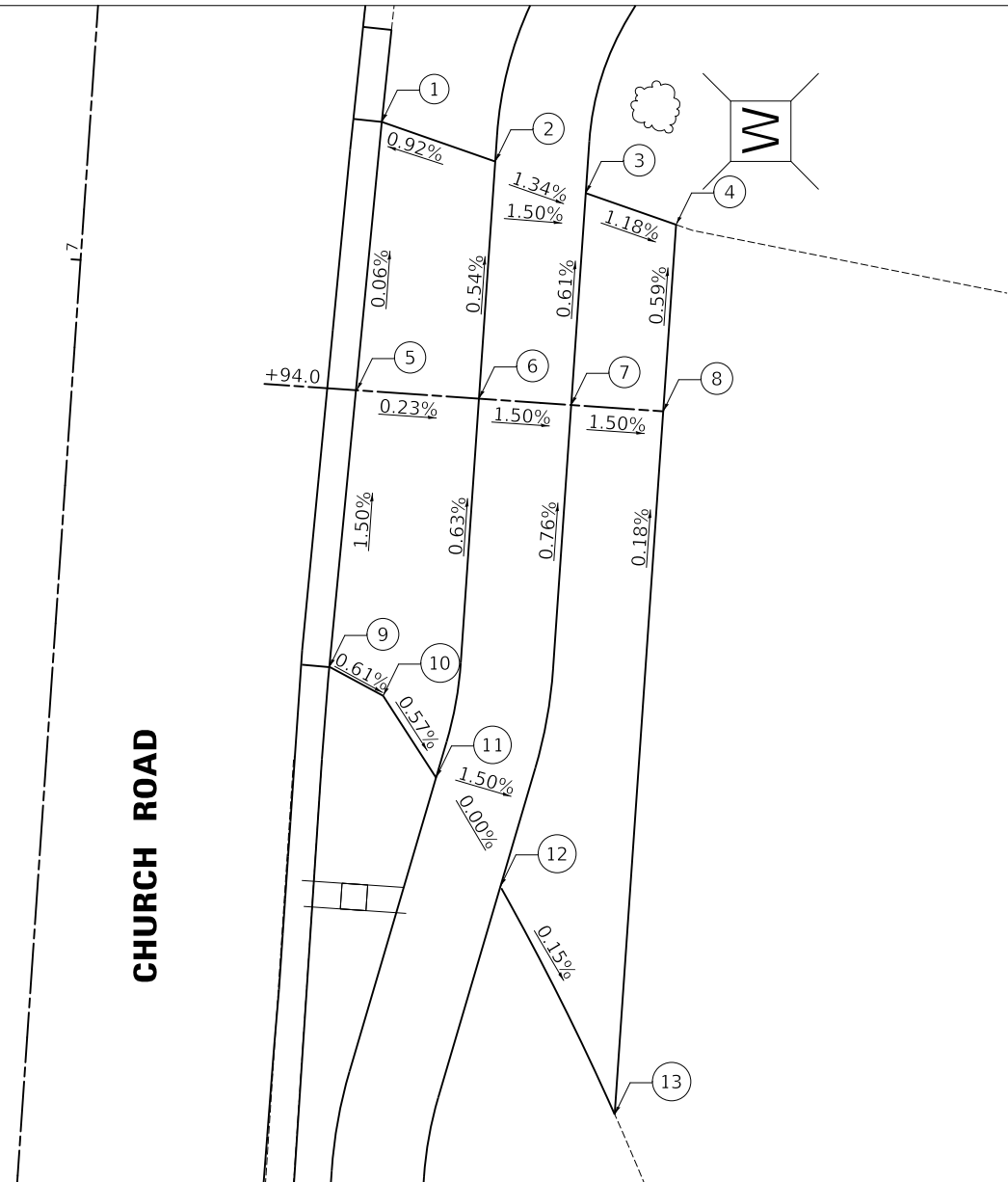
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	37
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
NO.	BY	

PROFILE	SURVEYED	DATE
NO.	BY	

CHURCH ROAD BASELINE
STATION 6+94.0 DRIVEWAY GRADING PLAN

	STATION	OFFSET	ELEVATION
1	7+08.6	15.7' RT	(673.67)
2	7+06.8	22.0' RT	673.73
3	7+05.5	27.0' RT	673.66
4	7+04.1	31.9' RT	(673.60)
5	6+94.0	15.4' RT	(673.82)
6	6+94.0	22.0' RT	673.80
7	6+94.0	27.0' RT	673.73
8	6+94.0	31.9' RT	(673.66)
9	6+79.0	15.0' RT	(673.98)
10	6+77.6	17.9' RT	673.96
11	6+73.4	21.1' RT	673.93
12	6+67.8	25.0' RT	673.93
13	6+55.9	32.0' RT	(673.73)



LEGEND

X% / XXX.XX PROPOSED SLOPE / ELEVATION
(X%) / (XXX.XX) EXISTING SLOPE / ELEVATION

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CHURCH ROAD AND MAIN STREET RESURFACING
CHURCH ROAD DRIVEWAY LAYOUT STA 6 + 94.0

SHEET NO. 3 OF 6 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	38
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



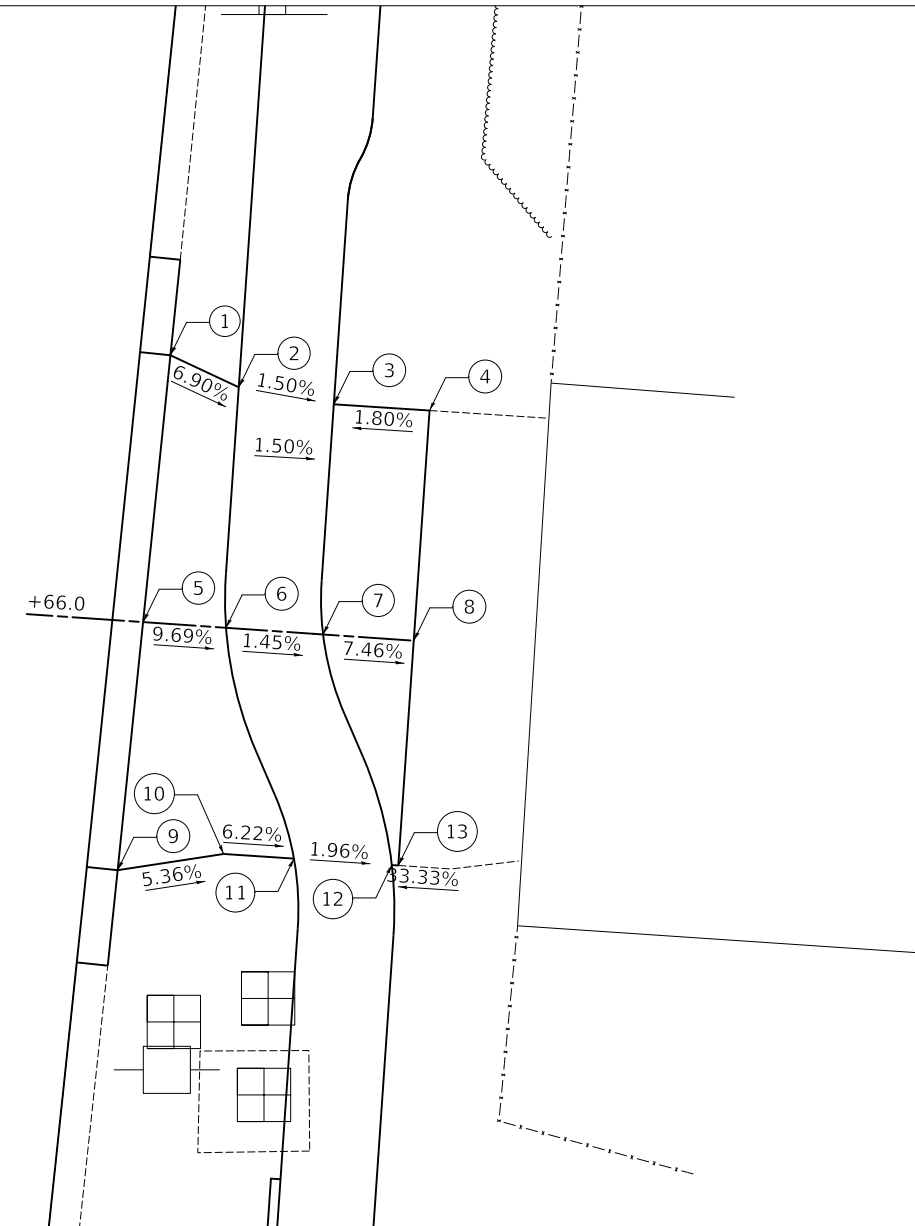
CHURCH ROAD BASELINE
STATION 7+66.0 DRIVEWAY GRADING PLAN

	STATION	OFFSET	ELEVATION
1	7+80.0	18.3' RT	(672.92)
2	7+78.5	22.0' RT	672.65
3	7+78.0	27.0' RT	672.58
4	7+78.0	32.0' RT	(672.67)
5	7+66.0	17.9' RT	(673.10)
6	7+66.0	22.2' RT	672.67
7	7+66.0	27.3' RT	672.60
8	7+66.0	32.0' RT	(672.21)
9	7+53.0	17.4' RT	(673.26)
10	7+54.2	22.9' RT	672.96
11	7+54.2	26.6' RT	672.73
12	7+54.3	31.7' RT	672.63
13	7+54.3	32.0' RT	(672.73)

PLAN	DATE
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CHECKED	
PT. OF WAY CHECKED	
CADD FILE NAME	

PROFILE	DATE
SURVEYED	
PLOTTED	
CHECKED	
STRUCTURE NOTATIONS CHKD	

CHURCH ROAD



LEGEND

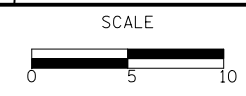
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(X%) / (XXX.XX) EXISTING SLOPE / ELEVATION



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**CHURCH ROAD AND MAIN STREET RESURFACING
CHURCH ROAD DRIVEWAY LAYOUT STA 7 + 66.0**

SHEET NO. 4 OF 6 SHEETS

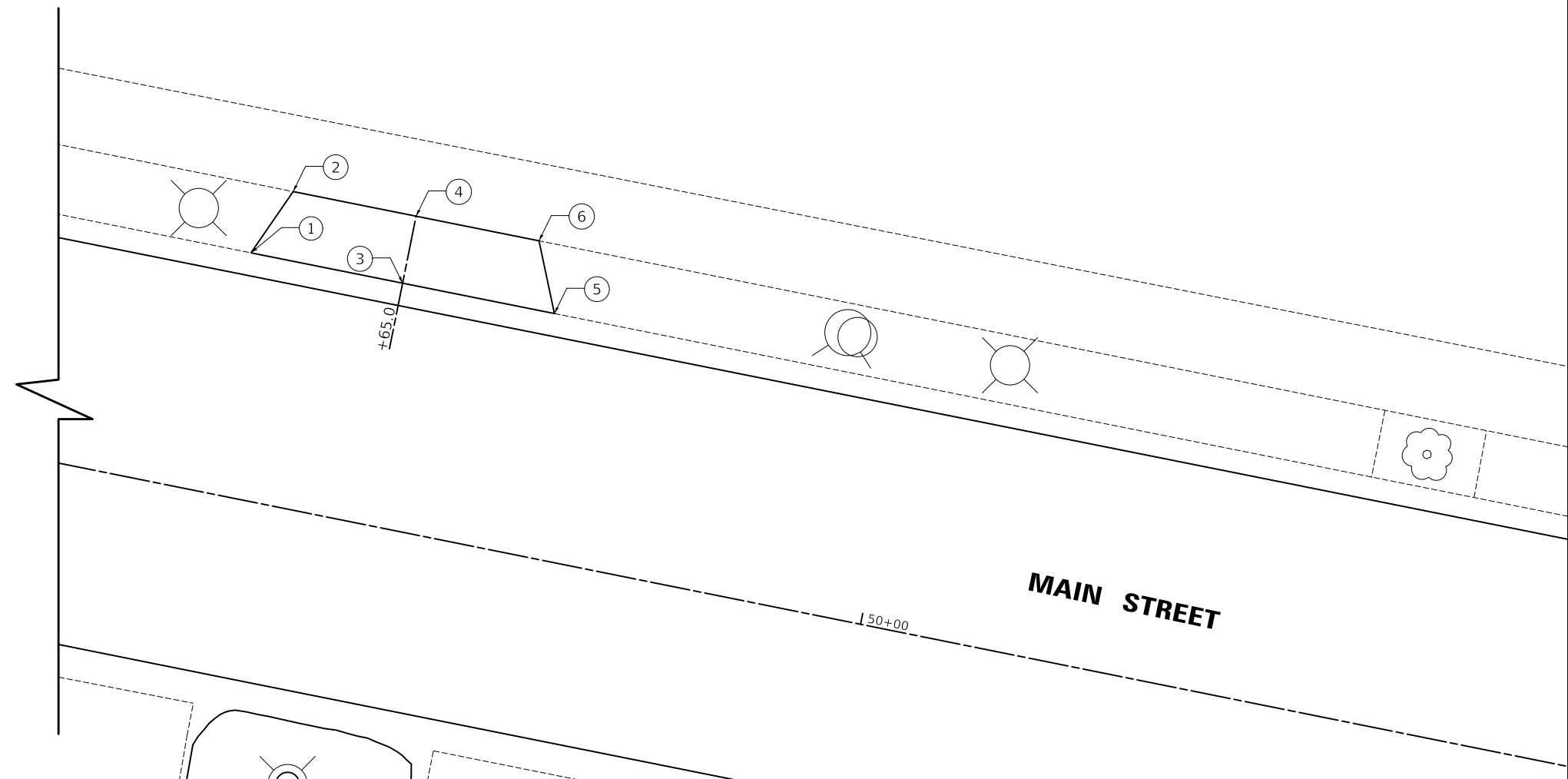
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	39
3001 MAIN	CONTRACT NO. 61K22			

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
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	NO. OF WAY CHECKED	
	ADD. FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
	BY	
	DATE	
	STRUCTURE NOTATIONS CHKD	

MAIN STREET BASELINE STATION 49+65.0 DRIVEWAY LAYOUT		
	STATION	OFFSET
1	49+54.5	16.5' LT
2	49+56.5	21.2' LT
3	49+65.0	16.6' LT
4	49+65.0	21.2' LT
5	49+75.5	16.6' LT
6	49+73.5	21.2' LT



LEGEND

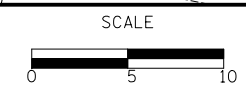
X% / XXX.XX PROPOSED SLOPE / ELEVATION
 (X%) / (XXX.XX) EXISTING SLOPE / ELEVATION



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**CHURCH ROAD AND MAIN STREET RESURFACING
 MAIN STREET DRIVEWAY LAYOUT STA 49 + 65.0**

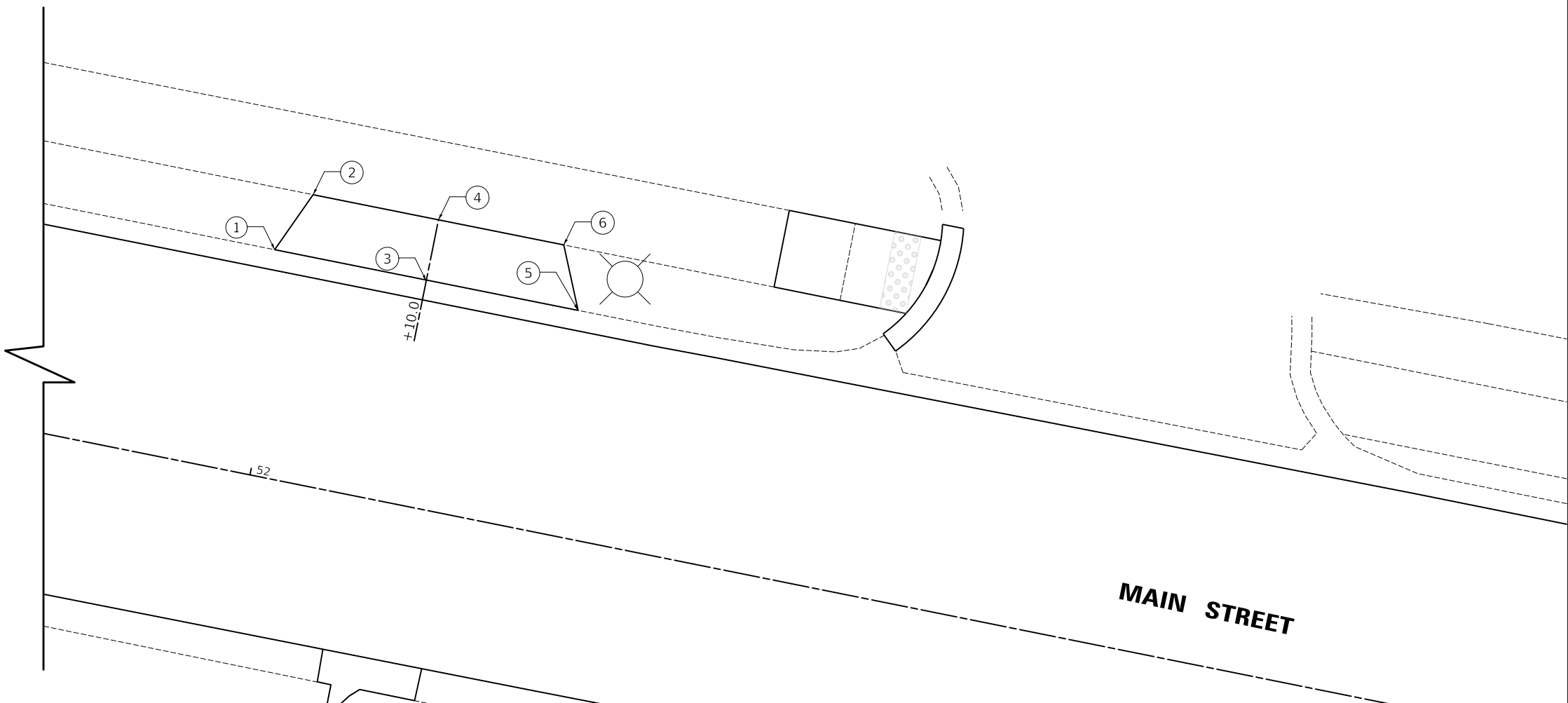
SHEET NO. 5 OF 6 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	40
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
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	ADD. FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	
	CHECKED	
	BY	
	DATE	
	STRUCTURE NOTATIONS CHKD	

MAIN STREET BASELINE STATION 52+10.0 DRIVEWAY LAYOUT		
	STATION	OFFSET
1	51+98.5	16.8' LT
2	52+00.5	21.3' LT
3	52+10.0	16.8' LT
4	52+10.0	21.3' LT
5	52+21.5	16.8' LT
6	52+19.5	21.4' LT



LEGEND

X% / XXX.XX PROPOSED SLOPE / ELEVATION
 (X%) / (XXX.XX) EXISTING SLOPE / ELEVATION



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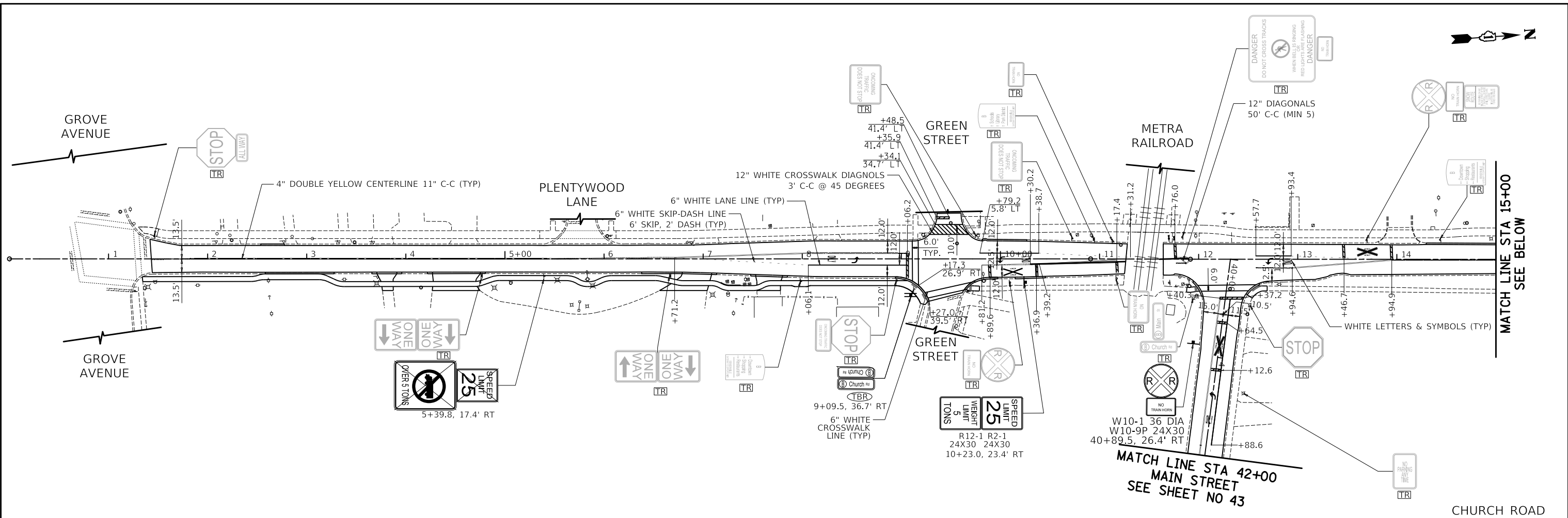


**CHURCH ROAD AND MAIN STREET RESURFACING
 MAIN STREET DRIVEWAY LAYOUT STA 52+10.0**

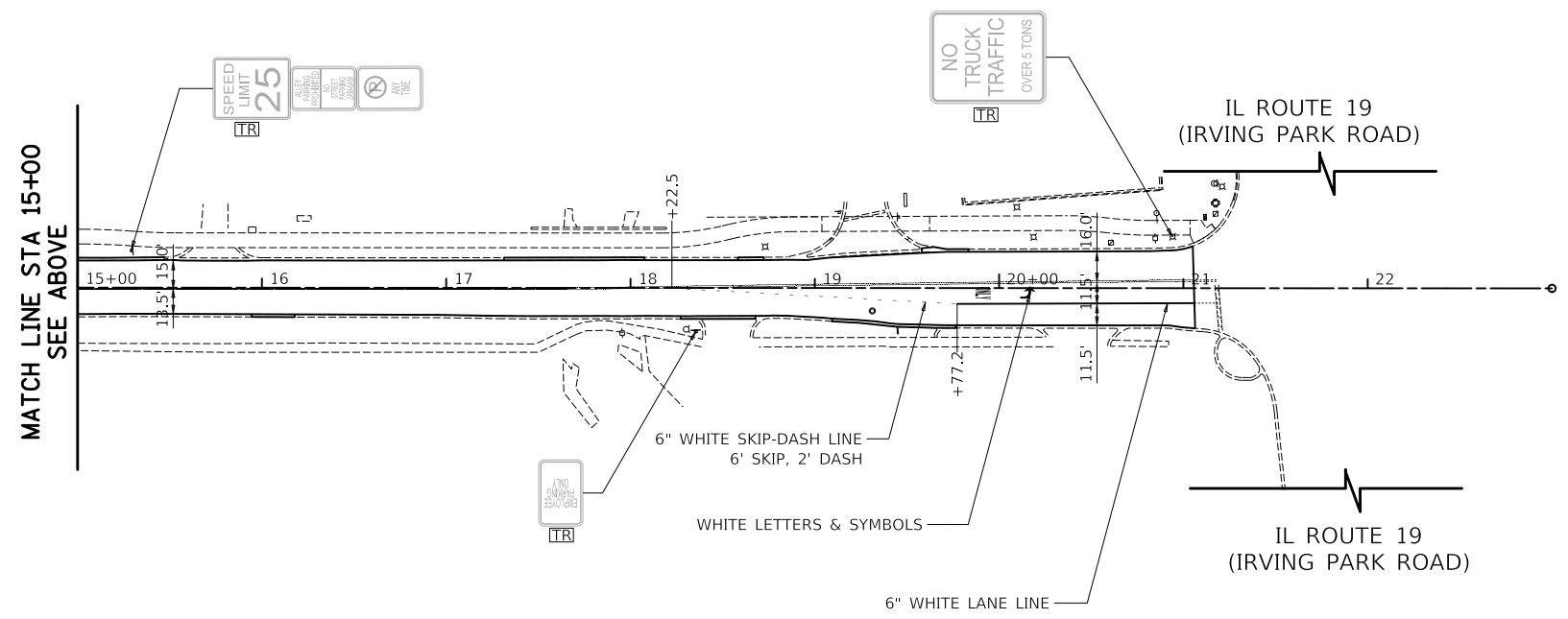
SHEET NO. 6 OF 6 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	41
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
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	NO. OF WAY CHECKED		
	CADD FILE NAME		
	NO.		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. OF WAY CHECKED		
	NO. OF WAY CHECKED		
	STRUCTURE NOTATIONS CHKD		
	NO.		



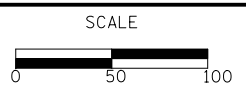
- NOTES**
- ALL PROPOSED PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
 - SIGNING WORK TO BE PAID FOR AS:
 - SIGN PANEL - TYPE 1
 - REMOVE SIGN PANEL ASSEMBLY - TYPE A
 - REMOVE SIGN PANEL ASSEMBLY - TYPE B
 - RELOCATE SIGN PANEL ASSEMBLY - TYPE A
 - TELESCOPING STEEL SIGN SUPPORT
 - BASE FOR TELESCOPING STEEL SIGN SUPPORT

- LEGEND**
- (TR) SIGN TO RELOCATED
 - (TR) SIGN TO REMAIN
 - (X) SIGN TO BE REMOVED

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DEPARTMENT OF TRANSPORTATION



**CHURCH ROAD AND MAIN STREET RESURFACING
 PAVEMENT MARKING AND SIGNING PLAN**

SHEET NO. 1 OF 2 SHEETS STA. 1+00 TO STA. 22+00

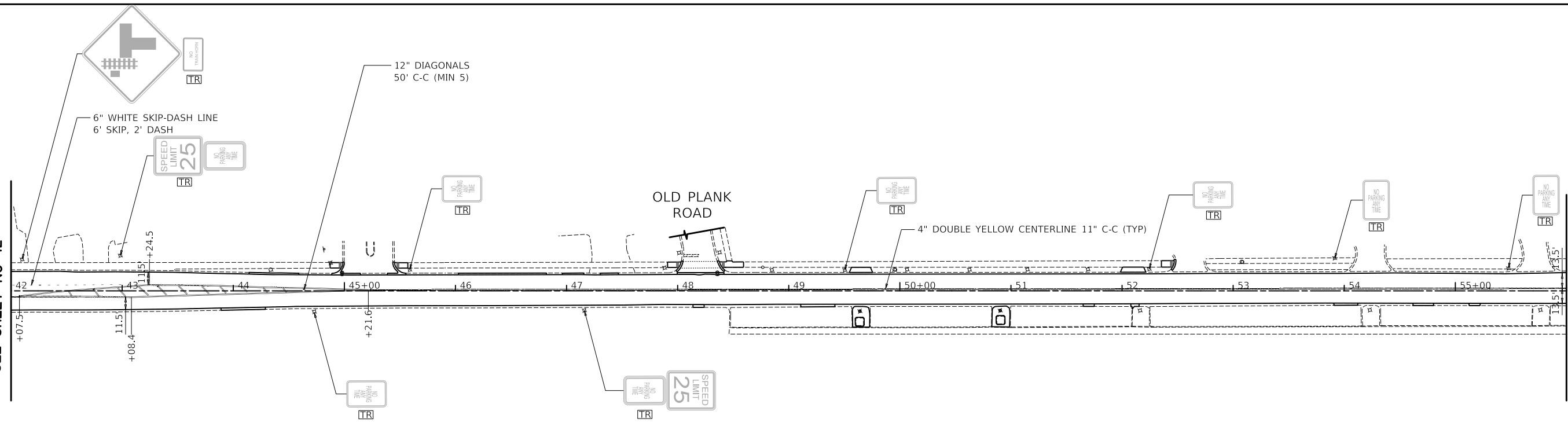
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	42
3001 MAIN				
CONTRACT NO. 61K22				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
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	CHECKED		
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	NO.		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	NO. OF WAY CHECKED		
	NO.		
	STRUCTURE NOTATIONS CHKD		

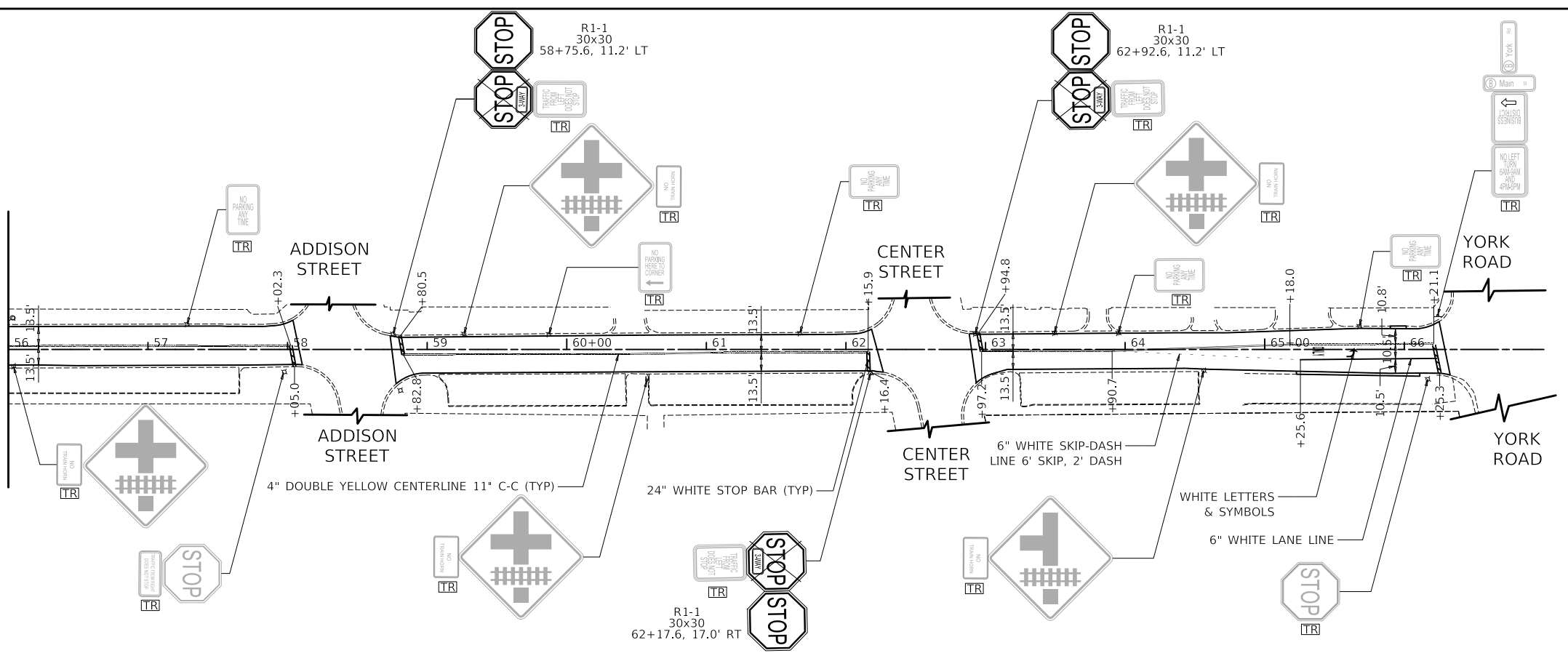
MATCH LINE STA 42+00
SEE SHEET NO 42

MATCH LINE STA 56+00
SEE BELOW



MAIN STREET

MATCH LINE STA 56+00
SEE ABOVE



MAIN STREET

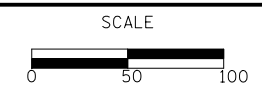
- NOTES**
- ALL PROPOSED PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
 - SIGNING WORK TO BE PAID FOR AS:
 - SIGN PANEL - TYPE 1
 - REMOVE SIGN PANEL ASSEMBLY - TYPE A
 - REMOVE SIGN PANEL ASSEMBLY - TYPE B
 - RELOCATE SIGN PANEL ASSEMBLY - TYPE A
 - TELESCOPING STEEL SIGN SUPPORT
 - BASE FOR TELESCOPING STEEL SIGN SUPPORT

- LEGEND**
- (TR) SIGN TO RELOCATED
 - (TR) SIGN TO REMAIN
 - X SIGN TO BE REMOVED

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CHURCH ROAD AND MAIN STREET RESURFACING
PAVEMENT MARKING AND SIGNING PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	43
3001 MAIN				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61K22	

SHEET NO. 2 OF 2 SHEETS STA. 42+00 TO STA. 66+50

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND			SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM			FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM			GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM					
SIGNAL HEAD			RELOCATE ITEM					
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM					
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED					
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED					
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED					
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIRMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

MODEL: D:\draft...
 FILE NAME: P:\01108481\BID\NITEG...
 PROJECT: 20-00100-00-RS...
 SHEET: 71 OF 71

USER NAME = footemj	DESIGNED - IP	REVISIONS
PLOT SCALE = 50,0000' / in.	DRAWN - IP	REVISIONS
PLOT DATE = 3/4/2019	CHECKED - LP	REVISIONS
	DATE - 9/29/2016	REVISIONS

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

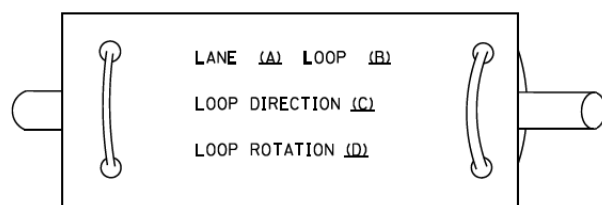
SCALE: NONE SHEET 1 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-05			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				

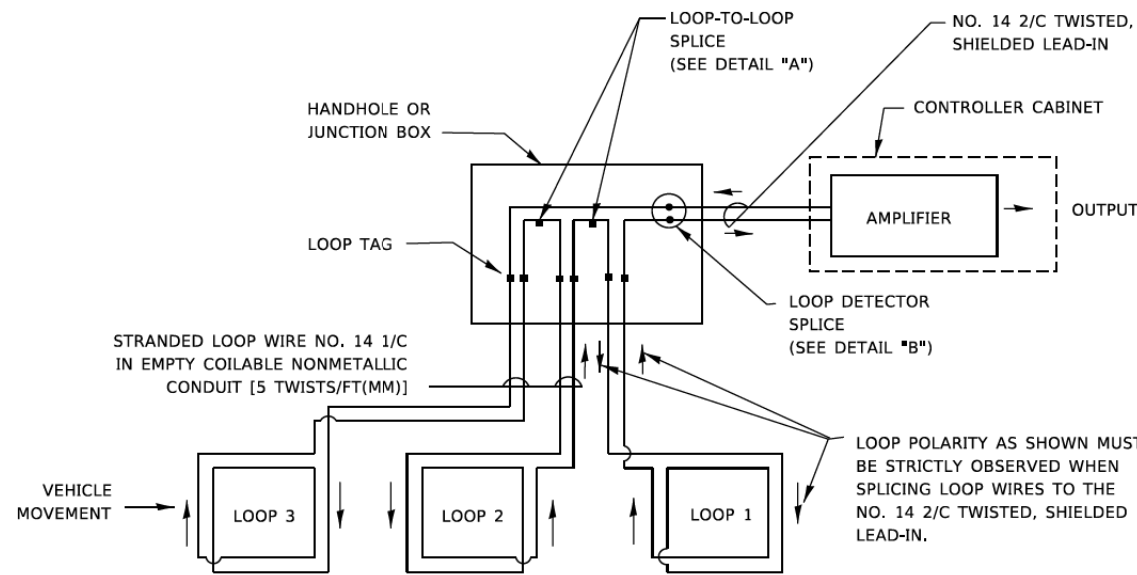
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

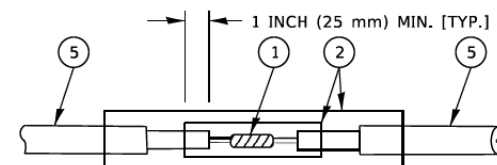


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

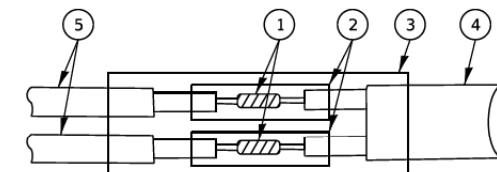


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

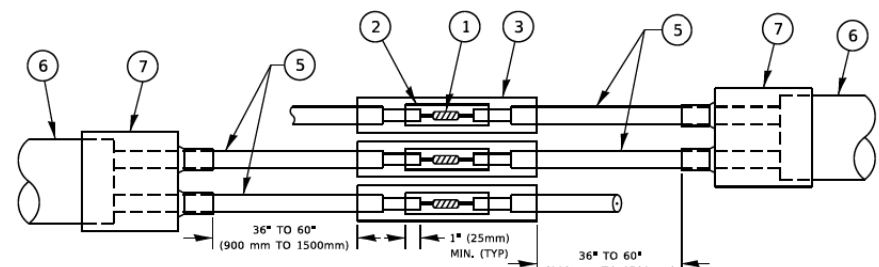


DETAIL "A"
LOOP-TO-LOOP SPLICE

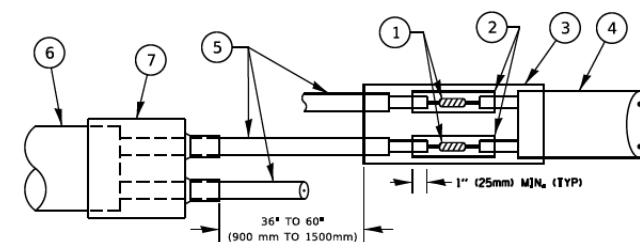


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3' (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE, PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

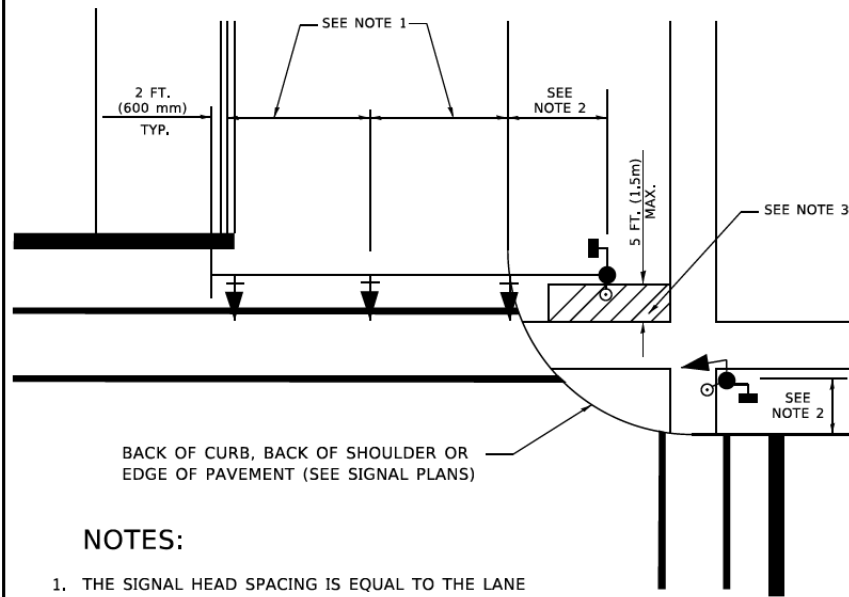
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE. 2667	SECTION 20-00100-00-RS	COUNTY DUPAGE	TOTAL SHEETS 71	SHEET NO. 45
TS-05			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

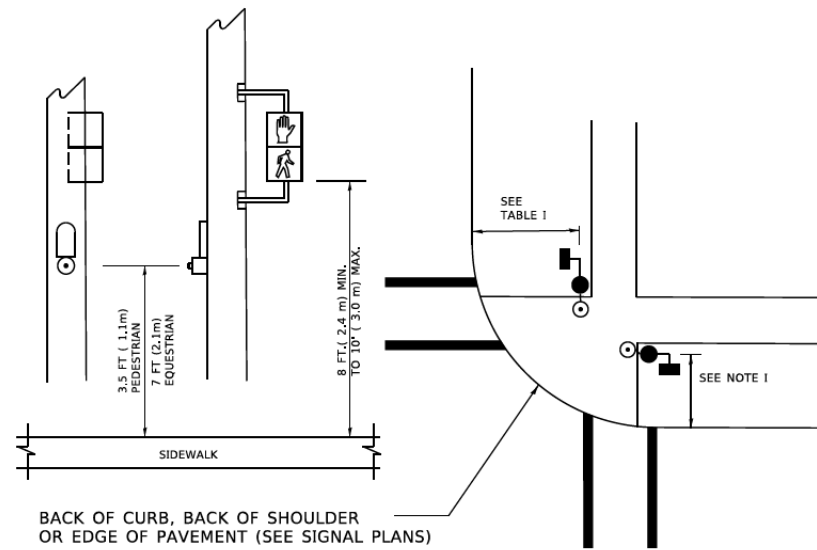
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

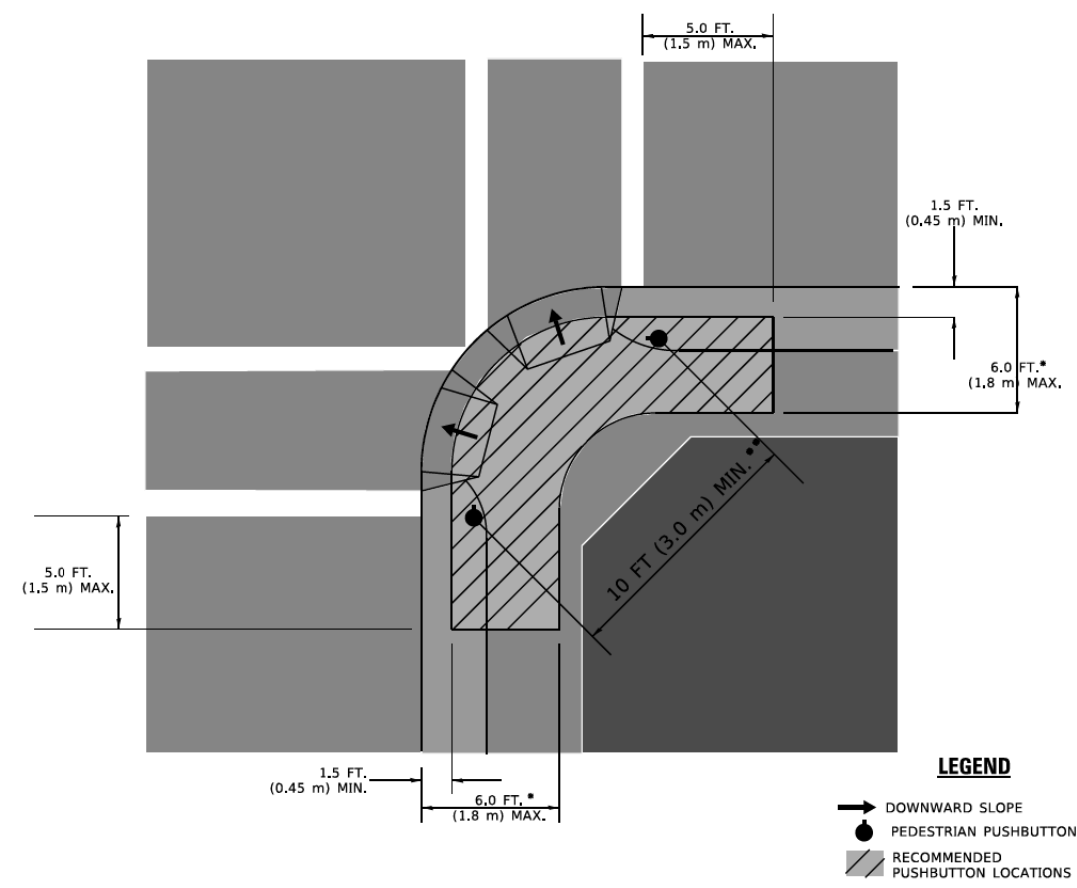
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

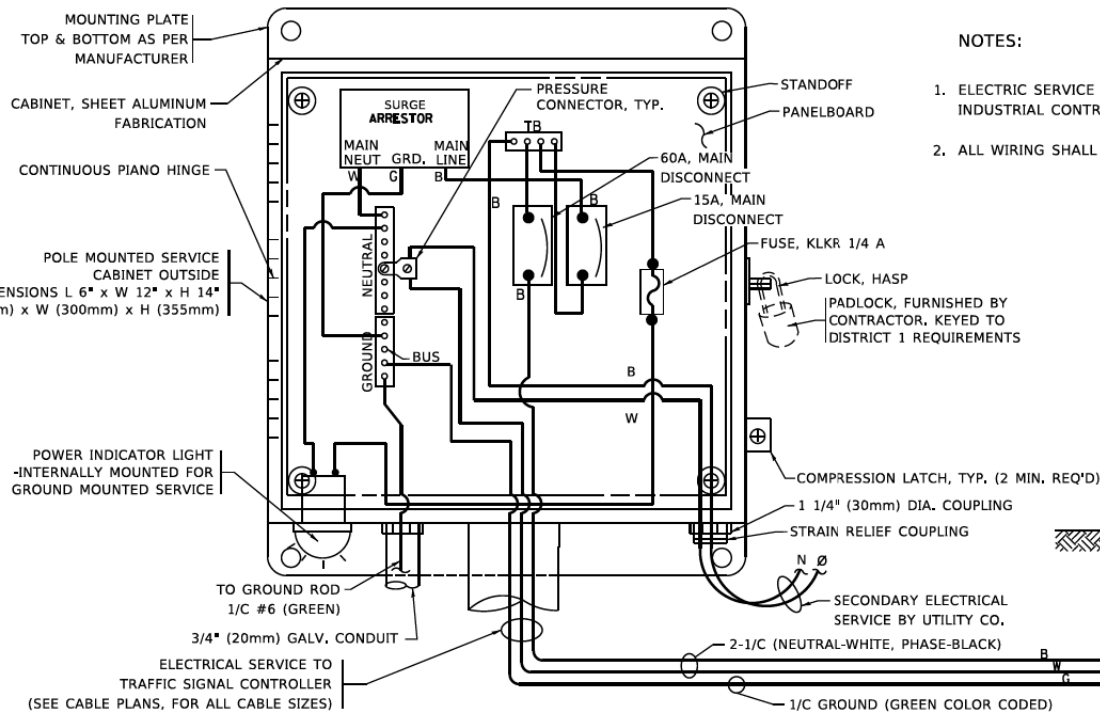
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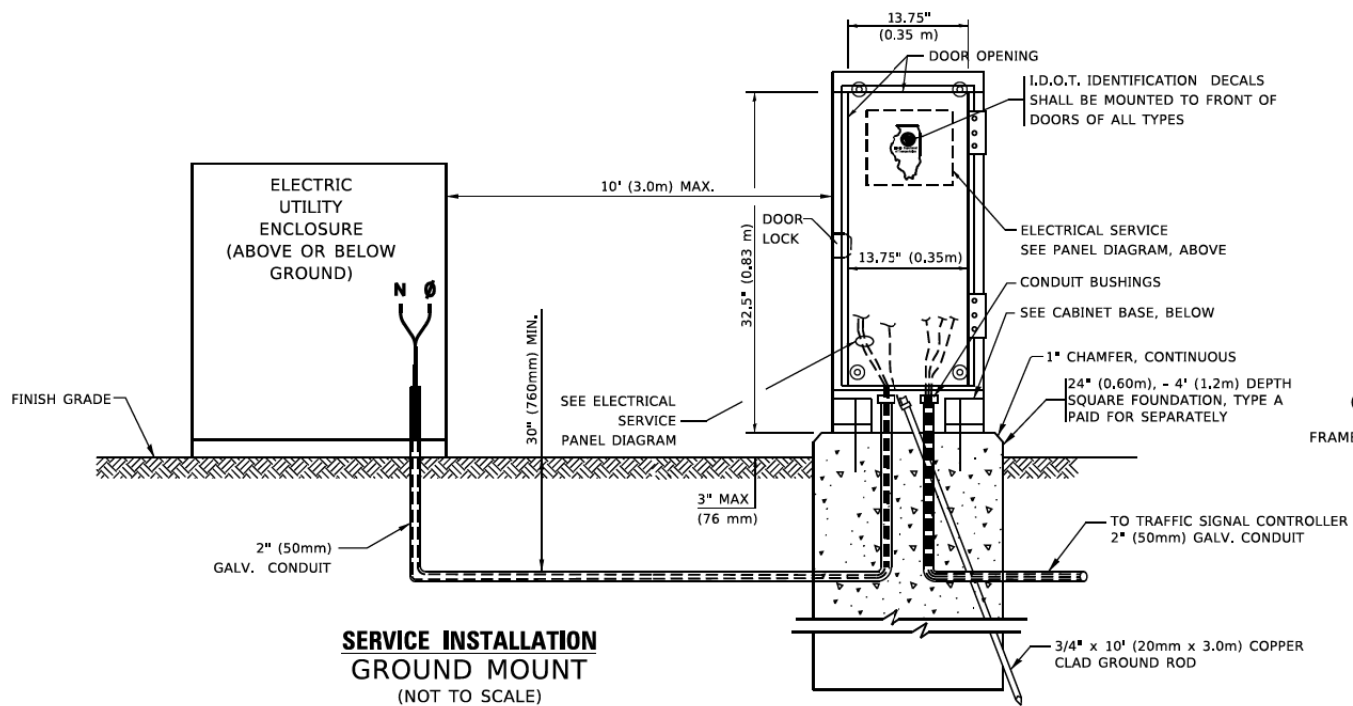
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

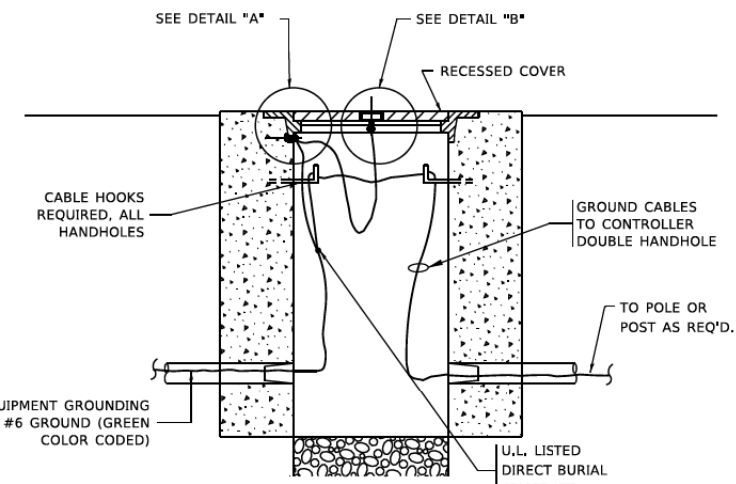
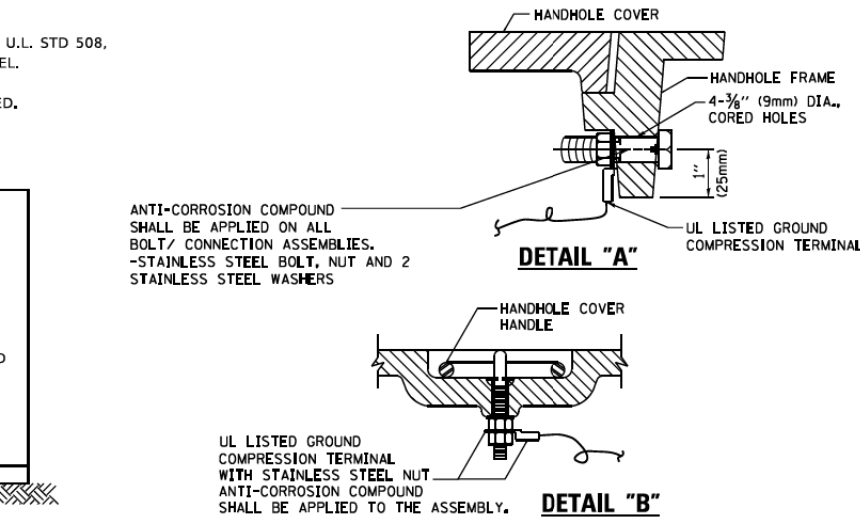
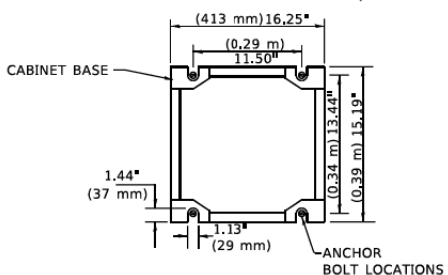
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TS-05			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				



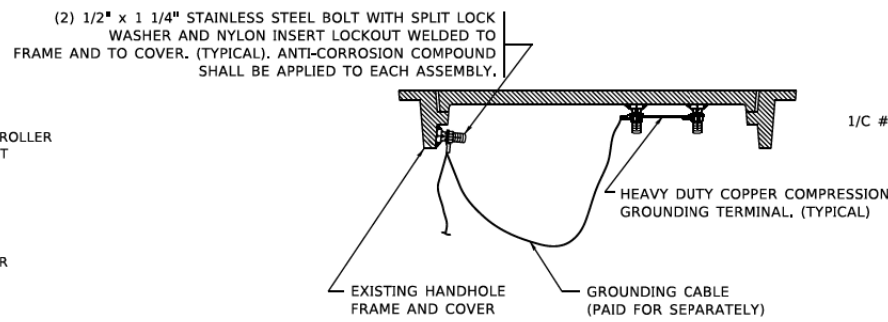
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SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



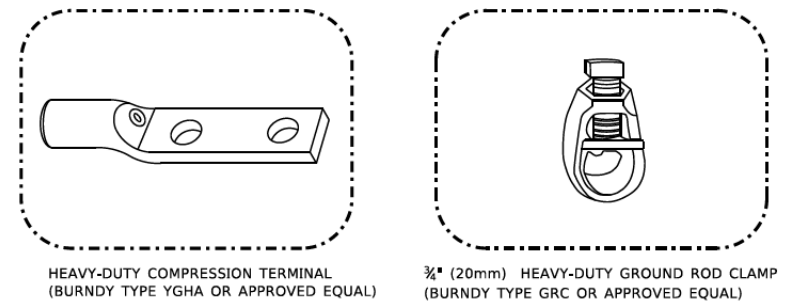
CABINET – BASE BOLT PATTERN
 (NOT TO SCALE)



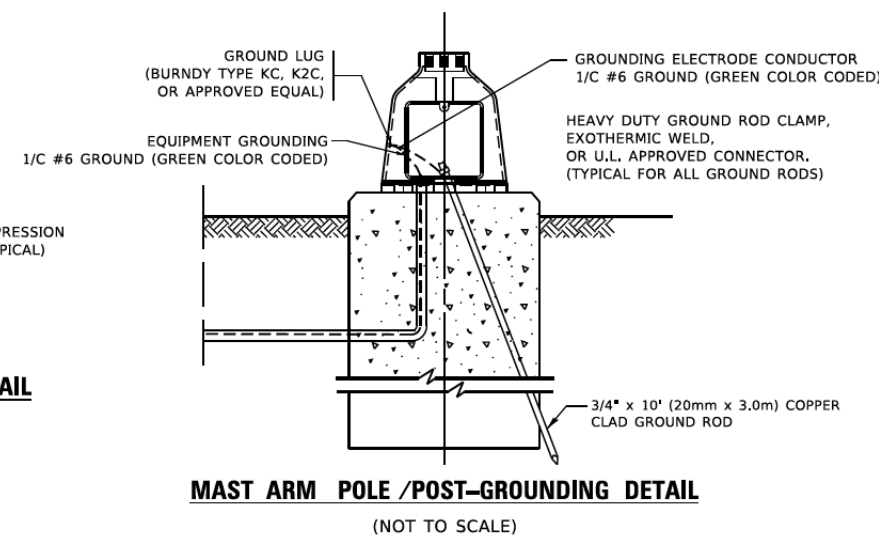
HANDHOLE COVER & FRAME – GROUNDING DETAIL
 (NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
 (NOT TO SCALE)



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
 - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
 - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
 - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



NOTES:
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

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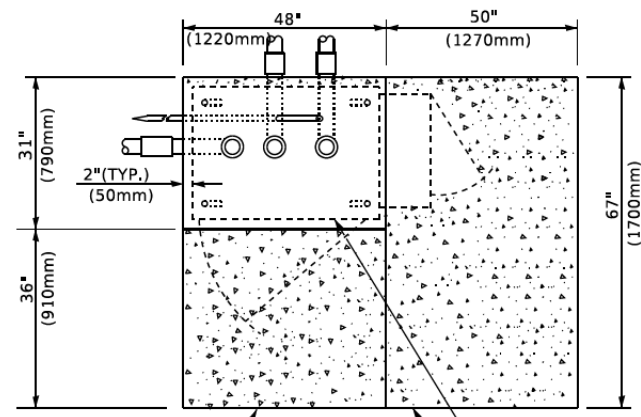
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

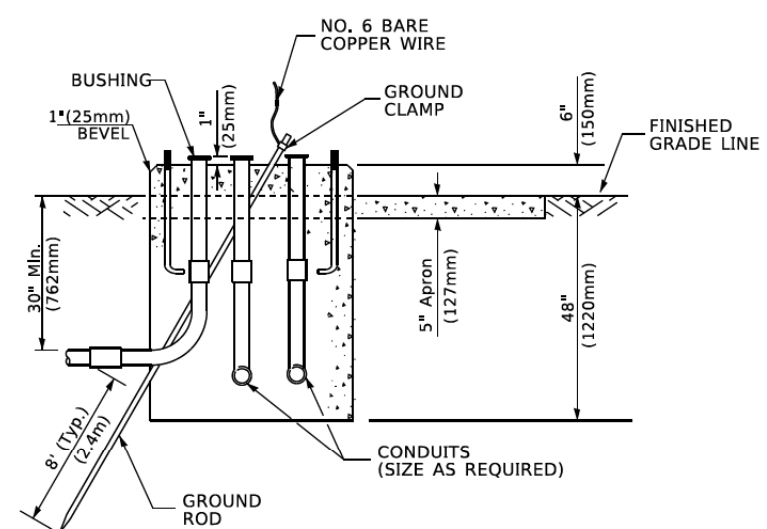
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

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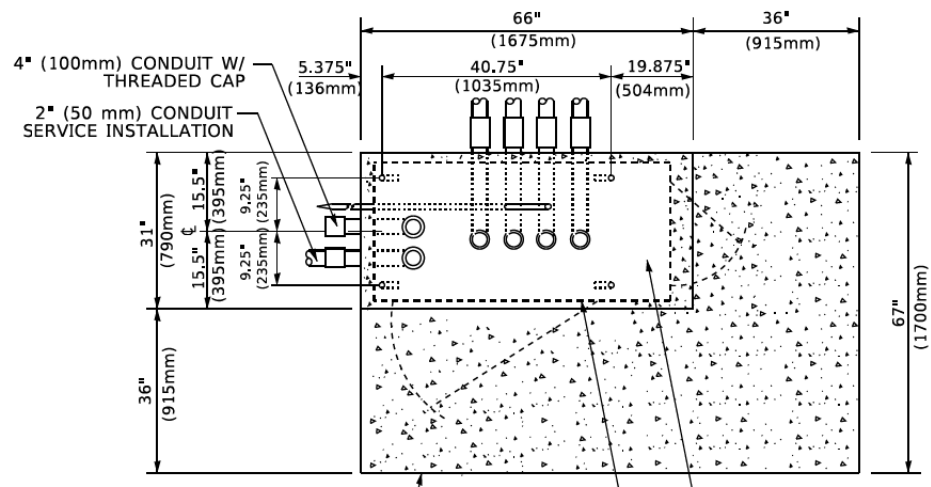
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TS-05		CONTRACT NO. 61K22		
ILLINOIS FED. AID PROJECT				



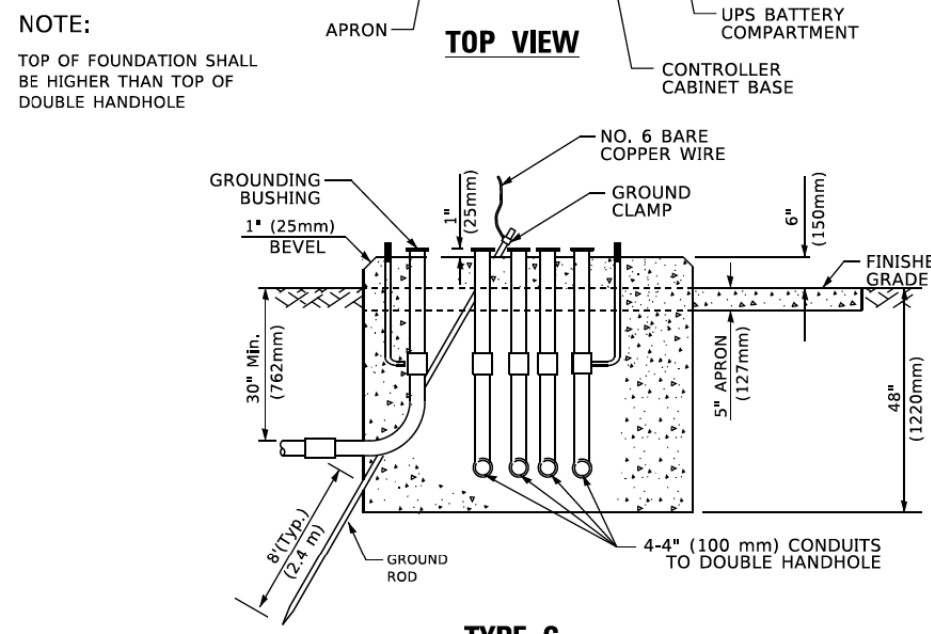
TOP VIEW



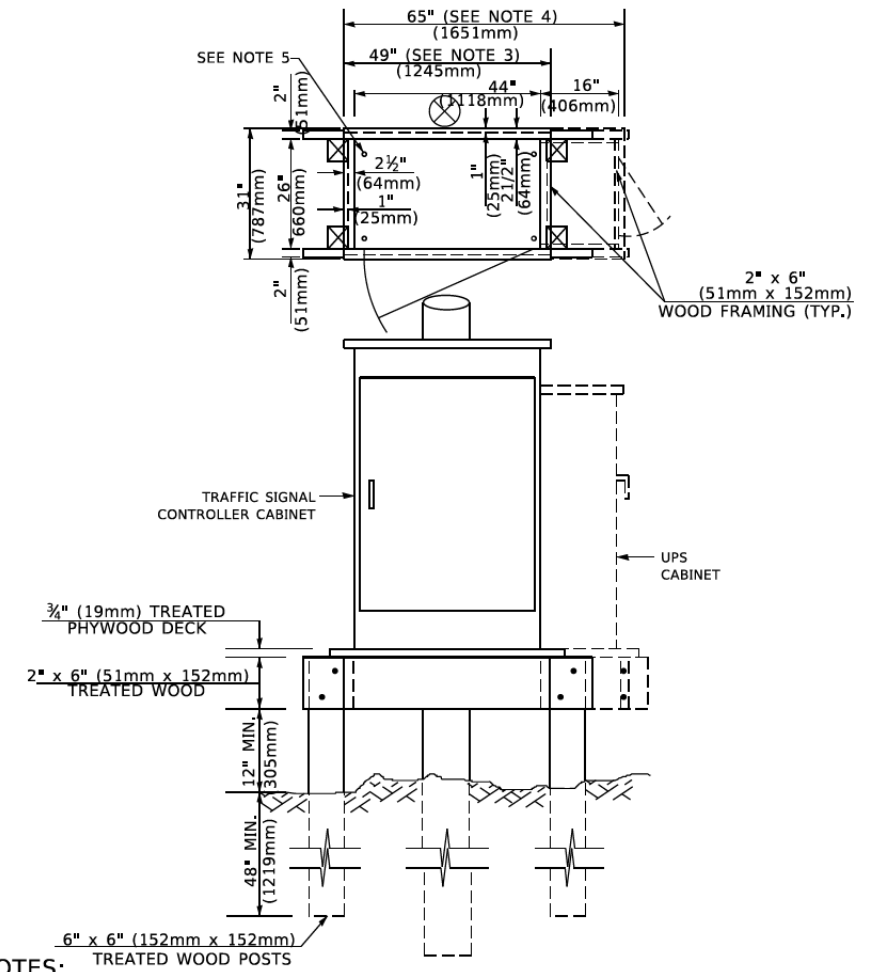
**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



TOP VIEW



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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PLOT DATE = 3/4/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

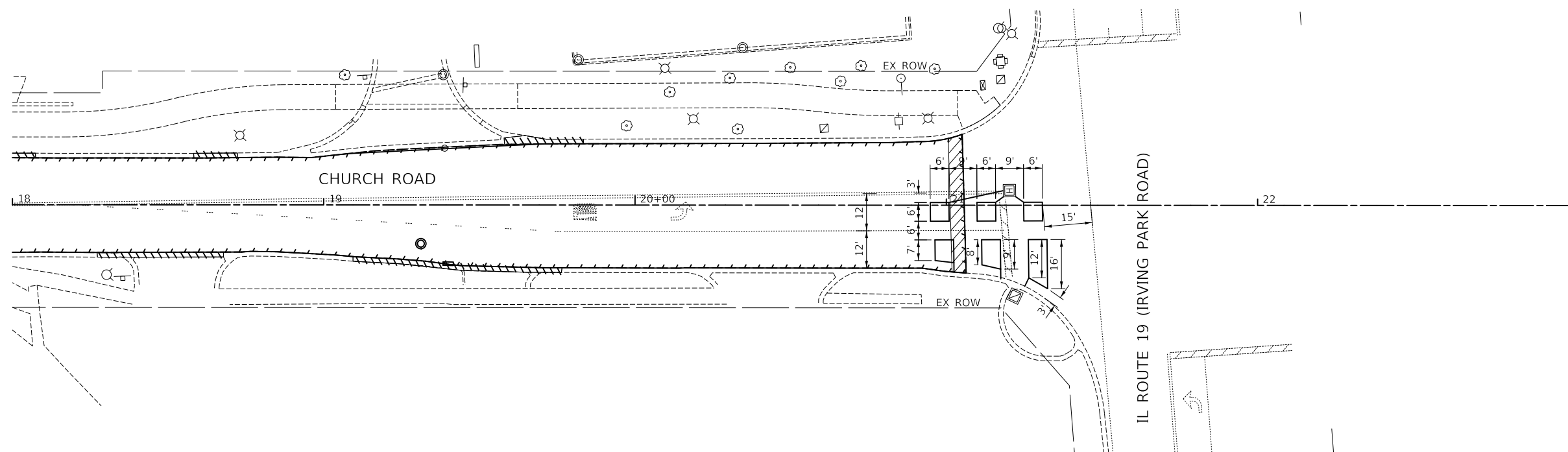
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STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET 5	OF 7 SHEETS	STA. TO STA.

F.A.U. RTE. 2667	SECTION 20-00100-00-RS	COUNTY DUPAGE	TOTAL SHEETS 71	SHEET NO. 48
TS-05		CONTRACT NO. 61K22		
ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
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PROFILE	SURVEYED	DATE
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	DATE	



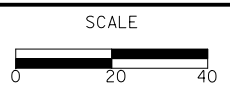
CHURCH ROAD



Two Pierce Place, Suite 1400
Itasca, Illinois 60143
Tel: 630.773.3900 Fax: 630.773.3975
www.civiltechinc.com

DESIGNED - JRR	REVISED -
DRAWN - JRR	REVISED -
CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



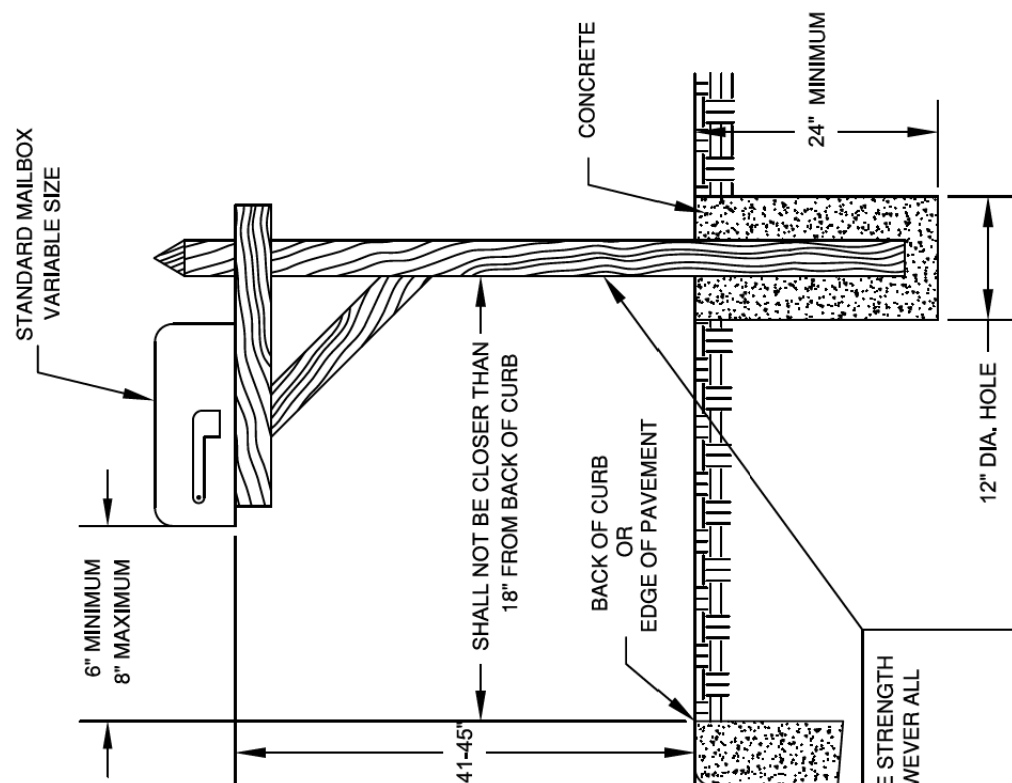
CHURCH ROAD AND MAIN STREET RESURFACING
DETECTOR LOOP REPLACEMENT PLAN

SHEET NO. 1 OF 1 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	51
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	BY		
	DATE		

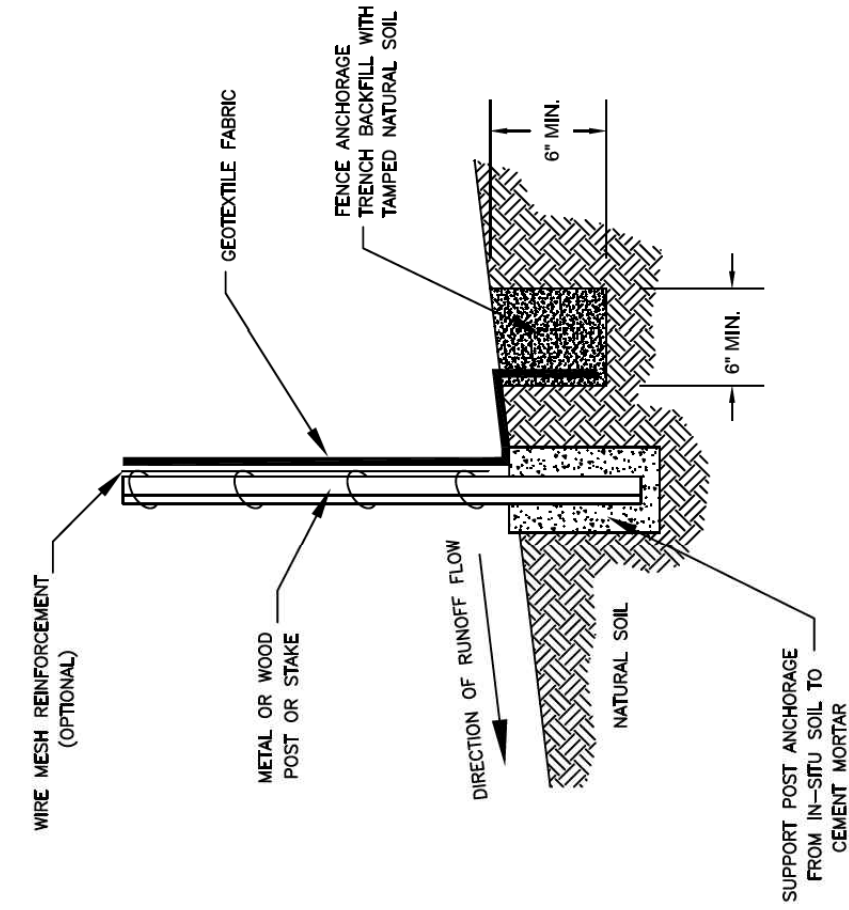
PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	BY		
	DATE		



- NOTE**
- EXTEND MAILBOX BEYOND EDGE OF SUPPORT TO ALLOW FOR HINGE ON DOOR. ATTACH BOARD HOLDING BOX TO POST WITH SCREWS OR DOUBLE HEADED NAILS, PERMITTING EASY REMOVAL IF NECESSARY.
 - MAILBOX NUMBER SHALL BE PERMANENTLY MARKED (PAINTED) ON THE BOX IN NUMERALS NOT LESS THAN ONE INCH HIGH.
 - PRIOR TO EXCAVATING SUPPORT POST FOUNDATION HOLE, THE HOMEOWNER SHALL CONTACT THE VILLAGE OF BENSENVILLE AT 630-350-3435 AND J.U.L.I.E. AT 1-800-892-0123 48 HRS. PRIOR TO DIGGING IN ORDER TO AVOID DAMAGE TO UNDERGROUND UTILITIES OR INJURY(IES) TO PERSON(S).
 - ALL MAILBOX SUPPORTS AND ANY VARIATION TO THIS STANDARD MUST BE APPROVED BY THE VILLAGE OF BENSENVILLE PRIOR TO INSTALLATION.
 - MAILBOX FACE SHALL NOT BE CLOSER THAN 6" FROM BACK OF CURB OR EDGE OF PAVEMENT IF THERE IS NO CURB.

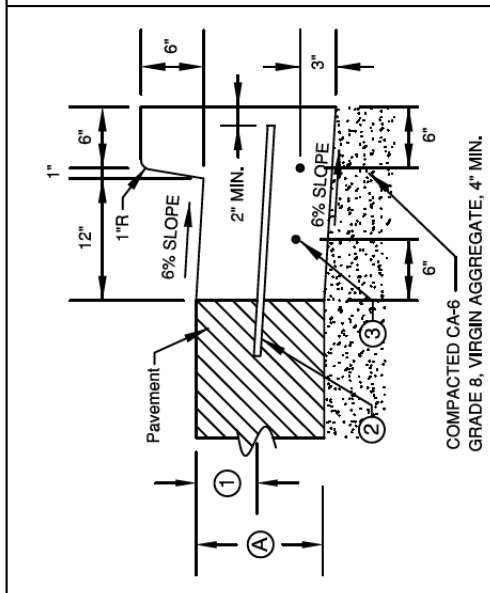
POST TO BE NEAT AND OF ADEQUATE STRENGTH AND SIZE TO SUPPORT MAILBOX. HOWEVER ALL POSTS SHALL MEET THE FOLLOWING REQUIREMENTS:
 MAXIMUM SIZE POSTS:
 4" X 4" WOOD POST
 4-1/2" DIA. WOOD POST

	VILLAGE OF BENSENVILLE 12 S. CENTER STREET BENSENVILLE, IL 60106 WWW.BENSENVILLE.IL.US	MAILBOX SUPPORT AND ATTACHMENT DESIGN	VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS FILE LOCATION	REVISIONS
	VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS FILE LOCATION	G:ENGINEERING\STANDARDS\NEW STANDARDS\GENERAL	VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS FILE LOCATION	REVISIONS

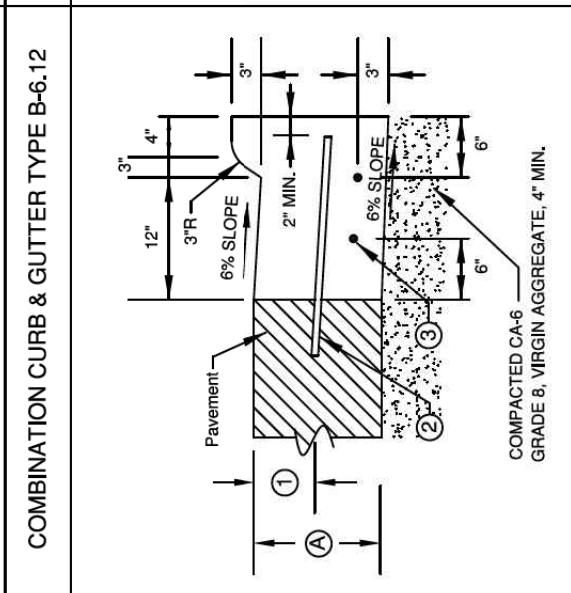


NOTE: DEPENDING UPON CONFIGURATION, ATTACH FABRIC TO WIRE MESH W/HOG RINGS, STEEL POSTS W/TIE WIRES, WOOD POSTS W/NAIIS.

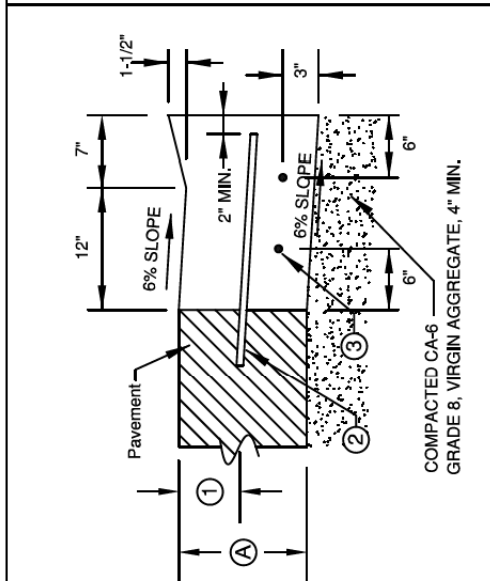
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	VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS FILE LOCATION	G:ENGINEERING\STANDARDS\NEW STANDARDS\EROSION CONTROL	VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS FILE LOCATION	REVISIONS



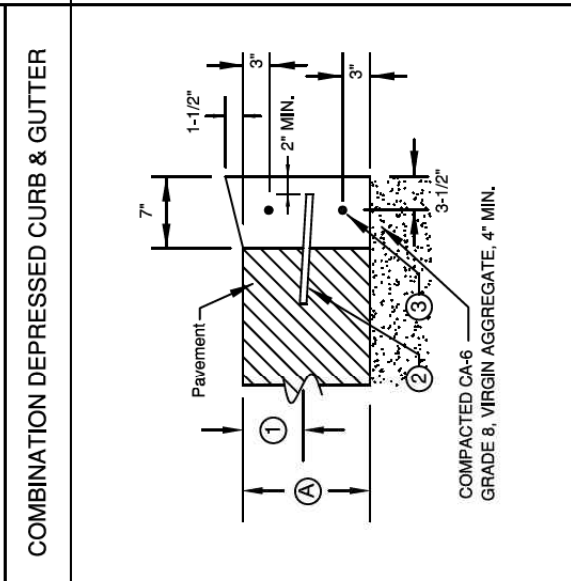
COMPACTED CA-6
 GRADE 8, VIRGIN AGGREGATE, 4" MIN.



COMPACTED CA-6
 GRADE 8, VIRGIN AGGREGATE, 4" MIN.

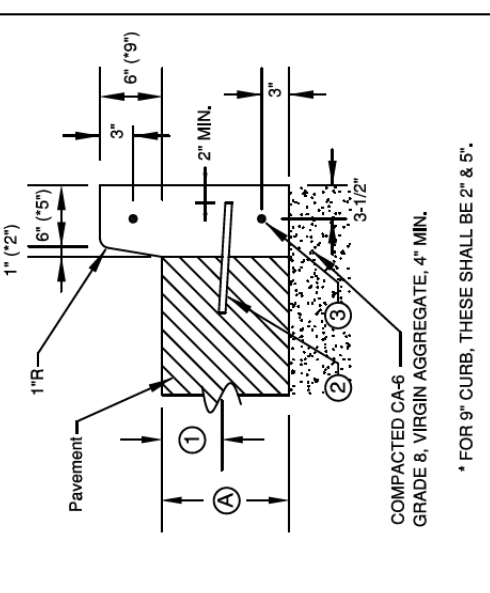


COMPACTED CA-6
 GRADE 8, VIRGIN AGGREGATE, 4" MIN.



COMPACTED CA-6
 GRADE 8, VIRGIN AGGREGATE, 4" MIN.

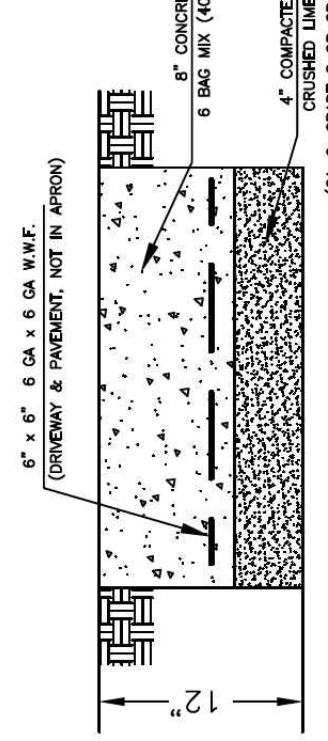
- Ⓐ PAVEMENT DEPTH OR 9" MIN.
- ① TO BE CENTERED OR SET AS REQUIRED IN FIELD.
- ② 1/2" DIA. STEEL TIE BAR AT 2'-6" CENTERED WHEN PAVEMENT IS CONCRETE. OMIT WHEN ADJACENT PAVEMENT IS FLEXIBLE.
- ③ TWO 1" DIAMETER DOWEL BARS DRILLED AND GROUTED WHERE NEW CURB MEETS EXISTING.
- ④ PROVIDE DEPRESSED CURBS AT DRIVEWAYS, CROSSWALKS, AND SIDEWALKS, ETC.
- ⑤ ALL CONCRETE SHALL HAVE 4,000 P.S.I. COMPRESSIVE STRENGTH AT 28 DAYS.
- ⑥ ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO LATEST EDITION OF IDOT STANDARD SPECS FOR ROAD AND BRIDGE CONSTRUCTION.
- ⑦ APPLY TWO COATS OF WHITE PIGMENT CURING COMPOUND IMMEDIATELY AFTER INITIAL SET.



COMPACTED CA-6
 GRADE 8, VIRGIN AGGREGATE, 4" MIN.

* FOR 9" CURB, THESE SHALL BE 2" & 5".

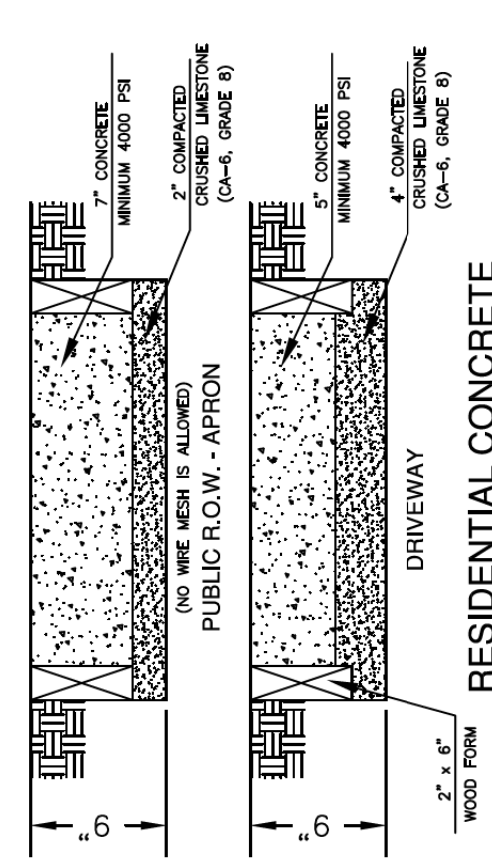
<p>VILLAGE OF BENSENVILLE 12 S. CENTER STREET BENSENVILLE, IL 60106 WWW.BENSENVILLE.IL.US</p>	<p>VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS</p>		<p>REVISIONS</p>
	<p>FILE LOCATION</p>		
	<p>G:ENGINEERING\STANDARDS\NEW STANDARDS\ROADWAY</p>		
	<p>DRIVEWAY STANDARD</p>		



COMPACTED CA-6
 GRADE 8 OR GRADE 9, VIRGIN

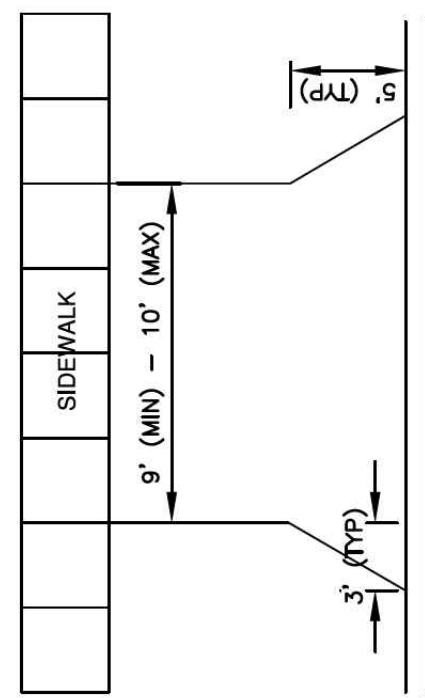
(*) RECOMMEND 10" IN TRUCK TRAFFIC & LOADING AREAS

COMMERCIAL DRIVEWAY



WOOD FORM
 2" x 6"

RESIDENTIAL CONCRETE



SINGLE CAR
 RESIDENTIAL DRIVEWAY

NOTE: REFER TO 10-11-7-1 & 10-11-7-2 OF VILLAGE CODE FOR MORE INFORMATION.

<p>VILLAGE OF BENSENVILLE 12 S. CENTER STREET BENSENVILLE, IL 60106 WWW.BENSENVILLE.IL.US</p>	<p>VILLAGE OF BENSENVILLE SPECIFICATIONS AND DETAILS</p>		<p>REVISIONS</p>
	<p>FILE LOCATION</p>		
	<p>G:ENGINEERING\STANDARDS\NEW STANDARDS\ROADWAY</p>		
	<p>DRIVEWAY STANDARD</p>		

PROFILE	SURVEYED	BY	DATE
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PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
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	NO. OF WAY CHECKED		
	ADD. FILE NAME		



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CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

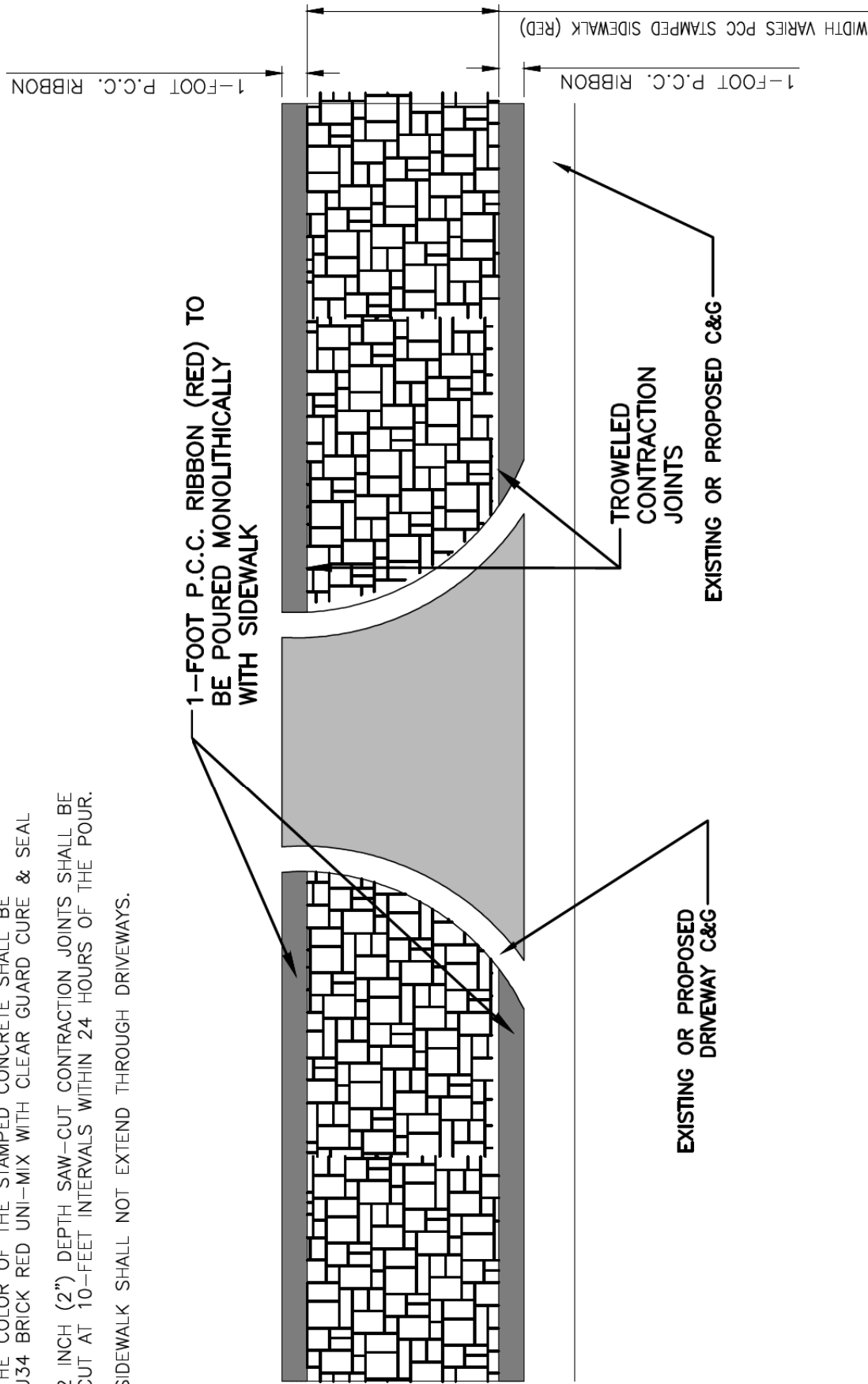
CHURCH ROAD AND MAIN STREET RESURFACING
CONSTRUCTION DETAILS

SHEET NO. 3 OF 4 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	54
3001 MAIN	CONTRACT NO. 61K22			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES:

1. STAMPED COLORED PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE ASHLAR SLATE PATTERN (BST-4000) PATTERN AS SHOWN IN THE DETAIL.
2. THE COLOR OF THE STAMPED CONCRETE SHALL BE U34 BRICK RED UNI-MIX WITH CLEAR GUARD CURE & SEAL
3. 2 INCH (2") DEPTH SAW-CUT CONTRACTION JOINTS SHALL BE CUT AT 10-FOOT INTERVALS WITHIN 24 HOURS OF THE POUR.
4. SIDEWALK SHALL NOT EXTEND THROUGH DRIVEWAYS.



VILLAGE OF BENSENVILLE
123 CENTER STREET
BENSENVILLE, ILLINOIS
WWW.BENSENVILLE.IL.US

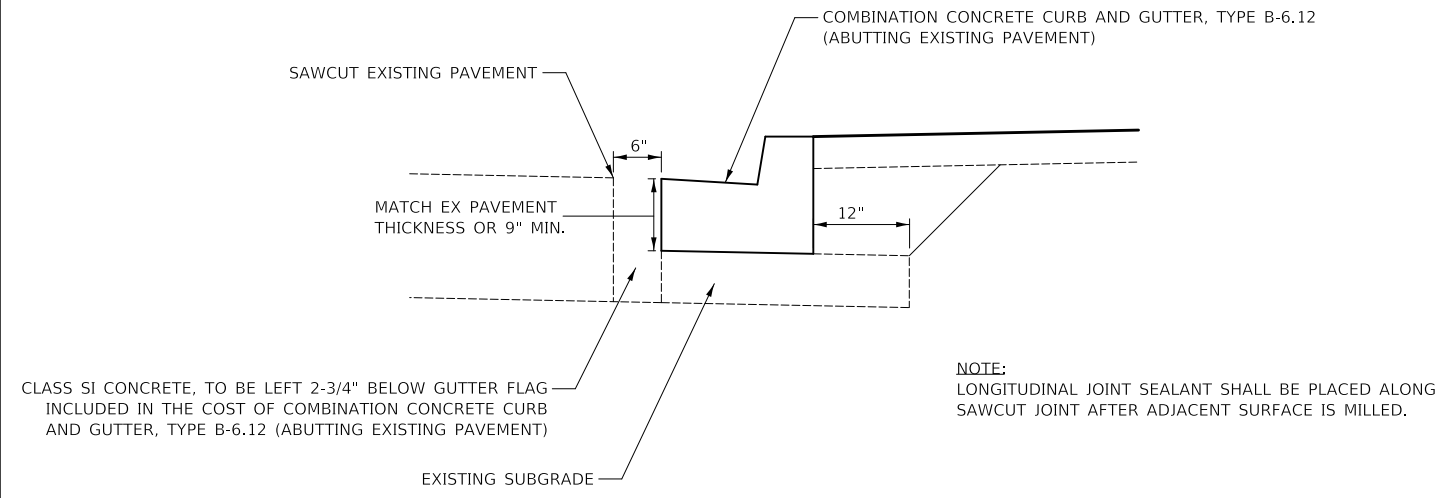
STAMPED CONCRETE SIDEWALK
BST6000 ASHLAR SLATE PATTERN
(N.T.S.)

VILLAGE OF BENSENVILLE
SPECIFICATIONS AND DETAILS
FILE LOCATION
ENGINEERING STANDARDS NEW
STANDARDS/ROADWAY

REVISIONS

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
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	NO. OF WAY CHECKED		
	BY		
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PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	BY		
	DATE		
	STRUCTURE NOTATIONS CHKD		



CLASS SI CONCRETE, TO BE LEFT 2-3/4" BELOW GUTTER FLAG INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (ABUTTING EXISTING PAVEMENT)

NOTE:
LONGITUDINAL JOINT SEALANT SHALL BE PLACED ALONG SAWCUT JOINT AFTER ADJACENT SURFACE IS MILLED.

COMBINATION CONCRETE CURB AND GUTTER (ABUTTING EXISTING PAVEMENT)



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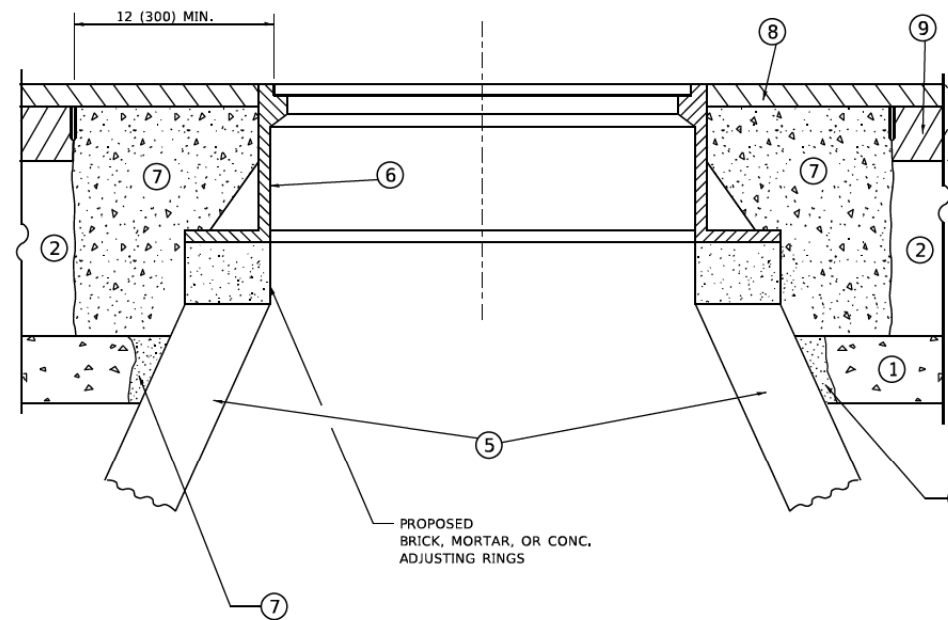
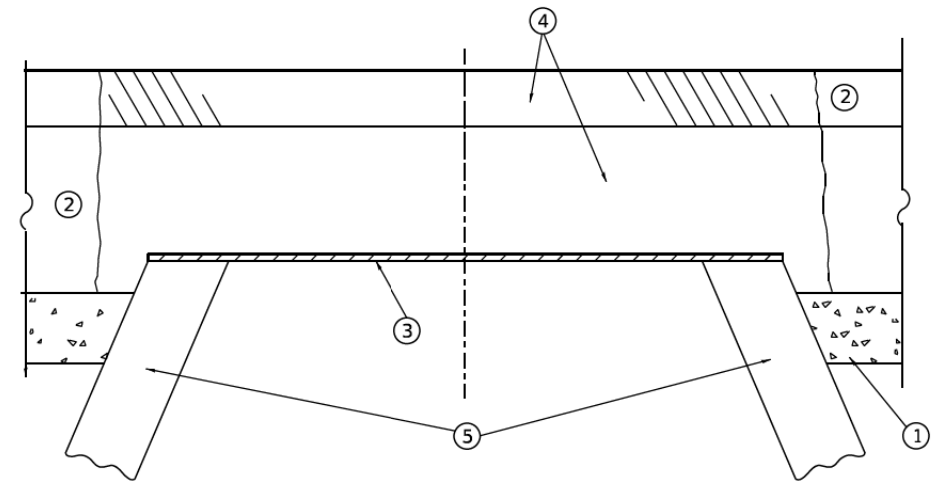
DESIGNED - JRR	REVISED -
DRAWN - JRR	REVISED -
CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CHURCH ROAD AND MAIN STREET RESURFACING
CONSTRUCTION DETAILS**

SHEET NO. 4 OF 4 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	55
3001 MAIN			CONTRACT NO. 61K22	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-2* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- 1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- 2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- 3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- 4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

NOTES

- 1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- 3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- 4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- 5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

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	DRAWN -	REVISED - R. BORO 12-06-11
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PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

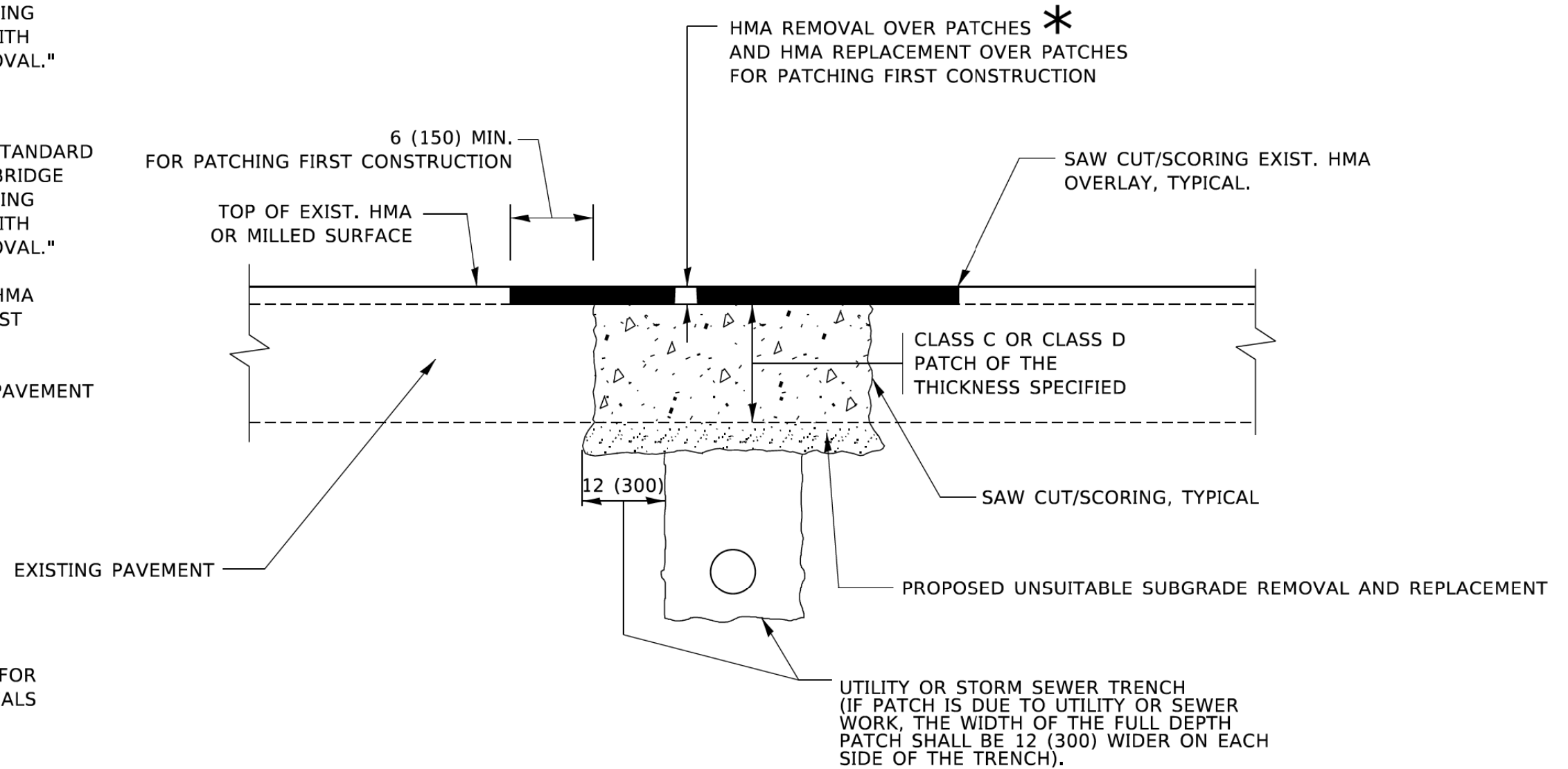
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2667	CHURCH 20-00100-00-RS	DUPAGE	71	56
BD600-03 (BD-08)			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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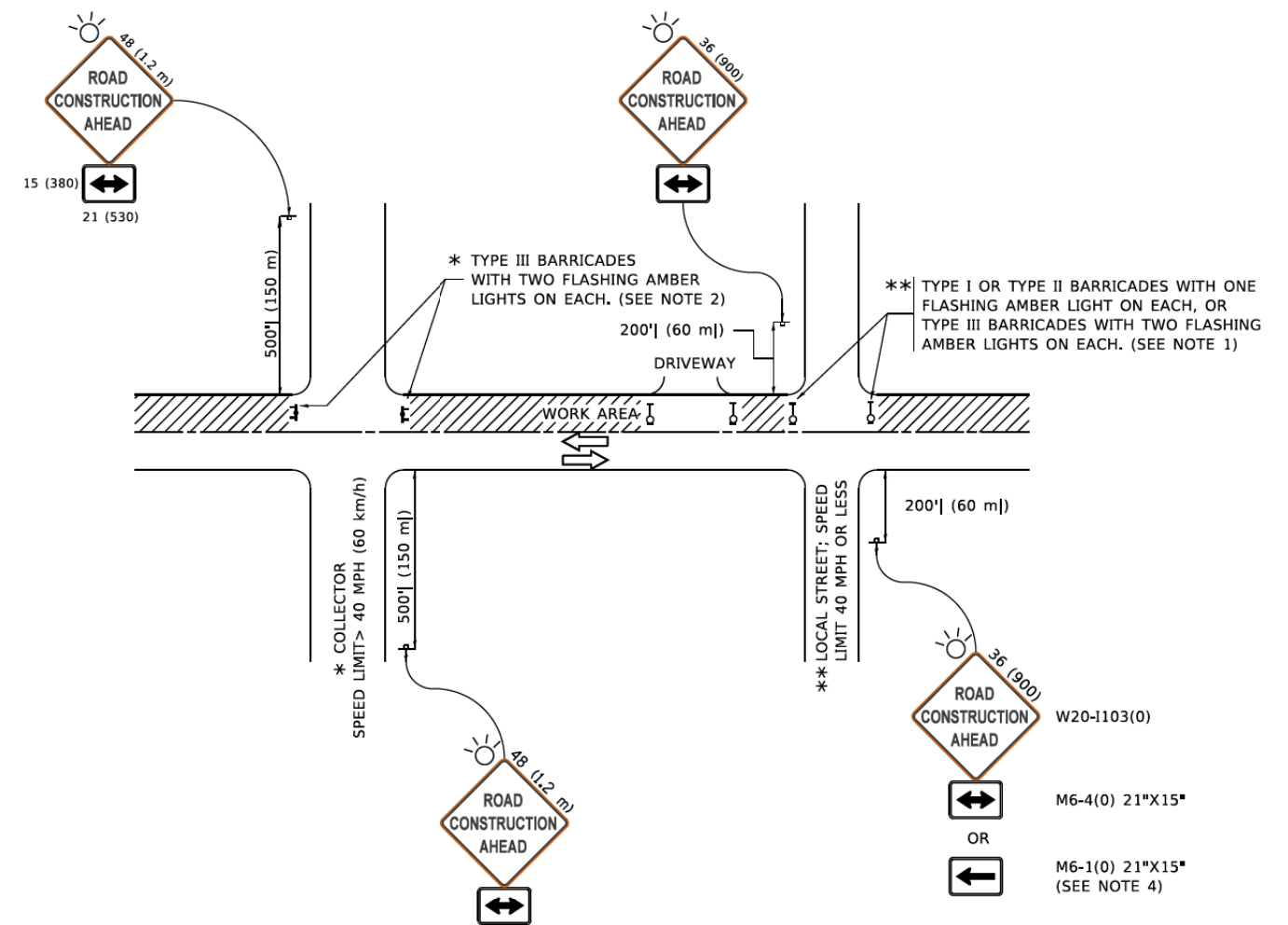
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PLOT DATE = 11/18/2022	DATE - 10-25-94	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH 20-00100-00-RS	DUPAGE	71	57
BD400-04 (BD-22)			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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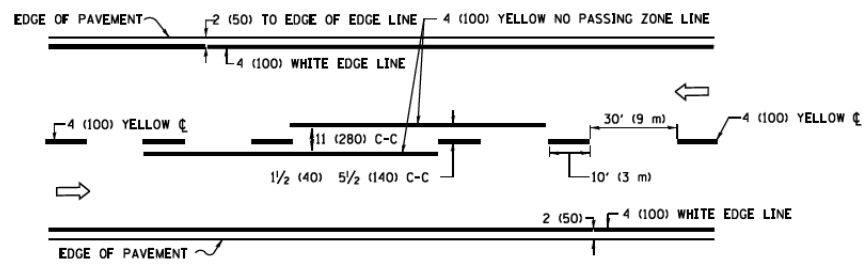
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PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

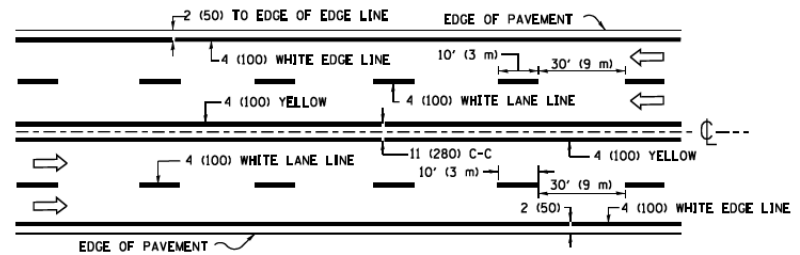
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

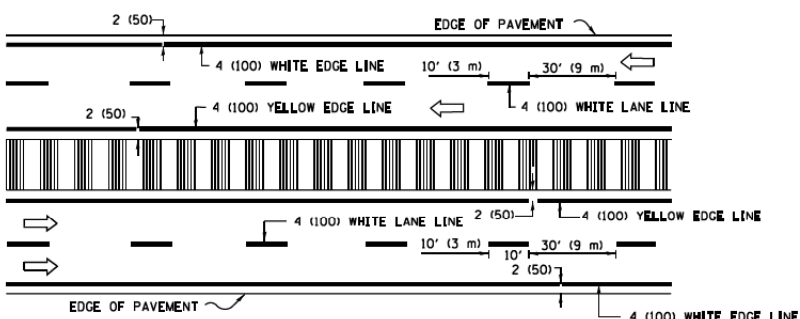
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH 20-00100-00-RS	DUPAGE	71	59
TC-10			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

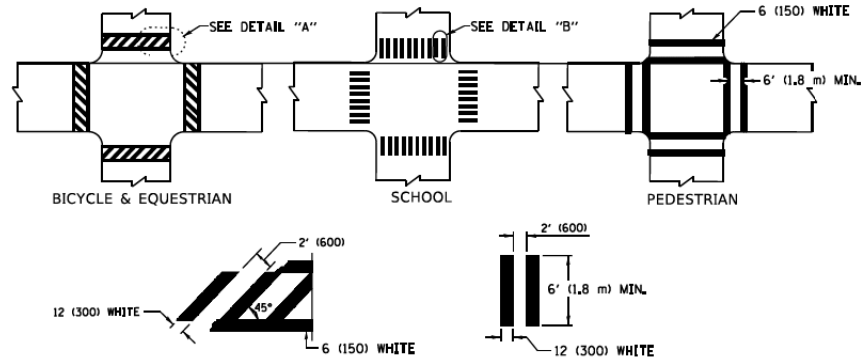


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

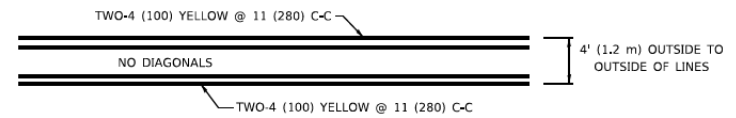


DETAIL "A"

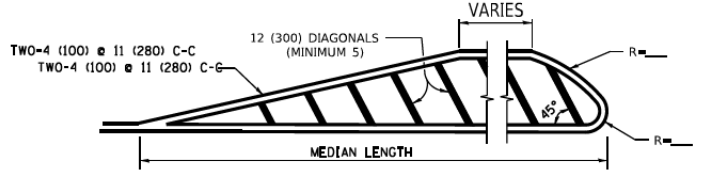
DETAIL "B"

TYPICAL CROSSWALK MARKING

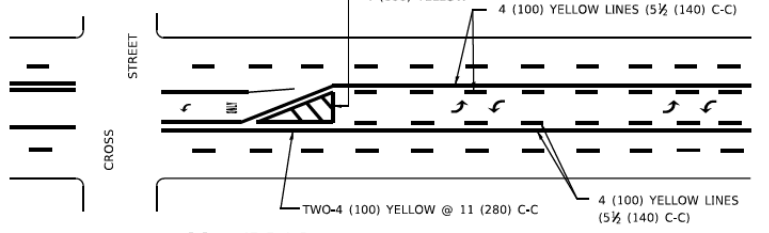
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY

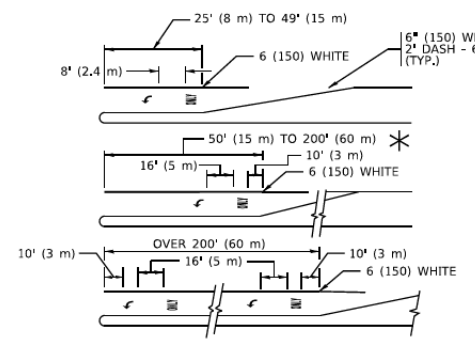


MEDIANS OVER 4' (1.2 m) WIDE



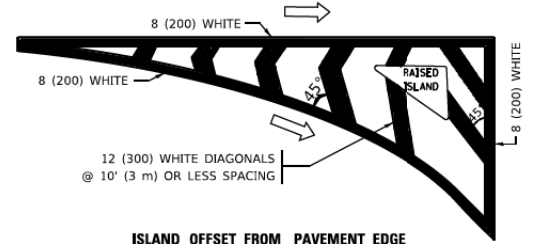
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

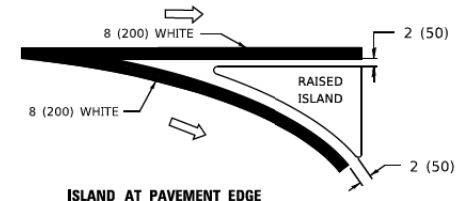


TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

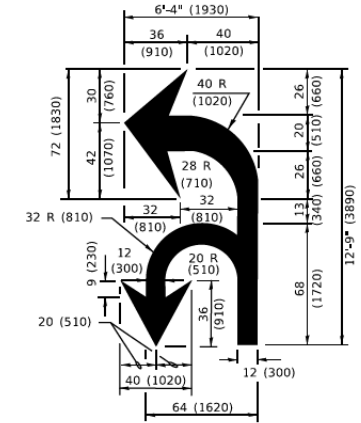


ISLAND OFFSET FROM PAVEMENT EDGE

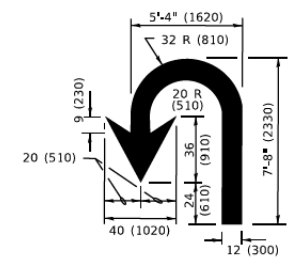


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES; FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ² EACH) "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

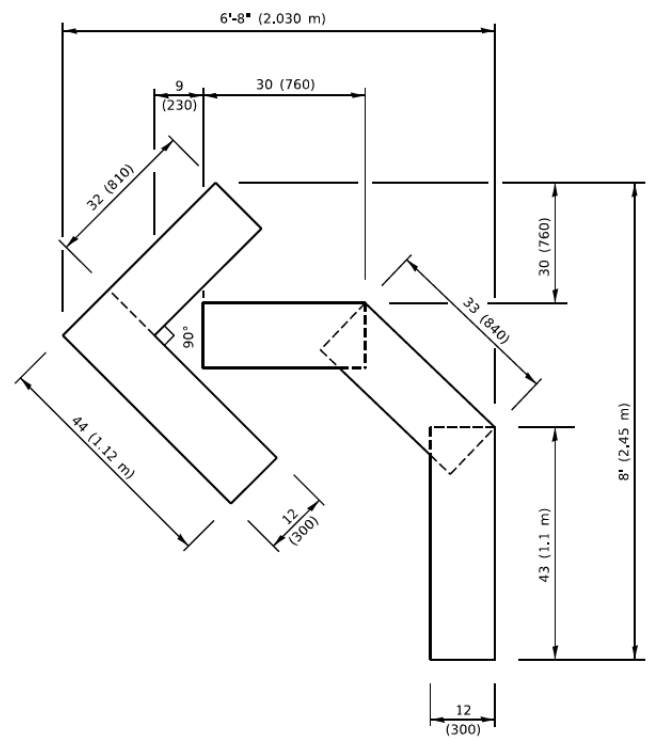
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		REVISED - C. JUCIUS 04-12-16

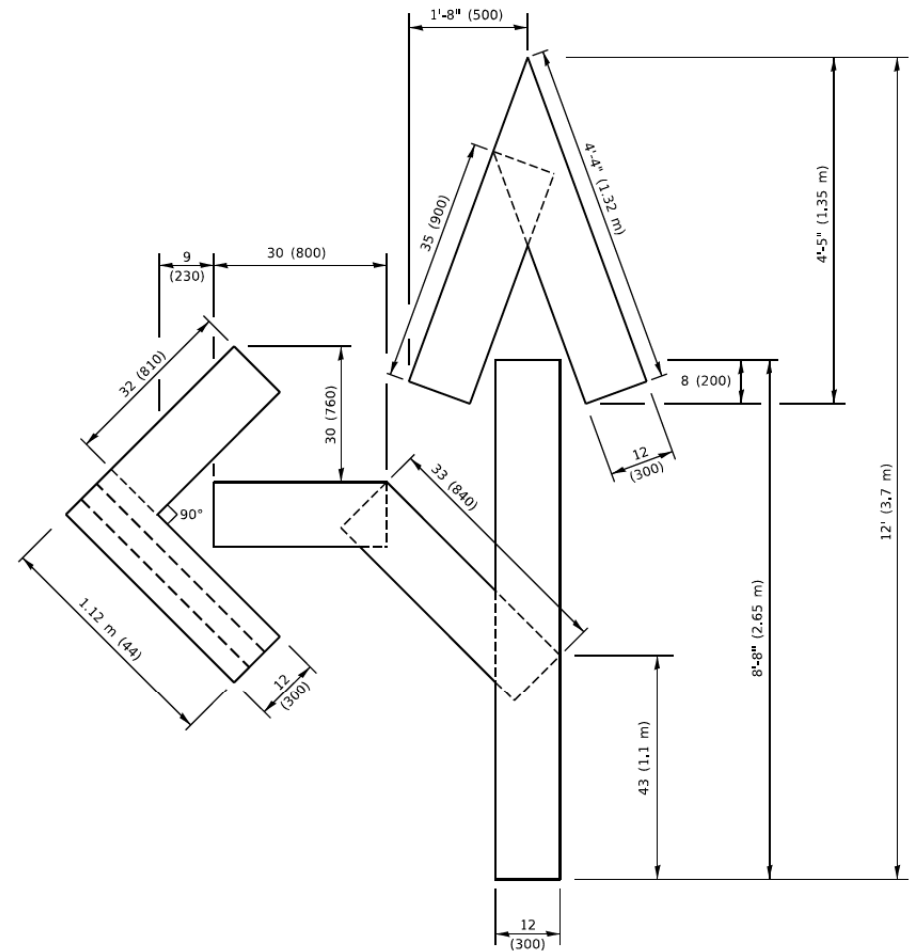
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE	
TYPICAL PAVEMENT MARKINGS	
SCALE: NONE	SHEET 1 OF 2 SHEETS
STA.	TO STA.

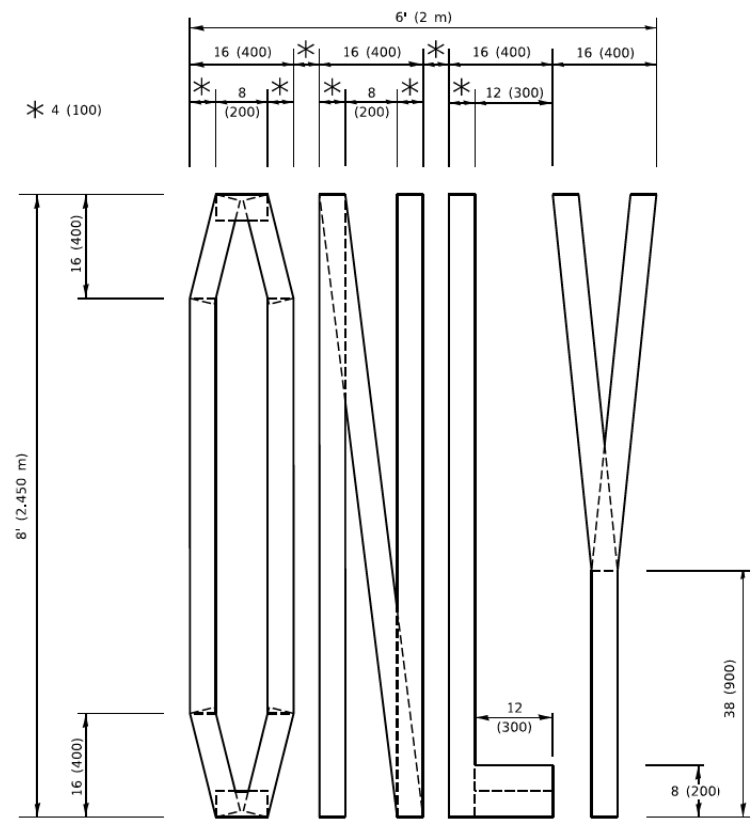
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH 20-00100-00-RS	DUPAGE	71	60
TC-13		CONTRACT NO. 61K22		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

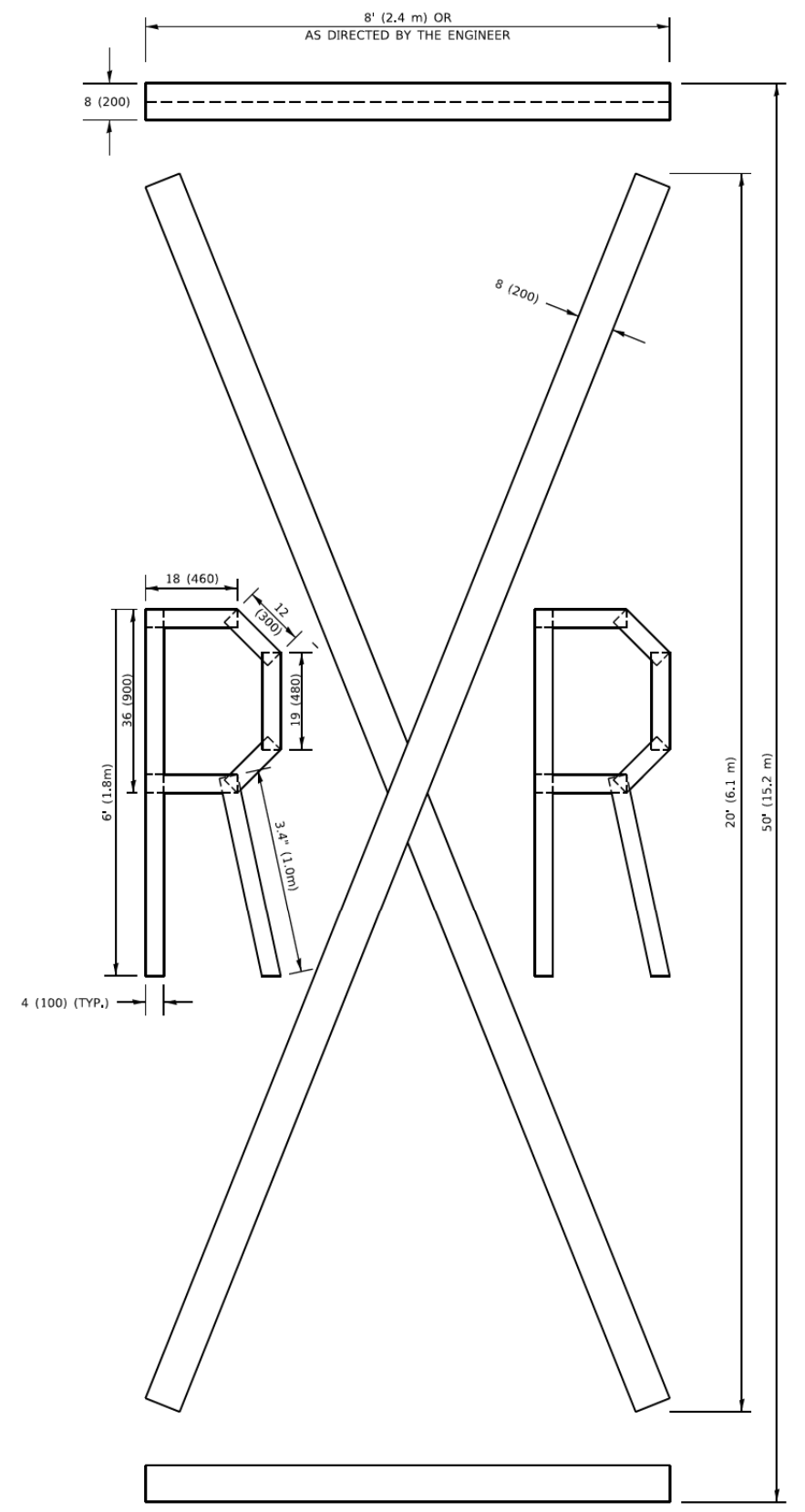


QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

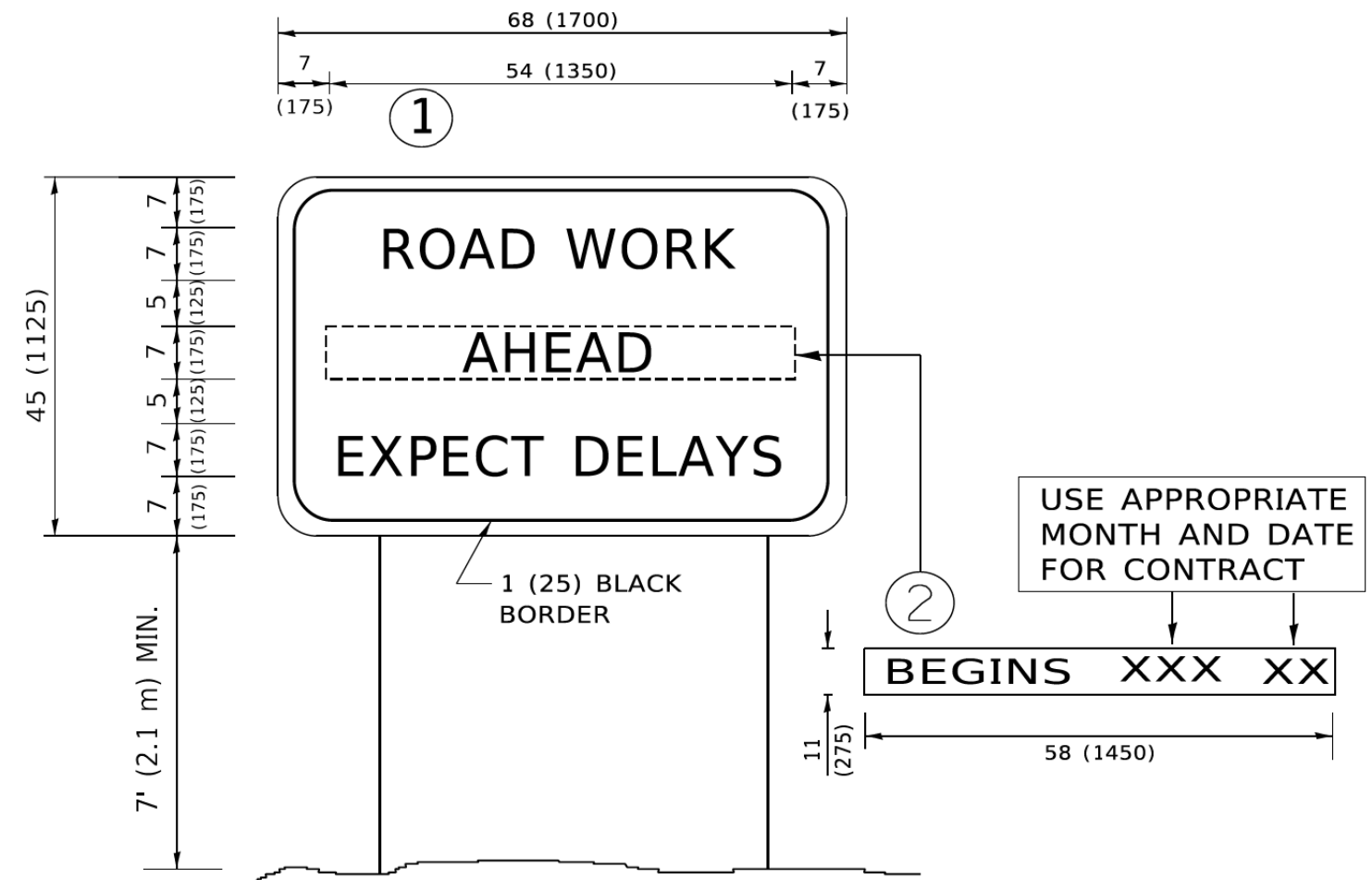
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PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH 20-00100-00-RS	DUPAGE	71	62
TC-16			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED - R. MIRS 12-11-97
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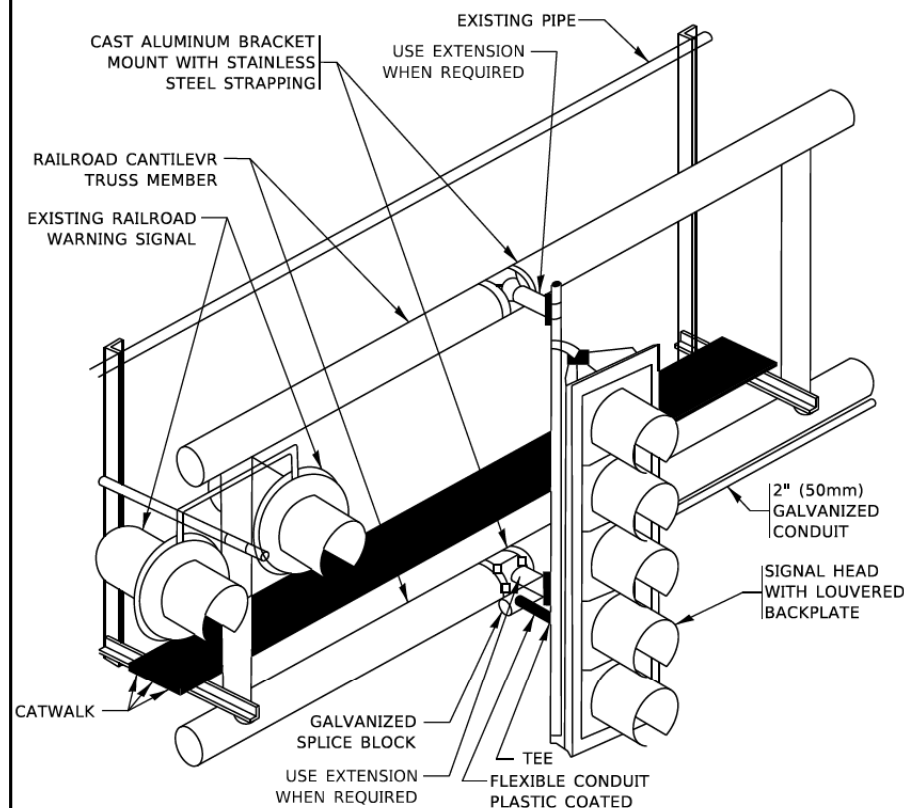
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	63
TC-22			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				

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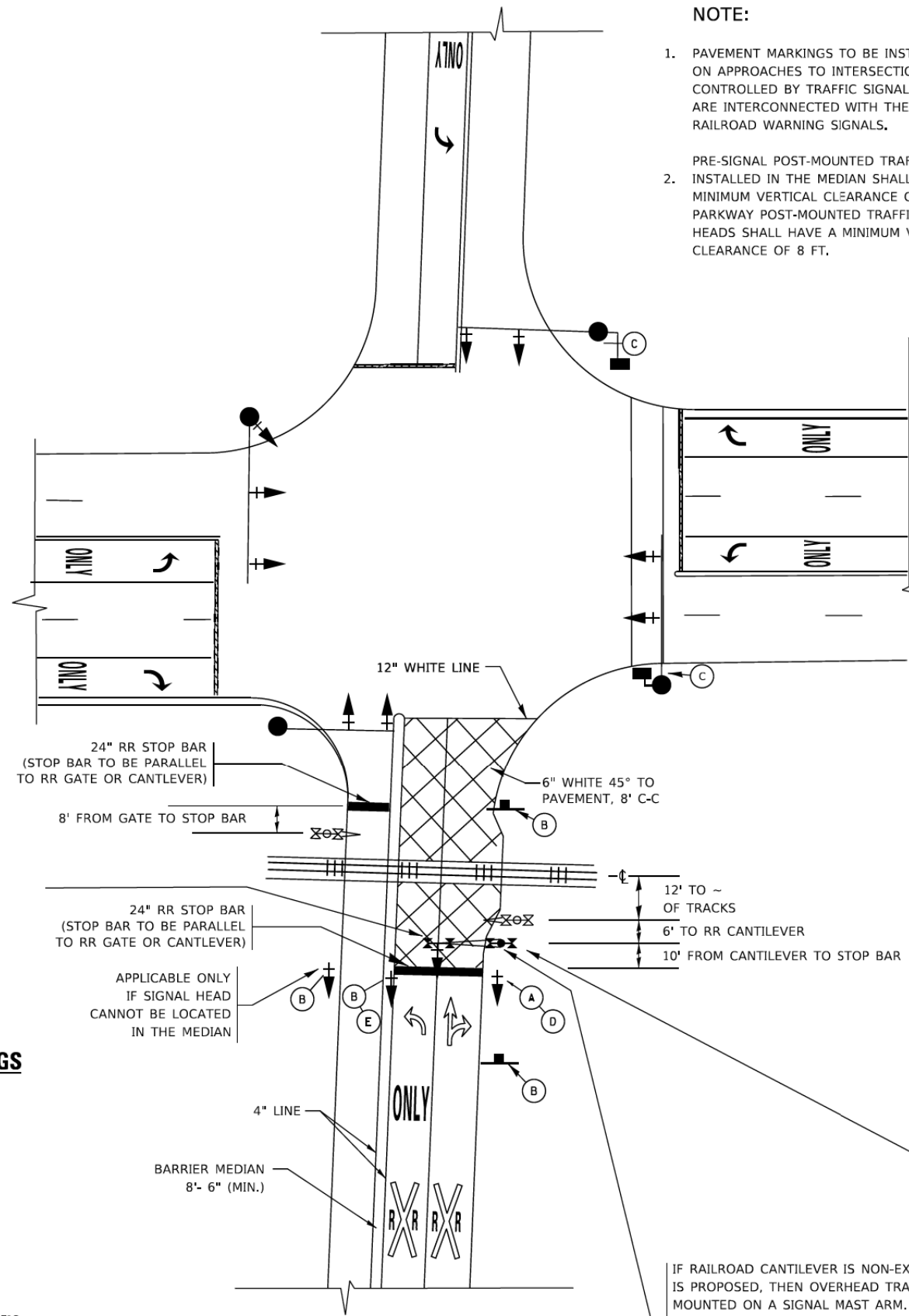
RAILROAD CANTILEVER SIGNAL HEAD MOUNTING
 USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION
 N.T.S.

SIGNING AND PAVEMENT MARKING AT RAILROAD CROSSINGS

SIGNING AND PAVEMENT MARKING TRAFFIC CONTROL STANDARD (TC-23) HAS BEEN DEVELOPED IN CONSULTATION WITH THE ILLINOIS COMMERCE COMMISSION AND THE U.S. DEPARTMENT OF TRANSPORTATION'S GRADE CROSSING SAFETY TASK FORCE. THIS STANDARD PROVIDES INFORMATION ON UPDATES TO THE PAVEMENT MARKING AND SIGNING DETAILS IN ORDER TO INCORPORATE CHANGES ADOPTED IN THE 2009 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE (MUTCD). THESE NEW DETAILS HAVE BEEN STUDIED AND TESTED BY THE DEPARTMENT AND ACCEPTED BY THE ILLINOIS COMMERCE COMMISSION.

THIS APPLIES TO PROJECTS WHICH INCLUDE RAILROAD INTERCONNECTED TRAFFIC SIGNALS, WITH OR WITHOUT PRE-SIGNALS. THIS STANDARD ALSO APPLIES TO NON-SIGNALIZED INTERSECTIONS THAT ARE WITHIN 81 FEET OF A RAILROAD GRADE CROSSING. THE ILLINOIS SUPPLEMENT TO THE MUTCD SHOULD BE CONSULTED FOR ADDITIONAL INFORMATION ON SIGN REQUIREMENTS AT NON-SIGNALIZED INTERSECTIONS NEAR RAILROAD GRADE CROSSINGS.

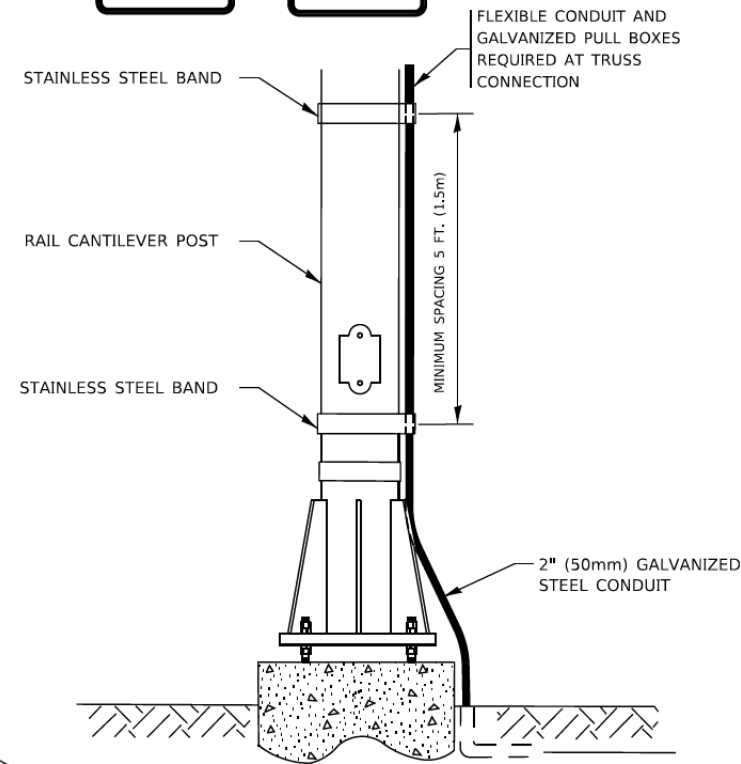
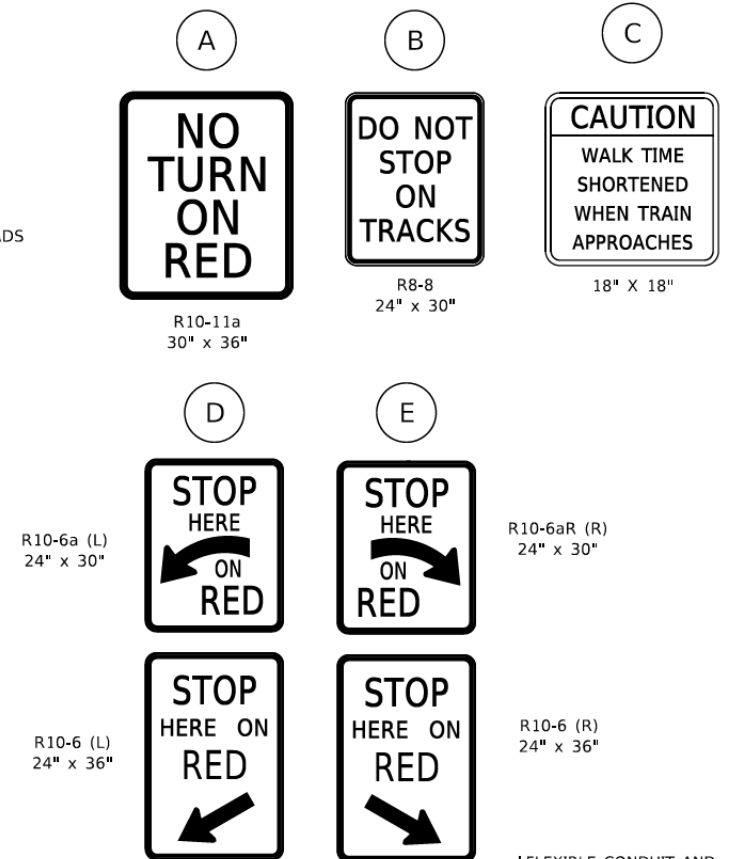
THESE DETAILS WILL BE INCLUDED IN A FUTURE UPDATE TO THE BUREAU OF OPERATIONS TRAFFIC POLICIES AND PROCEDURES MANUAL.



SIGNALIZED INTERSECTION WITH NEAR-SIDE TRAFFIC SIGNAL

NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- PRE-SIGNAL POST-MOUNTED TRAFFIC SIGNAL HEADS INSTALLED IN THE MEDIAN SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 4.5 FT. PARKWAY POST-MOUNTED TRAFFIC SIGNAL HEADS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 8 FT.



SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

IF RAILROAD CANTILEVER IS NON-EXISTANT AND NONE IS PROPOSED, THEN OVERHEAD TRAFFIC SIGNAL TO BE MOUNTED ON A SIGNAL MAST ARM. SIGNAL MAST ARM AND SIGNAL HEADS SHALL BE INSTALLED AS CLOSE AS PRACTICABLE TO THE RAILROAD TRACKS WITHOUT OBSTRUCTING ANY RAILROAD WARNING DEVICES. SIGNAL MAST ARM SHALL BE AT LEAST 12 FT. FROM NEAREST RAIL.

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	DRAWN -	REVISED - 04-26-12
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PLOT DATE = 8/22/2019	DATE -	REVISED - D.G. 8-22-19

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

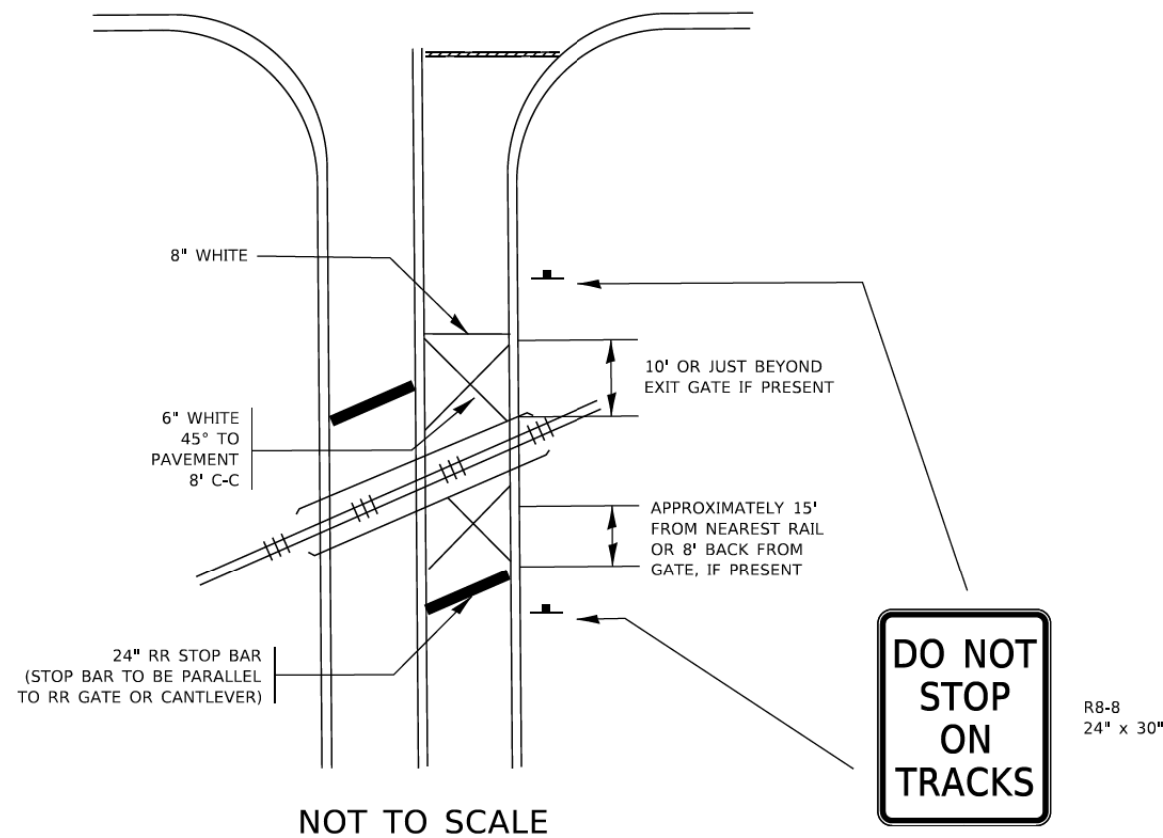
TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
 TREATMENT FOR RAILROAD CROSSINGS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH 20-00100-00-RS	DUPAGE	71	64
TC-23			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

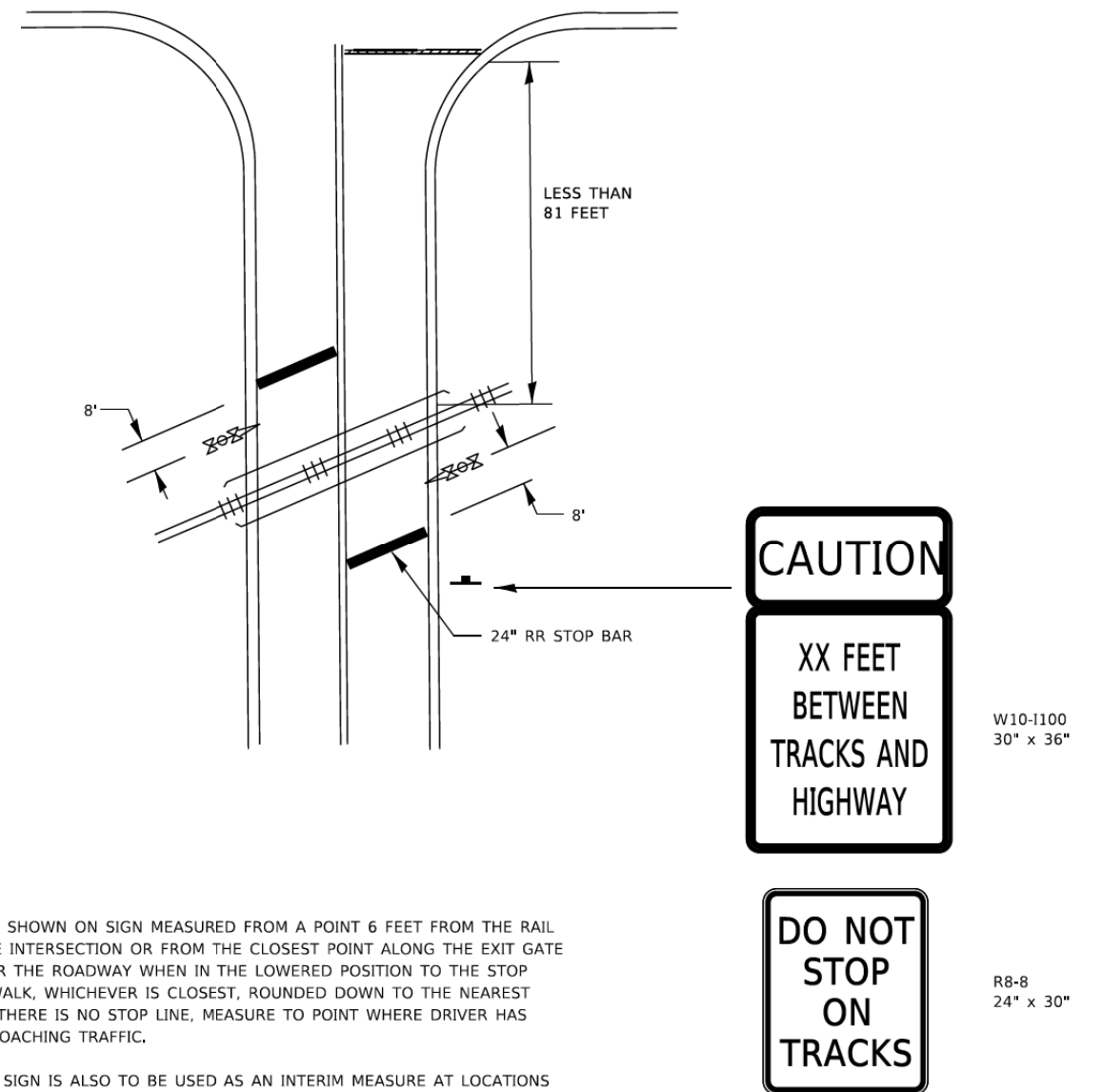
WITH SIGNALIZED INTERSECTION



NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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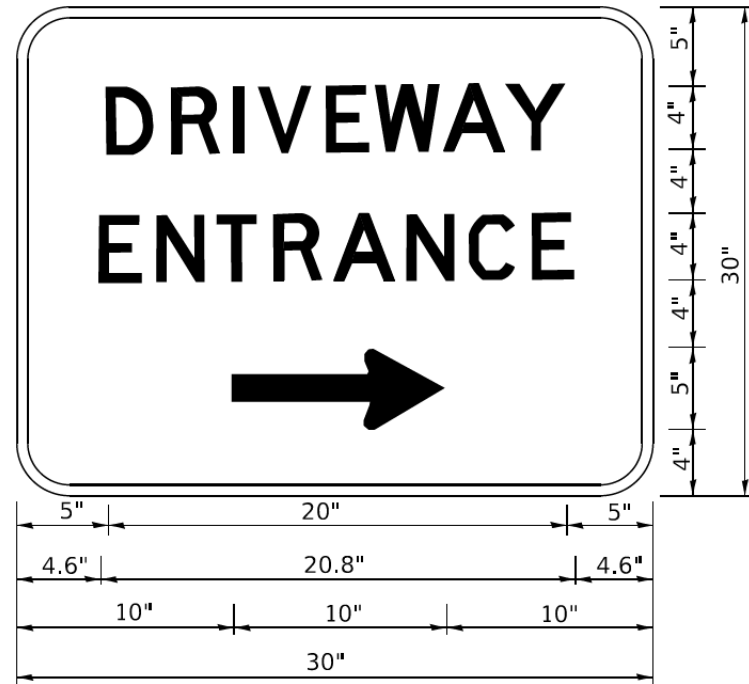
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PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
TREATMENT FOR RAILROAD CROSSINGS

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH	20-00100-00-RS	71	65
TC-23			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

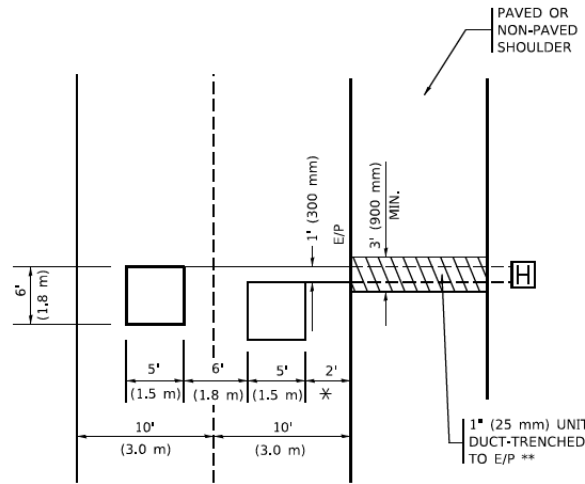
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH 20-00100-00-RS	DUPAGE	71	66
TC-26			CONTRACT NO. 61K22	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

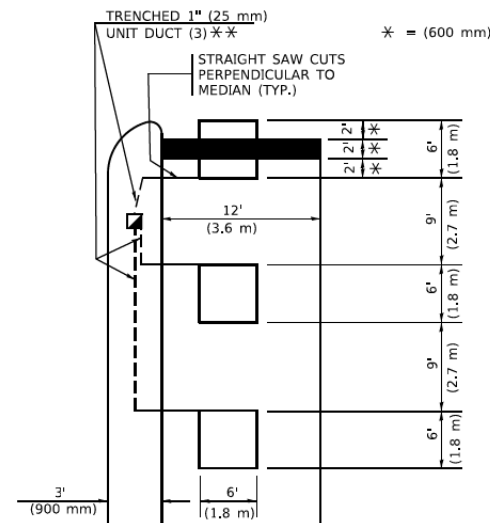


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

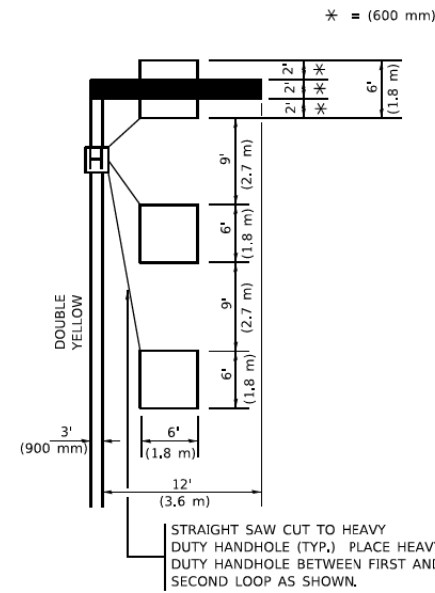


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

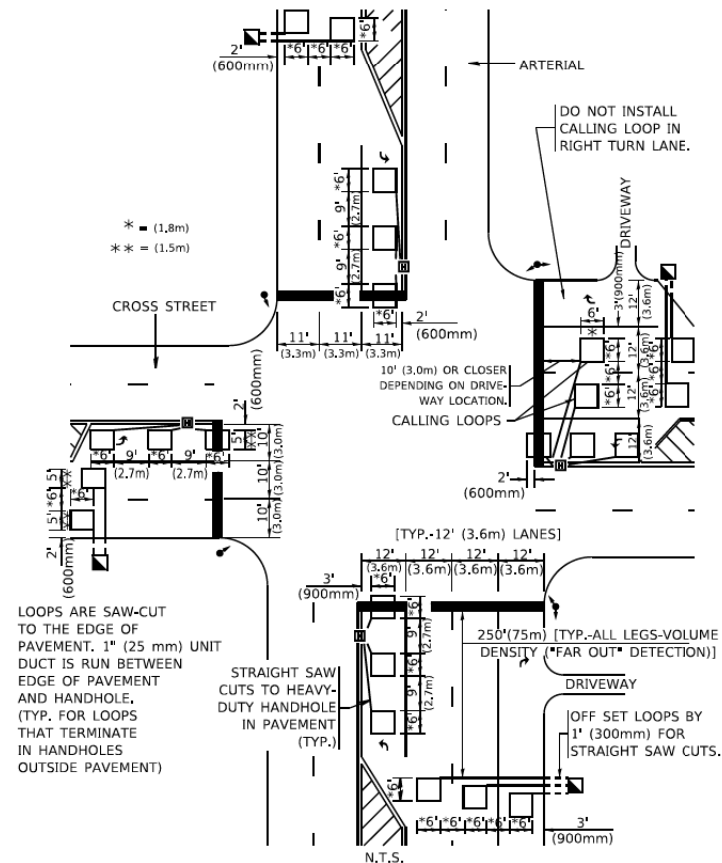
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



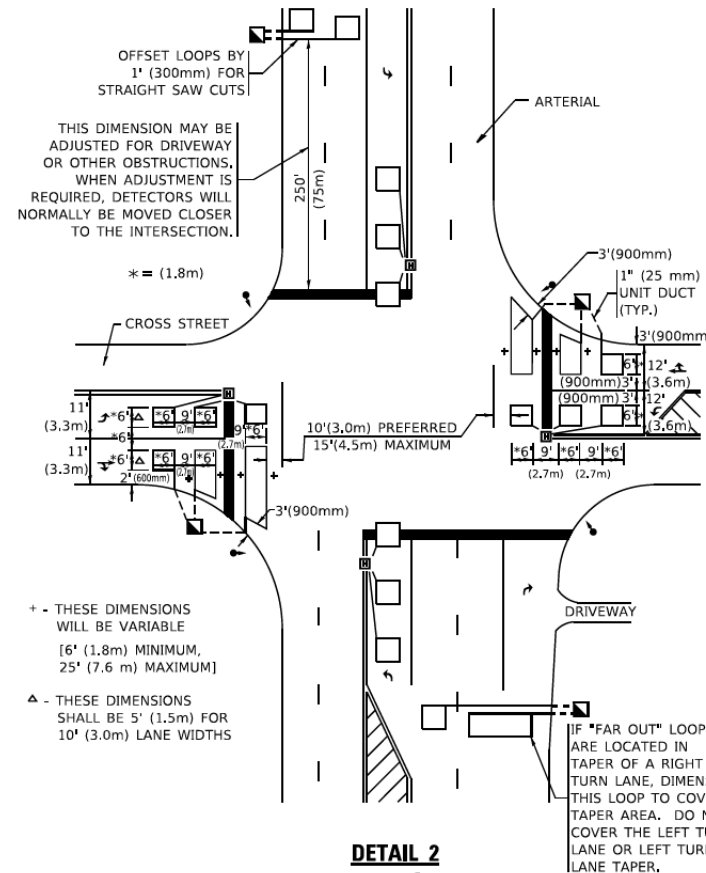
* = (1.8m)
 ** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



OFFSET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS

THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
 N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

MODEL: D:\draft...
 FILE NAME: PROJECTS\...
 PROJECT: DISTRICT 1 - DETECTOR LOOP INSTALLATION

USER NAME = footemj	DESIGNED -	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 3/4/2019	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

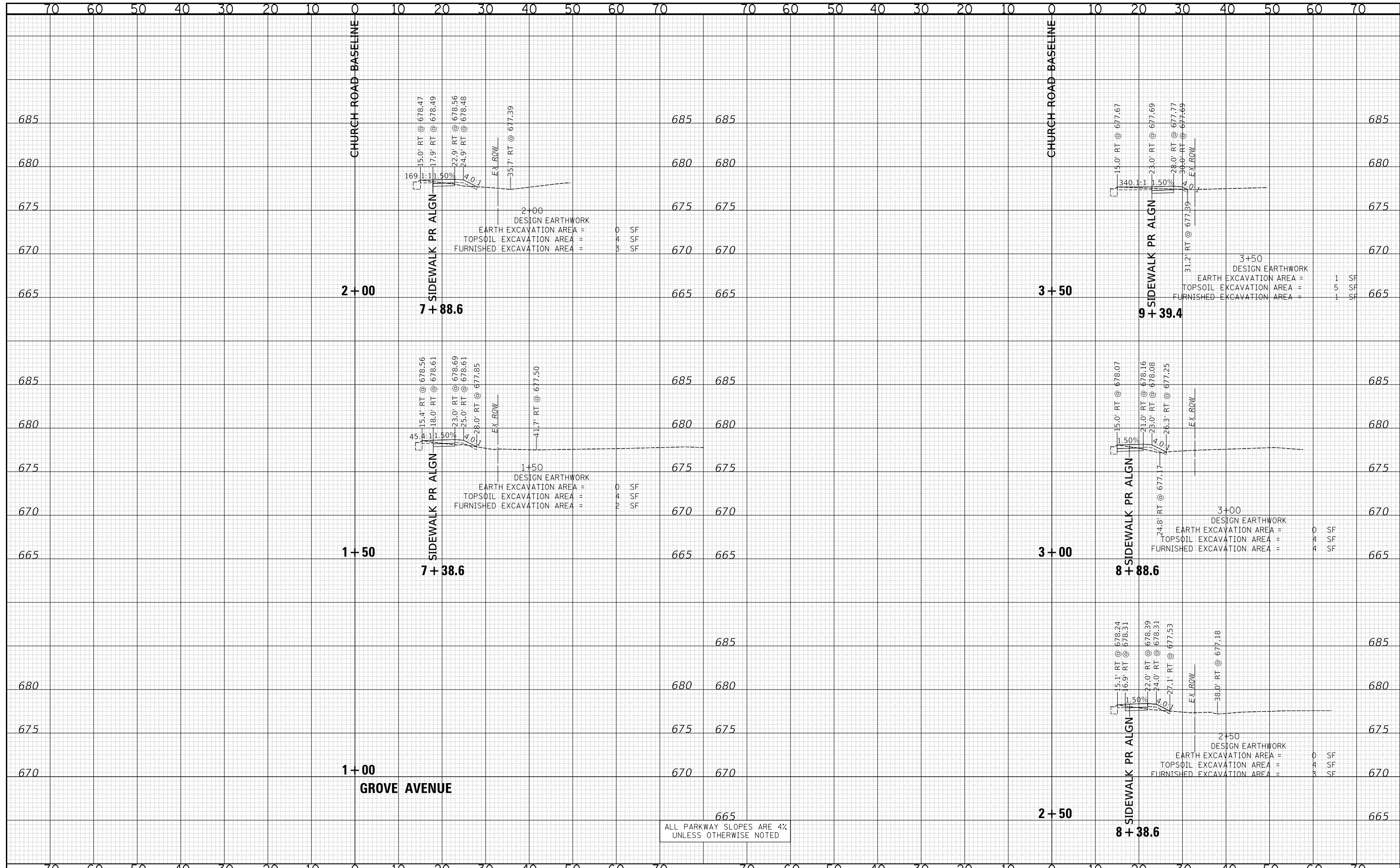
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION	
DETAILS FOR ROADWAY RESURFACING	
SCALE: NONE	SHEET 1 OF 1 SHEETS
STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667	CHURCH 20-00100-00-RS	DUPAGE	71	67
TS-07		CONTRACT NO. 61K22		
ILLINOIS / FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

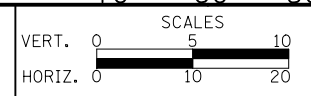
DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



CIVILTECH
 Two Pierce Place, Suite 1400
 Itasca, Illinois 60143
 Tel: 630.773.3900 Fax: 630.773.3975
 www.civiltechinc.com

DESIGNED - JRR	REVISED -
DRAWN - JRR	REVISED -
CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

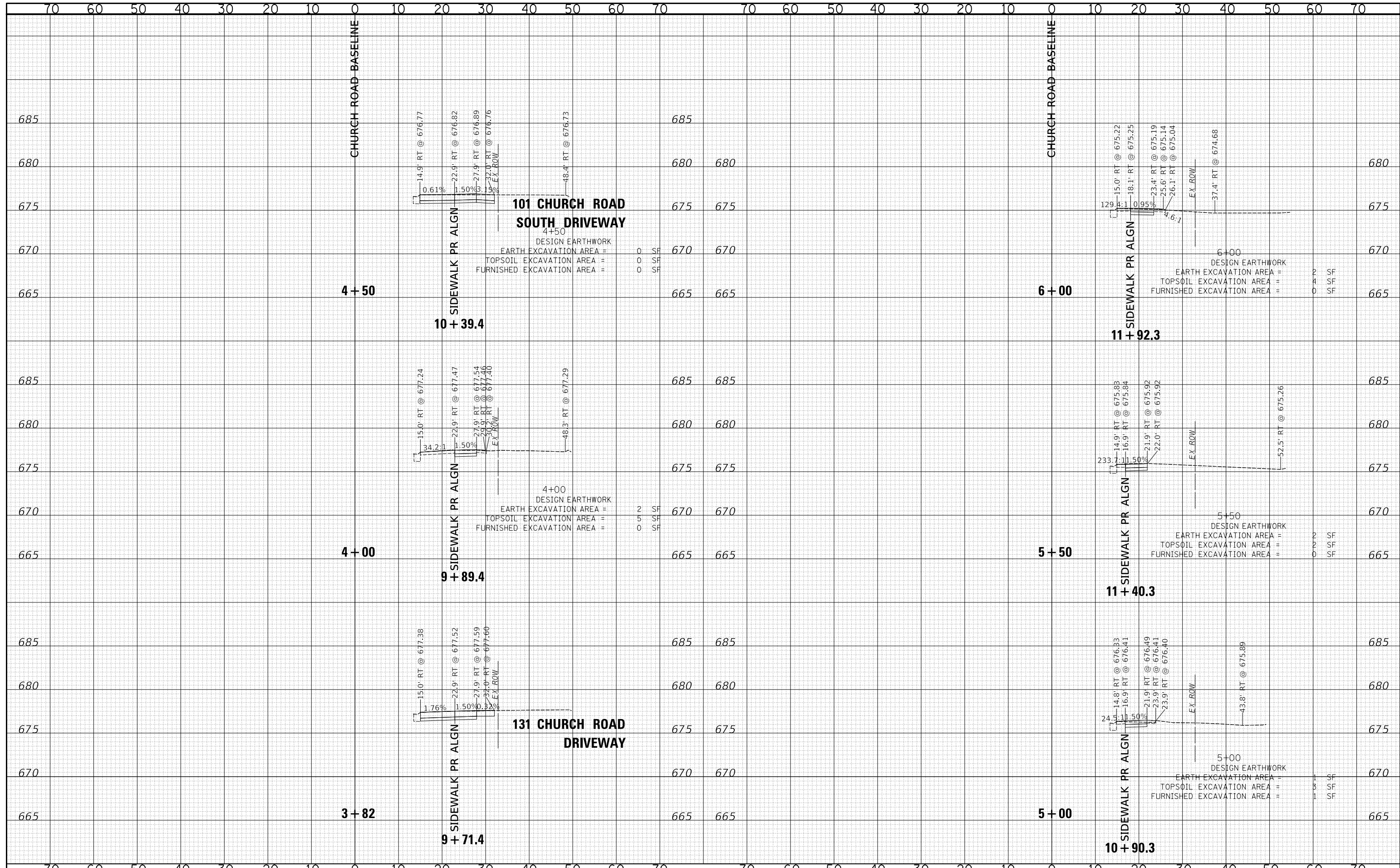


CHURCH ROAD AND MAIN STREET RESURFACING
CHURCH ROAD BASELINE SIDEWALK CROSS SECTIONS

F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	68
3001 MAIN				
CONTRACT NO. 61K22				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

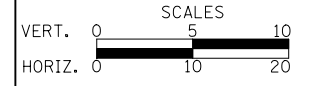
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SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
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CHECKED - KRK	REVISED -
DATE - 12/26/2023	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

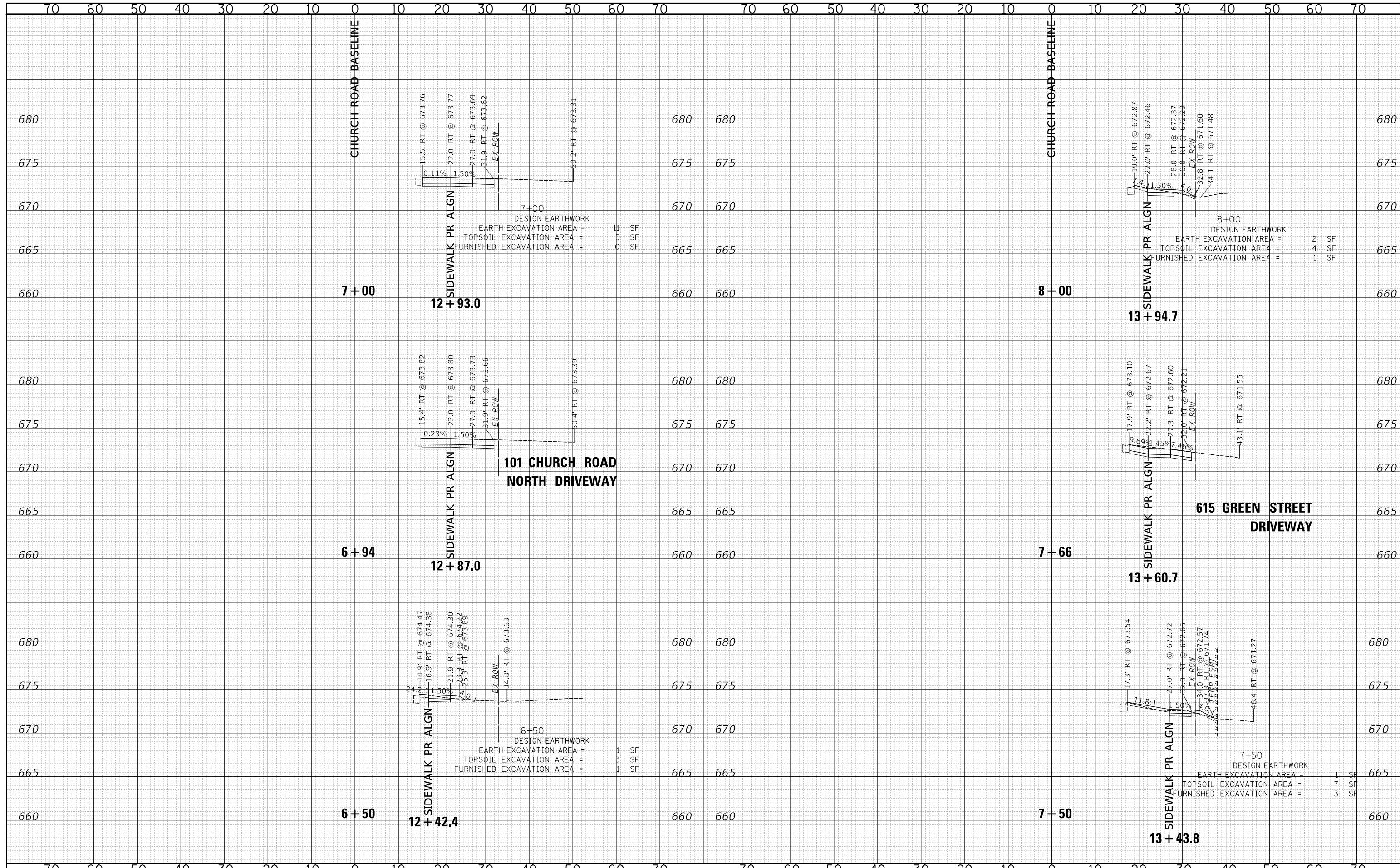


**CHURCH ROAD AND MAIN STREET RESURFACING
 CHURCH ROAD BASELINE SIDEWALK CROSS SECTIONS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	69
3001 MAIN		CONTRACT NO.	61K22	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
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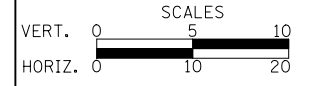
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SURVEYED	
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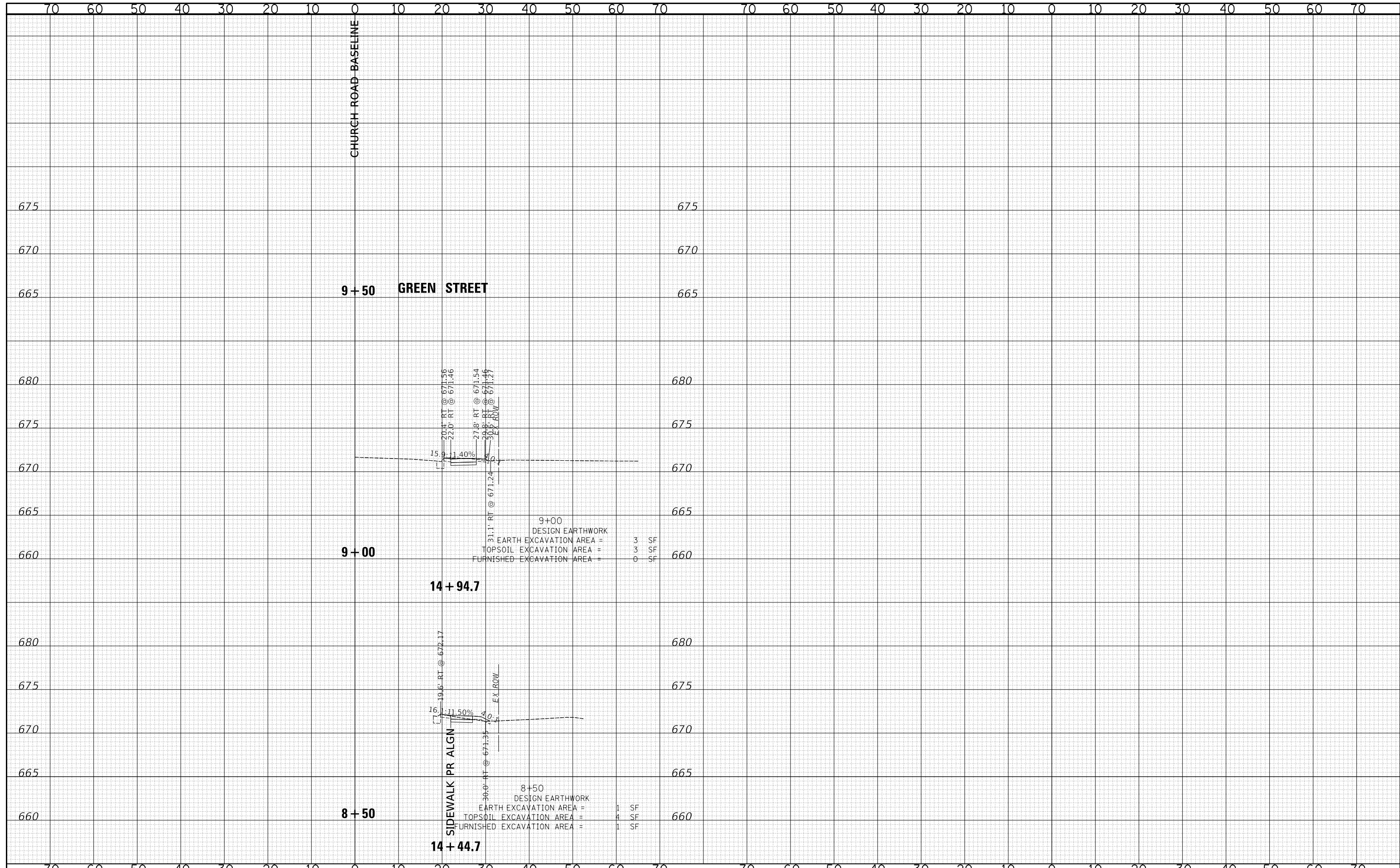
CHURCH ROAD AND MAIN STREET RESURFACING
CHURCH ROAD BASELINE SIDEWALK CROSS SECTIONS

SHEET NO. 3 OF 4 SHEETS STA. 6+50 TO STA. 8+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	70
3001 MAIN			CONTRACT NO. 61K22	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
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FINAL SURVEY	
NOTE BOOK	
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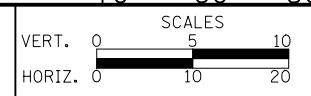
DATE	
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PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



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DATE - 12/26/2023	REVISED -

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DEPARTMENT OF TRANSPORTATION



CHURCH ROAD AND MAIN STREET RESURFACING
CHURCH ROAD BASELINE SIDEWALK CROSS SECTIONS

SHEET NO. 4 OF 4 SHEETS STA. 8+50 TO STA. 9+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2667 CHURCH	20-00100-00-RS	DUPAGE	71	71
3001 MAIN				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61K22	