

ITEM NO. 08A

IDOT LETTING: MARCH 8, 2024

PN012  
TOTAL SHEETS = 13

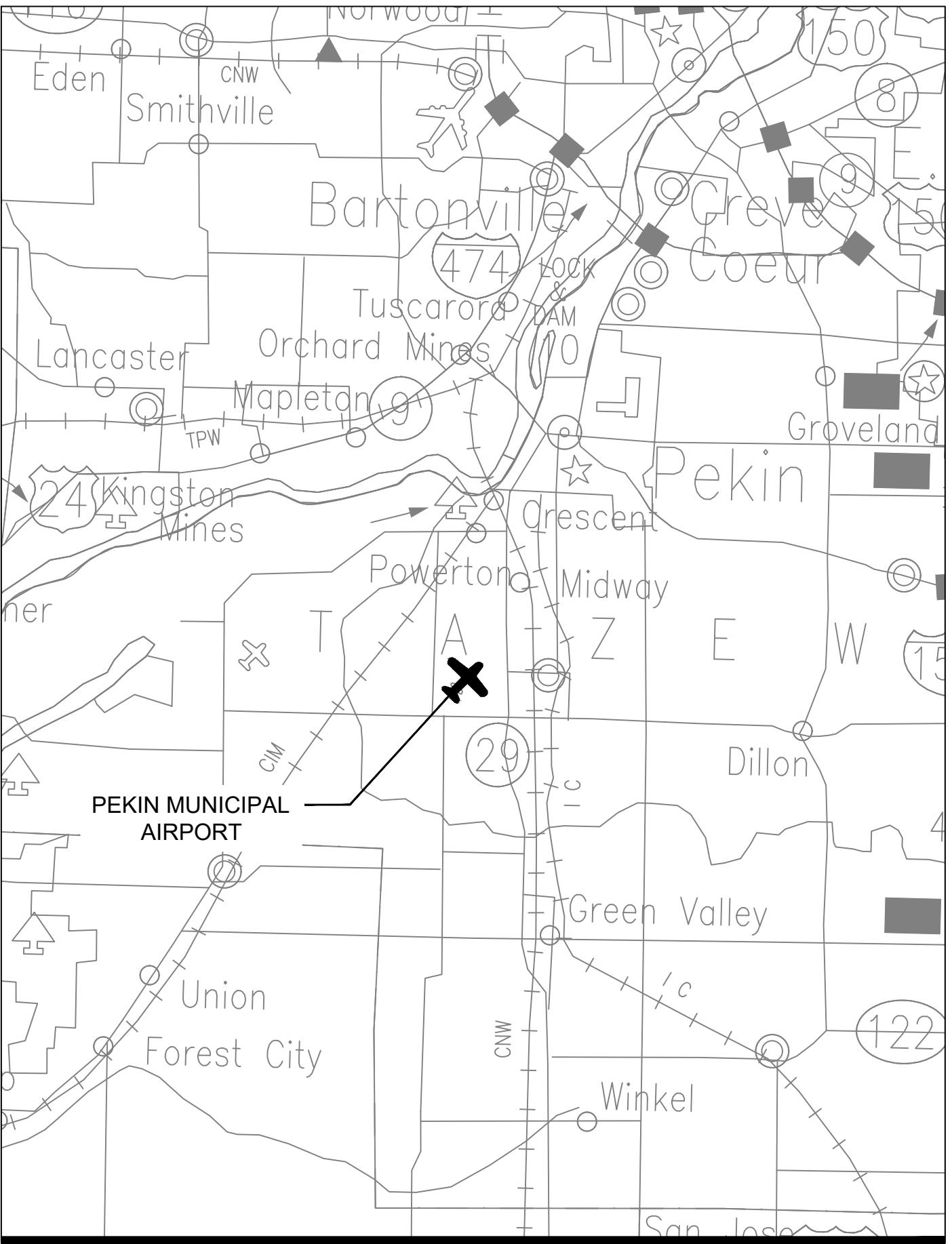
**CONSTRUCTION PLANS**

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

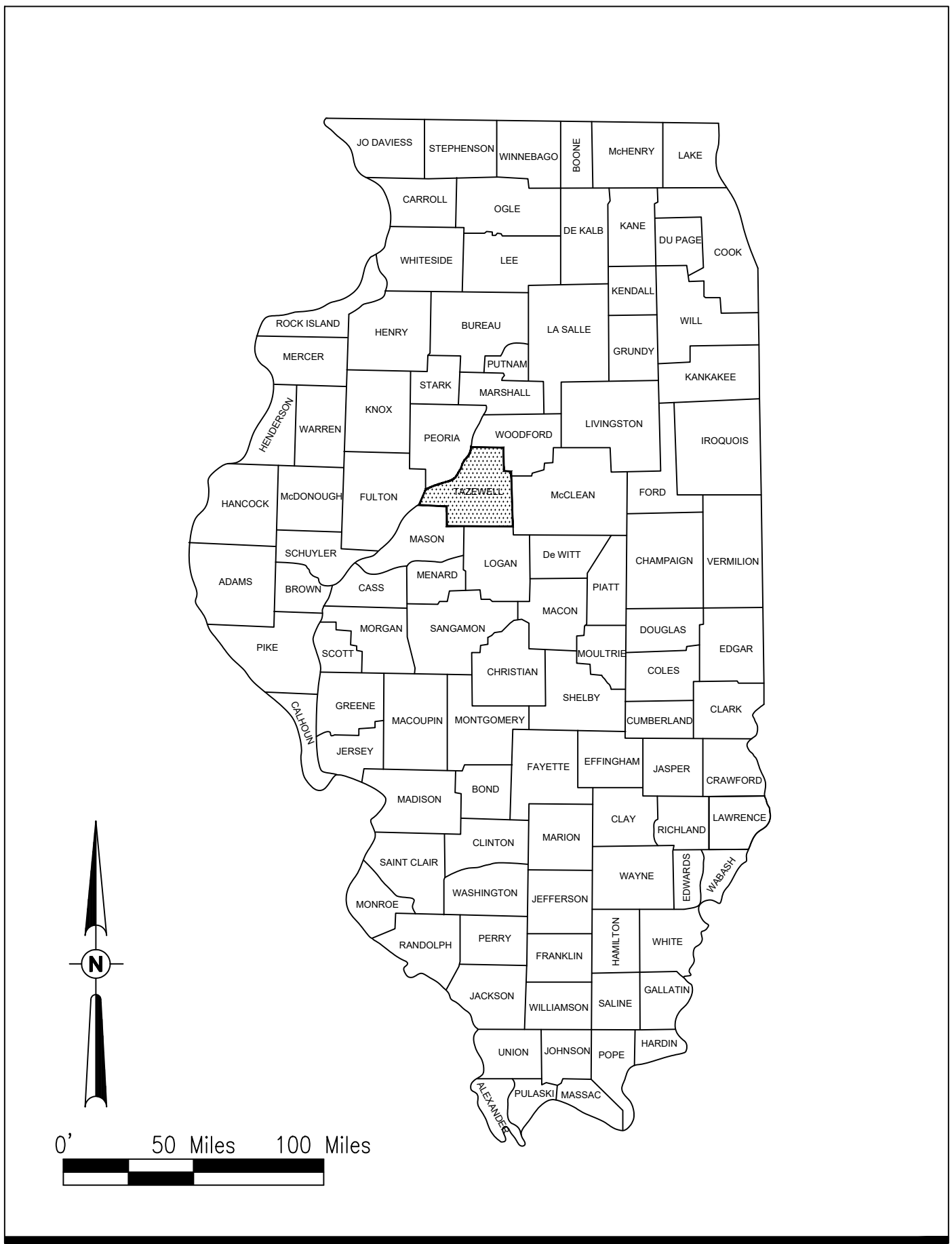
**CITY OF PEKIN, ILLINOIS  
PEKIN MUNICIPAL AIRPORT (C15)  
PEKIN, TAZEWELL COUNTY, ILLINOIS**

**IDA PROJECT NO. C15-5019  
SBG PROJECT NO. N/A**

**JANUARY 12, 2024  
(100% SUBMITTAL)**



**VICINITY MAP**



**LOCATION MAP**

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By

**HANSON**  
HANSON PROFESSIONAL SERVICES INC.  
1525 S. Sixth St.  
Springfield, Illinois 62703  
Telephone: 217.788.2450  
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*Lindsay Hausman*  
Lindsay D. Hausman, P.E.  
Project Manager - Civil Design

January 12, 2024  
Date

**PEKIN MUNICIPAL AIRPORT**  
111 South Capitol Street  
Pekin, Illinois 61554  
Telephone: 309.348.3693

*Mike Cruce*  
Mike Cruce  
Airport Manager

January 12, 2024  
Date





INDEX OF SHEETS	
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SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR150530	TRAFFIC MAINTENANCE	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	1749	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	5724	
AR154606	GRANULAR DRAINAGE SUBBASE-6"	SQ YD	5724	
AR156510	SILT FENCE	FOOT	1180	
AR209604	CRUSHED AGG. BASE COURSE-4"	SQ YD	5638	
AR401613	BIT. SURF. CSE.-METHOD I, SUPERPAVE	TON	639	
AR401660	SAW & SEAL BIT. JOINTS	FOOT	114	
AR403613	BIT. BASE CSE.-METHOD I, SUPERPAVE	TON	639	
AR602510	BITUMINOUS PRIME COAT	GALLON	1666	
AR603510	BITUMINOUS TACK COAT	GALLON	833	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	965	
AR620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	965	
AR705506	6" PERFORATED UNDERDRAIN	FOOT	1197	
AR705640	UNDERDRAIN CLEANOUT	EACH	4	
AR901510	SEEDING	ACRE	0.36	
AR905520	TOPSOILING (FROM OFF SITE)	CU YD	286	
AR908510	MULCHING	ACRE	0.36	

**SCOPE OF WORK:**

PROJECT SHALL CONSIST OF CONSTRUCTION OF T-HANGAR PAVEMENT AND ACCESS ROAD AT THE PEKIN MUNICIPAL AIRPORT. ASSOCIATED WORK ITEMS INCLUDE SITE PREPARATION AND GRADING, UNDERDRAIN INSTALLATION, FULL DEPTH BITUMINOUS PAVEMENT, PAVEMENT MARKING, EROSION CONTROL AND SITE RESTORATION.

**GENERAL NOTES**

- THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
- ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- THE LOCATION OF THE ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
- THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
- APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- NPDES PERMIT - THIS PROJECT WILL DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS REQUIRED.
- MATERIAL CERTIFICATIONS - MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
- CERTIFIED PAYROLLS - THE RESIDENT ENGINEER CANNOT FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.



PROJECT IS LOCATED IN NORTH 1/2 OF SECTION 33 & THE NORTHEAST 1/4 OF SECTION 32, CINCINNATI TOWNSHIP, TAZEVELL COUNTY

**NOTES**

- VERTICAL COORDINATES ARE IN NAD 88. HORIZONTAL COORDINATES ARE IN STATE PLANE NAD 83 ILLINOIS WEST.
- STATIONS, OFFSETS AND ELEVATIONS SHOWN ARE IN FEET.
- THE APPROACH END OF RUNWAY 9 IS STATION 100+00.
- THE AIRPORT REFERENCE CODE FOR RUNWAY 9-27 IS B-II. BOTH RUNWAY 9 AND 27 HAVE A NON-PRECISION APPROACH WITH VISIBILITY MINIMUM OF 1 MILE.

**RUNWAY END COORDINATES**

DESCRIPTION	LATITUDE	LONGITUDE	RUNWAY STATION	RUNWAY ELEVATION
RUNWAY 9 END	40°29'17.286" N	89°41'05.586" W	100+00	530.2
RUNWAY 27 END	40°29'17.724" N	89°40'00.883" W	150+00	525.7

CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD

IDA No: C15-5019

Contract No. PN012

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JANUARY 12, 2024  
PROJECT NO: 19A0112D  
CAD FILE: c-002-S00.DWG  
DESIGN BY: JP 11/27/23  
DRAWN BY: JP 11/27/23  
REVIEWED BY: LDH 1/5/24

SHEET TITLE

SUMMARY OF QUANTITIES, SHEET INDEX AND GENERAL NOTES



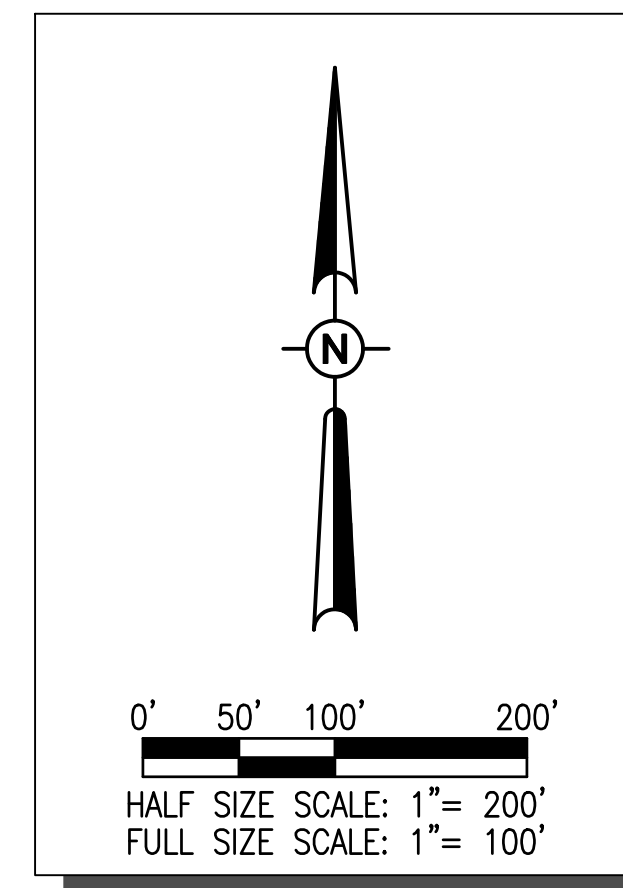


SURVEY CONTROL POINTS				
NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
△	PEKPORT STAINLESS ROD IN SLEEVE	1392306.49	2435501.20	521.41
△	109 MAG NAIL	1392991.00	2435749.00	522.76
△	NW CORNER CONC. BASE FOR GATE OPENER	1392752.73	2435615.73	523.70

CRITICAL POINTS					
POINT #	DESCRIPTION	ELEVATION	LATITUDE	LONGITUDE	HEIGHT (AGL)
1	CONST. EQUIP.	518.51	040° 29' 23.22"	-089° 40' 18.35"	25'
2	CONST. EQUIP.	521.57	040° 29' 23.24"	-089° 40' 14.86"	25'
3	CONST. EQUIP.	522.50	040° 29' 25.84"	-089° 39' 58.09"	25'

**NOTES**

- CONTRACTOR SHALL PROTECT HAUL ROUTE AND ALL EXISTING ADJACENT PAVEMENTS TO REMAIN IN PLACE FROM DAMAGE THROUGH USE OF TEMPORARY COVER MATERIAL, AND/OR AVOIDING TRAVERSING WITH EQUIPMENT.
- WORK MAY NOT BEGIN UNTIL BARRICADES HAVE BEEN PLACED.
- CONTRACTOR SHALL TRANSPORT OR ESCORT EMPLOYEES FROM CONSTRUCTION AREA TO EMPLOYEE PARKING AREA



PROPOSED	LEGEND
	CONSTRUCTION LIMITS
	PROPOSED PAVEMENT AREA
	STAGING AREA
	LOW PROFILE BARRICADE
	TEMPORARY HAUL ROUTE / ACCESS ROUTE
	PAVEMENT OFF LIMITS TO CONTRACTOR

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

IDA No: C15-5019

Contract No. PN012


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JANUARY 12, 2024  
PROJECT NO: 19A0112D  
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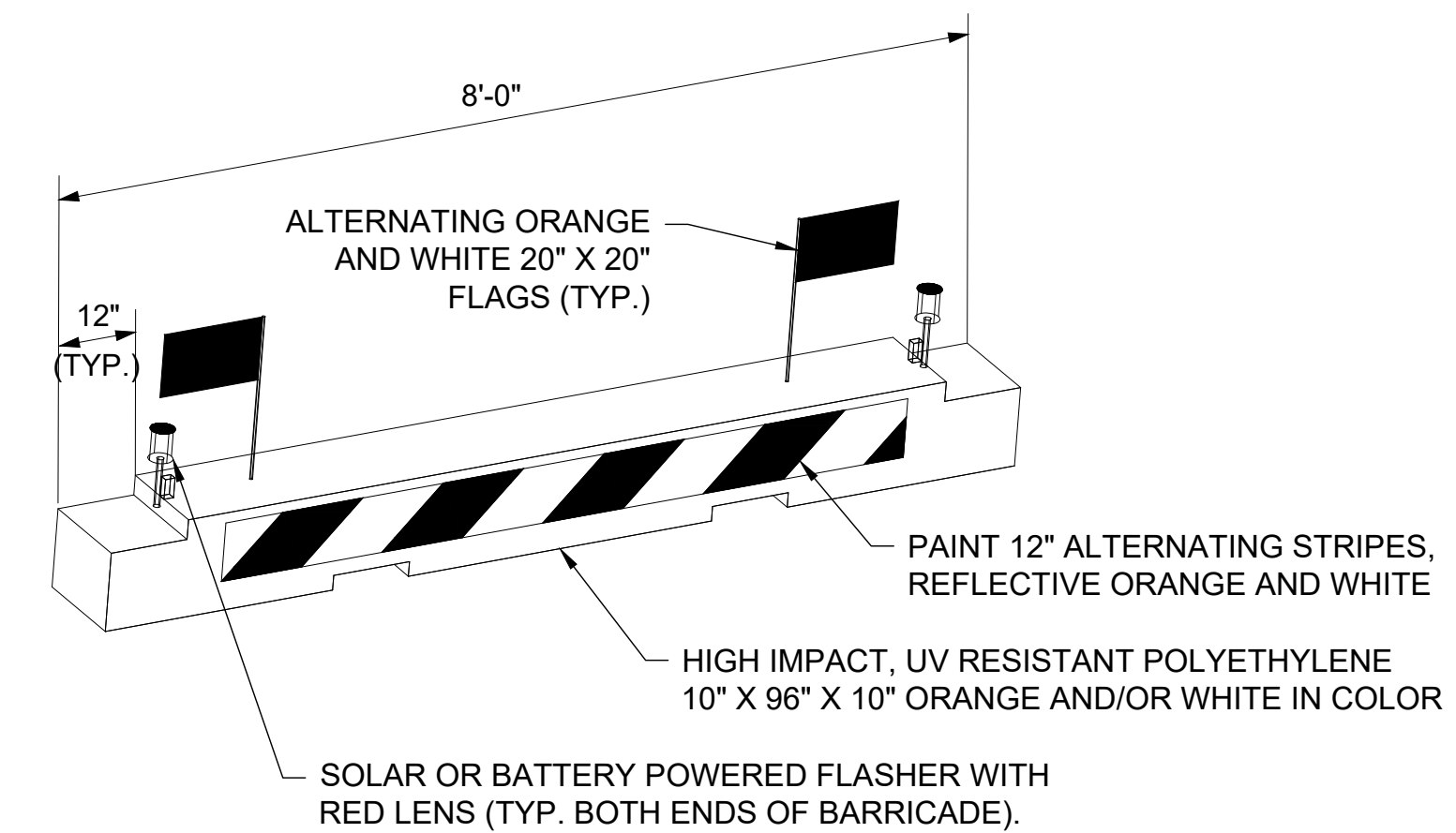
**CONSTRUCTION SAFETY AND PHASING PLAN**



**SAFETY NOTES**

- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN. ANY MODIFICATIONS TO THIS PLAN MUST BE APPROVED BY THE FAA AND THE AIRPORT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- PRIOR TO ACCESSING THE AIRFIELD, ANY DESIGNATED CONTRACTOR OR SUBCONTRACTOR EMPLOYEES WHO WILL BE OPERATING OR ESCORTING A VEHICLE ON AN ACTIVE AREA OF THE AIRFIELD MUST BE FAMILIAR WITH THE "FAA GUIDE TO GROUND VEHICLE OPERATIONS", AND KEEP A HARD COPY IN THE VEHICLE FOR REFERENCE. THE GUIDE CAN BE FOUND AT: [https://www.faa.gov/airports/runway\\_safety/media/Ground\\_Vehicle\\_Guide\\_Proof\\_Final.pdf](https://www.faa.gov/airports/runway_safety/media/Ground_Vehicle_Guide_Proof_Final.pdf)
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE (OPEN) AIRFIELD PAVEMENT AREA WITHOUT AN APPROPRIATE ESCORT. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF RWY 9/27 (DISTANCES MEASURED FROM ACTIVE CENTERLINES) UNLESS CLOSED OR OTHERWISE NOTED. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL ALSO NOT BE PERMITTED WITHIN 65.5' OF ANY ACTIVE AIRPORT TAXIWAY CENTERLINE OR APRON UNLESS OTHERWISE NOTED.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRE-CONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 65.5' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION", LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND ADDITIONAL AIRSPACE APPROVAL BY THE FAA. AIRSPACE APPROVALS REQUIRE CONSIDERABLE LEAD TIME AND SHOULD BE REQUESTED WELL IN ADVANCE.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEP, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- CONTRACTOR SHALL TAKE MEASURES TO AVOID TRACKING BITUMINOUS TACK COAT ASSOCIATED WITH PAVING PROJECTS ONTO ADJACENT PAVEMENT AREAS, ESPECIALLY GROOVED RUNWAY PAVEMENTS, UNLESS SUFFICIENT PROTECTION HAS BEEN APPLIED. HEAVY TRACKING OR DAMAGE TO ADJACENT PAVEMENTS AND GROOVED SURFACES MAY BE CAUSE FOR STOPPING THE WORK UNTIL ACCEPTABLE PROTECTION OR CHANGE IN WORK METHODS HAS BEEN PROVIDED.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.

- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR IMPLEMENTING MEASURES TO CONTROL OR AVOID CREATING ATTRACTANTS TO WILDLIFE. MEASURES MAY INCLUDE CONTINUOUSLY REMOVING ANY WASTE OR LOOSE MATERIALS, PLACEMENT OF MATERIALS IN APPROPRIATE STORAGE CONTAINERS, PROPERLY MAINTAINING FENCES AND GATES TO PREVENT ACCESS, AND PREVENTING PONDING OF WATER THROUGHOUT THE SITE.
- UNLESS SPECIFIED OTHERWISE, COST FOR SAFETY, STAGING, AND TRAFFIC MAINTENANCE ITEMS SHALL BE PAID UNDER ITEM AR150530. SEPARATE PAYMENT SHALL NOT BE MADE.
- THE CONTRACTOR SHALL HAVE THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS DETAILED IN THE SPECIAL PROVISIONS, SUBMITTED AND APPROVED PRIOR TO BEING ISSUED "NOTICE TO PROCEED".
- ALL PAVEMENT CLOSURES SHALL BE COORDINATED WITH AIRPORT MANAGEMENT A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME TO ALLOW FOR THE PROPER COORDINATION. AIRPORT MANAGEMENT HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.



**LOW PROFILE AIRCRAFT BARRICADE DETAIL**

**BARRICADE NOTES**

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE INTERLOCKED END TO END OVER THE LENGTH OF THE PAVEMENT WHERE PROTECTING OPEN RUNWAYS, AND SPACED END TO END A MAXIMUM OF 4 FEET IN OTHER ALL OTHER AREAS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE PAID UNDER ITEM AR150530

JAN 10, 2024 9:02 AM PEARC00937 1:19:08S19A011219A0112D\CAD\AIRPORT\ISHEET\C-100-CSPP.DWG



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Illinois Licensed  
Professional Service Corporation  
#184-001084



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111 South Capitol Street  
Pekin, Illinois 61554  
Telephone: 309.477.2300

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

IDA No: C15-5019

Contract No. PN012

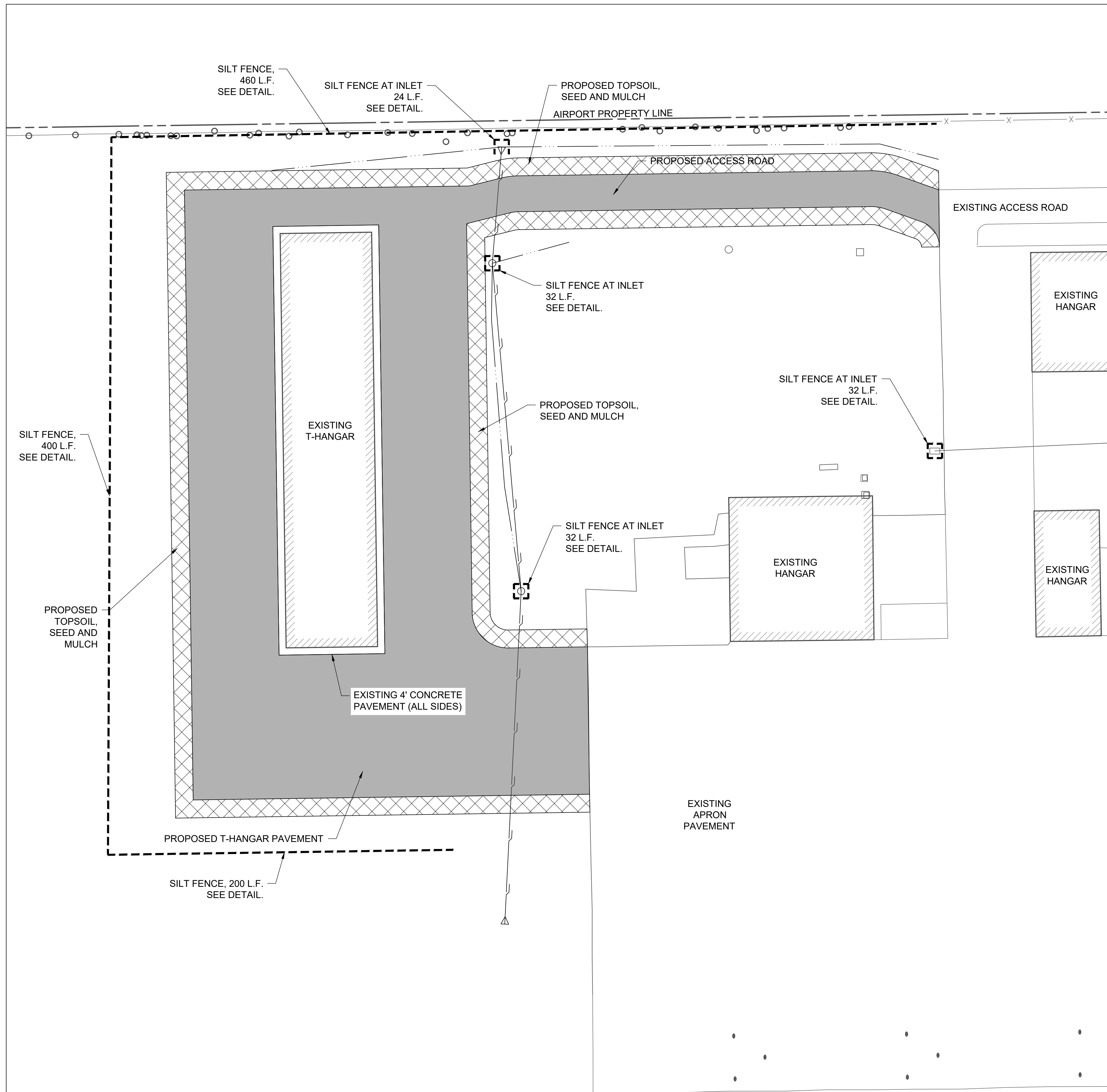
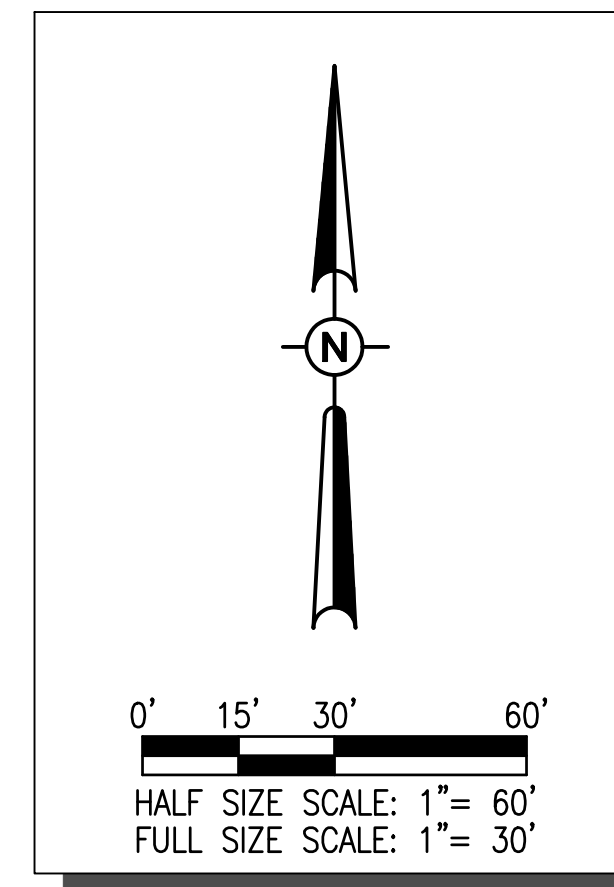
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		DES	DWN	REV

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PROJECT NO: 19A0112D  
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DRAWN BY: JP 11/27/23  
REVIEWED BY: LDH 1/5/24

SHEET TITLE

**CONSTRUCTION SAFETY AND PHASING PLAN NOTES**





**CONTRACTOR'S CERTIFICATION STATEMENT**

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

**PROJECT INFORMATION:**

AIRPORT: \_\_\_\_\_ PROJECT: \_\_\_\_\_

PROJECT NO: \_\_\_\_\_ COUNTY: \_\_\_\_\_

CONTRACT NUMBER: \_\_\_\_\_

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: \_\_\_\_\_ DATE: \_\_\_\_\_

PRINTED NAME: \_\_\_\_\_ TITLE: \_\_\_\_\_

NAME OF FIRM: \_\_\_\_\_

STREET ADDRESS: \_\_\_\_\_

CITY, STATE, ZIP: \_\_\_\_\_

PHONE NUMBER: \_\_\_\_\_

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

PROPOSED	LEGEND
-----	SILT FENCE/INLET PROTECTION

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

IDA No: C15-5019

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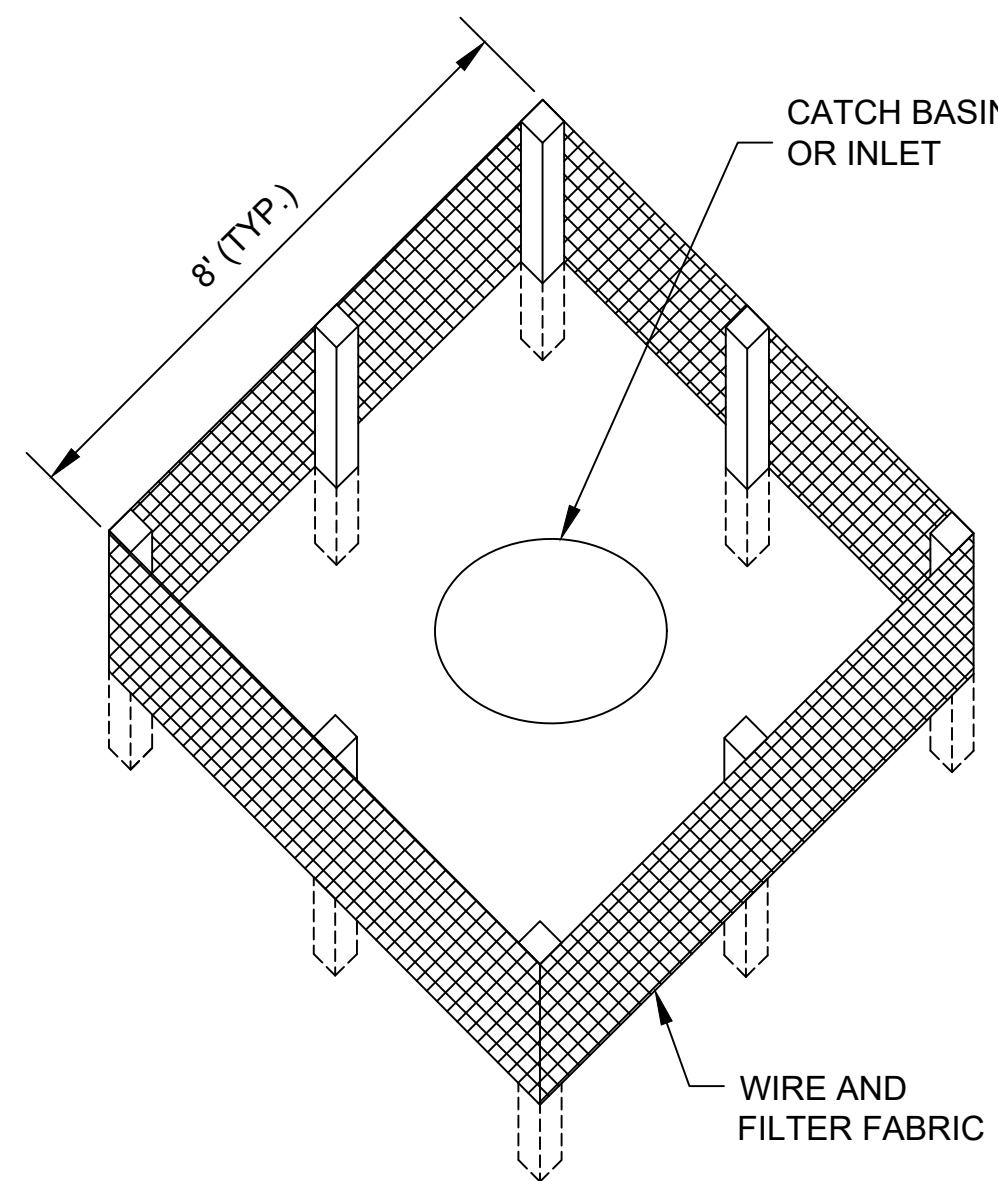
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		DES	DWN	REV

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PROJECT NO: 19A0112D  
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SHEET TITLE

**STORMWATER POLLUTION PREVENTION PLAN**

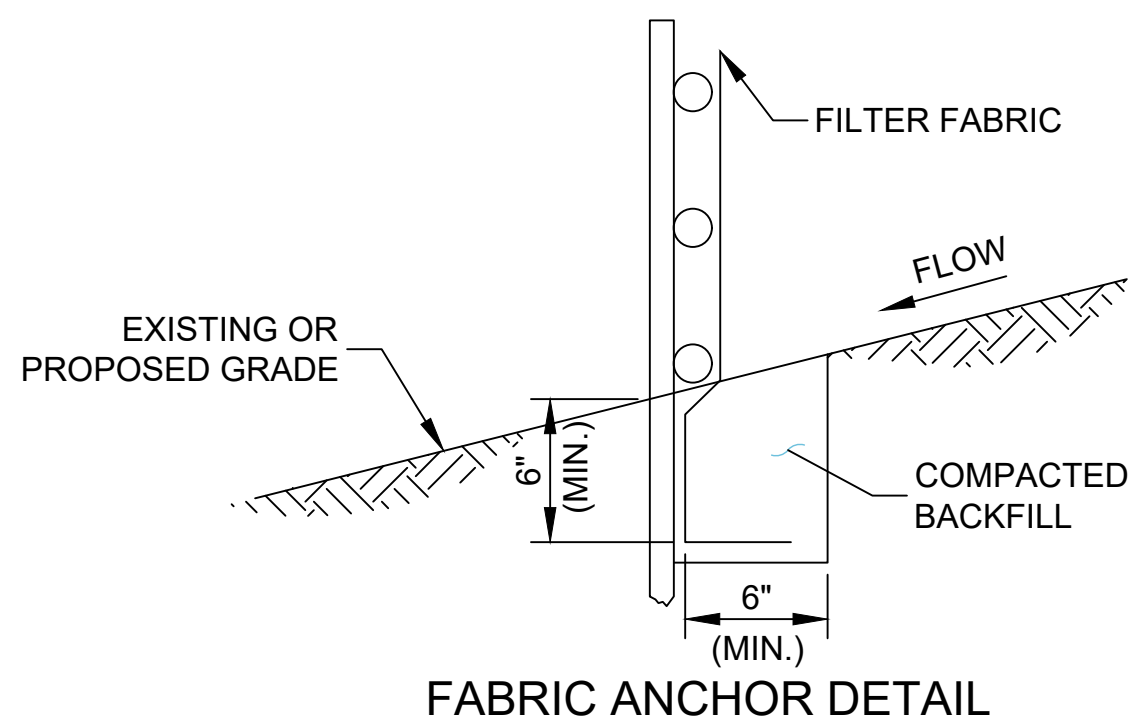




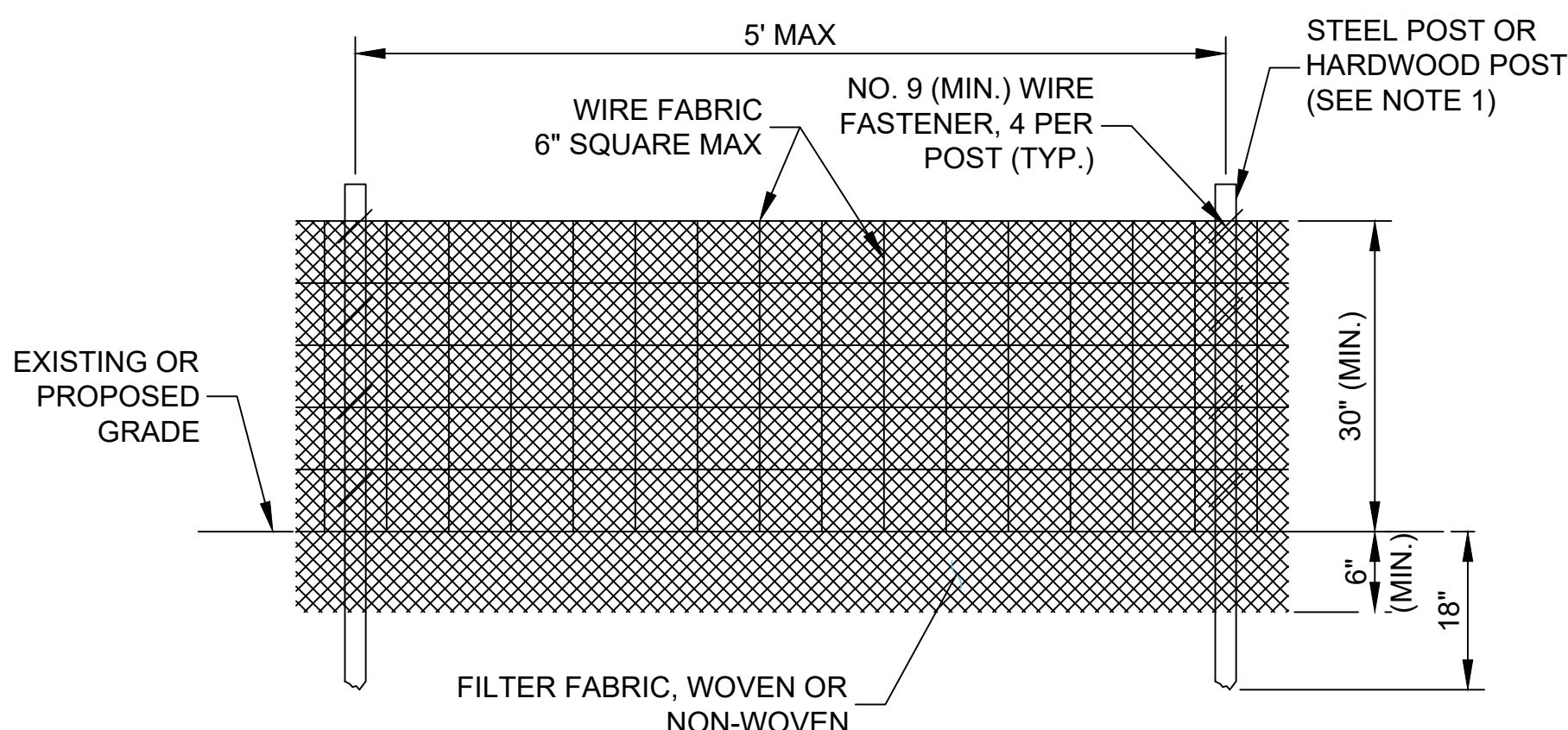
**SILT FENCE AT MANHOLES IN PERVIOUS AREAS**

**NOTES:**

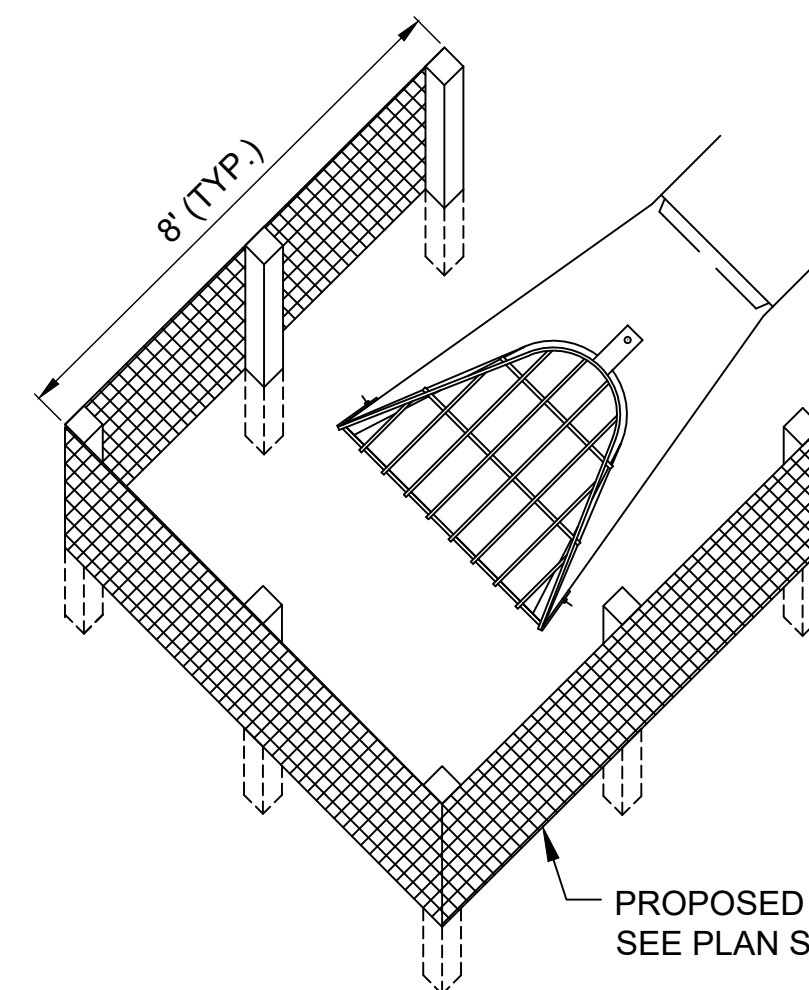
1. FILTER FABRIC SHALL BE EMBEDDED 8" INTO THE SOIL.
2. INSPECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
3. SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS AT THE DIRECTION OF THE AIRPORT REPRESENTATIVE OR OWNER SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. CONTRACTOR SHALL PLACE SEED AND MULCH AROUND STRUCTURES PER LANDSCAPING PLAN. COST OF REMOVAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SILT FENCE.
4. AREAS DISTURBED OUTSIDE OF CONSTRUCTION LIMITS DURING PLACEMENT OF INLET PROTECTION TO BE RE-GRADED, SEEDED AND MULCHED, COST INCIDENTAL TO SILT FENCE.
5. FENCE AND POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
6. PAID UNDER AR156510 SILT FENCE



**FABRIC ANCHOR DETAIL**



**ELEVATION**



**SILT FENCE PLACEMENT AT FLARED END SECTIONS (FES)**

**SEDIMENTATION AND EROSION CONTROL NOTES:**

- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
  - UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
  - AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- D. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- E. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
- I. APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- J. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- O. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

**NOTES:**

1. FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
4. FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.j.1.f.i, AS AMENDED), OR EQUIVALENT.
7. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
8. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
9. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
10. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
11. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
12. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
13. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
14. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

**STORM WATER POLLUTION PREVENTION NOTES**

GENERAL  
THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

POLLUTION PREVENTION MEASURES  
THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

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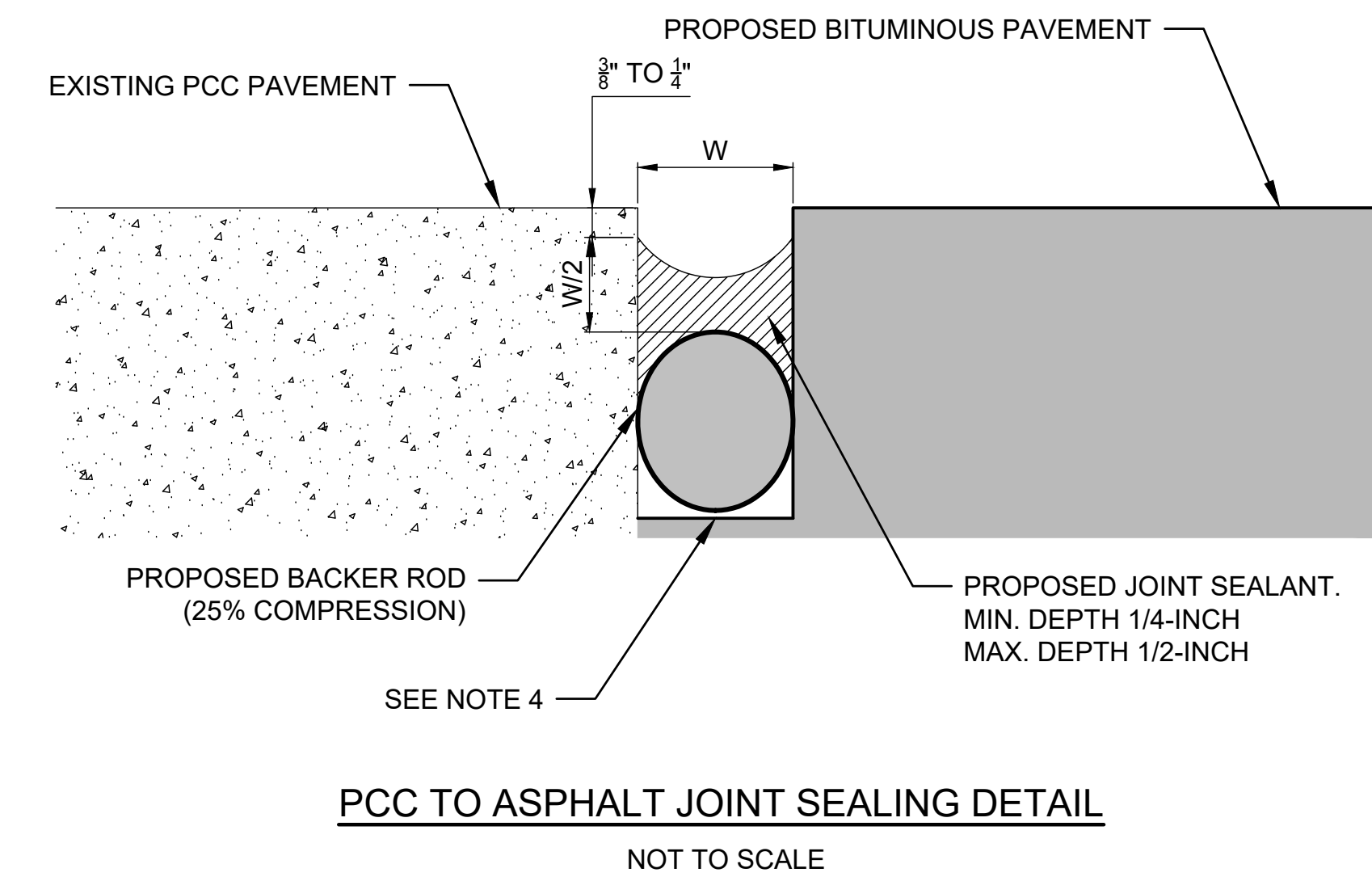
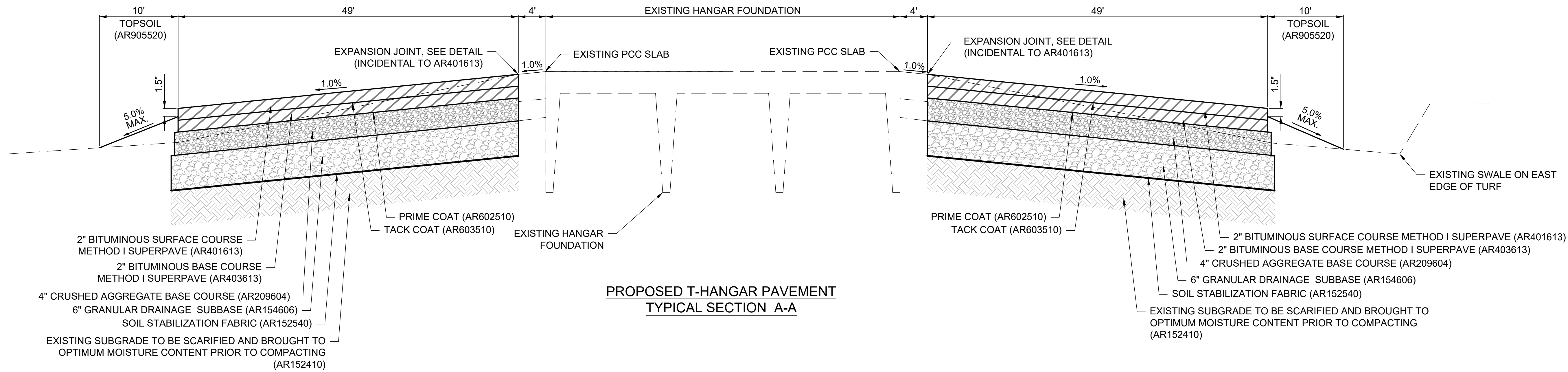
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JANUARY 12, 2024  
PROJECT NO: 19A0112D  
CAD FILE: C-181-SWP.DWG  
DESIGN BY: JP 11/27/23  
DRAWN BY: JP 11/27/23  
REVIEWED BY: LDH 1/5/24

SHEET TITLE

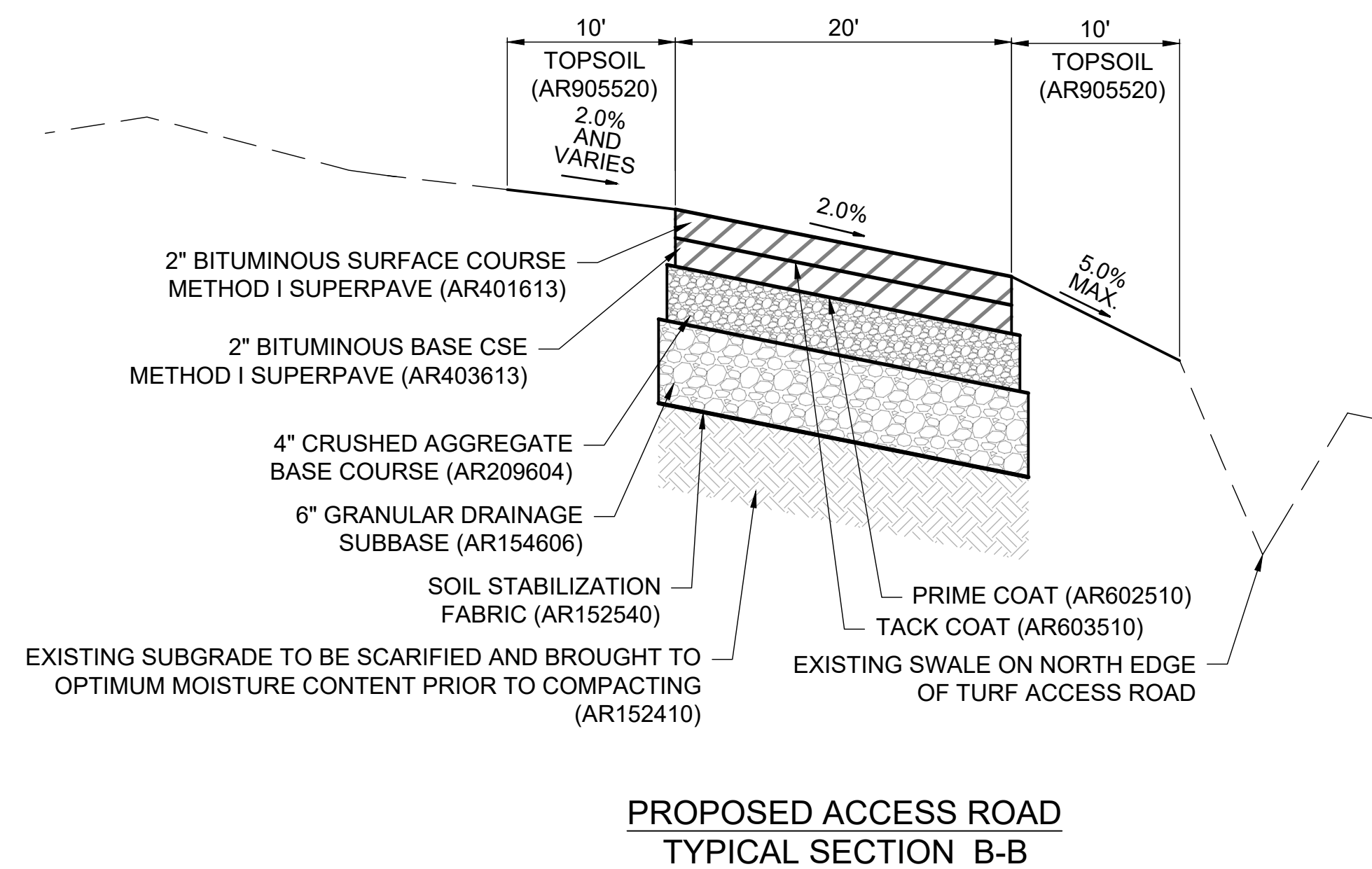
**SWPP DETAILS**





**NOTES:**

- SEALANT SHALL BE DOW 890SL, CRAFCO ROADSaver SILICONE SL, OR EQUIVALENT.
- SEALANT SHALL BE RECESSED A MINIMUM 3/8-INCH TO 1/4-INCH BELOW PAVEMENT (BASED ON DEPTH OF LOWEST SLAB).
- JOINT SHALL HAVE A DEPTH/WIDTH RATIO OF 0.5 OR AS SPECIFIED BY THE MANUFACTURER.
- A SHALLOW CUT MAY BE USED WHERE THE BACKER ROD IS PLACED AT THE BOTTOM OF THE JOINT.
- BACKER ROD SIZING SHALL BE BASED ON THE MANUFACTURER'S RECOMMENDATION.
- ASPHALT MUST BE COMPLETELY REMOVED FROM CONCRETE FACE.
- NEW ASPHALT PAVEMENT MUST BE CLEAN, DRY, AND CURED. CURE TIME WILL BE BASED ON THE RESIDENT ENGINEER'S APPROVAL.



CONSTRUCT AIRCRAFT  
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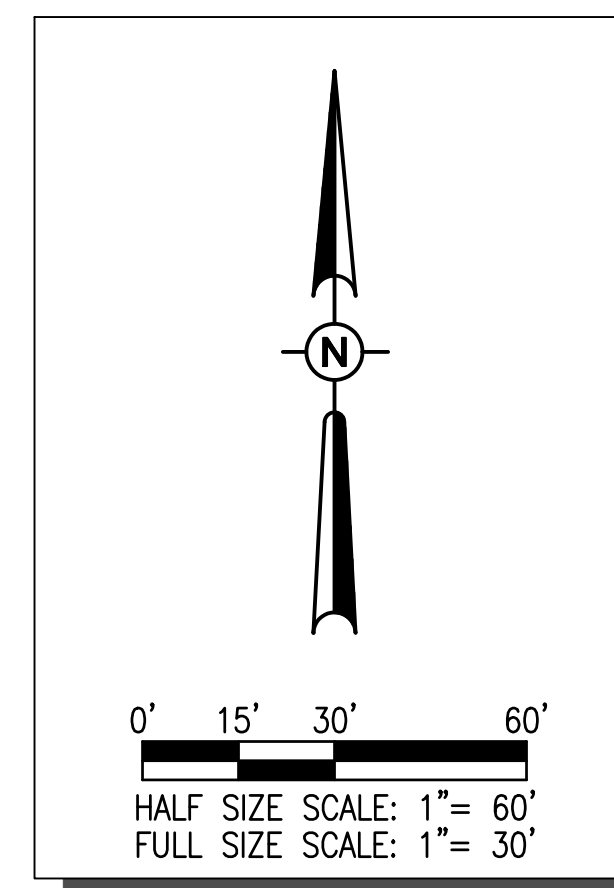

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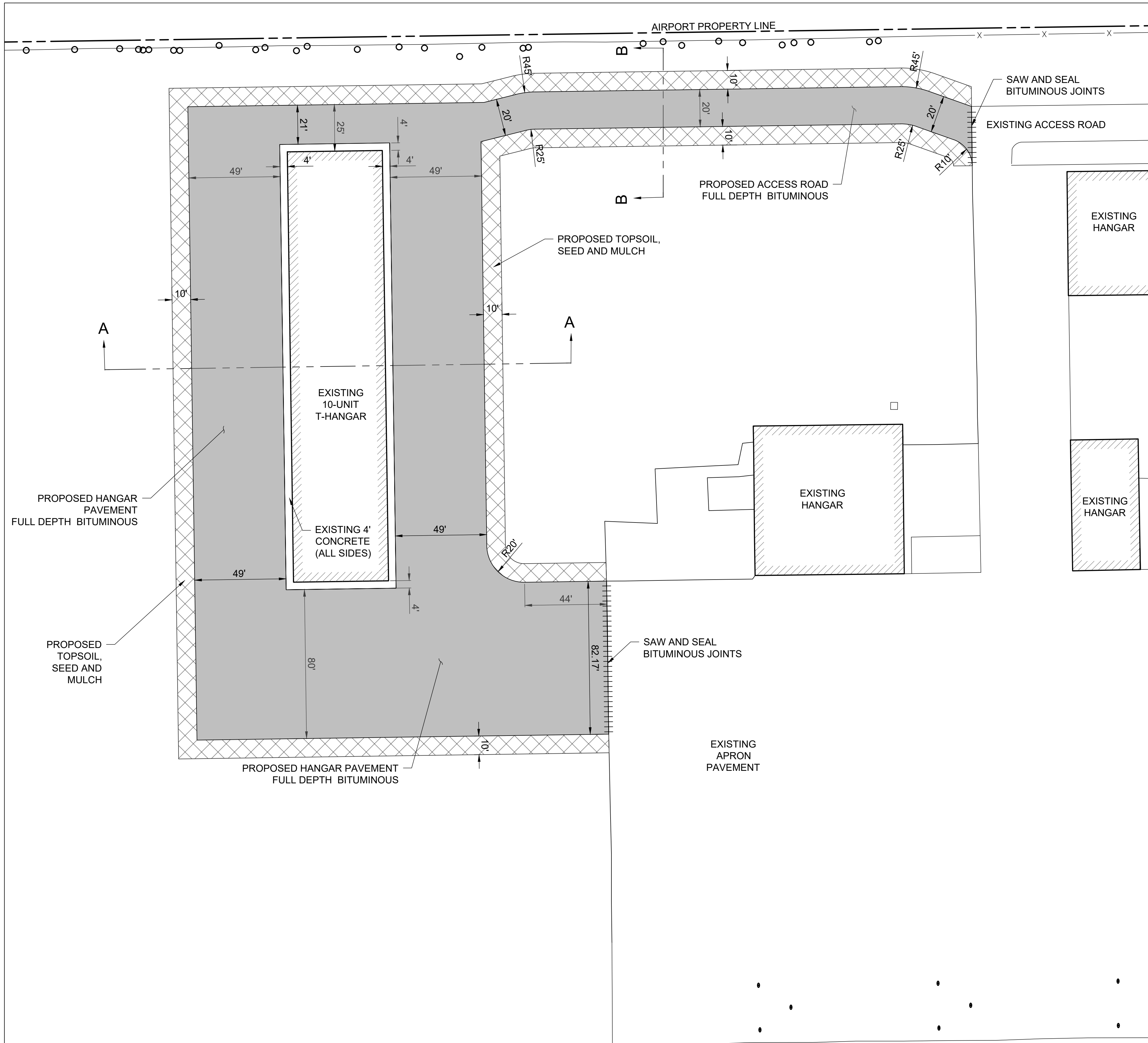
TYPICAL SECTIONS





**NOTES**

1. REFER TO SHEET 9 - GRADING AND SITE LAYOUT PLAN AND SHEET 10 - SITE LAYOUT POINT TABLES FOR COORDINATE DATA.
2. REFER TO SHEET 7 FOR TYPICAL SECTIONS.
3. REFER TO SHEET 11 DRAINAGE PLAN FOR PROPOSED UNDERDRAIN LAYOUT AND INFORMATION.



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**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

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Contract No. PN012

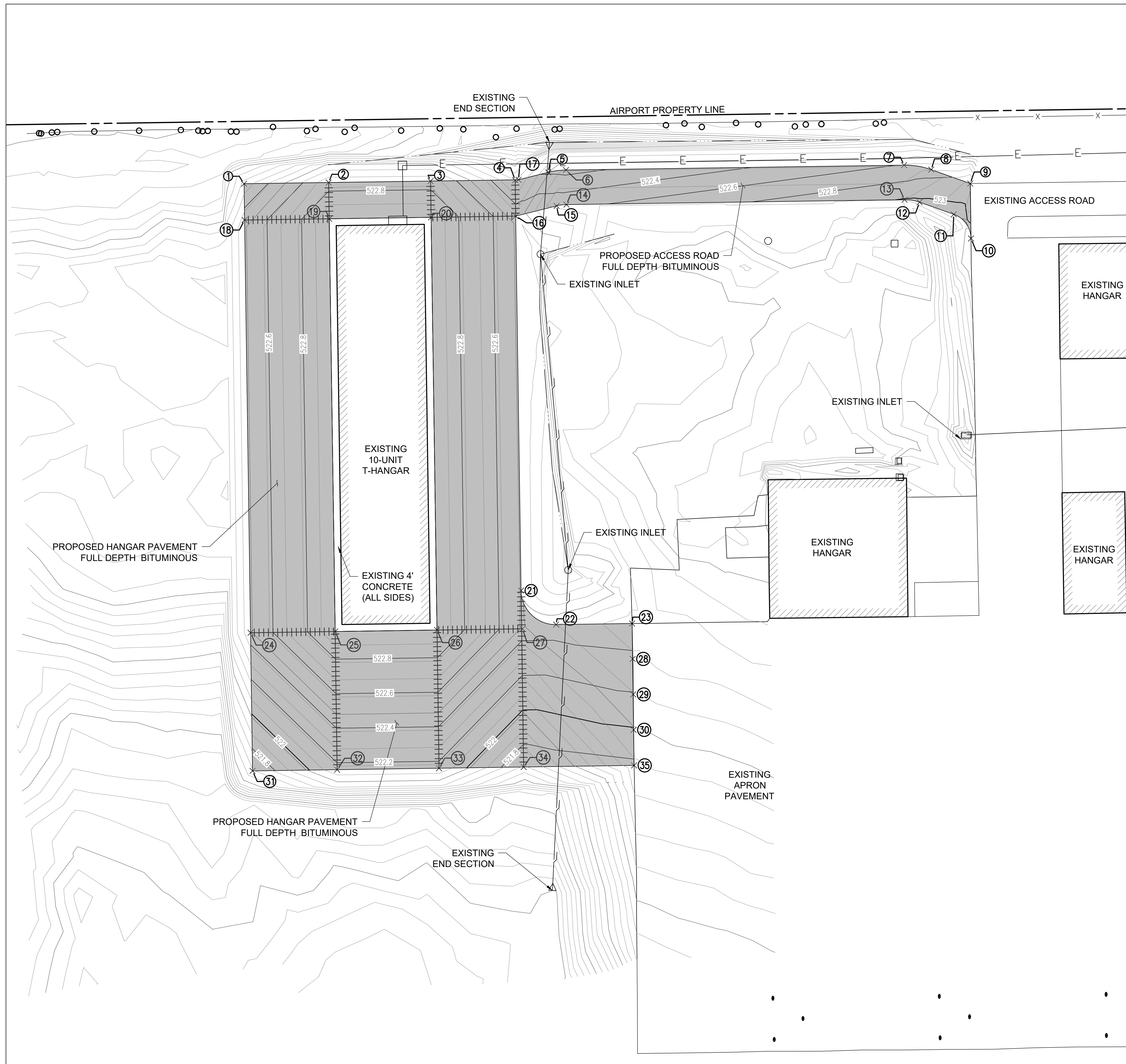
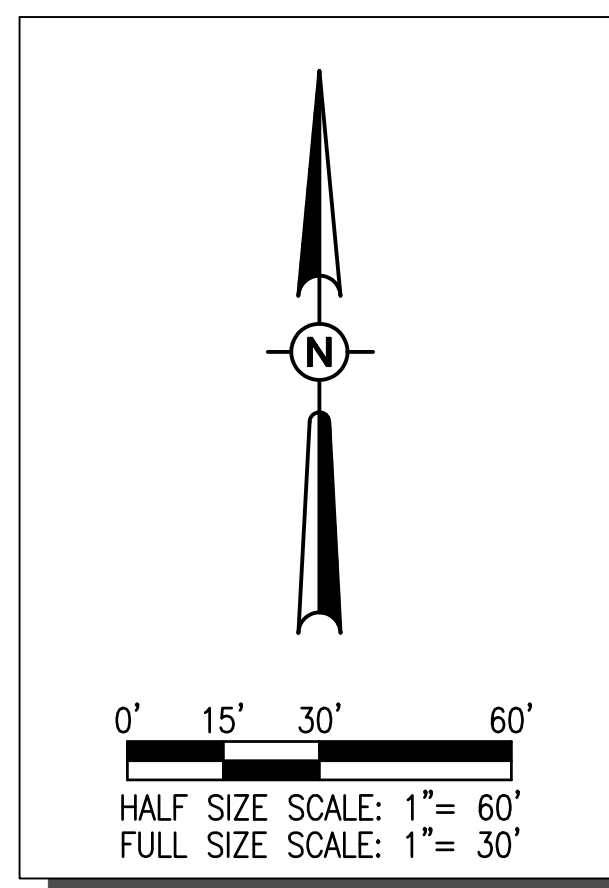
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SHEET TITLE

**PROPOSED SITE PLAN**





**NOTES**

- REFER TO SHEET 10 - SITE LAYOUT POINT TABLES FOR COORDINATE DATA.
- ALL DISTURBED AND GRADED AREAS SHALL BE FINISHED WITH 6" OF TOPSOIL MEETING THE SPECIFICATION REQUIREMENTS AND BE SEEDED AND MULCHED. REFER TO SHEET 5 - STORMWATER POLLUTION PREVENTION PLAN FOR MORE INFORMATION.

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

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		DES	DWN	REV

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REVIEWED BY: LDH 1/5/24

SHEET TITLE

**GRADING AND SITE LAYOUT PLAN**

EXISTING	PROPOSED	LEGEND
— x —		FENCE
— - - -		AIRPORT PROPERTY LINE
...	...	DITCHLINE
□/○/▽		INLET/FES
	█	HMA PAVEMENT
	+++++	GRADE BREAKLINE
	~~~~~	CONTOURS
▨		BUILDING





SITE LAYOUT POINTS				
POINT #	ELEVATION	NORTHING	EASTING	DESCRIPTION
1	522.26	1393037.3625	2434260.7982	PAVEMENT SURFACE
2	522.75	1393038.0692	2434309.7931	PAVEMENT SURFACE
3	522.75	1393038.9201	2434368.7869	PAVEMENT SURFACE
4	522.26	1393039.4282	2434417.7831	PAVEMENT SURFACE
5	522.18	1393043.9606	2434436.9178	PAVEMENT SURFACE
6	522.14	1393045.3030	2434447.1965	PAVEMENT SURFACE
7	522.61	1393048.1266	2434642.9615	PAVEMENT SURFACE
8	522.72	1393045.5320	2434658.6830	PAVEMENT SURFACE
9	522.87	1393037.4919	2434681.3009	PAVEMENT SURFACE
10	523.13	1393005.6703	2434681.7294	PAVEMENT SURFACE
11	523.08	1393019.6594	2434671.7542	PAVEMENT SURFACE
12	523.03	1393026.6872	2434651.9841	PAVEMENT SURFACE
13	523.01	1393028.1287	2434643.2500	PAVEMENT SURFACE
14	522.54	1393025.3050	2434447.4850	PAVEMENT SURFACE
15	522.53	1393024.5593	2434441.7746	PAVEMENT SURFACE
16	522.47	1393018.6290	2434418.0847	PAVEMENT SURFACE
17	522.25	1393039.6547	2434419.7169	PAVEMENT SURFACE
18	522.47	1393016.3647	2434261.1010	PAVEMENT SURFACE
19	522.96	1393017.0714	2434310.0959	PAVEMENT SURFACE
20	522.96	1393017.9223	2434369.0898	PAVEMENT SURFACE
21	522.47	1392801.8199	2434421.2119	PAVEMENT SURFACE
22	522.49	1392782.1105	2434441.4983	PAVEMENT SURFACE
23	522.53	1392782.7456	2434485.4900	PAVEMENT SURFACE
24	522.47	1392777.3896	2434264.5480	PAVEMENT SURFACE
25	522.96	1392778.0965	2434313.5582	PAVEMENT SURFACE
26	522.96	1392778.9472	2434372.5367	PAVEMENT SURFACE
27	522.47	1392779.6302	2434422.0350	PAVEMENT SURFACE
28	522.36	1392762.2417	2434485.8126	PAVEMENT SURFACE
29	522.19	1392741.6943	2434486.1359	PAVEMENT SURFACE
30	521.98	1392721.1554	2434486.3653	PAVEMENT SURFACE
31	521.67	1392697.3979	2434265.7017	PAVEMENT SURFACE
32	522.16	1392698.1046	2434314.6966	PAVEMENT SURFACE
33	522.16	1392698.9555	2434373.6905	PAVEMENT SURFACE
34	521.67	1392699.6622	2434422.6854	PAVEMENT SURFACE
35	521.76	1392700.5887	2434486.6451	PAVEMENT SURFACE

CONSTRUCT AIRCRAFT  
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ISSUE: JANUARY 12, 2024

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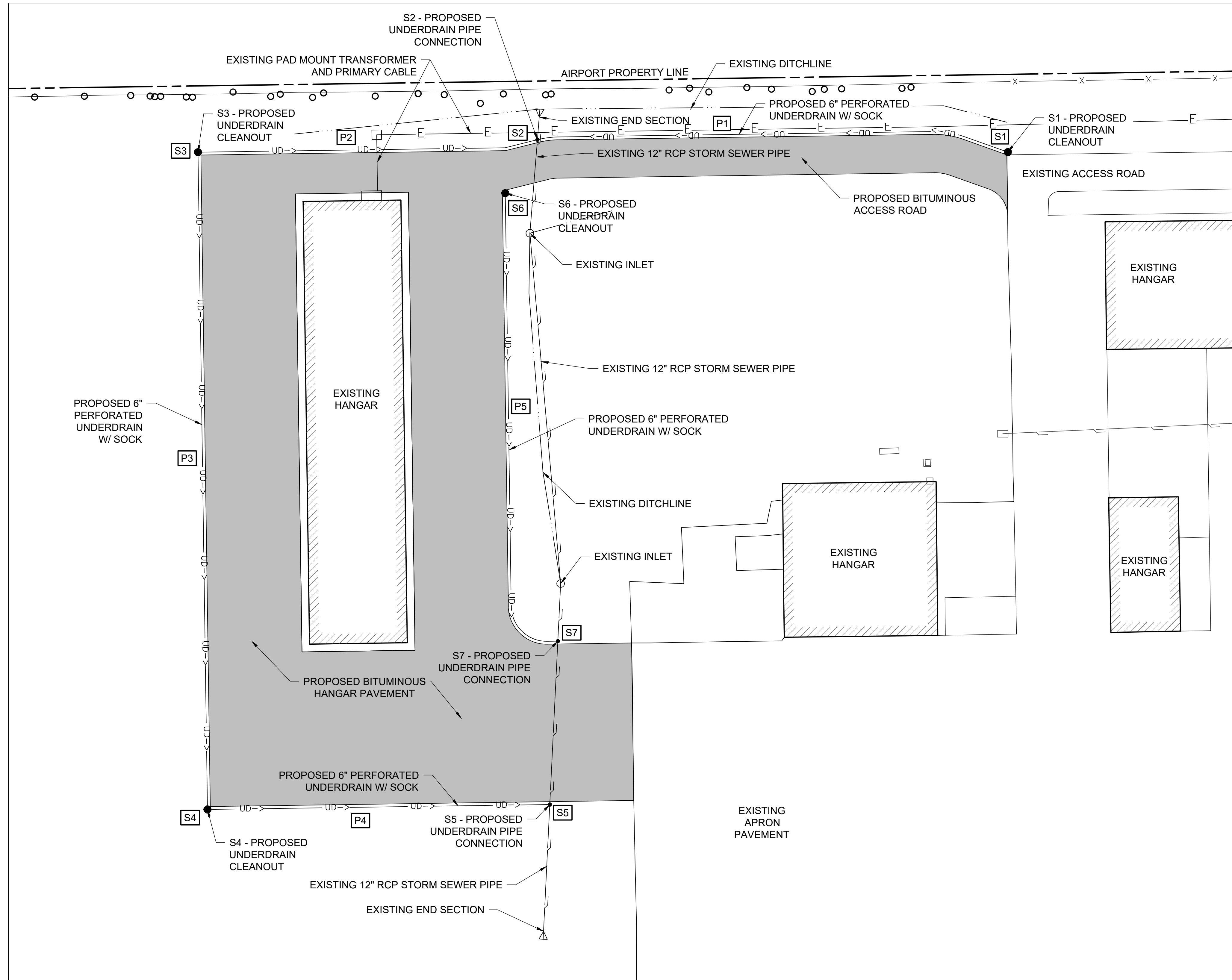
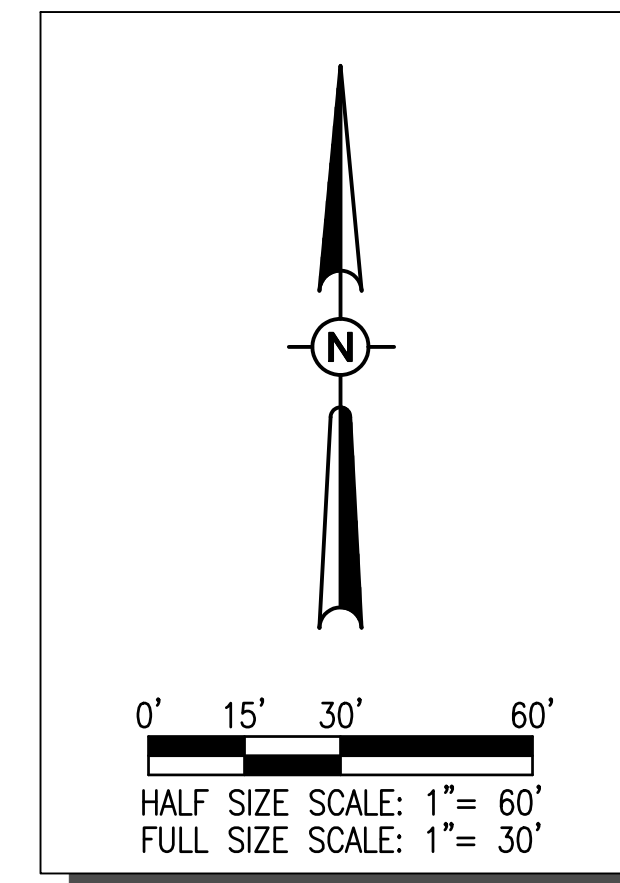
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SHEET TITLE

SITE LAYOUT POINT  
TABLES





**LEGEND**

- UD— PROPOSED UNDERDRAIN PIPE
- — — EXISTING STORM SEWER PIPE
- PROPOSED CLEANOUT STRUCTURE
- PROPOSED DRAINAGE CONNECTION
- [P2] PROPOSED PIPE CALLOUT (SEE DRAINAGE SCHEDULE)
- [TD1] [S2] [UD2] PROPOSED DRAINAGE STRUCTURE CALLOUT (SEE DRAINAGE SCHEDULE)

DRAINAGE STRUCTURE SCHEDULE					
STRUCT#	NORTHING	EASTING	RIM	INVERT	TYPE
S1	1393038.91	2434681.80	522.79	520.44	UNDERDRAIN CLEANOUT STRUCTURE
S2	1393045.37	2434436.37	-	519.70	UNDERDRAIN TO PIPE CONNECTION
S3	1393038.84	2434259.28	522.14	520.23	UNDERDRAIN DUAL CLEANOUT STRUCTURE
S4	1392695.88	2434264.22	521.59	519.10	UNDERDRAIN DUAL CLEANOUT STRUCTURE
S5	1392698.45	2434442.83	-	518.50	UNDERDRAIN TO PIPE CONNECTION
S6	1393017.463	2434419.60	522.39	519.89	UNDERDRAIN CLEANOUT STRUCTURE
S7	1392783.69	2434447.03	-	519.14	UNDERDRAIN TO PIPE CONNECTION

DRAINAGE PIPE SCHEDULE					
PIPE NO.	FROM	TO	L.F.	SLOPE	TYPE
P1	S1	S2	247.3	0.30%	6" PERFORATED UNDERDRAIN W/SOCK
P2	S3	S2	177.6	0.30%	6" PERFORATED UNDERDRAIN W/SOCK
P3	S3	S4	343.0	0.33%	6" PERFORATED UNDERDRAIN W/SOCK
P4	S4	S5	178.6	0.33%	6" PERFORATED UNDERDRAIN W/SOCK
P5	S6	S7	250.3	0.30%	6" PERFORATED UNDERDRAIN W/SOCK

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

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NO.	DATE	DESCRIPTION
1	9/9/22	ADDENDUM 1 LDH   LDH   LDH
		DES   DWN   REV

ISSUE: JANUARY 12, 2024

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CAD FILE: C-131-UTL.DWG

DESIGN BY: JP 11/27/23

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SHEET TITLE

**DRAINAGE PLAN**





CONSTRUCT AIRCRAFT  
T-HANGAR PAVEMENT  
AND AIRPORT ACCESS  
ROAD

IDA No: C15-5019

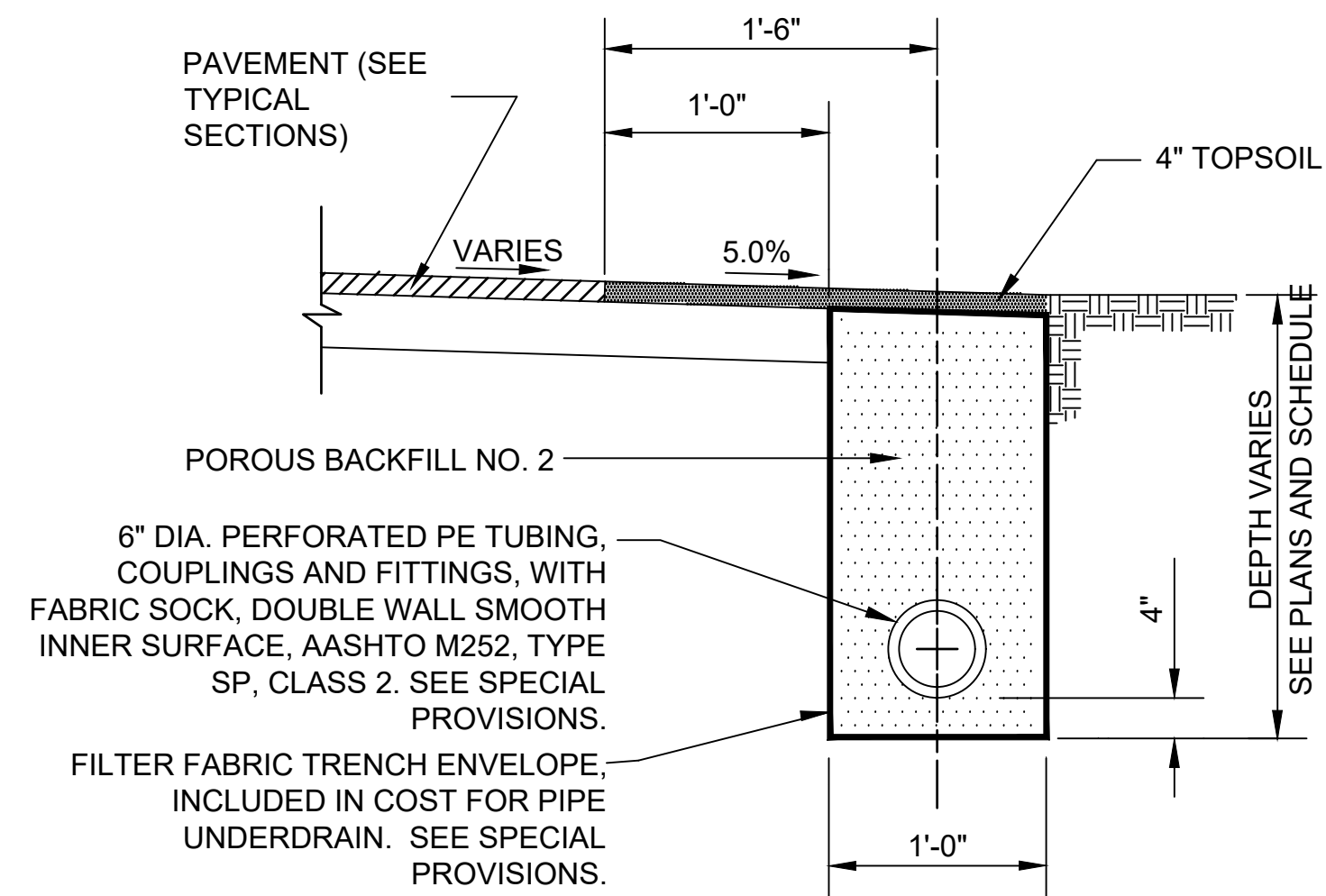
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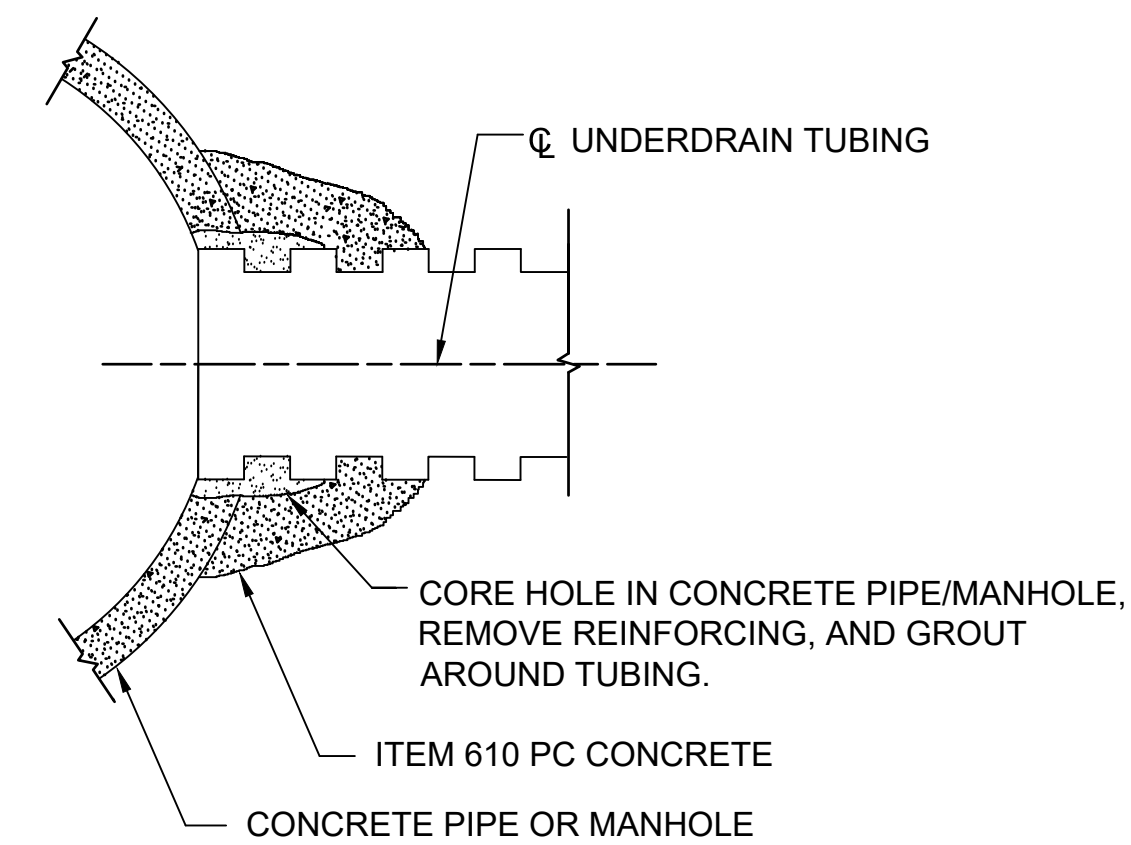
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SHEET TITLE

DRAINAGE DETAILS



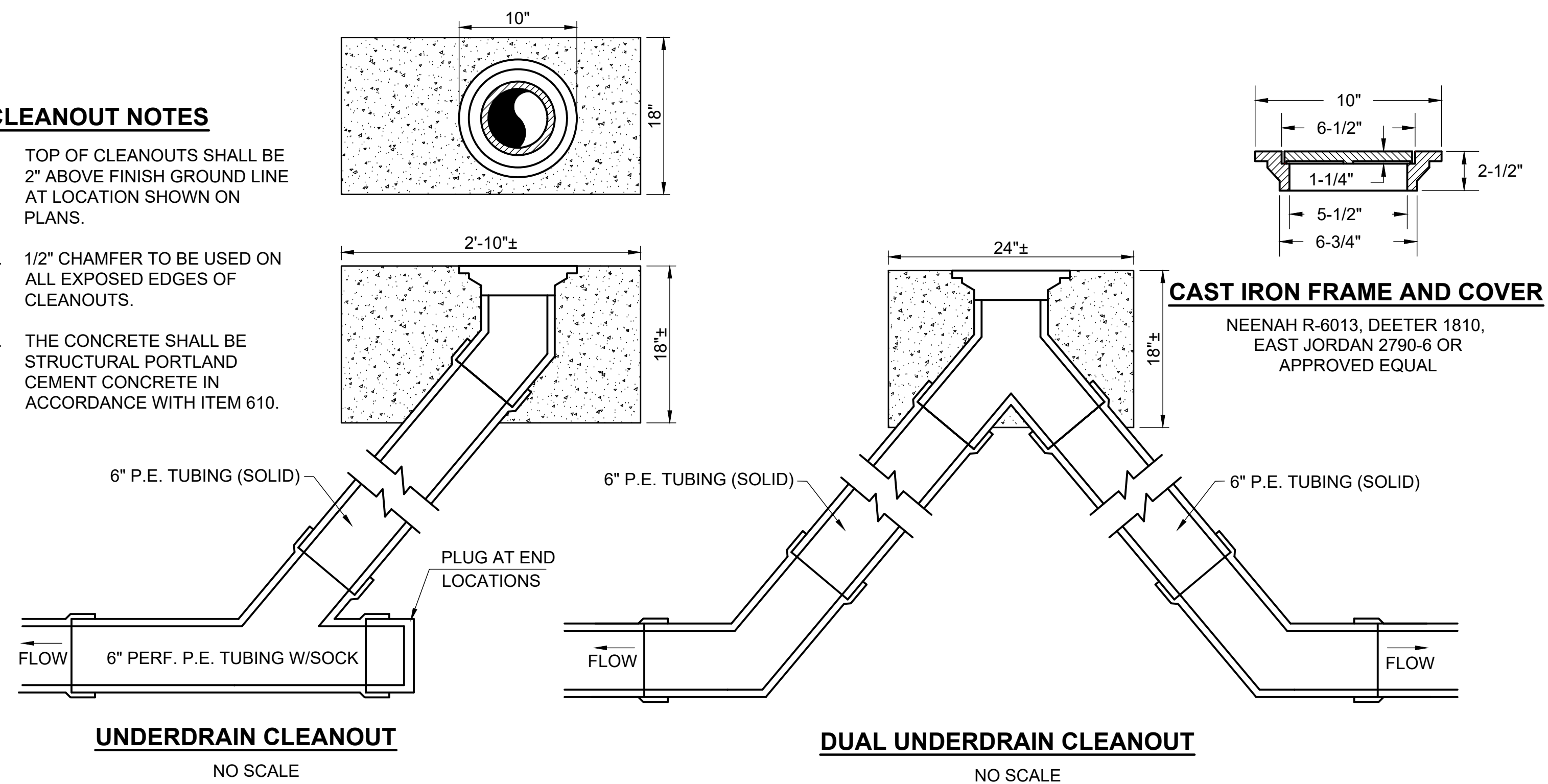
**UNDERDRAIN ALONG PAVEMENT EDGE**



**STORM SEWER CONCRETE COLLAR AND GROUT CONNECTION**

**CLEANOUT NOTES**

1. TOP OF CLEANOUTS SHALL BE 2" ABOVE FINISH GROUND LINE AT LOCATION SHOWN ON PLANS.
2. 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF CLEANOUTS.
3. THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE IN ACCORDANCE WITH ITEM 610.



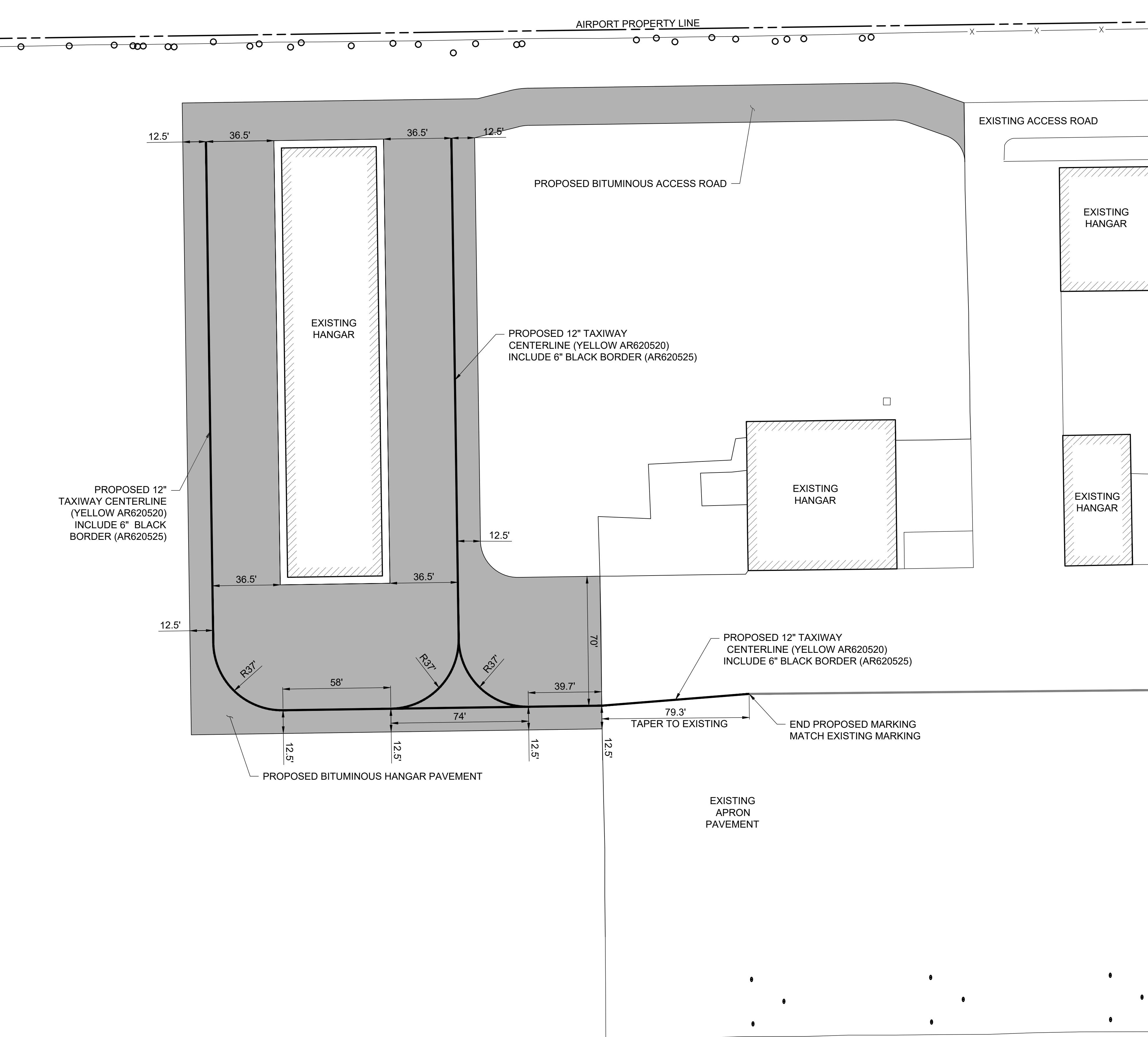
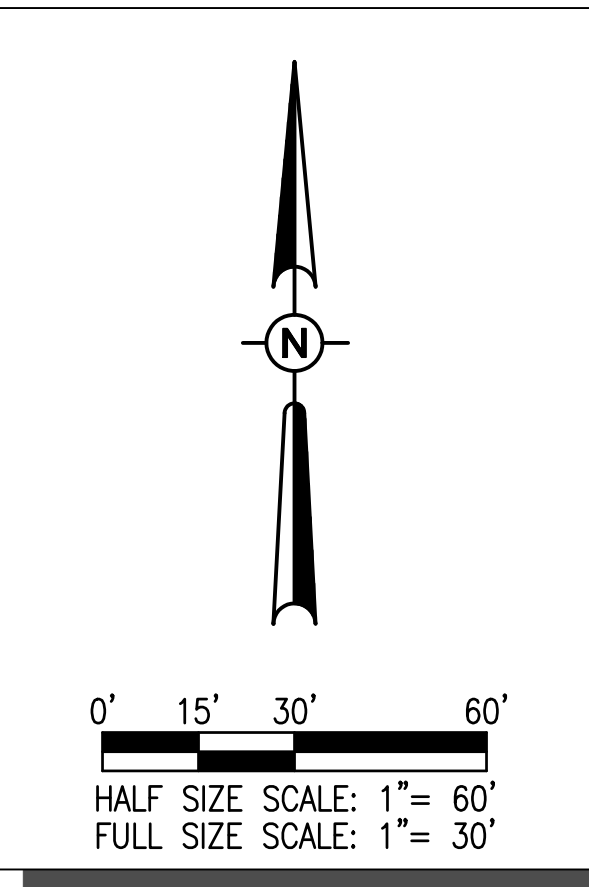
**UNDERDRAIN CLEANOUT**

NO SCALE

**DUAL UNDERDRAIN CLEANOUT**

NO SCALE





**WATERBORNE PAVEMENT MARKING NOTES**

1. A 30-DAY WAITING PERIOD IS REQUIRED BETWEEN THE PLACEMENT OF BITUMINOUS SURFACE COURSE AND THE FINAL APPLICATION OF WATERBORNE PAINT. AFTER THE PLACEMENT OF SURFACE COURSE AND PRIOR TO THE 30-DAY CURE PERIOD AN INITIAL COAT OF MARKINGS SHALL BE PLACED AT 50% OF THE NORMAL APPLICATION RATE. BLACK MARKINGS SHALL NOT BE PLACED DURING THE INITIAL COAT. THE REMAINDER OF THE MARKINGS, INCLUDING THE BLACK BORDERS, SHALL BE PLACED AFTER THE 30-DAY CURE PERIOD HAS ELAPSED. (IF ONLY MARKING REMAINS, THE CALENDAR DAYS MAY BE SUSPENDED DURING THE 30 DAY CURE PERIOD.)
2. WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS AND WILL BE APPLIED IN ONE APPLICATION.
3. GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
4. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
5. THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES THAT ARE OUTSIDE OF THE PROPOSED WORK LIMITS.

**CONSTRUCT AIRCRAFT T-HANGAR PAVEMENT AND AIRPORT ACCESS ROAD**

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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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SHEET TITLE

**PROPOSED MARKING PLAN**

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