

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)I	WOODFORD	38	1
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. 4		ILLINOIS FED. AID PROJECT		

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

FAP 702 (IL 117)
 SECTION (113BR-1) I
 PROJECT ACBHF-0702 (230)
WOODFORD COUNTY
C-94-169-06

LIST OF ILLINOIS DOT HIGHWAY STANDARDS

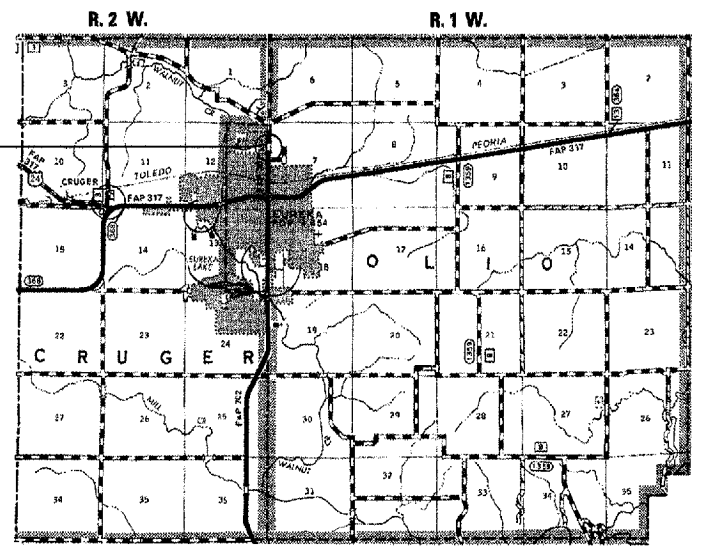
00001-04	701201-02
353001-03	701301-02
630001-07	701311-02
630301-04	701321-08
631032-03	702001-06
635006-02	704001-03
635011-01	780001-01
701006-02	781001-02
701011-01	

PROPOSED PROJECT ENDS
 -STA. 17+00

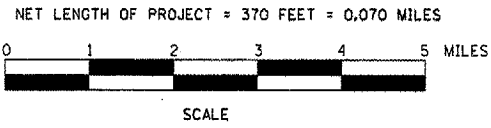
PROPOSED PRECAST PRESTRESSED CONCRETE DECK BEAM SUPERSTRUCTURE ON EXISTING SINGLE SPAN SUBSTRUCTURE 44'-3 1/2" BK.-BK. ABUTMENTS, 39'-0" CLEAR DECK WIDTH WITH STEEL BRIDGE RAIL, TYPE SM., 0° SKEW S.N. 102-0018
 E - STA. 15+45

PROPOSED PROJECT BEGINS
 -STA. 13+30

SUPERSTRUCTURE REPLACEMENT OVER WALNUT CREEK



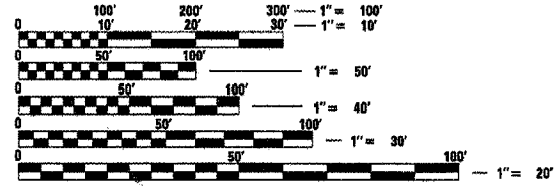
LOCATION MAP



D-94-096-02



ADT = 2950 (2005)
% SU = 4.0 (2005)
% MU = 4.0 (2005)
TOWNSHIP: CRUGER /OLIO
FUNCTIONAL CLASSIFICATION: MINOR ARTERIAL (NON-URBAN)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123

PROJECT ENGINEER: RICH DOTSON (309)-671-3455



Christopher P. Kohler 12/8/06
 EXPIRATION DATE: 11/30/07

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED *Dreys* 20 06

Eric E. Harman
 DEPUTY DIRECTOR OF HIGHWAYS REGION THREE
 February 2, 2007
 ENGINEER OF DESIGN AND ENVIRONMENT

Milton R. Sess P.E.
 DIRECTOR, DIVISION OF HIGHWAYS
 February 2, 2007

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CONTRACT NO. 68635

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	2
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FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				

ENVIRONMENTAL REVIEWS

PRIOR TO THE USE OF ANY PROPOSED BORROW AREAS, USE AREAS, (TEMPORARY ACCESS ROADS, DETOURS, RUN-AROUNDS, ETC.) AND/OR WASTE AREAS, THE CONTRACTOR SHALL FILE THE REQUIRED ENVIRONMENTAL RESOURCE REQUEST SURVEYS ACCORDING TO SECTION 107.22 OF THE STANDARD SPECIFICATIONS. THESE SURVEYS ARE REQUIRED IN ORDER FOR THE DEPARTMENT TO CONDUCT CULTURAL AND BIOLOGICAL RESOURCE SURVEYS FOR THE PROPOSED SITE.

PRIOR TO ANY WASTE MATERIALS BEING REMOVED FROM THE CONSTRUCTION SITE THE REQUIRED ENVIRONMENTAL RESOURCE SURVEYS WILL NEED TO BE OBTAINED AND FILED BY THE CONTRACTOR. EXCESS WASTE PRODUCTS REMOVED FROM THE CONSTRUCTION SITE SHALL BE DISPOSED OF AS REQUIRED IN SECTION 202.03 OF THE STANDARD SPECIFICATIONS.

ANY PROTRUDING METAL BARS SHALL BE REMOVED PRIOR TO THE DISPOSAL OF BROKEN CONCRETE AT APPROVED DISPOSAL SITES.

THE REQUIRED ENVIRONMENTAL RESOURCE DOCUMENTATION SHALL INCLUDE THE FOLLOWING:

- BDE FORM 2289 (ENVIRONMENTAL SURVEY REQUEST)
- A LOCATION MAP SHOWING THE SIZE LIMITS AND LOCATION OF THE USE AREA
- SIGNED PROPERTY OWNER AGREEMENT FORM
- COLOR PHOTOGRAPHS DEPICTING THE USE AREA

PLEASE NOTE THAT A MINIMUM OF TWO WEEKS SHALL BE ALLOWED FOR THE DISTRICT TO OBTAIN THE REQUIRED ENVIRONMENTAL CLEARANCES.

PROPERTY OWNER ACCESS REQUIREMENT

ACCESS MUST BE MAINTAINED TO ALL EXISTING PROPERTIES DURING CONSTRUCTION PER ARTICLE 107.09 UNLESS ARRANGEMENTS ARE MADE IN WRITING BY THE CONTRACTOR WITH THE PROPERTY OWNERS WITH A COPY TO THE ENGINEER FOR SHORT-TERM CLOSURES.

GENERAL NOTES

- 1.) THE THICKNESS OF BITUMINOUS MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- 2.) EXCEPT AS NOTED IN THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.
- 3.) WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER OR AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
- 4.) SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION. AREAS TO BE SEEDED SHALL BE DETERMINED BY THE ENGINEER AND SEEDED AS SOON AS POSSIBLE.
- 5.) ALL SAW CUTS, NECESSARY TO COMPLETE THE WORK DETAILED IN THESE PLANS, SHALL BE INCLUDED IN THE COST FOR THE VARIOUS PAY ITEMS INVOLVED, THE MINIMUM SAW CUT DEPTH IN THE PAVEMENT SHALL BE 1/2" UNLESS UNLESS OTHERWISE SPECIFIED IN A DETAIL SHOWN IN THE PLANS.
- 6.) UNLESS DIRECTED BY THE ENGINEER, PAVEMENT MARKING LINES SHALL NOT BE LAID DIRECTLY OVER A LONGITUDINAL CRACK OR JOINT NOR OVER A TAR OR ASPHALT PAINTED LINE. THE EDGE OF A CENTERLINE OR LANE LINE SHALL BE OFFSET A MINIMUM DISTANCE OF 2" FROM A LONGITUDINAL CRACK OR JOINT. EDGE LINES SHALL BE APPROXIMATELY 2" FROM THE EDGE LINE OF PAVEMENT. SEE SECTION 780 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
- 7.) ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OUTSIDE THE LIMITS OF RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPERATELY BUT SHALL BE INCLUDED IN THE COST PER CUBIC YARD FOR EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 8.) ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED IN THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- 9.) IN ADDITION TO THE FIELD SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- 10.) THE EXISTING ROAD SIGNS THAT INTERFERE WITH CONSTRUCTION WILL BE REMOVED OR RELOCATED AS DIRECTED BY THE ENGINEER. AFTER THE CONSTRUCTION IS COMPLETED, THE CONTRACTOR WILL REPLACE THE SIGNS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT, AND NO COMPENSATION WILL BE ALLOWED.
- 11.) THE LOCATION OF ALL UTILITIES ARE BASED ON INFORMATION PROVIDED BY OTHERS AND IS INTENDED TO BE APPROXIMATE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE HIS CONSTRUCTION ACTIVITIES WITH THE VARIOUS UTILITY OWNERS. ALL POTENTIAL CONFLICTS SHALL BE INVESTIGATED AND REMEDIAL ACTION TAKEN PRIOR TO INTERRUPTION OF THE CONTRACTOR'S PROGRESS.
- 12.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE J.L.L.I.E. NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED.
- 13.) ALL ELEVATIONS SHOWN ON THE PLANS ARE BASED ON U.S.G.S. MEAN SEA LEVEL DATUM. ALL STATION AND OFFSET REFERENCES ARE TO THE ROADWAY CENTERLINE UNLESS OTHERWISE NOTED. THE STATE PLANE COORDINATE SYSTEM HAS BEEN USED FOR THE HORIZONTAL CONTROL.
- 14.) THE DISTRICT BUREAU OF OPERATIONS SHALL BE NOTIFIED AT LEAST 14 DAYS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS (PH: 309-671-4460)

COMMITMENTS: NONE

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED TO CALCULATE THE PLAN QUANTITIES:

HOT MIX ASPHALT MATERIALS (PRIME COAT)	0.00038 TON/SQ. YD. (ON PAVEMENT)
HOT MIX ASPHALT MATERIALS (PRIME COAT)	0.001425 TON/SQ.YD. (ON AGG)
HOT MIX ASPHALT SURFACE / BINDER	0.056 TON/SQ. YD. PER 1"
AGGREGATE MATERIAL	2.05 TON/CU. YD.
RIPRAP	1.35 TON/CU. YD.
NITROGEN FERTILIZER NUTRIENT	90 LBS./ACRE
PHOSPHOROUS FERTILIZER NUTRIENT	90 LBS./ACRE
POTASSIUM FERTILIZER NUTRIENT	90 LBS./ACRE
AGGREGATE PRIME COAT	0.002 TON/SQ. YD.

MIXTURE REQUIREMENTS

MIXTURE USE(S)	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50	LEVELING BINDER (MACHINE METHOD), N50 & TEMPORARY RAMP	PCC 5" BRIDGE DECK OVERLAY
AC/PG	PG 64-22	PG 64-22	CALL I.D.O.T. D-4 MATERIALS FOR EXACT AGGREGATE OPTIMIZED CLASS BD MIX DESIGN. OVERLAY MIX WILL BE CONVENTIONAL BD MIX WITH ADDITIONAL 10% CEMENT MATERIAL.
MAX. RAP %	15	15	
DESIGN AIR VOIDS	4.2% @ N DESIGN = 50	4.2% @ N DESIGN = 50	
MIXTURE COMPOSITION	1L 9.5 OR 12.5	1L 9.5 ONLY	
FRICITION AGGREGATE	MIX D	N/A	

* IF THE RAP OPTIONS SELECTED THE ASPHALT CEMENT GRADE MAY BE ADJUSTED. THIS WILL BE DETERMINED BY THE ENGINEER.

NOTES, DETAILS AND TYPICAL ROADWAY SECTIONS
 IL ROUTE 117 OVER WALNUT CREEK
 F.A.P. RTE. 702 - SECTION (113B)
 WOODFORD COUNTY
 STA.15+45.00
 S.N. 102-0018

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	3
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. 4		ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES

CODE NO.	SUMMARY OF QUANTITIES PAY ITEM	UNIT	TOTAL QUANTITY	ROADWAY	STRUCTURE
				FAP 714 80% FEDERAL 20% STATE	S.N. 102-0018 80% FEDERAL 20% STATE
			CONSTRUCTION TYPE CODE		
			1000-2A	X080-2A	
20400800	FURNISHED EXCAVATION	CU YD.	154	154	
25000200	SEEDING, CLASS 2	ACRE	0.2	0.2	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	18	18	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	18	18	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	18	18	
25100630	EROSION CONTROL BLANKET	SQ YD.	968	968	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	20	20	
28000400	PERIMETER EROSION BARRIER	FOOT	600	600	
31100300	SUB-BASE GRANULAR MATERIAL TYPE A 4"	SQ. YD.	140	140	
35300400	PORTLAND CEMENT CONCRETE BASE COURSE 9"	SQ. YD.	140	140	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.15	0.15	
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	9	9	
40600982	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ. YD.	194	194	
40600990	TEMPORARY RAMP	SQ. YD.	44	44	
40603335	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	34	34	
42001300	PROTECTIVE COAT	SQ. YD.	181		181
44000100	PAVEMENT REMOVAL	SQ. YD.	140	140	
50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1		1
50102400	CONCRETE REMOVAL	CU YD.	2.0		2.0
50300225	CONCRETE STRUCTURES	CU. YD.	2.6		2.6
50300260	BRIDGE DECK GROOVING	SQ. YD.	172		172
50400105	PRECAST CONCRETE BRIDGE SLAB	SQ. FT.	299		299
50400305	PRECAST PRESTRESSED CONCRETE DECK BEAMS (17" DEPTH)	SQ. FT.	1,627		1,627
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2,640		2,640
50800515	BAR SPLICERS	EACH	48		48
50901050	STEEL RAILING, TYPE SM	FOOT	165		165
51500100	NAME PLATES	EACH	1		1
59000200	EPOXY CRACK INJECTION	FOOT	30		30
*63000000	STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	75		75
*63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	4		4
*63100167	TRAFFIC BARRIER TERMINAL TYPE 1. SPECIAL (TANGENT)	EACH	4		4
67100100	MOBILIZATION	L. SUM	1		1
70100450	TRAFFIC CONTROL AND PROTECTION STANDARD 701201	L. SUM	1		1
70101205	TRAFFIC CONTROL AND PROTECTION STANDARD 701321, SPECIAL	EACH	1		1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1		1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	125		125
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	2245		2245

SUMMARY OF QUANTITIES

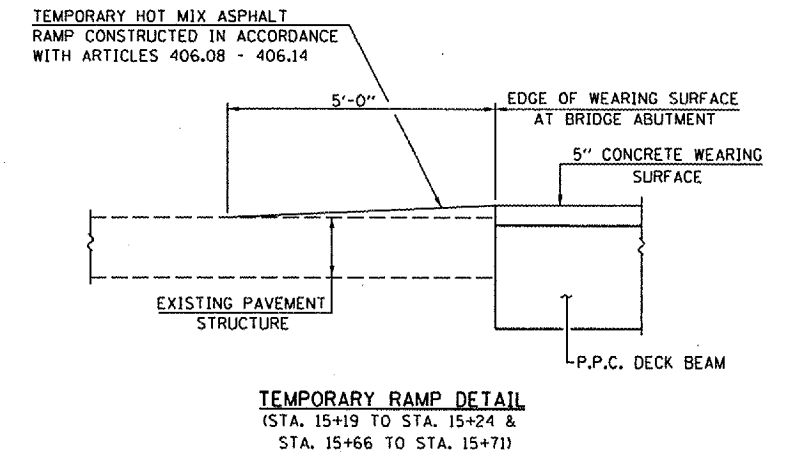
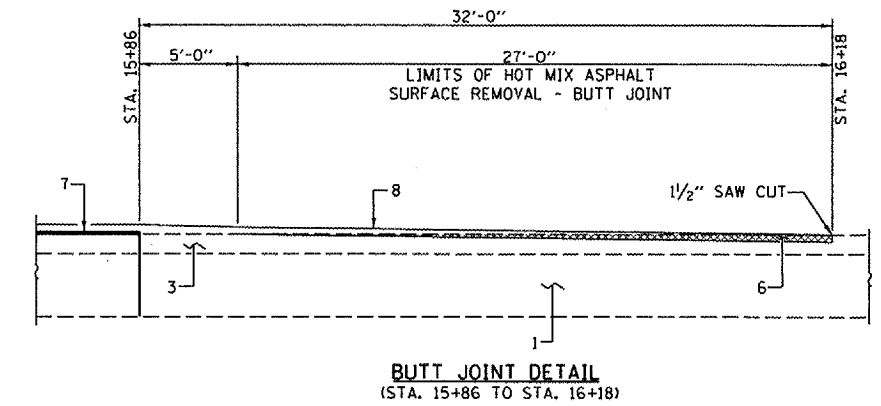
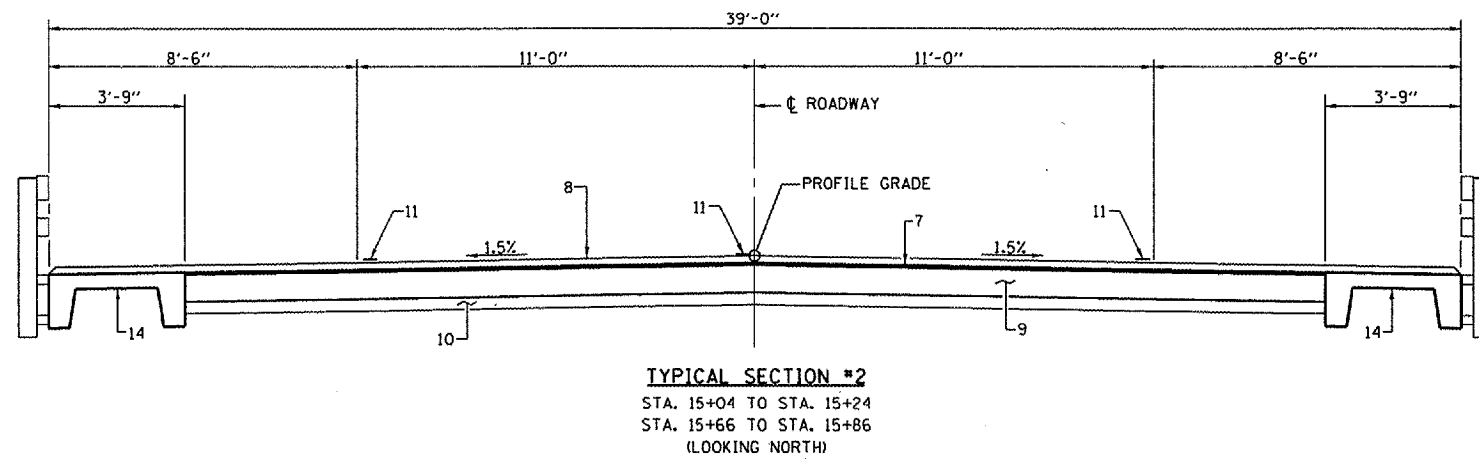
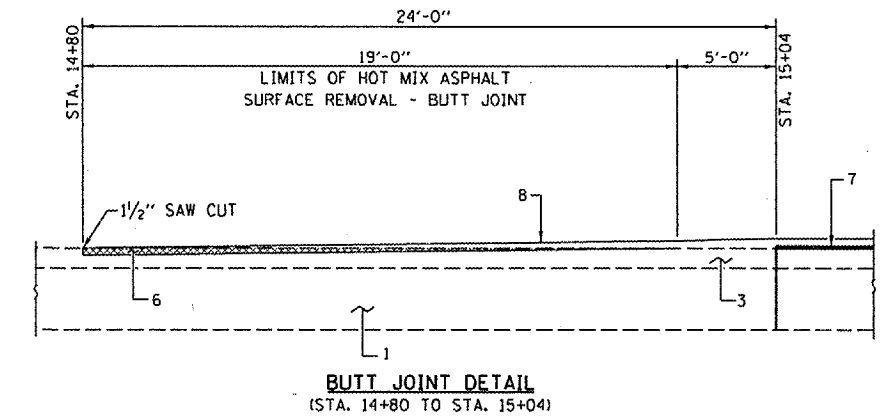
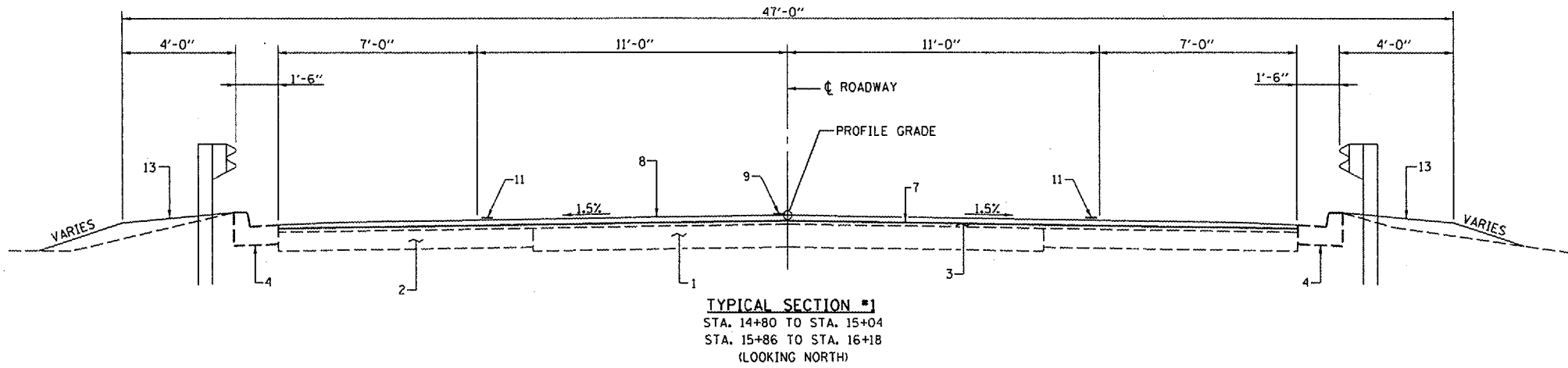
CODE NO.	SUMMARY OF QUANTITIES PAY ITEM	UNIT	TOTAL QUANTITY	ROADWAY	STRUCTURE
				FAP 702 80% FEDERAL 20% STATE	S.N. 102-0018 80% FEDERAL 20% STATE
			CONSTRUCTION TYPE CODE		
			1000-2A	X080-2A	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	24	24	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ. FT.	798	798	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	275	275	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	275	275	
*78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1776	1776	
78200410	GUARDRAIL MARKERS, TYPE A	EACH	8	8	
78201000	TERMINAL MARKER DIRECT APPLIED	EACH	4	4	
78300100	PAVEMENT MARKING REMOVAL	SQ. FT.	513	513	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2	2	
X0301512	GUARDRAIL AGGREGATE EROSION CONTROL	TON	115	115	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5")	SQ. FT.	13		13
X5030305	CONCRETE WEARING SURFACE, 5"	SQ. YD.	181		181
52000110	PREFORMED JOINT STRIP SEAL	FOOT	41		41
Z0013798	CONSTRUCTION LAYOUT	L. SUM	1		1
Z0030250	IMPACT ATTENUATORS TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2		2
Z0030350	IMPACT ATTENUATORS RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2		2
Z0001900	ASBESTOS BEARING PAD REMOVAL	EACH	26		26

* SPECIALTY ITEM
● SFTY-3N

SUMMARY OF QUANTITIES
IL ROUTE 117 OVER WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

FILE NAME: 50 (REV. 12/28/03)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	4
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PAVEMENT LEGEND

1. EXISTING P.C.C. PAVEMENT
2. EXISTING P.C.C. BASE COURSE WIDENING 8"
3. EXISTING HOT MIX ASPHALT OVERLAY
4. EXISTING TYPE B-6.12 CONCRETE CURB & GUTTER
5. EXISTING PRECAST CONCRETE BRIDGE SLAB
6. PROPOSED HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
7. PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (MIN. 3/4")
8. PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "C", N50 (1/2")
9. PROPOSED P.C.C. PAVEMENT 9"
10. PROPOSED SUB-BASE GRANULAR MATERIAL TYPE A 4"
11. PROPOSED PAINT PAVEMENT MARKING - LINE 4" (STRIPE FOR 11'-0" LANE)
12. PROPOSED TEMPORARY PAVEMENT MARKING LINE - 4"
13. PROPOSED EARTH SHOULDER
14. PROPOSED PROPOSED CONCRETE BRIDGE SLAB

DETAILS AND TYPICAL ROADWAY SECTIONS
 ILL. ROUTE 117 OVER WALNUT CREEK
 F.A.P. RTE. 702 - SECTION (113B)
 WOODFORD COUNTY
 STA. 15+45.00
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	5
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GUARDRAIL SCHEDULE

LOCATION	TRAFFIC BARRIER TERMINAL TYPE 1 SPECIAL (TANGENT) (EACH)	STEEL PLATE BEAM GUARDRAIL TYPE A (FOOT)	TRAFFIC BARRIER TERMINAL TYPE 6A (EACH)
STAGE I			
STA. 14+09.7 TO STA. 14+59.7 LT.	1		
STA. 14+59.7 TO STA. 15+04 LT.			1
STA. 15+86 TO STA. 16+30.3 LT.			1
STA. 16+30.3 TO STA. 16+80.3 LT.	1		
STAGE II			
STA. 13+34.7 TO STA. 13+84.7 RT.	1		
STA. 13+84.7 TO STA. 14+59.7 RT.		75	
STA. 14+59.7 TO STA. 15+04 RT.			1
STA. 15+86 TO STA. 16+30.3 RT.			1
STA. 16+30.3 TO STA. 16+80.3 RT.	1		
TOTAL	4	75	4

BITUMINOUS SCHEDULE

LOCATION	TEMPORARY RAMP (SQ. YD.)	LEVELING BINDER (MACHINE METHOD), N50 (TON)	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50 (TON)	BITUMINOUS MATERIALS (PRIME COAT) (TON)
STA. 14+80 TO STA. 15+24			15.6	0.07
STA. 15+04 TO STA. 15+24		4.5		
STA. 15+66 TO STA. 15+86		4.5		
STA. 15+66 TO STA. 16+18			18.4	0.08
STA. 15+19 TO STA. 15+24	22			
STA. 15+66 TO STA. 15+71	22			
TOTAL	44	9	34	0.15

PAVEMENT MARKING SCHEDULE

LOCATION	LENGTH (FT.)	PAVEMENT MK. REMOVAL		SHORT TERM PVMT. MARKING LINE - 4"		PAINT PVMT. MK. LINE - 4"	
		WHITE (SQ. FT.)	YELLOW SKIP DASH & NO PASSING (SQ. FT.)	WHITE (FT.)	YELLOW (FT.)	WHITE (SQ. FT.)	YELLOW SKIP DASH & NO PASSING (SQ. FT.)
STA. 12+70 TO STA. 15+24 RT. (EDGE) ST. I	254	85					
STA. 15+66 TO STA. 18+50 RT. (EDGE) ST. I	284	95					
STA. 12+32.5 TO STA. 15+24 (C) ST. I	291.5		52				
STA. 15+66 TO STA. 18+57.5 (C) ST. I	291.5		101				
STA. 12+70 TO STA. 15+24 LT. (EDGE) ST. II	254	85					
STA. 15+66 TO STA. 18+25 LT. (EDGE) ST. II	284	95					
STA. 12+32.5 TO STA. 18+57.5 (C)	625			125			641
STA. 12+70 TO STA. 18+50 LT. (EDGE)	580					580	
STA. 12+70 TO STA. 18+25 RT. (EDGE)	555					555	
STA. 12+32.5 RT. (STOP BAR)	12						
STA. 18+57.5 LT. (STOP BAR)	12						
TOTAL		513		125		1776	

NOTE: SHORT-TERM PAVEMENT MARKING QUANTITIES ARE FOR TWO APPLICATIONS.
 * 10% OF TOTAL LENGTH FOR SHORT-TERM PAVEMENT MARKING

SCHEDULE GUARDRAIL & TERMINAL MARKERS

LOCATION	GUARDRAIL MARKERS TYPE A (EACH)	TERMINAL MARKER DIRECT APPLIED (EACH)
STA. 13+34.7 RT.		1
STA. 14+09.7 LT.		1
STA. 16+80.3 LT.		1
STA. 16+80.3 RT.		1
STA. 13+90 RT.	1	
STA. 14+70 RT.	1	
STA. 15+50 RT.	1	
STA. 16+30 RT.	1	
STA. 14+60 LT.	1	
STA. 15+15 LT.	1	
STA. 15+70 LT.	1	
STA. 16+25 LT.	1	
TOTAL	8	4

SCHEDULE GUARDRAIL AGGREGATE EROSION CONTROL

LOCATION	QUANTITY (TON)
STA. 13+30 TO STA. 15+23 RT.	42
STA. 13+87.5 TO STA. 15+23 LT.	25
STA. 15+67.3 TO STA. 17+00 RT.	24
STA. 15+67.3 TO STA. 17+00 LT.	24
TOTAL	115

SCHEDULE RAISED REFLECTIVE PAVEMENT MARKER

LOCATION	RAISED REFL PAVT MRK REM (EACH)
STA. 14+85 (C)	1
STA. 15+65 (C)	1
TOTAL	2

SCHEDULE PRECAST CONCRETE BRIDGE SLAB

LOCATION	QUANTITY (SQ. YD.)
STA. 15+04 TO STA. 15+23.92 LT. & RT.	143.5
STA. 15+66.08 TO STA. 15+86 LT. & RT.	143.5
TOTAL	287

SCHEDULE PAVEMENT REMOVAL

LOCATION	QUANTITY (SQ. YD.)
STA. 15+04 TO STA. 15+24	70
STA. 15+66 TO STA. 15+86	70
TOTAL	140

SCHEDULE PERMANENT SEEDING

LOCATION	SEEDING CLASS 2 (ACRE)	NITROGEN FERT. NUT. (POUND)	PHOSPHORUS FERT. NUT. (POUND)	POTASSIUM FERT. NUT. (POUND)	EROSION CONTROL BLANKET (SQ. YD.)	TEMPORARY EROSION CONTROL SEEDING (POUND)
STA. 13+30 TO STA. 17+00	0.20	18	18	18	968	20
TOTAL	0.20	18	18	18	968	20

SCHEDULE HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

LOCATION	QUANTITY (SQ. YD.)
STA. 14+80 TO STA. 14+99	80
STA. 15+91 TO STA. 16+18	114
TOTAL	194

EARTHWORK SCHEDULE

LOCATION	EARTH EXCAVATION	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
	CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD
STA. 13+30 TO STA. 15+04	0	0	83	-83
STA. 15+86 TO STA. 17+00	0	0	71	-71
TOTAL	0	0	154	-154

SCHEDULE PERIMETER EROSION BARRIER

LOCATION	QUANTITY (FEET)
STA. 13+50 TO STA. 15+25 RT.	200
STA. 14+00 TO STA. 15+25 LT.	150
STA. 15+67 TO STA. 16+50 RT.	100
STA. 15+67 TO STA. 17+00 LT.	150
TOTAL	600

SCHEDULE SUB-BASE GRANULAR MATERIAL TYPE A

LOCATION	QUANTITY (SQ. YD.)
STA. 15+04 TO STA. 15+24	70
STA. 15+66 TO STA. 15+86	70
TOTAL	140

SCHEDULE PORTLAND CEMENT CONCRETE BASE COURSE 9"

LOCATION	QUANTITY (SQ. YD.)
STA. 15+04 TO STA. 15+24	70
STA. 15+66 TO STA. 15+86	70
TOTAL	140

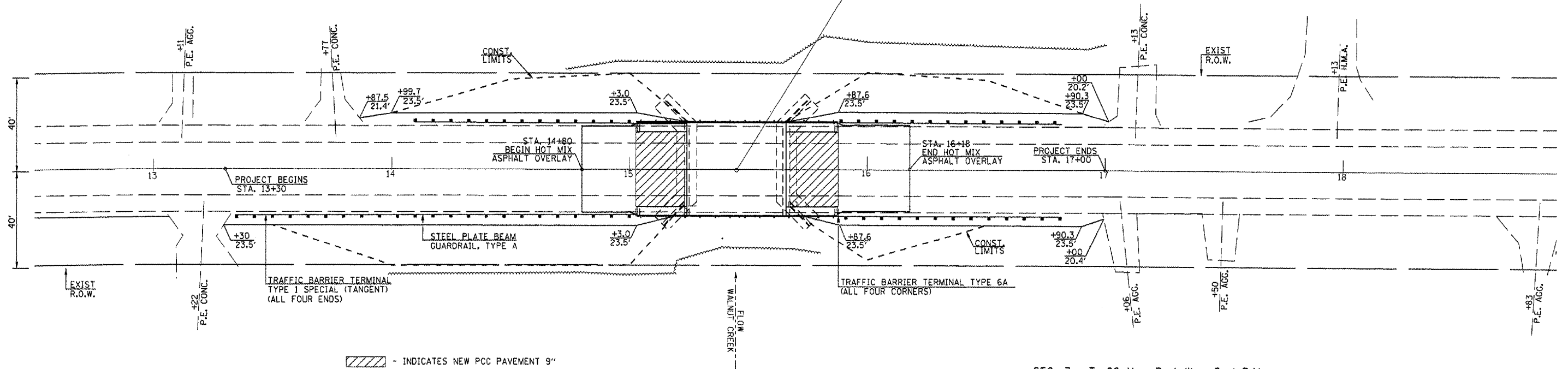
QUANTITY SCHEDULES
 IL ROUTE 117 OVER WALNUT CREEK
 F.A.P. RTE. 702 - SECTION (113B)
 WOODFORD COUNTY
 STA. 15+45.00
 S.N. 102-0018

FILE NAME: 05 REV. 12/7/06

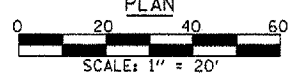
SEC. 12, T. 26 N., R. 2 W., 3rd P.M.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	6
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				

STA. 15+45 - @ RDWY. AT @ STRUCTURE
 PROPOSED STRUCTURE: PRECAST CONCRETE DECK BEAM SUPERSTRUCTURE ON EXISTING SINGLE SPAN SUBSTRUCTURE. 44'-3 1/2" BK.-BK. ABUTMENTS, 39'-0" CLEAR DECK WIDTH STEEL BRIDGE RAIL, TYPE SM, 0° SKEW



▨ - INDICATES NEW PCC PAVEMENT 9"

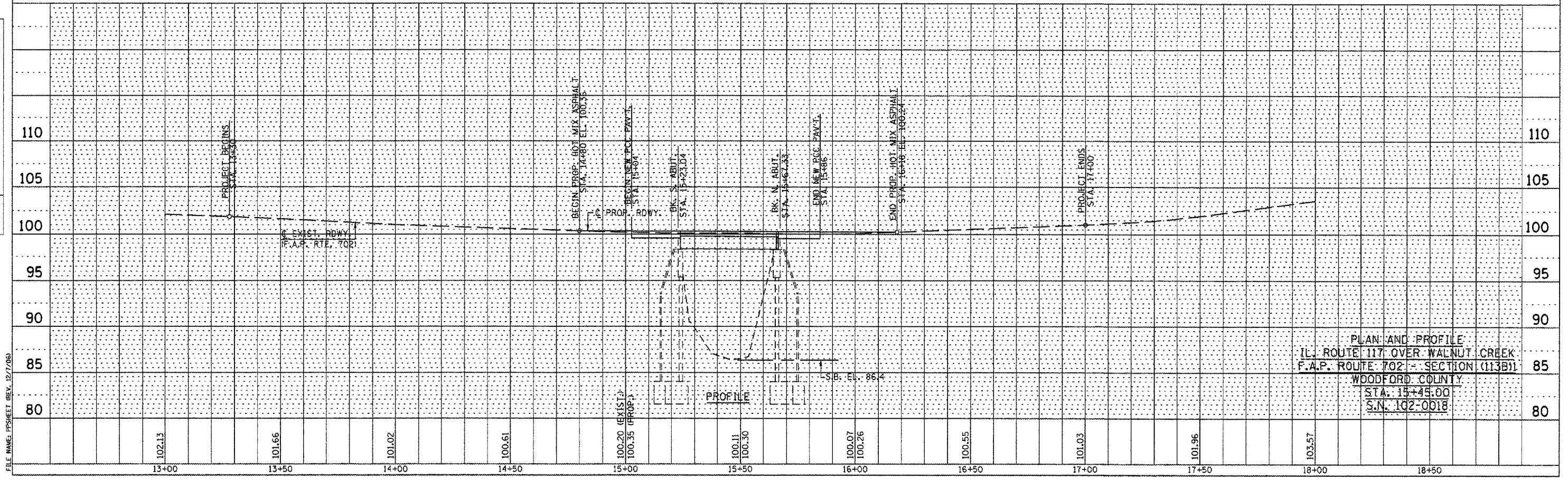


SEC. 7, T. 26 N., R. 1 W., 3rd P.M.

BM#1 CHISELED SQUARE IN S.W. BRIDGE SEAT
 ELEV. 98.05

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAYS CHECKED	
	FILE NAME	

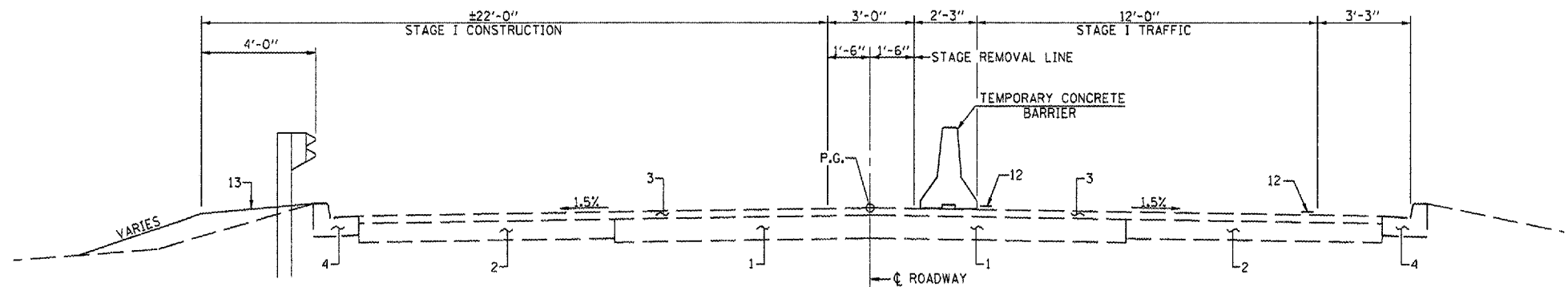
PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAYS CHECKED	
	FILE NAME	



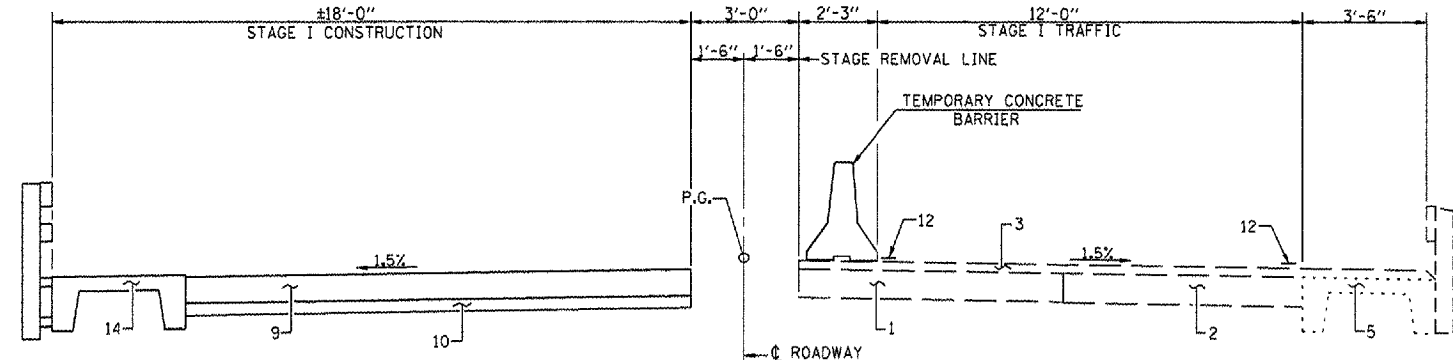
PLAN AND PROFILE
 ILL. ROUTE 117 OVER WALNUT CREEK
 F.A.P. ROUTE 702 - SECTION (113B)
 WOODFORD COUNTY
 STA. 15+45.00
 S.N. 102-0018

FILE NAME: PPSHEET REV. 12/7/06

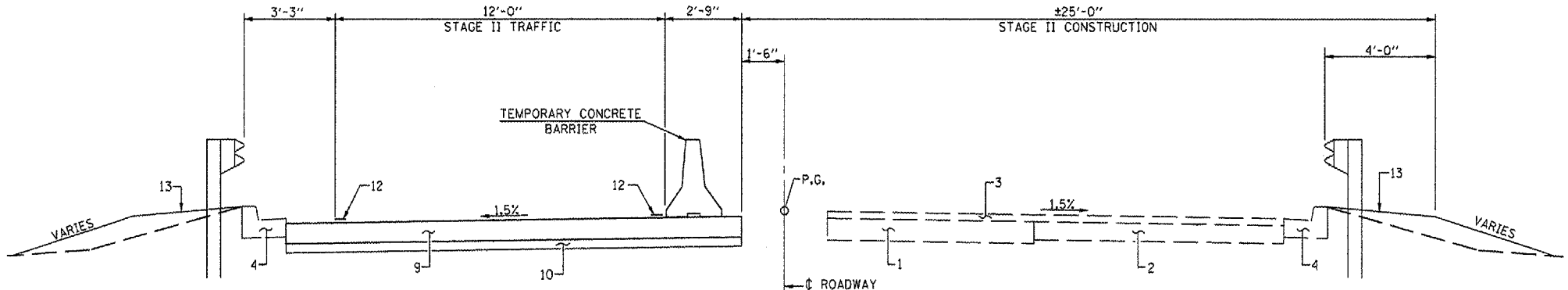
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	7
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				



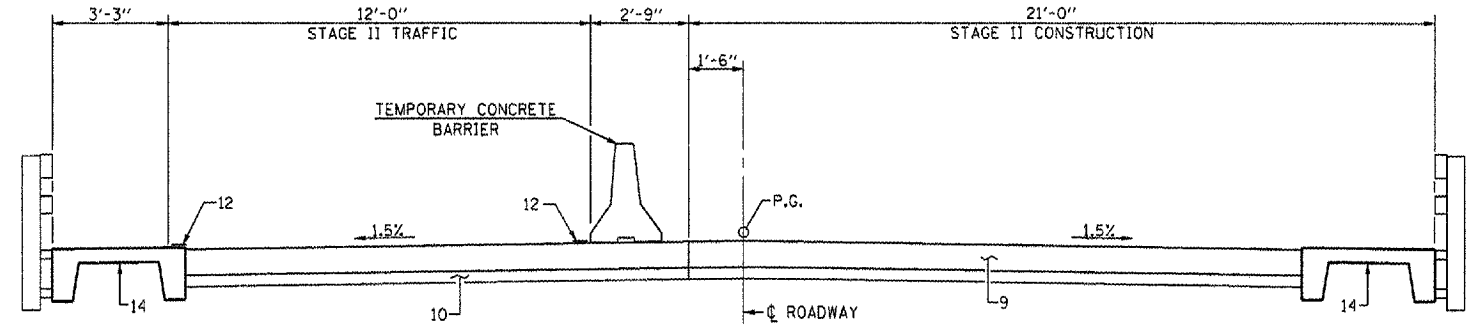
STAGE I TRAFFIC
 STA. 14+82.5 TO STA. 15+04 &
 STA. 15+86 TO STA. 16+07.5



STAGE I TRAFFIC
 STA. 15+04 TO STA. 15+24 &
 STA. 15+66 TO STA. 15+86



STAGE II TRAFFIC
 STA. 14+82.5 TO STA. 15+04 &
 STA. 15+86 TO STA. 16+07.5



STAGE II TRAFFIC
 STA. 15+04 TO STA. 15+24 &
 STA. 15+66 TO STA. 15+86

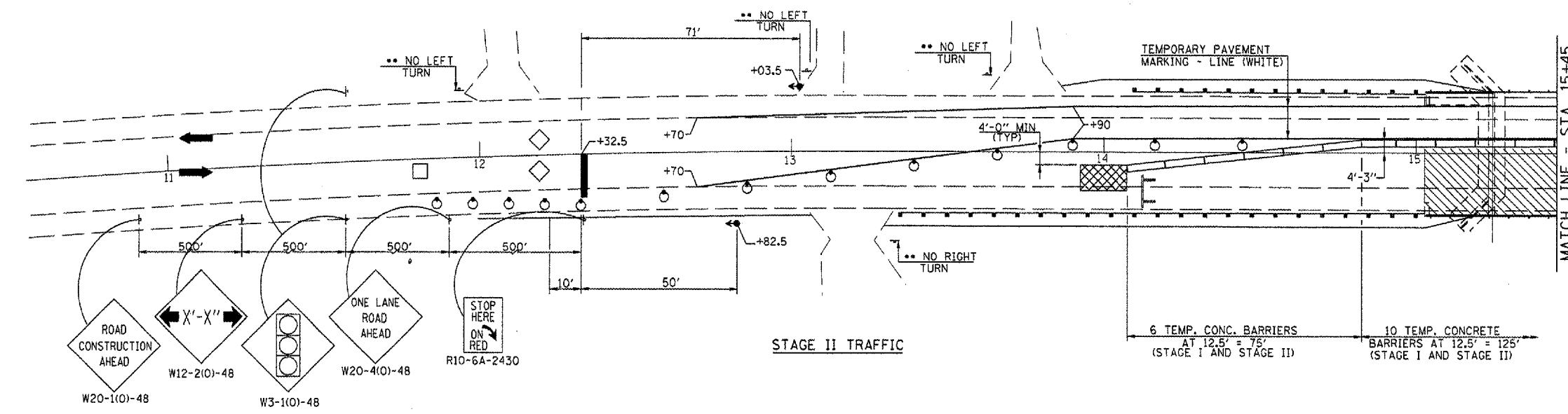
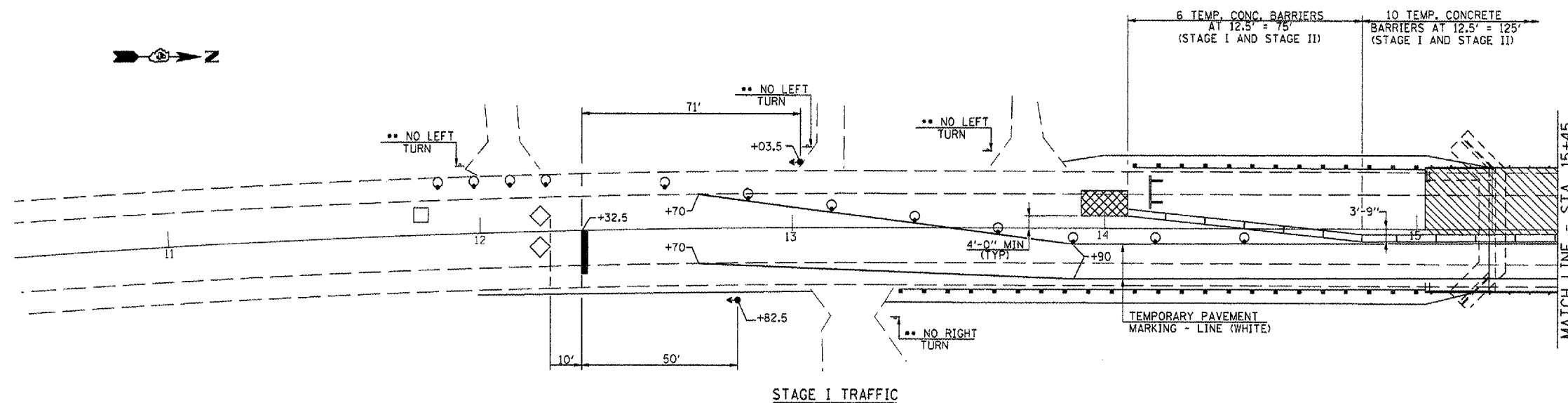
PAVEMENT LEGEND

1. EXISTING P.C.C. PAVEMENT
2. EXISTING P.C.C. BASE COURSE WIDENING 8"
3. EXISTING HOT MIX ASPHALT OVERLAY
4. EXISTING TYPE B-6.12 CONCRETE CURB & GUTTER
5. EXISTING PRECAST CONCRETE BRIDGE SLAB
6. PROPOSED HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
7. PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (MIN. 3/4")
8. PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "C", N50 (1 1/2")
9. PROPOSED P.C.C. PAVEMENT 9"
10. PROPOSED SUB-BASE GRANULAR MATERIAL TYPE A 4"
11. PROPOSED PAINT PAVEMENT MARKING - LINE 4" (STRIPE FOR 11'-0" LANE)
12. PROPOSED TEMPORARY PAVEMENT MARKING LINE - 4"
13. PROPOSED EARTH SHOULDER
14. PROPOSED PROPOSED CONCRETE BRIDGE SLAB

NOTES: ALL TYPICAL SECTIONS LOOKING NORTH
 WORK THIS SHEET WITH SHEETS 8 & 9 OF 38.

STAGE CONSTRUCTION TRAFFIC DETAILS
 IL ROUTE 117 OVER WALNUT CREEK
 F.A.P. RTE. 702 - SECTION (113B)
 WOODFORD COUNTY
 STA. 15+45.00
 S.N. 102-0018

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	8
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. 4 (ILLINOIS) FED. AID PROJECT				



SUGGESTED STAGE CONSTRUCTION SEQUENCE

STAGE I

1. ERECT TRAFFIC CONTROL FOR STAGE I.
2. REMOVE EXISTING DECK LEFT, @ STA. 15+45.
3. CONSTRUCT PROPOSED STAGE I PRECAST PRESTRESSED DECK @ STA. 15+45 AND TEMPORARY RAMP.
4. CONSTRUCT PROPOSED GUARDRAIL & TERMINALS LT, STA. 14+09.70 TO STA. 16+80.30.

STAGE II

1. ERECT TRAFFIC CONTROL FOR STAGE II.
2. REMOVE EXISTING STRUCTURE RIGHT, @ STA. 15+45.
3. CONSTRUCT PROPOSED STAGE II PRECAST PRESTRESSED DECK @ STA. 15+45 AND TEMPORARY RAMP.
4. CONSTRUCT PROPOSED GUARDRAIL & TERMINALS RT, STA. 13+34.70 TO STA. 16+80.30.

FINAL

1. REMOVE ALL STAGE TRAFFIC CONTROL AND RE-ESTABLISH NORMAL TRAFFIC PATTERNS.
2. COMPLETE HOT MIX ASPHALT SURFACE REMOVAL, BINDER AND SURFACE COURSE UNDER TRAFFIC WITH FLAGGERS.
3. FINAL STRIPING, SEEDING AND MISCELLANEOUS CLEANUP.

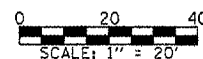
GENERAL NOTES

1. THIS TRAFFIC CONTROL DETAIL SHALL BE USED IN CONJUNCTION WITH STANDARD 701321.
2. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE REVISED STAGE TRAFFIC PATTERNS DURING ALL PHASES OF STAGE CONSTRUCTION SHALL BE REMOVED AS SPECIFIC IN SECTION 783 OF THE STANDARD SPECIFICATIONS AND PAID FOR AS "PAVEMENT MARKING REMOVAL".
3. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PRIVATE AND COMMERCIAL PROPERTIES DURING ALL PHASES OF CONSTRUCTION.
4. EACH DETECTOR LOOP SHALL BE CONNECTED TO A SEPERATE DETECTOR AMPLIFIER.
5. SIGNING FOR STAGE II SAME AS STAGE I.

••THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT THE SIGNS AT THE LOCATIONS, AS DIRECTED BY THE RESIDENT ENGR./TECH. ALL SIGNS SHALL BE POST MOUNTED.
 THE ABOVE NOTED WORK, INCLUDING SIGNS, POSTS, HARDWARE, ALL TYPE III BARRICADES AND LABOR SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE, EACH, FOR TRAFFIC CONTROL & PROTECTION STD. 701321 (SPECIAL) AND NO OTHER COMPENSATION WILL BE ALLOWED.

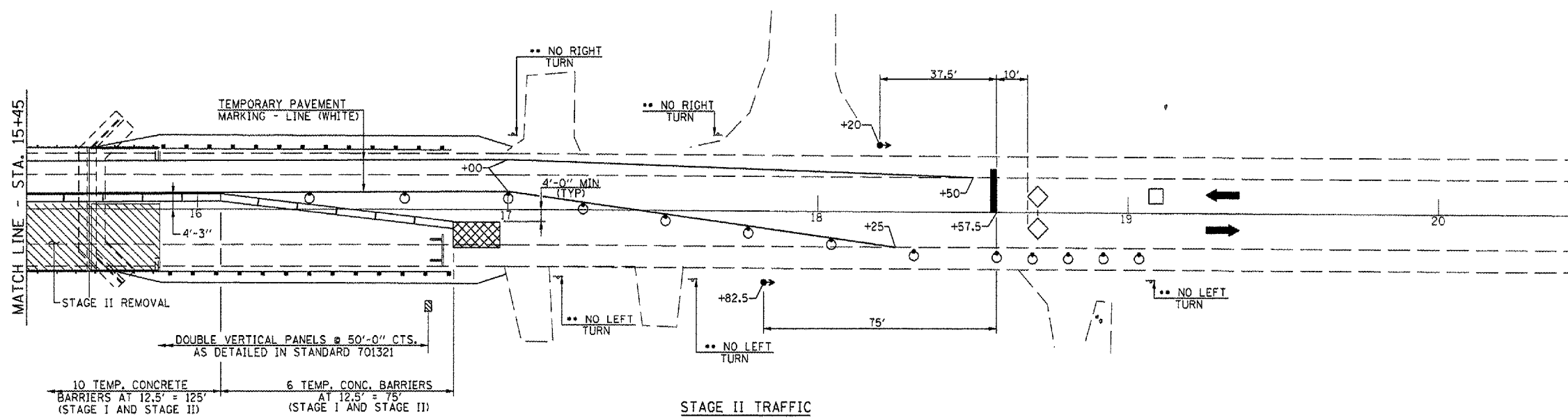
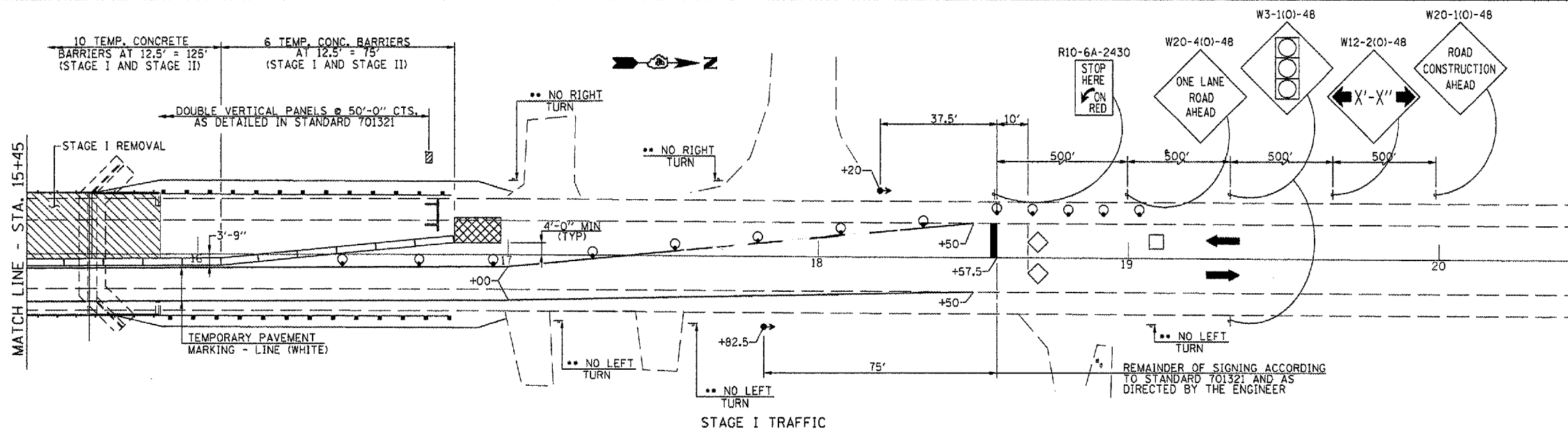
NOTE: WORK THIS SHEET WITH SHEETS 7 & 9 OF 38.

- SYMBOLS**
- ▨ WORK AREA
 - ⊥ SIGN
 - ⌋ TYPE III BARRICADE
 - ⊙ DRUM WITH STEADY BURNING LIGHT
 - ◄◄ TRAFFIC SIGNAL
 - ▤ TEMPORARY RUMBLE STRIP
 - ◻◇ INDUCTION LOOP DETECTOR
 - ▧ DOUBLE VERTICAL PANEL
 - ◇ TYPE C BIDIRECTIONAL REFLECTOR
 - ▬ TEMPORARY CONCRETE BARRIER
 - ▩ IMPACT ATTENUATOR
 - STEADY BURNING LIGHTS AND DOUBLE VERTICAL PANELS



FILE NAME: STAGESHEET (REV. 12/7/06)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	9
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL SCHEDULE

LOCATION STATION TO STATION	TEMP. CONC. BARRIER (FOOT)	RELOCATE TEMP. CONC. BARRIER (FOOT)	IMPACT ATTENUATOR TEMPORARY (EACH)	RELOCATE IMPACT ATTENUATOR (EACH)
STAGE I				
STA. 13+92.5 TO STA. 14+07.5			1	
STA. 14+07.5 TO STA. 16+82.5	275			
STA. 16+82.5 TO STA. 16+97.5			1	
STAGE II				
STA. 13+92.5 TO STA. 14+07.5				1
STA. 14+07.5 TO STA. 16+82.5		275		
STA. 16+82.5 TO STA. 16+97.5				1
TOTAL	275	275	2	2

SCHEDULE TEMPORARY PAVEMENT MARKING & WORK ZONE PAVEMENT MARKING REMOVAL

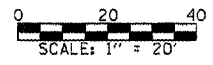
LOCATION STATION TO STATION	TEMP. PAV'T. MARKING LINE - 4" (FOOT)	TEMP. PAV'T. MARKING LINE - 24" (FOOT)	WORK ZONE PAVEMENT MARKING REMOVAL (FOOT)
STAGE I			
STA. 12+32.5 RT.		12	24
STA. 12+70 TO STA. 18+50 RT.	530		177
STA. 12+70 TO STA. 18+50 (C)	580		194
STA. 18+57.5 LT.		12	24
STAGE II			
STA. 12+70 TO STA. 18+25 LT.	555		185
STA. 12+70 TO STA. 18+50 (C)	580		194
TOTAL	2245	24	798

- SYMBOLS**
- WORK AREA
 - SIGN
 - TYPE III BARRICADE
 - DRUM WITH STEADY BURNING LIGHT
 - TRAFFIC SIGNAL
 - TEMPORARY RUMBLE STRIP
 - INDUCTION LOOP DETECTOR
 - DOUBLE VERTICAL PANEL
 - TYPE C BIDIRECTIONAL REFLECTOR
 - TEMPORARY CONCRETE BARRIER
 - IMPACT ATTENUATOR
 - STEADY BURNING LIGHTS AND DOUBLE VERTICAL PANELS

**THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT THE SIGNS AT THE LOCATIONS, AS DIRECTED BY THE RESIDENT ENGR./TECH. ALL SIGNS SHALL BE POST MOUNTED.
 THE ABOVE NOTED WORK, INCLUDING SIGNS, POSTS, HARDWARE, ALL TYPE III BARRICADES AND LABOR SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE, EACH, FOR TRAFFIC CONTROL & PROTECTION STD. 701321 (SPECIAL) AND NO OTHER COMPENSATION WILL BE ALLOWED.

NOTE: WORK THIS SHEET WITH SHEETS 7 & 8 OF 38.

STAGE CONSTRUCTION TRAFFIC DETAILS
 IL ROUTE 117 OVER WALNUT CREEK
 F.A.P. RTE. 702 - SECTION (113B)
 WOODFORD COUNTY
 STA.15+45.00
 S.N. 102-0018



FILE NAME: STAGESHEET (REV. 12/7/06)

Benchmark: Chiseled "□" on top of S.W. Abutment Cap, El. 98.05
 Existing Structure: S.N. 102-0018 was originally built as S.B.I. Route 117 Section 113
 in 1928 at Sta. 15+45. The original structure was a single span R.C. Deck Girder
 with R.C. Closed Abutments. In 1979, the superstructure was removed and replaced
 with P.P.C. Deck Beams under F.A.P. 702 Section (113B)I at Sta. 15+45.
 Proposed Improvement: Existing P.P.C. Deck Beams are to be removed and replaced and the
 substructure repaired. Traffic to be maintained utilizing stage construction.
 No Salvage

Note: These plans have been developed in accordance with the
 September 14, 2006 memo to all Deputy Directors of Highways
 from Milton R. Sees by Ralph E. Anderson. Per the memo, the
 existing structure was inspected and analyzed for the proposed
 Stage I Traffic and determined to be adequate. However, the
 remaining life of the structure was not established. Therefore,
 verification of the structural adequacy of the existing structure
 is required prior to the start of construction.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)I	WOODFORD	38	10
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
Contract No. 68635				

SHEET NO. 1
OF 14 SHEETS

GENERAL NOTES

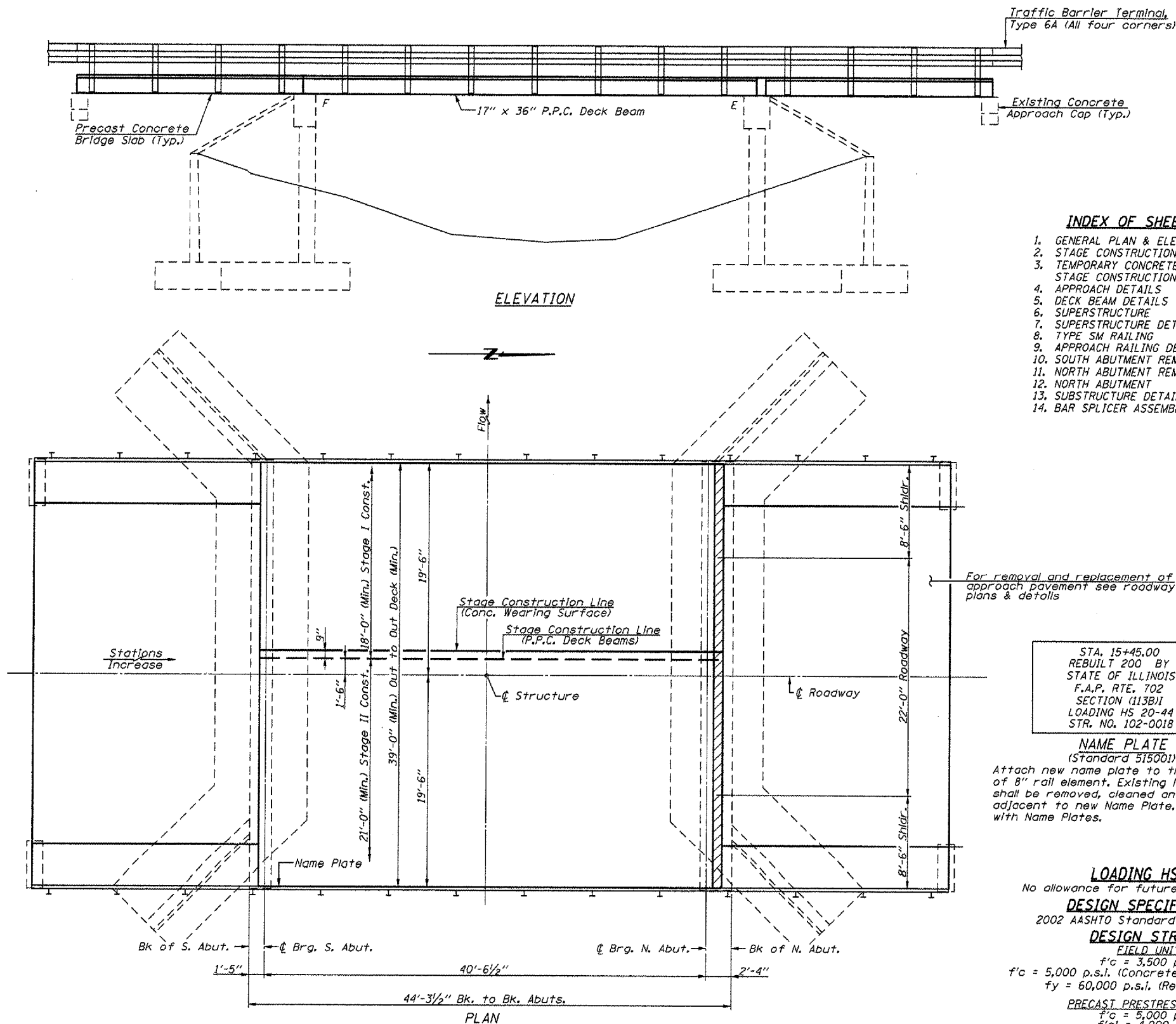
Reinforcement bars shall conform to the requirements of ASTM A 706 Grade 60 (IL Modified). See Special Provisions.
 Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation or a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price for the work.
 All construction joints shall be bonded.
 The cut strands at each beam end shall be given two coats of zinc dust spray or paint meeting requirements of ASTM A 780. The zinc dust spray or paint shall be applied before corrosion appears and allowed to dry according to manufacturer's specifications prior to another coat of zinc. A concrete sealer meeting the requirements of Section 587 of the Standard Specifications shall be applied to the exterior face and 9" in on the underside of the fascia beams. The sealer shall be applied after visible crack growth has subsided. This work shall be performed by the producer and included with the cost of the beam.
 The minimum thickness of the concrete overlay shall be 5" and varies as required to adjust for the new profile grade and beam camber.
 The Contractor is advised that the existing P.P.C. Deck Beams are in a deteriorated condition with reduced load bearing capacity. It is the Contractor's responsibility to account for the condition of the beams when developing construction procedures for removing and replacement of the superstructure.
 If the Contractor's procedure for existing beam removal or placement of new beams involves placement of cranes or other heavy equipment on new beams, a detailed procedure shall be submitted to the Engineer for approval. The procedure shall include calculations prepared and sealed by an Illinois Licensed Structural Engineer verifying that the equipment and procedure used will not overstress the new beams. To distribute load to multiple beams and protect the concrete in all cases a double layer mat of heavy timbers shall be used at all times under crane tracks or wheels and any outriggers in the down position. If necessary, shims shall be used under the crane mat to insure uniform contact with the underlying beams. Prior to placement of the timber mats, the following shall be done: placement and tightening of transverse tie assemblies, grouting and curing the dowel rods 24 hours minimum, and grouting and curing the shear keys.
 No instream work will be allowed on this project.
 All structural steel shall be shop painted with inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Preformed Joint Strip Seal.
 Repairs of abutments shall be completed prior to placement of the new deck beams.

INDEX OF SHEETS

1. GENERAL PLAN & ELEVATION
2. STAGE CONSTRUCTION DETAILS
3. TEMPORARY CONCRETE BARRIER
4. APPROACH DETAILS
5. DECK BEAM DETAILS
6. SUPERSTRUCTURE
7. SUPERSTRUCTURE DETAILS
8. TYPE SM RAILING
9. APPROACH RAILING DETAILS
10. SOUTH ABUTMENT REMOVAL & REPAIR
11. NORTH ABUTMENT REMOVAL & REPAIR
12. NORTH ABUTMENT
13. SUBSTRUCTURE DETAILS
14. BAR SPLICER ASSEMBLY DETAILS

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Protective Coat	Sq. Yd.	181		181
Removal of Existing Superstructures	Each	1		1
Concrete Removal	Cu. Yd.		2.0	2.0
Concrete Structures	Cu. Yd.		2.6	2.6
Bridge Deck Grooving	Sq. Yd.	172		172
Precast Concrete Bridge Slab	Sq. Ft.	299		299
Precast Prestressed Concrete Deck Beams (17" Depth)	Sq. Ft.	1627		1627
Reinforcement Bars, Epoxy Coated	Pound	2240	400	2640
Bar Splicers	Each	42	6	48
Steel Railing, Type SM	Foot	165		165
Name Plates	Each	1		1
Epoxy Crack Injection	Foot		30	30
Structural Repair of Concrete (Depth Equal To Or Less Than 5")	Sq. Ft.		15	15
Concrete Wearing Surface, 5"	Sq. Yd.	181		181
Preformed Joint Strip Seal	Foot	41		41
Asbestos Bearing Pad Removal	Each	26		26



STA. 15+45.00
 REBUILT 200 BY
 STATE OF ILLINOIS
 F.A.P. RTE. 702
 SECTION (113B)I
 LOADING HS 20-44
 STR. NO. 102-0018

NAME PLATE
(Standard 51500)

Attach new name plate to the backside of 8" rail element. Existing Name Plate shall be removed, cleaned and relocated adjacent to new Name Plate. Cost included with Name Plates.

LOADING HS20-44

No allowance for future wearing surface.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications.

DESIGN STRESSES

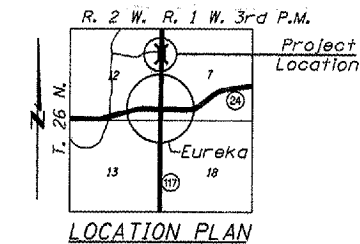
- FIELD UNITS**
 $f'_c = 3,500$ p.s.i.
 $f'_c = 5,000$ p.s.i. (Concrete Wearing Surface)
 $f_y = 60,000$ p.s.i. (Reinforcement)
PRECAST PRESTRESSED UNITS
 $f'_c = 5,000$ p.s.i.
 $f'_{cl} = 4,000$ p.s.i.
 $f'_s = 270,000$ p.s.i. ($1/2$ " ϕ low lax strands)
 $f'_{sl} = 201,960$ p.s.i. ($1/2$ " ϕ low lax strands)
PRECAST CONCRETE UNITS
 $f'_c = 4,500$ p.s.i.
 $f'_y = 60,000$ p.s.i. (Reinforcement)

APPROVED
 FOR STRUCTURAL ADEQUACY ONLY
Ralph E. Anderson
 ENGINEER OF BRIDGES AND STRUCTURES



Gerald B. Rothbard 12/16/06
 Expiration 11/30/08

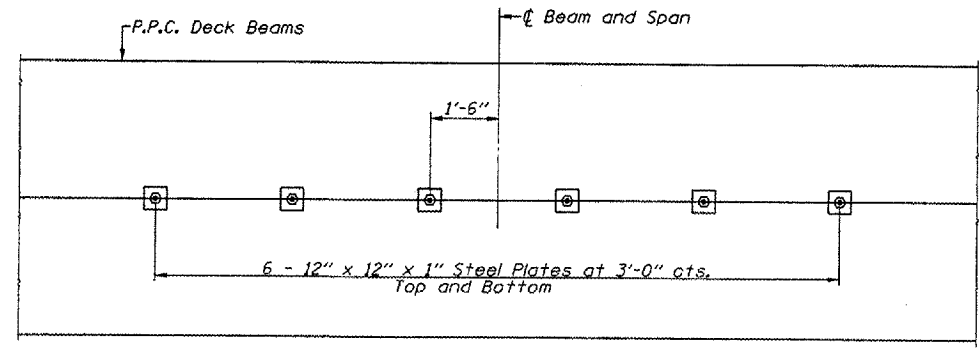
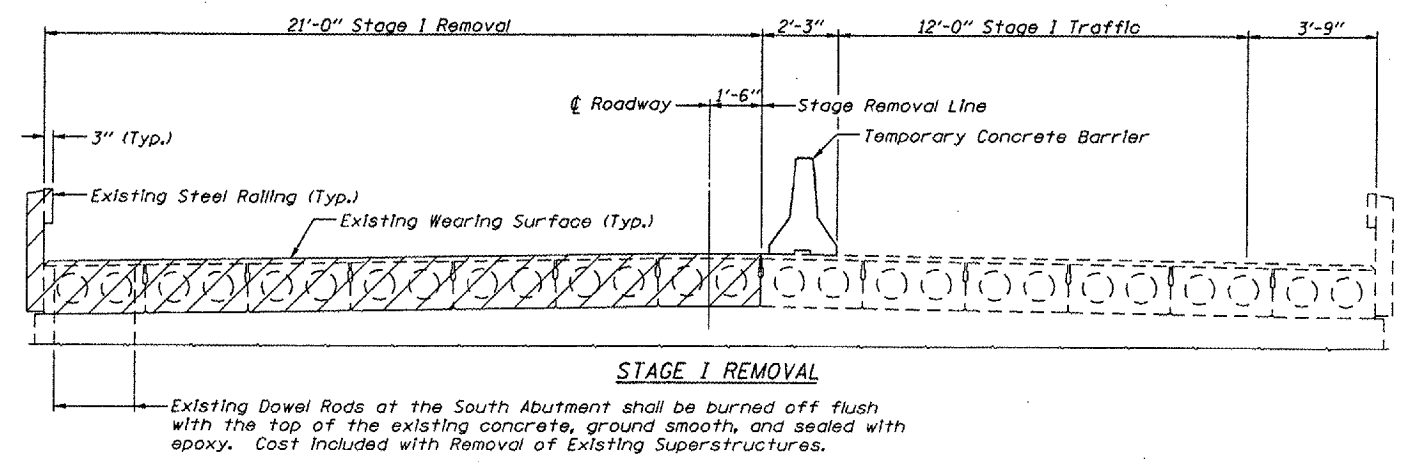
GENERAL PLAN AND ELEVATION
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)I
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018



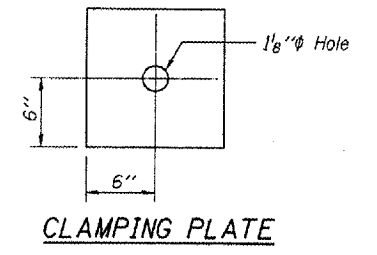
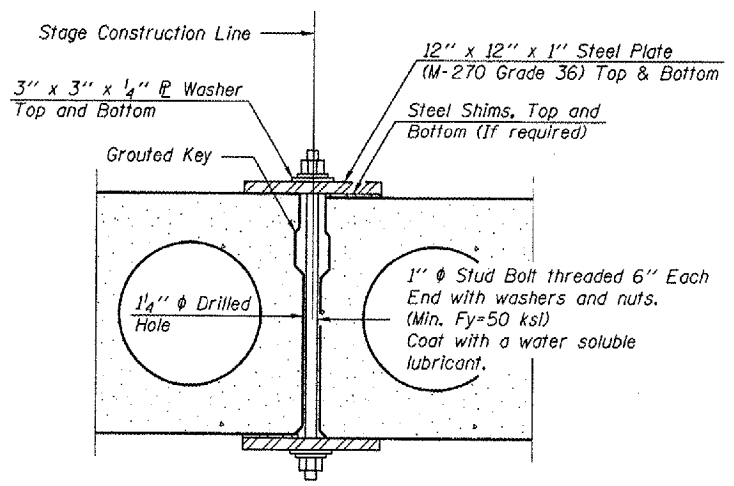
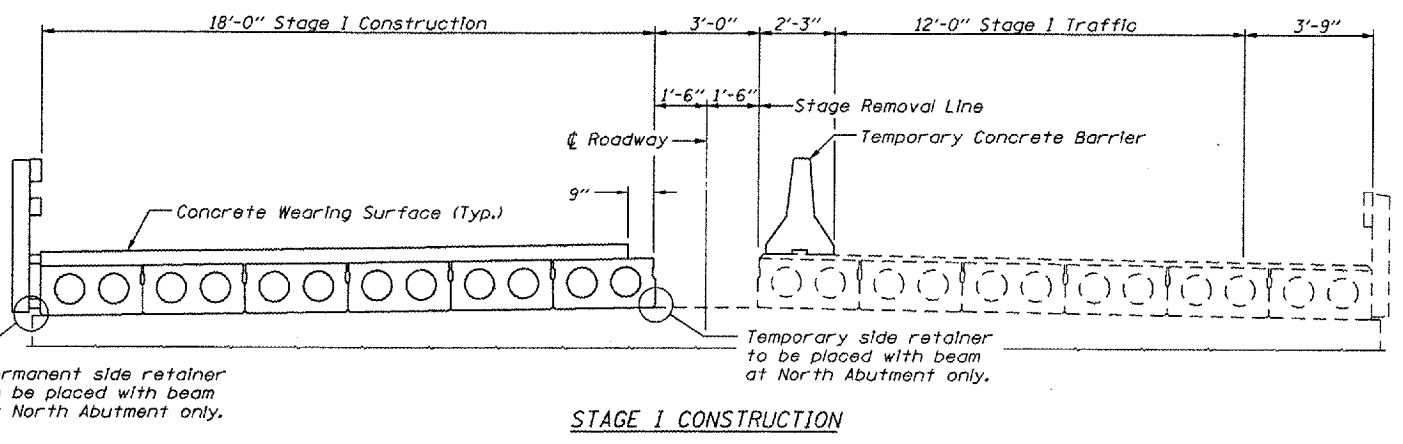
Designed: G.B.R.
 Checked: M.A.H.
 Drawn: F.L.L.
 Checked: G.B.R.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)I	WOODFORD	38	11
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
Contract No. 68635				

SHEET NO. 2
OF 14 SHEETS



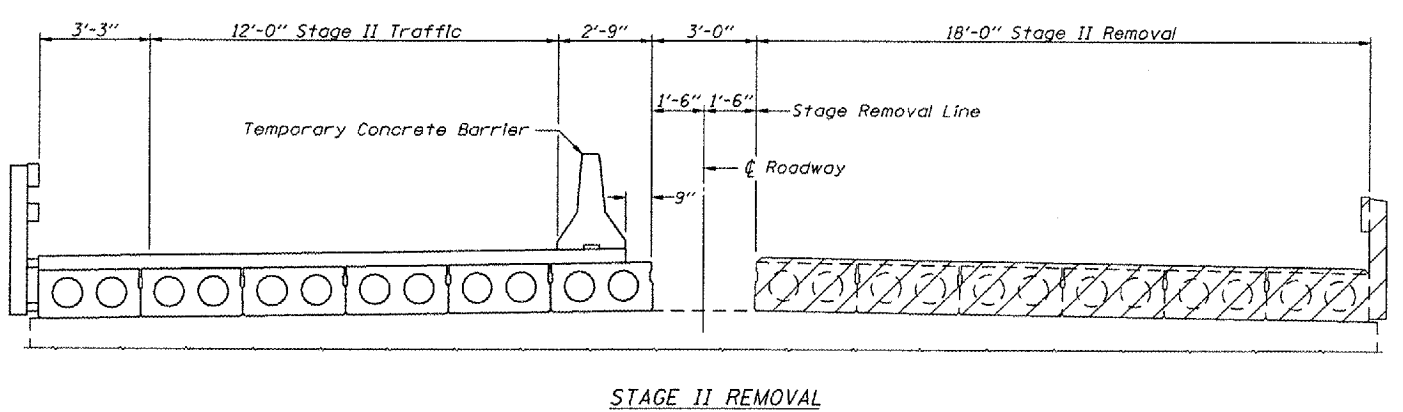
PLAN AT STAGE CONSTRUCTION LINE



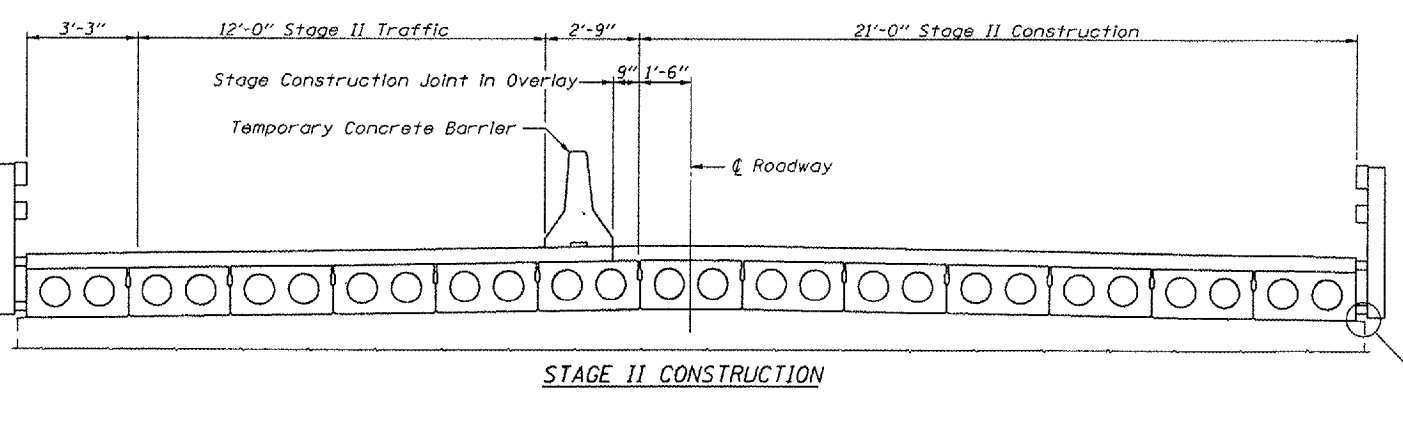
SECTION

SHEAR KEY CLAMPING DETAILS AT STAGE CONST. JT.

See Article 504.06 (d) of the Standard Specifications for Stage Construction of Precast Prestressed Concrete Deck Beams.
Cost included with "Precast Prestressed Concrete Deck Beams".
See Stage Construction Details for traffic lanes.



STAGE II REMOVAL



STAGE II CONSTRUCTION

Notes: All Cross sections are looking North.
Hatched area indicates "Removal of Existing Superstructures."
Cost of removal of the existing wearing surface, existing precast concrete bridge slab and steel railing (including the railing on the approach) are included with "Removal of Existing Superstructures".
For quantity of Temporary Concrete Barrier see Roadway Plans.
For Permanent & Temporary Side Retainer details see Sheet 13 of 14.

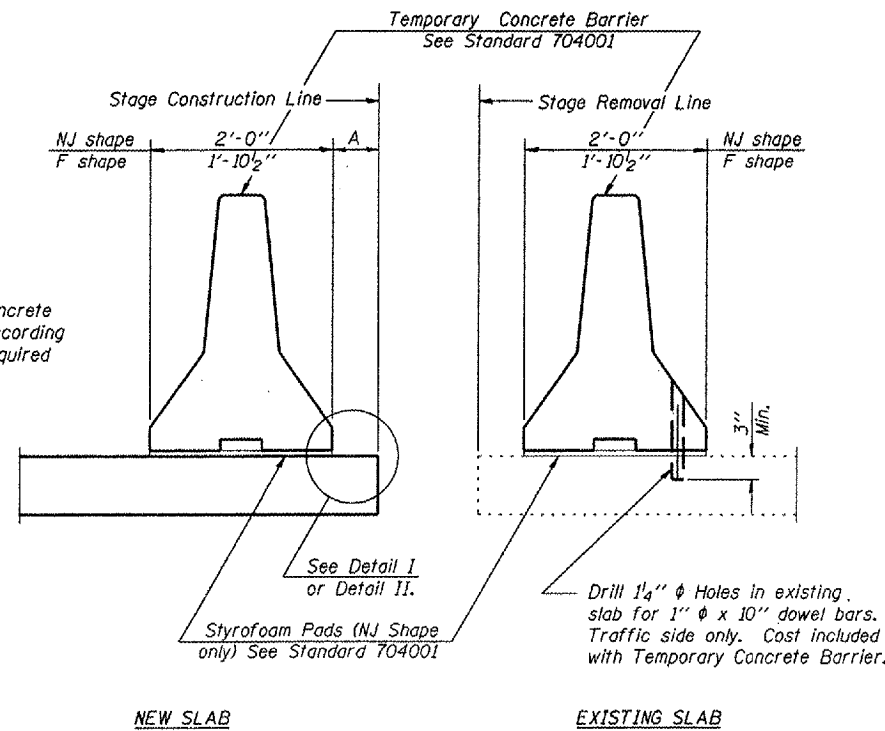
STAGE CONSTRUCTION DETAILS
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)I
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

FILE NAME: STRUCTURE PLANS (REV. 12/1/06)

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)I	WOODFORD	38	12
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

Contract No. 68635

SHEET NO. 3
OF 14 SHEETS

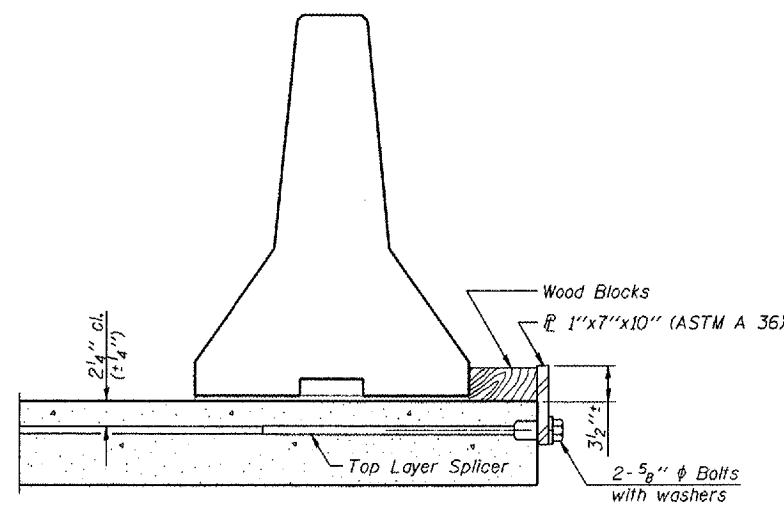


When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

NOTES

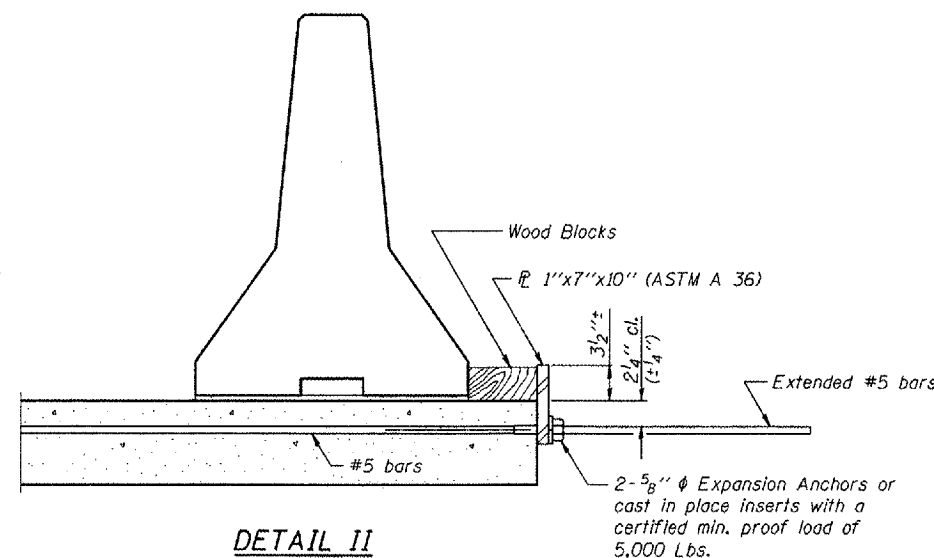
- Detail I - With Bar Splicer or Couplers:**
Connect one (1) 1"x7"x10" steel \bar{r} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{c} of each barrier panel.
- Detail II - With Extended Reinforcement Bars:**
Connect one (1) 1"x7"x10" steel \bar{r} to the concrete slab with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{c} of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier.

SECTIONS THRU SLAB



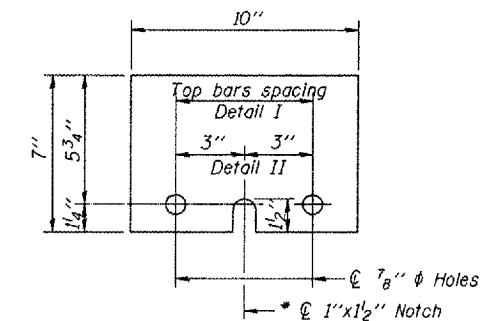
DETAIL I

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and reinforcement bars are in place.



DETAIL II

The 1"x7"x10" Plate shall not be removed until Stage II Construction forms and all reinforcement bars are in place and the concrete is ready to be placed.



1"x7"x10"

* Required only with Detail II

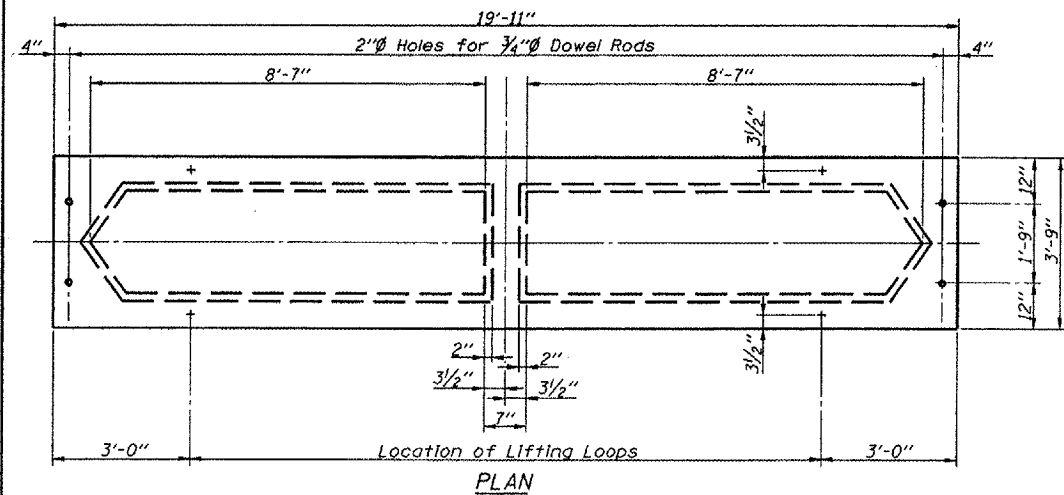
**TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)I
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018**

FILE NAME: STRUCTURE PLANS/REV. 12/14/06I

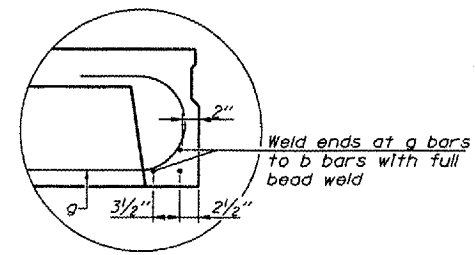
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)	WOODFORD	38	13
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
Contract No. 68635				

SHEET NO. 4
OF 14 SHEETS

Note: Tack welding of stirrups to bottom longitudinal reinforcement bars will not be permitted except as otherwise authorized in writing by the Engineer.

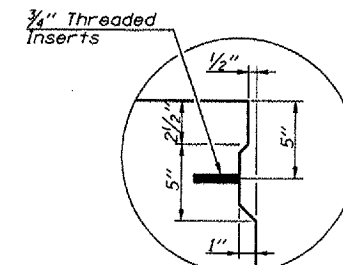


PLAN

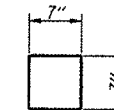


DETAIL A

The surface of the member shall not deviate more than 1/1200 of the full length of the member from a straight line connecting the two end points on the member's surface. In addition to State inspection and prior to erection, the beam shall be tested and approved by the resident Engineer at the jobsite.
The units shall remain on the bottom supporting forms until the concrete has attained a compressive strength of not less than 3,500 pounds per square inch.
The bearing seat surfaces shall be adjusted by shimming to assure firm and even bearing. Two 1/8" fabric adjusting shims of the dimensions shown shall be provided for each bearing.

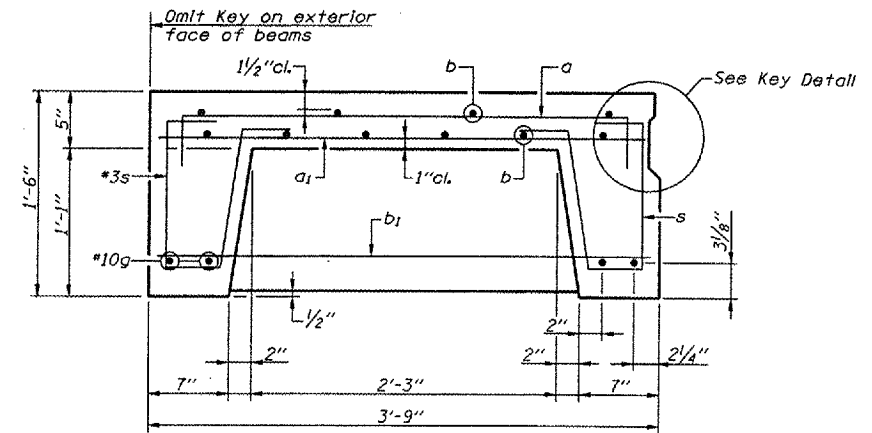


KEY DETAIL



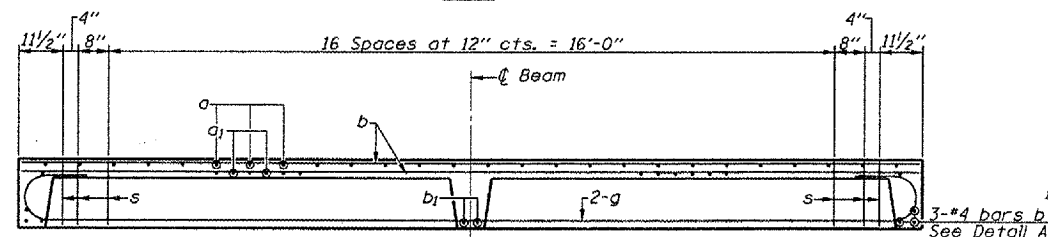
BEARING PADS
(1" - 8 Required)
(1/4" - 8 Required)

** Joint shall be filled with non-shrink grout. Dimension may vary to accommodate tolerance in beam lengths.

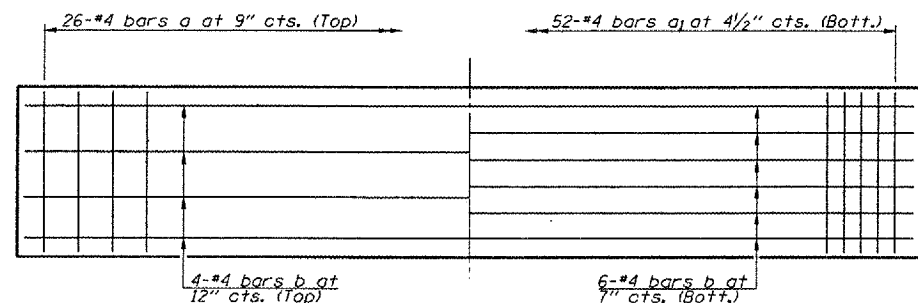


SECTION THRU BEAM

* Existing Dowel Rods shall be cut off and ground flush with the top of the existing concrete. Cost to be included in the cost of "Removal of Existing Superstructures". New Dowel Rods shall be routed after beams are in place and allowed to cure a minimum of 24 hours prior to pouring the new approach.

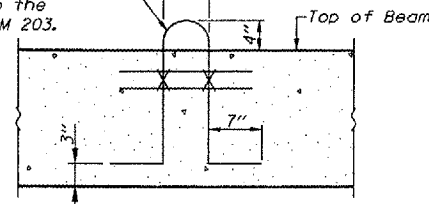


LONGITUDINAL SECTION



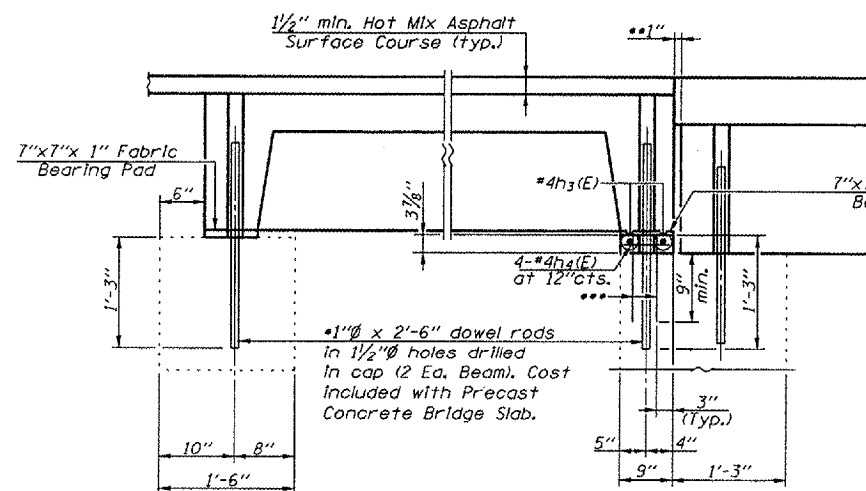
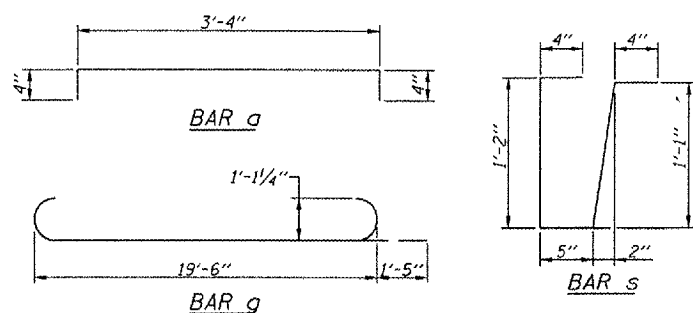
SLAB REINFORCEMENT

2-1/2"Ø Strands/Loop, 2 - Ea. End. Each Beam Loop shall be burned off after beams have been erected. Strands shall conform to the requirements of AASHTO M 203.



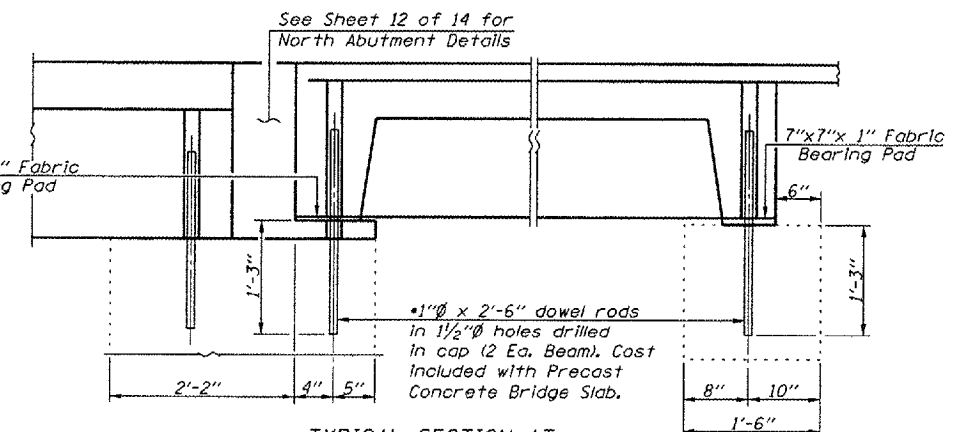
LIFTING LOOP

Approved alternate may be substituted for the above.



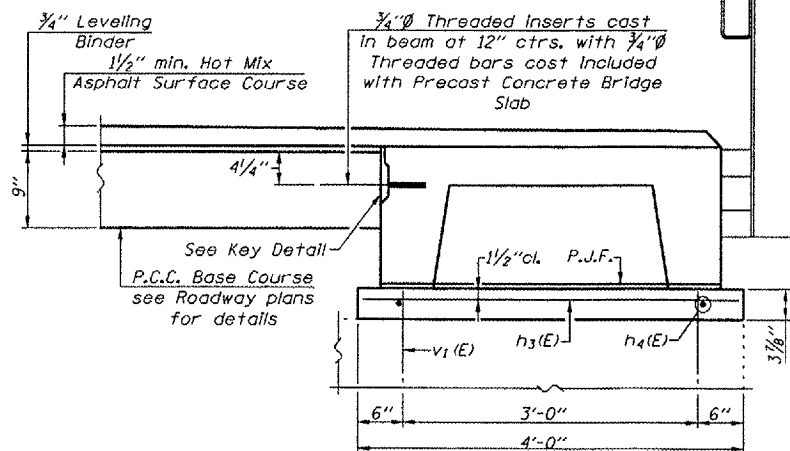
TYPICAL SECTION AT SOUTH ABUTMENT

*** 8-#4v1(E) bars at 12" cts. Drill & grout bars in accordance with Article 584 of the Standard Specifications.

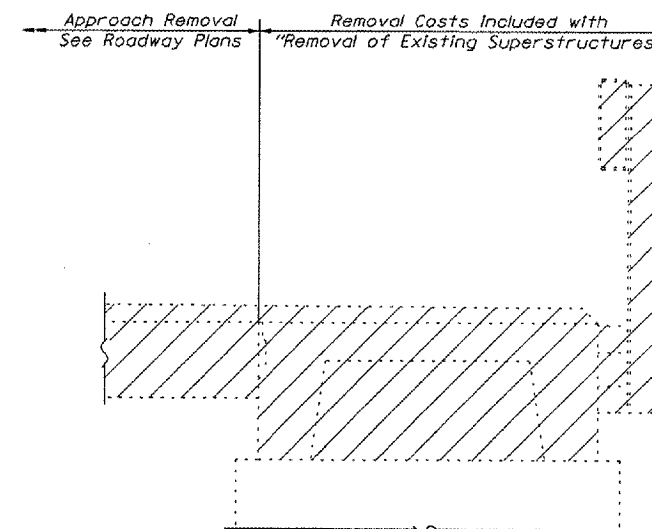


TYPICAL SECTION AT NORTH ABUTMENT

Note: Space reinforcement to miss dowel rods.



TYPICAL APPROACH SECTION AT SOUTH ABUT.



TYPICAL APPROACH REMOVAL SECTION

BILL OF MATERIAL

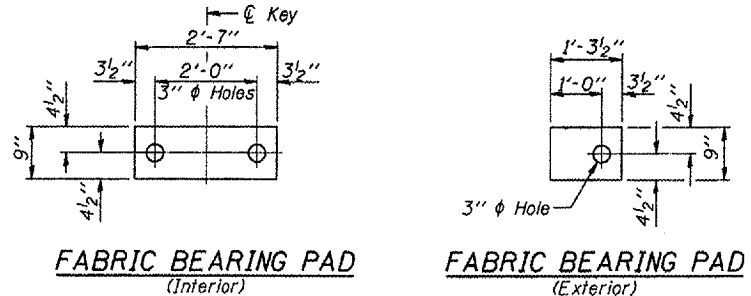
BAR	NO.	SIZE	LENGTH	SHAPE
h3(E)	4	#4	3'-8"	—
h4(E)	8	#4	0'-6"	—
v1(E)	16	#4	0'-11"	—
Concrete Structures			Cu. Yd.	0.1
Precast Concrete Bridge Slab			Sq. Ft.	299
Reinforcement Bars, Epoxy Coated			Pound	30

APPROACH DETAILS
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

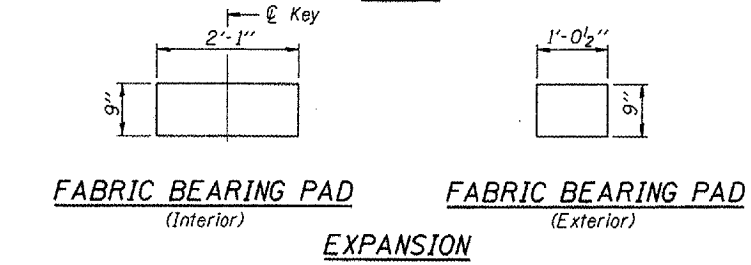
FILE NAME: STRUCTURE PLANS (REV. 12/7/06)

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B1)	WOODFORD	38	14
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
Contract No. 68635				

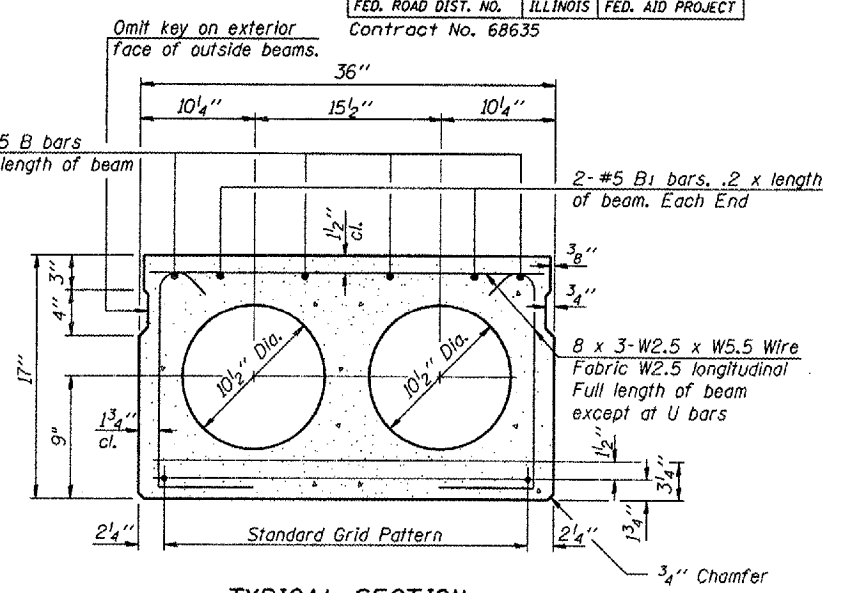
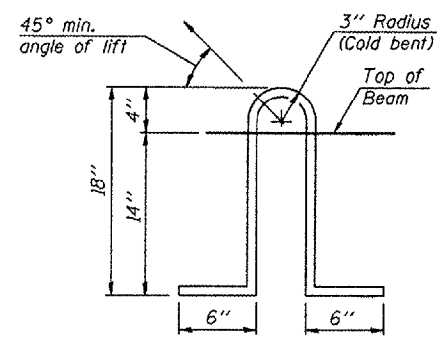
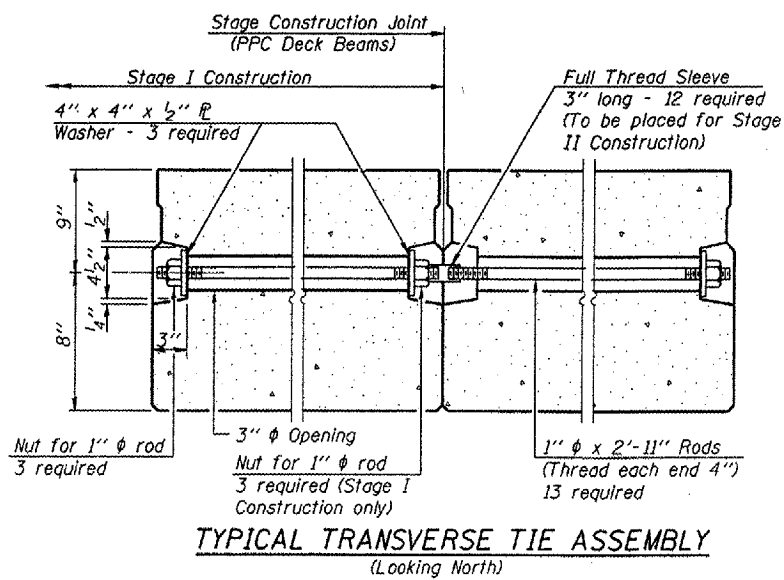
SHEET NO. 5
OF 14 SHEETS



FIXED



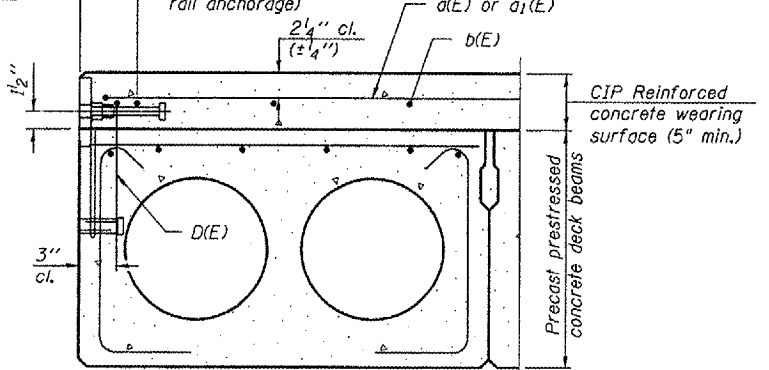
EXPANSION



TYPICAL SECTION

1/2" φ Strands, Each Strand Stressed to 30,900 Lbs.
9-Strands 1 3/4" up, 4-Strands 3/4" up, 2-strands 12" up

Note: Place strands symmetrically about centerline of beam.



NOTES

The rail anchorage shall be cast with the beam and the wearing surface shall be cast in the field. Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and or additional inserts cast into the beam. Drilling into the beam will not be permitted.

Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in. Lifting loops shall be 2-1/2" φ-270 ksi strands, as shown. Non prestressing steel shall conform to ASTM A 706 Grade 60. (IL Modified). See Special Provisions.

The 1" φ rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside shall be filled with grout after transverse assembly is in place.

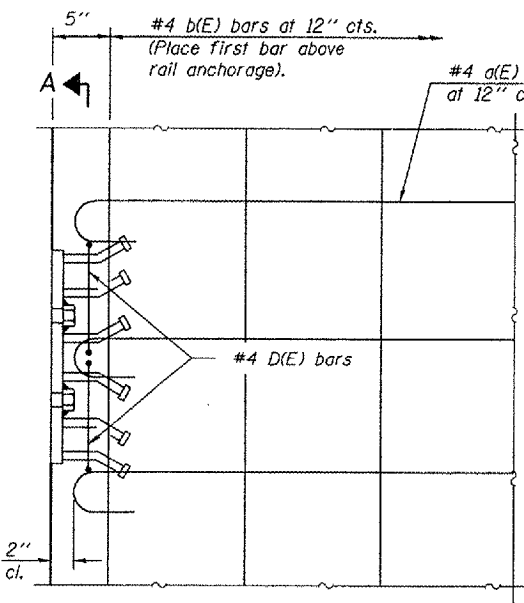
The bearing seat surfaces shall be adjusted by shimming to assure firm and even bearing. Two 1/2" fabric adjusting shims of the dimensions of the Exterior Bearing Pad shall be provided for each bearing.

Keyway surfaces shall be cleaned to remove form oil or other bond breaking material prior to shipment of the beams. Cleaning shall be done by sandblasting the keyway areas between top of the beam and the bottom edge of the key.

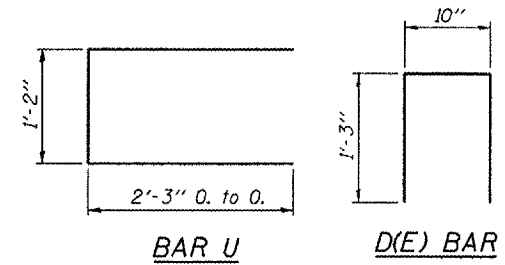
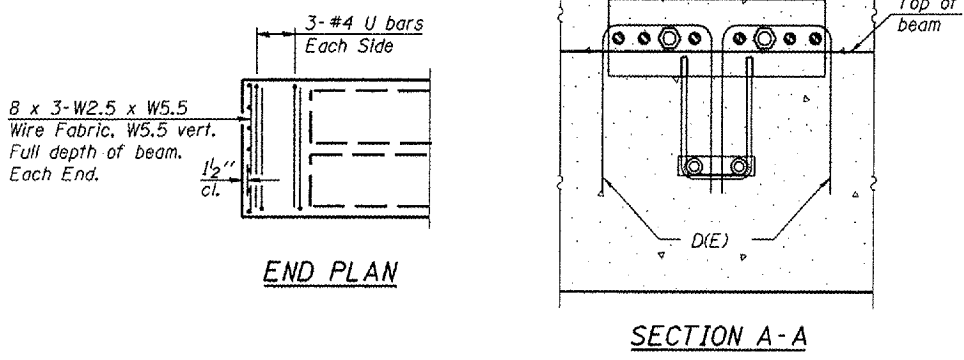
Corrosion Inhibitor, as covered in the Special Provisions, shall be used in the concrete for precast prestressed concrete deck beams.

Required Release Strength, f'ci, shall be 4000 p.s.i. See Sheet 7 of 14 for rail anchorage locations.

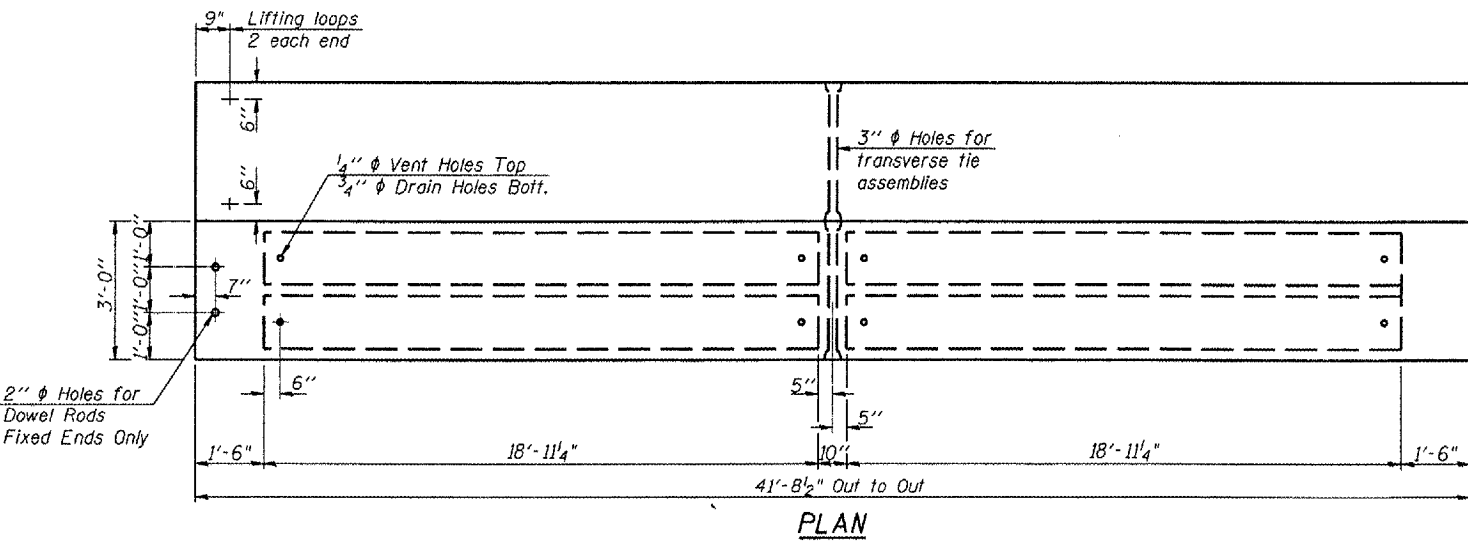
DECK BEAM DETAILS
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B1)
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018



PARTIAL DECK PLAN AT RAIL ANCHORAGE



BAR U **D(E) BAR**



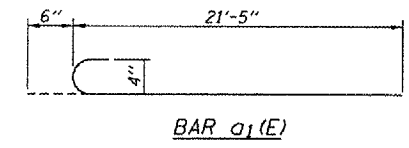
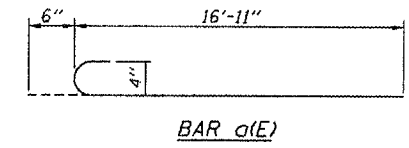
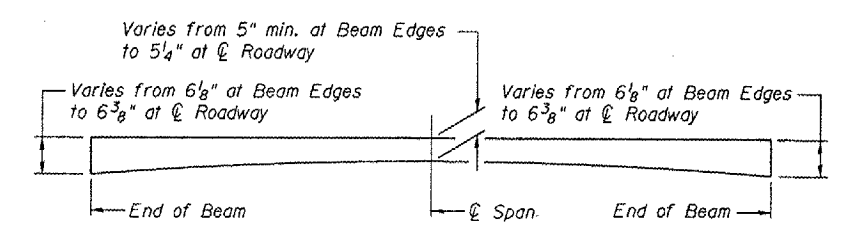
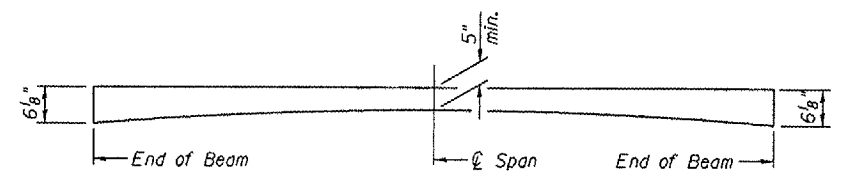
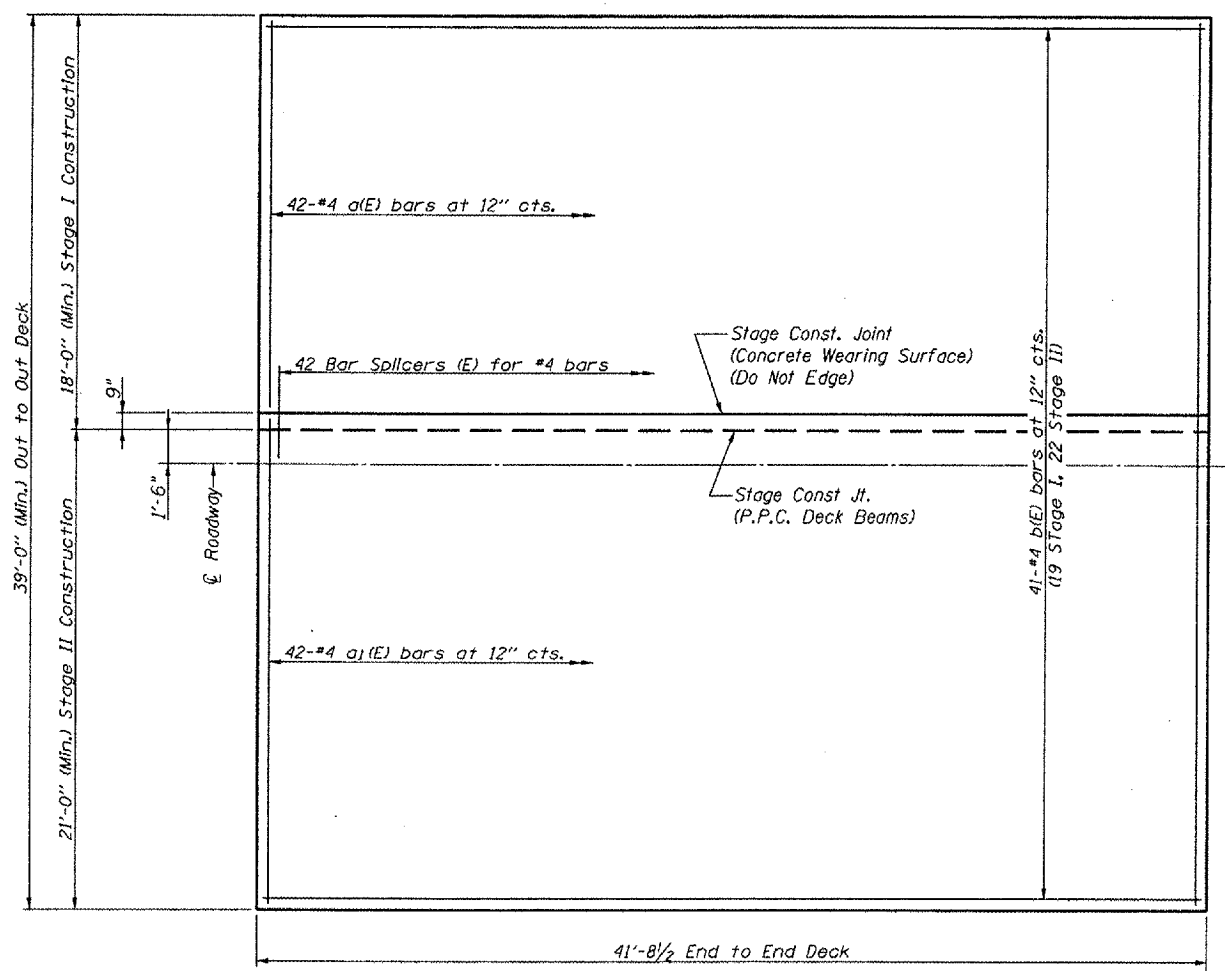
BILL OF MATERIAL

Item	Unit	Quantity
Precast Prestressed Concrete Deck Beams 17"	Sq. Ft.	1627

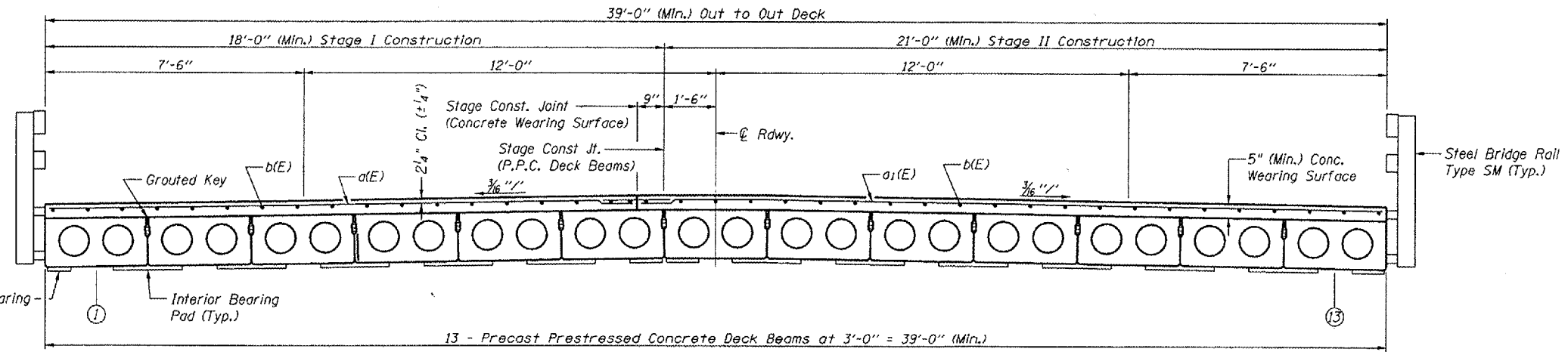
FILE NAME: STRUCTURE PLANS (REV. 12/18/05)

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)I	WOODFORD	38	15
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
Contract No. 68635				

SHEET NO. 6
OF 14 SHEETS



Note: For remainder of Superstructure Details see Sheet 7 of 14



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	42	#4	17'-5"	C
a1(E)	42	#4	21'-11"	C
b(E)	41	#4	41'-4"	—
Reinforcement Bars, Epoxy Coated			Pound	2240
Bar Splicers			Each	42

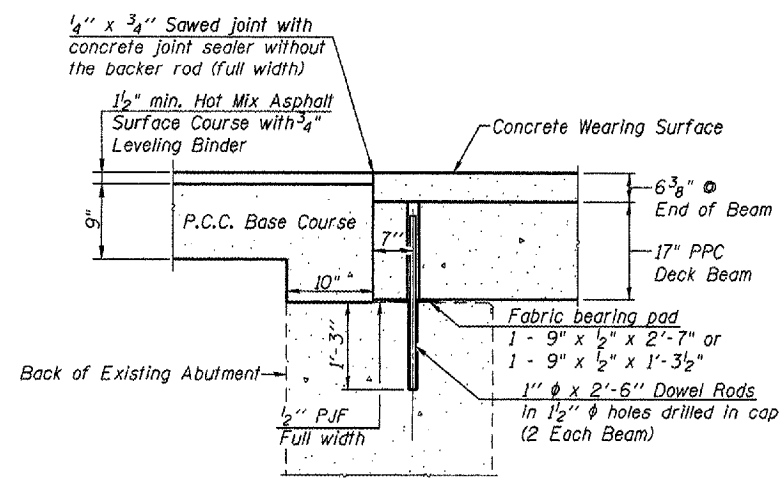
Reinforcement bars designated (E) shall be Epoxy Coated.

SUPERSTRUCTURE
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)I
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

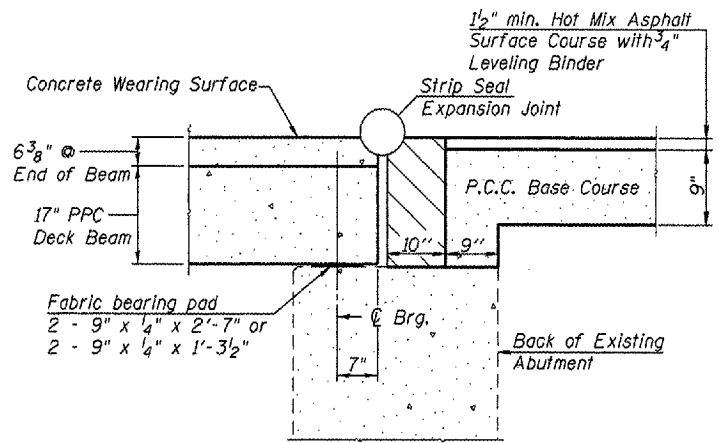
FILE NAME: STRUCTURE PLANSREV. 12/1/2001

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	113B1	WOODFORD	38	16
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
Contract No. 68635				

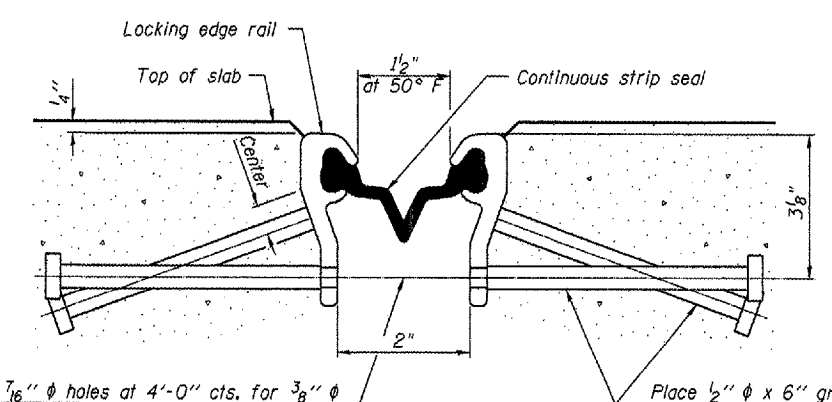
SHEET NO. 7
OF 14 SHEETS



SECTION THRU SOUTH ABUTMENT
At @ Roadway

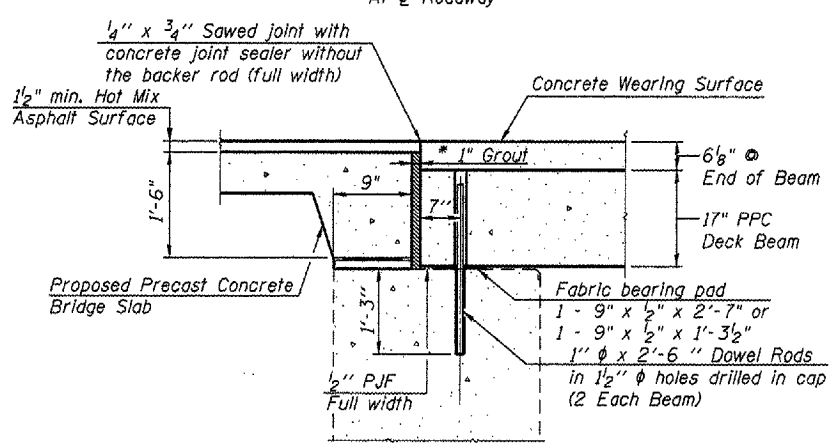


SECTION THRU NORTH ABUTMENT
At @ Roadway

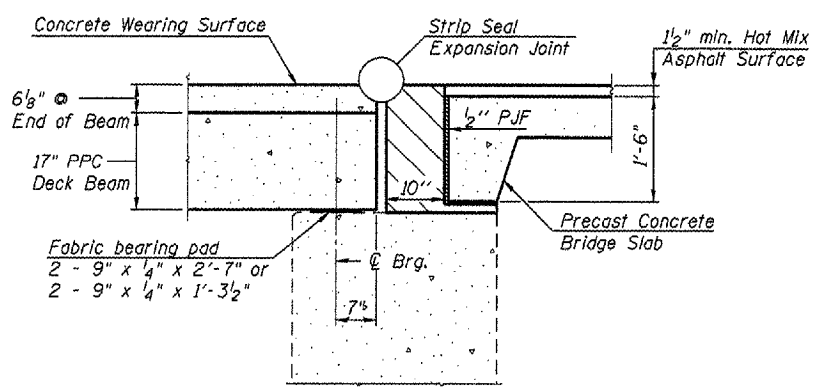


SECTION THRU STRIP SEAL JOINT FOR OVERLAY OVER DECK BEAMS
(17 Studs Stage I, 22 Studs Stage II)

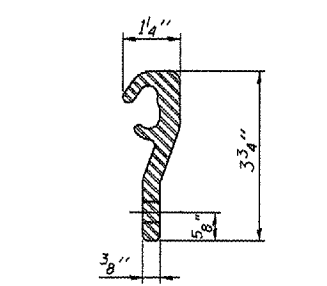
Place 1/2" ϕ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.



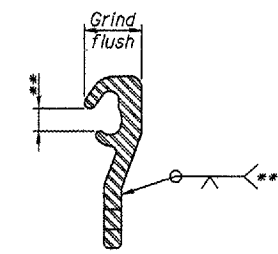
SECTION THRU SOUTH ABUTMENT
At Outside Beam



SECTION THRU NORTH ABUTMENT
At Outside Beam



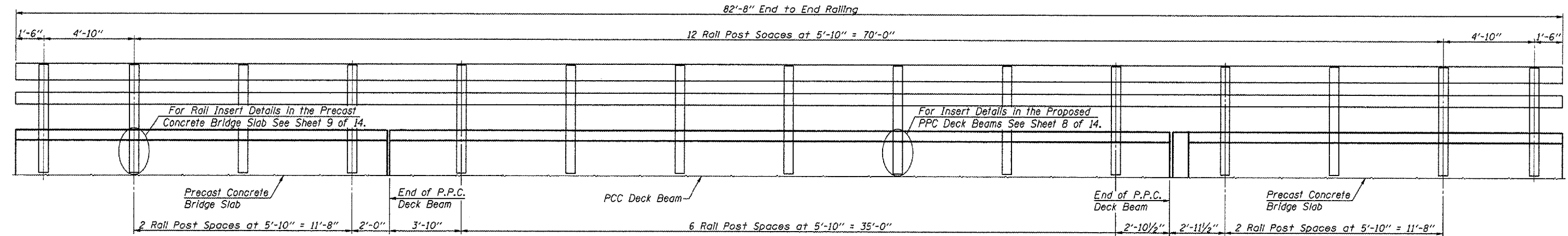
LOCKING EDGE RAIL



LOCKING EDGE RAIL SPLICE

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.
Strip seal to extend 12" beyond edge of deck (both ends).
The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.
The inside of the Locking Edge Rail groove shall be free of weld residue.
Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.
The manufacturer's recommended installation methods shall be followed.
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
After fabrication, the steel locking edge rail assembly shall be hot dip galvanized according to AASHTO M111 and ASTM A123.

Notes:
After beams have been erected, holes shall be drilled into substructure and anchor dowels placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure min. 24 hrs. prior to grouting the shear keys.
Hatched area to be poured after concrete wearing surface is in place.
See sheet 5 of 14 for bearing pad details.
* 1" Jt. shall be filled with non-shrink grout. 1" dimension may vary to accommodate tolerance in beam lengths.



RAIL POST SPACING FOR STEEL BRIDGE RAIL

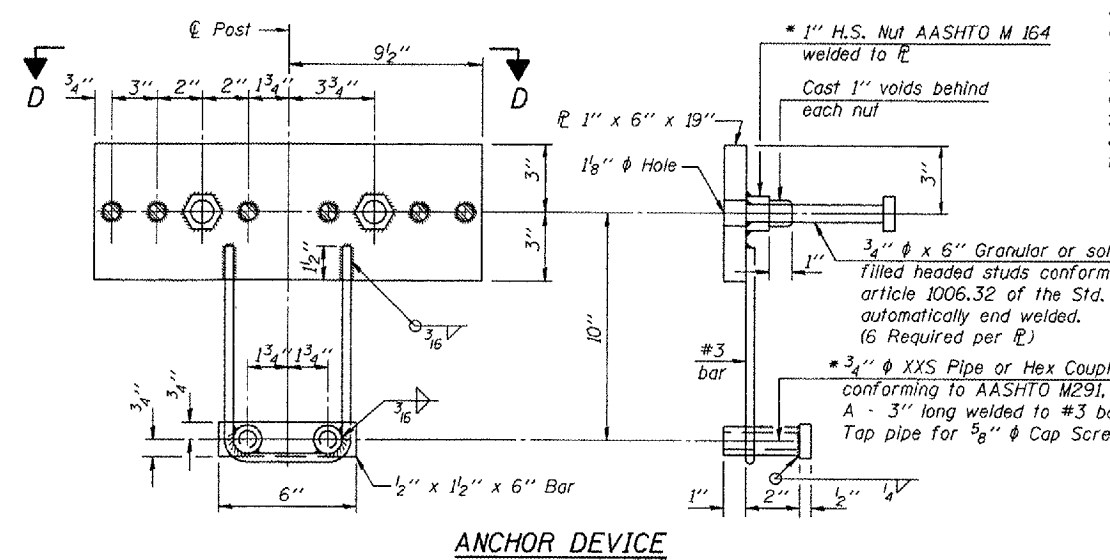
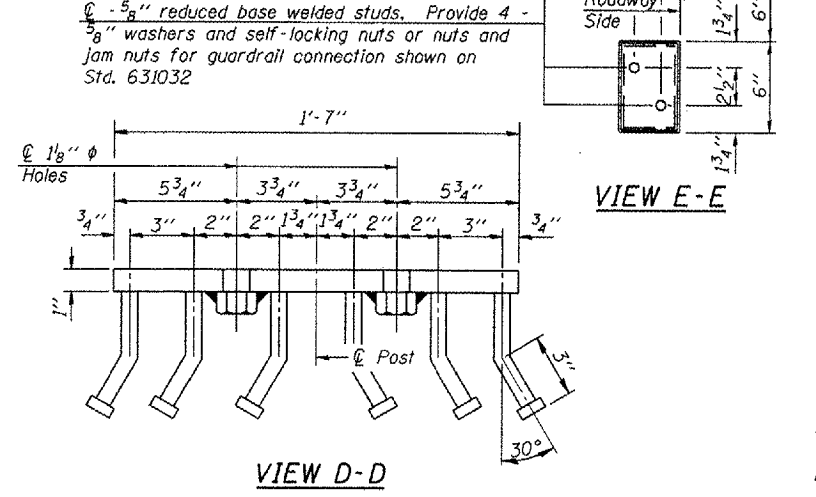
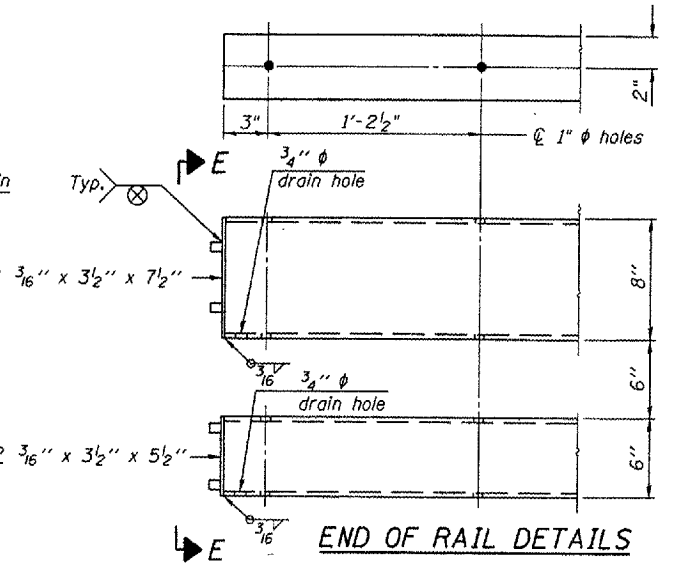
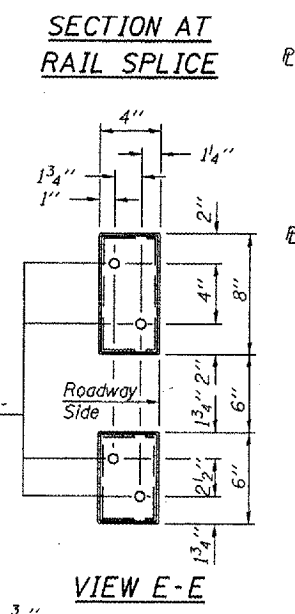
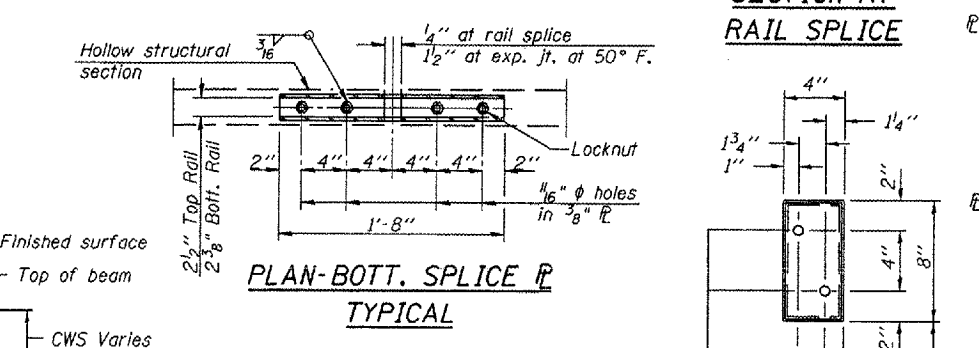
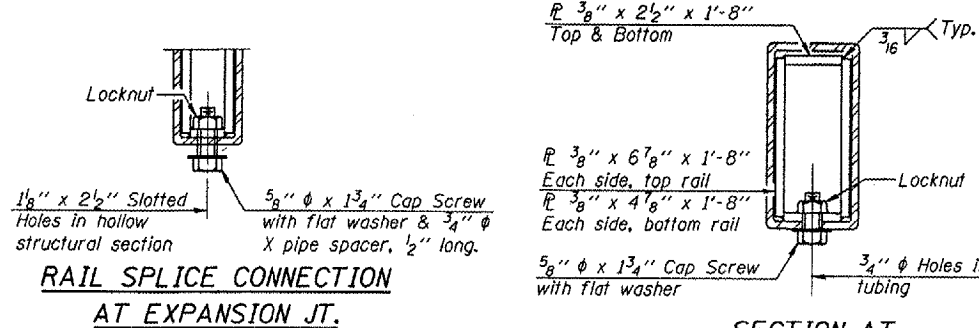
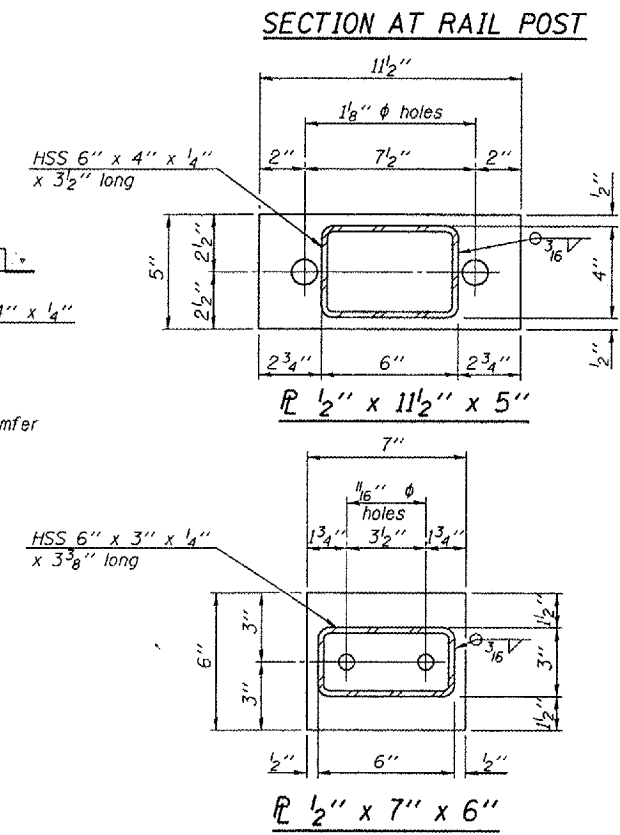
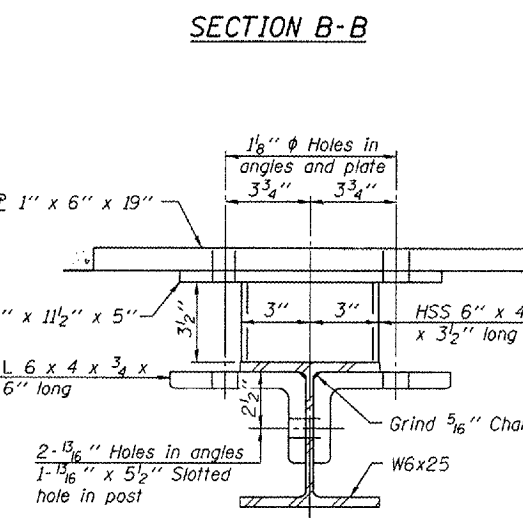
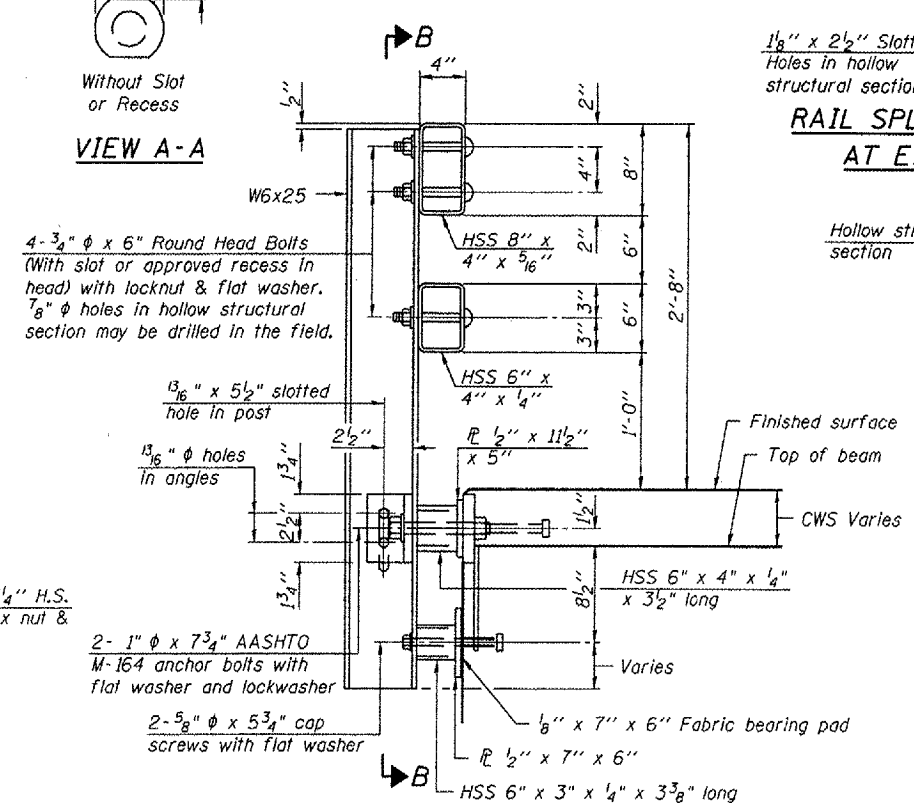
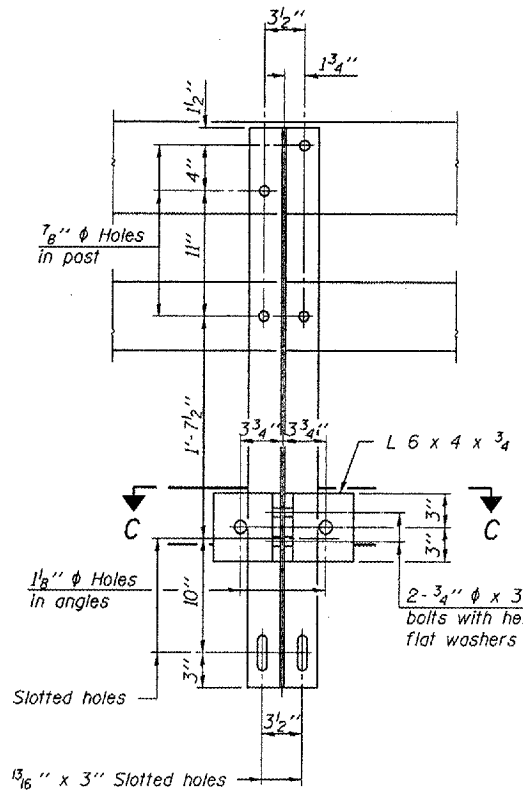
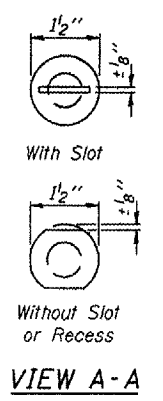
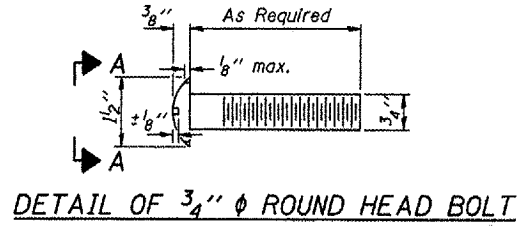
Note: The rail post spaces shown on the existing precast concrete approach beams are per the existing plans. Actual spacing may vary in the field. It shall be the Contractor's responsibility to verify such dimensions and make necessary approved adjustments to the rail post spacing of the proposed ppc deck beams prior to construction or ordering of materials. Maximum rail post spacing = 6'-3"

SUPERSTRUCTURE DETAILS
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B1)
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

FILE NAME: STRUCTURE PLANSREV. 12/4/09

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)I	WOODFORD	38	17
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

Contract No. 68635



NOTES

Hollow structural sections shall conform to the requirements of ASTM designation A 500 Grade B Structural Steel Tubing and shall meet the longitudinal CVN requirements of 15 ft-lbs at 0° F.

All other steel shapes and plates shall conform to the requirements of AASHTO M 270 Grade 36 except posts and angles shall conform to AASHTO M 270, Grade 50.

Bolts, cap screws, and nuts shall conform to the requirements of ASTM designation A 307 except for high strength bolts, nuts and washers noted which shall conform to AASHTO M 164.

All bolts, nuts, cap screws, washers and lock washers shall be galvanized according to AASHTO M 232.

All posts, railing, rail splices, anchor devices and angles shall be galvanized after shop fabrication according to AASHTO M 111 and ASTM A 385. Galvanized rail shall not be painted.

Railing shall be according to Section 509 of the Standard Specifications, except as noted, and will be paid for at the contract unit price per foot for Steel Bridge Rail, Type SM.

All field drilled holes shall be coated with an approved zinc rich paint before erection.

For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Bridge Rail, Type SM.

The 3/4" high strength bolts used to connect the 6 x 4 x 3/4 angles to the post shall be tightened according to Article 505.04(f)(2) of the Standard Specifications. The 1" high strength bolts connecting the angles to the concrete shall be tightened to a snug fit and given an additional 1/8 turn. The 5/8" cap screws in bottom of posts shall be tightened to a snug fit only.

Work with Sheet with Sheet 9 of 14.

BILL OF MATERIAL

Item	Unit	Quantity
Steel Bridge Rail, Type SM	Foot	165

TYPE SM
STEEL BRIDGE RAIL SIDE MOUNTED WITH CONCRETE WEARING SURFACE
IL ROUTE 117 OVER WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)I
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

* Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.

FILE NAME: STRUCTURE PLANSREV. 12-14-08

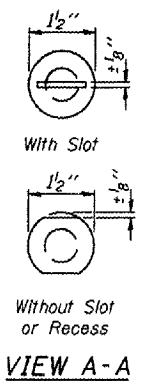
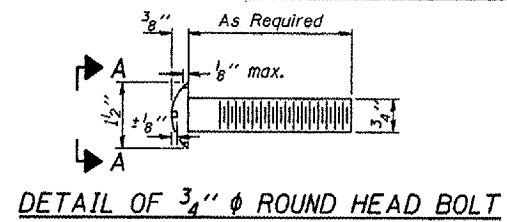
R-34CWS

10-28-05 (6'-3" Maximum Post Spacing) (5" minimum to 7/8" maximum CWS thickness)

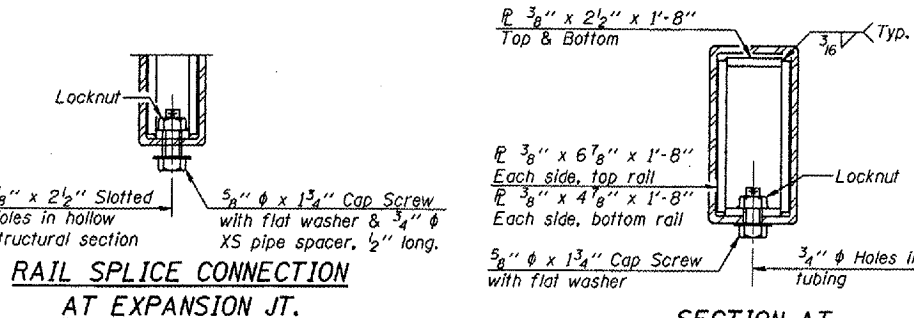
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)1	WOODFORD	38	18
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

Contract No. 68635

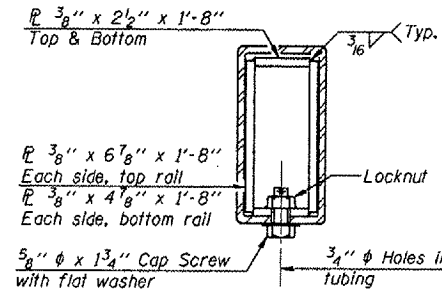
SHEET NO. 9
OF 14 SHEETS



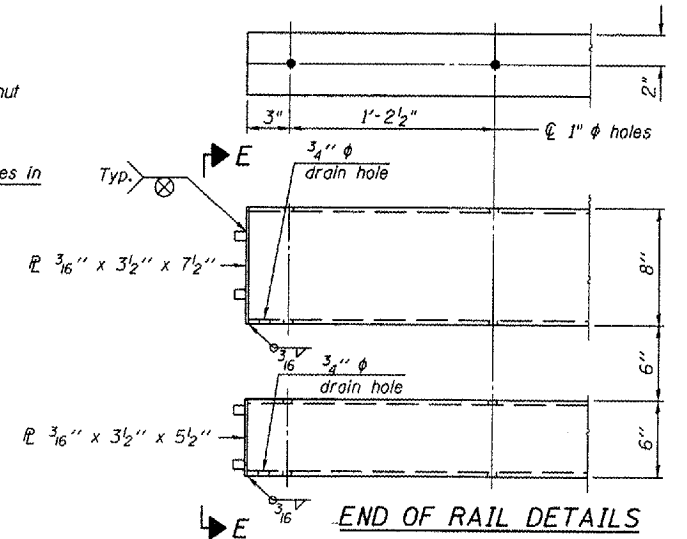
4- 3/4" ϕ x 6" Round Head Bolts
(With slot or approved recess in head) with locknut & flat washer.
7/8" ϕ holes in hollow structural section may be drilled in the field.



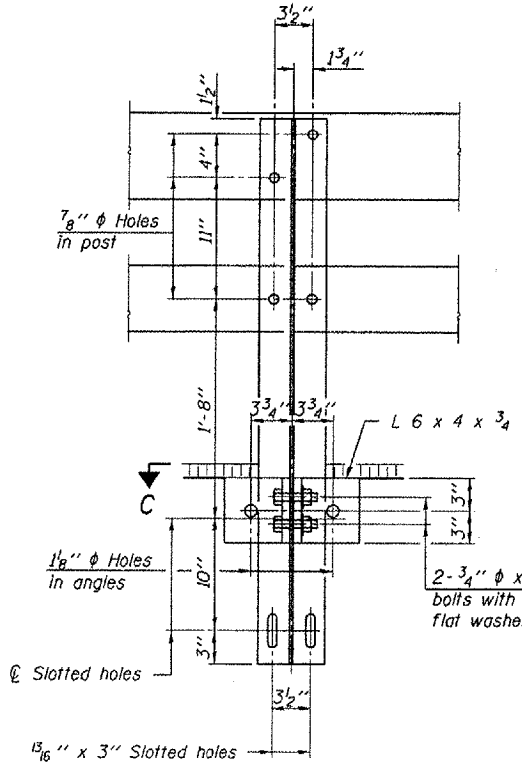
RAIL SPLICE CONNECTION AT EXPANSION JT.



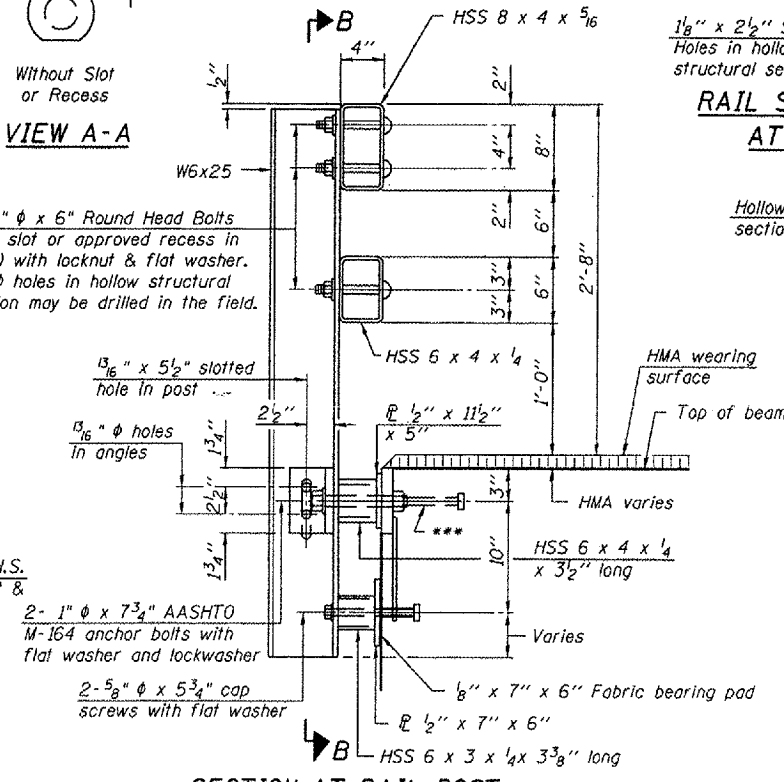
SECTION AT RAIL SPLICE



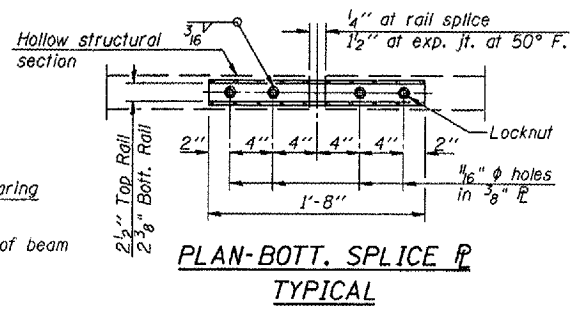
END OF RAIL DETAILS



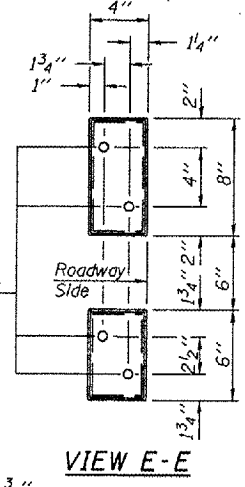
SECTION B-B



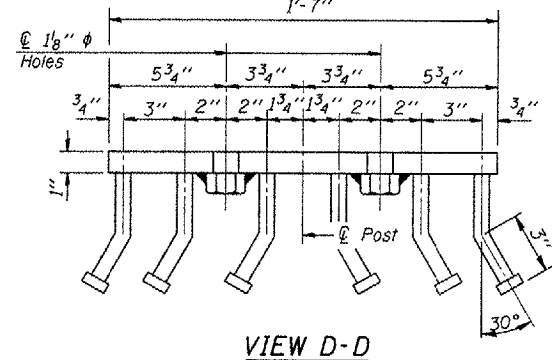
SECTION AT RAIL POST



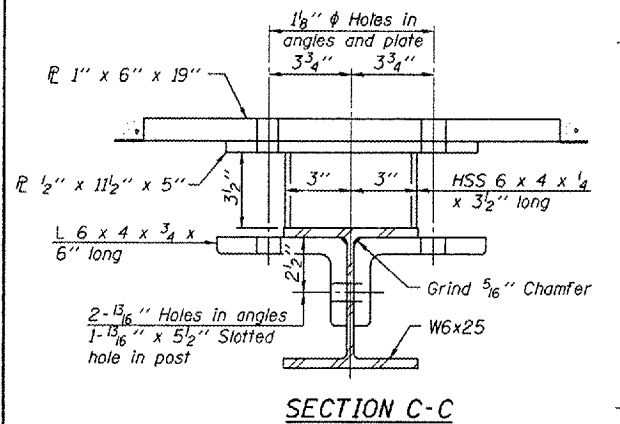
PLAN-BOTT. SPLICE P TYPICAL



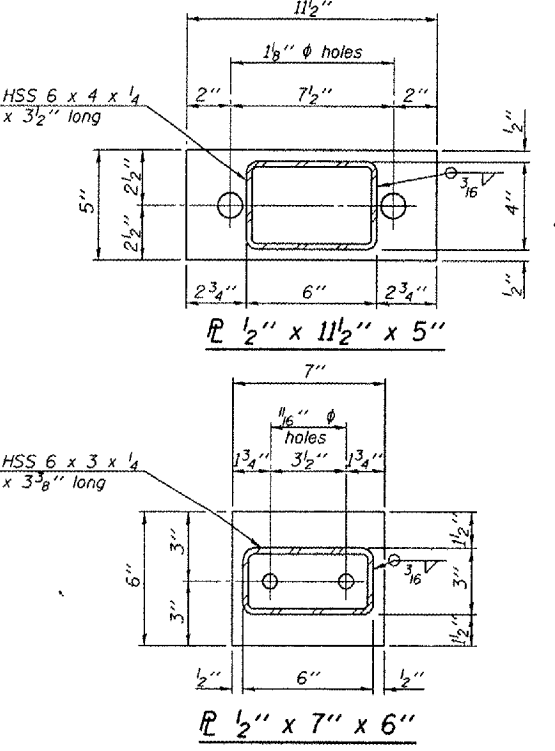
VIEW E-E



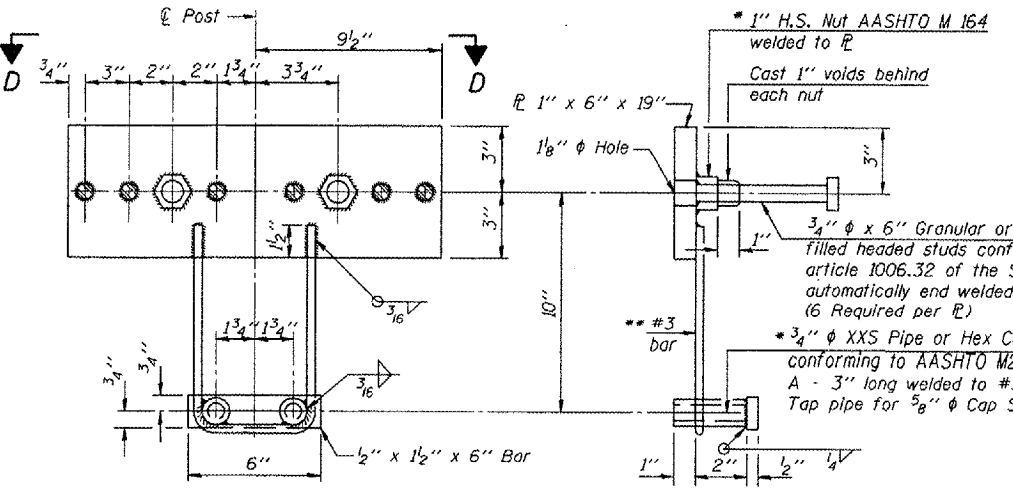
VIEW D-D



SECTION C-C



ANCHOR DEVICE



Notes:
All field drilled holes shall be coated with an approved zinc paint before erection.
For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Bridge Rail, Type SM.
All steel rail members shall be galvanized according to Article 509.05 of the Standard Specifications.
The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

* Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.

* 1" H.S. Nut AASHTO M 164 welded to R
Cast 1" voids behind each nut
3/4" ϕ x 6" Granular or solid flux filled headed studs conforming to article 1006.32 of the Std. Specs. automatically end welded. (6 Required per R)
* 3/4" ϕ XXS Pipe or Hex Coupler Nuts conforming to AASHTO M291, Grade A - 3" long welded to #3 bar and Tap pipe for 5/8" ϕ Cap Screw.

** Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".

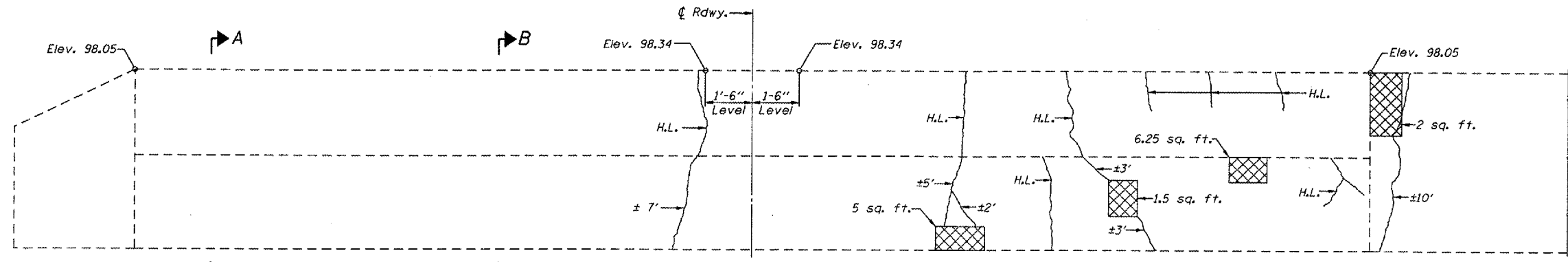
APPROACH RAILING DETAILS
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)1
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

FILE NAME: STRUCTURE PLAN/REV. 12/1/06

R-34HMAWS 11-1-06 (6'-3" Maximum Post Spacing) (1/4" minimum to 3/8" maximum HMA thickness)

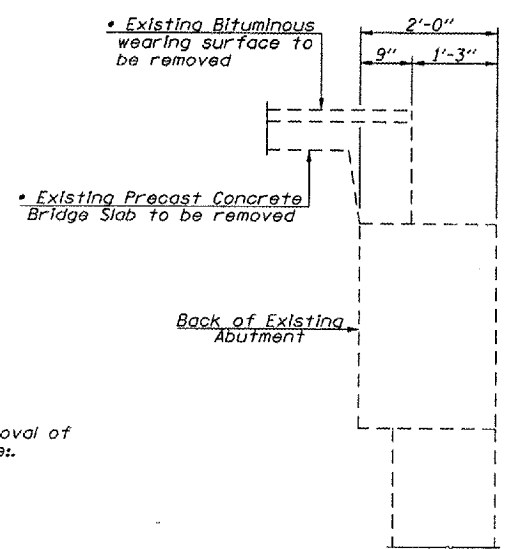
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 10 OF 14 SHEETS
F.A.P. 702	(113B)I	WOODFORD	38	19	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT			

Contract No. 68635

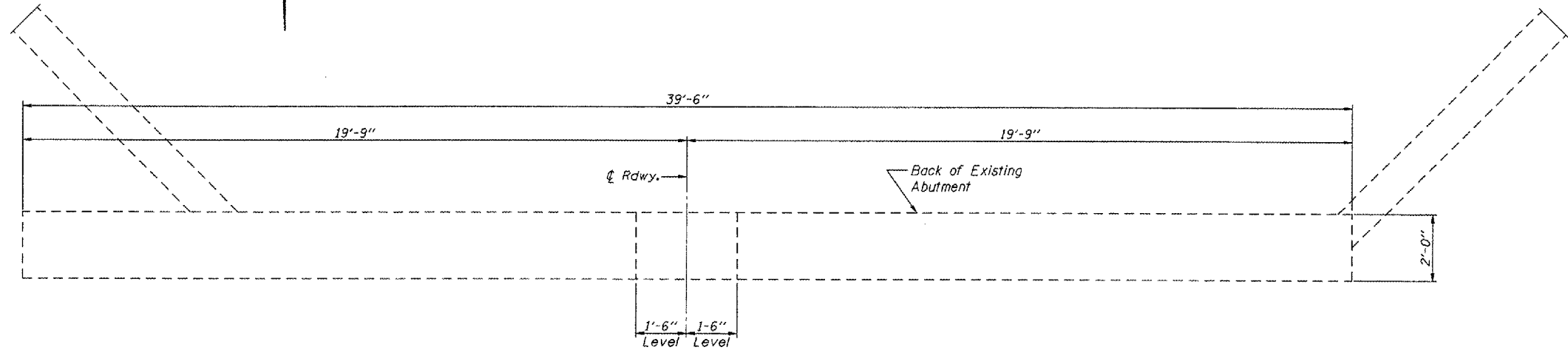


ELEVATION
(Looking South)

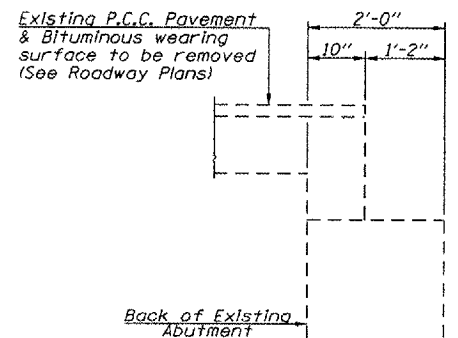
• Cost included with "Removal of Existing Superstructure."



SECTION A-A



PLAN



SECTION B-B

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Epoxy Crack Injection	Foot	30
Structural Repair of Concrete (Depth Equal To Or Less Than 5')	Sq. Ft.	15

- LEGEND**
- Structural Repair of Concrete (Depth equal to or less than 5')
 - ± Epoxy Crack Injection (Crack widths shown are approx. 1/16" to 1/8" in width)
 - H.L. Hairline Crack (Not to be sealed)

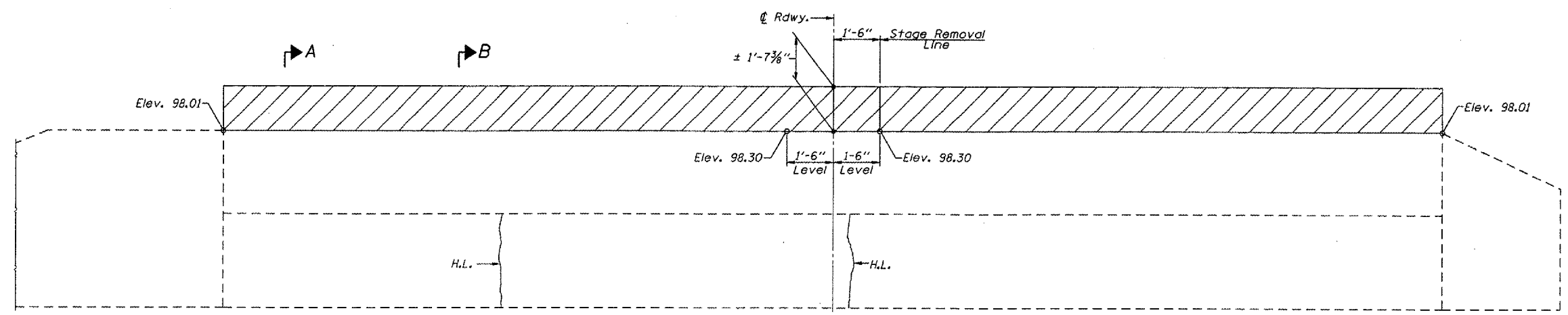
**SOUTH ABUTMENT
REMOVAL & REPAIR
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)I
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018**

FILE NAME: STRUCTURE PLANSREV. 12/25/08

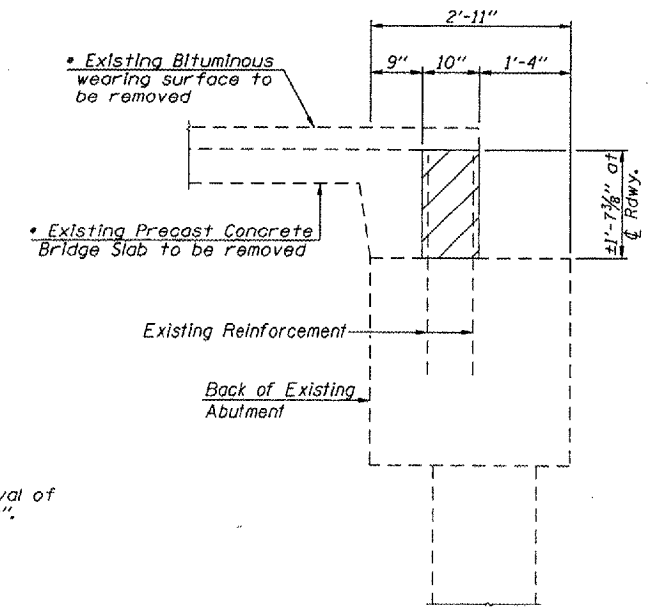
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)	WOODFORD	30	20
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

Contract No. 68635

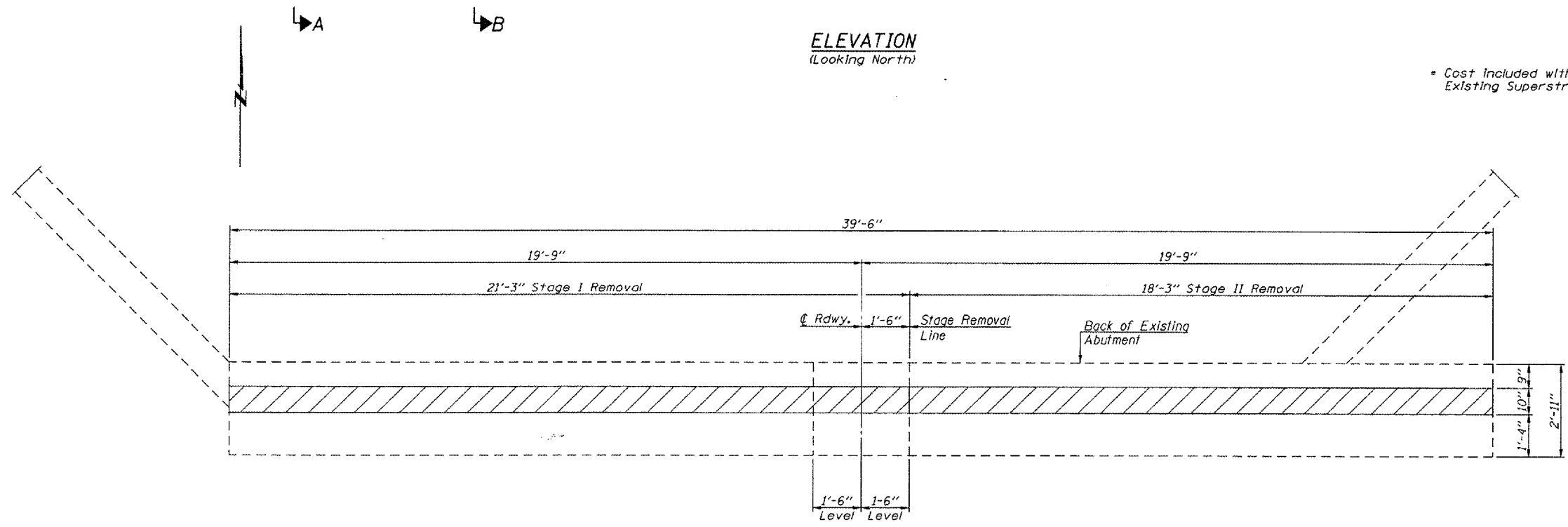
SHEET NO. 11
OF 14 SHEETS



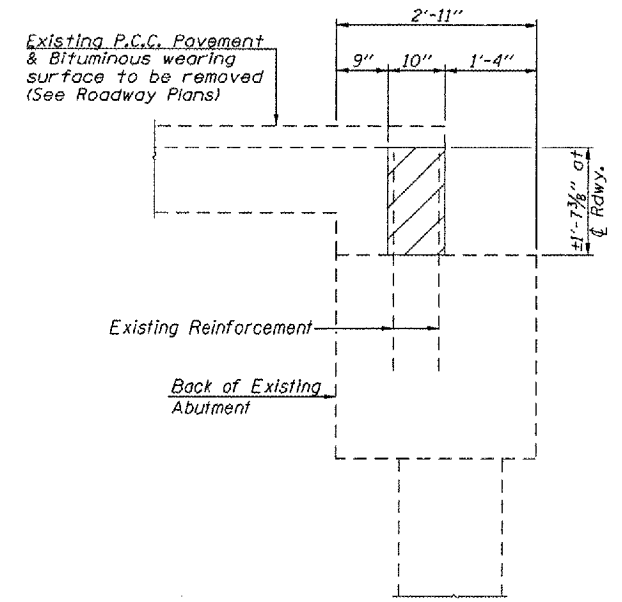
ELEVATION
(Looking North)



SECTION A-A



PLAN



SECTION B-B

- LEGEND**
- Concrete Removal
 - Hairline Crack (Not to be sealed)

Notes: Existing vertical reinforcing steel to be incorporated into new work shall be cleaned, straightened, cut (if required), and incorporated into new construction. Damaged reinforcing steel shall be replaced or removed as directed by the Engineer. Cost included with "Concrete Removal".
Removal of the existing joint system is included in the cost of "Concrete Removal".

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	2.0

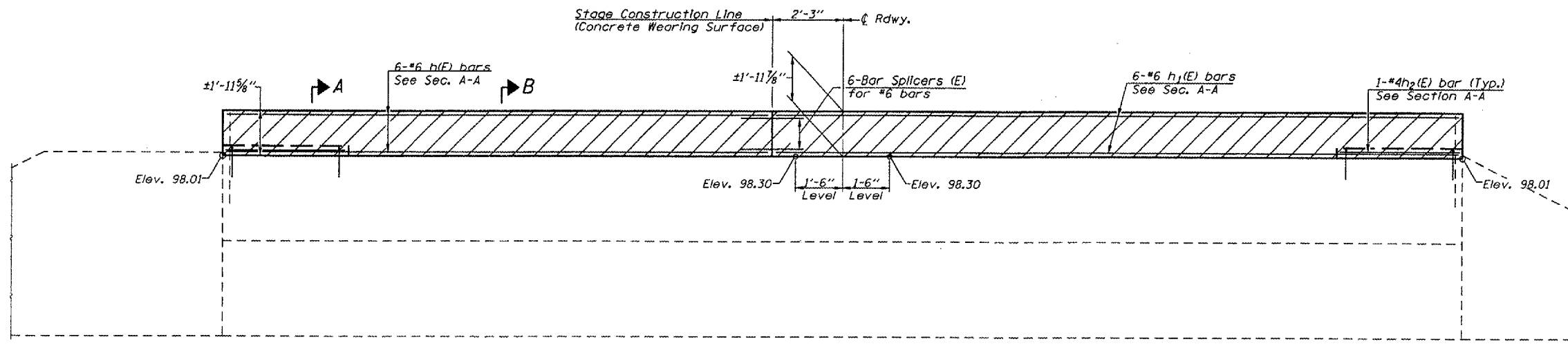
**NORTH ABUTMENT
REMOVAL & REPAIR
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018**

FILE NAME: STRUCTURE PLANS/REV. 12/16/08

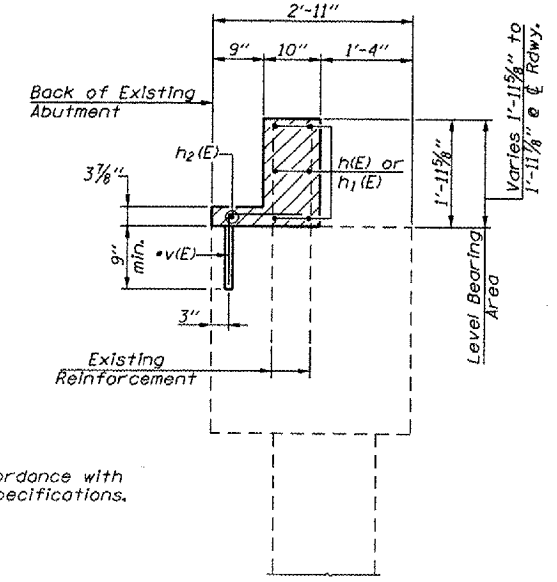
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)1	WOODFORD	38	21
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SHEET NO. 12 OF 14 SHEETS

Contract No. 68635

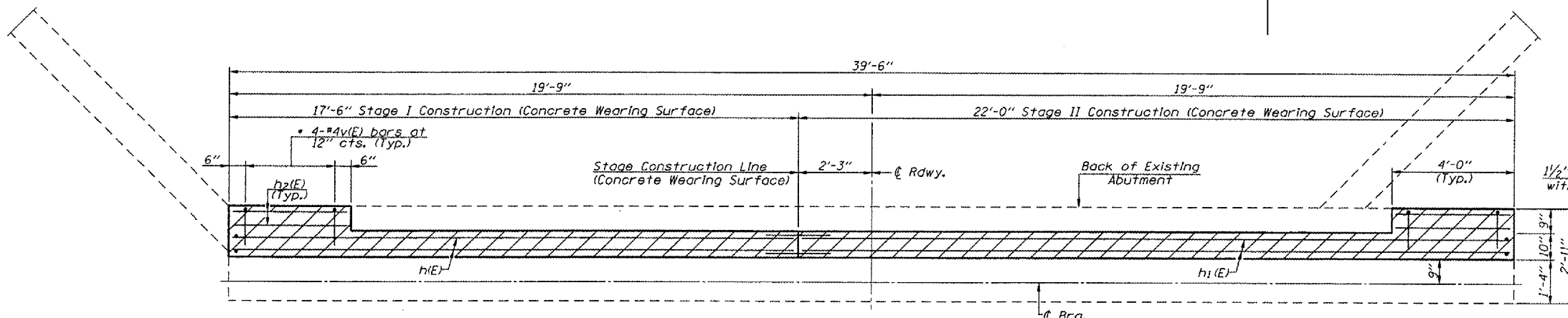


ELEVATION
(Looking North)

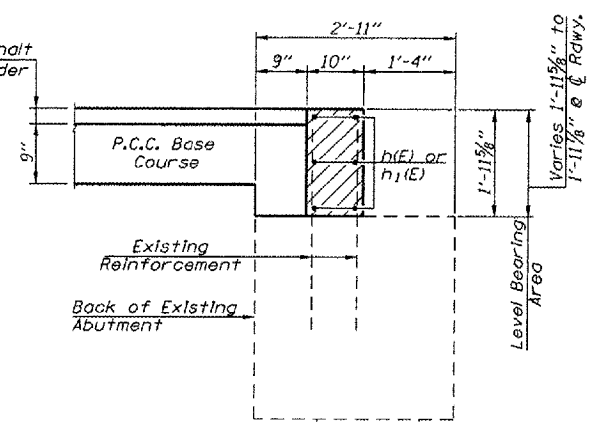


SECTION A-A

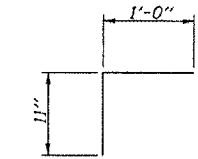
• Drill and grout v(E) bars in accordance with Article 584 of the Standard Specifications.



PLAN



SECTION B-B



BAR v(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1(E)	6	#6	17'-2"	—
h2(E)	2	#4	3'-8"	—
v(E)	8	#4	1'-11"	—
Reinforcement Bars, Epoxy Coated			Pound	370
Bar Splicers			Each	6
Concrete Substructure			Cu. Yd.	2.5

Reinforcement bars designated (E) shall be Epoxy Coated.

Notes: Hatched area to be poured after Concrete Wearing Surface is in place. Space reinforcement bars to miss dowel rods.

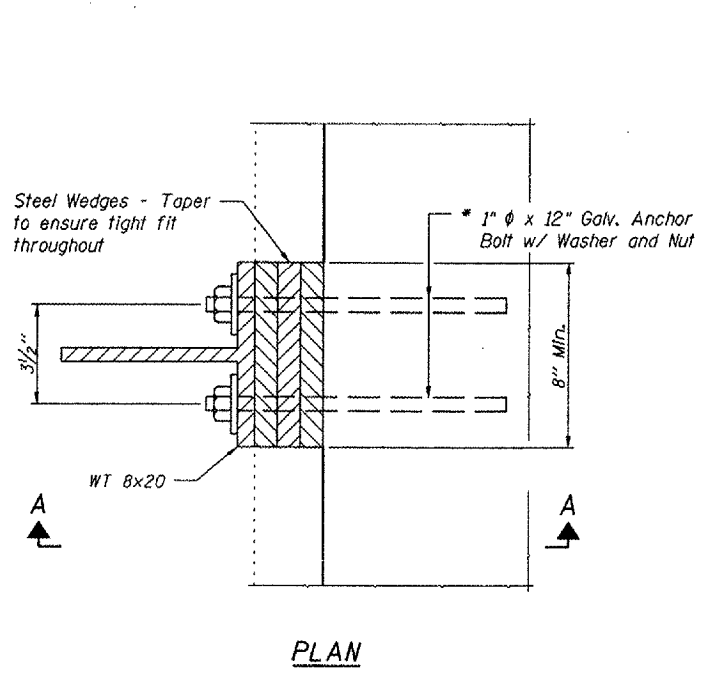
NORTH ABUTMENT
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)1
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

FILE NAME: STRUCTURE PLANS REV. 12/8/05

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 702	(113B)	WOODFORD	38	22
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

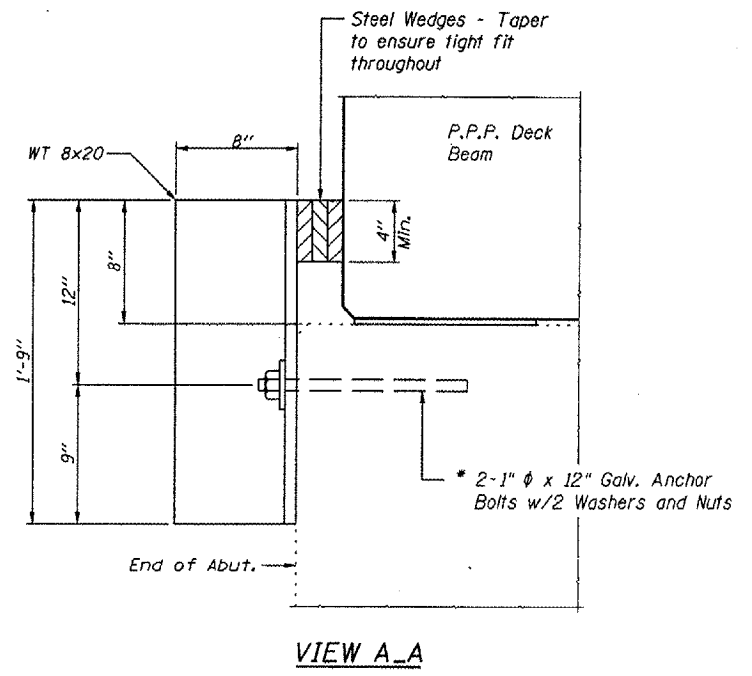
SHEET NO. 13
OF 14 SHEETS

Contract No. 68635



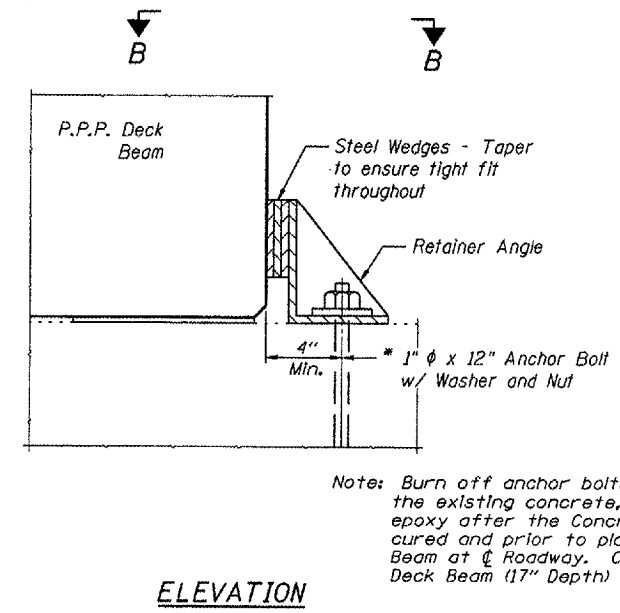
PLAN

PERMANENT SIDE RETAINER AT NORTH ABUTMENT



VIEW A-A

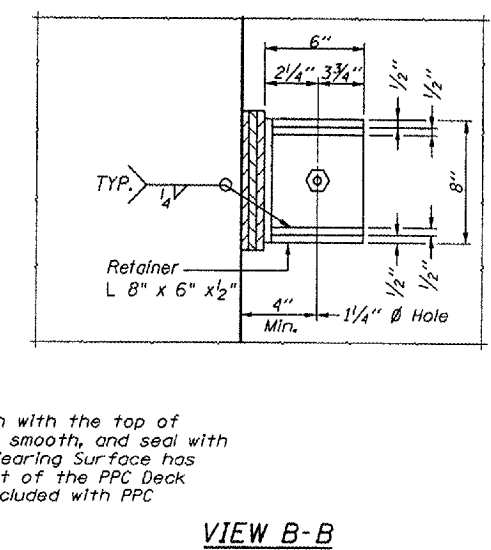
* Epoxy grout 1" anchor bolts in 9" (min.) drilled holes according to Section 584 of the Standard Specification. Cost of retainer and accessories are included with PPC Deck Beams (17" Depth)



ELEVATION

TEMPORARY SIDE RETAINER AT NORTH ABUTMENT

Note: Burn off anchor bolts flush with the top of the existing concrete, grind smooth, and seal with epoxy after the Concrete Wearing Surface has cured and prior to placement of the PPC Deck Beam at Roadway. Cost Included with PPC Deck Beam (17" Depth)



VIEW B-B

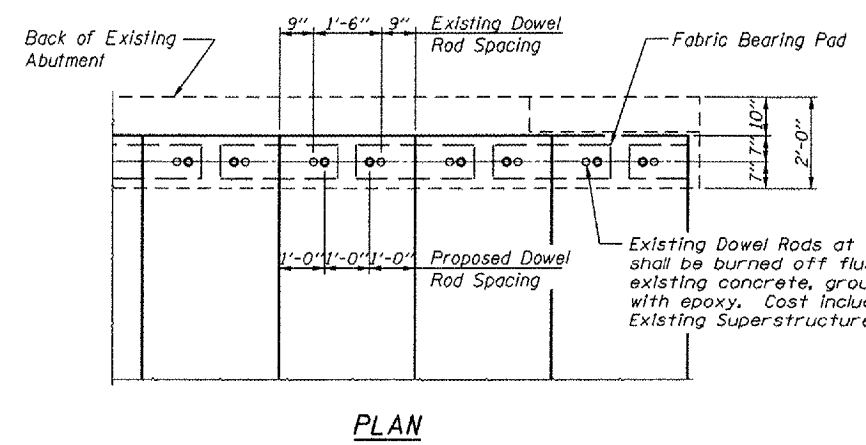
ANCHOR BOLTS FOR RETAINERS
GENERAL NOTES

Holes in the masonry for anchor bolts shall be drilled through the base plates to the diameter and depth shown or according to the manufacturer's recommendation after beams or girders have been erected and adjusted. Prior to setting the bolts, the holes shall be dry and all dust and loose particles shall be removed by the use of compressed air or vacuuming. The anchor bolts, furnished and installed and including the epoxy grout or capsules shall not be paid for separately but shall be included in the unit bid price for PPC Deck Beams (17" Depth). The Contractor may use, at his option, the capsule or the adhesive cartridge type anchor rods that have been previously tested and given a prior approval by the Department. The Contractor shall install these anchor rods in pre-drilled holes according to the manufacturer's recommendations and procedures. The capsule or the adhesive cartridge type anchor rods shall be a two part system composed of:

1. A threaded rod stud with nut and washer of the type specified.
2. A sealed glass capsule or a sealed glass adhesive cartridge containing premeasured amounts of the adhesive chemical.

Location	Type
N. Abut.	A325

ASTM F 1554 Grade 105, ASTM A 449 and AASHTO M 314 Grade 105 anchor bolts may be substituted for the anchor bolts shown above.



PLAN

DOWEL ROD SPACING (SOUTH ABUTMENT)

Existing Dowel Rods at the South Abutment shall be burned off flush with the top of the existing concrete, ground smooth, and sealed with epoxy. Cost included with Removal of Existing Superstructures.

SUBSTRUCTURE DETAILS
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

FILE NAME: STRUCTURE PLANS/REV. 12/16/06J

Contract No. 68635

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

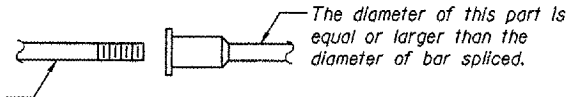
- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_t$
- ② Minimum *Pull-out Strength (Tension in kips) = $1.25 \times f_{s_{allow}} \times A_t$

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 $f_{s_{allow}}$ = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A_t = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

The diameter of this part is the same as the diameter of the bar spliced.

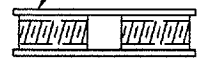


ROLLED THREAD DOWEL BAR



** ONE PIECE

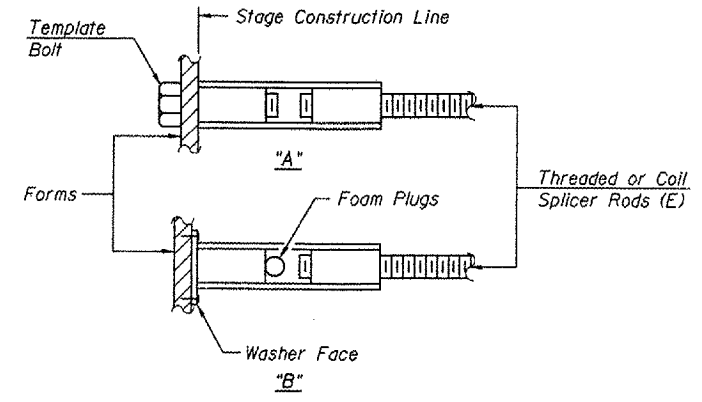
Wire Connector



WELDED SECTIONS

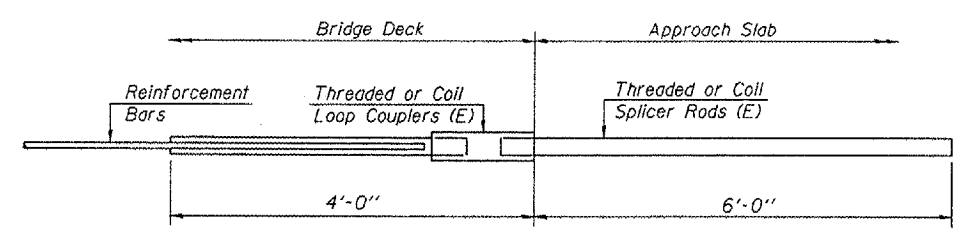
BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



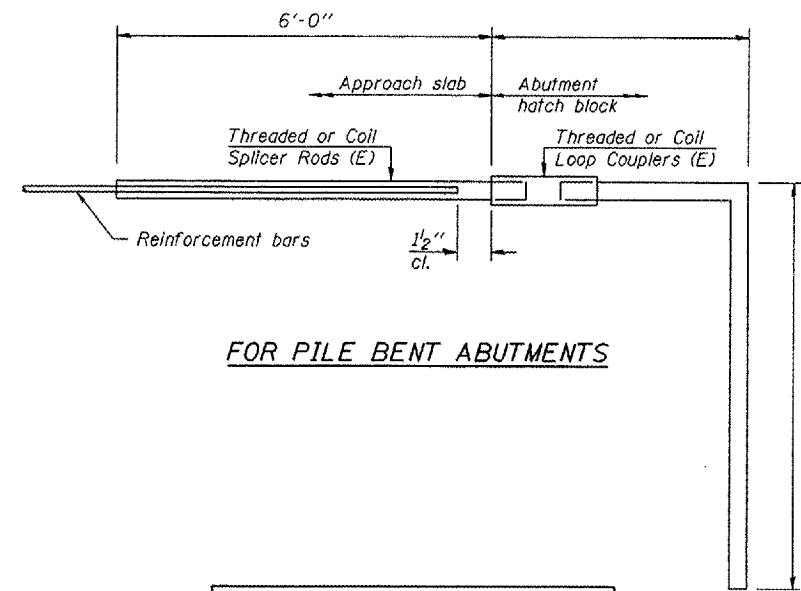
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



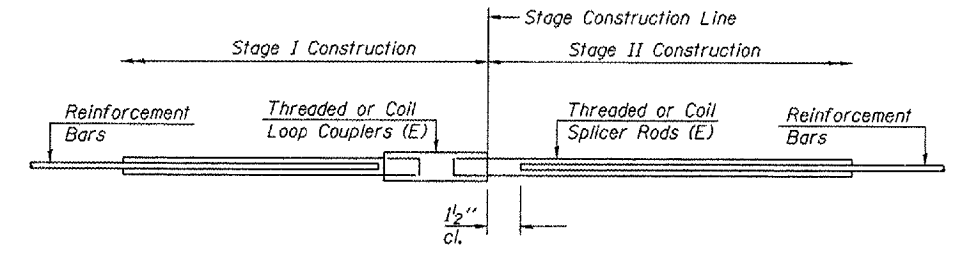
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



FOR PILE BENT ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



STANDARD

Bar Size	No. Assemblies Required	Location
#4	42	Conc. Wearing Surface
#6	6	North abutment

BAR SPLICER ASSEMBLY DETAILS
IL ROUTE 117 OVER
WALNUT CREEK
F.A.P. RTE. 702 - SECTION (113B)1
WOODFORD COUNTY
STA. 15+45.00
S.N. 102-0018

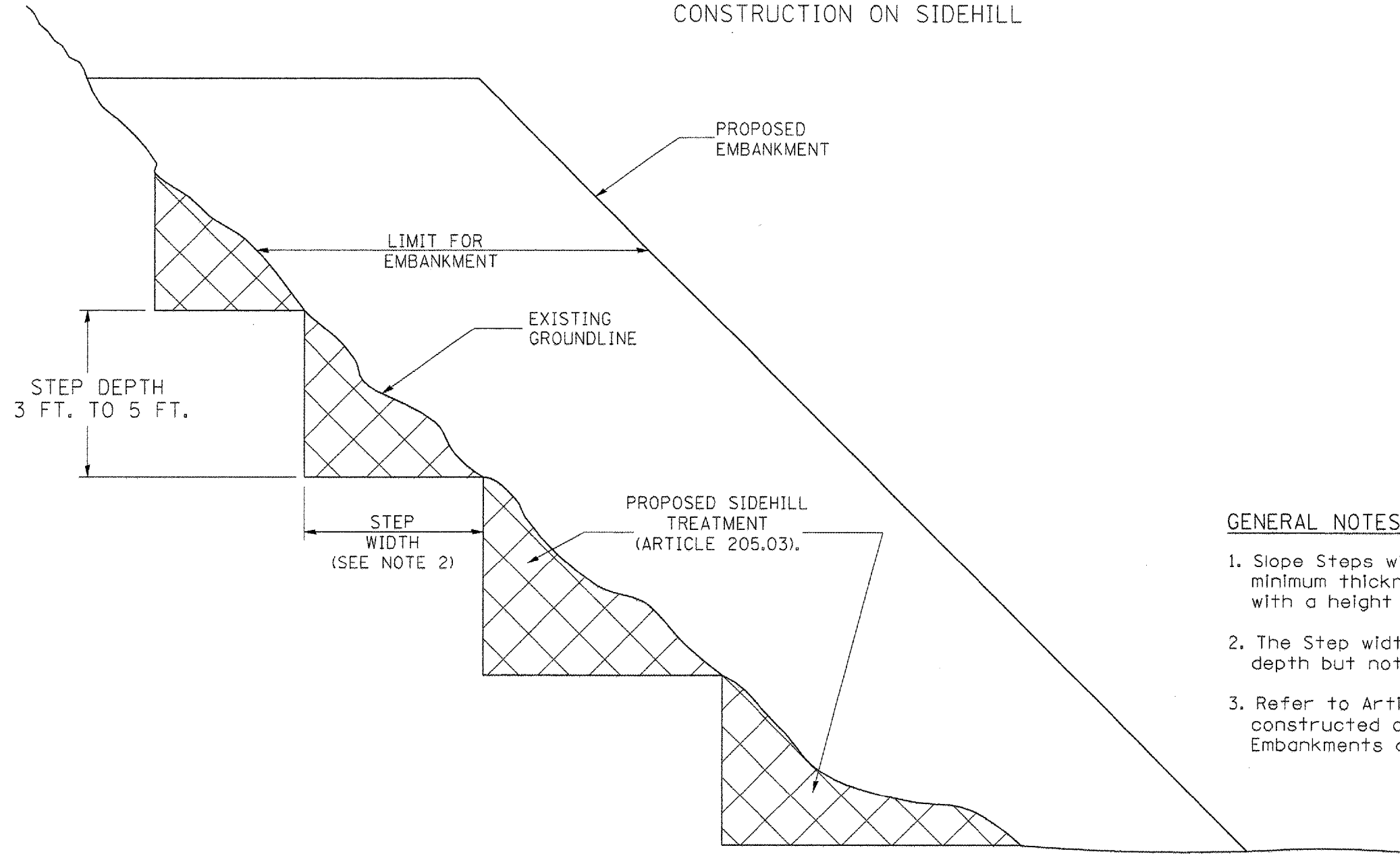
FILE NAME: STRUCTURE PLANSREV. 12/6/061

BSD-1

10-22-04

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	24
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. _		ILLINOIS FED. AID PROJECT		

SLOPE STEPS DETAIL
TYPICAL CROSS-SECTION EMBANKMENT
CONSTRUCTION ON SIDEHILL



GENERAL NOTES:

1. Slope Steps will be required for all 300(12) minimum thickness "silver fills" and on a fills with a height of 3.0m(10').
2. The Step width shall be twice the Step depth but not less than 6 feet.
3. Refer to Article 205.03 for Embankment to be constructed on Hillside or Slopes, or if existing Embankments are to be widened.

DESIGNER NOTE:
1. EACH PROJECT SHOULD BE REVIEWED INDEPENDENTLY FOR TREATMENT REQUIRED.
2. REFER TO THIS DETAIL WITH NOTE ON APPLICABLE TYPICAL SECTIONS.

REPLACEMENT MATERIAL:



STANDARD EMBANKMENT
(IN ACCORDANCE WITH
205 OF THE STANDARD SPECIFICATION).

All dimensions are in millimeters
(inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

DATE	REVISIONS	BY
1-1-97	RENUM. L-5.03, NEW REVISION BOX, REVISED TITLE BOX, REVISED GENERAL NOTES.	T.P.

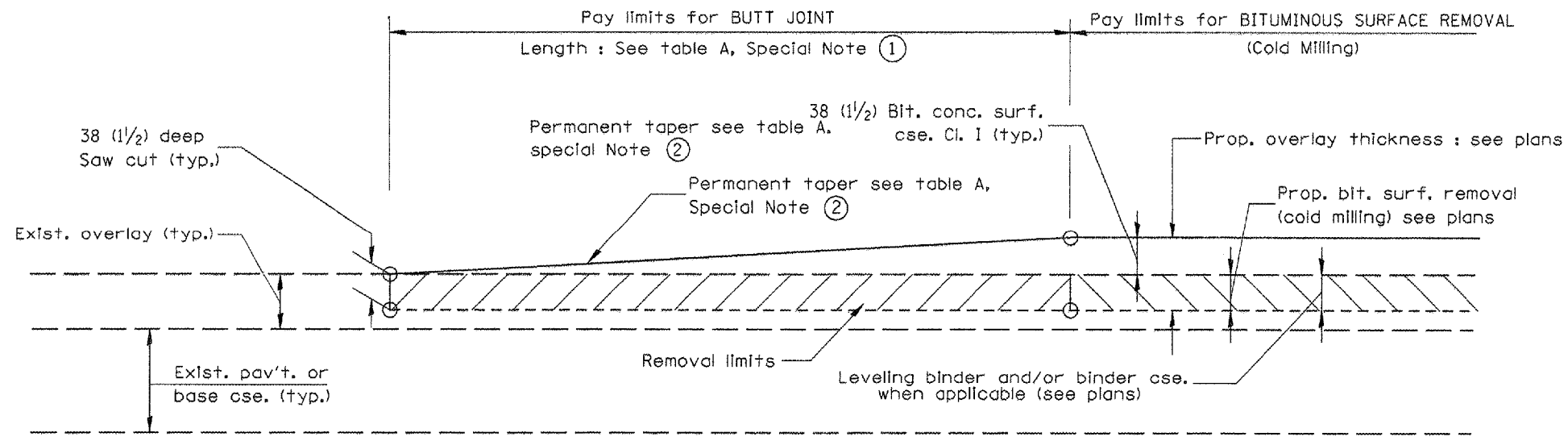
**SLOPE STEPS
DETAIL**

CADD STD. NO. 205001-D4
SCALE: NOT DRAWN TO SCALE
DATE **DATE**
DRAWN BY CADD
CHECKED BY

205001-D4

DATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	25
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



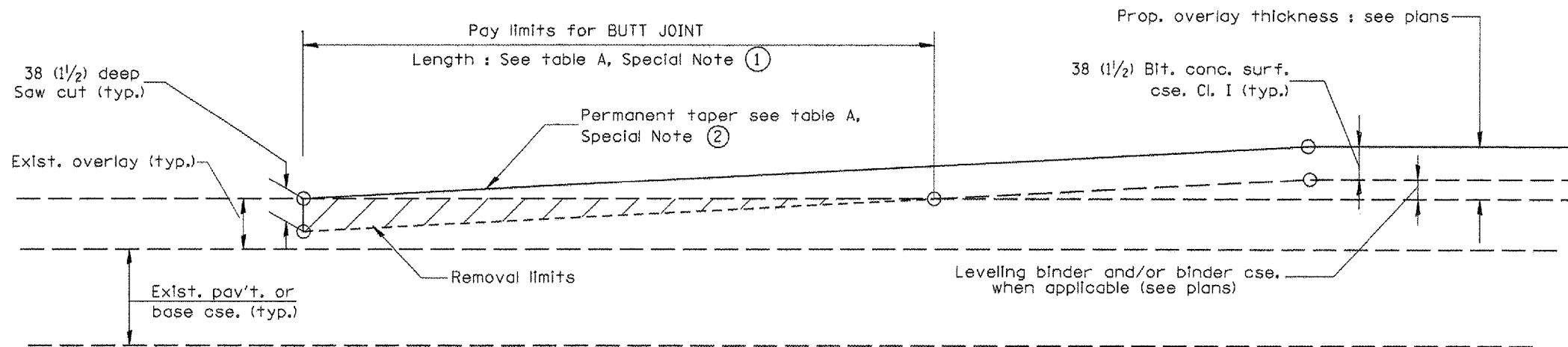
CASE 1 : WITH BITUMINOUS SURFACE REMOVAL (COLD MILLING)

TABLE A
(LENGTHS AND TAPER RATES)

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	LENGTH OF BUTT JOINT	18.0 m(60')	9.0 m(30')
②	PERMANENT TAPER RATE	1:480	1:240
③	TEMPORARY RAMP TAPER RATE	1:80	1:40
④	TEMPORARY RAMP LENGTH	3.0 m(10')	1.5 m(5')
⑤	LENGTH OF BUTT JOINT	3.0 m(10')	3.0 m(10')

GENERAL NOTES

- The work shall be done in accordance with Article 406.18 and the Special Provision for Butt Joints.
- The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.03 and the Special Provisions for Butt Joints.
- The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.06.



CASE 2 : NO BITUMINOUS SURFACE REMOVAL (COLD MILLING)

All dimensions are in millimeters (inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

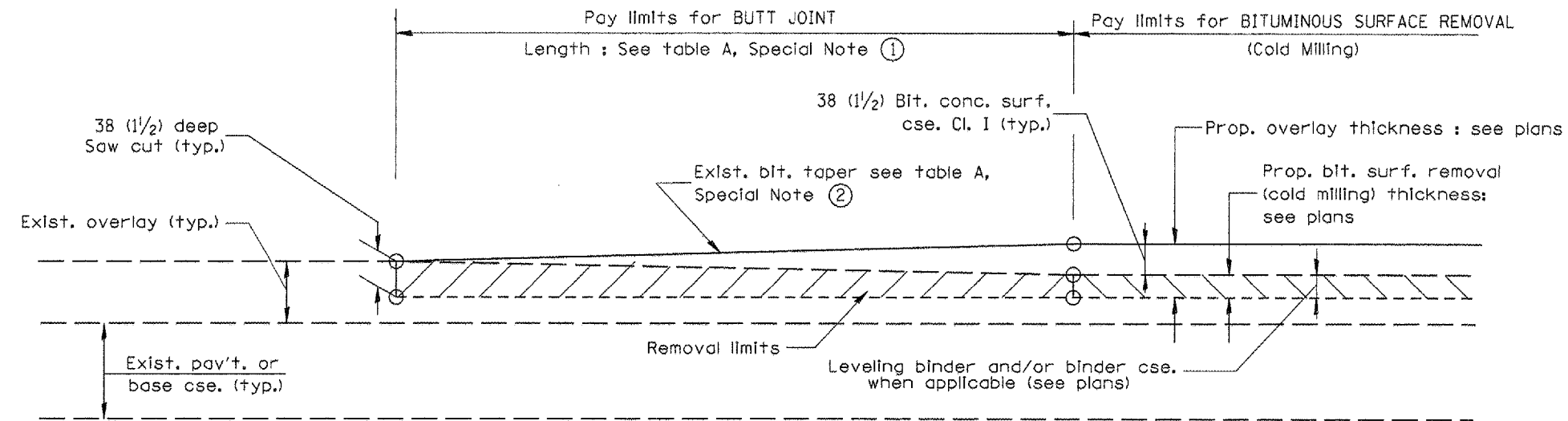
BUTT JOINTS
CADD STD NO. 406101-D4 SHEET 1 OF 3
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
DATE **DATE** CHECKED BY

DATE	REVISIONS	BY
1-1-97	RENUM. C-23.01, NEW REVISION BOX	T.P.
4-1-97	CORRECTION TO DEPTH	J.A.
9-15-05	REVISED DESIGNER NOTE	M.M.A.

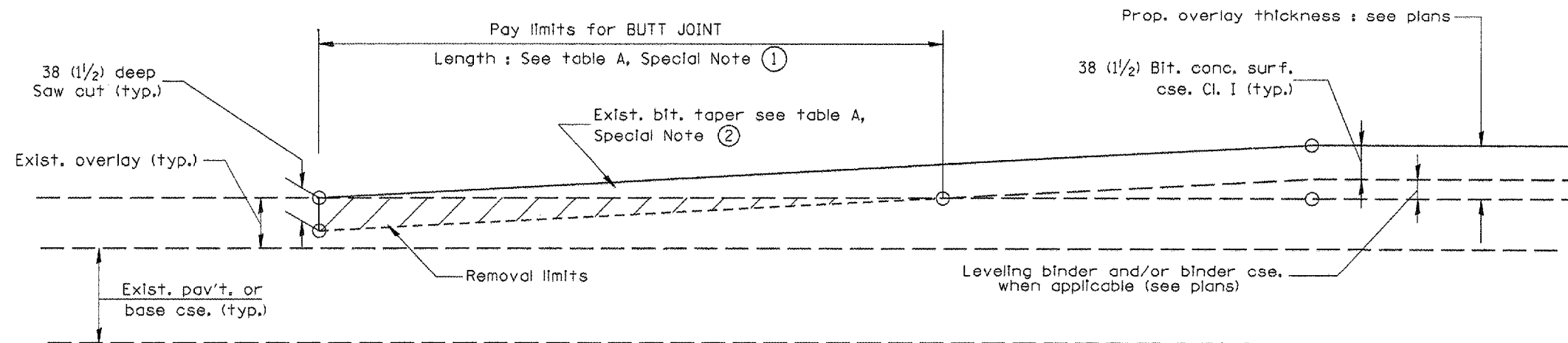
DESIGNER NOTES:
1. Include District Special Provision for Butt Joints & for Bituminous Surface Removal (Cold Milling).
2. The butt joints pay item includes the saw cut & temporary ramp. Payment for the Butt Joint applies whether or not the project features Bituminous Surface Removal (Cold Milling).

DATE

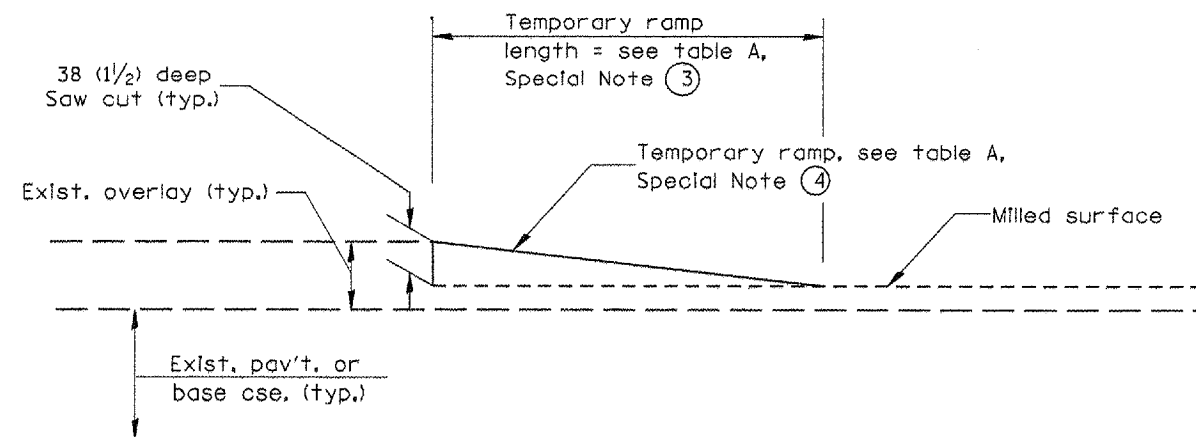
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	26
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**CASE 3 : WITH BITUMINOUS SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER**



**CASE 4 : NO BITUMINOUS SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER**



DETAIL TEMPORARY RAMP

All dimensions are in millimeters (inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

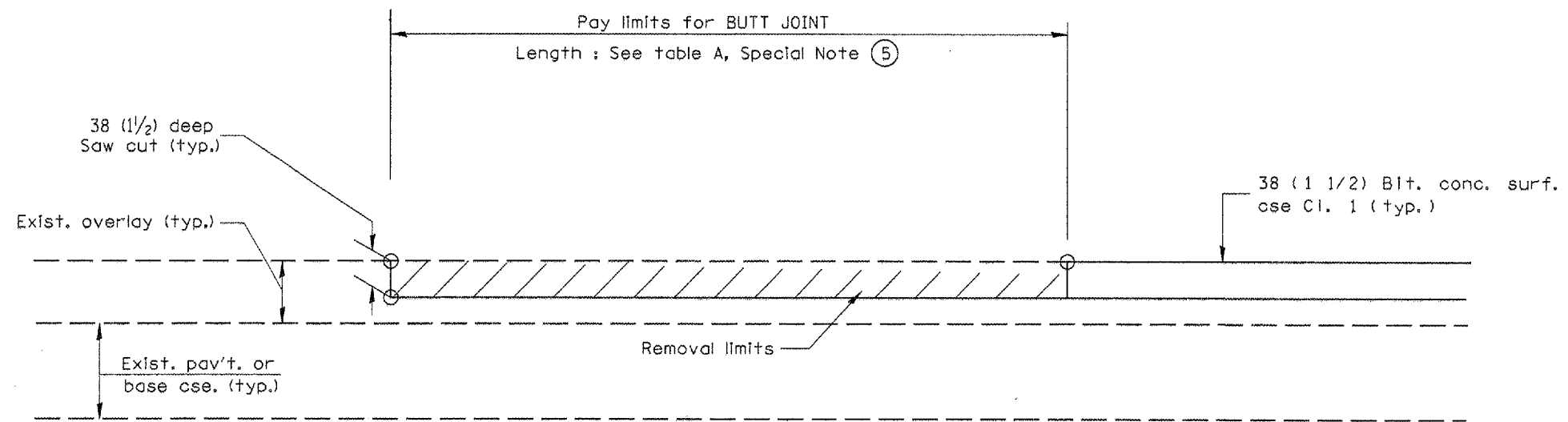
BUTT JOINTS

CADD STD NO. 406101-D4 SHEET 2 OF 3
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
CHECKED BY

406101-D4 (2)

\$\$\$DATE\$\$\$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	27
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



CASE 5 : WITH BITUMINOUS SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in millimeters
(inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

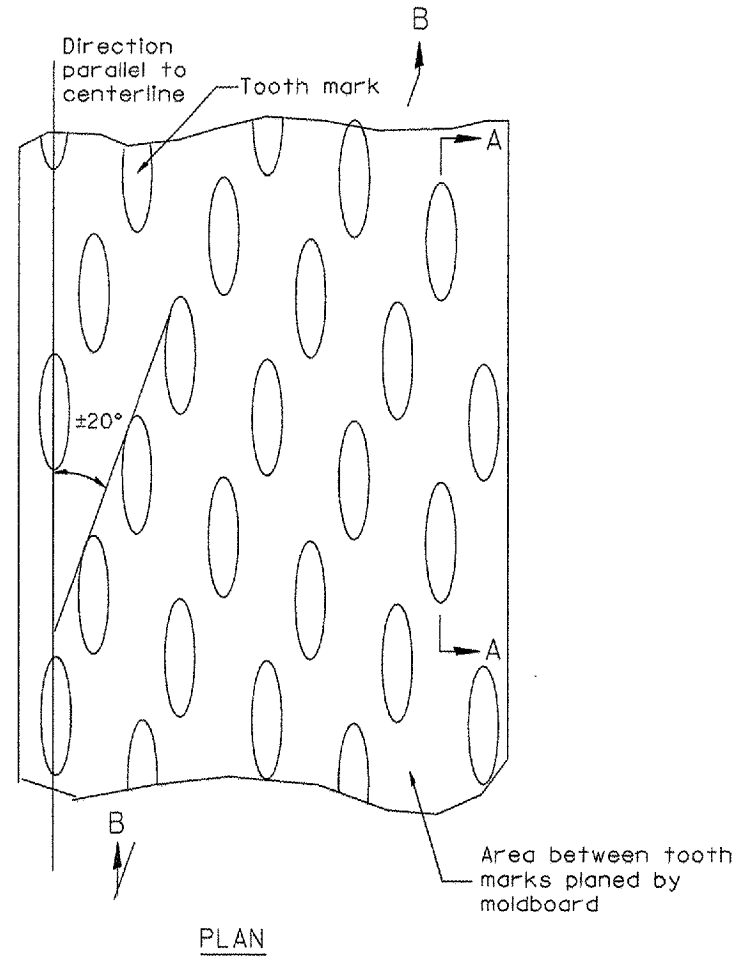
BUTT JOINTS

CADD STD NO. 406101-D4 SHEET 3 OF 3
DRAWN BY CADD
SCALE: NOT DRAWN TO SCALE CHECKED BY

406101-D4 (3)

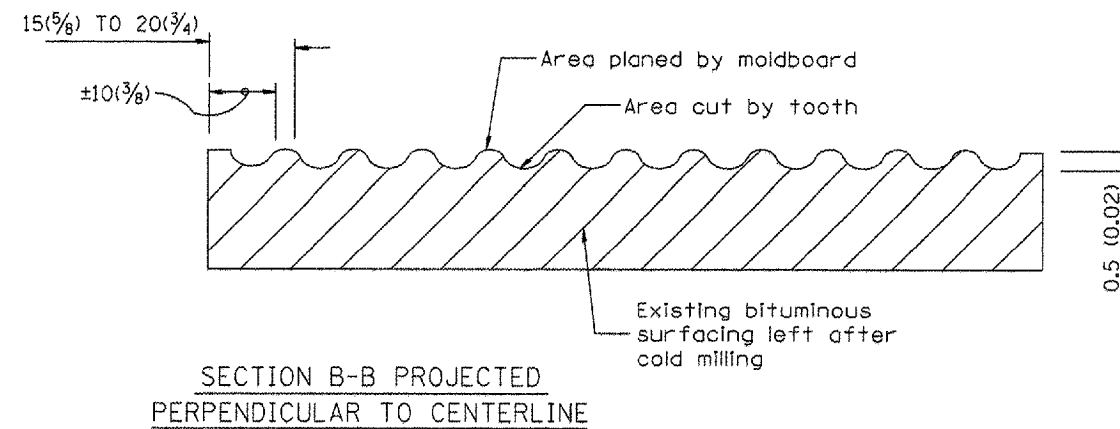
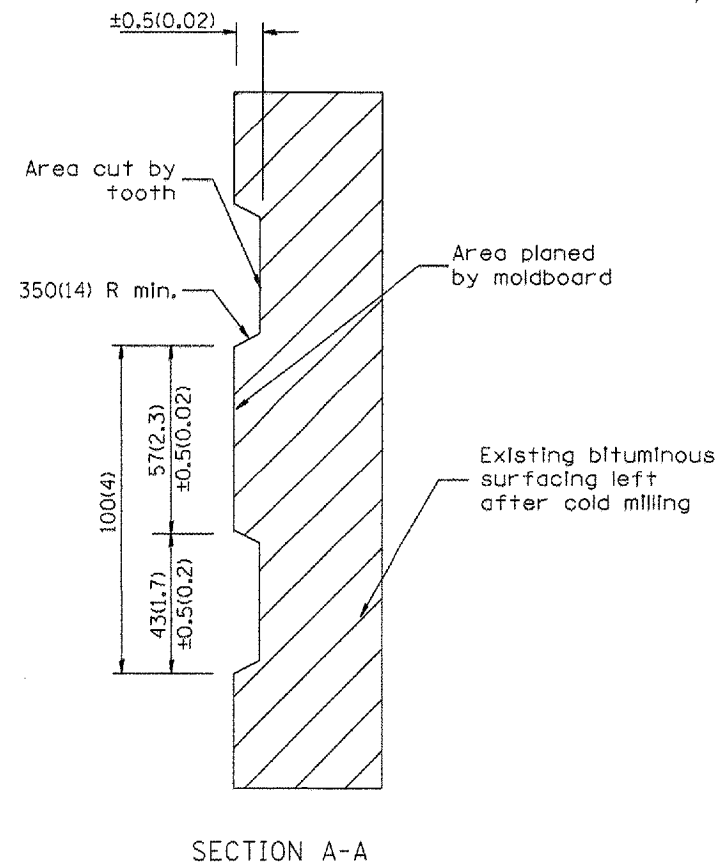
\$\$\$DATE\$\$\$

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	28
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



All dimensions are in millimeters (inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

DATE	REVISIONS	BY
1-1-97	RENUM. C-104.01, NEW REVISION BOX	T. P.
4-20-98	REMOVED MILLING DETAIL FROM STD.	J. A.
9-08-98	CORRECT NOTE LEADER PLACEMENT	R. W.

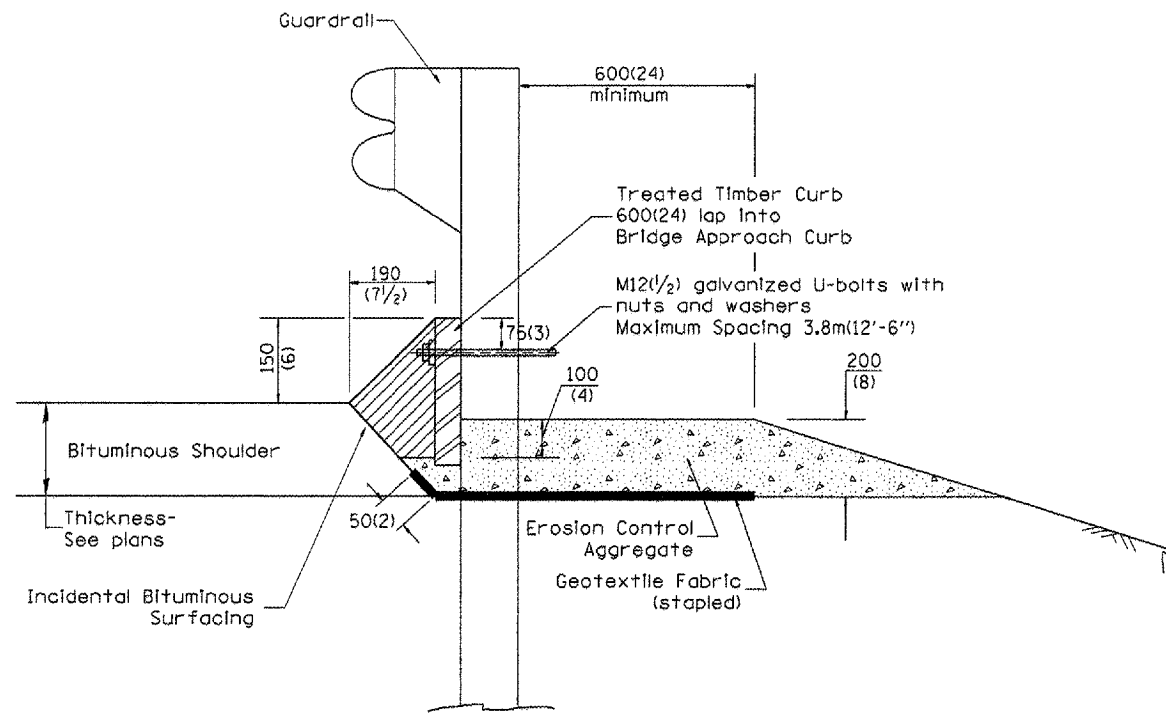
BITUMINOUS SURFACE REMOVAL (COLD MILLING)

CADD STD NO. 440001-D4
SCALE: NOT DRAWN TO SCALE
DATE **DATE**
DRAWN BY CADD
CHECKED BY

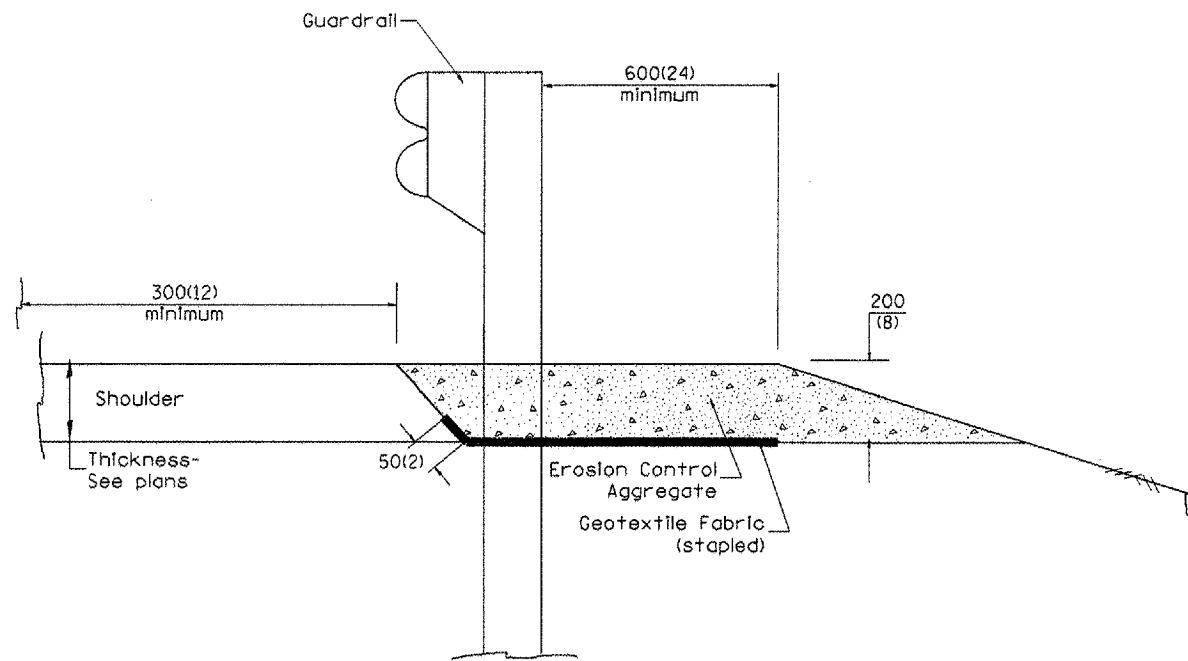
DESIGNER NOTE
1. INCLUDE DISTRICT SPECIAL PROVISION, IF APPLICABLE.

\$\$\$DATE\$\$\$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	29
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



TYPICAL SECTION WITH EROSION CONTROL CURB



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: EROSION CONTROL CURB

1. This work shall consist of grading as needed, installing hardware and treated timber boards, furnishing and placing mastic material and incidental bituminous surfacing in front of Steel Plate Beam Guardrail in accordance with Plan Details.
2. Timber shall be treated in accordance with Article 1007.12. All preservatives specified in the article will be allowed. Waterborne preservatives "asa" and "oca" shall have a minimum retention of 6.4 kg/m³ (0.40 lbs./cu. ft.)

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 300(12) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.

All dimensions are in millimeters (inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

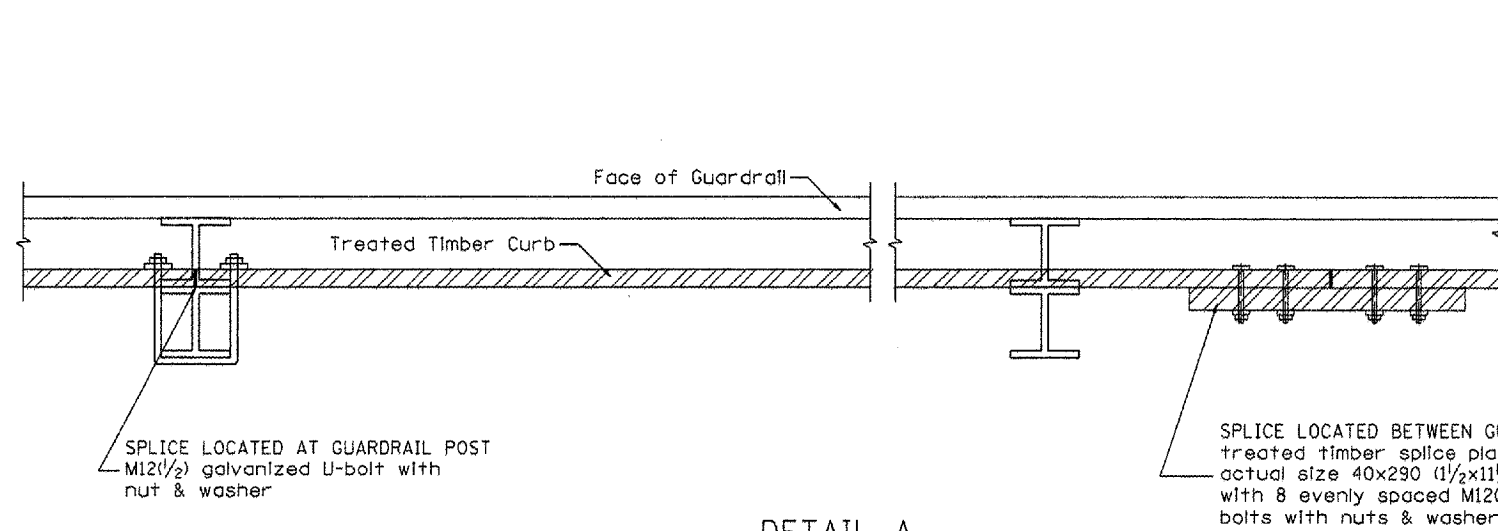
GUARDRAIL EROSION CONTROL TREATMENTS

CADD STD NO. 630101-D4(1) SHEET 1 OF 2
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
DATE **DATE** CHECKED BY

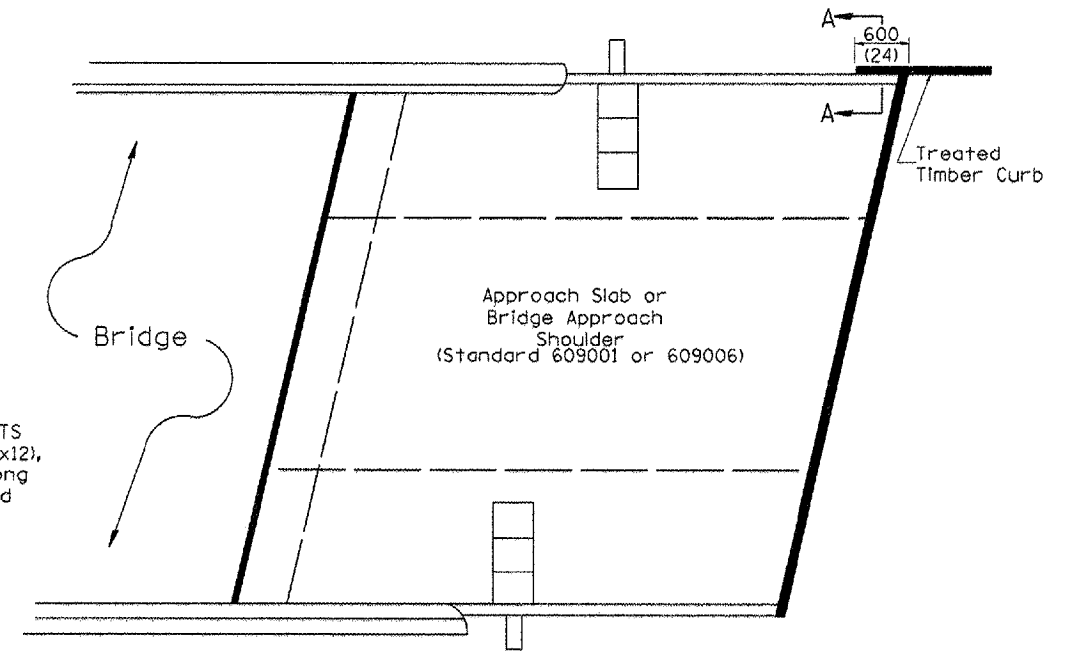
DATE	REVISIONS	BY
1-1-97	RENUM. C-22.01, NEW REVISION BOX	T.P.
3-1-97	CORRECT STD. NUMBERS IN NOTES PG. 2	J.A.
11-3-00	CORRECTION TO NOTES	M.A.

DESIGNER NOTE:
 1. Use EROSION CONTROL CURB at guardrail installations where grades are equal to or greater than 1% and at inlets. (Include District Special Provision)
 2. Use GUARDRAIL AGGREGATE EROSION CONTROL at guardrail installations where grades are less than 1% (Include District Special Provision)
 3. Include State Standards 609001, 609006 or 610001 if applicable.
 4. Include the following District Cadd Standards as needed: Slope Drains for Exposed Pipes; Seepage Collars for Buried Pipes; Seepage Collars for Buried Pipes; Slope Drains for Exposed Pipes; Slope Drains for Exposed Pipes; Concrete Thrust Blocks and Pipe Elbow.
 5. Include District Special Provision "Aggregate Quality" for projects located in the Western Area of the District - approx. dividing line is IL 97.
 DATE

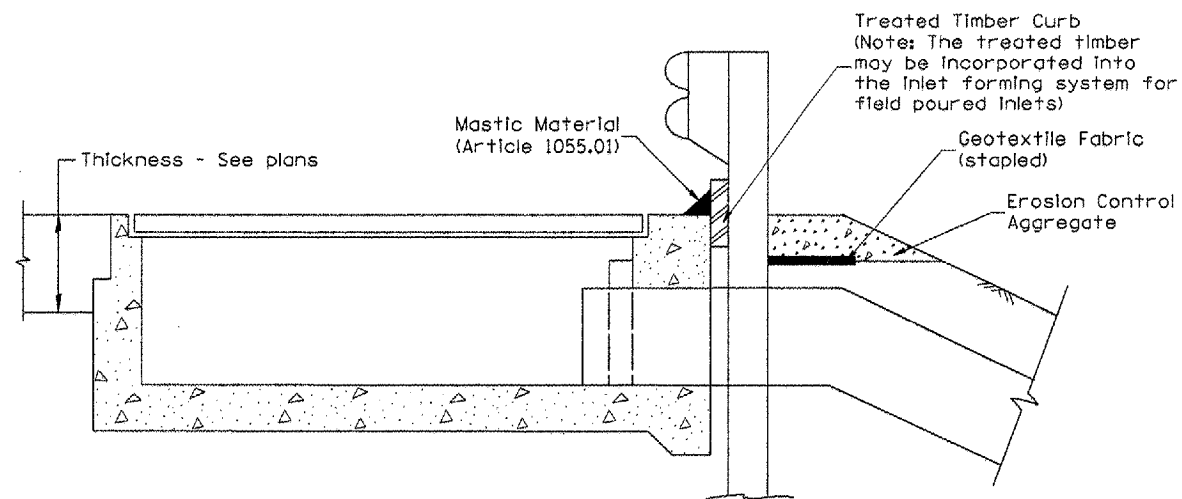
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	30
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. .		ILLINOIS FED. AID PROJECT		



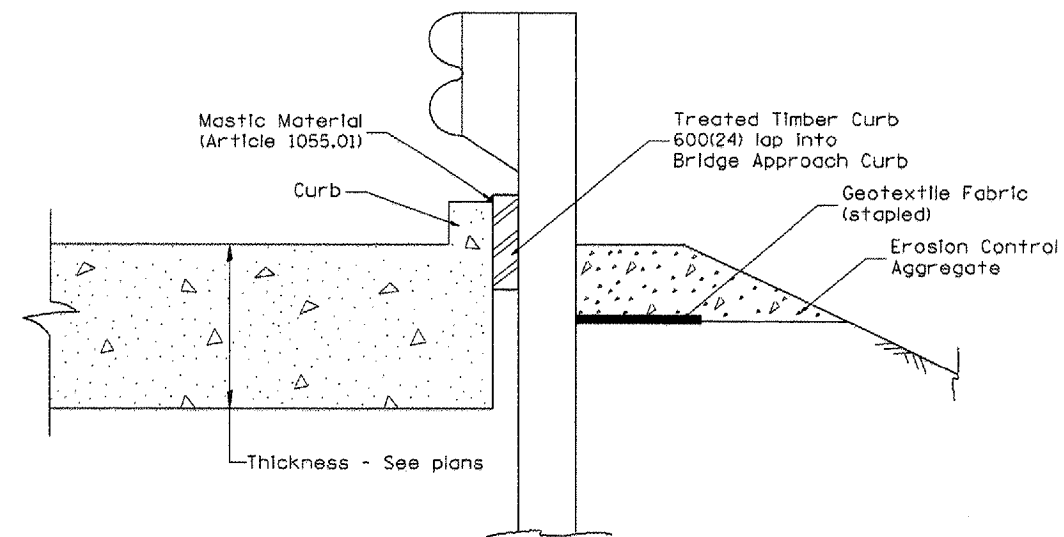
DETAIL A
(Typical Treated Timber Splices)



PLAN VIEW
APPROACH SLAB OR BRIDGE APPROACH SHOULDER
(STANDARD 609001 or 609006)



TYPICAL SECTION WITH EROSION CONTROL CURB
AT INLETS TYPE E & F (STANDARD 610001)

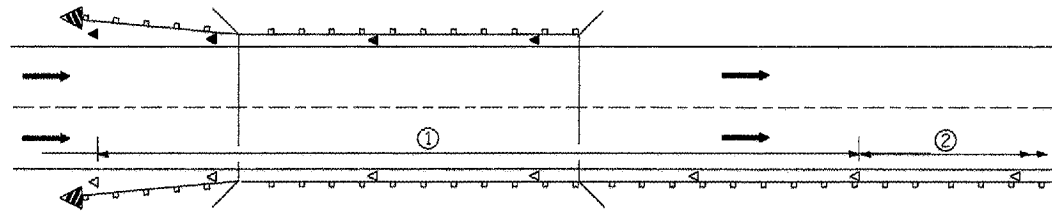


SECTION A-A
TYPICAL SECTION WITH EROSION CONTROL CURB
AT BRIDGE APPROACH CURB
(STANDARD 609001 OR 609006)

All dimensions are in millimeters (inches) unless otherwise noted.

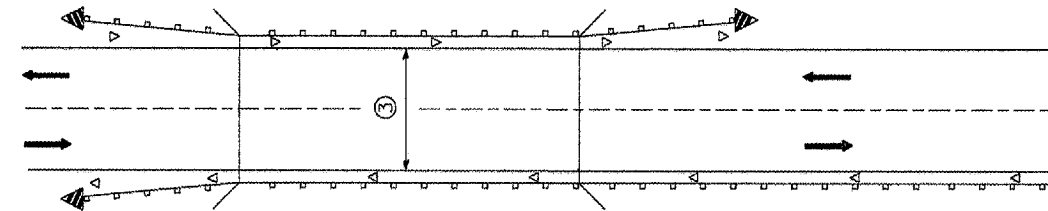
ILLINOIS DEPARTMENT OF TRANSPORTATION	
DISTRICT CADD STANDARD	
GUARDRAIL EROSION CONTROL TREATMENTS	
CADD STD NO. 630101-D4(2)	SHEET 2 OF 2
SCALE: NOT DRAWN TO SCALE	DRAWN BY CADD
DATE **DATE**	CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	31
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



- ① Spacing 24 m (80 ft.) max. for first 122 m (400 ft.) or curve spacing shown in Standard 635001, whichever is less (min. 4 reflectors regardless of length).
- ② After 122 m (400 ft.), transition to normal delineator spacing shown in Standard 635001, and continue as required.

ONE-WAY TRAFFIC



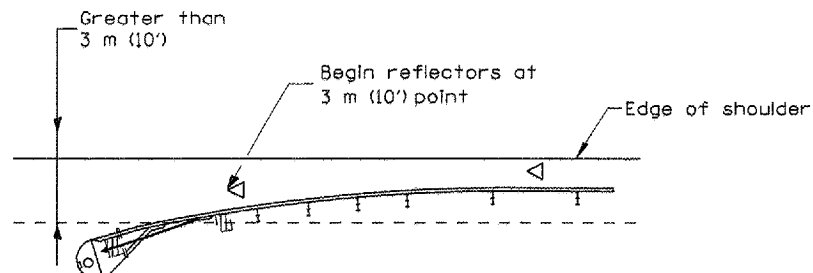
- ③ Bidirectional silver/silver should be used in lieu of monodirectional silver on both sides of two-lane bridges where the bridge pavement is less than 610 (24) wider than the pavement approaching the bridge.

TWO-WAY TRAFFIC

GUARDRAIL / BARRIER WALL / BRIDGE RAIL REFLECTORS

LEGEND

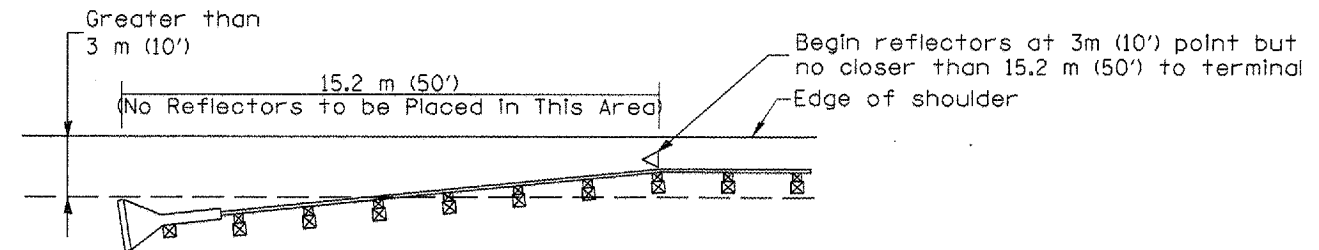
- ◁ Monodirectional silver
- ◄ Monodirectional amber
- ◄ Terminal Marker - Black/Yellow
Left or Right as appropriate



NOTE: Omit terminal marker when terminal over 3 m (10') from edge of paved shoulder or break point of unpaved shoulder, or when terminal buried in backslope.

Traffic Barrier Terminal Type(*) and/or Turned-Down Terminal

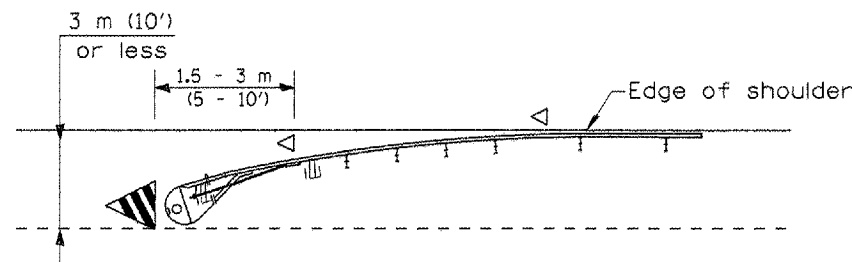
[Terminal over 3 m (10') from edge of shoulder]
*See Plans for Type



NOTE: Omit terminal marker when terminal over (10') from edge of paved shoulder or break point of unpaved shoulder.

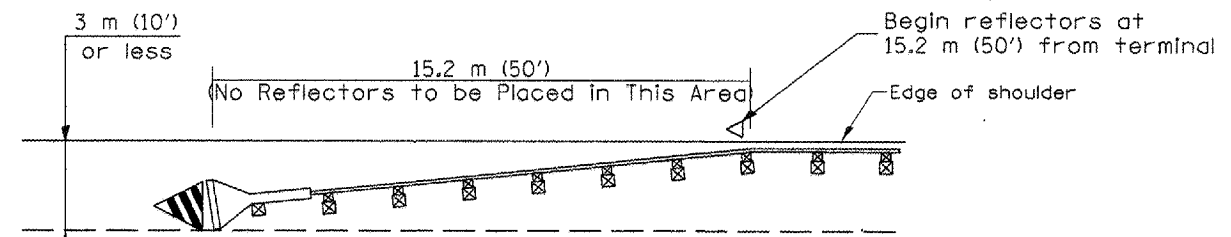
Traffic Barrier Terminal Type 1 (Special)

[Terminal over 3 m (10') from edge of shoulder]



Traffic Barrier Terminal Type(*) and/or Turned-Down Terminal

[Terminal over 3 m (10') or less from edge of shoulder]
*See Plans for Type



Traffic Barrier Terminal Type 1(Special)

[Terminal 3 m (10') or less from edge of shoulder]

All dimensions are in millimeters (Inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

GUARDRAIL AND BARRIER WALL DELINEATION

CADD STD. NO. 635101-D4 SHEET 1 OF 3
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
DATE **DATE** CHECKED BY

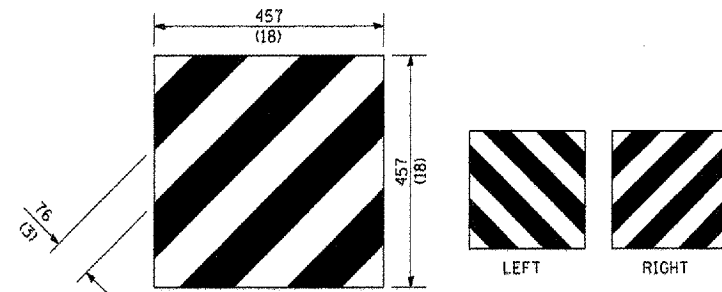
DATE	REVISIONS	BY
1-1-97	RENUM. E-10.02, NEW REVISION BOX	T.P.
3-1-97	CORRECT STD. SPEC. *	J.A.

TERMINAL MARKER PLACEMENT

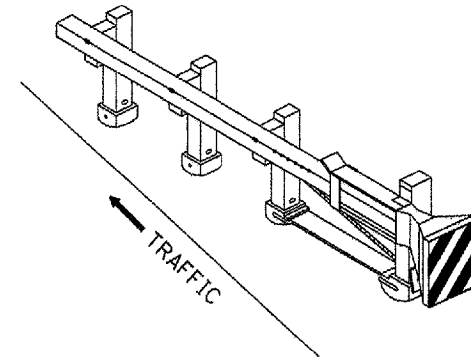
DESIGNER NOTE: 1. INCLUDE APPROPRIATE SPECIAL PROVISIONS FOR "GUARD RAIL DELINEATION POLICY; 1. TERMINAL MARKER, 2. TERMINAL MARK POST, AND 3. GUARDRAIL AND BARRIER WALL MARKERS." FROM INTERIM SPECIAL PROVISIONS 94-74; "GUARDRAIL AND BARRIER WALL DELINEATION." 2. IF POST MOUNT TERMINAL MARKER IS USED, INCLUDE STATE STD. T20011.

DATE

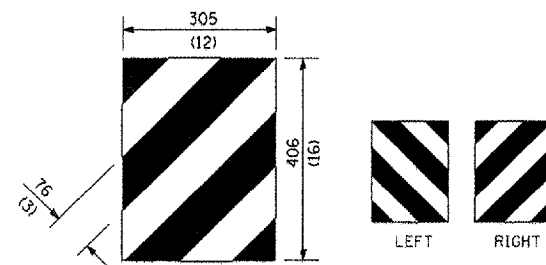
F.A.P. RYEL	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)1	WOODFORD	38	32
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. _		ILLINOIS FED. AID PROJECT		



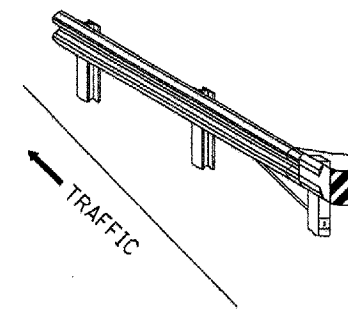
For Traffic Barrier Terminal Type 1 (Special)



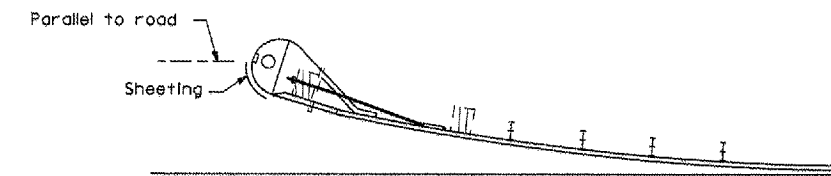
Standard Treatment - Direct Applied Sheeting
Traffic Barrier Terminal Type 1 (Special)



For Traffic Barrier Terminal Type (*)
and Post Mount
• See Plans for Type



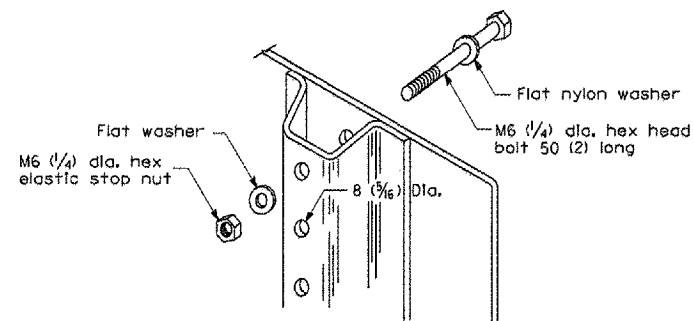
Standard Treatment - Direct Applied Sheeting
Traffic Barrier Terminal Type (*)
• See Plans for Type



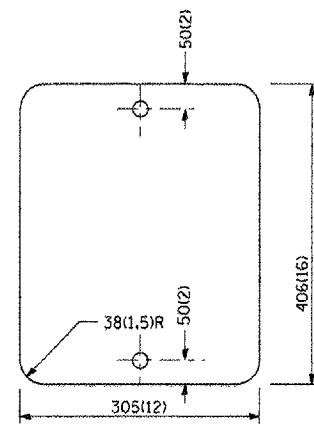
Sheeting Position for
Traffic Barrier Terminal Type (*)
• See Plans for Type

TERMINAL MARKER DETAILS

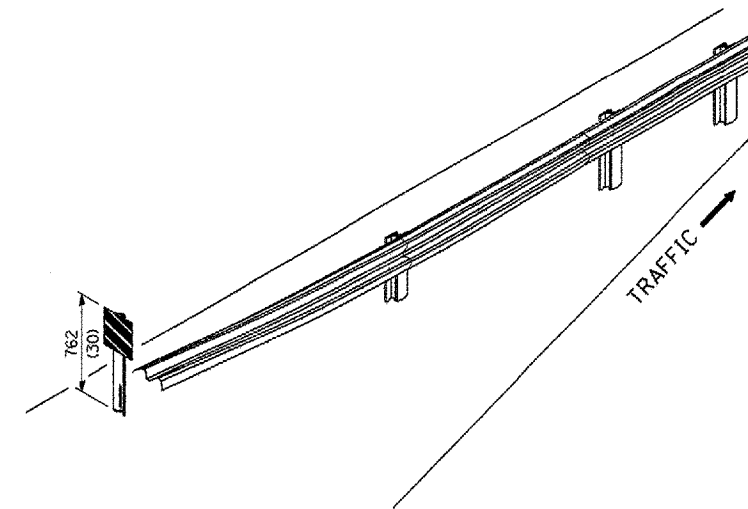
- Color: Black / Yellow reflectorized
- OM - I100 (L or R) Direct applied reflective sheeting
- OM - I200 (L or R) Post mounted



DETAIL OF MOUNTING TERMINAL MARKER TO POST



STANDARD TERMINAL MARKER



ALTERNATE TREATMENT - POST MOUNTED
(For turned-down terminal where sheeting cannot be direct applied)

TERMINAL MARKER TREATMENTS

GENERAL NOTES

All dimensions are in millimeters (Inches) unless otherwise noted.

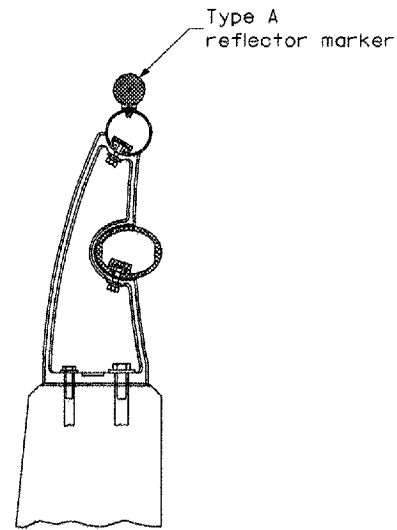
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

GUARDRAIL AND
BARRIER WALL DELINEATION

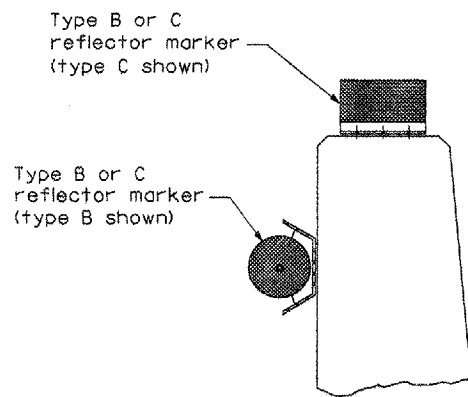
CADD STD. NO. 635101-D4 SHEET 2 OF 3
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
DATE **DATE** CHECKED BY

DATE**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B1)	WOODFORD	38	33
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

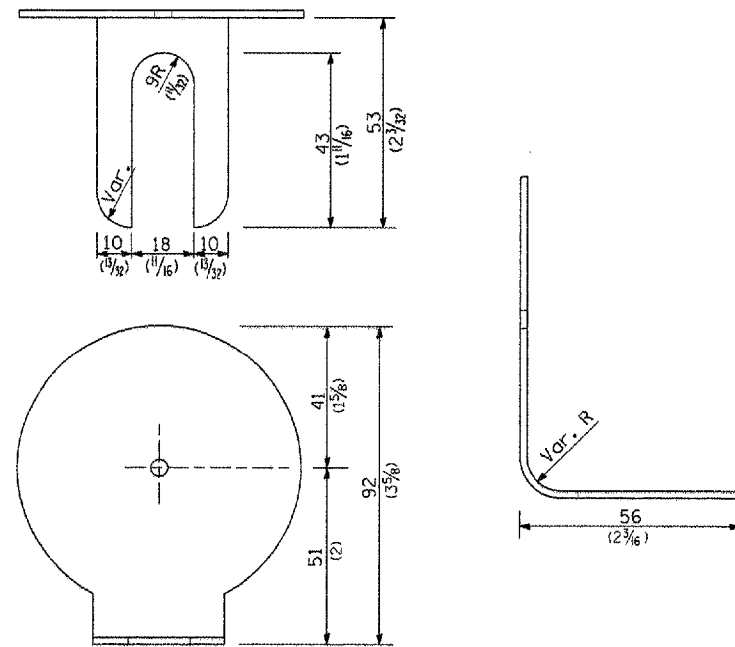


TYPICAL MOUNTING DETAIL FOR BRIDGE RAIL REFLECTOR

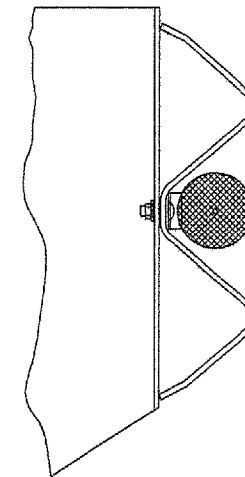


TYPICAL MOUNTING DETAIL FOR BARRIER WALL REFLECTOR

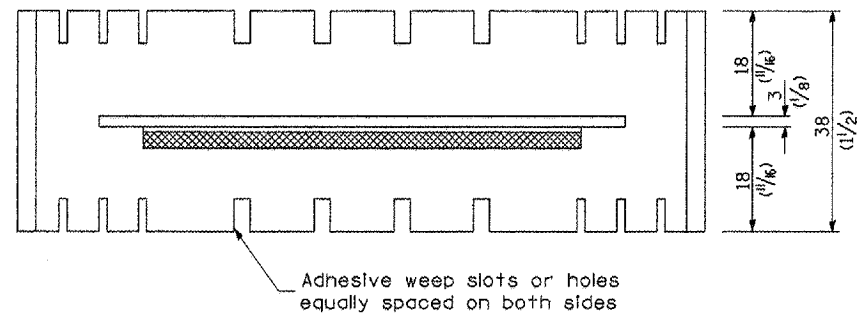
REFLECTOR MOUNTING



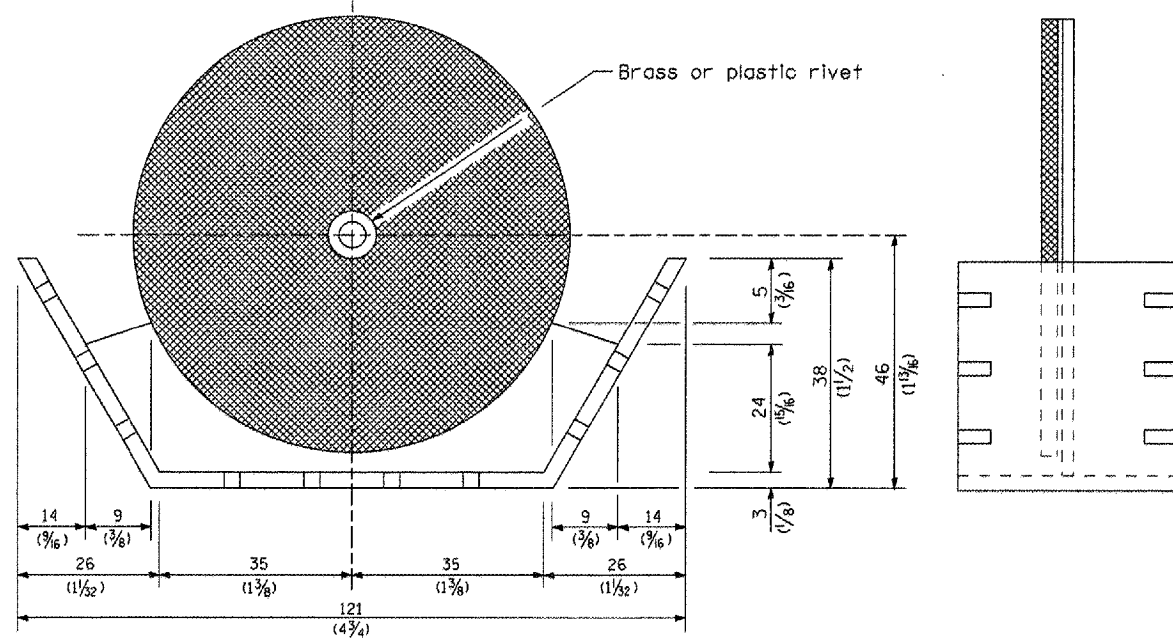
REFLECTOR MARKER TYPE A



TYPICAL GUARDRAIL MOUNTING WITH REFLECTOR MARKER TYPE A

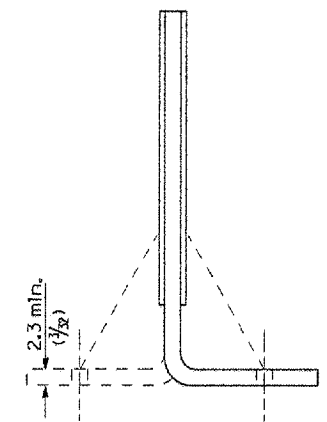
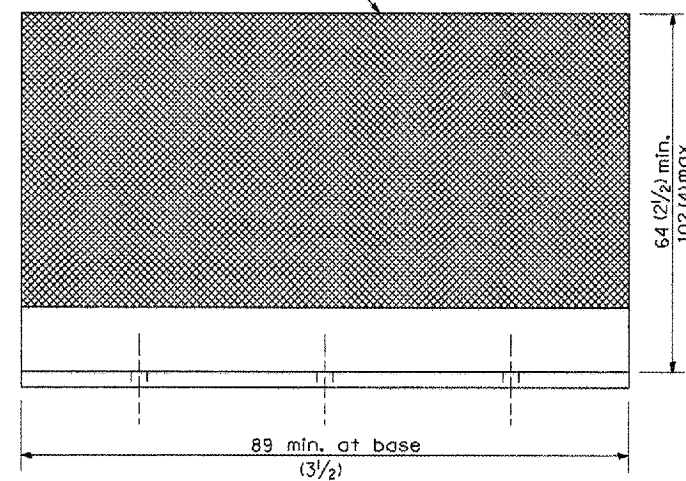


Adhesive weep slots or holes equally spaced on both sides



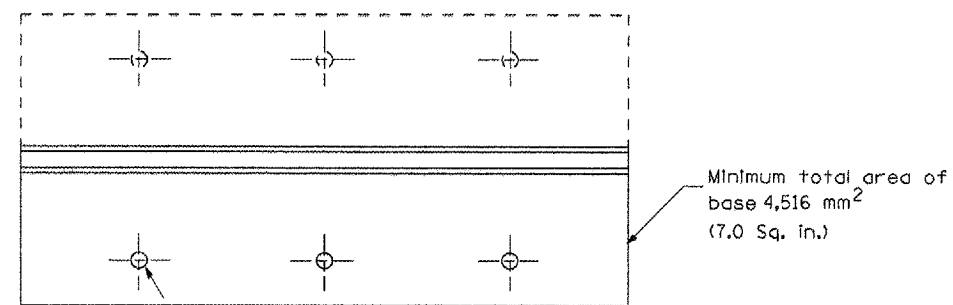
REFLECTOR MARKER TYPE B

Min. reflective area 4,194 mm² (6 1/2 Sq. in.) each side. May be rectangular or slight trapezoid.



Cross section may be "T" or "L" shaped and may have side supports at ends.

REFLECTORS



Minimum total area of base 4,516 mm² (7.0 Sq. in.)

3 min. adhesive weep holes or slots each side, variable spacing.

REFLECTOR MARKER TYPE C

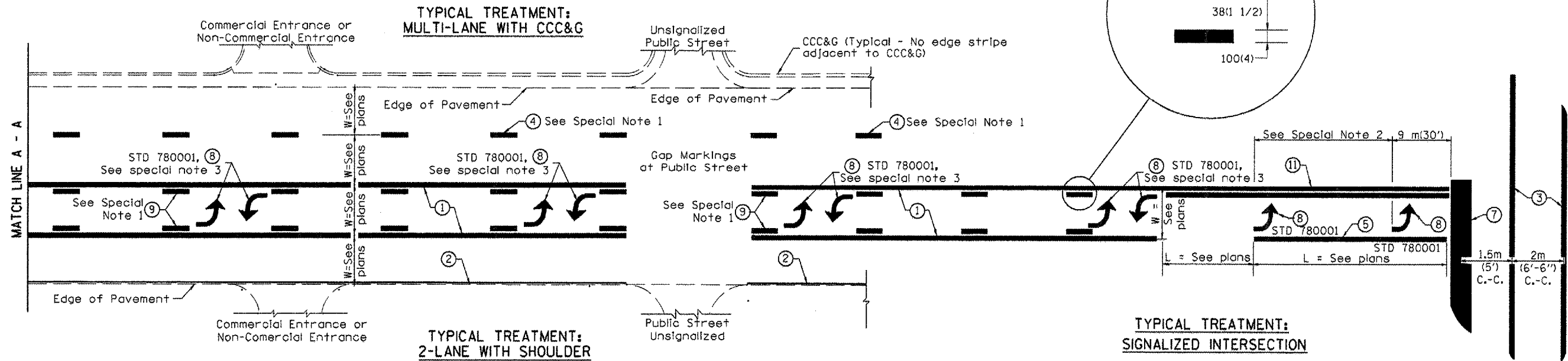
All dimensions are in millimeters (inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

GUARDRAIL AND
BARRIER WALL DELINEATION

CADD STD. NO. 635101-D4 SHEET 3 OF 3
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD
DATE **DATE** CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B1)	WOODFORD	38	34
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 100(4) Solid (Yellow)
- ② 100(4) Solid (White)
- ③ 2-150(6) Crosswalk @ 2m (6'-6")min C.-C. (White)
2-200(8) Crosswalk @ 2m (6'-6")min C.-C. (White) (When traffic signals are present.)
- ④ 150(6) Skip-Dash (White) (See Special Note 1)
- ⑤ 200(8) Solid (White)
- ⑥ 300(12) Diagonal (White) (Item 6 is shown on Std. 780001)
- ⑦ 600(24) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 100(4) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 300(12) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 100(4) Double Solid (Yellow) (See Table A)

SPECIAL NOTES

- Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
- The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 24 m (80').
 - C. Arrows shall be evenly spaced if three (3) or more are required.
- The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 61 m (200').
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between BI Directional Left Turn Arrows is 10 m (33').

GENERAL NOTES

- Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
- See Plans for Pavement Markings adjacent to curbed Islands and medians, and through lane reductions.

All dimensions are in millimeters (inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

TYPICAL PAVEMENT MARKINGS

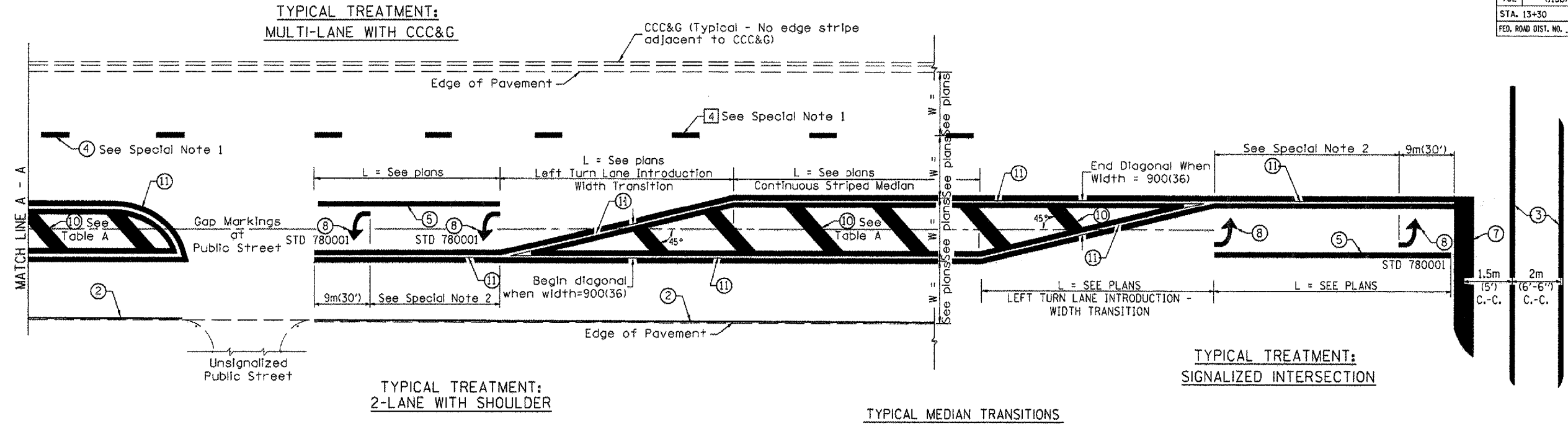
DATE	REVISIONS	BY
1-1-97	RENUM. F-8.03, NEW REVISION BOX	T.P.
2-7-97	ADD BI DIRECTIONAL DIMENSION	J.A.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.
8-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.

CADD STANDARD 780001-D4 SHEET 1 OF 2
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD CHECKED BY

DESIGNER NOTES:
1. Include State Standard 780001 (Typical Pavement Markings)

\$\$\$DATE\$\$\$

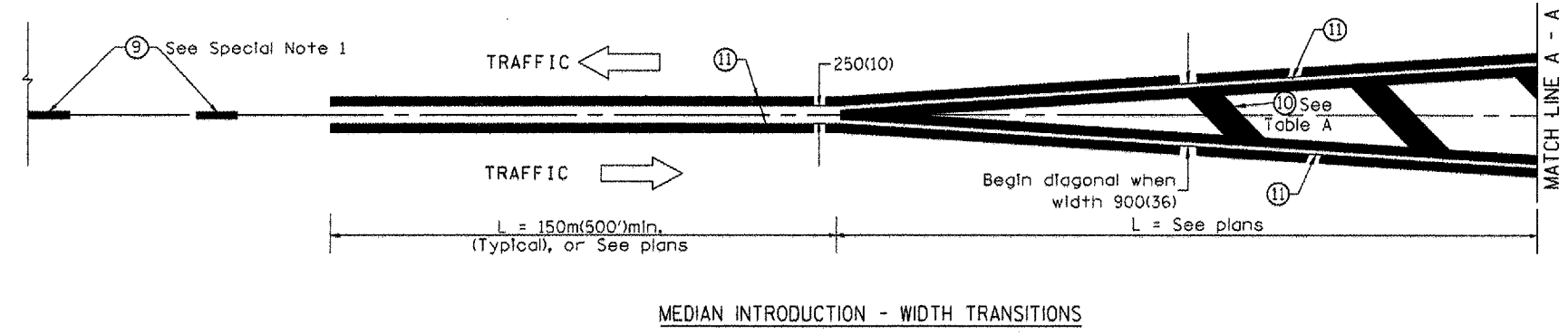
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B)	WOODFORD	38	35
STA. 13+30		TO STA. 17+00		
FED. ROAD DIST. NO. -		ILLINOIS FED. AID PROJECT		



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 50 km/h (30 mph)	15m (50')	5m (15')
50 - 70 km/h (30 - 45 mph)	23m (75')	6m (20')
Over 70 km/h (45 mph)	46m (150')	9m (30')



MEDIAN INTRODUCTION - WIDTH TRANSITIONS

All dimensions are in millimeters (inches) unless otherwise noted.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT CADD STANDARD

TYPICAL PAVEMENT MARKINGS

CADD STANDARD 780001-D4 SHEET 2 OF 2
SCALE: NOT DRAWN TO SCALE DRAWN BY CADD CHECKED BY

\$\$\$DATE\$\$\$

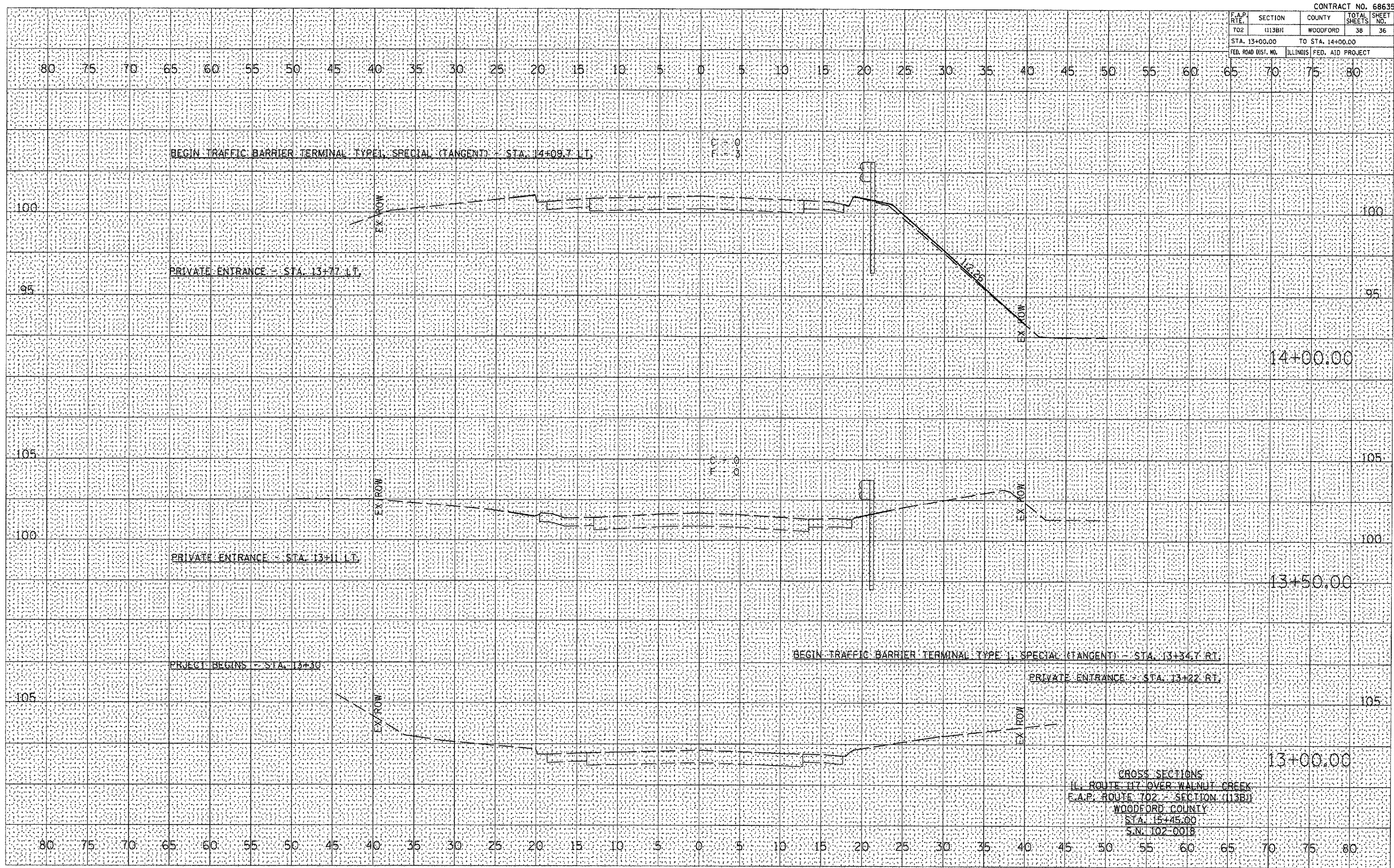
DDN-ONLY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
102	(113B)	WOODFORD	38	36
STA. 13+00.00		TO STA. 14+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	

DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	

PLOT DATE = 08/28/88
 FILE NAME = 414E14
 PLOT NAME = 414E14
 USER NAME = 414E14



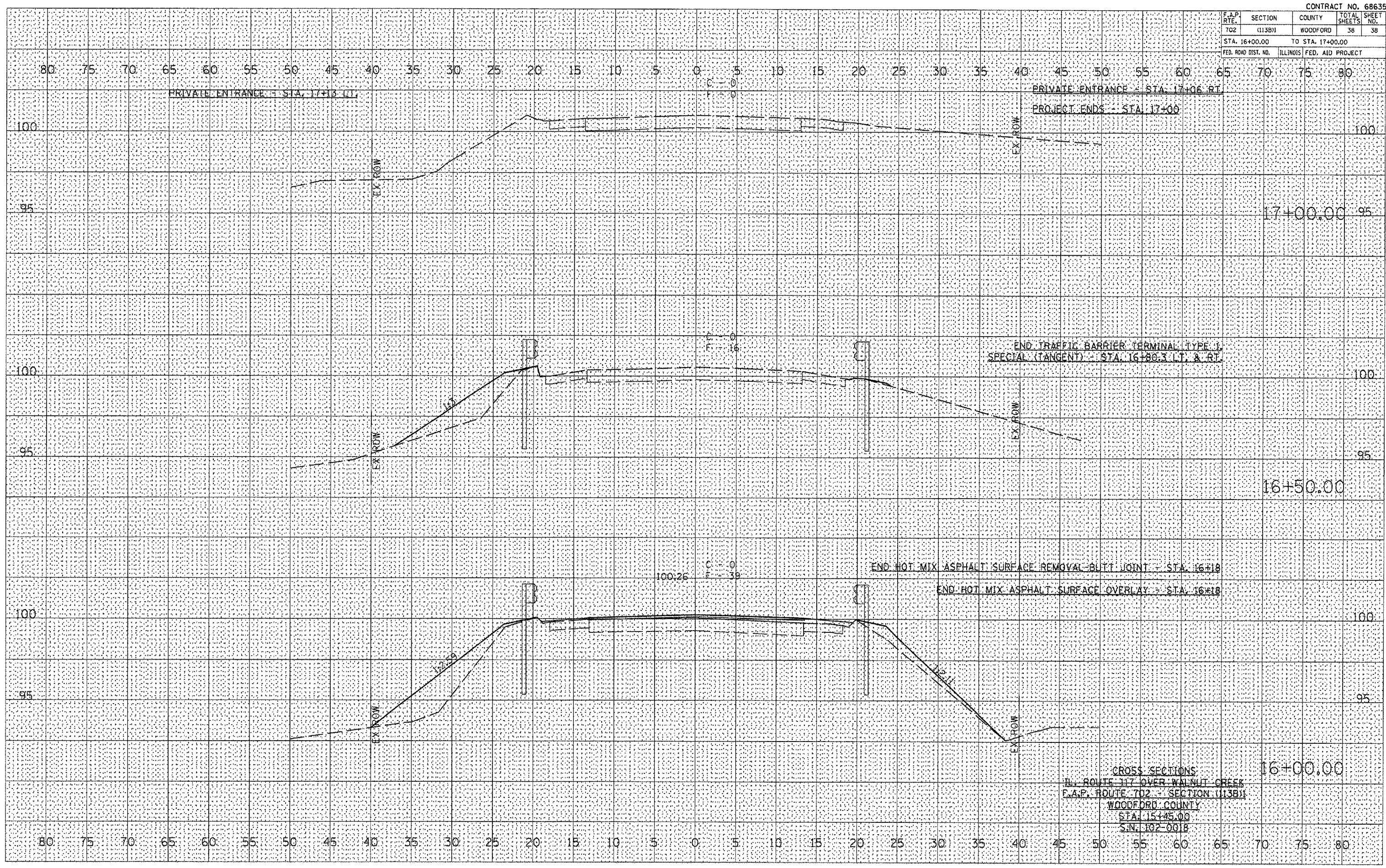
CROSS SECTIONS
 ILL. ROUTE 117 OVER WALNUT CREEK
 E.A.P. ROUTE 102 - SECTION (113B)
 WOODFORD COUNTY
 STA. 15+45.00
 S.N. 102-001B

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
702	(113B1)	WOODFORD	38	38
STA. 16+00.00		TO STA. 17+00.00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DATE	
BY	
CONVERTED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	

DATE	
BY	
CONVERTED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	

PLOT DATE = 04/15/08
 FILE NAME = #FILE#
 PLOT NAME = #PLOT#
 USER NAME = #USER#



CROSS SECTIONS
 ILL. ROUTE 117 OVER WALNUT CREEK
 F.A.P. ROUTE 702 - SECTION (113B1)
 WOODFORD COUNTY
 STA. 15+45.00
 S.N. 102-0018