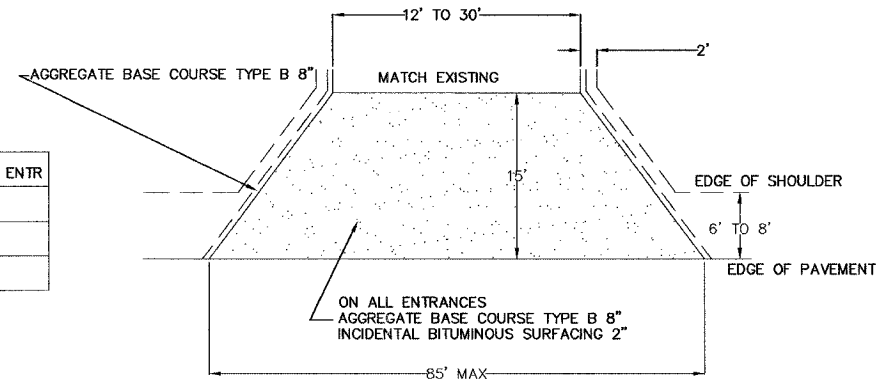




BITUMINOUS APPROACHES & MAILBOX RETURNS

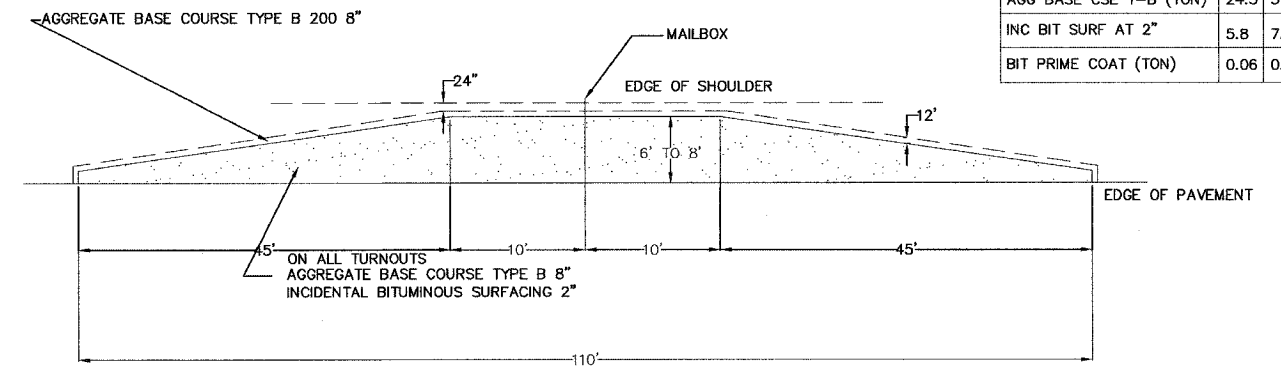
FIELD ENTRANCE



35' FIELD ENTRANCE

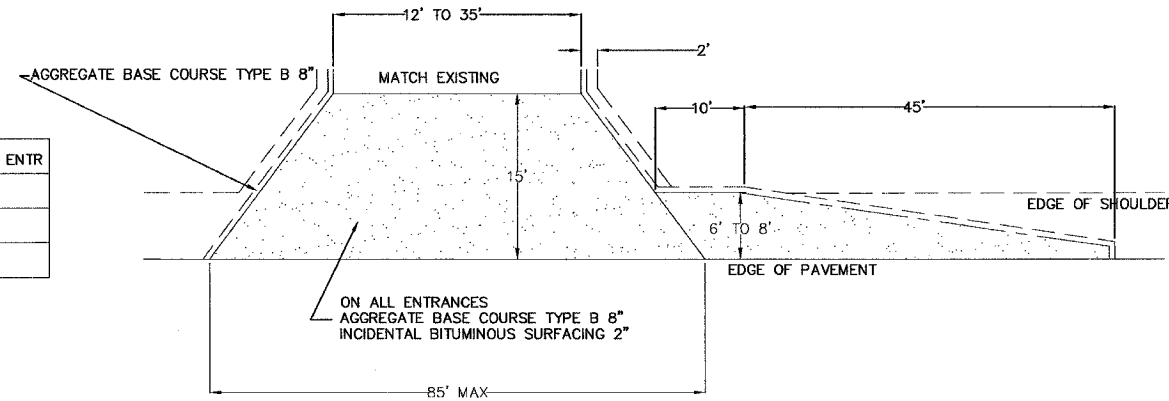
	6'	8'	PER FOOT ENTR
AGG BASE CSE T-B (TON)	65.9	68.5	1.87
INC BIT SURF AT 2"	20.5	21.2	0.54
BIT PRIME COAT (TON)	0.19	0.20	0.006

MAILBOX TURNOUT



	6'	8'
AGG BASE CSE T-B (TON)	24.5	31.1
INC BIT SURF AT 2"	5.8	7.8
BIT PRIME COAT (TON)	0.06	0.08

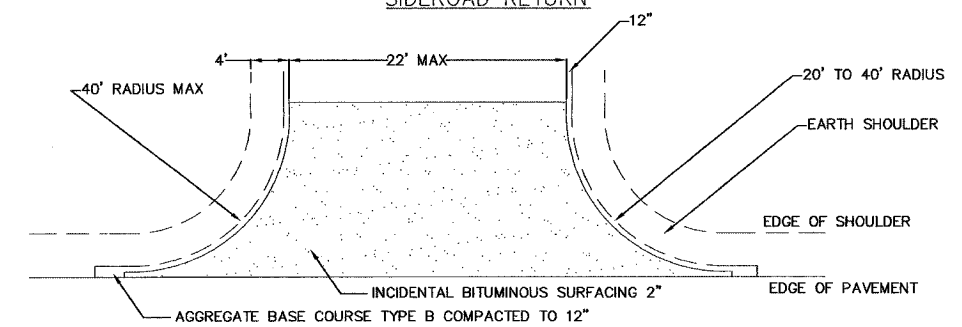
FARM OR SMALL COMERCIAL ENTRANCE



35' COMMERCIAL ENTRANCE

	6'	8'	PER FOOT ENTR
AGG BASE CSE T-B (TON)	65.9	68.5	1.87
INC BIT SURF AT 2"	20.5	21.2	0.54
BIT PRIME COAT (TON)	0.19	0.20	0.006

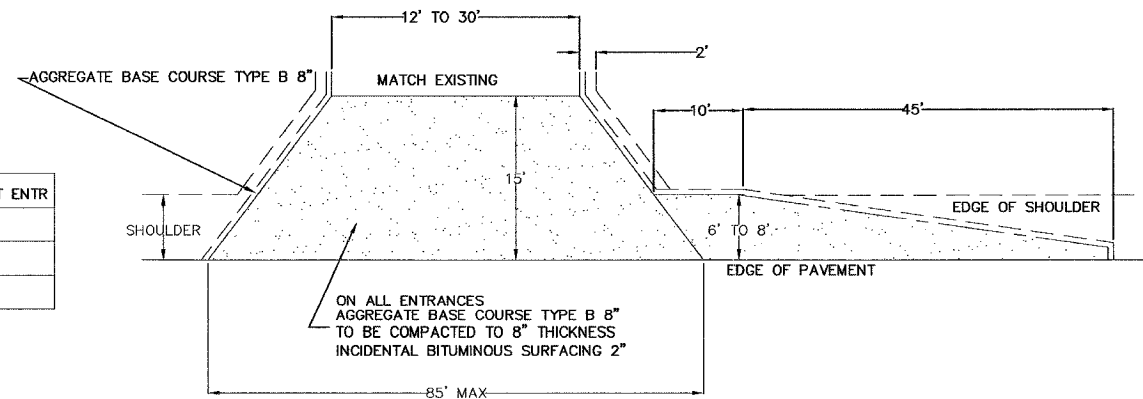
SIDEROAD RETURN



	RADIUS 20'			RADIUS 30'			RADIUS 40'			RADIUS 50'		
	18'	20'	22'	18'	20'	22'	18'	20'	22'	18'	20'	22'
AGG BASE CSE T-B (TON)	45.1	48.2	51.2	77.5	82.0	86.6	116.3	122.4	128.5	161.5	169.1	179.9
INC BIT SURF AT 2"	6.6	7.2	7.6	11.6	12.2	13.0	17.6	18.6	19.8	24.6	25.8	27.0
BIT PRIME COAT (TON)	0.08	0.09	0.10	0.15	0.16	0.17	0.22	0.24	0.25	0.31	0.33	0.35

NOTE: USE 2" INC BIT SURF ON EXISTING RETURNS

PRIVATE ENTRANCE



12' PRIVATE ENTRANCE

	6'	8'	PER FOOT ENTR
AGG BASE CSE T-B (TON)	43.8	48.3	0.71
INC BIT SURF AT 2"	13.5	14.4	0.19
BIT PRIME COAT (TON)	0.12	0.13	0.002

NOTES

1. ALL PE & CE ARE TO BE SURFACED TO RIGHT OF WAY LINE.
2. EXISTING APPROACHES MAILBOX RETURNS SHALL BE WIDENED TO CONFORM WITH STANDARD.
3. LARGE COMMERCIAL ENTRANCES ARE TO BE DESIGNED INDIVIDUALLY AND TO BE APPROVED BY THE WINNEBAGO COUNTY HIGHWAY DEPARTMENT
4. DRIVE PROFILES SHALL BE CONSTRUCTED WITH A 20' VERTICAL CURVE BEGINNING AT THE EDGE OF SHOULDER TO PROVIDE A SMOOTH TRANSITION FROM THE DRIVE PROFILE TO THE SHOULDER.
5. WHEN RESURFACING EXISTING SIDEROAD RETURN PRIVATE ENTRANCE MAILBOX TURNOUT AND MAIN LINE SURFACING IS GREATER THAN 2" TAPER BITUMINOUS FROM EDGE OF PAVEMENT TO 2" IN 4'. RESURFACING OF EXISTING SIDEROAD, PRIVATE ENTRANCE AND FIELD ENTRANCE SHALL CONFORM TO EXISTING CONFIGURATION.