

VILLAGE OF SCHAUMBURG SCHAUMBURG, ILLINOIS

CONSTRUCTION PLANS FOR SCHAUMBURG REGIONAL AIRPORT

REHABILITATE AND REMARK RUNWAY 11/29

ILLINOIS PROJECT: 06C-3808
AIP PROJECT: 3-17-0124-B28

JANUARY 20, 2012

11256-03



GRAVORG MURPHY & TILLY, INC.
CONSULTING ENGINEERS

SUBMITTED BY *Seán M. Smith*
SEÁN M. SMITH, P.E.

DATE *20 January 2012*

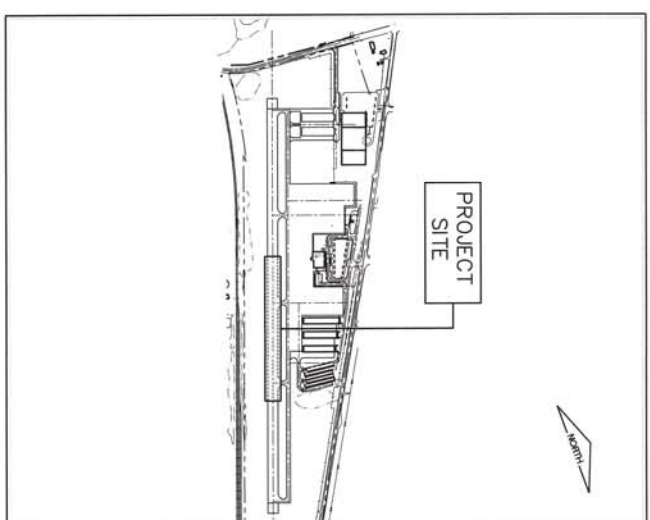


VILLAGE OF SCHAUMBURG
SCHAUMBURG REGIONAL AIRPORT

APPROVED BY *Stacy R. Rindstad*
DIRECTOR OF ENGINEERING AND PUBLIC WORKS

DATE *1-18-12*

V:\11256\03\DRAWINGS\SHEETS\01-COVER



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JULIE
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INFORMATION FOR EXCAVATORS
www.illinois811.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION, SIZE AND TYPE OF EXISTING UTILITIES AND BE RESPONSIBLE FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL JULIE FOR UTILITY INFORMATION AT 811.

DESIGN AIRCRAFT APPROACH CATEGORY: B

DESIGN AIRCRAFT GROUP: I

SCHAUMBURG REGIONAL AIRPORT

TOWNSHIP: 40 NORTH TOWNSHIP: 41 NORTH
 RANGE: 10 EAST RANGE: 10 EAST
 DUPAGE COUNTY COOK COUNTY
 BLOOMINGDALE TOWNSHIP SCHAUMBURG TOWNSHIP
 (SECTION: 4) (SECTION: 33)

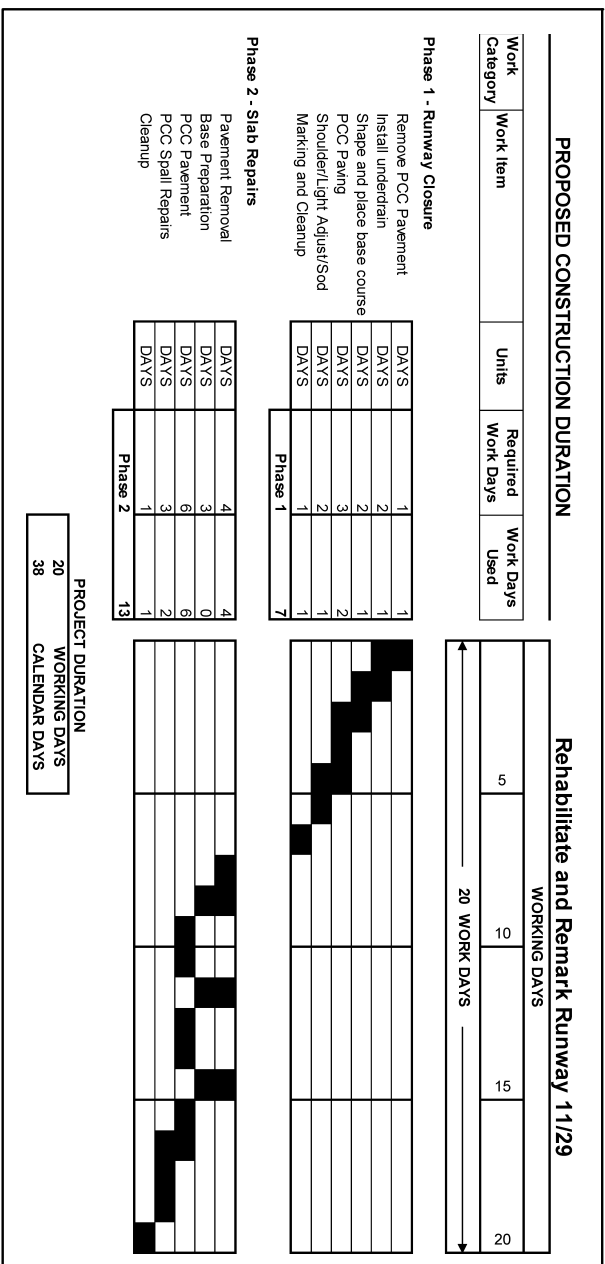
UNICOM RADIO FREQUENCY: 123.00 MHZ

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SUMMARY OF QUANTITIES

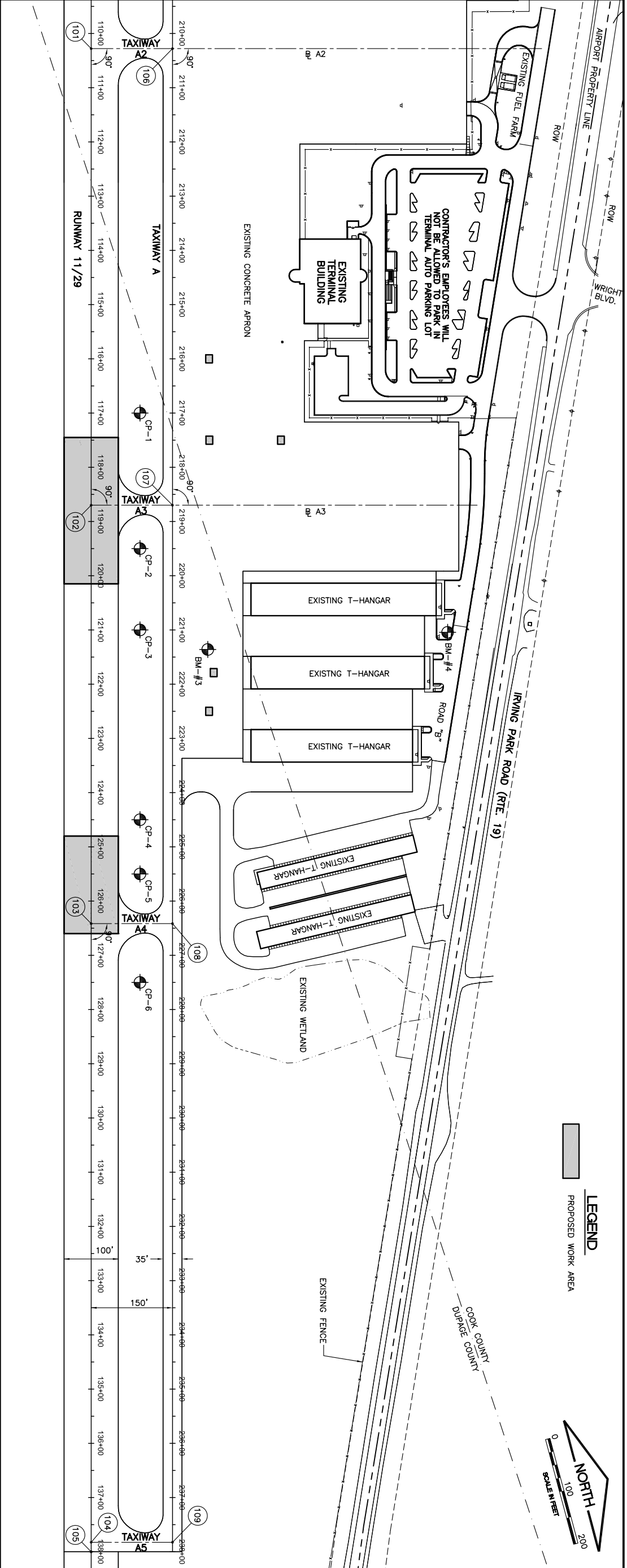
| ITEM NO. | DESCRIPTION | UNIT | TOTAL QUANTITY | RECORD QUANTITY |
|--|----------------------------------|------|----------------|-----------------|
| BASE BID - REHABILITATE AND REMARK RUNWAY 11/29 | | | | |
| AR125942 | ADJUST BASE MOUNTED LIGHT | EACH | 3 | |
| AR150520 | MOBILIZATION | LS | 1 | |
| AR152410 | UNCLASSIFIED EXCAVATION | CY | 2,940 | |
| AR152480 | SHOULDER ADJUSTMENT | SY | 1,190 | |
| AR156510 | SILT FENCE | LF | 625 | |
| AR208515 | POROUS GRANULAR EMBANKMENT | CY | 2,465 | |
| AR208604 | 4" AGGREGATE BASE COURSE | SY | 5,230 | |
| AR501506 | 6" PCC PAVEMENT | SY | 2,105 | |
| AR501508 | 8" PCC PAVEMENT | SY | 3,000 | |
| AR501530 | PCC TEST BATCH | EACH | 1 | |
| AR501900 | REMOVE PCC PAVEMENT | SY | 5,105 | |
| AR620520 | PAVEMENT MARKING - WATERBORNE | SF | 13,350 | |
| AR620525 | PAVEMENT MARKING - BLACK BORDER | SF | 3,200 | |
| AR705506 | 6" PERFORATED UNDERDRAIN | LF | 1,200 | |
| AR705610 | CONCRETE HEADWALL FOR UNDERDRAIN | EACH | 8 | |
| AR705924 | REPLACE UNDERDRAIN CLEANOUT | EACH | 1 | |
| AR800009 | SPALL REPAIR | SF | 150 | |
| AR904510 | SODDING | SY | 1,190 | |

| ADDITIVE ALTERNATE 1 - FURNISH AND INSTALL REINFORCING MESH FOR 6" PCC PAVEMENT | | | | |
|--|---------------------------------|----|-------|--|
| AS800021 | REINFORCING MESH - 6" PCC PAVMT | SY | 2,000 | |
| ADDITIVE ALTERNATE 2 - FURNISH AND INSTALL REINFORCING MESH FOR 8" PCC PAVEMENT | | | | |
| AT800022 | REINFORCING MESH - 8" PCC PAVMT | SY | 3,000 | |



- THE PROPOSED RUNWAY CLOSURE DURATION WAS DEVELOPED USING THE FOLLOWING ASSUMPTIONS:
 - REMOVAL OPERATIONS WOULD BEGIN IN ONE AREA AND BE COMPLETED PRIOR TO MOVING TO THE SECOND AREA, WITH BOTH AREAS BEING COMPLETED IN ONE DAY;
 - DURING REMOVAL OPERATIONS IN THE SECOND AREA, WORK TO COMPLETE THE UNDERCUT AND BACKFILL, INSTALL THE UNDERDRAIN AND PLACE AGGREGATE BASE COURSE WOULD BEGIN IN THE ALREADY REMOVED AREA;
 - PAVING LANES WOULD BE 25 FEET WIDE, REQUIRING FOUR PLACEMENT OPERATIONS WITH TWO OPERATIONS COMPLETED EACH DAY (450 CY ±);
 - THE SOUTHERN LANES WOULD BE PLACED FIRST IN BOTH AREAS;
 - JOINT SAWING AND SEALING WOULD BE CONCURRENT WITH SHOULDER ADJUSTMENT;
 - PAVEMENT MARKING OUTSIDE OF THE REPAIR AREAS WOULD BE COMPLETED DURING DAYS 1 THROUGH 6 OF THE RUNWAY CLOSURE;
- IN ORDER TO COMPLETE THE WORK WITHIN THE 7 CALENDAR DAY CLOSURE, IT IS ANTICIPATED THAT THE CONTRACTOR WILL BE REQUIRED TO WORK LONGER THAN 8 HOUR DAYS AND WITH MULTIPLE CREWS, WORK ON SATURDAYS AND SUNDAYS MAY BE REQUIRED TO COMPLETE THE REPAIRS WITHIN THE ALLOTTED CLOSURE TIME.
- AT A MINIMUM, THE PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS, INCLUDING JOINT SAWING AND SEALING, SHOULDER ADJUSTMENT, BASE MOUNTED LIGHT ADJUSTMENTS AND SODDING, MUST BE COMPLETED IN ORDER TO OPEN THE RUNWAY TO AIR TRAFFIC.
- PAVEMENT MARKING MAY BE COMPLETED USING A SHORTER DURATION CLOSURE. DAILY CLOSURES FROM 10 AM TO 4 PM TO COMPLETE THIS WORK SHALL BE SCHEDULED AT LEAST 3 BUSINESS DAYS IN ADVANCE THROUGH THE RESIDENT ENGINEER.
- AT HIS OPTION, THE CONTRACTOR SHALL BE ALLOWED TO COMPLETE THE PCC SLAB REPAIRS ON THE APRON AT THE SAME TIME AS THE RUNWAY REPAIRS PROVIDED HIS SEQUENCING DOES NOT CAUSE THE RUNWAY CLOSURE TO BE EXTENDED.
- THE CONTRACTOR SHALL NOT BE ALLOWED TO WORK ON MORE THAN TWO (2) OF THE APRON PCC SLAB REPAIRS AT ONE TIME. WORKING ON MORE THAN 2 AREAS AT ONE TIME REDUCES THE AVAILABLE AREA TO TAXING AIRCRAFT.
- THE 2012 G8 SUMMIT WILL BE HELD IN CHICAGO THE WEEK OF MAY 15TH THROUGH MAY 22ND. IT IS PROPOSED THAT THE RUNWAY BE CLOSED DURING THIS TIME DUE TO THE EXPECTED AIRSPACE RESTRICTIONS THAT WILL BE IN PLACE SURROUNDING CHICAGO.
- BY SUBMITTING HIS BID, THE CONTRACTOR AGREES THAT HE WILL BE ABLE TO COMPLETE THE RUNWAY REPAIRS DURING THE PROPOSED RUNWAY CLOSURE, BARRING WEATHER EVENTS, NATURAL DISASTERS AND DECLARED EMERGENCIES OUTSIDE OF HIS CONTROL.
- THE 2012 IDOT BID SCHEDULE INDICATES A WORK START DATE OF MAY 1, 2012. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT ALL MATERIAL CERTIFICATIONS BEGINNING THIS DATE FOR APPROVAL PRIOR TO MAY 15, 2012. THE PCC TEST BATCH SHALL BE REQUIRED TO BE COMPLETED AND APPROVED BETWEEN MAY 1ST AND MAY 15, 2012 IN ORDER TO BEGIN CONSTRUCTION ON TIME.
- PER THE IDOT-DOA WORKING DAY PER MONTH CALENDAR, 20 WORKING DAYS SHALL BE 38 CALENDAR DAYS DURING THE MONTHS OF MAY AND JUNE.

| | | | | | | |
|---|--|--|---|--|--|-----------------------|
| I.L. CONTRACT: SH025 I.L. LETTING ITEM: 2A A.I.P. PROJECT: 3-17-0124-828 | SURVEY BOOK # 11256-03-00 REVISIONS NUMBER BY DATE | 0 1 2 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22). | VILLAGE OF SCHAUMBURG SCHAUMBURG REGIONAL AIRPORT SCHAUMBURG, IL REHABILITATE AND REMARK RUNWAY 11/29 INDEX TO SHEETS, SUMMARY OF QUANTITIES AND PROPOSED PROJECT SCHEDULE | © Copyright CMT, Inc. CMT CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS License No. 184-000613 | DESIGN BY: SMS DRAWN BY: CMT CHECKED BY: DLP APPROVED BY: DLP DATE: 01/20/2012 JOB No: 11256-03 FINAL | SHEET 02 OF 15 SHEETS |
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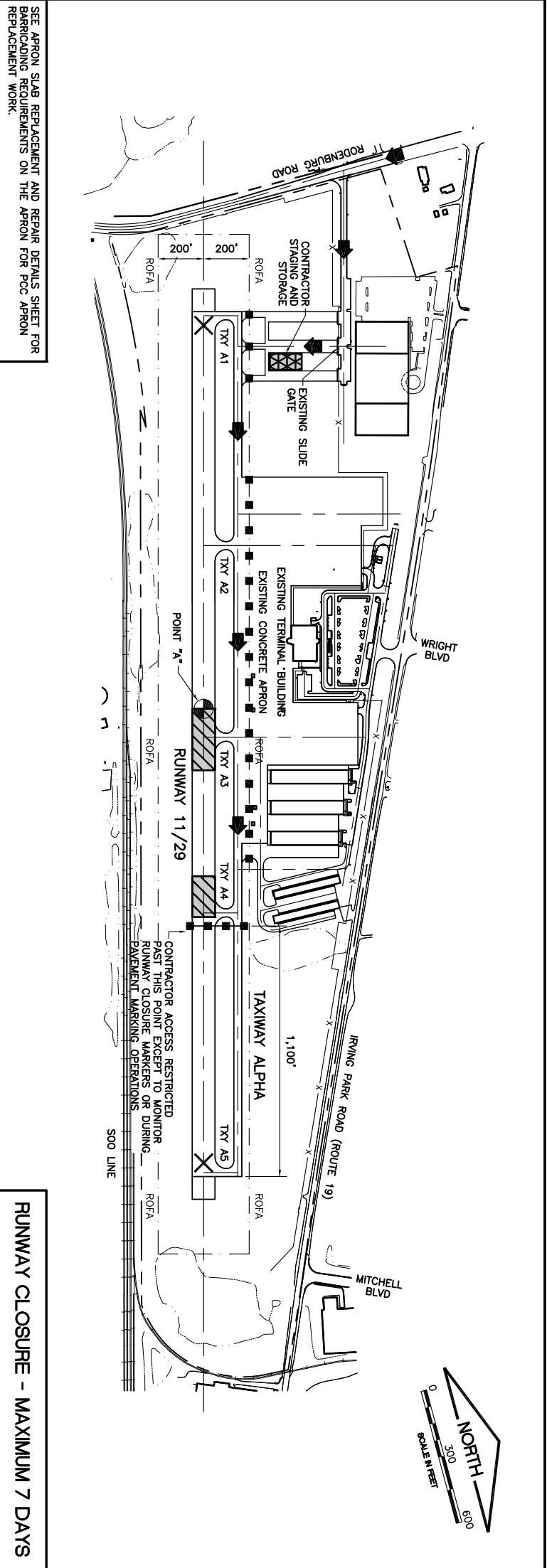
| POINT NUMBER | DESCRIPTION | NORTHING | EASTING | STATION | OFFSET |
|--------------|---|---------------|---------------|--------------------------|-----------|
| 101 | RUNWAY 11/29 AND TAXIWAY A2 | 1,939,016.834 | 1,046,503.336 | 110+27.88 @ RUNWAY 11/29 | 0.00' |
| 102 | RUNWAY 11/29 AND TAXIWAY A3 | 1,938,799.016 | 1,047,316.674 | 118+69.88 @ RUNWAY 11/29 | 0.00' |
| 103 | RUNWAY 11/29 AND TAXIWAY A4 | 1,938,599.394 | 1,048,062.067 | 126+41.54 @ RUNWAY 11/29 | 0.00' |
| 104 | RUNWAY 11/29 AND TAXIWAY A5 | 1,938,304.239 | 1,049,164.185 | 137+82.50 @ RUNWAY 11/29 | 0.00' |
| 105 | RUNWAY 29 END | 1,938,299.712 | 1,049,181.090 | 138+00.00 @ RUNWAY 11/29 | 0.00' |
| 106 | TAXIWAY A AND TAXIWAY A2 | 1,939,161.728 | 1,046,542.139 | 210+27.88 @ TAXIWAY A | 0.00' |
| 107 | TAXIWAY A AND TAXIWAY A3 | 1,938,943.910 | 1,047,355.478 | 218+69.88 @ TAXIWAY A | 0.00' |
| 108 | TAXIWAY A AND TAXIWAY A4 | 1,938,744.288 | 1,048,100.871 | 226+41.54 @ TAXIWAY A | 0.00' |
| 109 | TAXIWAY A AND TAXIWAY A5 | 1,938,449.133 | 1,049,202.989 | 237+82.50 @ TAXIWAY A | 0.00' |
| CP-1 | IRON ROD - MASTER PROJECT CONTROL POINT | 1,938,930.164 | 1,047,175.236 | 116+99.33 @ RUNWAY 11/29 | 90.10' LT |
| CP-2 | IRON ROD - MASTER PROJECT CONTROL POINT | 1,938,863.955 | 1,047,421.973 | 119+54.80 @ RUNWAY 11/29 | 89.97' LT |
| CP-3 | IRON ROD - MASTER PROJECT CONTROL POINT | 1,938,825.239 | 1,047,566.735 | 121+04.65 @ RUNWAY 11/29 | 90.19' LT |
| CP-4 | IRON ROD - MASTER PROJECT CONTROL POINT | 1,938,736.031 | 1,047,899.980 | 124+49.63 @ RUNWAY 11/29 | 90.06' LT |
| CP-5 | IRON ROD - MASTER PROJECT CONTROL POINT | 1,938,708.877 | 1,048,001.341 | 125+54.56 @ RUNWAY 11/29 | 90.05' LT |
| CP-6 | IRON ROD - MASTER PROJECT CONTROL POINT | 1,938,658.418 | 1,048,190.049 | 127+49.90 @ RUNWAY 11/29 | 90.12' LT |

HORIZONTAL CONTROL

VERTICAL CONTROL

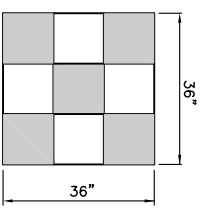
| BENCHMARK | DESCRIPTION | ELEVATION |
|-----------|--|-----------|
| BM - #1 | GPS SURVEY MONUMENT MARKED "1034", 2" ALUMINUM CAP GROUTED IN CONCRETE PAD ON SOUTHWEST CORNER OF ROSENBERG ROAD AND SOO LINE RAILROAD INTERSECTION. BENCHMARK "1034" IS A MONUMENT WHICH IS PART OF THE VILLAGE OF SCHAUMBURG "GENEX" BENCHMARK SYSTEM. (NOT SHOWN ON THIS SHEET) | 796.19 |
| BM - #2 | NGS F.A. O.C. POINT "A", IRON ROD STA. 111+36.148' RT @ RUNWAY 11/29 (NOT SHOWN ON THIS SHEET) | 793.81 |
| BM - #3 | CHISELED "X" ON CONCRETE PAD AT FUEL FARM STA. 221+36.65' LT @ TRV. A | 796.96 |
| BM - #4 | CHISELED "D" NORTH SIDE OF CONCRETE BASE FOR BEACON LIGHT STA. 24+80.09, 20.84' RT @ ROAD B | 804.41 |
| CP-1 | IRON ROD - PROJECT MASTER CONTROL POINT | 797.88 |
| CP-2 | IRON ROD - PROJECT MASTER CONTROL POINT | 798.30 |
| CP-3 | IRON ROD - PROJECT MASTER CONTROL POINT | 798.19 |
| CP-4 | IRON ROD - PROJECT MASTER CONTROL POINT | 798.05 |
| CP-5 | IRON ROD - PROJECT MASTER CONTROL POINT | 798.39 |
| CP-6 | IRON ROD - PROJECT MASTER CONTROL POINT | 798.48 |

| <p>ILL. CONTRACT: SH025 I.L. LETTING ITEM: 2A I.L. PROJECT: 06C-3808 A.I.P. PROJECT: 3-17-0124-828</p> | | <p>SURVEY BOOK # 11256-03-00</p> | | | | | | |
|---|----|---|--------|----|------|--|--|--|
| <p>VILLAGE OF SCHAUMBURG SCHAUMBURG REGIONAL AIRPORT SCHAUMBURG, IL</p> | | <p>REVISIONS</p> <table border="1"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | NUMBER | BY | DATE | | | |
| NUMBER | BY | DATE | | | | | | |
| | | | | | | | | |
| <p>REHABILITATE AND REMARK RUNWAY 11/29</p> | | <p>THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).</p> | | | | | | |
| <p>SITE PLAN AND PROJECT CONTROL PLAN</p> | | <p>Copyright CMT, Inc.</p> | | | | | | |
| <p>CMT CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS License No. 184-000613</p> | | <p>DESIGN BY: SMS DRAWN BY: CMT CHECKED BY: DLP APPROVED BY: DLP DATE: 01/20/2012 JOB No: 11256-03</p> | | | | | | |
| <p>SHEET 03 OF 15 SHEETS</p> | | <p>FINAL</p> | | | | | | |



SEE APRON SLAB REPLACEMENT AND REPAIR DETAILS SHEET FOR BARRICADING REQUIREMENTS ON THE APRON FOR PCC APRON REPLACEMENT WORK.

RUNWAY CLOSURE - MAXIMUM 7 DAYS



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
 NOT TO SCALE

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25 FEET

LEGEND

- EXISTING PCC PAVEMENT TO BE REMOVED AND REPLACED
- CONTRACTOR STAGING, STORAGE AND EMPLOYEE PARKING
- CONTRACTOR'S ACCESS TO SITE
- ROFA
- RUNWAY OBJECT FREE AREA
- EXISTING AIRFIELD FENCE
- LIGHTED RUNWAY CLOSURE MARKER
- TYPE II BARRICADE PLACEMENT
- AIRPORT PROPERTY LINE/RIGHT-OF-WAY (R.O.W.)

SUGGESTED SEQUENCE OF CONSTRUCTION

1. SUBMIT MATERIAL SOURCES AND APPROPRIATE SHOP DRAWINGS. COMPLETE PCC TEST BATCH.
2. LOCATE ALL UTILITIES.
3. MOBILIZE EQUIPMENT AND MATERIALS.
4. SETUP BARRICADES AND LIGHTED RUNWAY CLOSURE MARKERS. RUNWAY CLOSED FOR 7 CALENDAR DAYS MAXIMUM.
5. REMOVE EXISTING PCC PAVEMENT AS DETAILED.
6. COMPLETE UNDERCUT, PLACE PGE, INSTALL UNDERDRAIN AND PLACE AGGREGATE BASE COURSE.
7. PLACE PCC PAVEMENT.
8. GRADE TURF SHOULDER AND RESTORE DISTURBED AREAS TO ORIGINAL CONDITION. PLACE TOPSOIL ALONG PAVEMENT EDGE AND SOD.
9. INSTALL PAVEMENT MARKINGS.
10. REMOVE BARRICADES AND LIGHTED RUNWAY CLOSURE MARKERS. OPEN PAVEMENT TO AIRCRAFT.
11. COMPLETE APRON PCC PAVEMENT REPLACEMENTS AND SPALL REPAIRS.

LIMITATIONS ON CONSTRUCTION WITHIN OBJECT FREE AREA (OFA) OUTSIDE OF 7 DAY RUNWAY CLOSURE

1. WORK WITHIN THE OBJECT FREE AREA (O.F.A.) SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE R.S.A.
2. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO PASS, THEY SHALL DO SO AT NO ADDITIONAL COST TO THE CONTRACTOR.
3. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO (2) WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES, WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT.
4. THE RUNWAY AND/OR TAXIWAY WILL BE TEMPORARILY CLOSED WHEN CONSTRUCTION EQUIPMENT OR WORK IS WITHIN THE RUNWAY/TAXIWAY O.F.A. AT THE END OF EACH WORKING DAY THE RUNWAY/TAXIWAY SHALL BE REOPENED. THE COST OF REMOVING AND REPAIRING BARRICADES AND CLOSED RUNWAY MARKERS IS INCIDENTAL TO THE CONTRACT. THIS NOTE APPLIES TO WORK REQUIRING CLOSURE OF THE RUNWAY BEYOND THE 7 CALENDAR DAY CLOSURE.

PROJECT OVERVIEW

1. THE PROPOSED PROJECT INVOLVES THE REHABILITATION OF PCC PAVEMENT ON RUNWAY 11/29 AND THE APRON.
2. THE RUNWAY SHALL BE CLOSED NO LONGER THAN 7 CALENDAR DAYS. THE RUNWAY PAVEMENT REPLACEMENT MUST BE COMPLETED WITHIN THAT TIME.
3. HELICOPTER TRAFFIC SHALL BE ALLOWED DURING THE RUNWAY CLOSURE.

GENERAL NOTES

1. ALL CONSTRUCTION SEQUENCING AND OPERATIONS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF AC 150/5370-2F (LATEST EDITION) SAFETY ON AIRPORTS. THE CONTRACTOR SHALL MAINTAIN ADEQUATE PAVEMENT MAINTENANCE OF TRAFFIC AND TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY PAVEMENT MARKING, TEMPORARY BARRICADES, MARKING REMOVALS, THE MOVING AND MAINTENANCE OF BARRICADES, SIGNING, OBJECT FREE AREAS (O.F.A.) AND LATER AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. THE CONTRACTOR MUST STAGE CONSTRUCTION AROUND AIRPORT OPERATIONS. STAGING SHOWN IS SUGGESTED AND IS INTENDED TO PROVIDE THE CONTRACTOR WITH MAJOR WORK AREAS WHILE MINIMIZING DISRUPTIONS TO AIRPORT OPERATIONS. THE CONTRACTOR MAY USE ALTERNATE STAGING PLANS; HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE VILLAGE OF SCHAUMBURG AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS.
3. BARRICADES AT 15' CENTERS SHALL BE PLACED AT THE LOCATIONS SHOWN OR AS DIRECTED BY THE VILLAGE OF SCHAUMBURG IN CONSULTATION WITH THE RESIDENT ENGINEER. BARRICADES SHALL BE WEIGHED TO PREVENT BLOWING OVER. HAVE A FLASHING RED LIGHT HIGH VISIBILITY ORANGE FLASHING SIGNALS SHALL BE INSTALLED BETWEEN ALL BARRICADES.
4. THE VILLAGE OF SCHAUMBURG, IN CONSULTATION WITH THE RESIDENT ENGINEER, SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
5. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENTS, EXCEPT AS SHOWN OR WITH THE PRIOR APPROVAL OF THE VILLAGE OF SCHAUMBURG.
6. EXISTING AREAS BEYOND PROJECT LIMITS INCLUDING THE HAUL ROAD(S) AND STAGING AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE RESIDENT ENGINEER AND VILLAGE OF SCHAUMBURG.
7. EACH DAY AT THE COMPLETION OF WORK, OR MORE FREQUENTLY AS DETERMINED BY THE VILLAGE OF SCHAUMBURG, THE CONTRACTOR SHALL BE REQUIRED TO REPAIR AND MAINTAIN EXISTING AIRCRAFT OPERATING ROADS BEING USED FOR SITE ACCESS AND HAULING OPERATIONS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. THE COST OF SWEEPING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WITHIN THE LIMITS OF EACH PHASE AS DESIGNATED BY THE VILLAGE OF SCHAUMBURG WHEN CONSTRUCTION IS NOT IN PROGRESS. CONTRACTOR SHALL PLACE EQUIPMENT/MATERIAL STORAGE AND EMPLOYEE PARKING AREA WITHIN THE CONSTRUCTION SITE BOUNDARIES. AT NO TIME WILL THIS AREA BE ALLOWED ON EXISTING PAVEMENTS OR NEWLY CONSTRUCTED PROPOSED PAVEMENT.
9. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER. LIGHTING SHALL NOT BE USED IF THE AIRCRAFT LIGHT SAFETY OBSTACLES OR WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
10. MATERIALS REMOVED FROM THE PROJECT SHALL BE DISPOSED OF AT AN APPROVED SITE OFF THE AIRPORT PROPERTY.
11. THE CONTRACTOR SHALL CONTACT THE RESIDENT ENGINEER AND THE VILLAGE OF SCHAUMBURG A MINIMUM OF FIVE (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
12. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/TERMINAL BUILDING SHALL NOT BE USED.
13. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A STONE DELIVERY DUMP TRUCK, WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
14. ALL EXISTING ROADS USED AS A HAUL ROADS BY THE CONTRACTOR SHALL BE RESTORED TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND VILLAGE OF SCHAUMBURG. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING HAUL ROADS SHALL BE INCIDENTAL TO THE CONTRACT.
15. DAILY JOBSITE MEETINGS SHALL BE HELD TO COORDINATE THE WORK, PARTICULARLY RELATING TO ANY ACTIVITIES THAT MAY IMPACT OR INTERFERE WITH OTHER CONTRACTORS AND AIRPORT OPERATIONS.
16. OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH ORANGE FLAGS AND BARRICADES THAT SHALL BE WEIGHED TO PREVENT BLOWING OVER AND HAVE A FLASHING RED LIGHT, CONFORMING TO DOT STANDARD 701901-02, TYPE II DURING THE HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO UNAUTHORIZED AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND OR AGENTS.
18. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS.
19. THE CONTRACTOR SHALL COORDINATE WORK ON ALL CONCURRENT PROJECTS, WHICH MAY ARISE, AND COORDINATE WITH THE VILLAGE OF SCHAUMBURG AND AIRPORT OPERATIONS. CONFLICTS WILL BE CONSIDERED. MAINTENANCE OF TRAFFIC OR WORK CONFLICTS WILL BE CONSIDERED.
20. SHOULD THE CONTRACTOR NOT BE ABLE TO COMPLETE ALL WORK WITHIN THE 7 DAY RUNWAY CLOSURE BUT THE RUNWAY IS CAPABLE OF BEING OPENED, THE REMAINING WORK CAN BE COMPLETED USING PARTIAL DAY CLOSURES. PARTIAL DAY CLOSURES SHALL BE COORDINATED WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER. EXAMPLES OF SUCH WORK INCLUDE REMARKING THE RUNWAY AND PLACING SOI.
21. THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACONS (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT, EXCEPT HAUL VEHICLES, AT ALL TIMES DURING CONSTRUCTION.
22. IN THE CASE OF AN EMERGENCY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
23. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORKSITE.

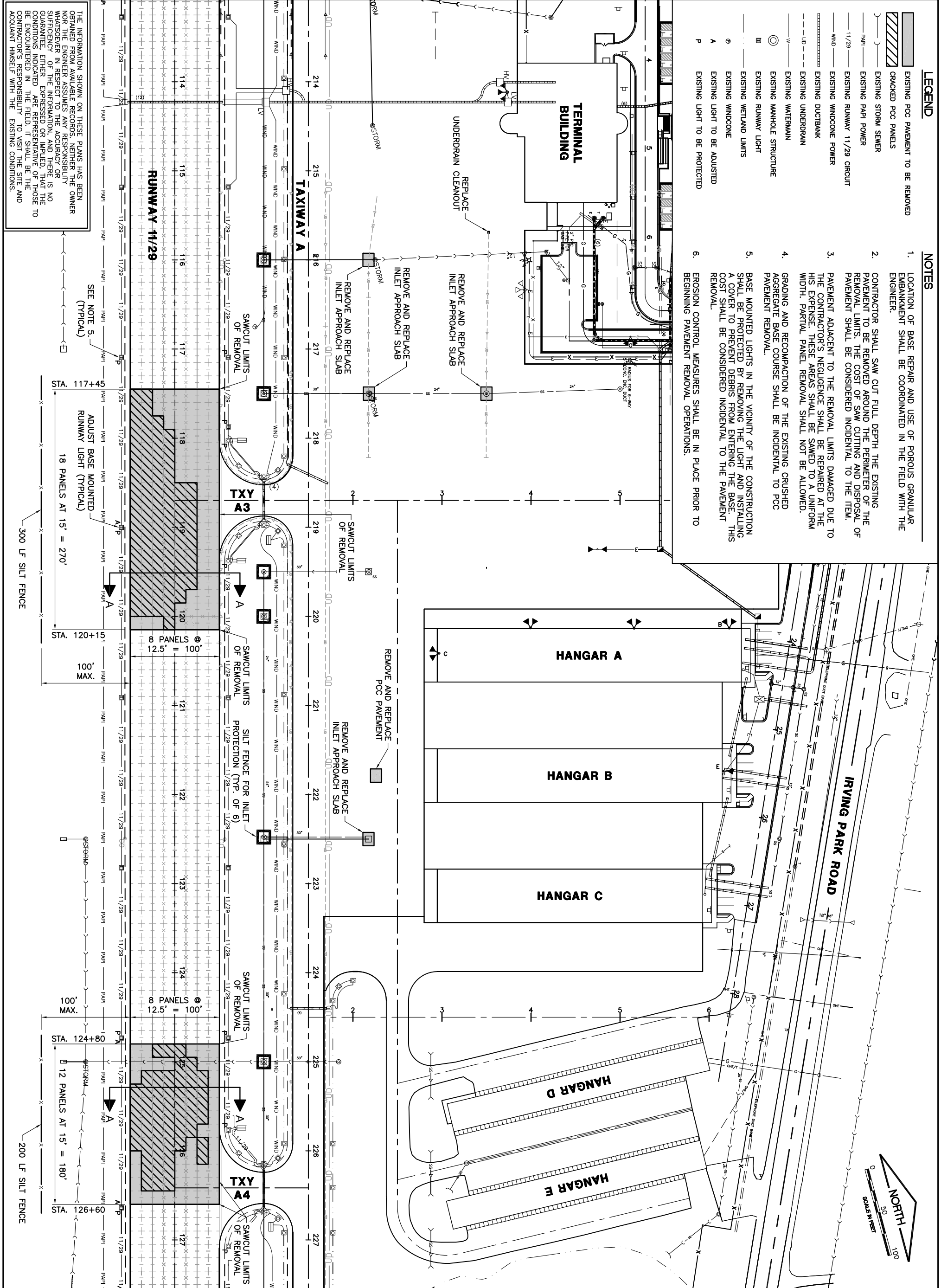
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|--|
| DESIGN AIRCRAFT APPROACH CATEGORY: B |
| DESIGN AIRCRAFT GROUP: I |
| CLOSEST POINT ON CONSTRUCTION SITE TO RUNWAY 11/29 POINT "A" LATITUDE: 41°59'22.02704" LONGITUDE: 88°06'06.45459" |

ADDITIONAL RUNWAY/TAXIWAY CLOSURE APPLIES TO CLOSURES BEYOND 7 DAY CLOSURE
 WORK HOURS WITHIN THE OBJECT FREE AREA (200' FROM CENTERLINE RUNWAY AND 44.5' FROM CENTERLINE TAXIWAY) MUST BE SUBMITTED FOR APPROVAL TO THE OWNER 14 DAYS BEFORE CONSTRUCTION BEGINS. WORK SHALL BE EXPEDITED WITHIN THE OBJECT FREE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW RUNWAY/TAXIWAY TO BE REOPENED TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

| I.L. CONTRACT: SH025 I.L. LETTING ITEM: 2A I.L. PROJECT: 06C-3808 A.I.P. PROJECT: 3-17-0124-828 | SURVEY BOOK # 11256-03-00 REVISIONS <table border="1"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | NUMBER | BY | DATE | | | | VILLAGE OF SCHAUMBURG SCHAUMBURG REGIONAL AIRPORT SCHAUMBURG, IL | REHABILITATE AND REMARK RUNWAY 11/29 SEQUENCE OF CONSTRUCTION PER AC 150/5370-2F (LATEST EDITION) | © Copyright CMT, Inc. CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS License No. 184-000613 |
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| LEGEND | |
|--------|-------------------------------------|
| | EXISTING PCC PAVEMENT TO BE REMOVED |
| | CRACKED PCC PANELS |
| | EXISTING STORM SEWER |
| | EXISTING PAPI POWER |
| | EXISTING RUNWAY 11/29 CIRCUIT |
| | EXISTING WINDCONE POWER |
| | EXISTING DUCTBANK |
| | EXISTING UNDERDRAIN |
| | EXISTING WATERMAIN |
| | EXISTING MANHOLE STRUCTURE |
| | EXISTING RUNWAY LIGHT |
| | EXISTING WETLAND LIMITS |
| | EXISTING WINDCONE |
| | EXISTING LIGHT TO BE ADJUSTED |
| | EXISTING LIGHT TO BE PROTECTED |

- NOTES**
- LOCATION OF BASE REPAIR AND USE OF POROUS GRANULAR EMBANKMENT SHALL BE COORDINATED IN THE FIELD WITH THE ENGINEER.
 - CONTRACTOR SHALL SAW CUT FULL DEPTH THE EXISTING PAVEMENT TO BE REMOVED AROUND THE PERIMETER OF THE REMOVAL LIMITS. THE COST OF SAW CUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
 - PAVEMENT ADJACENT TO THE REMOVAL LIMITS DAMAGED DUE TO THE CONTRACTOR'S NEGLIGENCE SHALL BE REPAIRED AT THE HIS EXPENSE. THESE AREAS SHALL BE SAWED TO A UNIFORM WIDTH. PARTIAL PANEL REMOVAL SHALL NOT BE ALLOWED.
 - GRADING AND RECOMPACT OF THE EXISTING CRUSHED AGGREGATE BASE COURSE SHALL BE INCIDENTAL TO PCC PAVEMENT REMOVAL.
 - BASE MOUNTED LIGHTS IN THE VICINITY OF THE CONSTRUCTION SHALL BE PROTECTED BY REMOVING THE LIGHT AND INSTALLING A COVER TO PREVENT DEBRIS FROM ENTERING THE BASE. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL.
 - EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO BEGINNING PAVEMENT REMOVAL OPERATIONS.



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION, AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS SHOWN ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT IS THE SITE AND CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACCOUNT HIMSELF WITH THE EXISTING CONDITIONS.

SEE NOTE 5 (TYPICAL)

ADJUST BASE MOUNTED RUNWAY LIGHT (TYPICAL)
 18 PANELS AT 15' = 270'

300 LF SILT FENCE

100' MAX.

100' MAX.

100' MAX.

12 PANELS AT 15' = 180'

200 LF SILT FENCE

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 APPROVED BY: DLP
 DATE: 01/20/2012
 JOB No: 11256-03

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SHEET 05 OF 15 SHEETS

VILLAGE OF SCHAUMBURG
 SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, IL

REHABILITATE AND REMARK RUNWAY 11/29

EXISTING CONDITIONS
 AND PROPOSED REMOVALS

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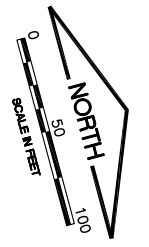
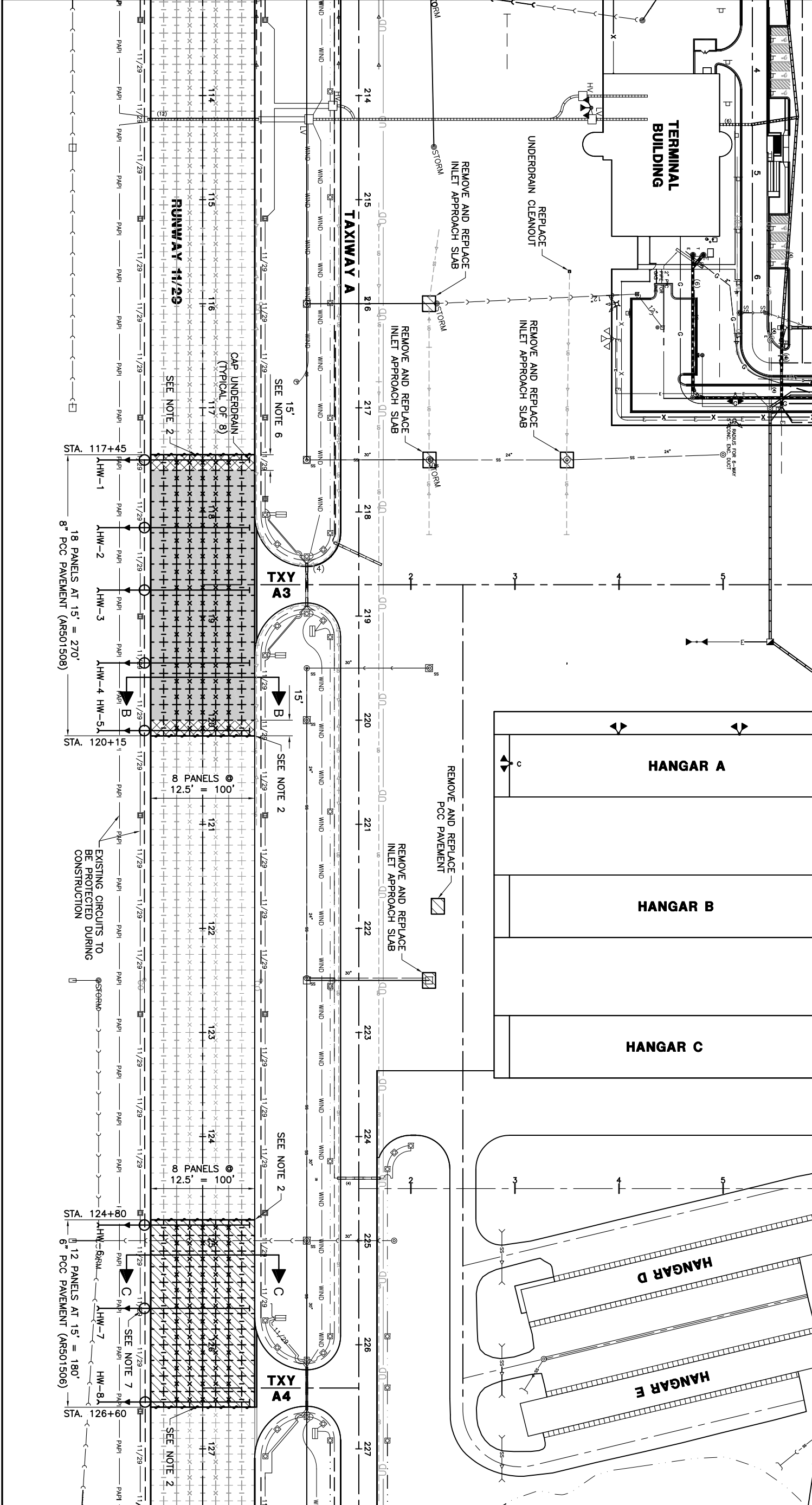
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| IL CONTRACT: SH025 |
| IL LETTING ITEM: 2A |
| IL PROJECT: 06C-3808 |
| A.I.P. PROJECT: 3-17-0124-828 |
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LEGEND

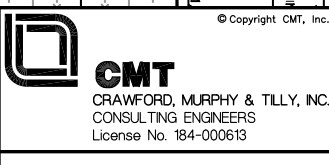
- TYPE B THICKENED EDGE EXPANSION JOINT
- TYPE D DOWELED CONSTRUCTION JOINT
- TYPE E TIED CONSTRUCTION JOINT
- TYPE F DOWELED CONSTRUCTION JOINT
- TYPE H DUMMY CONTRACTION JOINT
- TYPE G TIED CONTRACTION JOINT
- TYPE S DOWELED CONSTRUCTION JOINT
- NEW 6" PCC PAVEMENT
- NEW 6" TO 8" PCC PAVEMENT
- NEW 8" PCC PAVEMENT
- NEW 6" PERFORATED PVC UNDERDRAIN
- NEW CONCRETE HEADWALL FOR UNDERDRAIN

NOTES

1. SEE UNDERDRAIN DETAILS SHEET FOR UNDERDRAIN SCHEDULE.
2. THE INTERFACE BETWEEN THE EXISTING AND NEW PAVEMENT SHALL BE A DOWELED CONSTRUCTION JOINT. CARE SHALL BE TAKEN NOT TO DAMAGE THE EXISTING PAVEMENT DURING DRILLING OPERATIONS.
3. ALL JOINTS TO BE SEALED WITH SILICONE JOINT SEALER.
4. CHANGES TO THE JOINT LAYOUT PLAN SHALL BE REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO THE BEGINNING OF PAVING.
5. ANY PAVEMENT ADJACENT TO THE REMOVAL LIMITS DAMAGED DUE TO THE CONTRACTOR'S NEGLIGENCE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THESE AREAS SHALL BE SAVED TO A UNIFORM WIDTH.
6. THE TRANSITION FROM 6" TO 8" PCC PAVEMENT SHALL BE PAID UNDER ITEM AR501508 8" PCC PAVEMENT.
7. WHERE POSSIBLE, CONNECT EXISTING UNDERDRAIN TO PROPOSED UNDERDRAIN.

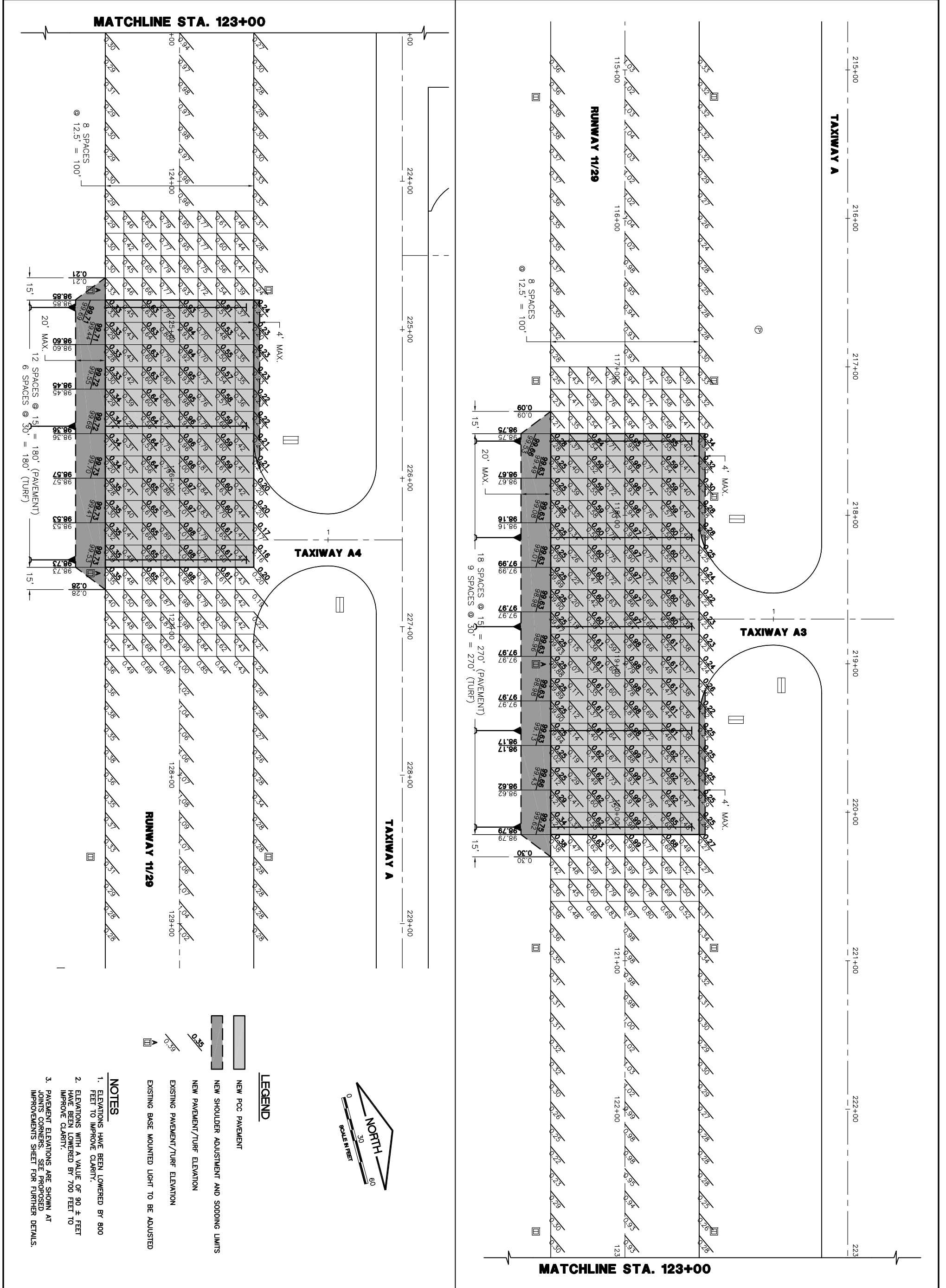


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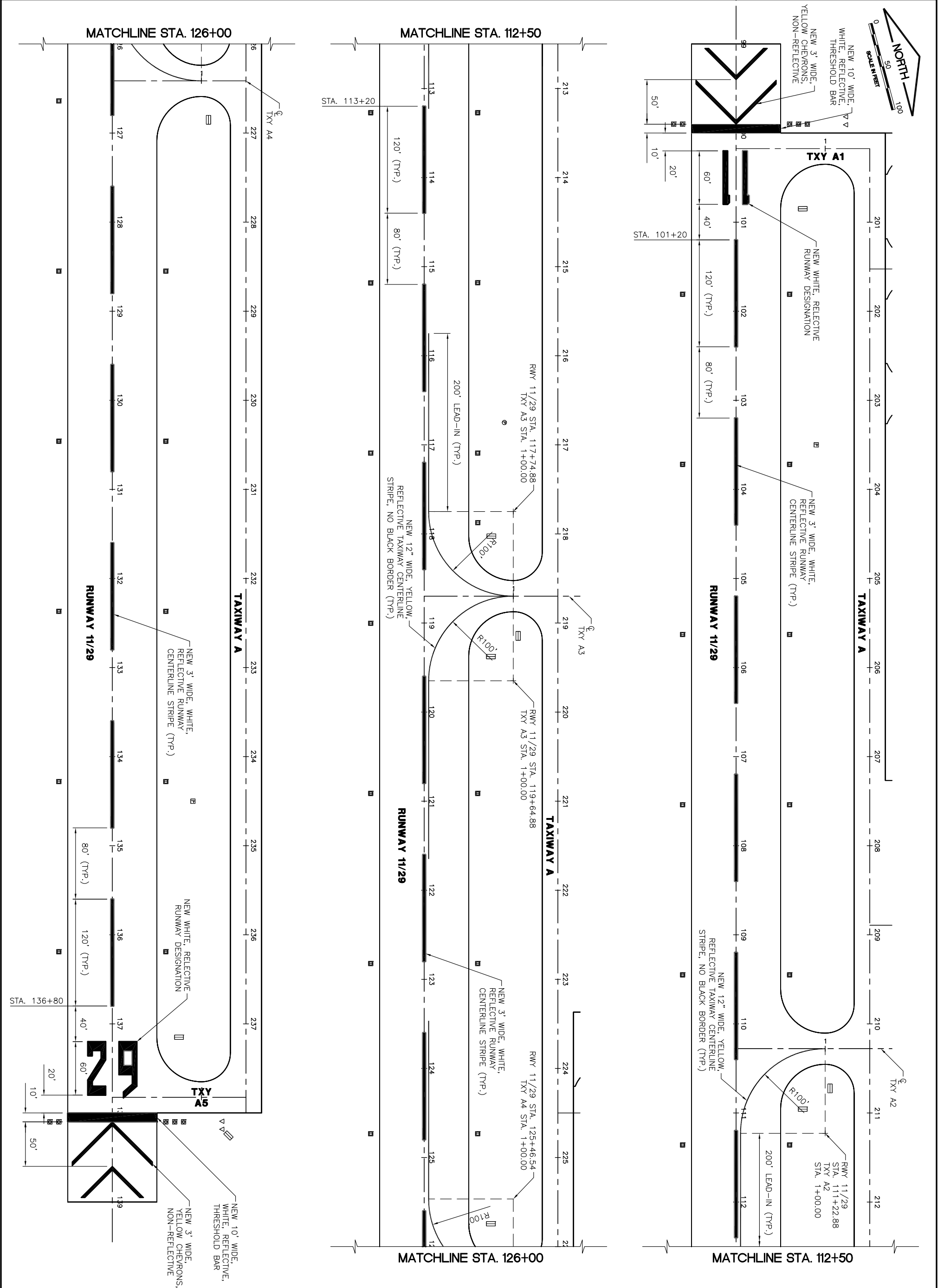


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SHEET 06 OF 15 SHEETS



| <p>IL. CONTRACT: SH025 IL. LETTING ITEM: 2A IL. PROJECT: 06C-3808 A.I.P. PROJECT: 3-17-0124-828</p> | <p>SURVEY BOOK # 11256-03-00</p> <table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th colspan="3">REVISIONS</th> </tr> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | REVISIONS | | | NUMBER | BY | DATE | | | | | | | <p>VILLAGE OF SCHAUMBURG SCHAUMBURG REGIONAL AIRPORT SCHAUMBURG, IL</p> <p>REHABILITATE AND REMARK RUNWAY 11/29</p> <p>PROPOSED GRADING PLAN</p> | <p>0 1 2 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).</p> | <p>© Copyright CMT, Inc.</p> <p>CMT CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS License No. 184-000613</p> |
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| <p>DESIGN BY: SWS DRAWN BY: CMT CHECKED BY: DLP DATE: 01/20/2012 JOB No: 11256-03</p> <p style="text-align: center; font-weight: bold; font-size: 1.2em;">FINAL</p> | <p>SHEET 07 OF 15 SHEETS</p> | | | | | | | | | | | | | | | |



IL CONTRACT: SH025
 IL LETTING ITEM: 2A
 A.I.P. PROJECT: 3-17-0124-828

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VILLAGE OF SCHAUMBURG
 SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, IL

REHABILITATE AND REMARK RUNWAY 11/29
 PROPOSED PAVEMENT MARKING PLAN

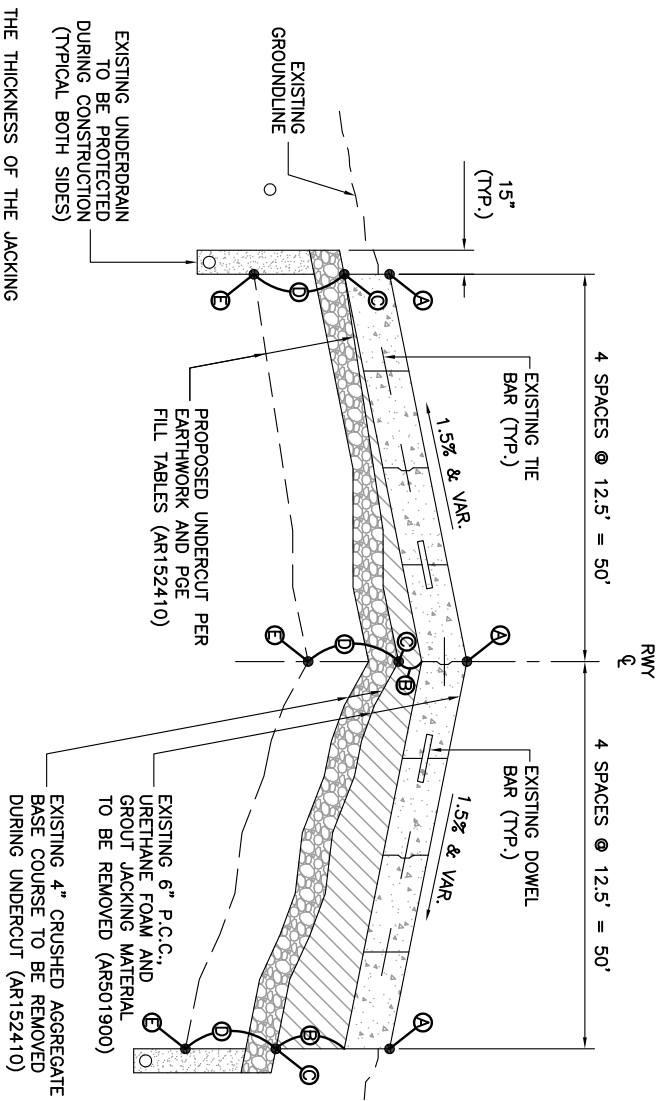
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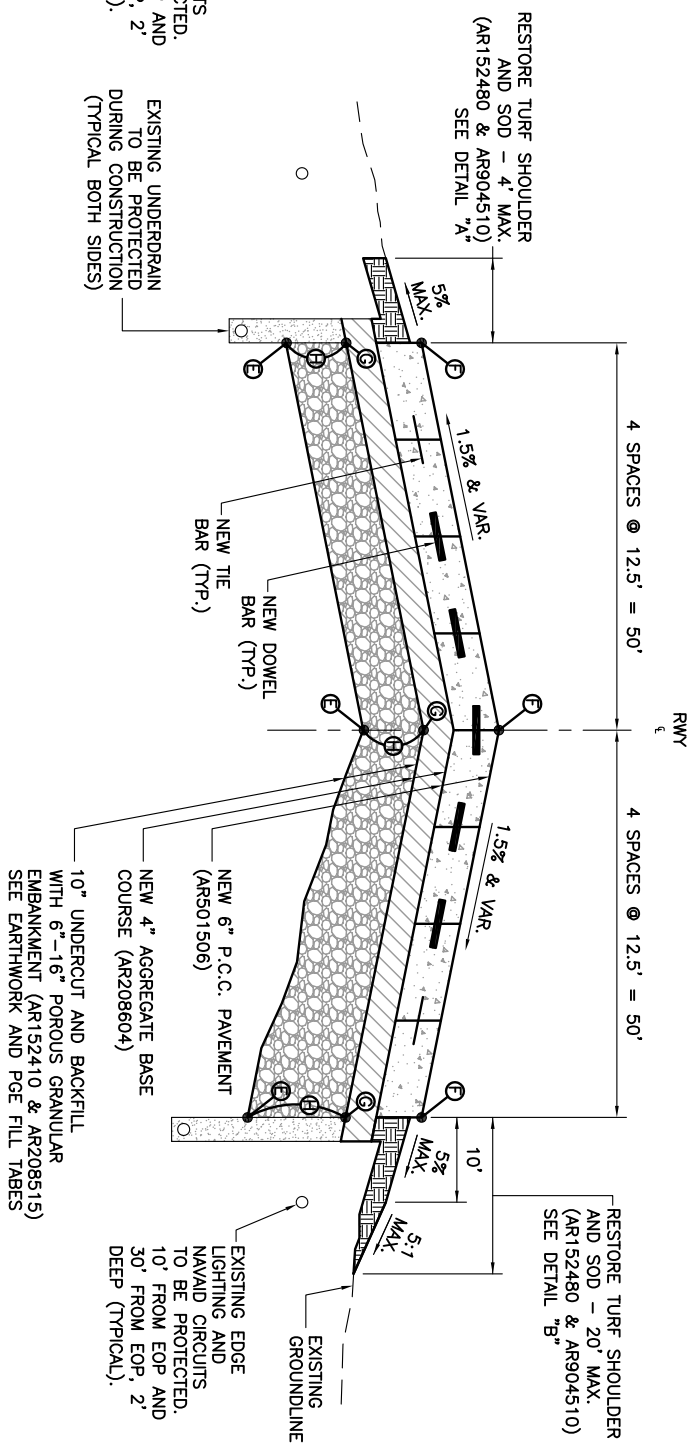
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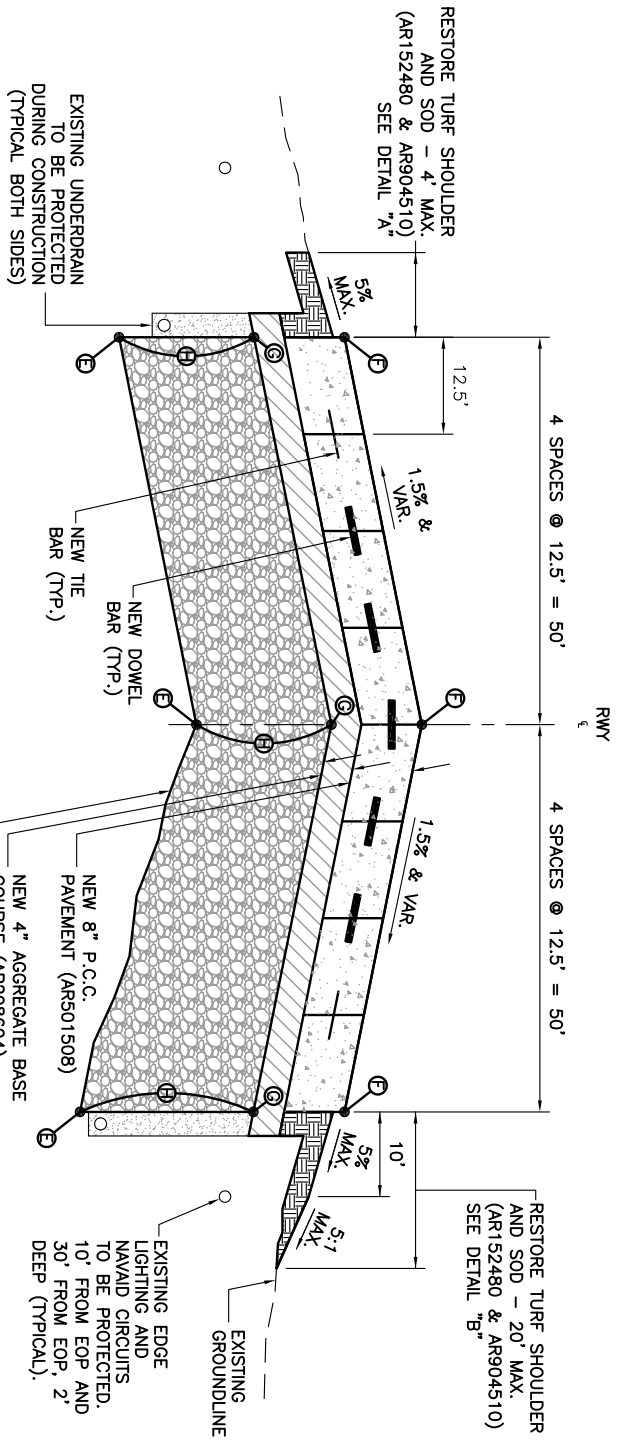
SHEET 08 OF 15 SHEETS



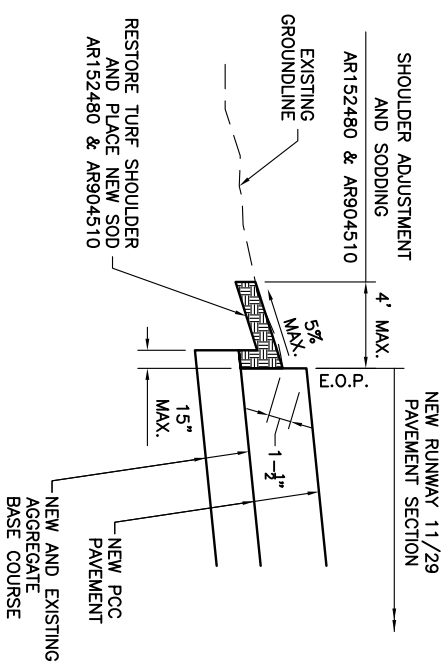
SECTION A-A
RUNWAY 11/29 - EXISTING TYPICAL SECTION
STA. 117+45 TO STA. 120+15 AND STA. 124+80 TO STA. 126+60
 N.T.S.



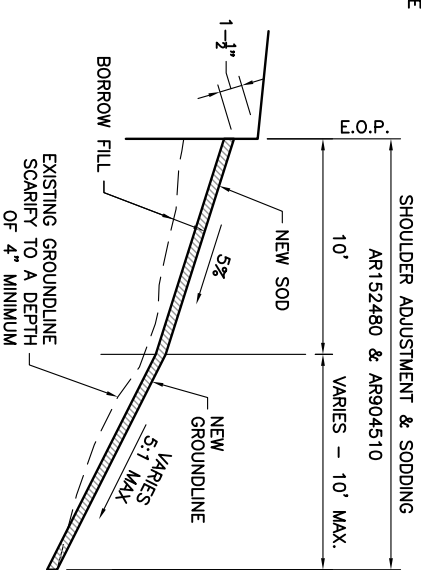
SECTION C-C
STA. 124+80 TO STA. 126+60
RUNWAY 11/29 - PROPOSED TYPICAL SECTION
 N.T.S.



SECTION B-B
STA. 117+45 TO STA. 120+15
RUNWAY 11/29 - PROPOSED TYPICAL SECTION
 N.T.S.



TURF SHOULDER DETAIL 'A'
 N.T.S.



TURF SHOULDER DETAIL 'B'
 N.T.S.

IL. CONTRACT: **SH025**
 IL. LETTING ITEM: **2A**
 IL. PROJECT: **06C-3808**
 A.I.P. PROJECT: **3-17-0124-828**

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VILLAGE OF SCHAUMBURG
 SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, IL

REHABILITATE AND REMARK RUNWAY 11/29

TYPICAL SECTIONS

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| FINAL | |
| SHEET | 09 OF 15 SHEETS |

TABLE 1

| PAVEMENT THICKNESS T - INCHES | DEPTH OF CONTRACTION JOINT INITIAL SAW CUT ±1/4" INCHES |
|-------------------------------|---|
| 5 | 1.25" |
| 6 | 1.50" |
| 7 | 1.75" |
| 8 | 2.00" |
| 9 | 2.25" |
| 10 | 2.50" |
| 11 | 2.75" |
| 12 | 3.00" |
| 13 | 3.25" |
| 14 | 3.50" |
| 15 | 3.75" |
| 16 | 4.00" |
| 17 | 4.25" |
| 18 | 4.50" |

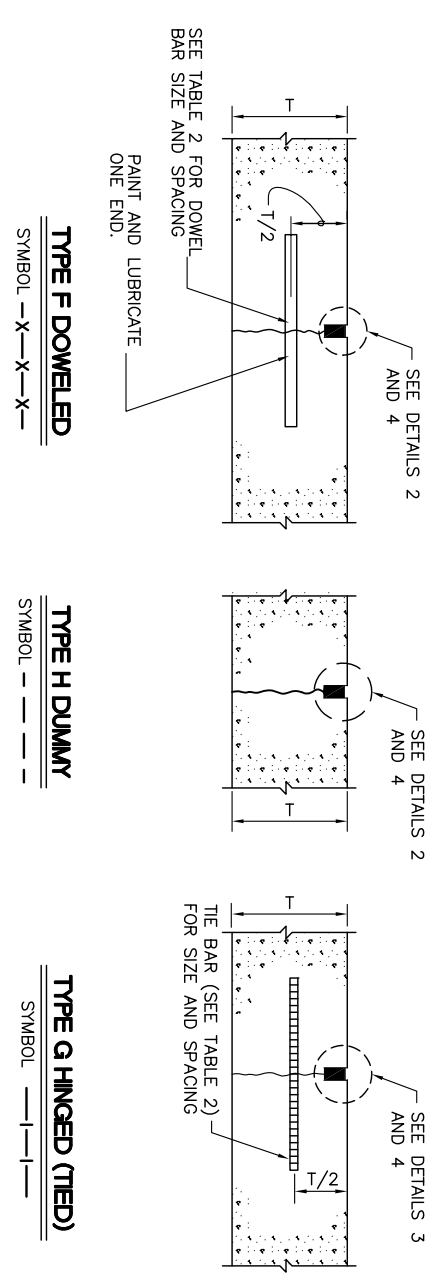
TABLE 2

| PAVEMENT THICKNESS T - INCHES | DOWEL BAR DETAILS | | | TIE BAR DETAILS | | |
|-------------------------------|-------------------|--------|---------|-----------------|--------|---------|
| | DIA. | LENGTH | SPACING | BAR SIZE | LENGTH | SPACING |
| 5 | 5/8" | 12" | 12" | #4 | 24" | 30" |
| 6 | 3/4" | 18" | 12" | #5 | 30" | 30" |
| 7 | 3/4" | 18" | 12" | #5 | 30" | 30" |
| 8 | 1" | 19" | 12" | #5 | 30" | 30" |
| 9 | 1" | 19" | 12" | #5 | 30" | 30" |
| 10 | 1" | 19" | 12" | #5 | 30" | 30" |
| 11 | 1" | 19" | 12" | #5 | 30" | 30" |
| 12 | 1" | 19" | 12" | #5 | 30" | 30" |
| 13 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 14 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 15 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 16 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 17 | 1 - 1/2" | 20" | 18" | #5 | 30" | 30" |
| 18 | 1 - 1/2" | 20" | 18" | #5 | 30" | 30" |

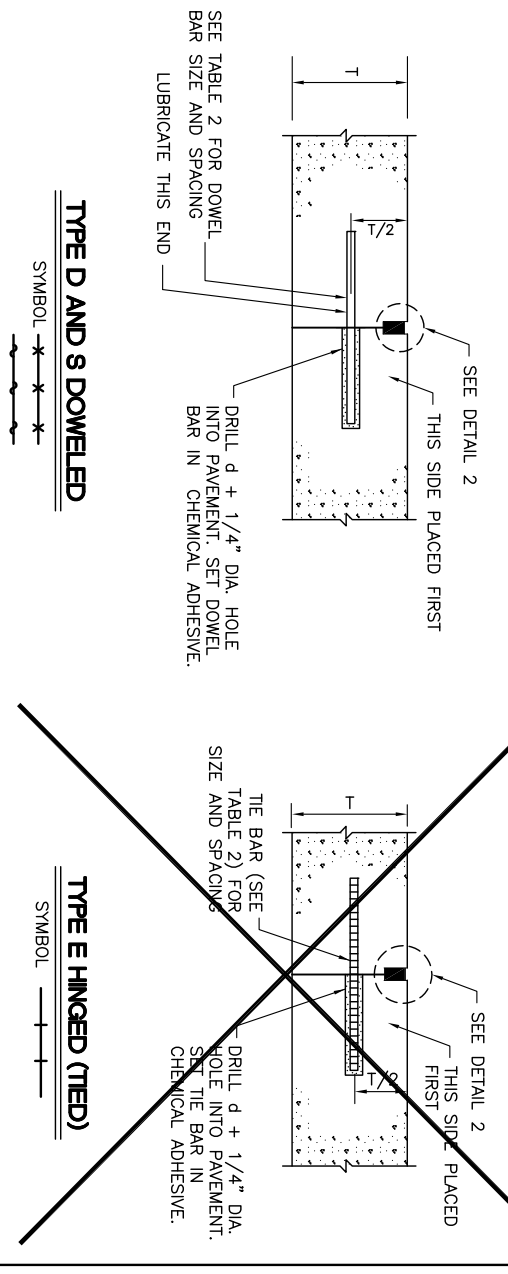
JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- THE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTEND DURING CONSTRUCTION. THE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COSTS OF ALL JOINT SAWING, CLEANING AND SEALING NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- DOWEL BASKET ASSEMBLES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLES SHALL CONFORM TO TABLE 2.
- ALL SEALANT TO BE DOW CORNING 888 SILICONE JOINT SEALANT, GUN GRADE.

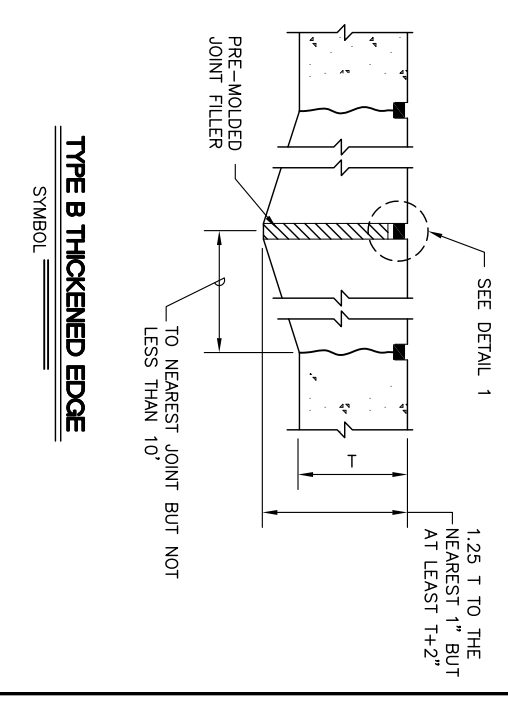
CONTRACTION JOINTS



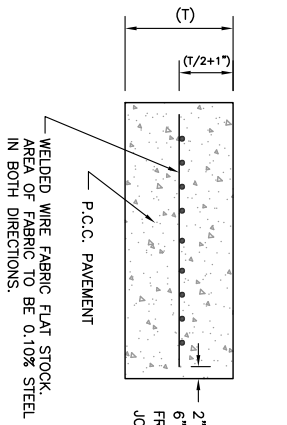
CONSTRUCTION JOINTS



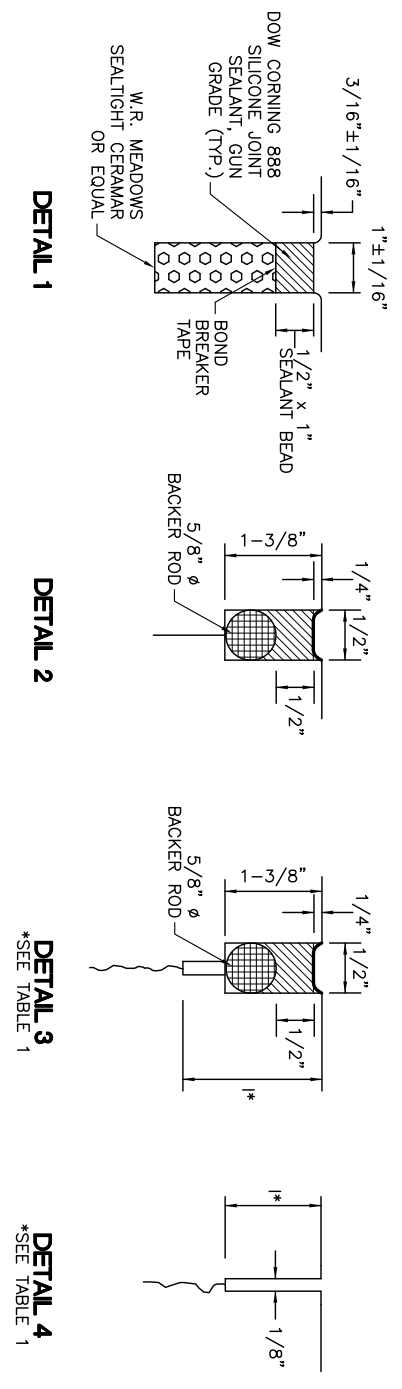
EXPANSION JOINTS



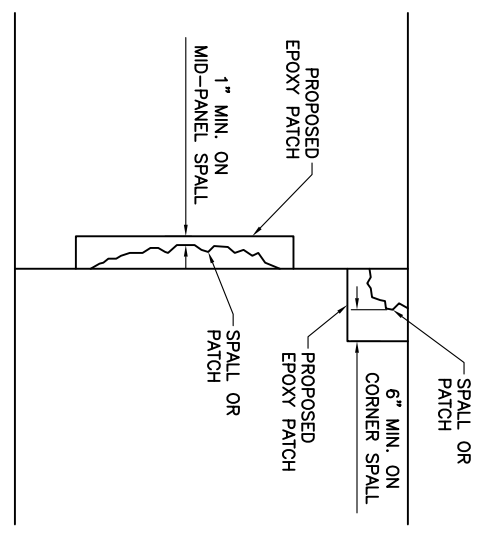
PANEL REINFORCEMENT ADDITIVE ALTERNATE 1 AND 2



JOINT SEALING DETAILS



SPALL REPAIR DETAIL

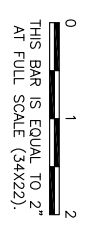


NOTES

- SAWCUT SHALL BE AT LEAST 1" FROM EDGE OF SPALL.
- DEPTH OF CUT SHALL BE AT LEAST 2".
- CONTRACTOR SHALL NOT OVERSAW.
- EXACT LOCATION OF SPALL/PATCHES TO BE REPLACED WILL BE DETERMINED IN THE FIELD BY RESIDENT ENGINEER.

ILL. CONTRACT: SH025
 ILL. LETTING ITEM: 2A
 ILL. PROJECT: 06C-3808
 A.I.P. PROJECT: 3-17-0124-828

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VILLAGE OF SCHAUMBURG
 SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, IL

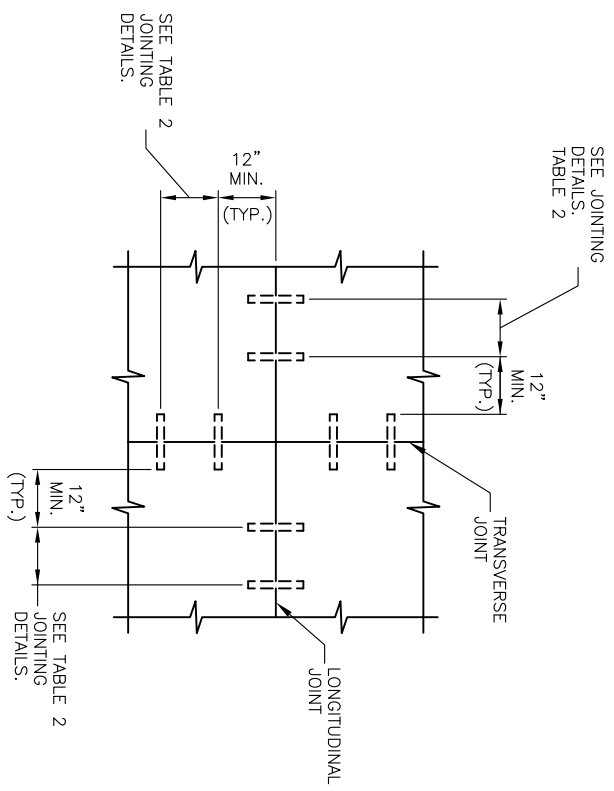
**REHABILITATE AND REMARK RUNWAY 11/29
 JOINTING DETAILS**

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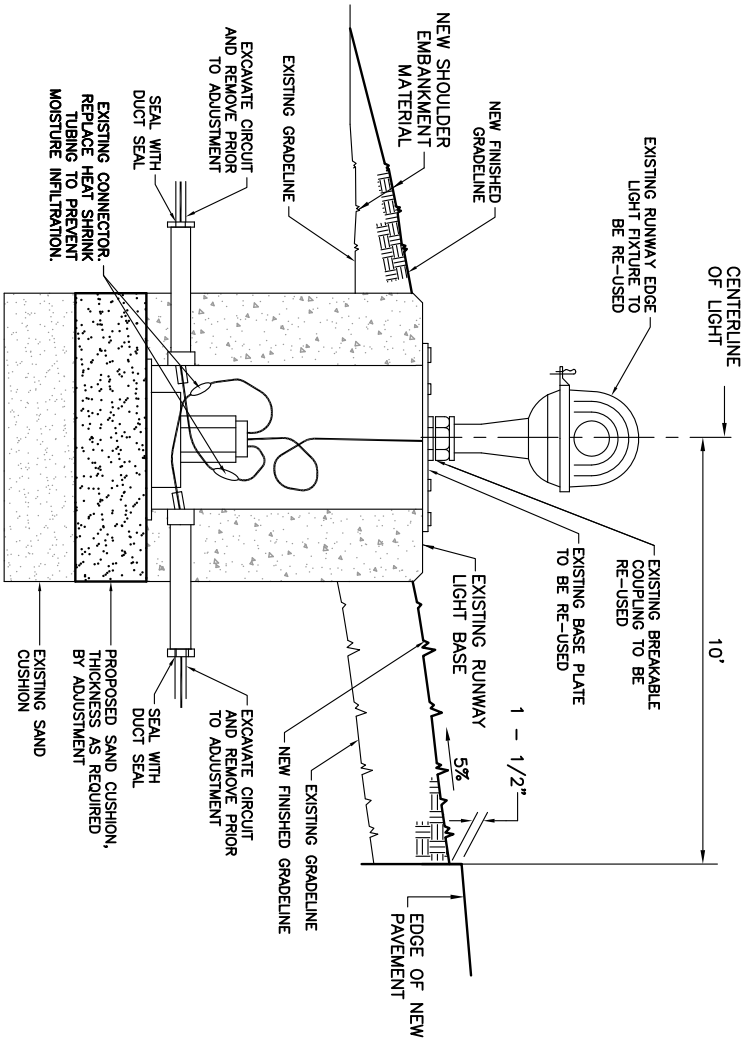
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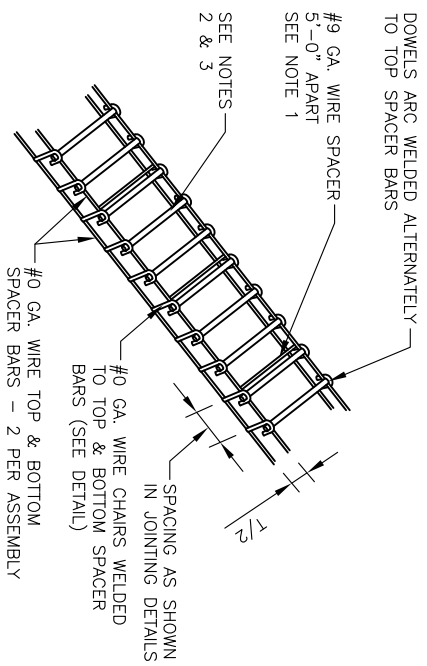
DOWEL INSTALLATION AT JOINT CORNERS DETAIL
 N.T.S.



METHOD OF CONSTRUCTION

1. UNPLUG AND REMOVE FIXTURE. ADJUST SHOULDERS TO GRADE.
2. EXCAVATE BASE MOUNTED LIGHT AND LIGHTING CIRCUIT.
3. REMOVE LIGHTING CIRCUIT FROM BASE CAN.
4. PLACE AND COMPACT SAND CUSHION AS REQUIRED BY AMOUNT OF ADJUSTMENT.
5. REPLACE BASE CAN AND REPLACE LIGHTING CIRCUIT. SEAL CABLE ENTRANCES WITH DUCT SEAL.
6. PLUG FIXTURE BACK INTO CIRCUIT AND REPLACE HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE OVER ENTIRE CONNECTOR (2 PER FIXTURE).
7. BACKFILL EXCAVATED EARTH MATERIALS AND COMPACT.

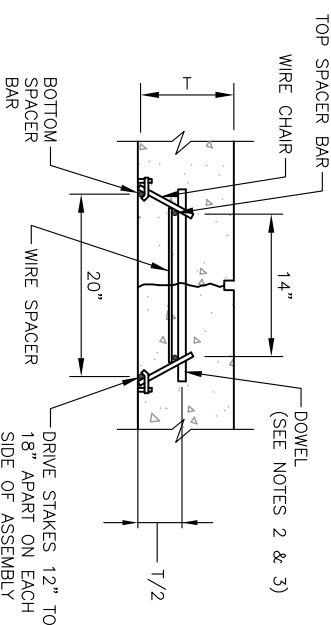
BASE MOUNTED LIGHT ADJUSTMENT
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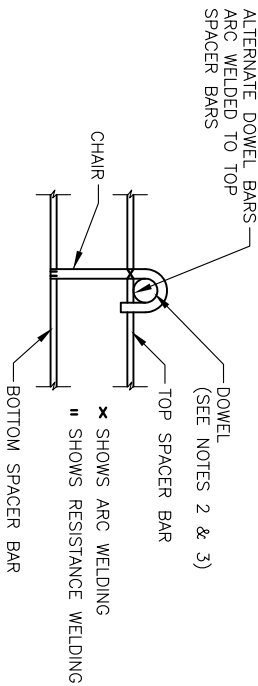
DOWEL BASKET ASSEMBLY DETAIL
 N.T.S.

DOWEL BASKET NOTES

- 1.) #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING). 3 REQUIRED PER UNIT.
- 2.) DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2 OF JOINTING DETAILS SHEET.
- 3.) DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL ONLY.

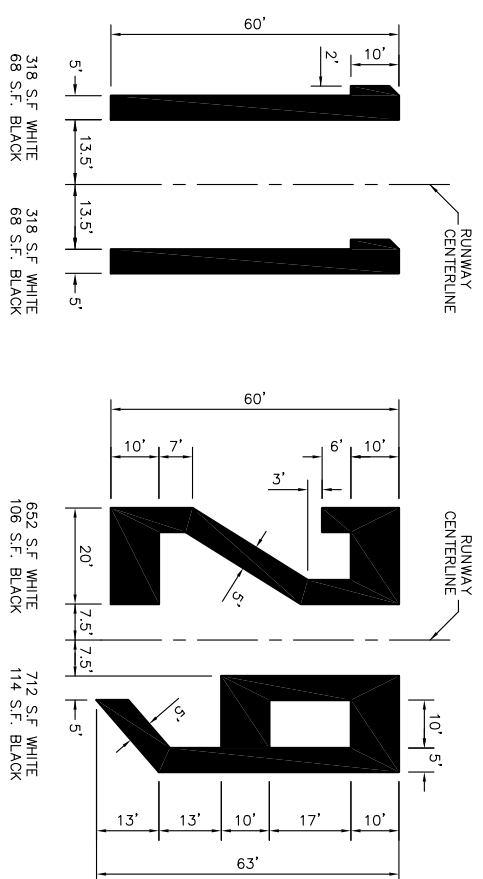


DOWEL BAR INSTALLATION DETAIL
 N.T.S.



TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR
 N.T.S.

DOWEL BASKET DETAILS
 N.T.S.

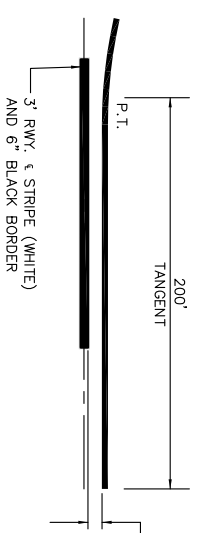


RUNWAY DESIGNATION MARKING DETAILS
 N.T.S.

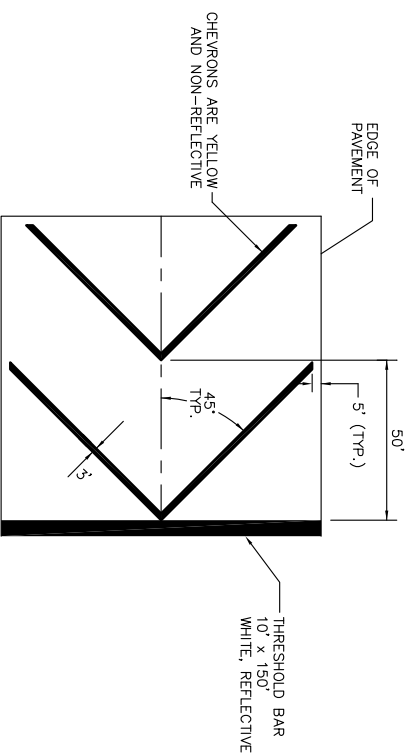


TAXIWAY CENTERLINE CONTINUOUS
 N.T.S.

RUNWAY CENTERLINE - DASHED
 N.T.S.



DETAIL TAXIWAY EXIT MARKING
 N.T.S.



NOTE:
 6\"/>

DETAIL - CHEVRON
 N.T.S.

PAVEMENT MARKING DETAILS

IL. CONTRACT: **SH025**
 IL. LETTING ITEM: **2A**
 A.I.P. PROJECT: **3-17-0124-828**

SURVEY BOOK # 11256-03-00

| NUMBER | BY | DATE |
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0 1 2
 THIS BAR IS EQUAL TO 2\"/>

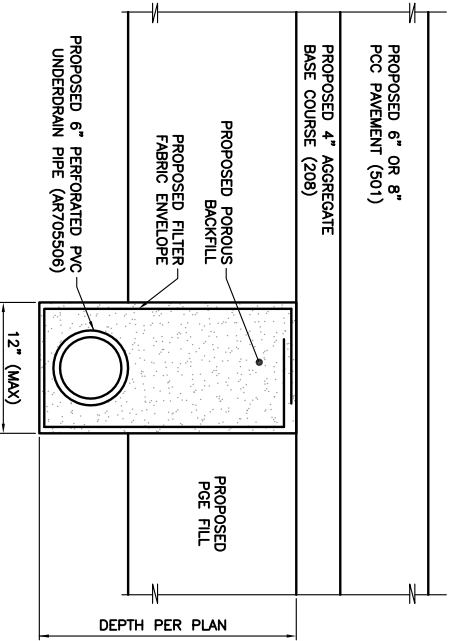
**VILLAGE OF SCHAUMBURG
 SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, IL**

**REHABILITATE AND REMARK RUNWAY 11/29
 MISCELLANEOUS DETAILS**

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DESIGN BY: SWS
 DRAWN BY: CMT
 CHECKED BY: DLP
 APPROVED BY: DLP
 DATE: 01/20/2012
 JOB No: 11256-03

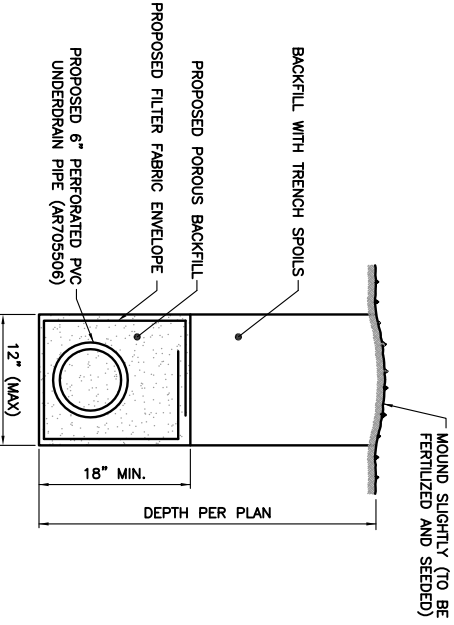
FINAL



- NOTES:**
- EXCAVATION, FILTER FABRIC ENVELOPE AND POROUS BACKFILL SHALL BE INCIDENTAL TO 6" PERFORATED PVC UNDERDRAIN (A8705506).
 - AT LOCATIONS WHERE THE EXISTING UNDERDRAIN ALONG THE SOUTH EDGE OF THE PAVEMENT MATCHES THE PROPOSED UNDERDRAIN, THE EXISTING UNDERDRAIN SHALL BE CONNECTED TO THE NEW UNDERDRAIN SYSTEM. TEES SHALL BE USED TO CONNECT THE EXISTING PIPE TO THE PROPOSED PIPE. COSTS TO CONNECT THE EXISTING UNDERDRAIN TO THE PROPOSED UNDERDRAIN SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED UNDERDRAIN.

UNDERDRAIN DETAIL - UNDER PROPOSED PAVEMENT

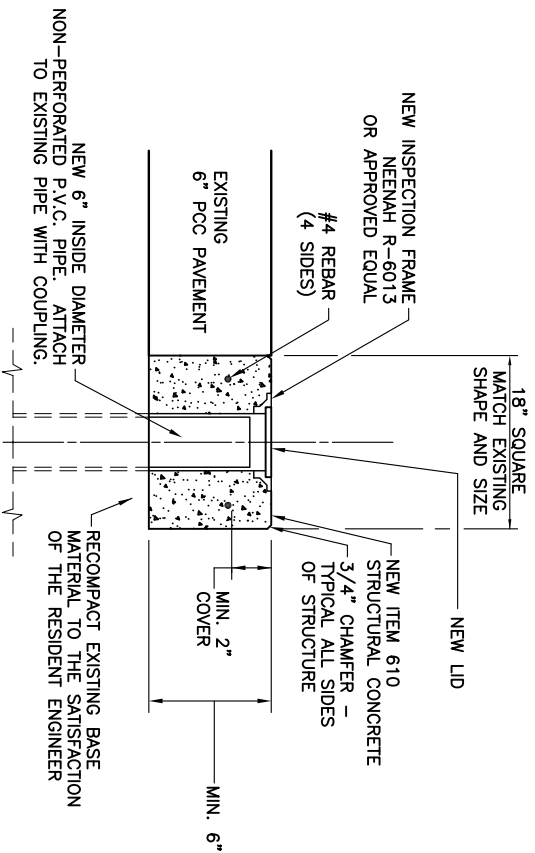
NOT TO SCALE



- NOTES:**
- EXCAVATION, FILTER FABRIC ENVELOPE, POROUS BACKFILL AND EARTH BACKFILL SHALL BE INCIDENTAL TO 6" PERFORATED PVC UNDERDRAIN (A8705506).
 - LANDSCAPING REQUIRED FOR TRENCH RESTORATION SHALL BE INCIDENTAL TO THE UNDERDRAIN INSTALLATION.

UNDERDRAIN DETAIL - NON-PAVED AREA

NOT TO SCALE

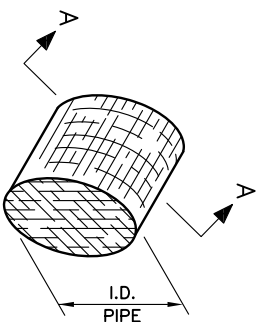


METHOD OF CONSTRUCTION

- REMOVE EXISTING PCC, FRAME AND LID.
- CUT EXISTING PVC RISER BELOW BOTTOM OF PAVEMENT.
- CONNECT NEW 6" I.D. PVC RISER PIPE TO EXISTING PVC RISER WITH COUPLING.
- PLACE NEW PCC. SET FRAME INTO NEW PCC. FRAME SHALL NOT REST ON PVC RISER.
- BID PRICE SHALL BE FULL COMPENSATION FOR THE REMOVAL OF THE EXISTING CLEANOUT, NEW FRAME AND LID, PVC RISER AND COUPLING, AND PCC PAVEMENT.
- A TYPE II BARRICADE WITH RED LIGHT SHALL BE PLACED OVERTOP OF THE WORK AREA UNTIL SUCH TIME THAT THE PCC HAS ATTAINED A COMPRESSIVE STRENGTH OF 3,500 PSI.

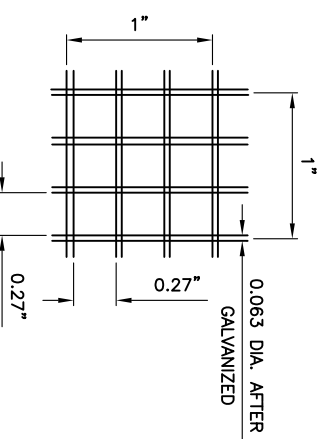
UNDERDRAIN CLEANOUT REPLACEMENT DETAIL - PAVED AREA

NOT TO SCALE



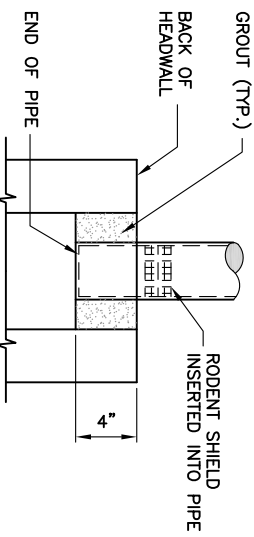
RODENT SHIELD DETAIL

N.T.S.



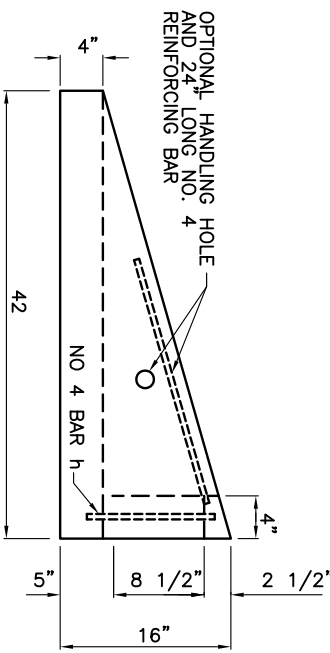
SECTION A-A

N.T.S.



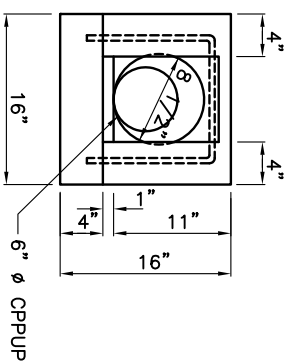
RODENT SHIELD PLACEMENT

NOT TO SCALE



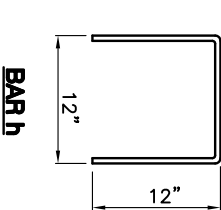
SIDE VIEW

N.T.S.



FRONT VIEW

N.T.S.



BAR h

AN ALTERNATE PAVED INVERT MEETING THE APPROVAL OF THE ENGINEER MAY BE SUBSTITUTED FOR THAT SHOWN IN THE SIDE VIEW.

| UNDERDRAIN STRUCTURE SCHEDULE | | | | | | |
|-------------------------------|----------|---------|----------|------------------|------------|--|
| ID | TYPE | STATION | OFFSET | INVERT ELEVATION | PIPE SLOPE | |
| HW-1 | HEADWALL | 117+50 | 100' RT. | 795.60 | | |
| HW-2 | HEADWALL | 118+15 | 100' RT. | 795.70 | | |
| HW-3 | HEADWALL | 118+75 | 100' RT. | 795.20 | | |
| HW-4 | HEADWALL | 119+45 | 100' RT. | 795.70 | | |
| HW-5 | HEADWALL | 120+10 | 100' RT. | 796.30 | | |
| HW-6 | HEADWALL | 124+65 | 100' RT. | 796.00 | | |
| HW-7 | HEADWALL | 125+65 | 100' RT. | 795.80 | | |
| HW-8 | HEADWALL | 126+55 | 100' RT. | 795.80 | | |
| CAP-1 | END CAP | 117+50 | 50' LT. | 797.50 | 1.27% | |
| CAP-2 | END CAP | 118+15 | 50' LT. | 797.40 | 1.13% | |
| CAP-3 | END CAP | 118+75 | 50' LT. | 797.40 | 1.47% | |
| CAP-4 | END CAP | 119+45 | 50' LT. | 797.10 | 0.93% | |
| CAP-5 | END CAP | 120+10 | 50' LT. | 797.40 | 0.73% | |
| CAP-6 | END CAP | 124+65 | 50' LT. | 798.40 | 1.60% | |
| CAP-7 | END CAP | 125+65 | 50' LT. | 798.40 | 1.73% | |
| CAP-8 | END CAP | 126+55 | 50' LT. | 798.40 | 1.73% | |

UNDERDRAIN HEADWALL DETAIL

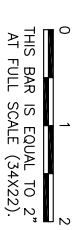
NOT TO SCALE

IDOT STANDARD 60101-01

IL CONTRACT: SH025
 IL LETTING ITEM: 2A
 IL PROJECT: 06C-3808
 A.I.P. PROJECT: 3-17-0124-B28

SURVEY BOOK # 11256-03-00

| NUMBER | BY | DATE |
|--------|----|------|
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VILLAGE OF SCHAUMBURG
 SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, IL

REHABILITATE AND REMARK RUNWAY 11/29

UNDERDRAIN DETAILS

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DRAWN BY: CMT

CHECKED BY: DLP

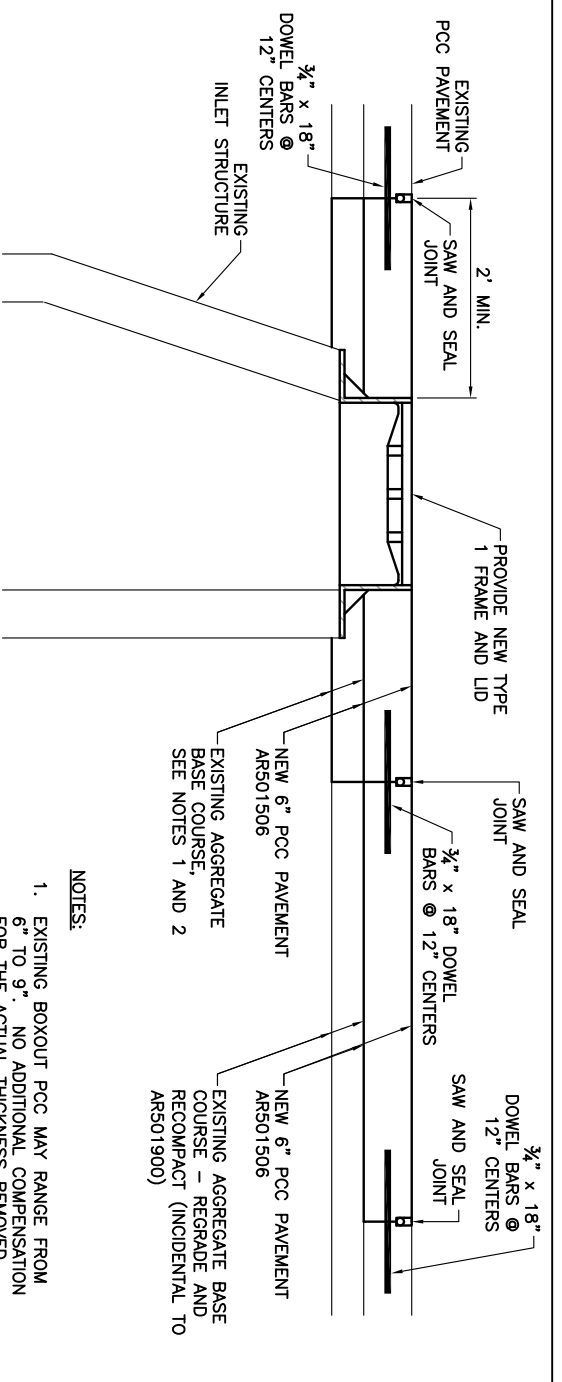
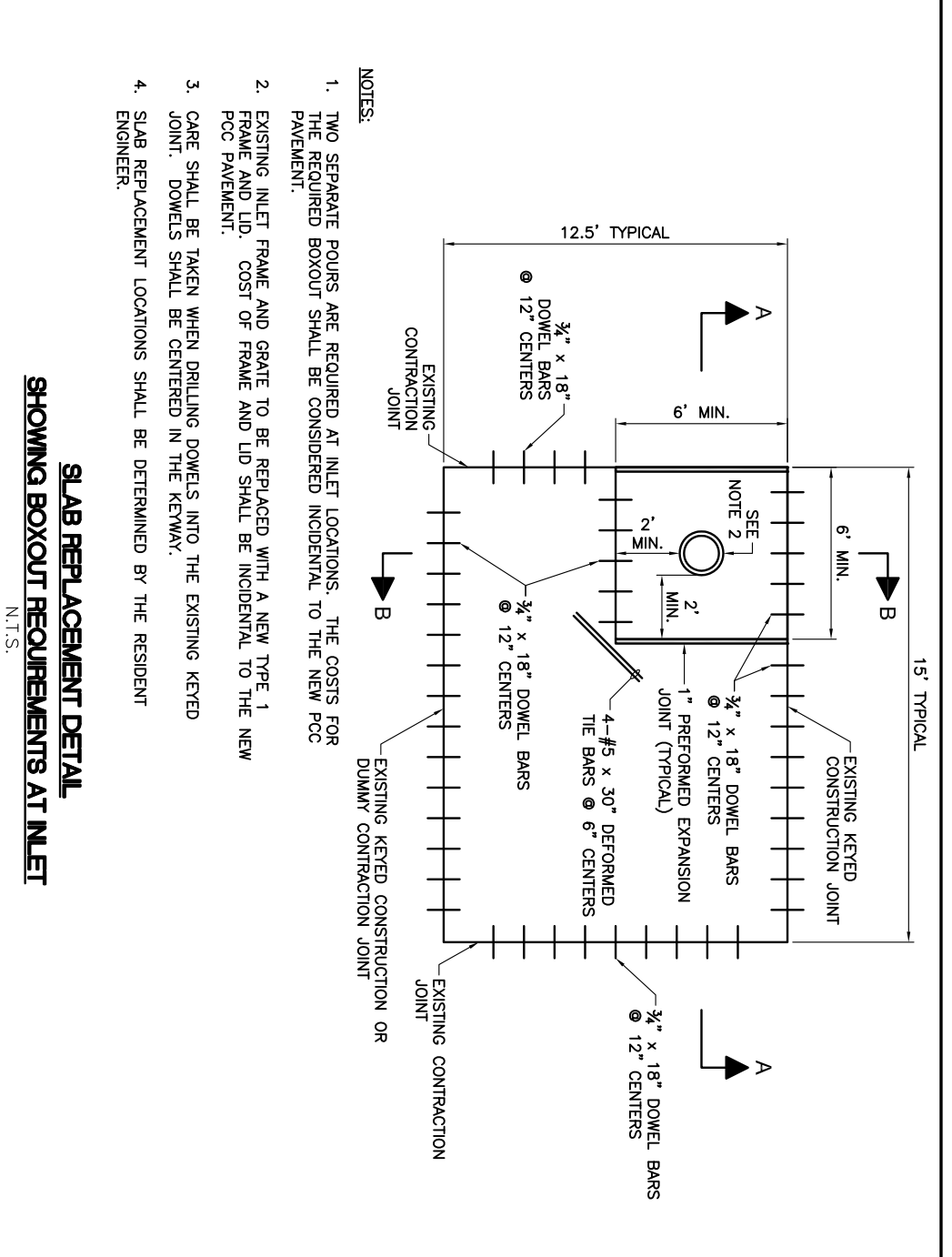
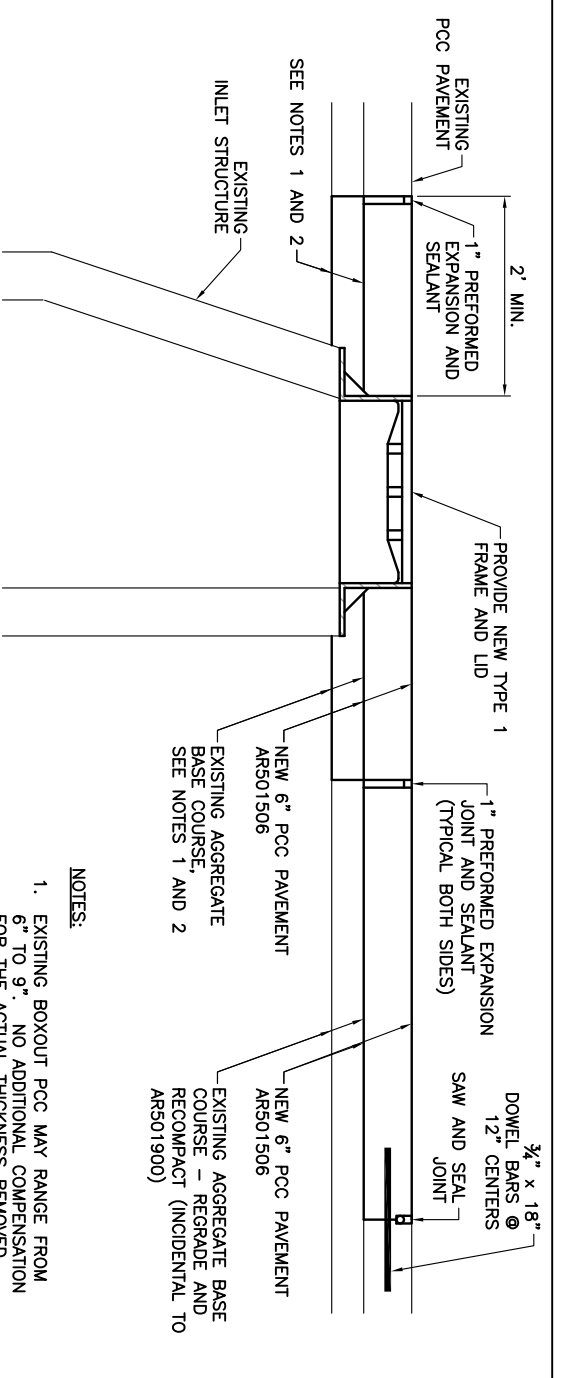
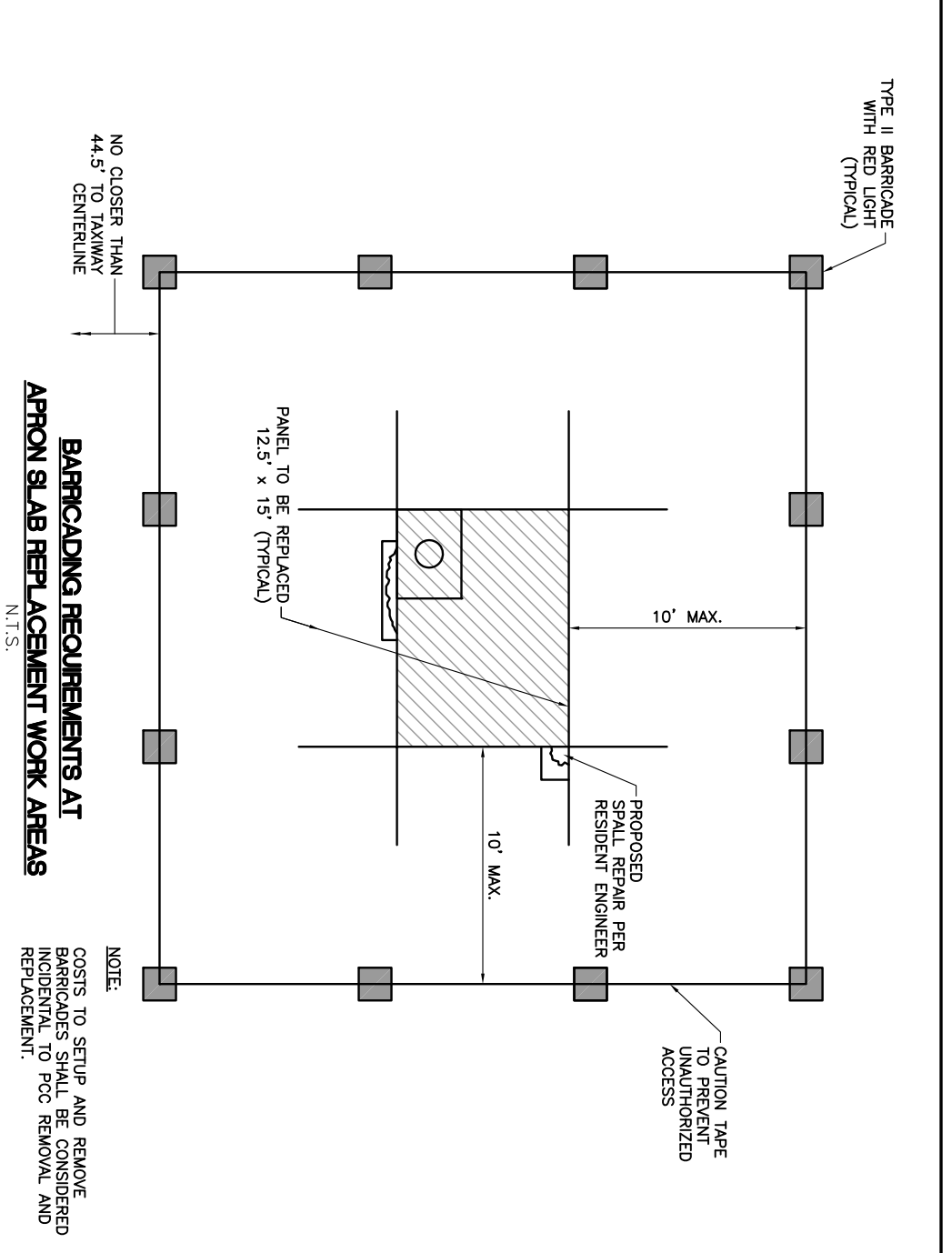
APPROVED BY: DLP

DATE: 01/20/2012

JOB No: 11256-03

FINAL

SHEET 12 OF 15 SHEETS



| <p>ILL. CONTRACT: SH025 I.L. LETTING ITEM: 2A I.L. PROJECT: 06C-3808 A.I.P. PROJECT: 3-17-0124-828</p> | | | | | | | |
|---|----|--------|----|------|--|--|--|
| <p>SURVEY BOOK # 11256-03-00</p> | | | | | | | |
| <p>REVISIONS</p> <table border="1"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | | NUMBER | BY | DATE | | | |
| NUMBER | BY | DATE | | | | | |
| | | | | | | | |
| <p>0 1 2 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).</p> | | | | | | | |
| <p>VILLAGE OF SCHAUMBURG SCHAUMBURG REGIONAL AIRPORT SCHAUMBURG, IL</p> <p>REHABILITATE AND REMARK RUNWAY 11/29</p> <p>APRON SLAB REPLACEMENT AND REPAIR DETAILS</p> | | | | | | | |
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| <p>DESIGN BY: SMS DRAWN BY: CMT CHECKED BY: DLP APPROVED BY: DLP DATE: 01/20/2012 JOB No: 11256-03</p> <p>FINAL</p> | | | | | | | |
| <p>SHEET 13 OF 15 SHEETS</p> | | | | | | | |

