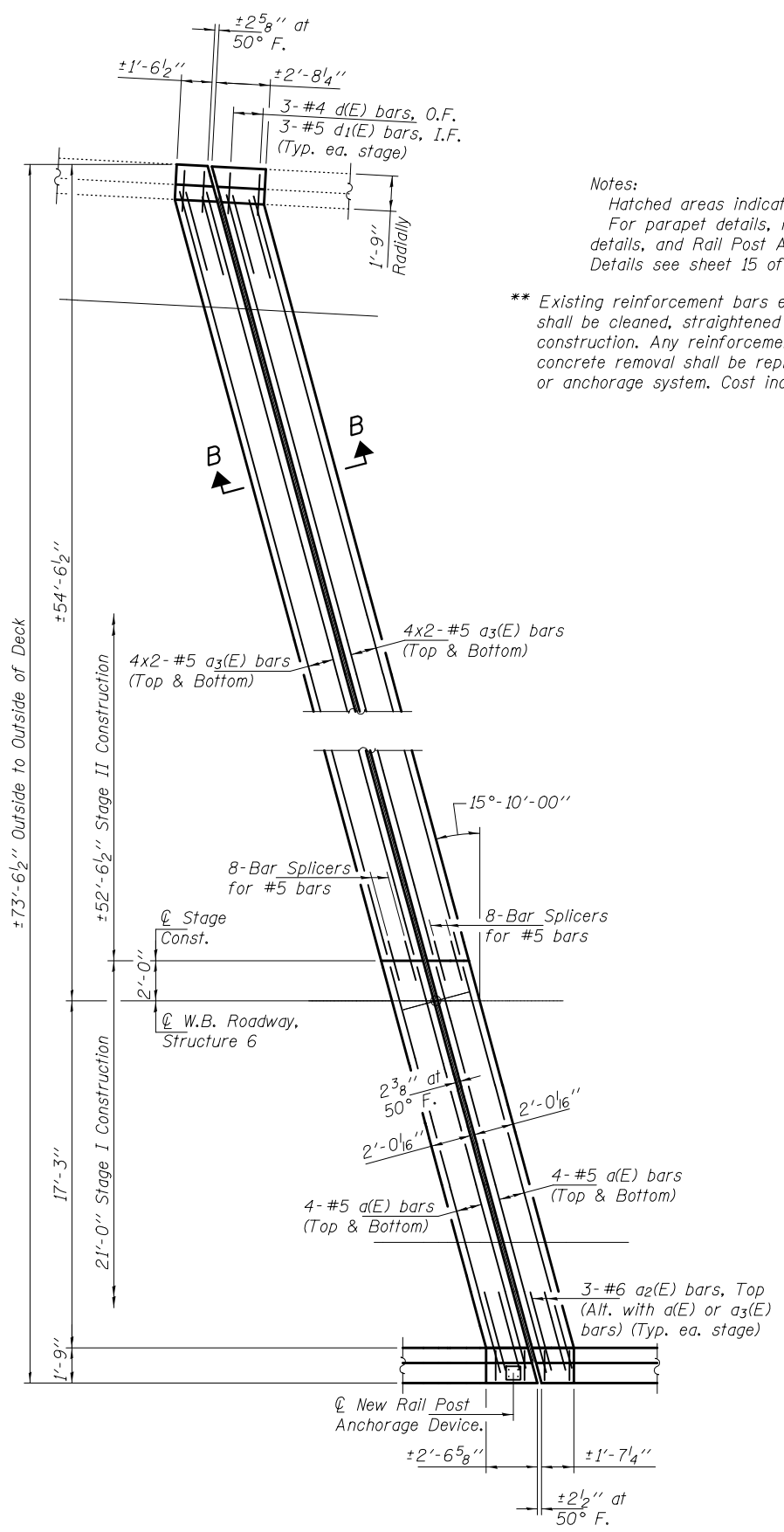
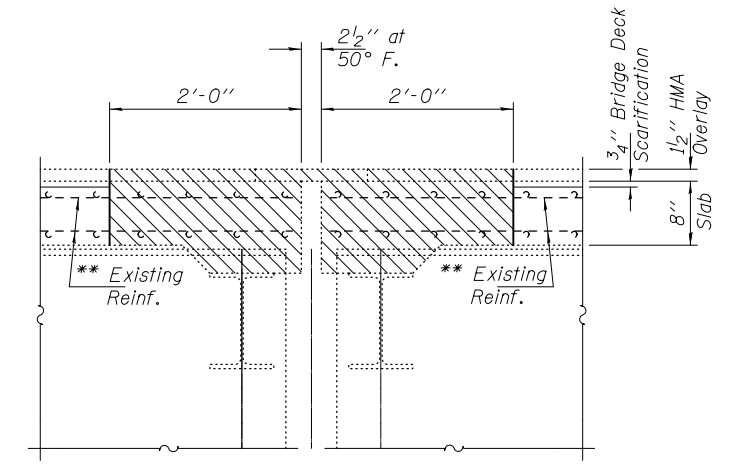


CONCRETE REMOVAL PLAN

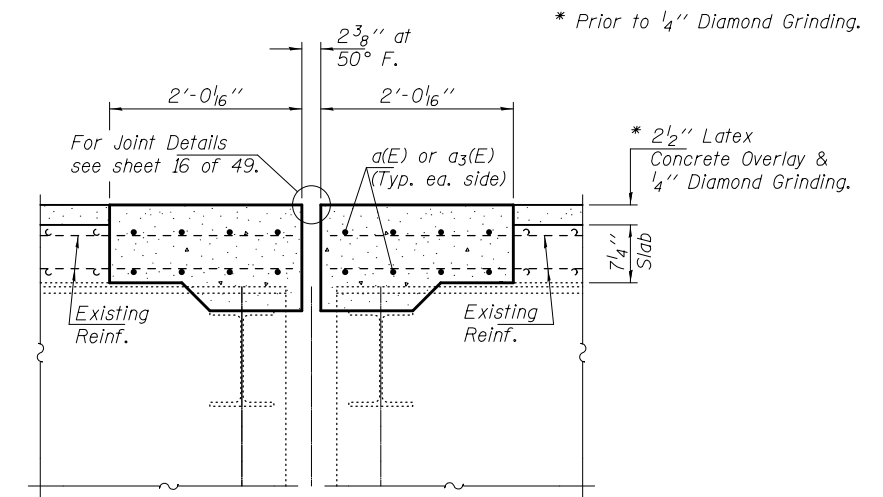


CONCRETE REPLACEMENT PLAN

Notes:
 Hatched areas indicate Concrete Removal.
 For parapet details, reinforcement bending details, and Rail Post Anchorage Device Details see sheet 15 of 49.
 ** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



SECTION A-A
 (Dims. at Rt. L's)



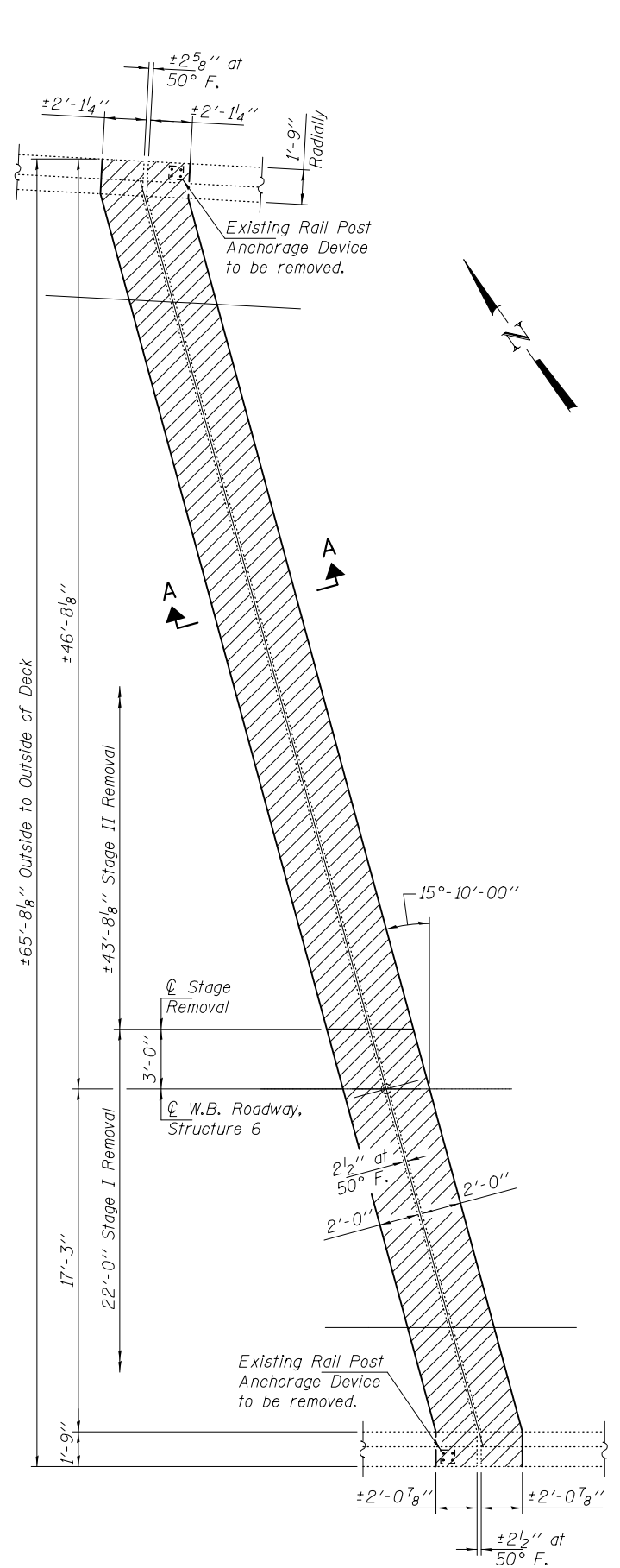
SECTION B-B
 (Dims. at Rt. L's)

BILL OF MATERIAL

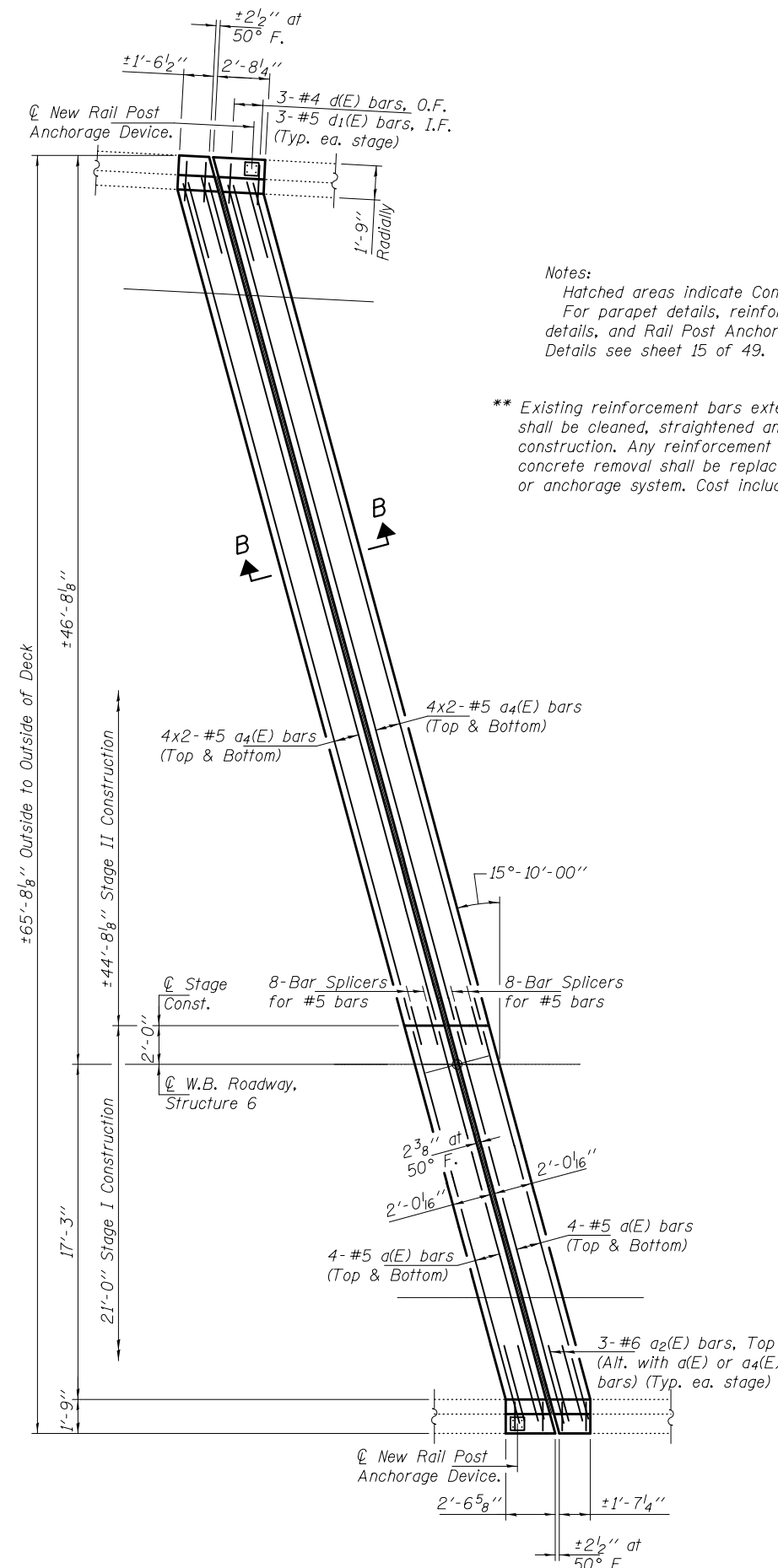
Bar	No.	Size	Length	Shape
a(E)	16	#5	21'-5"	—
a2(E)	12	#6	4'-0"	—
a3(E)	32	#5	28'-3"	—
d(E)	12	#4	4'-10"	L
d1(E)	12	#5	3'-8"	L
d2(E)	2	#4	2'-1"	□
Concrete Removal			Cu. Yd.	11.9
Concrete Superstructure			Cu. Yd.	11.9
Bar Splicers			Each	16
Reinforcement Bars, Epoxy Coated			Pound	1460

Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

MIN. LAP LENGTHS
 #5 bars = 3'-6"



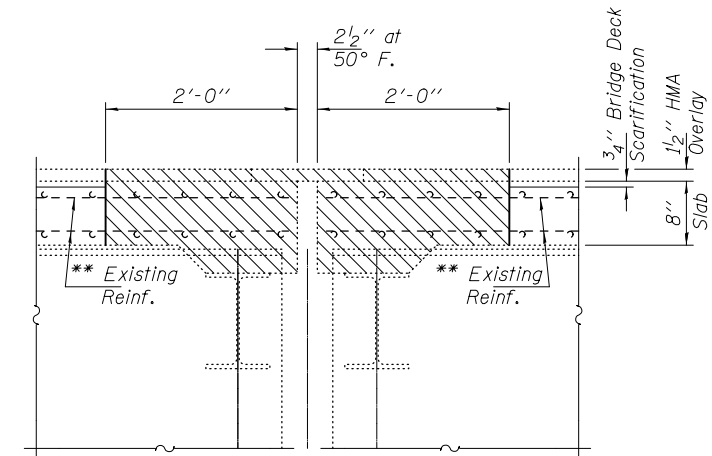
CONCRETE REMOVAL PLAN



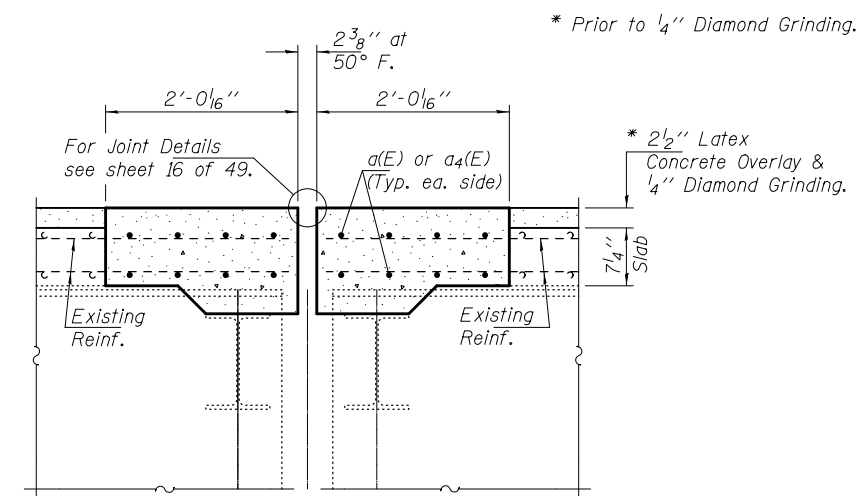
CONCRETE REPLACEMENT PLAN

Notes:
 Hatched areas indicate Concrete Removal.
 For parapet details, reinforcement bending details, and Rail Post Anchorage Device Details see sheet 15 of 49.

** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



SECTION A-A
 (Dims. at Rt. L's)



SECTION B-B
 (Dims. at Rt. L's)

MIN. LAP LENGTHS
 #5 bars = 3'-6"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	16	#5	21'-5"	—
a2(E)	12	#6	4'-0"	—
a4(E)	32	#5	24'-3"	—
d(E)	12	#4	4'-10"	L
d1(E)	12	#5	3'-8"	L
d2(E)	4	#4	2'-1"	□
Concrete Removal			Cu. Yd.	10.7
Concrete Superstructure			Cu. Yd.	10.7
Bar Splicers			Each	16
Reinforcement Bars, Epoxy Coated			Pound	1330

Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

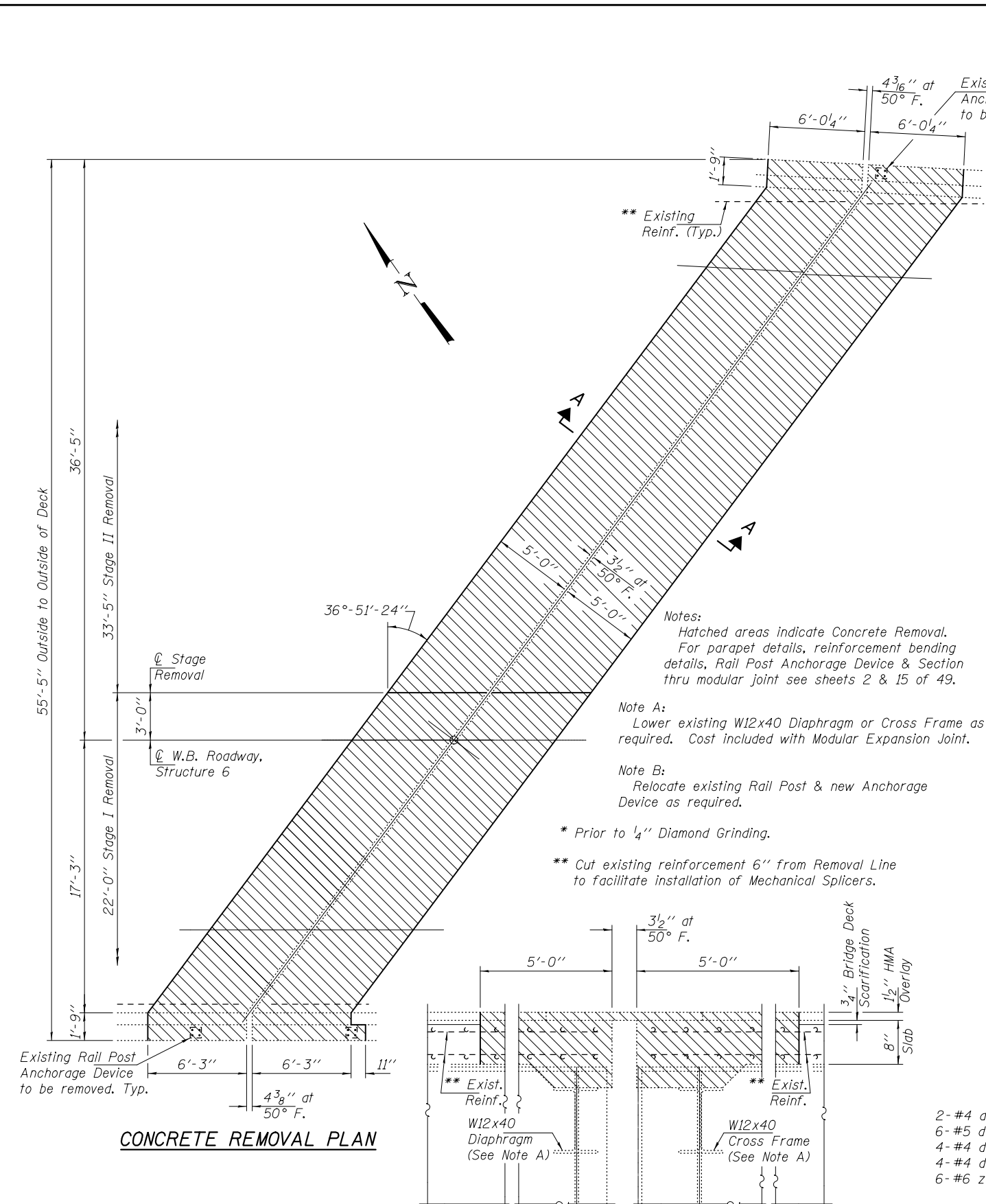
DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kruger</i>
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
REVISIONS
REVISIONS

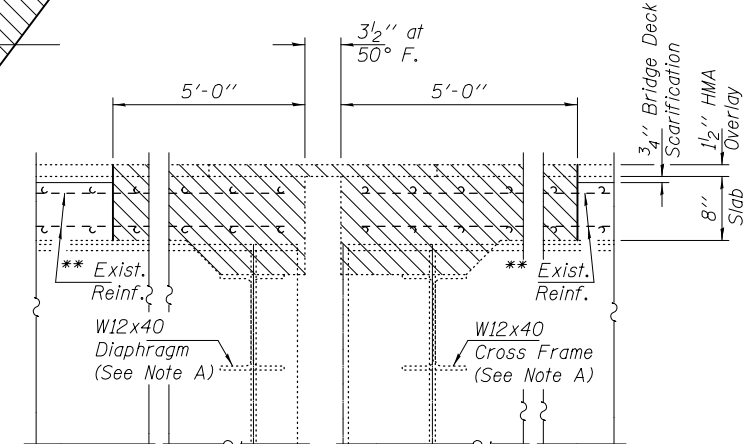
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 4 JOINT REMOVAL & REPLACEMENT DETAILS
SN 072-0132 (W.B.)

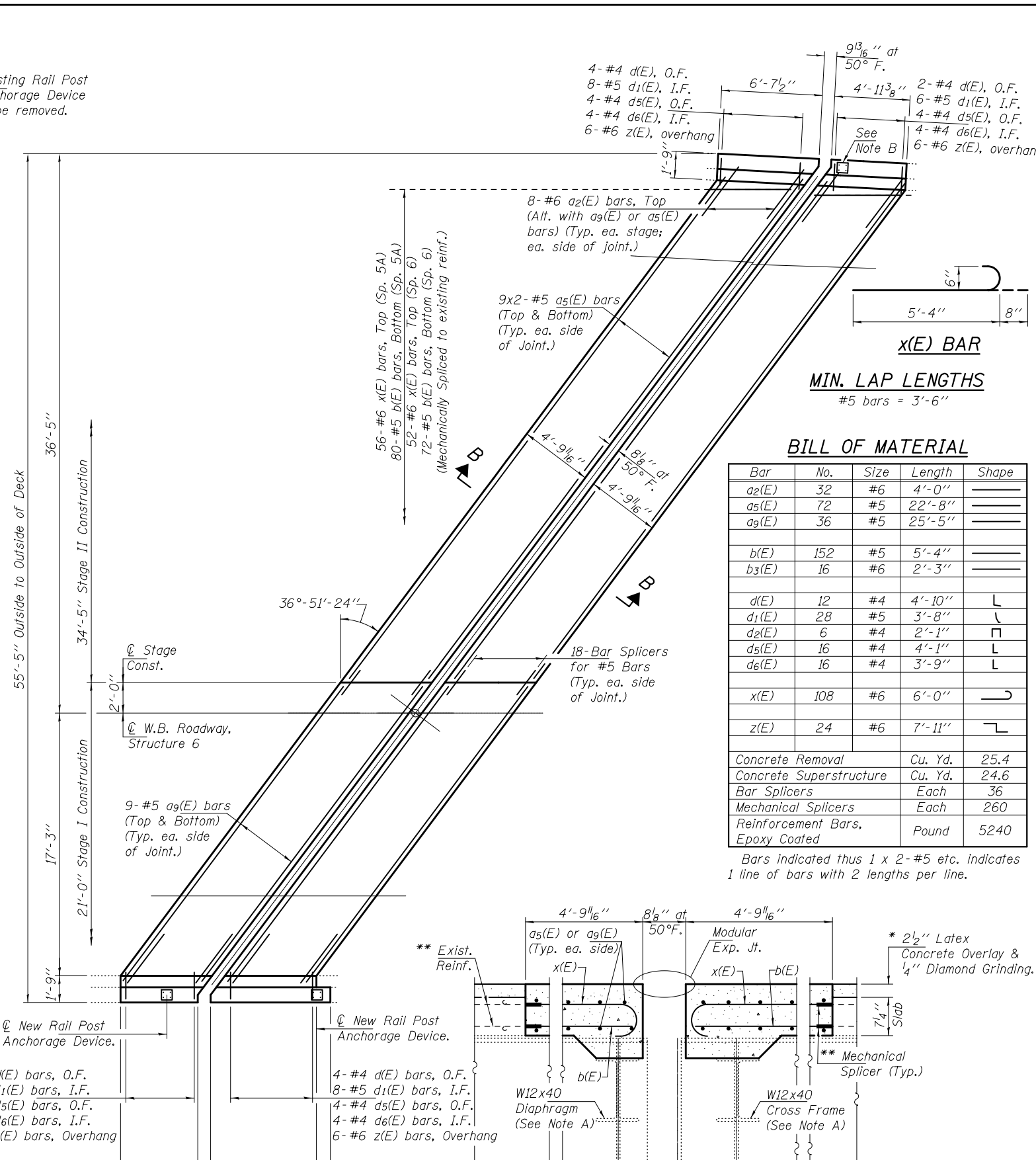
F.A.I. RTE. 474	SECTION 72-4(HB, HVB-1, HVB)B-R	COUNTY PEORIA	TOTAL SHEETS 196	SHEET NO. 102
CONTRACT NO. 68887				ILLINOIS FED. AID PROJECT



CONCRETE REMOVAL PLAN



SECTION A-A
(Dims. at Rt. L's)



SECTION B-B
(Dims. at Rt. L's)
(Modular Joint not shown for clarity.)

x(E) BAR
MIN. LAP LENGTHS
#5 bars = 3'-6"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a2(E)	32	#6	4'-0"	—
a5(E)	72	#5	22'-8"	—
a9(E)	36	#5	25'-5"	—
b(E)	152	#5	5'-4"	—
b3(E)	16	#6	2'-3"	—
d(E)	12	#4	4'-10"	L
d1(E)	28	#5	3'-8"	U
d2(E)	6	#4	2'-1"	□
d5(E)	16	#4	4'-1"	L
d6(E)	16	#4	3'-9"	L
x(E)	108	#6	6'-0"	J
z(E)	24	#6	7'-11"	L
Concrete Removal			Cu. Yd.	25.4
Concrete Superstructure			Cu. Yd.	24.6
Bar Splicers			Each	36
Mechanical Splicers			Each	260
Reinforcement Bars, Epoxy Coated			Pound	5240

Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

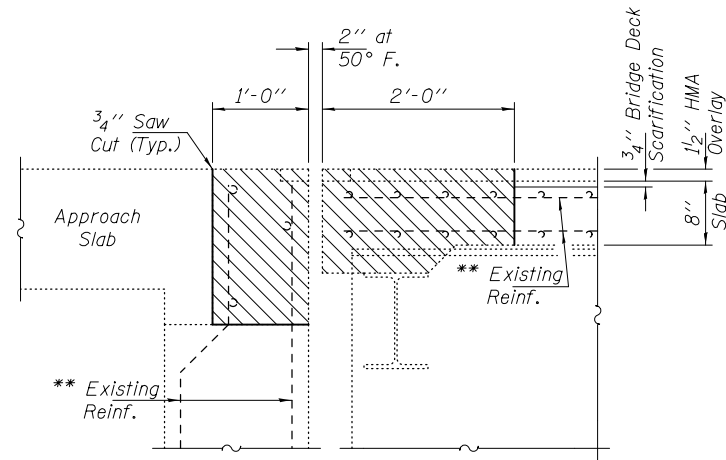
CONCRETE REPLACEMENT PLAN

DESIGNED - SMR	EXAMINED - <i>Timothy A. Doolittle</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kruger</i>	REVISOR
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

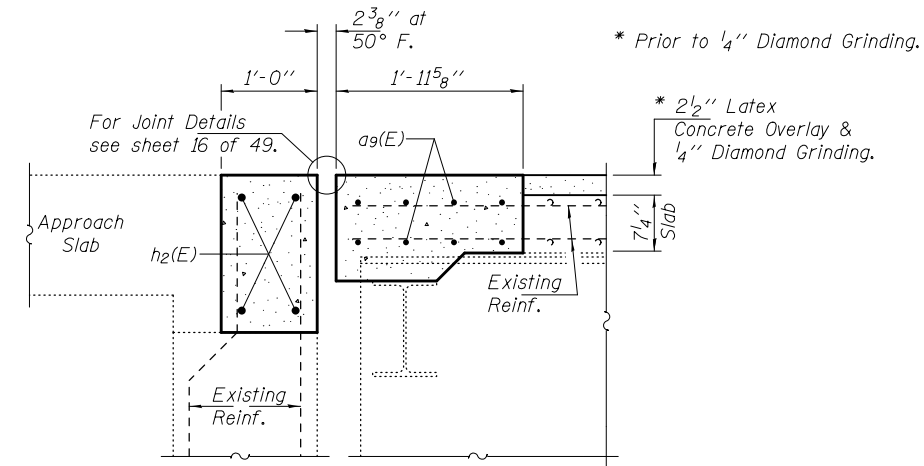
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 5 JOINT REMOVAL & REPLACEMENT DETAILS
SN 072-0132 (W.B.)

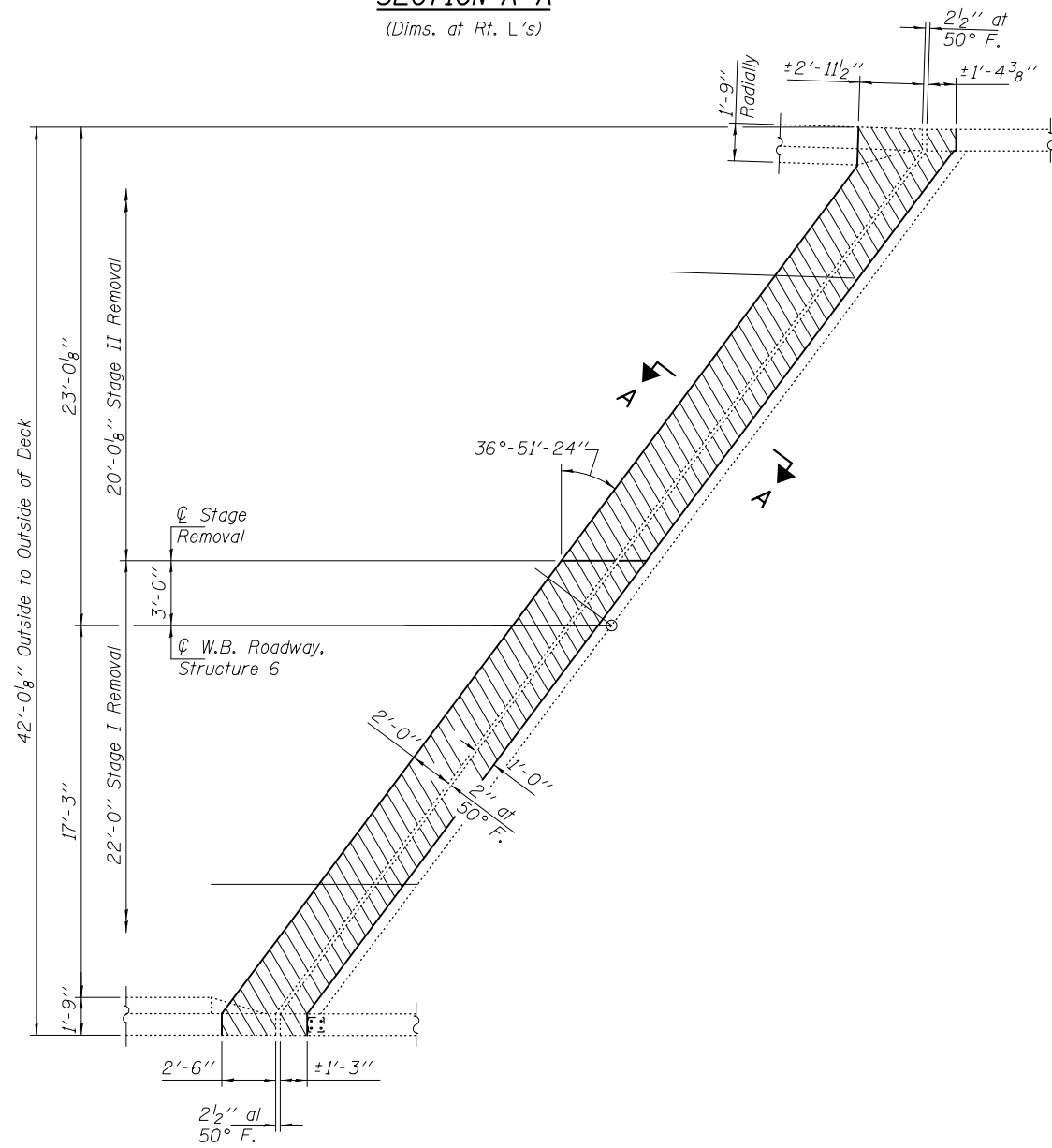
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	103
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



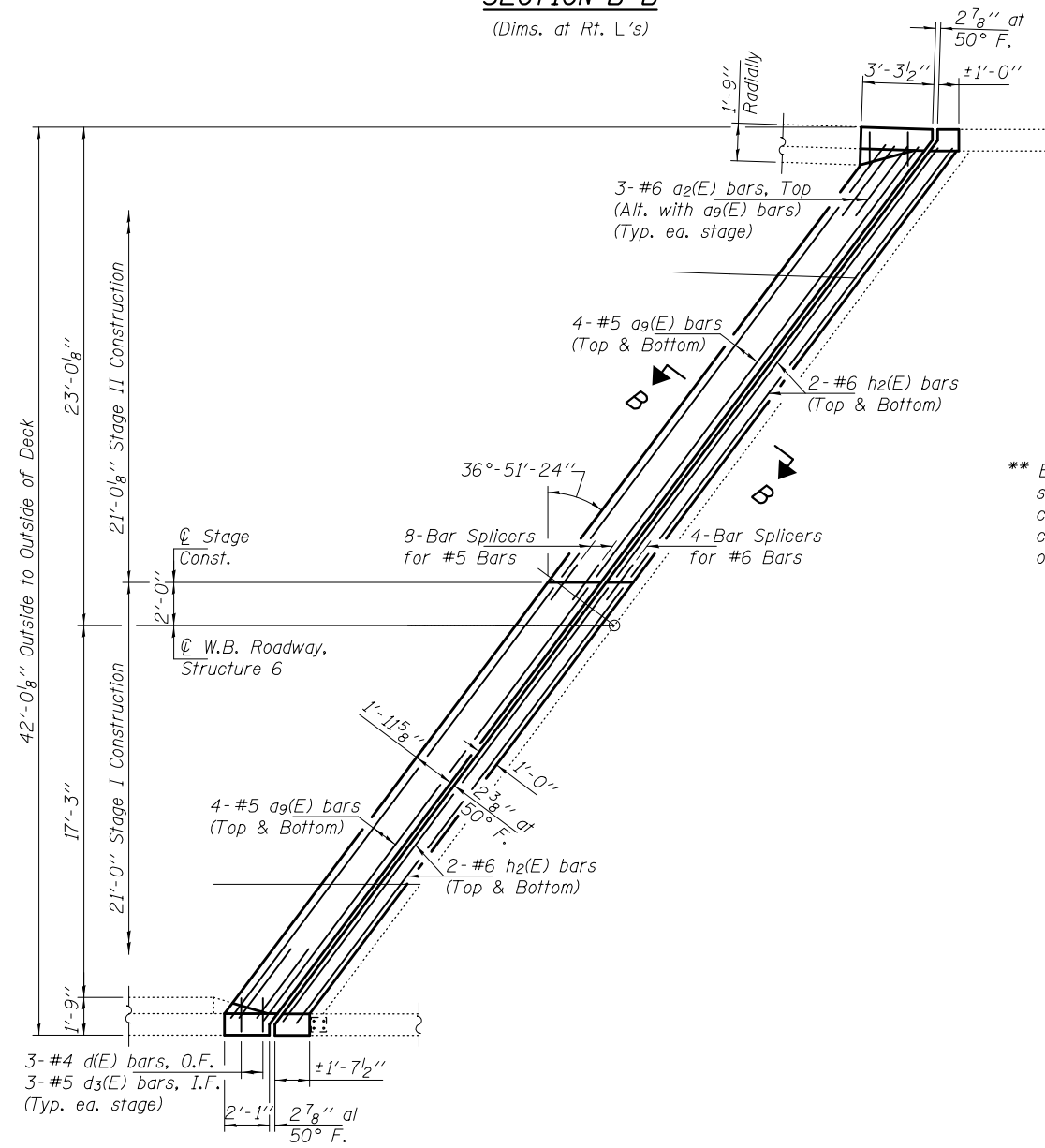
SECTION A-A
(Dims. at Rt. L's)



SECTION B-B
(Dims. at Rt. L's)



CONCRETE REMOVAL PLAN



CONCRETE REPLACEMENT PLAN

Notes:
Hatched areas indicate Concrete Removal.
For parapet details and reinforcement bending details see sheet 15 of 49.

** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₂ (E)	6	#6	4'-0"	—
a ₉ (E)	16	#5	25'-5"	—
d(E)	6	#4	4'-10"	L
d ₃ (E)	6	#5	4'-6"	J
h ₂ (E)	8	#6	25'-5"	—
Concrete Removal			Cu. Yd.	8.2
Concrete Superstructure			Cu. Yd.	8.2
Bar Splicers			Each	12
Reinforcement Bars, Epoxy Coated			Pound	820

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

EXAMINED
PASSED
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

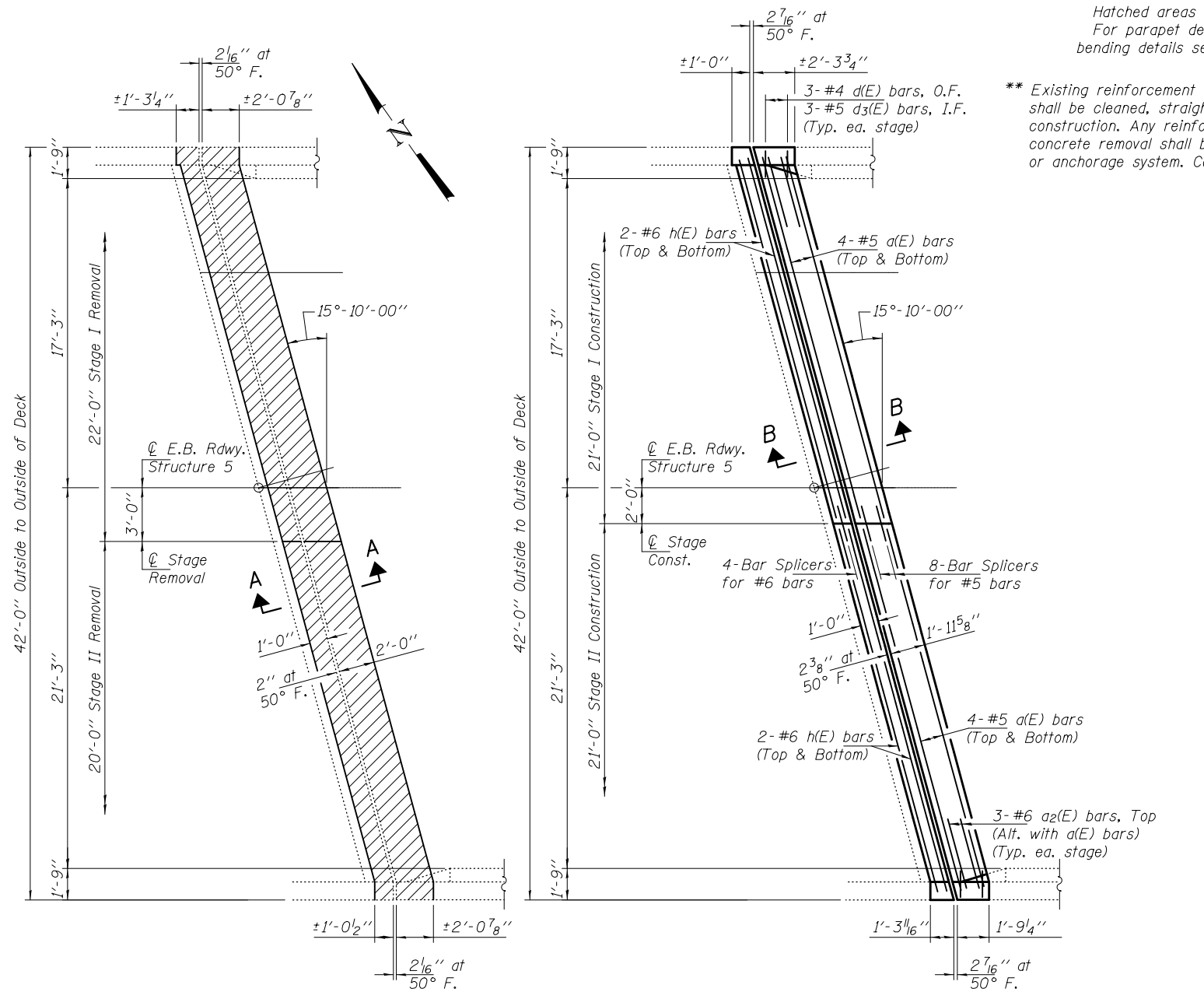
DATE - JANUARY 31, 2018
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT JOINT REMOVAL & REPLACEMENT DETAILS
SN 072-0132 (W.B.)

SHEET NO. 9 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	104
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

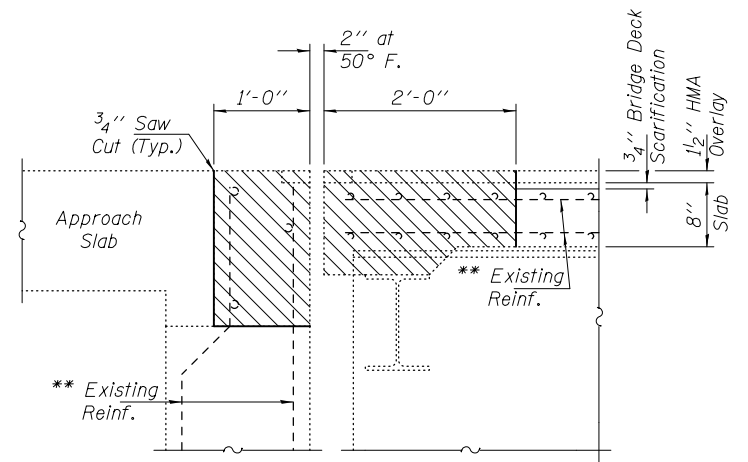


CONCRETE REMOVAL PLAN

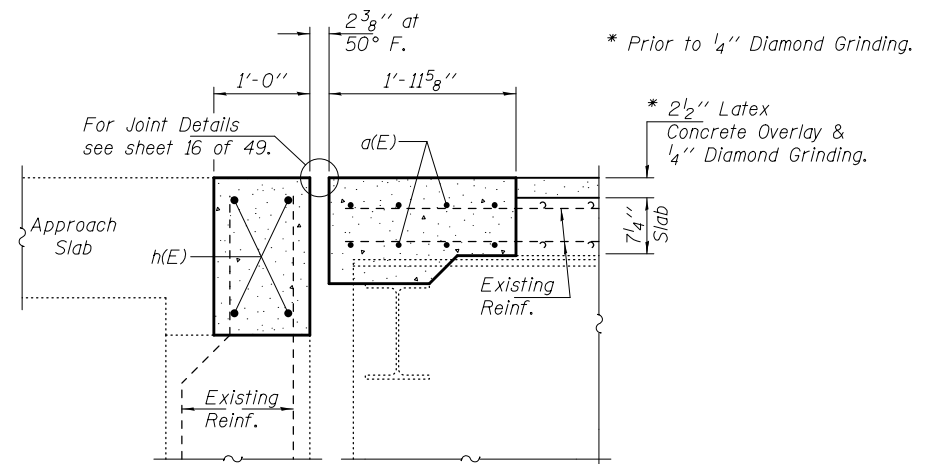
CONCRETE REPLACEMENT PLAN

Notes:
 Hatched areas indicate Concrete Removal.
 For parapet details and reinforcement bending details see sheet 15 of 49.

** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



SECTION A-A
 (Dims. at Rt. L's)



SECTION B-B
 (Dims. at Rt. L's)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	16	#5	21'-5"	—
a2(E)	6	#6	4'-0"	—
d(E)	6	#4	4'-10"	L
d3(E)	6	#5	4'-6"	J
h(E)	8	#6	21'-5"	—
Concrete Removal			Cu. Yd.	6.5
Concrete Superstructure			Cu. Yd.	6.4
Bar Splicers			Each	12
Reinforcement Bars, Epoxy Coated			Pound	700

DESIGNED - SMR
 CHECKED - RPN
 DRAWN - Kyle M. Steffen
 CHECKED - SMR RPN

EXAMINED
 PASSED
 ENGINEER OF STRUCTURAL SERVICES
 ENGINEER OF BRIDGES AND STRUCTURES

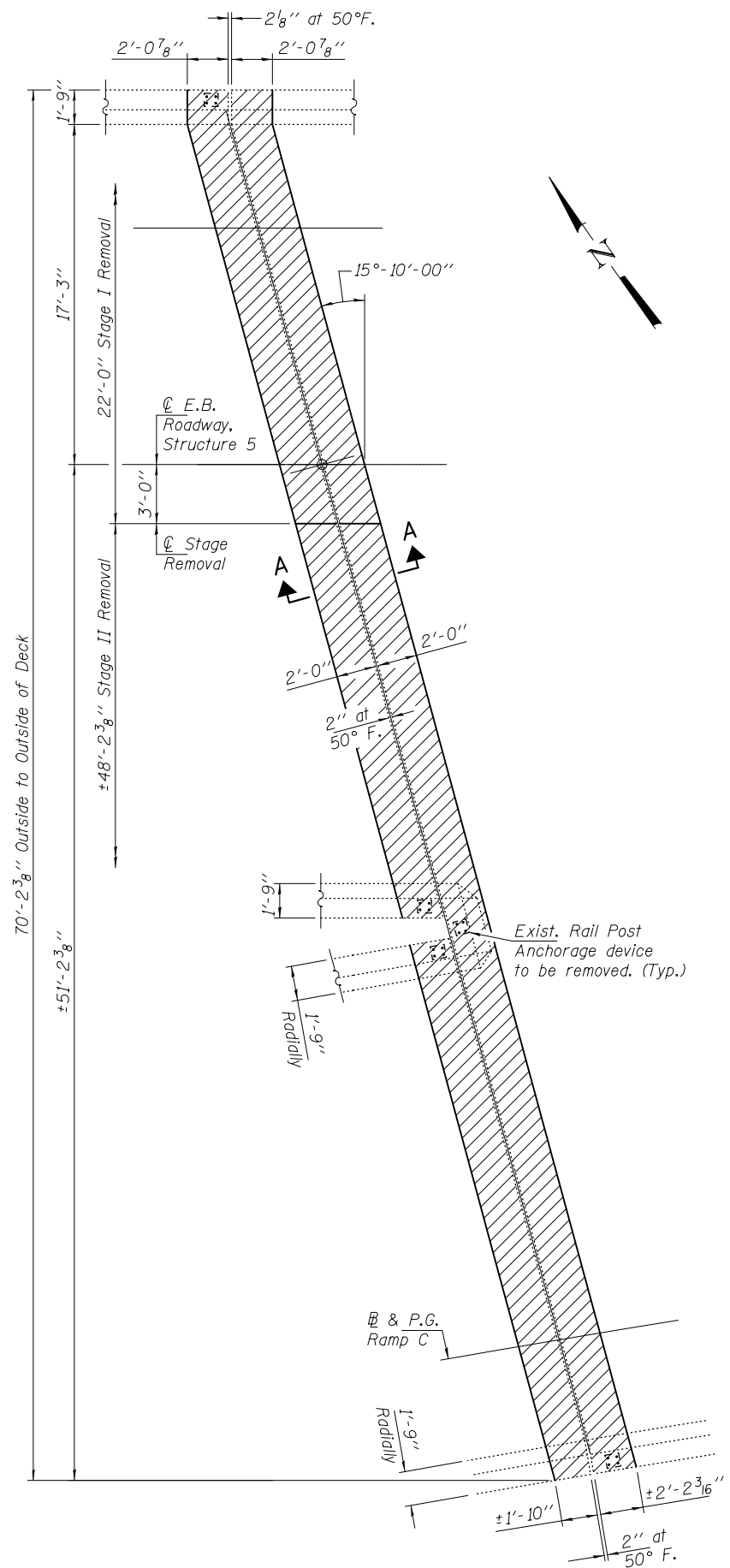
DATE - JANUARY 31, 2018
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

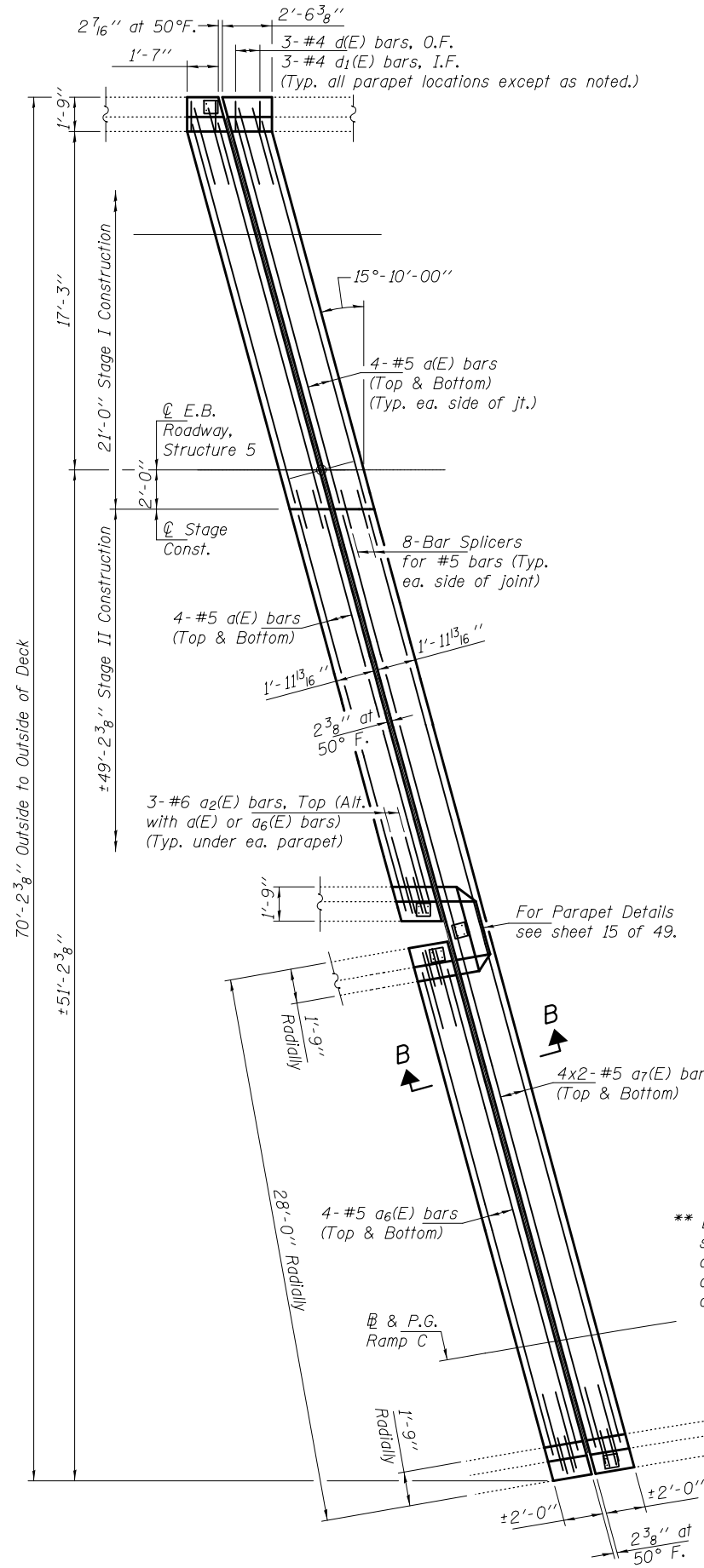
NORTH ABUTMENT JOINT REMOVAL & REPLACEMENT DETAILS
 SN 072-0131 (E.B.)

SHEET NO. 10 OF 49 SHEETS

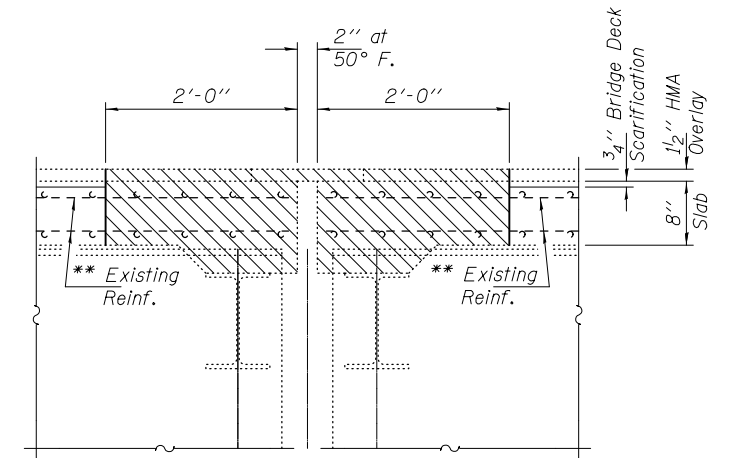
F.A.I. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 474 72-4(HB, HVB-1, HVB)B-R PEORIA 196 105
 CONTRACT NO. 68887
 ILLINOIS FED. AID PROJECT



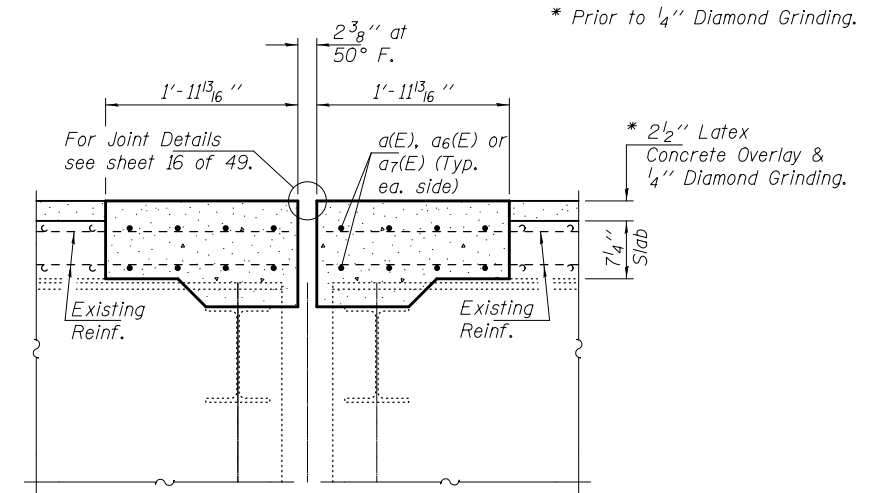
CONCRETE REMOVAL PLAN



CONCRETE REPLACEMENT PLAN



SECTION A-A
(Dims. at Rt. L's)



SECTION B-B
(Dims. at Rt. L's)

Notes:
Hatched areas indicate Concrete Removal.
For parapet details and reinforcement bending details see sheet 15 of 49.

** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

MIN. LAP LENGTHS
#5 bars = 3'-6"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	24	#5	21'-5"	—
a2(E)	18	#6	4'-0"	—
a6(E)	8	#5	25'-3"	—
a7(E)	16	#5	27'-1"	—
d(E)	18	#4	4'-10"	L
d1(E)	24	#5	3'-8"	L
d2(E)	4	#4	2'-1"	□
d3(E)	6	#5	4'-6"	L
d4(E)	6	#4	2'-3"	□
e(E)	4	#8	2'-5"	—
e1(E)	4	#4	2'-5"	—
Concrete Removal			Cu. Yd.	12.1
Concrete Superstructure			Cu. Yd.	12.1
Bar Splicers			Each	16
Reinforcement Bars, Epoxy Coated			Pound	1530

Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

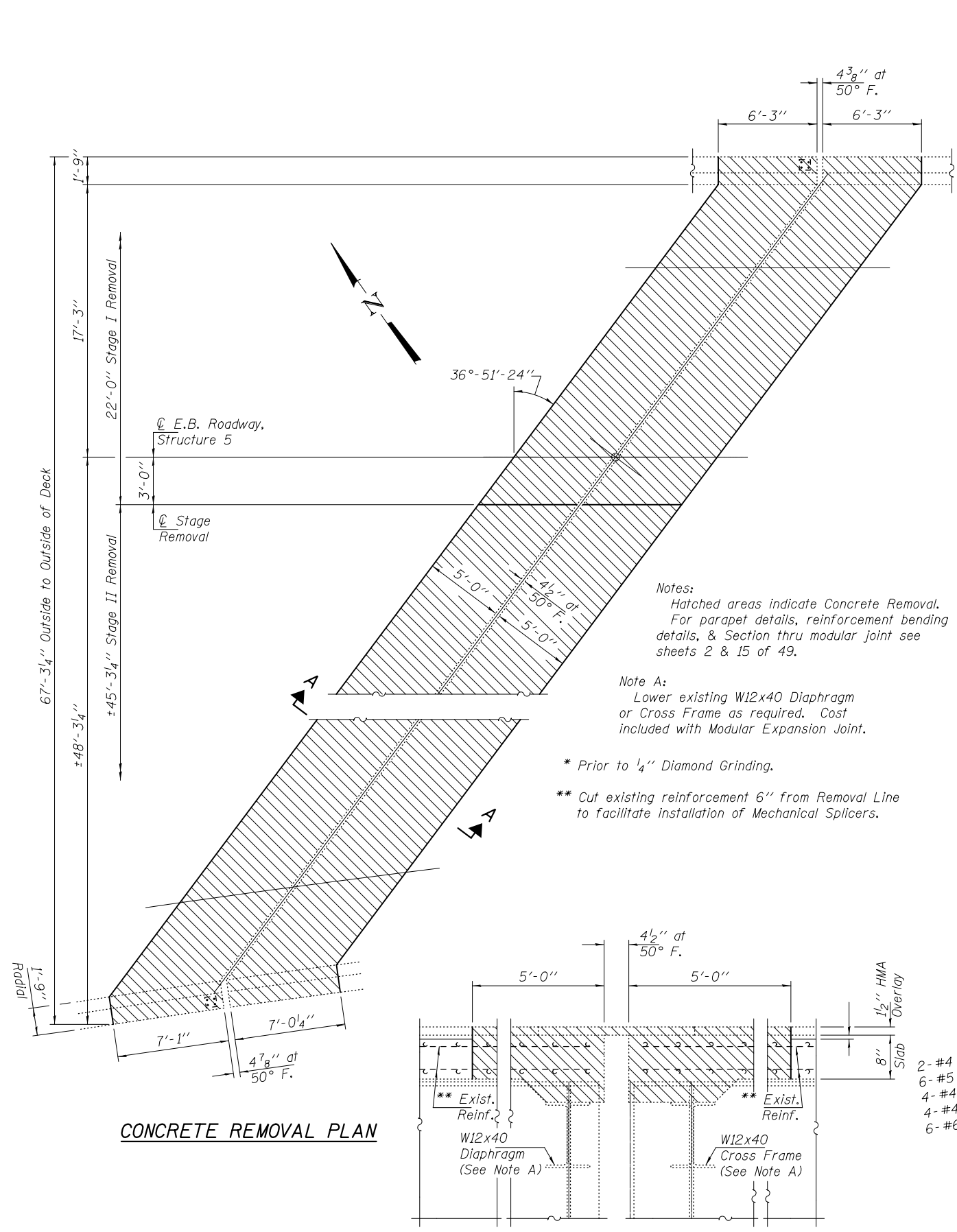
DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kruger</i>	REVISOR
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 4 JOINT REMOVAL & REPLACEMENT DETAILS
SN 072-0131 (E.B.) & RAMP "C"

SHEET NO. 11 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	106
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	



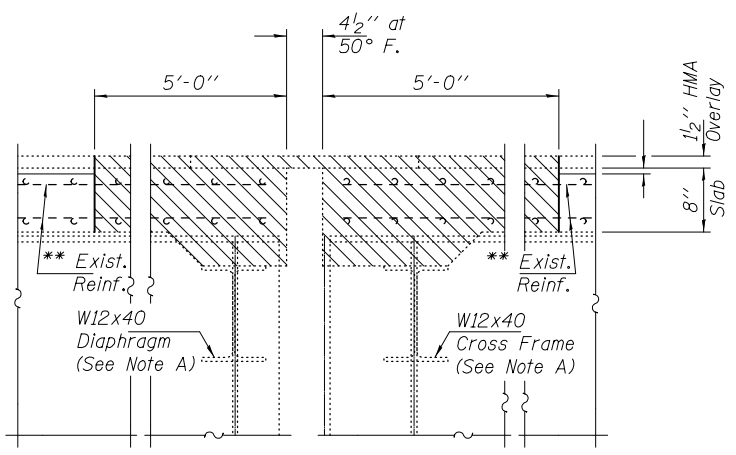
CONCRETE REMOVAL PLAN

Notes:
 Hatched areas indicate Concrete Removal. For parapet details, reinforcement bending details, & Section thru modular joint see sheets 2 & 15 of 49.

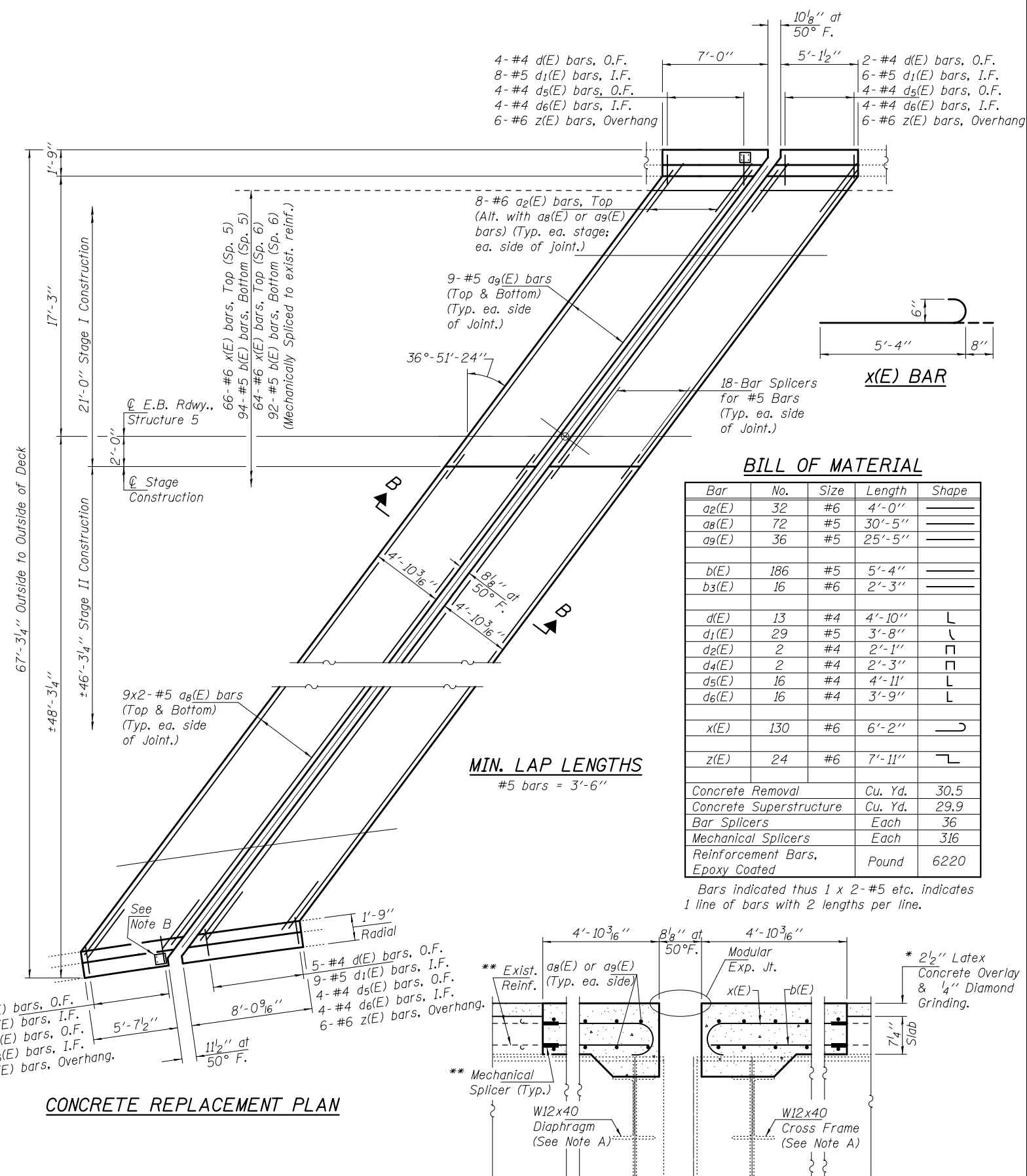
Note A:
 Lower existing W12x40 Diaphragm or Cross Frame as required. Cost included with Modular Expansion Joint.

* Prior to 1/4" Diamond Grinding.

** Cut existing reinforcement 6" from Removal Line to facilitate installation of Mechanical Splicers.



SECTION A-A
 (Dims. at Rt. L's)



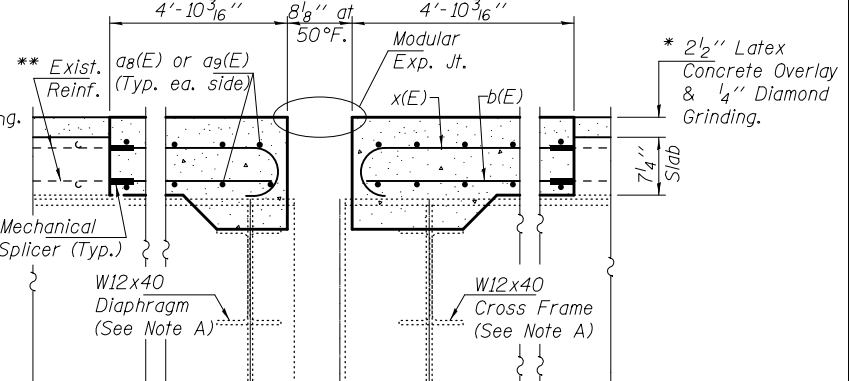
CONCRETE REPLACEMENT PLAN

MIN. LAP LENGTHS
 #5 bars = 3'-6"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a2(E)	32	#6	4'-0"	—
a8(E)	72	#5	30'-5"	—
a9(E)	36	#5	25'-5"	—
b(E)	186	#5	5'-4"	—
b3(E)	16	#6	2'-3"	—
d(E)	13	#4	4'-10"	L
d1(E)	29	#5	3'-8"	L
d2(E)	2	#4	2'-1"	□
d4(E)	2	#4	2'-3"	□
d5(E)	16	#4	4'-11"	L
d6(E)	16	#4	3'-9"	L
x(E)	130	#6	6'-2"	J
z(E)	24	#6	7'-11"	L
Concrete Removal			Cu. Yd.	30.5
Concrete Superstructure			Cu. Yd.	29.9
Bar Splicers			Each	36
Mechanical Splicers			Each	316
Reinforcement Bars, Epoxy Coated			Pound	6220

Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.



SECTION B-B
 (Dims. at Rt. L's)
 (Modular Joint not shown for clarity.)

DESIGNED - SMR
 CHECKED - RPN
 DRAWN - Kyle M. Steffen
 CHECKED - SMR RPN

EXAMINED
 PASSED
 ENGINEER OF STRUCTURAL SERVICES
 ENGINEER OF BRIDGES AND STRUCTURES

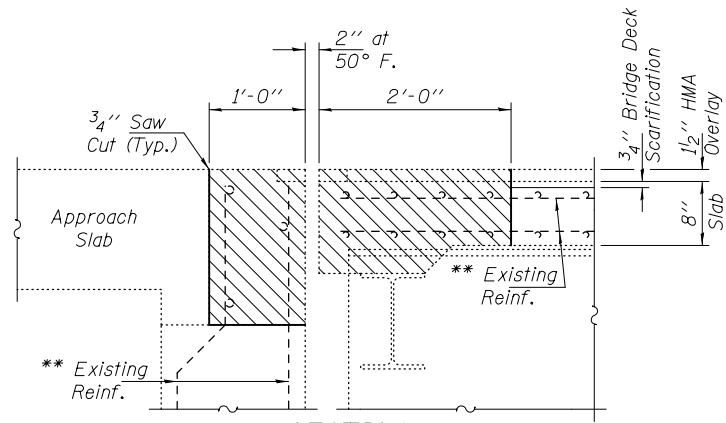
DATE - JANUARY 31, 2018
 REVISED
 REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

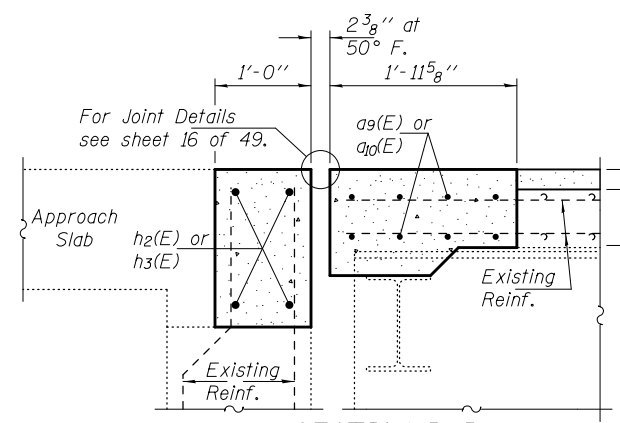
PIER 5 JOINT REMOVAL & REPLACEMENT DETAILS
SN 072-0131 (E.B.)

SHEET NO. 12 OF 49 SHEETS

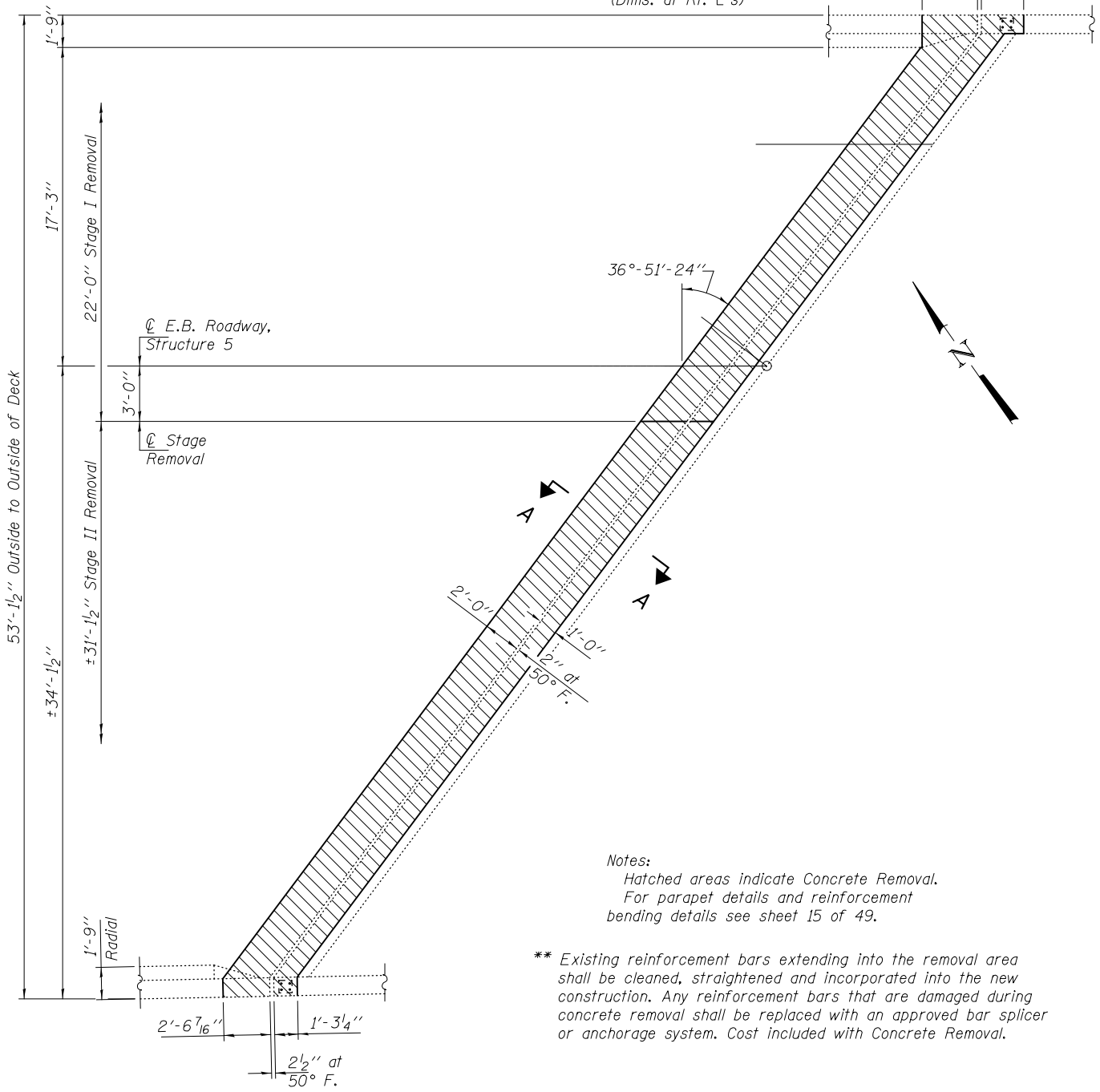
F.A.I. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 474 72-4(HB, HVB-1, HVB)B-R PEORIA 196 107
 CONTRACT NO. 68887
 ILLINOIS FED. AID PROJECT



SECTION A-A
(Dims. at Rt. L's)

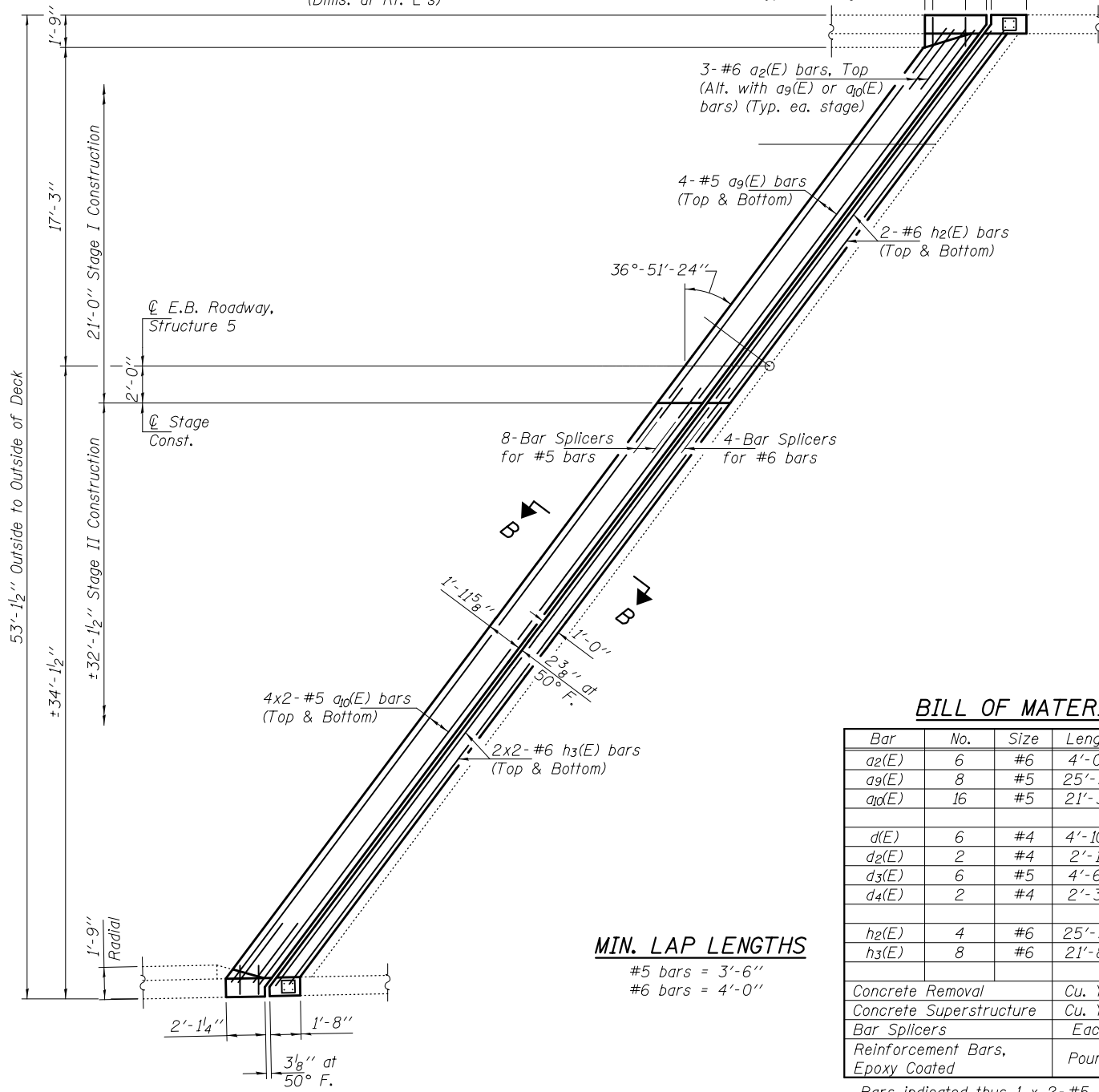


SECTION B-B
(Dims. at Rt. L's)



CONCRETE REMOVAL PLAN

Notes:
Hatched areas indicate Concrete Removal.
For parapet details and reinforcement bending details see sheet 15 of 49.
** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



CONCRETE REPLACEMENT PLAN

MIN. LAP LENGTHS
#5 bars = 3'-6"
#6 bars = 4'-0"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₂ (E)	6	#6	4'-0"	—
a ₉ (E)	8	#5	25'-5"	—
a ₁₀ (E)	16	#5	21'-3"	—
d(E)	6	#4	4'-10"	L
d ₂ (E)	2	#4	2'-1"	□
d ₃ (E)	6	#5	4'-6"	J
d ₄ (E)	2	#4	2'-3"	□
h ₂ (E)	4	#6	25'-5"	—
h ₃ (E)	8	#6	21'-8"	—
Concrete Removal			Cu. Yd.	9.9
Concrete Superstructure			Cu. Yd.	9.8
Bar Splicers			Each	12
Reinforcement Bars, Epoxy Coated			Pound	1070

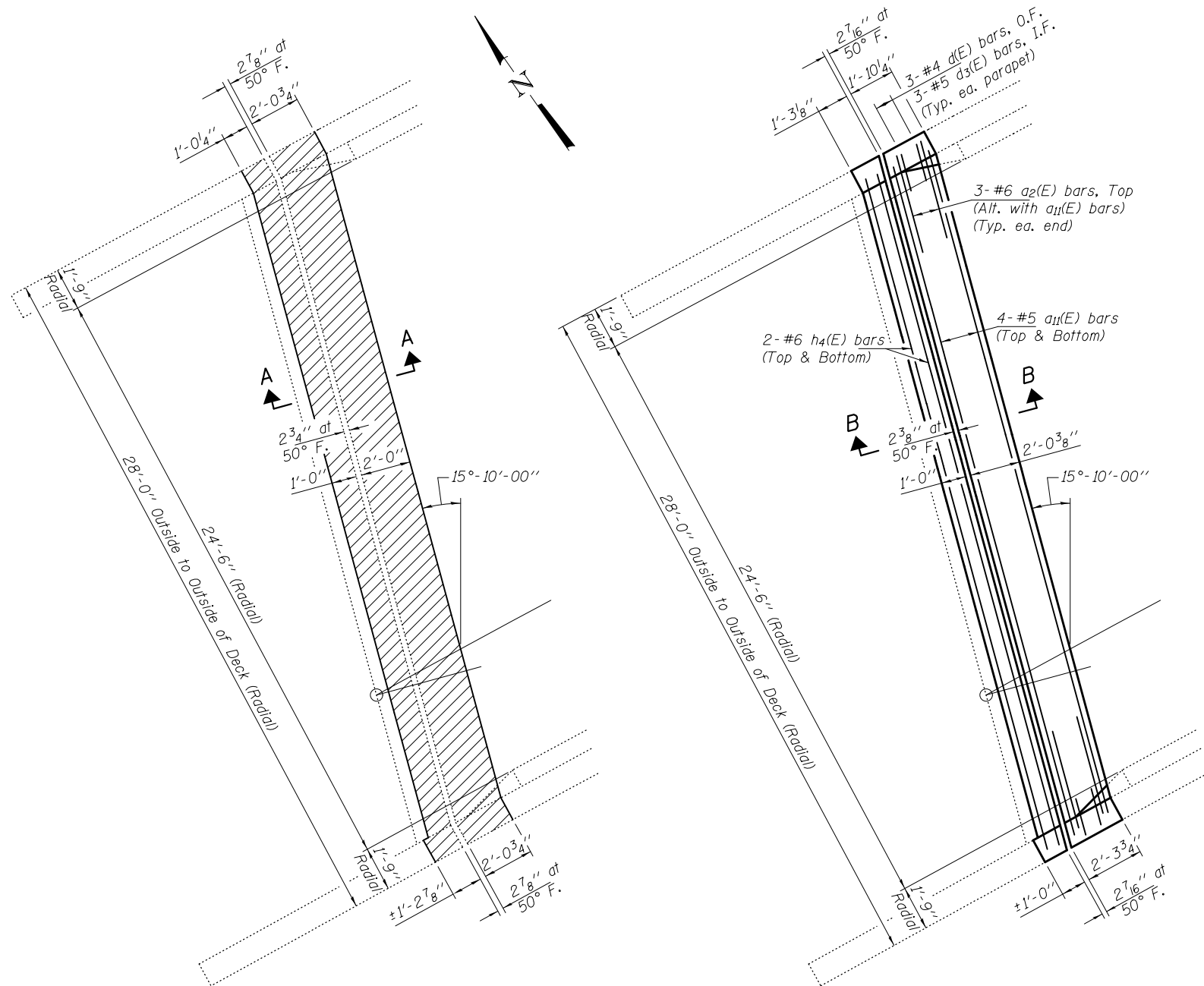
Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kroyer</i>	REVISED
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

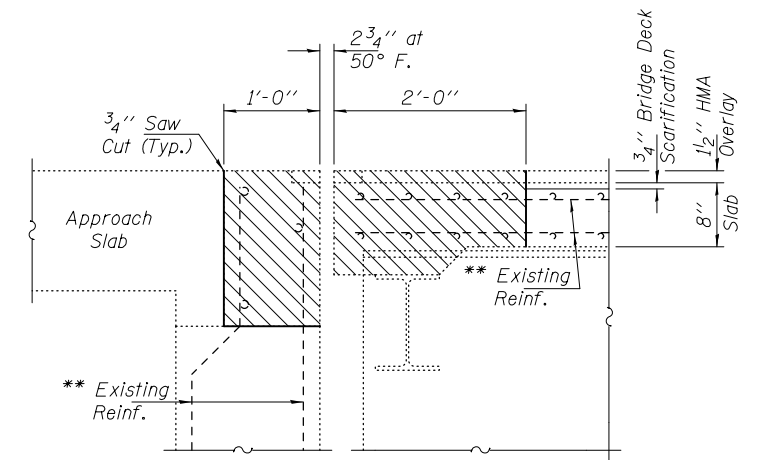
SOUTH ABUTMENT JOINT REMOVAL & REPLACEMENT DETAILS
SN 071-0131 (E.B.)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	108
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	

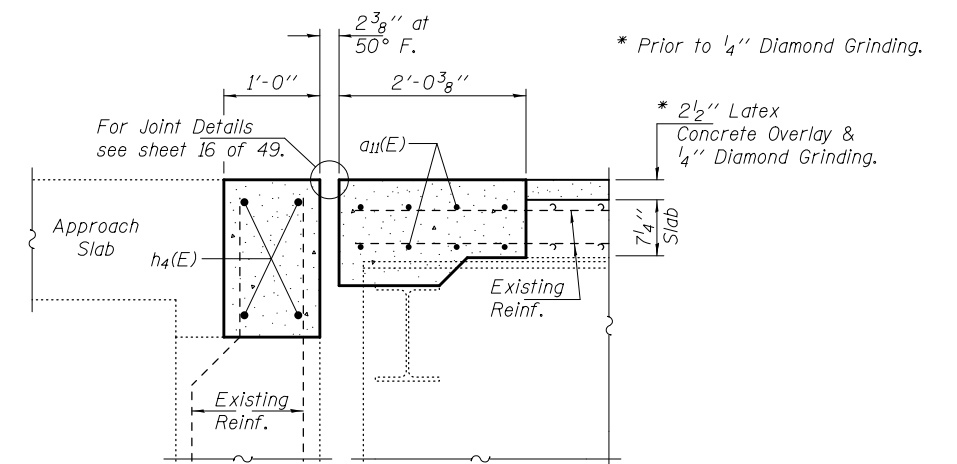


CONCRETE REMOVAL PLAN

CONCRETE REPLACEMENT PLAN



SECTION A-A
(Dims. at Rt. L's)



SECTION B-B
(Dims. at Rt. L's)

Notes:
Hatched areas indicate Concrete Removal.
For parapet details and reinforcement bending details see sheet 15 of 49.

** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₂ (E)	6	#6	4'-0"	—
a ₁₁ (E)	8	#5	28'-6"	—
d(E)	6	#4	4'-10"	L
d ₃ (E)	6	#5	4'-6"	J
h ₄ (E)	4	#6	28'-6"	—
Concrete Removal			Cu. Yd.	4.3
Concrete Superstructure			Cu. Yd.	4.3
Reinforcement Bars, Epoxy Coated			Pound	490

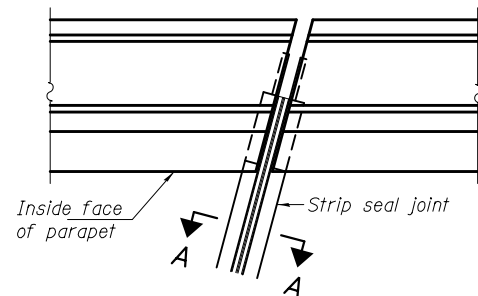
DESIGNED - SMR	EXAMINED - <i>Timothy A. D... ENGINEER OF STRUCTURAL SERVICES</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	PASSED - <i>Carl... ENGINEER OF BRIDGES AND STRUCTURES</i>	REVISED
DRAWN - Kyle M. Steffen		REVISED
CHECKED - SMR RPN		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RAMP "C" NORTH ABUTMENT JOINT REPLACEMENT DETAILS
SN 072-0131 (E.B.)

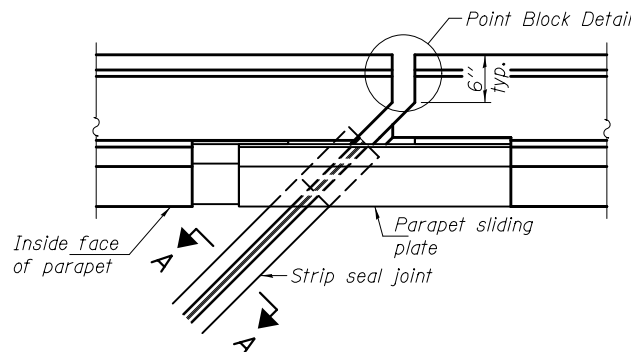
SHEET NO. 14 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	109
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	

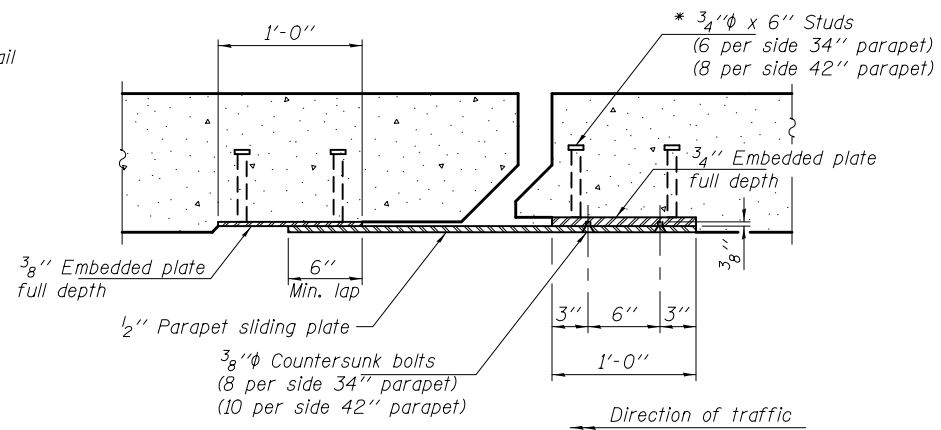


FOR SKEWS = 30°

PLAN AT PARAPET



FOR SKEWS > 30°



SECTION B-B

Direction of traffic

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

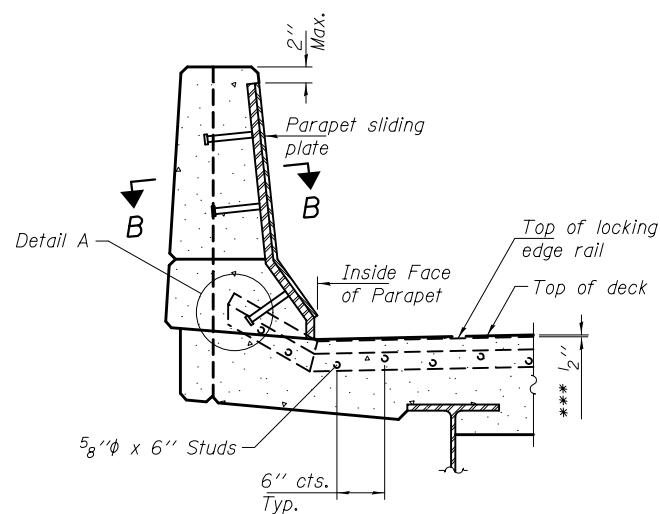
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

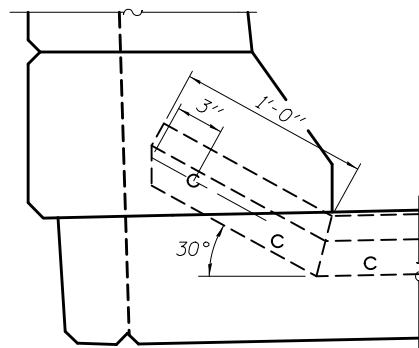
34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

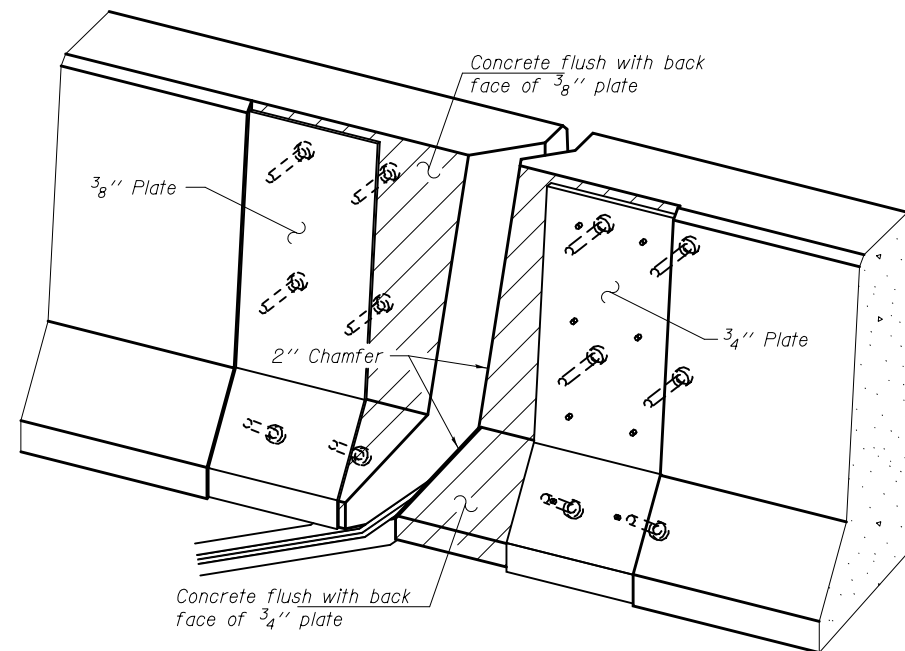


ELEVATION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

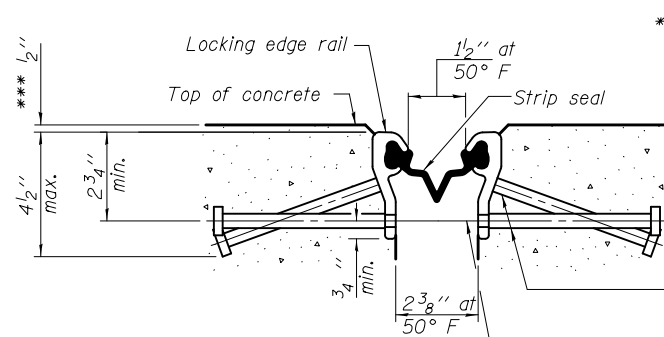


DETAIL A



TRIMETRIC VIEW

(Showing embedded plates only)



SHOWING ROLLED RAIL JOINT

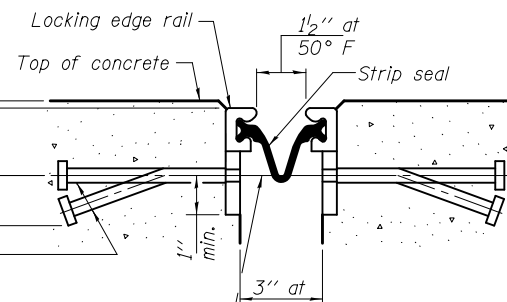
*** Prior to 1/4" Diamond Grinding.

* 5/8" x 6" studs at 6" cts. (alternate angled/bent studs with horizontal studs)

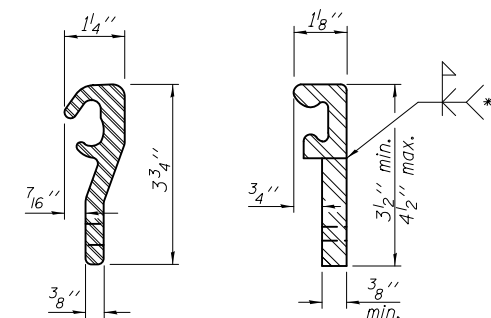
3/8" threaded rods in 7/16" holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



SHOWING WELDED RAIL JOINT

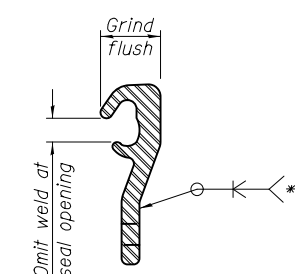


ROLLED (EXTRUDED) RAIL

WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	500

EJ-SS

4-10-17

DESIGNED - SMR	CHECKED - RPN	DRAWN - Kyle M. Steffen	CHECKED - SMR RPN
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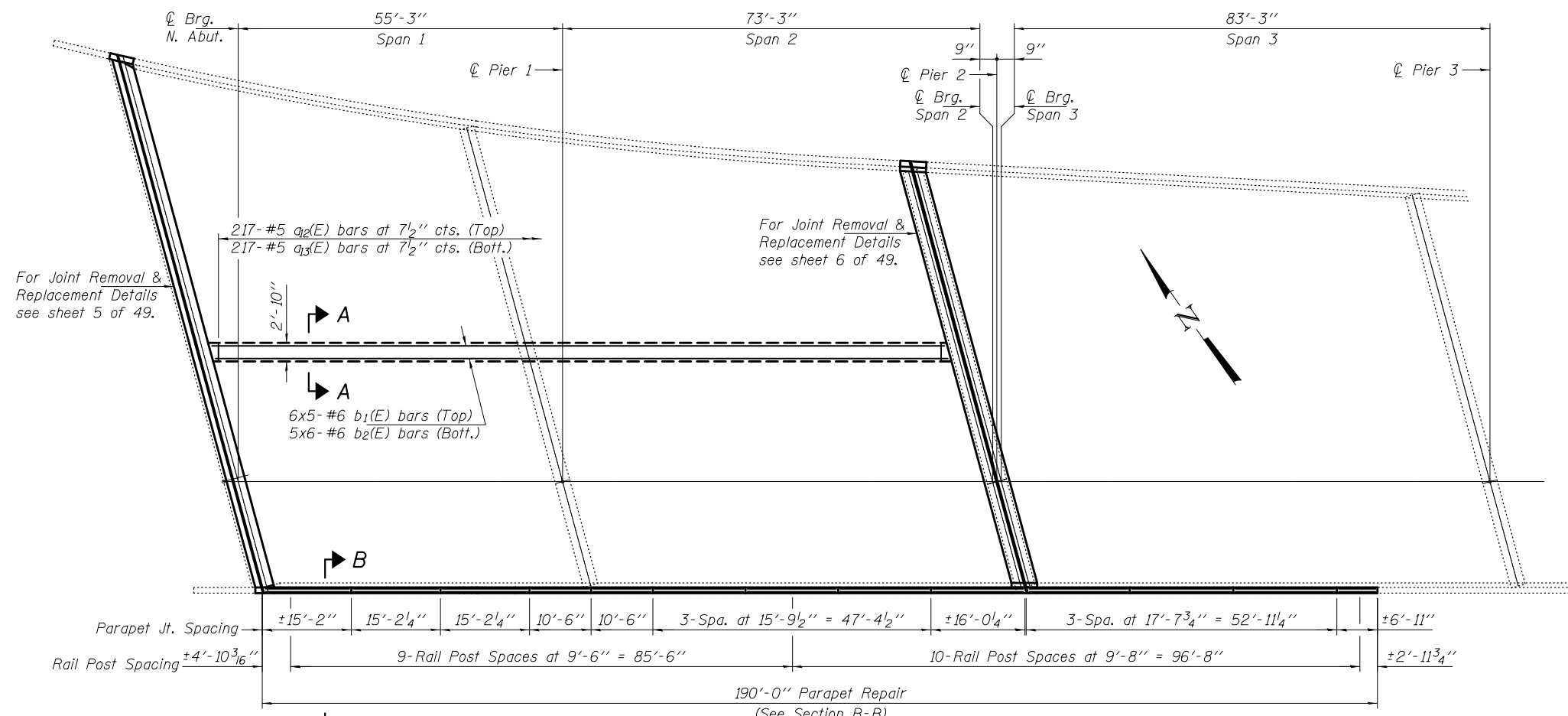
EXAMINED	PASSED	DATE - JANUARY 31, 2018
 ENGINEER OF STRUCTURAL SERVICES		 ENGINEER OF BRIDGES AND STRUCTURES
REVISOR	REVISION	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS
SN 072-0131 (E.B.) & -0132 (W.B.)

SHEET NO. 16 OF 49 SHEETS

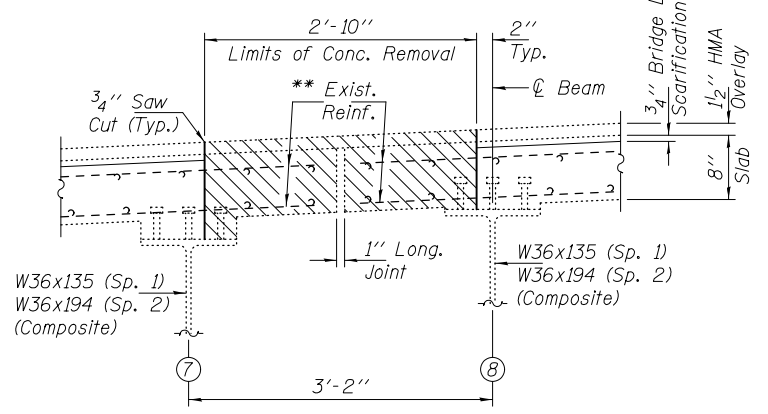
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	111
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



PARTIAL PLAN - SPANS 1 THRU 3

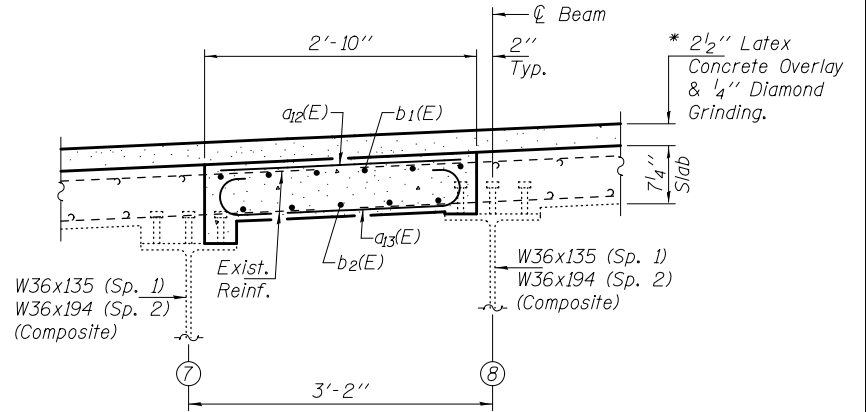
For Joint Removal & Replacement Details see sheet 5 of 49.

For Joint Removal & Replacement Details see sheet 6 of 49.



SECTION A-A SHOWING REMOVAL

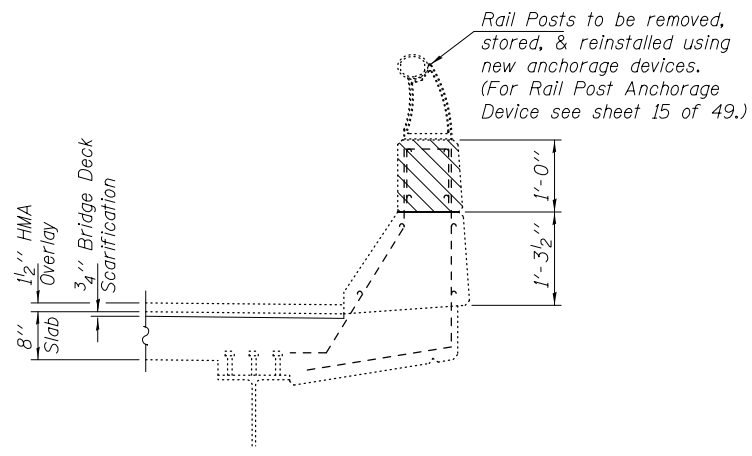
** Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



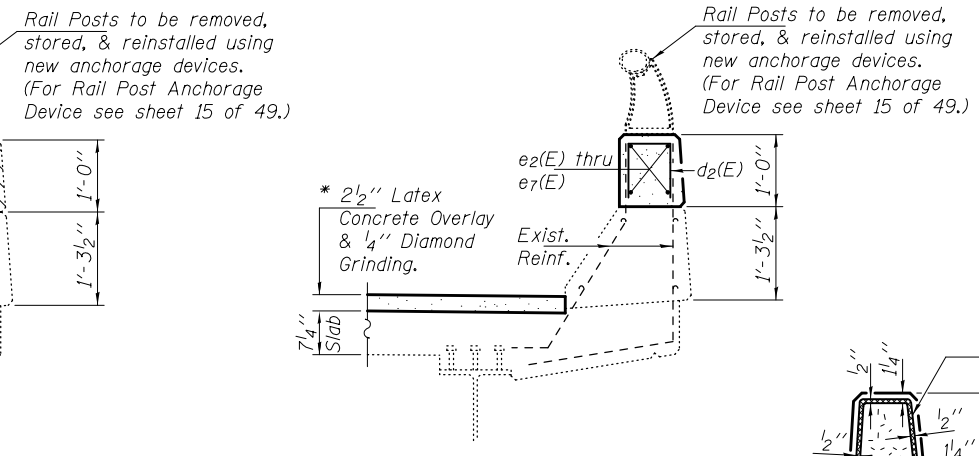
SECTION A-A SHOWING REPLACEMENT

* Prior to 1/4" Diamond Grinding.

Notes:
Hatched areas indicate Concrete Removal.

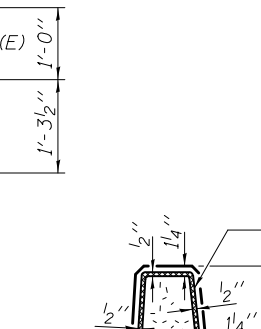


SECTION B-B SHOWING REMOVAL

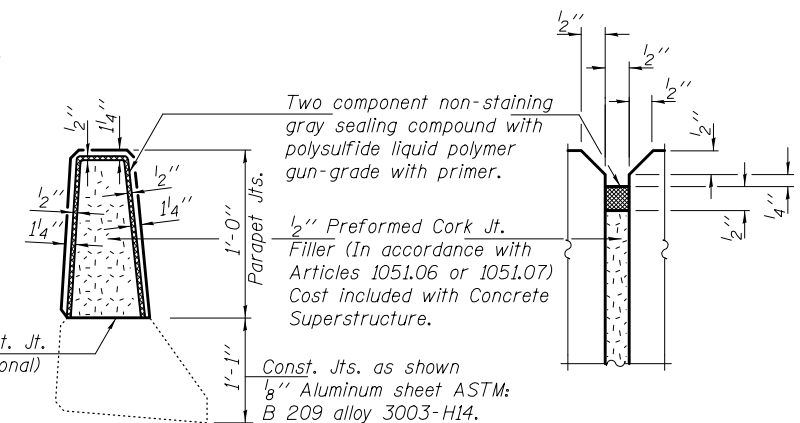


SECTION B-B SHOWING REPLACEMENT

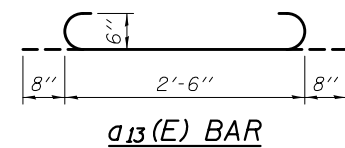
Rail Posts to be removed, stored, & reinstalled using new anchorage devices. (For Rail Post Anchorage Device see sheet 15 of 49.)



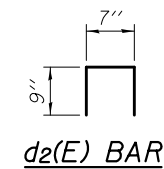
Const. Jts. (Optional)



PARAPET JOINT DETAILS



a13(E) BAR



d2(E) BAR

MIN. LAP LENGTHS
#6 bars = 4'-0"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a2(E)	217	#5	2'-7"	—
a3(E)	217	#5	3'-10"	U
b1(E)	30	#6	28'-4"	—
b2(E)	30	#6	24'-3"	—
d2(E)	38	#4	2'-1"	□
e2(E)	4	#4	6'-8"	—
e3(E)	12	#4	17'-4"	—
e4(E)	4	#4	15'-9"	—
e5(E)	12	#4	15'-6"	—
e6(E)	8	#4	10'-2"	—
e7(E)	12	#4	14'-11"	—

Concrete Removal	Cu. Yd.	16.6
Concrete Superstructure	Cu. Yd.	14.2
Reinforcement Bars, Epoxy Coated	Pound	4380

Bars indicated thus 1 x 2-#5 etc. indicates 1 line of bars with 2 lengths per line.

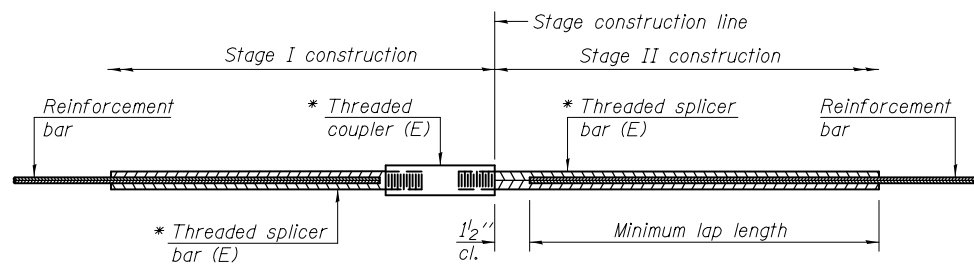
DESIGNED - SMR	EXAMINED - <i>Timothy A. Smith</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kreyer</i>	REVISED
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LONGITUDINAL JOINT CLOSURE AND PARAPET REPAIR DETAILS
SN 072-0132 (W.B.)

SHEET NO. 17 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	112
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	



STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1/2" + thread length

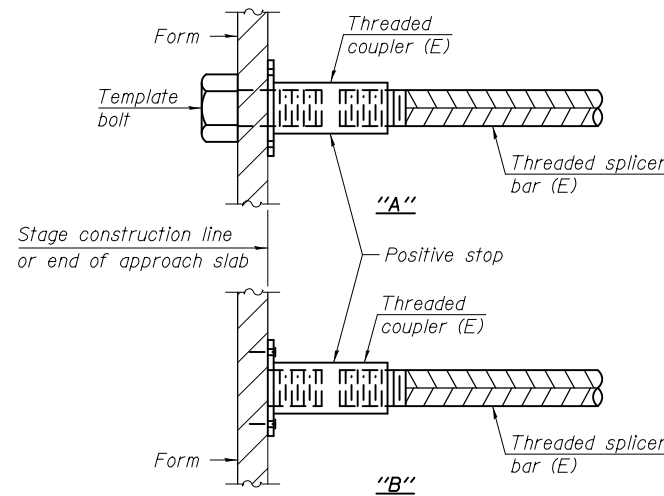
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

SN 072-0131 (E.B.)

Location	Bar size	No. assemblies required	Minimum lap length
N. Abut.	#5	8	3'-6"
N. Abut.	#6	4	4'-0"
Pier 4	#5	16	3'-6"
Pier 5	#5	36	3'-6"
S. Abut.	#5	8	3'-6"
S. Abut.	#6	4	4'-0"

SN 072-0132 (W.B.)

Location	Bar size	No. assemblies required	Minimum lap length
N. Abut.	#5	8	3'-6"
N. Abut.	#6	4	4'-0"
Pier 2	#5	16	3'-6"
Pier 4	#5	16	3'-6"
Pier 5	#5	36	3'-6"
S. Abut.	#5	8	3'-6"
S. Abut.	#6	4	4'-0"

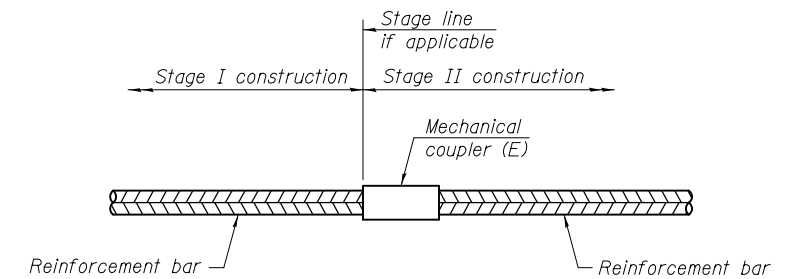


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

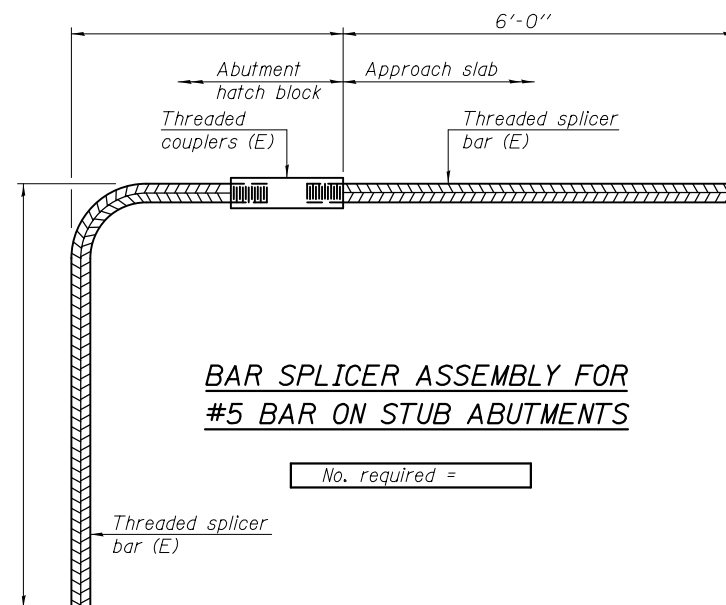
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required
Pier 5 (E.B.)	#5	316
Pier 5 (W.B.)	#5	260



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

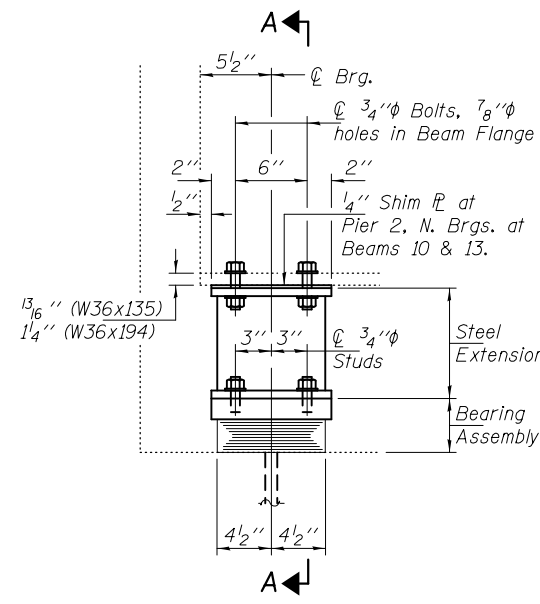
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

6-8-15

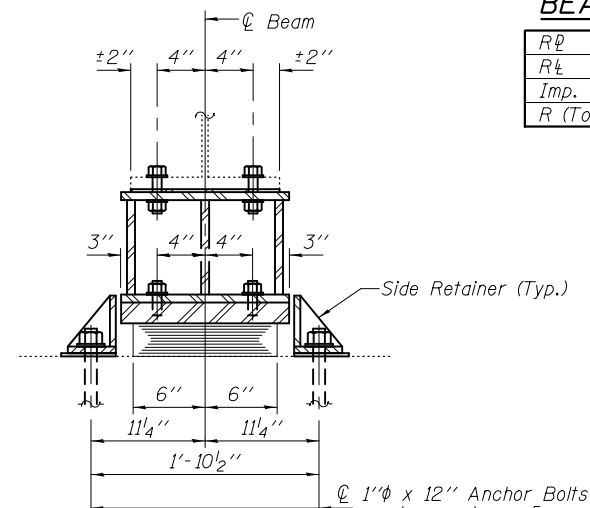
DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>	DATE - JANUARY 31, 2018	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS SN 072-0131 (E.B.) & -0132 (W.B.)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
CHECKED - RPN	PASSED - <i>Carl Kroyer</i>	REVIS			474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	113	
DRAWN - Kyle M. Steffen	ENGINEER OF BRIDGES AND STRUCTURES	REVIS			CONTRACT NO. 68887					
CHECKED - SMR RPN					SHEET NO. 18 OF 49 SHEETS					

ILLINOIS FED. AID PROJECT



ELEVATION

TYPE I ELASTOMERIC EXP. BRG.



SECTION A-A

**NORTH ABUTMENT
BEAM REACTIONS**

R _∅	(K)	25.4
R _⊥	(K)	42.4
Imp.	(K)	11.8
R (Total)	(K)	79.6

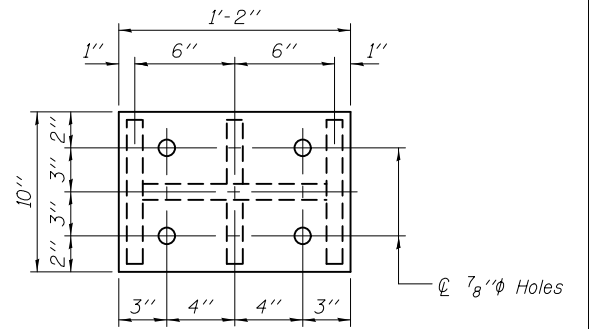
**PIER 2 NORTH
BEAM REACTIONS**

R _∅	(K)	41.0
R _⊥	(K)	45.2
Imp.	(K)	11.4
R (Total)	(K)	97.6

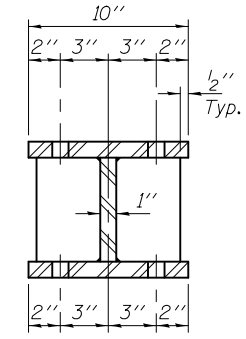
**PIER 4 NORTH
BEAM REACTIONS**

R _∅	(K)	26.8
R _⊥	(K)	42.1
Imp.	(K)	11.3
R (Total)	(K)	80.2

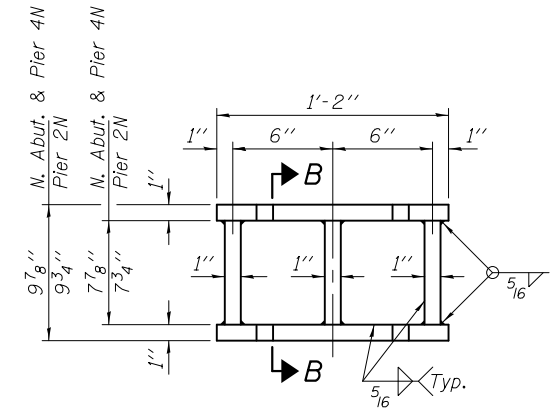
Notes:
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
 Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).
 Min. jack capacity = 52 Tons.
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Cost of Side retainers and Stainless Steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.
 New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.



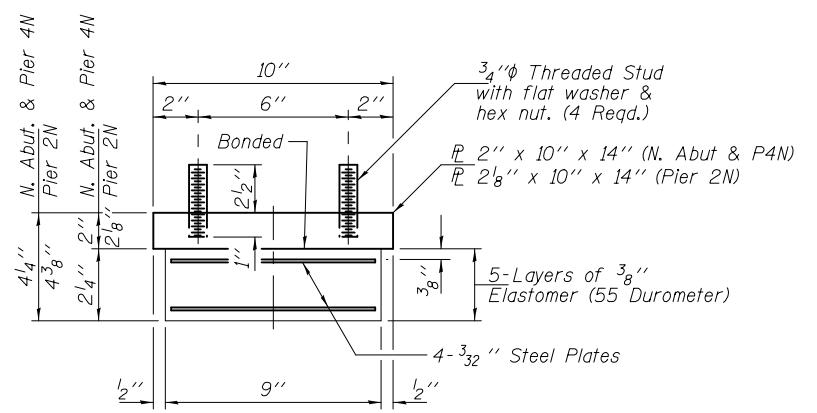
PLAN TOP AND BOTTOM PLATE



SECTION B-B



STEEL EXTENSION DETAIL



BEARING ASSEMBLY

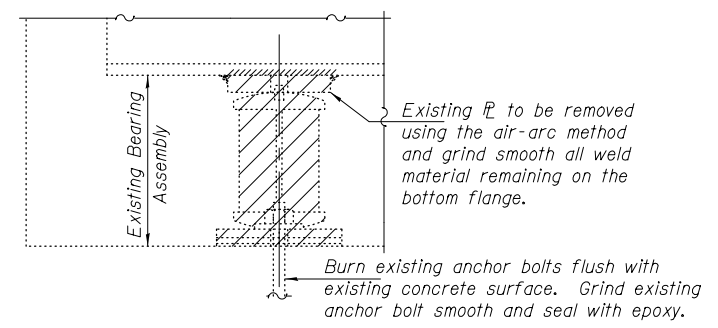
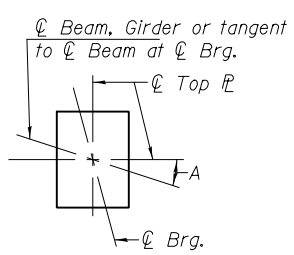
Note:
 Shim plates shall not be placed under Bearing Assembly.

SN 072-0132 (W.B.) - SPANS 1 & 2

Beam	∅ Brg. at N. Abut.	∅ N. Brg. at Pier 2
1	12°-35'-25"	3°-01'-34"
2	5°-41'-44"	5°-41'-44"
3	3°-50'-18"	3°-50'-18"
4	1°-56'-21"	1°-56'-21"
5-13	0°-00'-00"	0°-00'-00"

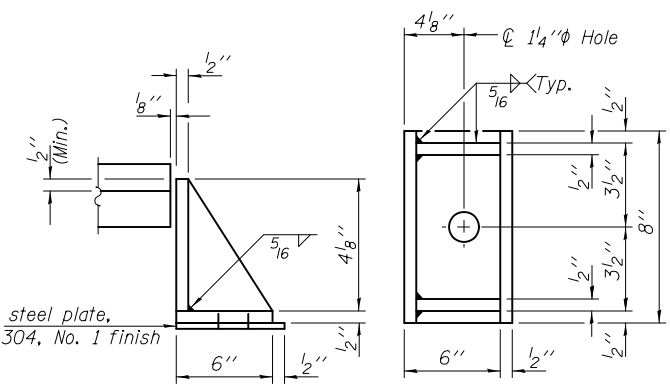
SN 072-0132 (W.B.)
 SPANS 3 & 4

Beam	Angle A
1	3°-01'-34"
2	1°-31'-11"
3-10	0°-00'-00"



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	36
Jack and Remove Existing Bearings	Each	36
Furnishing and Erecting Structural Steel	Pound	5950
Anchor Bolts 1"∅	Each	72

TYI/REPS 1-18-2017

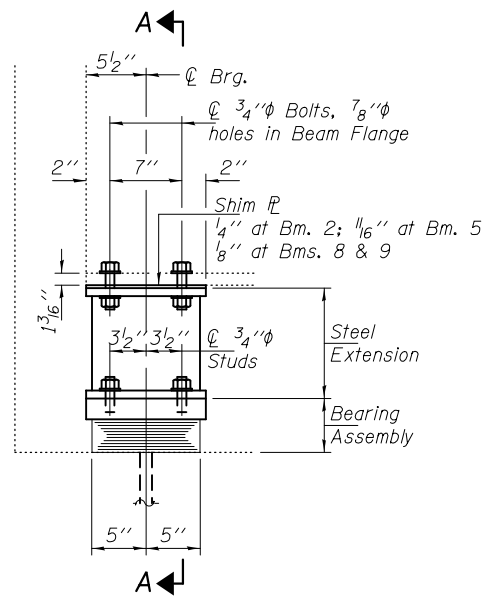
DESIGNED - SMR	EXAMINED - <i>Timothy A. D... ENGINEER OF STRUCTURAL SERVICES</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	PASSED - <i>Carl... ENGINEER OF BRIDGES AND STRUCTURES</i>	REVISED
DRAWN - Kyle M. Steffen		REVISED
CHECKED - SMR RPN		

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BEARING REPLACEMENT DETAILS
 NORTH ABUTMENT, PIER 2 NORTH, & PIER 4 NORTH
 SN 072-0132 (W.B.)

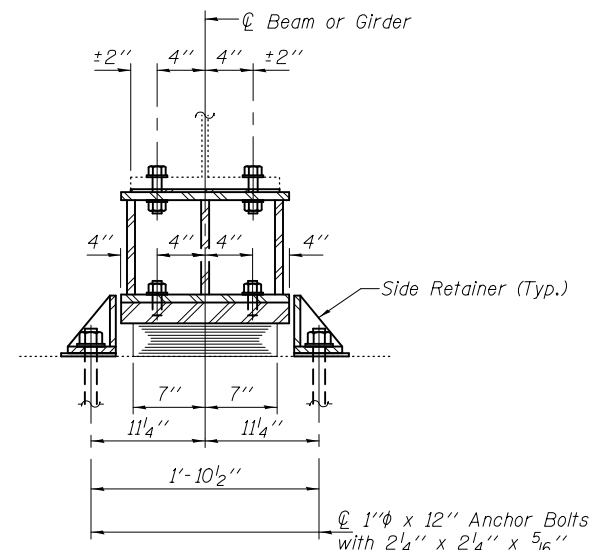
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	114
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

SHEET NO. 19 OF 49 SHEETS



ELEVATION

TYPE I ELASTOMERIC EXP. BRG.

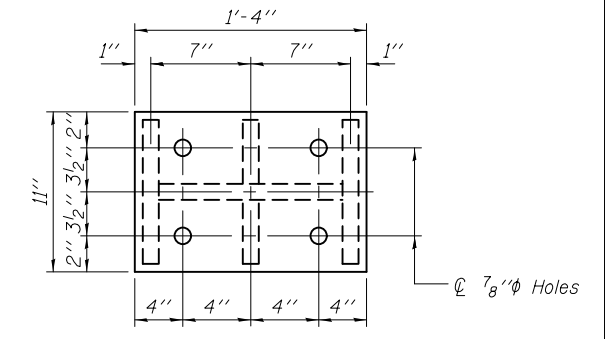


SECTION A-A

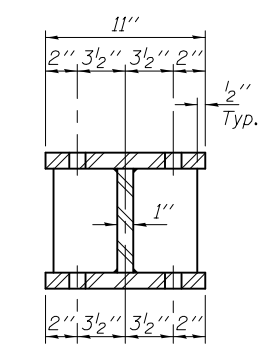
PIER 2 SOUTH GIRDER REACTIONS

R ₁	(K)	45.6
R ₂	(K)	44.7
Imp.	(K)	10.7
R (Total)	(K)	101.0

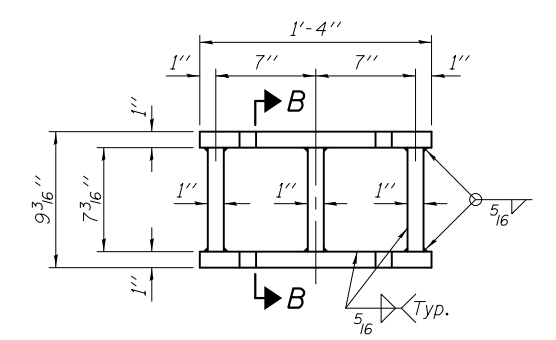
Notes:
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel. Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).
 Min. jack capacity = 60 Tons.
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Cost of Side retainers and Stainless Steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.
 New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.



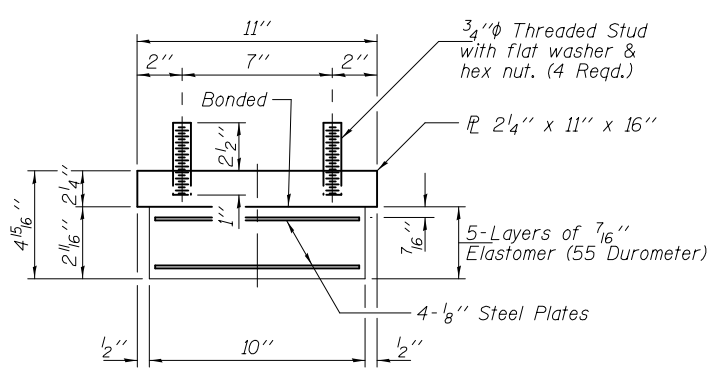
PLAN TOP AND BOTTOM PLATE



SECTION B-B

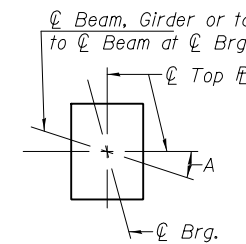


STEEL EXTENSION DETAIL

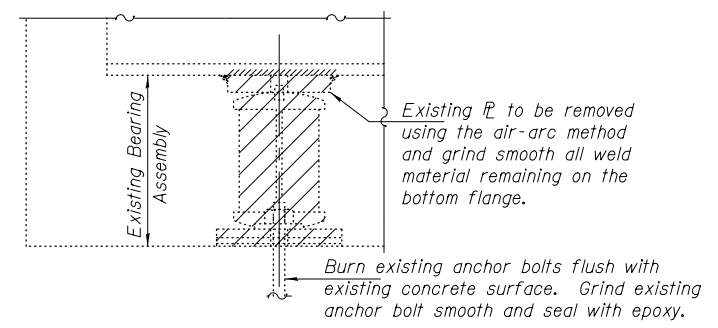


BEARING ASSEMBLY

Note:
 Shim plates shall not be placed under Bearing Assembly.

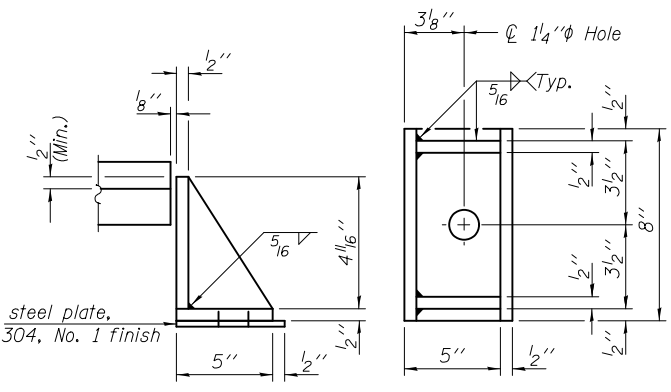


SN 072-0132 (W.B.)	
SPANS 3 & 4	
Beam	Angle A
1	3°-01'-34"
2	1°-31'-11"
3-10	0°-00'-00"



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.



PIER 2 SOUTH SIDE RETAINER

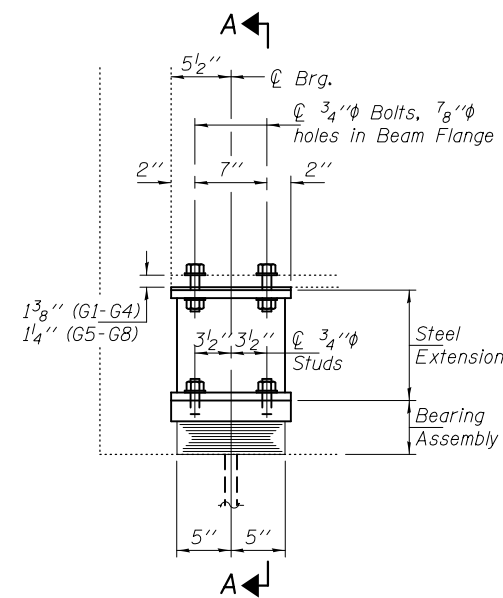
(20-Required)
 Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

1/4" Stainless steel plate, A240, Type 304, No. 1 Finish

BILL OF MATERIAL

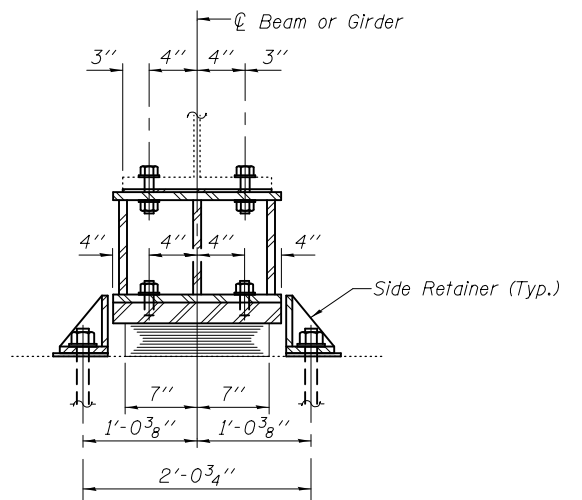
Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	10
Jack and Remove Existing Bearings	Each	10
Furnishing and Erecting Structural Steel	Pound	1930
Anchor Bolts 1" ϕ	Each	20

TYI/REPS 1-18-2017



ELEVATION

TYPE I ELASTOMERIC EXP. BRG.

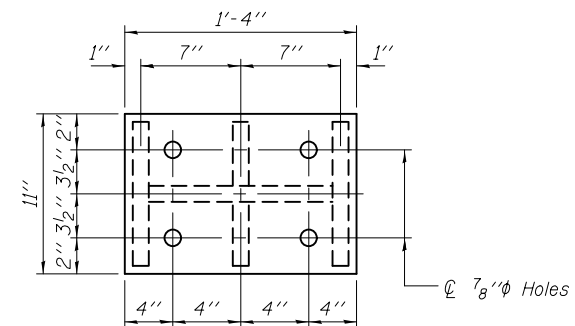


SECTION A-A

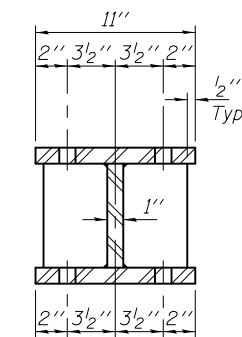
PIER 4 SOUTH GIRDER REACTIONS

R ₁	(K)	47.8
R ₂	(K)	51.0
Imp.	(K)	11.9
R (Total)	(K)	110.7

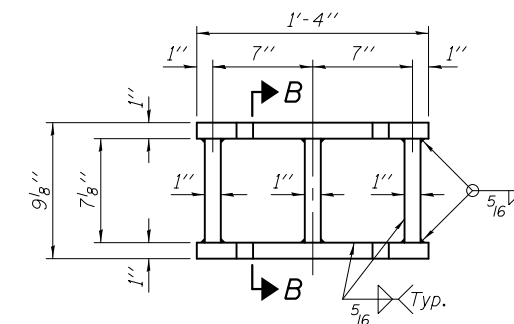
Notes:
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
 Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).
 Min. jack capacity = 60 Tons.
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Drilled and set anchor bolts shall be installed according to Article 52.106 of the Standard Specifications.
 Cost of Side retainers and Stainless Steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.
 New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.



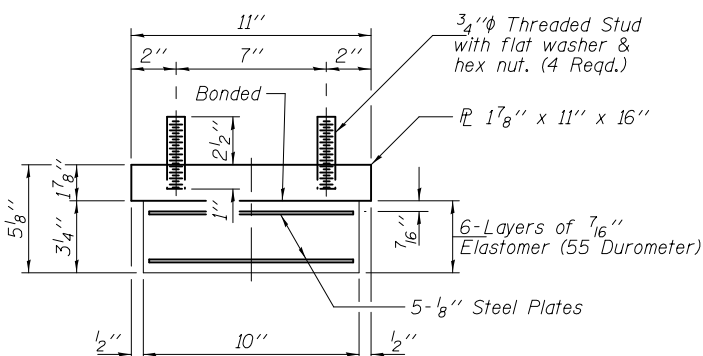
PLAN TOP AND BOTTOM PLATE



SECTION B-B

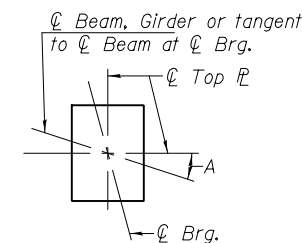


STEEL EXTENSION DETAIL

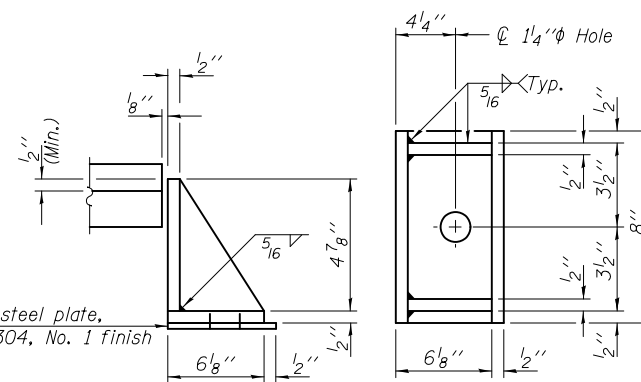


BEARING ASSEMBLY

Note:
 Shim plates shall not be placed under Bearing Assembly.

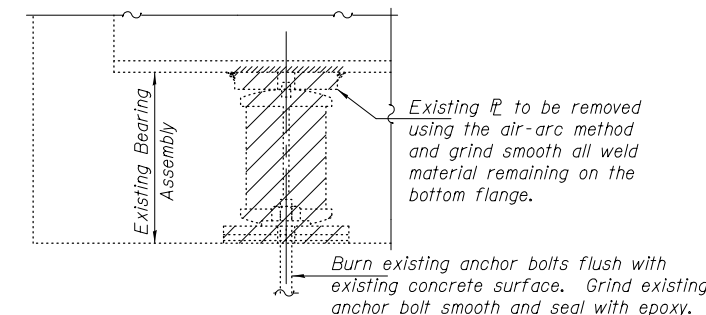


SN 072-0132 (W.B.)	
SPAN 5	
Girder	Angle A
1	3°-01'-37"
2	1°-33'-04"
3-8	0°-00'-00"



PIER 4 SOUTH SIDE RETAINER

(16-Required)
 Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	8
Jack and Remove Existing Bearings	Each	8
Furnishing and Erecting Structural Steel	Pound	1500
Anchor Bolts 1" ϕ	Each	16

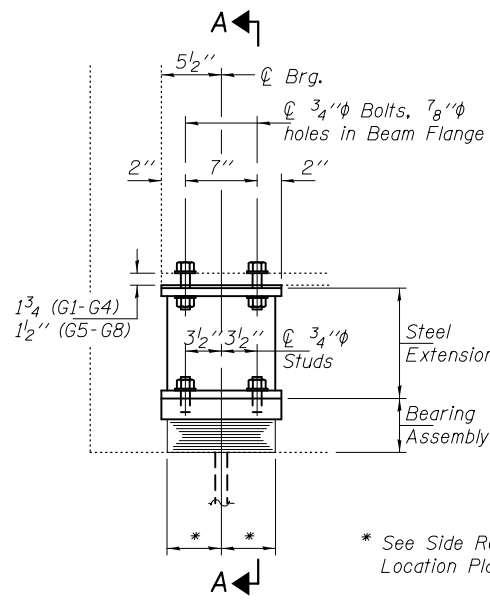
TYI/REPS 1-18-2017

DESIGNED - SMR	EXAMINED - <i>Timothy A. Dwyer</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kroyer</i>	REVISED
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BEARING REPLACEMENT DETAILS
 PIER 4 SOUTH
 SN 072-0132 (W.B.)
 SHEET NO. 21 OF 49 SHEETS

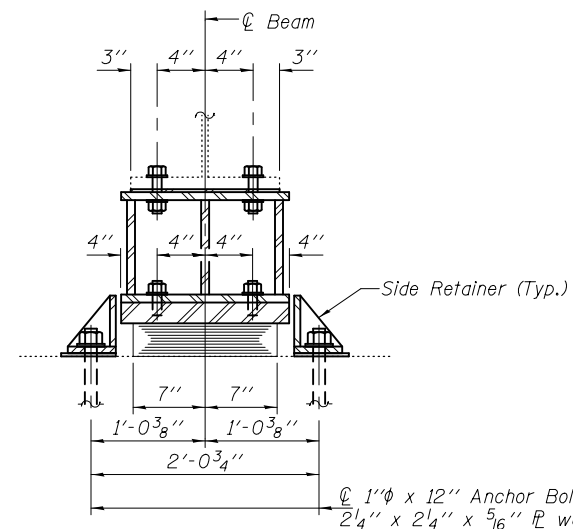
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	116
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



ELEVATION AT PIER

TYPE I ELASTOMERIC EXP. BRG.

* See Side Retainer Location Plan.



SECTION A-A

1" x 12" Anchor Bolts with 2 1/4" x 2 1/4" x 5/16" P washer under nut.

PIER 5 NORTH GIRDER REACTIONS

R ₁	(K)	53.9
R ₂	(K)	51.6
Imp.	(K)	11.8
R (Total)	(K)	117.3

Notes:
Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.

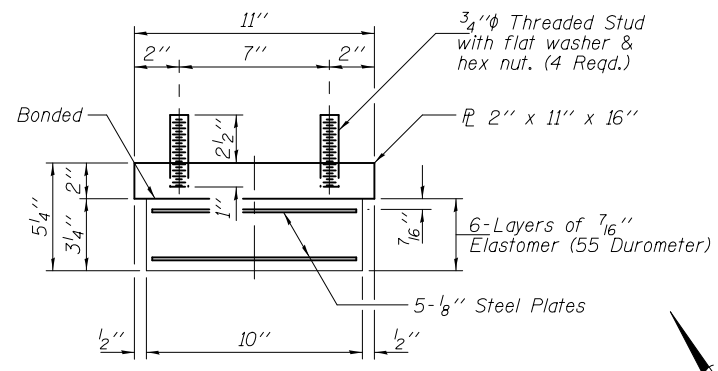
New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel. Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).

Min. jack capacity = 65 Tons.
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

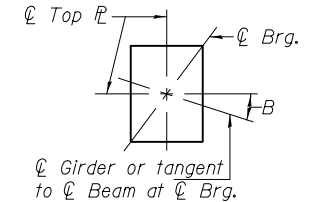
Cost of Side retainers and Stainless Steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.

New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.

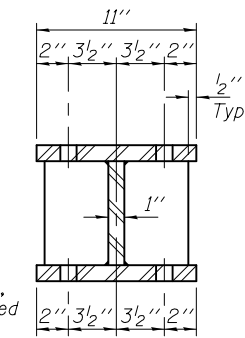


BEARING ASSEMBLY

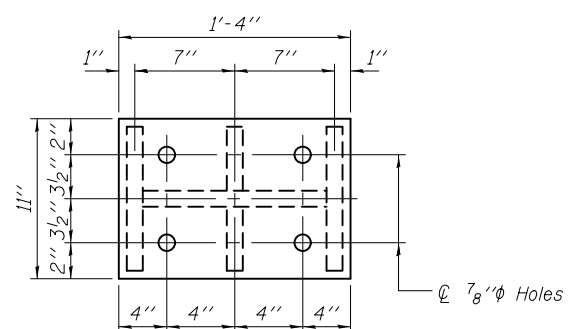
Note:
Shim plates shall not be placed under Bearing Assembly.



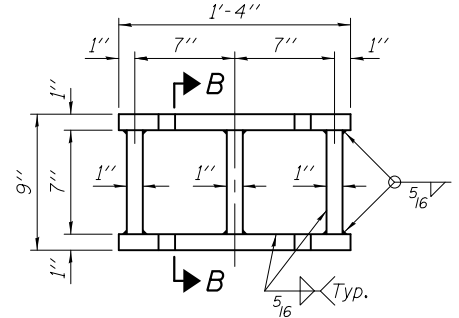
SN 072-0132 (W.B.)	
SPAN 5A	
Girder	Angle B
1	3°-01'-37"
2	1°-33'-04"
3-8	0°-00'-00"



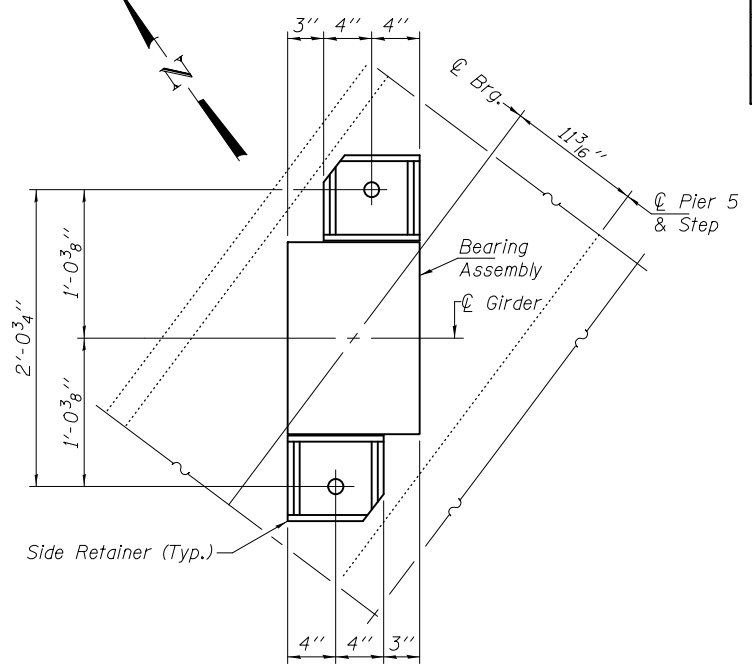
SECTION B-B



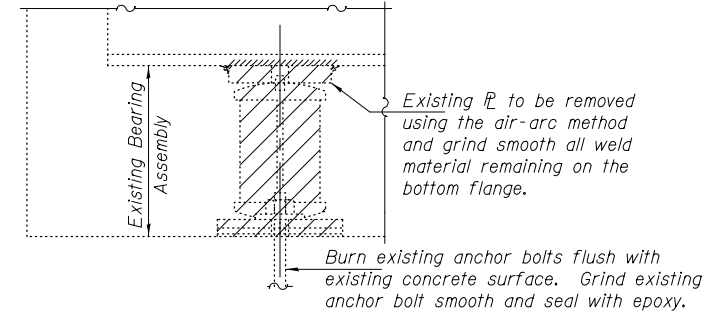
PLAN TOP AND BOTTOM PLATE



STEEL EXTENSION DETAIL

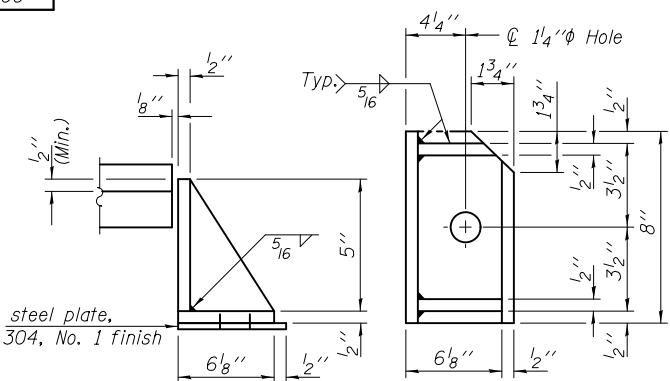


SIDE RETAINER LOCATION PLAN



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	8
* Jack and Remove Existing Bearings	Each	4
Furnishing and Erecting Structural Steel	Pound	1490
Anchor Bolts 1"φ	Each	16

* Temporary Shoring & Cribbing to be used instead of Jack and Remove Existing Bearings at 4 locations. See sheet 37 of 49.

TYI/REPS 1-18-2017

DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kroyer</i>
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 5 NORTH BEARING REPLACEMENT DETAILS
SN 072-0132 (W.B.)

SHEET NO. 22 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB/B-R)	PEORIA	196	117
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

**PIER 5 SOUTH
GIRDER REACTIONS**

R _L	(K)	68.2
R _R	(K)	51.0
Imp.	(K)	10.2
R (Total)	(K)	129.4

Notes:
Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).

Min. jack capacity = 75 Tons.
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts for Type III bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.

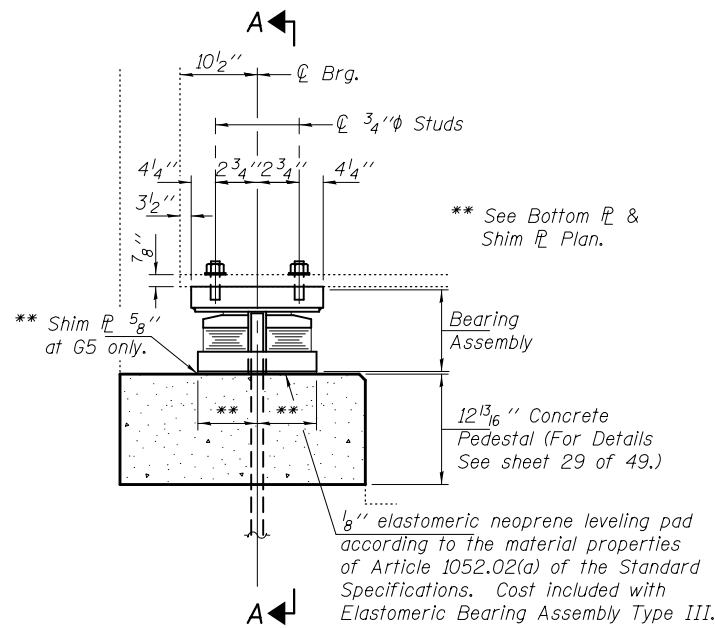
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type III.

The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

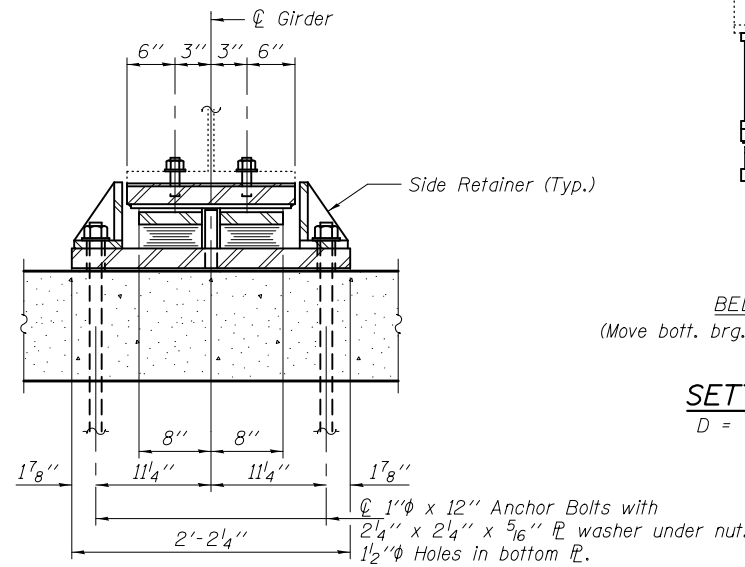
Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.

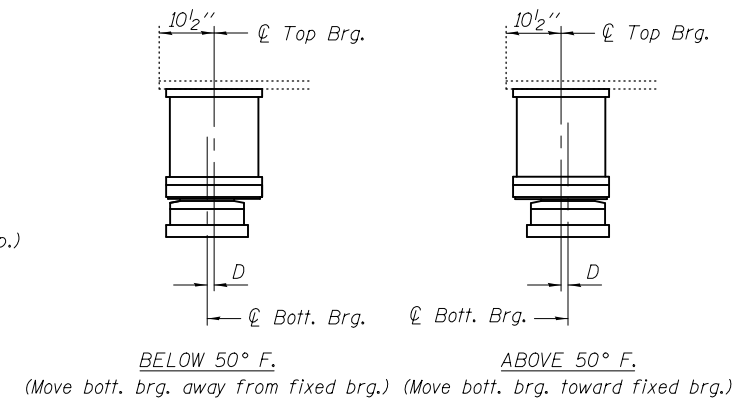


ELEVATION AT PIER 5 SOUTH

TYPE III ELASTOMERIC EXP. BRG.

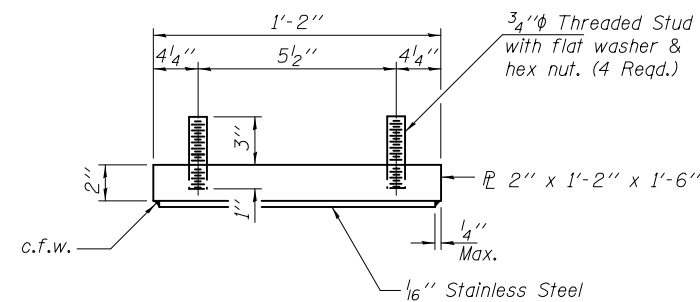


SECTION A-A

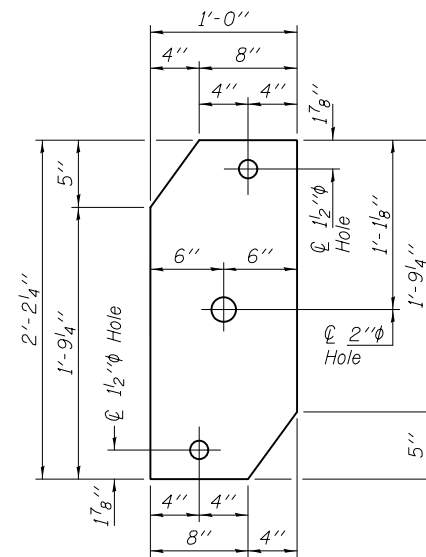


SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

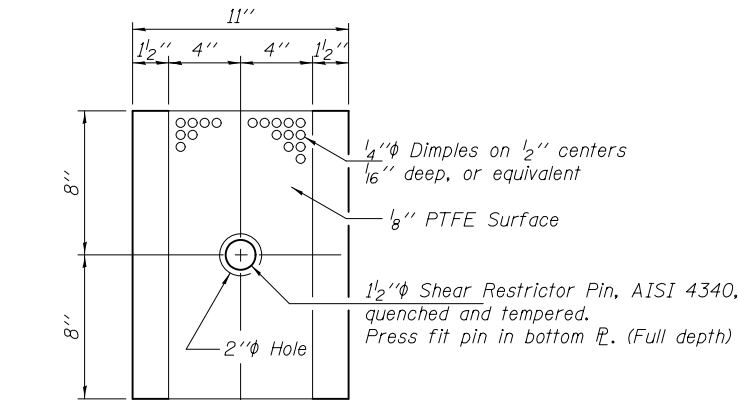


TOP BEARING ASSEMBLY

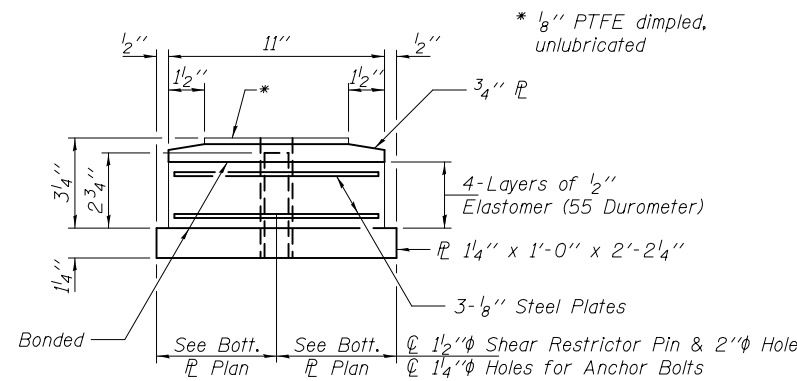


BOTTOM PL & SHIM PL PLAN

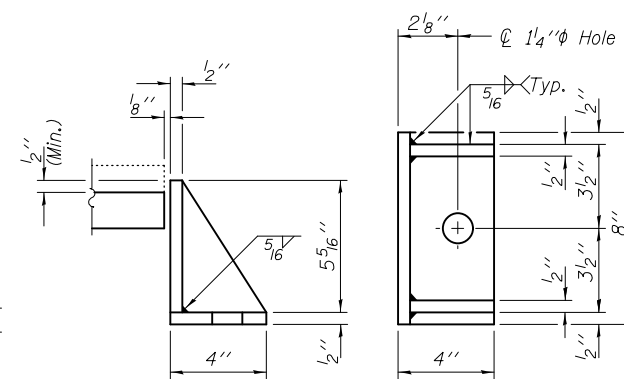
Bottom PL 1 1/4" x 1'-0" x 2'-2 1/4" (7-Req'd)
Shim PL 5/8" x 1'-0" x 2'-2 1/4" (1-Req'd)



PLAN-PTFE ELASTOMERIC BRG.

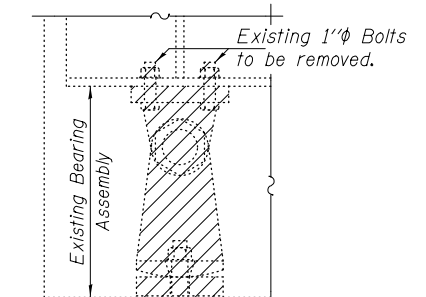


BOTTOM BEARING ASSEMBLY



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type III	Each	7
Temporary Shoring and Cribbing	Each	7
Furnishing and Erecting Structural Steel	Pound	60
Anchor Bolts 1"φ	Each	14

TYIII/REPS 12-03-2008

DESIGNED - SMR	CHECKED - RPN	DRAWN - Kyle M. Steffen	CHECKED - SMR RPN
----------------	---------------	-------------------------	-------------------

EXAMINED	<i>Timothy A. ...</i>	DATE - JANUARY 31, 2018
PASSED	<i>Carl ...</i>	REVISOR

REVISOR	REVISOR
---------	---------

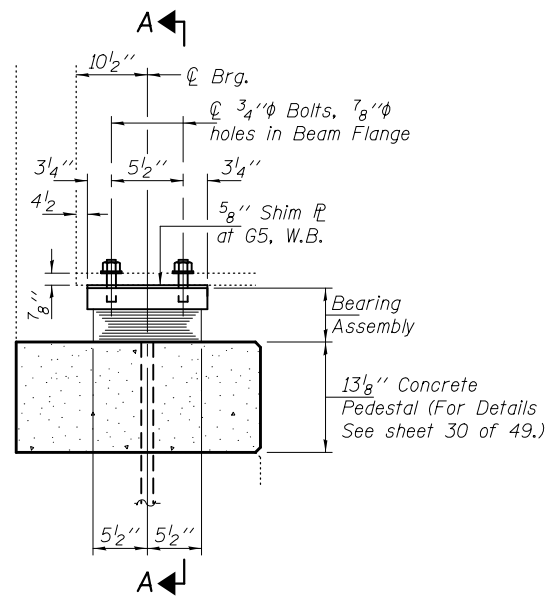
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 5 SOUTH BEARING REPLACEMENT DETAILS
SN 072-0132 (W.B.)**

SHEET NO. 23 OF 49 SHEETS

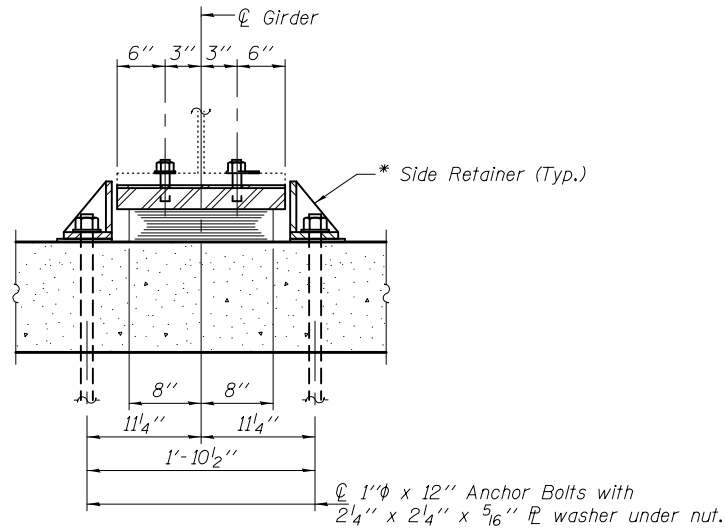
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	118
CONTRACT NO. 68887				

ILLINOIS FED. AID PROJECT

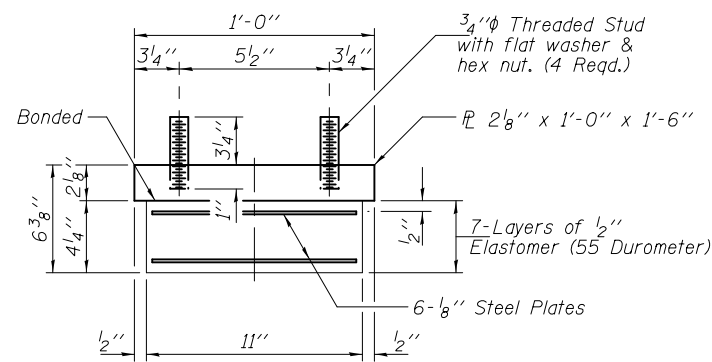


ELEVATION AT SOUTH ABUTMENT

TYPE I ELASTOMERIC EXP. BRG.

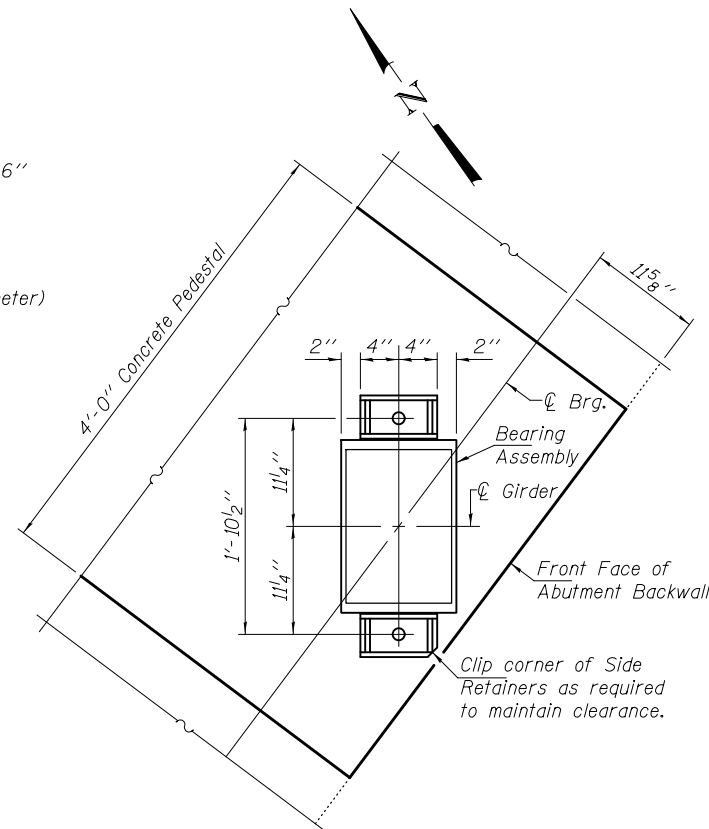


SECTION A-A



BEARING ASSEMBLY

Note: Shim plates shall not be placed under Bearing Assembly.



SIDE RETAINER LOCATION PLAN

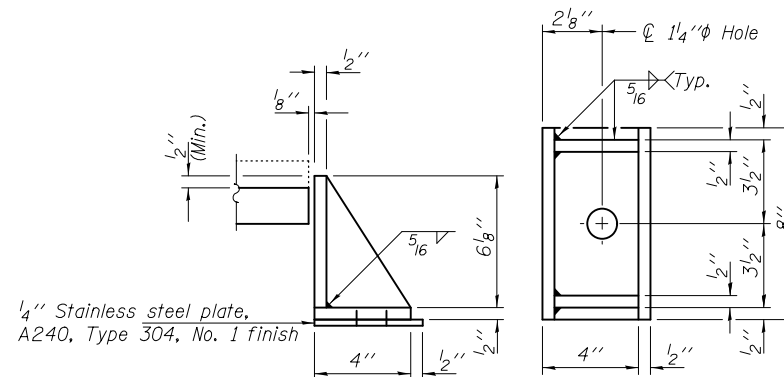
SN 072-0131 (E.B.) GIRDER REACTIONS

R _D	(K)	69.6
R _L	(K)	49.7
Imp.	(K)	9.8
R (Total)	(K)	129.1

SN 072-0132 (W.B.) GIRDER REACTIONS

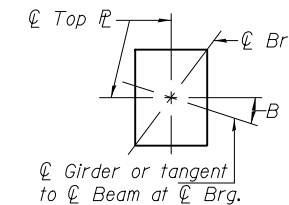
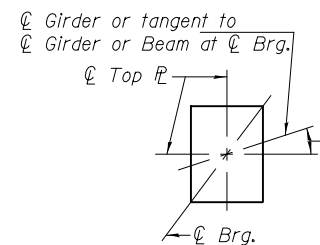
R _D	(K)	70.3
R _L	(K)	50.8
Imp.	(K)	10.0
R (Total)	(K)	131.1

Notes:
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
 Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).
 Min. jack capacity = 76 Tons.
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (F_y=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Cost of Side retainers and Stainless Steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.
 New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.



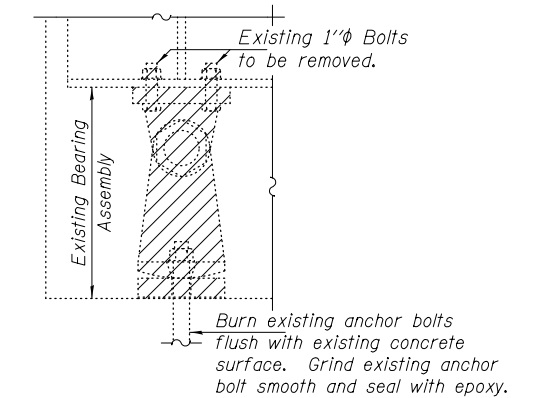
SIDE RETAINER

(30-Required)
 Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



SN 072-0131 (E.B.) SPAN 8	
Girder	Angle D
8-11	0°-00'-00"
12	0°-23'-10"
13	0°-46'-06"
14-15	1°-08'-46"

SN 072-0132 (W.B.) SPAN 8	
Girder	Angle B
1 & 2	1°-50'-36"
3	1°-13'-26"
4	0°-36'-17"
5-7	0°-00'-00"



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	15
Temporary Shoring and Cribbing	Each	15
Furnishing and Erecting Structural Steel	Pound	40
Anchor Bolts 1"φ	Each	30

TYI/REPS 1-18-2017

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

EXAMINED	<i>Timothy A. ...</i>	DATE - JANUARY 31, 2018
PASSED	<i>Carl ...</i>	REVISIONS

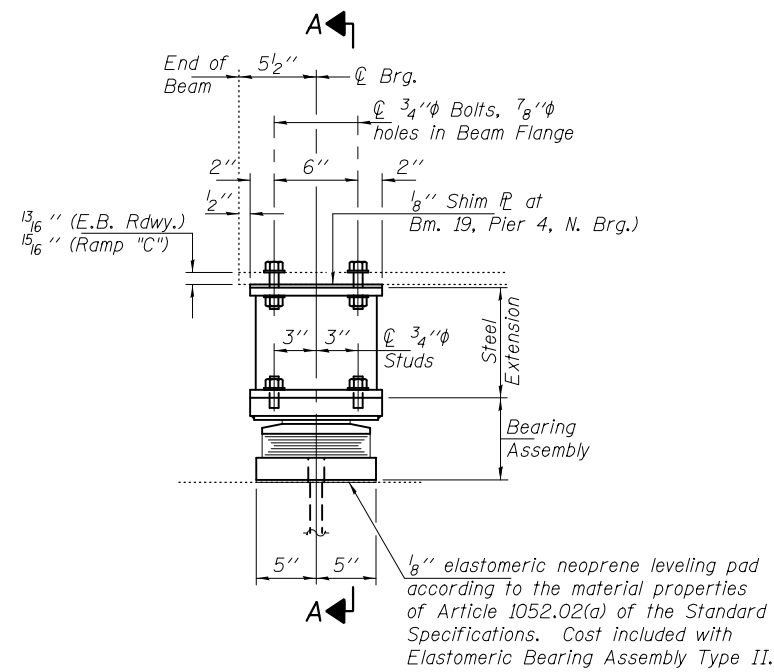
DATE - JANUARY 31, 2018
REVISIONS

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

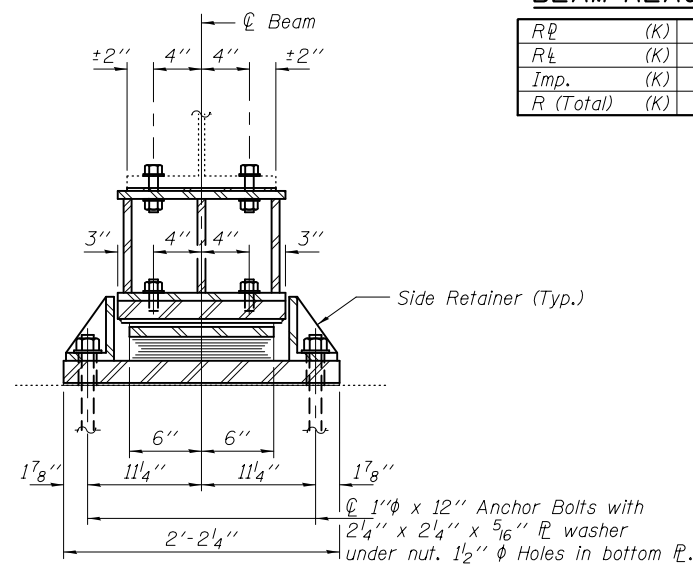
SOUTH ABUTMENT BEARING REPLACEMENT DETAILS SN 072-0131 (E.B.) & -0132 (W.B.)

SHEET NO. 24 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB/B-R)	PEORIA	196	119
				CONTRACT NO. 68887
ILLINOIS FED. AID PROJECT				



ELEVATION



SECTION A-A

E.B. ROADWAY NORTH ABUTMENT BEAM REACTIONS

R ₀	(K)	27.5
R ₁	(K)	40.0
Imp.	(K)	11.1
R (Total)	(K)	78.6

E.B. ROADWAY PIER 4 NORTH BEAM REACTIONS

R ₀	(K)	29.5
R ₁	(K)	40.6
Imp.	(K)	10.9
R (Total)	(K)	81.0

RAMP "C" PIER 4 NORTH BEAM REACTIONS

R ₀	(K)	30.0
R ₁	(K)	41.3
Imp.	(K)	11.2
R (Total)	(K)	82.5

Notes:
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
 Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).
 Min. jack capacity = 45 Tons.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

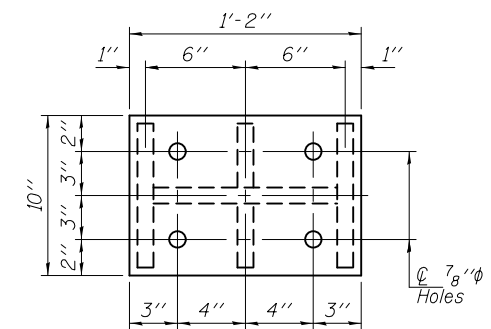
Anchor bolts for Type II bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type II.

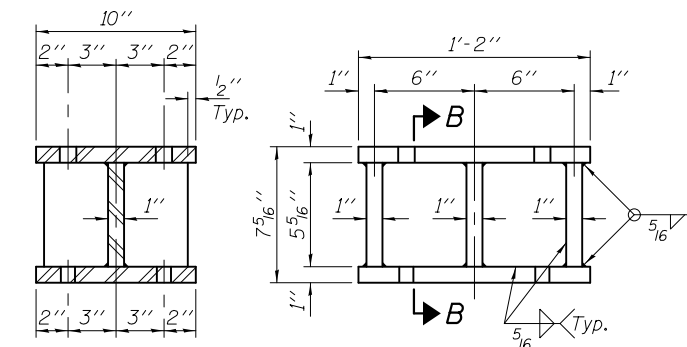
The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.



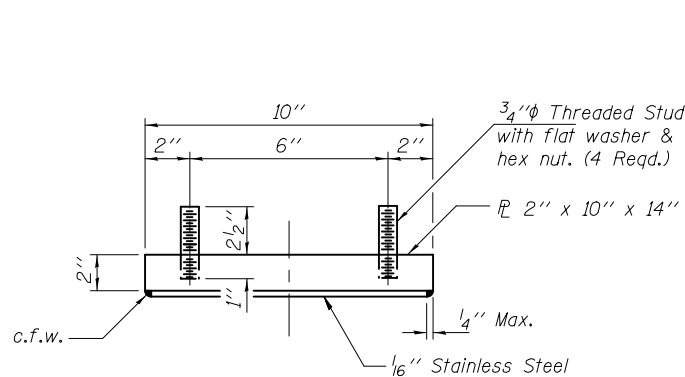
PLAN TOP AND BOTTOM PLATE



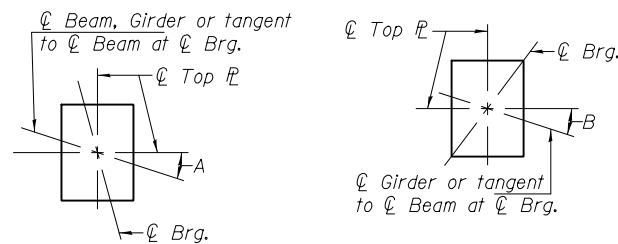
SECTION B-B

STEEL EXTENSION DETAIL

TYPE II TFE ELASTOMERIC EXP. BRG.

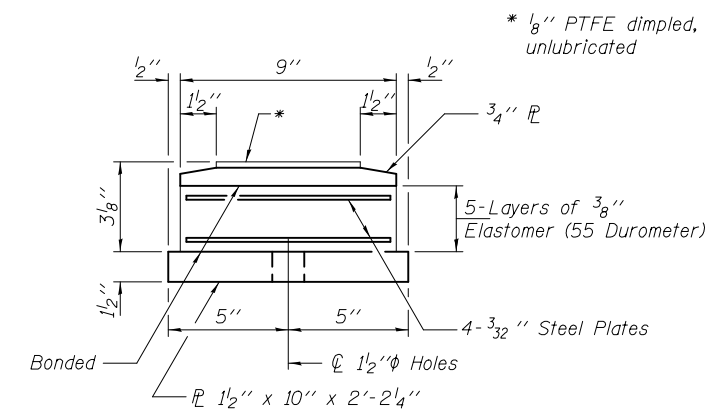


TOP BEARING ASSEMBLY



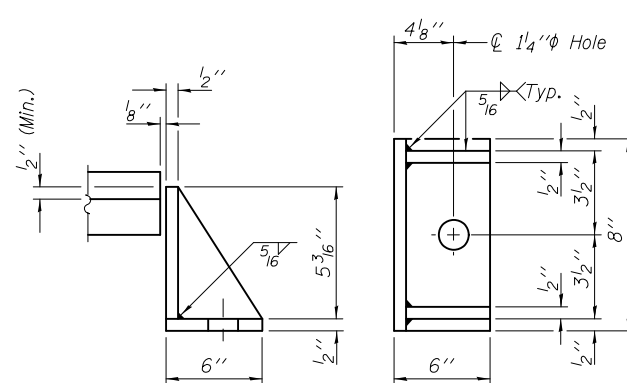
SN 072-0131 (E.B.)	
SPANS 1 & 4	
Beam	Angle A
14-19	0°-00'-00"

RAMP C - SPAN 4		
φ N. Brg. at Pier 4		
Beam	Angle A	Angle B
1	-	0°-04'-46"
2	-	0°-01'-29"
3	0°-01'-31"	-
4	0°-04'-45"	-



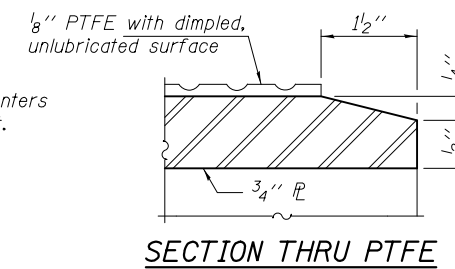
BOTTOM BEARING ASSEMBLY

* 1/8" PTFE dimpled, unlubricated

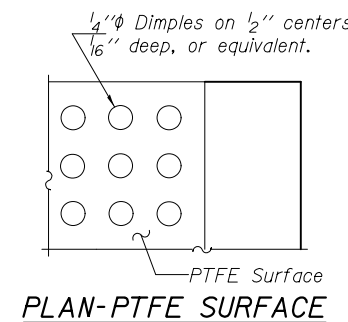


SIDE RETAINER

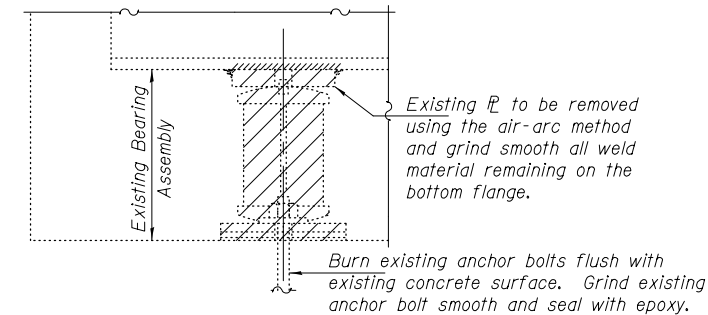
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



SECTION THRU PTFE

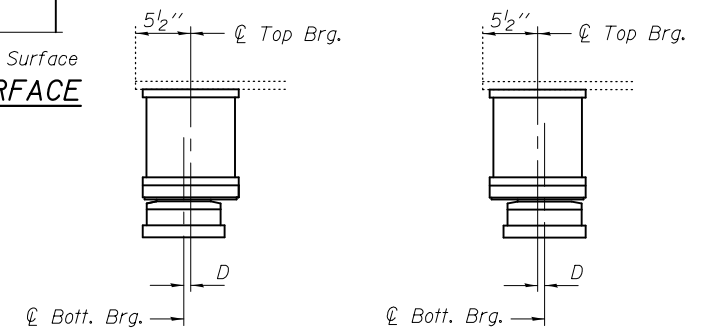


PLAN-PTFE SURFACE



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.



BELOW 50° F. ABOVE 50° F.
 (Move bott. brg. away from fixed brg.) (Move bott. brg. toward fixed brg.)

SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type II	Each	16
Jack and Remove Existing Bearings	Each	16
Furnishing and Erecting Structural Steel	Pound	2220
Anchor Bolts 1" φ	Each	32

TYII/REPS 12-03-2008

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

EXAMINED	Timothy A. [Signature]
PASSED	Carl [Signature]
ENGINEER OF BRIDGES AND STRUCTURES	

DATE - JANUARY 31, 2018
REVISED
REVISED

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

BEARING REPLACEMENT DETAILS	
N. ABUT. OF E.B. RDWY. & PIER 4 NORTH OF E.B. RDWY. & RAMP C	
SN 072-0131 (E.B.) & RAMP C	
SHEET NO. 25 OF 49 SHEETS	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB/B-R)	PEORIA	196	120
				CONTRACT NO. 68887
ILLINOIS FED. AID PROJECT				

**PIER 5 NORTH
GIRDER REACTIONS**

R _D	(K)	56.3
R _L	(K)	48.0
Imp.	(K)	12.1
R (Total)	(K)	116.4

Notes:

Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.

New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).

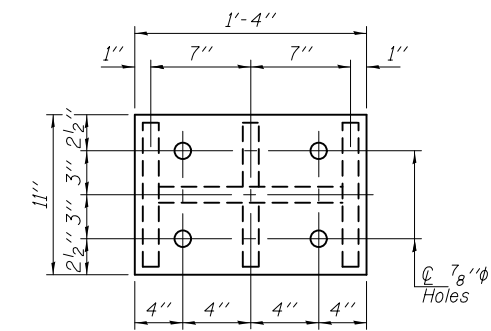
Min. jack capacity = 65 Tons.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

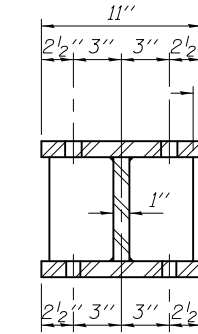
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Cost of Side retainers and Stainless Steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.

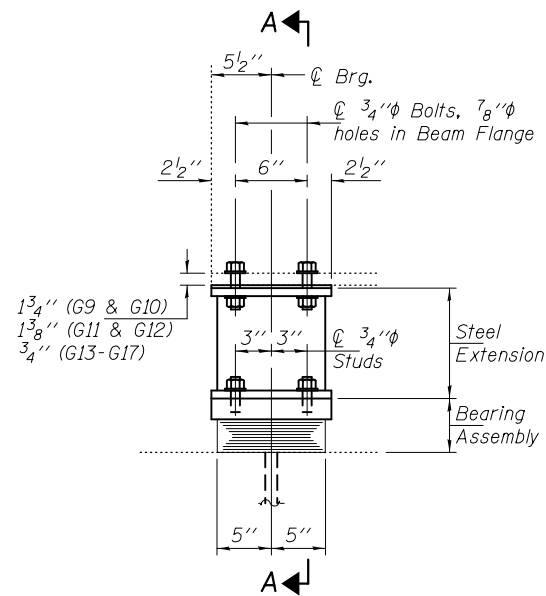
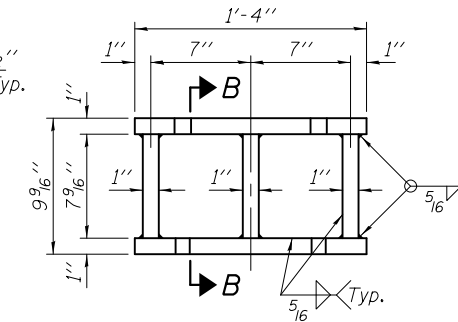
New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.



PLAN TOP AND BOTTOM PLATE

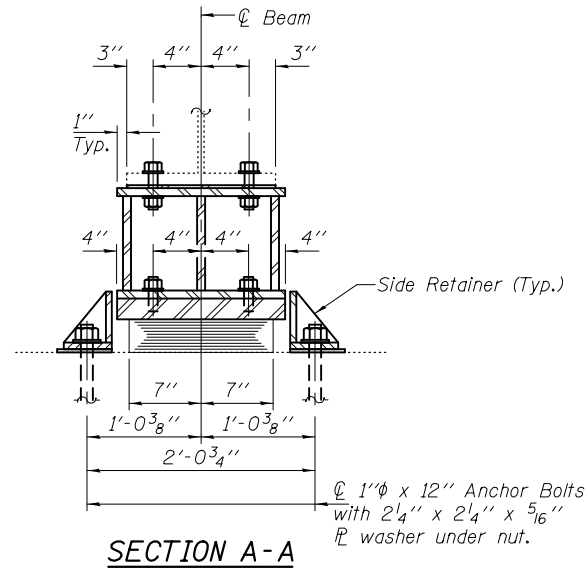


SECTION B-B

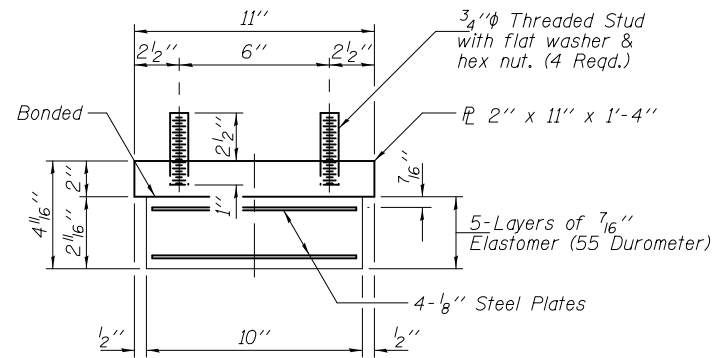


ELEVATION AT ABUTMENT

TYPE I ELASTOMERIC EXP. BRG.

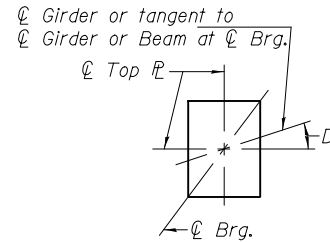


SECTION A-A

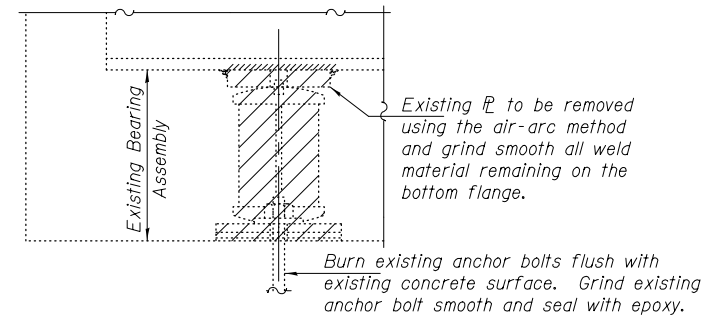


BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.

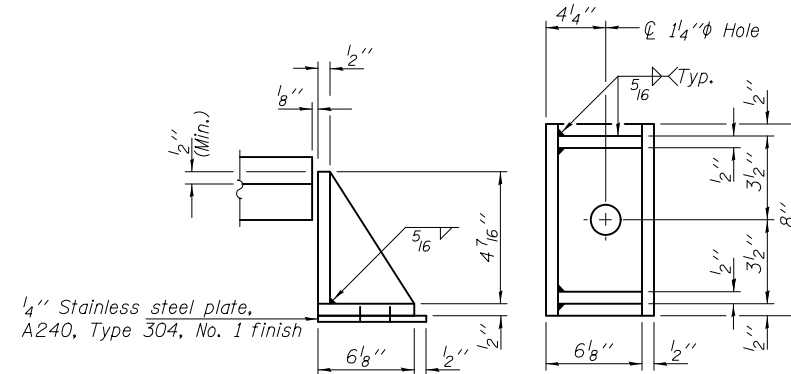


SN 072-0131 (E.B.)	
SPAN 5	
Girder	Angle D
9-16	0°-00'-00"
17	8°-45'-17"



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	9
* Jack and Remove Existing Bearings	Each	3
Furnishing and Erecting Structural Steel	Pound	1740
Anchor Bolts 1"φ	Each	18

* Temporary Shoring & Cribbing to be used instead of Jack and Remove Existing Bearings at 3 locations. See sheet 32 of 49.

TYI/REPS 1-18-2017

DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kreyer</i>
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
REVISED
REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 5 NORTH BEARING REPLACEMENT DETAILS
SN 072-0131 (E.B.)**

SHEET NO. 26 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	121
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	

**PIER 5 SOUTH
GIRDER REACTIONS**

R _L	(K)	67.3
R _R	(K)	49.8
Imp.	(K)	10.0
R (Total)	(K)	127.1

Notes:
Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).
Min. Jack capacity = 75 Tons.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (F_y=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts for Type III bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.

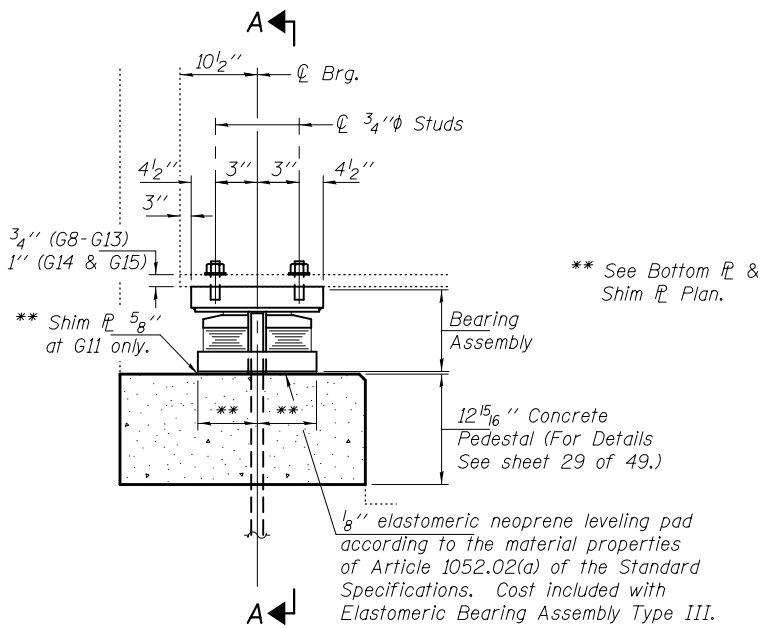
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type III.

The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

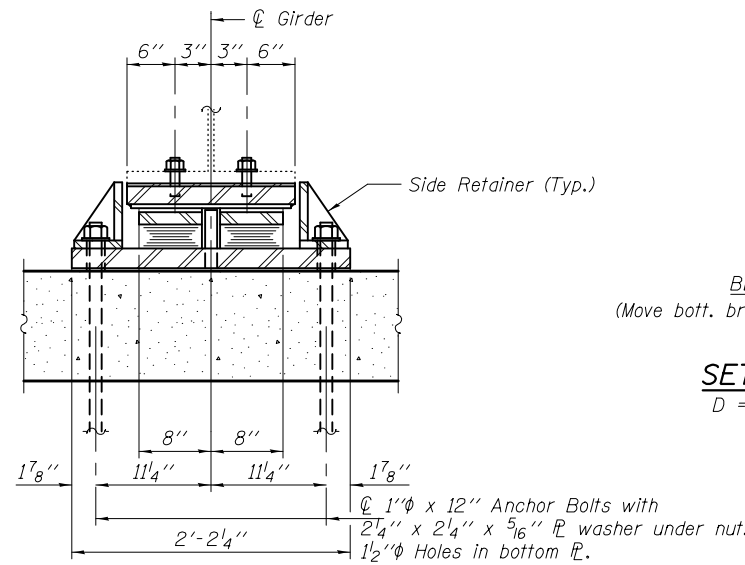
Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.

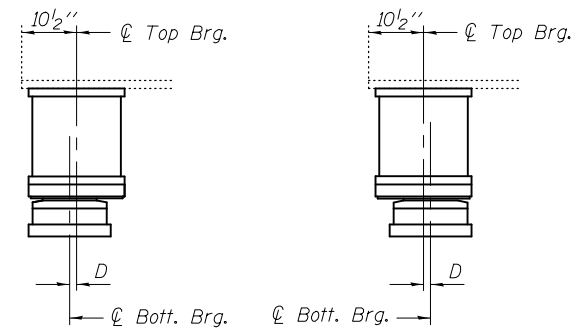


ELEVATION AT PIER 5 SOUTH

TYPE III ELASTOMERIC EXP. BRG.



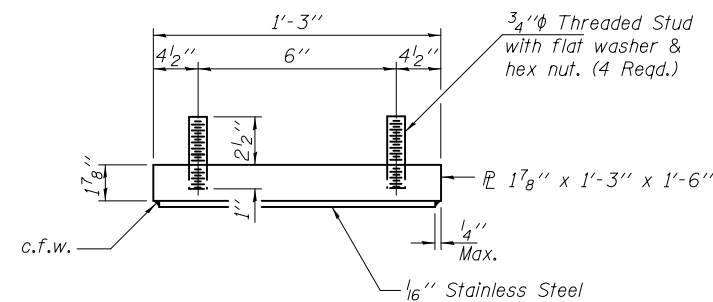
SECTION A-A



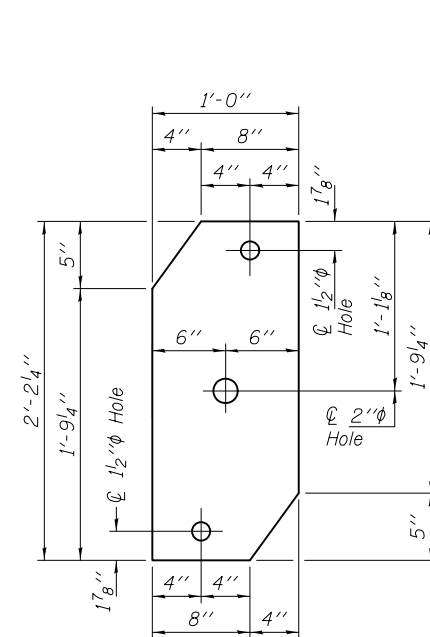
BELOW 50° F. (Move bott. brg. away from fixed brg.)
ABOVE 50° F. (Move bott. brg. toward fixed brg.)

SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

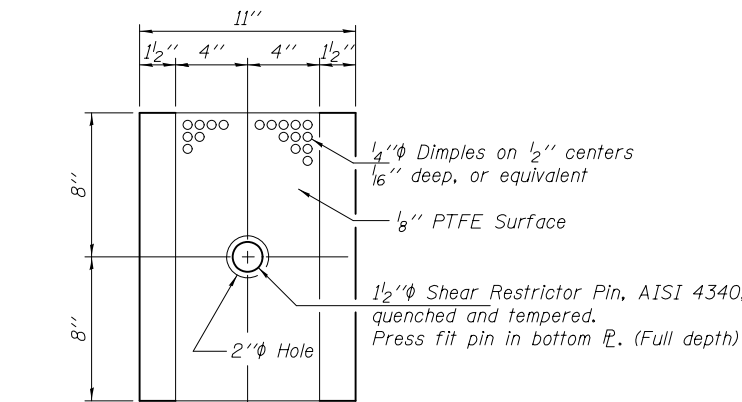


TOP BEARING ASSEMBLY

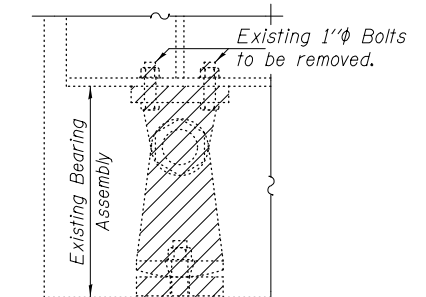
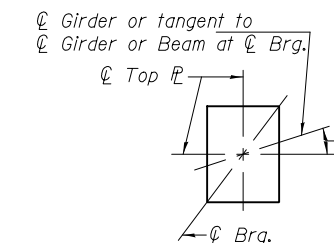


BOTTOM PL & SHIM PL PLAN

Bottom PL 1 1/4" x 1'-0" x 2'-2 1/4" (8-Req'd)
Shim PL 5/8" x 1'-0" x 2'-2 1/4" (1-Req'd)

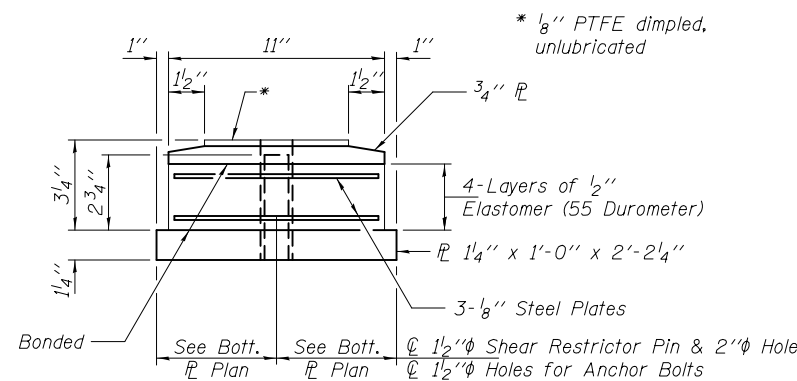


PLAN-PTFE ELASTOMERIC BRG.

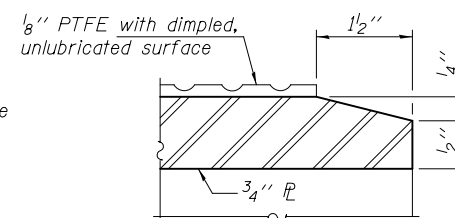


EXISTING BEARING REMOVAL DETAIL

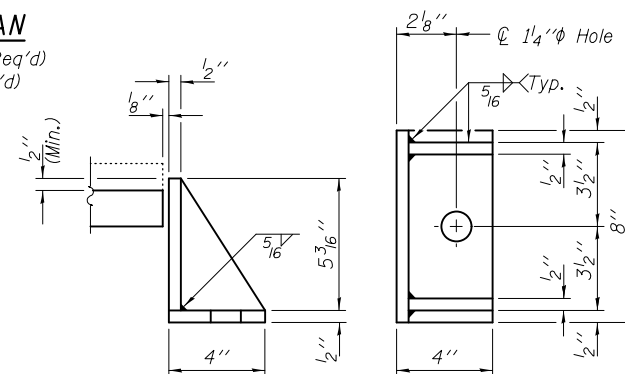
Cost included with Jack and Remove Existing Bearings.



BOTTOM BEARING ASSEMBLY



SECTION THRU PTFE



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

SN 072-0131 (E.B.)	
SPAN 6	
Girder	Angle D
8-11	0°-00'-00"
12	0°-23'-10"
13	0°-46'-06"
14	1°-08'-46"
15	7°-50'-26"

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type III	Each	8
Temporary Shoring and Cribbing	Each	8
Furnishing and Erecting Structural Steel	Pound	60
Anchor Bolts 1"φ	Each	16

TYIII/REPS 12-03-2008

DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl R. Roper</i>	REVISOR
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 5 SOUTH BEARING REPLACEMENT DETAILS
SN 072-0131 (E.B.)**

SHEET NO. 27 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	122
			CONTRACT NO. 68887	
ILLINOIS FED. AID PROJECT				

**RAMP "C"
NORTH ABUTMENT
BEAM REACTIONS**

R _P	(K)	16.0
R _L	(K)	37.2
Imp.	(K)	11.2
R (Total)	(K)	64.4

Notes:

Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.

New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).

Min. jack capacity = 30 Tons.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts for Type II bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.

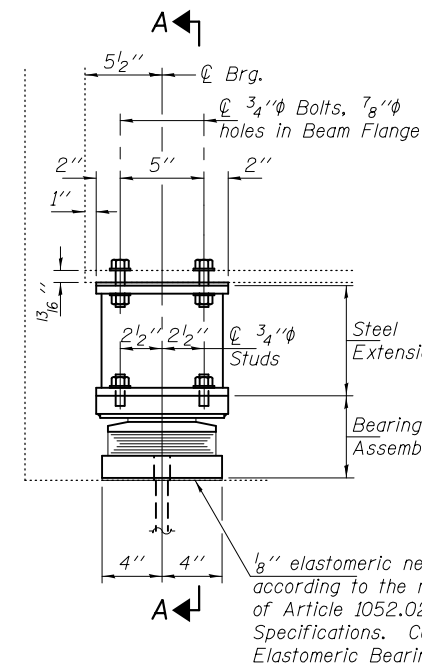
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type II.

The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

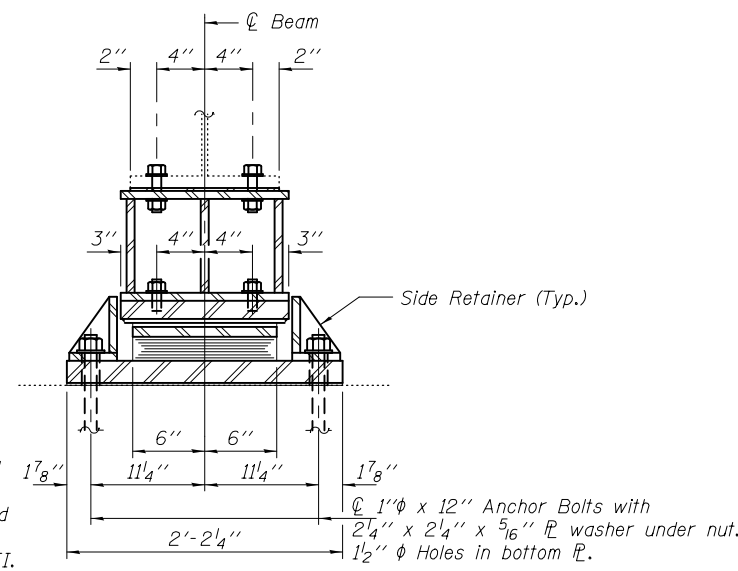
Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

New bearing plates, steel extensions, shim plates, side retainers, anchor bolts, connection bolts, nuts and washers shall be galvanized according to AASHTO M111 or M232 as applicable.

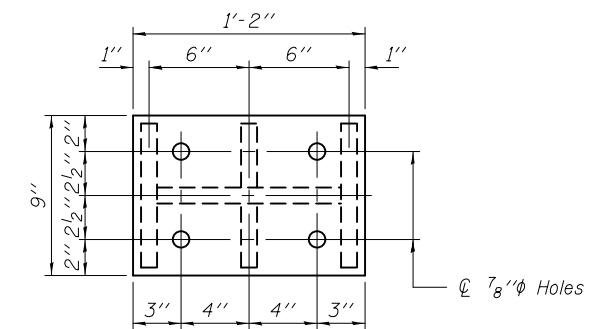


ELEVATION AT ABUTMENT

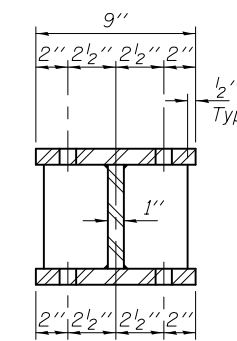
TYPE II TFE ELASTOMERIC EXP. BRG.



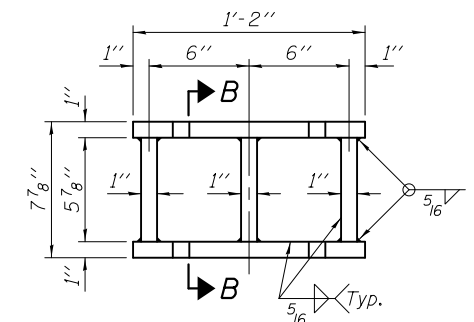
SECTION A-A



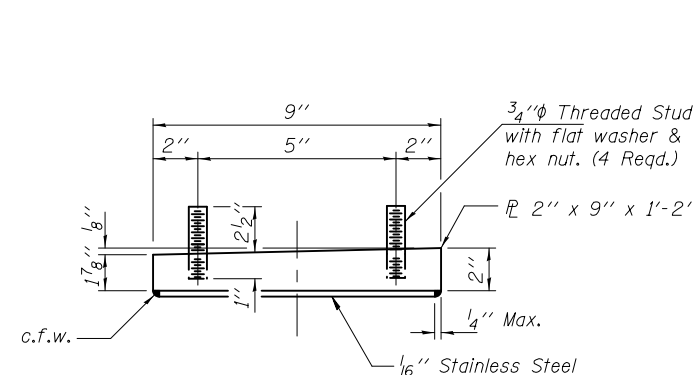
PLAN TOP AND BOTTOM PLATE



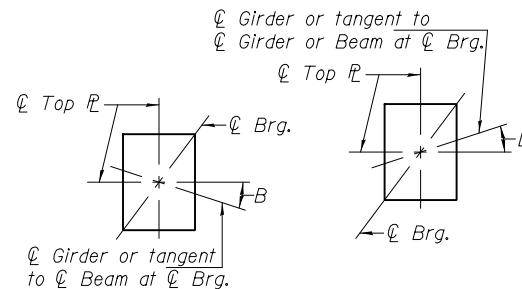
SECTION B-B



STEEL EXTENSION DETAIL

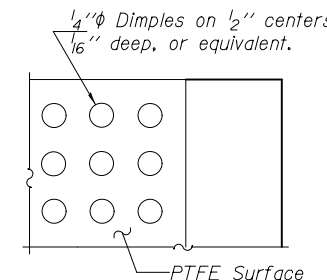


TOP BEARING ASSEMBLY

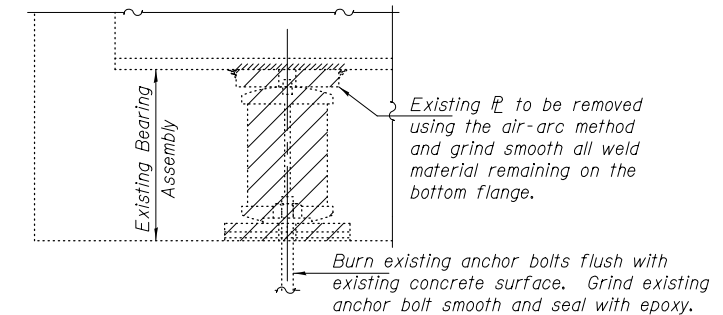


PLAN-PTFE SURFACE

RAMP C - SPAN 1		
Beam	Brg. at N. Abut.	
	Angle D	Angle B
1	-	0°-11'-44"
2	-	0°-04'-04"
3	0°-03'-46"	-
4	0°-11'-44"	-

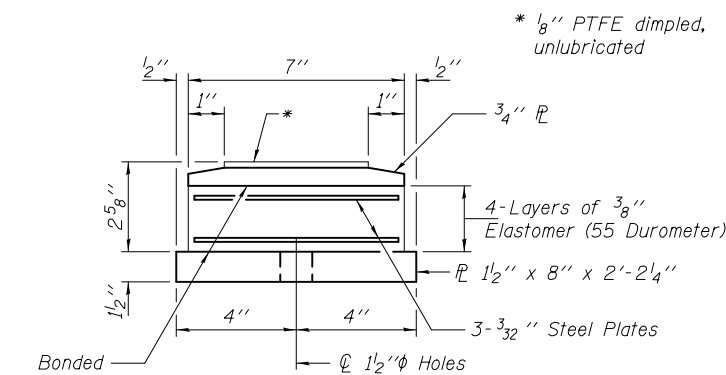


SECTION THRU PTFE

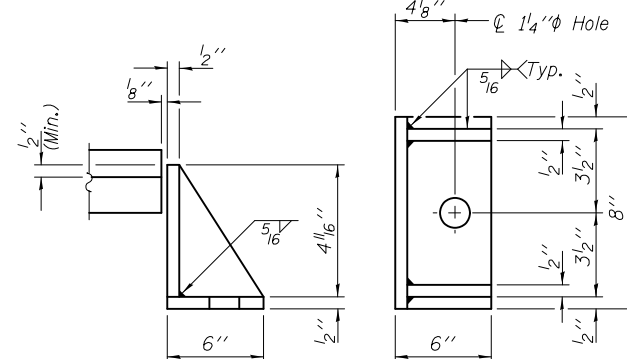


EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.

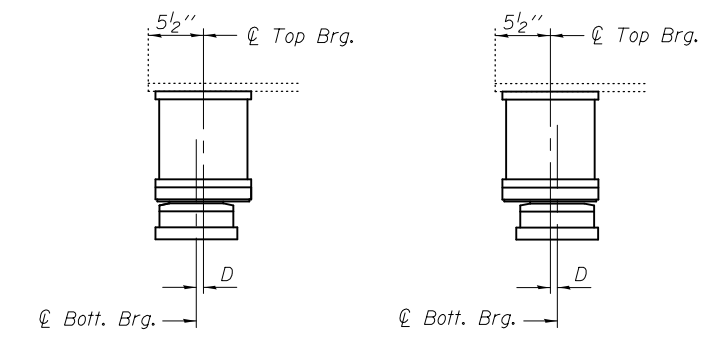


BOTTOM BEARING ASSEMBLY



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



SETTING ANCHOR BOLTS AT EXP. BRG.

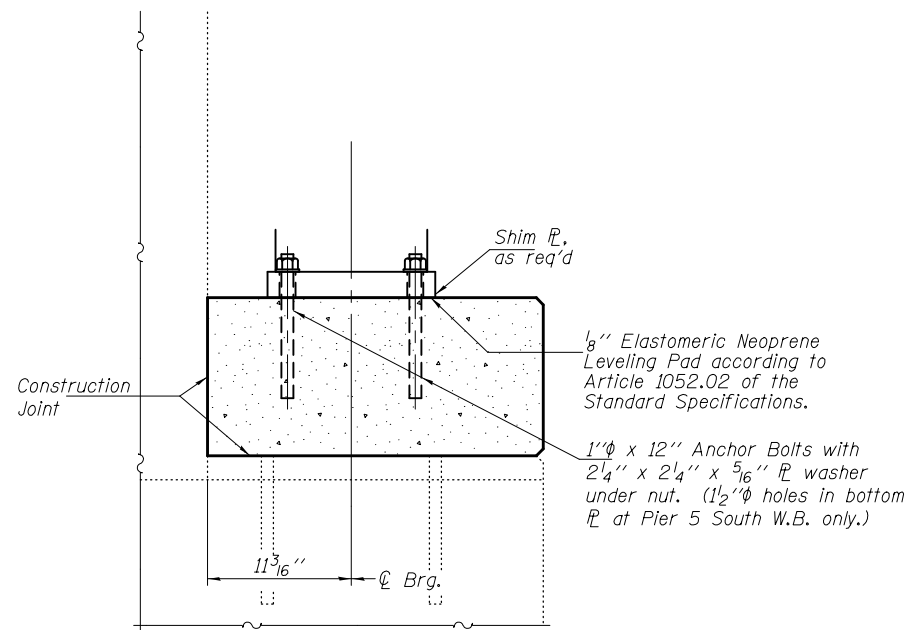
BELOW 50° F. (Move bott. brg. away from fixed brg.) ABOVE 50° F. (Move bott. brg. toward fixed brg.)

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

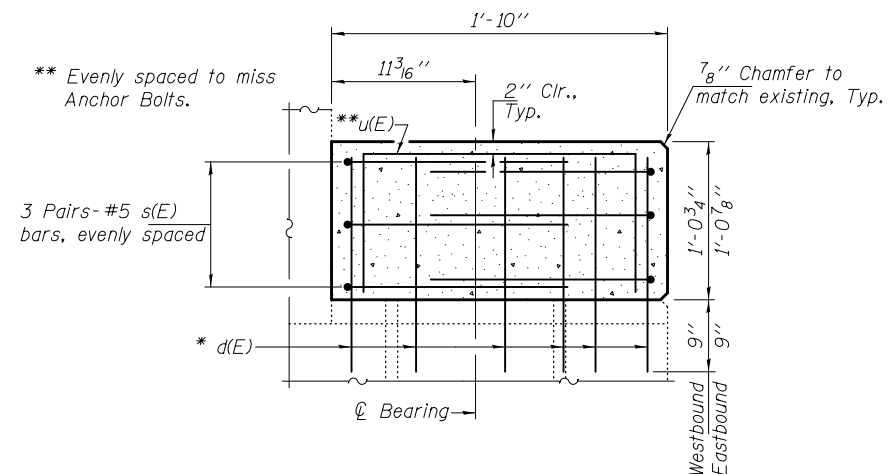
BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type II	Each	4
Jack and Remove Existing Bearings	Each	4
Furnishing and Erecting Structural Steel	Pound	520
Anchor Bolts 1"φ	Each	8

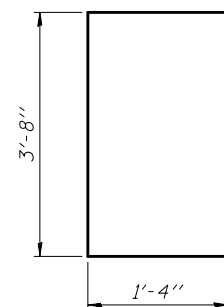
TYII/REPS 12-03-2008



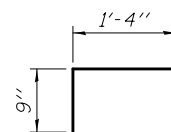
PIER 5 SOUTH ELEVATION



PIER 5 SOUTH PEDESTAL ELEVATION



BAR s(E)



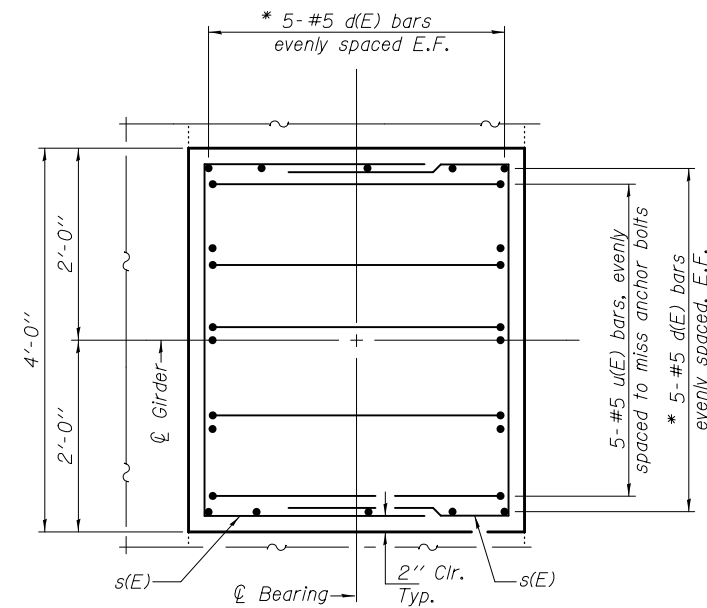
u(E) BAR

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	240	#5	1'-10"	—
s(E)	90	#5	6'-4"	□
u(E)	75	#5	2'-10"	□
Concrete Structures			Cu. Yd.	4.4
Reinforcement Bars, Epoxy Coated			Pound	1280

Reinforcement bars designated (E) shall be epoxy coated.

* Epoxy grout bars in accordance with Article 584 of the Standard Specifications. Cost is included in cost of Reinforcement Bars, Epoxy Coated.



PIER 5 SOUTH PEDESTAL PLAN

DESIGNED - SMR
 CHECKED - RPN
 DRAWN - Kyle M. Steffen
 CHECKED - SMR RPN

EXAMINED
 PASSED
 ENGINEER OF STRUCTURAL SERVICES
 ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
 REVISED
 REVISED

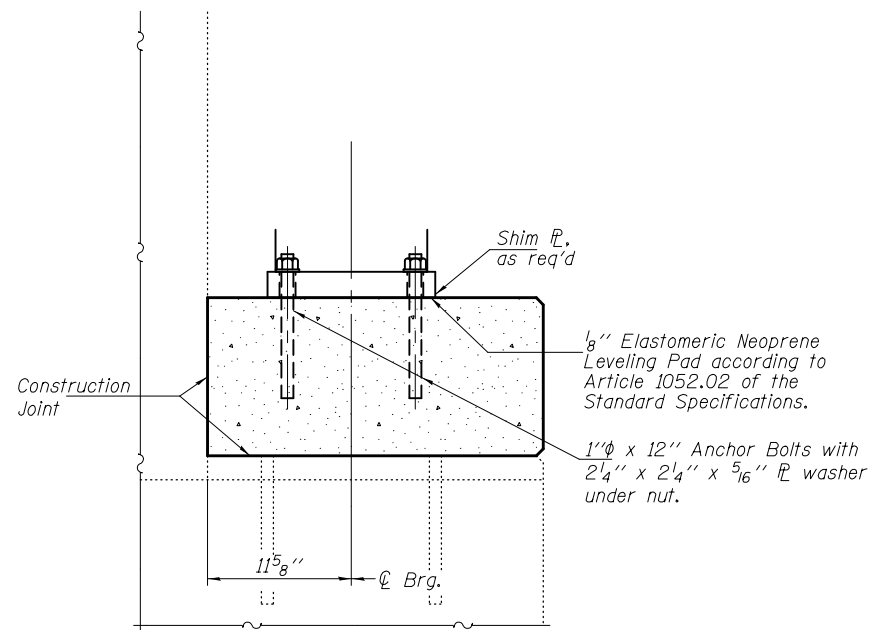
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CONCRETE PEDESTAL DETAILS FOR PIER 5 SOUTH
 SN 072-0131 (E.B.) & -0132 (W.B.)

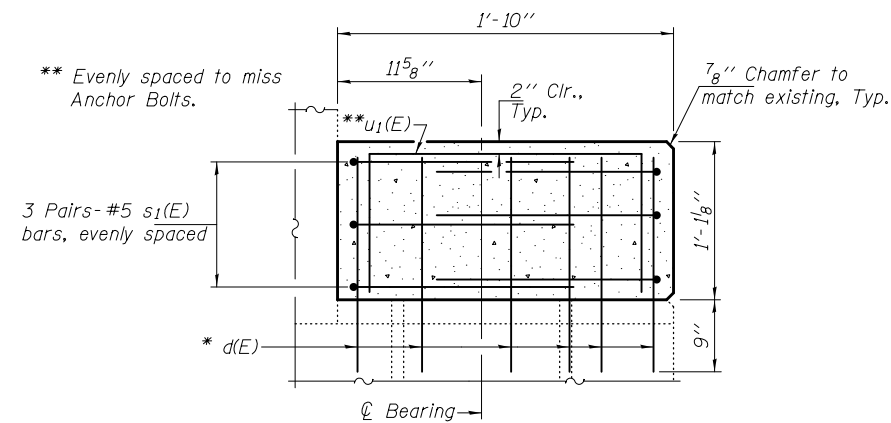
SHEET NO. 29 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	124
				CONTRACT NO. 68887

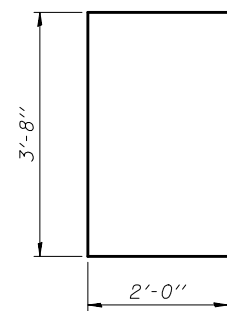
ILLINOIS FED. AID PROJECT



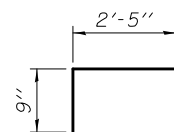
SOUTH ABUTMENT ELEVATION



SOUTH ABUTMENT PEDESTAL ELEVATION



BAR s1(E)



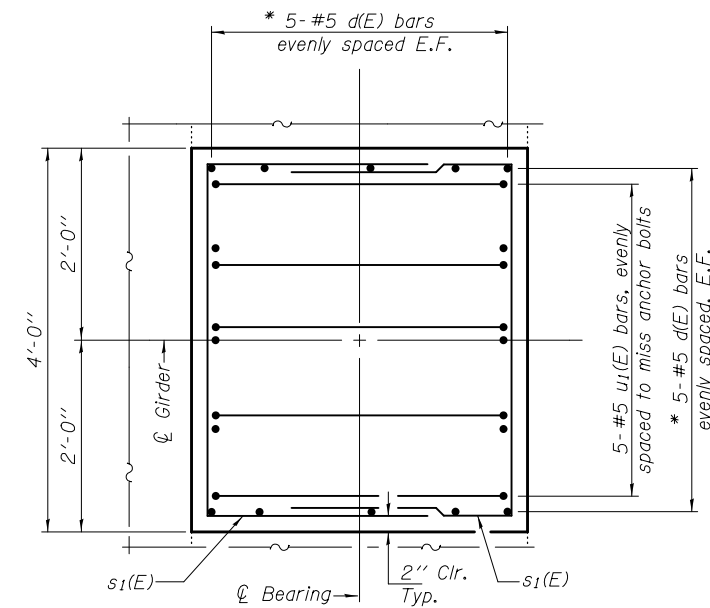
u1(E) BAR

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	240	#5	1'-10"	—
s1(E)	90	#5	7'-8"	□
u1(E)	75	#5	3'-11"	□
Concrete Structures			Cu. Yd.	4.5
Reinforcement Bars, Epoxy Coated			Pound	1490

Reinforcement bars designated (E) shall be epoxy coated.

* Epoxy grout bars in accordance with Article 584 of the Standard Specifications. Cost is included in cost of Reinforcement Bars, Epoxy Coated.



SOUTH ABUTMENT PEDESTAL PLAN

DESIGNED - SMR
 CHECKED - RPN
 DRAWN - Kyle M. Steffen
 CHECKED - SMR RPN

EXAMINED
 PASSED
 ENGINEER OF STRUCTURAL SERVICES
 ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
 REVISED
 REVISED

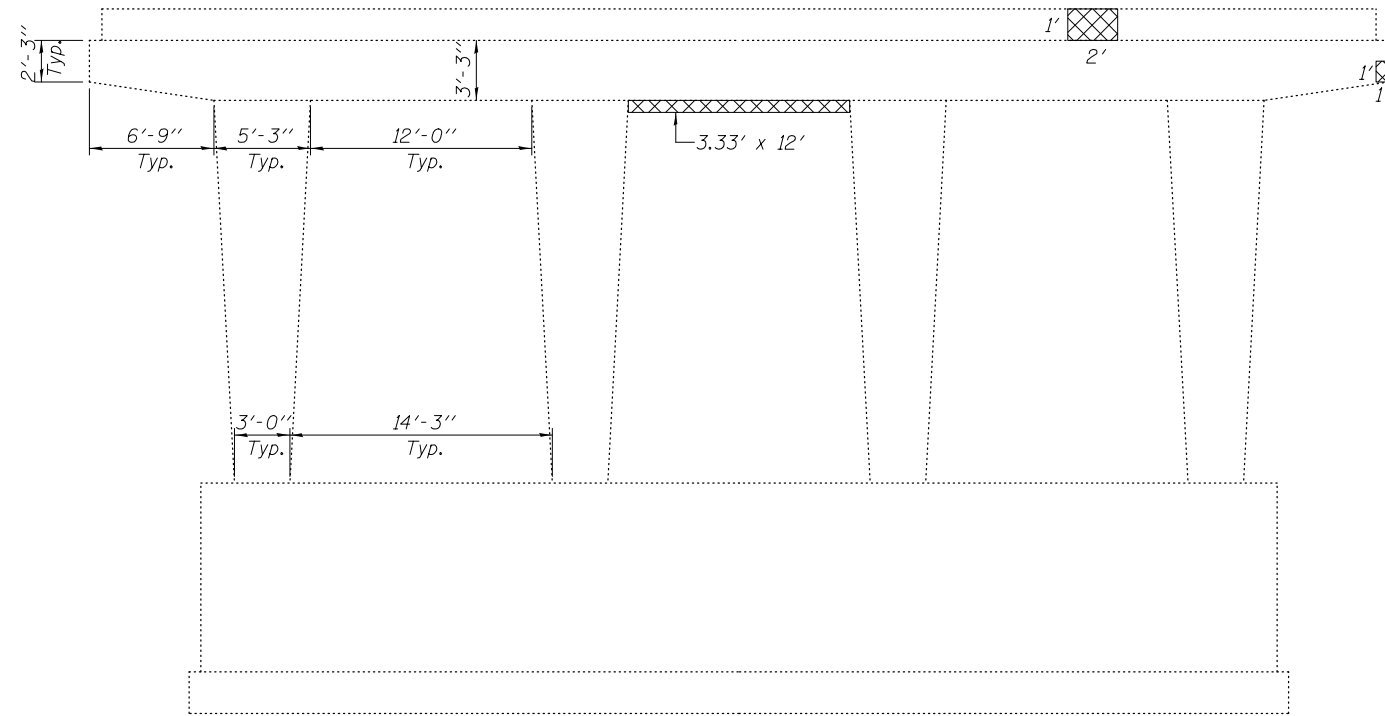
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CONCRETE PEDESTAL DETAILS FOR SOUTH ABUTMENT
 SN 072-0131 (E.B.) & -0132 (W.B.)

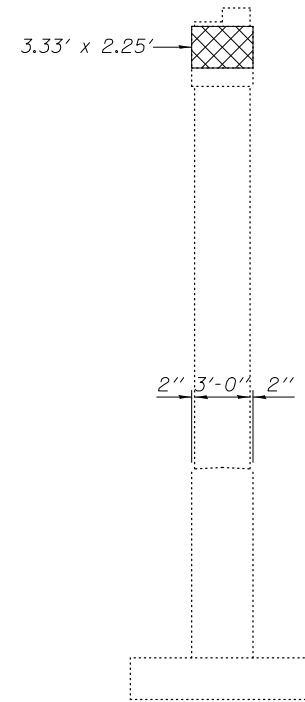
SHEET NO. 30 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB)-1, HVB/B-R	PEORIA	196	125
CONTRACT NO. 68887				

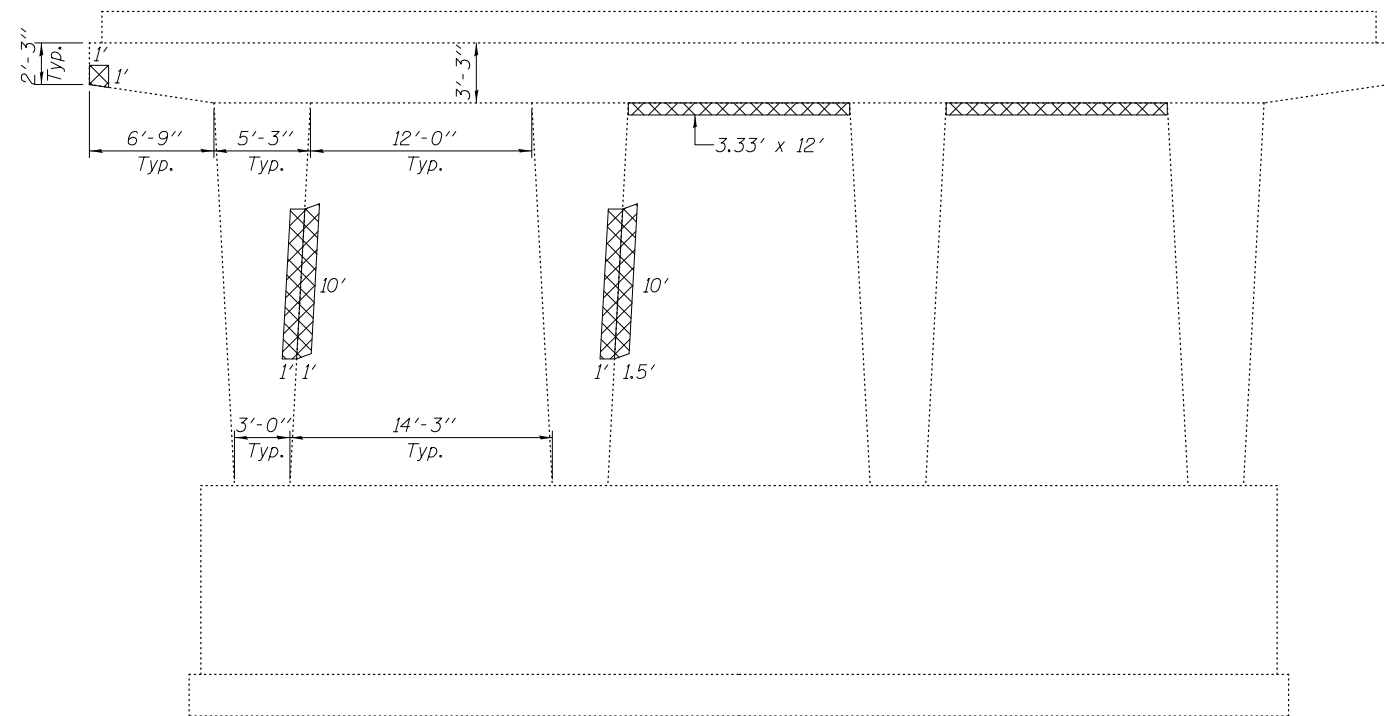
ILLINOIS FED. AID PROJECT



ELEVATION PIER 4 (E.B.)
(Looking South)



VIEW A-A



ELEVATION PIER 4 (E.B.)
(Looking North)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	97

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

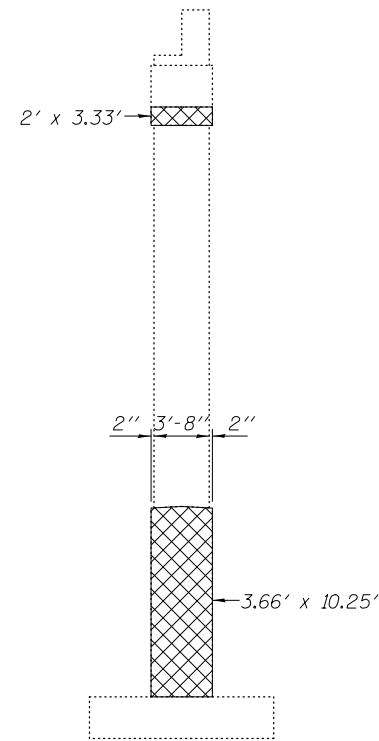
EXAMINED
PASSED
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
REVISED
REVISED

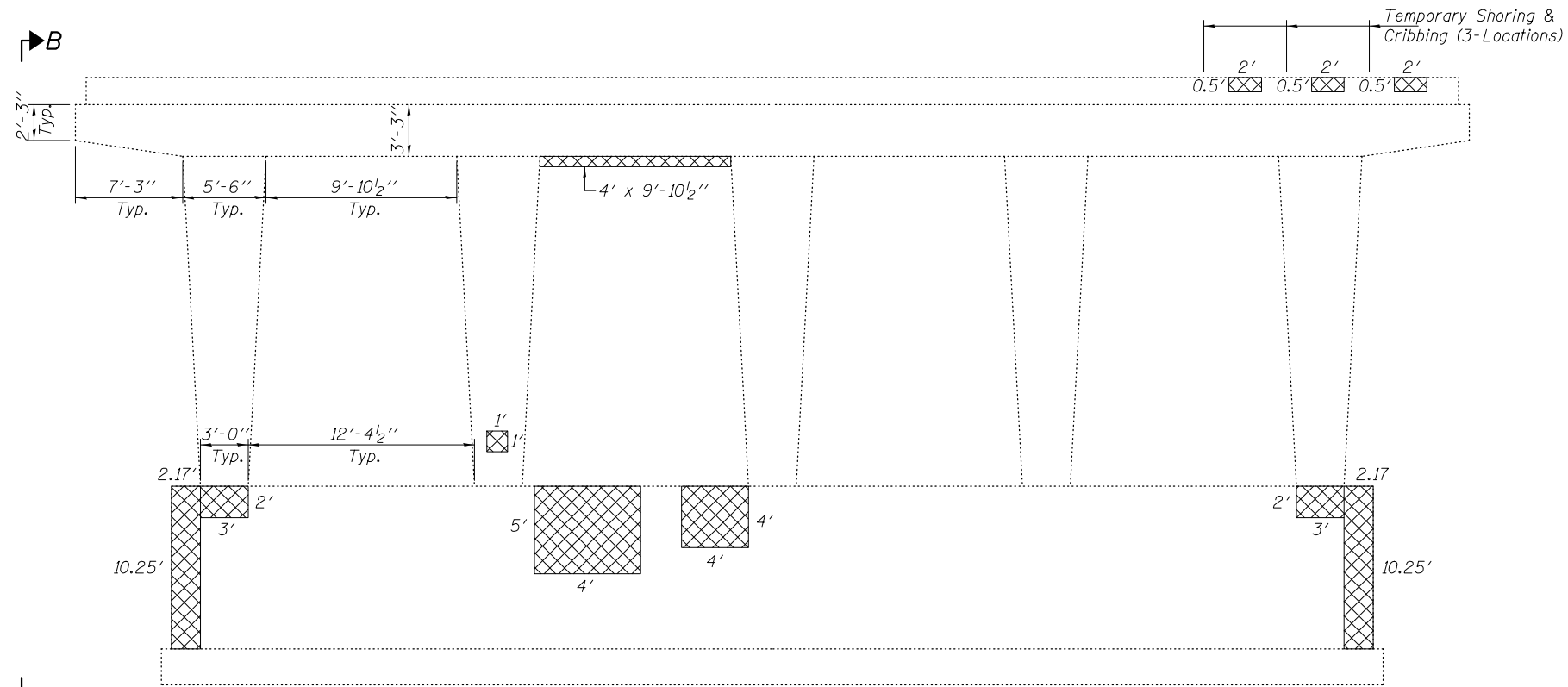
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS FOR PIER 4
SN 072-0131 (E.B.)
SHEET NO. 31 OF 49 SHEETS

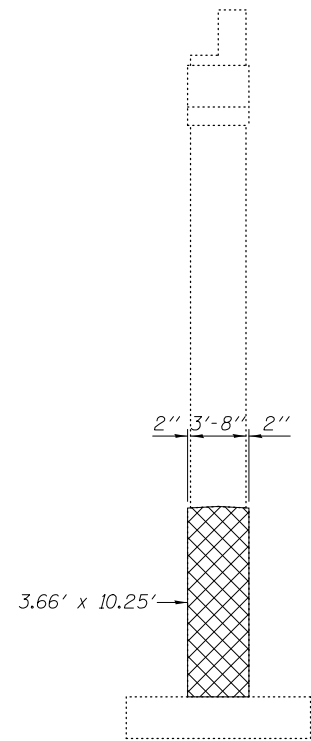
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	126
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



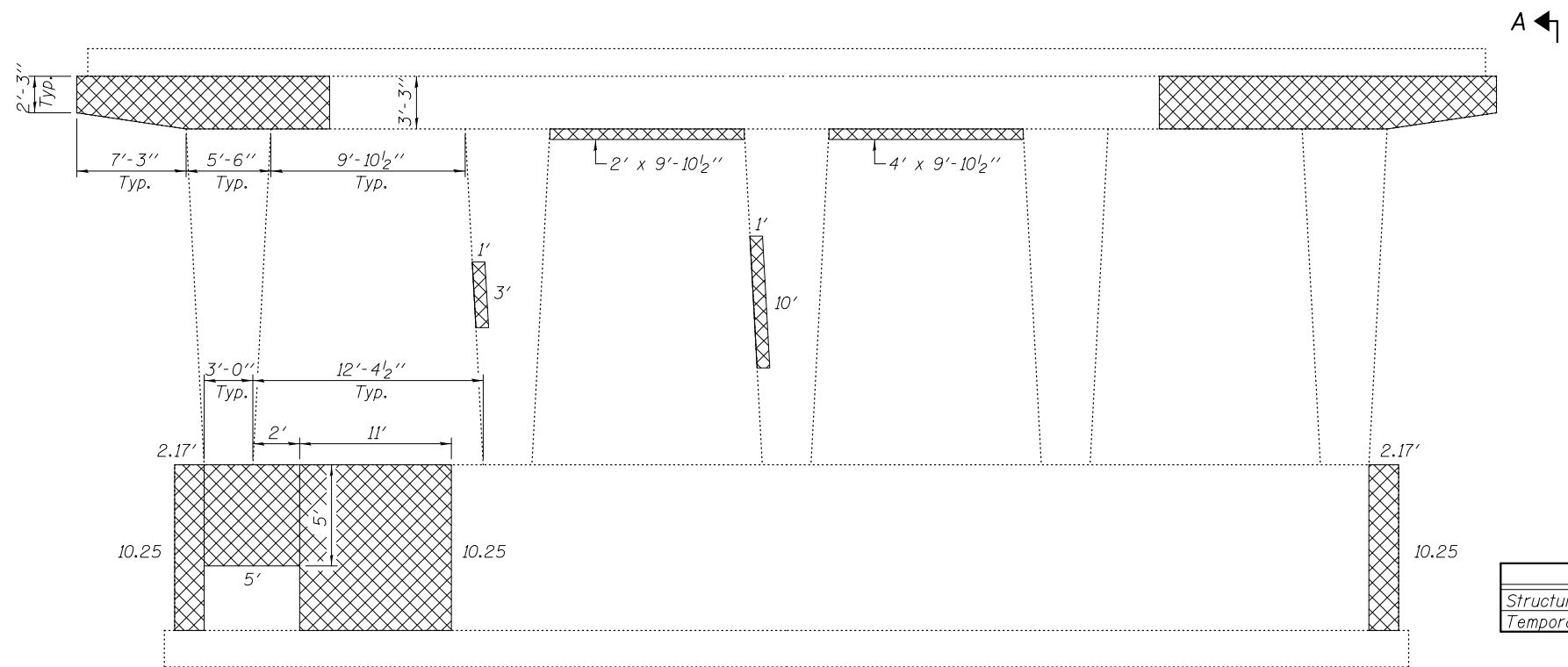
VIEW B-B



ELEVATION PIER 5 (E.B.)
(Looking South)



VIEW A-A



ELEVATION PIER 5 (E.B.)
(Looking North)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	567
Temporary Shoring & Cribbing	Each	3

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR	EXAMINED - <i>Timothy A. [Signature]</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl [Signature]</i>	REVISOR
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISION

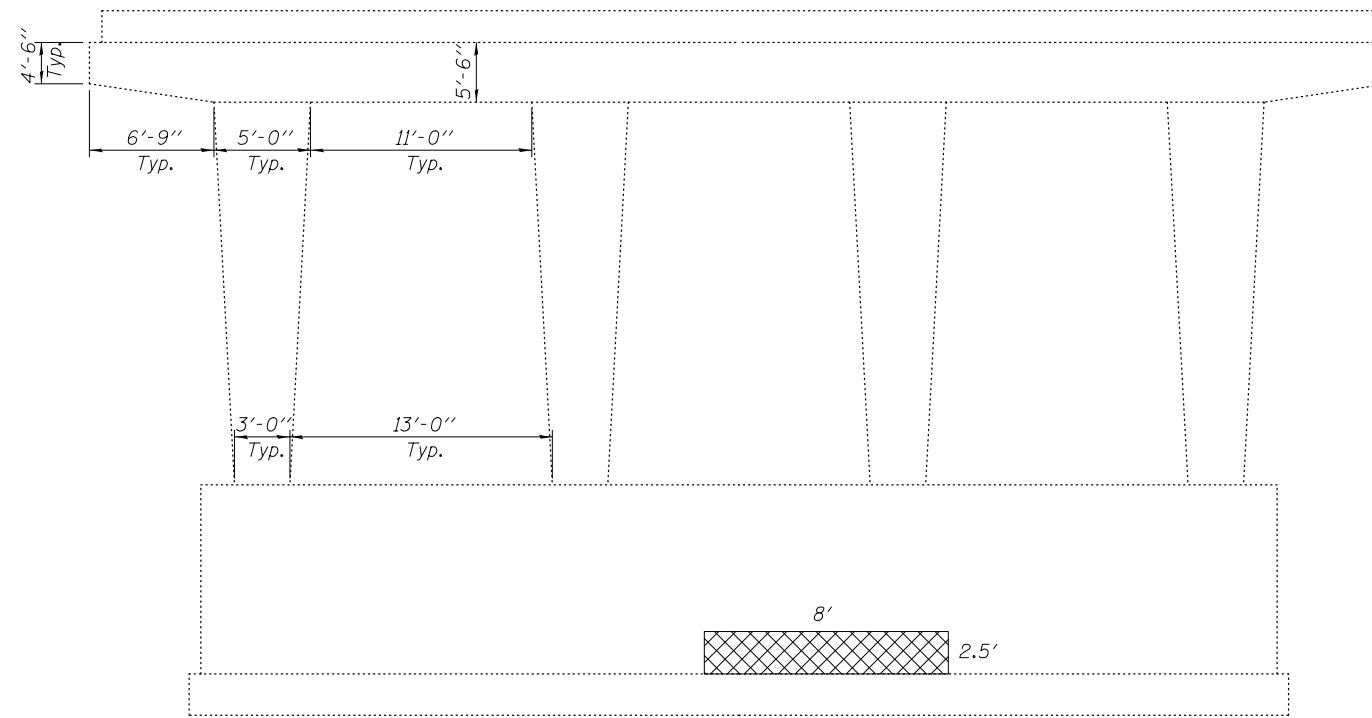
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS FOR PIER 5
SN 072-0131 (E.B.)

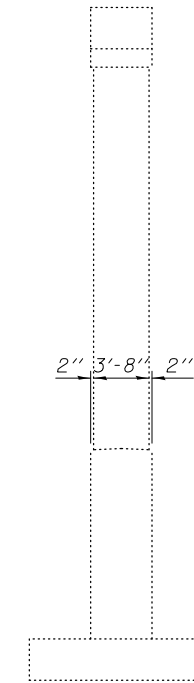
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	127
CONTRACT NO. 68887				

SHEET NO. 32 OF 49 SHEETS

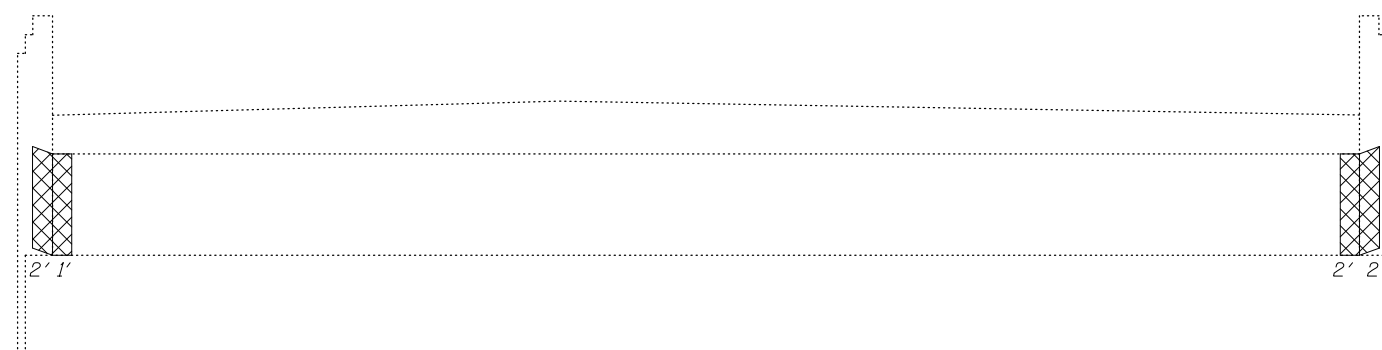
ILLINOIS FED. AID PROJECT



ELEVATION PIER 7 (E.B.)
(Looking South)



END VIEW



ELEVATION SOUTH ABUTMENT (E.B.)
(Looking South)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	71

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

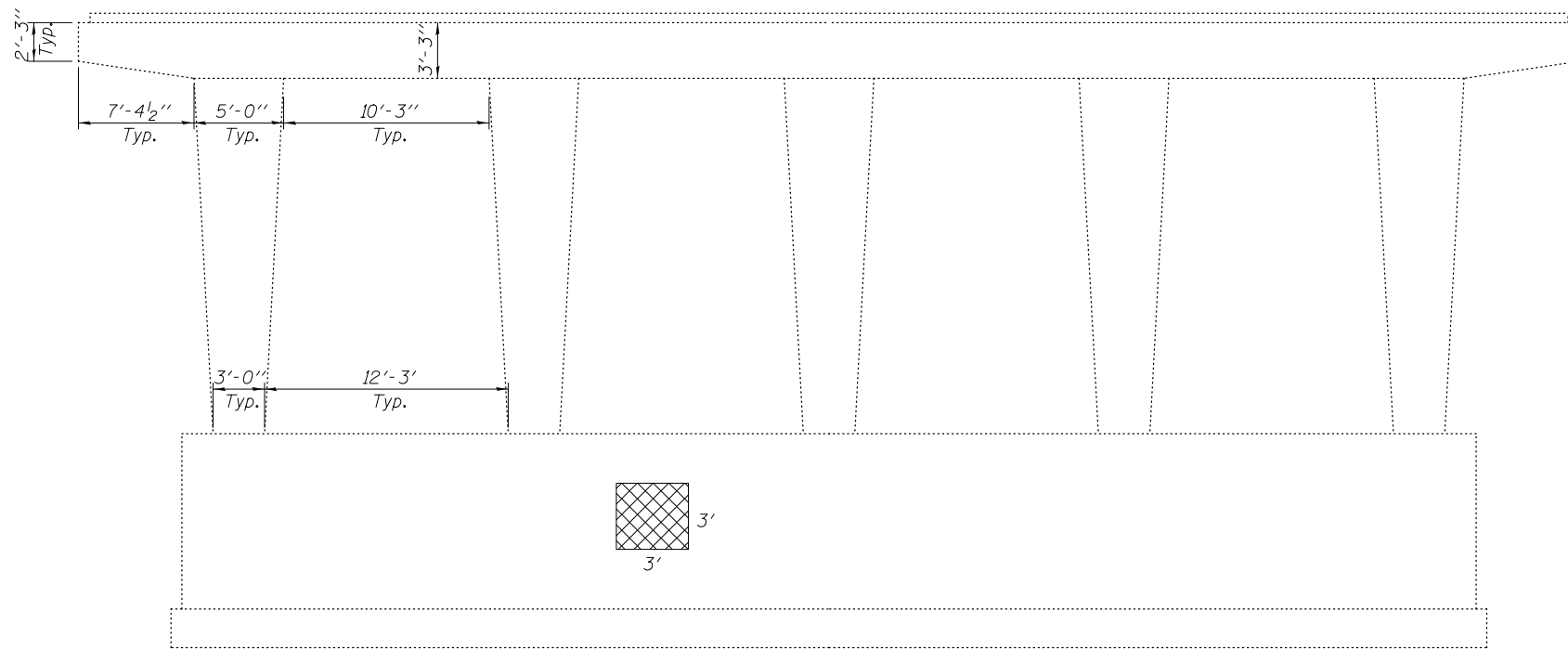
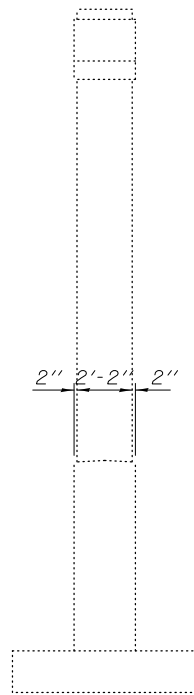
EXAMINED	<i>Timothy A. Daulton</i> ENGINEER OF STRUCTURAL SERVICES
PASSED	<i>Carl Kroyer</i> ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
REVISED
REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

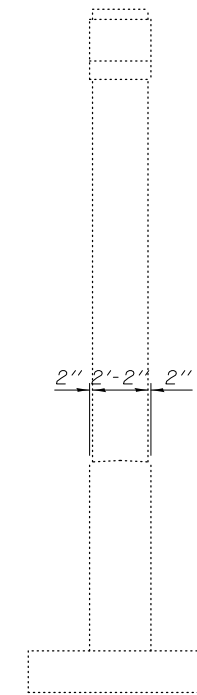
**SUBSTRUCTURE REPAIR DETAILS FOR PIER 7 & S. ABUT.
SN 072-0131 (E.B.)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	128
				CONTRACT NO. 68887
ILLINOIS FED. AID PROJECT				



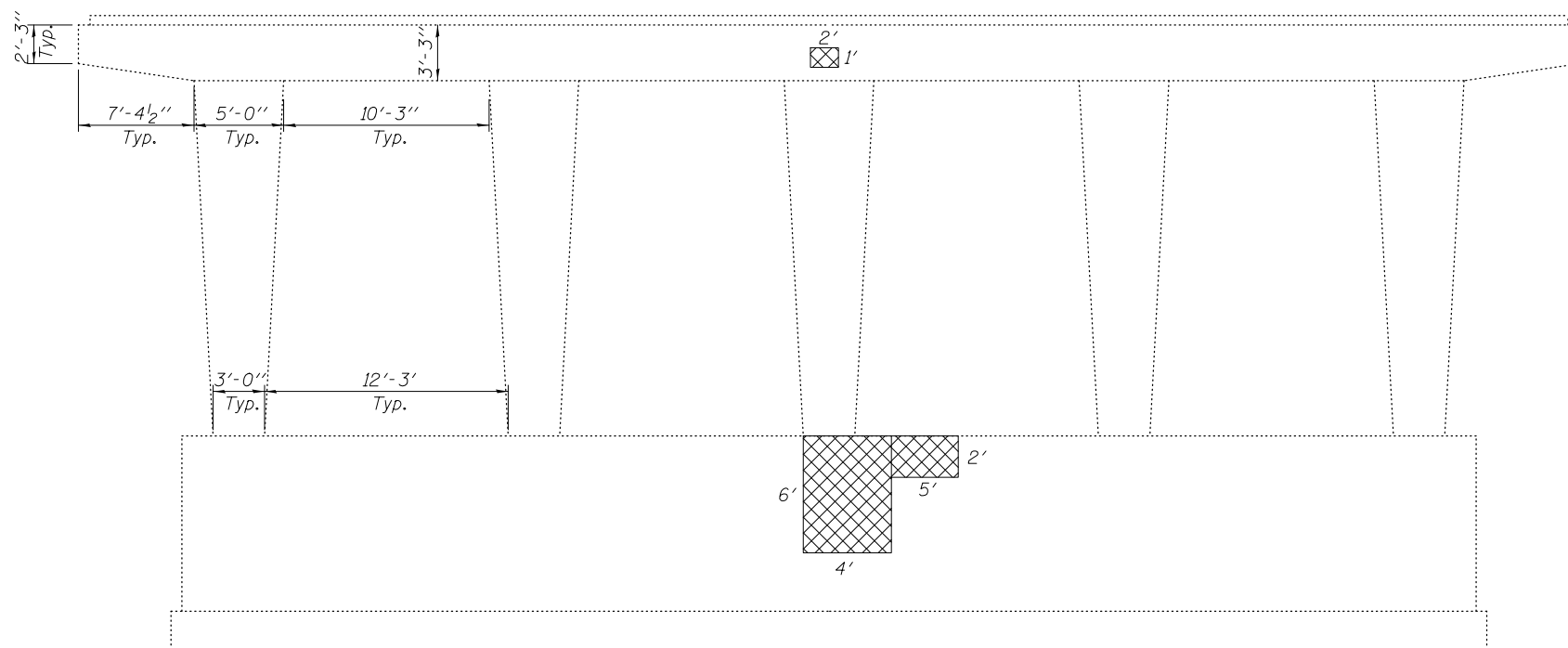
ELEVATION PIER 1 (W.B.)
(Looking South)

A ↙



VIEW A-A

A ↙



ELEVATION PIER 1 (W.B.)
(Looking North)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	45

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

EXAMINED
PASSED
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

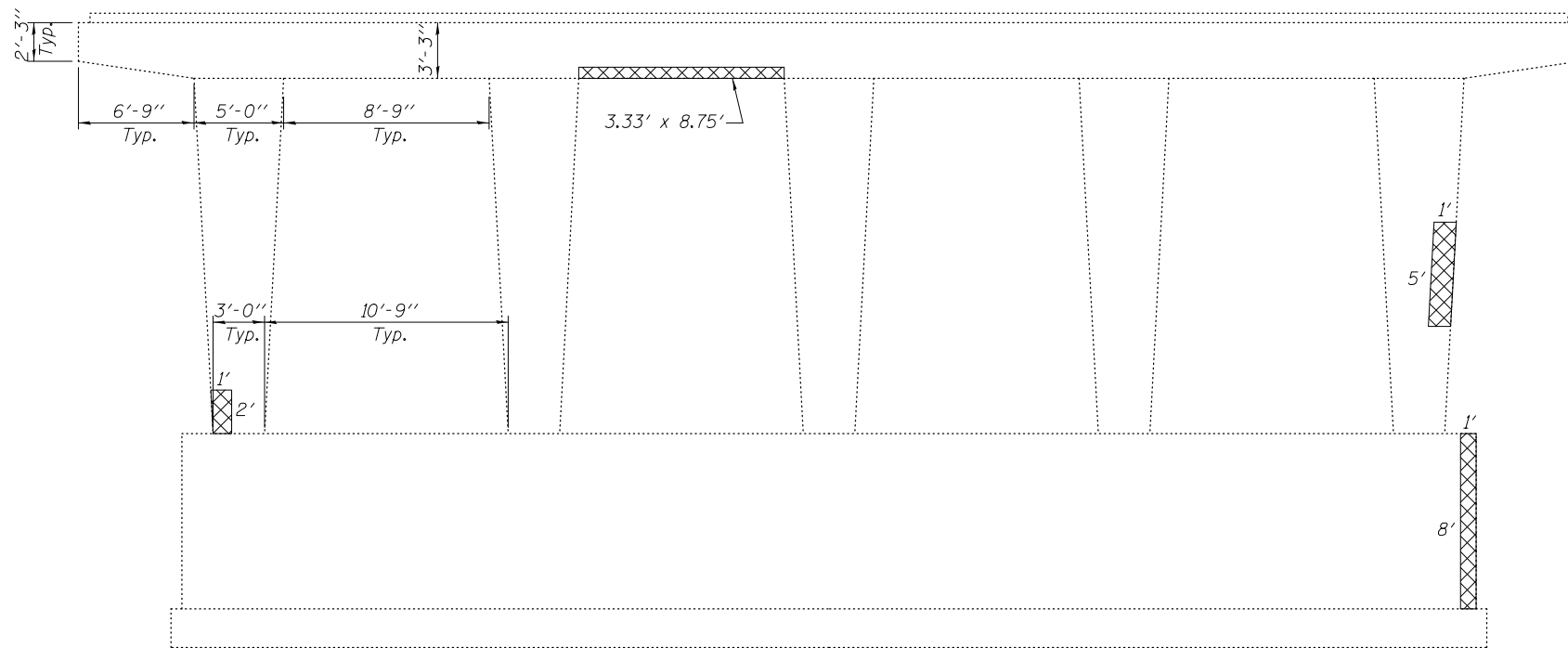
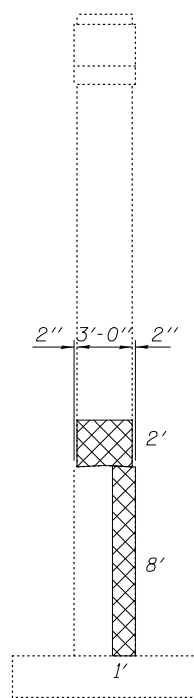
DATE - JANUARY 31, 2018
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS FOR PIER 1
SN 072-0132 (W.B.)

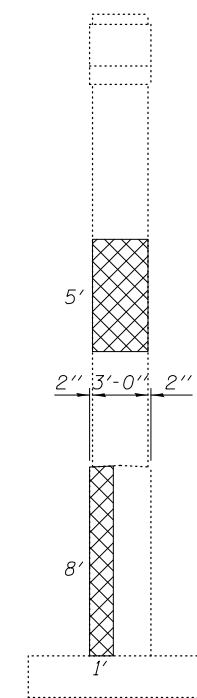
SHEET NO. 34 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	129
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	

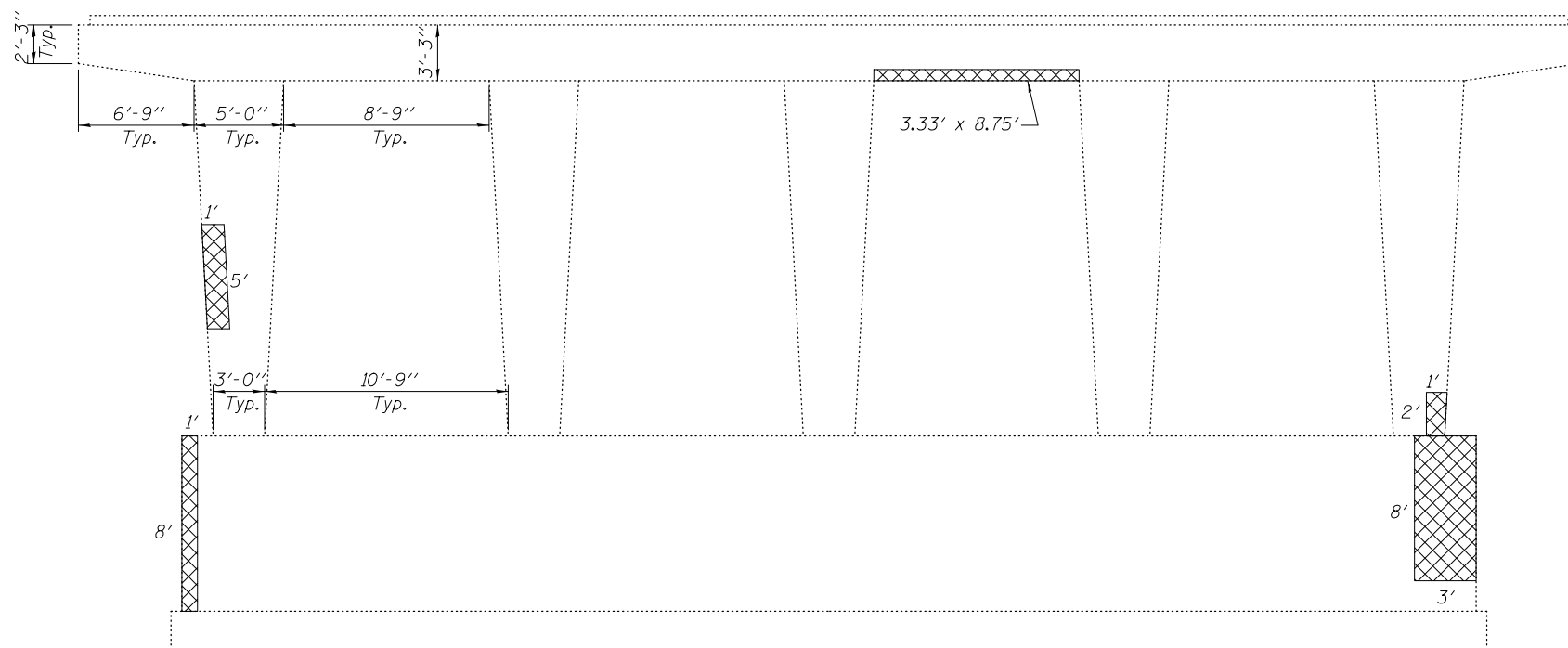


ELEVATION PIER 2 (W.B.)
(Looking South)

A ←



VIEW A-A



ELEVATION PIER 2 (W.B.)
(Looking North)

A ←

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	121

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

EXAMINED
PASSED
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
REVISED
REVISED

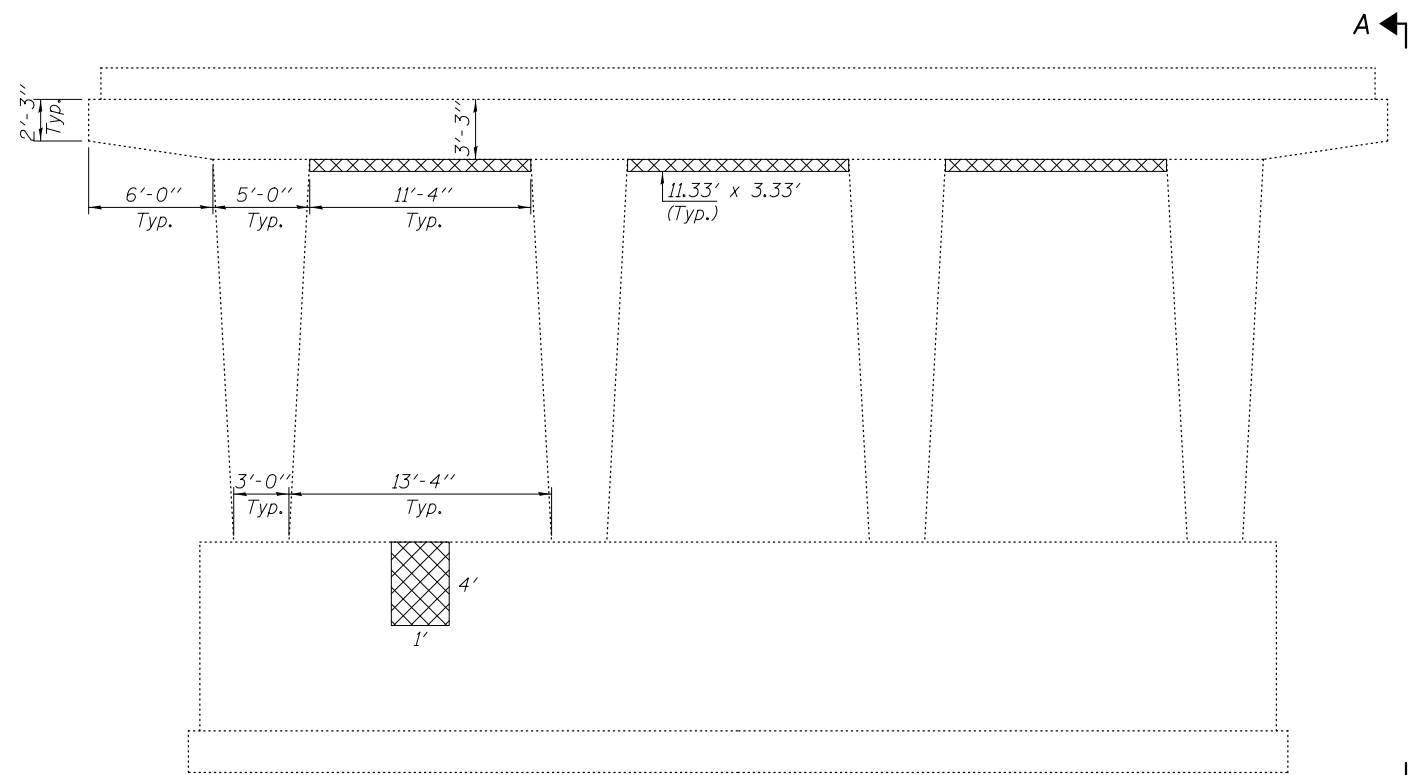
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS FOR PIER 2
SN 072-0132 (W.B.)

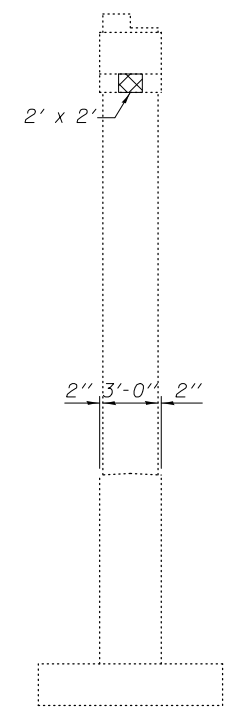
SHEET NO. 35 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	130
CONTRACT NO. 68887				

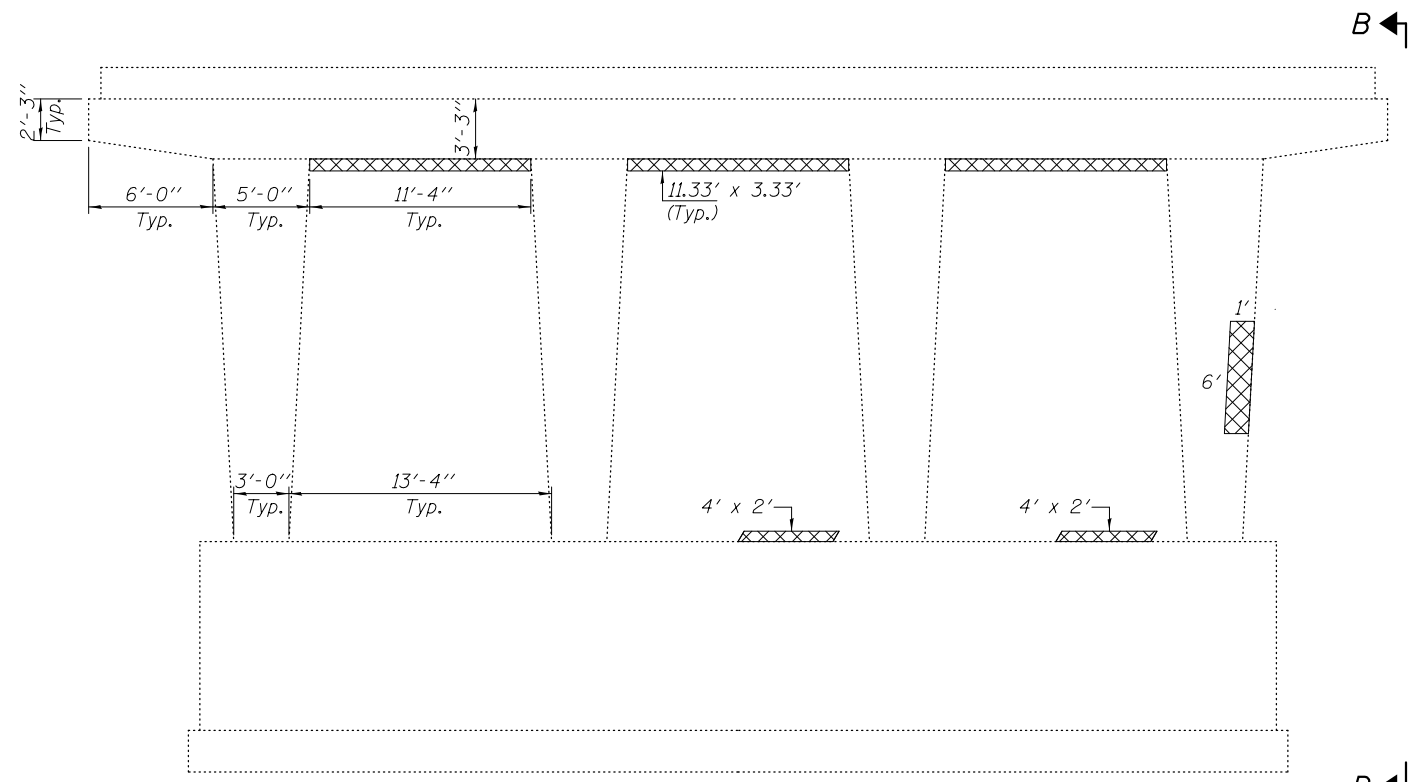
ILLINOIS FED. AID PROJECT



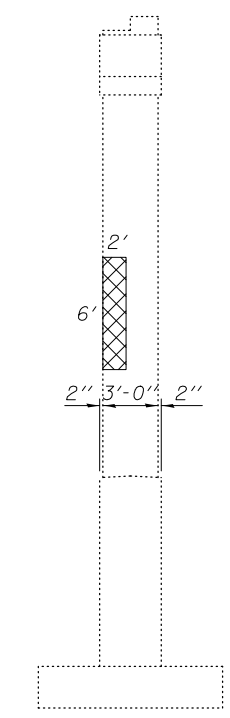
ELEVATION PIER 4 (W.B.)
(Looking South)



VIEW A-A



ELEVATION PIER 4 (W.B.)
(Looking North)



VIEW B-B

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	156

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR	EXAMINED - <i>Timothy A. Daulton</i>	DATE - JANUARY 31, 2018
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl Kuyper</i>	REVISOR
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

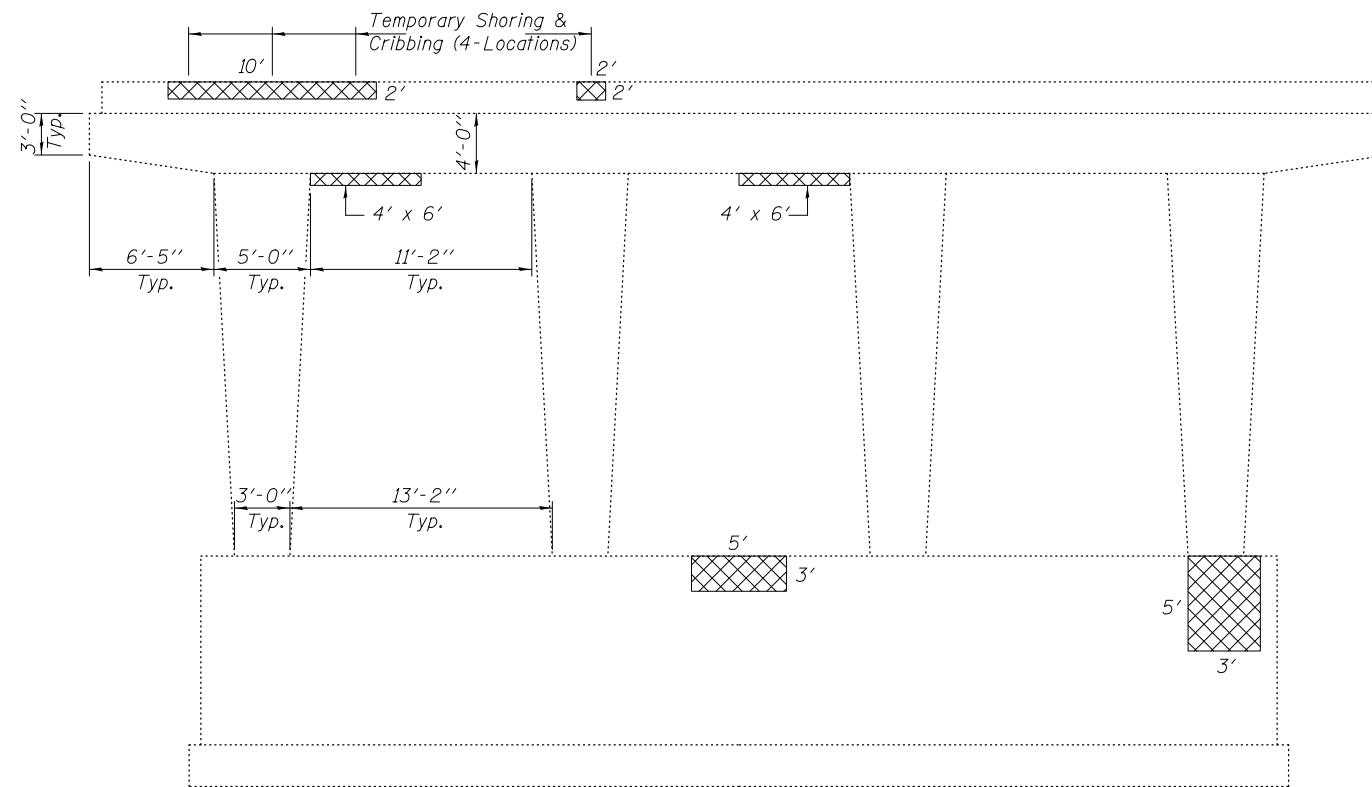
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS FOR PIER 4
SN 072-0132 (W.B.)

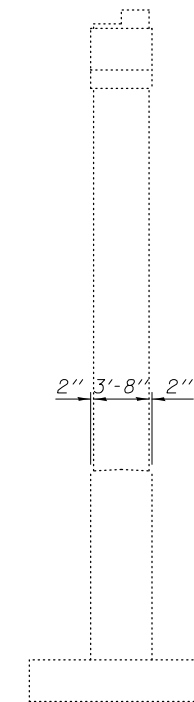
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	131
CONTRACT NO. 68887				

SHEET NO. 36 OF 49 SHEETS

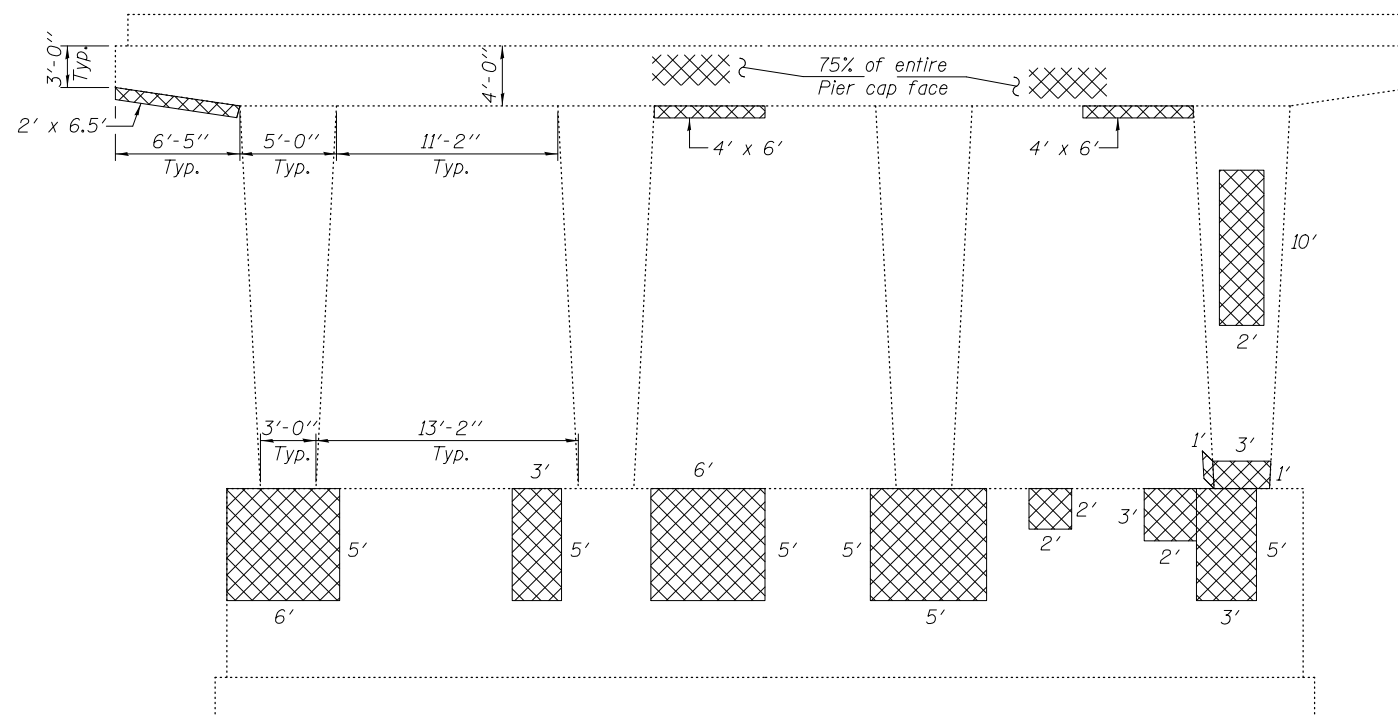
ILLINOIS FED. AID PROJECT



ELEVATION PIER 5 (W.B.)
(Looking South)



VIEW A-A



ELEVATION PIER 5 (W.B.)
(Looking North)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	463
Temporary Shoring & Cribbing	Each	4

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

EXAMINED
PASSED
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2018
REVISED
REVISED

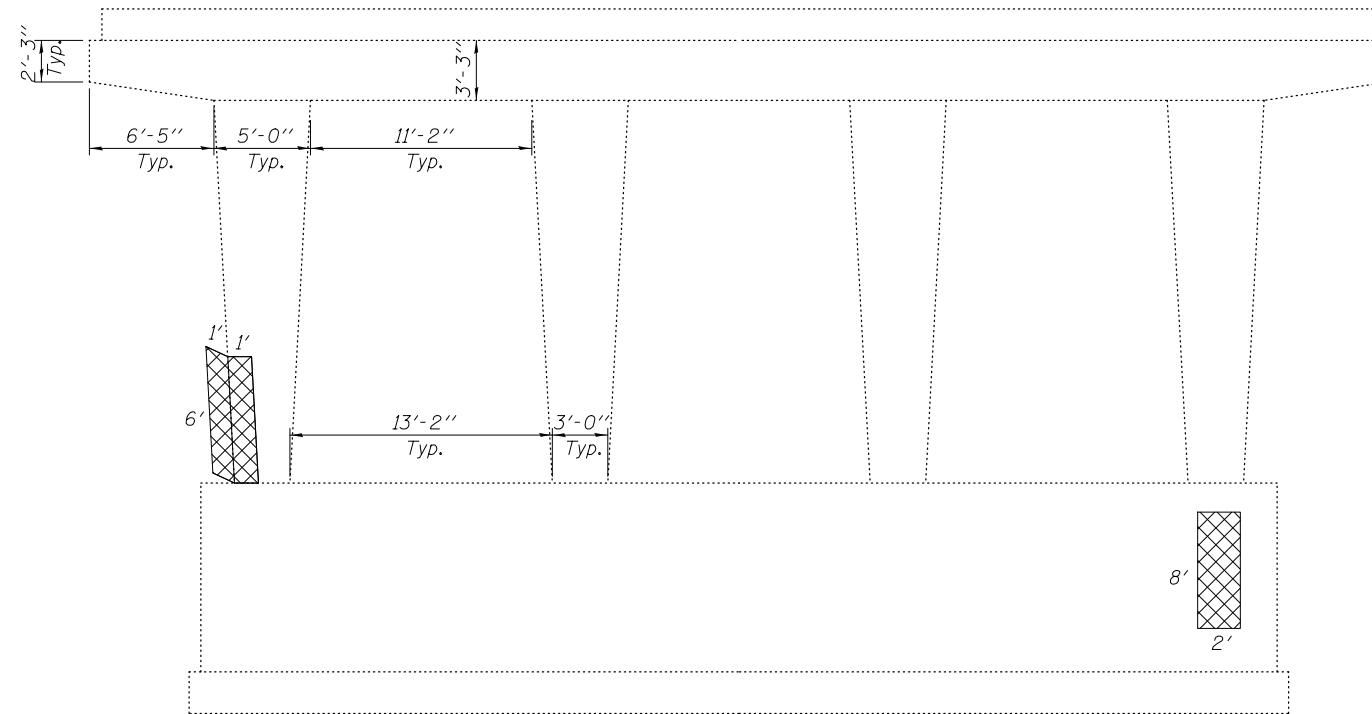
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS FOR PIER 5
SN 072-0132 (W.B.)

SHEET NO. 37 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	132
CONTRACT NO. 68887				

ILLINOIS FED. AID PROJECT



ELEVATION PIER 6 (W.B.)
(Looking North)



ELEVATION SOUTH ABUTMENT (W.B.)
(Looking South)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth ≤ 5')	Sq. Ft.	113

Notes:
Crosshatched areas indicate Structural Repair of Concrete (Depth ≤ 5')

DESIGNED - SMR	EXAMINED	DATE - JANUARY 31, 2018
CHECKED - RPN	<i>Timothy A. Daulton</i> ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED	REVISED
CHECKED - SMR RPN	<i>Carl Kuyper</i> ENGINEER OF BRIDGES AND STRUCTURES	REVISED

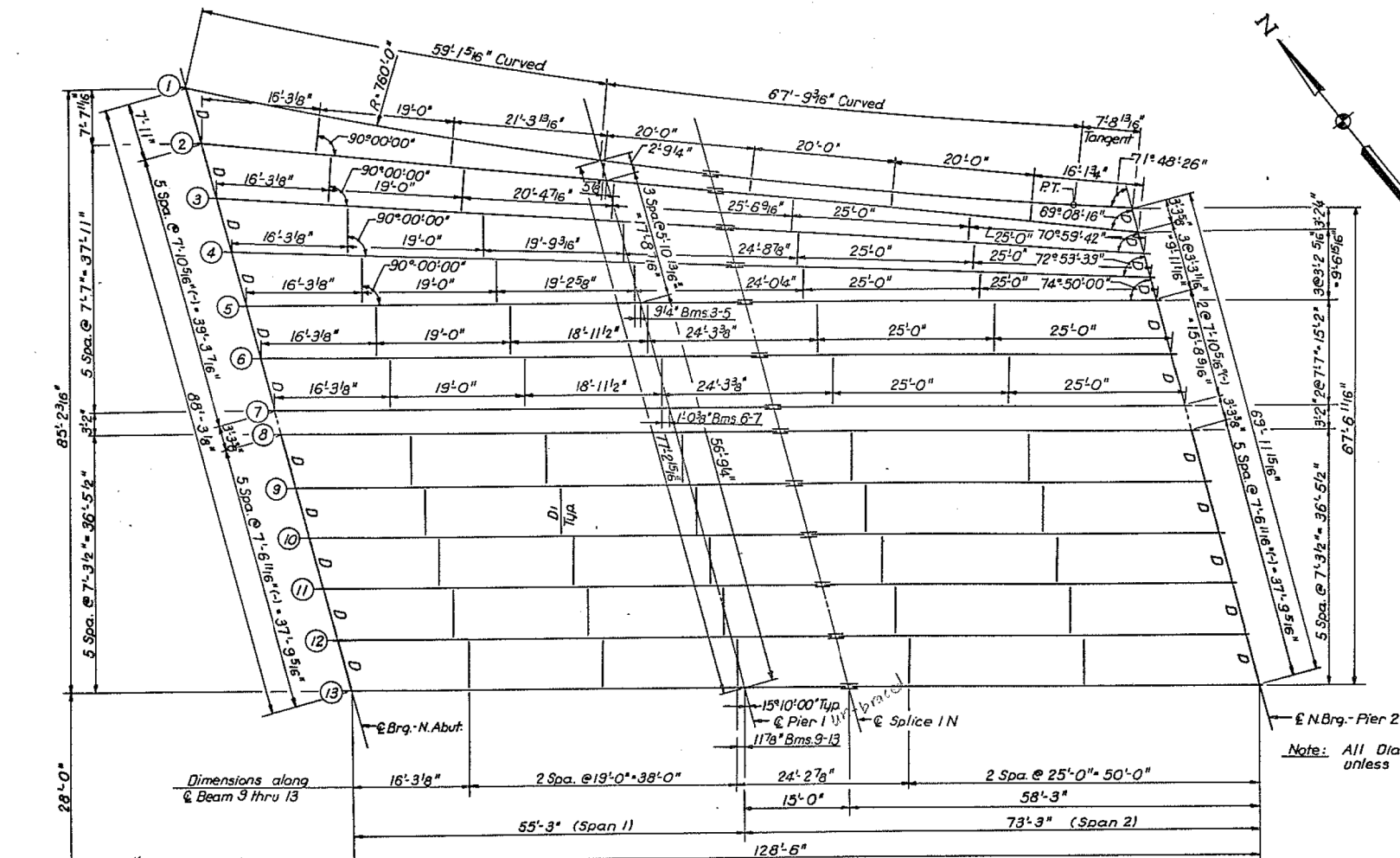
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE REPAIR DETAILS FOR PIER 6 & S. ABUT.
SN 072-0132 (W.B.)

SHEET NO. 38 OF 49 SHEETS

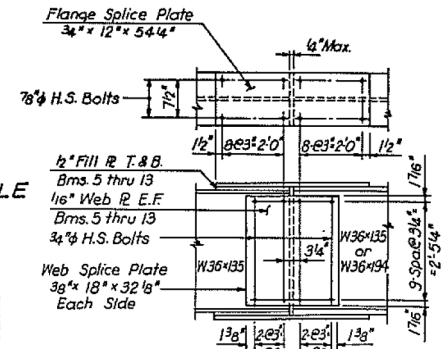
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB)B-R	PEORIA	196	133
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HVB-1.4A	PEORIA	208	75
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		



INTERIOR BEAM MOMENT TABLE

	.4 Span 1	Pier 1	.6 Span 2
Is (in ⁴)	7,820	12,318	12,100
Ic (in ⁴)	21,628	—	29,919
Ss (in ³)	44.0	669	665
Sc (in ³)	663	—	954
Q (k/ft)	.977	.977	.977
M _Q (k)	143.3	536.2	414.5
fs _Q (k.s.i)	3.9	9.6	7.5
g _Q (k/ft)	.407	.407	.407
M _{SQ} (k)	83.6	163.5	196.6
M _L (k)	415.9	298.6	625.0
M _{Imp.} (k)	115.6	79.1	157.5
Total (k)	615.1	541.2	979.1
fs _{SQ+I} (k.s.i)	11.1	9.7	12.3
fs _{total} (k.s.i)	15.0	19.3	19.8
VR (k)	53.6	—	57.6



DETAIL OF SPLICE IN

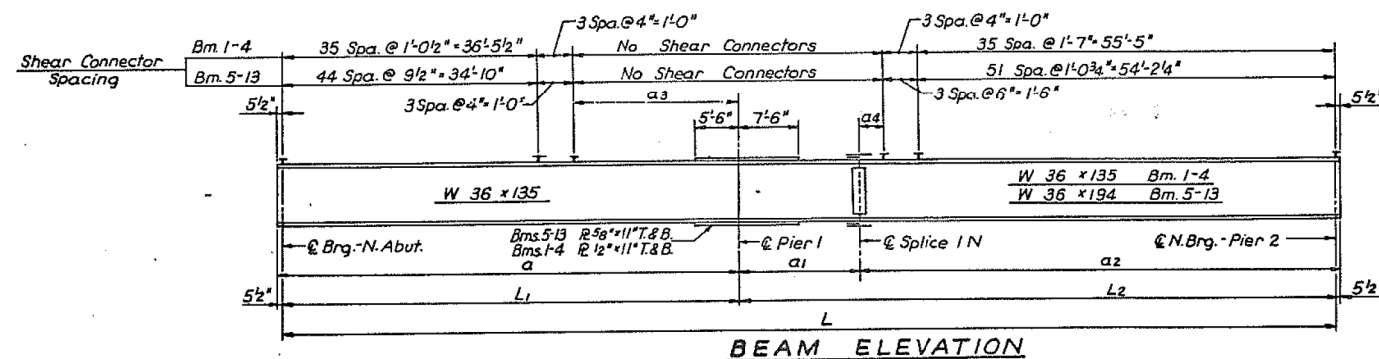
Is and Ss are the moment of inertia and section modulus of the steel section used in computing fs_Q.
Ic and Sc are the moment of inertia and section modulus of the composite section used in computing fs_{SQ+I}.
VR is the maximum shear range in span, used to determine shear connector spacing.

TOP OF BEAM ELEVATION

Beams	@ Brq. N. Abut.	@ Pier 1	@ Splice IN	@ N. Brq. Pier 2
1	484.01	484.80	485.01	485.51
2	484.64	485.07	485.19	485.76
3	485.26	485.53	485.60	486.00
4	485.68	485.95	486.02	486.08
5	486.10	486.33	*486.44	486.49
6	486.52	486.76	*486.86	486.92
7	486.76	487.00	*487.10	487.16
8	487.07	487.14	*487.21	487.26
9	487.30	487.38	*487.44	487.49
10	487.43	487.51	*487.57	487.63
11	487.50	487.58	*487.64	487.70
12	487.39	487.47	*487.53	487.58
13	487.26	487.33	*487.40	487.45

Notes:
For Diaphragms D & Di see Sh. 18.
For Bearing Details see Sh. 22.

FOR INFORMATION ONLY

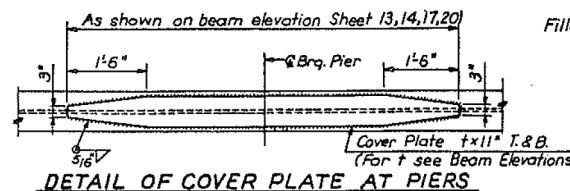


BEAM SCHEDULE

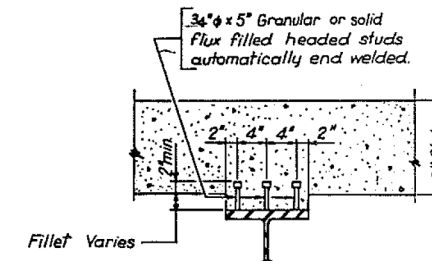
Beams	L	L1	L2	a	a1	a2	a3	a4
1	134'-7 5/8"	59'-1 5/8"	75'-6"	59'-6 13/16"	15'-8 5/16"	60'-3 3/8"	21'-7 13/16"	3'-4 1/16"
2	132'-8 1/16"	57'-0 13/16"	75'-7 7/8"	57'-6 5/16"	15'-5 15/16"	60'-7 1/2"	19'-7 5/16"	3'-9"
3	131'-2 1/8"	56'-4 13/16"	74'-9 5/16"	56'-10 5/16"	15'-3 3/4"	59'-11 1/16"	18'-11 5/16"	3'-0 9/16"
4	129'-9 3/16"	55'-9 3/16"	73'-11 5/8"	56'-3 1/16"	15'-1 3/4"	59'-3 3/8"	18'-4 1/16"	2'-4 7/8"
5-13	128'-6"	55'-3"	73'-3"	55'-8 1/2"	15'-0"	58'-8 1/2"	19'-5"	2'-6 3/4"

BY: D.P.
BY: K.M.
BY: A.T.

* See Framing Plan



DETAIL OF COVER PLATE AT PIERS



SHEAR CONNECTOR DETAIL

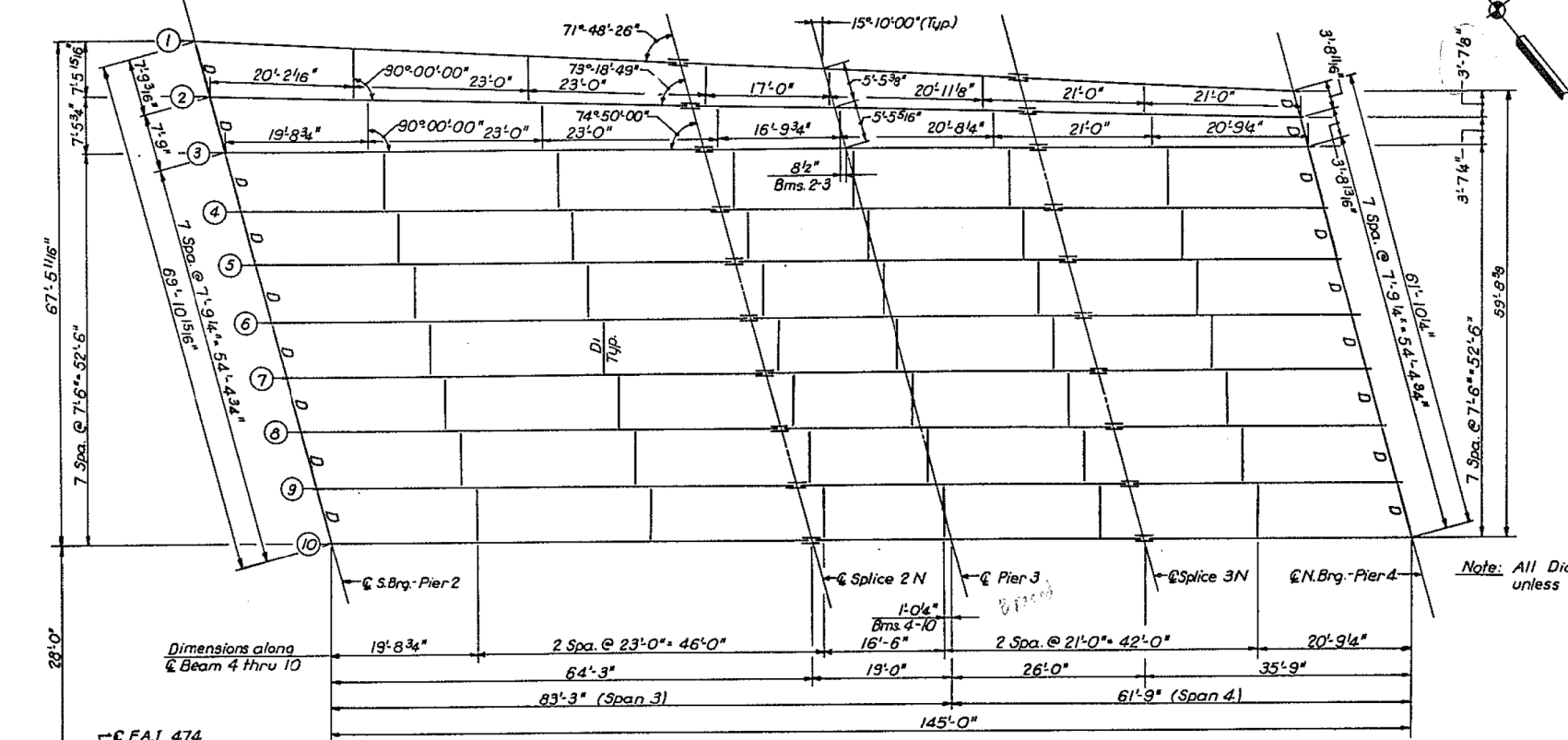
No. Req'd. = 3,717 N.B. Spans 1-2
3,150 N.B. Spans 3-4
3,618 S.B. Spans 1-4
2,472 Ramp C Spans 1-4

INTERIOR BEAM REACTION TABLE

	N. Abut.	Pier 1	Pier 2 N. Brq.
R _Q (k)	25.4	111.0	41.0
R _L (k)	42.4	54.2	45.2
Imp. (k)	11.8	14.4	11.4
R _{total} (k)	79.6	179.6	97.6

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
STRUCTURAL STEEL
NORTHBOUND ROADWAY & RAMP B
F.A.I. ROUTE 474 & RAMPS B AND C
OVER C.R.I. & P. R.R., P. & U. RY. AND
RELOCATED KICKAPOO CREEK
STATION 390 + 67.76
F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4 HVB-1
CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HVB-1, 4A-1	PEORIA	208	76
FED. ROAD DIV. NO. 7		ILLINOIS	PROJECT	



BEAM SCHEDULE

Beams	L	L1	L2	a	a1	a2	a3	a4	a5
1	147'-3 3/4"	84'-6 1/16"	62'-8 1/16"	17'-8 1/16"	19'-3 3/8"	26'-5"	36'-9 5/16"	4'-7 1/16"	3'-0 13/16"
2	146'-1 3/16"	83'-10 3/16"	62'-2 5/8"	17'-2 5/16"	19'-1 3/8"	26'-2 3/8"	36'-5 3/4"	4'-1 5/16"	2'-9 1/4"
3-10	145'-0"	83'-3"	61'-9"	16'-8 1/2"	19'-0"	26'-0"	35'-2 1/2"	3'-7 1/2"	2'-6"

TOP OF BEAM ELEVATION

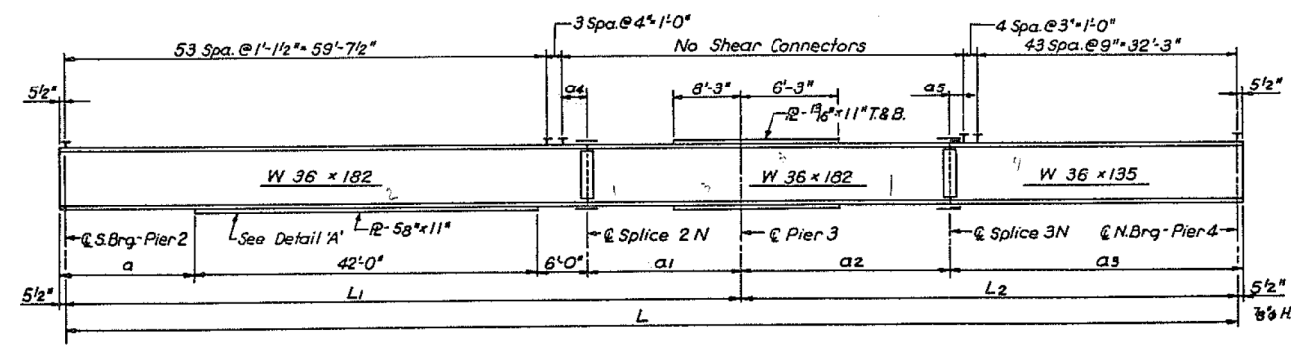
	1	2	3	4	5	6	7	8	9	10
@ S.Brg. Pier 2	485.52	486.10	486.86	487.06	487.30	487.47	487.60	487.68	487.56	487.42
@ Splice 2 N	485.88	486.20	486.52	486.92	487.16	487.33	487.46	487.54	487.42	487.28
@ Pier 3	486.02	486.28	486.54	486.89	487.13	487.30	487.43	487.51	487.39	487.25
@ Splice 3 N	486.20	486.39	486.58	486.84	487.08	487.25	487.38	487.46	487.34	487.20
@ N.Brg. Pier 4	486.47	486.57	486.67	486.81	487.05	487.22	487.35	487.43	487.31	487.17

* @ W 36 x 182

INTERIOR BEAM REACTION TABLE

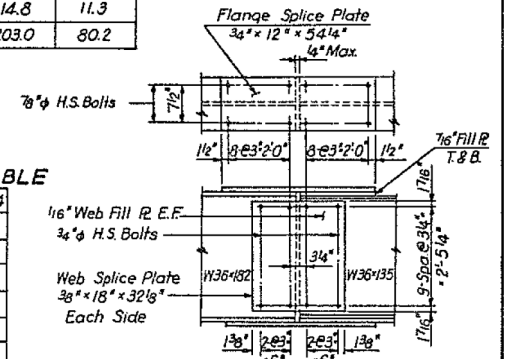
	Pier 2 S. Brg.	Pier 3	Pier 4 N. Brg.
R @ (k)	45.6	129.9	26.8
R L (k)	44.7	58.3	42.1
Imp. (k)	10.7	14.8	11.3
R total (k)	101.0	203.0	80.2

FRAMING PLAN SPANS 3-4 NORTHBOUND ROADWAY



INTERIOR BEAM MOMENT TABLE

	.4 Span 3	Pier 3	.6 Span 4
I _s (in ⁴)	13,379	17,462	7,820
I _c (in ⁴)	34,796	—	21,515
S _s (in ³)	802	920	440
S _c (in ³)	1,142	—	662
Q (k/')	.960	.960	.960
M @ (k')	487.8	764.4	137.0
f _s @ (ksi)	7.3	10.0	3.7
S @ (k/')	.438	.438	.438
M _s @ (k')	258.9	257.7	98.9
M _L (k)	694.4	407.1	468.5
M _{Imp} (k)	166.7	103.4	125.5
Total (k)	1,120.0	768.2	692.9
f _s (s@+t)(ksi)	11.8	10.0	12.6
f _s total (k.s.i.)	19.1	20.0	16.3
VR (k)	54.8	—	52.2

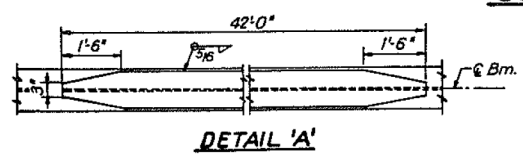


DETAIL OF SPLICE 3 N

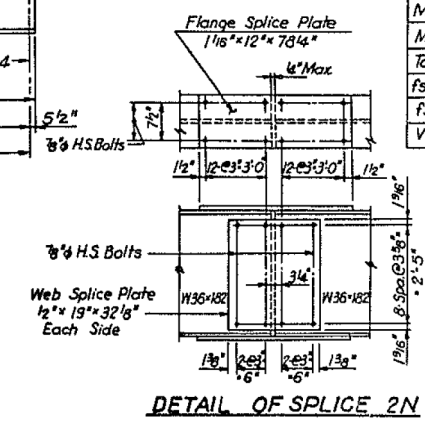
I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_s @.

I_c and S_c are the moment of inertia and section modulus of the composite section used in computing f_s(s@+t).

VR is the maximum V_t Impact shear range in span, used to determine shear connector spacing.



FOR INFORMATION ONLY

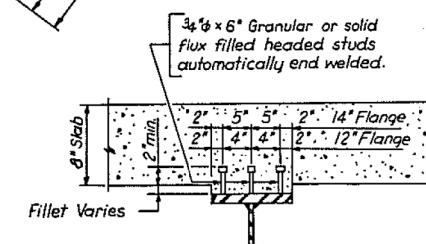
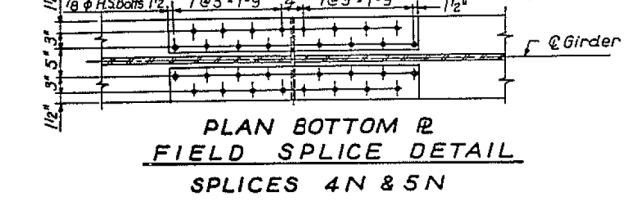
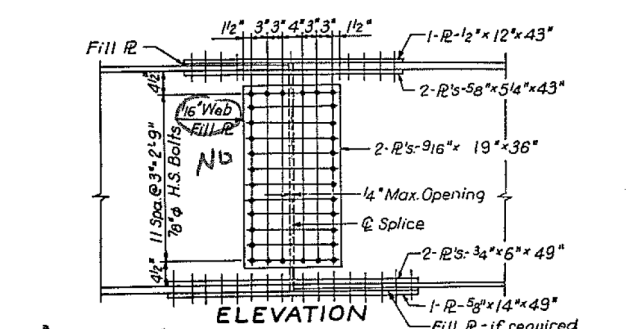
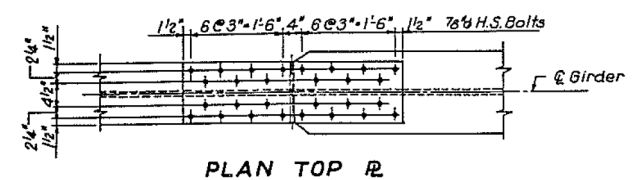
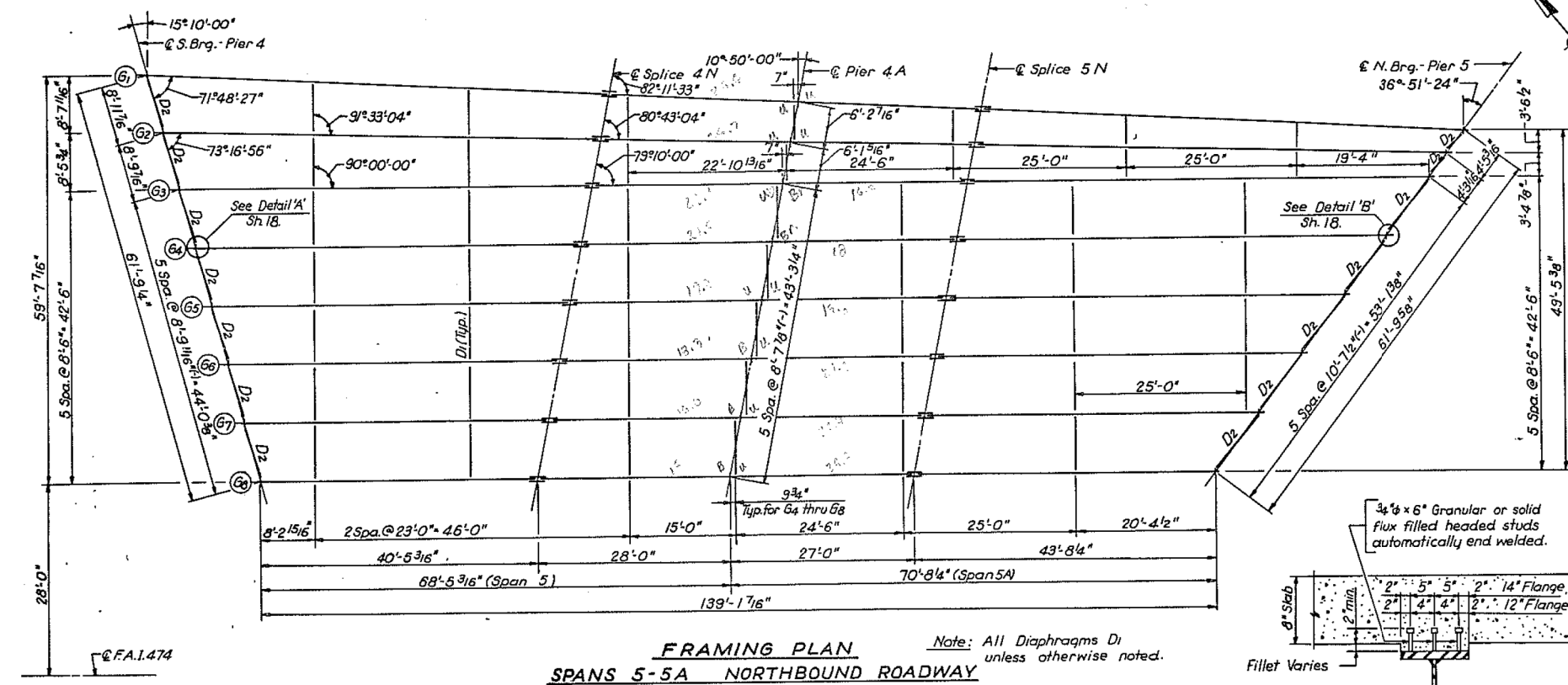


Notes:
 For Diaphragms D & D₁ see Sh. 18.
 For Cover R Detail @ Pier 8
 Shear Connectors see Sh. 13.
 For Bearing Details see Sh. 22

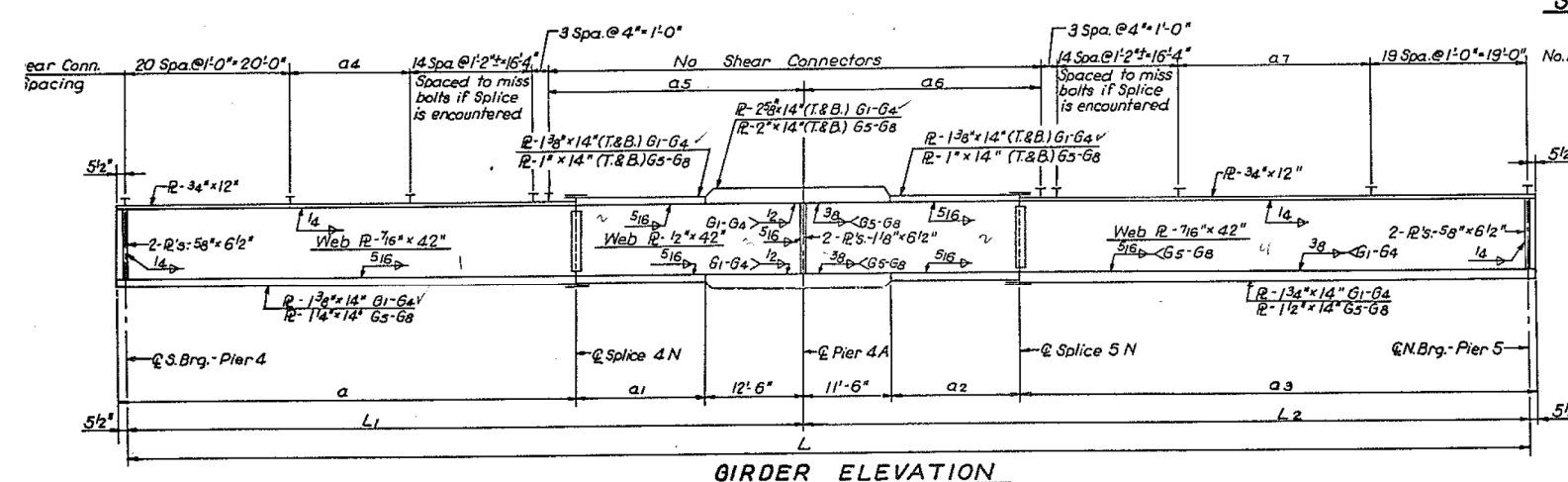
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
STRUCTURAL STEEL
 NORTHBOUND ROADWAY & RAMP B
 F.A.I. ROUTE 474 & RAMP B AND C
 OVER C.R.I. & R.R., P. & R.U. RY. AND
 RELOCATED KICKAPOO CREEK
 STATION 390 + 67.76
 F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-1
 CHRISTIAN-ROGE AND ASSOC.
 ENGINEERS
 CHICAGO, ILLINOIS

SHEET NO. 14 OF 70

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-474	72-4HB-1,4A-1	PEORIA	208	77
FED. ROAD DIV. NO. 7		ILLINOIS	PROJECT	



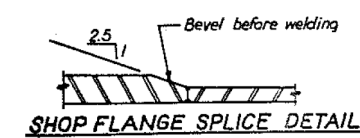
	1	2	3	4	5	6	7	8
@ S.Brg.-Pier 4	486.42	486.56	486.69	486.87	487.08	487.21	487.10	486.94
@ Splice 4N	486.24	486.39	486.53	486.71	486.94	487.08	486.98	486.83
@ Pier 4A	486.11	486.26	486.41	486.59	486.81	486.95	486.86	486.71
@ Splice 5N	485.99	486.14	486.29	486.46	486.69	486.83	486.73	486.59
@ N.Brg.-Pier 5	485.74	485.89	486.04	486.24	486.43	486.64	486.56	486.43



	.4 Span 5	Pier 4A	.6 Span 5A
Is (in ⁴)	14,787	39,721	16,272
Ic (in ⁴)	42,789	—	49,659
Ss (in ³)	843	1,681	1,009
Sc (in ³)	1,164	—	1,394
@ (K/')	1.12	1.12	1.12
M @ (K)	467.3	1,433.6	614.0
Fs @ (ksi)	6.7	10.2	7.3
S @ (K/')	.452	.452	.452
Ms @ (K)	247.4	431.3	306.7
M @ (K)	829.2	698.7	913.4
M imp (K)	194.0	161.4	208.3
Total (K)	1,270.6	1,291.4	1,428.4
Fs @ + I (ksi)	13.1	9.2	12.3
Fs total (Ksi)	13.8	13.4	19.6
VR (K)	64.2	—	64.1

	Pier 4 S.Brg.	Pier 4A	Pier 5 N.Brg.
R @ (K)	47.8	183.8	53.9
R @ (K)	51.0	77.6	51.6
Imp. (K)	11.9	17.9	11.8
R total (K)	110.7	279.3	117.3

Notes:
For Bearing Stiffener Details & Diaphragms D1 & D2 see Sh. 18.
For Bearing Details see Sh. 23.



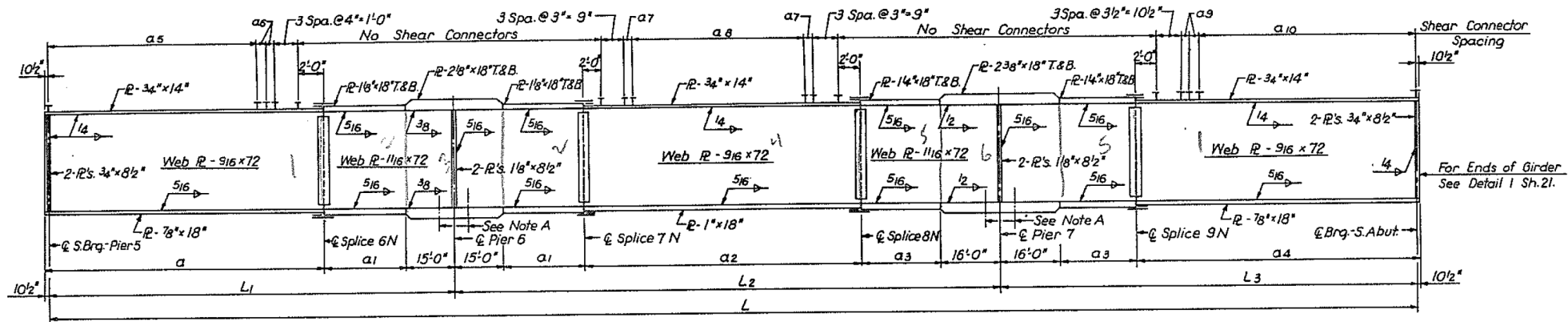
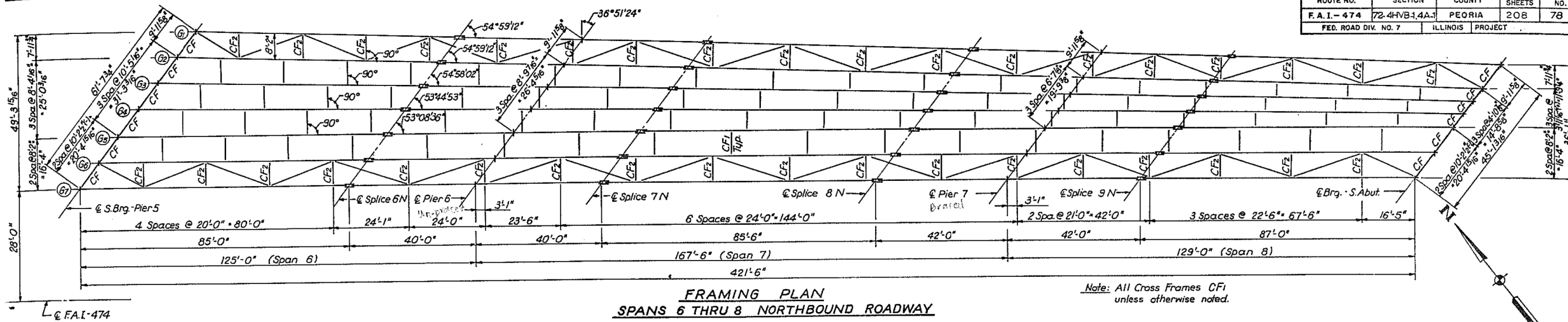
Girder	L	L1	L2	a	a1	a2	a3	a4	a5	a6	a7
1	192'-7 7/16"	95'-2 1/8"	97'-5 5/16"	67'-10 1/2"	15'-3 1/8"	15'-3 3/16"	71'-1 5/8"	22 Spa.@1'-3"=27'-6"	30'-4 1/8"	29'-10 5/16"	25 Spa.@1'-3"=31'-3"
2	187'-5 1/8"	91'-6 13/16"	95'-10 5/16"	64'-1 15/16"	15'-4 3/8"	15'-4 7/16"	69'-5 3/8"	20 Spa.@1'-3"=25'-0"	29'-2 13/16"	28'-3 5/16"	25 Spa.@1'-3"=31'-3"
3	182'-6 1/16"	88'-1 1/16"	94'-5"	60'-6 9/16"	15'-6"	15'-6"	67'-10 1/2"	18 Spa.@1'-3"=22'-6"	28'-3 1/16"	26'-10"	25 Spa.@1'-3"=31'-3"
4	173'-9 15/16"	84'-1 7/8"	89'-8 1/16"	56'-7 3/8"	15'-6"	15'-6"	63'-1 9/16"	16 Spa.@1'-3"=20'-0"	26'-9 7/8"	24'-7 1/16"	23 Spa.@1'-3"=28'-9"
5	165'-1 13/16"	80'-2 11/16"	84'-1 11/8"	52'-8 3/16"	15'-6"	15'-6"	58'-4 5/8"	14 Spa.@1'-3"=17'-6"	25'-4 11/16"	23'-7 1/8"	20 Spa.@1'-3"=25'-0"
6	156'-5 3/4"	76'-3 9/16"	80'-2 3/16"	48'-9 1/16"	15'-6"	15'-6"	53'-7 11/16"	12 Spa.@1'-3"=15'-0"	23'-11 9/16"	22'-7 3/16"	17 Spa.@1'-3"=21'-3"
7	147'-9 3/8"	72'-4 3/8"	75'-5 1/4"	44'-9 7/8"	15'-6"	15'-6"	48'-10 3/4"	10 Spa.@1'-3"=12'-6"	22'-6 3/8"	21'-7 1/4"	14 Spa.@1'-3"=17'-6"
8	139'-1 7/16"	68'-5 3/16"	70'-8 1/4"	40'-10 11/16"	15'-6"	15'-6"	44'-1 3/4"	10 Spa.@1'-0"=10'-0"	21'-1 3/16"	20'-4 1/4"	14 Spa.@1'-0"=14'-0"

Is and Ss are the moment of inertia and section modulus of the steel section used in computing Fs @.
Ic and Sc are the moment of inertia and section modulus of the composite section used in computing Fs (@+I).
VR is the maximum 1/4" Impact shear range in span, used to determine shear connector spacing.

STATE OF ILLINOIS
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STRUCTURAL STEEL
NORTHBOUND ROADWAY & RAMP B
F.A.I. ROUTE 474 & RAMPS B AND C
OVER C.R.I. & P.R.R., F. & R.U. RY. AND
RELOCATED KICKAPOO CREEK
STATION 390 + 67.76
F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-
CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

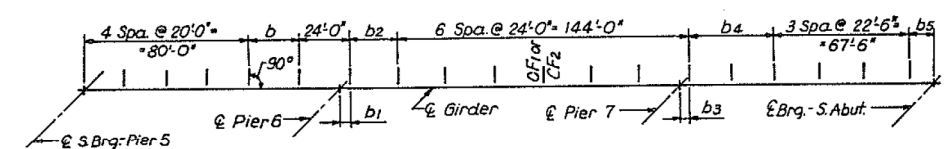
FOR INFORMATION ONLY

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HVB-1,4A-1	PEORIA	208	78
FED. ROAD DIV. NO. 7	ILLINOIS PROJECT			



Note A: For location of 1/16" Holes see Bearing Details.

GIRDER ELEVATION



CROSS FRAME SPACING

Girder	b	b1	b2	b3	b4	b5
2	21'-0 7/16"	2'-11"	19'-7 1/16"	2'-11"	2 Spa @ 20'-6" = 41'-0"	14'-7 5/16"
3	21'-7 3/4"	2'-7"	20'-2 7/8"	1'-11"	2 Spa @ 20'-8" = 41'-4"	16'-3 1/16"
4	22'-7 5/16"	2'-7"	21'-6 5/16"	1'-11"	2 Spa @ 20'-10" = 41'-8"	16'-10 5/16"
5	23'-7"	2'-7"	22'-10"	1'-11"	2 Spa @ 21'-0" = 42'-0"	17'-7"
6 & 7	24'-1"	3'-1"	23'-6"	3'-1"	2 Spa @ 21'-0" = 42'-0"	16'-5"

FOR INFORMATION ONLY

GIRDER SCHEDULE

Girders	L	L1	L2	L3	a	a1	a2	a3	a4	a5	a6	a7	a8	a9	a10
1 & 2	411'-9 7/16"	122'-1 7/16"	163'-7 1/16"	126'-0 5/16"	83'-11"	24'-0 15/16"	83'-6 3/8"	25'-0 3/8"	85'-10 7/16"	51 Spa @ 1'-6 1/2" = 78'-7 1/2"	8 1/2"	9 3/16"	51 Spa @ 1'-6" = 76'-6"	8 1/4"	51 Spa @ 1'-7" = 80'-9"
3	414'-11 1/16"	123'-0 3/4"	164'-10 7/8"	127'-0 1/16"	84'-6 1/16"	24'-4 3/16"	84'-2 8/16"	25'-4 3/16"	86'-6 3/8"	51 Spa @ 1'-6 1/2" = 78'-7 1/2"	1'-0 3/4"	1'-1 1/16"	51 Spa @ 1'-6" = 76'-6"	1'-0 3/16"	51 Spa @ 1'-7" = 80'-9"
4	418'-2 3/16"	124'-0 5/16"	166'-2 5/16"	127'-11 15/16"	85'-2 3/16"	24'-8 1/4"	84'-10"	25'-8 1/16"	87'-2 3/8"	51 Spa @ 1'-7" = 80'-9"	3 1/2"	4 1/4"	51 Spa @ 1'-6" = 76'-6"	3 7/16"	51 Spa @ 1'-7 1/2" = 82'-10 1/2"
5-7	421'-6"	125'-0"	167'-6"	129'-0"	85'-10 1/2"	25'-0"	85'-6"	26'-0"	87'-10 1/2"	51 Spa @ 1'-7" = 80'-9"	7 1/2"	8 1/4"	51 Spa @ 1'-6 1/2" = 78'-7 1/2"	7 1/2"	51 Spa @ 1'-7 1/2" = 82'-10 1/2"

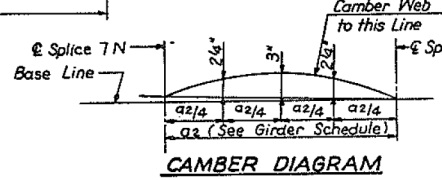
DESIGNED BY: DP
 DRAWN BY: K.M.
 CHECKED BY: A.T.

INTERIOR GIRDER MOMENT TABLE

	4 Span 6	Pier 6	5 Span 7	Pier 7	6 Span 8
I _s (in ⁴)	51,765	126,510	54,291	139,678	51,765
I _c (in ⁴)	121,030	—	128,372	—	121,030
S _s (in ³)	1,522	3,318	1,644	3,640	1,522
S _c (in ³)	2,105	—	2,262	—	2,105
Q (k/ft)	1.15	1.15	1.15	1.15	1.15
M _D (k)	1,011.0	2,862.8	1,068.5	3,066.1	1,070.0
f _{sD} (ksi)	8.0	10.4	7.8	10.1	8.4
s _D (k/ft)	.423	.423	.423	.423	.423
M _{SD} (k)	437.9	887.8	563.2	952.4	463.7
M _L (k)	1,213.5	1,459.8	1,346.3	1,541.8	1,253.5
M _{IMP} (k)	242.7	268.6	230.2	282.2	246.9
Total (k)	1,894.1	2,616.2	2,139.7	2,776.4	1,964.1
f _{sD} + f _{sL} (ksi)	10.8	9.5	11.4	9.2	11.2
f _s total (ksi)	18.8	19.9	19.2	19.3	19.6
VR (k)	70.7	—	77.3	—	71.8

INTERIOR GIRDER REACTION TABLE

	Pier 5	Pier 6	Pier 7	S. Abut.
R _Q (k)	68.2	258.2	265.8	70.3
R _L (k)	51.0	102.9	104.6	50.8
Imp. (k)	10.2	18.9	19.2	10.0
R total (k)	129.4	380.0	389.6	131.1



CAMBER DIAGRAM

TOP OF WEB ELEVATION

	G1	G2	G3	G4	G5	G6	G7
Q S. Brq. Pier 5	485.87	486.10	486.27	486.42	486.57	486.52	486.43
Q Splice 6N	485.22	485.45	485.62	485.77	485.91	485.85	485.77
Q Pier 6	484.92	485.15	485.32	485.46	485.60	485.54	485.46
Q Splice 7N	484.62	484.84	485.01	485.15	485.29	485.23	485.15
Q Splice 8N	483.84	484.06	484.23	484.36	484.49	484.44	484.35
Q Pier 7	483.42	483.65	483.81	483.94	484.07	484.01	483.92
Q Splice 9N	483.01	483.24	483.39	483.52	483.64	483.59	483.50
Q Brq. S. Abut.	482.29	482.52	482.66	482.78	482.90	482.85	482.76

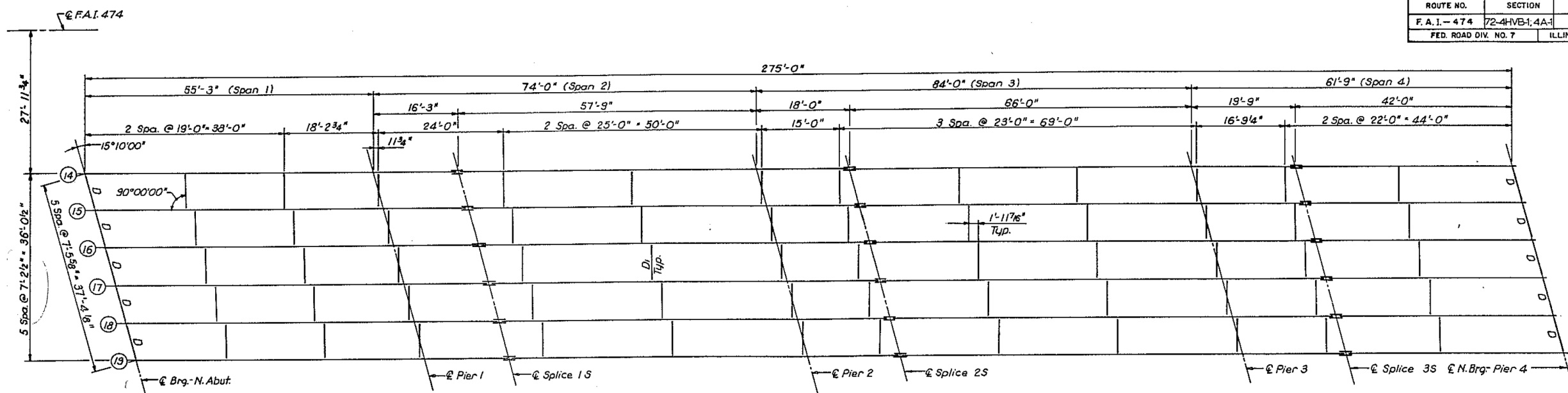
I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_{sD}.
 I_c and S_c are the moment of inertia and section modulus of the composite section used in computing f_{sD} + f_{sL}.
 VR is the maximum impact shear range in span, used to determine shear connector spacing.

Notes:
 For Cross Frames, Lateral Bracing, Field Splice & Bearing Stiffeners see Sh. 21.
 For Shear Connectors & Shop Flange Splice Detail see Sh. 15.
 For Bearing Details see Sh. 23.

STATE OF ILLINOIS
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 STRUCTURAL STEEL
 NORTHBOUND ROADWAY & RAMP B
 F.A.I. ROUTE 474 & RAMPS B AND C
 OVER C.R.I. & P. R.R., P. & U. RY. AND
 RELOCATED KICKAPOO CREEK
 STATION 390 + 67.76
 F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-
 CHRISTIAN-ROGE AND ASSOC.
 ENGINEERS
 CHICAGO, ILLINOIS

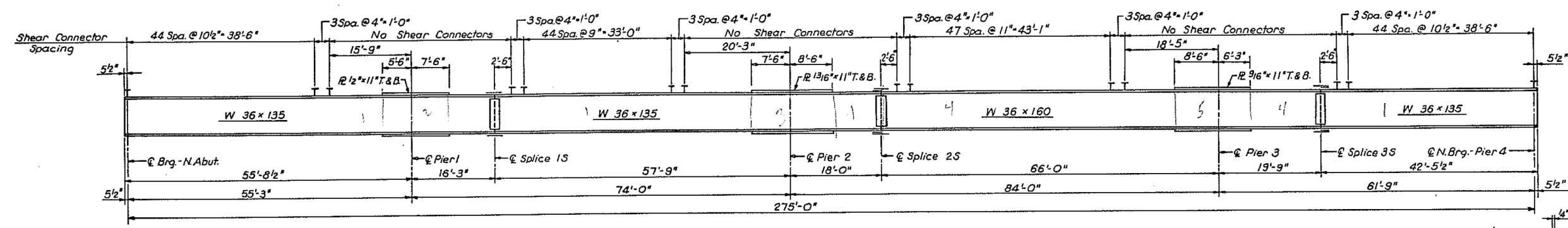
SHEET
16 OF

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-474	72-4(HV)-1,4A-1	PEORIA	208	79
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		



FRAMING PLAN
SPANS 1 THRU 4 SOUTHBOUND ROADWAY

Note: All Diaphragms D1 unless otherwise noted.



BEAM ELEVATION

TOP OF BEAM ELEVATION

	14	15	16	17	18	19
Brq. N. Abut.	487.41	487.54	487.65	487.58	487.46	487.31
Pier 1	487.40	487.53	487.64	487.58	487.46	487.30
Splice 1S	487.40	487.53	487.64	487.57	487.55	487.30
Pier 2	487.35	487.48	487.59	487.53	487.41	487.25
Splice 2S	487.35	487.49	487.60	487.53	487.41	487.25
Pier 3	487.21	487.34	487.45	487.39	487.27	487.12
Splice 3S	487.17	487.30	487.41	487.35	487.23	487.08
N. Brq. Pier 4	487.10	487.23	487.34	487.27	487.15	487.00

* @ W 36 x 160

INTERIOR BEAM REACTION TABLE

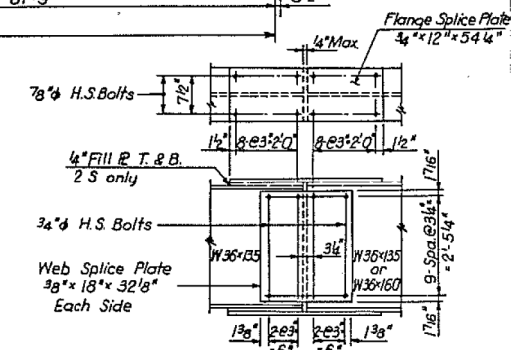
	N. Abut.	Pier 1	Pier 2	Pier 3	Pier 4
R @ (K)	27.5	95.1	109.8	111.6	29.5
R L (K)	40.0	50.3	55.4	54.4	40.6
Imp. (K)	11.1	13.3	13.6	13.7	10.9
R total (K)	78.6	158.7	178.8	179.7	81.0

I_s and S_c are the moment of inertia and section modulus of the steel section used in computing f_s@. I_c and S_c are the moment of inertia and section modulus of the composite section used in computing f_s(s_g+t). VR is the maximum L+Impact shear in span, used to determine shear connector spacing.

Notes:
For Diaphragms D & D1 see Sh. 18.
For Cover R Detail @ Pier & Shear Connector Detail see Sh. 13.
For Bearing Details see Sh. 22.

INTERIOR BEAM MOMENT TABLE

	.4Span 1	Pier 1	.5Span 2	Pier 2	.5Span 3	Pier 3	.6Span 4
I _s (in ⁴)	7,820	11,370	7,820	13,705	9,760	13,874	7,820
I _c (in ⁴)	21,269	—	21,269	—	24,961	—	21,269
S _s (in ³)	440	622	440	737	542	747	440
S _c (in ³)	659	—	659	—	786	—	659
Q (k/')	.941	.941	.941	.941	.941	.941	.941
M @ (1'k)	181.7	407.2	166.0	548.8	270.1	570.8	202.2
f _s @ (K.s.i)	5.0	7.9	4.5	8.9	6.0	9.2	5.5
S @ (k/')	.420	.420	.420	.420	.420	.420	.420
M s @ (1'k)	96.2	144.0	112.9	205.0	167.8	200.1	112.1
M L (1'k)	399.8	286.8	467.5	370.8	561.8	354.7	457.5
M Imp. (1'k)	110.9	75.6	117.5	90.9	134.4	89.6	122.5
Total (1'k)	605.9	506.4	697.9	666.7	864.0	644.4	692.1
f _s s _g +t (K.s.i)	11.1	9.8	12.7	10.9	13.2	10.4	12.6
f _s total (K.s.i)	16.1	17.7	17.2	19.8	19.2	19.6	18.1
VR (k)	530	—	55.3	—	54.9	—	53.5



DETAIL OF SPLICE 1S, 2S & 3S

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
STRUCTURAL STEEL
EAST SOUTHBOUND ROADWAY
F.A.I. ROUTE 474 & RAMPS B AND C
OVER C.R.I. & R.R., P & P.U. RY. AND
RELOCATED KICKAPOO CREEK
STATION 390 + 67.76
F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HV8-1
CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

SHEET 17 OF 70

FOR INFORMATION ONLY

DESIGNED BY: D.P.
CHECKED BY: K.M.
DRAWN BY: A.T.

EXAMINED: *Timothy A. Doolittle*
ENGINEER OF STRUCTURAL SERVICES
PASSED: *Carl Kroyer*
ENGINEER OF BRIDGES AND STRUCTURES

DATE: JANUARY 31, 2018
REVISED: _____
REVISED: _____

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

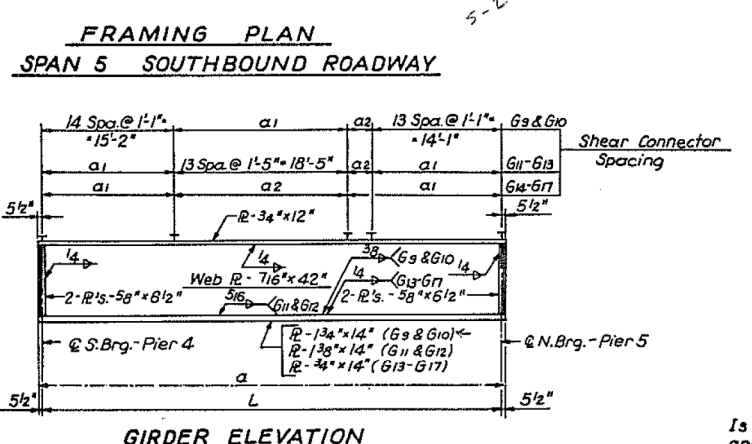
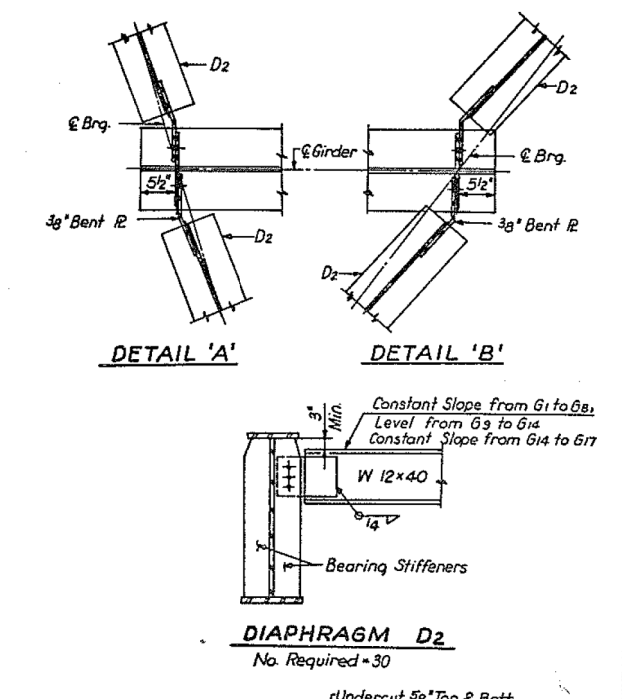
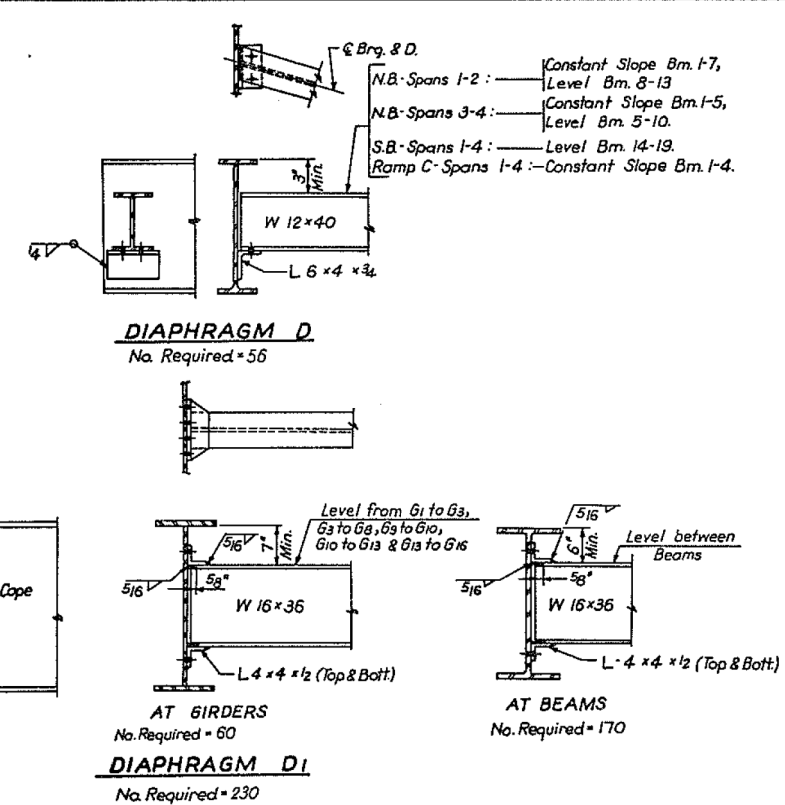
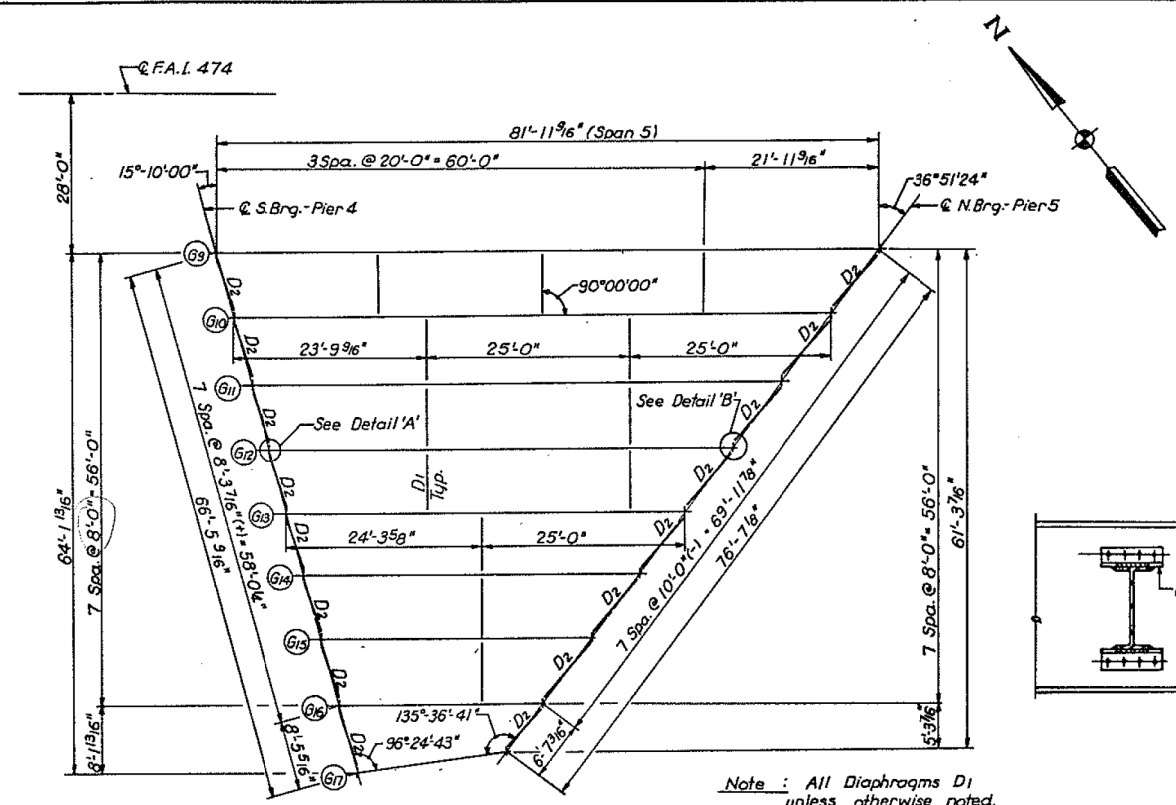
STRUCTURAL STEEL DETAILS (FOR INFORMATION ONLY)
SN 072-0131 (E.B.) & -0132 (W.B.)

SHEET NO. 43 OF 49 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HV), HV8-B-R	PEORIA	196	138
CONTRACT NO. 68887				ILLINOIS FED. AID PROJECT

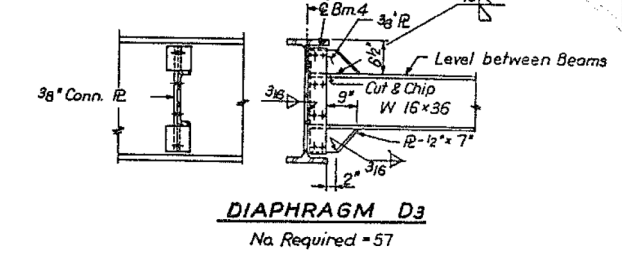
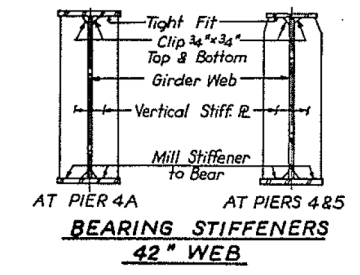
DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HVB-1,4A	PEORIA	208	80
FED. ROAD DIV. NO. 7		ILLINOIS PROJECT		



INTERIOR GIRDER MOMENT TABLE

	.5 Span 5
I_a (in ⁴)	16,272
I_c (in ⁴)	49,659
S_s (in ³)	1,009
S_c (in ³)	1,394
Q (k/ft)	1.065
M_D (k)	728.9
$f_s Q$ (k.s.i)	8.7
$S Q$ (k/ft)	.457
$M_S Q$ (k)	312.8
M_L (k)	764.9
M_{Imp} (k)	192.0
Total (k)	1,269.7
$f_s(Q+I+I)$ (k.s.i)	10.9
f_s total (k.s.i)	19.6
VR (k)	57.2



GIRDER SCHEDULE

Girder	L	a	a1	a2
9	81'-11 9/16"	82'-10 9/16"	39 Spa. @ 1'-4" = 52'-0"	8 3/16"
10	73'-9 9/16"	74'-8 9/16"	33 Spa. @ 1'-4" = 44'-0"	8 3/16"
11	65'-7 9/16"	66'-6 9/16"	20 Spa. @ 1'-2" = 23'-4"	6 5/8"
12	57'-5 9/16"	58'-4 9/16"	16 Spa. @ 1'-2 1/2" = 19'-4"	4 5/8"
13	49'-3 9/16"	50'-2 9/16"	13 Spa. @ 1'-2" = 15'-2"	6 5/8"
14	41'-1 9/16"	42'-0 9/16"	18 Spa. @ 1'-1 1/2" = 20'-3"	7 5/8"
15	32'-11 9/16"	33'-10 9/16"	18 Spa. @ 10 1/2" = 15'-9"	1'-5 9/16"
16	24'-9 1/16"	25'-8 1/16"	18 Spa. @ 8" = 12'-0"	9 1/16"
17	18'-10 1/4"	19'-9 1/4"	18 Spa. @ 6" = 9'-0"	10 1/4"

INTERIOR GIRDER REACTION TABLE

	Pier 4 or Pier 5
R _Q (k)	56.3
R _L (k)	48.0
Imp (k)	12.1
R total (k)	116.4

TOP OF WEB ELEVATION

	G9	G10	G11	G12	G13	G14	G15	G16	G17
@ S.Brg. - Pier 4	487.03	487.17	487.29	487.15	487.00	486.87	486.44	485.88	485.40
@ N.Brg. - Pier 5	486.51	486.70	486.87	486.79	486.69	486.60	486.23	485.72	485.27

FOR INFORMATION ONLY

DESIGNED BY: D.P.
 CHECKED BY: K.M.
 DRAWN BY: A.T.

EXAMINED: *Timothy A. [Signature]*
 ENGINEER OF STRUCTURAL SERVICES
 PASSED: *Carl [Signature]*
 ENGINEER OF BRIDGES AND STRUCTURES
 DATE: JANUARY 31, 2018

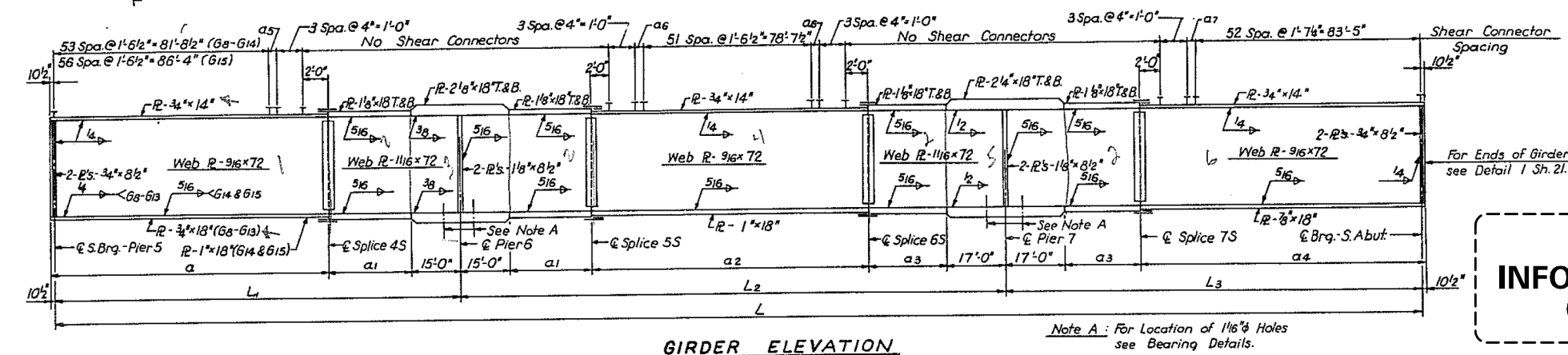
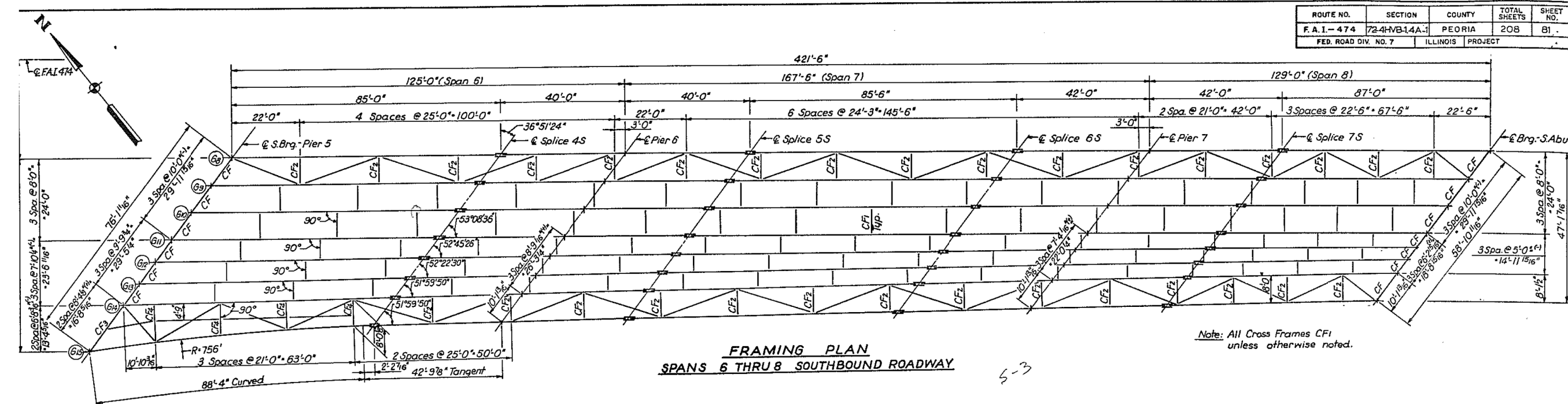
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

STRUCTURAL STEEL DETAILS (FOR INFORMATION ONLY)
 SN 072-0131 (E.B.) & -0132 (W.B.)
 SHEET NO. 44 OF 49 SHEETS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 STRUCTURAL STEEL
 SOUTHBOUND ROADWAY & RAMP C
 F.A.I. ROUTE 474 & RAMPS B AND C
 OVER C.R.I. & P.R.R., P. & U. RY. AND
 RELOCATED KICKAPOO CREEK
 STATION 390 + 67.76
 F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-1
 CHRISTIAN-ROGE AND ASSOC.
 ENGINEERS
 CHICAGO, ILLINOIS
 SHEET 18 OF 70

DESIGNED - SMR
 CHECKED - RPN
 DRAWN - Kyle M. Steffen
 CHECKED - SMR RPN

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HB-1A-1	PEORIA	208	81
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		



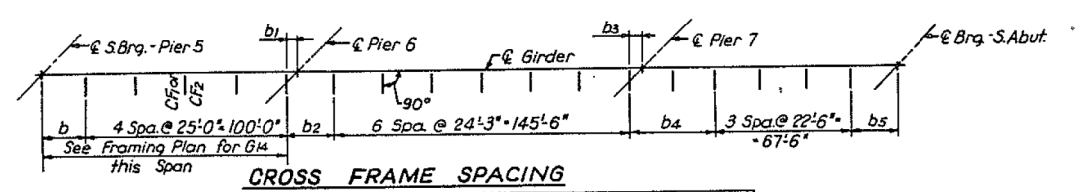
INTERIOR GIRDER MOMENT TABLE

	4 Span 6	Pier 6	5 Span 7	Pier 7	6 Span 8
Is (in ⁴)	49,083	126,510	54,291	133,073	51,765
Ic (in ⁴)	113,487	—	128,372	—	121,030
Ss (in ³)	1,400	3,318	1,644	3,479	1,522
Sc (in ³)	1,947	—	2,262	—	2,105
Q (k/ft)	1.12	1.12	1.12	1.12	1.12
M _Q (k)	976.9	2,807.5	1,033.4	2,991.2	1,044.0
f _s @ (k/si)	8.4	10.2	7.5	10.3	8.2
S _D (k/ft)	.436	.436	.436	.436	.436
M _S @ (k)	448.6	922.1	576.4	983.0	477.4
M _L (k)	1,181.3	1,440.9	1,320.5	1,514.0	1,222.7
M _{IMP} (k)	236.2	265.1	225.8	277.1	240.9
Total (k)	1,866.1	2,628.1	2,122.7	2,774.1	1,941.0
f _s @ +I (k/si)	11.5	9.5	11.3	9.6	11.1
f _s total (k/si)	19.9	19.7	18.8	19.9	19.3
VR (k)	69.3	—	75.8	—	70.3

FOR INFORMATION ONLY

INTERIOR GIRDER REACTION TABLE

	Pier 5	Pier 6	Pier 7	S.Abut.
R _Q (k)	67.3	255.8	262.6	69.6
R _L (k)	49.8	100.8	102.5	49.7
Imp (k)	10.0	18.6	18.7	9.8
R total (k)	127.1	375.2	383.8	129.1



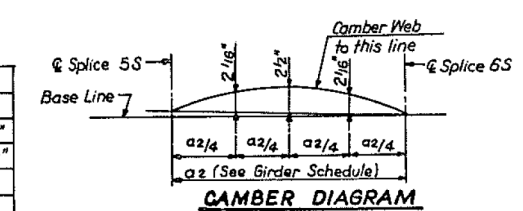
GIRDER SCHEDULE

Girders	L	L ₁	L ₂	L ₃	a	a ₁	a ₂	a ₃	a ₄	a ₅	a ₆	a ₇
8-11	421'-6"	125'-0"	167'-6"	129'-0"	85'-10 1/2"	25'-0"	85'-6"	25'-0"	87'-10 1/2"	3 1/2' i	5'4"	7'
12	423'-7 1/16"	125'-7 1/16"	168'-4 1/4"	129'-7 7/8"	86'-3 3/4"	25'-2 7/16"	85'-11 1/4"	25'-2 9/16"	88'-3 13/16"	8 3/4"	7'8"	1'-0 5/16"
13	425'-9 9/16"	126'-3 3/8"	169'-2 9/16"	130'-3 3/8"	86'-8 15/16"	25'-4 15/16"	86'-4 7/16"	25'-5 3/16"	88'-9 3/16"	1'-1 15/16"	10'2 1/4"	1'-5 1/16"
14	428'-0 1/16"	126'-11 3/16"	170'-1"	130'-11 7/8"	87'-2 1/4"	25'-7 7/16"	86'-9 9/16"	25'-7 3/4"	89'-2 5/8"	1'-7 1/4"	1'-1 1/8" (f)	1'-11 1/8"
15	432'-2 3/4"	131'-1 7/8"	170'-1"	130'-11 7/8"	91'-5"	25'-7 7/16"	86'-9 9/16"	25'-7 3/4"	89'-2 5/8"	1'-2 1/2"	1'-1 1/8" (f)	1'-11 1/8"

TOP OF WEB ELEVATION

	G8	G9	G10	G11	G12	G13	G14	G15
☉ S.Brg. - Pier 5	486.56	486.76	486.94	486.87	486.81	486.72	486.23	485.37
☉ Splice 4S	486.00	486.20	486.38	486.31	486.25	485.98	485.60	485.14
☉ Pier 6	485.73	485.93	486.11	486.04	485.98	485.75	485.48	485.13
☉ Splice 5S	485.46	485.66	485.84	485.77	485.71	485.53	485.35	485.11
☉ Splice 6S	484.77	484.97	485.15	485.08	485.01	484.92	484.83	484.72
☉ Pier 7	484.35	484.55	484.73	484.66	484.59	484.50	484.41	484.30
☉ Splice 7S	483.94	484.14	484.32	484.25	484.18	484.08	483.99	483.88
☉ Brq. - S.Abut.	483.19	483.39	483.57	483.50	483.42	483.33	483.23	483.12

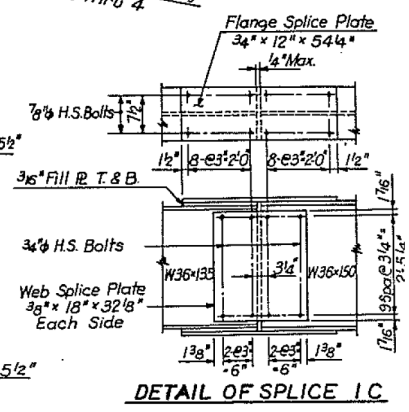
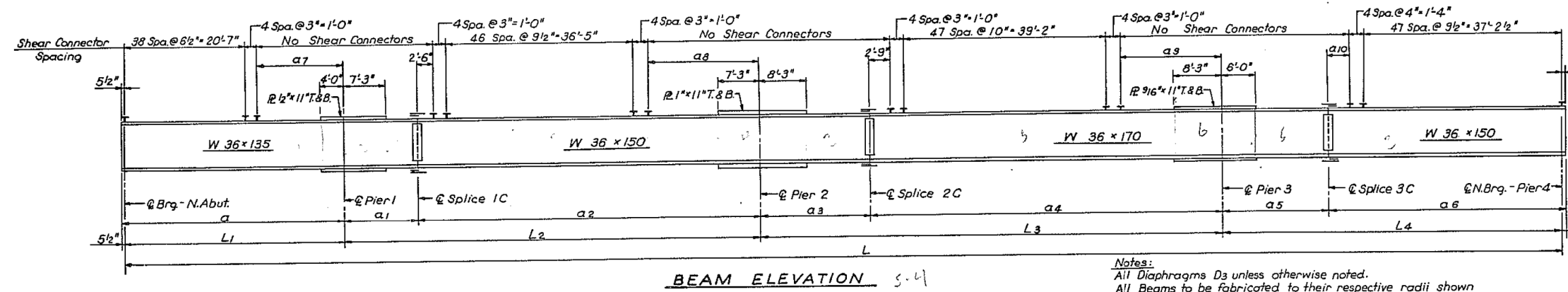
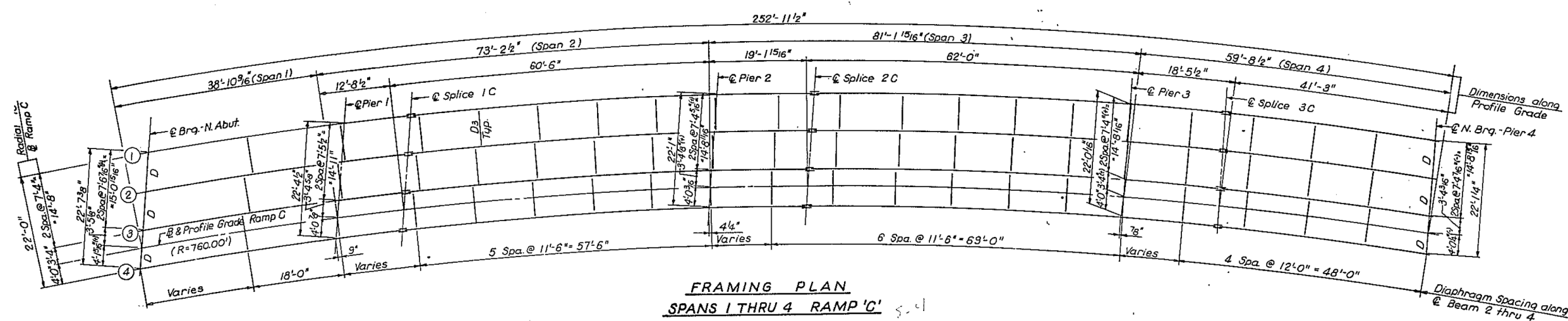
Is and Ss are the moment of inertia and section modulus of the steel section used in computing f_s@.
Ic and Sc are the moment of inertia and section modulus of the composite section used in computing f_s(s₂+t₁).
VR is the maximum t₁+Impact shear range in span, used to determine shear connector spacing.



Notes:
For Stringer between G14 & G15, Cross Frames, Lateral Bracing, Field Splice & Bearing Stiffeners see Sh. 21.
For Shear Connectors & Shop Flange Splice Detail see Sh. 15.
For Bearing Details see Sh. 23.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
STRUCTURAL STEEL
SOUTHBOUND ROADWAY & RAMP C
F.A.I. ROUTE 474 & RAMP B AND C
OVER C.R.I.B.P. R.R., P.A.P.U. R.Y. AND
RELOCATED KICKAPOO CREEK
STATION 390 + 67.76
F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-1
CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HVB-1, 4A-	PEORIA	208	82
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		

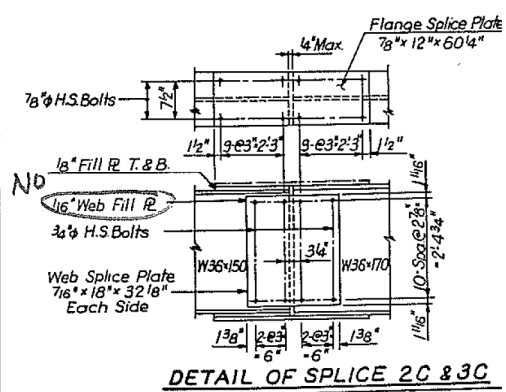


Beam	L	L1	L2	L3	L4	a	a1	a2	a3	a4	a5	a6	a7	a8	a9	a10
1	252'-10 1/2"	38'-10 1/16"	73'-2 1/16"	81'-1 7/8"	59'-8 1/2"	39'-3 9/16"	12'-8 3/8"	60'-5 11/16"	19'-1 15/16"	61'-11 15/16"	18'-5 1/2"	41'-8 1/2"	17'-3 1/4"	19'-6 13/16"	18'-0 15/16"	2'-8 1/2"
2	252'-10 7/8"	38'-10 1/4"	73'-2 1/4"	81'-1 7/8"	59'-8 1/2"	39'-3 3/4"	12'-8 7/16"	60'-5 13/16"	19'-1 15/16"	61'-11 15/16"	18'-5 1/2"	41'-8 1/2"	17'-3 1/4"	19'-6 13/16"	18'-0 15/16"	2'-8 1/2"
3	252'-11 3/8"	38'-10 1/2"	73'-2 7/16"	81'-1 15/16"	59'-8 1/2"	39'-4"	12'-8 1/2"	60'-5 15/16"	19'-1 15/16"	62'-0"	18'-5 1/2"	41'-8 1/2"	17'-3 1/2"	19'-6 15/16"	18'-1"	2'-8 1/2"
4	252'-11 3/4"	38'-10 11/16"	73'-2 9/16"	81'-1 15/16"	59'-8 9/16"	39'-4 3/16"	12'-8 9/16"	60'-6"	19'-1 15/16"	62'-0"	18'-5 1/2"	41'-8 9/16"	17'-3 11/16"	19'-7"	18'-1"	2'-8 9/16"

	1	2	3	4
Brq. N. Abut.	486.45	485.87	485.28	484.69
Pier 1	486.85	486.26	485.67	485.09
Splice 1C	486.99	486.40	485.82	485.23
Pier 2	487.26	486.67	486.08	485.50
Splice 2C	487.35	486.76	486.18	485.59
Pier 3	487.17	486.67	486.08	485.50
Splice 3C	487.12	486.64	486.06	485.47
N. Brq. Pier 4	487.02	486.63	486.05	485.46

	N. Abut.	Pier 1	Pier 2	Pier 3	Pier 4
R@ (k)	16.0	85.7	114.9	110.9	30.0
R _l (k)	37.2	48.5	56.4	54.5	41.3
Imp. (k)	11.2	13.4	13.9	13.9	11.2
R total (k)	64.4	147.6	185.2	179.3	82.5

	.4 Span 1	Pier 1	.5 Span 2	Pier 2	.5 Span 3	Pier 3	.6 Span 4
I _s (in ⁴)	7,920	11,970	9,030	16,478	10,500	14,642	9,030
I _c (in ⁴)	21,363	—	23,664	—	26,444	—	23,664
S _s (in ³)	440	622	504	871	580	786	504
S _c (in ³)	660	—	738	—	836	—	738
Q (k/ft)	.938	.938	.938	.938	.938	.938	.938
f _s @ (k.s.i)	44.4	314.1	190.6	561.4	233.3	516.5	194.7
f _s @ (k.s.i)	1.2	6.1	4.5	7.7	4.8	7.9	4.6
S @ (k/ft)	.478	.478	.478	.478	.478	.478	.478
M ₃ @ (k)	35.4	128.0	137.8	236.9	169.4	211.3	119.9
M ₄ (k)	245.0	250.4	476.0	380.7	551.0	348.5	448.0
M _{imp} (k)	73.5	69.1	120.1	94.2	129.9	89.2	121.3
Total (k)	353.9	447.5	733.9	711.8	850.3	649.0	689.2
f _s (S _s +I _s)/I _s (k.s.i)	6.4	8.6	11.9	9.8	12.2	9.9	11.2
f _s total (k.s.i)	7.6	14.7	16.4	17.5	17.0	17.8	15.8
VR (k)	51.5	—	57.2	—	56.6	—	55.3
M _q (k)	1.61	4.74	1.98	4.92	2.30	3.71	2.07
M _l (k)	4.93	2.68	2.88	2.35	3.14	1.78	2.95
M _{imp} (k)	1.48	.74	.73	.58	.74	.46	.80
f _s total (k.s.i)	5.6	3.6	3.2	2.3	3.0	2.0	3.3



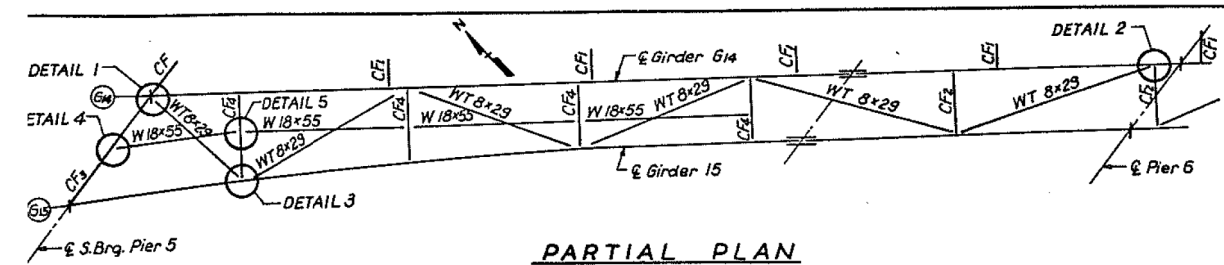
SIGNED BY: D.P.
 DRAWN BY: K.M.
 CHECKED BY: A.T.

FOR INFORMATION ONLY

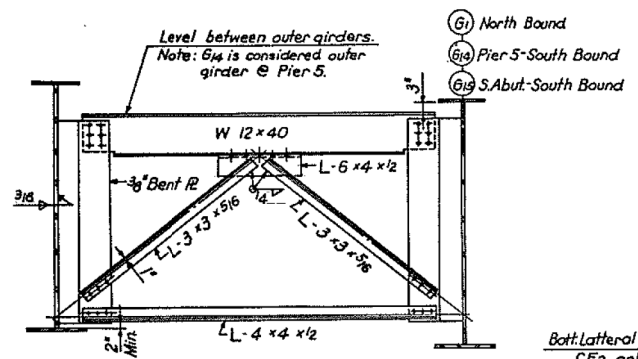
I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_s. I_c and S_c are the moment of inertia and section modulus of the composite section used in computing f_s(S_s+I_s)/I_s. VR is the maximum k+Impact shear range in span, used to determine shear connector spacing.

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
STRUCTURAL STEEL
 (E.B.) RAMP C
 F.A.I. ROUTE 474 & RAMPS B AND C
 OVER C.R.I. & P. R.R., P. & U. RY. AND
 RELOCATED KICKAPOO CREEK
 STATION 390 + 67.76
 F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-1
 CHRISTIAN-ROGE AND ASSOC.
 ENGINEERS
 CHICAGO, ILLINOIS

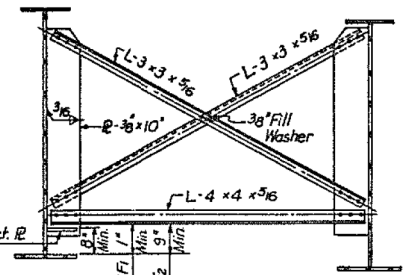
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HVB-1,4A-1	PEORIA	208	83
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		



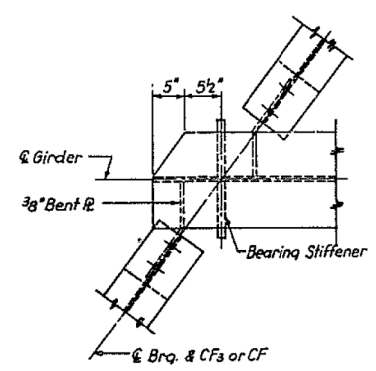
**PARTIAL PLAN
SPAN 6 - S.B. ROADWAY**



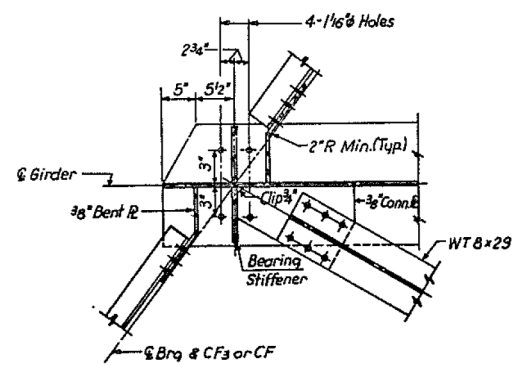
CROSS FRAME CF
No. Req'd. = 25



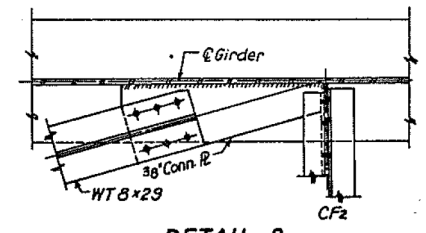
CROSS FRAME CF1 No. Req'd. = 157
CROSS FRAME CF2 No. Req'd. = 67



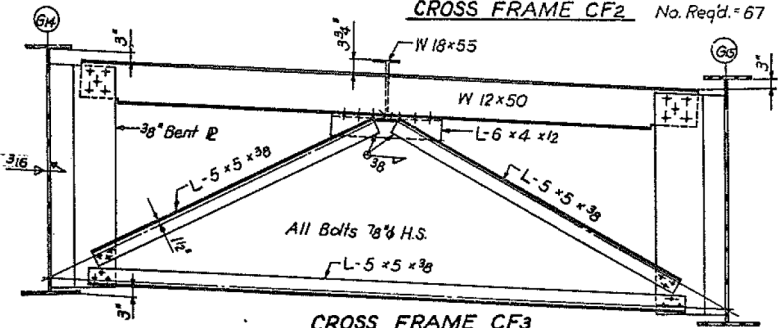
**DETAIL 1
TOP CONNECTION**



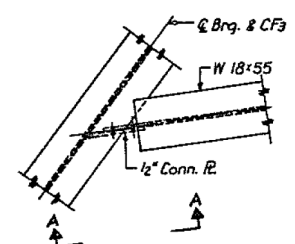
**DETAIL 1
BOTTOM CONNECTION**



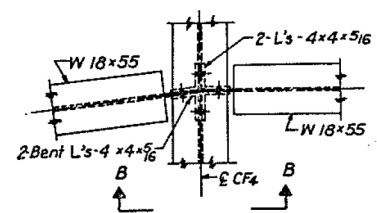
**DETAIL 2
BOTTOM CONNECTION**



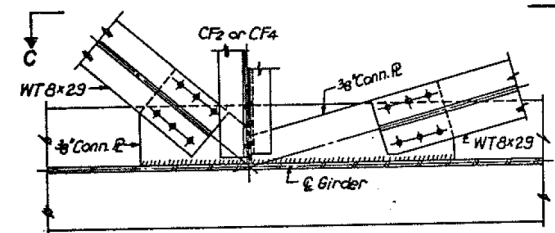
CROSS FRAME CF3
No. Req'd. = 1



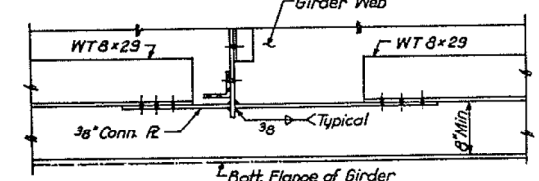
**DETAIL 4
TOP CONNECTION**



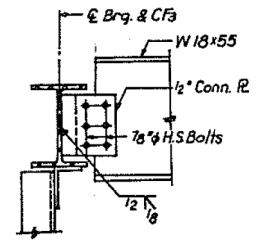
**DETAIL 5
TOP CONNECTION**



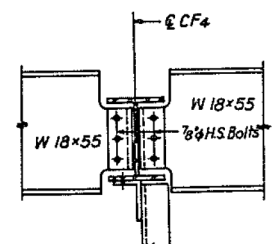
**DETAIL 3
BOTTOM CONNECTION**



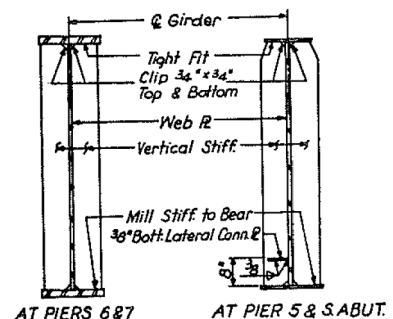
ELEVATION C-C



ELEVATION A-A

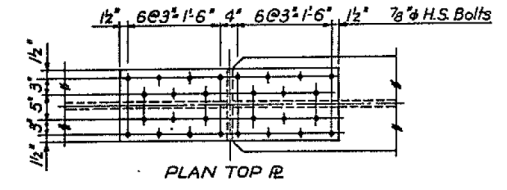


ELEVATION B-B

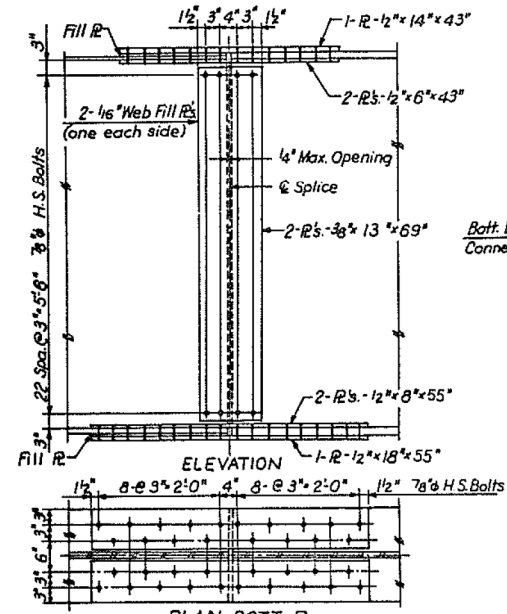


**BEARING STIFFENERS
72" WEB**

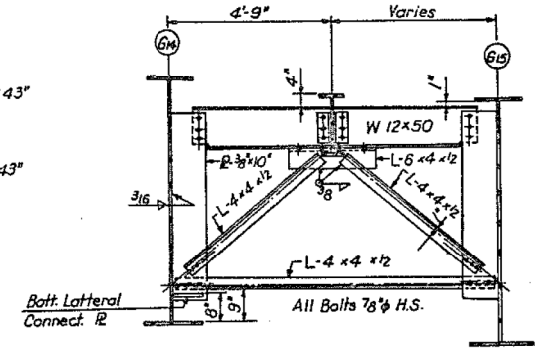
**FOR
INFORMATION
ONLY**



PLAN TOP R



FIELD SPLICE DETAIL
SPLICES: 6N, 7N, 8N, 9N, 4S, 5S, 6S & 7S



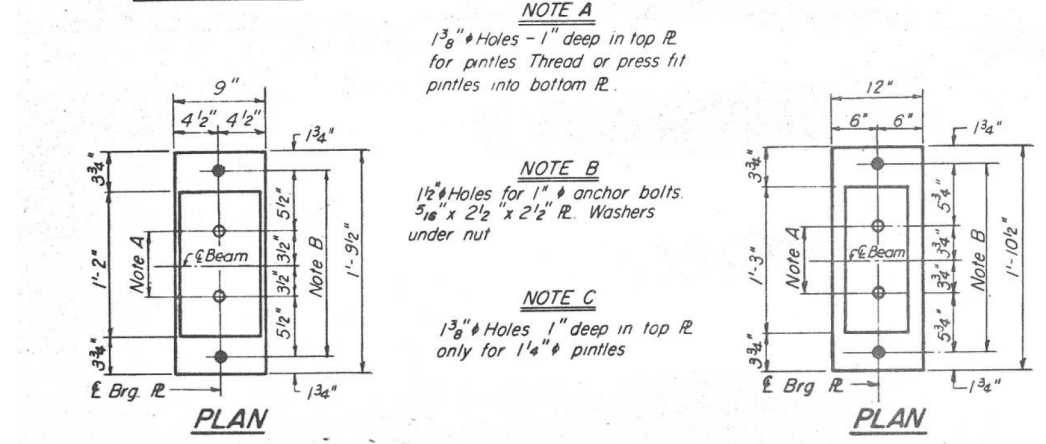
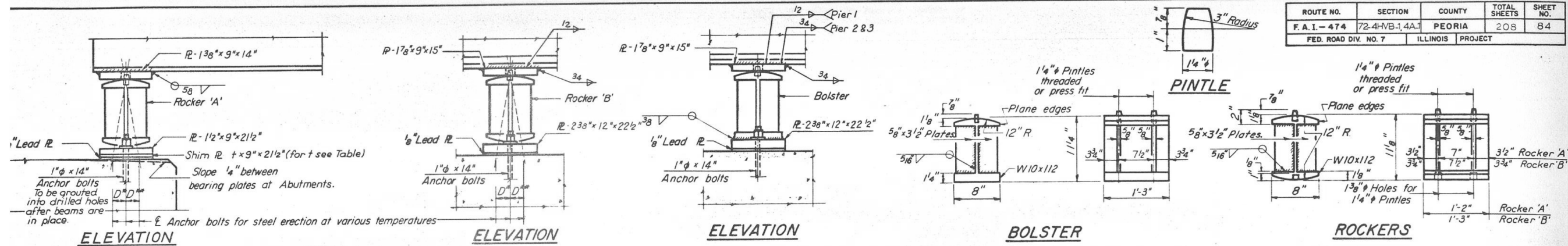
CROSS FRAME CF4
No. Req'd. = 4

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
STRUCTURAL STEEL
F.A.I. ROUTE 474 & RAMPS B AND C
OVER C.R.I. & R.R., P. & U. RY. AND
RELOCATED KICKAPOO CREEK
STATION 390 + 67.76
F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-1
CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

SHEET
21 OF 70

DESIGNED - SMR	EXAMINED - <i>Timothy A. Dredger</i>	DATE - JANUARY 31, 2018	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STRUCTURAL STEEL DETAILS (FOR INFORMATION ONLY) SN 072-0131 (E.B.) & -0132 (W.B.)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
CHECKED - RPN	ENGINEER OF STRUCTURAL SERVICES	REVISIONS			474	72-4(HB, HVB-1, HVB/B-R)	PEORIA	196	142	
DRAWN - Kyle M. Steffen	PASSED - <i>Carl R. Rugeley</i>	REVISIONS			CONTRACT NO. 68887		ILLINOIS FED. AID PROJECT			
CHECKED - SMR RPN	ENGINEER OF BRIDGES AND STRUCTURES	REVISIONS			SHEET NO. 47 OF 49 SHEETS					

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-474	72-4-NB-1,4A	PEORIA	208	84
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		



AT N. ABUTMENTS,
S. BRGS. PIER 2 N.B. & N. BRG. PIER 4

AT PIER 1 & 3
S.B. AND RAMP C

AT PIER 1 & 3 N.B.
AND PIER 2 S.B. & RAMP C

NOTE A
1 3/8" Holes - 1" deep in top R for pintles Thread or press fit pintles into bottom R.

NOTE B
1 1/2" Holes for 1" anchor bolts. 5/16 x 2 1/2 x 2 1/2" R. Washers under nut

NOTE C
1 3/8" Holes 1" deep in top R only for 1 1/4" pintles

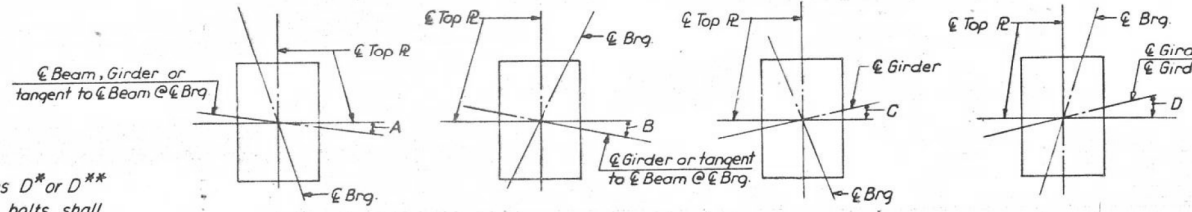
BEARING ASSEMBLY DETAILS FOR BEAMS

NOTES ON SETTING OF ANCHOR BOLTS AT EXP. BRGS.

D* (Side of brg away from fixed brg)
D* = 1/8" per each 100' of expansion for every 15° fall below the normal temp. of 50°F

D** (Side of brg toward fixed brg)
D** = 1/8" per each 100' of expansion for every 15° rise above the normal temp. of 50°F

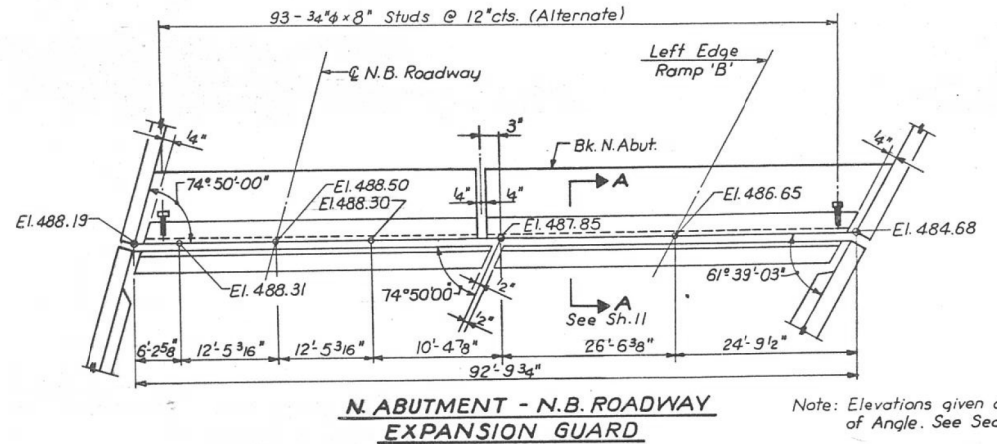
After beams have been erected and dimensions D* or D** determined, holes shall be drilled and anchor bolts shall be grouted in place. All fixed anchor bolts may be built into the masonry.



DETAIL OF TOP BEARING R TO BEAM OR GIRDER CONNECTION

SHIM PLATE THICKNESS (t)

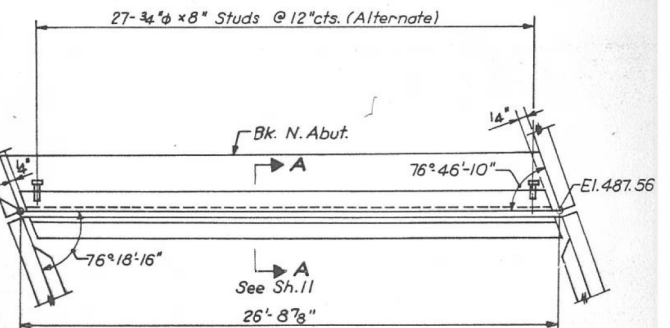
Beam No.	1	2	3	4	5	6	7	8	9	10	11	12	13	19
Pier 2 N.B. Rdwy.	S. Brg.	-	1/4"	-	1/16"	-	-	1/8"	1/8"	-	-	-	-	-
Pier 4 S.B. Rdwy.	N. Brg.	-	-	-	-	-	-	-	-	1/4"	-	-	1/4"	-



N. ABUTMENT - N.B. ROADWAY EXPANSION GUARD

Note: Elevations given at Top of Angle. See Section A-A Sh. 11.

FOR INFORMATION ONLY



N. ABUTMENT - RAMP C EXPANSION GUARD

Note: Elevations given at Top of Angle. See Section A-A Sh. 11.

N.B. SPANS 1-2, Angle A

Beam	Brig. N. Abut.	Pier 1	N. Brg. Pier 2
1	12°35'25"	8°08'02"	3°01'34"
2	5°41'44"	1°33'04"	
3		3°50'18"	
4		1°56'21"	
5-13		0°00'00"	

N.B. SPANS 3-4

Beam	Angle A
1	3°01'34"
2	1°31'11"
3-10	0°00'00"

N.B. SPANS 5-5A

Girder	Angle A or B
1	3°01'37"
2	1°33'04"
3-8	0°00'00"

N.B. SPANS 6-8

Girder	Angle B
1 & 2	1°50'36"
3	1°43'26"
4	0°36'17"
5-7	0°00'00"

S.B. SPANS 1-4

Beam	Angle A
14-19	0°00'00"

S.B. SPANS 6-8

Girder	Angle D
8-11	0°00'00"
12	0°23'10"
13	0°46'06"
14 & 15*	1°08'46"

S.B. SPAN 5

Girder	Angle C or D
9-16	0°00'00"
17	6°23'43"

*At S Brg. - Pier 5 Angle D = 7°50'26" for 8-15 only.

RAMP C - SPANS 1-4

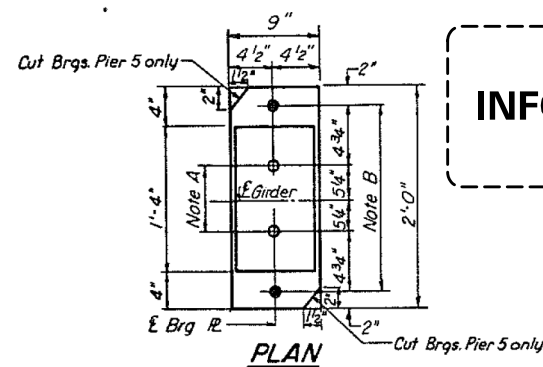
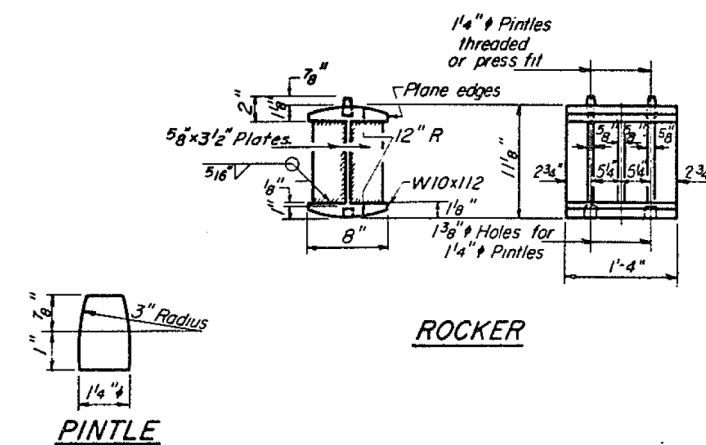
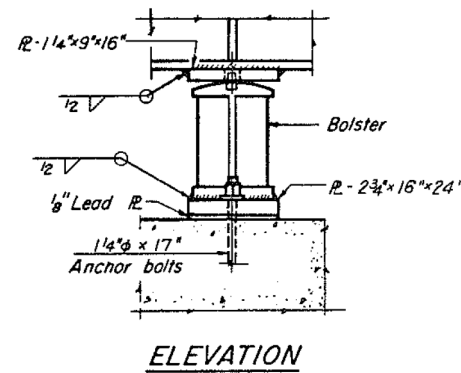
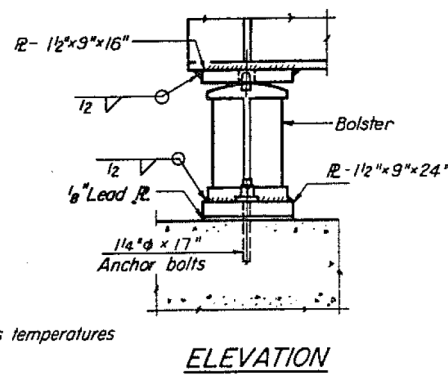
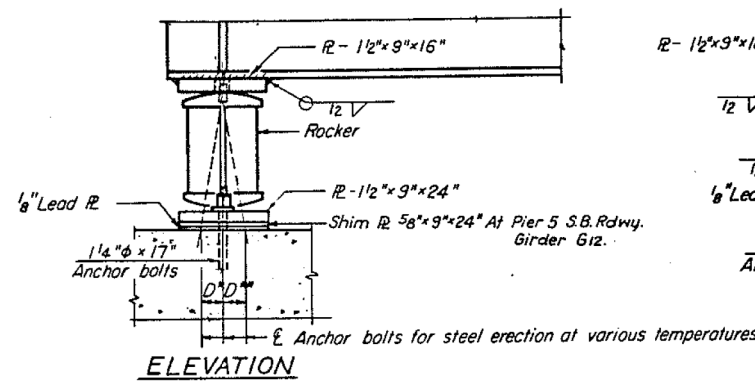
Beam	Brig. - N. Abut.	Pier 1	Pier 2	Pier 3	N. Brg. - Pier 4					
	Angle D	Angle B	Angle D	Angle B	Angle A	Angle B	Angle A	Angle B		
1		0°11'44"		0°09'07"		0°04'19"		0°0'54"		0°04'46"
2		0°04'04"		0°03'09"		0°01'29"		0°0'19"		0°01'29"
3	0°03'46"		0°02'56"		0°01'24"		0°0'17"		0°01'31"	
4	0°11'44"		0°09'08"		0°04'20"		0°0'54"		0°04'45"	

STATE OF ILLINOIS
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STRUCTURAL STEEL
F.A.I. ROUTE 474 & RAMPS B AND C
OVER C.R.I. & P.R.U. RY. AND
RELOCATED KICKAPOO CREEK
STATION 390 + 67.76
F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4-NB-1
CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

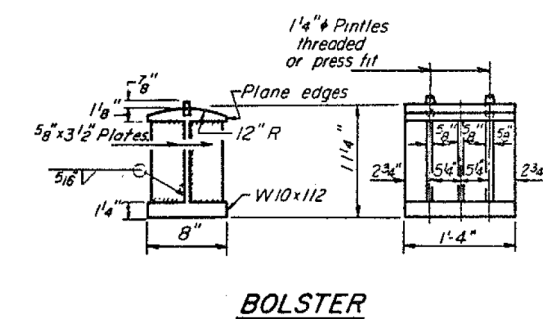
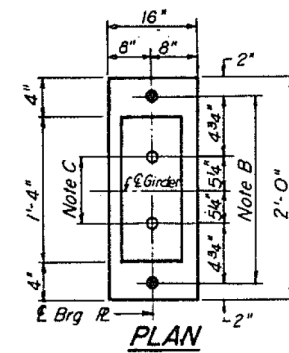
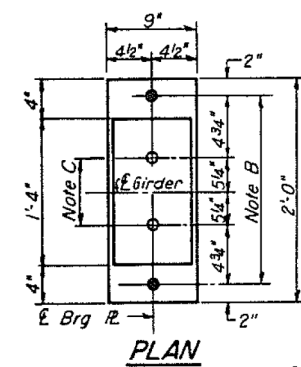
SHEET NO. 22 OF 70

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. - 474	72-4HVB-1,4A-	PEORIA	208	85
FED. ROAD DIV. NO. 7	ILLINOIS	PROJECT		

Note: All Steel for Bearing Details at Pier 4A shall conform to A.S.T.M. Specification Designation A-588.



FOR INFORMATION ONLY



AT S.BRG. PIER 4 N.B.
AND N.BRG. PIER 5 N.B. & S.B.

NOTE A
1 3/4" Holes - 1" deep in top R
for pintles Thread or press fit
pintles into bottom R

NOTE B
1 3/4" Holes for 1 1/4" anchor bolts
5/16" x 2 3/4" x 2 3/4" R Washers
under nut

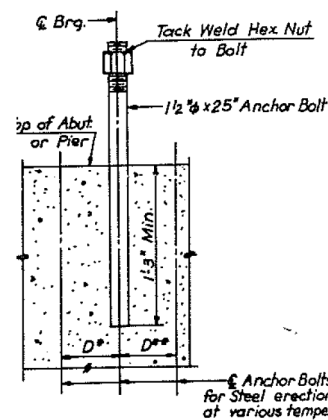
NOTE C
1 3/8" Holes 1" deep in top R
only for 1 1/4" pintles

For Notes on setting of Anchor Bolts & Detail of Top Bearing R
to Girder Connection see Sh. 22.

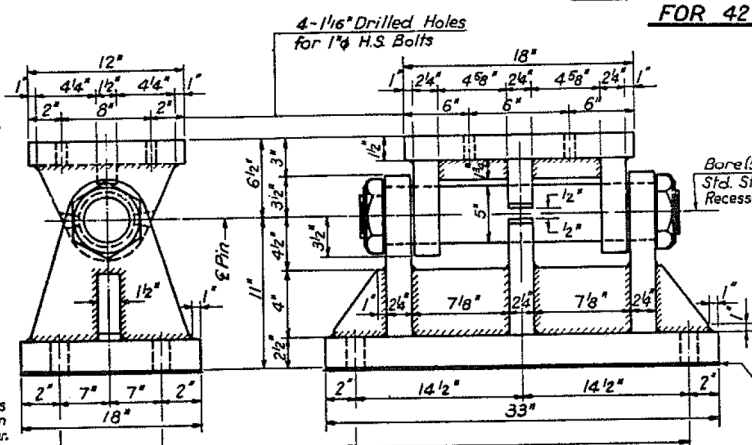
**BEARING ASSEMBLY DETAILS
FOR 42" PLATE GIRDERS**

SHIM PLATE THICKNESS (t)

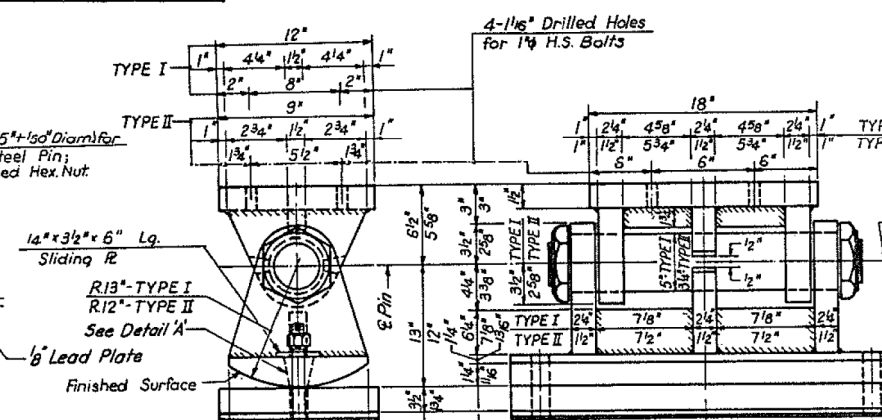
	Girder No.	G5	G11
Pier 5 N.B.Rdwy.	S.Brg.	5/8"	
S.Abutment N.B. Rdwy.	S.Brg.	5/8"	
Pier 5 S.B.Rdwy.	S.Brg.		5/8"



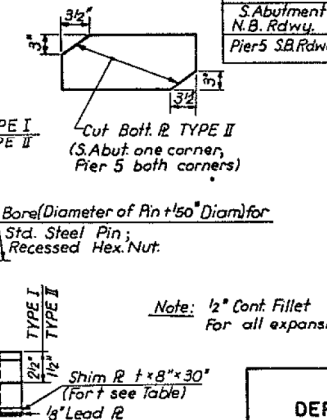
ANCHOR BOLT DETAIL



DETAIL OF FIXED BEARING
TYPE III - PIER 7 (No. Req'd = 15)



DETAIL OF EXPANSION BEARING
TYPE I - PIER 6 (No. Req'd = 15)
TYPE II - PIER 5 S.BRG. & S.ABUT. (No. Req'd = 30)



DETAIL A
PLAN OF TAPERED HOLE

Note: 1/2" Cont. Fillet welds throughout
for all expansion & fixed bearings.

DESIGNED BY: D.P.
DRAWN BY: K.M.
CHECKED BY: A.T.

EXAMINED: *Timothy A. Doolittle*
ENGINEER OF STRUCTURAL SERVICES
PASSED: *Carl R. Roper*
ENGINEER OF BRIDGES AND STRUCTURES

DATE: JANUARY 31, 2018
REVISED: _____
REVISED: _____

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURAL STEEL DETAILS (FOR INFORMATION ONLY)
SN 072-0131 (E.B.) & -0132 (W.B.)

SHEET NO. 49 OF 49 SHEETS

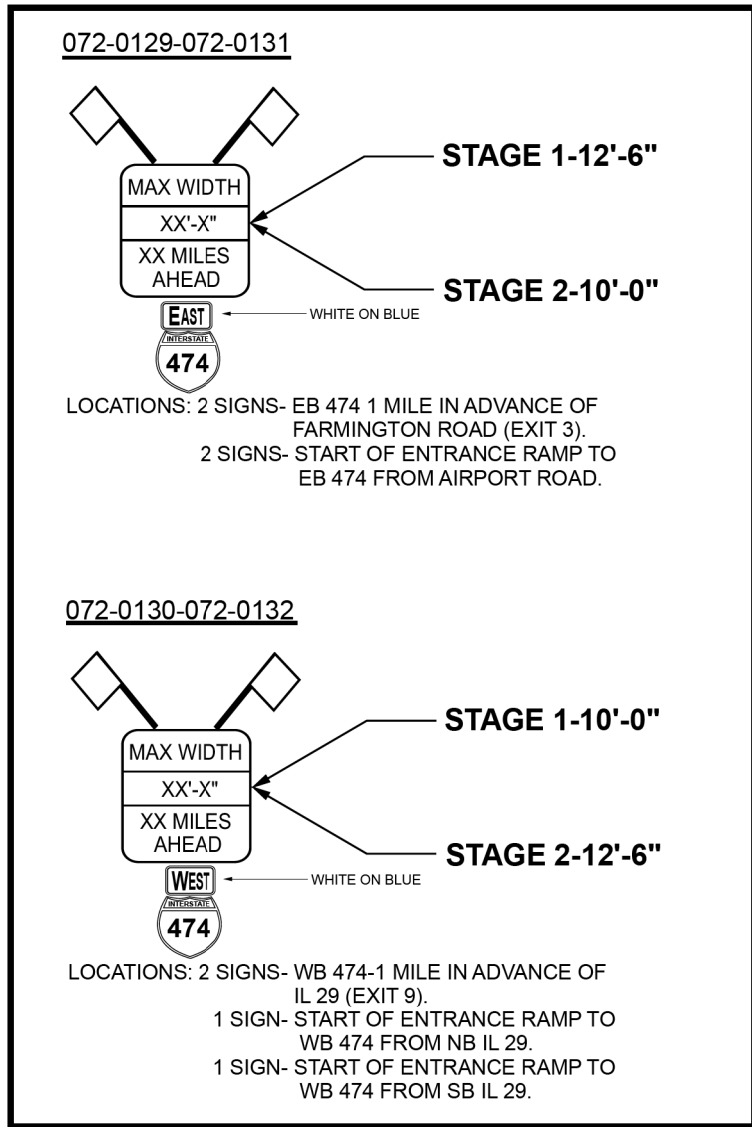
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
STRUCTURAL STEEL
F.A.I. ROUTE 474 & RAMPS B AND C
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F.A.I. RT. 474 PEORIA COUNTY SECTION 72-4HVB-1
CHRISTIAN-ROGE AND ASSOC.
ENGINEERS
CHICAGO, ILLINOIS

SHEET
23 OF 70

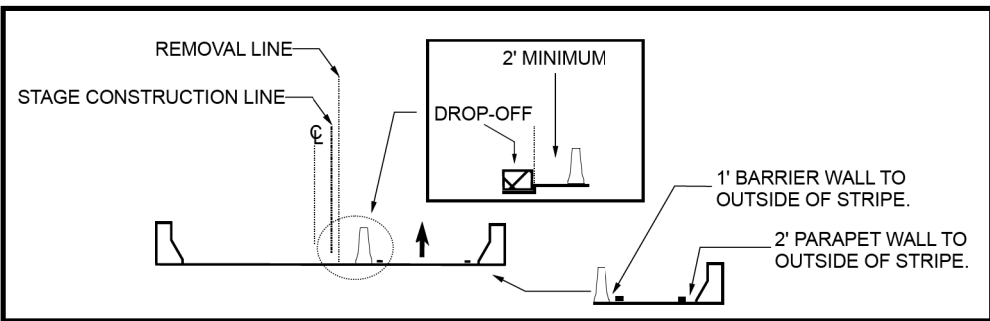
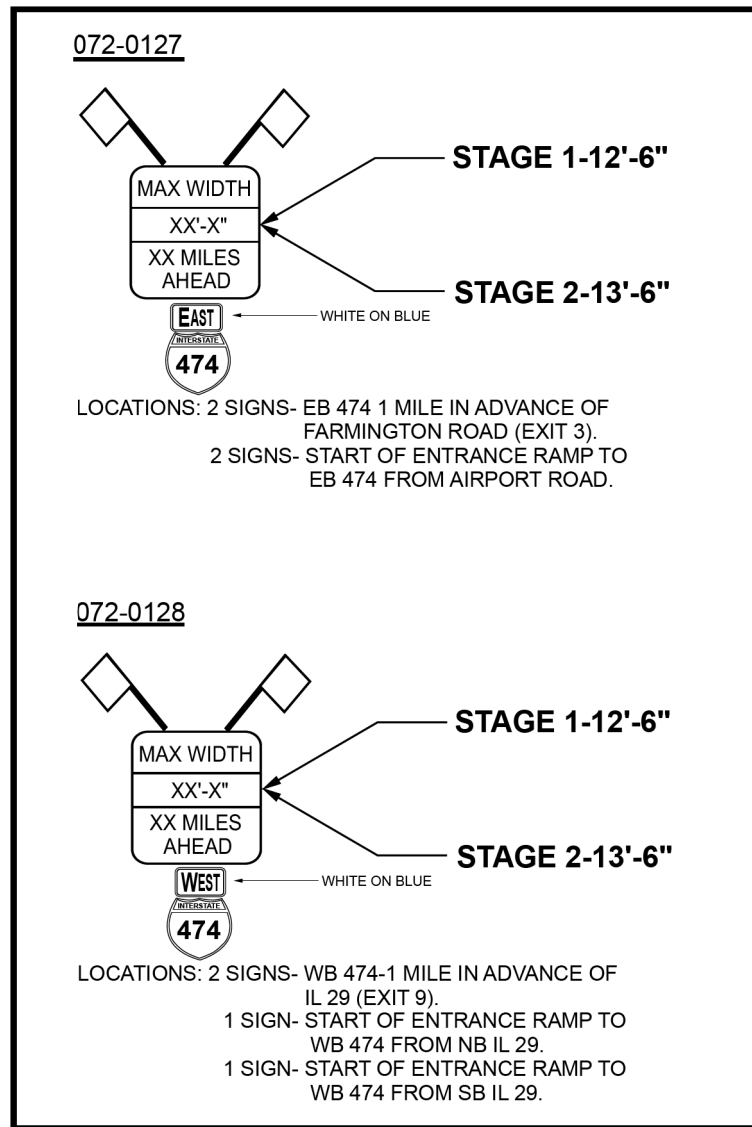
DESIGNED - SMR
CHECKED - RPN
DRAWN - Kyle M. Steffen
CHECKED - SMR RPN

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB, HVB-1, HVB/B-R)	PEORIA	196	144
				CONTRACT NO. 68887
				ILLINOIS FED. AID PROJECT

MAX WIDTH SIGNING



MAX WIDTH SIGNING



**TEMPORARY CONCRETE BARRIER
TYPICAL
--ALL STAGES--**

AS SHOWN ABOVE, TEMPORARY CONCRETE BARRIER SHALL ALWAYS BE PLACED WITH A 2' BUFFER BETWEEN ANY DROP-OFF. DROP-OFF CONDITIONS MAY BE THE CONSTRUCTION STAGE LINE OR THE REMOVAL LINE AS SHOWN ON THE PLAN TYPICALS AND/OR REMOVAL DETAILS.

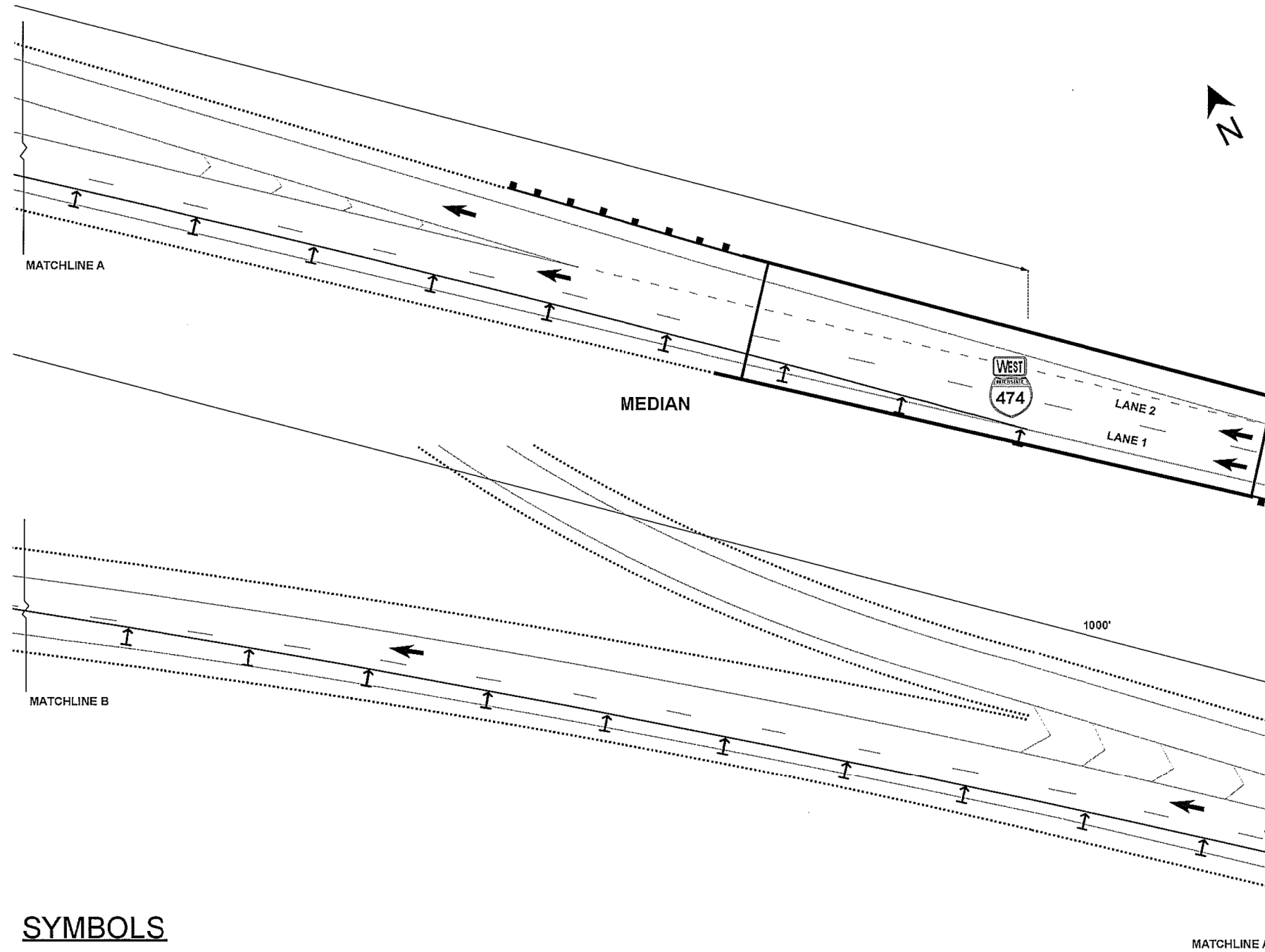
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	DRAWN -	REVISED -
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0127 & 072-0128			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	145
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



072-0127-WESTBOUND STAGE 1

THE FOLLOWING DETAILS REPRESENT A SUGGESTED STAGING OF TRAFFIC CONTROL:

WORKERS	EXPECT
AHEAD	TRAFFIC
4 MILES	DELAYS

NOTE: NOT PART OF STD. 701400.

1 PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) LOCATED 4 MILES IN ADVANCE OF US 24 PCMS TO BE USED FOR THE FOLLOWING:

PRE-STAGE 1 AND STAGE 1 INSTALLATION/REMOVAL CHANGING STAGE 1 TO STAGE 2, AND REMOVAL OF STAGE 2.

PCMS IS ONLY REQUIRED AT THIS LOCATION DURING THE ABOVE CONDITIONS. DURING NON-WORKING HOURS, WHEN NEEDED FOR THE NEXT DAY, THE PCMS SHALL HAVE THE MESSAGE READ: RIGHT/LEFT LANE CLOSED 4 MILES AHEAD.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN WORKERS ARE IN THE CLOSED LANE AND NOT BEHIND TEMPORARY CONCRETE BARRIER.

PRE-STAGE 1

1) UTILIZING STANDARDS 701400, 701401, & 701428, CLOSE LANE 2 OF WESTBOUND 474. REMOVE THE CONFLICTING PAVEMENT MARKINGS AND INSTALL NEW PAVEMENT MARKINGS FOR STAGE 1 TRAFFIC, WHERE ABLE TO IN LANE 2.

STAGE 1

1) UTILIZE STANDARDS 701400, 701402, & 701428, AND AS SHOWN TO CLOSE LANE 1 OF WESTBOUND 474 FOR STAGE 1 REMOVAL. STANDARD 701428 SHALL REMAIN IN USE FOR THE PLACEMENT OF THE APPROACH EDGE-LINE TAPER TO PROTECT THE WORKERS.

PLACE ALL DEVICES, REMAINING PAVEMENT MARKINGS FROM PRE-STAGE 1, AND AS SHOWN ON PLAN DETAILS AND HIGHWAY STANDARDS.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN PLACING EDGE-LINES, DEVICES, AND INSTALLING TEMPORARY CONCRETE BARRIER (TCB).

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

CONTINUED ON NEXT SHEET

SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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PLOT DATE = 12/13/2017	CHECKED -	REVISED -
	DATE -	REVISED -

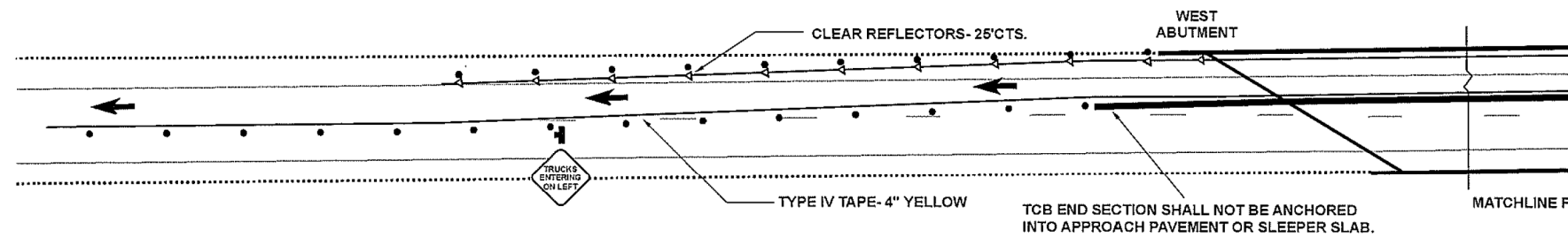
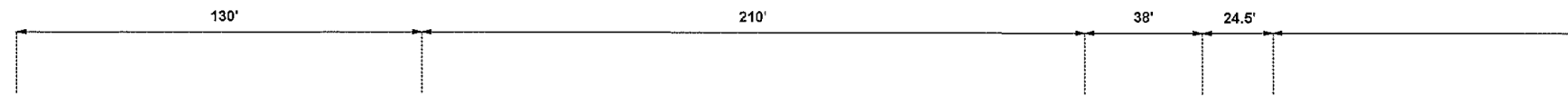
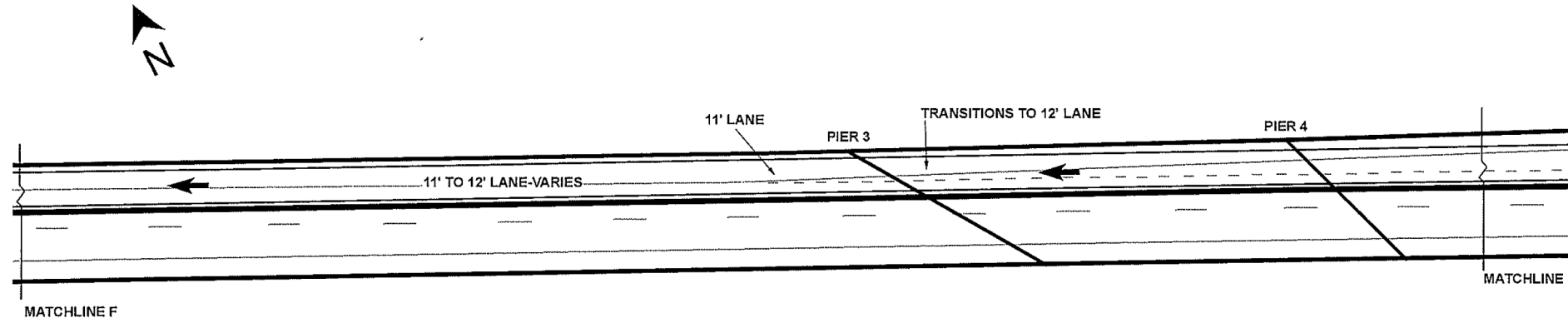
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0127 & 072-0128	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	146
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

+/- 755'

072-0127-WESTBOUND STAGE



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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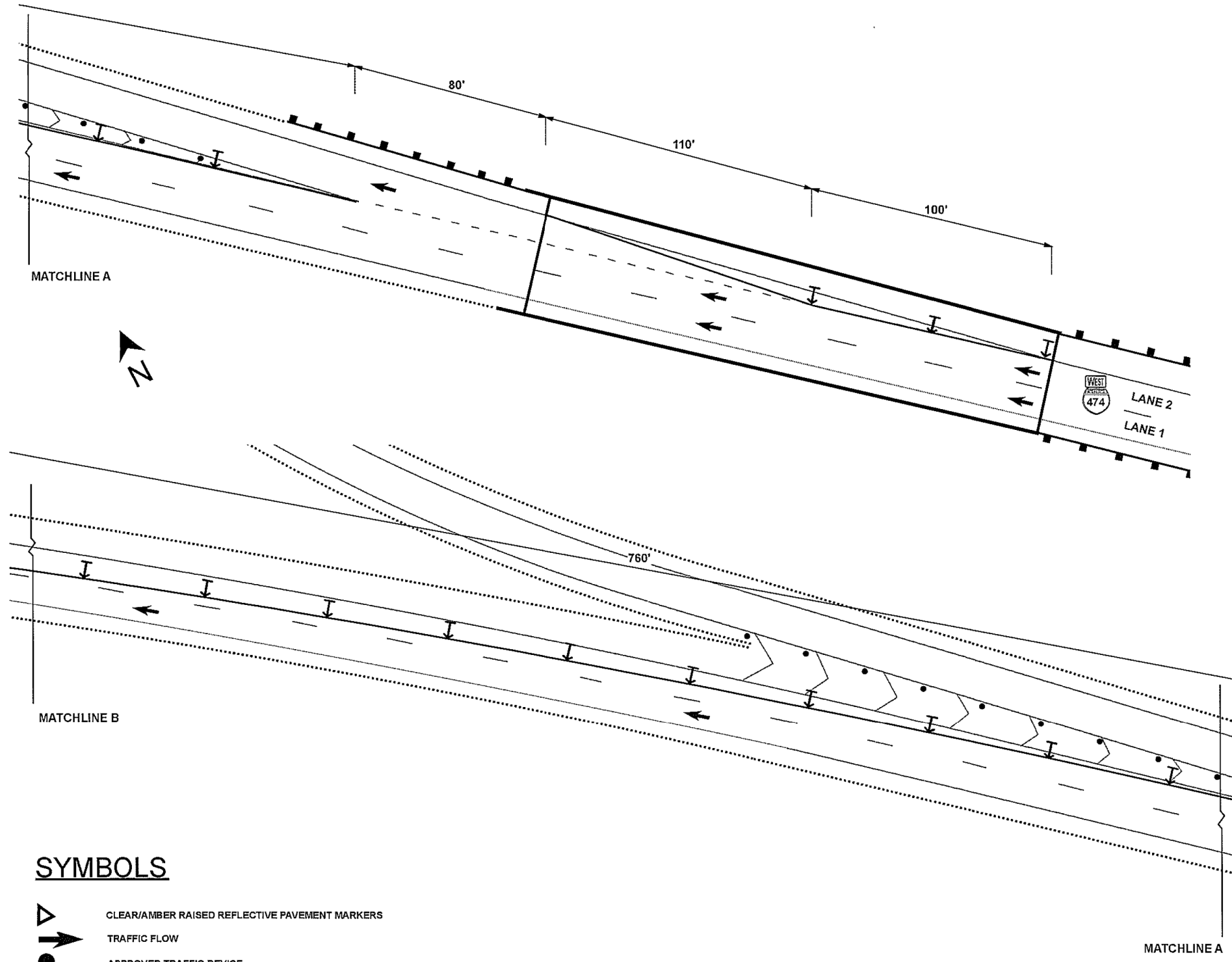
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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	148
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68887	



072-0127- WESTBOUND STAGE

PRE-STAGE 2

WHILE IN STAGE 1, COMPLETE THE FOLLOWING:

- 1) RELOCATE TEMPORARY CONCRETE BARRIER WALL TO STAGE 2 LOCATION. FOR THE APPROACH TCB TAPER, PLACE IN A LINEAR POSITION (TCB TAPER TO BE PLACED AFTER TRAFFIC IS IN STAGE 2-LANE 1 WHERE TCB WILL CONFLICT WITH STAGE 2 TRAFFIC.
- 2) REMOVE ANY CONFLICTING LANE 1 MARKINGS, PLACE NEW PAVEMENT MARKINGS, INSTALL CLEAR TCB REFLECTORS, AND ANCHOR EXIT TCB END SECTION.








STAGE 2

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

- 1) COMPLETE APPROACH TCB TAPER, ATTENUATORS, AND ANCHORING.
- 2) COMPLETE REMAINING PAVEMENT MARKING, SIGNS AND ETC.

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" - WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

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USER NAME = lababidism	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**








STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

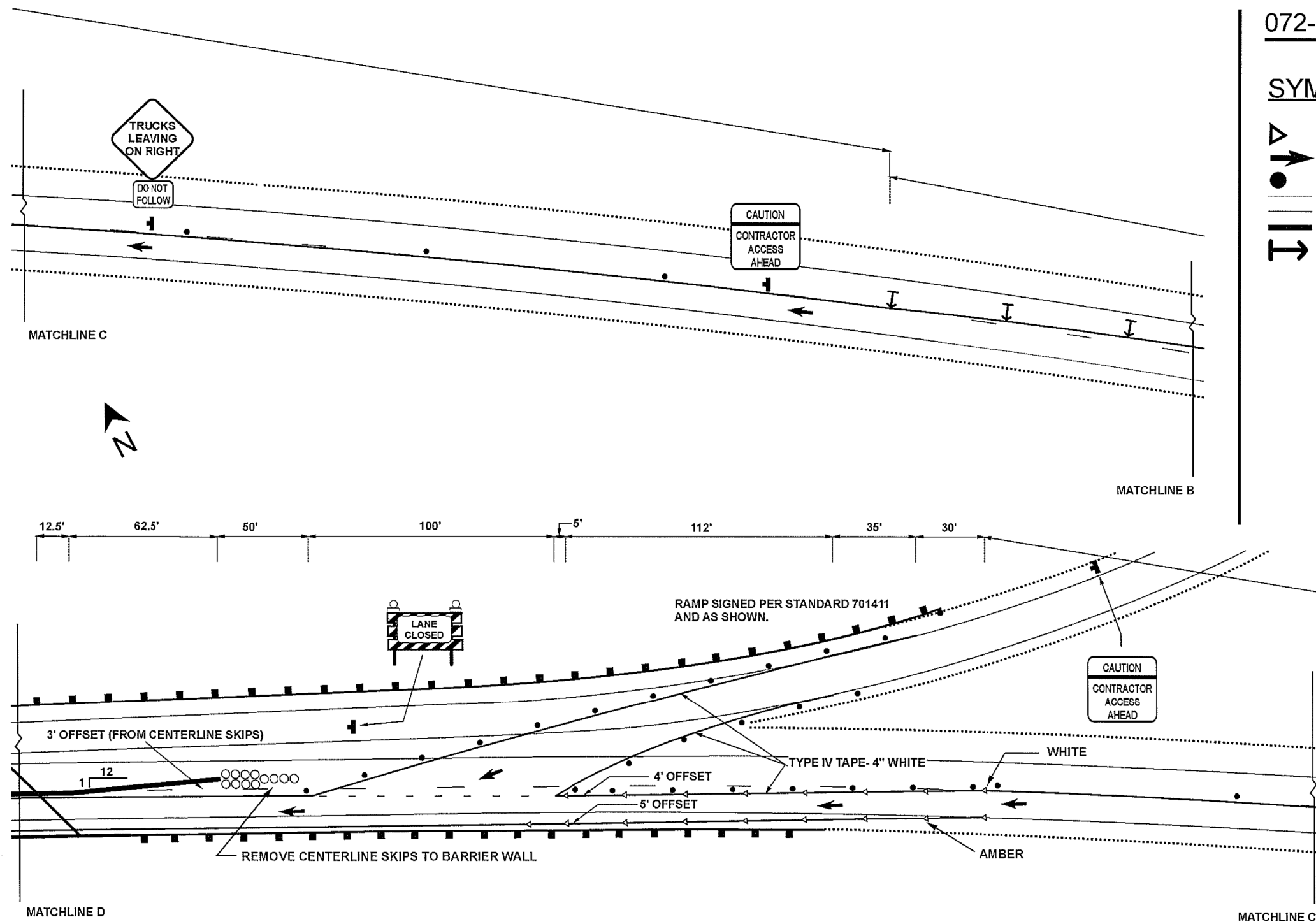
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	149
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0127- WESTBOUND STAGE

SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4"- WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

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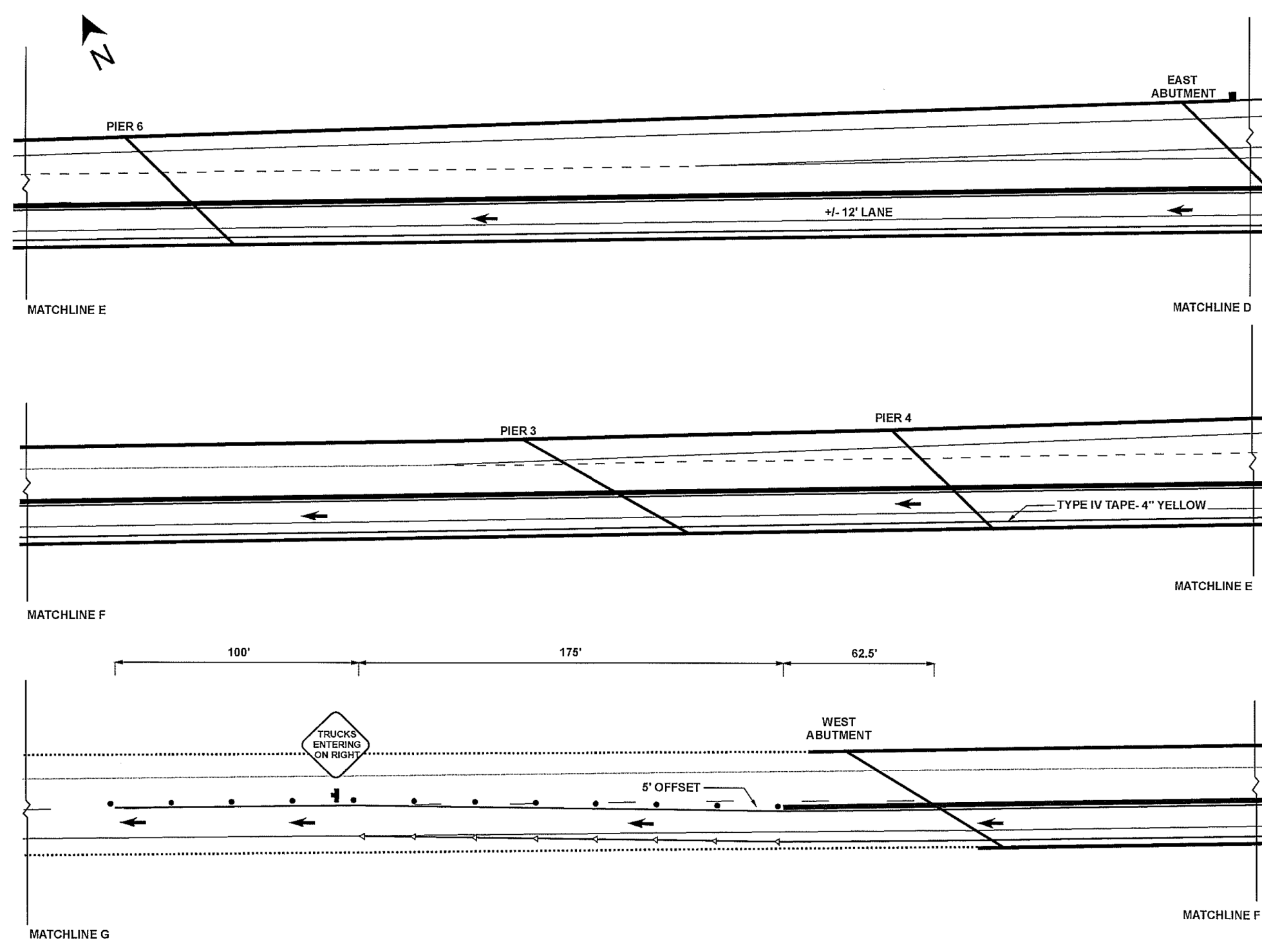
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	150
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68887	

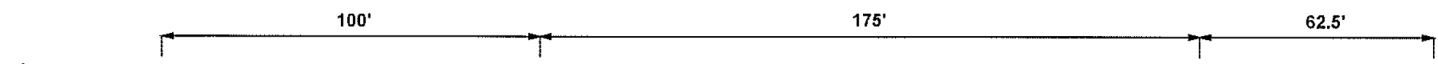


072-0127- WESTBOUND STAGE

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS ACTUAL FIELD CONDITIONS WILL PREVAIL.

SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4\"/>



5' OFFSET

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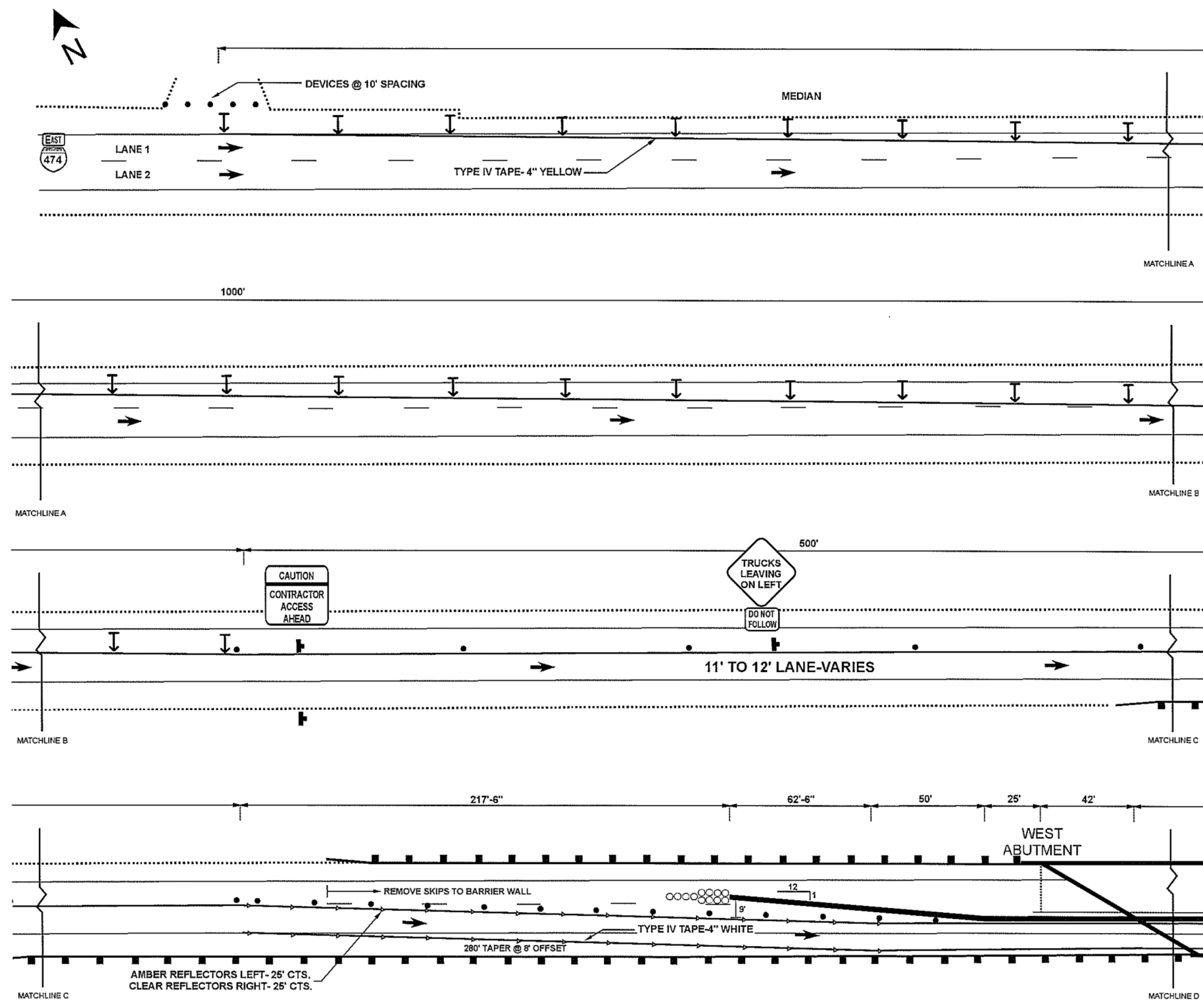
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	151
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



072-0128-EASTBOUND STAGE 1

THE FOLLOWING DETAILS REPRESENT A SUGGESTED STAGING OF TRAFFIC CONTROL:

WORKERS	EXPECT	NOTE: NOT PART OF STD. 701400.
AHEAD	TRAFFIC	
4 MILES	DELAYS	

1 PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) LOCATED 4 MILES IN ADVANCE OF AIRPORT ROAD. PCMS TO BE USED FOR THE FOLLOWING:

PRE-STAGE 1 AND STAGE 1 INSTALLATION/REMOVAL. CHANGING STAGE 1 TO STAGE 2, AND REMOVAL OF STAGE 2.

PCMS IS ONLY REQUIRED AT THIS LOCATION DURING THE ABOVE CONDITIONS. DURING NON-WORKING HOURS, WHEN NEEDED FOR THE NEXT DAY, THE PCMS SHALL HAVE THE MESSAGE READ: RIGHT/LEFT LANE CLOSED 4 MILES AHEAD.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN WORKERS ARE IN THE CLOSED LANE AND NOT BEHIND TEMPORARY CONCRETE BARRIER.

PRE-STAGE 1

- UTILIZING STANDARDS 701400, 701401, & 701428, CLOSE LANE 2 OF EASTBOUND 474. REMOVE THE CONFLICTING PAVEMENT MARKINGS AND INSTALL NEW PAVEMENT MARKINGS FOR STAGE 1 TRAFFIC, WHERE ABLE TO IN LANE 2.

STAGE 1

- UTILIZE STANDARDS 701400, 701402, & 701428, AND AS SHOWN TO CLOSE LANE 1 OF EASTBOUND 474 FOR STAGE 1 REMOVAL. STANDARD 701428 SHALL REMAIN IN USE FOR THE PLACEMENT OF THE APPROACH EDGE-LINE TAPER TO PROTECT THE WORKERS.

PLACE ALL DEVICES, REMAINING PAVEMENT MARKINGS FROM PRE-STAGE 1, AND AS SHOWN ON PLAN DETAILS AND HIGHWAY STANDARDS.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN PLACING EDGE-LINES, DEVICES, AND INSTALLING TEMPORARY CONCRETE BARRIER (TCB).

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

CONTINUED ON NEXT SHEET

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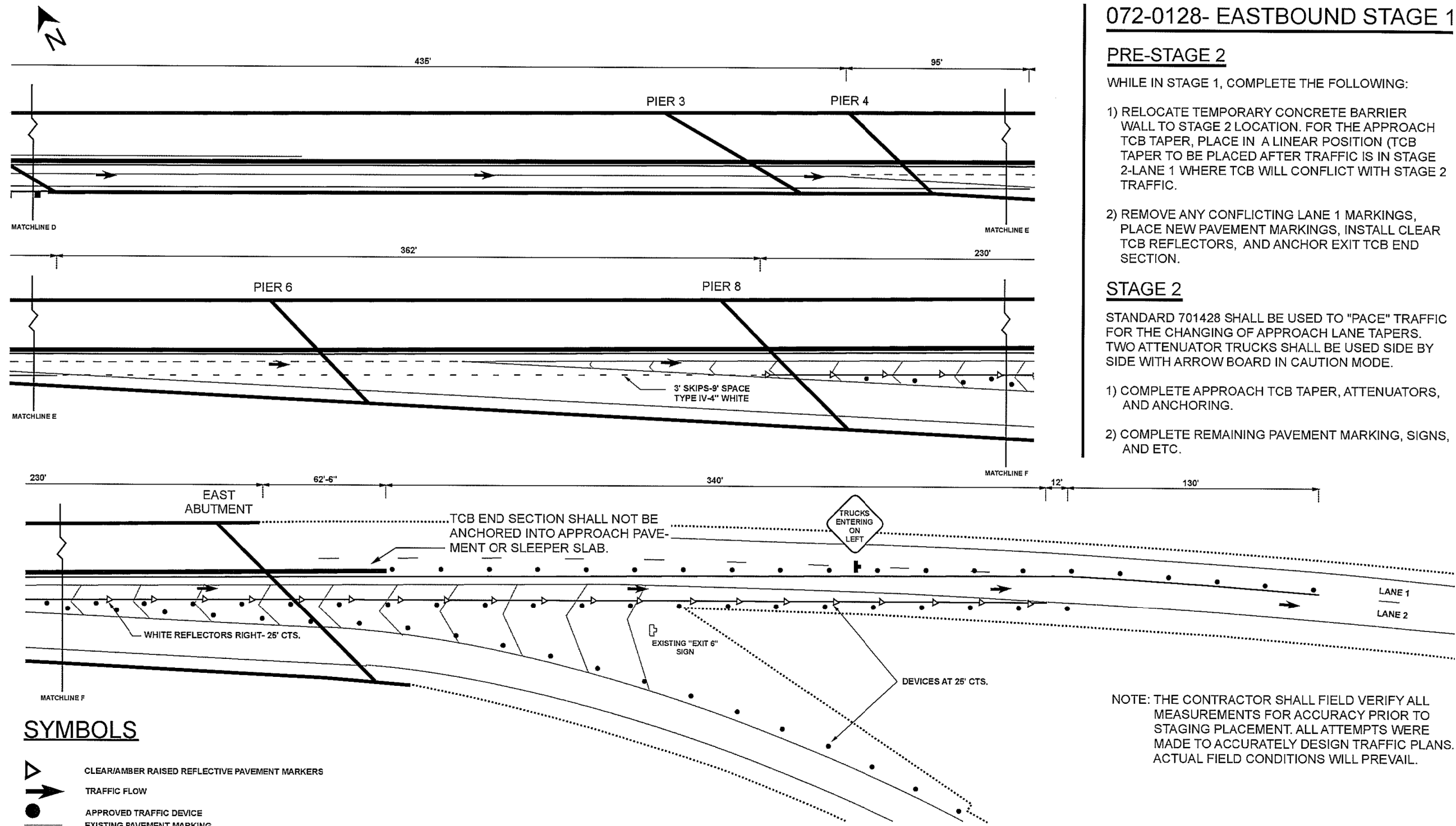
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	152
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



072-0128- EASTBOUND STAGE 1

PRE-STAGE 2

WHILE IN STAGE 1, COMPLETE THE FOLLOWING:

- 1) RELOCATE TEMPORARY CONCRETE BARRIER WALL TO STAGE 2 LOCATION. FOR THE APPROACH TCB TAPER, PLACE IN A LINEAR POSITION (TCB TAPER TO BE PLACED AFTER TRAFFIC IS IN STAGE 2-LANE 1 WHERE TCB WILL CONFLICT WITH STAGE 2 TRAFFIC.
- 2) REMOVE ANY CONFLICTING LANE 1 MARKINGS, PLACE NEW PAVEMENT MARKINGS, INSTALL CLEAR TCB REFLECTORS, AND ANCHOR EXIT TCB END SECTION.

STAGE 2

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

- 1) COMPLETE APPROACH TCB TAPER, ATTENUATORS, AND ANCHORING.
- 2) COMPLETE REMAINING PAVEMENT MARKING, SIGNS, AND ETC.

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

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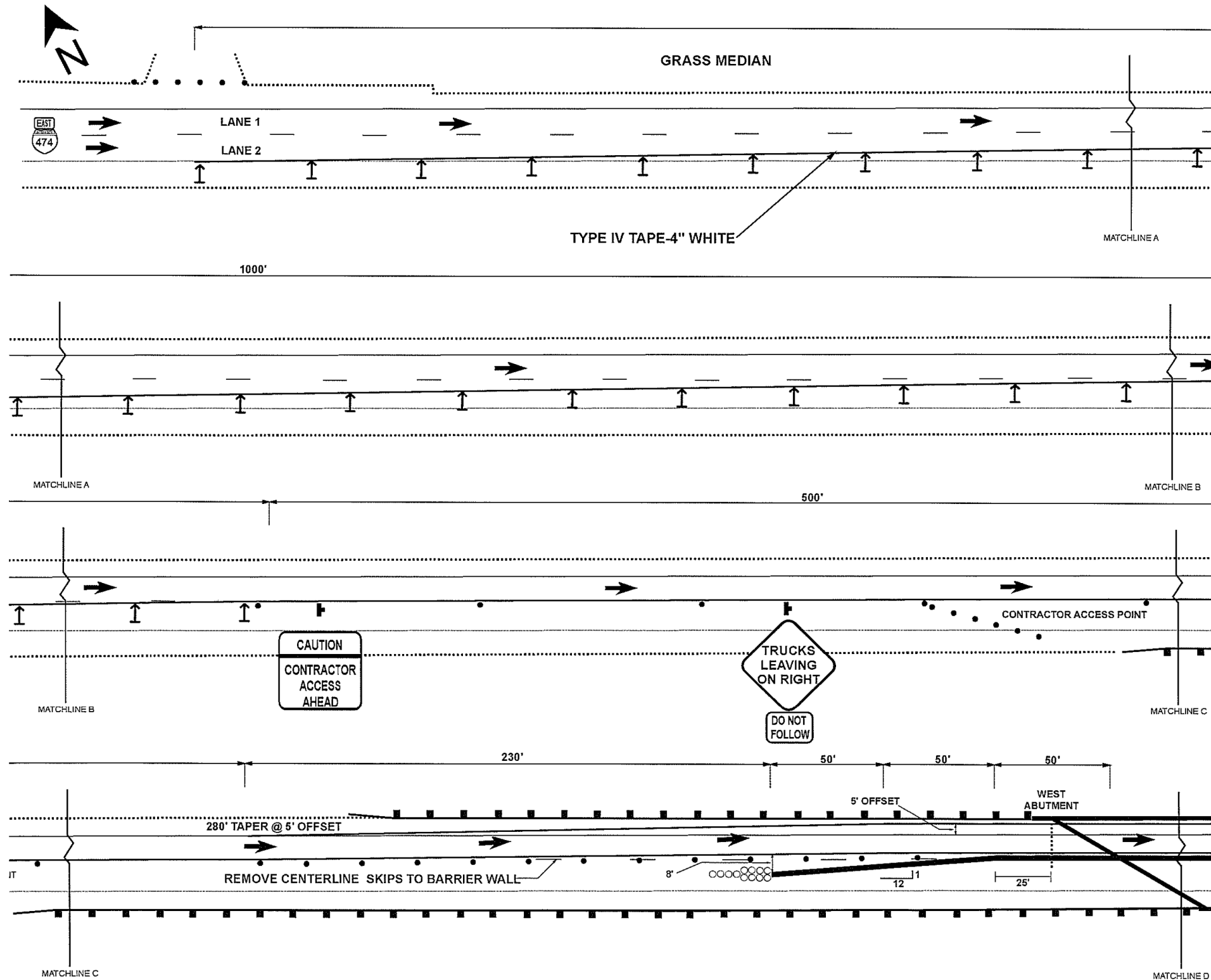
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	153
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68887	



072-0128- EASTBOUND STAGE 2

WORKERS	EXPECT	NOTE: NOT PART OF STD. 701400.
AHEAD	TRAFFIC	
4 MILES	DELAYS	

1 PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) LOCATED 4 MILES IN ADVANCE OF AIRPORT ROAD. PCMS TO BE USED FOR THE FOLLOWING:

CHANGING STAGE 1 TO STAGE 2, AND REMOVAL OF STAGE 2.

PCMS IS ONLY REQUIRED AT THIS LOCATION DURING THE ABOVE CONDITIONS. DURING NON-WORKING HOURS, WHEN NEEDED FOR THE NEXT DAY, THE PCMS SHALL HAVE THE MESSAGE READ: RIGHT/LEFT LANE CLOSED 4 MILES AHEAD.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN WORKERS ARE IN THE CLOSED LANE AND NOT BEHIND TEMPORARY CONCRETE BARRIER AS SHOWN PER STANDARD.

PRE-STAGE 2

WHILE IN STAGE 1, COMPLETE THE FOLLOWING:

1) RELOCATE TEMPORARY CONCRETE BARRIER WALL TO STAGE 2 LOCATION. FOR THE APPROACH TCB TAPER, PLACE IN A LINEAR POSITION (TCB TAPER TO BE PLACED AFTER TRAFFIC IS IN STAGE 2-LANE 1 WHERE TCB WILL CONFLICT WITH STAGE 2 TRAFFIC.

2) REMOVE ANY CONFLICTING LANE 1 MARKINGS, PLACE NEW PAVEMENT MARKINGS, INSTALL TCB REFLECTORS, AND ANCHOR EXIT TCB END SECTION.

STAGE 2

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

1) COMPLETE APPROACH TCB TAPER, ATTENUATORS, AND ANCHORING.
2) COMPLETE REMAINING PAVEMENT MARKING, SIGNS, AND ETC.

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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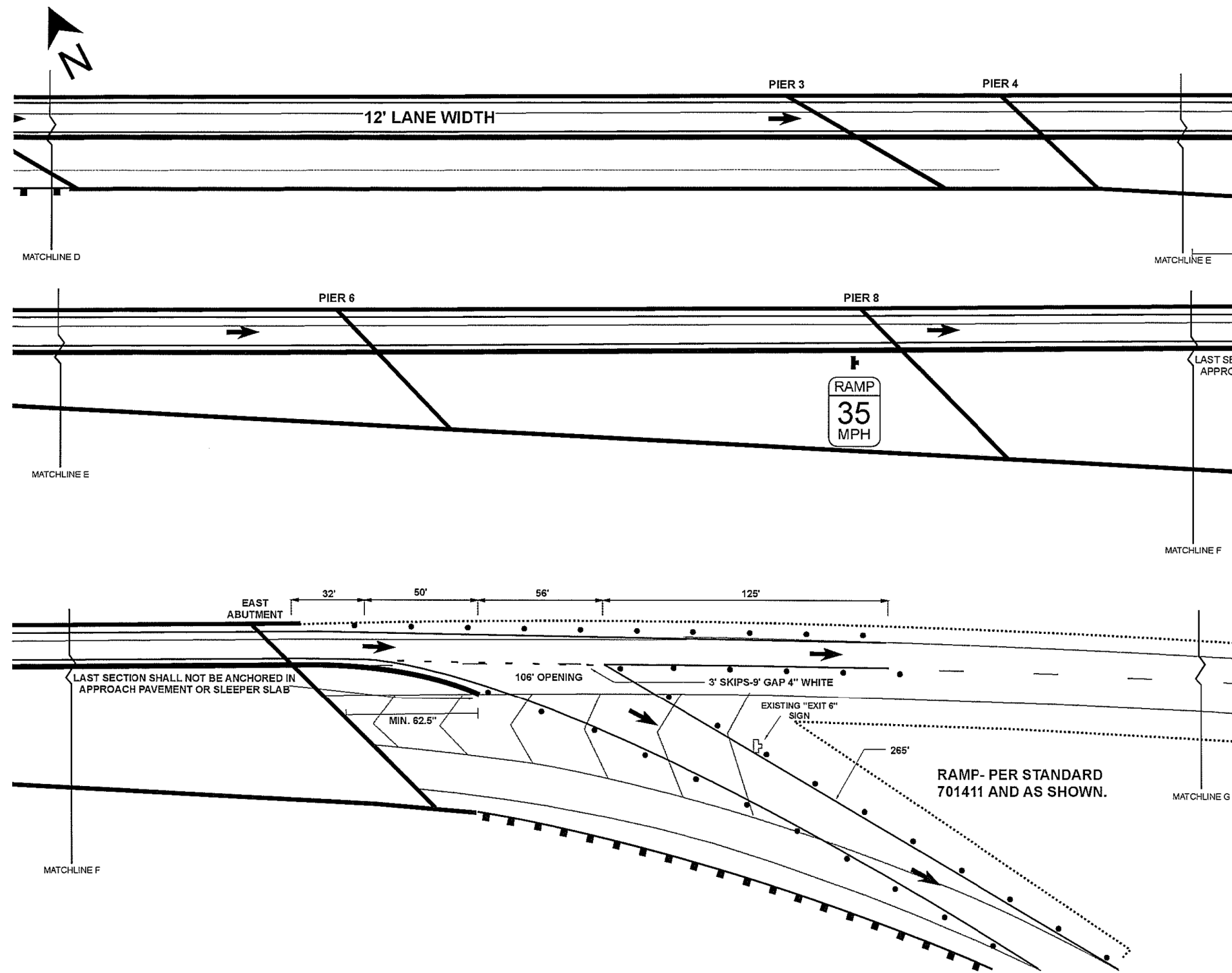
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	154
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



072-0128- EASTBOUND STAGE

POST STAGE 2

WHILE IN STAGE 2, COMPLETE THE FOLLOWING:

- 1) REMOVE TEMPORARY CONCRETE BARRIER WALL, REMOVE TEMPORARY PAVEMENT MARKINGS INSTALL APPLICABLE PERMANENT PAVEMENT MARKINGS IN THE CLOSED LANE, AND REMOVE REMAINING TRAFFIC CONTROL.
- 2) UTILIZING STANDARD 701426, CLOSE LANE 2 AND COMPLETE THE INSTALLATION OF PERMANENT PAVEMENT MARKINGS.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN WORKERS ARE IN THE CLOSED LANE AND NOT BEHIND TEMPORARY CONCRETE BARRIER AS SHOWN PER STANDARD.



BLACK ON YELLOW

BLACK ON ORANGE



BLACK ON ORANGE
36 X 48

STAGE 2 ONLY- THE SPEED LIMIT SHALL BE 45 MPH.

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLAN. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0127 & 072-0128

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	155
CONTRACT NO. 68887			ILLINOIS FED. AID PROJECT	

072-0129/0131 EASTBOUND STAGE 1

THE FOLLOWING DETAILS REPRESENT A SUGGESTED STAGING OF TRAFFIC CONTROL:

WORKERS	EXPECT
AHEAD	TRAFFIC
4 MILES	DELAYS

NOTE: NOT PART OF STD. 701400.

1 PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) LOCATED 4 MILES IN ADVANCE OF AIRPORT ROAD.
PCMS TO BE USED FOR THE FOLLOWING:

PRE-STAGE 1 AND STAGE 1 INSTALLATION AND REMOVAL. CHANGING STAGE 1 TO STAGE 2, AND REMOVAL OF STAGE 2.

PCMS IS ONLY REQUIRED AT THIS LOCATION DURING THE ABOVE CONDITIONS. DURING NON-WORKING HOURS, WHEN NEEDED FOR THE NEXT DAY, THE PCMS SHALL HAVE THE MESSAGE READ: RIGHT/LEFT LANE CLOSED 4 MILES AHEAD.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN WORKERS ARE IN THE CLOSED LANE AND NOT BEHIND TEMPORARY CONCRETE BARRIER.

PRE-STAGE 1

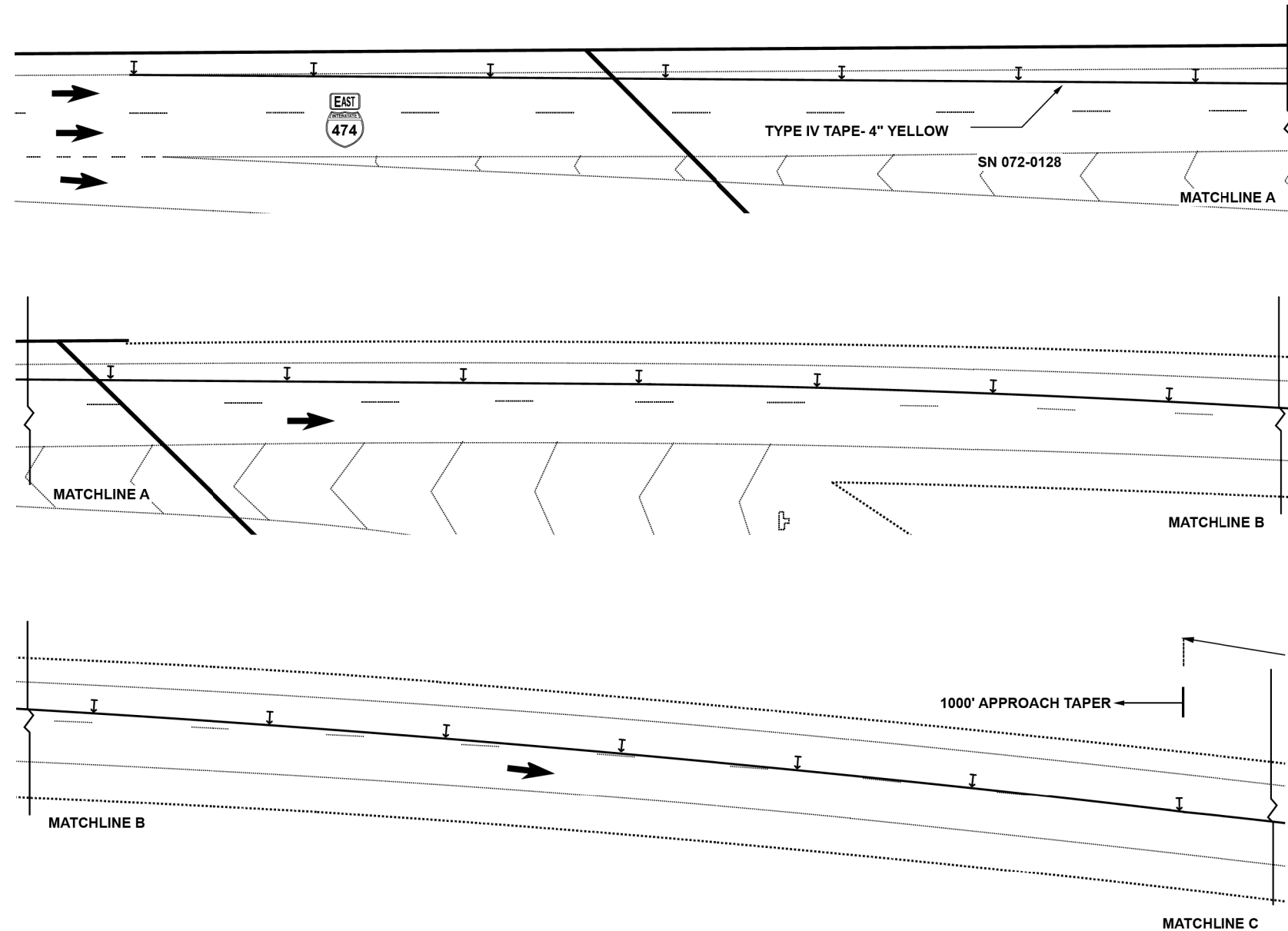
- UTILIZING STANDARDS 701400, 701401, & 701428, CLOSE LANE 2 OF EASTBOUND 474. REMOVE THE CONFLICTING PAVEMENT MARKINGS AND INSTALL NEW PAVEMENT MARKINGS FOR STAGE 1 TRAFFIC, WHERE ABLE TO IN LANE 2.

STAGE 1

- UTILIZE STANDARDS 701400, 701402, & 701428, AND AS SHOWN TO CLOSE LANE 1 OF EASTBOUND 474 FOR STAGE 1 REMOVAL. STANDARD 701428 SHALL REMAIN IN USE FOR THE PLACEMENT OF THE APPROACH EDGE-LINE TAPER TO PROTECT THE WORKERS.

PLACE ALL DEVICES, REMAINING PAVEMENT MARKINGS FROM PRE-STAGE 1, AND AS SHOWN ON PLAN DETAILS AND HIGHWAY STANDARDS.

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SYMBOLS

	CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
	TRAFFIC FLOW
	APPROVED TRAFFIC DEVICE
	EXISTING PAVEMENT MARKING
	TYPE IV TAPE-4"- WHITE/YELLOW
	TEMPORARY CONCRETE BARRIER
	DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0129 & 072-0131

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	156
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0129/0131 EASTBOUND STAGE 1

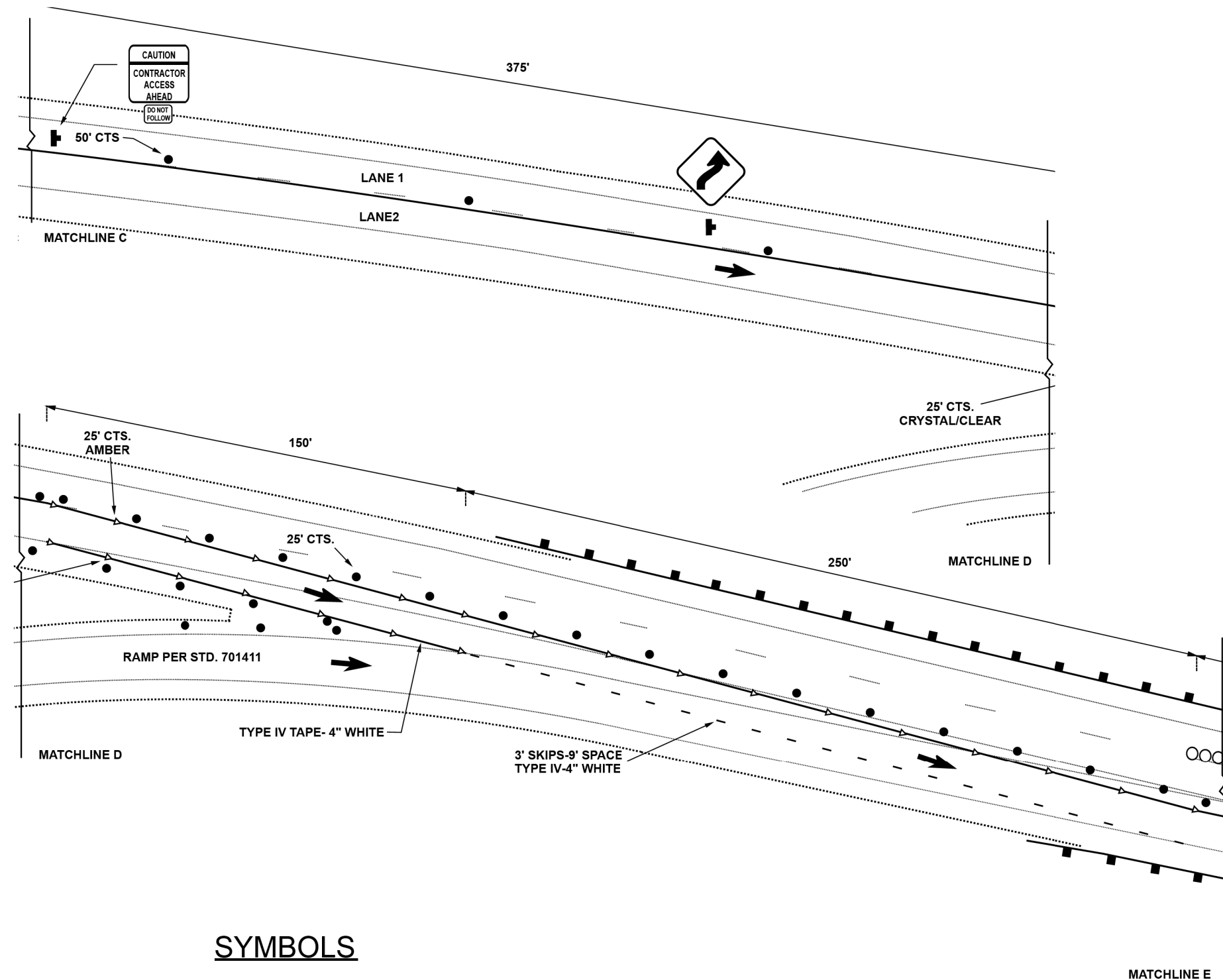
A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN PLACING EDGE-LINES, DEVICES, AND INSTALLING TEMPORARY CONCRETE BARRIER (TCB).

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

PRE-STAGE 2

WHILE IN STAGE 1, COMPLETE THE FOLLOWING:

- 1) RELOCATE TEMPORARY CONCRETE BARRIER WALL TO STAGE 2 LOCATION. FOR THE APPROACH TCB TAPER, PLACE IN A LINEAR POSITION (TCB TAPER TO BE PLACED AFTER TRAFFIC IS IN STAGE 2-LANE 1 WHERE TCB WILL CONFLICT WITH STAGE 2 TRAFFIC.
- 2) REMOVE ANY CONFLICTING LANE 1 MARKINGS, PLACE NEW PAVEMENT MARKINGS, INSTALL CLEAR TCB REFLECTORS, AND ANCHOR EXIT TCB END SECTION.



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4"- WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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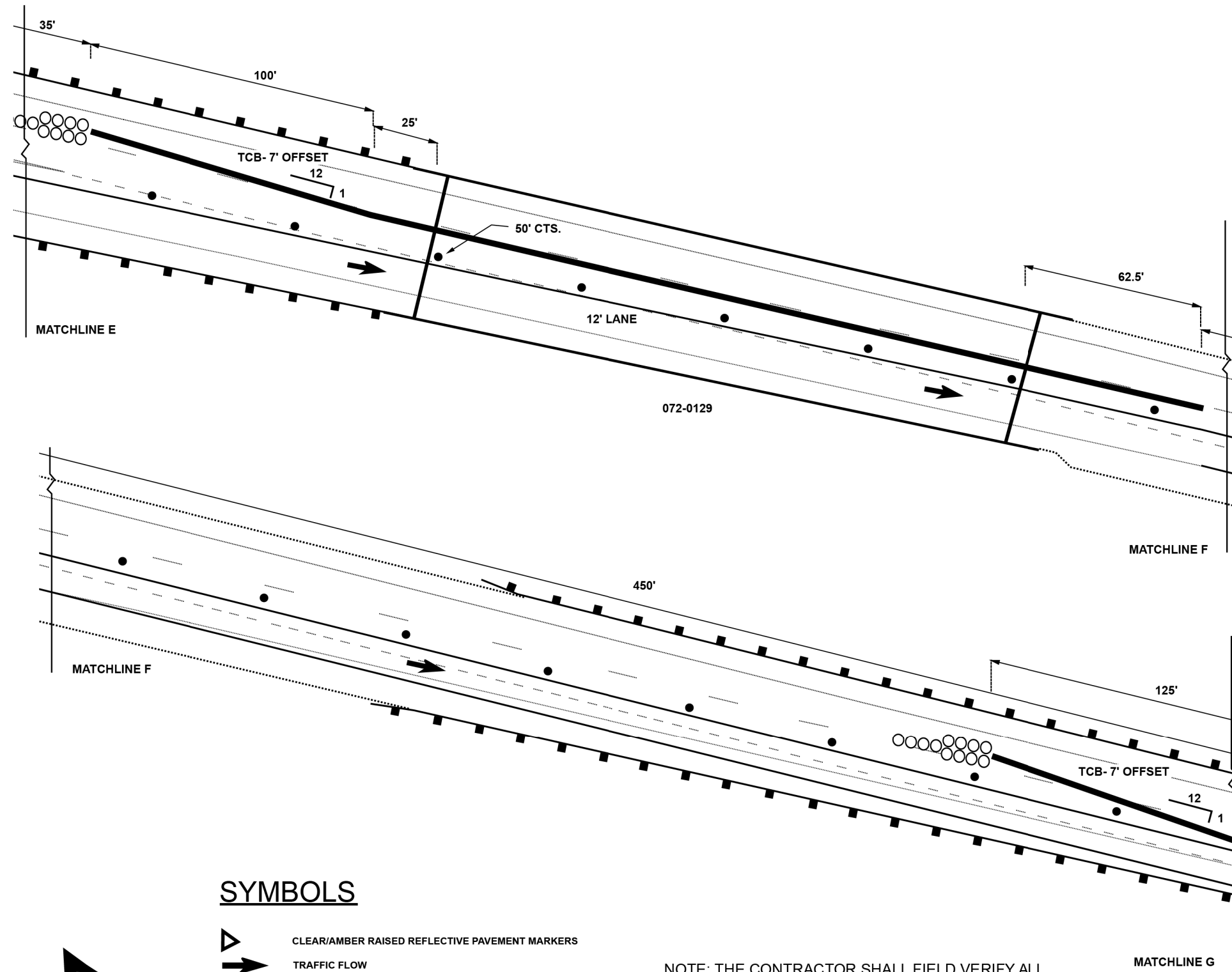
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0129 & 072-0131








SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	157
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0129/0131 EASTBOUND
STAGE 1



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" - WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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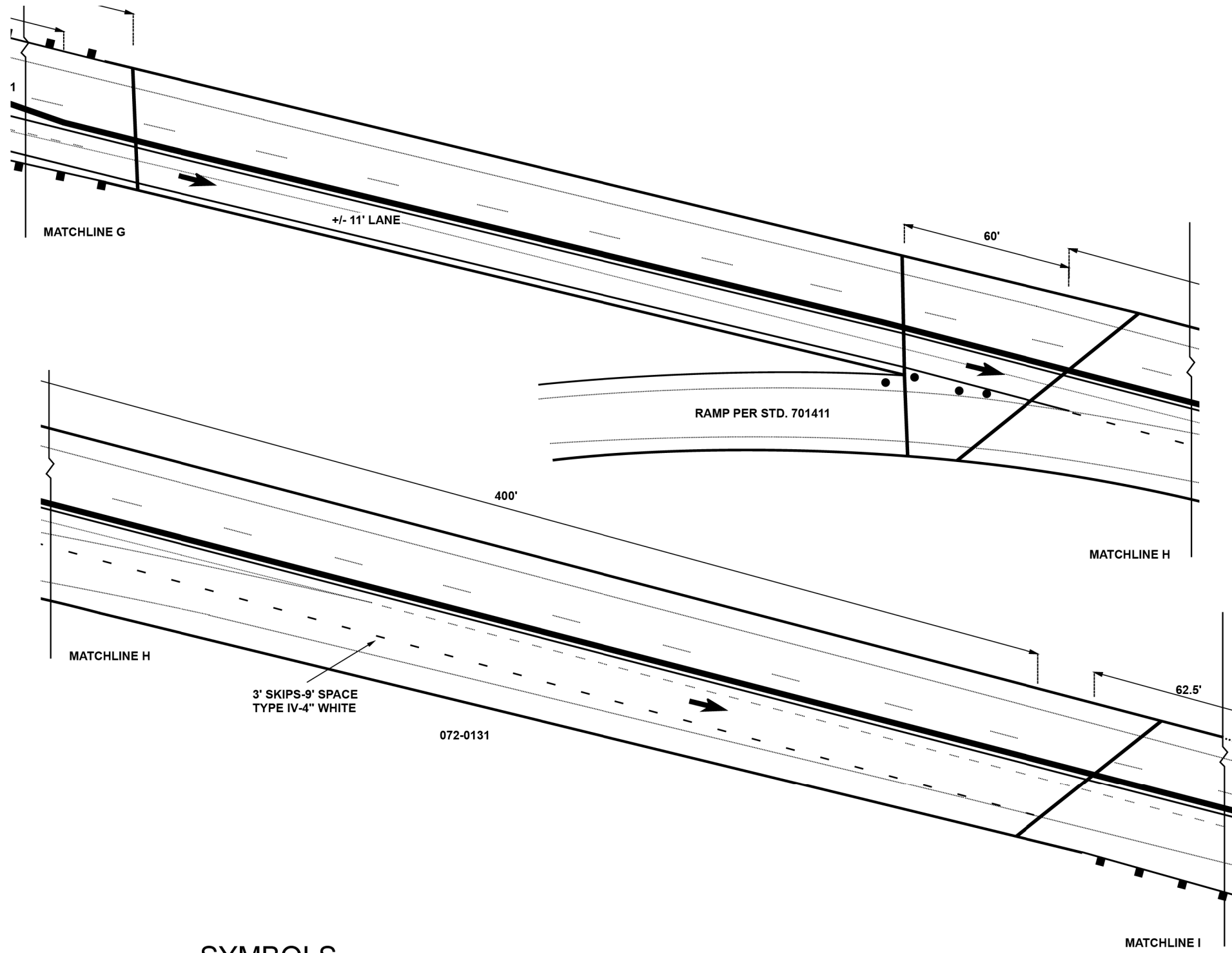
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0129 & 072-0131






SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0129/0131 EASTBOUND
STAGE 1



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4\"/>

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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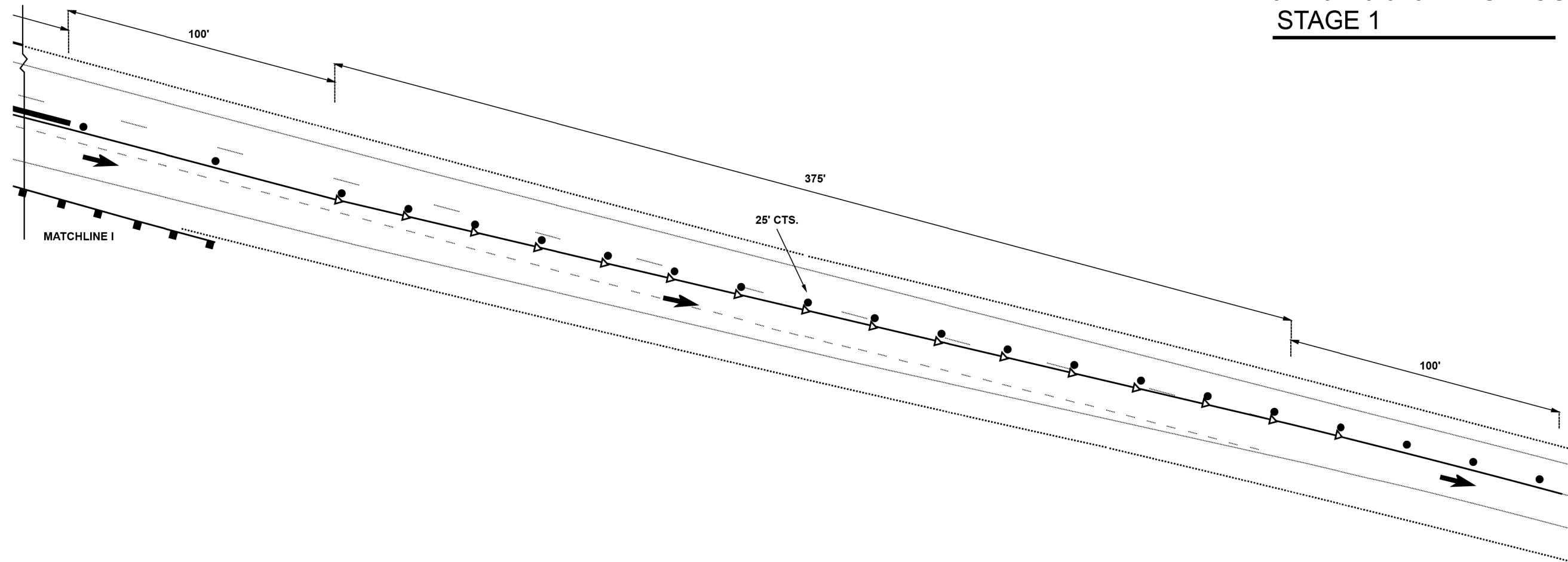
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**








STAGING DETAIL FOR SN 072-0129 & 072-0131			
SCALE:	SHEET	OF	SHEETS
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F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	159
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0129/0131 EASTBOUND
STAGE 1



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" - WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0129 & 072-0131

SCALE: SHEET OF SHEETS STA. TO STA.

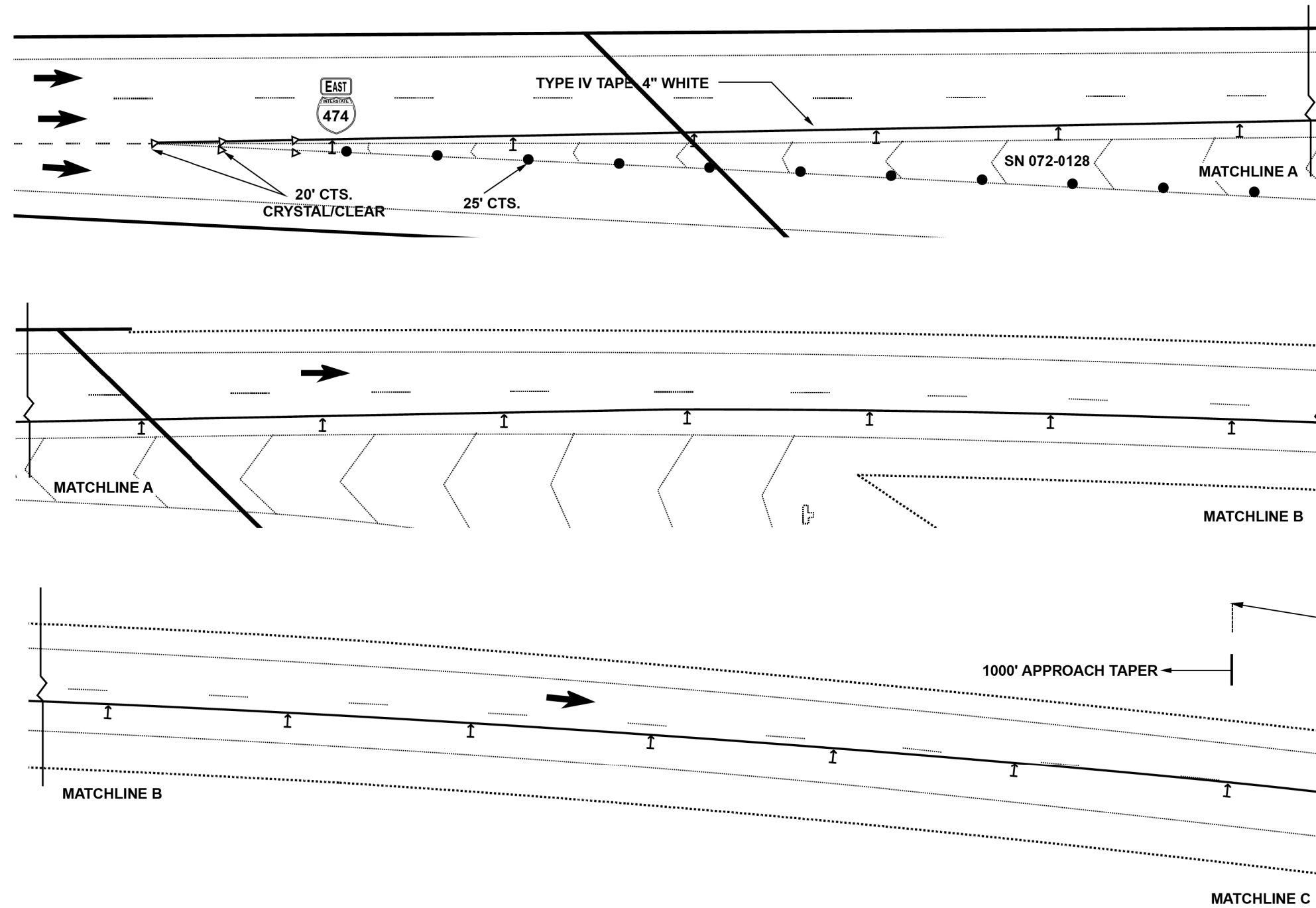
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	160
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68887	

072-0129/0131 EASTBOUND STAGE 2

STAGE 2

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

- 1) COMPLETE APPROACH TCB TAPER, ATTENUATORS, AND ANCHORING.
- 2) COMPLETE REMAINING PAVEMENT MARKING, SIGNS, AND ETC.



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4"- WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

MODEL: Default
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	DATE -	REVISED -

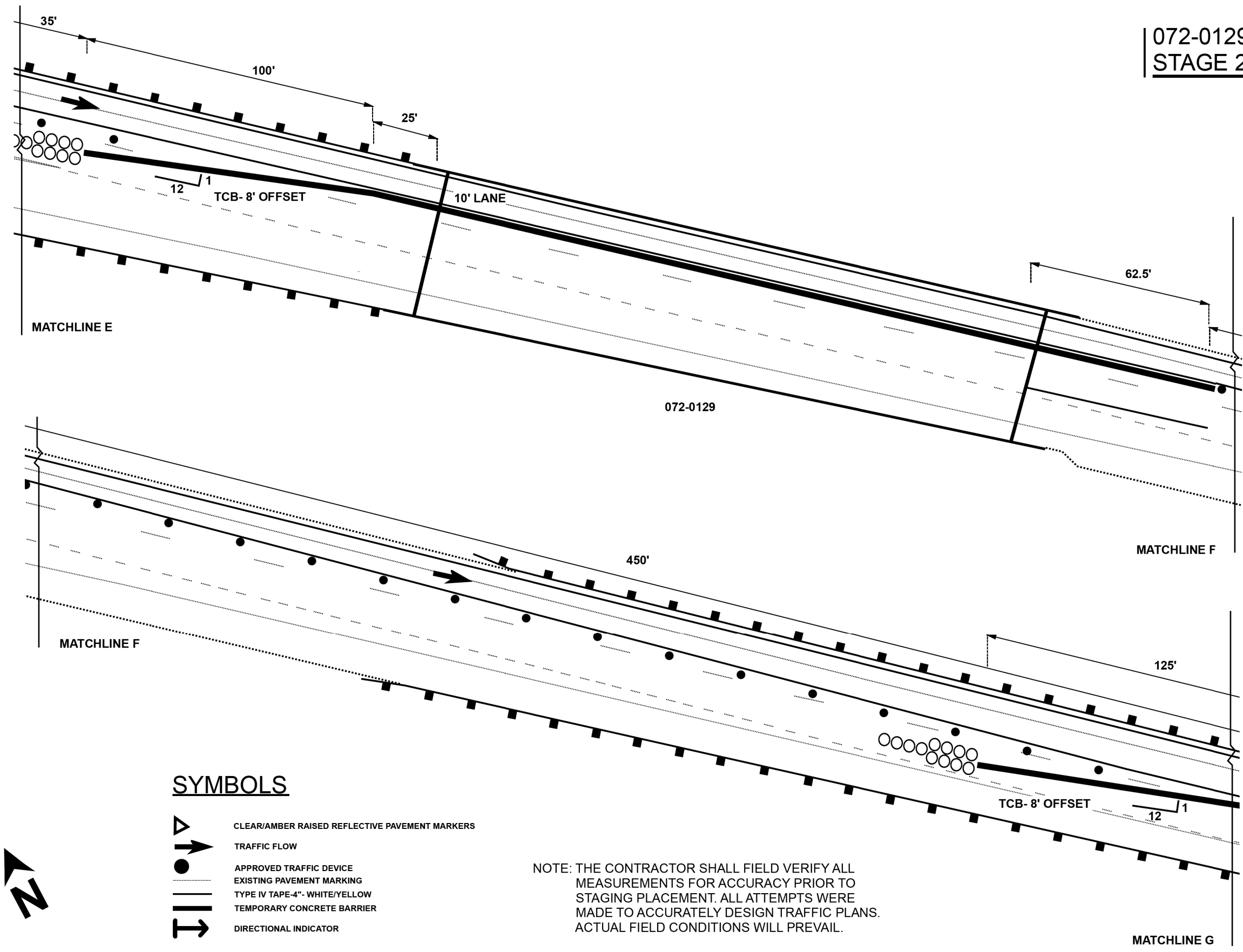
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0129 & 072-0131








SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	161
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0129/0131 EASTBOUND
STAGE 2



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" - WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.



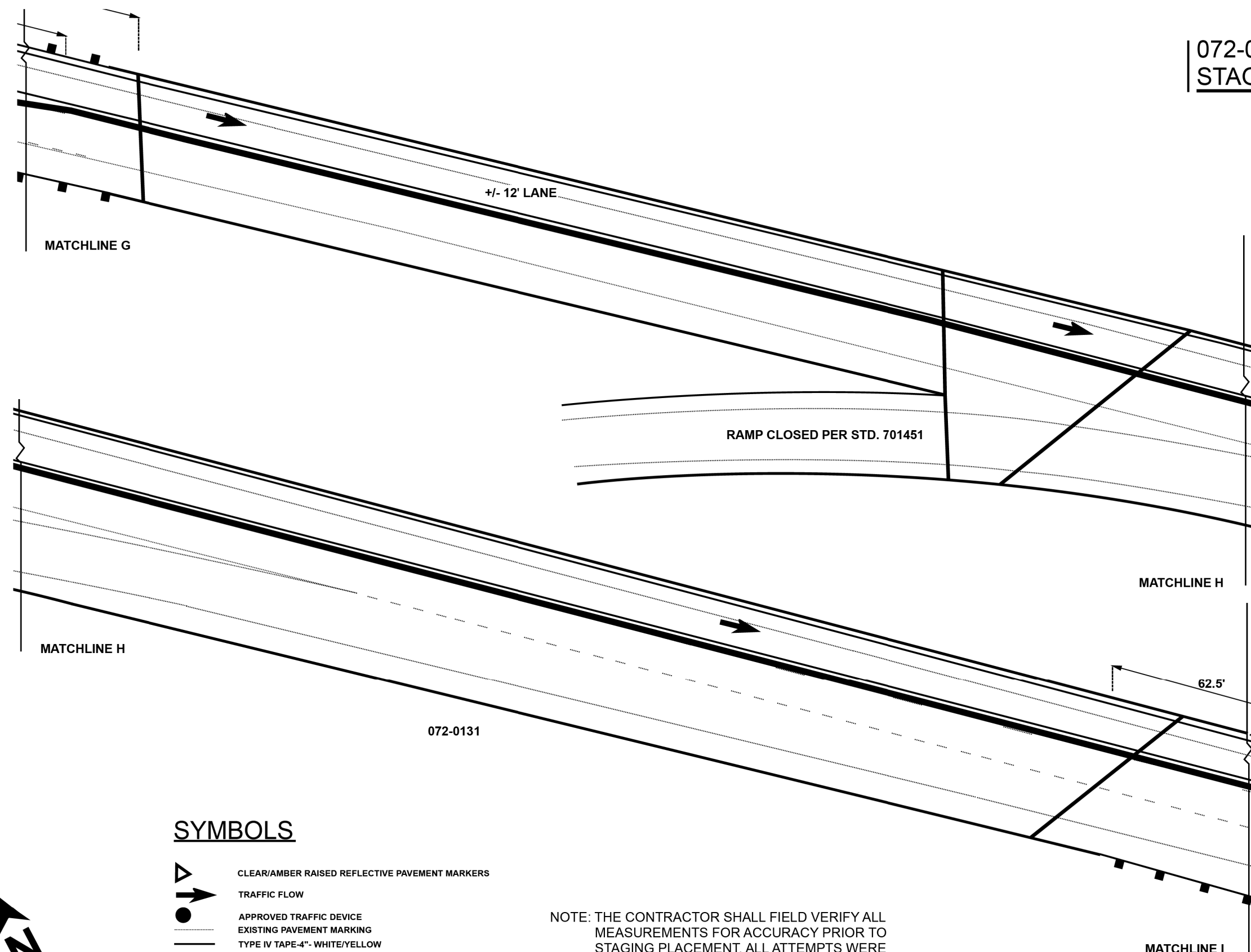
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PLOT DATE = 12/13/2017	CHECKED -	REVISIED -
	DATE -	REVISIED -








**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0129 & 072-0131				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	163
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4"- WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.



MODEL Default
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	DATE -	REVISED -

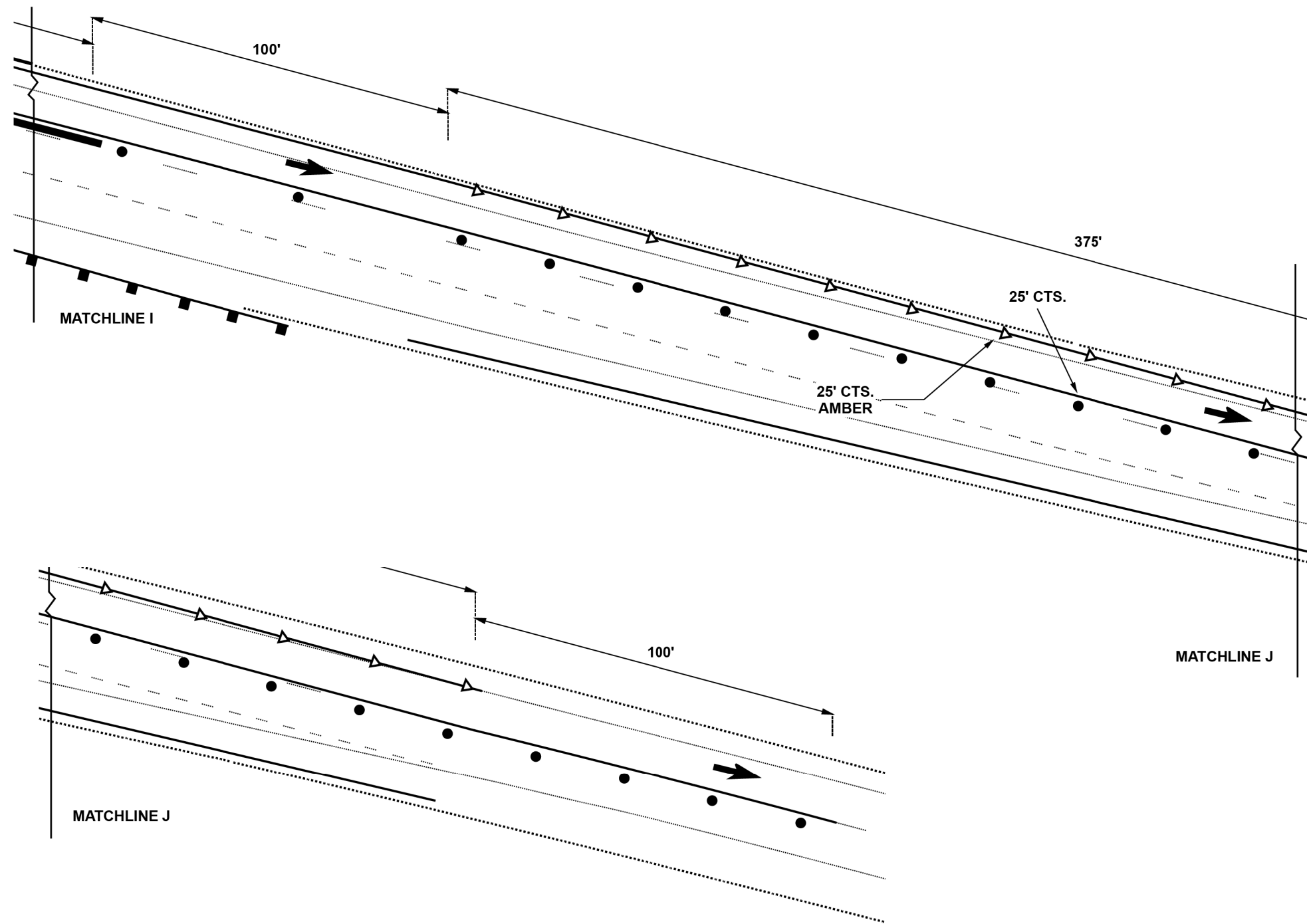
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0129 & 072-0131








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F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	164
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0129/0131 EASTBOUND
STAGE 2



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

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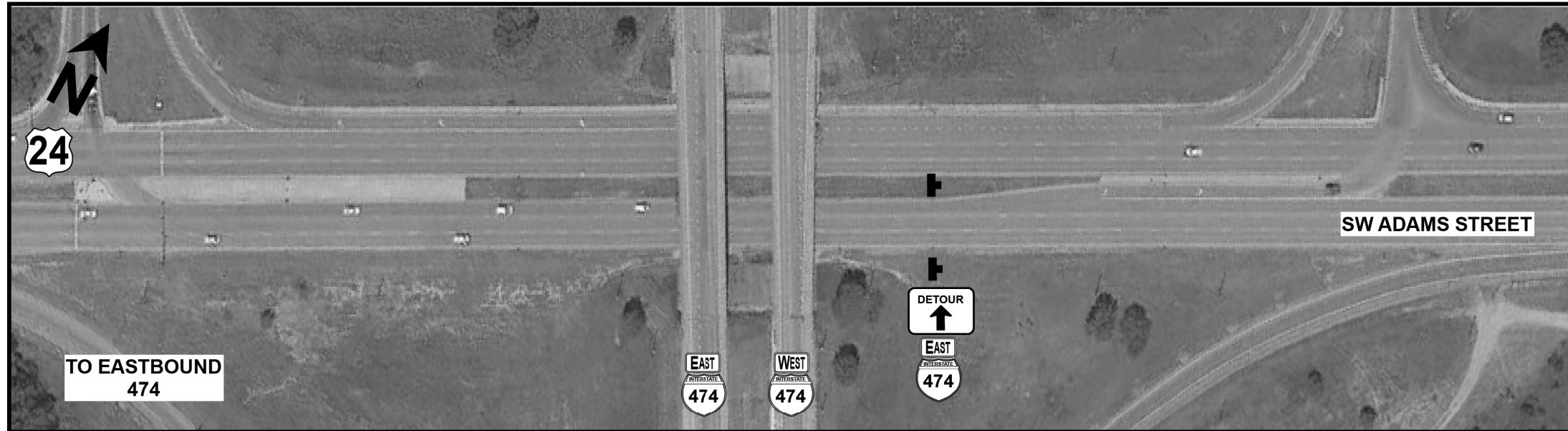
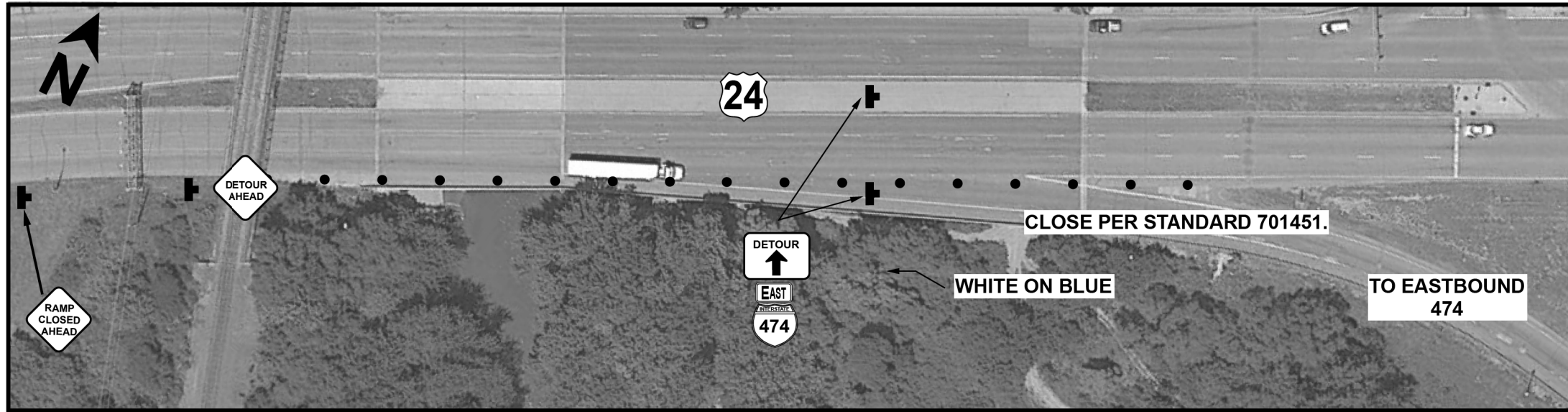
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0129 & 072-0131

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	165
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



US 24 TO EB 474 DETOUR SHEET 1/2

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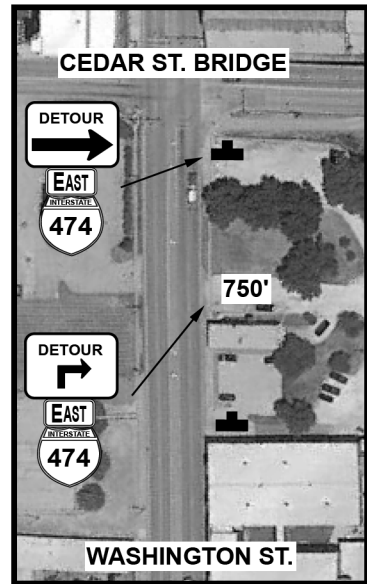
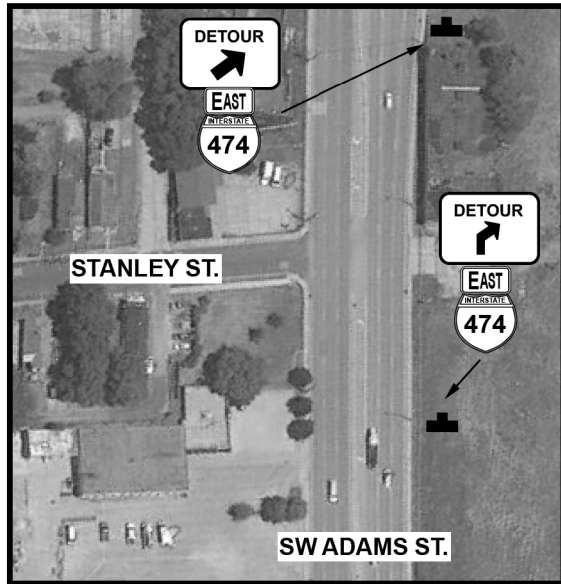
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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RAMP C CLOSURE - US 24 TO EB I-474 DETOUR

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	166
CONTRACT NO. 68887				
ILLINOIS		FED. AID PROJECT		




PLACE TWO SIGNS BETWEEN THE CEDAR ST. BRIDGE AND FISCHER ROAD ON SOUTHBOUND IL 29.



ALL TRAFFIC CONTROL DETOUR SIGNAGE SHOWN ON US 24 TO EB 474 DETOUR SHEETS SHALL BE AS SHOWN AND PAID FOR ACCORDING TO TRAFFIC CONTROL AND PROTECTION, SPECIAL. THIS SHALL INCLUDE PROVIDING, INSTALLING, AND REMOVING ALL ASSOCIATED TRAFFIC CONTROL DETOUR SIGNAGE.

DIRECTIONAL DETOUR SIGNS ON THE INTERSTATE SHALL BE 48 X 36 AND ALL SIGNS SHALL BE POST MOUNTED.

ACTUAL SIGN LOCATION WILL VARY BASED UPON ACTUAL FIELD CONDITIONS.

ALL SIGNS SHALL BE BLACK ON ORANGE EXCEPT INTERSTATE CARDINAL DIRECTION AND SHIELD.

US 24 TO EB 474 DETOUR SHEET 2/2

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PLOT DATE = 12/14/2017	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RAMP C CLOSURE - US 24 TO EB I-474 DETOUR

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	167
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0132/0130 -WESTBOUND STAGE 1

THE FOLLOWING DETAILS REPRESENT A SUGGESTED STAGING OF TRAFFIC CONTROL:

WORKERS	EXPECT
AHEAD	TRAFFIC
4 MILES	DELAYS

NOTE: NOT PART OF STD. 701400.

1 PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) LOCATED IN THE U-TURN ON THE WEST SIDE OF THE SHADE LOHMAN BRIDGE SHALL BE USED FOR THE FOLLOWING:

PRE-STAGE 1 AND STAGE 1 INSTALLATION/REMOVAL. CHANGING STAGE 1 TO STAGE 2, AND REMOVAL OF STAGE 2.

PCMS IS ONLY REQUIRED AT THIS LOCATION DURING THE ABOVE CONDITIONS. DURING NON-WORKING HOURS, WHEN NEEDED FOR THE NEXT DAY, THE PCMS SHALL HAVE THE MESSAGE READ: RIGHT/LEFT LANE CLOSED 4 MILES AHEAD.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN WORKERS ARE IN THE CLOSED LANE AND NOT BEHIND TEMPORARY CONCRETE BARRIER.

PRE-STAGE 1

1) UTILIZING STANDARDS 701400, 701401, & 701428, CLOSE LANE 2 OF WESTBOUND 474. REMOVE THE CONFLICTING PAVEMENT MARKINGS AND INSTALL NEW PAVEMENT MARKINGS FOR STAGE 1 TRAFFIC, WHERE ABLE TO IN LANE 2.

STAGE 1

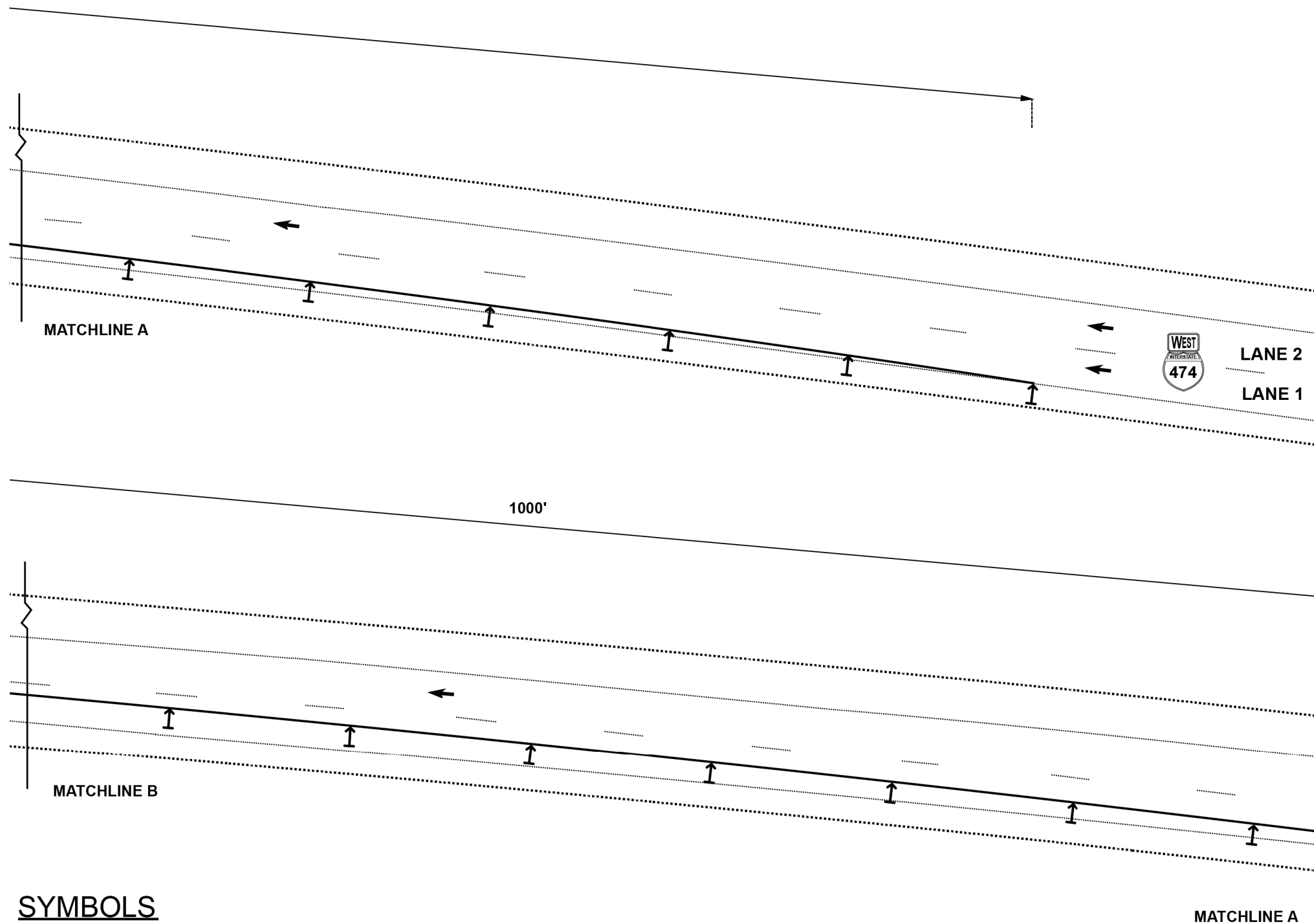
1) UTILIZE STANDARDS 701400, 701402, & 701428, AND AS SHOWN TO CLOSE LANE 1 OF WESTBOUND 474 FOR STAGE 1 REMOVAL. STANDARD 701428 SHALL REMAIN IN USE FOR THE PLACEMENT OF THE APPROACH EDGE-LINE TAPER TO PROTECT THE WORKERS.

PLACE ALL DEVICES, REMAINING PAVEMENT MARKINGS FROM PRE-STAGE 1, AND AS SHOWN ON PLAN DETAILS AND HIGHWAY STANDARDS.

A FLAGGER, WITH SIGNS, SHALL BE PRESENT WHEN PLACING EDGE-LINES, DEVICES, AND INSTALLING TEMPORARY CONCRETE BARRIER (TCB).

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

CONTINUED ON NEXT SHEET



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" - WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	168
CONTRACT NO. 68887				

ILLINOIS FED. AID PROJECT

USER NAME = lababidism	DESIGNED -	REVISED -
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PLOT DATE = 12/13/2017	CHECKED -	REVISED -
	DATE -	REVISED -

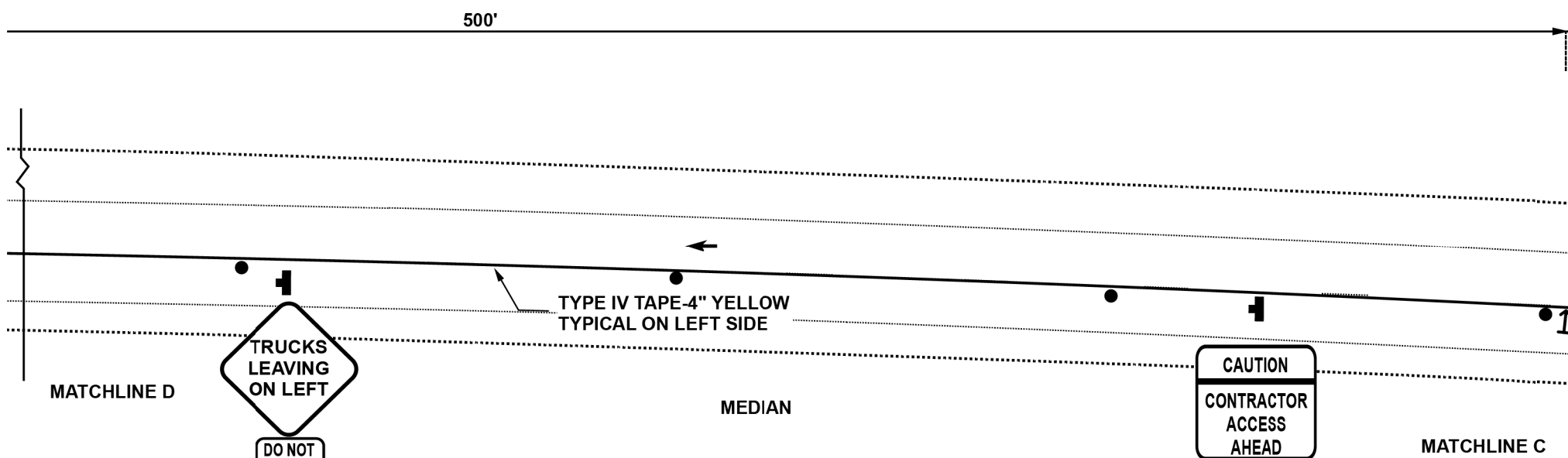
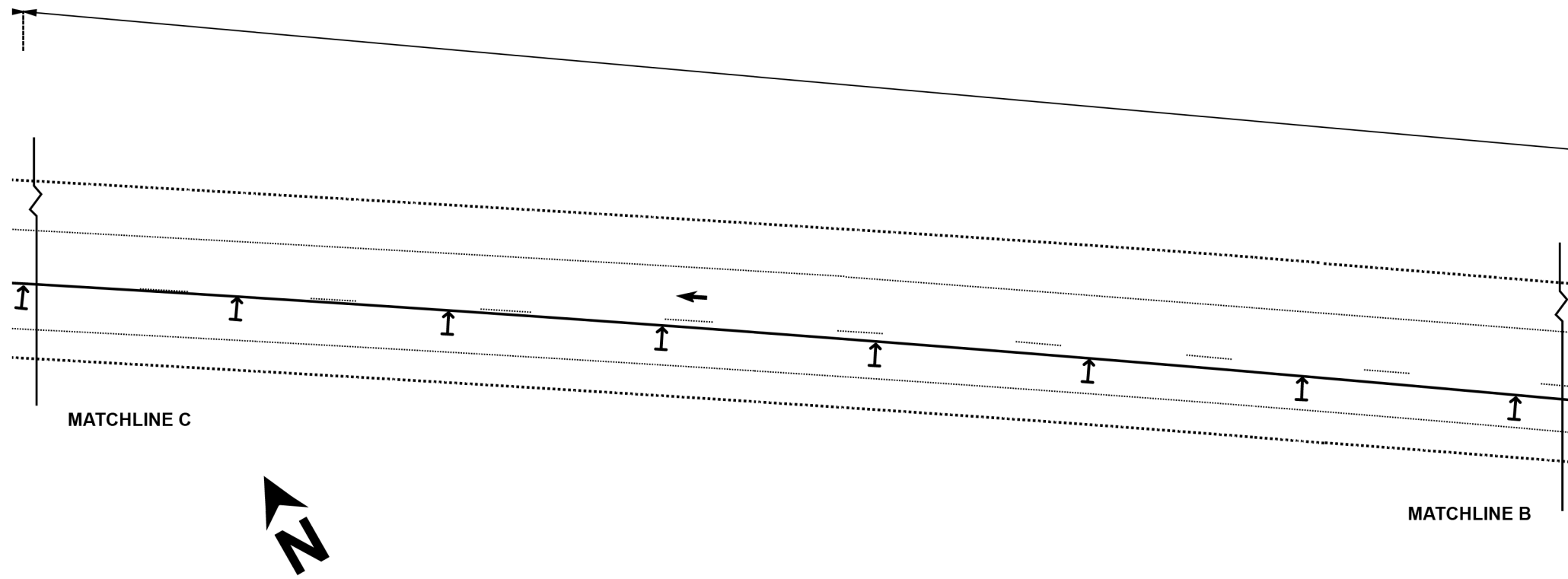
072-0132/0130 -WESTBOUND STAGE 1

PRE-STAGE 2

WHILE IN STAGE 1, COMPLETE THE FOLLOWING:

- 1) RELOCATE TEMPORARY CONCRETE BARRIER WALL TO STAGE 2 LOCATION. FOR THE APPROACH TCB TAPER, PLACE IN A LINEAR POSITION (TCB TAPER TO BE PLACED AFTER TRAFFIC IS IN STAGE 2-LANE 1 WHERE TCB WILL CONFLICT WITH STAGE 2 TRAFFIC.
- 2) REMOVE ANY CONFLICTING LANE 1 MARKINGS, PLACE NEW PAVEMENT MARKINGS, INSTALL CLEAR TCB REFLECTORS, AND ANCHOR EXIT TCB END SECTION.

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" - WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

USER NAME = lababidism	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

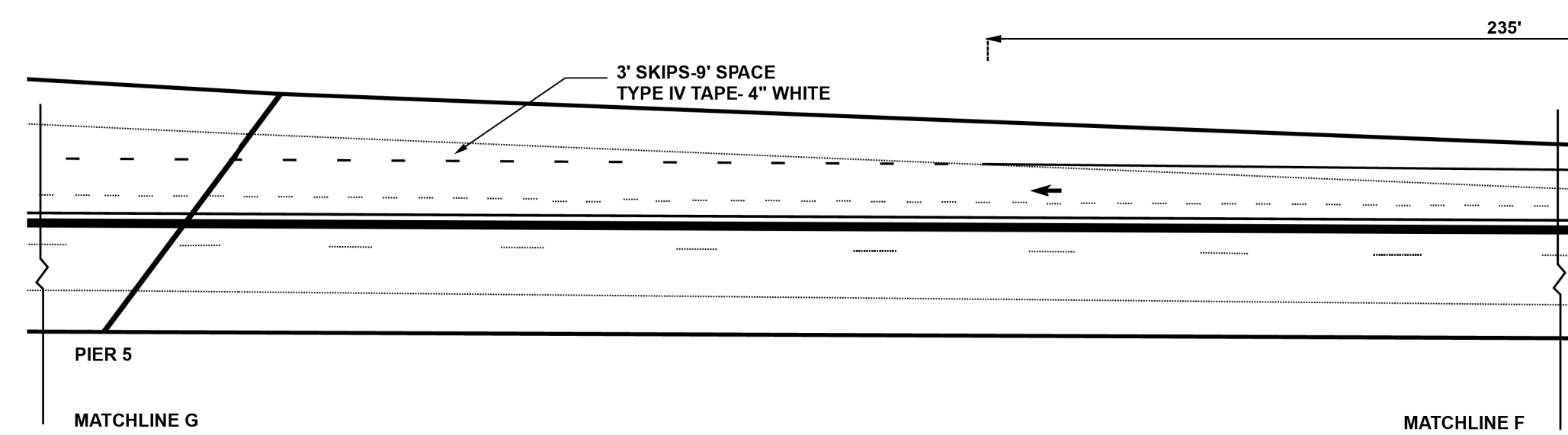
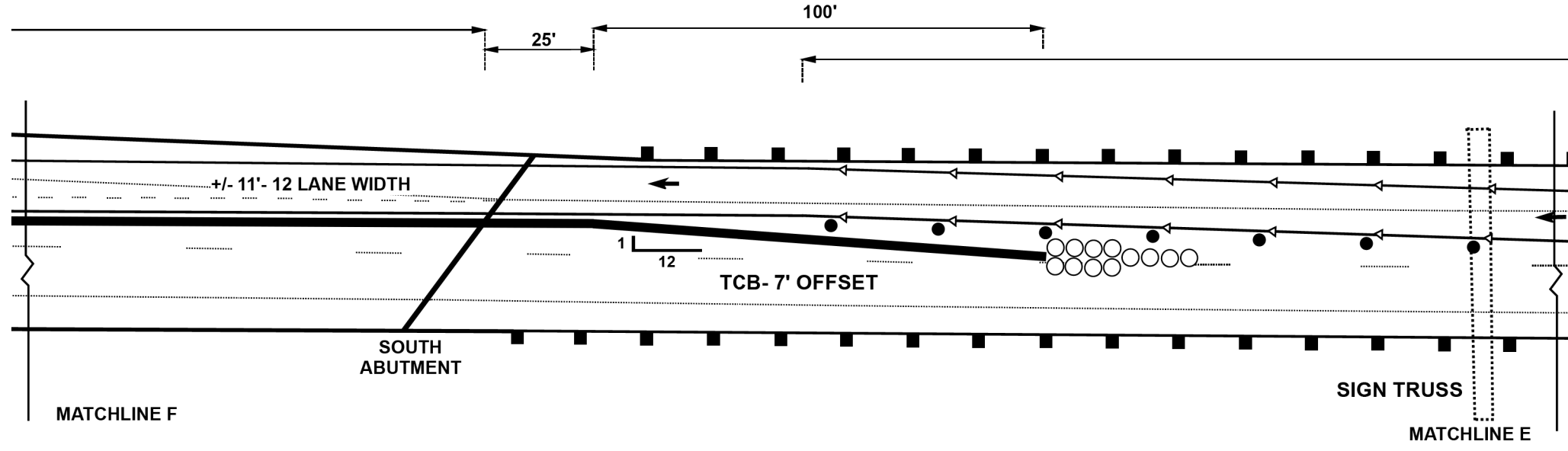
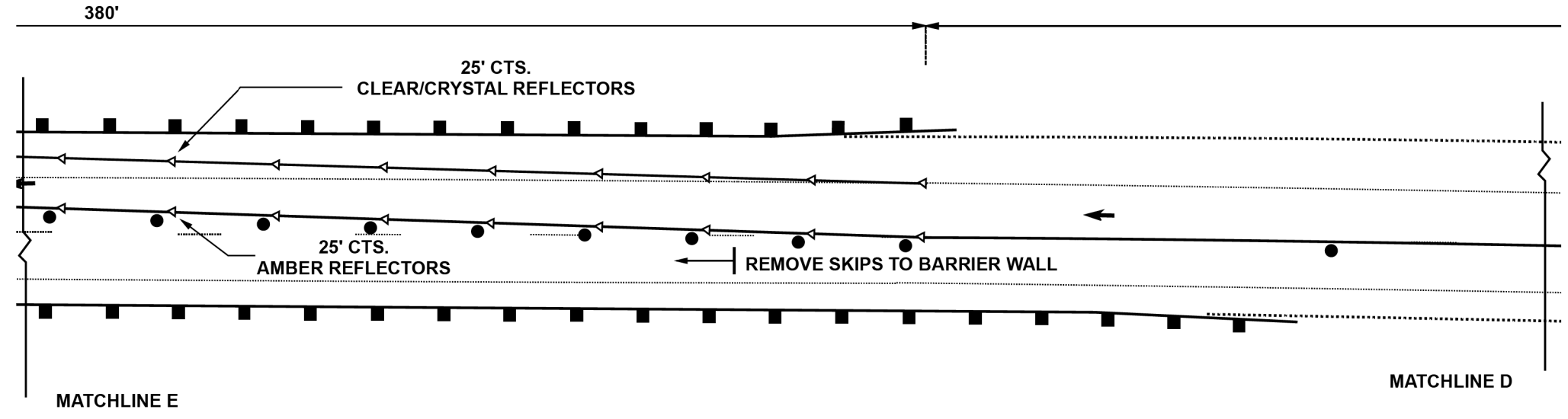
STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	169
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

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072-0132/0130 -WESTBOUND
STAGE 1



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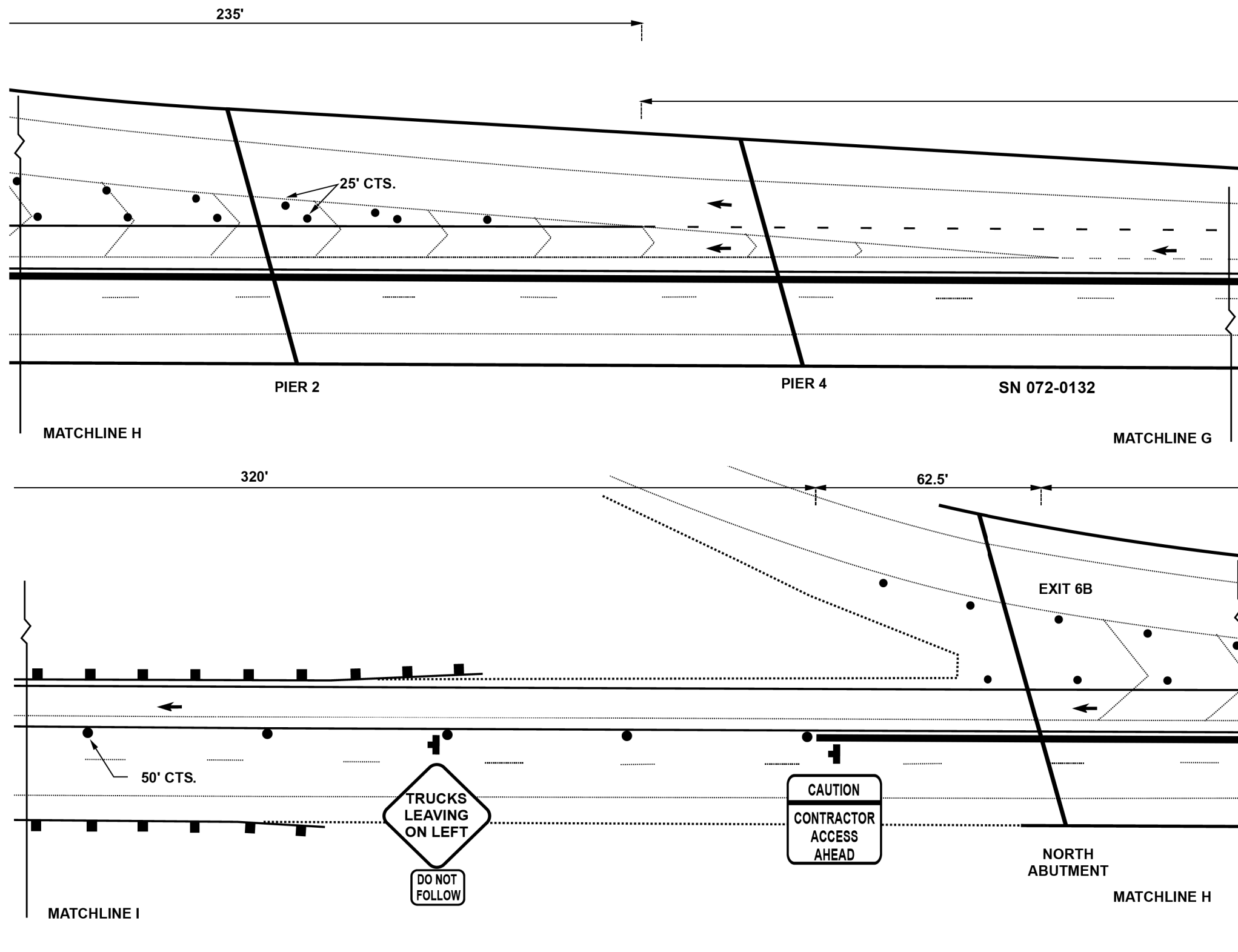
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PLOT DATE = 12/13/2017	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	170
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0132/0130 -WESTBOUND
STAGE 1



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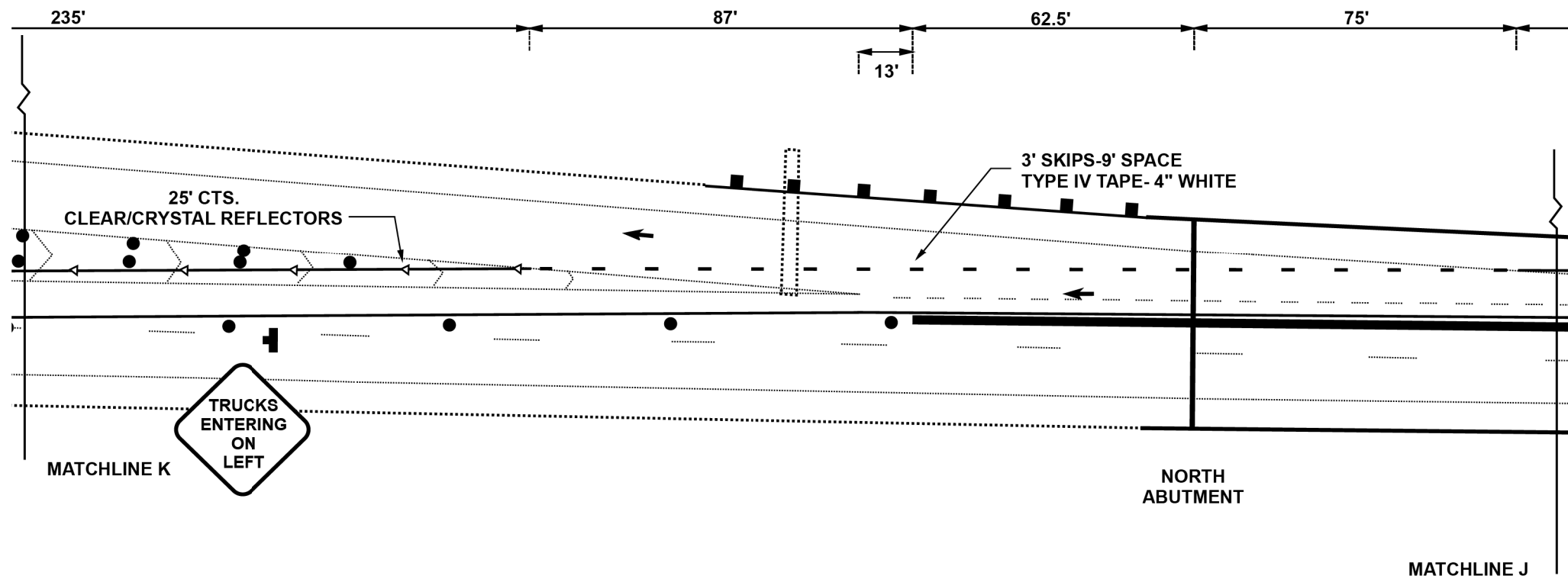
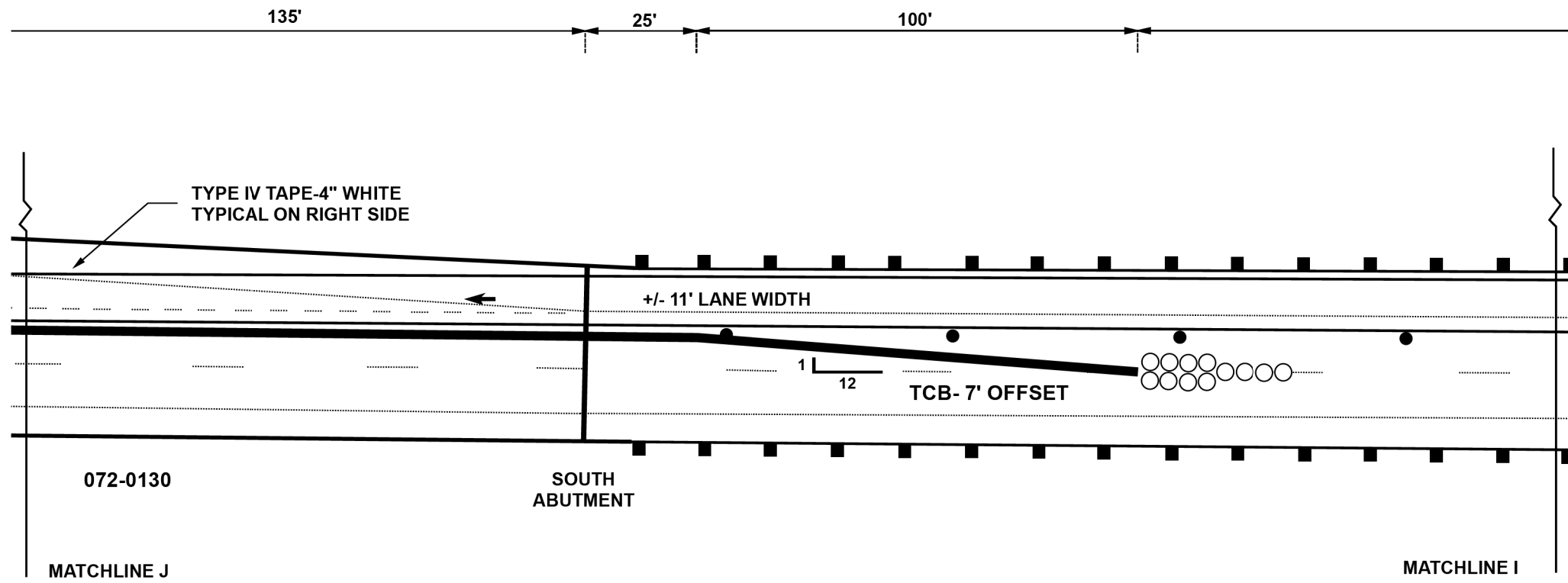
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	171
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0132/0130 -WESTBOUND
STAGE 1



MODEL Default
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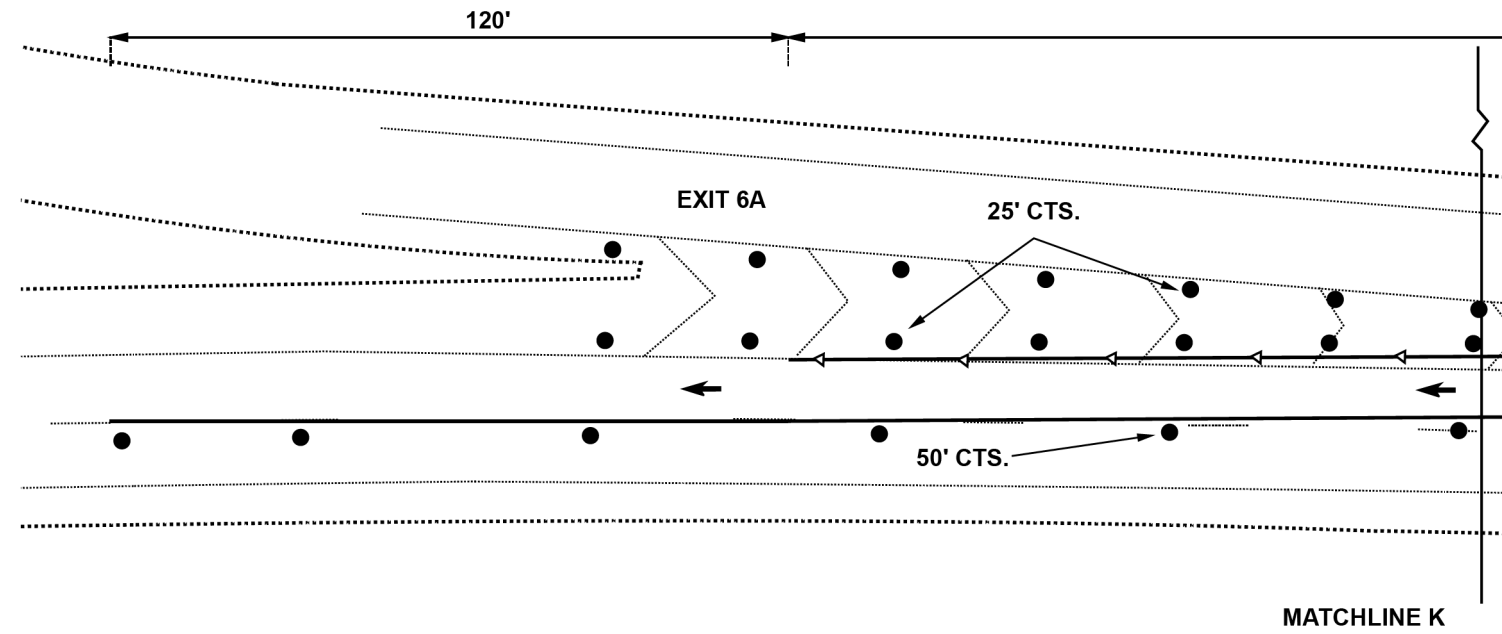
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132








SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	172
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

072-0132/0130 -WESTBOUND
 STAGE 1



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" - WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	173
CONTRACT NO. 68887				
		ILLINOIS	FED. AID PROJECT	

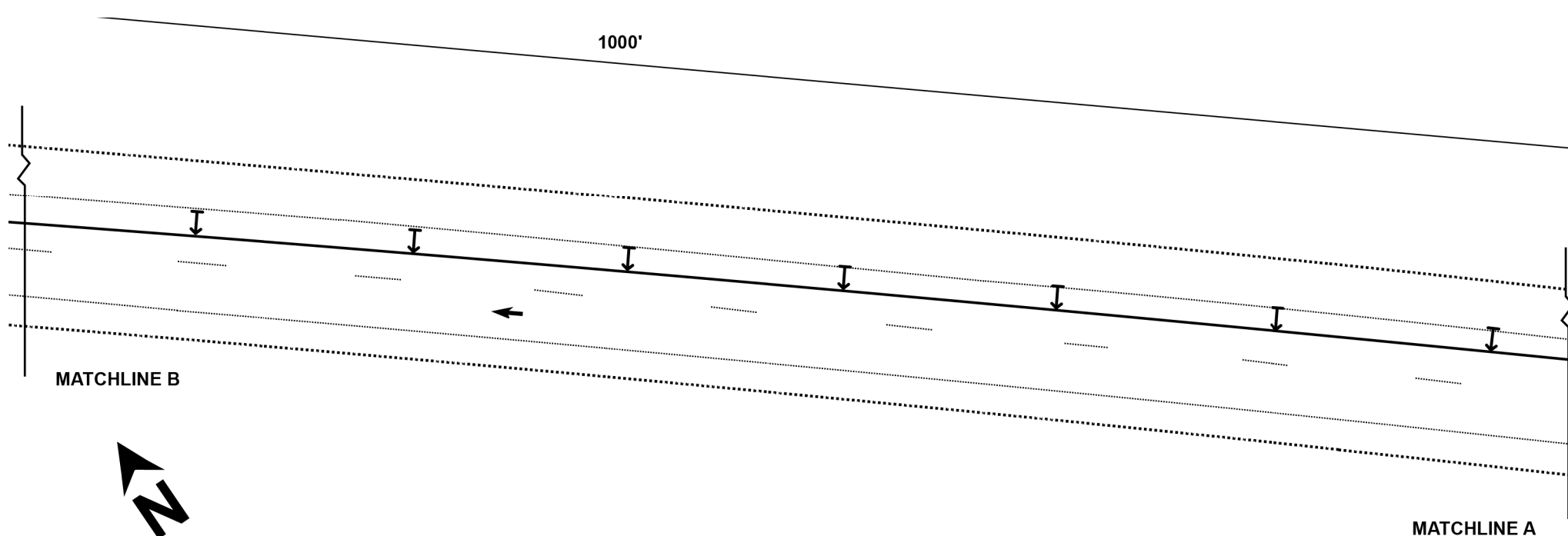
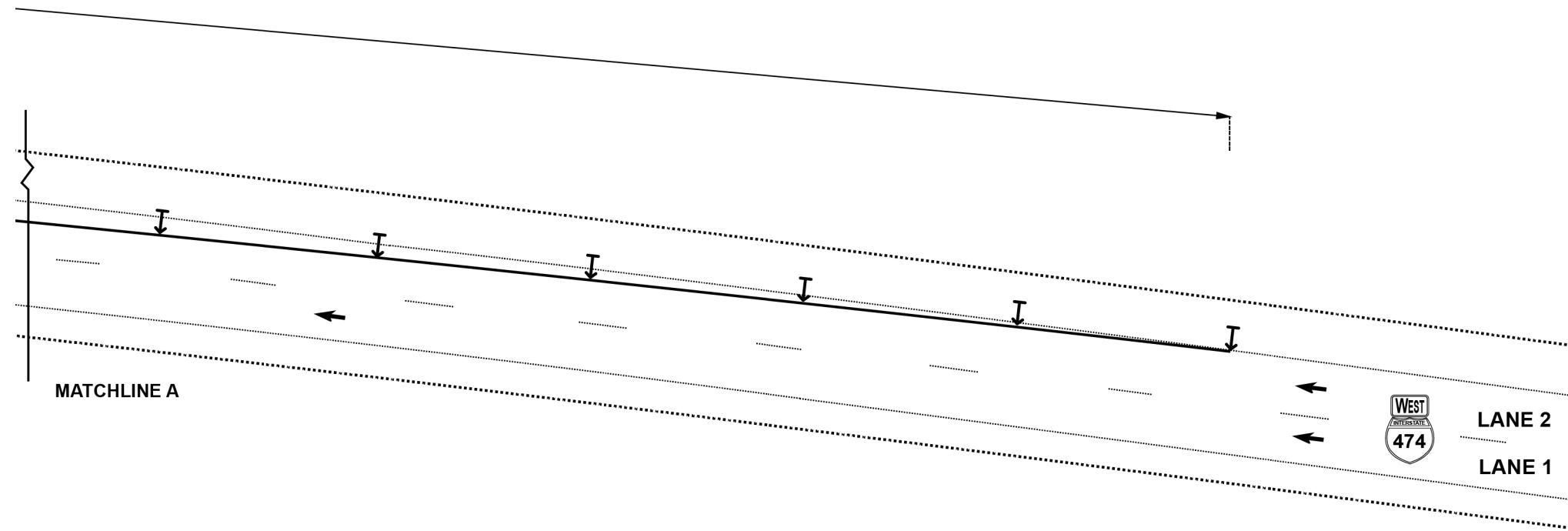
072-0132/0130 -WESTBOUND STAGE 2

STAGE 2

STANDARD 701428 SHALL BE USED TO "PACE" TRAFFIC FOR THE CHANGING OF APPROACH LANE TAPERS. TWO ATTENUATOR TRUCKS SHALL BE USED SIDE BY SIDE WITH ARROW BOARD IN CAUTION MODE.

- 1) COMPLETE APPROACH TCB TAPER, ATTENUATORS, AND ANCHORING.
- 2) COMPLETE REMAINING PAVEMENT MARKING, SIGNS, AND ETC.

NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL MEASUREMENTS FOR ACCURACY PRIOR TO STAGING PLACEMENT. ALL ATTEMPTS WERE MADE TO ACCURATELY DESIGN TRAFFIC PLANS. ACTUAL FIELD CONDITIONS WILL PREVAIL.



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" - WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

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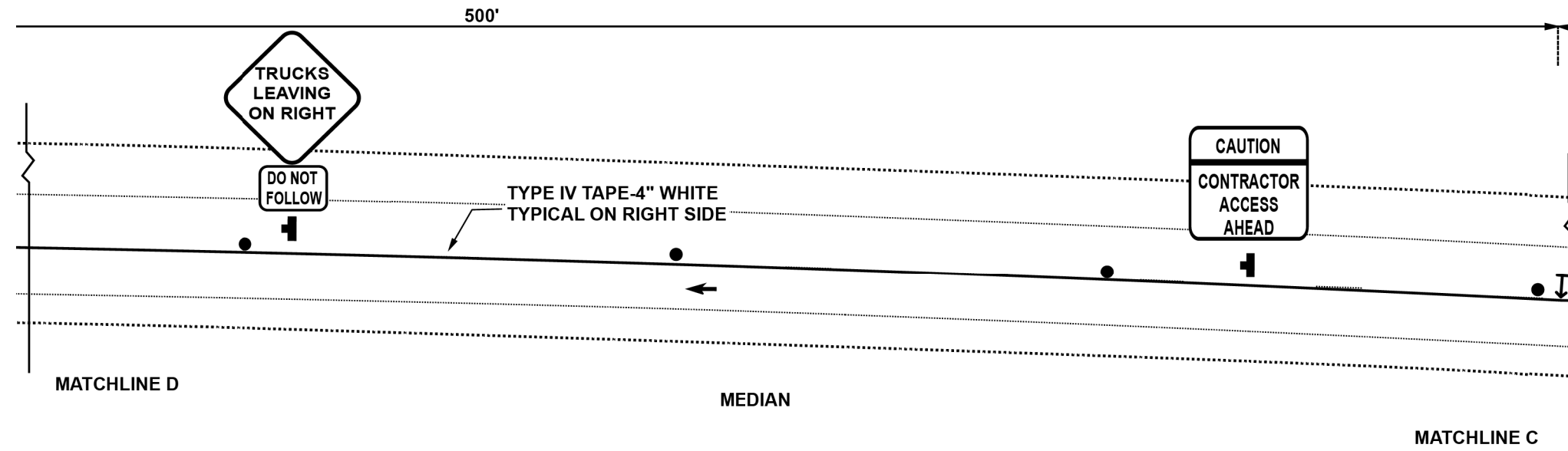
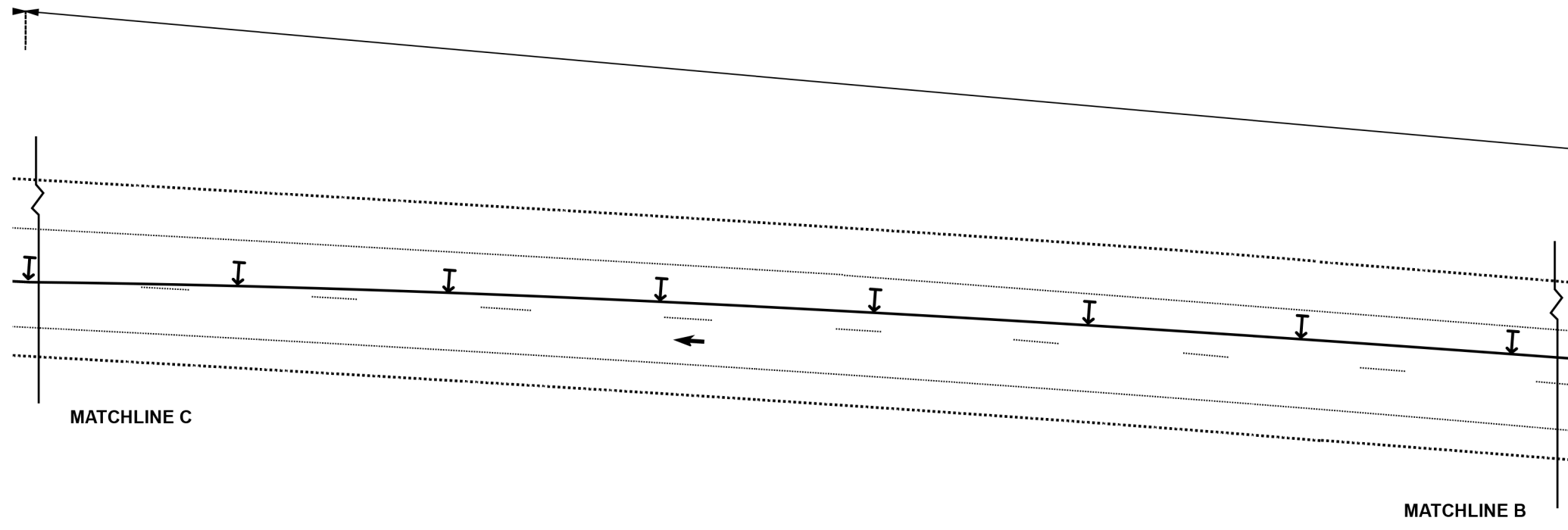
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132




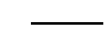



SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	174
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68887	

072-0132/0130 -WESTBOUND
STAGE 2



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" - WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

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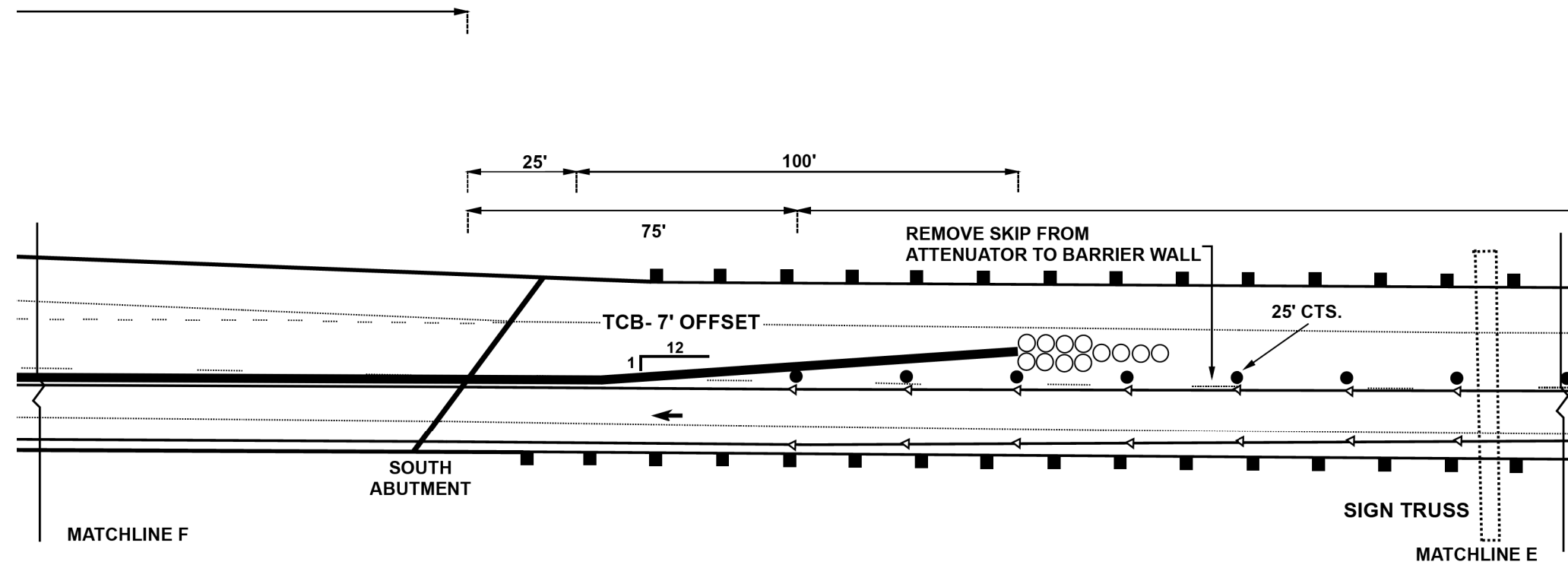
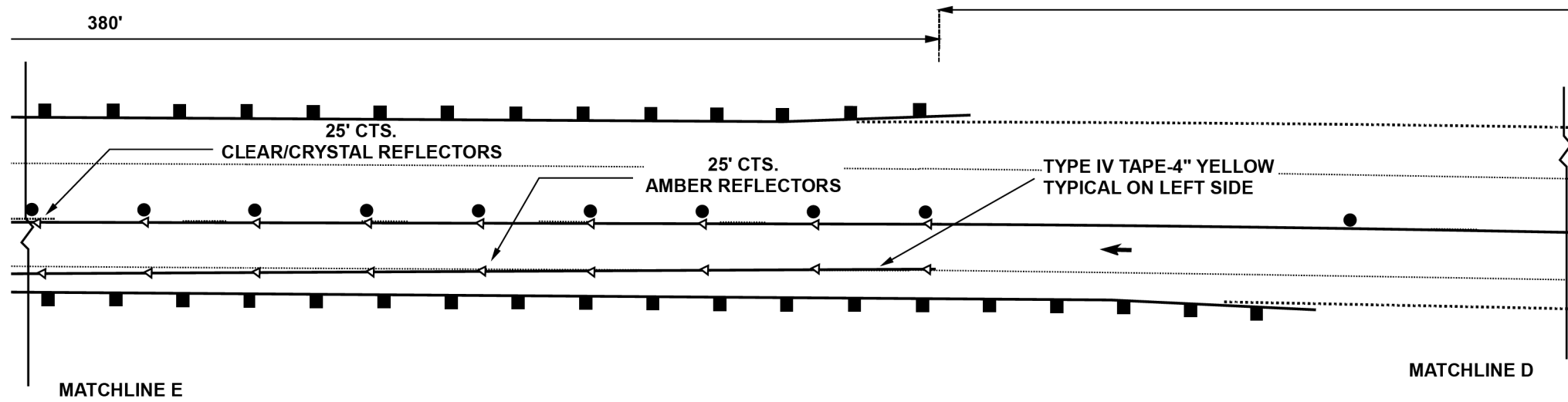
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	175
CONTRACT NO. 68887				
ILLINOIS		FED. AID PROJECT		

072-0132/0130 -WESTBOUND
STAGE 2



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" - WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

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PLOT DATE = 12/13/2017	DATE -	REVISED -

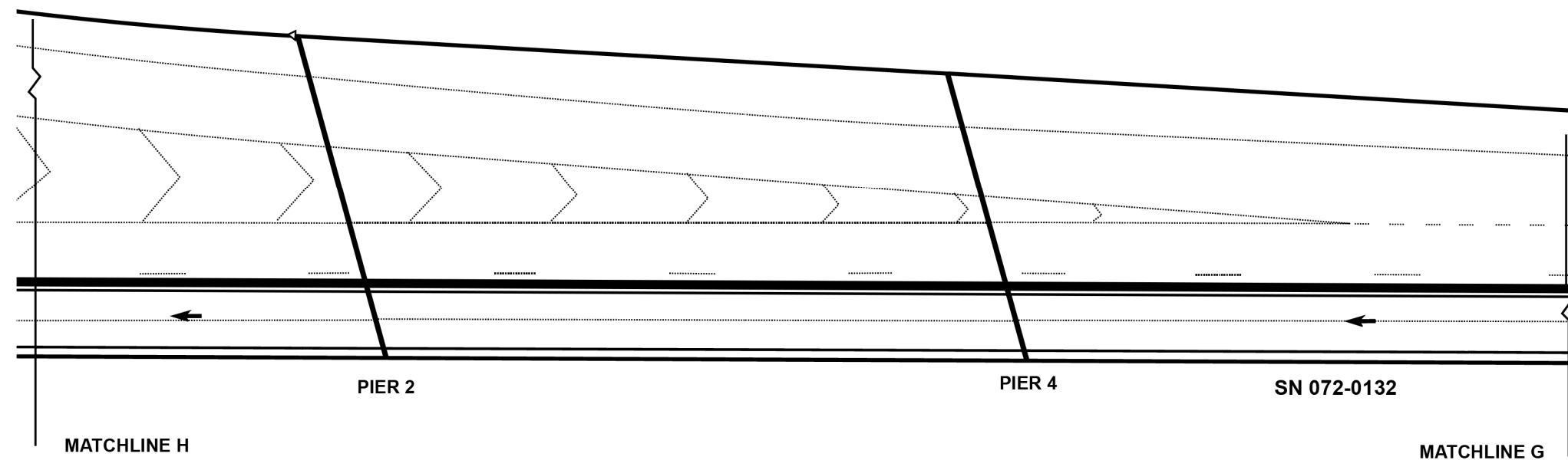
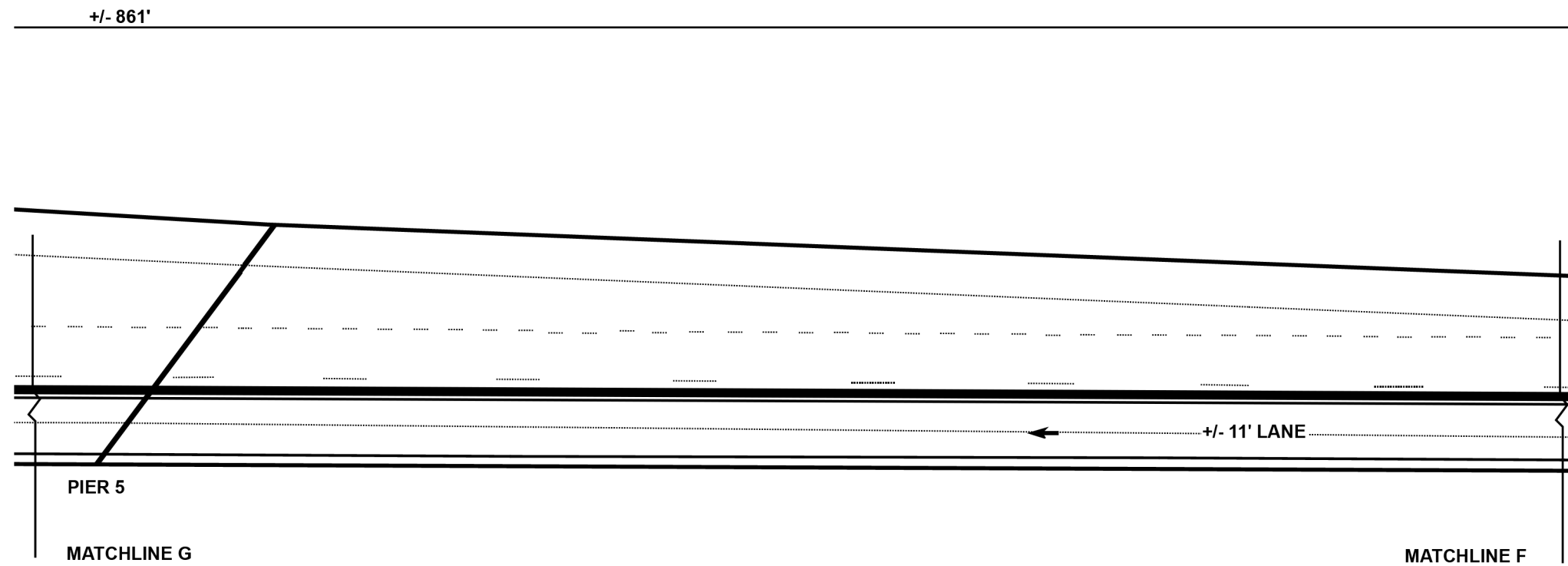
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	176
CONTRACT NO. 68887				
ILLINOIS		FED. AID PROJECT		

072-0132/0130 -WESTBOUND
STAGE 2



SYMBOLS

- CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
- TRAFFIC FLOW
- APPROVED TRAFFIC DEVICE
- EXISTING PAVEMENT MARKING
- TYPE IV TAPE-4" - WHITE/YELLOW
- TEMPORARY CONCRETE BARRIER
- DIRECTIONAL INDICATOR

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

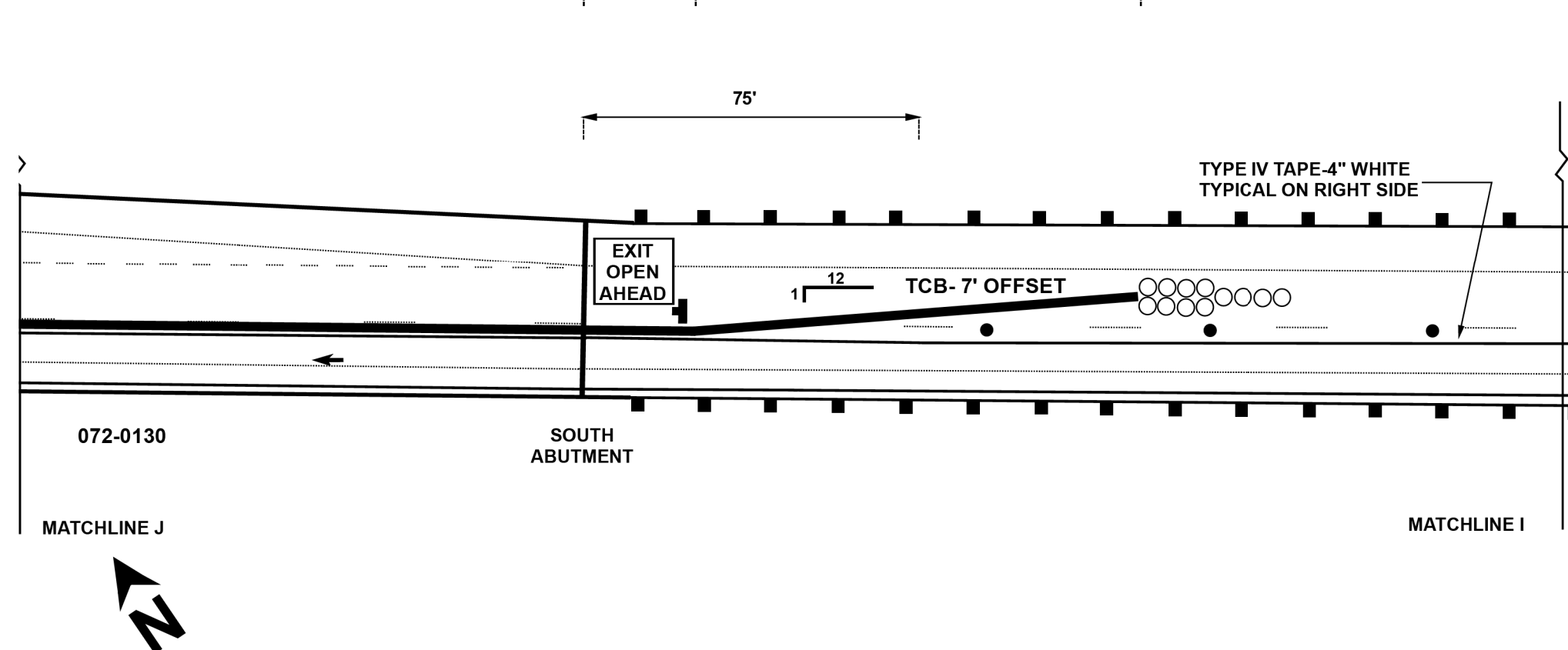
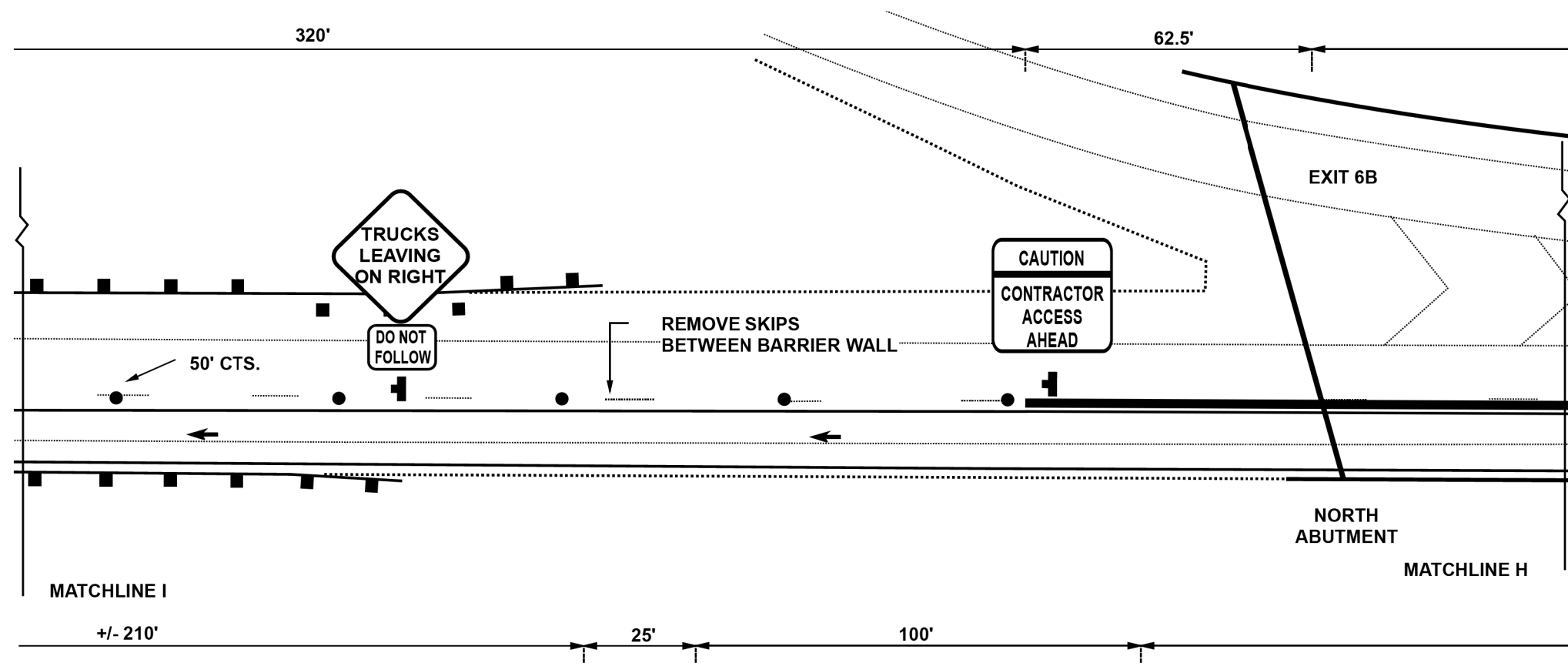
STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.








F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	177
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 68887

072-0132/0130 -WESTBOUND
STAGE 2



SYMBOLS

-  CLEAR/AMBER RAISED REFLECTIVE PAVEMENT MARKERS
-  TRAFFIC FLOW
-  APPROVED TRAFFIC DEVICE
-  EXISTING PAVEMENT MARKING
-  TYPE IV TAPE-4" - WHITE/YELLOW
-  TEMPORARY CONCRETE BARRIER
-  DIRECTIONAL INDICATOR

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PLOT DATE = 12/13/2017	DATE -	REVISED -

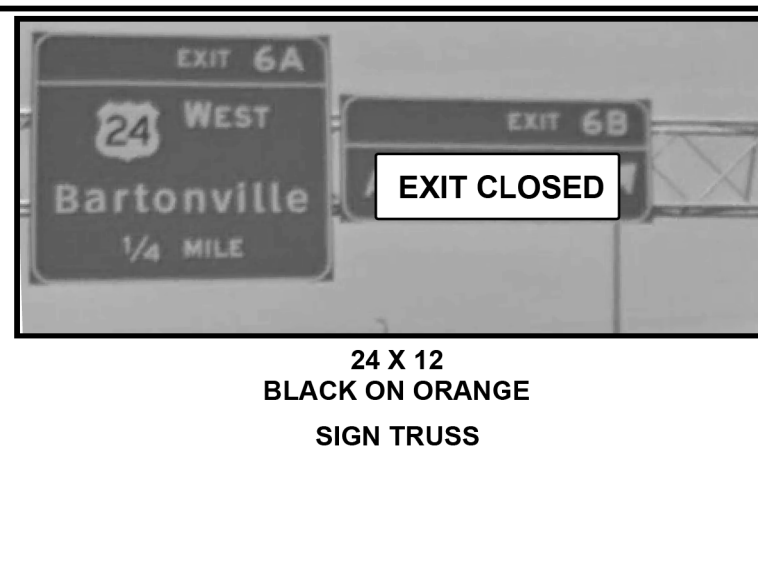
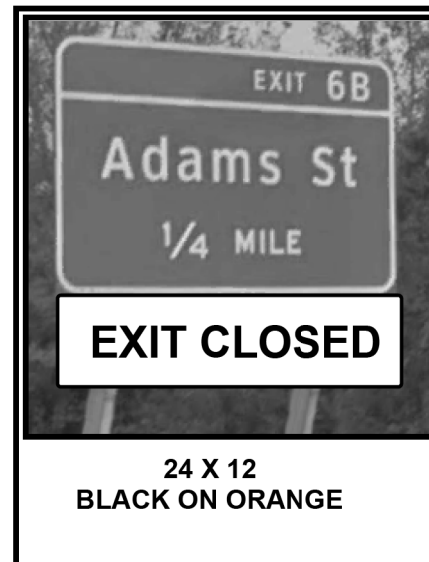
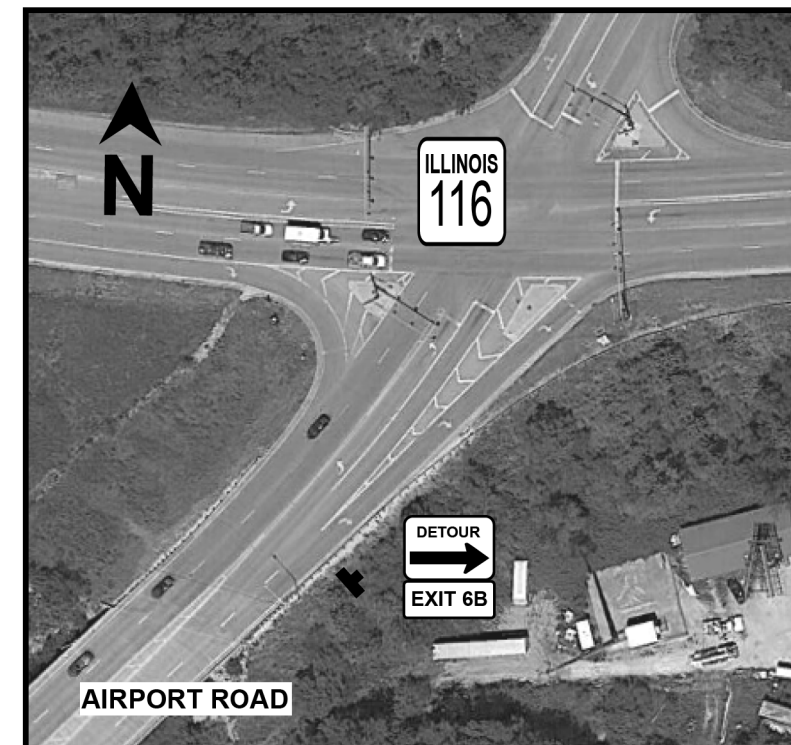
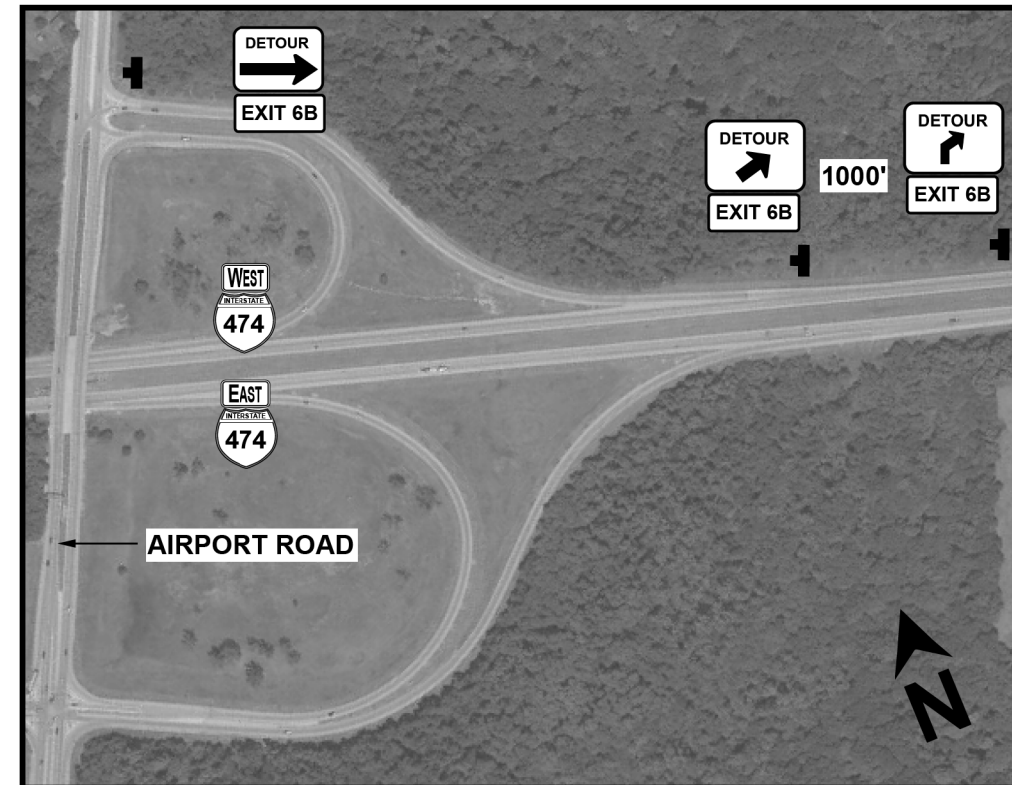
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR SN 072-0130 & 072-0132

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	178
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 68887



EXIT 6B DETOUR SHEET 1/2

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PLOT DATE = 12/13/2017	DATE -	REVISED -

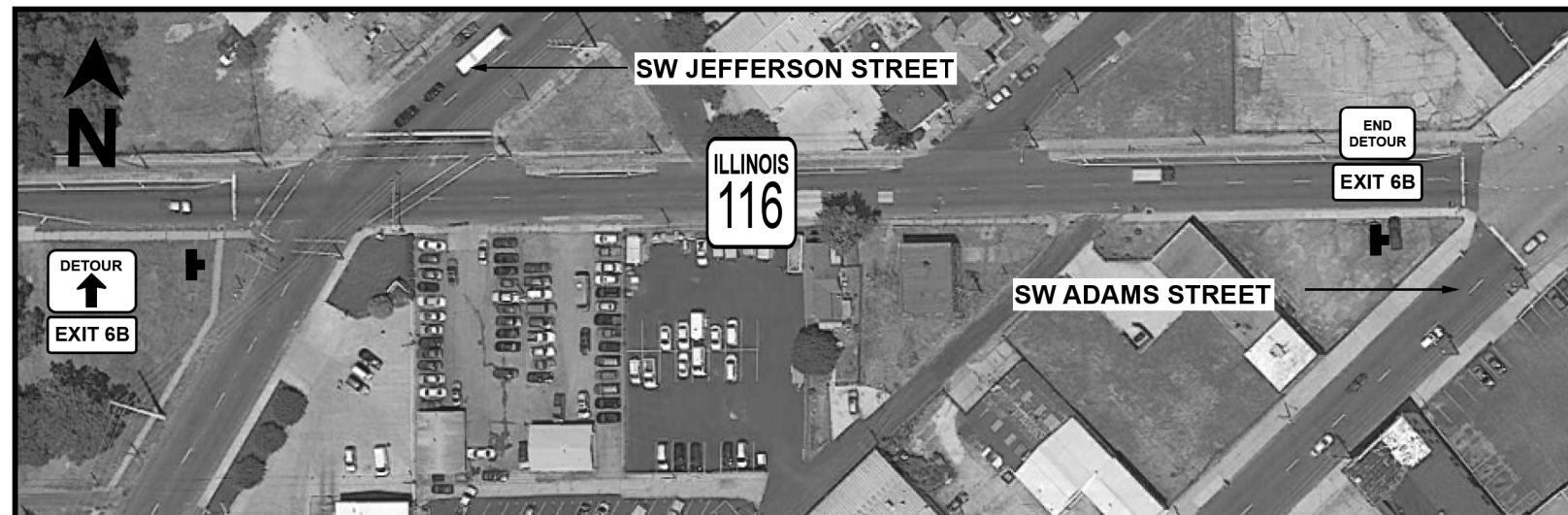
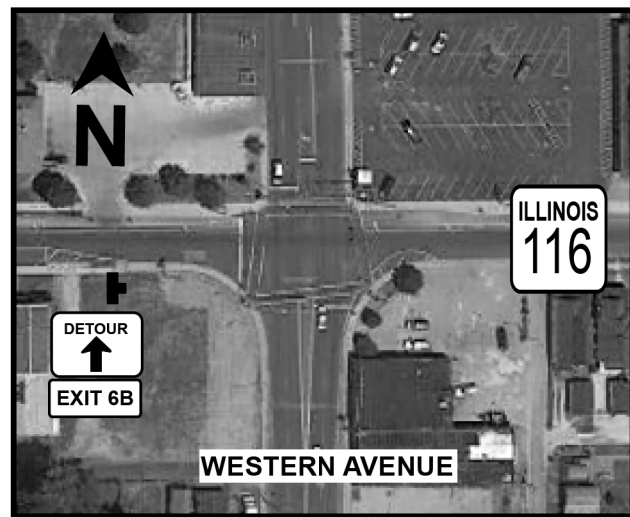
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RAMP B CLOSURE - I-474 WB TO US 24 DETOUR

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	180
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				

EXIT 6B DETOUR SHEET 2/2



ALL TRAFFIC CONTROL DETOUR SIGNAGE SHOWN ON EXIT 6B DETOUR SHEETS SHALL BE AS SHOWN AND PAID FOR ACCORDING TO TRAFFIC CONTROL AND PROTECTION, STANDARD 701451. THIS SHALL INCLUDE PROVIDING, INSTALLING, AND REMOVING ALL ASSOCIATED TRAFFIC CONTROL DETOUR SIGNAGE.

DIRECTIONAL DETOUR SIGNS ON THE INTERSTATE SHALL BE 48 X 36 AND POST MOUNTED.

DETOUR SIGNS LOCATED FROM MARTIN LUTHER KING JR DRIVE TO SW ADAMS STREET MAY BE STAND MOUNTED.

ACTUAL SIGN LOCATION WILL VARY BASED UPON ACTUAL FIELD CONDITIONS.

ALL SIGNS SHALL BE BLACK ON ORANGE.

MODEL Default
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

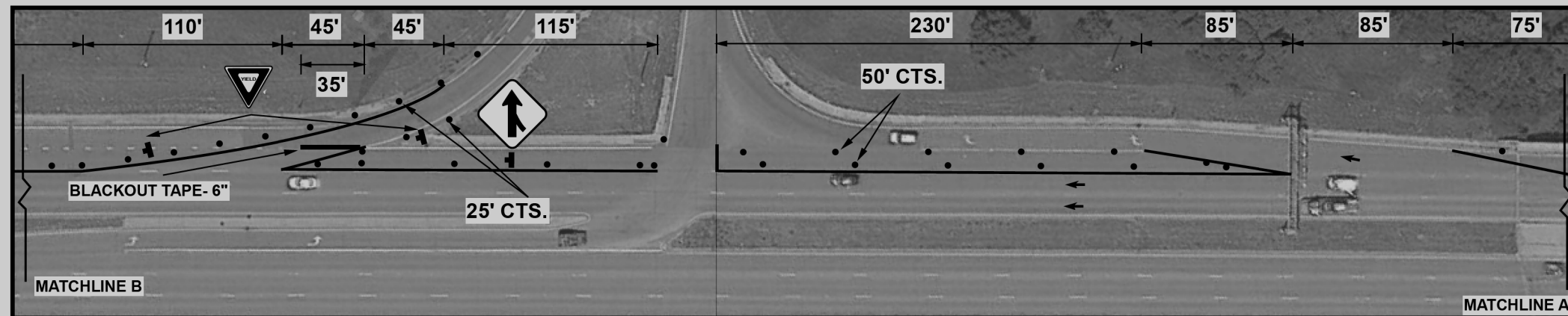
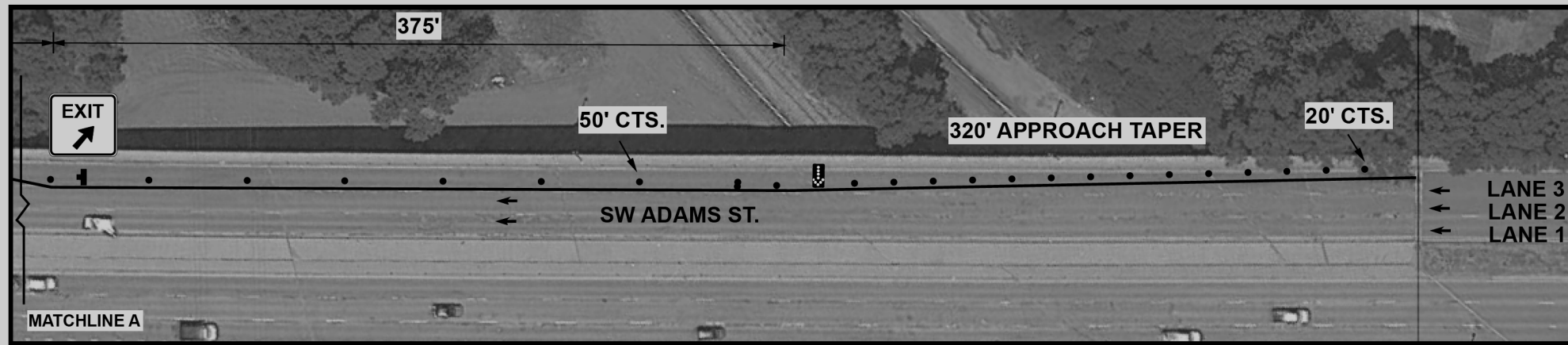
RAMP B CLOSURE - I-474 WB TO US 24 DETOUR

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	181
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 68887

PAINTING SN 072-0129 SN 072-0130 STAGE 1



STAGE 1 PEDESTRIAN ACCESS

PEDESTRIAN ACCESS SHALL BE MAINTAINED AT ALL TIMES UTILIZING THE EXISTING SIDEWALK. PEDESTRIANS SHALL BE PROTECTED FROM ALL BLASTING AND PAINTING OPERATIONS-INCLUDING THE BUILDING OF CONTAINMENT- AT ALL TIMES. THERE SHALL BE NO OBSTRUCTIONS ON THE SIDEWALK FROM ANY WORK ACTIVITIES. LIT DEVICES SHALL BE PLACED 25' IN ADVANCE OF THE PEDESTRIAN OPENING ON BOTH APPROACH SIDES AND IN BETWEEN THE STRUCTURES. THE DIAGRAM IS AN EXAMPLE OF THE MINIMUM DIMENSIONS REQUIRED. THE CONTRACTOR SHALL SUBMIT A PEDESTRIAN DETAIL AND COORDINATE AN APPROVAL WITH THE RESIDENT ENGINEER.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

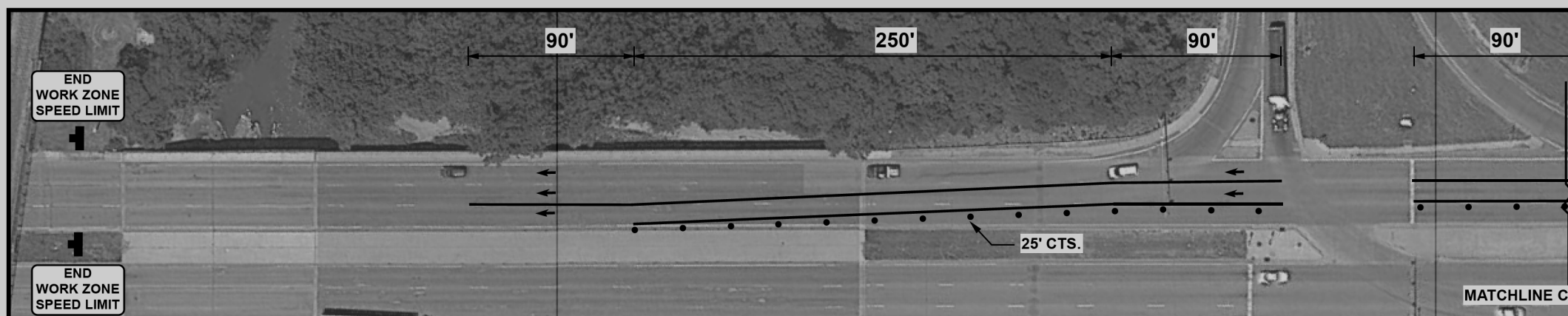
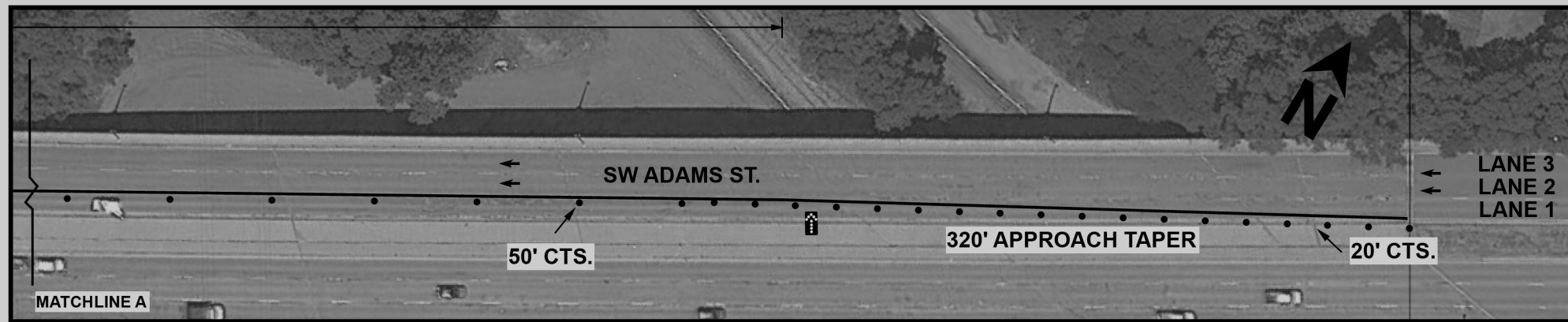
STAGING DETAIL FOR PAINTING SN 072-0129 & 072-0130

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PLOT DATE = 12/13/2017	DATE -	REVISED -

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	182
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68887	

PAINTING SN 072-0129 SN 072-0130 STAGE 2



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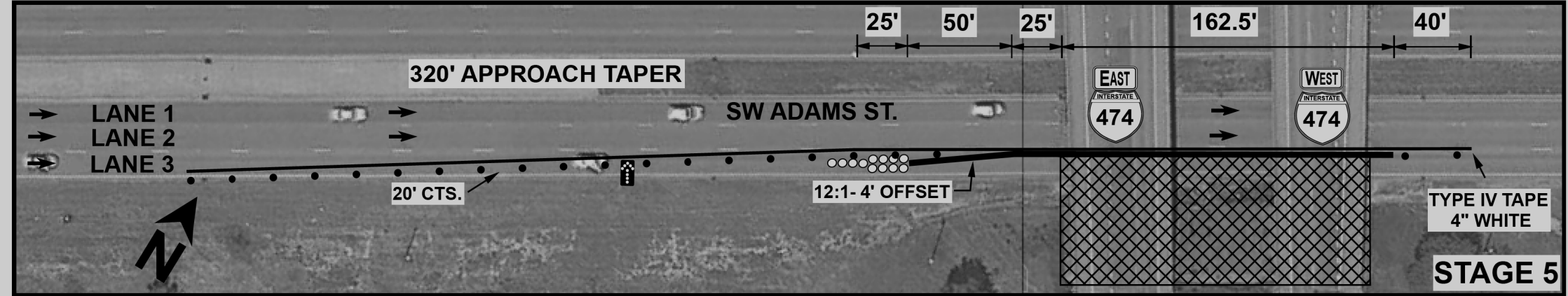
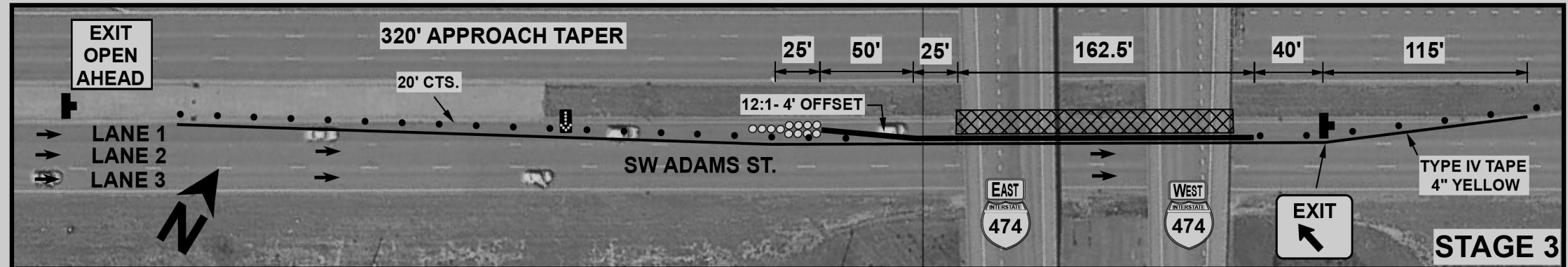
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR PAINTING SN 072-0129 & 072-0130

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	186	183
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



PAINTING
SN 072-0129
SN 072-0130
STAGE 3
STAGE 5

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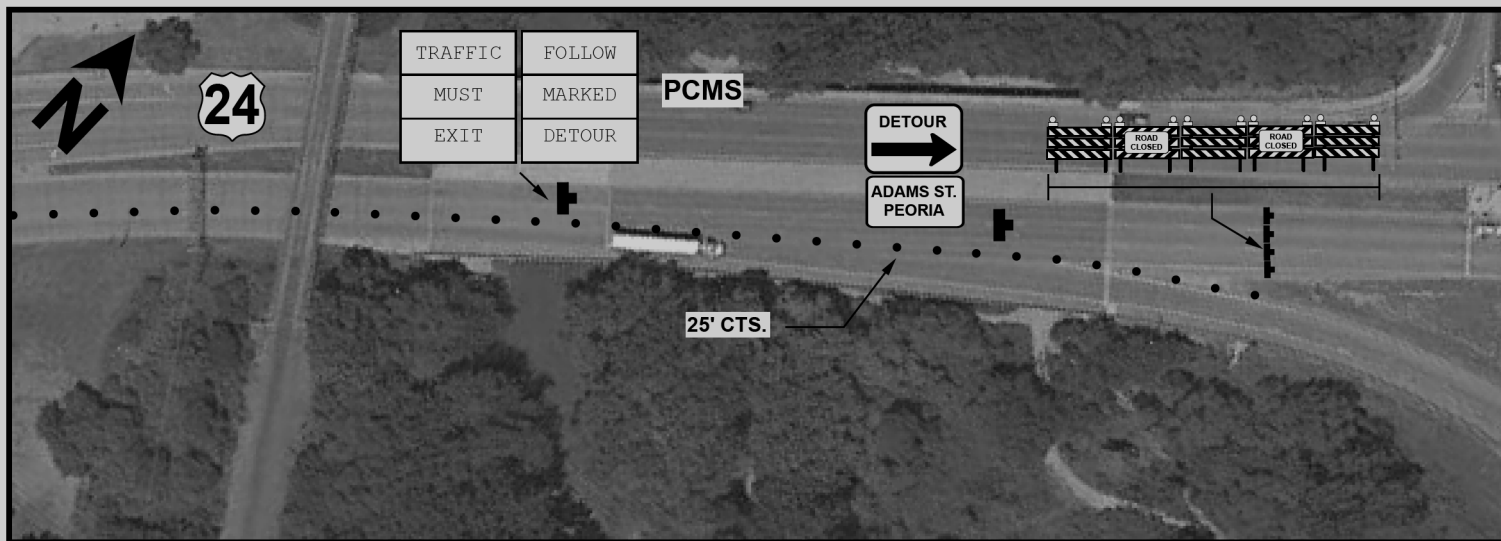
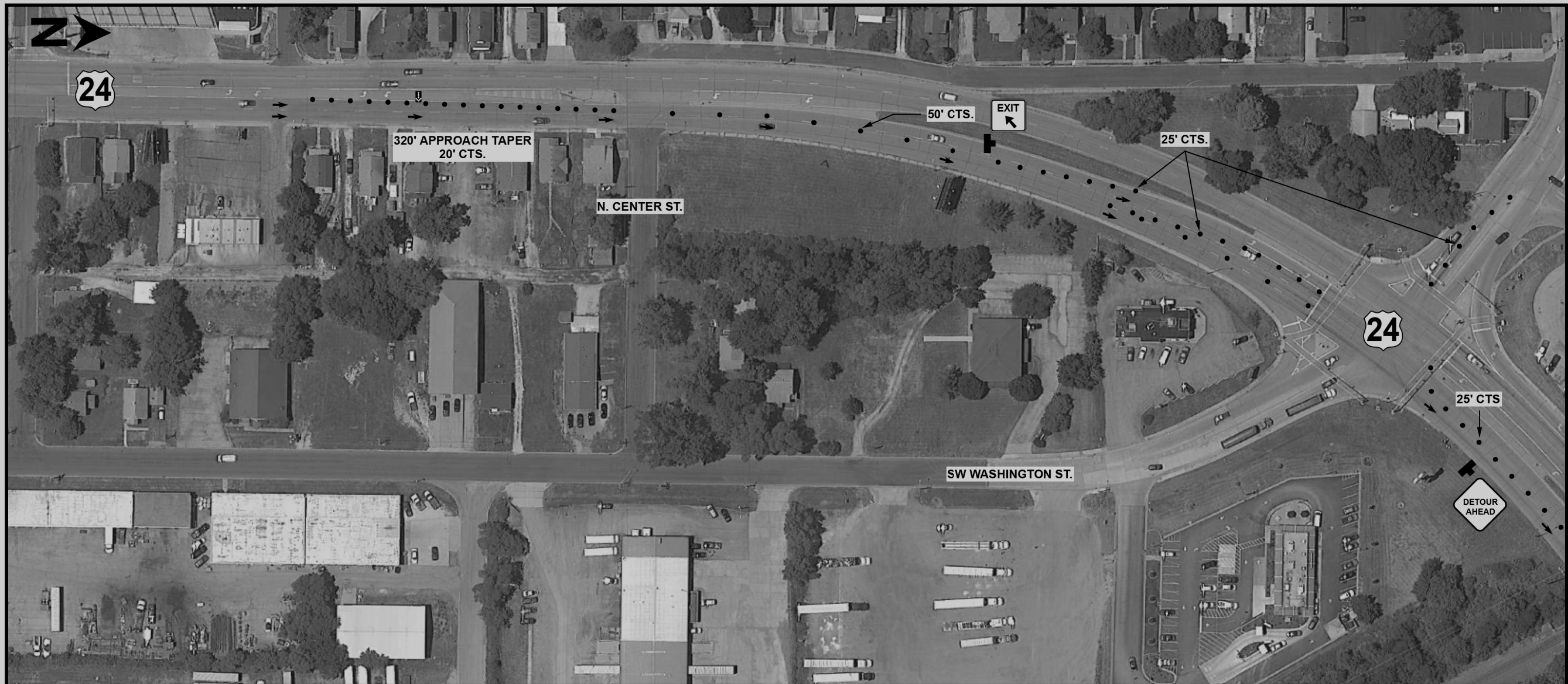
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PLOT DATE = 12/13/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR PAINTING SN 072-0129 & 072-0130

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	184
CONTRACT NO. 68887				
ILLINOIS FED. AID PROJECT				



**PAINTING
SN 072-0129
SN 072-0130
STAGE 4 1/3**

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAIL FOR PAINTING SN 072-0129 & 072-0130

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	185
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68887	



PAINTING
SN 072-0129
SN 072-0130
STAGE 4 2/3

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAIL FOR PAINTING SN 072-0129 & 072-0130

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	186
			CONTRACT NO. 68887	
		ILLINOIS	FED. AID PROJECT	

CONSTRUCTION NOTES	
1.	EXISTING UTILITY LOCATION INFORMATION IS NOT SHOWN ON THE PLAN SHEETS. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF ALL UTILITIES AND PRIVATELY OWNED FACILITIES PRIOR TO THE INSTALLATION OF ANY COMPONENTS. THE CONTRACTOR SHALL VERIFY EXISTING FIELD CONDITIONS AND TERRAIN PRIOR TO COMMENCING WORK ON THE PROJECT.
2.	THE LOCATION OF ALL UTILITIES AND PRIVATELY OWNED FACILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO THE INSTALLATION OF ANY COMPONENTS.
3.	THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING EXISTING IDOT ELECTRICAL FACILITIES AT HIS/HER OWN EXPENSE IF REQUIRED. THE CONTRACTOR SHALL ALSO BE LIABLE FOR ANY DAMAGE TO IDOT FACILITIES RESULTING FROM INACCURATE LOCATING.
4.	ELECTRICAL WORK SHALL CONFORM WITH NATIONAL, STATE, AND LOCAL CODES.
5.	THE EXISTING LIGHTING SHALL REMAIN IN OPERATION AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL FURNISH AND INSTALL ALL ITEMS REQUIRED TO MAINTAIN LIGHTING OPERATION, INCLUDING, BUT NOT LIMITED TO TEMPORARY WIRING, SPLICES, CIRCUITS, AND ALL OTHER ITEMS REQUIRED FOR OPERATION.
6.	ANY MAINTENANCE OF EXISTING LIGHTING WILL BE CONSIDERED EXTRA WORK IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
7.	ALL LIGHT POLES SHALL BE BONDED IN ACCORDANCE WITH NEC REQUIREMENTS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED LIGHT POLES.
8.	ALL SURPLUS MATERIALS SHALL BE DISPOSED OF IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.
9.	THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTING FIELD CONDITIONS, LIGHT POLE BOLT CIRCLES, ANCHOR BOLTS DIAMETERS, AND BOLT PROJECTIONS, PRIOR TO ORDERING MATERIALS. THIS WORK SHALL BE INCLUDED IN THE PRICE OF THE PROPOSED LIGHT POLES.
10.	THE CONTRACTOR SHALL REMOVE THE EXISTING LIGHT POLES AND LUMINAIRES AND DISPOSE OF THEM OFF OF THE JOB SITE. THIS WORK SHALL BE INCLUDED IN THE PRICE OF THE PROPOSED LIGHT POLES. THE CONTRACTOR SHALL REFLECT THE SALVAGE VALUE OF THE LIGHT POLES IN THE BID PRICE.
11.	REMOVAL OF EXISTING ITEMS (LIGHT POLES, LUMINAIRES, WIRING, ETC.) WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES FOR THE PROPOSED ITEMS.
12.	THE EXISTING FUSHOLDERS, FUSES, SURGE ARRESTORS, AND POLE WIRING MAY BE REUSED PROVIDED THAT THEY ARE OPERATIONAL AND UNDAMAGED. THE CONTRRCTOR SHALL FURNISH AND INSTALL NEW COMPONENTS IN THE EVENT THAT THESE ITEMS ARE DAMAGED OR NON-FUNCTIONAL. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PAY ITEM FOR THE PROPOSED LUMINAIRES.
13.	THE CONTRACTOR SHALL FURNISH AND INSTALL ALL ITEMS REQUIRED TO ATTACH THE CONDUITS AND JUNCTION BOXES TO THE STRUCTURE, INCLUDING, BUT NOT LIMITED TO UNI-STRUT, BRACKETS, SEAL-TITE, LBS, FITTINGS, HARDWARE, AND OTHER MISCELLANEOUS ITEMS. THESE ITEMS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE BID PRICE FOR THE CONDUIT ATTACHED TO STRUCTURE PAY ITEM.
14.	CONDUIT ATTACHMENT BRACKETS SHALL BE INSTALLED AT 8 FT. SPACINGS (MAXIMUM) ON STRUCTURES.
15.	THE CONTRACTOR WILL BE ALLOWED TO SUBSTITUTE NON-METALLIC SEAL TITE CONDUIT IN LEIU OF METALLIC SEAL-TITE CONDUIT.
16.	THE CONTRACTOR SHALL GROUND ALL EXPOSED STEEL CONDUITS IN ACCORDANCE WITH NEC REQUIREMENTS. THE CONTRACTOR SHALL MAINTAIN THE CONTINUITY OF THE GROUND SYSTEM WHEN USING NON-METALLIC SEAL-TITE CONDUIT BY INSTALLING A #6 GROUNDING CONDUCTOR INSIDE THE SEAL-TITE AND BONDING THIS WIRE TO THE GALVANIZED STEEL CONDUITS AT EACH END. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE CONDUIT PAY ITEMS.
17.	THE CONTRACTOR SHALL SPLICE ELECTRICAL CABLES IN JUNCTION BOXES AND LB FITTINGS AS REQUIRED TO REPLACE CONDUITS ATTACHED TO STRUCTURE AND SEAL-TITE CONDUIT. THE COST OF THESE SPLICES SHALL BE INCLUDED IN THE COST OF THE PROPOSED CONDUIT.
18.	ALL SEAL-TITE CONDUIT SHALL INCLUDE CONNECTOR WITH STAINLESS STELL KELLUM GRIPS AT THE ENDS FOR INCREASED STRENGTH AND DURABILITY
19.	ALL CONDUIT ATTACHMENT BRACKETS SHALL BE FULLY GALVANIZED AND ALL HARDWARE SHALL BE EITHER GALVANIZED OR STAINLESS STEEL.
20.	THE CONTRACTOR SHALL INSTALL THREAD LOCKER ON ALL ATTACHED CONDUIT THREADED CONNECTIONS TO PREVENT LOOSENING THROUGH VIBRATION.

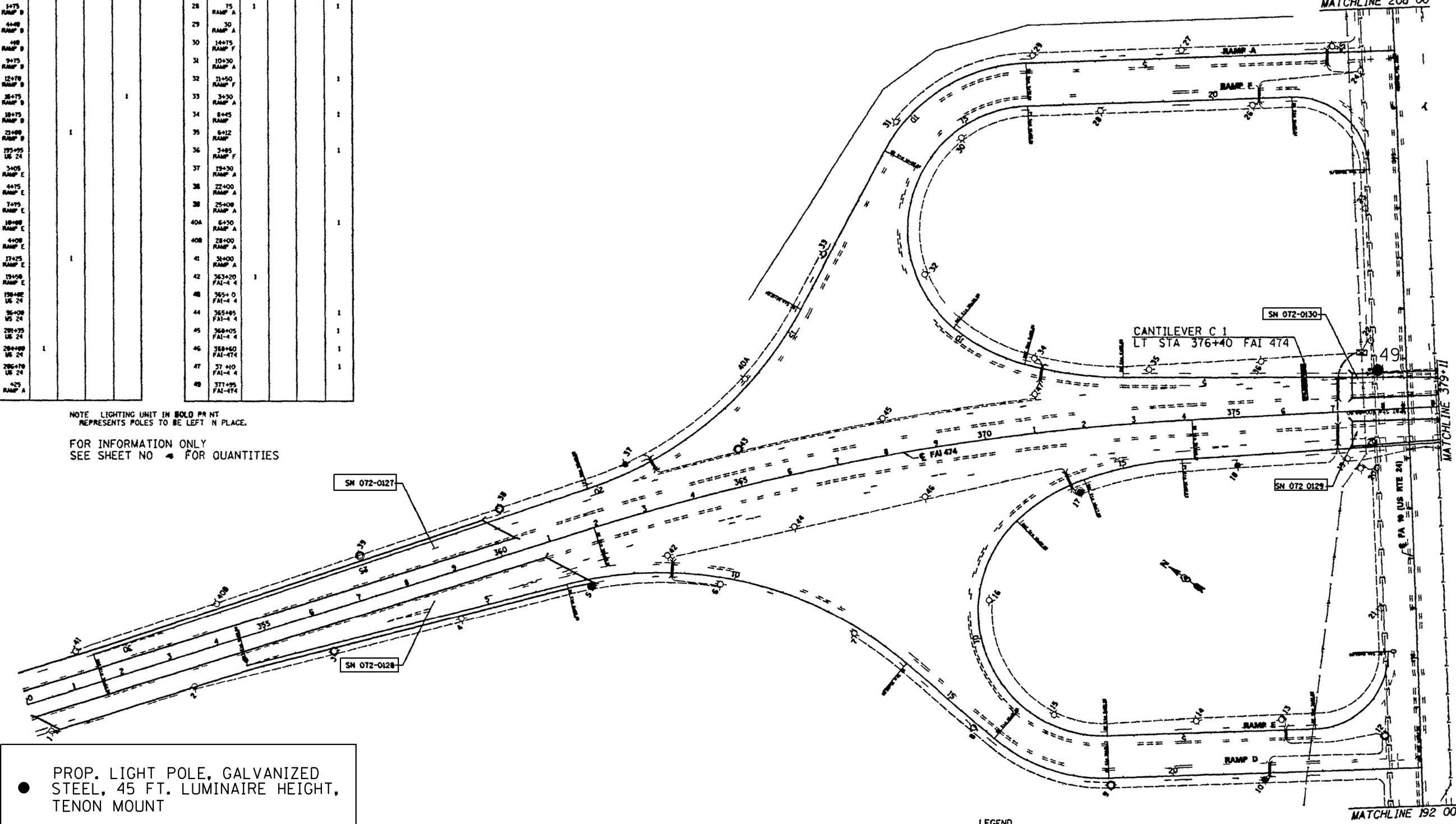
BILL OF MATERIALS I-474 OVERHEAD LIGHTING		
ITEM DESCRIPTION	UNIT	TOTAL QTY.
JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 6" X 6" X 4"	EACH	8.0
LIGHT POLE, GALVANIZED STEEL, 45 FT. M.H., TENON MOUNT	EACH	13.0
REMOVE CONDUIT ATTACHED TO STRUCTURE	FOOT	200.0
CONDUIT, FLEXIBLE, LIQUID TIGHT, METALLIC, 2" DIAMETER	FOOT	45.0
CONDUIT ATTACHED TO STRUCTURE, 2" DIA., STAINLESS STEEL	FOOT	150.0

P.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72 4L&90-SL	PEORIA	23	5
STA.		TO STA.		
FED. AID PROJ. NO.		FED. AID PROJECT		

LIGHT NO.	STATION	45 FT	55 F	400 W HPS	250 W HPS
2	358+38 FAI-474				
3	353+38 FAI-474				
4	3475 RAMP D				
5	3440 RAMP D				
6	3400 RAMP D				
7	3375 RAMP D				
8	3340 RAMP D				
9	3300 RAMP D				
10	3275 RAMP D				
11	3240 RAMP D				
12	3200 RAMP D				
13	3175 RAMP D				
14	3140 RAMP D				
15	3100 RAMP D				
16	3075 RAMP D				
17	3040 RAMP D				
18	3000 RAMP D				
19	2975 RAMP D				
20	2940 RAMP D				
21	2900 RAMP D				
22	2875 RAMP D				
23	2840 RAMP D				
24	2800 RAMP D				
25	2775 RAMP D				

LIGHT NO.	STATION	45	55	400 W HPS	250 W HPS
26	20+75 RAMP				
27	4 30 RAMP A	1			
28	75 RAMP A	1			1
29	30 RAMP A				
30	14+75 RAMP F				
31	10+30 RAMP A				
32	11+50 RAMP F				1
33	3+30 RAMP A				
34	8+45 RAMP				1
35	6+12 RAMP				
36	3+85 RAMP F				1
37	13+30 RAMP A				
38	22+00 RAMP A				
39	25+00 RAMP A				
40A	6+50 RAMP A				1
40B	28+00 RAMP A				
41	31+00 RAMP A				
42	263+20 FAI-4 4	1			
43	265+0 FAI-4 4				
44	265+85 FAI-4 4				1
45	268+05 FAI-4 4				1
46	268+60 FAI-474				1
47	37+10 FAI-4 4				1
48	377+95 FAI-474				1

NOTE: LIGHTING UNIT IN BOLD PRINT REPRESENTS POLES TO BE LEFT IN PLACE.
FOR INFORMATION ONLY SEE SHEET NO. 4 FOR QUANTITIES



● PROP. LIGHT POLE, GALVANIZED STEEL, 45 FT. LUMINAIRE HEIGHT, TENON MOUNT

- LEGEND**
- LIGHTING UNIT 400 W HPS 45 MH
 - LIGHTING UNIT 250 W HPS 45 MH
 - ELECTRIC CABLE UNIT DUCT 2-600 V6 P & 1 1/2 XLPG
 - ⊠ 200 AMP 240 480V 1Ø POWER SUPPLY
 - GALVANIZED STEEL CONDUIT PUSH
 - LIGHT POLE TO REMAIN
 - 41 LIGHT POLE NUMBER

BILL OF MATERIALS	UNIT	TOTAL QTY.
LIGHT POLE, GALVANIZED STEEL, 45 FT. M.H., TENON MOUNT	EACH	1.0

REVISIONS	
NAME	DATE

NOTE FOR BILL OF MATERIALS SEE SCHEDULE OF QUANTITIES SHEET NO. 4

ILLINOIS DEPARTMENT OF TRANSPORTATION

LIGHTING PLAN
PEORIA COUNTY

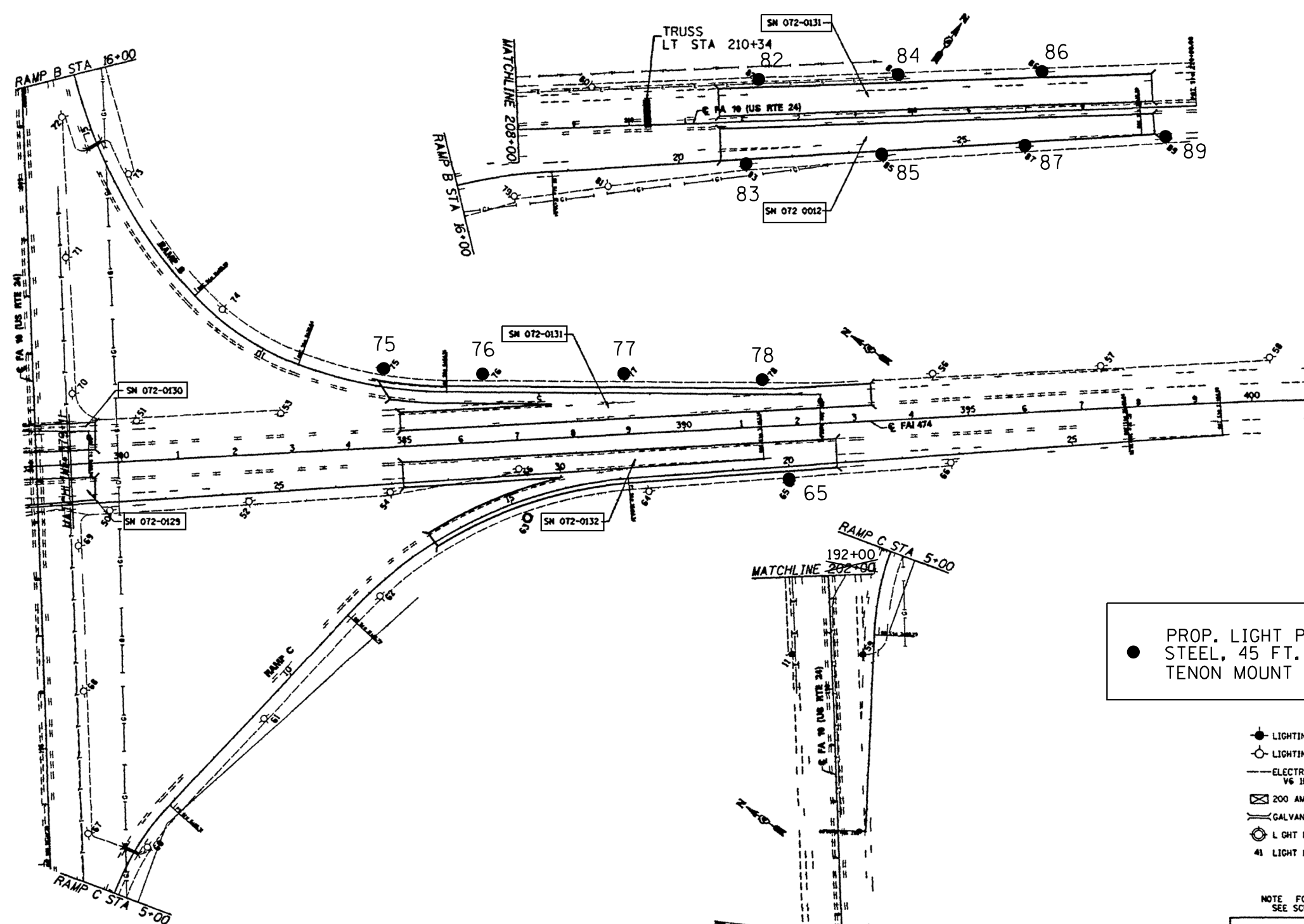
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FOR INFORMATION ONLY
SEE SHEET NO 4 FOR QUANTITIES

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72-4L&90-GL	PEORIA	23	19
STA. TO STA.			
FILE NO. PROJ. NO.		FILE NO. PROJECT	

LIGHT NO.	S. AT/WH	45 F	55 FT	400 W HPS	250 W HPS
50	30+63 US 24				
51	329+75 FAI 474				
52	386+30 FAI-4 4				
53	382 28 FAI-474				
54	382+83 FAI-4				
55	384+70 FAI-474				
56	38 400 FAI-474				
57	354+40 FAI-47				
58	397 35 FAI-4 4				
59	408 35 FAI-474				
60	545 RAMP C				
61	6+00 RAMP C				
62	8+18 RAMP C				
63	12+18 RAMP C				
64	17+50 RAMP C				
65	20+00 RAMP				
66	22+85 RAMP C				
67	183+45 US 24				
68	196+00 US 24				
69	38-5 US 24				
70	201+38 US 24				
71	203+75 US 24				
72	206+15 US 24				
73	34+00 RAMP B				
74	38+00 RAMP B				
75	485 RAMP B				
76	6+00 RAMP B				
77	3+47 RAMP B				
78	400 RAMP B				
79	408 RAMP B				
80	203+35 US 24				
81	203+60 US 24				
82	212 31.44 US 24				
83	211+98.76 US 24				
84	214+73.23 US 24				
85	214+50 US 24				
86	21 30.01 US 2				
87	216+98.31 US 24				
88	228+08 US 24				
89	219+58 US 24				

NOTE: LIGHTING UNIT IN BOLD PRINT REPRESENTS POLES TO BE LEFT IN PLACE.



● PROP. LIGHT POLE, GALVANIZED STEEL, 45 FT. LUMINAIRE HEIGHT, TENON MOUNT

- LEGEND**
- LIGHTING UNIT 400 W HPS 45 MH
 - LIGHTING UNIT 250 W HPS 45 MH
 - ELECTRIC CABLE UNIT DUCT 2-600 V6 IP & 1 #6 XLPG
 - ⊠ 200 AMP 240/480V 1Φ POWER SUPPLY
 - GALVANIZED STEEL CONDUIT PUSH
 - LIGHT POLE TO REMAIN
 - 41 LIGHT POLE NUMBER

NOTE FOR BILL OF MATERIALS SEE SCHEDULE OF QUANTITIES SHEET NO 4

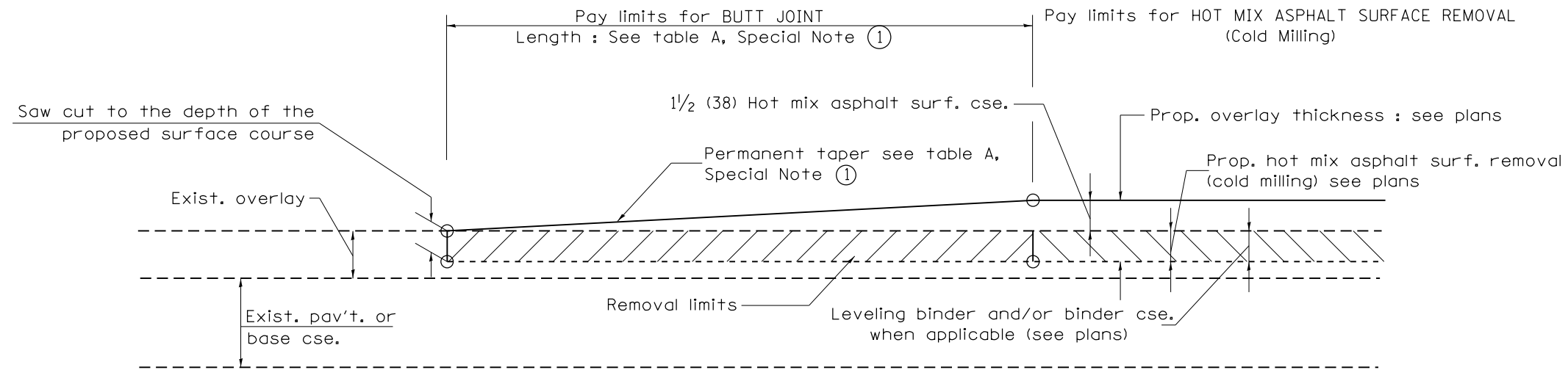
ILLINOIS DEPARTMENT OF TRANSPORTATION

LIGHTING PLAN
PEORIA COUNTY

REVISIONS	
NAME	DATE

SCALE VERT HOR Z
DATE DRAWN BY CHECKED BY

BILL OF MATERIALS	UNIT	TOTAL QTY.
LIGHT POLE, GALVANIZED STEEL, 45 FT. M.H., TENON MOUNT	EACH	12.0



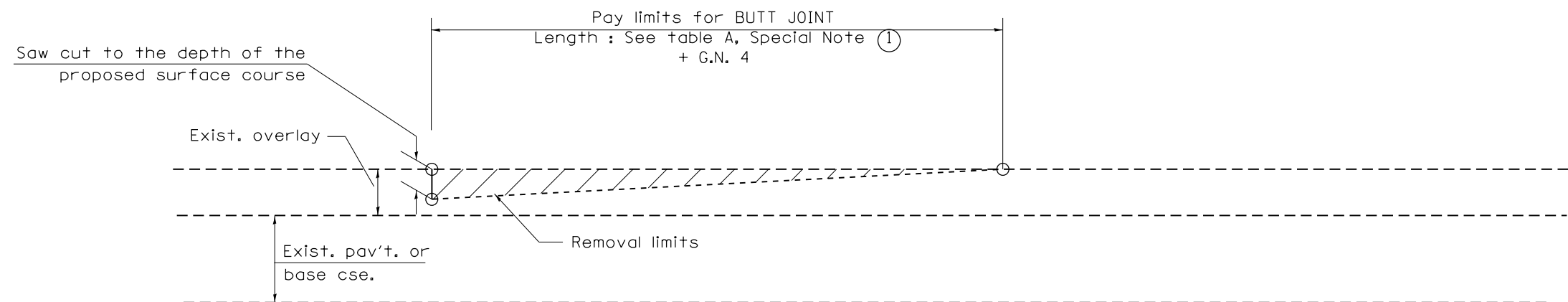
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.

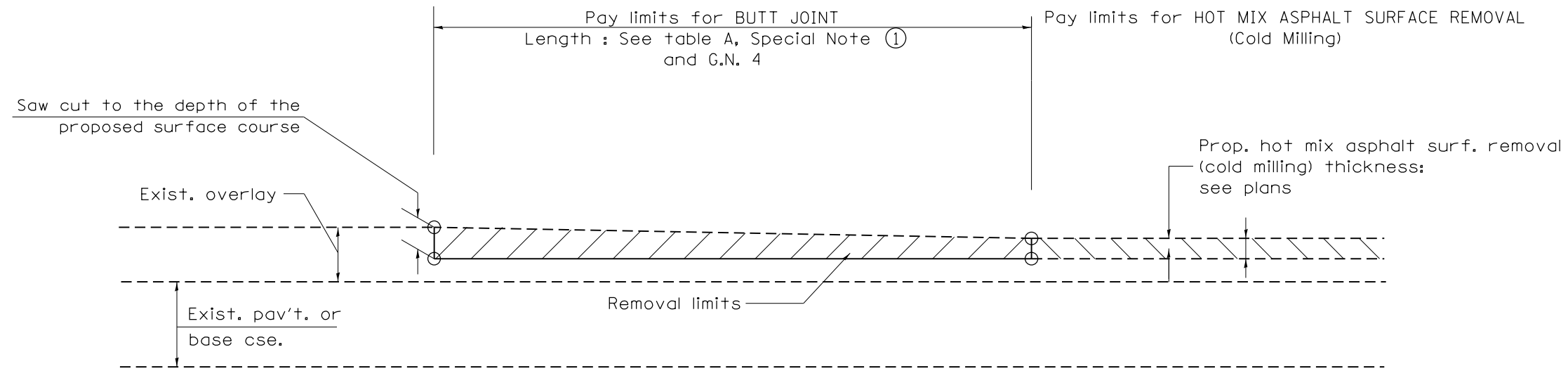


CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

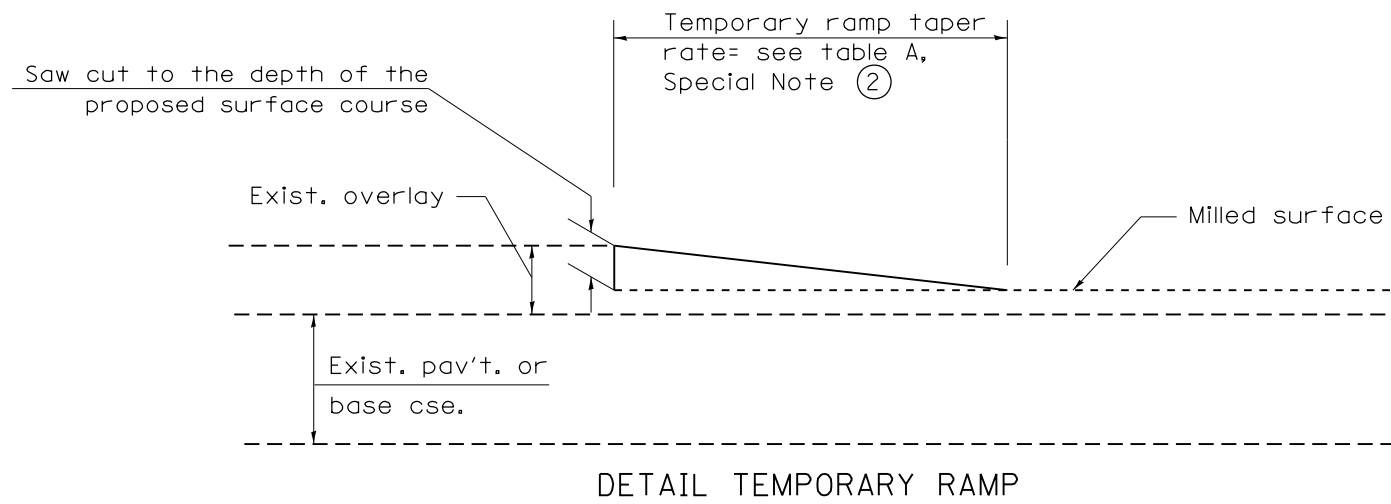
All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-23.01, NEW REVISION BOX	T.P.	08-21-13	MAJOR MODIFICATIONS	R.D.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	SHT. 1 OF 3 CADD STD. 406101-D4	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
04-01-97	CORRECTION TO DEPTH	J.A.	02-29-16	MINOR CORRECTIONS	R.D.				474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	191
09-15-05	REVISED DESIGNER NOTE	M.M.A.	04-12-16	MINOR CORRECTIONS	R.D.				CONTRACT NO. 68887				
10-16-06	REVISED TO 2007 SPEC.	M.A.	02-14-17	ADDED NOTE 5	R.D.				FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

NOT TO SCALE



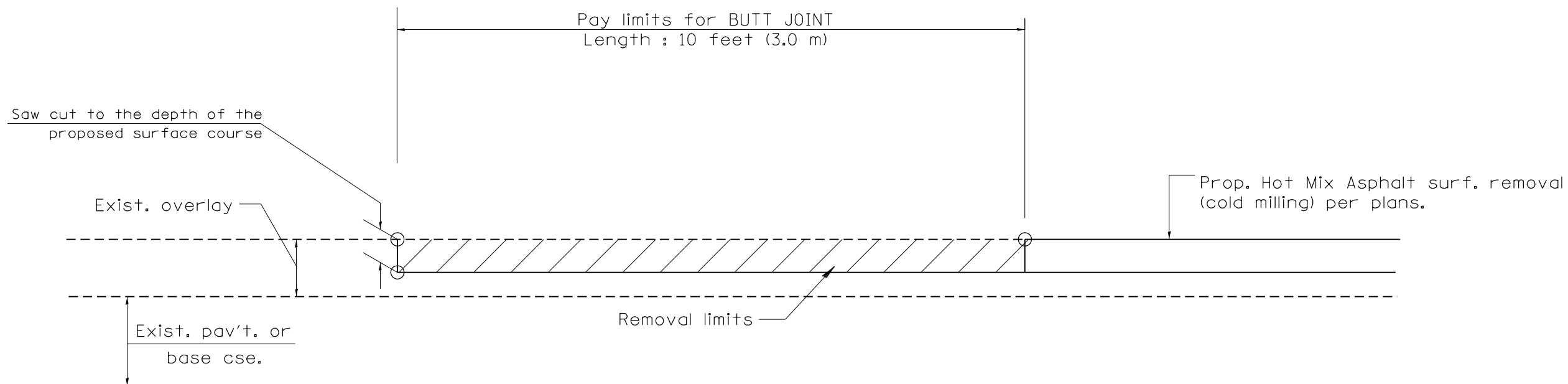
CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER



All dimensions are in inches (millimeters) unless otherwise noted.

				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINTS		SHT. 2 OF 3 CADD STD. 406101-D4	
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.					
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	192	CONTRACT NO. 68887				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT							

NOT TO SCALE



CASE 4 : SINGLE LIFT OVERLAY WITH EQUIVALENT DEPTH
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters) unless otherwise noted.

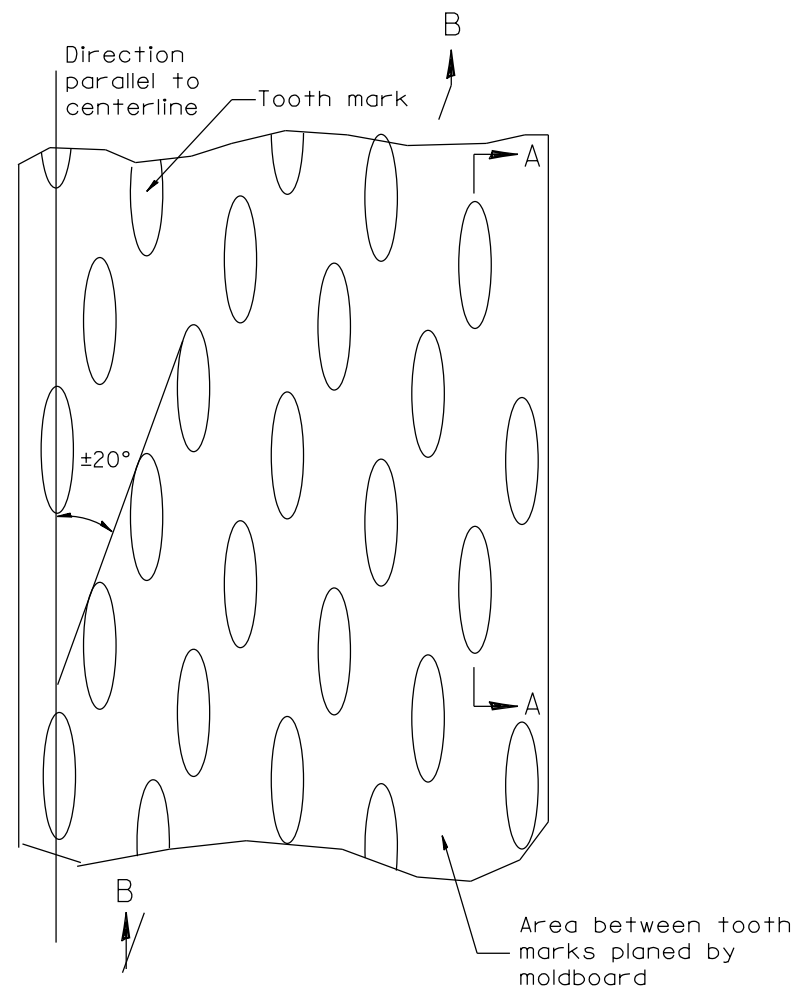
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINTS

NOT TO SCALE

SHT. 3 OF 3
CADD STD. 406101-D4

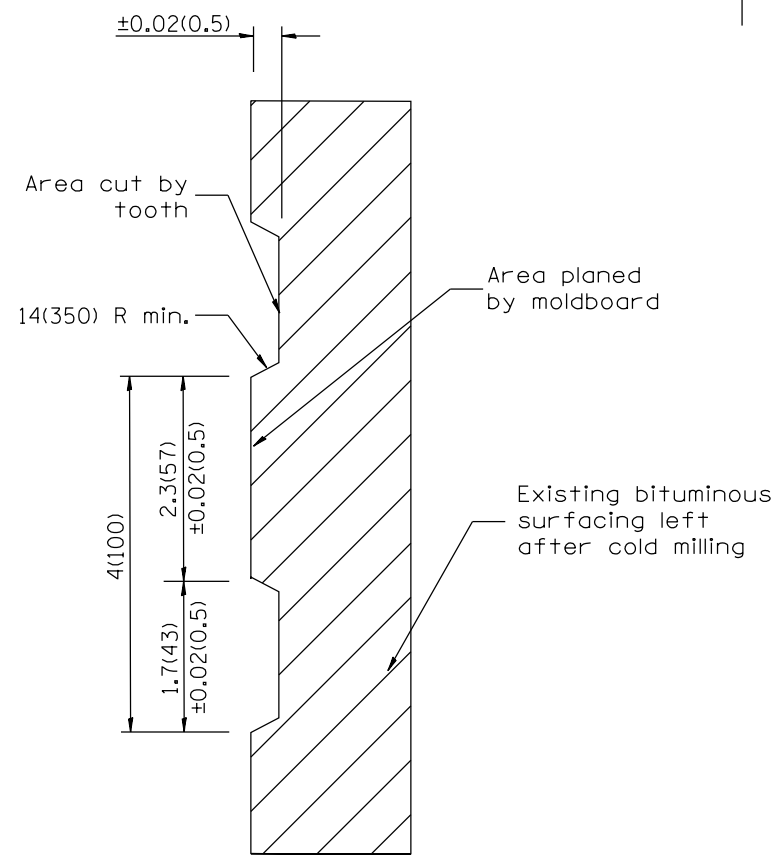
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	193
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 68887	



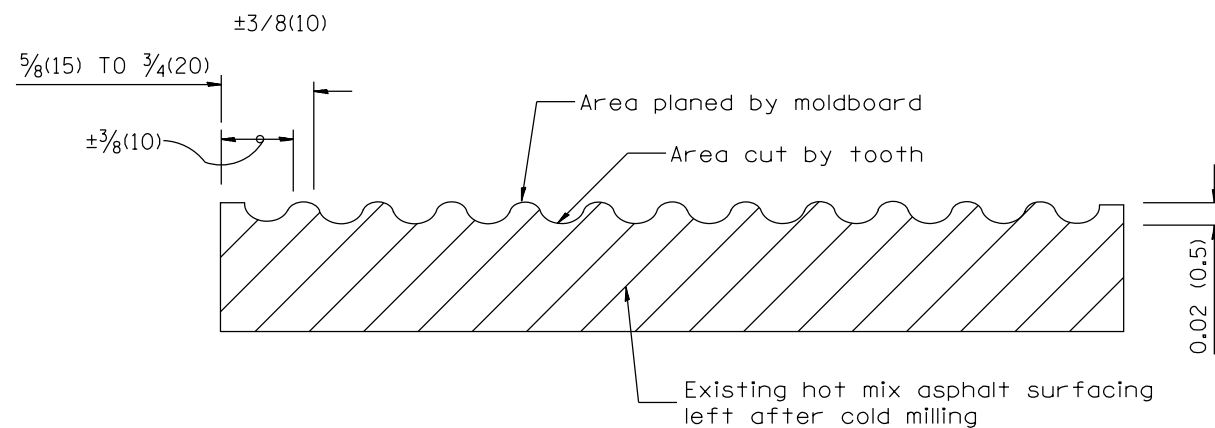
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED PERPENDICULAR TO CENTERLINE

DESIGNER NOTES:
1. INCLUDE DISTRICT SPECIAL PROVISION, IF APPLICABLE.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

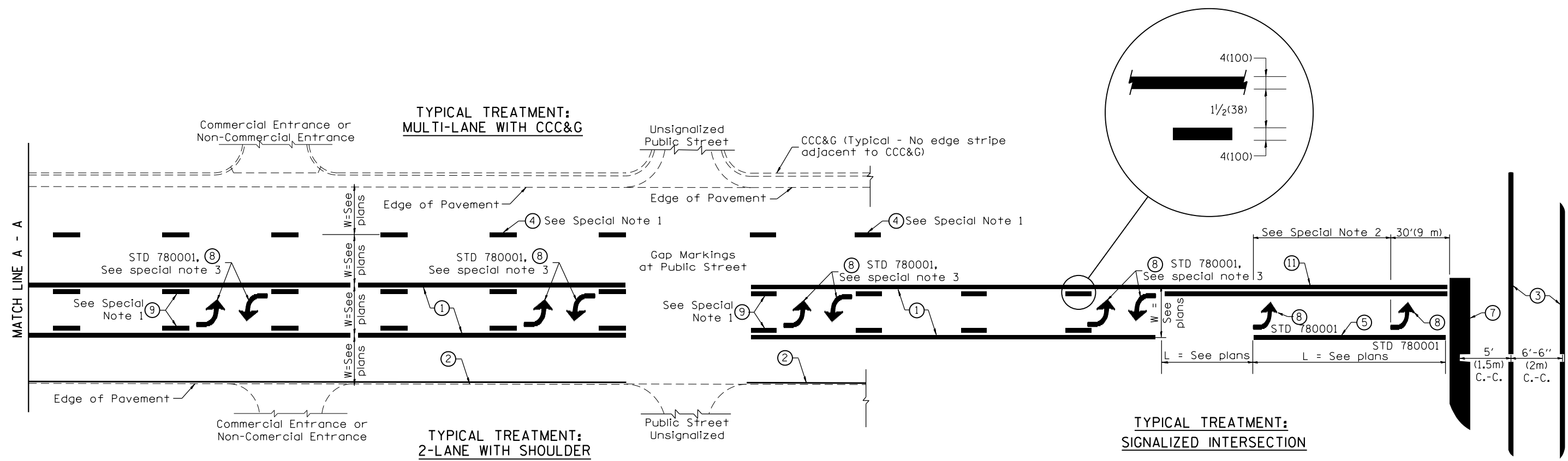
HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

NOT TO SCALE

CADD STD. 440001-D4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	194
CONTRACT NO. 68887				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DESIGNER NOTES:
1. Include State Standard 780001 (Typical Pavement Markings)



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND
(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) 45°
- ⑪ 4(100) Double Solid (Yellow) 11(280) C.-C. See Table A

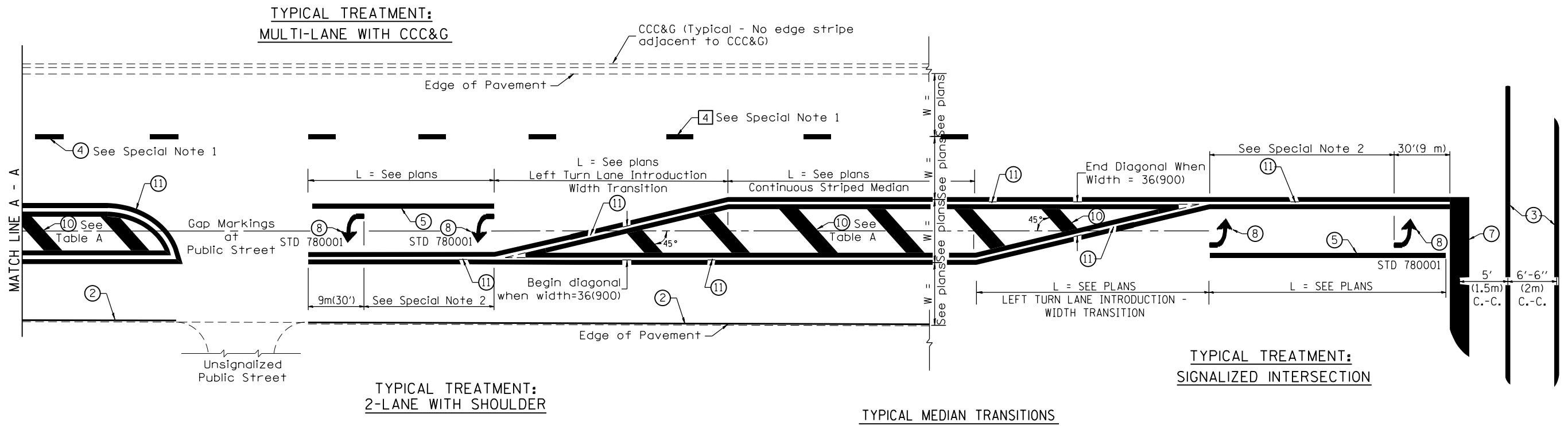
SPECIAL NOTES

- Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
- The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
- The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

- Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
- See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
- Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
- Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

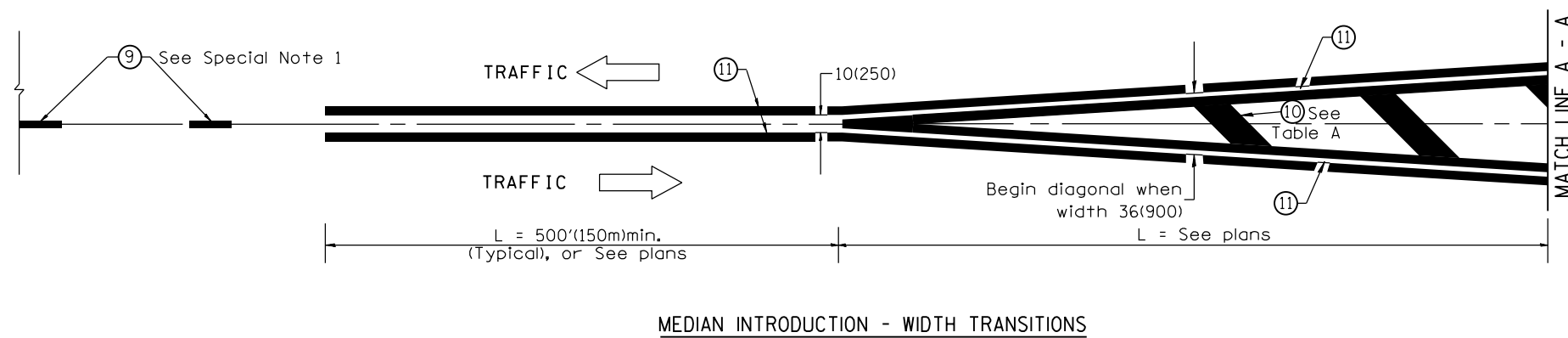
01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKINGS	F.A.I	SECTION	COUNTY	TOTAL	SHEET	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.			474	72-4(HB,HVB-1,HVB)B-R	PEORIA	196	195	
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.						SHT. 1 OF 2 CADD STD. 780001-D4					
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.						CONTRACT NO. 68887					
								FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.